

017

03-06-2020 LETTING ITEM 017

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F A P R T E	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	1
		ILLINOIS	CONTRACT NO. 62J20	

*44 + 1 = 45 TOTAL SHEETS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF
SUGAR GROVE

TRAFFIC DATA

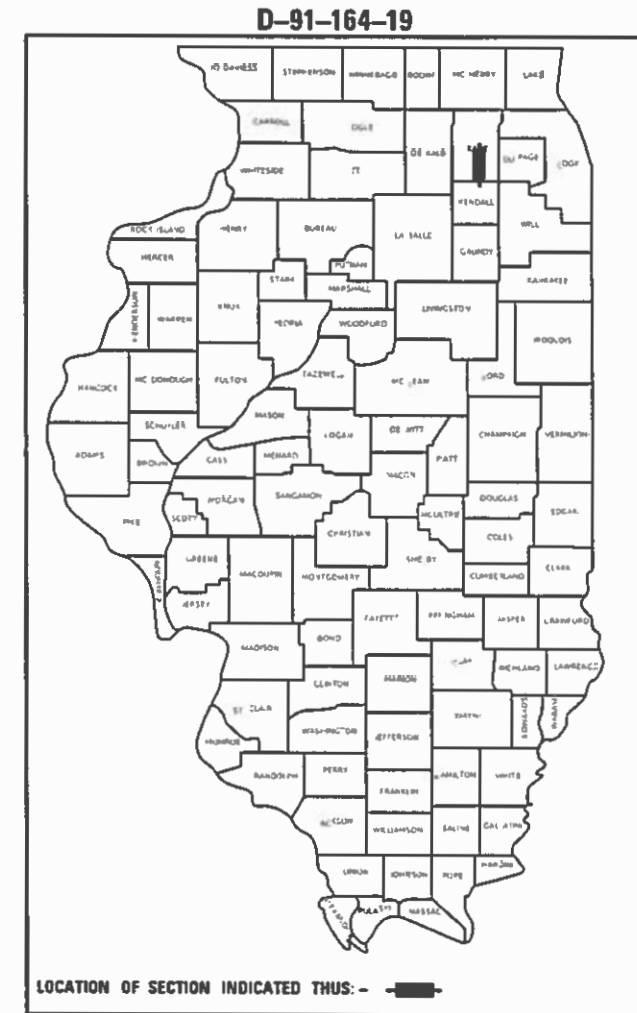
ADT 21,900 (2017)

POSTED SPEED LIMIT

45 MPH - 55 MPH

PROPOSED
HIGHWAY PLANS

FAP ROUTE 326: IL 47 - US 30
FINLEY RD. TO BASE LINE RD/KENDALL CO LINE
SECTION 2019-042-RS
PROJECT NHPP-D8L5 (738)
STANDARD OVERLAY, MILLED RUMBLE STRIP
AND PEDESTRIAN RAMPS
KANE COUNTY



PROJECT ENDS
385 + 44.7

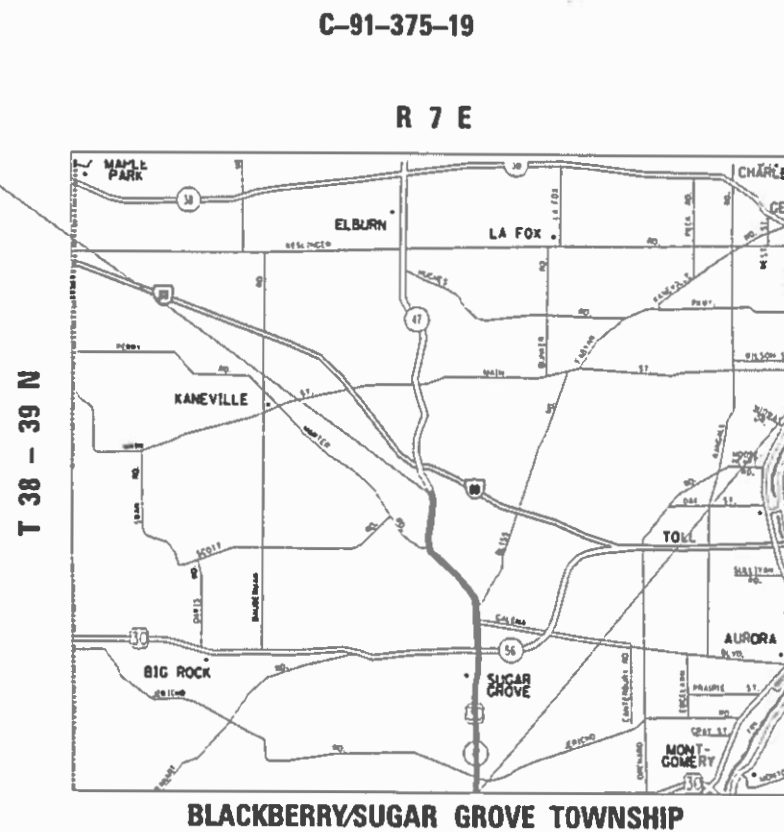
OMISSION:
STA 337 + 77.4 TO
STA 339 + 88.0

PROJECT BEGINS
15 + 56.2



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811



GROSS LENGTH = 36,988.5 FT. = 7.005 MILE
NET LENGTH = 36,777.9 FT. = 6.966 MILE

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62J20

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED ~~December 10, 2019~~
Quddy A. Awad / OAS REGIONAL ENGINEER
Jan 31 2020
Joe A. Elk ENGINEER OF DESIGN AND ENVIRONMENT
Jan 31 2020
James J. Gorman DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PREPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-05	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-12	STEEL PLATE BEAM GUARDRAIL
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
642006	SHOULDER RUMBLE STRIP, 8 IN.
701006-05	OFF RD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701011-04	OFF RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY FOR SPEEDS > 45 MPH
701336-07	LANE CLOSER, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS > 45 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS > 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-09	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE VILLAGE OF SUGAR GROVE.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA).
5. THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL."
6. SIDEWALK RAMPS MODIFICATIONS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO THE APPLICABLE DETAILS INCLUDED IN THE PLANS.
7. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
8. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
9. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE DIRECTED BY THE ENGINEER.
10. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
12. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
13. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
14. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
15. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
16. THE ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT(847) 741-9857 OR DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
17. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.
18. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
19. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
20. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
21. THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
22. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
23. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
24. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
25. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V+H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
26. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

USER NAME =	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEET			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN -	REVISED -		IL 47 (FINLEY TO BASE LINE RD)			326	2019-042-R5	KANE	44	2
PLOT SCALE =	CHECKED -	REVISED -					CONTRACT NO. 62J20				
PLOT DATE =	DATE - 03/13/2018	REVISED -					SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE 0005			
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	10	10				
20200100	EARTH EXCAVATION	CU YD	6	6				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	100	100				
25003210	INTERSEEDING, CLASS 2A	ACRE	0.38	0.38				
25003310	INTERSEEDING, CLASS 4	ACRE	8.96	8.96				
25200110	SODDING, SALT TOLERANT	SO YD	100	100				
25200200	SUPPLEMENTAL WATERING	UNIT	5	5				
31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	550	550				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	178873	178873				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	380	380				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	843	843				
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	10443	10443				
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	745	745				
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	4838	4838				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	20249	20249				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE 0005			
42001300	PROTECTIVE COAT	SO YD	1700	1700				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	610	610				
* 42400800	DETECTABLE WARNINGS	SO FT	126	126				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	253154	253154				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	4650	4650				
44000600	SIDEWALK REMOVAL	SO FT	610	610				
44003100	MEDIAN REMOVAL	SQ FT	29700	29700				
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	1969	1969				
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	62	62				
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	2946	2946				
48101620	AGGREGATE SHOULDERS, TYPE B 10"	SO YD	16	16				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	589	589				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1				
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1				
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1				
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	4175	4175				
63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	18	18				
60619600	CONCRETE MEDIAN, TYPE SB 6.12	FOOT	475	475				* - SPECIALTY ITEMS

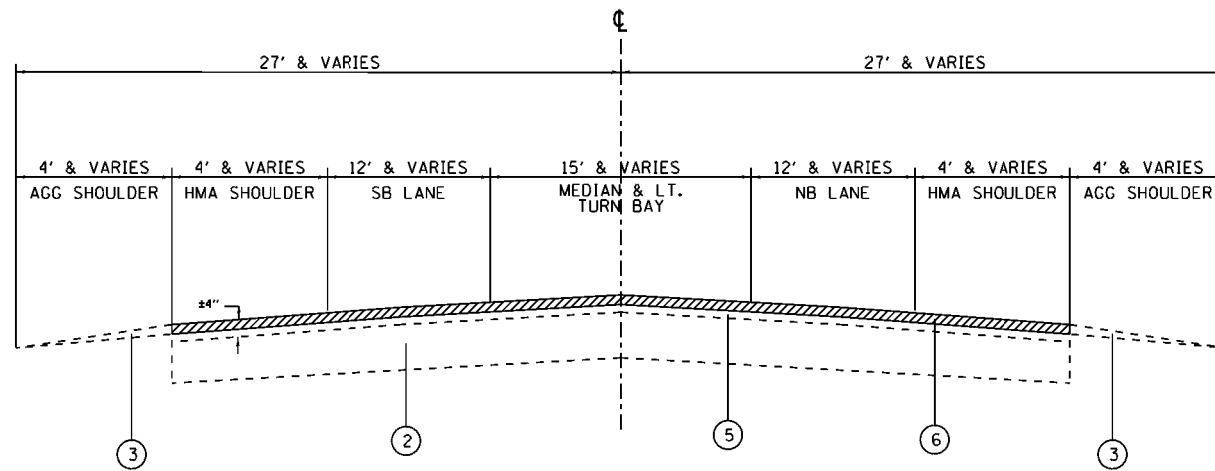
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PLOT SCALE = 100,0000' / 1in	CHECKED -	REVISED -	SCALE:			SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.	CONTRACT NO. 62J20		
PLOT DATE = 12/16/2019	DATE -	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE 0005					80% FEDERAL 20% STATE 0005		100% STATE 0005					
* 63000055	WEAK POST GUARDRAIL ATTACHED TO CULVERT CASE VI	FOOT	30	30					70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	1				
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1					70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1				
63200310	GUARDRAIL REMOVAL	FOOT	30	30					70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1				
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	61385	61385					70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	66217	66217					70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	6	6					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1					70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					70300100	SHORT TERM PAVEMENT MARKING	FOOT	133164	133164				
* 66901006	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	1	1					70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	11097	11097				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	2748	2748				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	140747	140747				
67100100	MOBILIZATION	L SUM	1	1					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	14211	14211				* - SPECIALTY ITEMS
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1													

FILE NAME =	USER NAME = allababdz	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL 47 (FINLEY RD TO BASE LINE RD) SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100,0000' / 1in	CHECKED -	REVISED -	CONTRACT NO. 62J20							
PLOT DATE = 12/16/2019	DATE -	REVISED -	SCALE:			SHEET NO. 2 OF 4 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

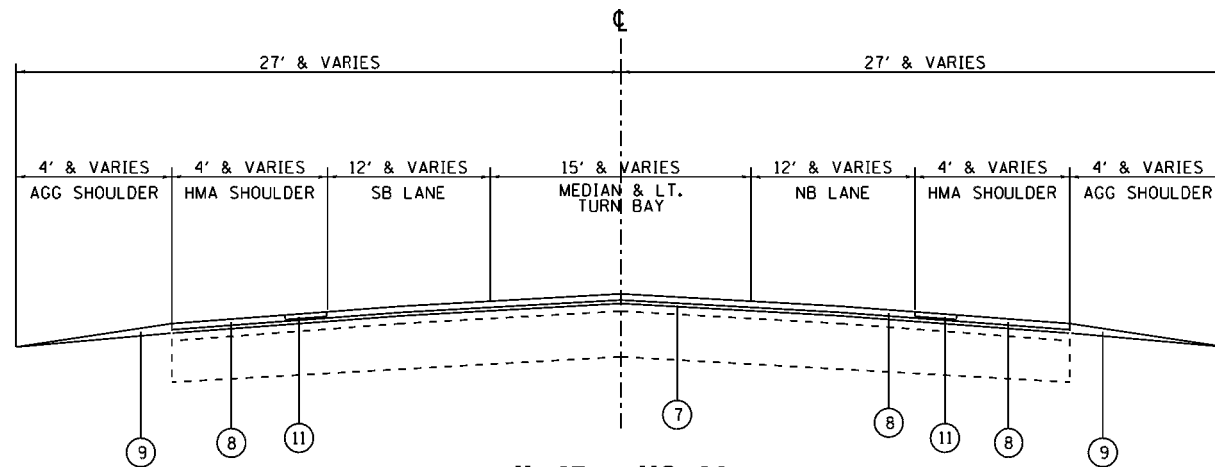
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70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2431	2431				* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	4723	4723					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	4577	4577				X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	808	808				X0326898	CENTER LINE - RUMBLE STRIP - 16"	FOOT	6850	6850					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	33291	33291				X0327120	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT	ACRE	9.34	9.34					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	2748	2748				X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	8.65	8.65					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	42400	42400				X2020110	GRADING AND SHAPING SHOULDERS	UNIT	327	327					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	14211	14211				X2503110	MOWING (SPECIAL)	ACRE	9.34	9.34					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2432	2432				X4400196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SO YD	7160	7160					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	4577	4577				X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	150	150					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	808	808				X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	11	11					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1822	1822				X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	64582	64582					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1822	1822				* X7800815	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH	FOOT	141302	141302					
								Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	600	600					

* - SPECIALTY ITEMS



**IL 47 – US 30
EXISTING TYPICAL SECTION**

13+72.9 TO 42+00
105+00 TO 117+00
123+60 TO 140+75



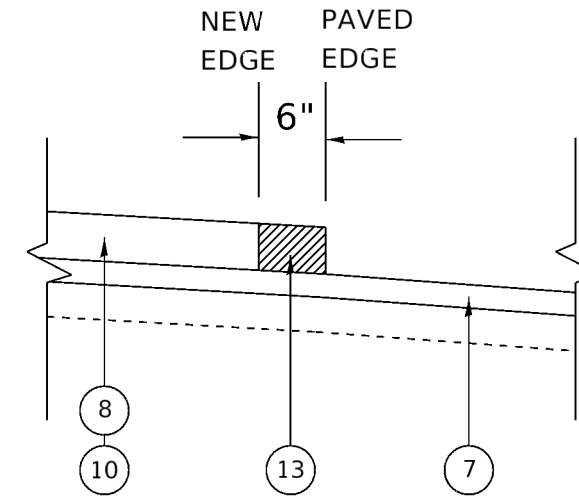
**IL 47 – US 30
PROPOSED TYPICAL SECTION**

13+72.9 TO 42+00
105+00 TO 117+00
123+60 TO 140+75

CENTERLINE RUMBLE STRIP, 16 INCH
STA 42+00 TO STA 105+00
STA 117+25 TO STA 123+50

SHOULDER RUMBLE STRIP, 8 INCH
STA 222+00 TO STA 325+00

SHOULDER RUMBLE STRIP, 16 INCH
STA 32+00 TO STA 107+00
STA 111+50 TO STA 133+00



DETAIL FOR PAVED EDGE /
HMA SURFACE REMOVAL (SPECIAL)

NOTE:
NOT APPLICABLE AT THE GUTTER EDGE

MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES	VOIDS @ Ndes	
PAVEMENT AND SHOULDER RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MX "F" N80	3.5% AT 80 GYR.	PFP
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50	4% AT 50 GYR.	QC/QA
PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N50, 4"	4% AT 50 GYR.	QC/QA
SHOULDERS (6' WIDTH AND GREATER)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5 N70	4% AT 70 GYR.	QC/QA
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50	3.5% AT 50 GYR.	QC/QA
PATCHING		
CLASS D PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.	QC/QA
QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); PAY FOR PERFORMANCE (PFP)		

NOTE:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ. YD. / IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS / SBR PG 76 -22" AND FOR NON POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

**THE CONTRACTOR SHALL MILL
FIRST BEFORE PATCHING**

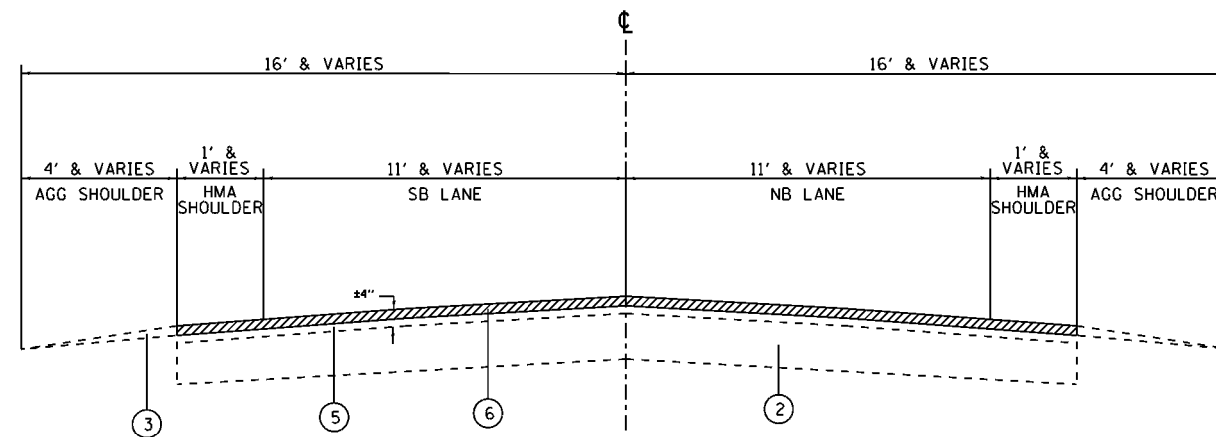
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USER NAME = Hassam	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/30/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

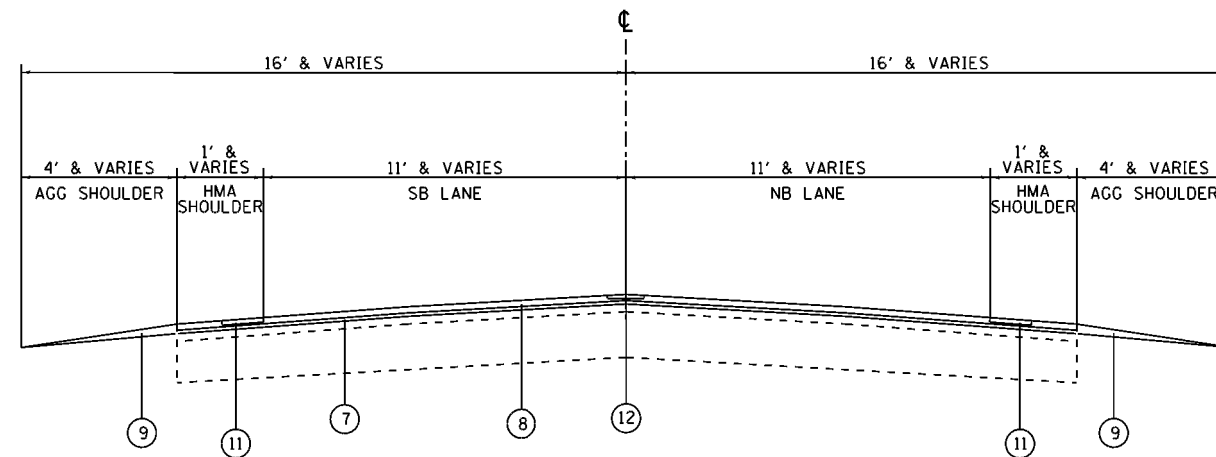
TYPICAL SECTIONS	
IL 47 (FINLEY RD TO BASE LINE RD)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	7
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				



**IL 77 - US 30
EXISTING TYPICAL SECTION**

42+00 TO 105+00
117+00 TO 123+60
140+75 TO 149+00
334+00 TO 385+45.3



**IL 77 - US 30
PROPOSED TYPICAL SECTION**

42+00 TO 105+00
117+00 TO 123+60
140+75 TO 149+00
334+00 TO 385+45.3

LEGEND

- ① EXISTING BITUMINOUS CONCRETE SURFACE ±7"
- ② EXISTING P.C.C. PAVEMENT ±10"
- ③ EXISTING AGGREGATE WEDGE SHOULDER
- ④ EXISTING BITUMINOUS CONCRETE SHOULDER 6' & VARIES
- ⑤ EXISTING H.M.A. SURFACE AFTER MILLING
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑦ PROPOSED POLYMERIZED HMA BINDER COURSE IL -4.75, N50, 3/4"
- ⑧ POLYMERIZED HMA SURFACE COURSE, SMA, 9.5 MIX "F" N80, 1 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER & GRADING & SHAPING SHOULDER
- ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N70, 1 3/4"
- ⑪ SHOULDER RUMBLE STRIPS
- ⑫ CENTERLINE RUMBLE STRIPS, 16 INCH
- ⑬ MEDIAN REMOVAL & COMB. CURB & GUTTER REMOVAL
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N50, 4"
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

