

03-08-2019 LETTING ITEM 182

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	46	1
		ILLINOIS	CONTRACT NO. 61F62	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**
FAU ROUTE 1397 (WASHINGTON STREET)
TOWN ROAD TO ARBOR AVENUE
ROADWAY RECONSTRUCTION, CURB AND SIDEWALK
SECTION: 16-00079-00-PV
PROJECT: 41ZS(517)
CITY OF WEST CHICAGO
DUPAGE COUNTY
C-91-201-16



LOCATION OF SECTION INDICATED THIS: — ■ —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED NOVEMBER 26, 2018
[Signature]
CITY OF WEST CHICAGO, DIRECTOR OF PUBLIC WORKS

PASSED 01/17 2019
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR
LIMITED REVIEW JANUARY 18 2019
[Signature]
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

PROFESSIONAL ENGINEER'S CERTIFICATION

I HEREBY CERTIFY THAT THIS SUBMISSION WAS PREPARED UNDER MY PERSONAL DIRECTION. THIS TECHNICAL SUBMISSION IS INTENDED TO BE USED AS AN INTEGRAL PART OF AND IN CONJUNCTION WITH THE PROJECT SPECIFICATIONS AND CONTRACT DOCUMENTS.

DATED THIS 26th DAY OF November, 2018

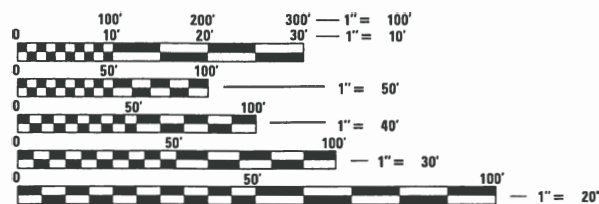
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KEVIN VANDEWOESTYNE
ILLINOIS REG. PROF. ENGINEER NO.062-061311 EXPIRATION DATE 11-30-2019



FEDERAL AID PROGRAM: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

WASHINGTON STREET

DESIGN DESIGNATION: MINOR ARTERIAL
SPEED LIMIT = 25 MPH
TRAFFIC = 13,000 ADT (2040)

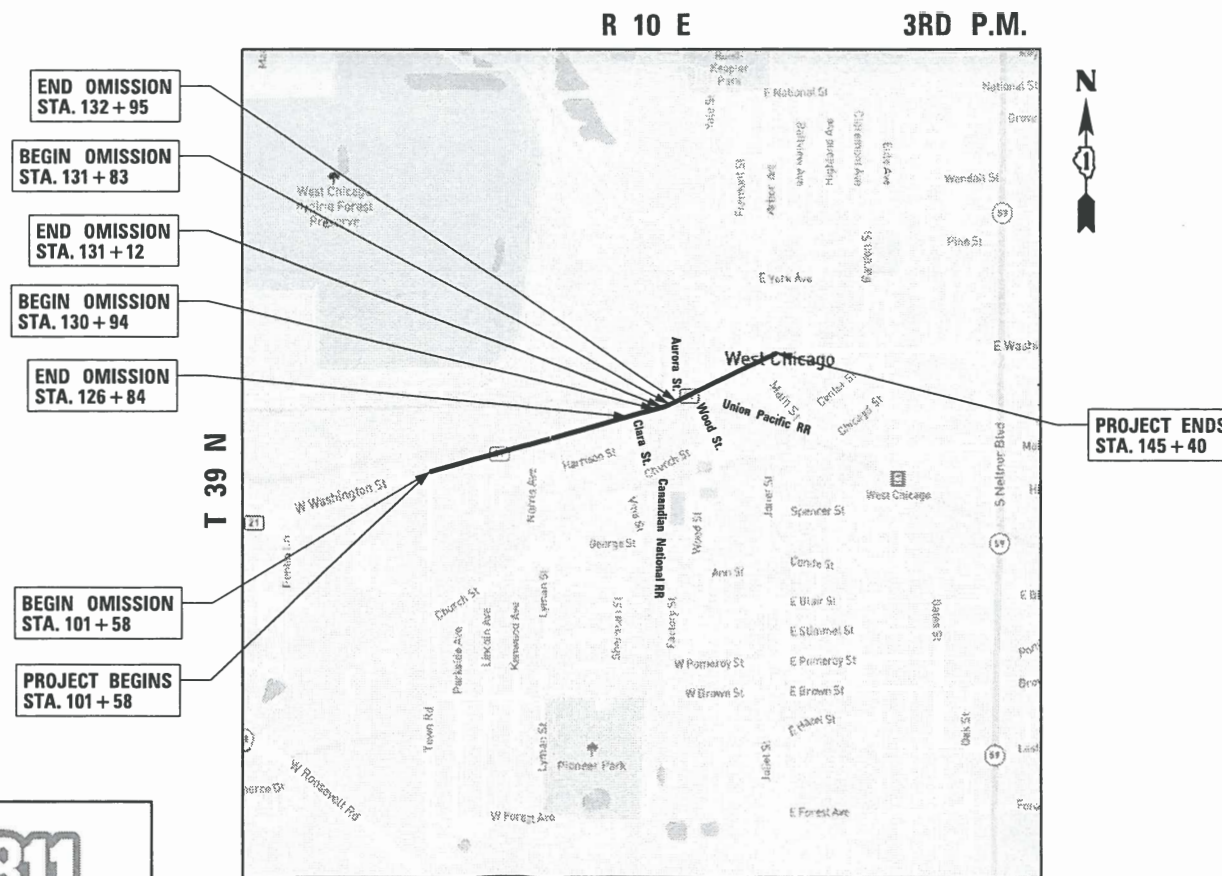
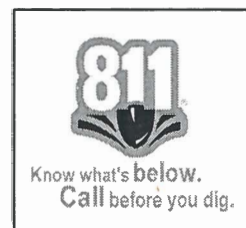


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT MANAGER: KEVIN VANDEWOESTYNE

CONTRACT NO. 61F62



MILTON TOWNSHIP

LOCATION MAP

WASHINGTON STREET GROSS LENGTH = 4,382 FT. = 0.83 MILES
WASHINGTON STREET NET LENGTH = 1,726 FT. = 0.33 MILES



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IDOT DISTRICT ONE DETAILS:

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- TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
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- TC-22 AERIAL ROAD INFORMATION SIGN
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LIST OF STATE STANDARDS:

- 000001-07 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
- 424001-11 CURB RAMPS FOR SIDEWALKS
- 442201-03 CLASS C AND D PATCHES
- 601001-05 PIPE UNDERDRAINS
- 604001-04 FRAME AND LIDS TYPE 1
- 604051-04 FRAME AND GRATE TYPE 11
- 604056-04 FRAME AND GRATE TYPE 11V
- 606001-07 CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
- 701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701101-05 OFF-ROAD OPERATIONS, MULTILANES, 15' TO 24" FROM PAVEMENT EDGE
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS- DAY ONLY
- 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS < 40 MPH
- 701501-06 URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
- 701502-09 URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
- 701606-10 URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
- 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701801-06 SIDEWALK CORNER OF CROSSWALK CLOSURE
- 701901-08 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS

GENERAL NOTES:

