

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

Contract # 66607

ROADWAY & PG

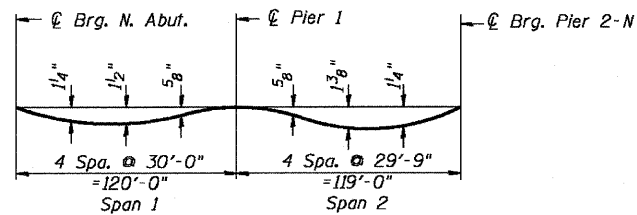
Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. N. Abut.	71+93.83	0.00	522.09	522.09
☉ Brg. N. Abut.	71+96.83	0.00	522.24	522.24
1A	72+06.83	0.00	522.73	522.77
1B	72+16.83	0.00	523.23	523.30
1C	72+26.83	0.00	523.73	523.82
1D	72+36.83	0.00	524.23	524.34
1E	72+46.83	0.00	524.72	524.84
1F	72+56.83	0.00	525.22	525.33
1G	72+66.83	0.00	525.72	525.81
1H	72+76.83	0.00	526.21	526.29
1I	72+86.83	0.00	526.71	526.76
1J	72+96.83	0.00	527.21	527.23
1K	73+06.83	0.00	527.70	527.71
☉ Pier 1	73+16.83	0.00	528.20	528.20
2A	73+26.83	0.00	528.70	528.70
2B	73+36.83	0.00	529.20	529.22
2C	73+46.83	0.00	529.69	529.74
2D	73+56.83	0.00	530.19	530.26
2E	73+66.83	0.00	530.69	530.78
2F	73+76.83	0.00	531.18	531.29
2G	73+86.83	0.00	531.68	531.79
2H	73+96.83	0.00	532.18	532.28
2I	74+06.83	0.00	532.67	532.76
2J	74+16.83	0.00	533.17	533.24
2K	74+26.83	0.00	533.67	533.70
☉ Brg Pier 2-N	74+35.83	0.00	534.12	534.12
☉ Pier 2	74+36.83	0.00	534.17	534.17

GIRDER 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. N. Abut.	71+93.83	6.13	521.99	521.99
☉ Brg. N. Abut.	71+96.83	6.13	522.14	522.14
1A	72+06.83	6.13	522.64	522.68
1B	72+16.83	6.13	523.14	523.21
1C	72+26.83	6.13	523.63	523.73
1D	72+36.83	6.13	524.13	524.24
1E	72+46.83	6.13	524.63	524.74
1F	72+56.83	6.13	525.12	525.23
1G	72+66.83	6.13	525.62	525.72
1H	72+76.83	6.13	526.12	526.19
1I	72+86.83	6.13	526.61	526.66
1J	72+96.83	6.13	527.11	527.14
1K	73+06.83	6.13	527.61	527.62
☉ Pier 1	73+16.83	6.13	528.11	528.11
2A	73+26.83	6.13	528.60	528.61
2B	73+36.83	6.13	529.10	529.12
2C	73+46.83	6.13	529.60	529.64
2D	73+56.83	6.13	530.09	530.16
2E	73+66.83	6.13	530.59	530.68
2F	73+76.83	6.13	531.09	531.19
2G	73+86.83	6.13	531.58	531.69
2H	73+96.83	6.13	532.08	532.19
2I	74+06.83	6.13	532.58	532.67
2J	74+16.83	6.13	533.08	533.14
2K	74+26.83	6.13	533.57	533.61
☉ Brg Pier 2-N	74+35.83	6.13	534.02	534.02
☉ Pier 2	74+36.83	6.13	534.07	534.07