CENTERLINE RUMBLE STRIP, 16 INCH
STA 42+00 TO STA 105+00
STA 117+25 TO STA 123+50

SHOULDER RUMBLE STRIP, 8 INCH
STA 222+00 TO STA 325+00

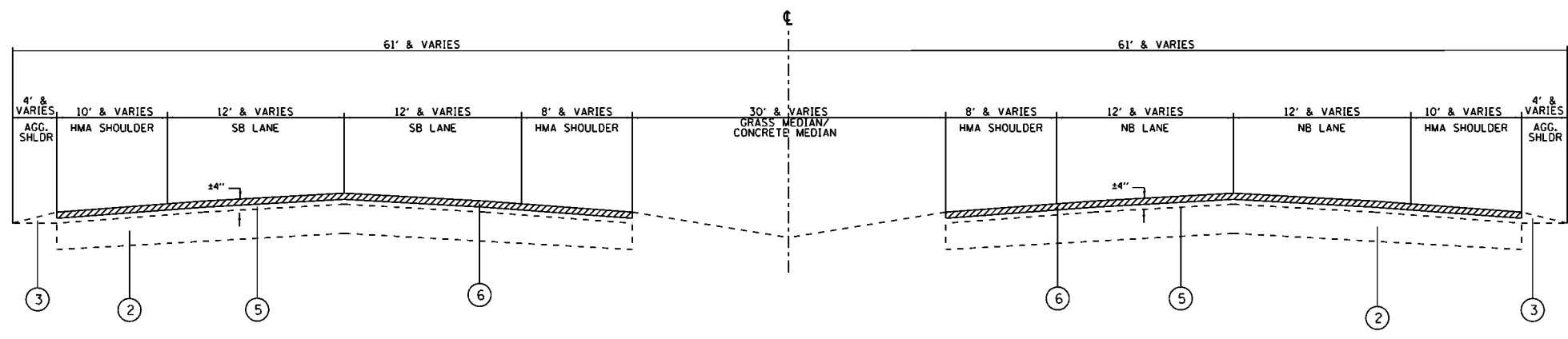
SHOULDER RUMBLE STRIP, 16 INCH
STA 32+00 TO STA 107+00
STA 111+50 TO STA 133+00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
IL 77 (FINLEY RD TO BASE LINE RD)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	8
CONTRACT NO. 62J20			ILLINOIS FED. AID PROJECT	

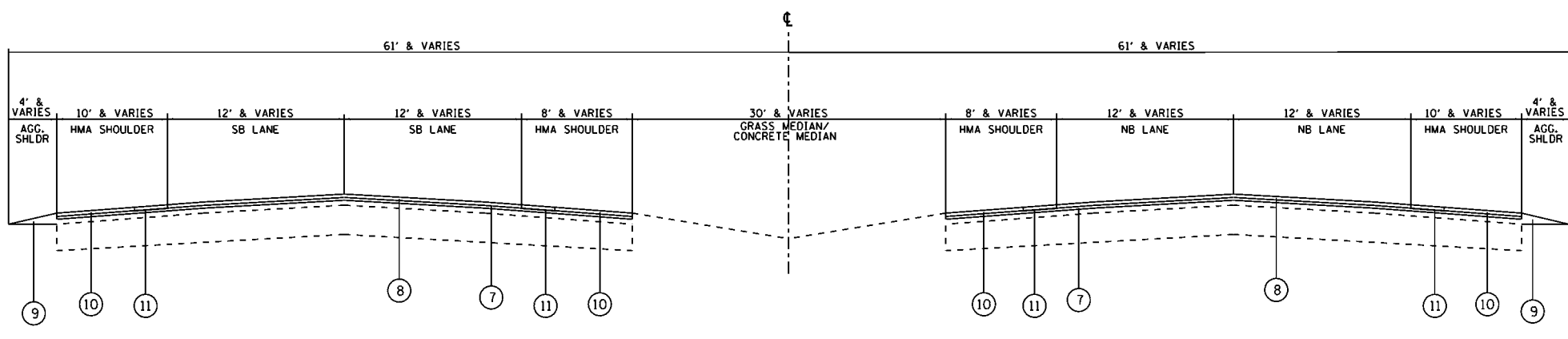
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IL 77 - US 30
EXISTING TYPICAL SECTION
 149+00 TO 334+00

LEGEND

- ① EXISTING BITUMINOUS CONCRETE SURFACE ±7"
- ② EXISTING P.C.C. PAVEMENT ±10"
- ③ EXISTING AGGREGATE WEDGE SHOULDER
- ④ EXISTING BITUMINOUS CONCRETE SHOULDER 6' & VARIES
- ⑤ EXISTING H.M.A. SURFACE AFTER MILLING
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑦ PROPOSED POLYMERIZED HMA BINDER COURSE IL -4.75, N50, 3/4"
- ⑧ POLYMERIZED HMA SURFACE COURSE, SMA, 9.5 MIX "F" N80, 1 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER & GRADING & SHAPING SHOULDER
- ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N70, 1 3/4"
- ⑪ SHOULDER RUMBLE STRIPS
- ⑫ CENTERLINE RUMBLE STRIPS, 16 INCH
- ⑬ MEDIAN REMOVAL & COMB. CURB & GUTTER REMOVAL
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N50, 4"
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12



IL 77 - US 30
PROPOSED TYPICAL SECTION
 149+00 TO 334+00

CENTERLINE RUMBLE STRIP, 16 INCH
 STA 42+00 TO STA 105+00
 STA 117+25 TO STA 123+50

SHOULDER RUMBLE STRIP, 8 INCH
 STA 222+00 TO STA 325+00

SHOULDER RUMBLE STRIP, 16 INCH
 STA 32+00 TO STA 107+00
 STA 111+50 TO STA 133+00

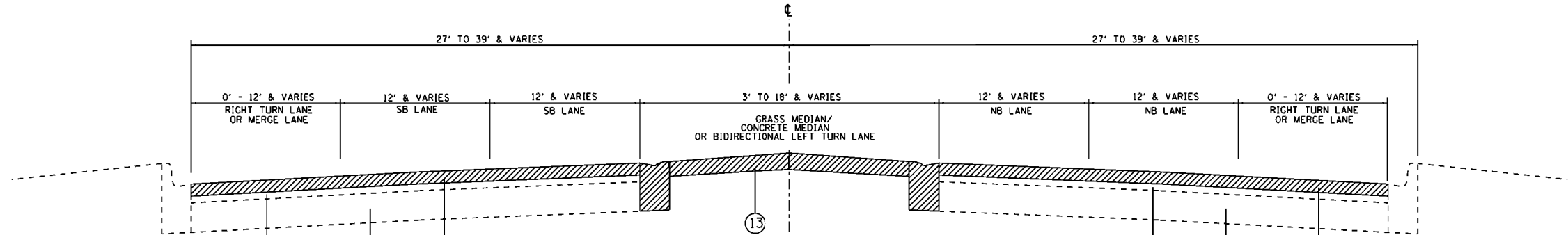
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PLOT DATE = 1/30/2020	DATE -	REVISED -

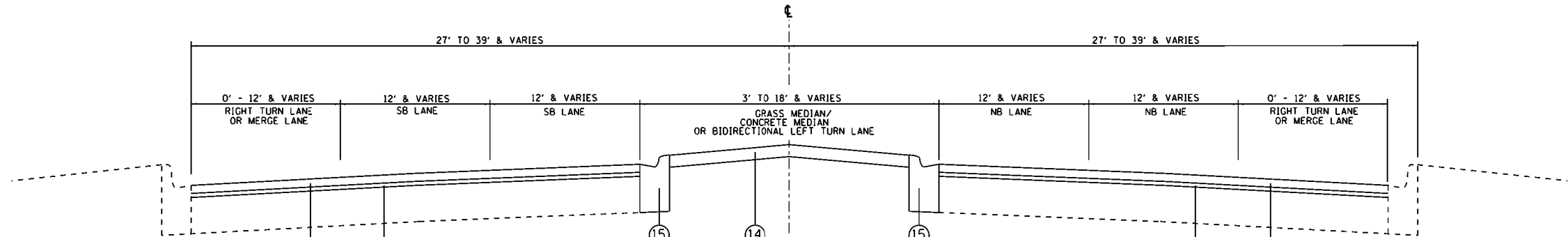
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS	
IL 77 (FINLEY RD TO BASE LINE RD)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	9
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J20	



IL 47 - US 30
EXISTING TYPICAL SECTION
 156+12 TO 215+31



IL 47 - US 30
PROPOSED TYPICAL SECTION
 156+12 TO 215+31

LEGEND

- ① EXISTING BITUMINOUS CONCRETE SURFACE ±7"
- ② EXISTING P.C.C. PAVEMENT ±10"
- ③ EXISTING AGGREGATE WEDGE SHOULDER
- ④ EXISTING BITUMINOUS CONCRETE SHOULDER 6' & VARIES
- ⑤ EXISTING H.M.A. SURFACE AFTER MILLING
- ⑥ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑦ PROPOSED POLYMERIZED HMA BINDER COURSE IL -4.75, N50, 3/4"
- ⑧ POLYMERIZED HMA SURFACE COURSE, SMA, 9.5 MIX "F" N80, 1 3/4"
- ⑨ PROPOSED AGGREGATE WEDGE SHOULDER & GRADING & SHAPING SHOULDER
- ⑩ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N70, 1 3/4"
- ⑪ SHOULDER RUMBLE STRIPS
- ⑫ CENTERLINE RUMBLE STRIPS, 16 INCH
- ⑬ MEDIAN REMOVAL & COMB. CURB & GUTTER REMOVAL
- ⑭ PROPOSED HMA SURFACE COURSE, MIX "D", IL-9.5 N50, 4"
- ⑮ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

CENTERLINE RUMBLE STRIP, 16 INCH
 STA 42+00 TO STA 105+00
 STA 117+25 TO STA 123+50

SHOULDER RUMBLE STRIP, 8 INCH
 STA 222+00 TO STA 325+00

SHOULDER RUMBLE STRIP, 16 INCH
 STA 32+00 TO STA 107+00
 STA 111+50 TO STA 133+00

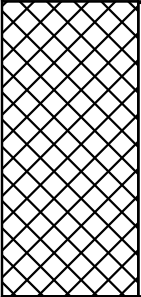
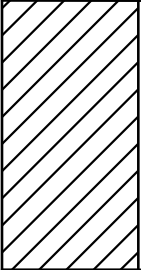
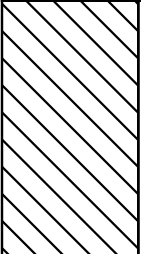
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	DRAWN -	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS			
IL 47 (FINLEY RD TO BASE LINE RD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	9A
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

KEY	2020 DATES	CODE	DESCRIPTION	UNIT
	AUGUST 1 - SEPTEMBER 15	X0327120	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT	ACRE
	SEPTEMBER 15 - OCTOBER 1	X2503110	MOWING (SPECIAL)	ACRE
	OCTOBER 15 - DECEMBER 1**	X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE
	OCTOBER 15 - DECEMBER 1	25003310	INTERSEEDING, CLASS 4	ACRE
	AUGUST 1 - SEPTEMBER 15	X0327120	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT	ACRE
	SEPTEMBER 15 - OCTOBER 1	X2503110	MOWING (SPECIAL)	ACRE
	OCTOBER 15 - DECEMBER 1	25003310	INTERSEEDING, CLASS 4	ACRE
	AUGUST 1 - SEPTEMBER 15	X0327120	WEED CONTROL, NATIVE LANDSCAPE ENHANCEMENT	ACRE
	SEPTEMBER 15 - OCTOBER 1	X2503110	MOWING (SPECIAL)	ACRE
	AUGUST 1 - NOVEMBER 1	25003210	INTERSEEDING, CLASS 2A	ACRE

** BECAUSE OF THE PROXIMITY OF NORTHERN LONG-EARED BAT HABITAT, ALL TREE REMOVAL SHALL TAKE PLACE AFTER OCTOBER 1, 2020

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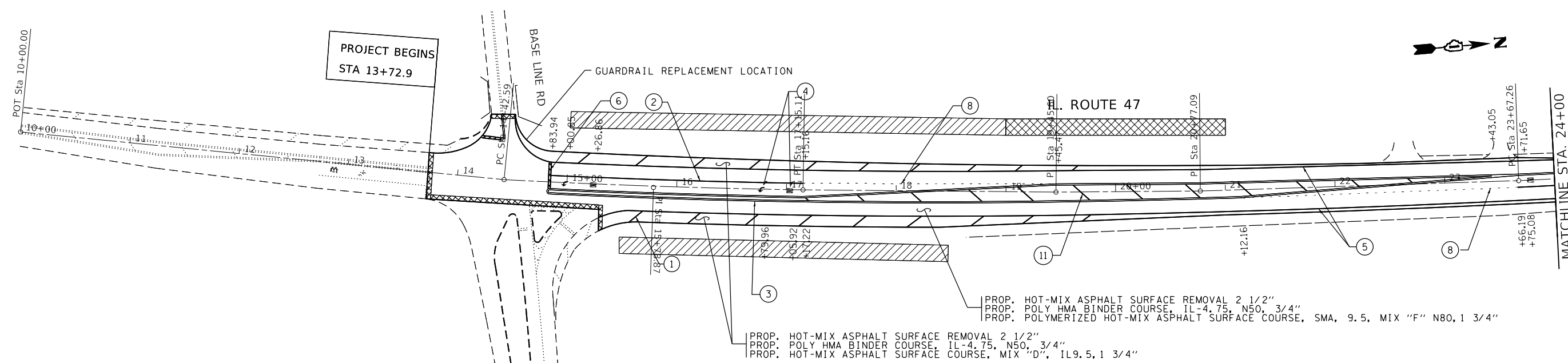
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PLOT DATE = 12/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LANDSCAPING SCHEDULE

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	10
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

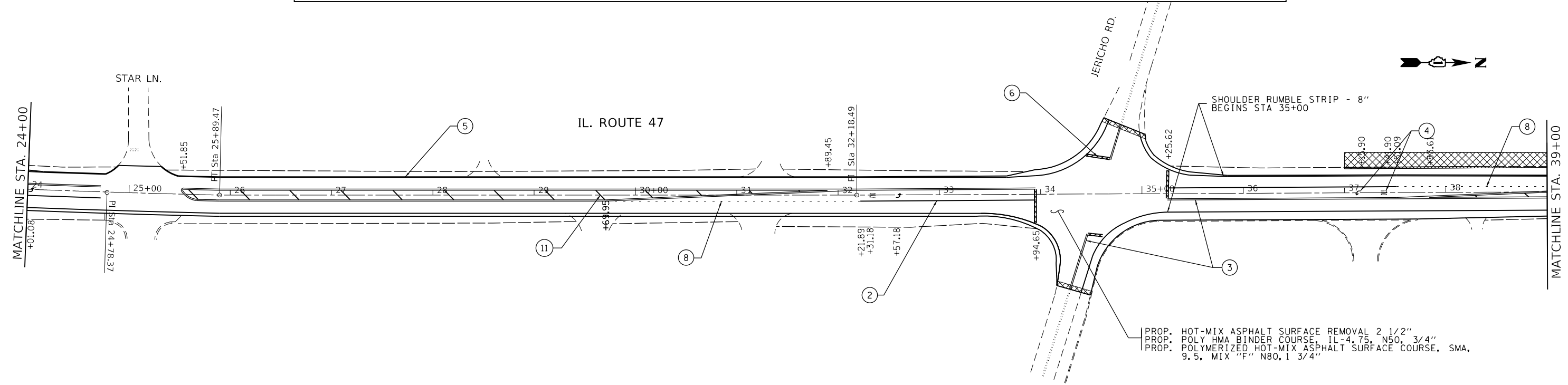


- | | | |
|---|--|--|
| <ul style="list-style-type: none"> ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | <ul style="list-style-type: none"> ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | <ul style="list-style-type: none"> ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |
|---|--|--|

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING

PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL9.5, 1 3/4"



PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"

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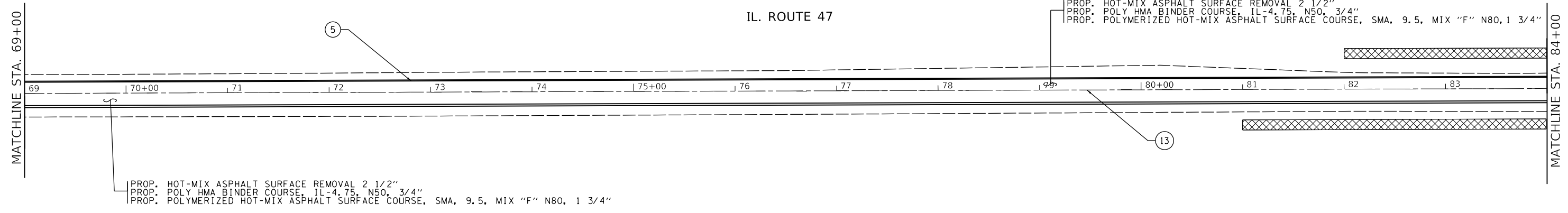
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
 IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

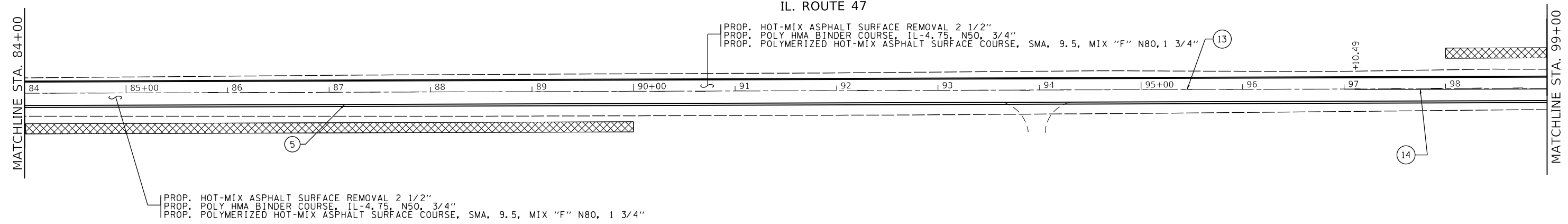
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	11
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.



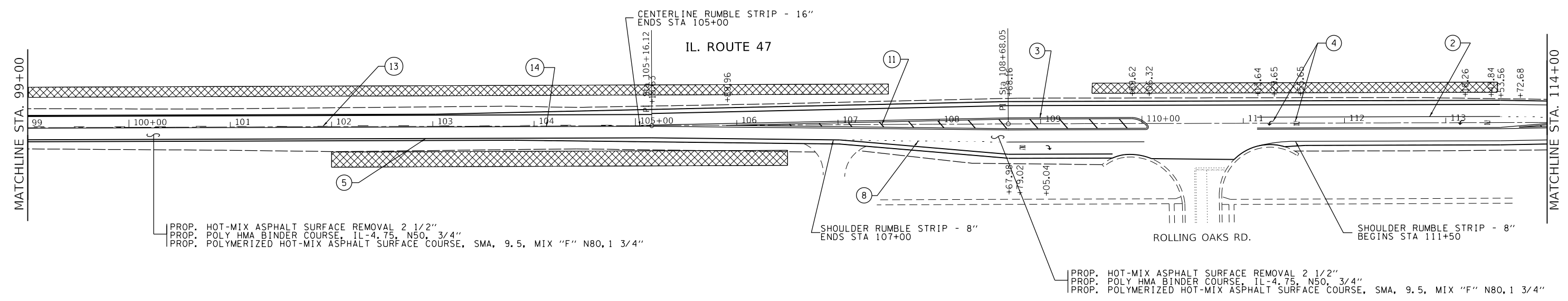
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| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



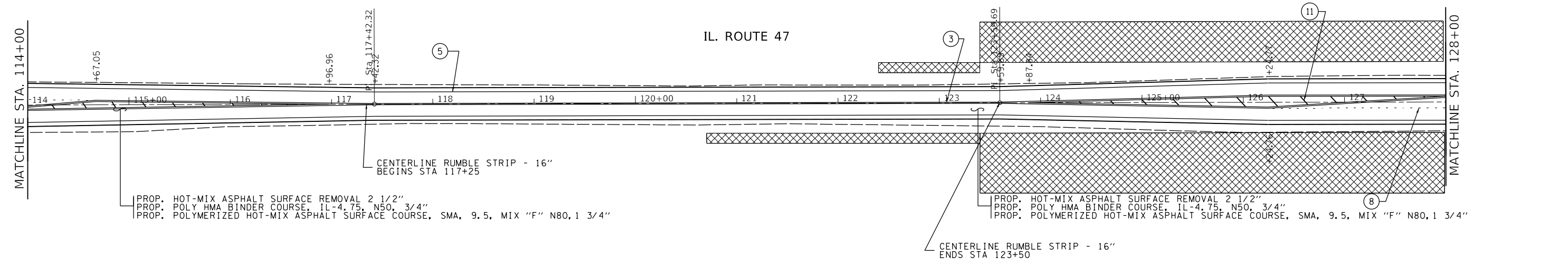
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	CHECKED - DATE -	REVISED - REVISED -		SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.	CONTRACT NO. 62J20		ILLINOIS FED. AID PROJECT			



- | | | |
|---|--|--|
| <ul style="list-style-type: none"> ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | <ul style="list-style-type: none"> ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | <ul style="list-style-type: none"> ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |
|---|--|--|

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



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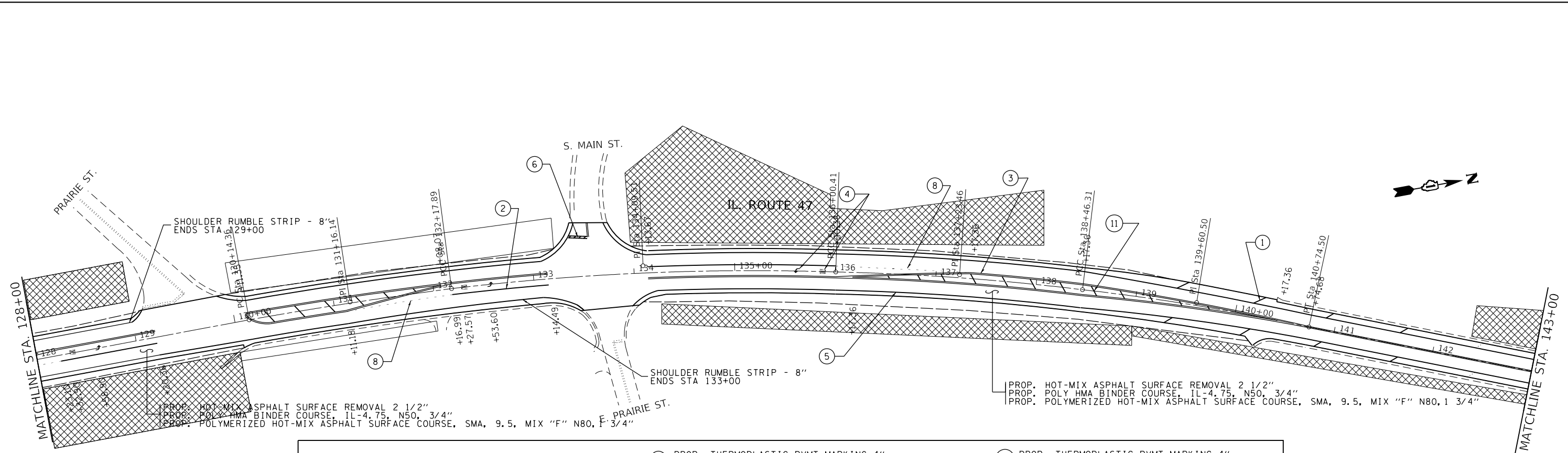
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
 IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

