

03-06-2020 LETTING ITEM 120

FOR INDEX OF SHEETS,
SEE SHEET NO. 2

FOR HIGHWAY STANDARDS,
SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU ROUTE 2593 (WOODWARD AVENUE)
75TH STREET (FAU 0369) TO
WOODWARD NORTH VILLAGE LIMIT (WOODWARD AVE)
RESURFACING, CURB, AND SIDEWALK
SECTION NO.: 19-00082-00-RS
PROJECT NO.: ZR18(517)
VILLAGE OF WOODRIDGE
DUPAGE COUNTY
JOB NO: C-91-357-19

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19 00082-00-RS	DUPAGE	38	1
		ILLINOIS	CONTRACT NO. 61G27	



LOCATION OF SECTION INDICATED THUS: —■—

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED Nov 30 20 19
[Signature]
VILLAGE OF WOODRIDGE, DIRECTOR OF PUBLIC WORKS

PASSED DEC 16 20 19
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

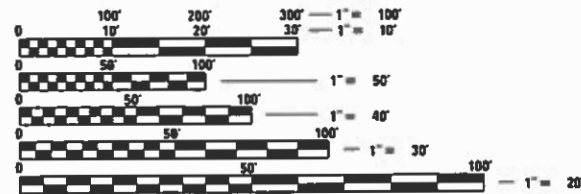
RELEASING FOR
BID BASED ON
LIMITED REVIEW December 17 20 19
[Signature]
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

WOODWARD AVENUE

DESIGN DESIGNATION: MINOR ARTERIAL
SPEED LIMIT = 35 MPH
TRAFFIC ADT = 22,117 (2019)/26,000 (2040)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

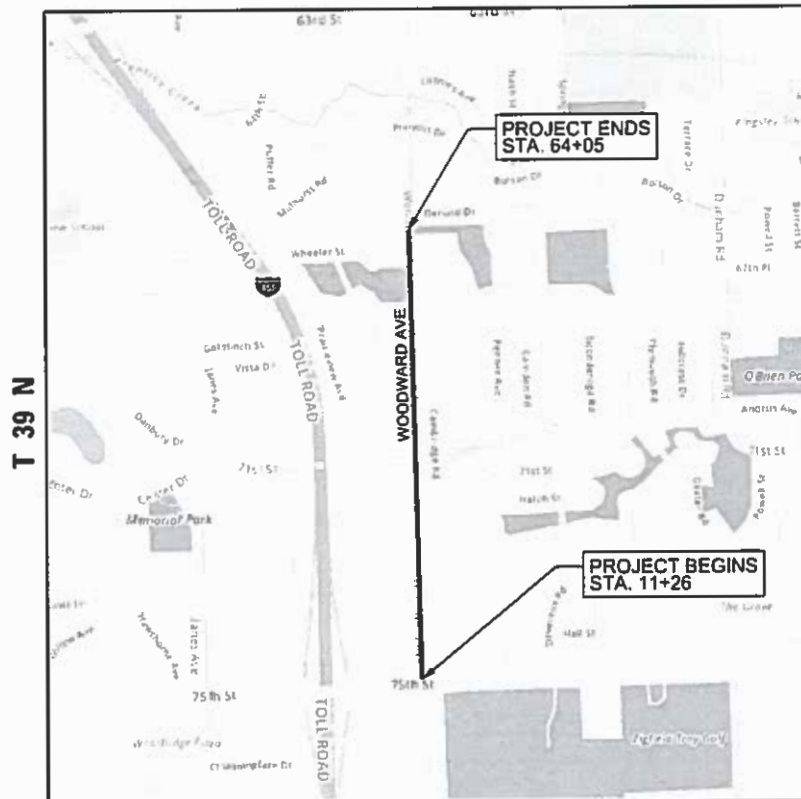
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROJECT MANAGER: KEVIN VANDEWOESTYNE

CONTRACT NO. 61G27

R 10 E 3RD P.M.



**DOWNERS GROVE TOWNSHIP
LOCATION MAP**

N.T.S.

WOODWARD AVENUE GROSS LENGTH = 5,279 FT. (1.00 MILES)

WOODWARD AVENUE NET LENGTH = 5,279 FT. (1.00 MILES)



[Signature] 11/20/19
KEVIN C. VANDEWOESTYNE, P.E.
NO. 062-060059
EXP. DATE 11-30-2021

INDEX OF SHEETS

1	TITLE SHEET
2	GENERAL NOTES
3-4	SUMMARY OF QUANTITIES
5-7	WOODWARD AVENUE TYPICAL SECTIONS
8-9	WOODWARD AVENUE ALIGNMENT, TIES AND BENCHMARKS
10-14	WOODWARD AVENUE EXISTING AND REMOVAL PLANS
15-19	WOODWARD AVENUE ROADWAY PLANS
20-24	WOODWARD AVENUE PAVEMENT MARKING, EROSION CONTROL AND LANDSCAPING PLANS
25-29	WOODWARD AVENUE SIDEWALK PLANS
30	WOODWARD AVE @ 75TH STREET EXISTING DETECTOR LOOP PLANS
31-39	DISTRICT 1 DETAILS

DISTRICT 1 DETAILS

BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
TC-13	TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TS-07	DISTRICT 1- DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-05	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS- DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTANT OR MOVING OPERATIONS FOR SPEEDS <
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701602-10	URBAN LANE CLOSURE, MULTILANE, 2-WAY W/ BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2020; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS, THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT 630-719-4753 AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. ATTENTION IS CALLED TO SECTION 701 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION. THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.

- THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT ONLY ONE TEMPORARY LANE CLOSURE ON ONE STREET IN ONE DIRECTION IS IMPLEMENTED AT A TIME. A LANE CLOSURE WILL ONLY BE PERMITTED DURING CONSTRUCTION OPERATIONS AND IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARDS. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN ALL SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES, INCLUDING FLAGGERS, REQUIRED TO MAINTAIN TRAFFIC FLOW.
- THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUT AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL CONCRETE CURB OR COMBINATION CONCRETE CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS AS SPECIFIED, OR AS DIRECTED BY THE ENGINEER.
- PRIOR TO APPLYING THE TACK COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE GRINDINGS, LEAVES, OF ALL DUST, DIRT WEEDS AND OTHER FOREIGN MATERIAL. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE.
- THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER. DEBRIS AND ANY SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION, DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF-SITE.
- THE CONTRACTOR SHALL CONFINE OPERATIONS WITHIN THE CONSTRUCTION LIMITS OF THE PROJECT. ANY DAMAGE INSIDE AND OUTSIDE OF SAID CONSTRUCTION LIMITS SHALL BE REPAIRED.
- DURING CONSTRUCTION, ALL SIGNS, BARRICADES, AND TEMPORARY PAVEMENT MARKINGS WILL CONFORM TO THE ILLINOIS MUTCD. THE VEHICULAR ACCESS TO LOCAL PROPERTIES WILL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION, EXCEPT WHEN CONSTRUCTION OPERATIONS OCCUR DIRECTLY ON OR IN FRONT OF DRIVEWAYS. IN THIS CASE, FLAG PERSONS WILL BE USED TO DIRECT AND CONTROL TRAFFIC.
- THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10" UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.
- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, APRIL 1, 2016 AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS JANUARY 1, 2020.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, THEIR AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE CONTRACTOR SHALL COORDINATE PAVING OPERATIONS FOR BOTH HMA LEVEL BINDER AND SURFACE COURSES SO THAT THE LONGITUDINAL JOINTS ARE CLOSED AND COMPACTED AT THE END OF EACH DAY. PAVING OPERATIONS SHALL BE SCHEDULED SO THAT ADJACENT LANES ARE PAVED IN THE SAME DIRECTION AS THE INITIAL LANE MINIMIZING THE TIME THE EDGE OF A PAVEMENT MAT IS ALLOWED TO COOL.
- THE CONTRACTOR SHALL USE 2 CHANGEABLE MESSAGE SIGNS AT LOCATIONS TO BE DETERMINED BY THE ENGINEER FOR A PERIOD FROM ONE WEEK PRIOR TO THE START OF CONSTRUCTION TO THE CONCLUSION OF THE PROJECT.
- THE CONTRACTOR SHALL ENSURE ALL WATER SYSTEM VALVES, VALVE VAULTS, AND SANITARY SEWER MANHOLES REMAIN READILY ACCESSIBLE FOR EMERGENCY OPERATIONS. THE LOCATIONS OF ALL WATER AND SANITARY FACILITIES SHALL BE MARKED AND READILY VISIBLE AT ALL TIMES.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF WATER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO SELECT DUMPING SITES. HE/SHE SHALL PROVIDE A LIST OF THESE SITES TO THE ENGINEER FOR HIS/HER EXAMINATION AND GENERAL INFORMATION.
- ALL EXCESS MATERIAL (BROKEN CONCRETE, ASPHALT, CULVERT PIPE, WASTE ROADWAY EXCAVATION, AND SURPLUS MATERIALS FROM UTILITY TRENCHES) SHALL BE WASTED OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.
- ALL PROPERTY AND SURFACE STRUCTURES WITHIN THE RIGHT-OF-WAY SHALL BE PROTECTED DURING CONSTRUCTION OPERATIONS UNLESS THE ENGINEER DIRECTS REMOVAL FOR PURPOSES RELATED TO CONSTRUCTION UNDER THIS CONTRACT. ANY FENCES, POLES, FLAGSTONE, DECORATIVE STONE, SPECIAL LANDSCAPING, OR OTHER MAN MADE SURFACE IMPROVEMENT WHICH IS REMOVED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY HIM TO ITS ORIGINAL CONDITION AFTER THE CONSTRUCTION ACTIVITIES ARE COMPLETED.
- THE CONTRACTOR SHALL MAKE FULL DEPTH SAW CUTS AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL COMBINATION CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS OR AS DIRECTED BY THE ENGINEER.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE AND PUBLIC DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES TO ACCEPT ALL STORM WATER THAT WILL BE DELIVERED BY THESE DRAINS AND BASINS AND SHALL DISCHARGE THE SAME. IF NECESSARY, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT AND TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF WATER WHICH IS RECEIVED FROM ALL TEMPORARY CONNECTIONS. THESE TEMPORARY FACILITIES SHALL BE MAINTAINED UNTIL ALL PERMANENT CONNECTIONS ARE COMPLETED.

- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE FOLLOWED WHEN EXISTING CURB AND GUTTER IS REMOVED AND EXISTING DRAINAGE STRUCTURES ARE TO REMAIN ACTIVE. THIS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE DETAIL PROVIDED IN THE CONTRACT DOCUMENTS.
 - THE THICKNESS OF THE HMA SHOWN ON THE PLANS IS NOMINAL. DEVIATIONS MAY OCCUR IN THE FIELD DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE UPON WHICH THE HMA IS BEING PLACED.
 - THE LOCATIONS AND ELEVATIONS OF THE VARIOUS UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE NOT TO BE TAKEN AS EXACT. THE CONTRACTOR SHALL EXERCISE CARE WHEN CONDUCTING CONSTRUCTION OPERATIONS NEAR UTILITIES TO PREVENT DAMAGE. THE FAILURE OF A UTILITY COMPANY TO ACCURATELY LOCATE THEIR UTILITY DOES NOT FREE THE CONTRACTOR FROM RESPONSIBILITY. THE MAJOR CONCERN OF THE VILLAGE OF WOODRIDGE IS PUBLIC SAFETY.
 - UNLESS OTHERWISE APPROVED BY THE ENGINEER, CONTRACTOR SHALL BE REQUIRED TO KEEP ALL DRIVEWAY ENTRANCES OPEN TO TRAFFIC. ALL CONCRETE WORK AT ENTRANCES SHALL BE CONSTRUCTED 1/2 AT A TIME TO ALLOW ACCESS DURING THE CONCRETE CURING PERIOD.
 - THE CONTRACTOR SHALL HAVE LINE AND/OR FORMS SET A MINIMUM OF FOUR WORKING HOURS PRIOR TO THE SCHEDULED ARRIVAL OF CONCRETE ON SITE FOR THE PLACEMENT OF CURB AND GUTTER, DRIVEWAYS, AND SIDEWALK TO ALLOW THE ENGINEER TIME TO CHECK LINE AND GRADE.
 - ALL RADII FOR PROPOSED COMBINATION CONCRETE CURB AND GUTTER SHALL BE PLACED AT THE EXISTING DIMENSION UNLESS OTHERWISE INDICATED ON THE PROJECT PLANS OR AS DIRECTED BY THE ENGINEER. ELEVATIONS SHOWN AT POINT ON THE CURB INDICATED FLOW LINE ELEVATIONS UNLESS NOTED OTHERWISE.
 - WHEN WATER SERVICE BOXES FALL WITHIN THE LIMITS OF CONCRETE FLATWORK, THE CONTRACTOR SHALL PROVIDE EITHER A SECTION OF 4" POLY VINYL CHLORIDE (PVC) PIPE OR 4" HIGH DENSITY POLYETHYLENE (HDPE) PIPE TO SLEEVE THE BOX. THE SLEEVE SHALL EITHER BE REMOVED OR TRIMMED TO MATCH THE FINISHED CONCRETE GRADE LEVEL. THE FINAL RESULT SHALL BE A SMOOTH FINISHED "BOX-OUT" AROUND THE SERVICE BOX WHICH SHALL FACILITATE EASY REMOVAL OF THE CAP AT MEET THE FINISHED GRADE. ALL WATER SERVICE BOXES WITHIN PAVEMENT RESURFACING SHALL BE ADJUSTED TO MEET THE FINISHED GRADE LEVEL.
 - WHEN A SEWER STRUCTURE FALLS WITHIN THE LIMITS OF A CONCRETE DRIVEWAY, THE CONTRACTOR SHALL PLACE EXPANSION MATERIAL IN A BOX APPROXIMATELY EIGHTEEN (18") INCHES FROM THE CENTER OF THE LID FORMING A SQUARE "BOX-OUT" AROUND THE FRAME. THE RESULT SHALL BE A CONDITION THAT WILL ALLOW FOR THE REMOVAL OF THE SQUARE OF CONCRETE FROM THE DRIVEWAY FOR THE PURPOSE OF REPLACING THE FRAME WITHOUT DAMAGING THE REMAINDER OF THE DRIVEWAY PAVEMENT.
 - ALL CONSTRUCTION PERSONNEL SHALL BE REQUIRED TO WEAR AN ANSI/ISEA 107-2015 COMPLIANT SAFETY VEST AT ALL TIMES WHILE ON THE CONSTRUCTION SITE.
 - WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1-1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
 - BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD THE LOCATIONS OF ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE REESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
 - IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
 - THE CONTRACTOR SHALL PROVIDE SAFE AND ORDERLY PASSAGE FOR TRAFFIC AND PEDESTRIANS WHERE CONSTRUCTION OPERATIONS IMPACT PUBLIC THOROUGHFARES AND ADJACENT PROPERTY. THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
 - ALL SIDEWALK CURB RAMPS SHALL BE COMPLIANT WITH ALL APPLICABLE ADA STANDARDS AND INCLUDED DETAILS. INDIVIDUAL DESIGN DETAILS HAVE BEEN PROVIDED FOR ALL LOCATIONS WITH AN EXISTING SLOPE OVER 5%.
 - A NOMINAL QUANTITY HAS BEEN INCLUDED FOR THE FOLLOWING PAY ITEMS:
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- AGGREGATE SUBGRADE IMPROVEMENT, 12"
- CLASS D PATCHES
 - ALL UTILITY STRUCTURES SHALL REMAIN ACCESSIBLE DURING CONSTRUCTION. WHEN UTILITY STRUCTURES ARE TO BE ADJUSTED UNDER TRAFFIC, TEMPORARY RAMPS SHALL BE CONSTRUCTED AND MAINTAINED AT BOTH UPSTREAM AND DOWNSTREAM ENDS OF THE UTILITY CASTING IN ACCORDANCE WITH ARTICLE 406.08.
- GENERAL NOTES - SEWERS:
- ALL FRAMES WITH CLOSED LIDS BEING FURNISHED FOR THIS PROJECT AS CONSTRUCTION, ADJUSTMENT, OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, INLET, OR WATER VALVE VAULT SHALL HAVE CAST INTO THE LID ONE OR MORE OF THE FOLLOWING WORDS:
- "STORM" - FOR STORM SEWER MANHOLE LIDS
- "SANITARY" - FOR SANITARY MANHOLE LIDS
- "WATER" - FOR WATER SYSTEM STRUCTURE LIDS
- "FISH" SYMBOL DIRECTLY ON THE CURB BACK - FOR CURB INLET FRAMES
 - IF AT ANY TIME DURING THE CONSTRUCTION OF THIS PROJECT LOOSE MATERIAL IS DEPOSITED INTO THE FLOW LINE OF A SEWER STRUCTURE IN SUCH A WAY AS TO RESTRICT OR OBSTRUCT THE NATURAL FLOW OF WATER IN THE STRUCTURE, THE MATERIAL SHALL BE REMOVED BEFORE THE CLOSE OF THE WORKING DAY DURING WHICH IT WAS DEPOSITED. AT THE CONCLUSION OF CONSTRUCTION ACTIVITIES, AND BEFORE THE ENGINEER ACCEPTS THE PROJECT, ALL SEWERS AND SEWER STRUCTURES THAT FALL WITHIN THE LIMITS OF THE PROJECT SHALL BE FREE OF CONSTRUCTION DEBRIS AND LOOSE MATERIAL.

FILE NAME = 02 General Notes.dgn



USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/12/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	2
CONTRACT NO.			61G27	
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	WOODWARD AVE. CONSTR CODE 0005 STU 70/30	WOODWARD AVE. CONSTR CODE 0021 STU 70/30
Δ *		20101200	TREE ROOT PRUNING	EACH	10	10	
*	CS	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	25	25	
		21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	700	700	
		25200110	SODDING, SALT TOLERANT	SQ YD	700	700	
*		25200200	SUPPLEMENTAL WATERING	UNIT	20	20	
		28000510	INLET FILTERS	EACH	47	47	
*		30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	50	50	
	CS	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	22,129	22129	
*		40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	16	16	
		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	194	194	
		40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	1,377	1377	
		40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	3,672	3672	
		44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"	SQ YD	32,784	32784	
	CS	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1,550	1550	
		44000600	SIDEWALK REMOVAL	SQ FT	8,550	8550	
*		44201737	CLASS D PATCHES, TYPE I, 8 INCH	SQ YD	164	164	

SUMMARY OF QUANTITIES

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	WOODWARD AVE. CONSTR CODE 0005 STU 70/30	WOODWARD AVE. CONSTR CODE 0021 STU 70/30
*		44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	164	164	
*		44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	164	164	
*		44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	328	328	
		60251500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 11 FRAME AND GRATE	EACH	27	27	
		60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	5	5	
		60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	10	10	
	CS	60603800	COMBINATION CURB AND GUTTER, TYPE B-6.12	FOOT	1,550	1550	
Δ *		66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10	
Δ		66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1	
		67100100	MOBILIZATION	L SUM	1	1	
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
		70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1	
		70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1	
		70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1	
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
		70107025	CHANGEABLE MESSAGE SIGN	CAL DAY	60	60	

Δ SPECIALTY ITEMS

FILE NAME = 03 SD 01.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40,0000' / 1"	DRAWN -	REVISED -
PLOT DATE = 12/16/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DU PAGE	TOTAL SHEETS 39	SHEET NO. 3
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	WOODWARD AVE. CONSTR CODE '0005 STU 70/30	WOODWARD AVE. CONSTR CODE '0021 STU 70/30
		70300100	SHORT TERM PAVEMENT MARKING	FOOT	10,238		10238
		70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1,126		1126
Δ		78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	447		447
Δ		78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15,228		15228
Δ		78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,028		3028
Δ		78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,172		1172
Δ		78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	304		304
		78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	520		520
Δ		88600600	DETECTOR LOOP REPLACEMENT	FOOT	3,480		3480
	CS	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1		1
	CS	X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	435	435	
		X0327890	DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	200	200	
	CS	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	8,350		8350
		X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	5	5	
		X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	7	7	
*	CS	X6061005	CONCRETE CURB, TYPE B (SPECIAL)	FOOT	60		60

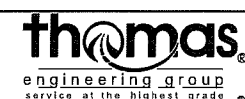
SUMMARY OF QUANTITIES

NOM.	SP. PROV.	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY	WOODWARD AVE. CONSTR CODE '0005 STU 70/30	WOODWARD AVE. CONSTR CODE '0021 STU 70/30
		Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51	51	
		Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	19,435	19435	
	CS	Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD	475	475	
	CS	Z0004544	HOT-MIX ASPHALT DRIVEWAY PAVEMENT REMOVAL	SQ YD	475	475	
		Z0017800	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED (SPECIAL)	EACH	3	3	
		Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	12	12	

CS = CONTRACT SPECIAL PROVISION (SEE SECTION III)
 * = NOMINAL QUANTITY PROVIDED. THE ACTUAL NEED WILL BE DETERMINED IN THE FIELD AT TIME OF CONSTRUCTION. QUANTITIES WILL BE DEDUCTED IF THE WORK IS NOT DEEMED NECESSARY.

Δ SPECIALTY ITEMS

FILE NAME = 04 50 02.dgn



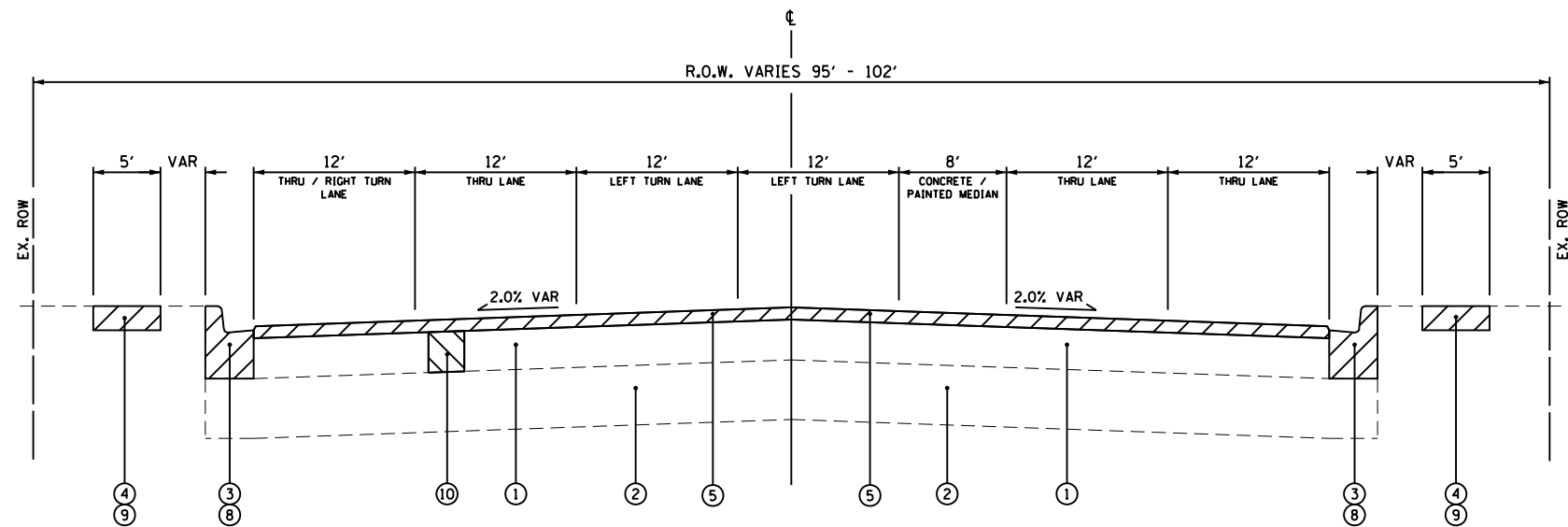
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

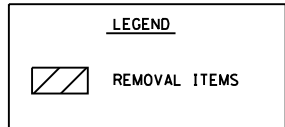
SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	4
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	



WOODWARD AVENUE- EXISTING TYPICAL SECTION
STA. 11+26 TO STA. 13+75
 (LOOKING NORTH)

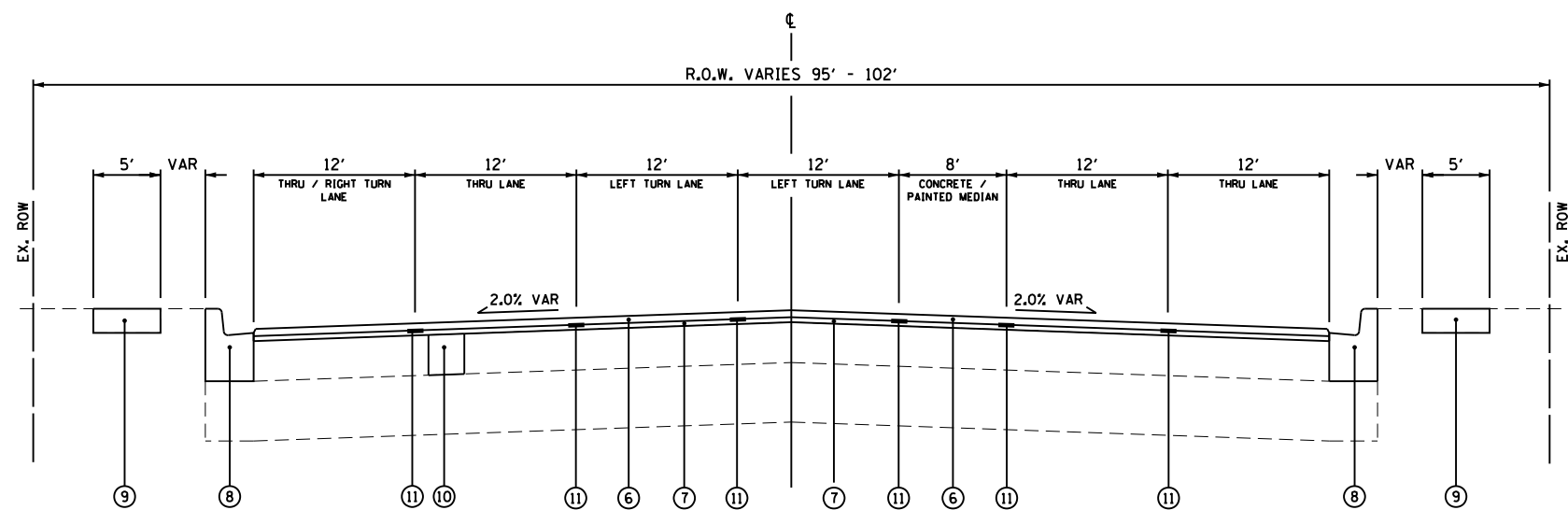


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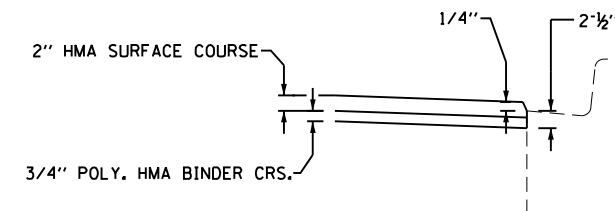
- ① EX. FULL DEPTH HMA PAVEMENT
- ② EX. AGGREGATE SUBGRADE
- ③ EX. COMB. CONC. CURB AND GUTTER
- ④ EX. P.C.C. SIDEWALK
- ⑤ PR. HMA SURFACE REMOVAL, 2-½"
- ⑥ PR. HMA SURFACE COURSE, MIX "D", N70, 2"
- ⑦ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, ¾"
- ⑧ PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
- ⑨ PR. P.C.C. SIDEWALK REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
- ⑩ PR. CLASS D PATCHING (INTERMITTENT) (SEE NOTE 1)
- ⑪ PR. LONGITUDINAL JOINT SEALANT

NOTES:

1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
2. PR AGG BASE COURSE IS INCLUDED IN THE PCC SIDEWALK PAY ITEM. SEE PROJECT SPECIFICATIONS FOR DETAILS.
3. HMA JOINT SEALANT UNDER SURFACE LIFT.