1. ALL IMPROVEMENTS SHALL BE IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, LATEST EDITION. ANY REFERENCE TO STANDARDS IN THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED TO BE LATEST STANDARDS OF IDOT.
2. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL COOPERATE WITH THE CITY OF WEST CHICAGO IF ANY MUNICIPAL, UTILITY IMPROVEMENTS ARE REQUIRED WITHIN THE DURATION OF THE CONTRACT.
5. QUANTITIES FOR REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL AND POROUS GRANULAR EMBANKMENT SUBGRADE HAVE BEEN PROVIDED IN THE CONTRACT BASED ON FIELD OBSERVATION OF EXISTING OBSERVATIONS OF EXISTING CONDITIONS. LOCATIONS SHOWN IN THE PLANS MAY DIFFER AND WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. IF PAVEMENT PATCHING IS NOT REQUIRED THE QUANTITY WILL BE DEDUCTED PER THE TERMS OF ARTICLE 104.02.
6. THE CONTRACTOR SHALL MAINTAIN PROPER DRAINAGE AT ALL TIMES DURING THE COURSE OF CONSTRUCTION AND SHALL PREVENT STORM WATER FROM RUNNING INTO OR STANDING IN EXCAVATED AREAS.
7. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE BUTT JOINT AND BITUMINOUS TAPER DETAILS SHEET INCLUDED IN THE PLANS (BD-32).
8. ALL PAVEMENTS, CURB AND GUTTER, SIDEWALKS, DRIVEWAYS TO BE REMOVED SHALL BE SAWCUT PRIOR TO REMOVAL TO PREVENT DAMAGE TO ITEMS TO REMAIN.
9. THE CONTRACTOR SHALL SET AND CHECK ALL CURB FORMS AND STRING LINES PRIOR TO PLACING CONCRETE TO ENSURE POSITIVE DRAINAGE ALONG THE ROADWAY. IMPROPERLY DRAINING CURB SHALL BE REMOVED AND REPLACED.
10. ALL STREETS AND COMMERCIAL AND PARKING LOT ENTRANCES SHALL REMAIN OPEN DURING CONSTRUCTION. WORK SHALL BE CAREFULLY PLANNED BY THE CONTRACTOR TO REDUCE DISRUPTION TO RESIDENTS, THE BUSINESSES AND THE PUBLIC SEEKING TO ACCESS THE BUSINESSES. AT LEAST ONE LANE OF TRAFFIC MUST REMAIN OPEN AT ALL TIMES.
11. PAVEMENT PATCHING SHALL BE SCHEDULED IMMEDIATELY FOLLOWING PAVEMENT REMOVAL TO REDUCE DEGRADATION OF THE EXISTING BASE.
12. THE MAXIMUM ALLOWABLE LANE DROP DIFFERENTIAL WILL BE 1-1/2"
13. THE CONTRACTOR SHALL PLACE FINAL THERMOPLASTIC PAVEMENT MARKINGS A MAXIMUM OF THREE (3) DAYS AFTER PLACEMENT OF THE FINAL BITUMINOUS SURFACE COURSE.
15. DURING AND AFTER CONSTRUCTION OPERATIONS, ANY LOOSE MATERIAL ON CITY ROADWAYS AS A RESULT OF CONTRACTOR OPERATIONS, INCLUDING BUT NOT LIMITED TO HMA SURFACE REMOVAL, BINDER COURSE AND SURFACE COURSE INSTALLATION, SHALL BE REMOVED AND DEPOSITED OFF SITE BY THE CLOSE OF EACH BUSINESS DAY.
16. THE THICKNESS OF BITUMINOUS MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESSES PERMITTED.
17. THE CONTRACTOR SHALL FURNISH WHITE, PINK, OR PURPLE MARKING PAINT IN AEROSOL CANS, FOR USE BY THE ENGINEER. THE CONTRACTOR AND SUBCONTRACTORS SHALL ONLY USE THESE SAME COLORS FOR THEIR OWN MARKINGS, THEREFORE, NOT USING J.U.L.I.E. UTILITY COLORS.
18. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS, AT LEAST 24 HOURS IN ADVANCE IF ANY RESIDENTS OR BUSINESSES WILL HAVE NO OR LIMITED DRIVEWAY ACCESS DUE TO WORK PERFORMED BY THE CONTRACTOR.
19. CONTRACTOR IS TO PLAN HIS/HER WORK SO THAT AT THE END OF EACH WORK DAY, THERE WILL BE NO OPEN HOLES IN THE PAVEMENT OR SIDEWALK AND THAT ALL BARRICADES WILL BE REMOVED FROM THE ROADWAY DURING NONWORKING HOURS, EXCEPT WHERE REQUIRED FOR PUBLIC SAFETY OR CURING OF CONCRETE.
20. ALL PROPOSED CONCRETE CURB AND GUTTER TYPES SHALL BE CONTINUOUSLY REINFORCED WITH TWO (2) NO. 4 REINFORCEMENT BARS.
21. AFTER SIDEWALK AND CURB AND GUTTER REPLACEMENT AND RESTORATION ACTIVITIES HAVE BEEN COMPLETED, THE CONTRACTOR SHALL REMOVE ALL LOOSE AND DEFECTIVE MATERIAL FROM THE SIDEWALK PAVEMENT, CURB AND GUTTER, AND PUBLIC RIGHT-OF-WAY TO THE SATISFACTION OF THE ENGINEER. THIS SHALL INCLUDE, BUT IS NOT LIMITED TO, BARRICADES, FORMS, GRAVEL, EXCESS TOP SOIL, EXCESS CONCRETE, ETC. AUXILIARY EQUIPMENT, SUCH AS BROOMS, SWEEPERS, SCRAPERS, ETC. SHALL BE PROVIDED AS NECESSARY TO PERFORM WORK.
22. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO COORDINATE WITH THE CANANDIAN NATIONAL RAILROAD AND UNION PACIFIC RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE CANANDIAN NATIONAL RAILROAD AND UNION PACIFIC RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
23. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
24. AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED, AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.
25. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT CONTRACTOR EXPENSE.
26. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAINS CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
27. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07 (b,c) OF THE SSRBC WILL NOT BE ALLOWED.

FILE NAME = 02 General Notes.dgn



USER NAME = DonN	DESIGNED -	REVISED -
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PLOT SCALE = 2.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	2
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

SP. PROV.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	20200100	EARTH EXCAVATION	CU YD	2,426
	20700220	POROUS GRANULAR EMBANKMENT	CU YD	703
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	8,521
	30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	703
	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	8,521
	40700100	BITUMINOUS MATERIALS (TACK COAT)	POUND	5,752
	40701906	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 11-1/4"	SQ YD	8,521
	42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	60
	44000100	PAVEMENT REMOVAL	SQ YD	8,521
	44000600	SIDEWALK REMOVAL	SQ FT	2,857
	550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	69
	55100500	STORM SEWER REMOVAL 12"	FOOT	40
	60108204	PIPE UNDERDRAINS TYPE 2, 4"	FOOT	47
	60207905	CATCH BASIN, TYPE C, TYPE 11 FRAME AND GRATE	EACH	2
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	1
	60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	29
	60400810	FRAMES, TYPE 11	EACH	3
	60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1
	60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	37
	67100100	MOBILIZATION	L SUM	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,279
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	141
	70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	526
	70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	3,466
	70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1,039
	70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	91
	70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	278
	70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3,197
	70300570	PAVEMENT MARKING TAPE, TYPE III 24"	FOOT	400
*	72000100	SIGN PANEL- TYPE 1	SQ FT	36

SP. PROV.	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
*	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	36
*	73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	4
*	78000100	THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	526
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3,466
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,039
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	91
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	278
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
*	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMINGS	EACH	1
CS	X0326144	TACTILE/DETECTABLE WARNING SURFACE	SQ FT	204
CS	X0327611	REMOVE AND REINSTALL BRICK PAVER	SQ FT	100
CS	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	5
CS	X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	5
CS	X4023000	TEMPORARY ACCESS (ROAD)	EACH	6
CS	X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	2,857
	X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	12
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1
	70107025	CHANGEABLE MESSAGE SIGN	CAL DAY	140
	X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	1,855
CS	XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	40
CS	XX003829	DRIVEWAY APPROACH REMOVAL & REPLACEMENT	SQ YD	40
CS	XX006425	RESTORATION	SQ YD	160
CS	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,324
	Z0013798	CONSTRUCTION LAYOUT	L SUM	1
	Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	2
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	176
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1
△	Z0076600	TRAINEES	HOUR	500
△	Z0076603	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500