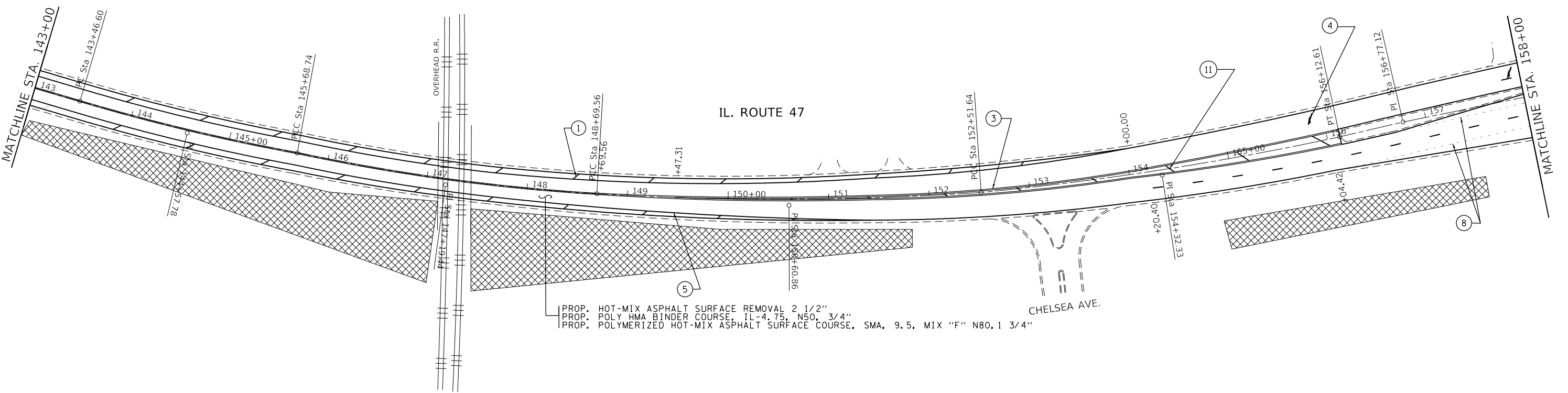
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F.A.P. RTE. 326	SECTION 2019-042-R5	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 14
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				



- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"

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	DATE -	REVISED -

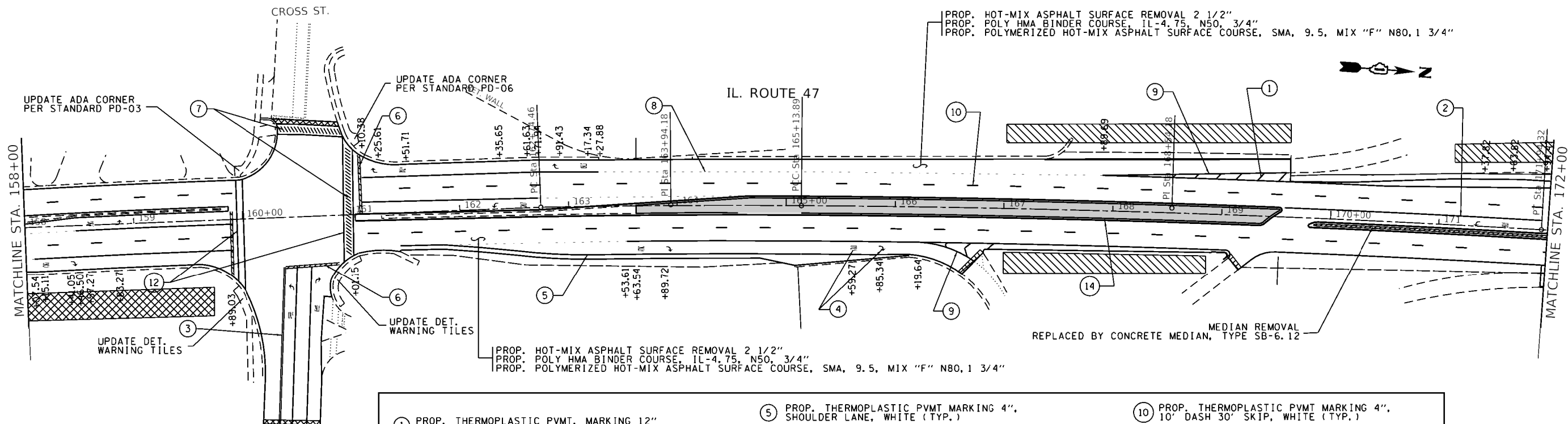
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
 IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

F.A.P. RTE. 326	SECTION 2019-042-R5	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 15
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

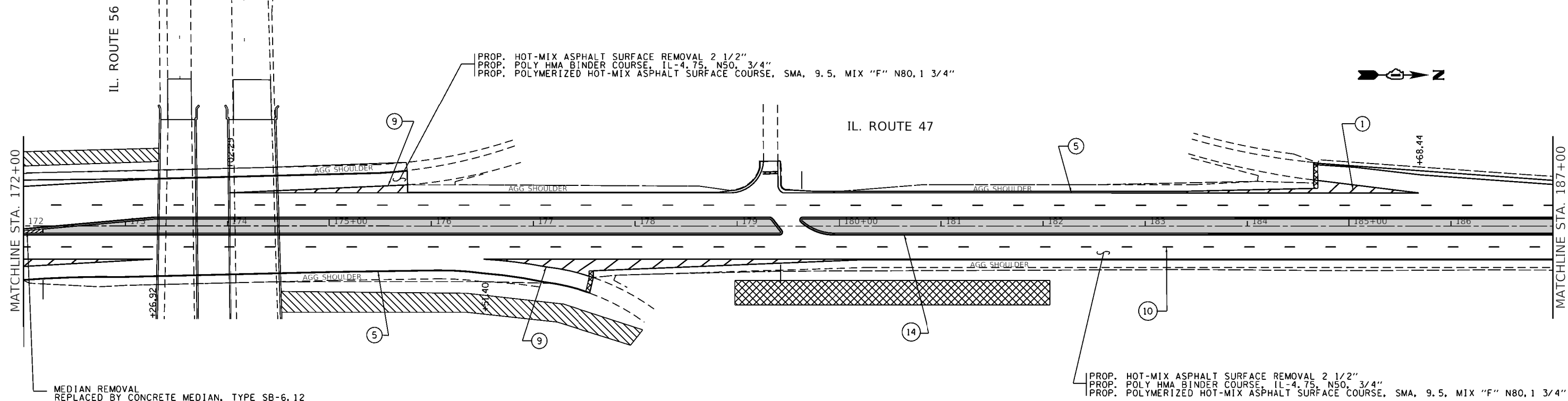
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MEDIAN REMOVAL
 SUBBASE GRANULAR MATERIAL, TYPE B
 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50
 BITUMINOUS MATERIALS (TACK COAT)



NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING

- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" Ø 45; WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT, MARKING 12" MEDIAN Ø 45", YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW Ø 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6' C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |



MEDIAN REMOVAL
 SUBBASE GRANULAR MATERIAL, TYPE B
 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50
 BITUMINOUS MATERIALS (TACK COAT)

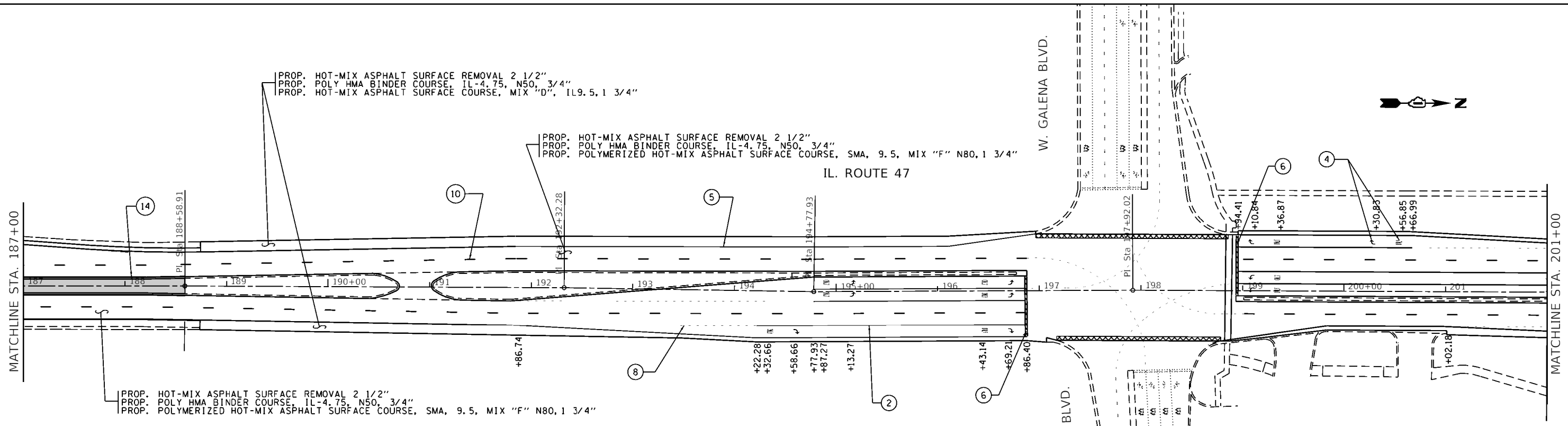
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PLOT DATE = 1/30/2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLAN	
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

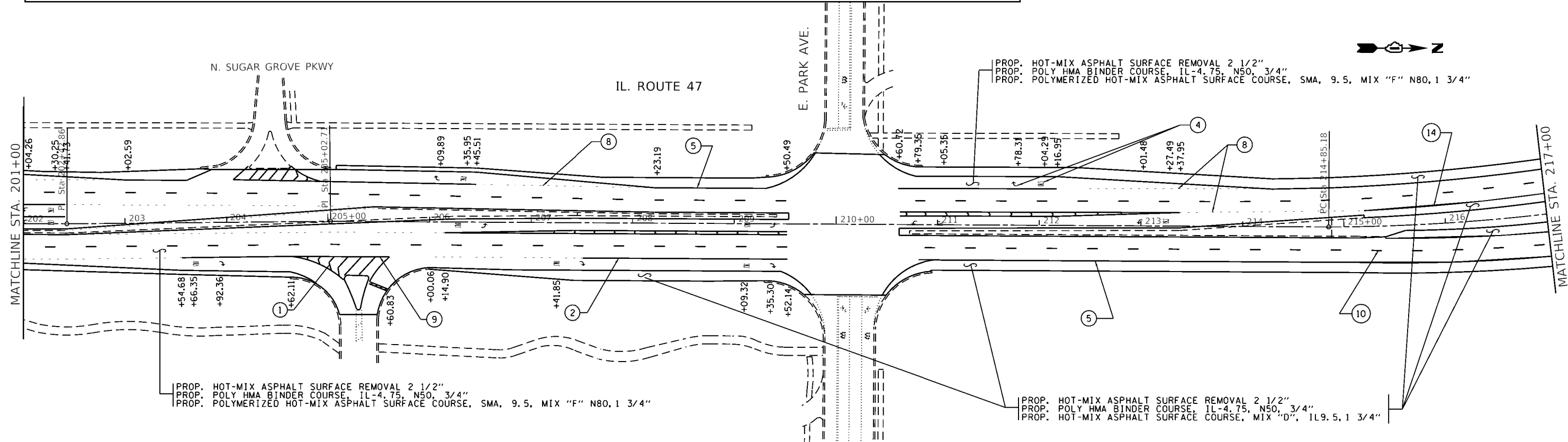
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	16
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				



- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |

MEDIAN REMOVAL
SUBBASE GRANULAR MATERIAL, TYPE B
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50
BITUMINOUS MATERIALS (TACK COAT)

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL9.5, 1 3/4"

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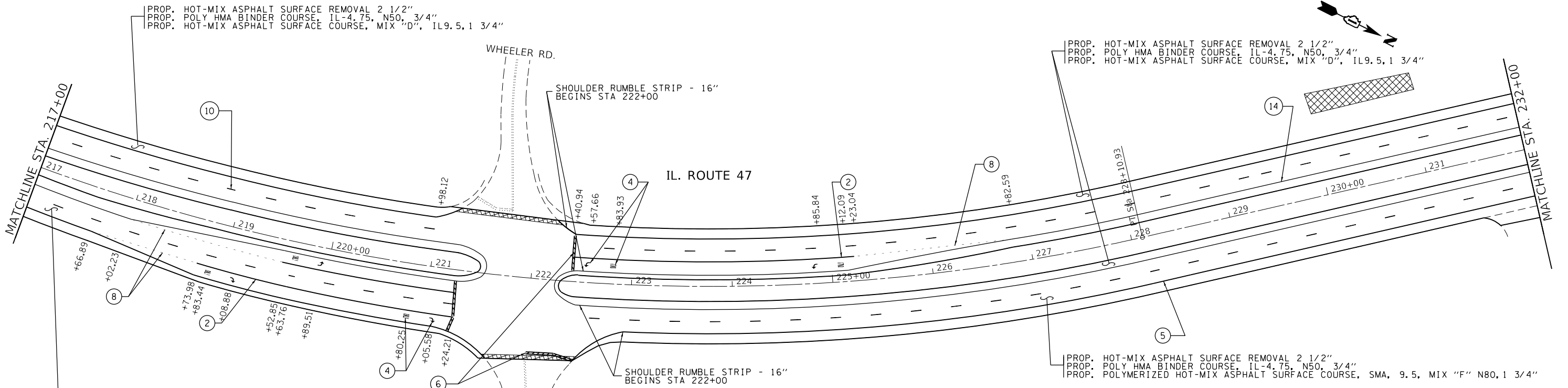
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLAN	
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 326	SECTION 2019-042-R5	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 17
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

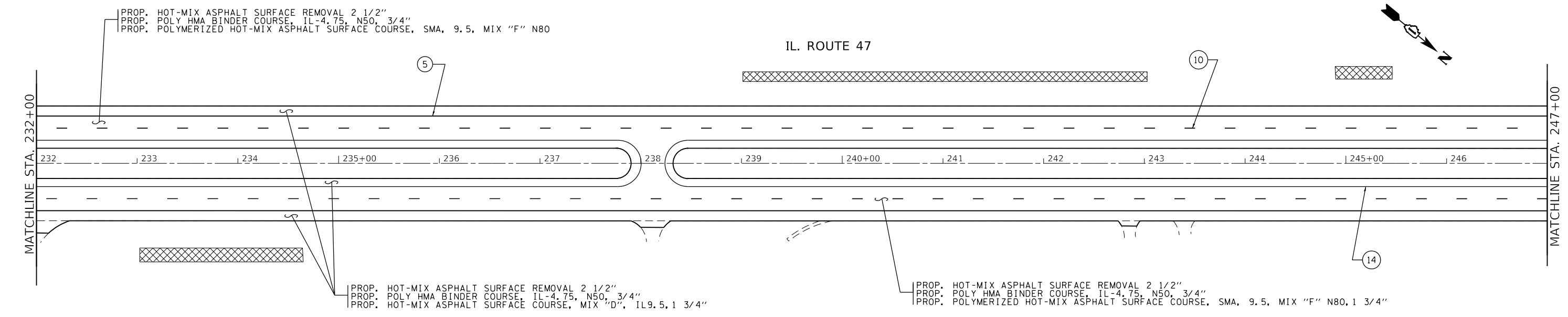
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 USER: allababidiz
 DATE: 12/16/2019



PROP. HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROP. POLY HMA BINDER COURSE, IL-4.75, N50, 3/4"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, MIX "F" N80, 1 3/4"

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING

- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.)
② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.)
③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.)
④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.)
⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)
⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.)
⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.)
⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.)
⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.)
⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.)
⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW (TYP.)
⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |
|---|--|--|

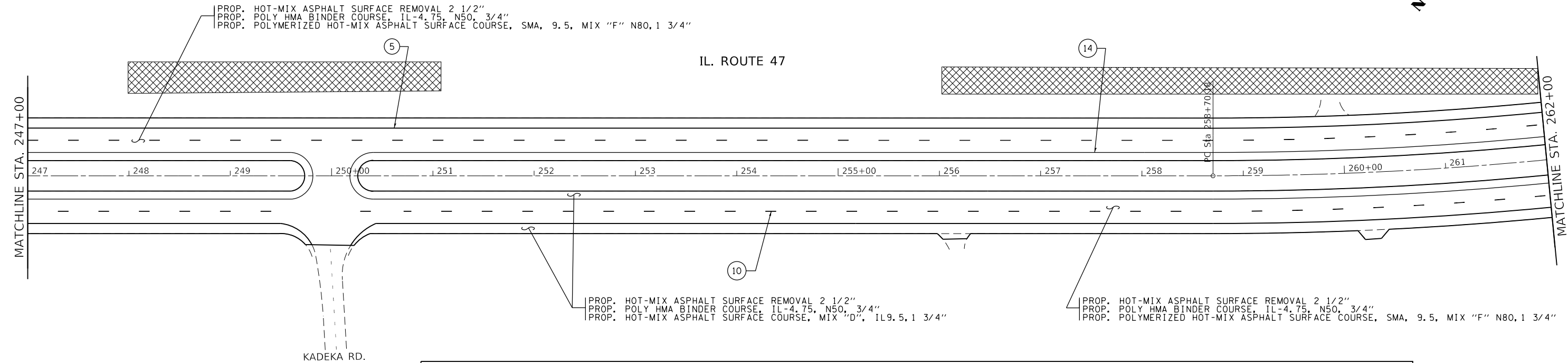
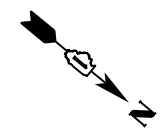


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DRAWN -	REVISED -	REVISED -
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PLOT DATE = 12/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

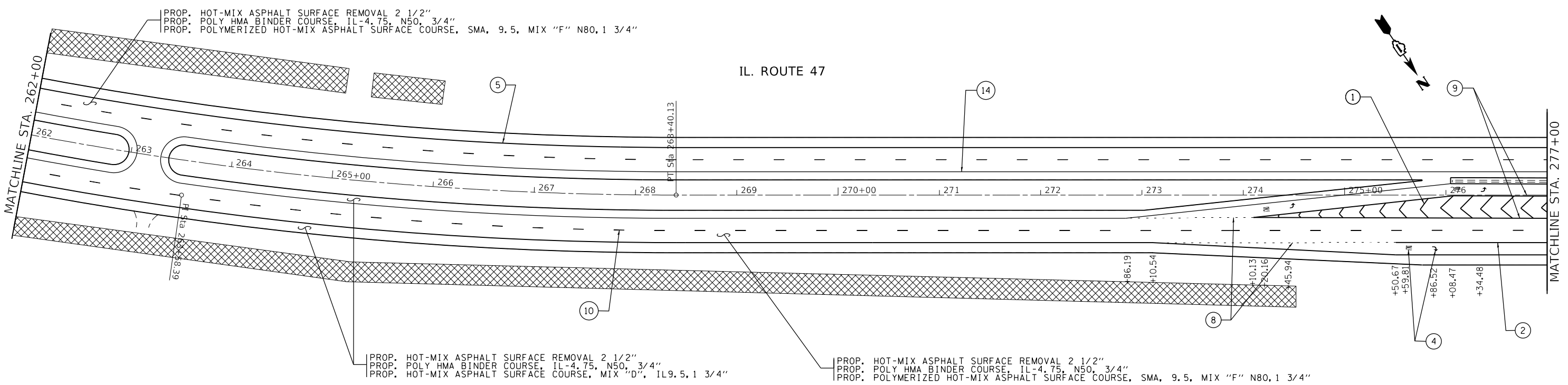
ROADWAY AND PAVEMENT MARKING PLAN	
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)	
SCALE: 1"=50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 326	SECTION 2019-042-R5	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 18
CONTRACT NO. 62J20				
ILLINOIS		FED. AID PROJECT		



NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING

- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |



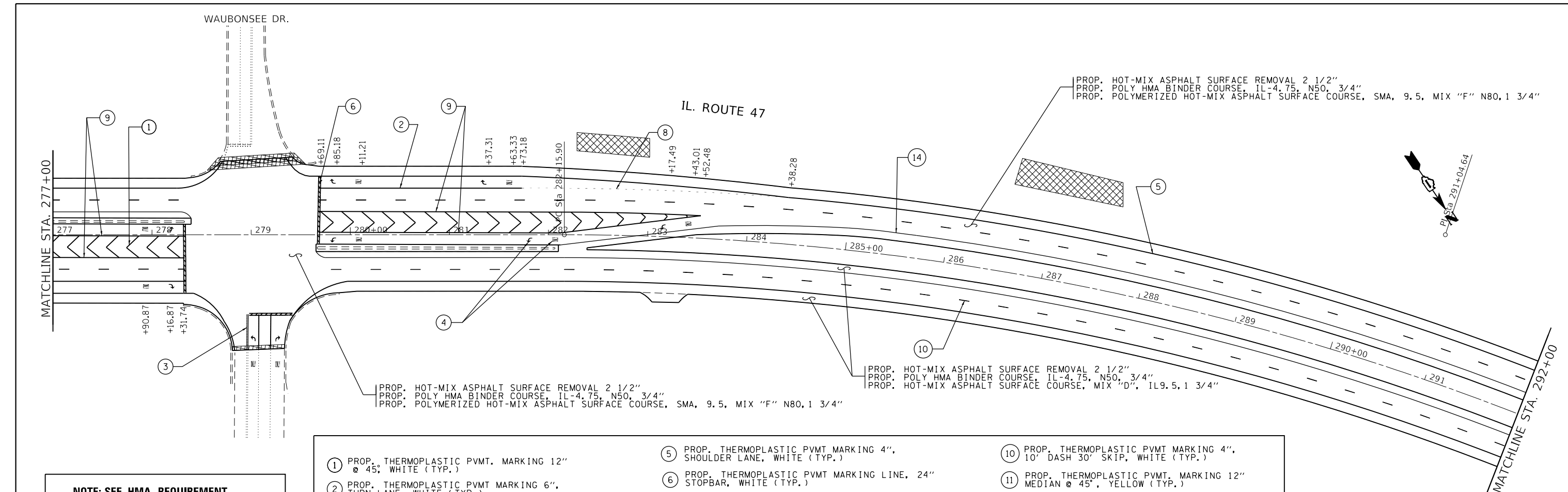
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)
SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