WOODWARD AVENUE- PROPOSED TYPICAL SECTION
STA. 11+26 TO STA. 13+75
 (LOOKING NORTH)



RESURFACING DETAIL

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
ROADWAY RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70	4% @ 70 Gyr.
POLYMERIZED HMA BINDER COURSE, IL-4.75, N50	3.5% @ 50 Gyr.
PATCHING	
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	4% @ 70 Gyr.
DRIVEWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50	4% @ 50 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIALS PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4. THE CONTRACTOR SHALL MILL BEFORE PATCHING.

FILE NAME = 95-07 sht-typical.dgn



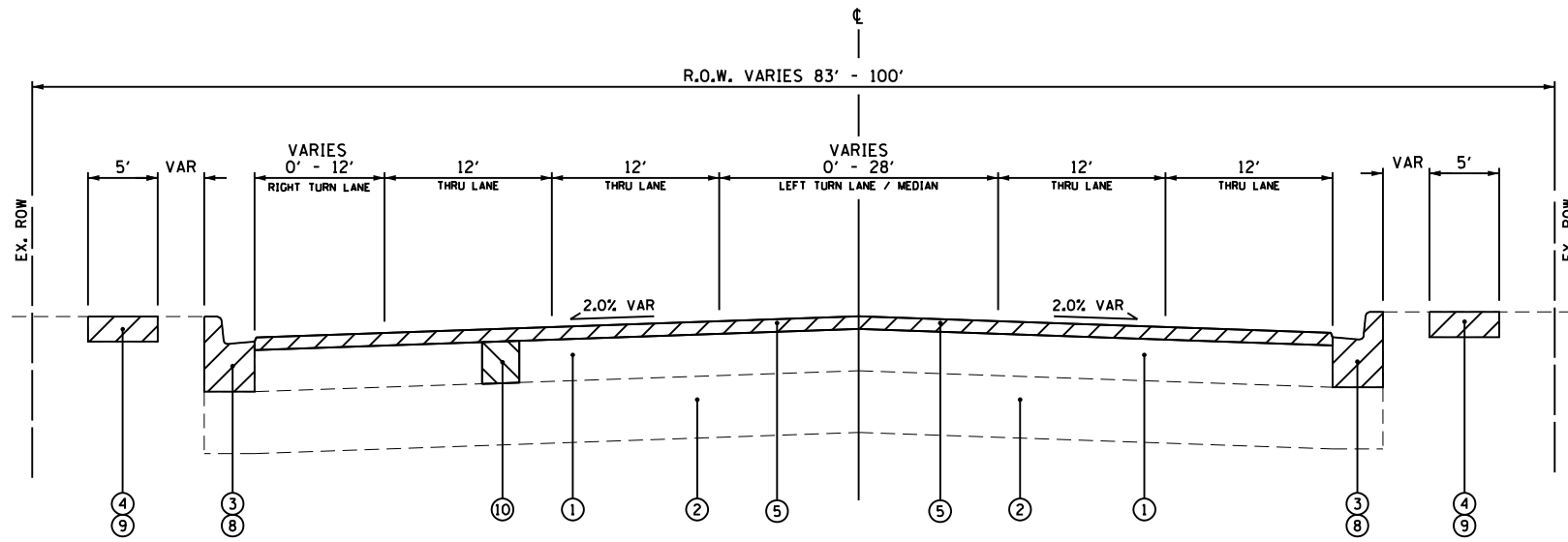
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

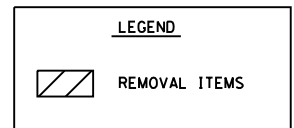
WOODWARD AVENUE
TYPICAL SECTIONS

SCALE: SHEET NTS OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	5
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	



WOODWARD AVENUE- EXISTING TYPICAL SECTION
STA. 13+75 TO STA. 40+00
 (LOOKING NORTH)

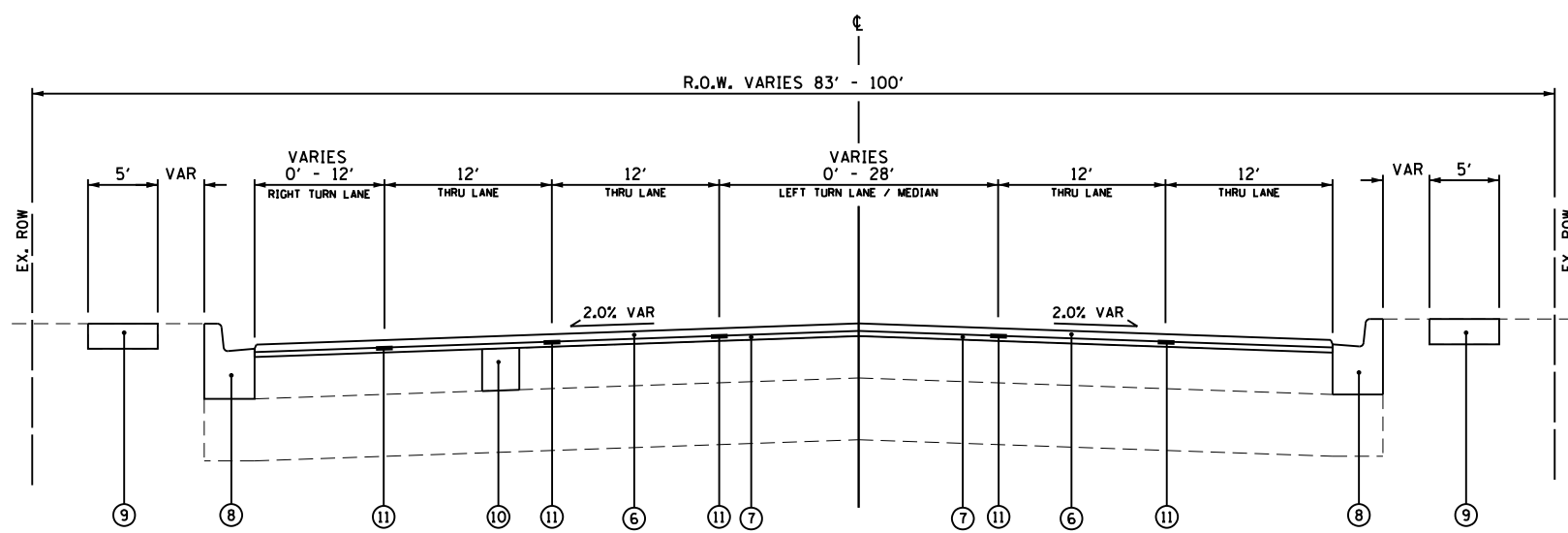


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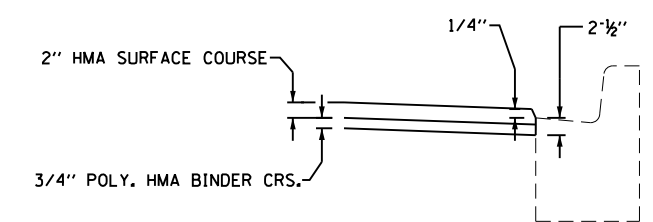
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- ② EX. AGGREGATE SUBGRADE
- ③ EX. COMB. CONC. CURB AND GUTTER
- ④ EX. P.C.C. SIDEWALK
- ⑤ PR. HMA SURFACE REMOVAL, 2-1/2"
- ⑥ PR. HMA SURFACE COURSE, MIX "D", N70, 2"
- ⑦ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, 3/4"
- ⑧ PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
- ⑨ PR. P.C.C. SIDEWALK REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
- ⑩ PR. CLASS D PATCHING (INTERMITTENT) (SEE NOTE 1)
- ⑪ PR. LONGITUDINAL JOINT SEALANT

NOTES:

1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
2. PR AGG BASE COURSE IS INCLUDED IN THE PCC SIDEWALK PAY ITEM. SEE PROJECT SPECIFICATIONS FOR DETAILS.
3. HMA JOINT SEALANT UNDER SURFACE LIFT.



WOODWARD AVENUE- PROPOSED TYPICAL SECTION
STA. 13+75 TO STA. 40+00
 (LOOKING NORTH)



RESURFACING DETAIL

FILE NAME = 95-07 sht-typical.dgn



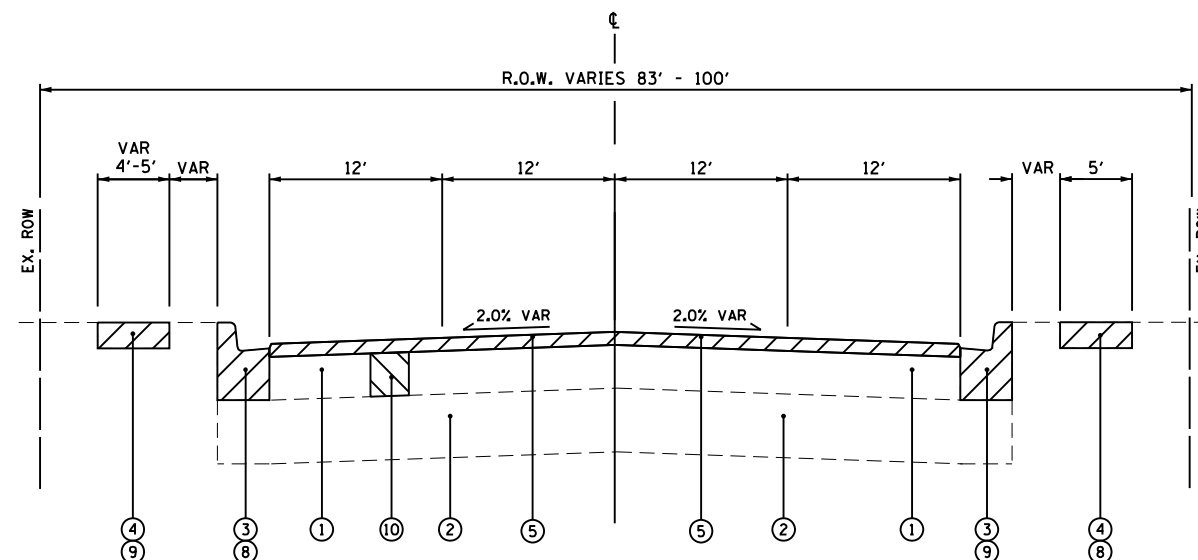
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

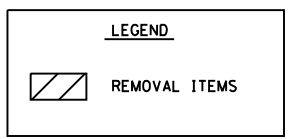
WOODWARD AVENUE
TYPICAL SECTIONS

SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	6
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	

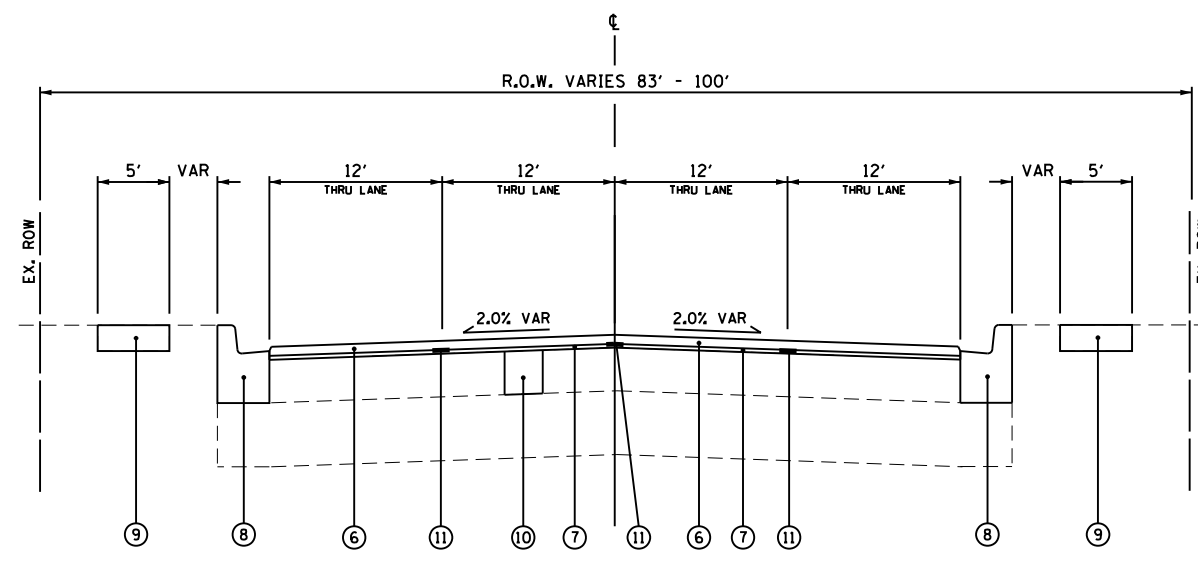


WOODWARD AVENUE- EXISTING TYPICAL SECTION
STA. 40+00 TO STA. 64+05
 (LOOKING NORTH)

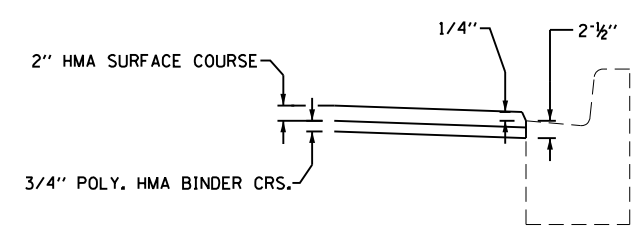


- LEGEND**
- ① EX. FULL DEPTH HMA PAVEMENT
 - ② EX. AGGREGATE SUBGRADE
 - ③ EX. COMB. CONC. CURB AND GUTTER
 - ④ EX. P.C.C. SIDEWALK
 - ⑤ PR. HMA SURFACE REMOVAL, 2-1/2"
 - ⑥ PR. HMA SURFACE COURSE, MIX "D", N70, 2"
 - ⑦ PR. POLYMERIZED HMA BINDER CRS, IL-4.75, N50, 3/4"
 - ⑧ PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
 - ⑨ PR. P.C.C. SIDEWALK REMOVAL AND REPLACEMENT (INTERMITTENT) (SEE NOTE 1)
 - ⑩ PR. CLASS D PATCHING (INTERMITTENT) (SEE NOTE 1)
 - ⑪ PR. LONGITUDINAL JOINT SEALANT

- NOTES:**
1. LOCATIONS WILL BE SPECIFIED BY THE ENGINEER IN THE FIELD DURING CONSTRUCTION
 2. PR AGG BASE COURSE IS INCLUDED IN THE PCC SIDEWALK PAY ITEM. SEE PROJECT SPECIFICATIONS FOR DETAILS.
 3. HMA JOINT SEALANT UNDER SURFACE LIFT.



WOODWARD AVENUE- PROPOSED TYPICAL SECTION
STA. 40+00 TO STA. 64+05
 (LOOKING NORTH)



RESURFACING DETAIL

FILE NAME = 95-07 sht-typical.dgn



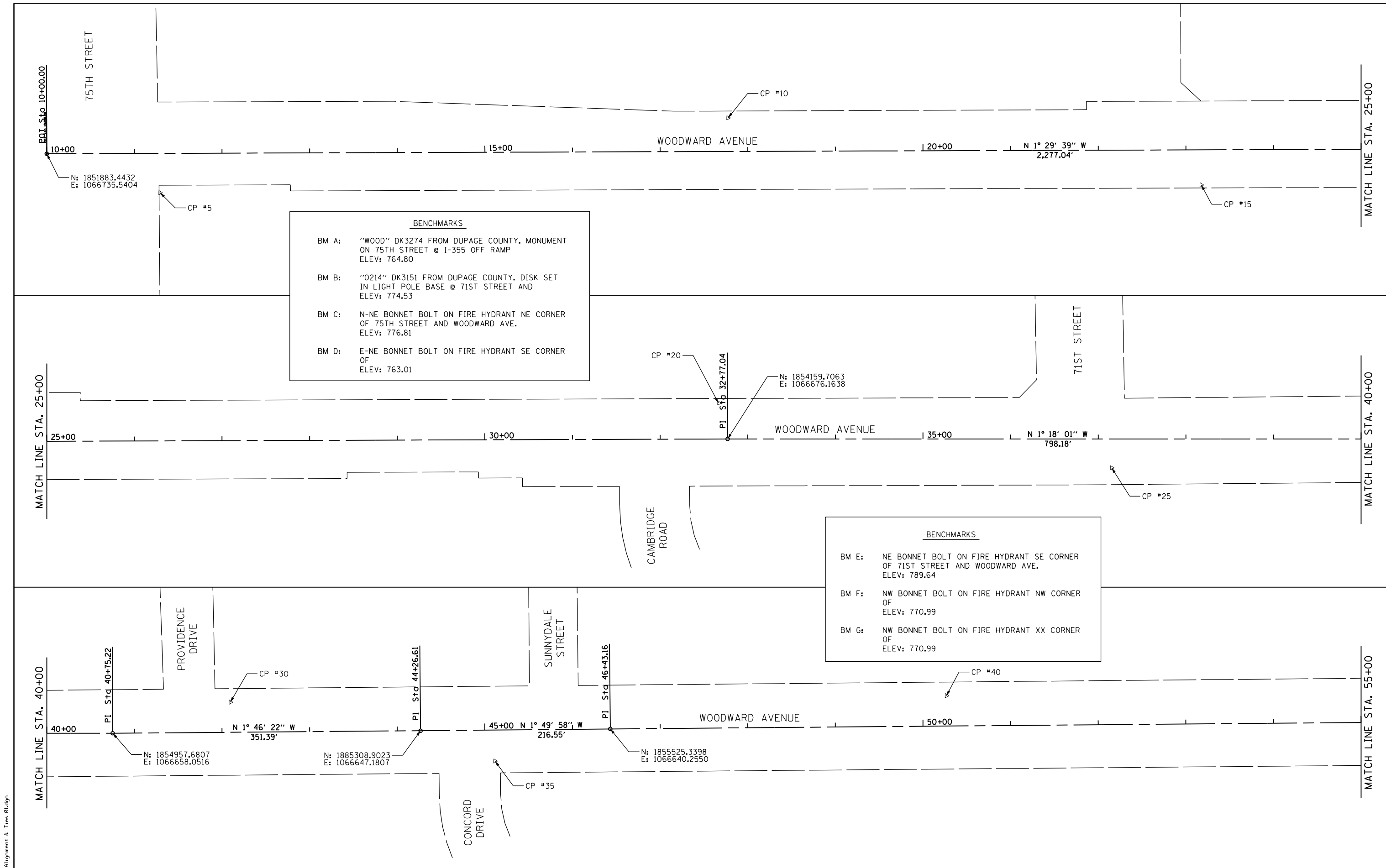
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WOODWARD AVENUE
TYPICAL SECTIONS

SCALE: NTS SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	7
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