CS = CONTRACT SPECIAL PROVISION (SEE SECTION III)

* SPECIALTY ITEM

△ 0042

FILE NAME = 03_S00.dgn



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 PLOT DATE = 12/21/2018

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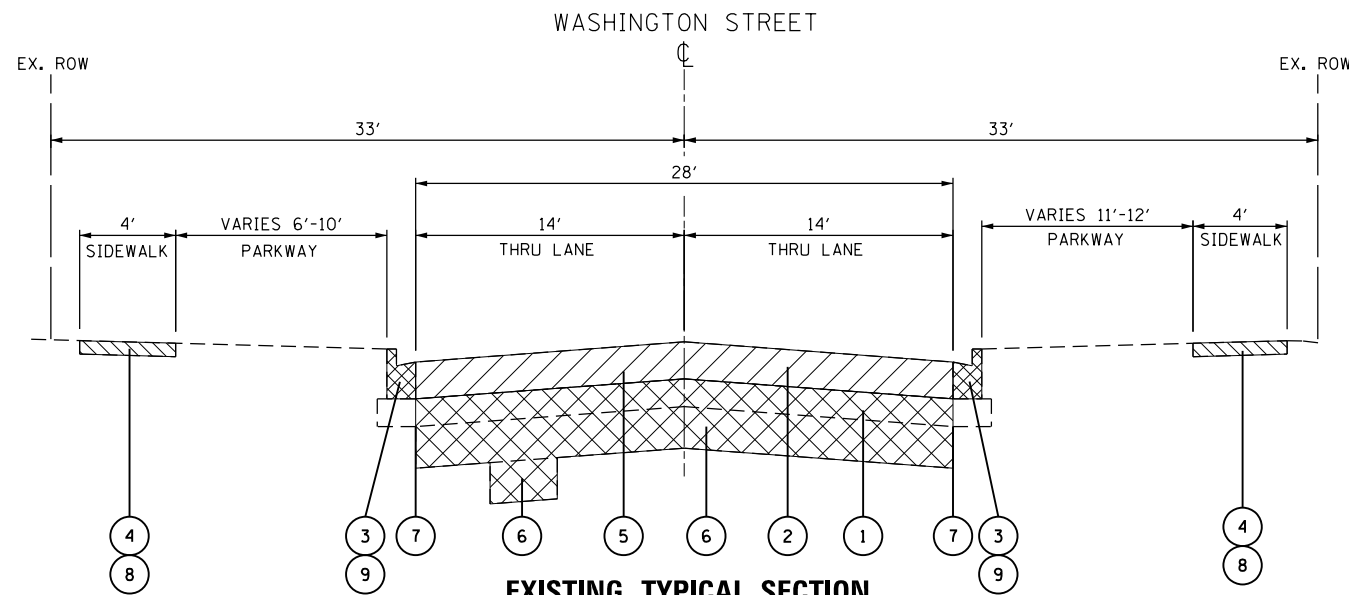
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

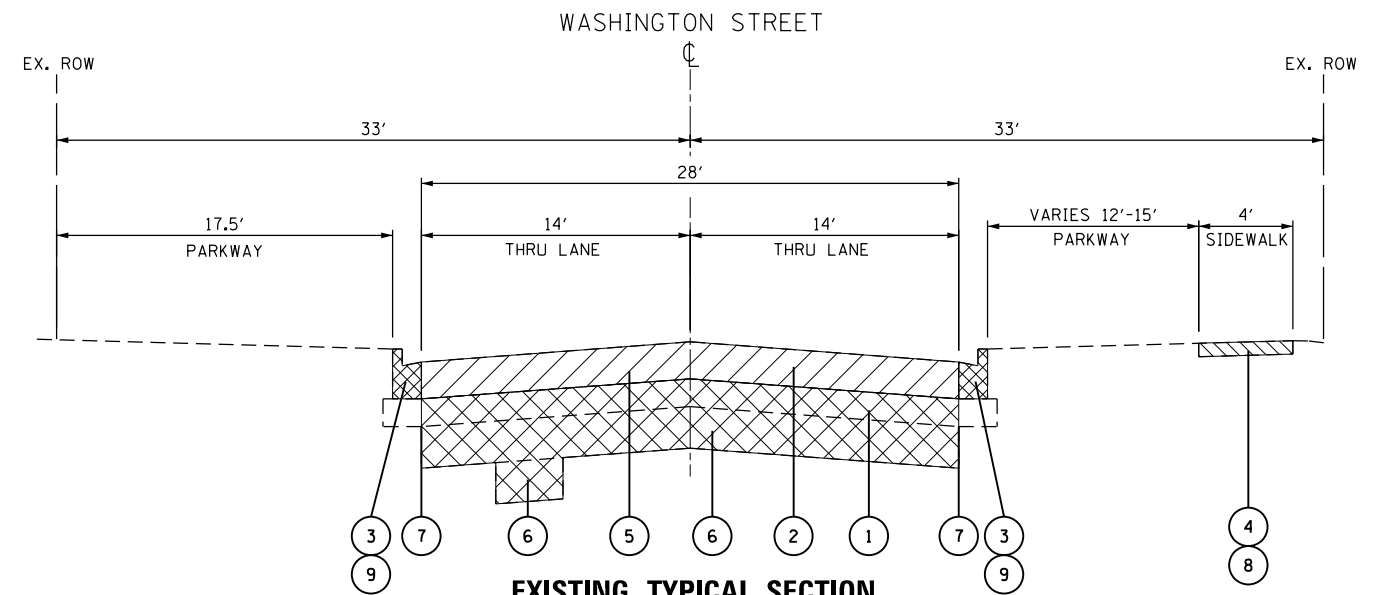
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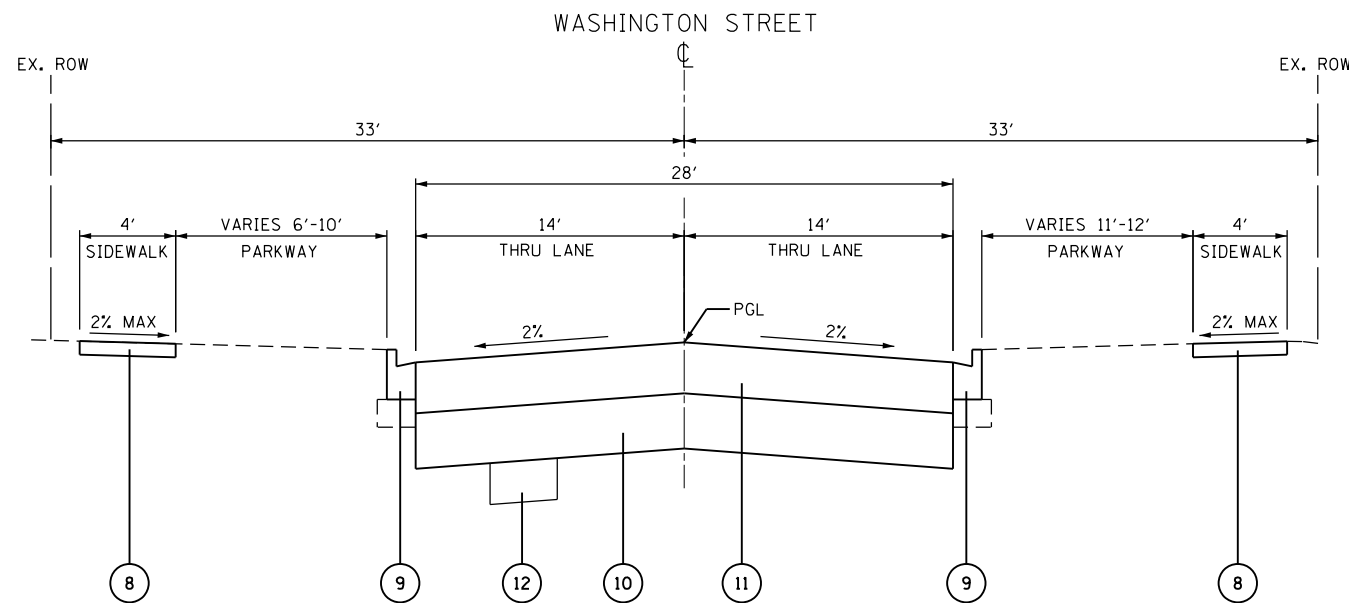
EXISTING TYPICAL SECTION