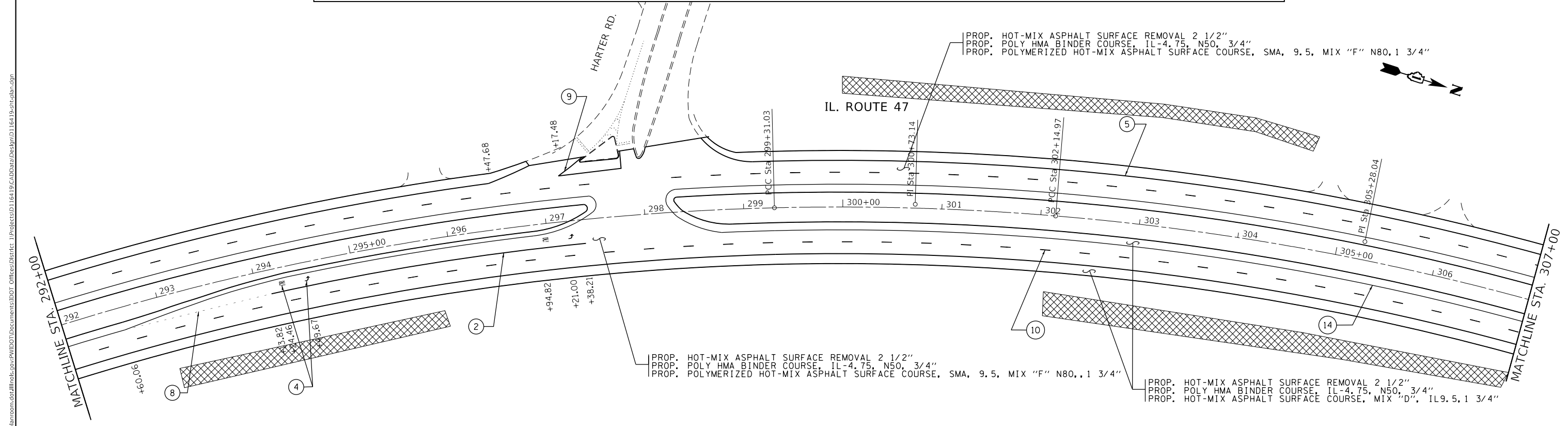
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	19
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

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NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING

- | | | |
|---|--|---|
| <ul style="list-style-type: none"> ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | <ul style="list-style-type: none"> ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | <ul style="list-style-type: none"> ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |
|---|--|---|



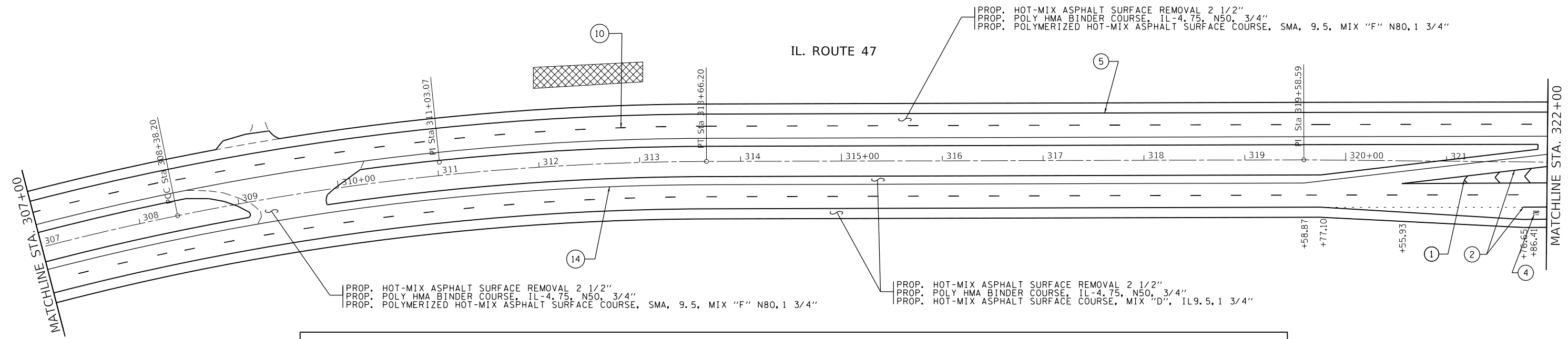
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PLOT DATE = 12/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

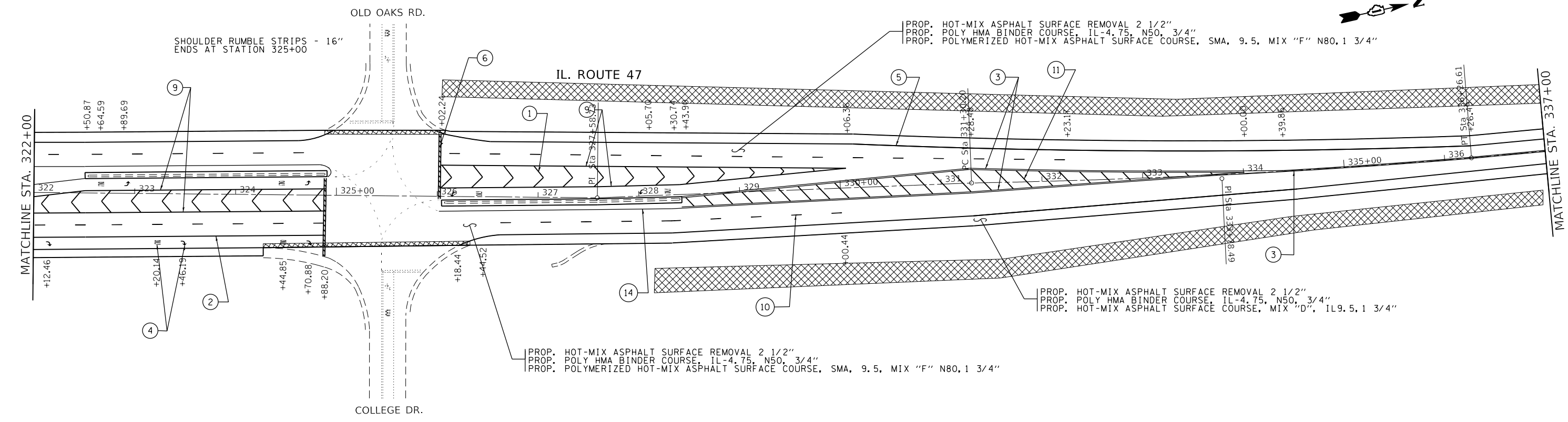
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CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

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- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



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DRAWN -	REVISED -	
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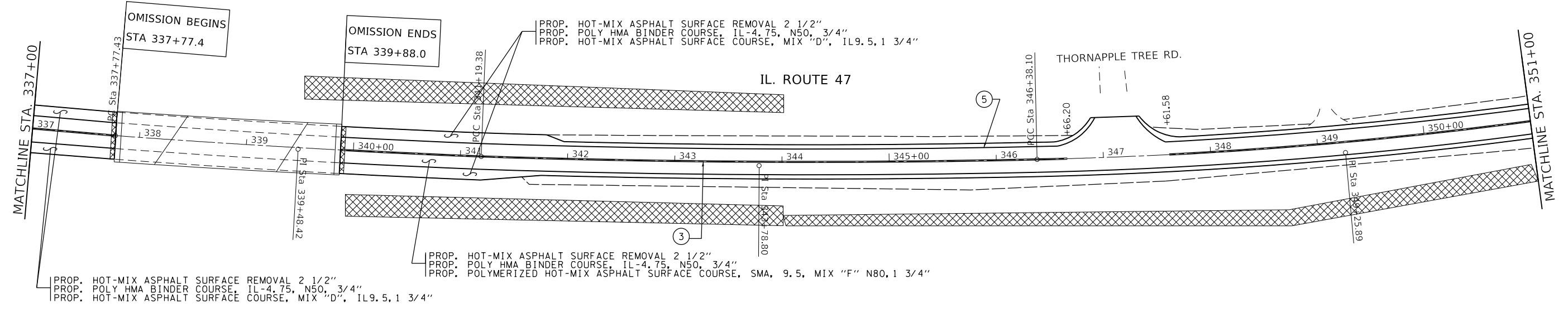
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	21
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

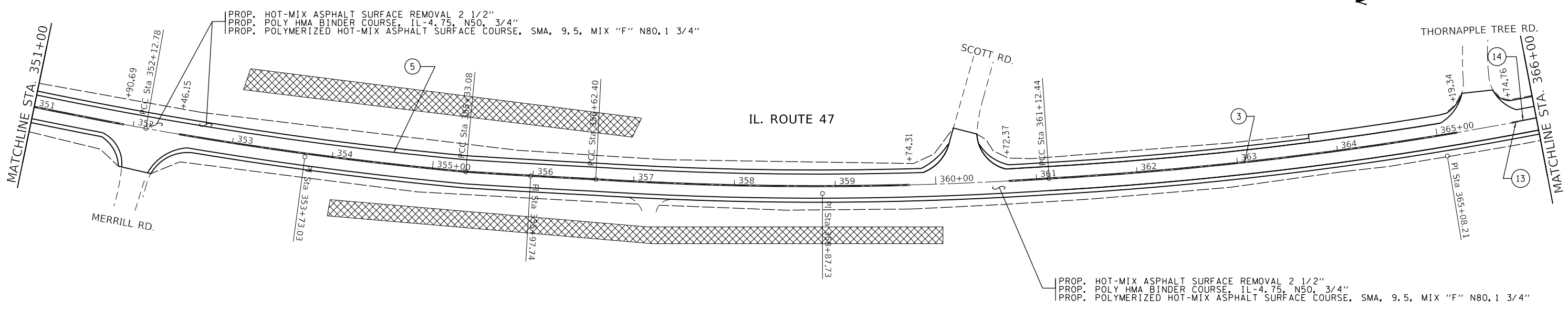
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SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.



- | | | |
|---|--|--|
| ① PROP. THERMOPLASTIC PVMT. MARKING 12" @ 45°, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT MARKING 4", SHOULDER LANE, WHITE (TYP.) | ⑩ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 6", TURN LANE, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PVMT. MARKING 12" MEDIAN @ 45°, YELLOW (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMO. PAVEMENT MARKING LINE, 12" CROSSWALK, WHITE (TYP.) | ⑫ PROP. THERMO. PAVEMENT MARKING LINE, 6" CROSSWALK, 6" C-C WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT. MARKING LETTERS AND SYMBOLS, WHITE (TYP.) | ⑧ PROP. THERMOPLASTIC PVMT MARKING 6", 2' DASH 6' SKIP, WHITE (TYP.) | ⑬ PROP. THERMOPLASTIC PVMT MARKING 4", 10' DASH 30' SKIP, YELLOW TYP.) |
| | ⑨ PROP. THERMOPLASTIC PVMT MARKING 8", EDGE LINE, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW (TYP.) |

NOTE: SEE HMA REQUIREMENT TABLE FOR SHOULDER RESURFACING



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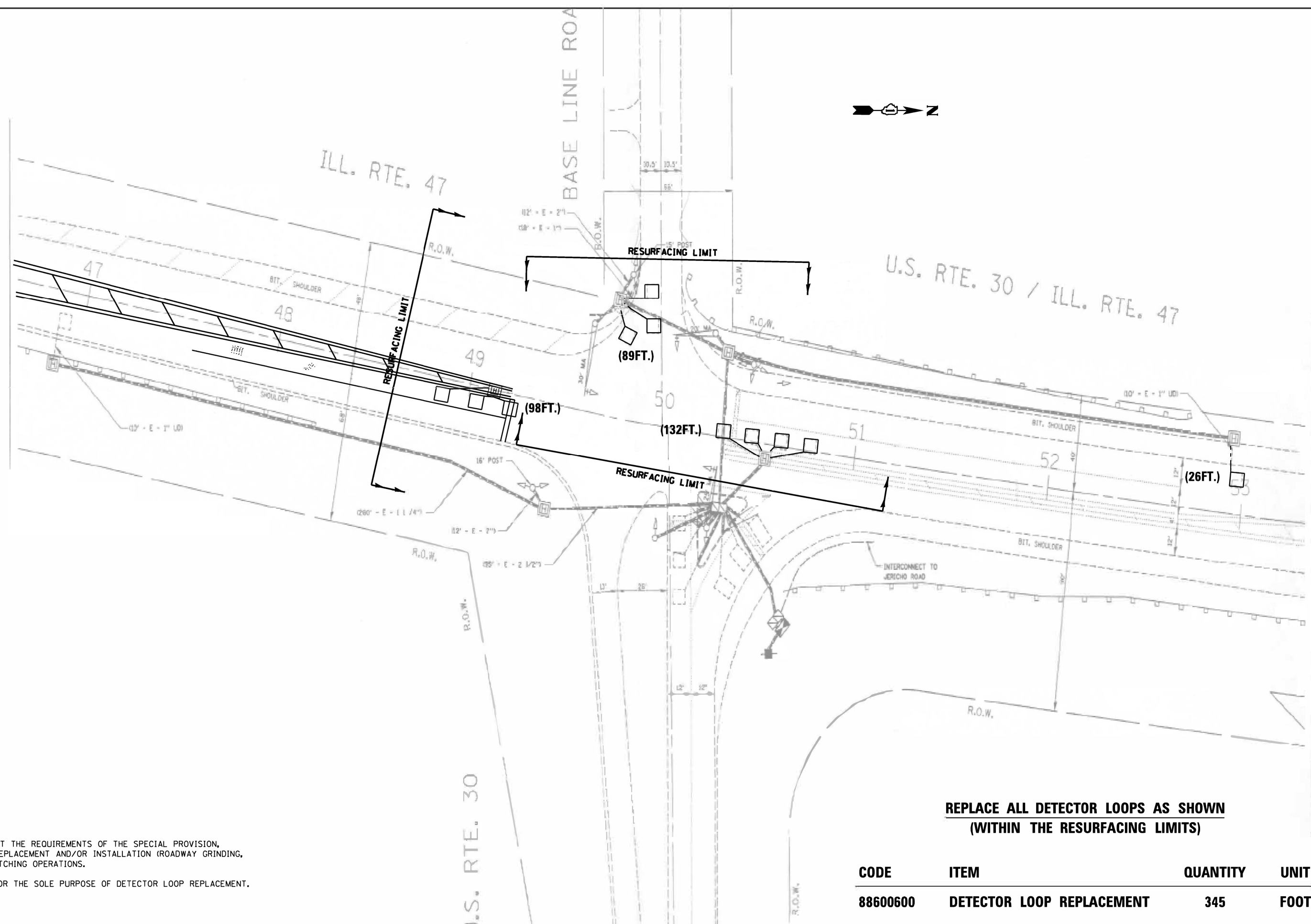
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PLOT DATE = 12/16/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLAN
IL. ROUTE 47 (SEAVEY RD. - BASE LINE RD./KENDALL CO LINE)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	22
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	345	FOOT

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	PLOT DATE = 3/26/2019	DATE - 03/18/2019	REVISED -

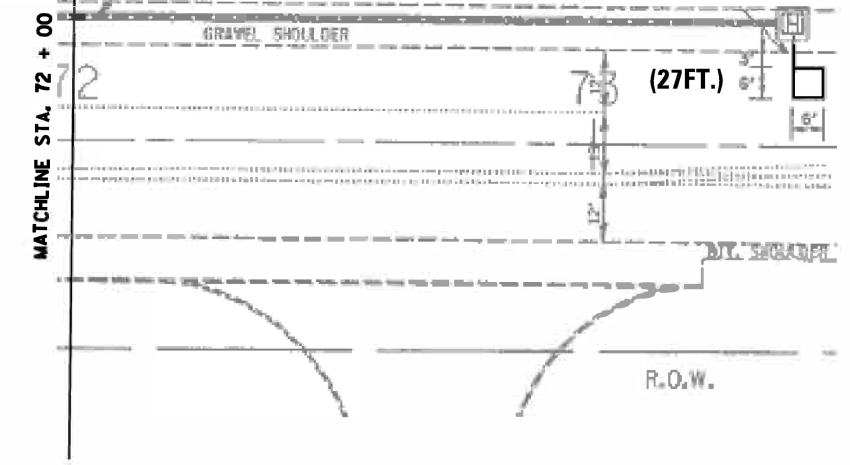
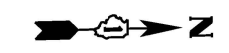
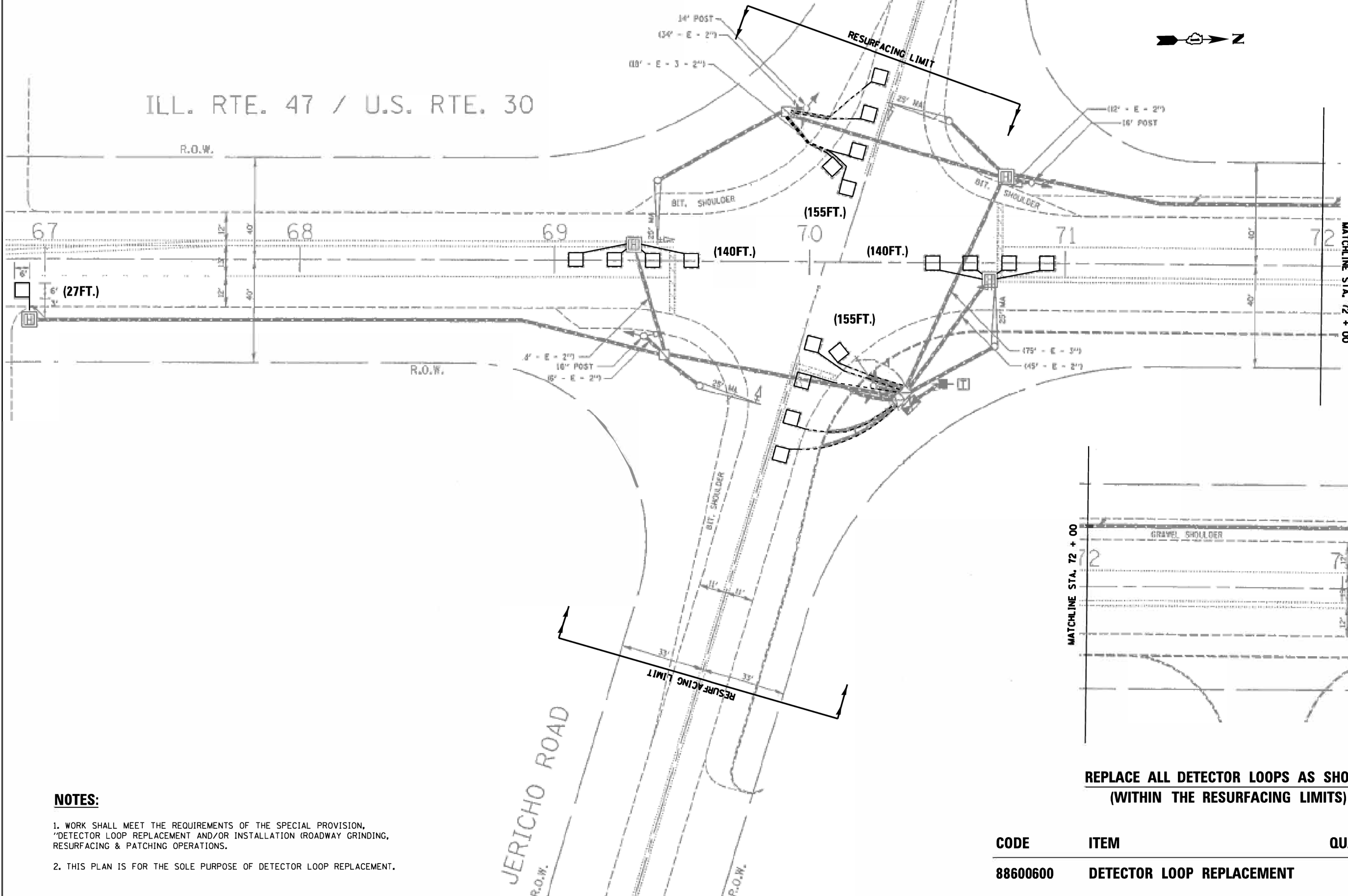
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
IL 47 AT US 30 (BASE LINE RD.)**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J20	

ILL. RTE. 47 / U.S. RTE. 30



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	644	FOOT

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Default	PLOT SCALE = 40.0000 / in.	CHECKED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