BENCHMARKS

BM A: "WOOD" DK3274 FROM DUPAGE COUNTY. MONUMENT ON 75TH STREET @ I-355 OFF RAMP
ELEV: 764.80

BM B: "0214" DK3151 FROM DUPAGE COUNTY. DISK SET IN LIGHT POLE BASE @ 71ST STREET AND
ELEV: 774.53

BM C: N-NE BONNET BOLT ON FIRE HYDRANT NE CORNER OF 75TH STREET AND WOODWARD AVE.
ELEV: 776.81

BM D: E-NE BONNET BOLT ON FIRE HYDRANT SE CORNER OF
ELEV: 763.01

BENCHMARKS

BM E: NE BONNET BOLT ON FIRE HYDRANT SE CORNER OF 71ST STREET AND WOODWARD AVE.
ELEV: 789.64

BM F: NW BONNET BOLT ON FIRE HYDRANT NW CORNER OF
ELEV: 770.99

BM G: NW BONNET BOLT ON FIRE HYDRANT XX CORNER OF
ELEV: 770.99

FILE NAME = 00 Alignment & Ties 01.dgn

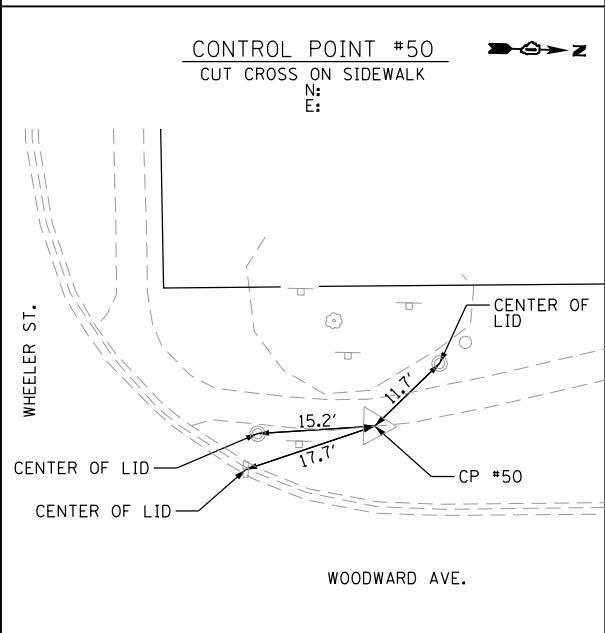
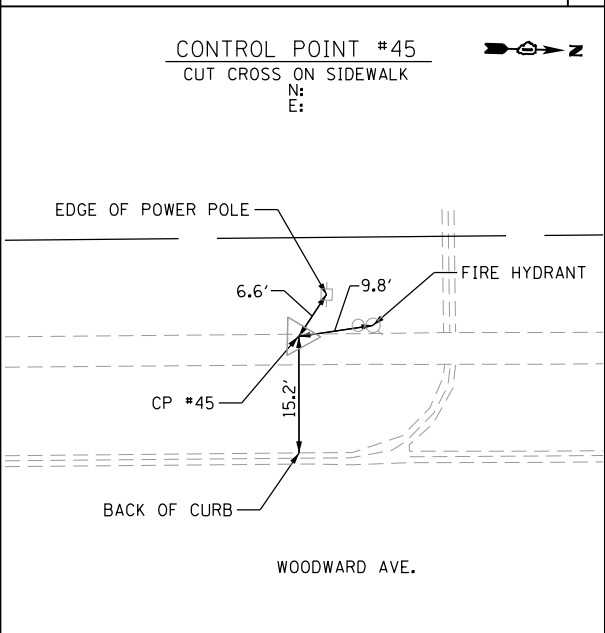
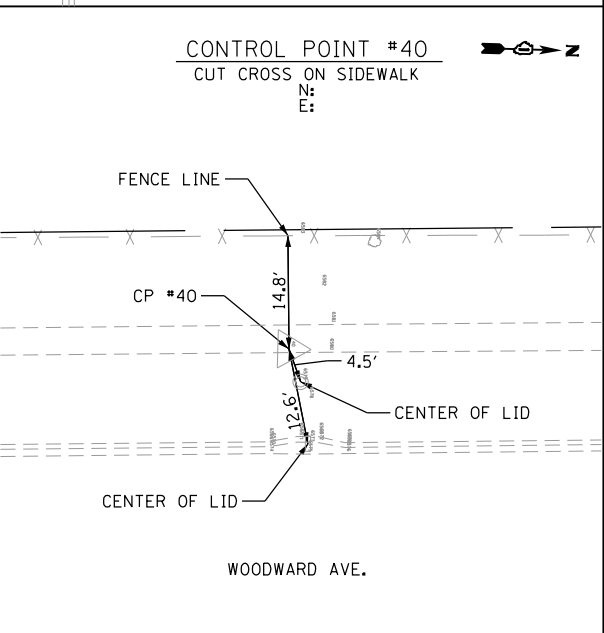
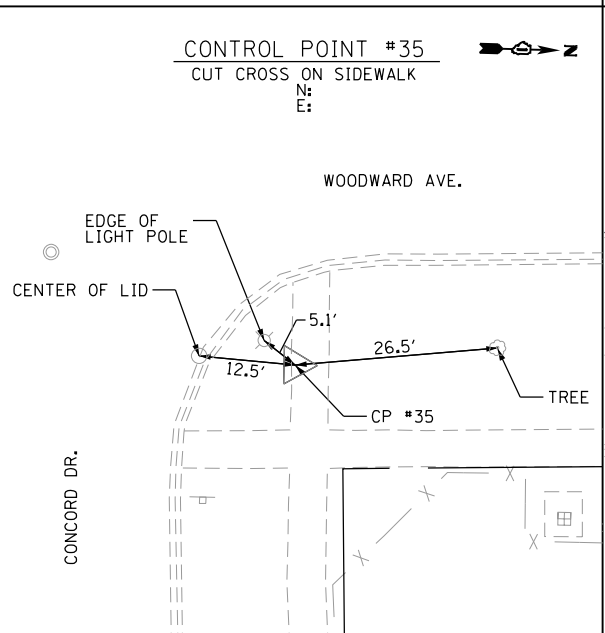
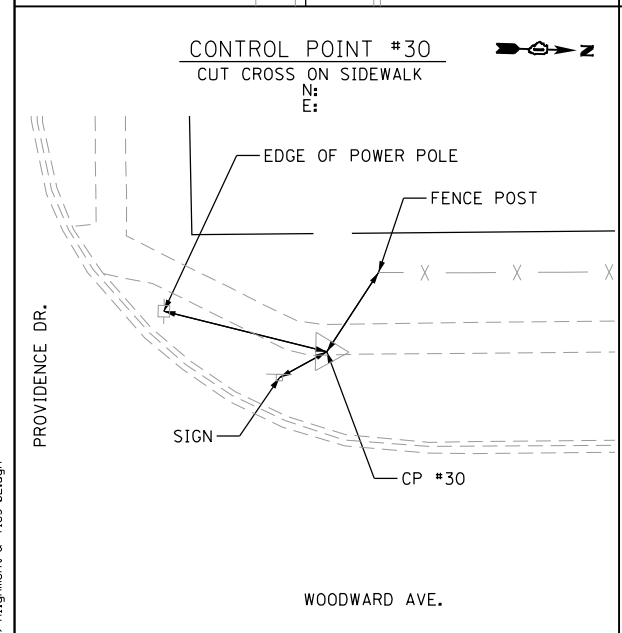
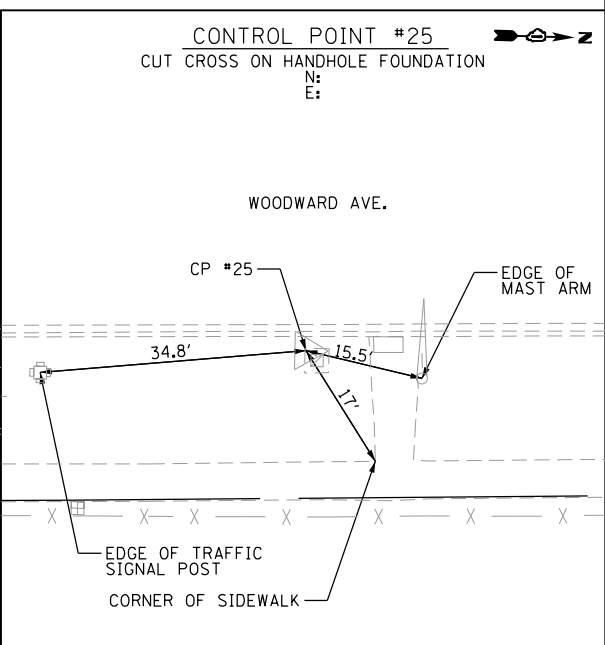
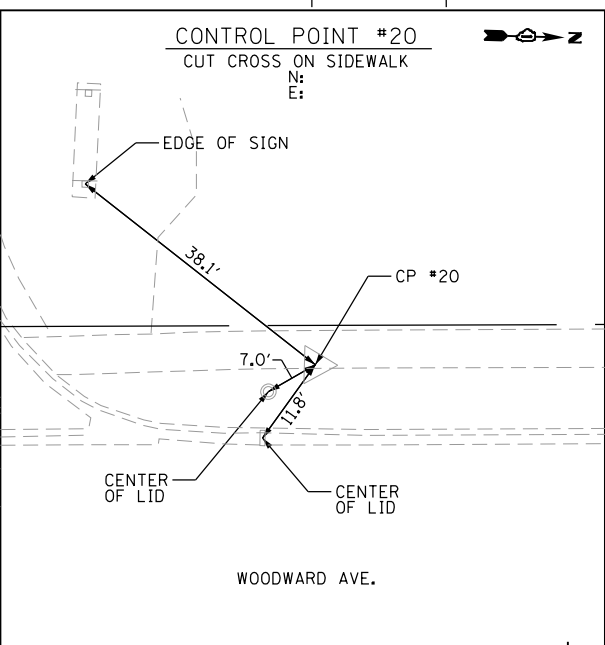
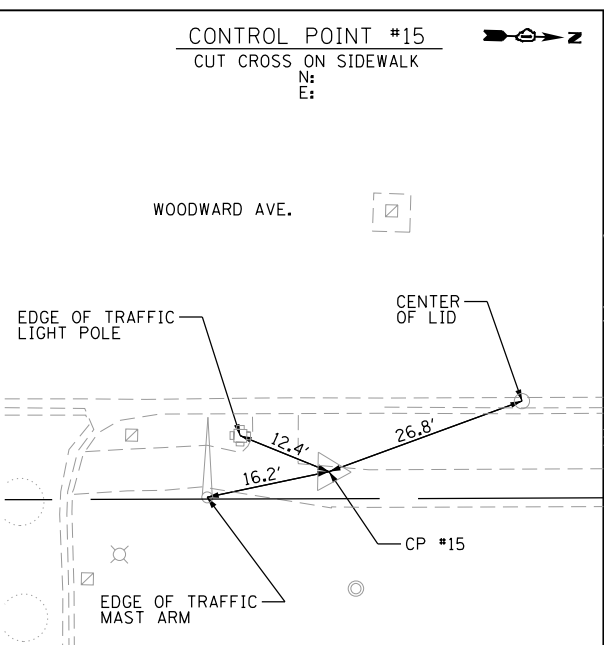
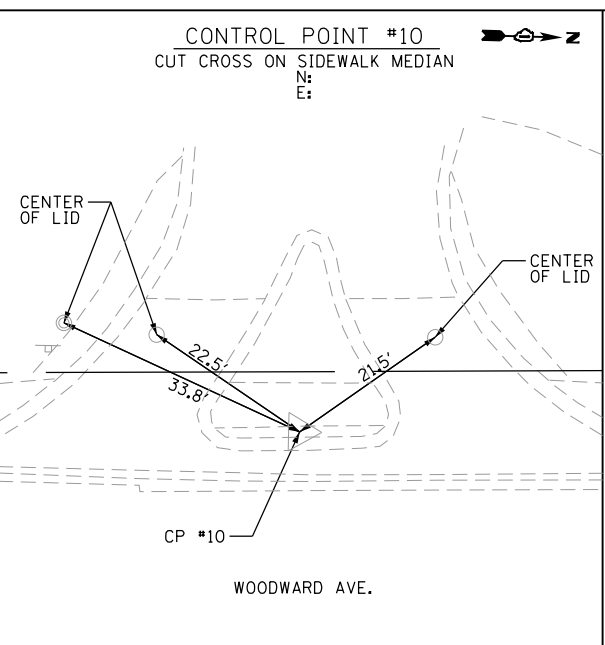
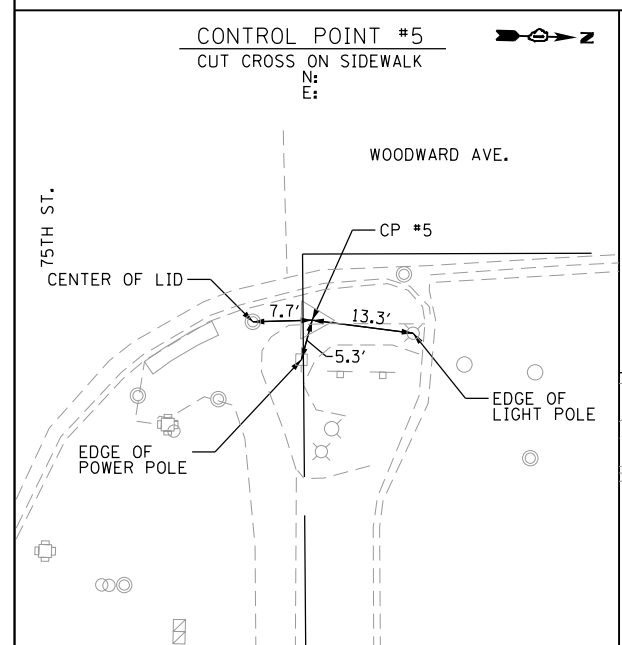
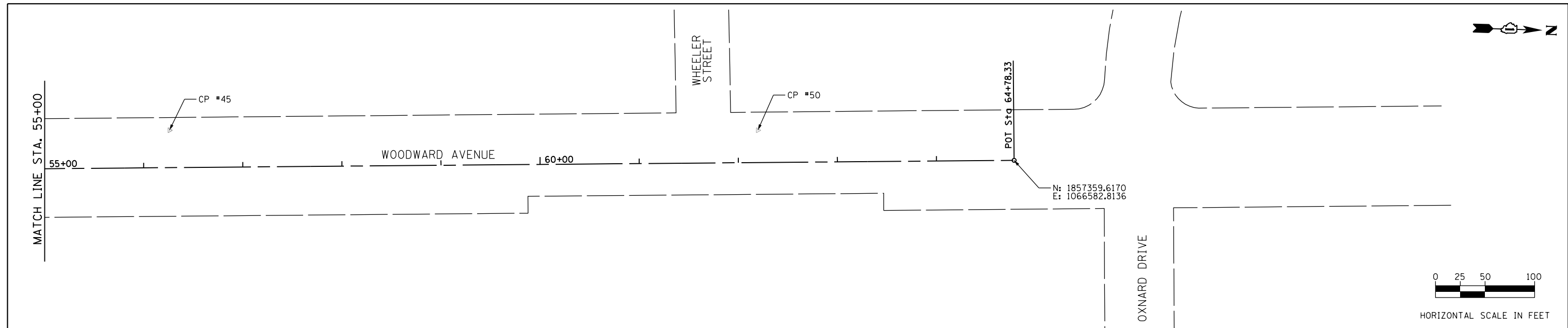


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PLOT DATE = 12/12/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, BENCHMARKS AND TIES			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	8
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 09 Alignment & Ties 02.dgn



USER NAME = DonN
PLOT SCALE = 100.0000' / 1" = 100
PLOT DATE = 12/12/2019

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
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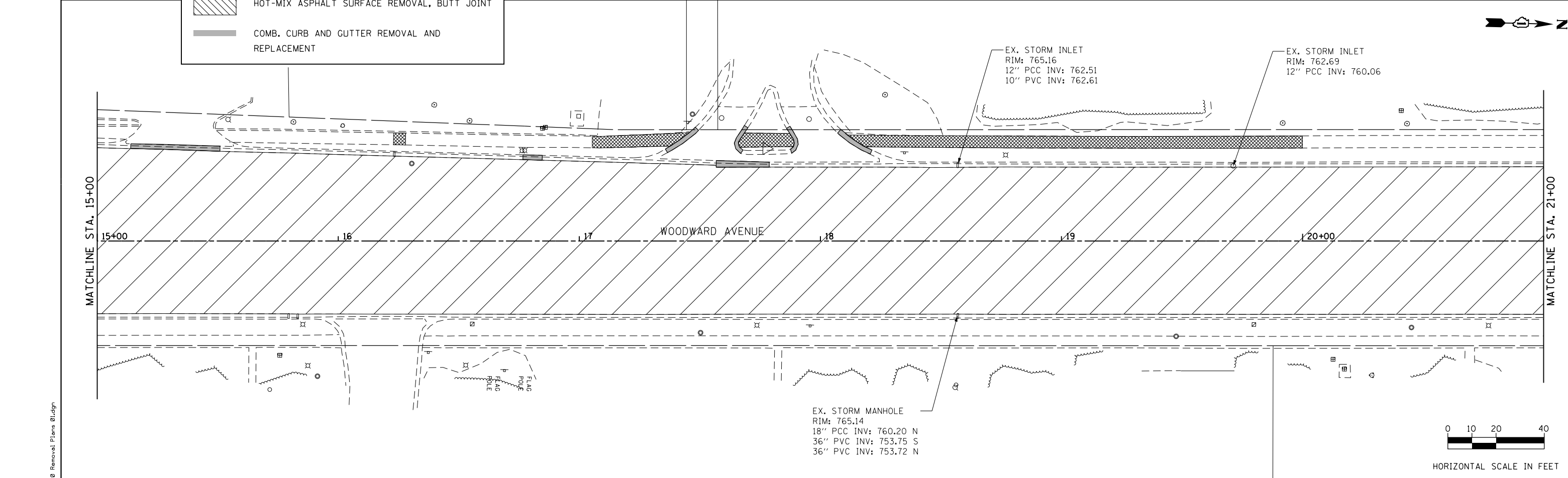
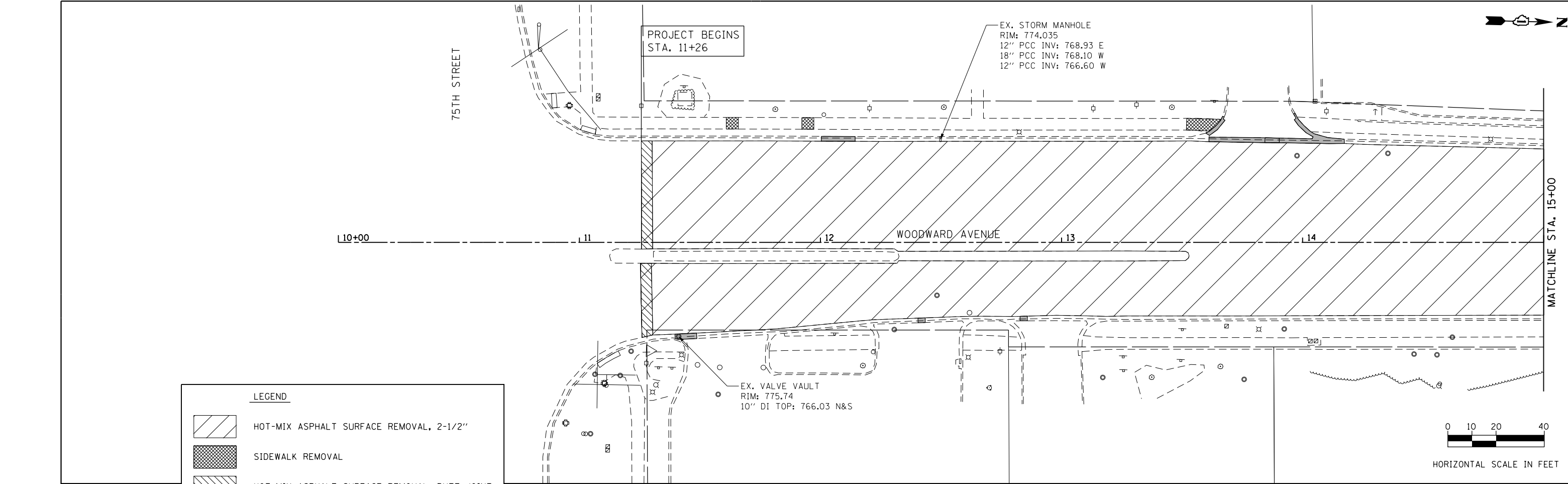
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, BENCHMARKS AND TIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	9
CONTRACT NO. 61G27				

ILLINOIS FED. AID PROJECT



LEGEND

	HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
	SIDEWALK REMOVAL
	HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
	COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT

FILE NAME = 10 Removal Plans 01.dgn



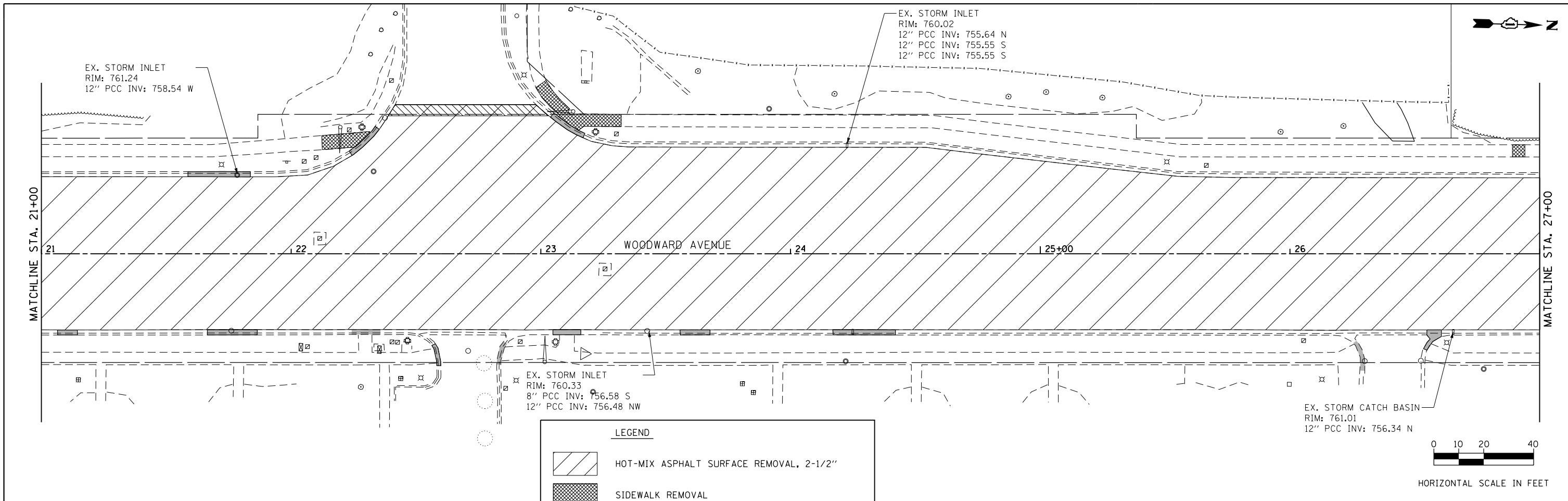
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
EXISTING AND REMOVAL PLANS**

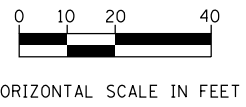
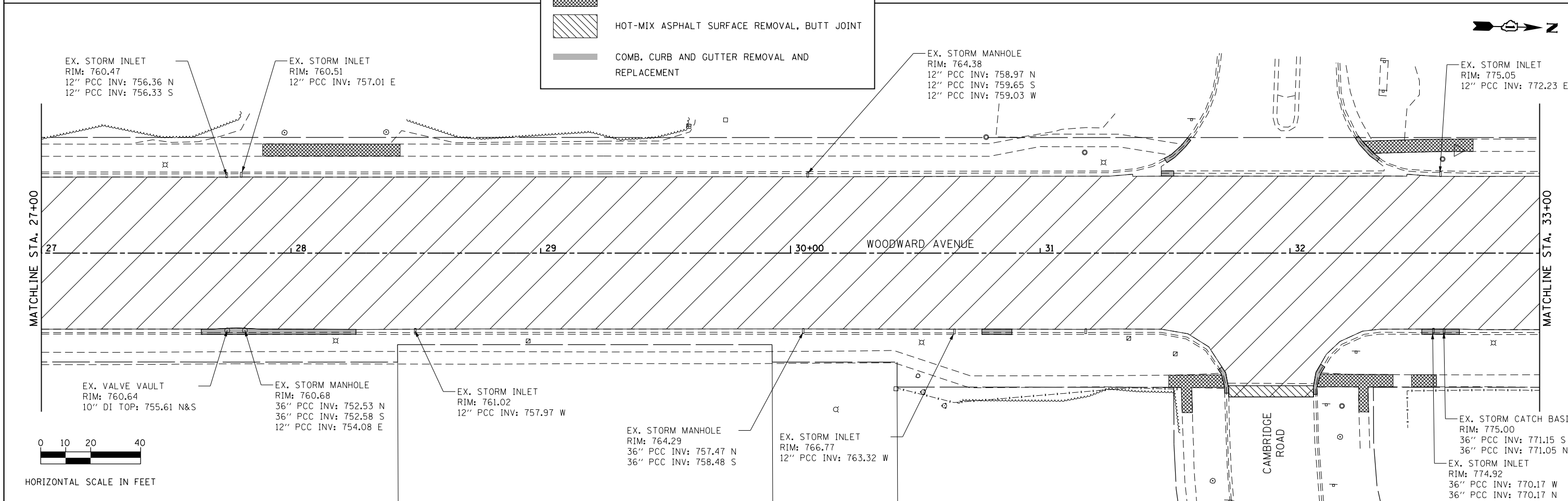
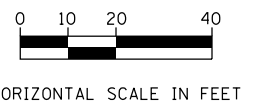
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	10
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
- COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT



FILE NAME = 11 Removal Plans 02.dgn



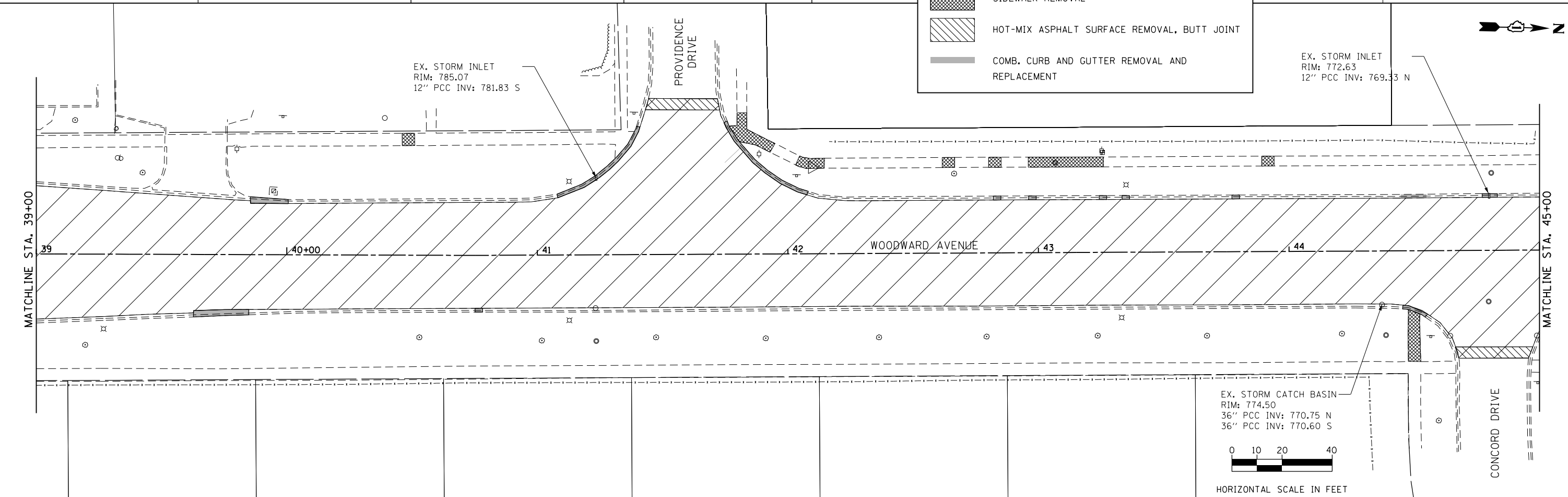
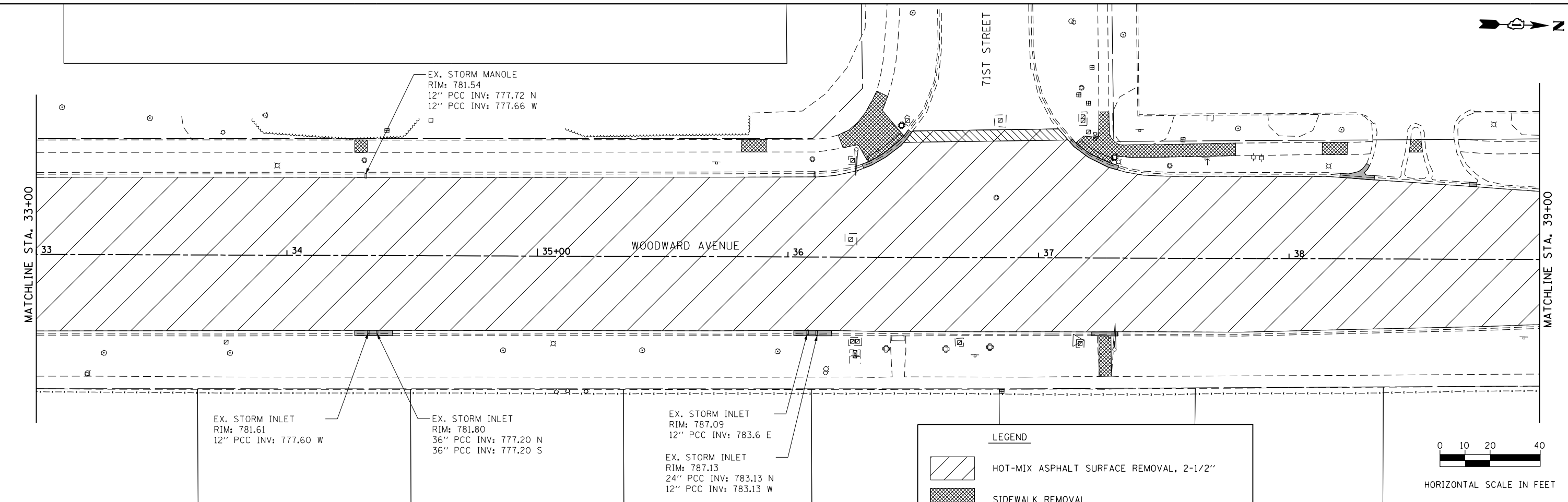
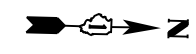
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PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
EXISTING AND REMOVAL PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	11
				CONTRACT NO. 61G27
ILLINOIS FED. AID PROJECT				