WASHINGTON STREET
STA. 126+84 TO STA. 130+64



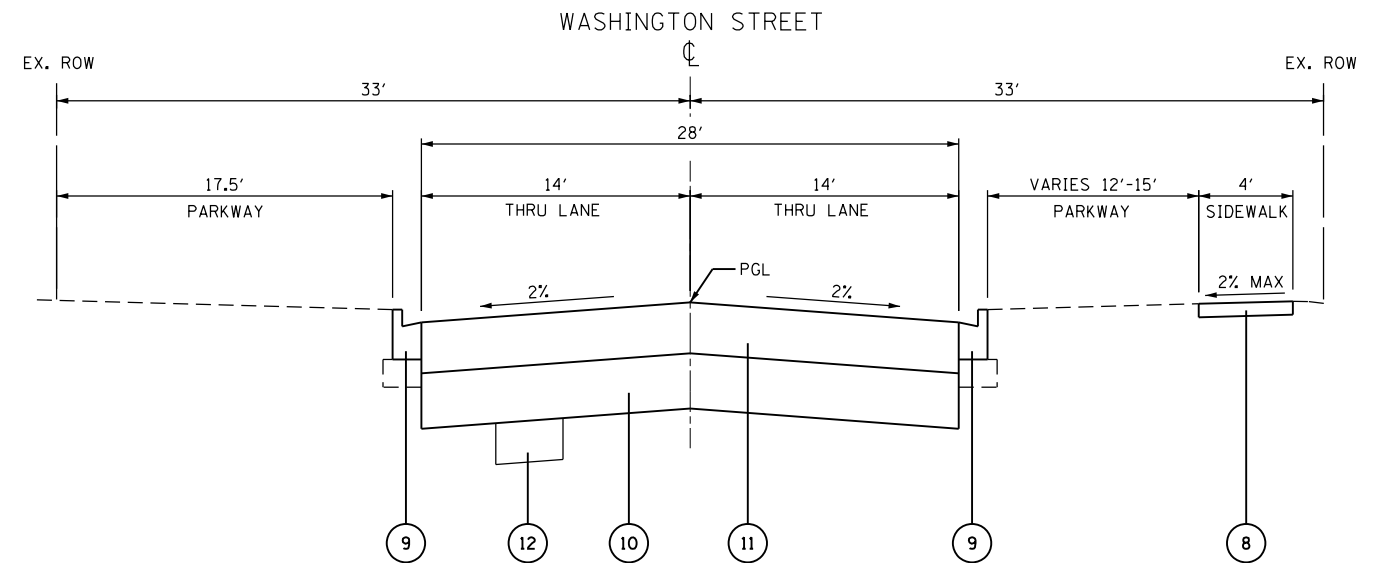
EXISTING TYPICAL SECTION

WASHINGTON STREET
STA. 130+64 TO STA. 130+94
OMISSION STA. 130+94 TO STA. 131+12
STA. 131+12 TO STA. 131+83
OMISSION STA. 131+83 TO STA. 132+95



PROPOSED TYPICAL SECTION

WASHINGTON STREET
STA. 126+84 TO STA. 130+64



PROPOSED TYPICAL SECTION

WASHINGTON STREET
STA. 130+64 TO STA. 130+94
OMISSION STA. 130+94 TO STA. 131+12
STA. 131+12 TO STA. 131+83
OMISSION STA. 131+83 TO STA. 132+95

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N 70 (IL 9.5 mm)	4% @ 70 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N 70	4% @ 70 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/ SQ YD/ IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL "PG 64-22 UNLESS MODIFIED BY DISTRICT ONE SPECIALS PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ON SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

LEGEND

- | | |
|---|---|
| ① EX. AGGREGATE SUB-GRADE, 4" | ⑧ INTERMITTENT PCC SIDEWALK REPLACEMENT |
| ② EX. P.C.C. PAVEMENT, 9" | ⑨ INTERMITTENT CURB AND GUTTER REMOVAL AND REPLACEMENT |
| ③ EX. CURB AND GUTTER, TYPE B-6.12 | ⑩ PR. AGGREGATE SUBGRADE IMPROVEMENT, 12" |
| ④ EX. SIDEWALK | ⑪ PR. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 11-1/4"
- HOT-MIX ASPHALT SURFACE COURSE, N70, 2"
- HOT-MIX ASPHALT BINDER COURSE, N70, 9-1/4" |
| ⑤ PR. PAVEMENT REMOVAL | ⑫ PR. AGGREGATE SUBGRADE IMPROVEMENT |
| ⑥ PR. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | |
| ⑦ PR. SAW CUTS | |

NOTES:

1. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL, INTERMITTENT PCC SIDEWALK REPLACEMENT, AND INTERMITTENT CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED IN THE FIELD DURING CONSTRUCTION PER THE DIRECTION OF THE ENGINEER.

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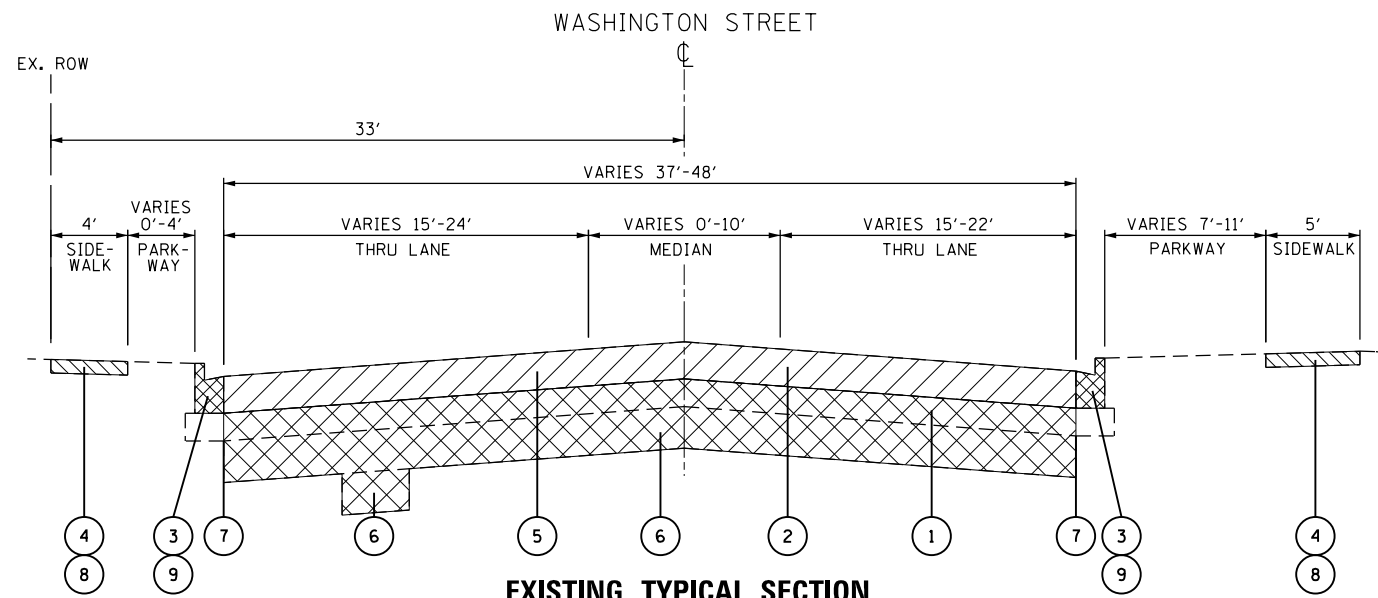
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PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
WASHINGTON STREET