GIRDER 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. N. Abut.	71+93.83	12.71	521.89	521.89
☉ Brg. N. Abut.	71+96.83	12.71	522.04	522.04
1A	72+06.83	12.71	522.53	522.57
1B	72+16.83	12.71	523.03	523.10
1C	72+26.83	12.71	523.53	523.62
1D	72+36.83	12.71	524.02	524.14
1E	72+46.83	12.71	524.52	524.64
1F	72+56.83	12.71	525.02	525.13
1G	72+66.83	12.71	525.51	525.61
1H	72+76.83	12.71	526.01	526.08
1I	72+86.83	12.71	526.51	526.56
1J	72+96.83	12.71	527.01	527.03
1K	73+06.83	12.71	527.50	527.51
☉ Pier 1	73+16.83	12.71	528.00	528.00
2A	73+26.83	12.71	528.50	528.50
2B	73+36.83	12.71	529.00	529.02
2C	73+46.83	12.71	529.49	529.54
2D	73+56.83	12.71	529.99	530.06
2E	73+66.83	12.71	530.48	530.57
2F	73+76.83	12.71	530.98	531.09
2G	73+86.83	12.71	531.48	531.59
2H	73+96.83	12.71	531.98	532.08
2I	74+06.83	12.71	532.47	532.56
2J	74+16.83	12.71	532.97	533.03
2K	74+26.83	12.71	533.47	533.50
☉ Brg Pier 2-N	74+35.83	12.71	533.91	533.91
☉ Pier 2	74+36.83	12.71	533.96	533.96



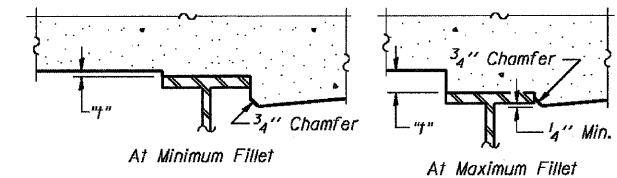
**DEAD LOAD DEFLECTION DIAGRAM**  
(Includes weight of concrete only.)

Note:  
The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown above.

DESIGNED - DEV
CHECKED - RJC
DRAWN - JHR
CHECKED - RJC

GIRDER 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflection
Bk. N. Abut.	71+93.83	19.29	521.75	521.75
☉ Brg. N. Abut.	71+96.83	19.29	521.90	521.90
1A	72+06.83	19.29	522.39	522.43
1B	72+16.83	19.29	522.89	522.97
1C	72+26.83	19.29	523.39	523.49
1D	72+36.83	19.29	523.89	524.00
1E	72+46.83	19.29	524.38	524.51
1F	72+56.83	19.29	524.88	525.00
1G	72+66.83	19.29	525.38	525.48
1H	72+76.83	19.29	525.87	525.95
1I	72+86.83	19.29	526.37	526.42
1J	72+96.83	19.29	526.87	526.90
1K	73+06.83	19.29	527.37	527.37
☉ Pier 1	73+16.83	19.29	527.86	527.86
2A	73+26.83	19.29	528.36	528.37
2B	73+36.83	19.29	528.86	528.88
2C	73+46.83	19.29	529.35	529.40
2D	73+56.83	19.29	529.85	529.92
2E	73+66.83	19.29	530.35	530.44
2F	73+76.83	19.29	530.84	530.95
2G	73+86.83	19.29	531.34	531.46
2H	73+96.83	19.29	531.84	531.95
2I	74+06.83	19.29	532.34	532.43
2J	74+16.83	19.29	532.83	532.90
2K	74+26.83	19.29	533.33	533.36
☉ Brg Pier 2-N	74+35.83	19.29	533.78	533.78
☉ Pier 2	74+36.83	19.29	533.83	533.83



To determine "t": After all structural steel has been erected, elevations of the top flanges of the girders shall be taken at intervals shown above. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown above, minus slab thickness, equals the fillet heights "t" above top flange of girders.

**FILLET HEIGHTS**

**TOP OF SLAB ELEVATIONS**  
UNIT 1 (2 OF 2)  
IL. 170 F.A.P. 786 OVER  
ILLINOIS RIVER AT SENECA  
PUBLIC WATERS  
LA SALLE COUNTY, SECTION 109 BR  
STATION 79+04.42  
STRUCTURE NO. 050-0246