FILE NAME = 12 Removal Plans 03.dgn

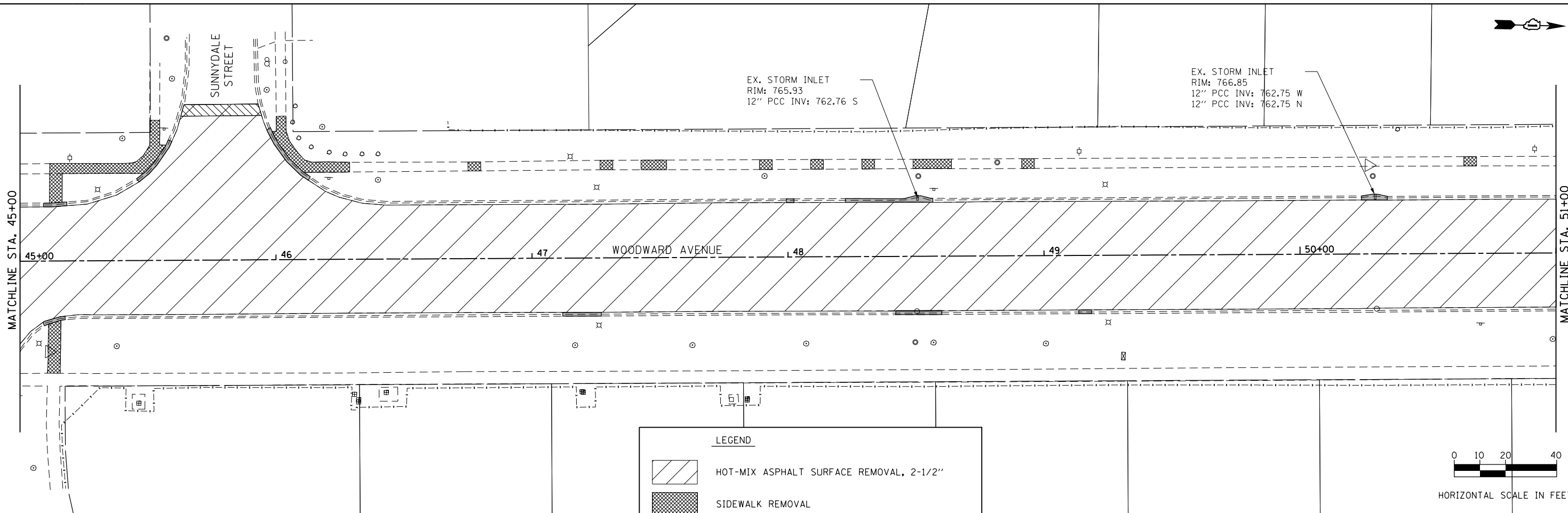
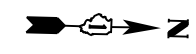
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
EXISTING AND REMOVAL PLANS**

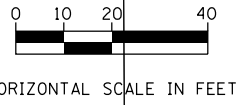
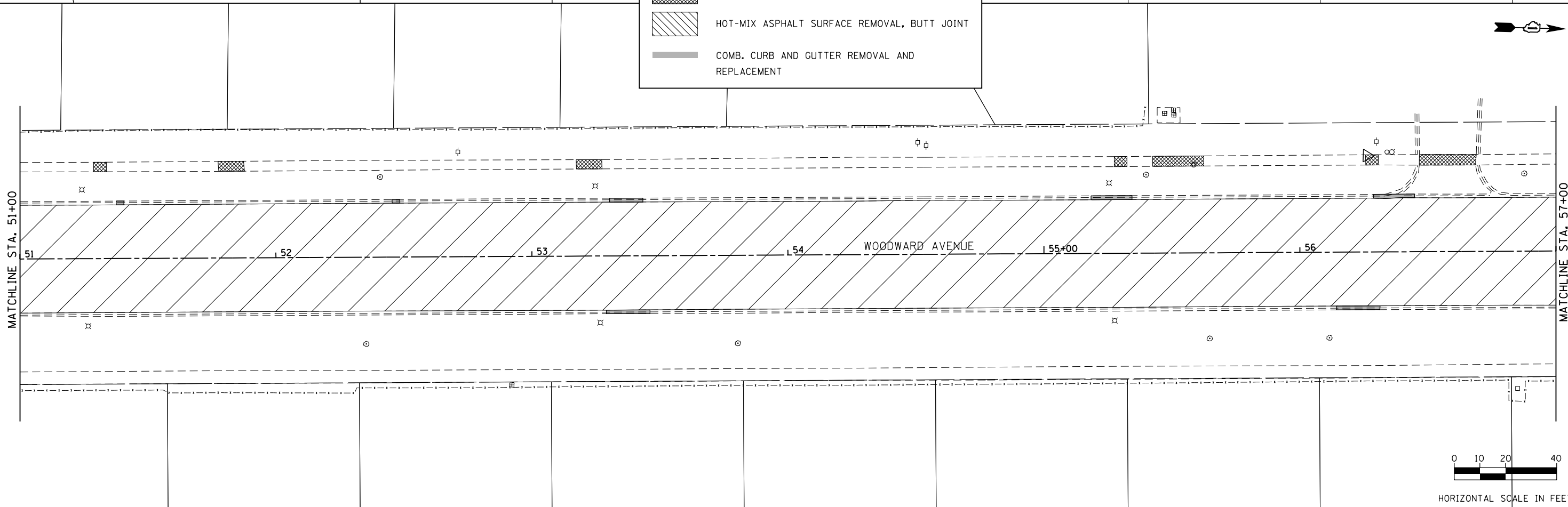
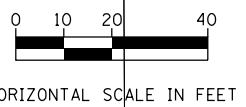
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	12
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
- COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT



FILE NAME = 13 Removal Plans 04.dgn



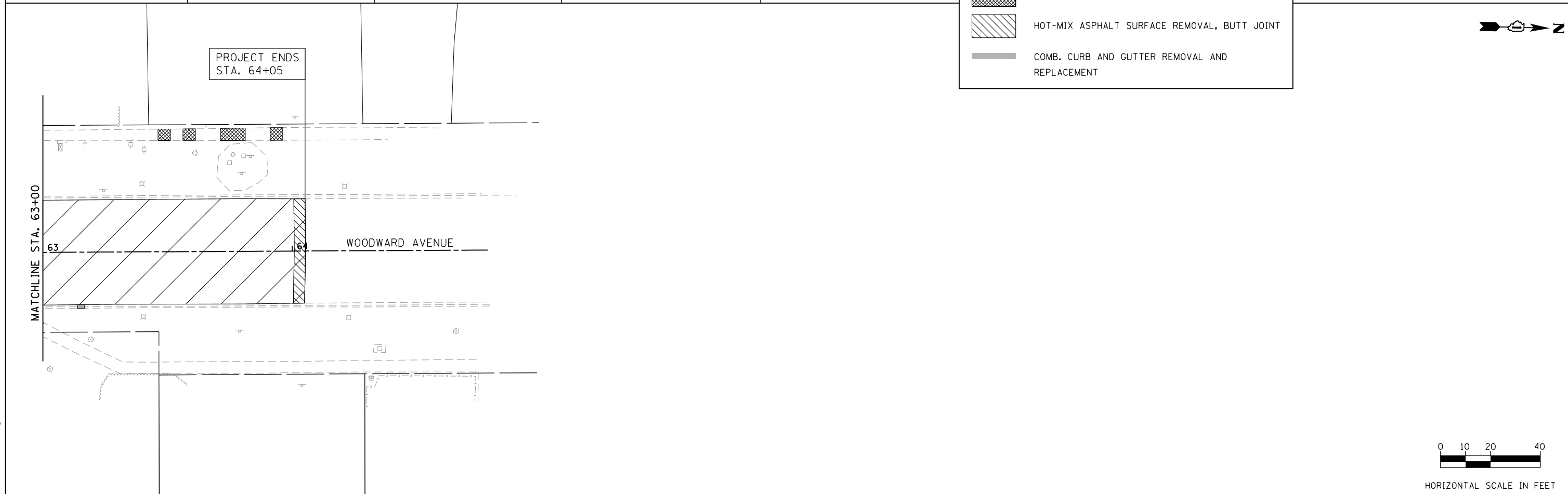
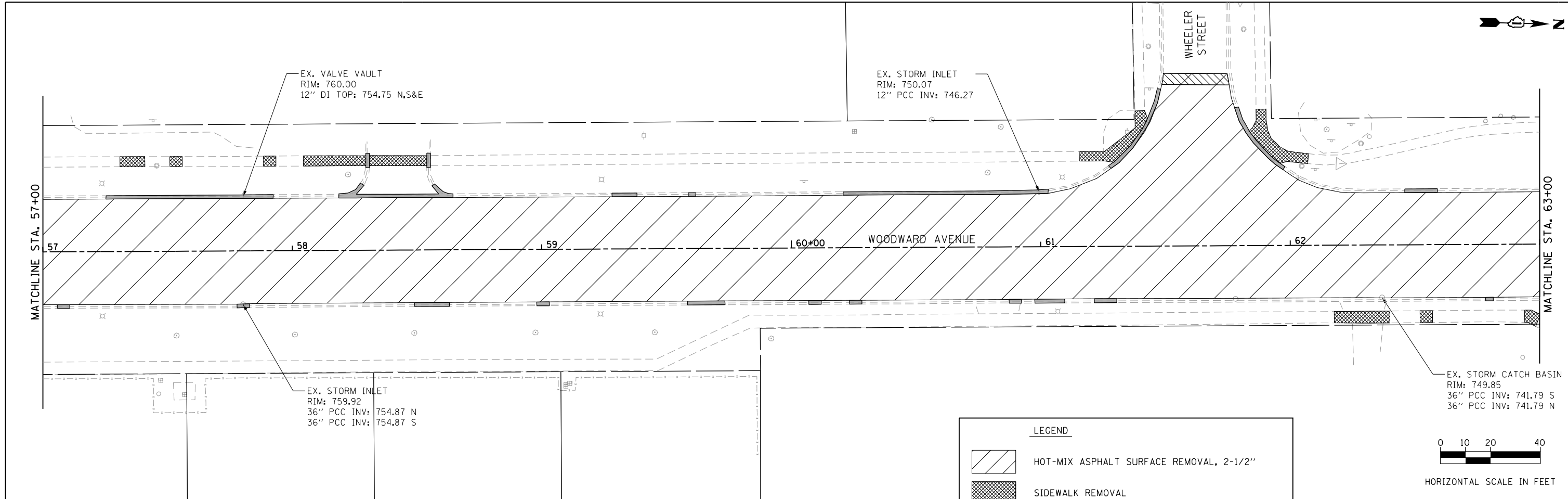
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
EXISTING AND REMOVAL PLANS**

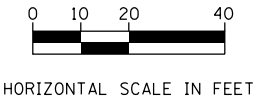
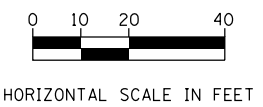
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 13
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, BUTT JOINT
- COMB. CURB AND GUTTER REMOVAL AND REPLACEMENT



FILE NAME = 14 Removal Plans 05.dgn

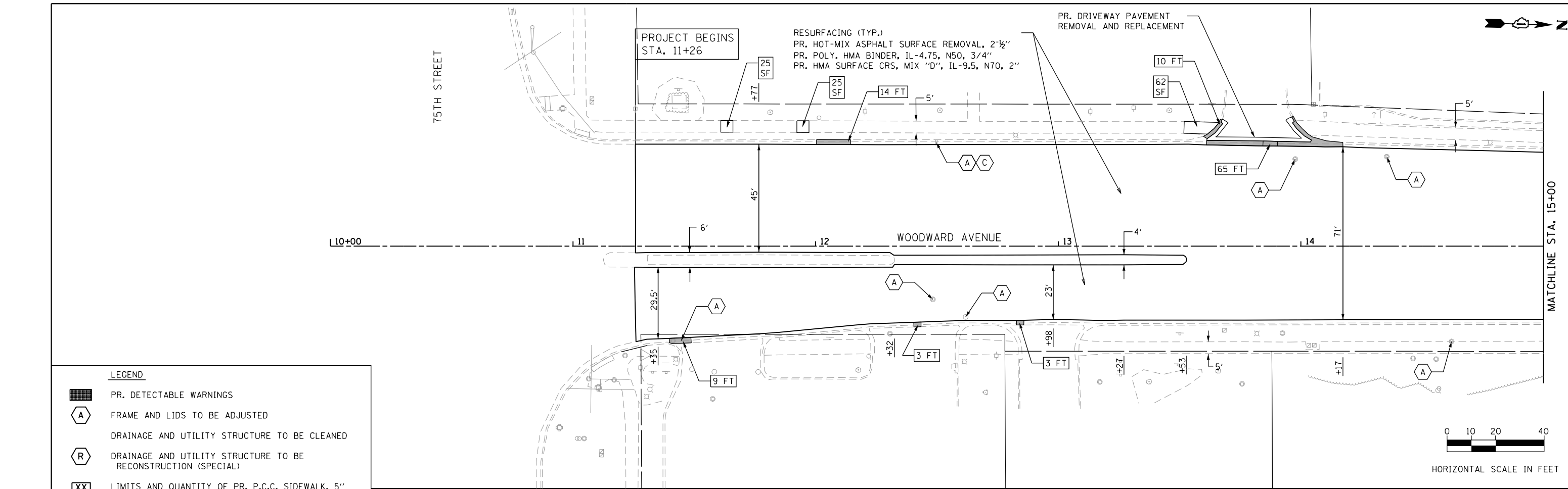


USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/12/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

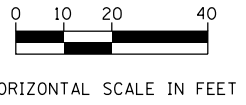
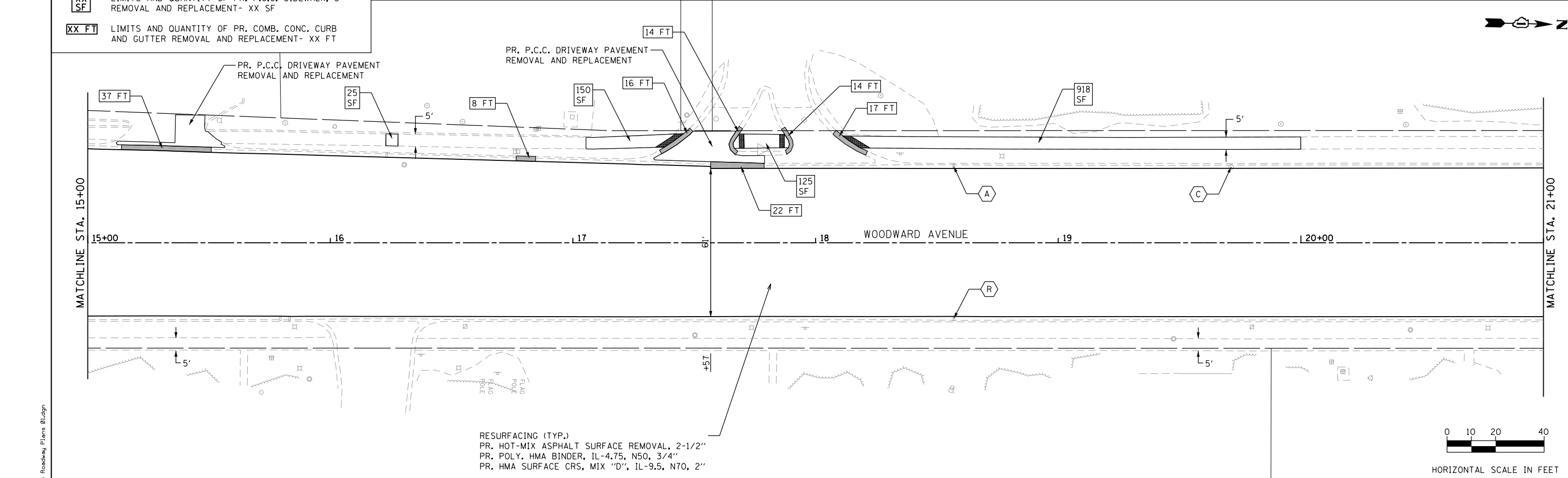
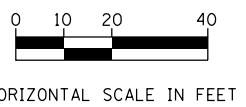
WOODWARD AVENUE EXISTING AND REMOVAL PLANS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	14
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



LEGEND

	PR. DETECTABLE WARNINGS
	FRAME AND LIDS TO BE ADJUSTED
	DRAINAGE AND UTILITY STRUCTURE TO BE CLEANED
	DRAINAGE AND UTILITY STRUCTURE TO BE RECONSTRUCTION (SPECIAL)
	LIMITS AND QUANTITY OF PR. P.C.C. SIDEWALK, 5' REMOVAL AND REPLACEMENT- XX SF
	LIMITS AND QUANTITY OF PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT- XX FT



FILE NAME = 15 Roadway Plans 01.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

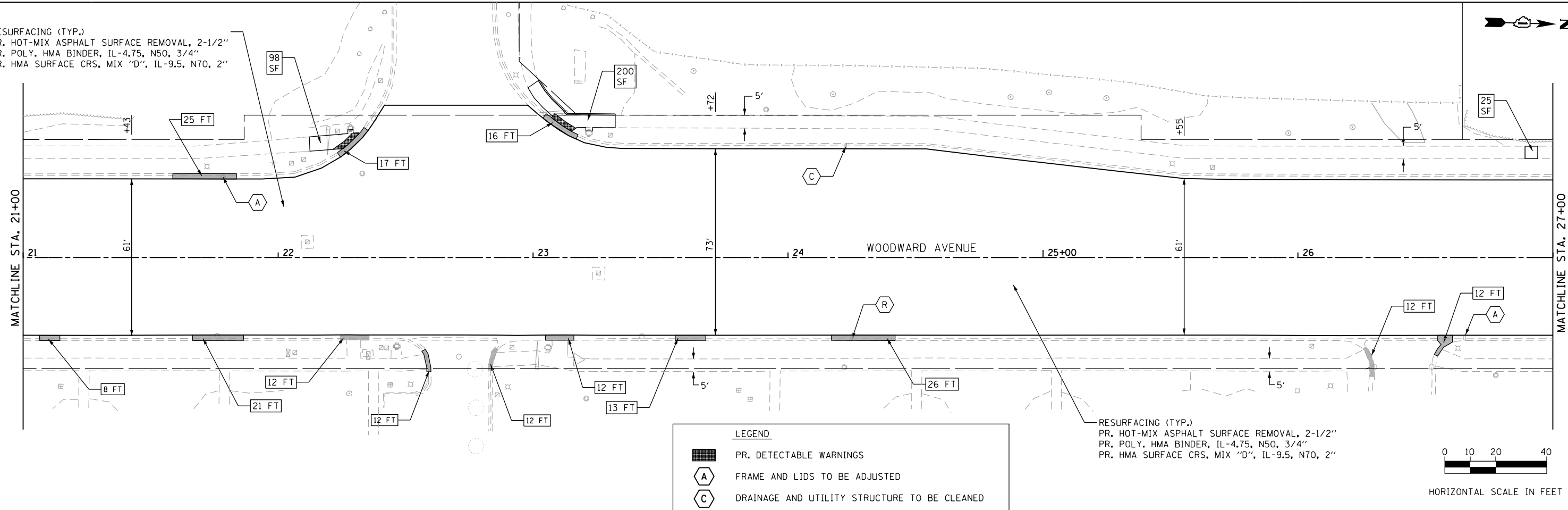
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
PROPOSED ROADWAY PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 15
CONTRACT NO. 61G27				ILLINOIS FED. AID PROJECT

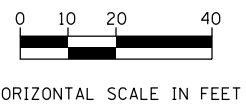
RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"



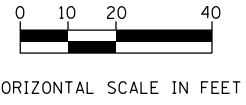
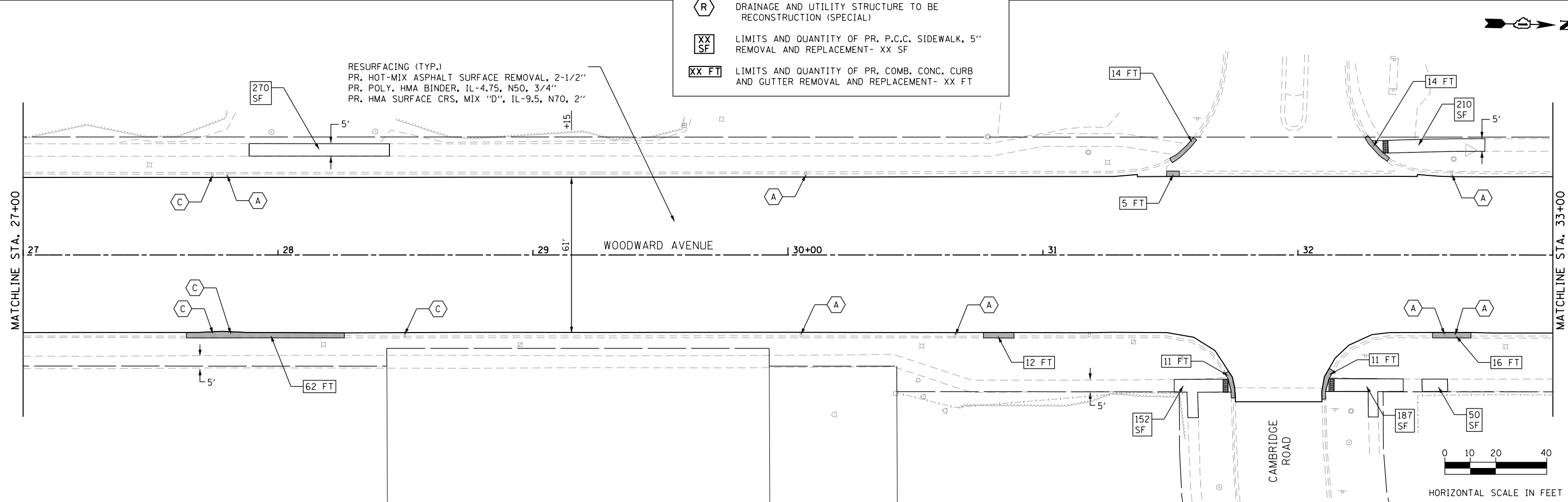
LEGEND

- PR. DETECTABLE WARNINGS
- FRAME AND LIDS TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURE TO BE CLEANED
- DRAINAGE AND UTILITY STRUCTURE TO BE RECONSTRUCTION (SPECIAL)
- LIMITS AND QUANTITY OF PR. P.C.C. SIDEWALK, 5" REMOVAL AND REPLACEMENT- XX SF
- LIMITS AND QUANTITY OF PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT- XX FT

RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"



RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"



FILE NAME = 16 Roadway Plans 02.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

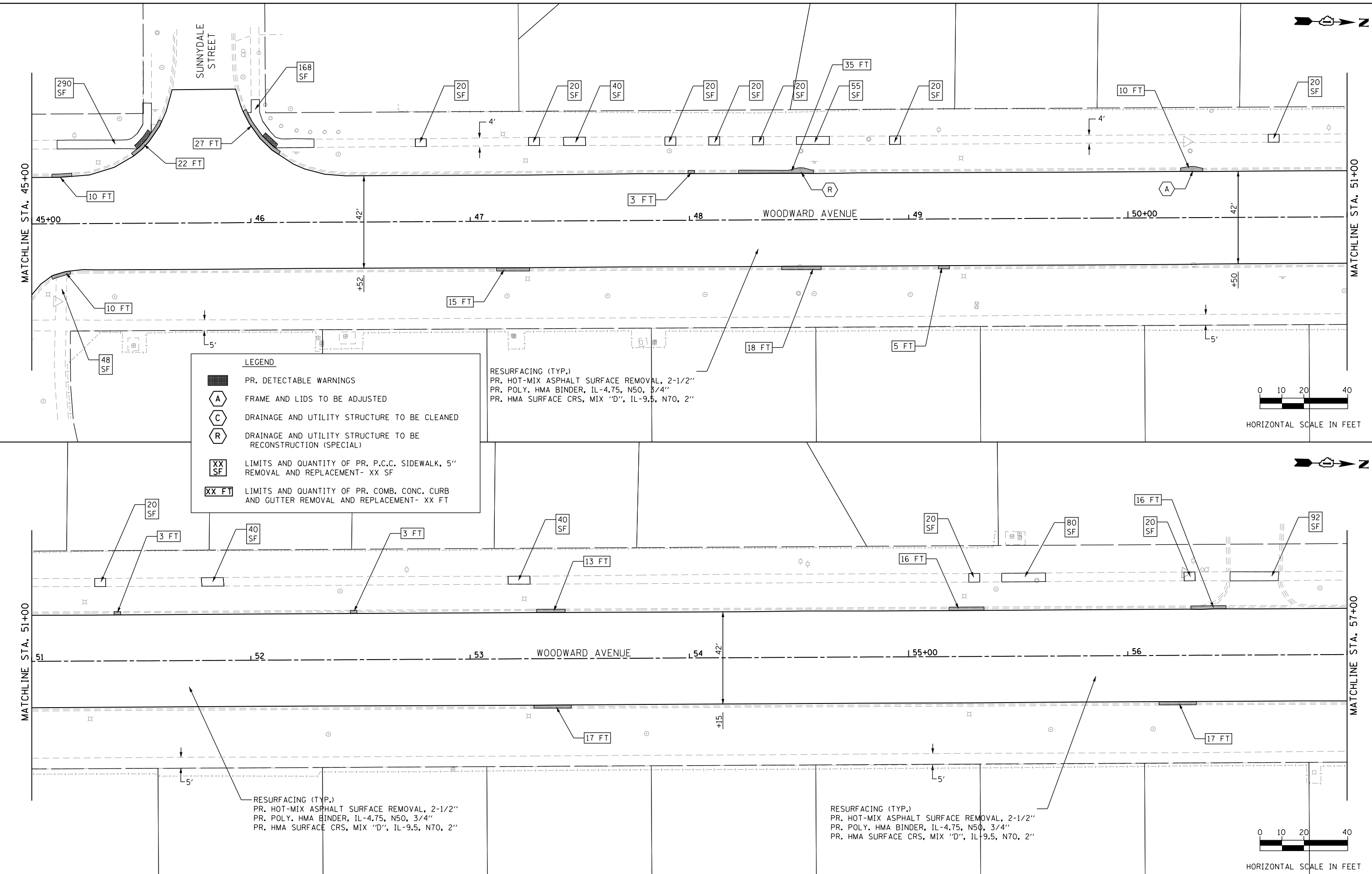
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
 PROPOSED ROADWAY PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	16
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	