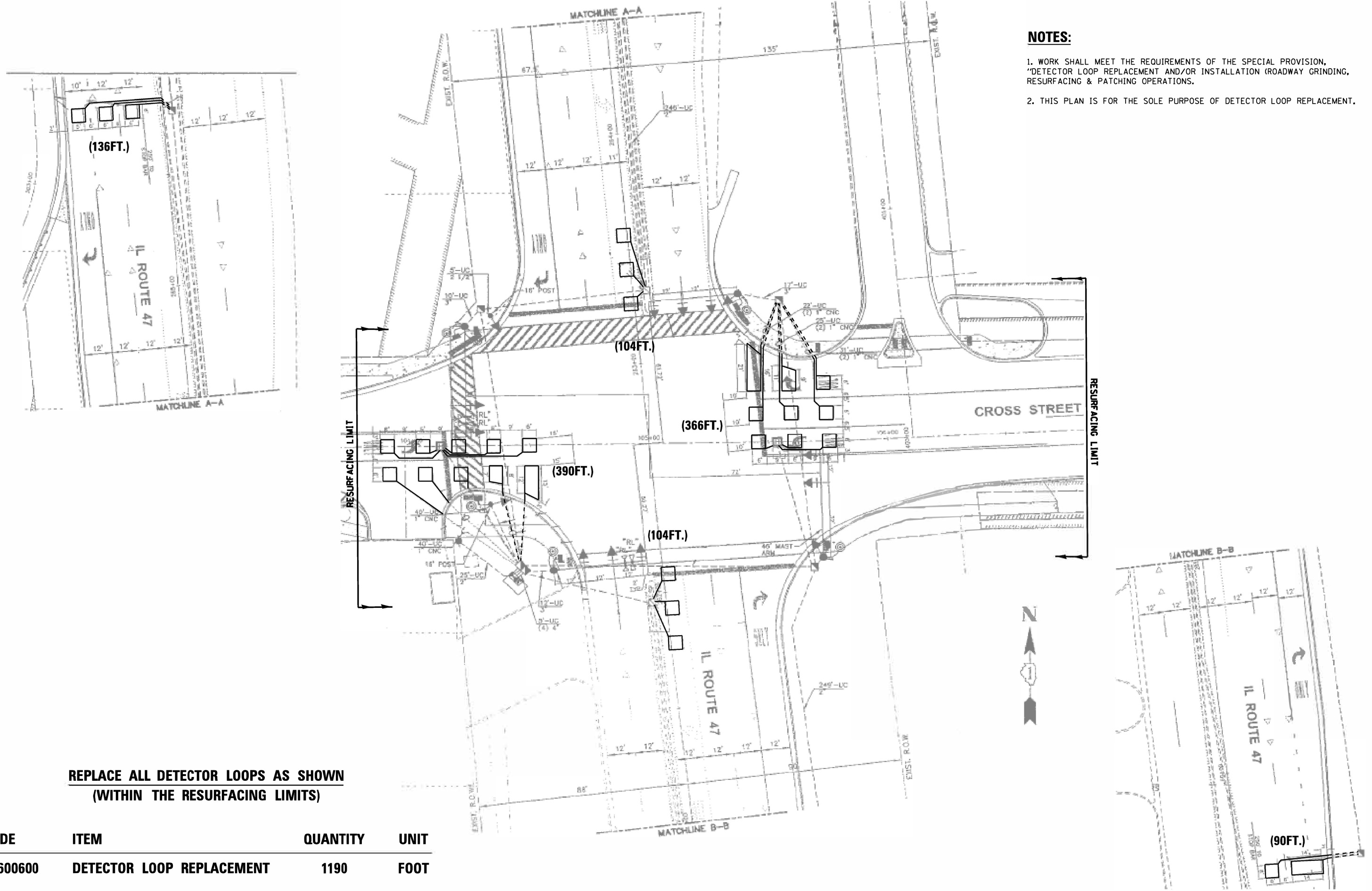
**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT JERICO RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	25
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J20	

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1190	FOOT

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	PLOT DATE = 3/19/2019	DATE - 03/19/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

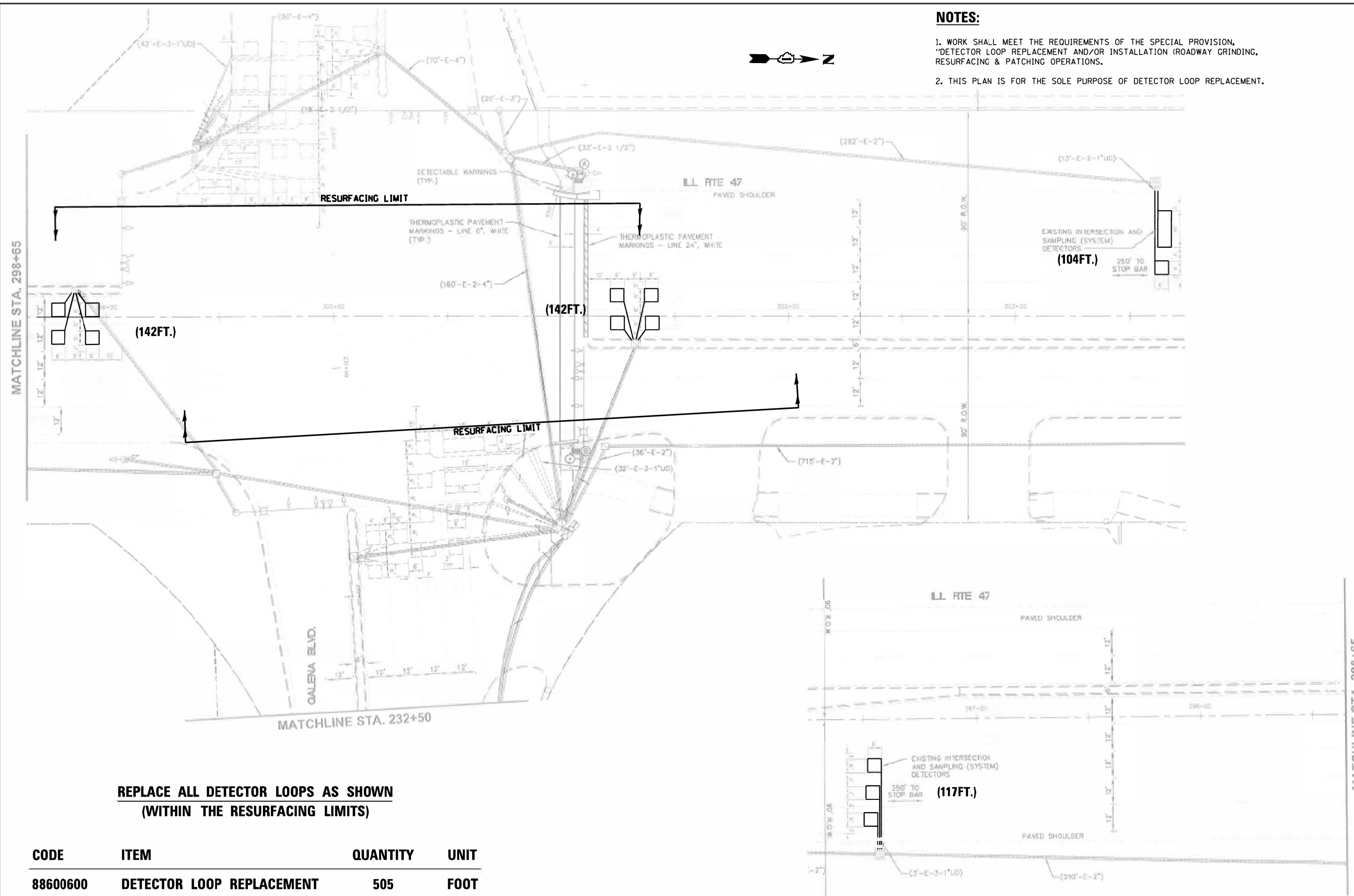
**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT CROSS STREET**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	26
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

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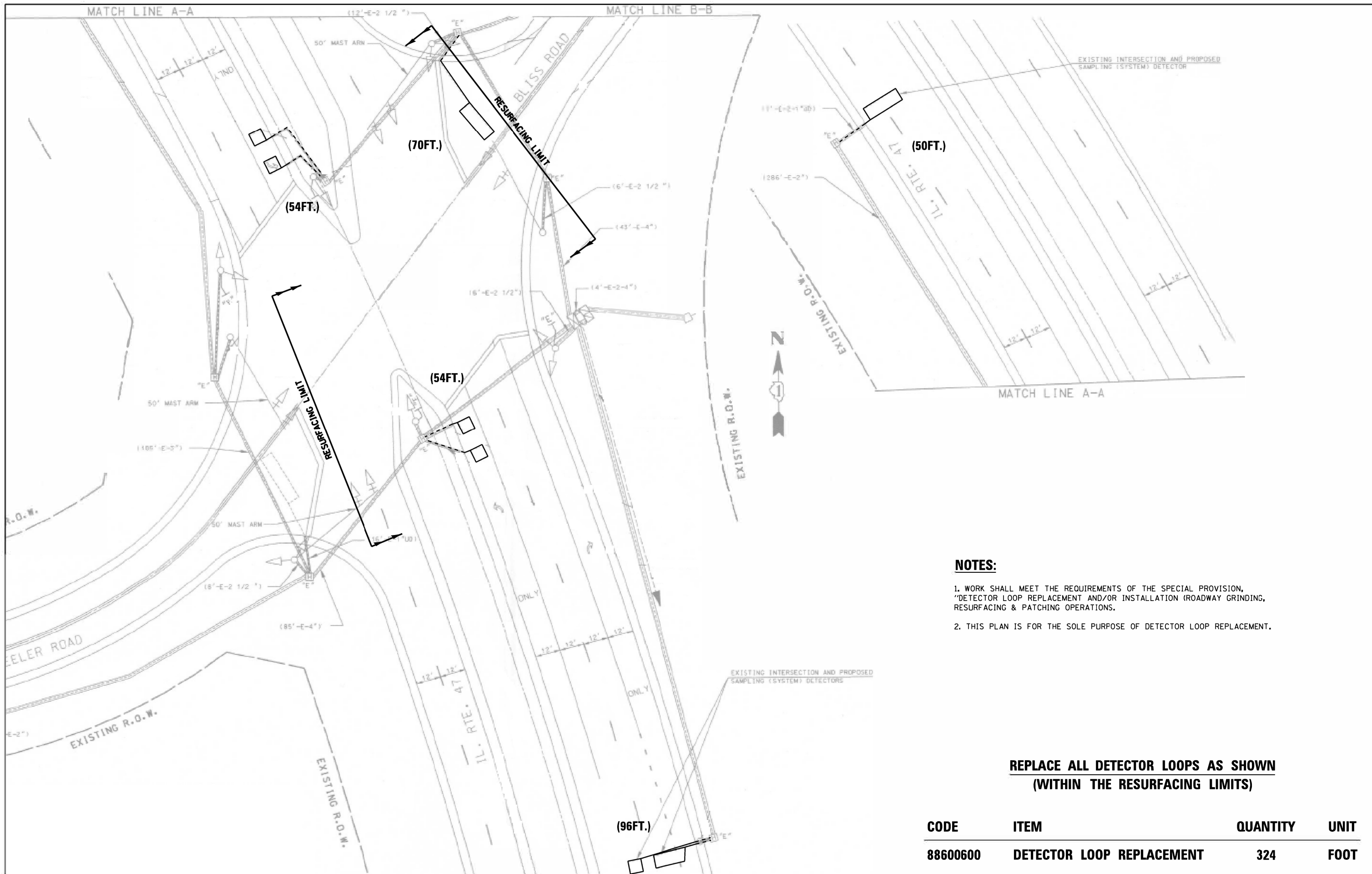
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT GALENA RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	27
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	324	FOOT

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

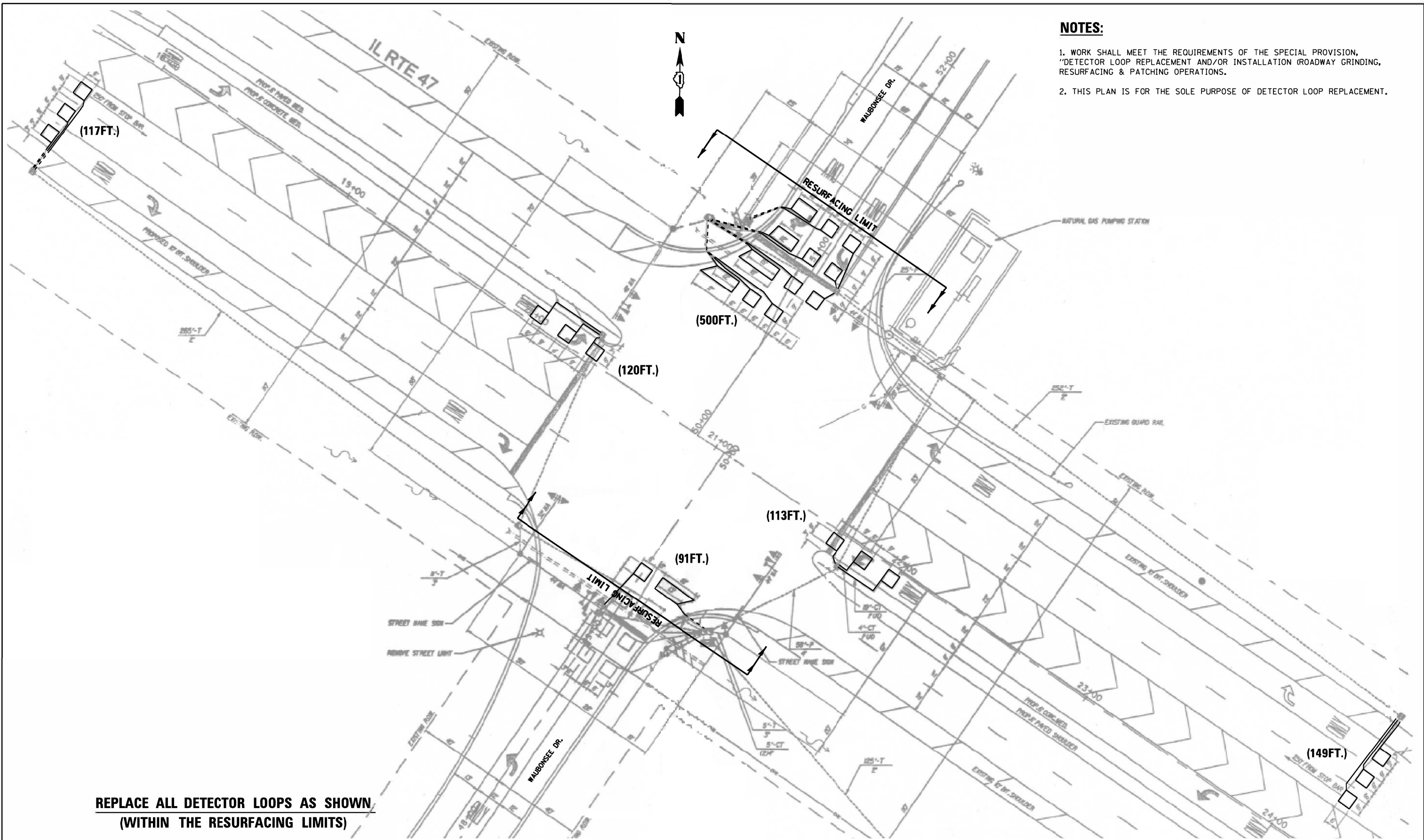
**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT BLISS RD. / WHEELER RD.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	28
CONTRACT NO. 62J20				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

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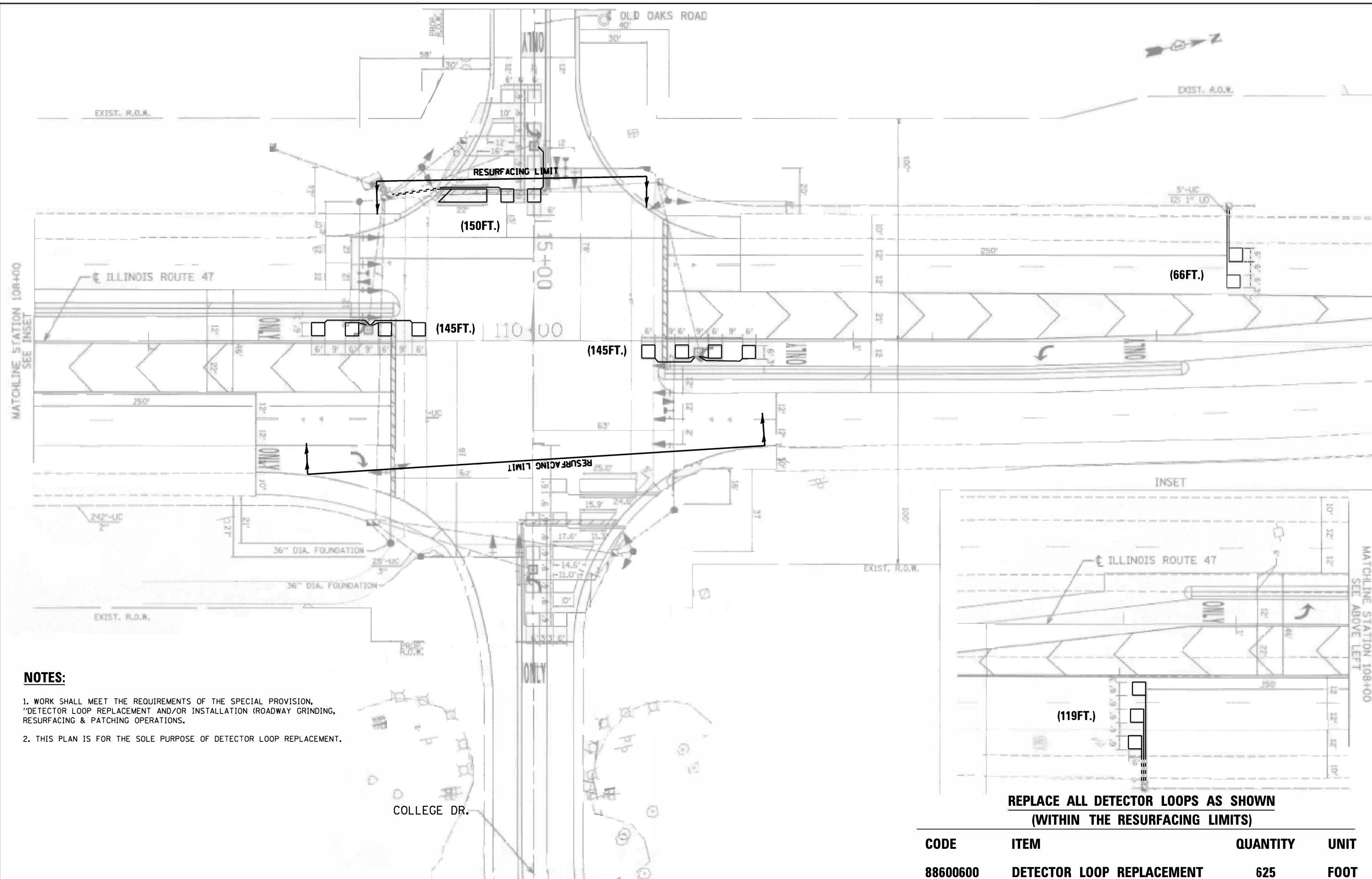
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT WAUBONSEE DR.**

SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-R5	KANE	44	29
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J20	



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	625	FOOT

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

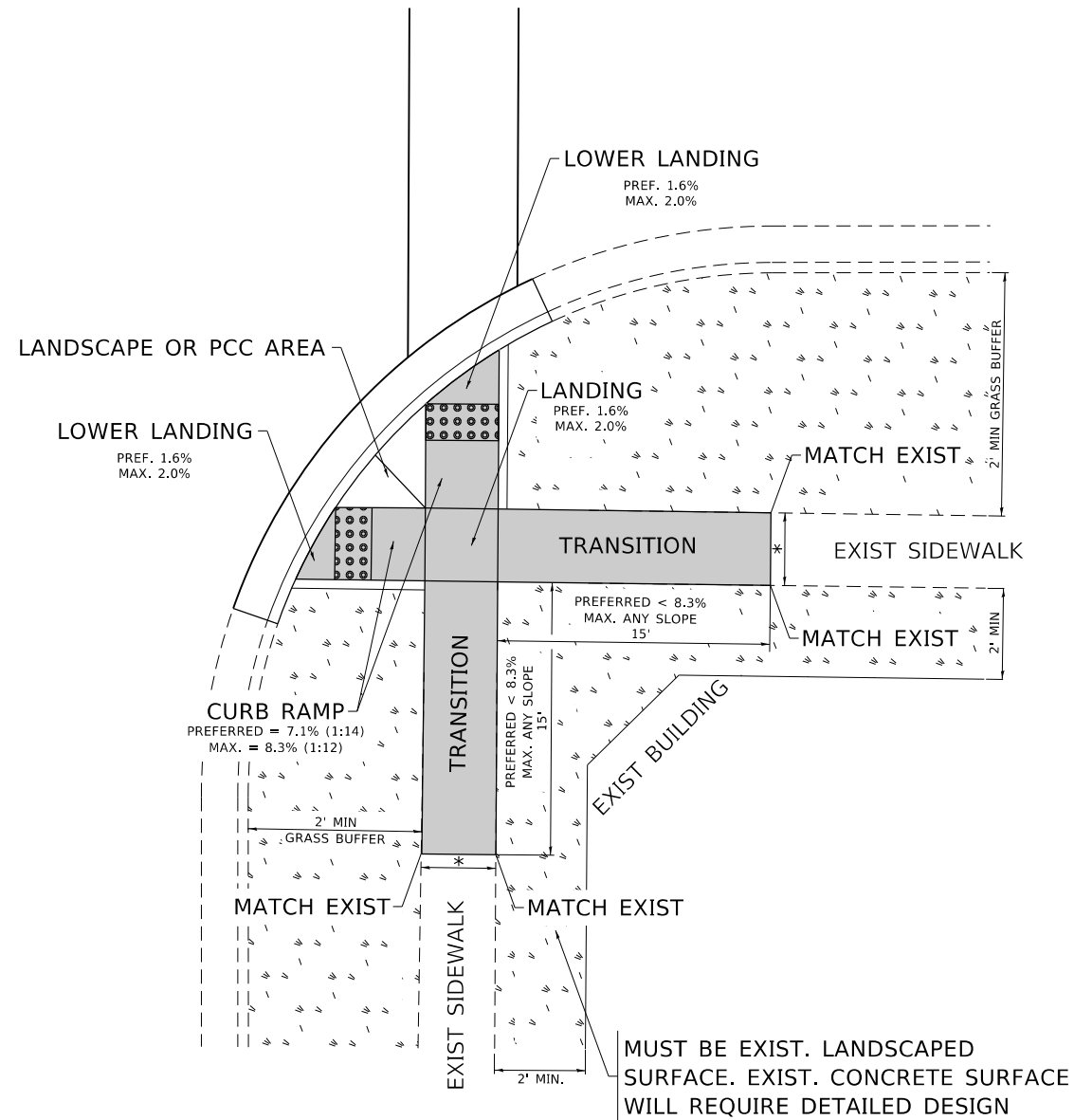
**DETECTOR LOOP REPLACEMENT PLAN
US 47 AT OLD OAKS RD.**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	30
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J20	