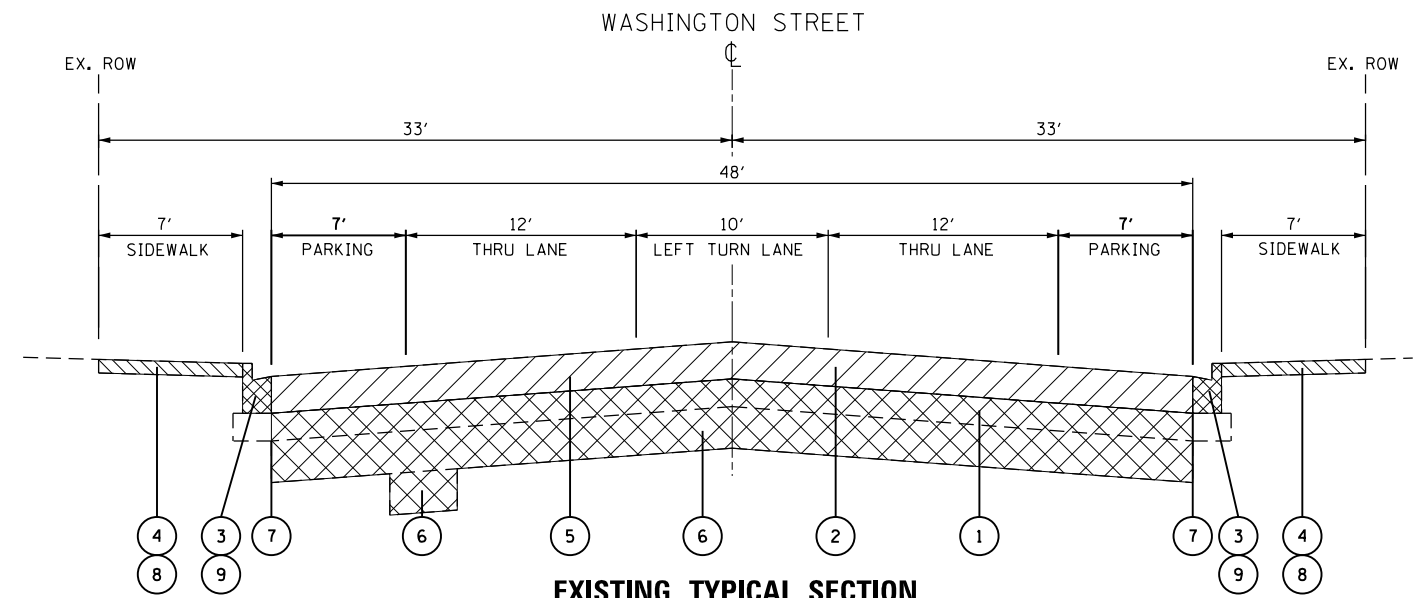
SCALE: N.T.S. SHEET 1 OF 4 SHEETS STA. TO STA.

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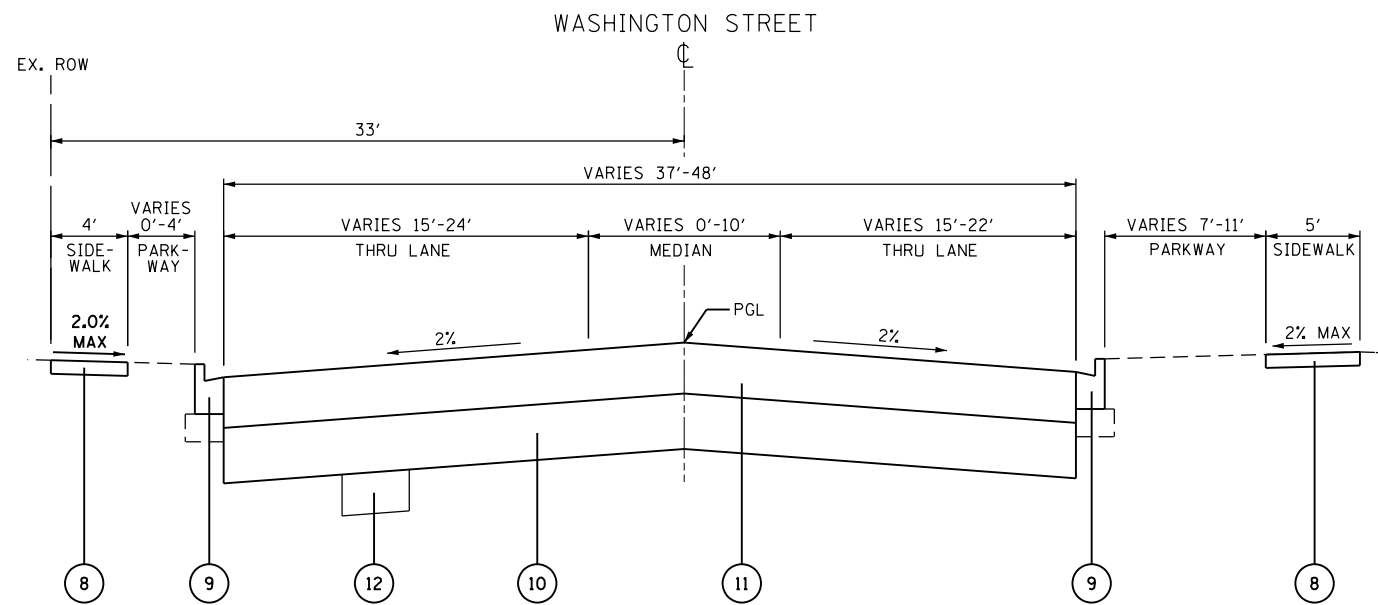
EXISTING TYPICAL SECTION