FILE NAME = 18 Roadway Plans 04.dgn



LEGEND

- PR. DETECTABLE WARNINGS
- FRAME AND LIDS TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURE TO BE CLEANED
- DRAINAGE AND UTILITY STRUCTURE TO BE RECONSTRUCTION (SPECIAL)
- LIMITS AND QUANTITY OF PR. P.C.C. SIDEWALK, 5" REMOVAL AND REPLACEMENT- XX SF
- LIMITS AND QUANTITY OF PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT- XX FT

RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"

RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"

RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

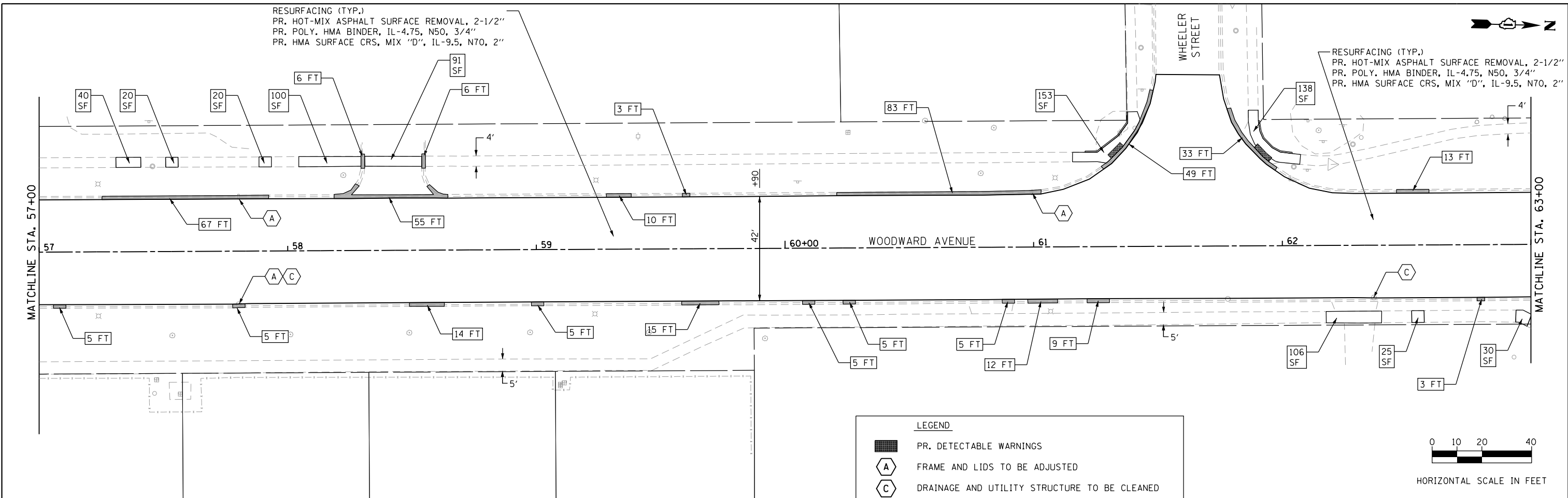
**WOODWARD AVENUE
 PROPOSED ROADWAY PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	18
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	

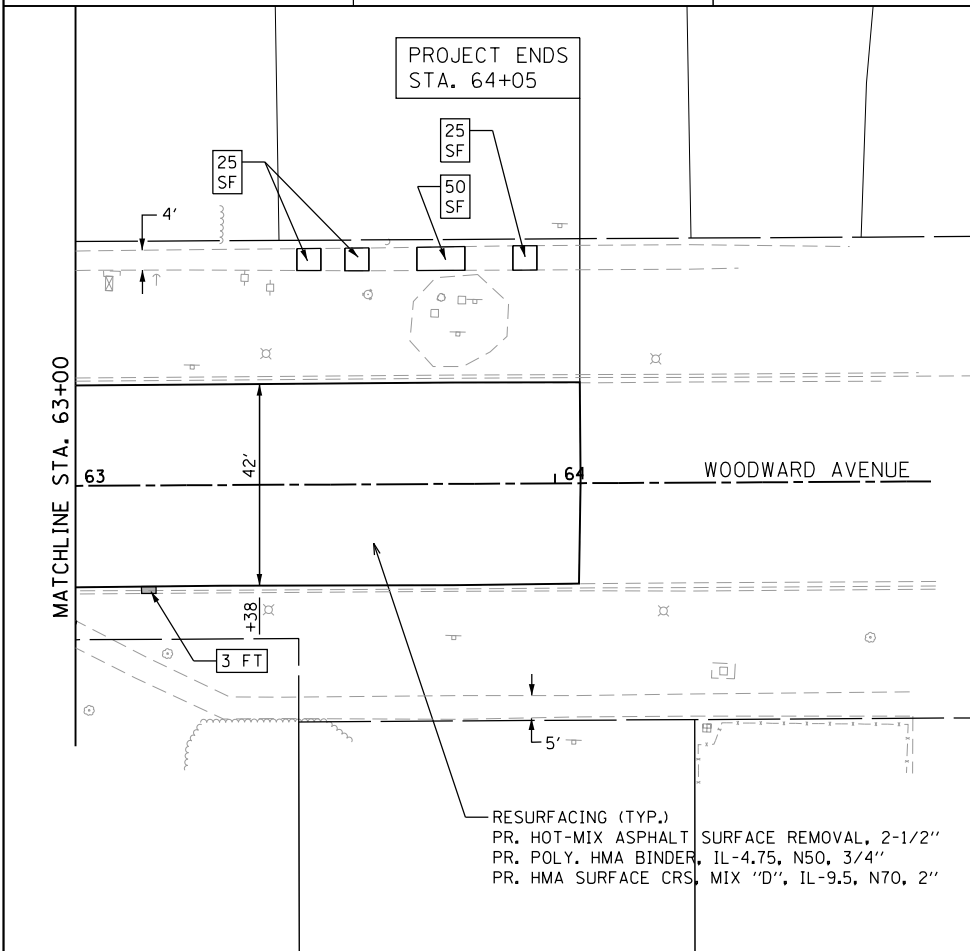
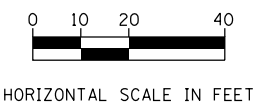
RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"

RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"

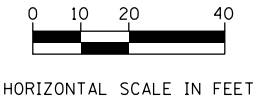


LEGEND

- PR. DETECTABLE WARNINGS
- FRAME AND LIDS TO BE ADJUSTED
- DRAINAGE AND UTILITY STRUCTURE TO BE CLEANED
- DRAINAGE AND UTILITY STRUCTURE TO BE RECONSTRUCTION (SPECIAL)
- LIMITS AND QUANTITY OF PR. P.C.C. SIDEWALK, 5" REMOVAL AND REPLACEMENT- XX SF
- LIMITS AND QUANTITY OF PR. COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT- XX FT



RESURFACING (TYP.)
 PR. HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/2"
 PR. POLY. HMA BINDER, IL-4.75, N50, 3/4"
 PR. HMA SURFACE CRS, MIX "D", IL-9.5, N70, 2"



FILE NAME = 19 Roadway Plans 05.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
 PROPOSED ROADWAY PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	19
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 12" STOP LINE, WHITE (TYP)

PROJECT BEGINS STA. 11+26

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 6" TURN LANE LINE, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" SKIP DASH, 30' SKIP- 10' DASH, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS LETTERS AND SYMBOLS, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 6" DOTTED LINE, WHITE (TYP)

110+00

111

112

WOODWARD AVENUE

113

114

MATCHLINE STA. 15+00

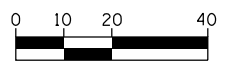
75TH STREET

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" SKIP DASH, 30' SKIP- 10' DASH, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" EDGE LINE, YELLOW (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" DOUBLE LINE, 11" C-C, YELLOW (TYPICAL)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 12" DIAGONAL MEDIAN LINE, YELLOW (TYP)



HORIZONTAL SCALE IN FEET

LEGEND

- INLET FILTERS
- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 4"



PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" SKIP DASH, 30' SKIP- 10' DASH, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS LETTERS AND SYMBOLS, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 6" DOTTED LINE, WHITE (TYP)

MATCHLINE STA. 15+00

15+00

116

WOODWARD AVENUE

117

118

119

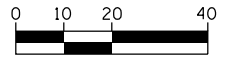
120+00

MATCHLINE STA. 21+00

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" DOUBLE LINE, 11" C-C, YELLOW (TYPICAL)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 4" SKIP DASH, 30' SKIP- 10' DASH, WHITE (TYP)

PR. THERMOPLASTIC PAVEMENT MARKINGS- LINE 6" TURN LANE LINE, WHITE (TYP)



HORIZONTAL SCALE IN FEET

FILE NAME = 20 PM.Land Plans 01.dgn



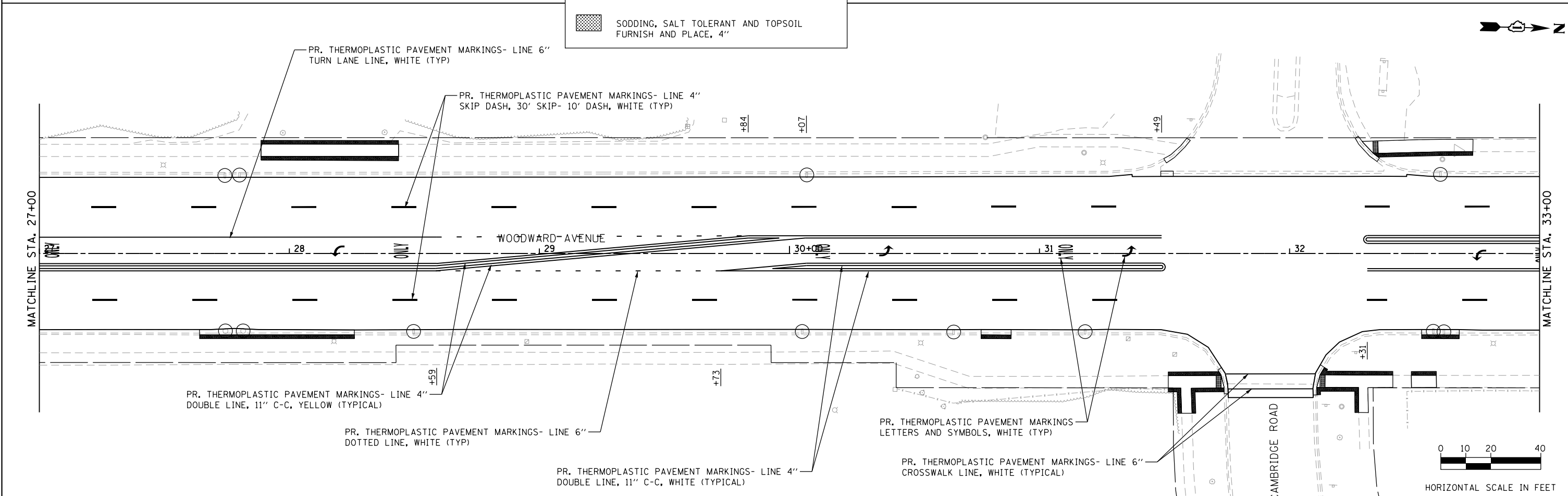
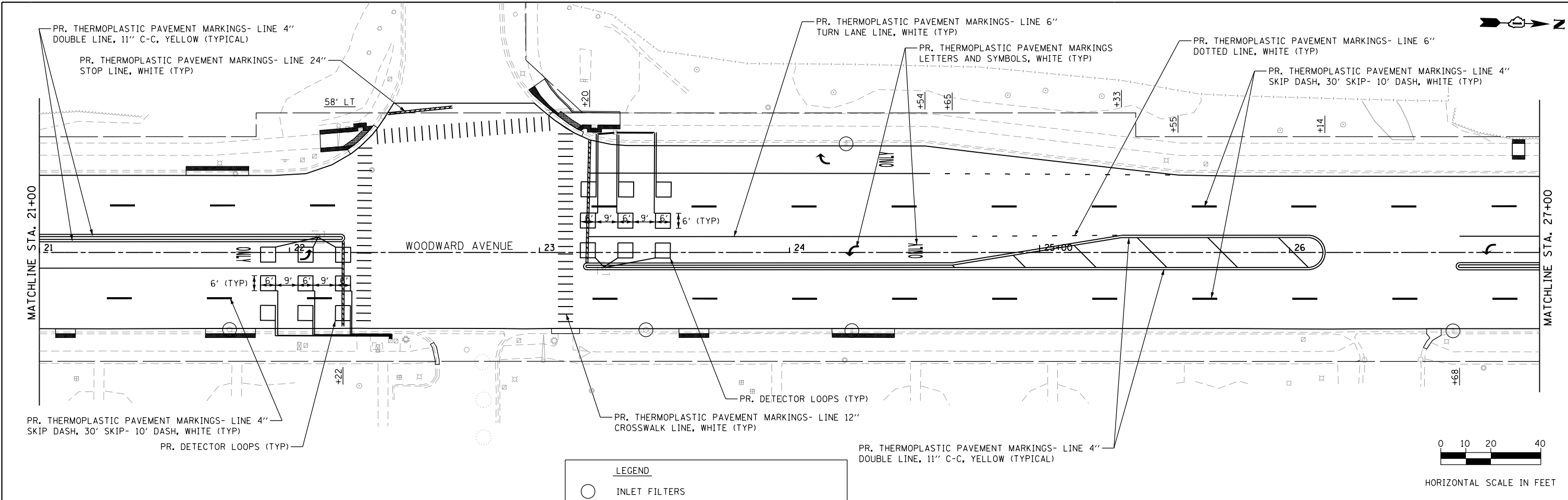
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**WOODWARD AVENUE
PROPOSED PAVEMENT MARKING, EROSION CONTROL
AND LANDSCAPING PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	20
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 21 PM_Land Plans 02.dgn



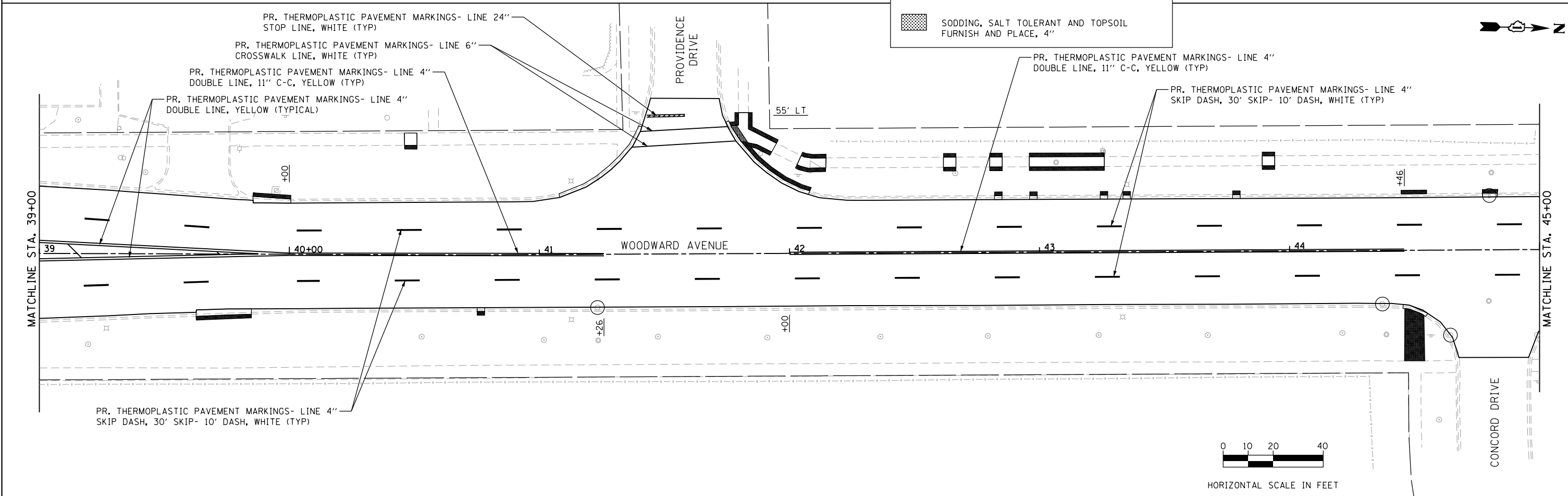
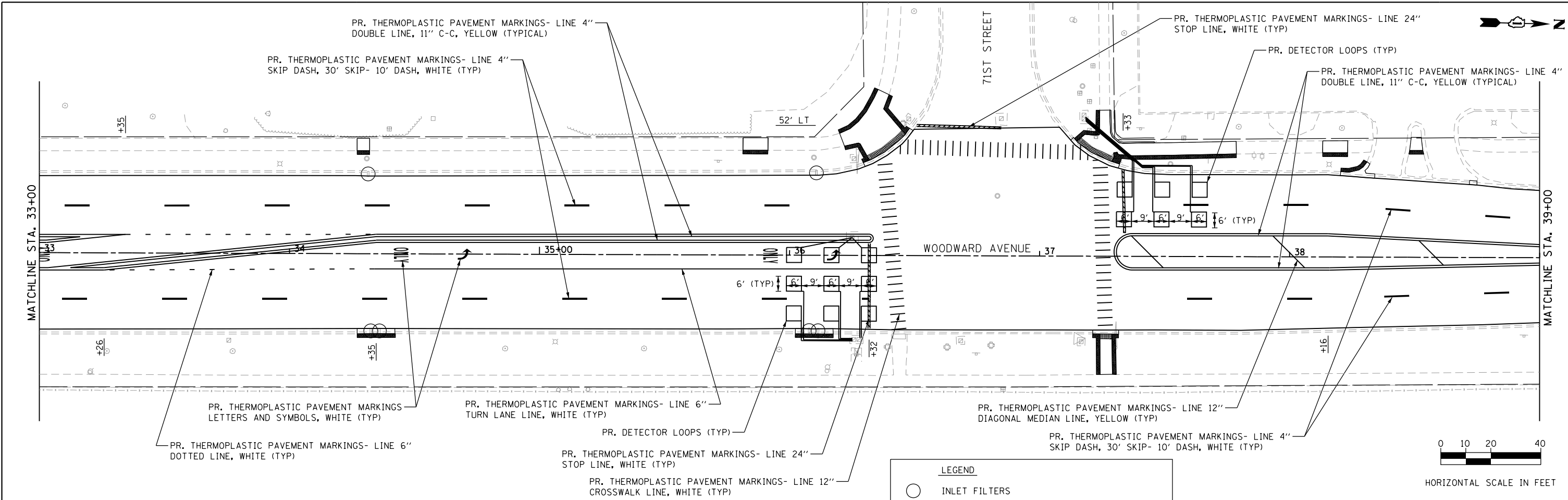
USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**WOODWARD AVENUE
PROPOSED PAVEMENT MARKING, EROSION CONTROL
AND LANDSCAPING PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	21
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 22 PM Land Plans 03.dgn



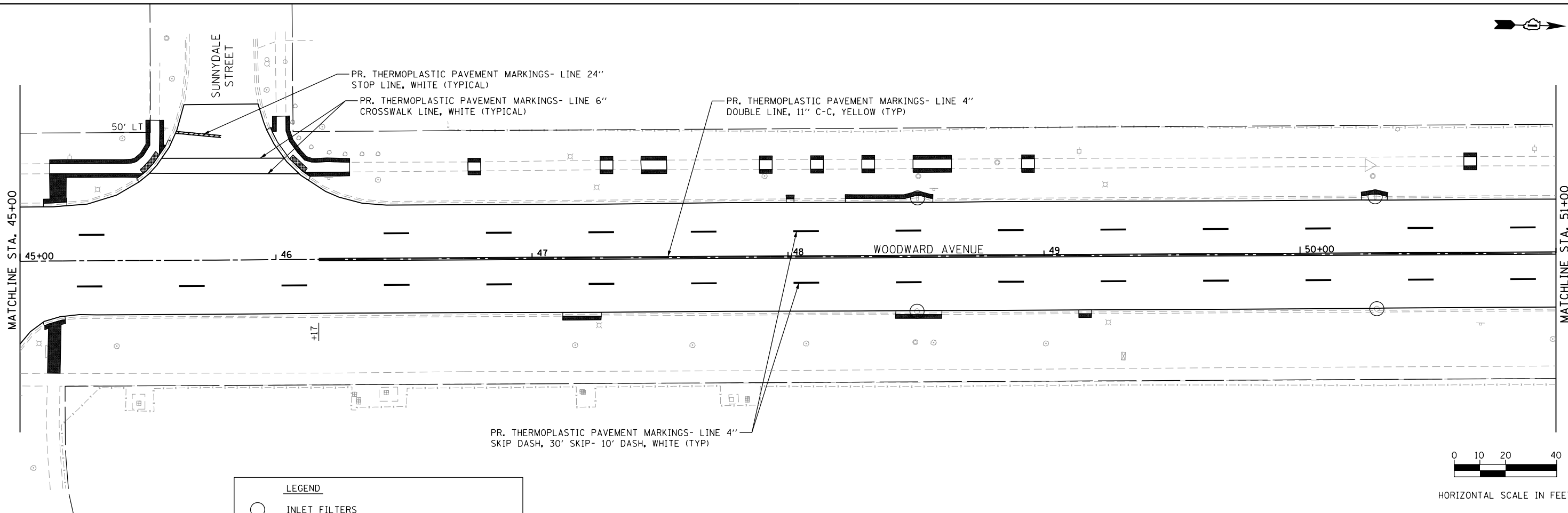
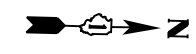
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**WOODWARD AVENUE
PROPOSED PAVEMENT MARKING, EROSION CONTROL
AND LANDSCAPING PLANS**

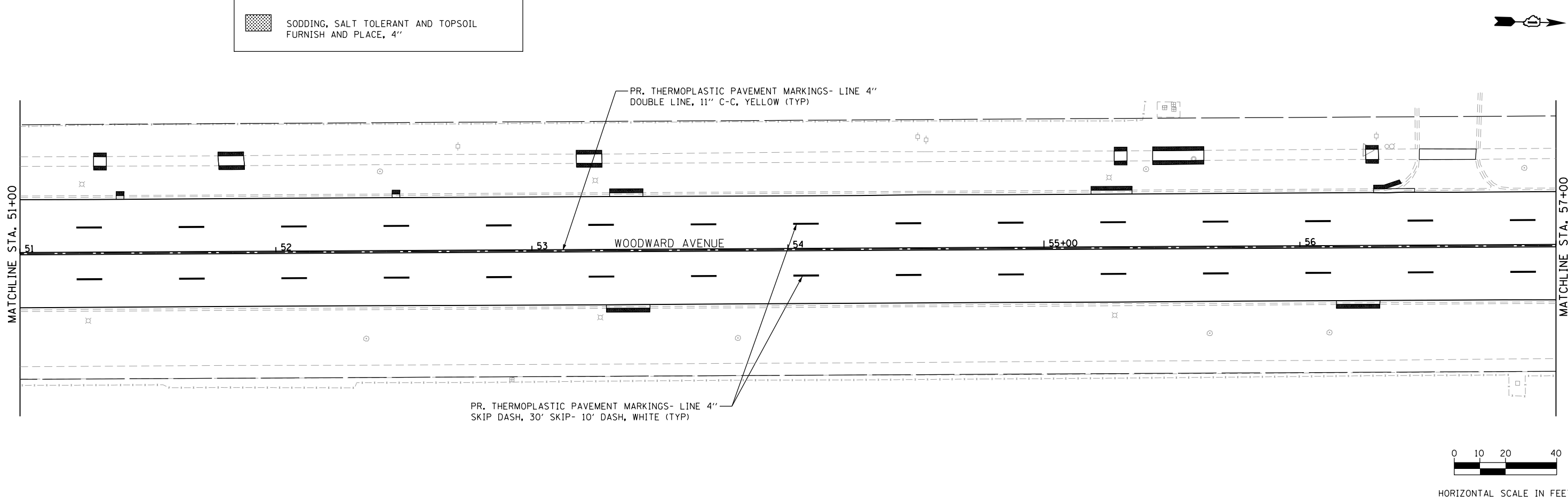
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 22
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	



LEGEND

- INLET FILTERS
- SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 4"



