

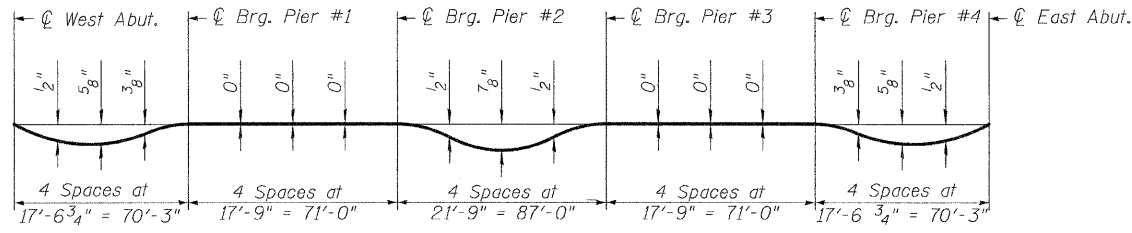
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ps:\02files\020409\02-0409.028 wo 17 bridge plans\bridge plans\014-0077_04_TopSlabElevs.dgn

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.S. 1780	27-1BR-1	CLINTON	69	29
FED. ROAD DIST. NO. 8	ILLINOIS	FED. AID PROJECT-		

SHEET NO. 4
27 SHEETS

Contract # 76961



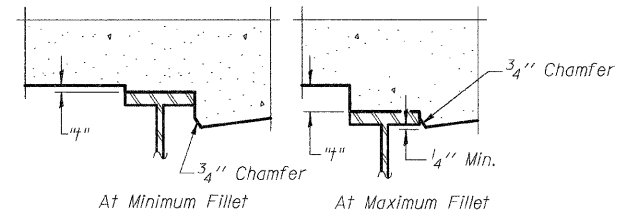
DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete only.)

Notes:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on sheets 5 thru 7 of 27.

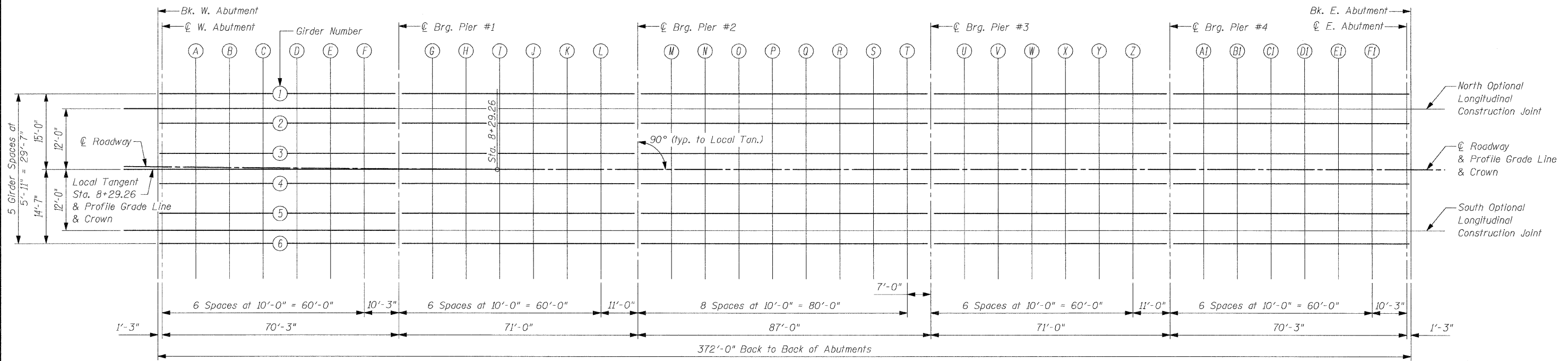
Offsets are measured in feet perpendicular from \odot Roadway. Offsets to left are negative. Offsets to right are positive.



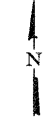
At Minimum Fillet At Maximum Fillet

To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown below. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on sheets 5 thru 7 of 27, minus slab thickness, equals the fillet heights "t" above top flange of beams.

FILLET HEIGHTS



PLAN



DESIGNED	ADL
CHECKED	WLW
DRAWN	KTH
CHECKED	WLW

TOP OF SLAB ELEVATIONS
FAS RTE 1780 (OLD US 50) OVER
SUGAR CREEK
SECTION 27-1BR-1
CLINTON COUNTY
STATION 9+14.50
STRUCTURE NUMBER 014-0077

Klingner & Assoc., P.C.