WASHINGTON STREET
STA. 132+95 TO STA. 134+50



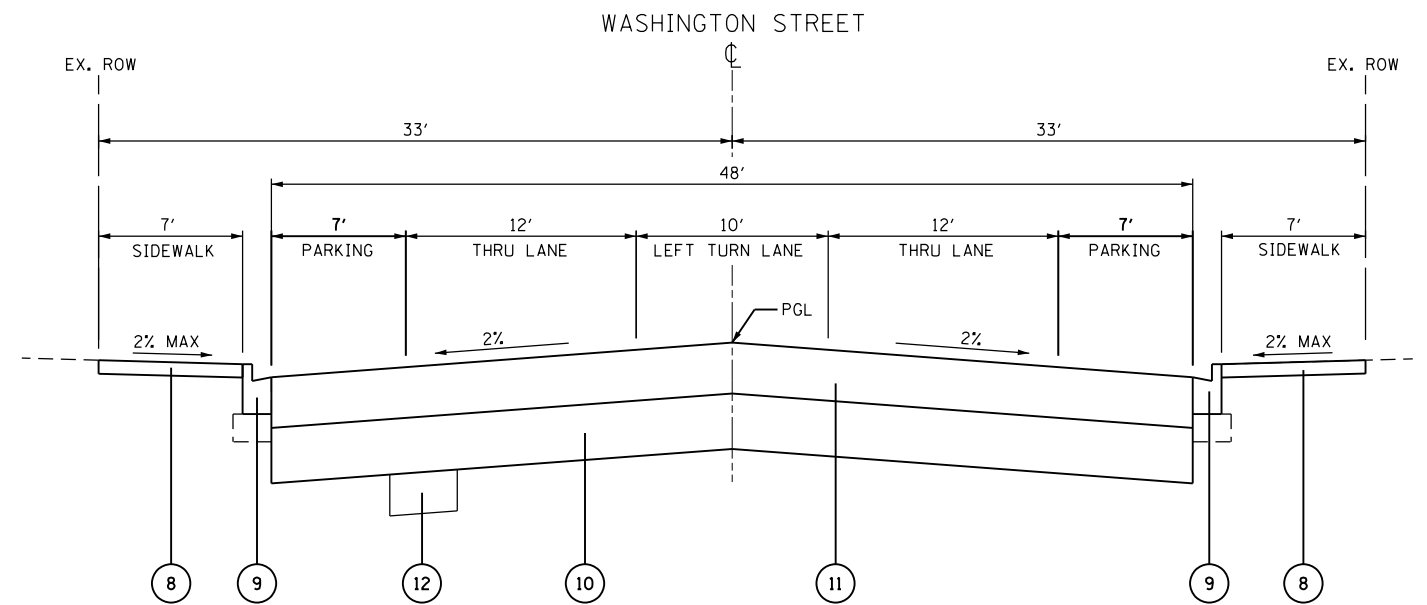
EXISTING TYPICAL SECTION

WASHINGTON STREET
STA. 134+50 TO STA. 136+85



PROPOSED TYPICAL SECTION

WASHINGTON STREET
STA. 132+95 TO STA. 134+50



PROPOSED TYPICAL SECTION

WASHINGTON STREET
STA. 134+50 TO STA. 136+85

LEGEND

- ① EX. AGGREGATE SUB-GRADE, 4"
- ② EX. P.C.C. PAVEMENT, 9"
- ③ EX. CURB AND GUTTER, TYPE B-6.12
- ④ EX. SIDEWALK
- ⑤ PR. PAVEMENT REMOVAL
- ⑥ PR. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ⑦ PR. SAW CUTS
- ⑧ INTERMITTENT PCC SIDEWALK REPLACEMENT
- ⑨ INTERMITTENT CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑩ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑪ PR. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 11-1/4"
- HOT-MIX ASPHALT SURFACE COURSE, N70, 2"
- HOT-MIX ASPHALT BINDER COURSE, N70, 9-1/4"
- ⑫ PR. AGGREGATE SUBGRADE IMPROVEMENT

NOTES:

1. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL, INTERMITTENT PCC SIDEWALK REPLACEMENT, AND INTERMITTENT CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED IN THE FIELD DURING CONSTRUCTION PER THE DIRECTION OF THE ENGINEER.