FILE NAME = 23 PM_Land Plans B4.dgn



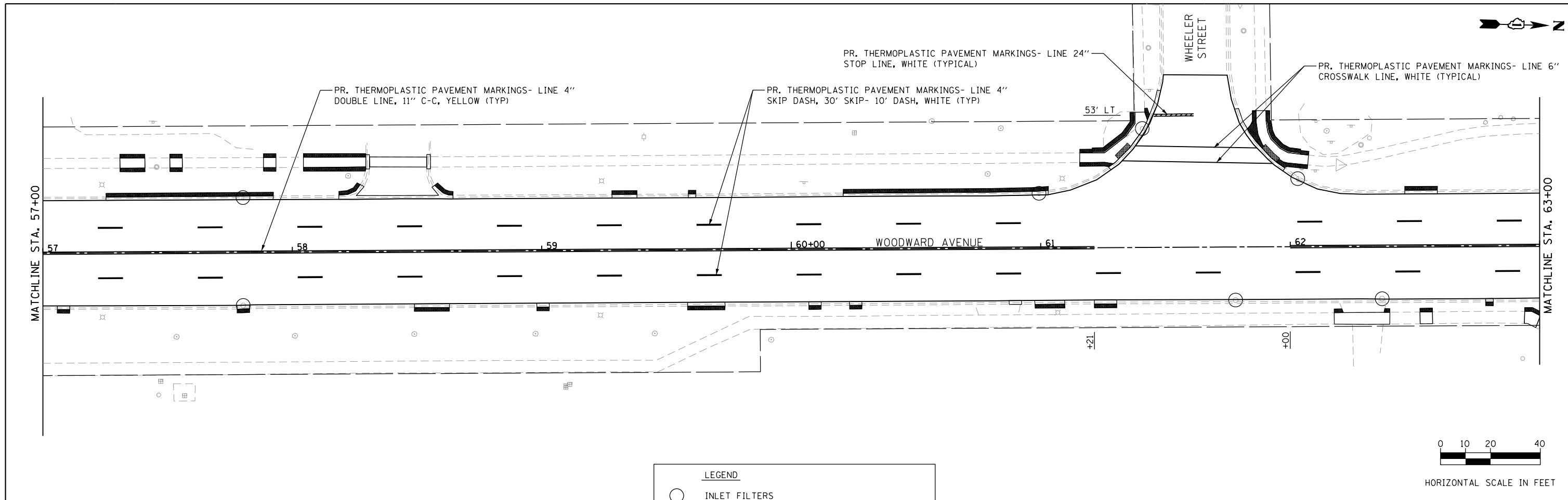
USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**WOODWARD AVENUE
PROPOSED PAVEMENT MARKING, EROSION CONTROL
AND LANDSCAPING PLANS**

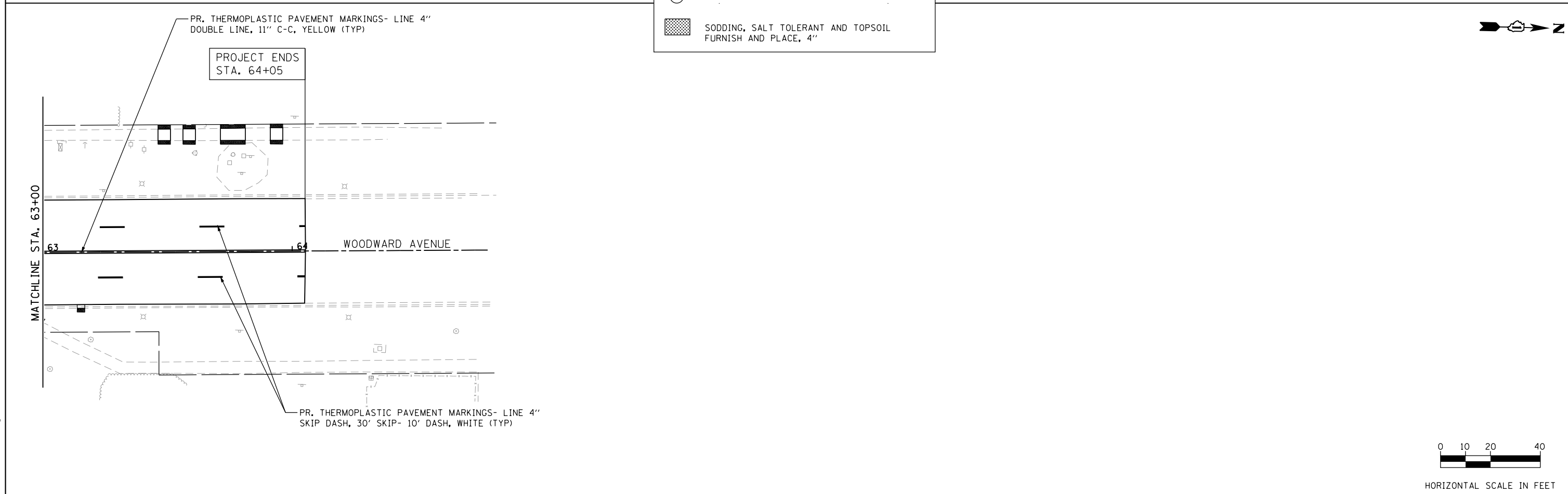
F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 23
CONTRACT NO. 61G27			ILLINOIS FED. AID PROJECT	

SCALE: SHEET OF SHEETS STA. TO STA.



LEGEND

-  INLET FILTERS
-  SODDING, SALT TOLERANT AND TOPSOIL FURNISH AND PLACE, 4"



FILE NAME = 24-PM-Land Plans 05.dgn



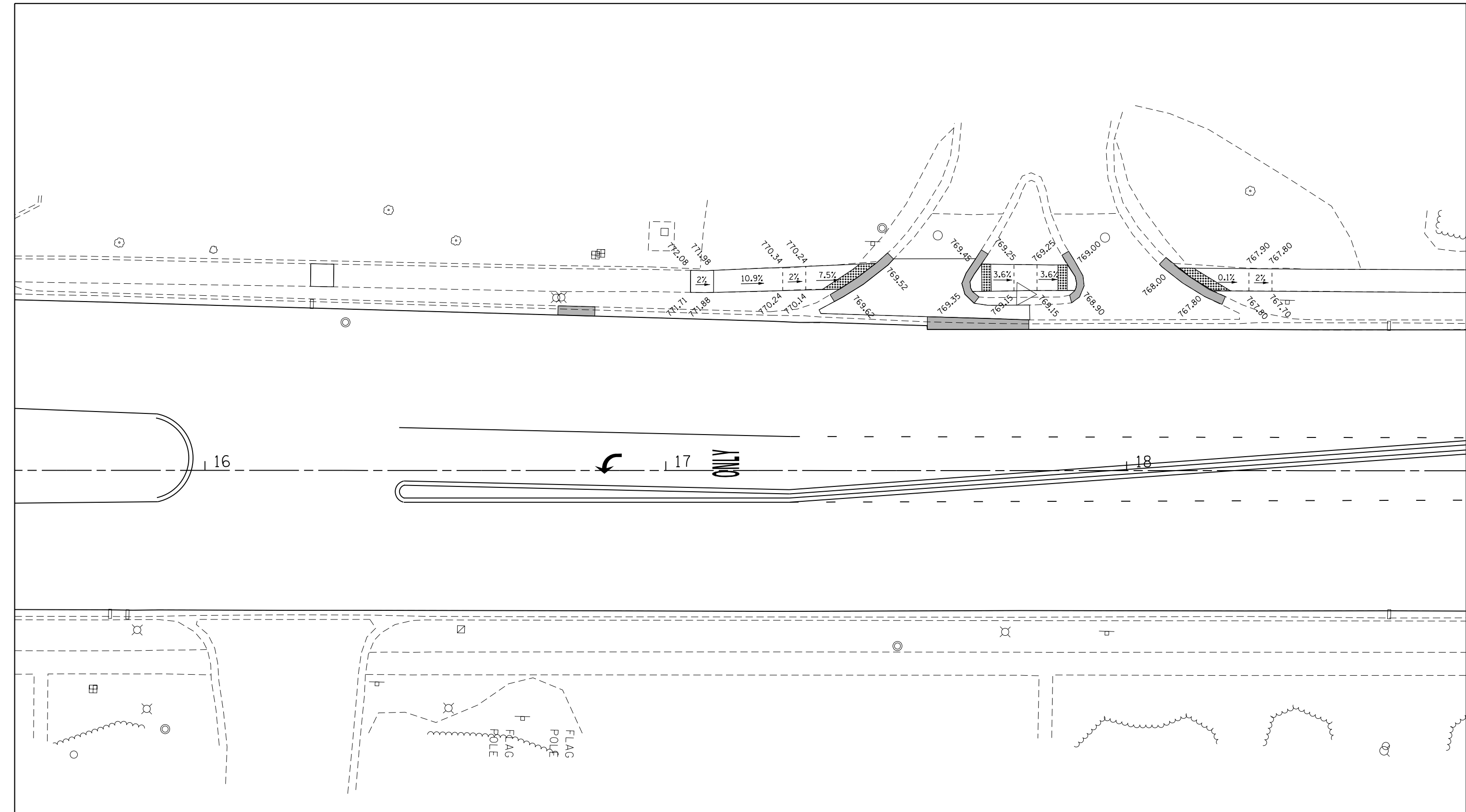
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

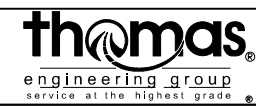
**WOODWARD AVENUE
PROPOSED PAVEMENT MARKING, EROSION CONTROL
AND LANDSCAPING PLANS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	24
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 25 Sidewalk Plans 01.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

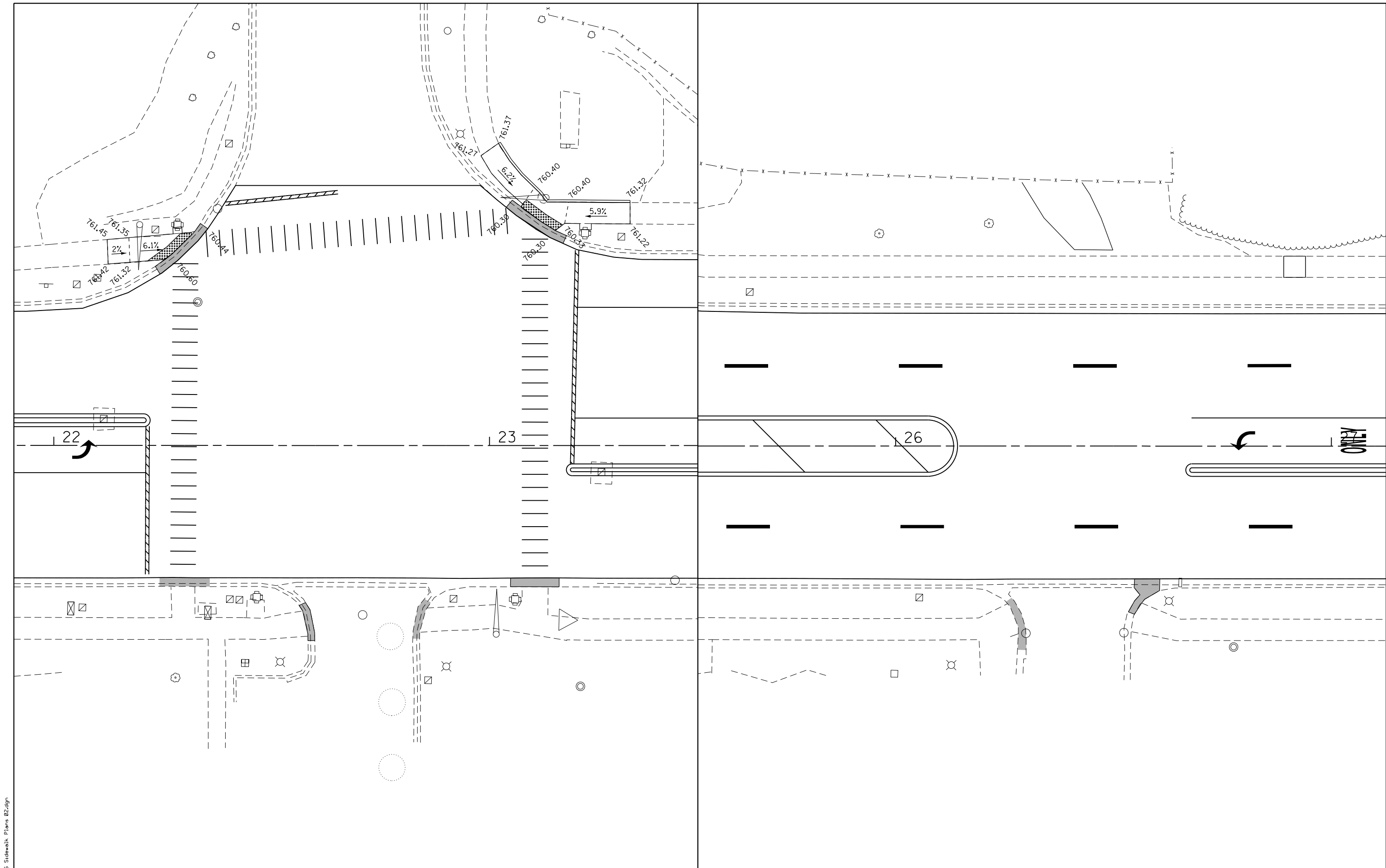
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	25
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				

FILE NAME = 26 Sidewalk Plans 02.dgn



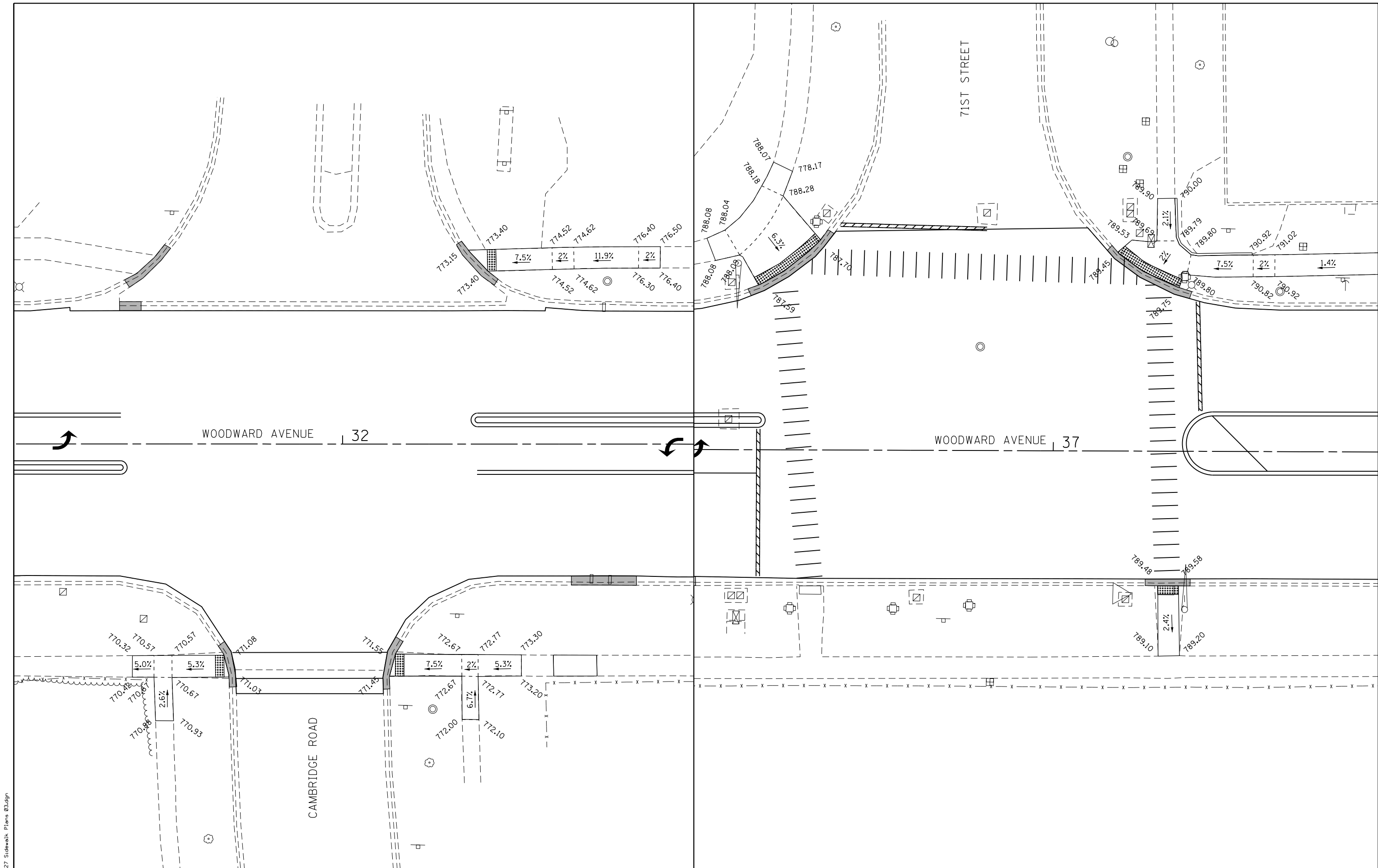
USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/12/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	26
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 27_Sidewalk_Plans_03.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

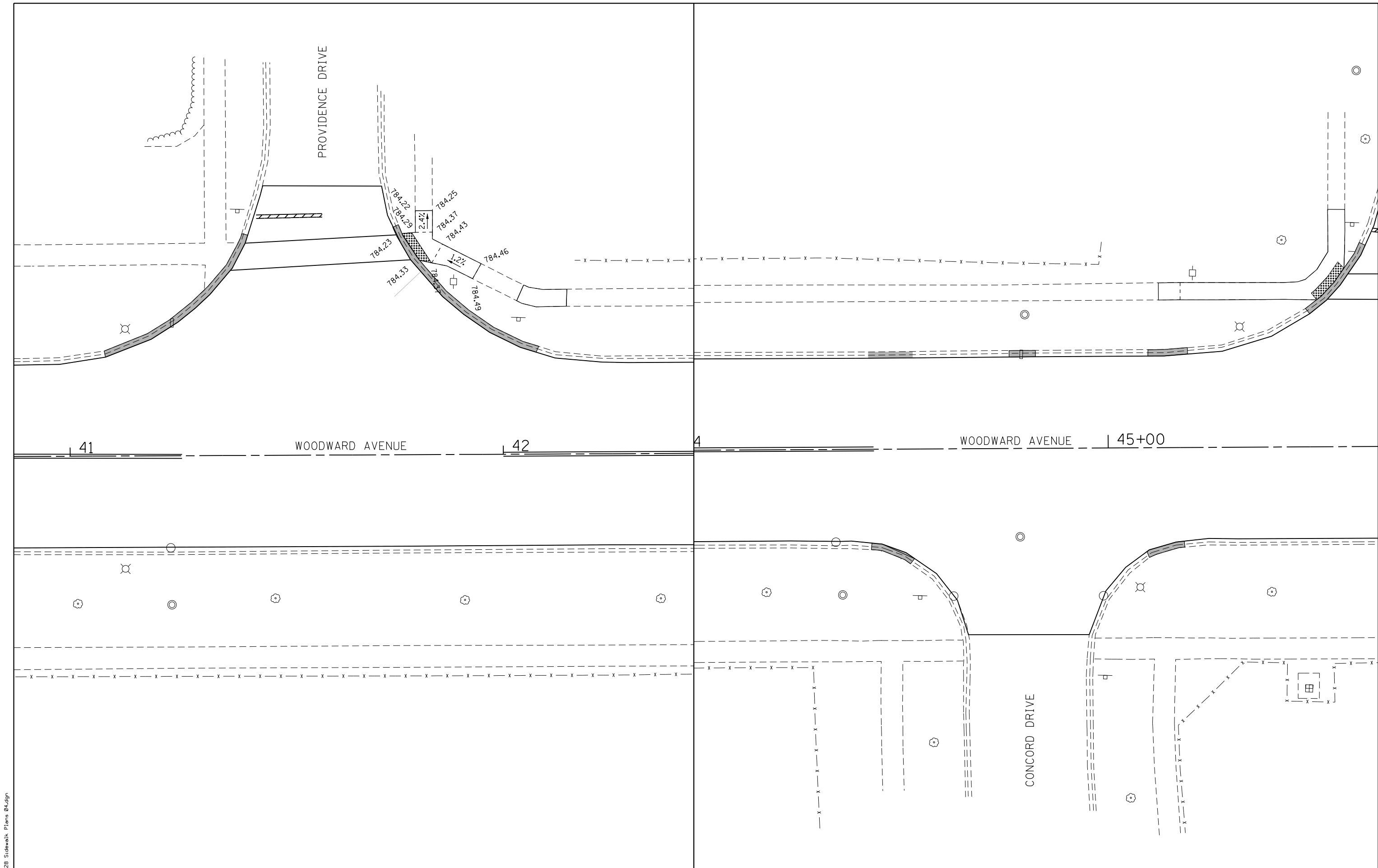
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 27
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				

FILE NAME = 28 Sidewalk Plans 04.dgn



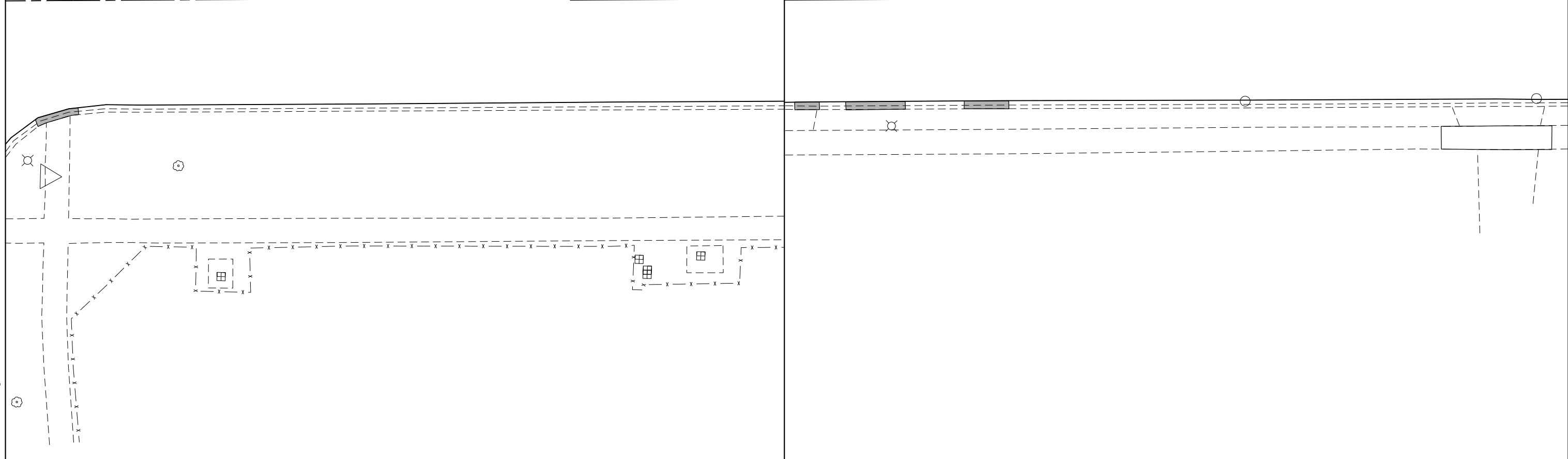
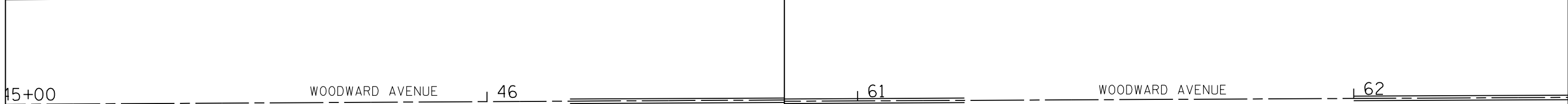
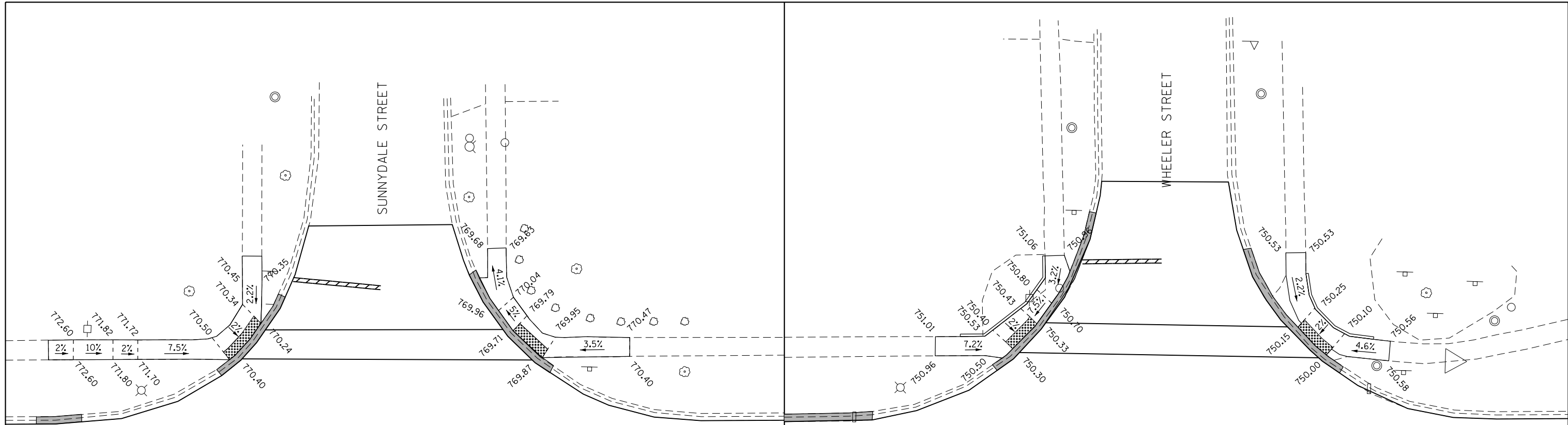
USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/12/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE. 2593	SECTION 19-00082-00-RS	COUNTY DUPAGE	TOTAL SHEETS 39	SHEET NO. 28
CONTRACT NO. 61G27				
ILLINOIS FED. AID PROJECT				