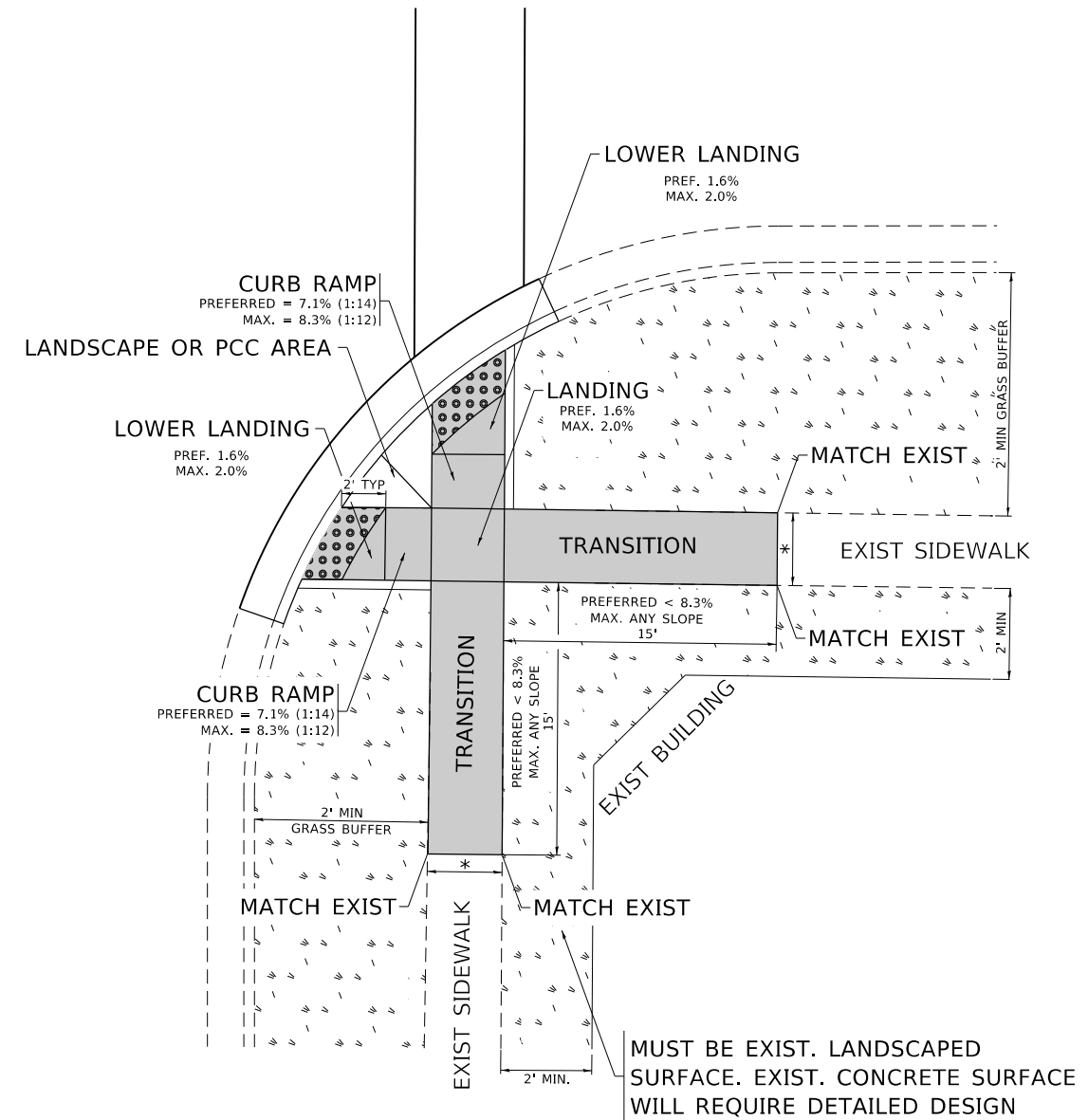
SCALE: _____ SHEET _____ OF _____ SHEETS STA. _____ TO STA. _____

ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

PD-03A



PD-03B



LEGEND

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

CONSTRUCTION NOTES:

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- * MATCH EXISTING SIDEWALK WIDTH

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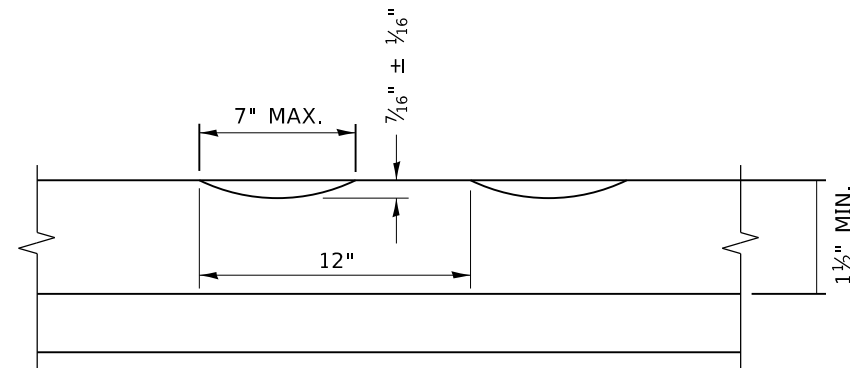
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

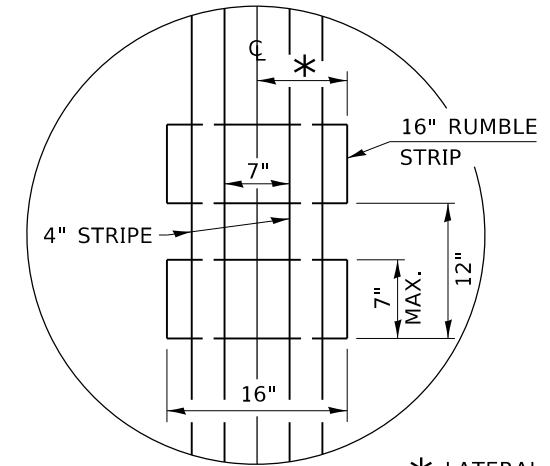
**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS
(PD-03)**

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	31
PD-03		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				



SECTION A-A



* LATERAL DEVIATION SHALL NOT EXCEED ONE INCH IN 100 FEET.

DETAIL B

GENERAL NOTES

CENTERLINE RUMBLE STRIPS SHALL BE CONSTRUCTED ACCORDING TO SECTION 642 ALONG THE CENTERLINE OF PAVEMENT.

SEE STANDARD 780001 FOR OTHER STRIPING LAYOUTS. RUMBLE STRIPS SHALL NOT BE PLACED ON BRIDGES.

ALL RUMBLE STRIPS SHALL BE MILLED.

CENTERLINE RUMBLE STRIPS SHALL BE CONTINUOUS THROUGH CONNECTIONS OF SIDEROADS WITH NO LEFT TURN LANES.

DISCONTINUE CENTERLINE RUMBLE STRIPS THROUGH THE LIMITS OF ALL LEFT TURN LANES, INCLUDING ANY LANE TAPER SECTIONS.

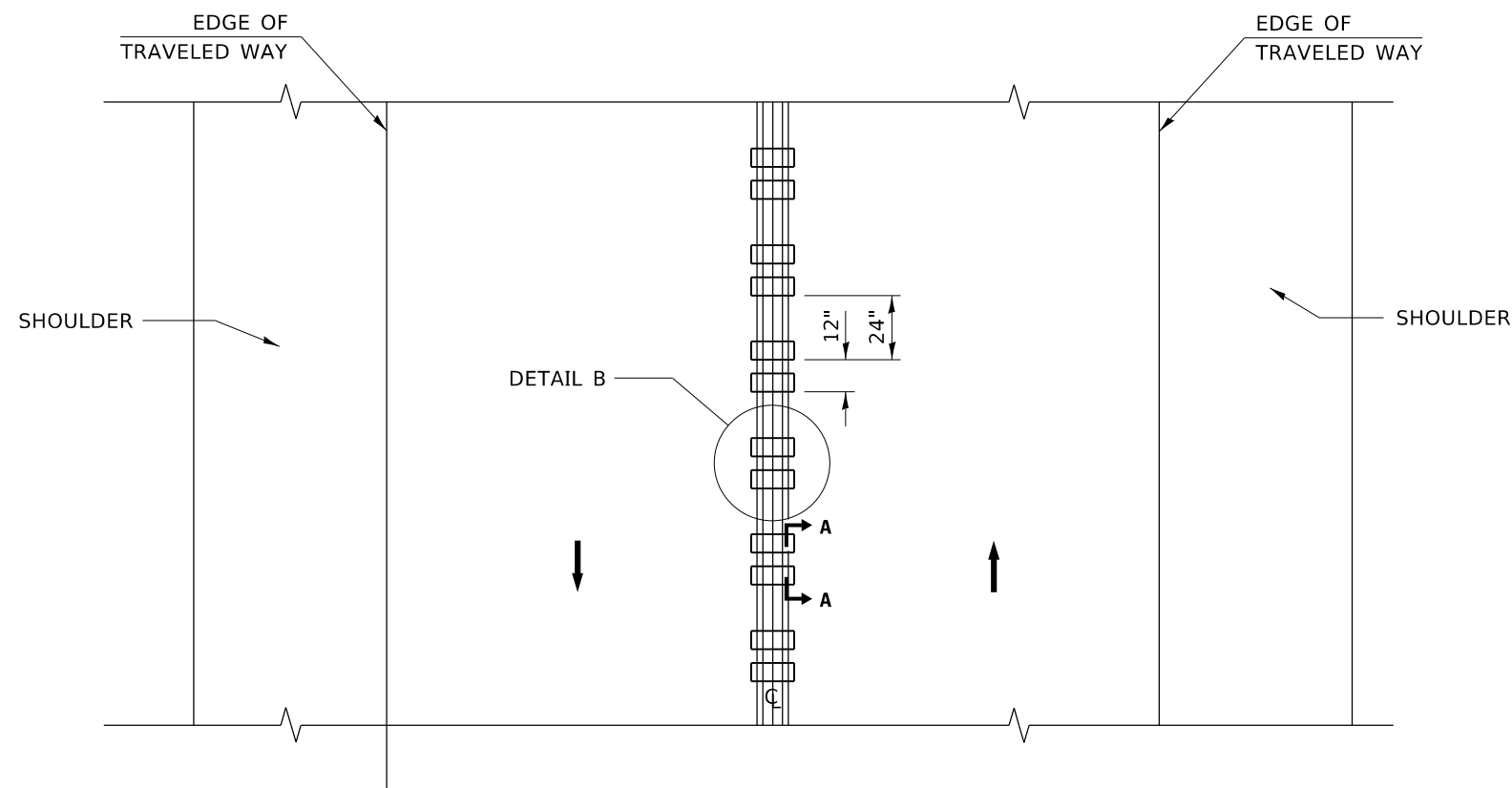
AFTER RUMBLE STRIPS ARE INSTALLED, THE PAVEMENT SURFACE SHALL BE SWEEPED CLEAN PRIOR TO THE PLACEMENT OF THE NEW PAVEMENT MARKINGS.

WHERE USED, ADJUST SPACING OF RAISED REFLECTIVE PAVEMENT MARKERS TO FALL IN WIDER GAP BETWEEN RUMBLE STRIPS.

BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR CENTERLINE-RUMBLE STRIP OF THE WIDTH SPECIFIED.

HOT-SPRAY THERMOPLASTIC PAVEMENT MARKING WILL BE USED OVER THE RUMBLE STRIPS, AND WILL BE PAID FOR SEPARATELY.



TWO-WAY ROAD

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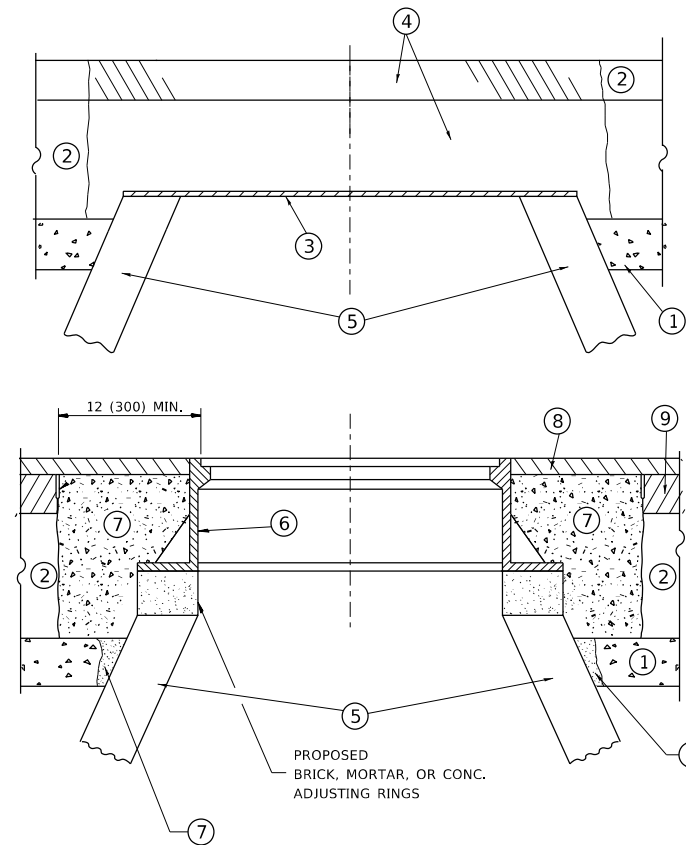
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PLOT DATE = 12/16/2019	DATE - 08-06-2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RUMBLE STRIPS FOR CENTERLINE, NON-FREEWAY

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BD 55			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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PLOT DATE = 12/16/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	34
BD600-03 (BD-8)		CONTRACT NO. 62J20		
ILLINOIS		FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE 2)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE 2)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

¼" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE 1).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

PROPOSED ¾" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

NOTE:

- SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED.
- CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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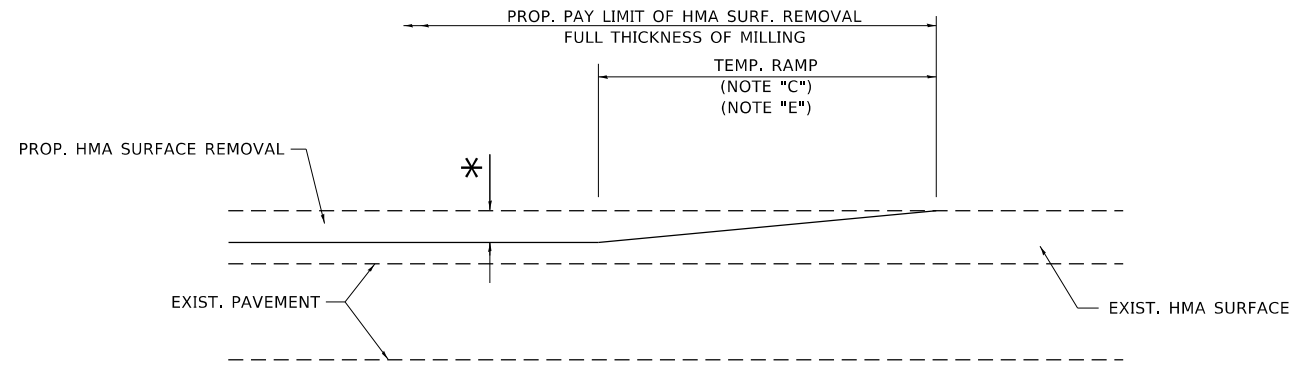
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

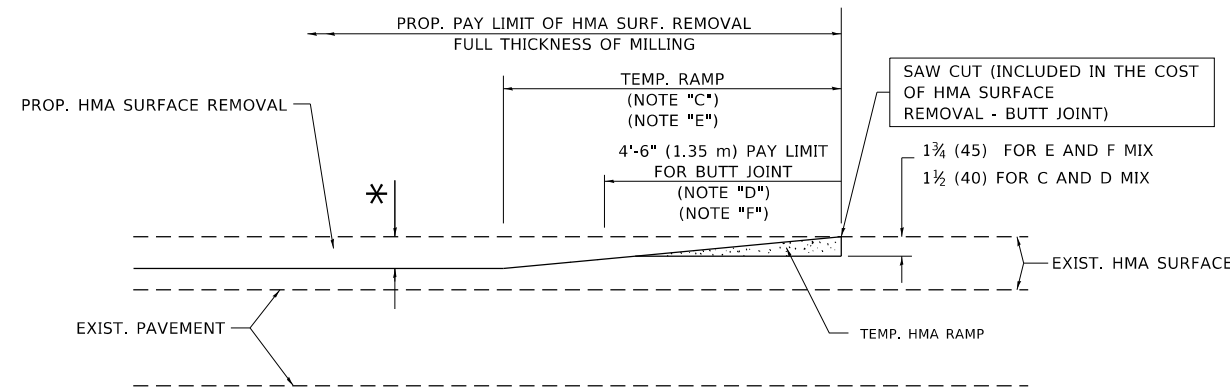
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326	2019-042-RS	KANE	44	36
BD600-06 (BD-24)		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

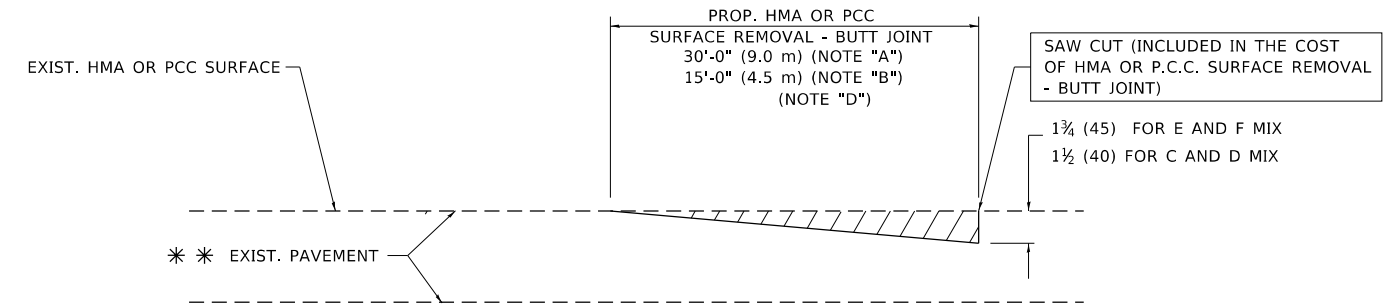


HMA CONSTRUCTED TEMPORARY RAMP

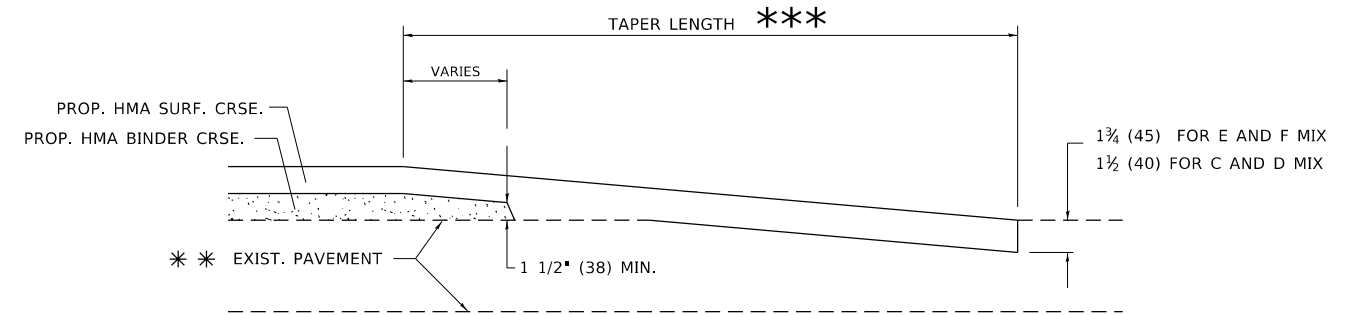
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

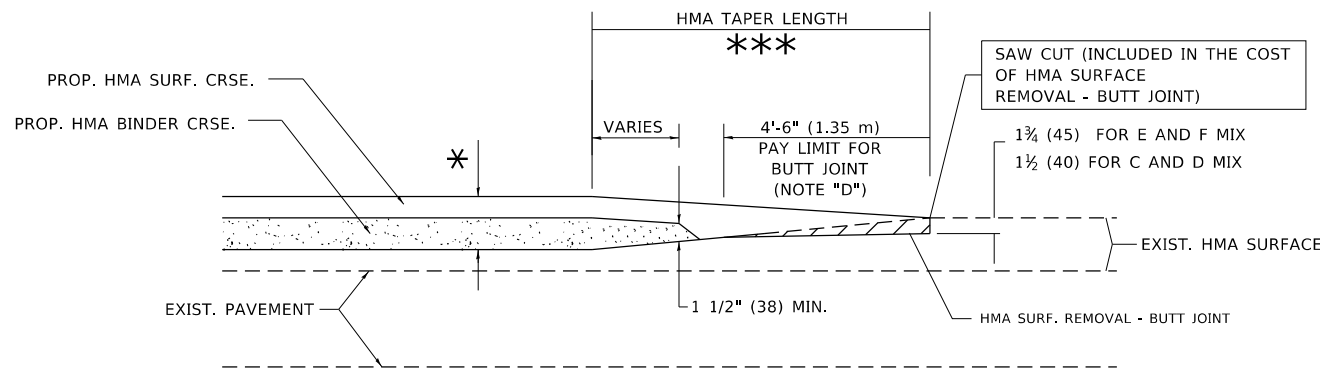
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