FILE NAME = 04-07 sht-typical.dgn



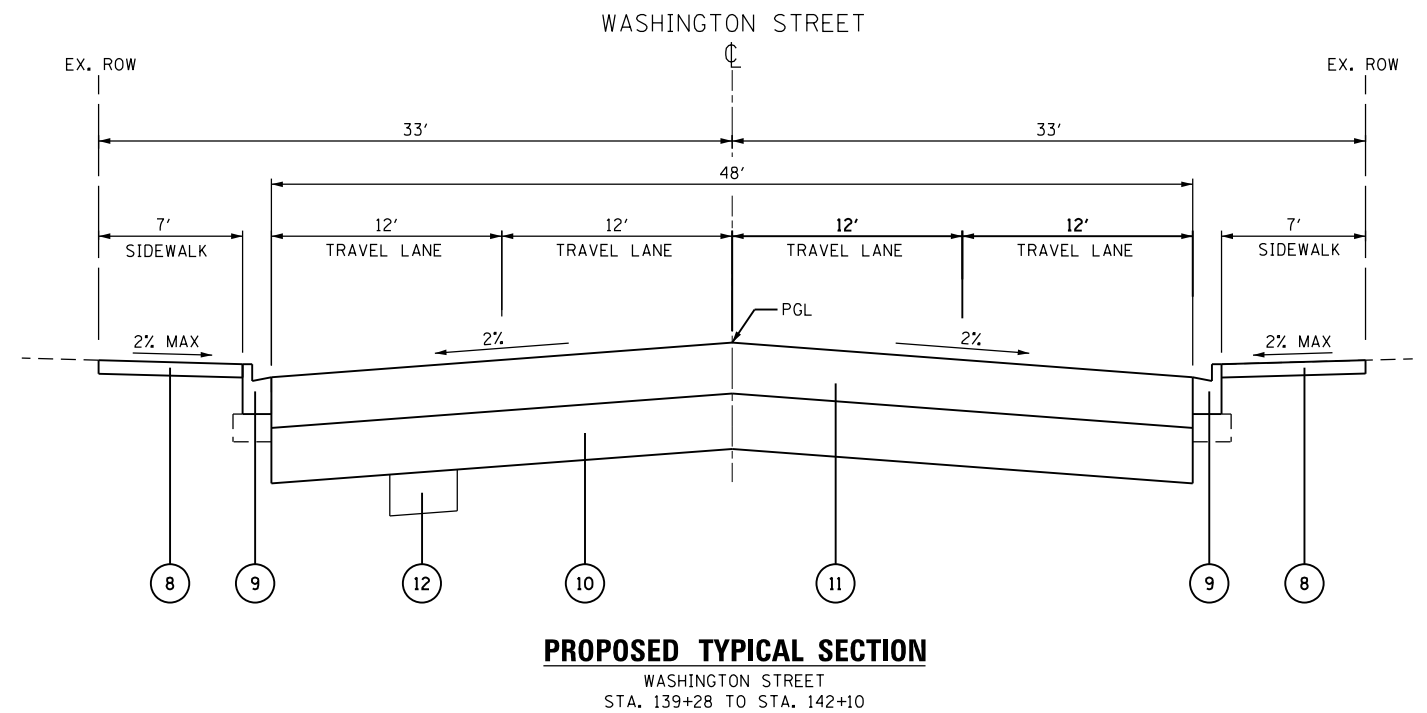
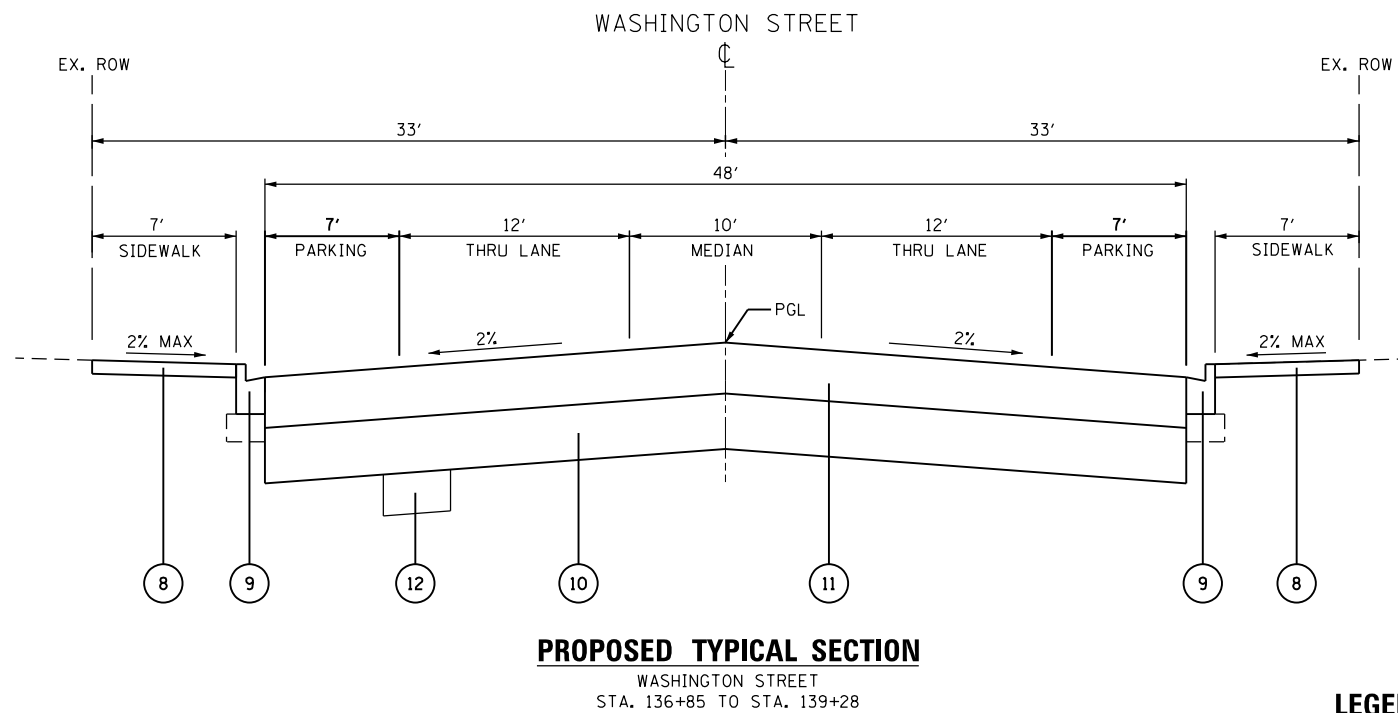
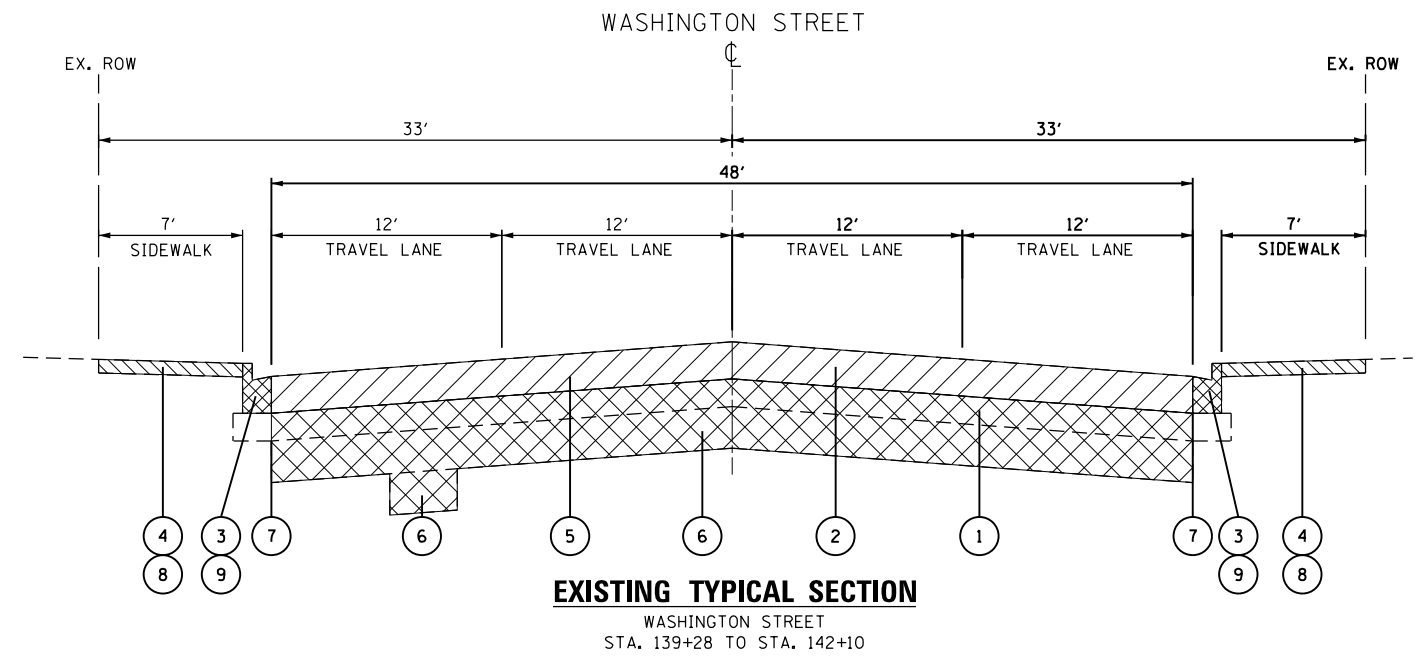
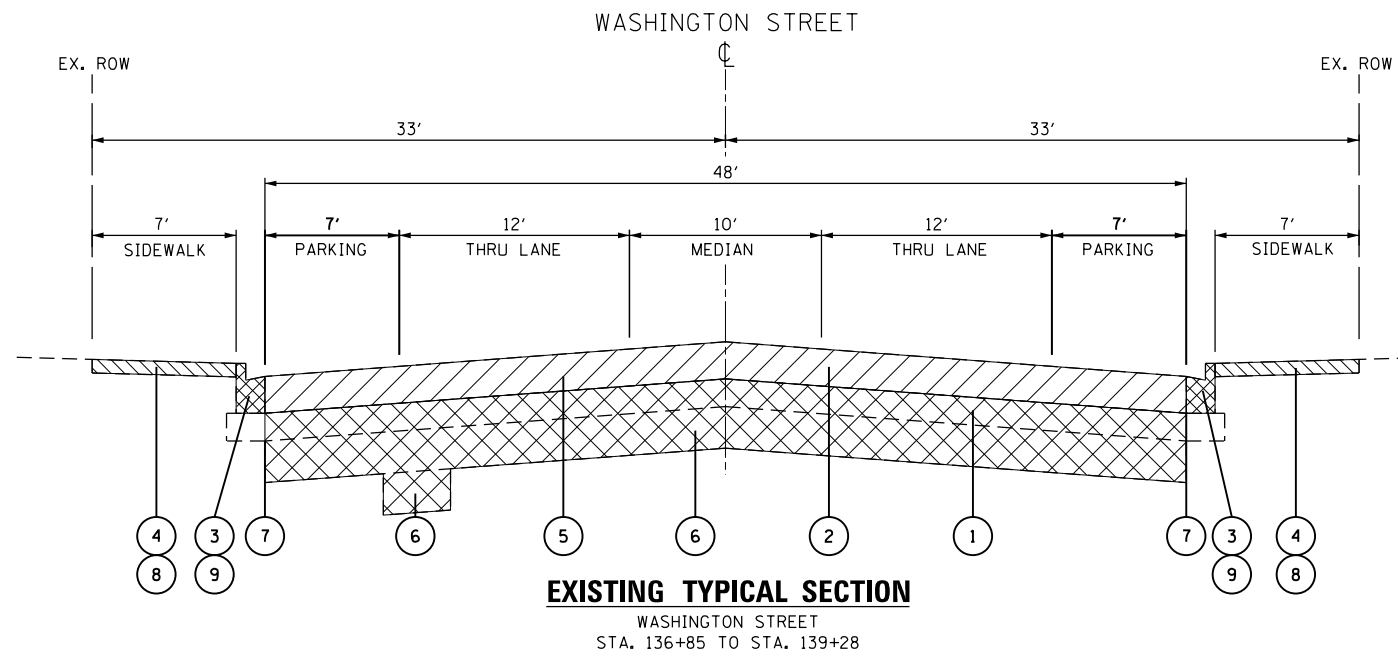
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PLOT SCALE = 10.0000' / 1"	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
WASHINGTON STREET**

SCALE: N.T.S. SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	5
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



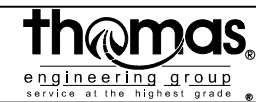
LEGEND

- ① EX. AGGREGATE SUB-GRADE, 4"
- ② EX. P.C.C. PAVEMENT, 9"
- ③ EX. CURB AND GUTTER, TYPE B-6.12
- ④ EX. SIDEWALK
- ⑤ PR. PAVEMENT REMOVAL
- ⑥ PR. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ⑦ PR. SAW CUTS
- ⑧ INTERMITTENT PCC SIDEWALK REPLACEMENT
- ⑨ INTERMITTENT CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑩ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑪ PR. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 11-1/4"
- HOT-MIX ASPHALT SURFACE COURSE, N70, 2"
- HOT-MIX ASPHALT BINDER COURSE, N70, 9-1/4"
- ⑫ PR. AGGREGATE SUBGRADE IMPROVEMENT

NOTES:

1. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL, INTERMITTENT PCC SIDEWALK REPLACEMENT, AND INTERMITTENT CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED IN THE FIELD DURING CONSTRUCTION PER THE DIRECTION OF THE ENGINEER.