FILE NAME = 29 Sidewalk Plans 05.dgn



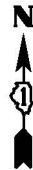
USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/12/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	29
				CONTRACT NO. 61G27
ILLINOIS FED. AID PROJECT				



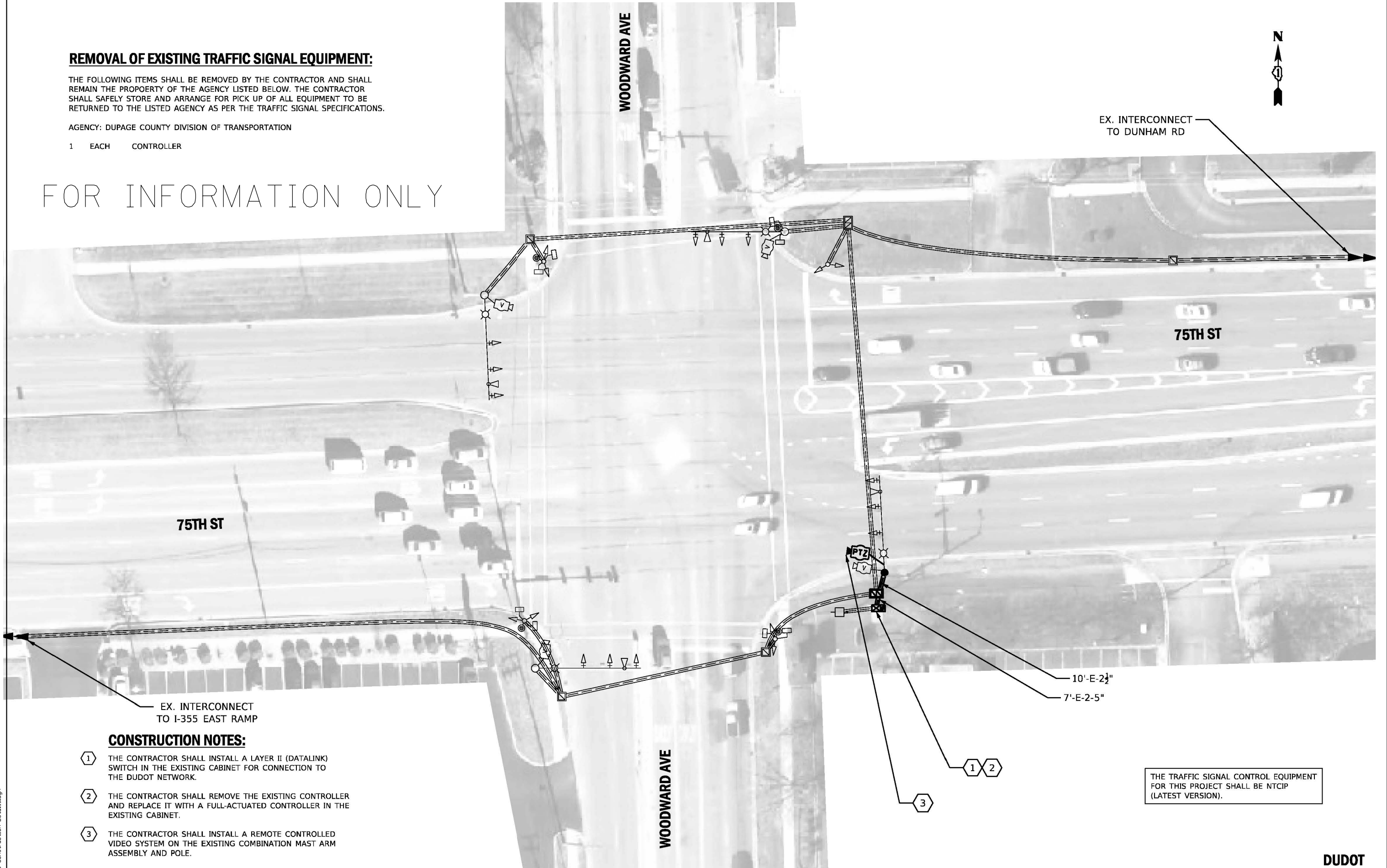
REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

AGENCY: DUPAGE COUNTY DIVISION OF TRANSPORTATION

1 EACH CONTROLLER

FOR INFORMATION ONLY



CONSTRUCTION NOTES:

- 1 THE CONTRACTOR SHALL INSTALL A LAYER II (DATALINK) SWITCH IN THE EXISTING CABINET FOR CONNECTION TO THE DUDOT NETWORK.
- 2 THE CONTRACTOR SHALL REMOVE THE EXISTING CONTROLLER AND REPLACE IT WITH A FULL-ACTUATED CONTROLLER IN THE EXISTING CABINET.
- 3 THE CONTRACTOR SHALL INSTALL A REMOTE CONTROLLED VIDEO SYSTEM ON THE EXISTING COMBINATION MAST ARM ASSEMBLY AND POLE.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE NTCIP (LATEST VERSION).

FILE NAME = 30 Construction Details.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT N.T.S., 2.0000" / in.	DRAWN -	REVISED -
PLOT DATE = 12/12/2019	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

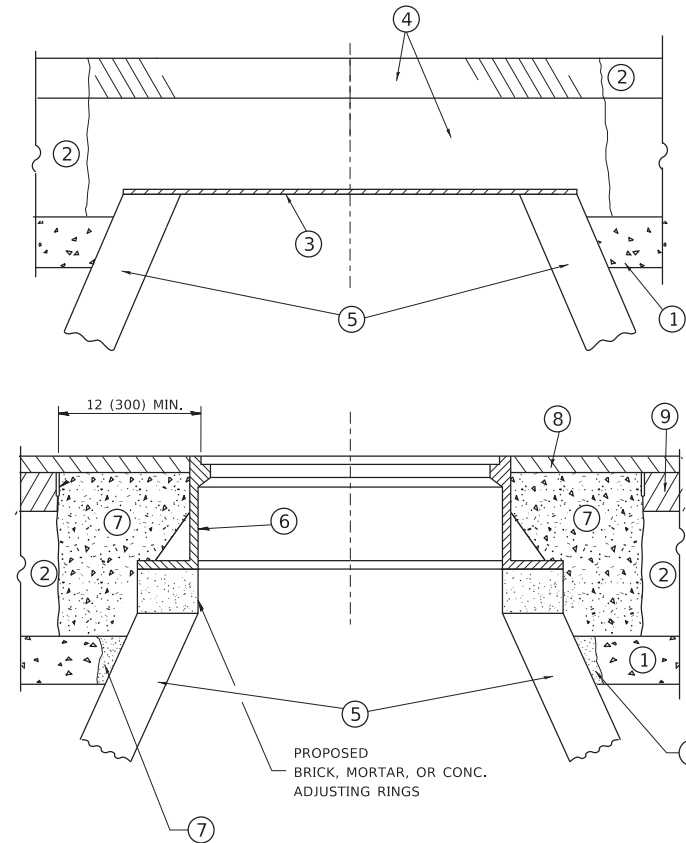
WOODWARD AVE AND 75TH STREET
EXISTING DETECTOR LOOP PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DUPAGE	39	30
CONTRACT NO. 61G27				

DUDOT

ILLINOIS FED. AID PROJECT



NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.*

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 *CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: D:\draft...
 FILE: M:\M\...
 PROJECT: ...
 OFFICE: ...
 DATE: 3/27/2019

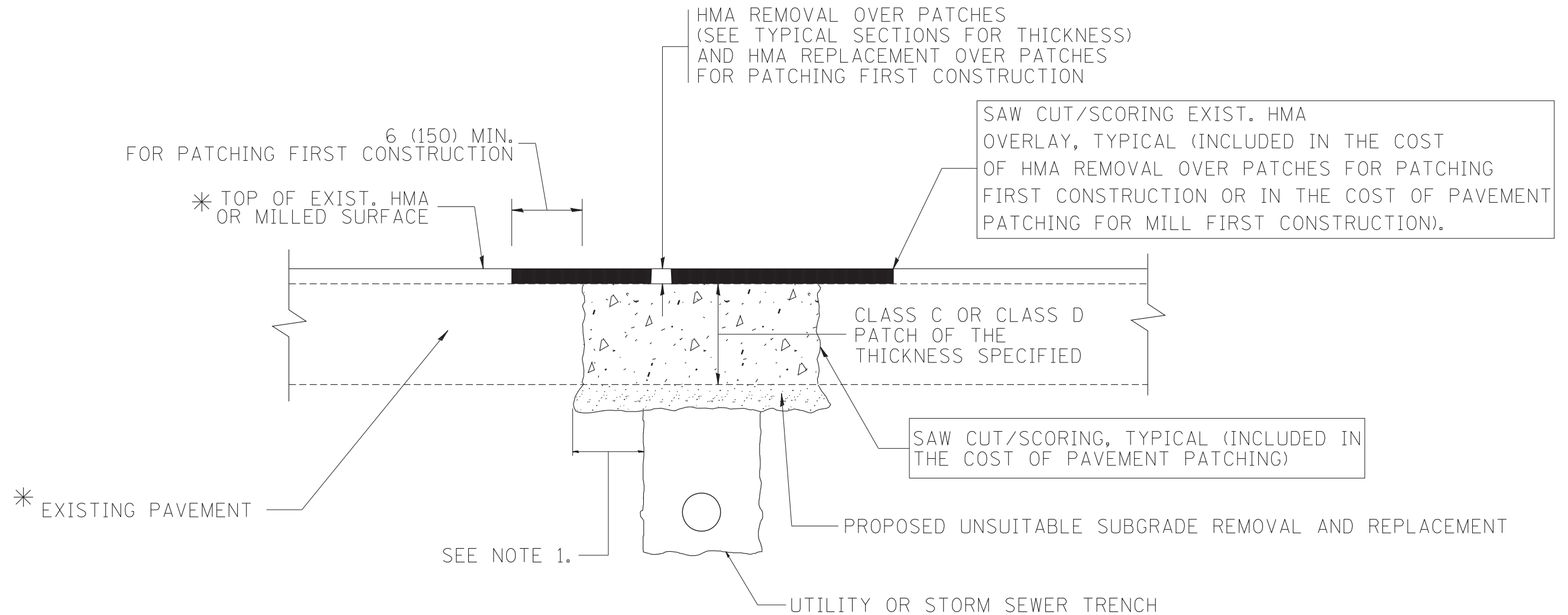
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	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 3/27/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	31
BD600-03 (BD-8)		CONTRACT NO. 61G27		
ILLINOIS		FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	32
BD400-04 (BD-22)		CONTRACT NO.	61G27	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

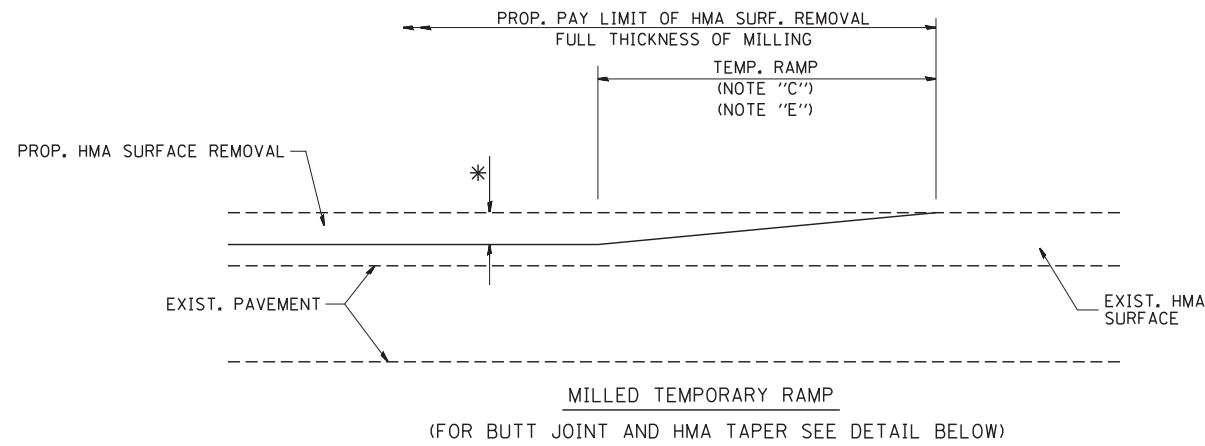
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

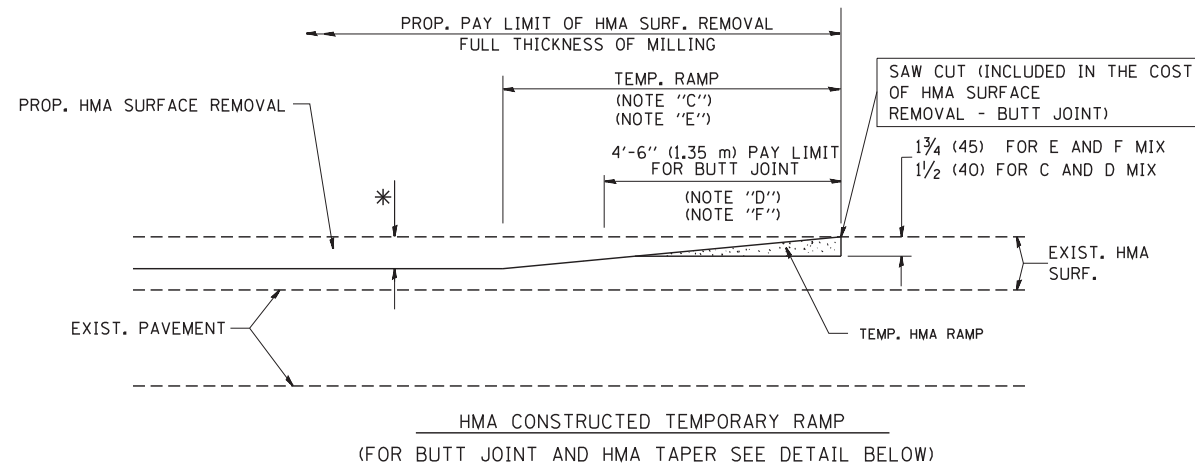
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p1dot\drivakosgn\0108315\bd24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			2593	19-00082-00-RS	DuPage	39	33
PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 61G27		
PLOT DATE = 12/15/2009	DATE - 03-11-94					FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

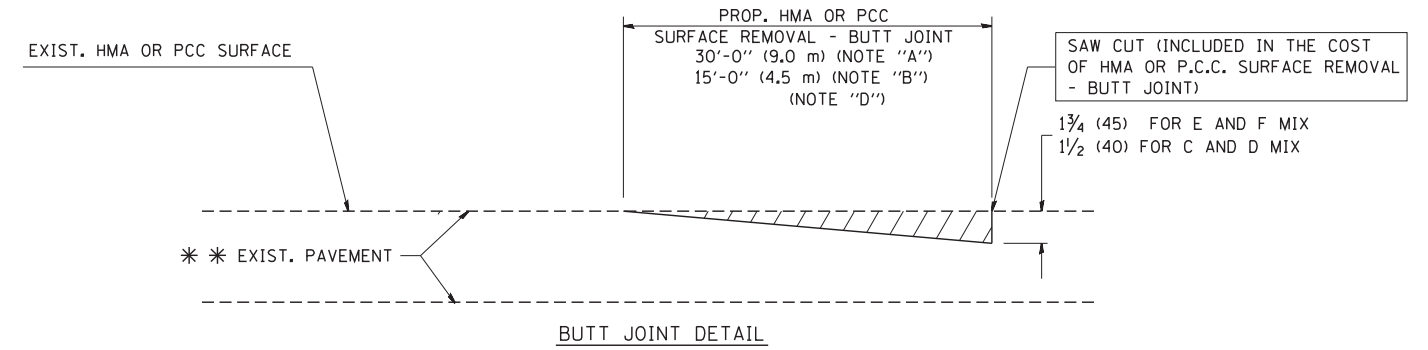


OPTION 1

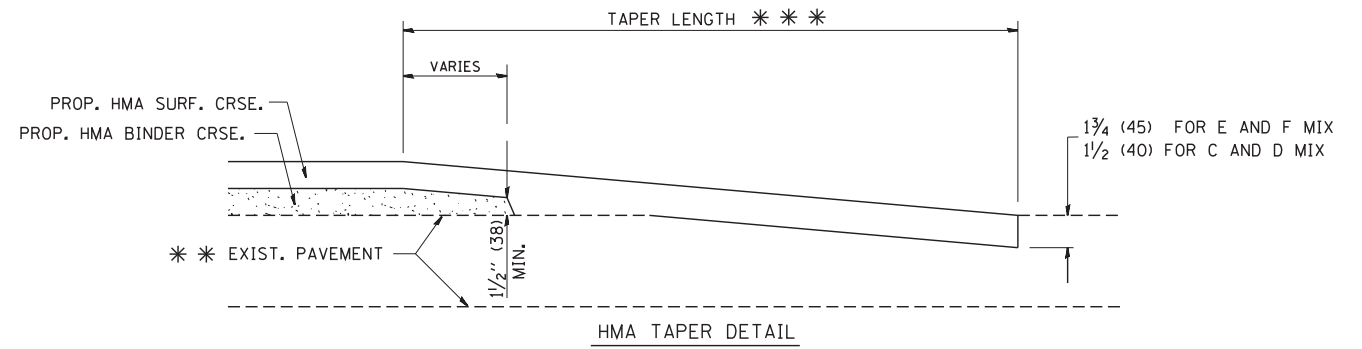


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

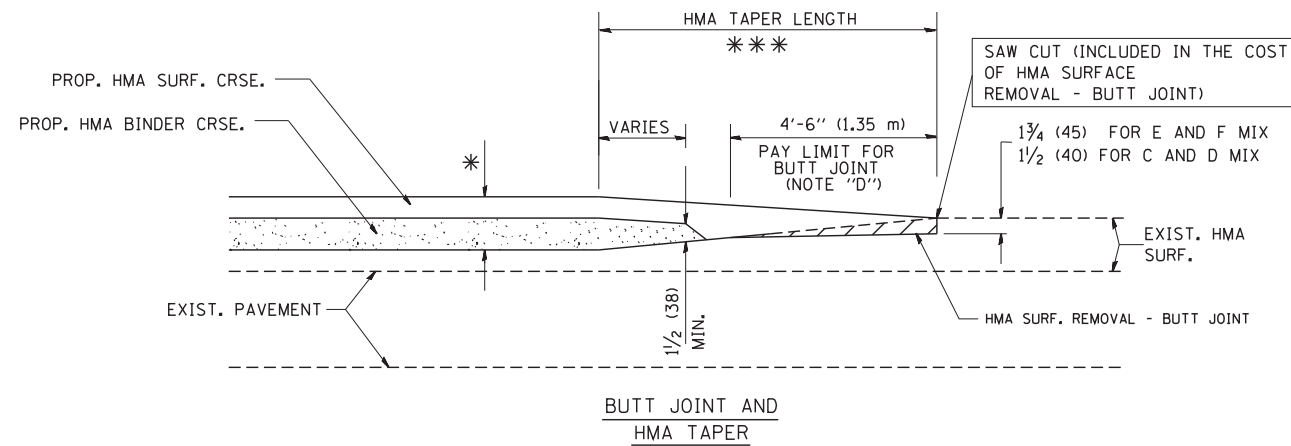
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



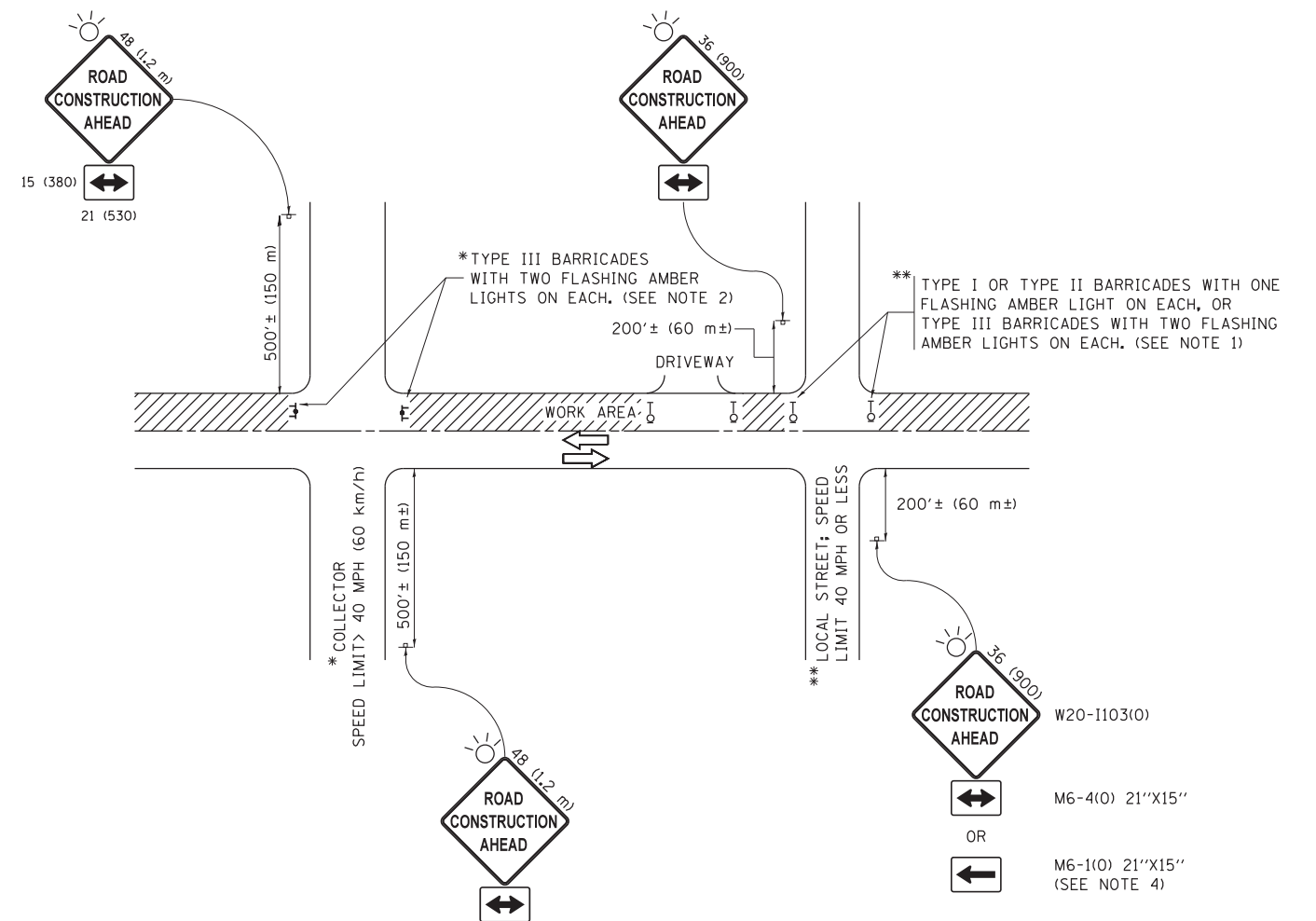
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gajlonobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	34
BD400-05 BD32			CONTRACT NO. 61G27	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

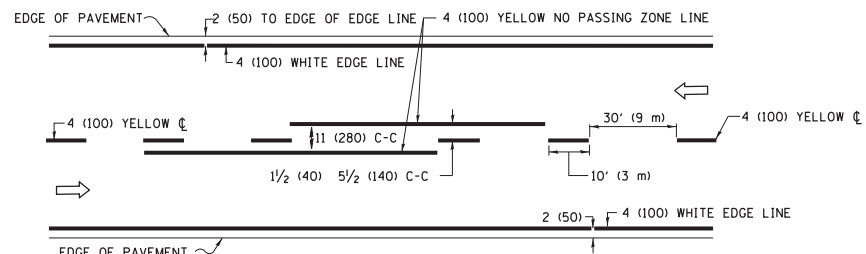
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

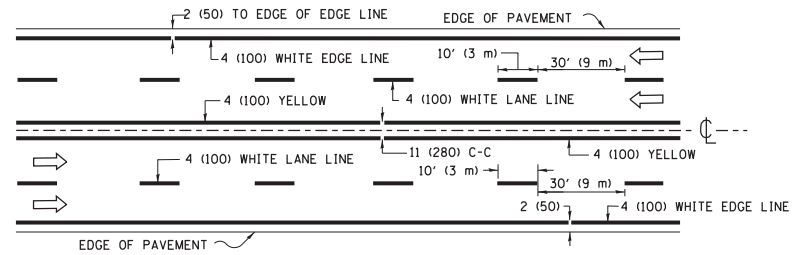
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

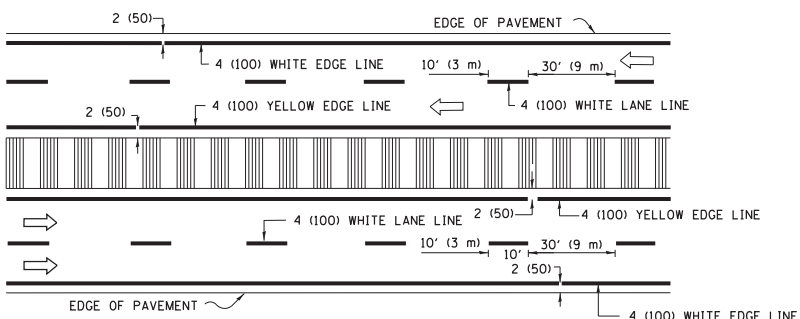
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	35
TC-10		CONTRACT NO.	61G27	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