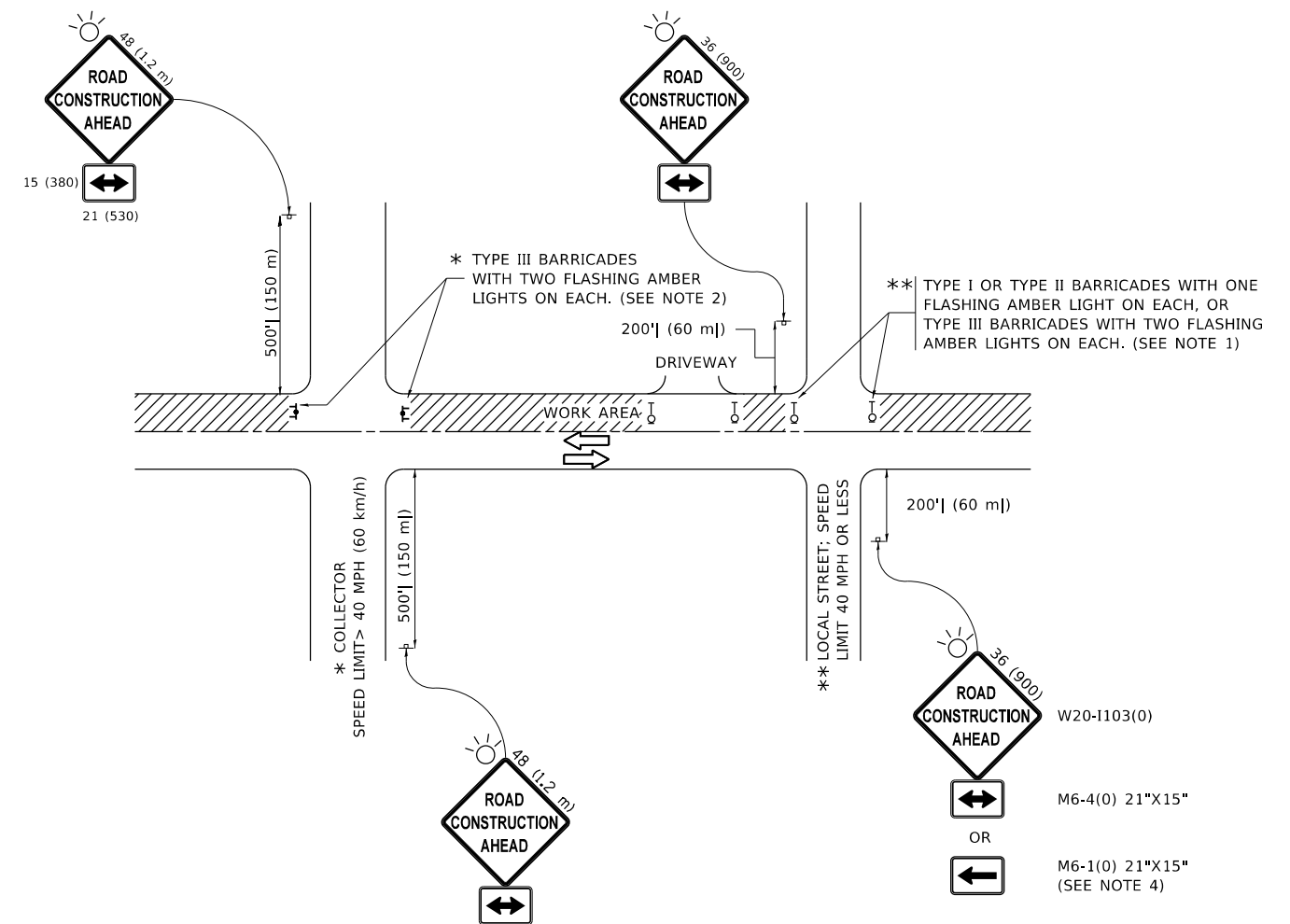
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	37
BD400-05 BD32		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				

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USER NAME = allababidiz	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT DATE = 12/16/2019	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R.BORO 01-01-07



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - b) CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
4. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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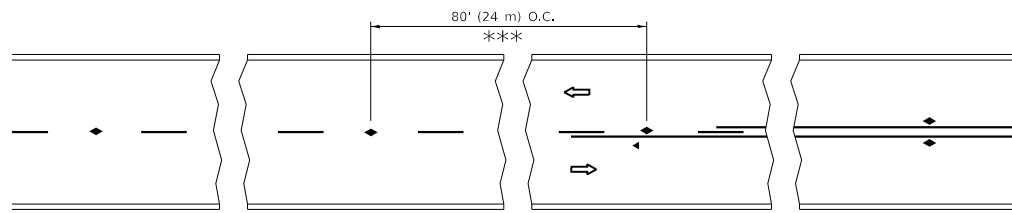
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PLOT DATE = 12/16/2019	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

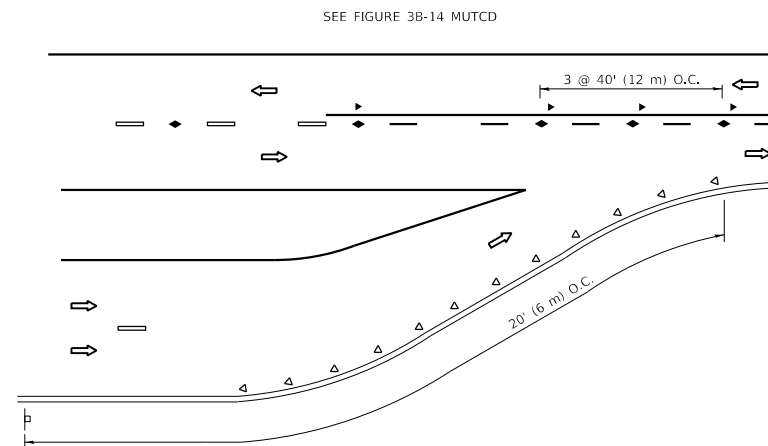
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62J20	
ILLINOIS FED. AID PROJECT				

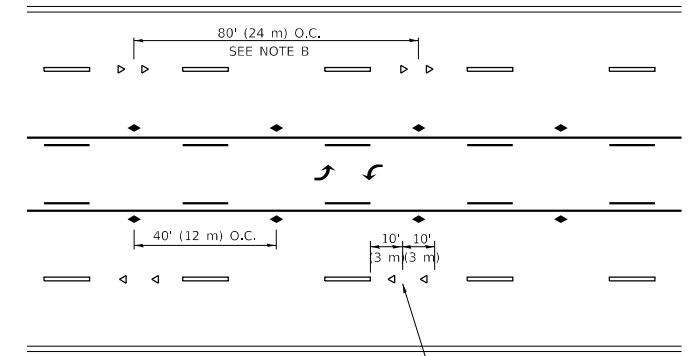


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

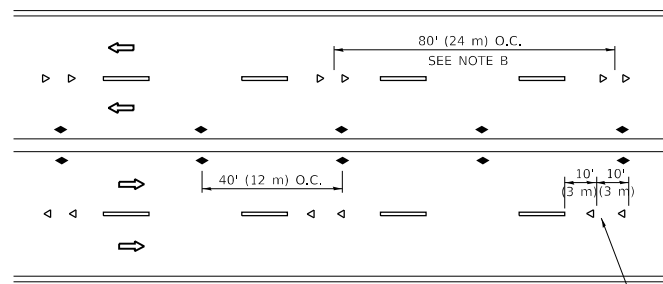
TWO-LANE/TWO-WAY



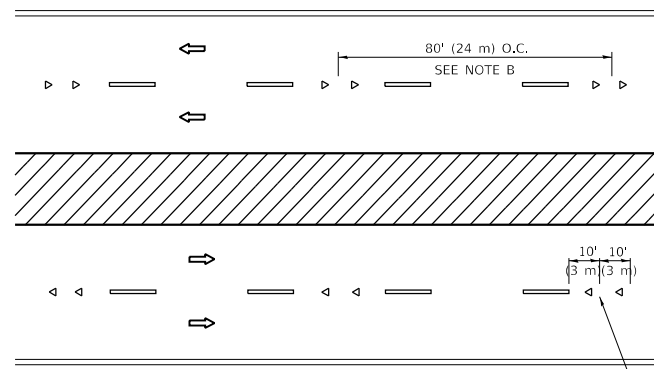
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

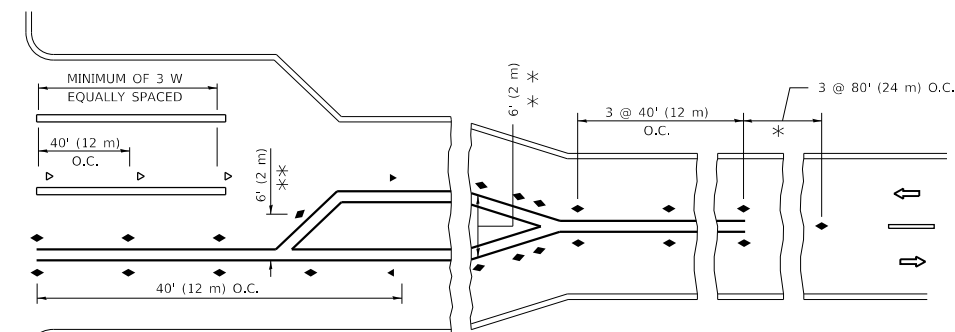
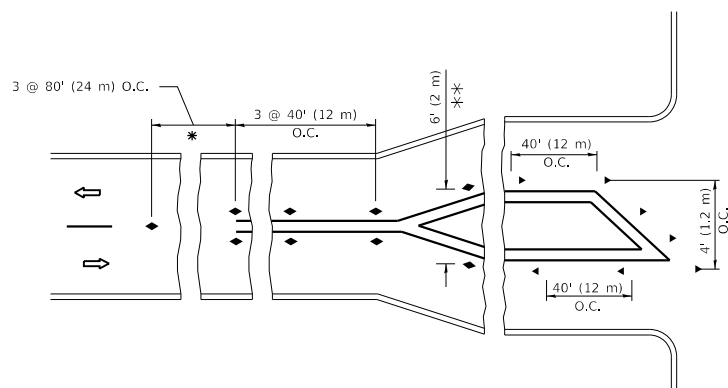
- YELLOW STRIPE
- ▬ WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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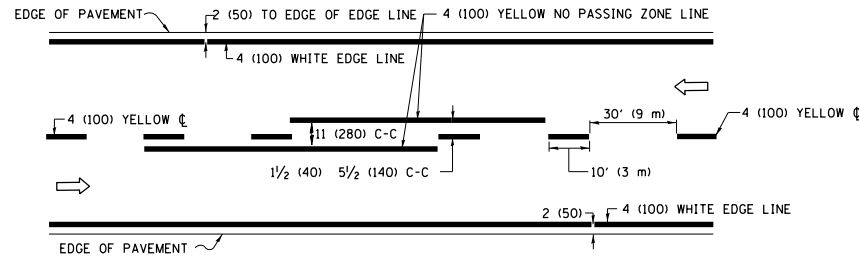
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PLOT DATE = 12/16/2019	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

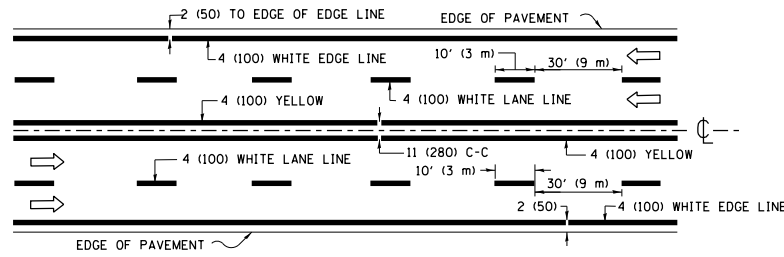
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

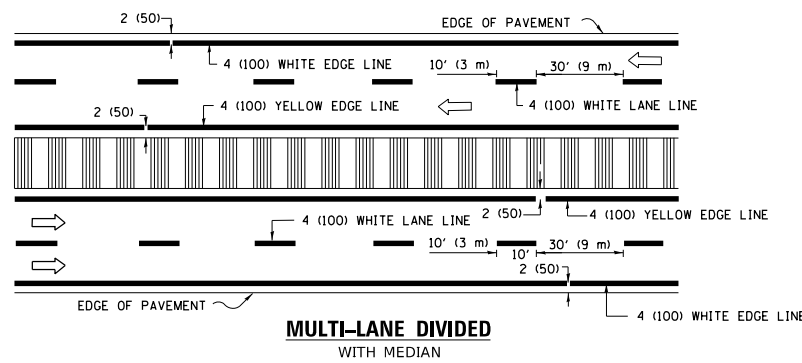
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	39
TC-11		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

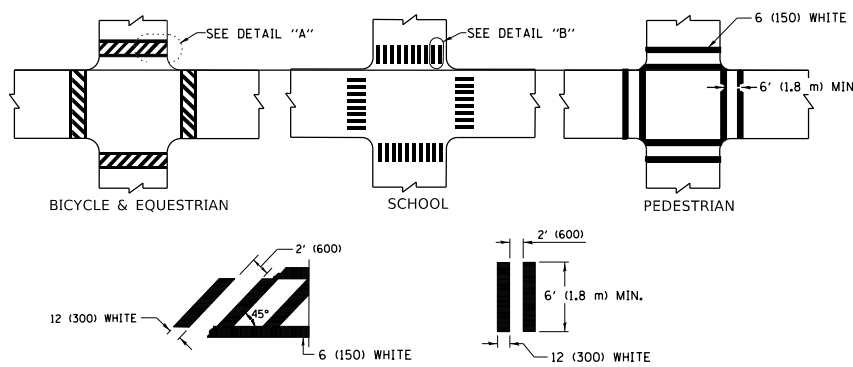


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

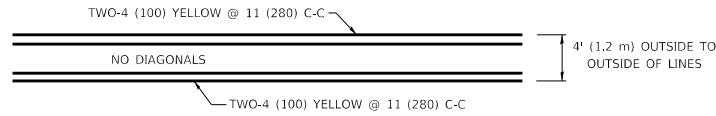


DETAIL "A"

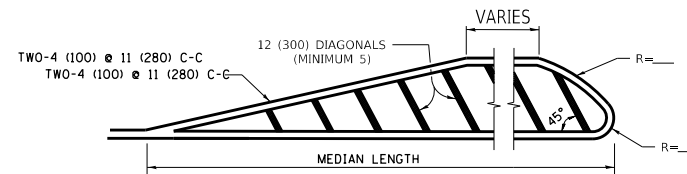
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

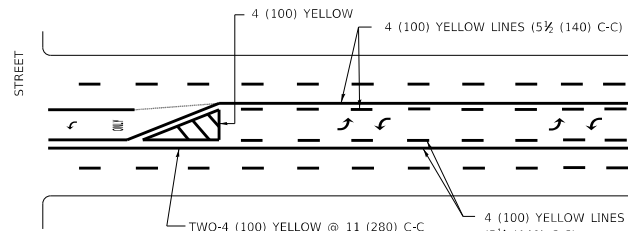


4' (1.2 m) WIDE MEDIANS ONLY



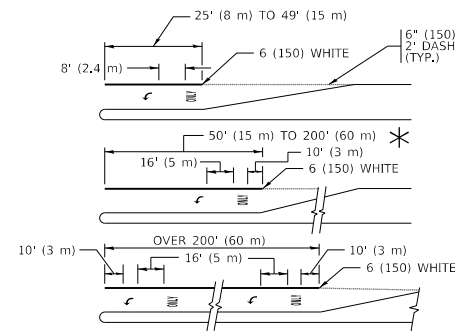
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

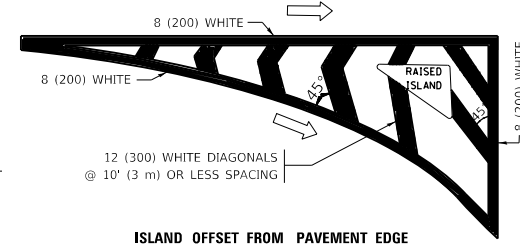
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**



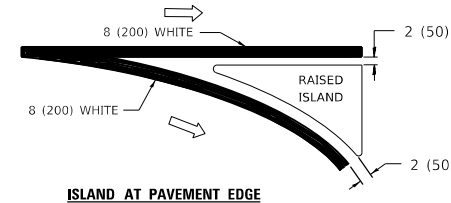
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

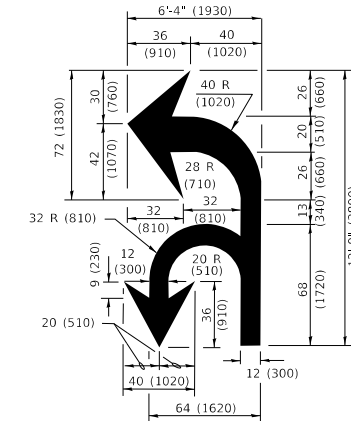


ISLAND OFFSET FROM PAVEMENT EDGE

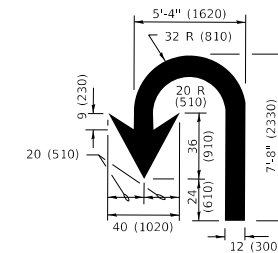


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES *RR* IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 12/16/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE TYPICAL PAVEMENT MARKINGS		
SCALE: NONE	SHEET 1 OF 2 SHEETS	STA. TO STA.

F.A.P. RTE. 326	SECTION 2019-042-RS	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 40
TC-13			CONTRACT NO. 62J20	
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