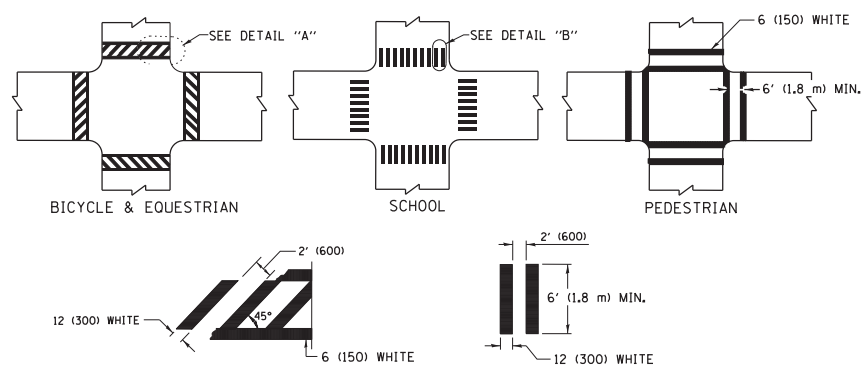


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

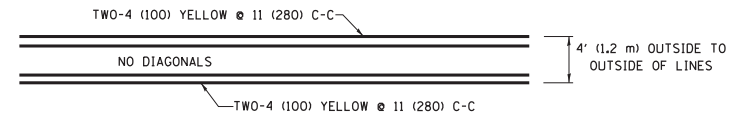


DETAIL "A"

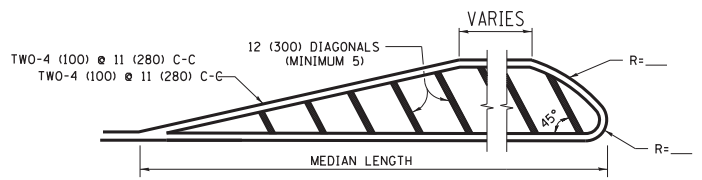
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

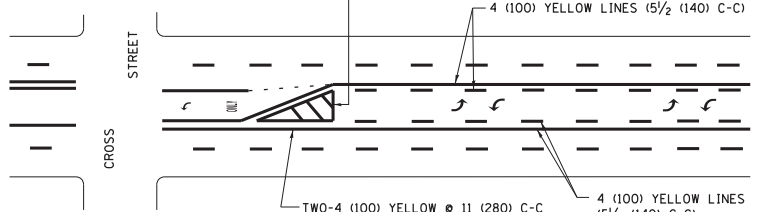


4' (1.2 m) WIDE MEDIANS ONLY

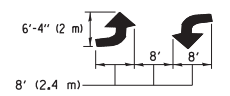


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

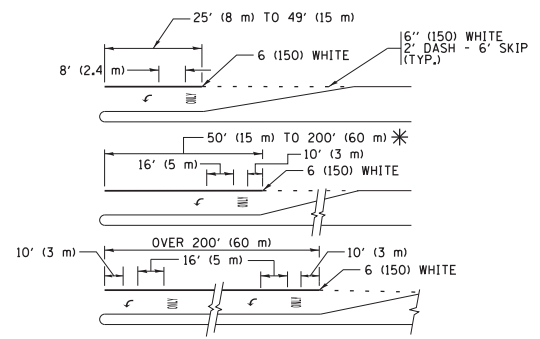


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

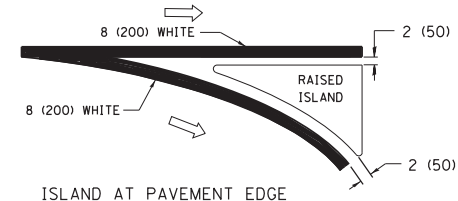
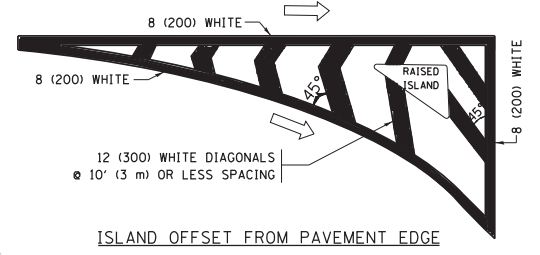


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

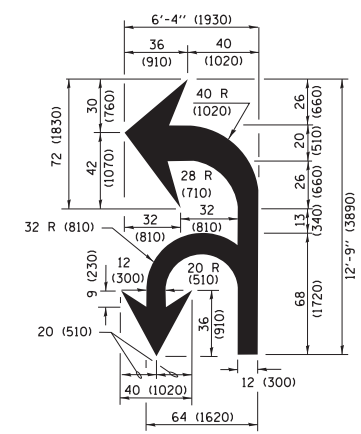
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

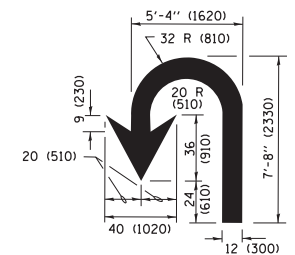
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

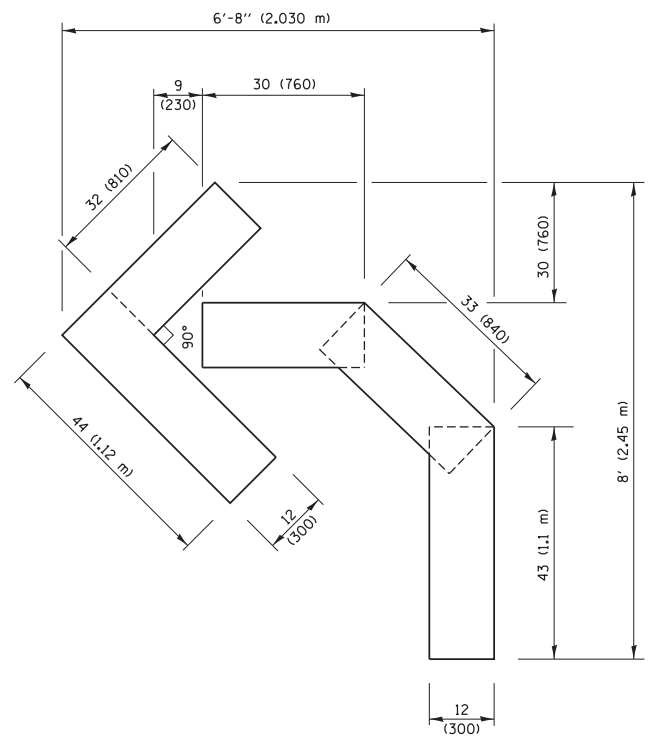
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	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

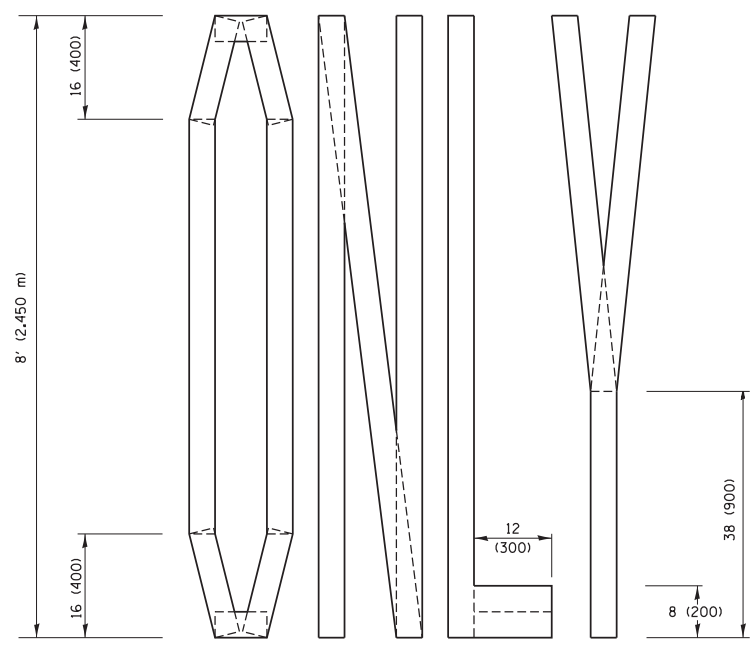
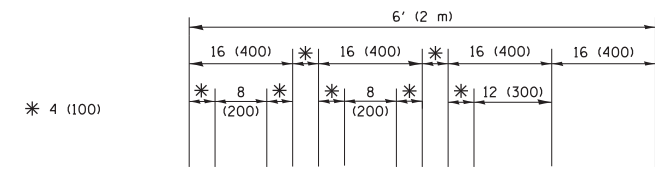
**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

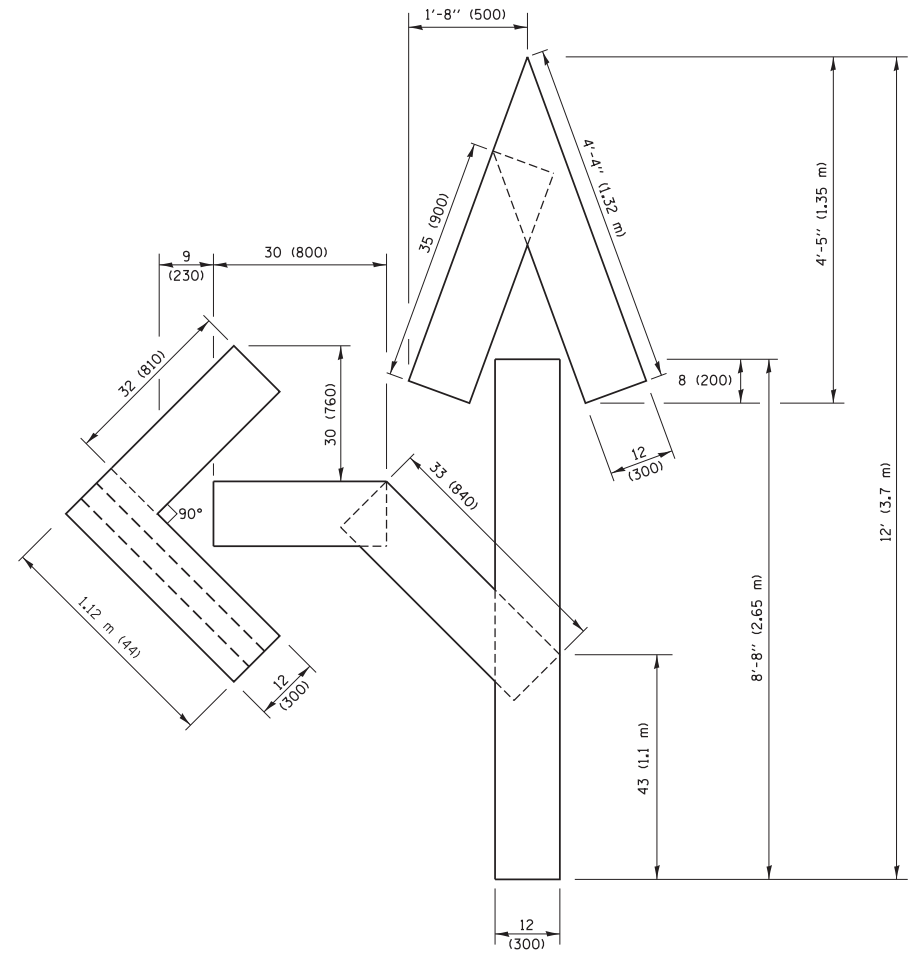
F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	36
TC-13		CONTRACT NO.	61G27	
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

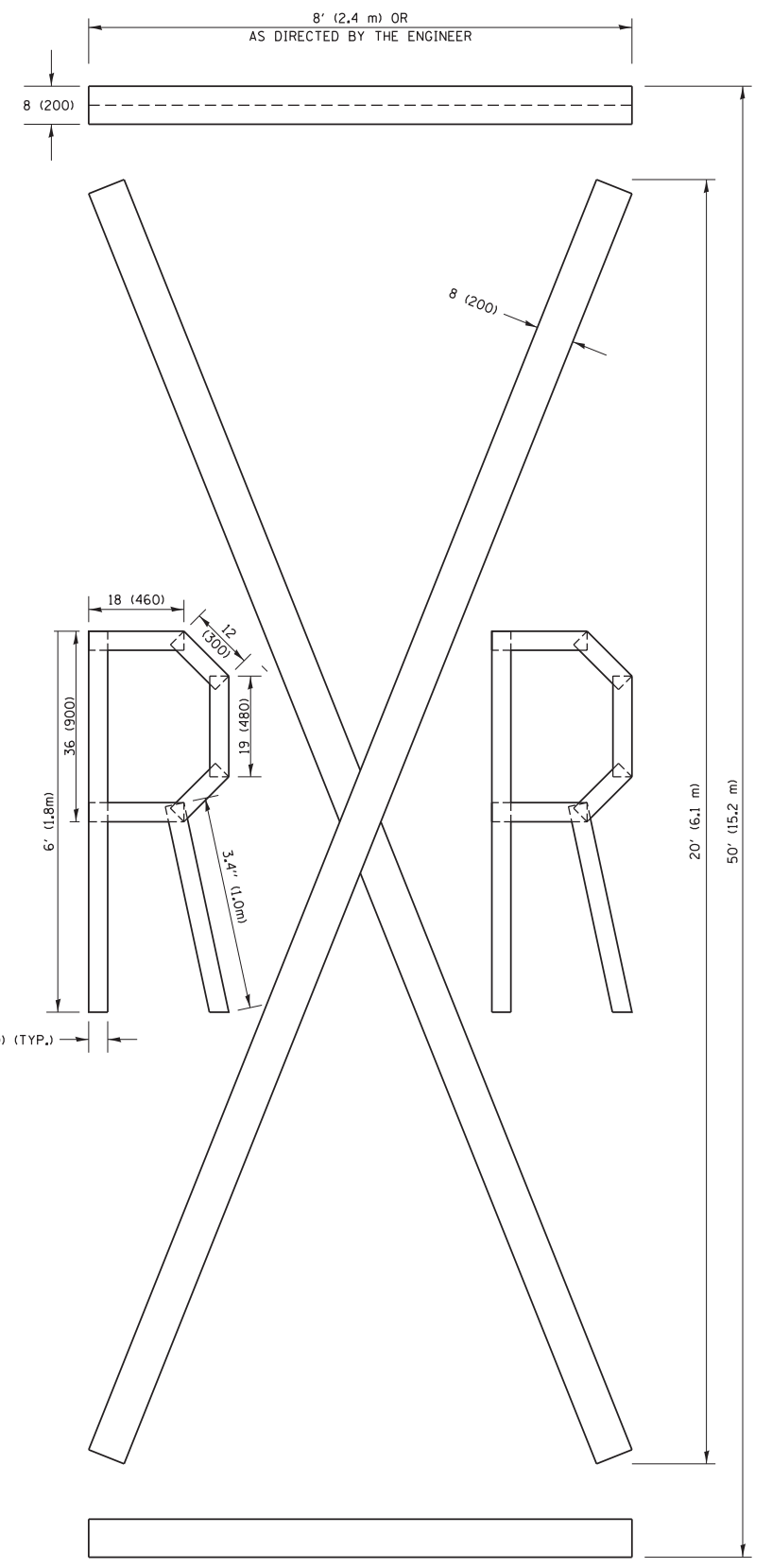


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

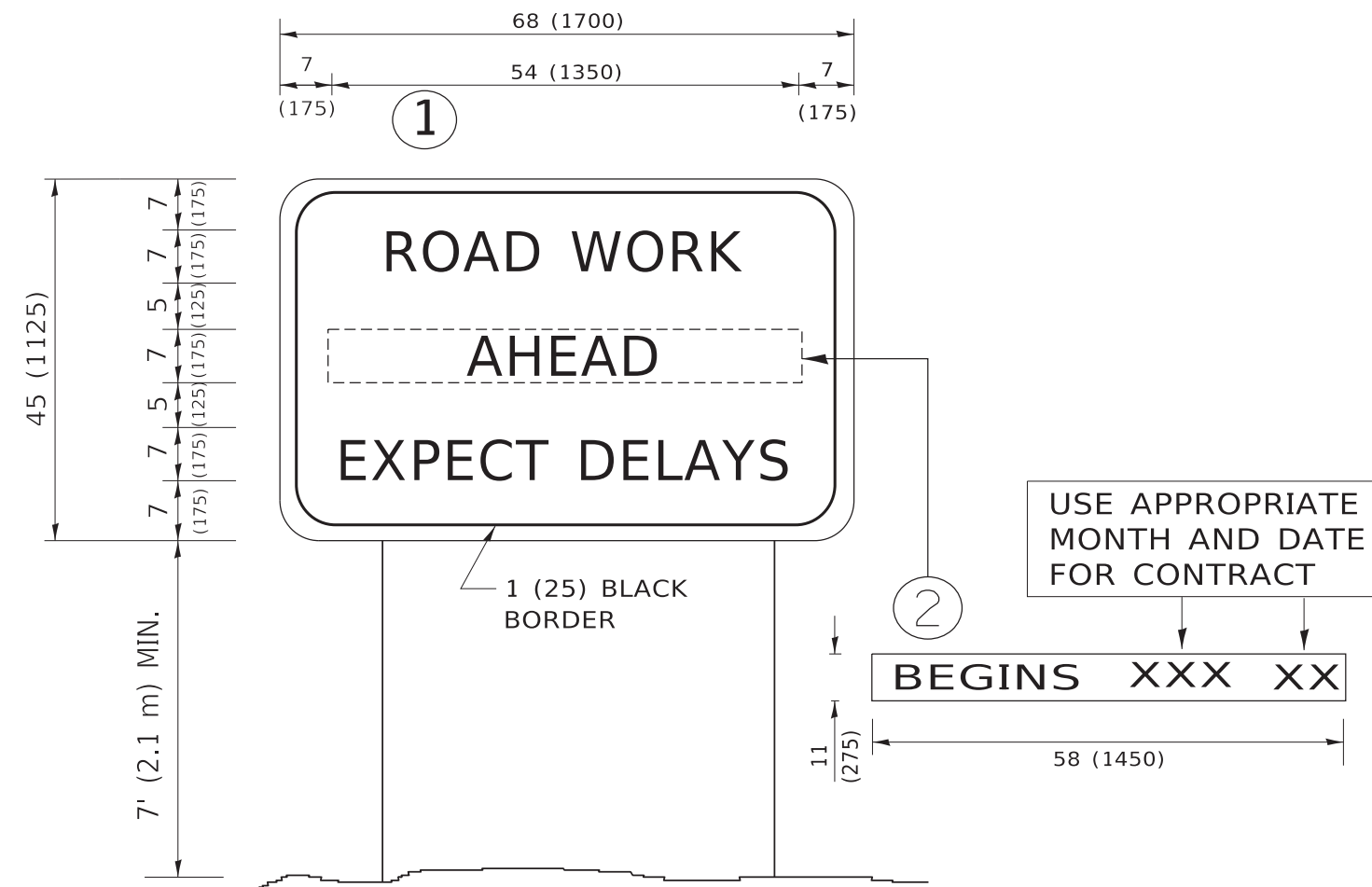
All dimensions are in inches (millimeters) unless otherwise shown.

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pw:\1\084EBIDINTEG\111nois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\084EBIDINTEG\CADD\Drawings\16.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	37
TC-16		CONTRACT NO. 61G27		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: D:\draft...
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USER NAME = footemj	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

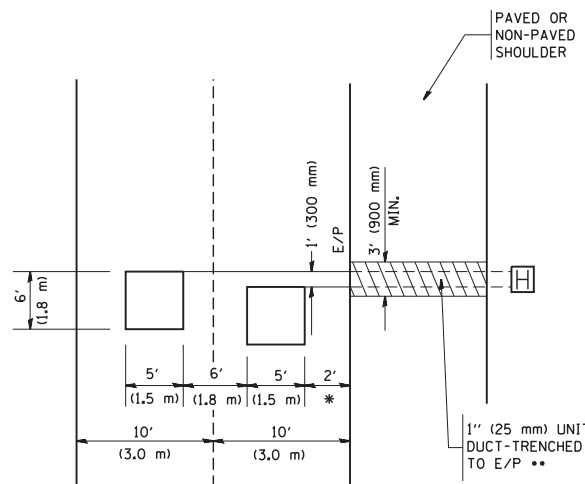
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	38
TC-22			CONTRACT NO. 61G27	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

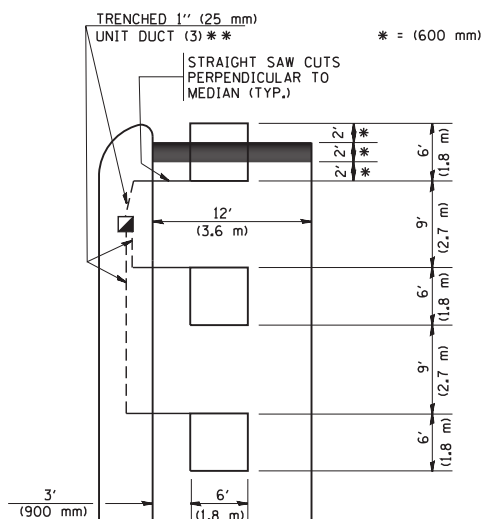


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

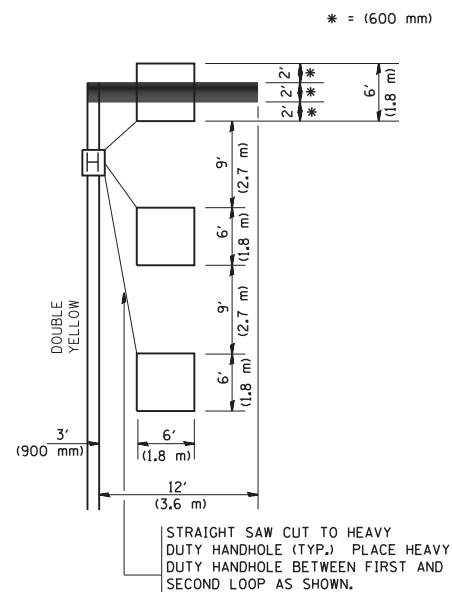
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

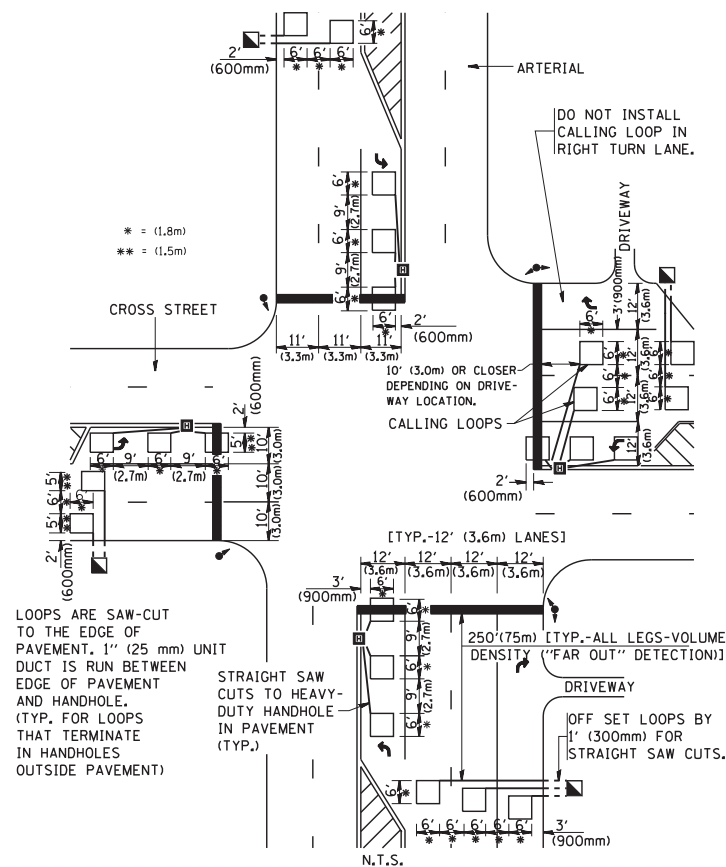
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



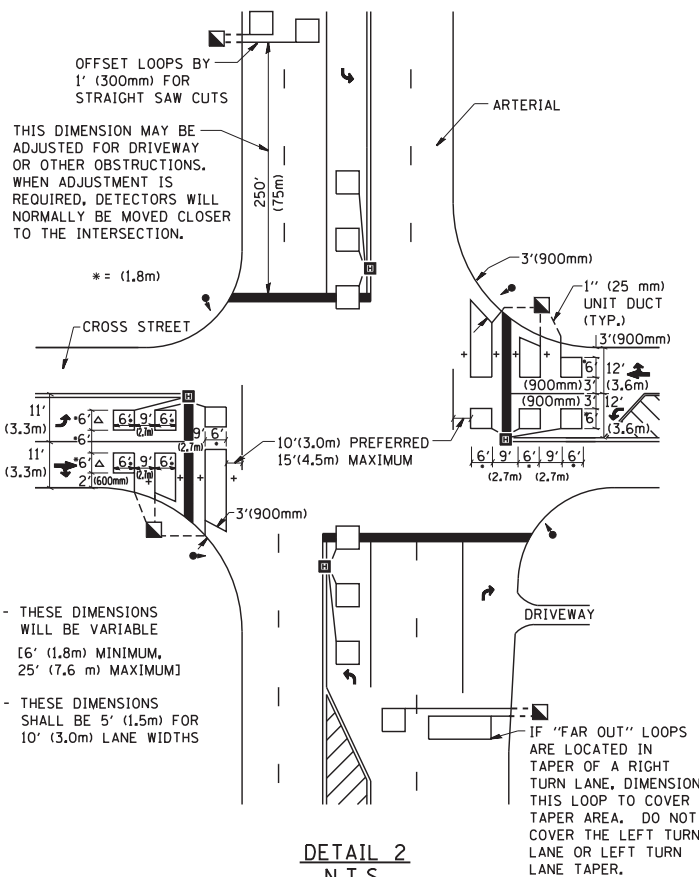
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gaglianobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2593	19-00082-00-RS	DuPage	39	39
TS-07		CONTRACT NO.	61G27	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				