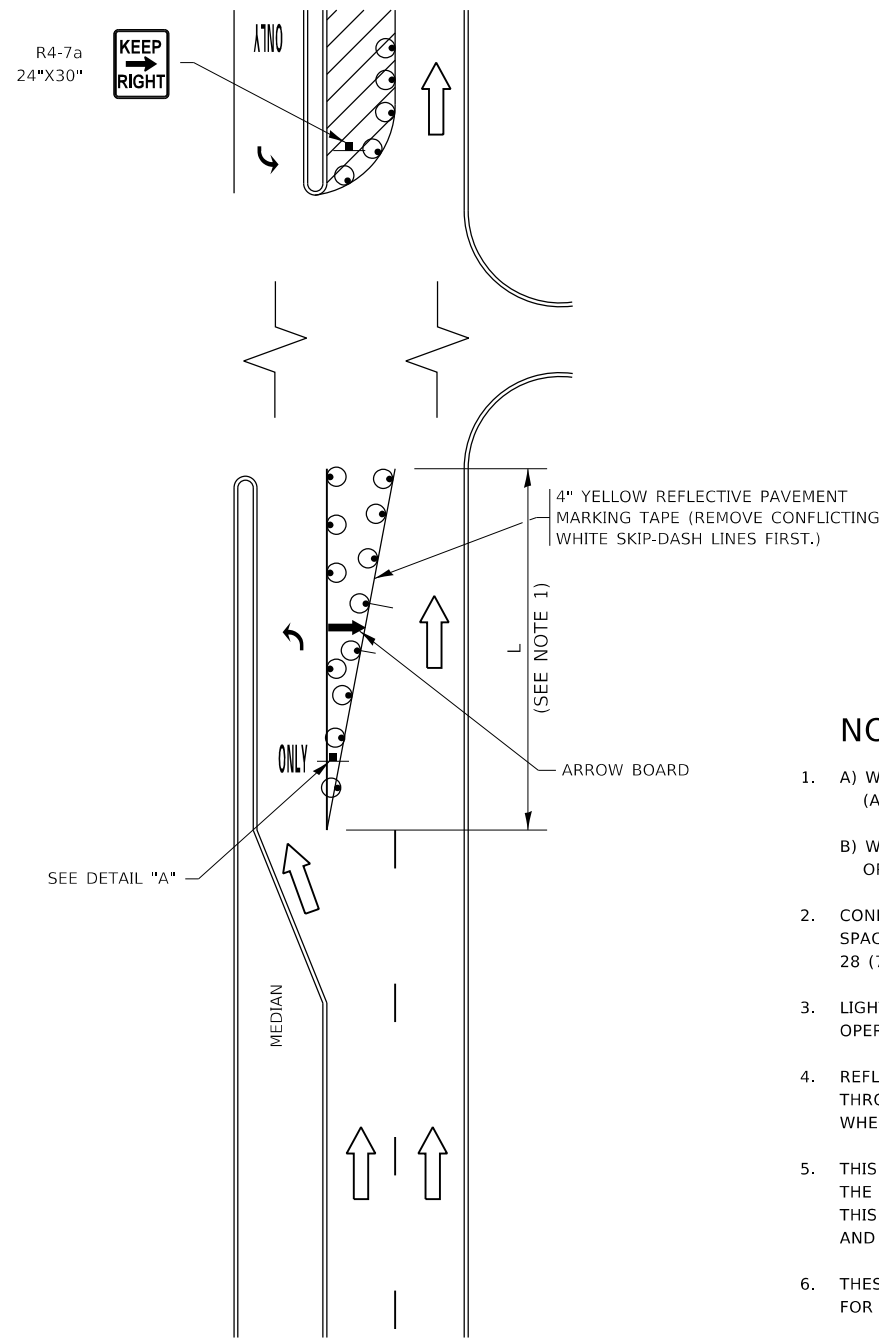


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

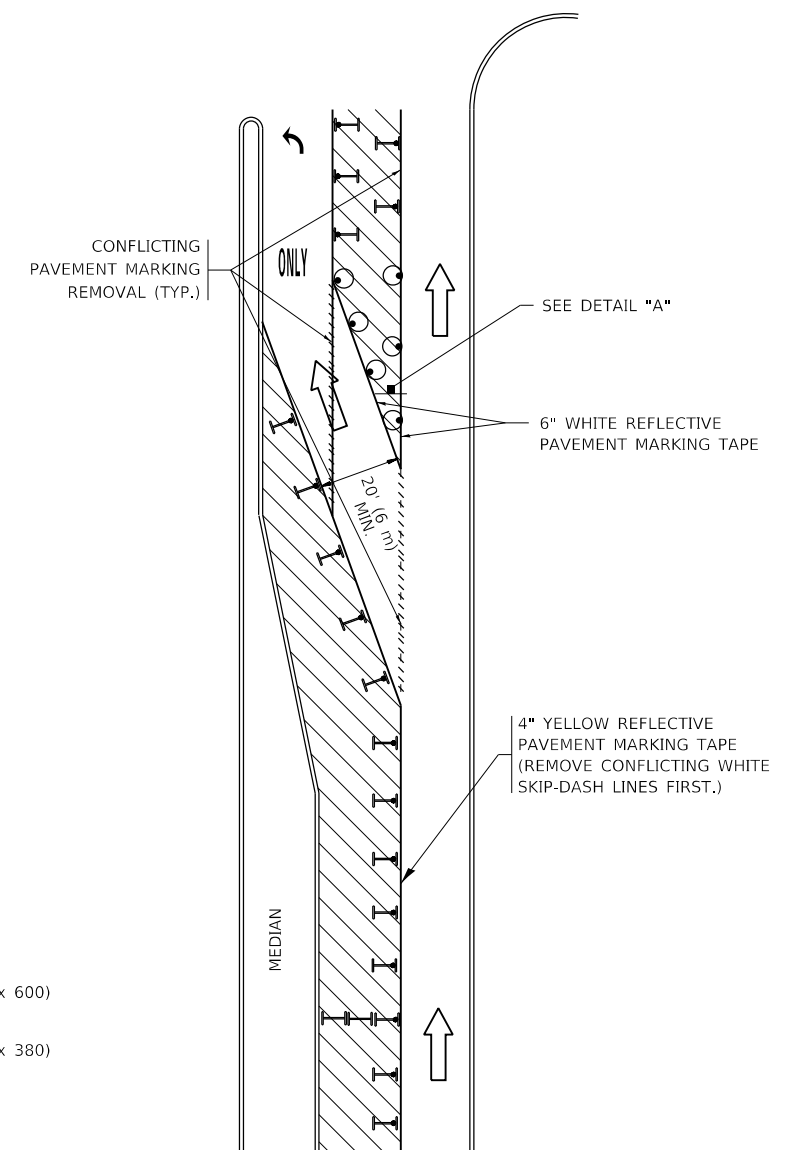


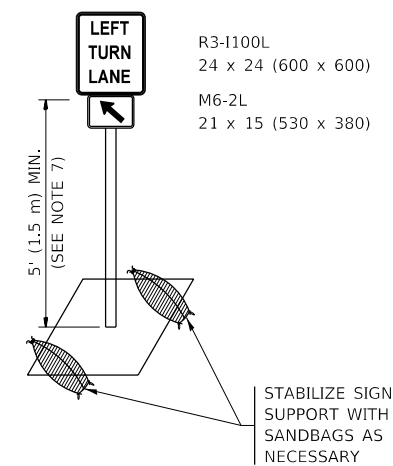
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
 B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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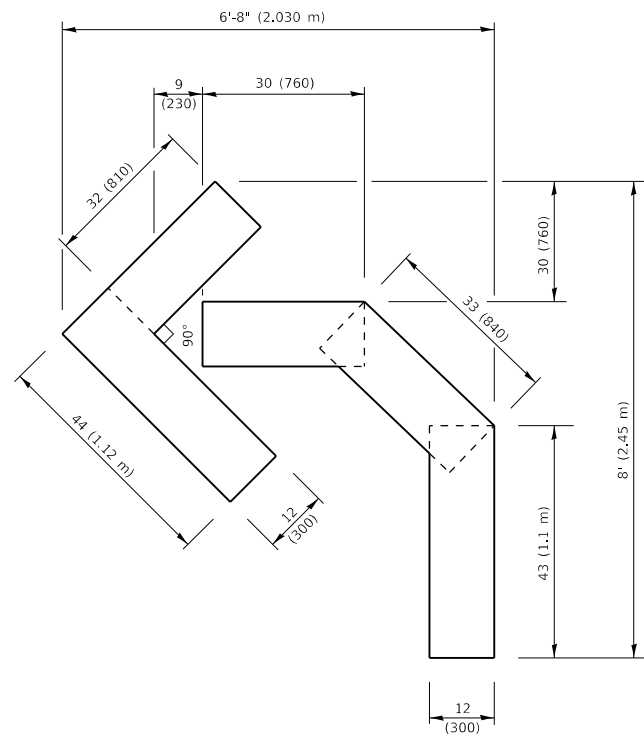
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DRAWN - A. HOUSEH 11-07-95	REVISOR - A. SCHUETZE 07-01-13	
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 12/16/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

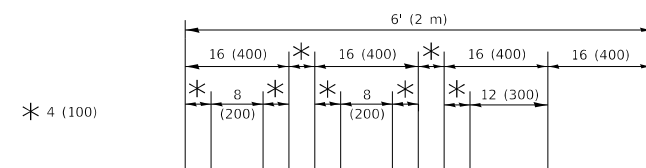
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F.A.P. RTE. 326	SECTION 2019-042-RS	COUNTY KANE	TOTAL SHEETS 44	SHEET NO. 41
TC-14		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				



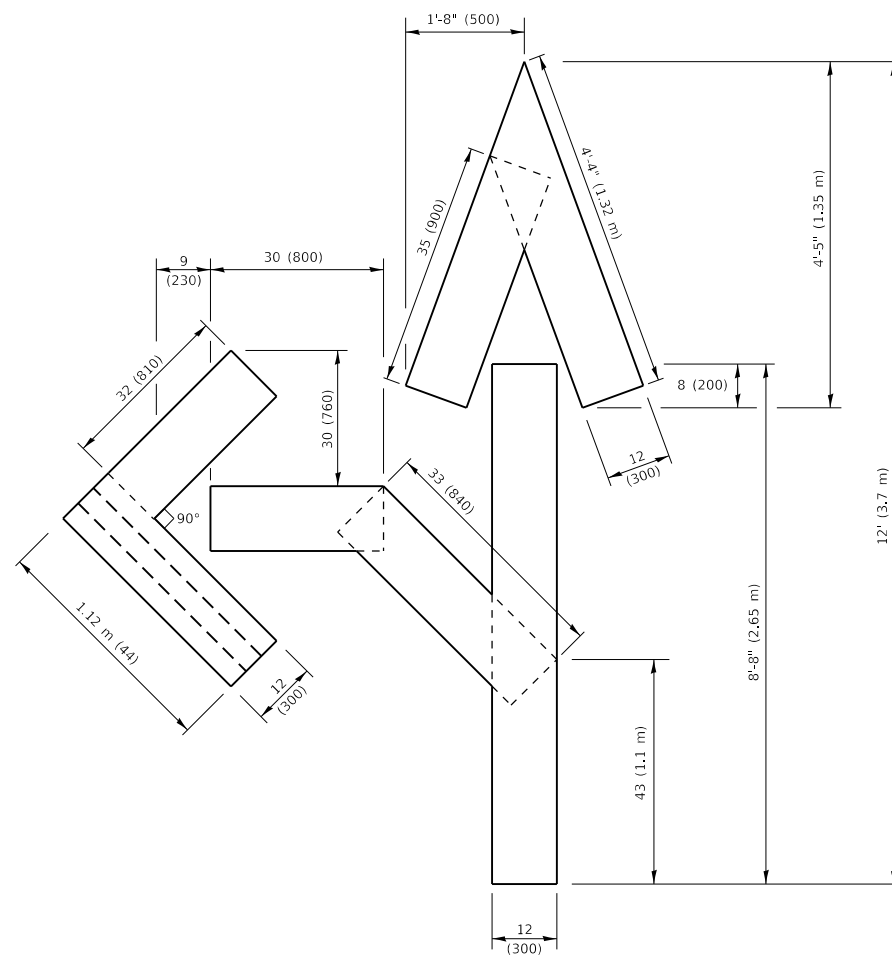
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

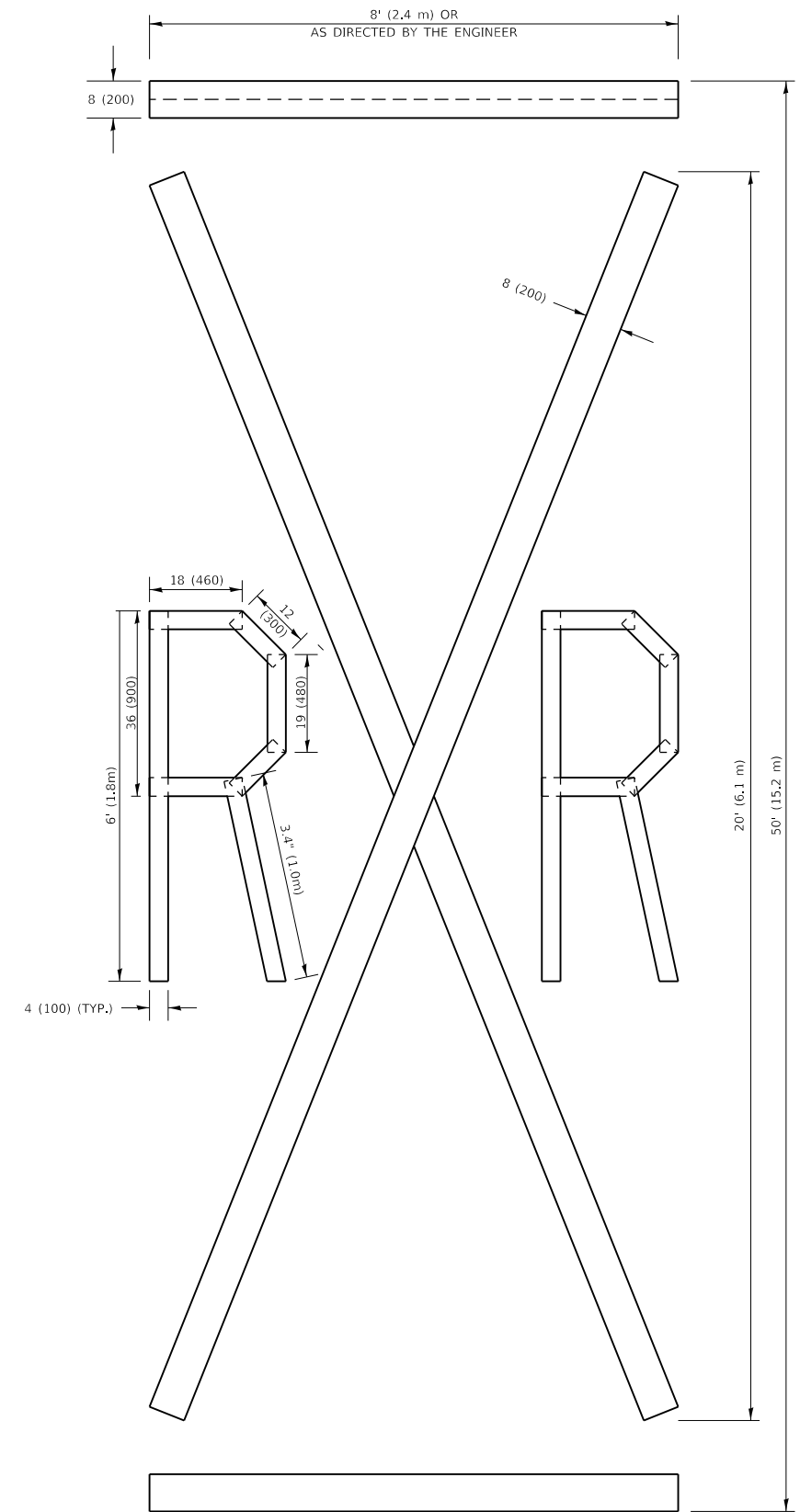


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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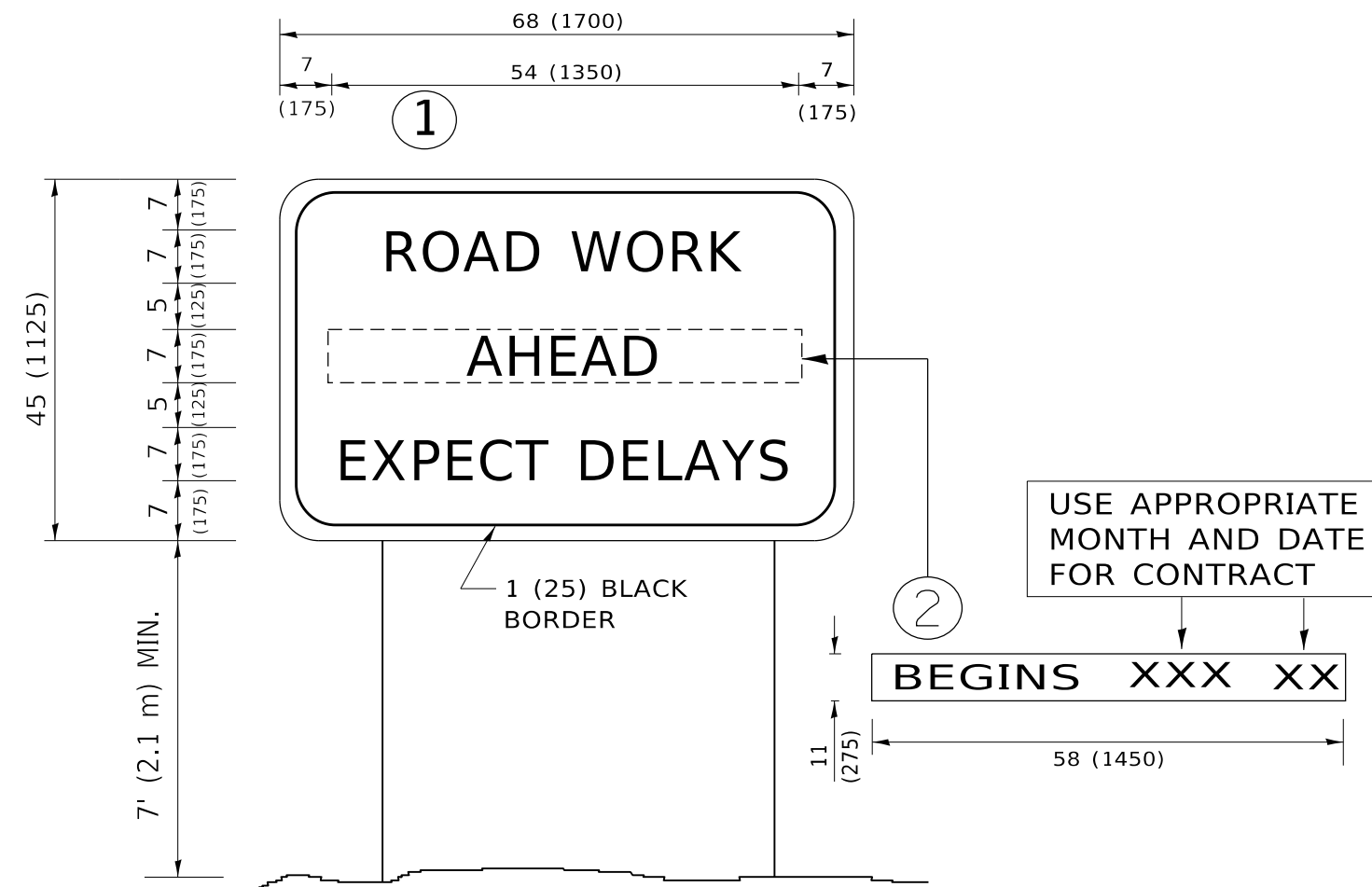
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PLOT DATE = 12/16/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	42
TC-16		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE: \\nas01c.psu.edu\lanroom\data\illinois.gov\RWIDOT\Documents\DOT_Offices\District_1\Projects\116419\CADD\Drawings\Design\DESIGN.dgn

USER NAME = allababidiz	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 12/16/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

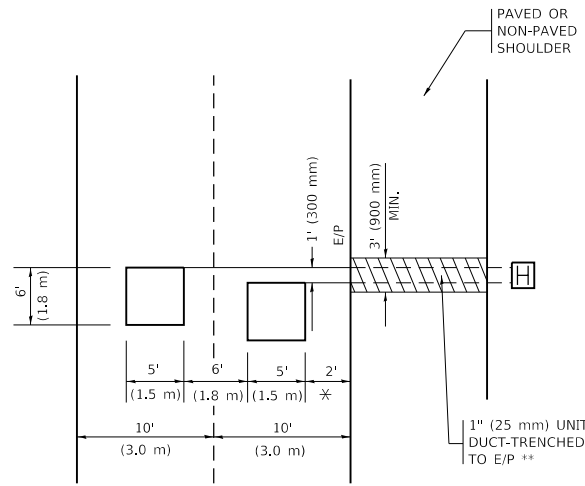
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	43
TC-22			CONTRACT NO. 62J20	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



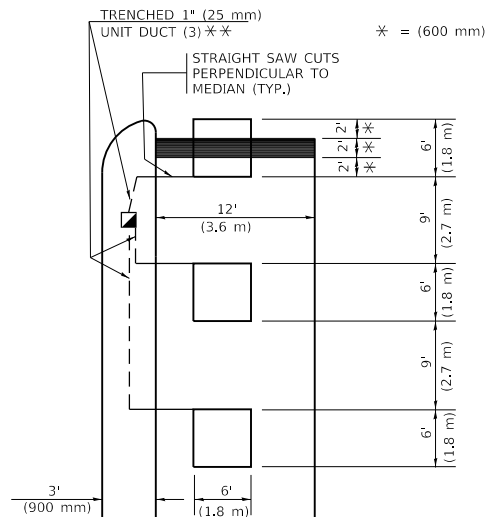
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



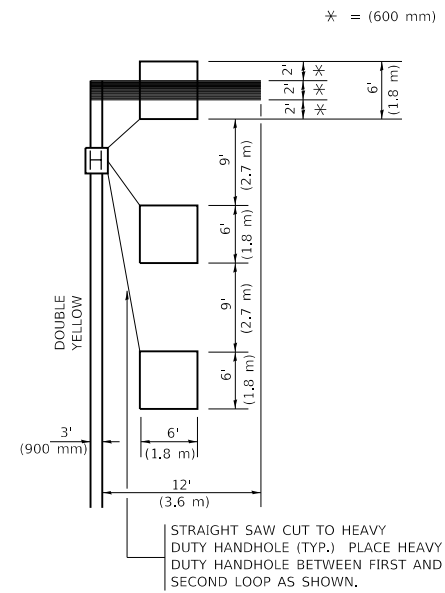
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS

VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH (PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

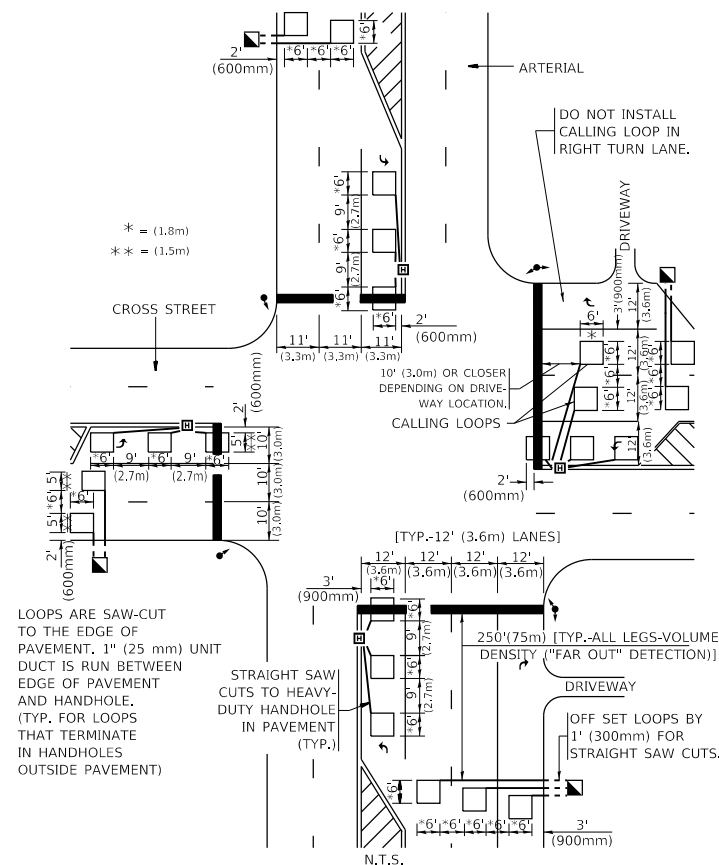
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

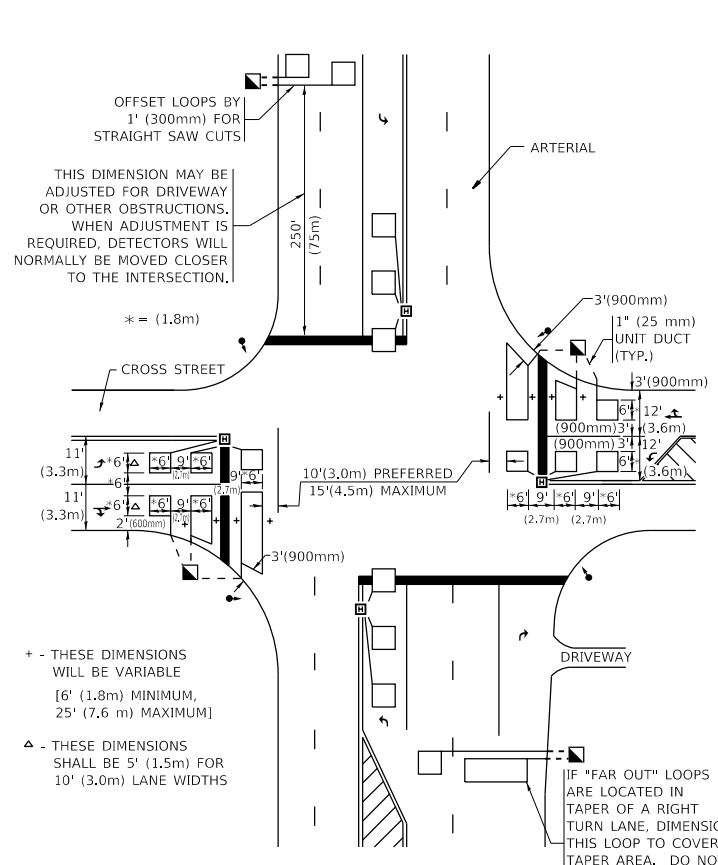
ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



* = (1.8m)
** = (1.5m)

DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]
△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

DETAIL 2
N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

USER NAME = allababidiz	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/16/2019	CHECKED - R.K.F.	REVISED -
	DATE -	REVISED -

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
326	2019-042-RS	KANE	44	44
TS-07		CONTRACT NO. 62J20		
ILLINOIS FED. AID PROJECT				