FILE NAME = 04-07 sht-typical.dgn



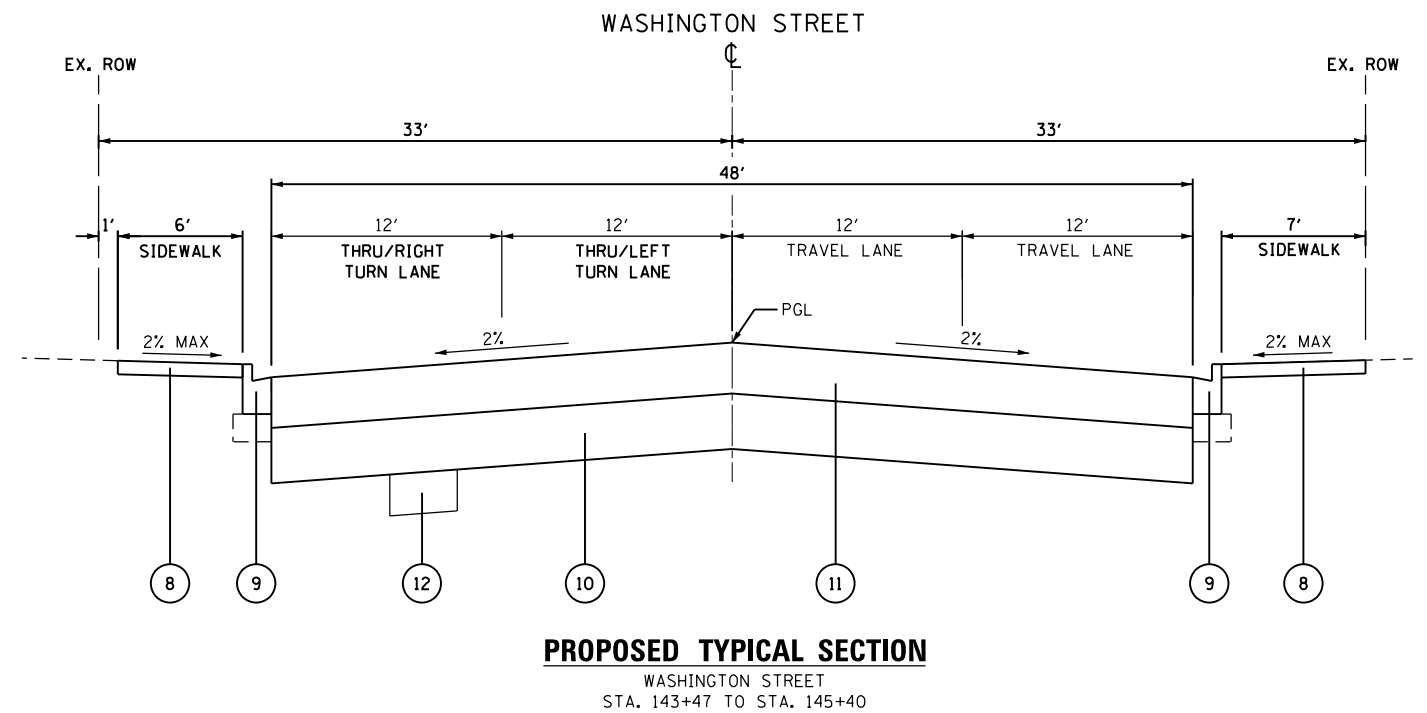
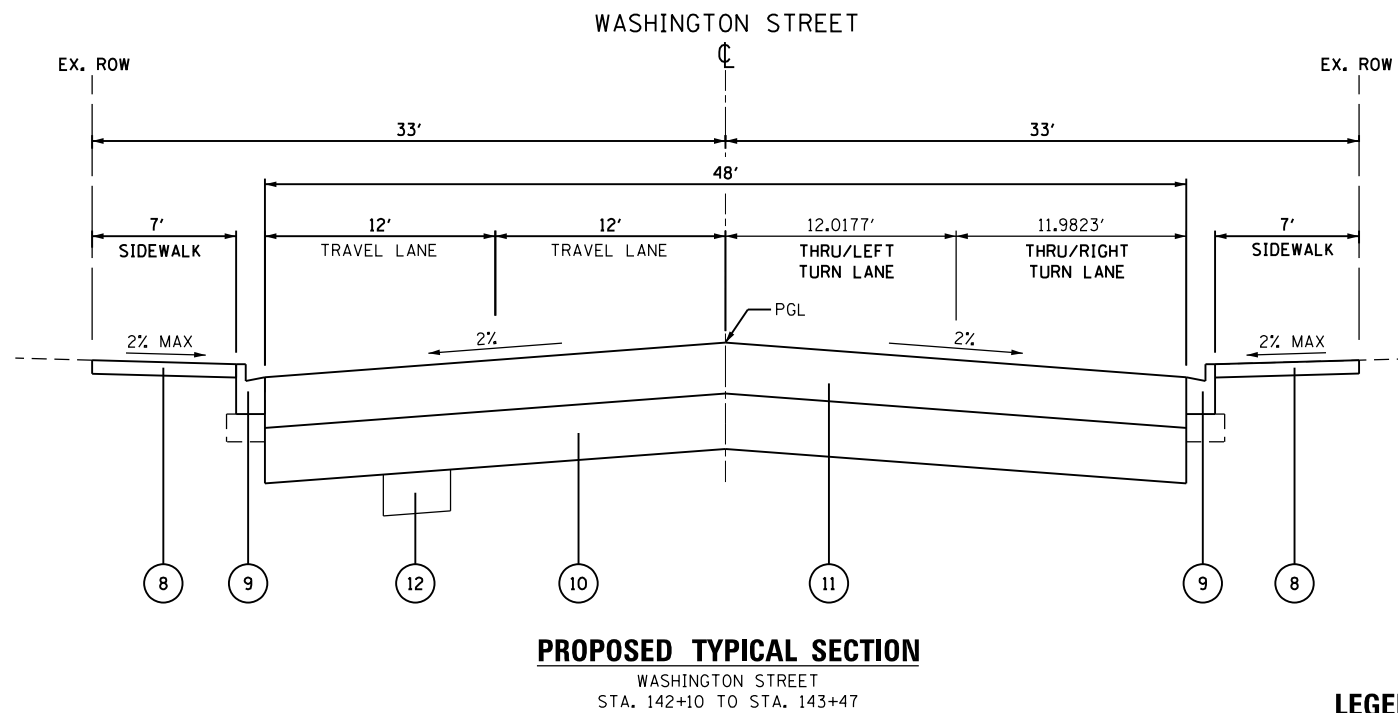
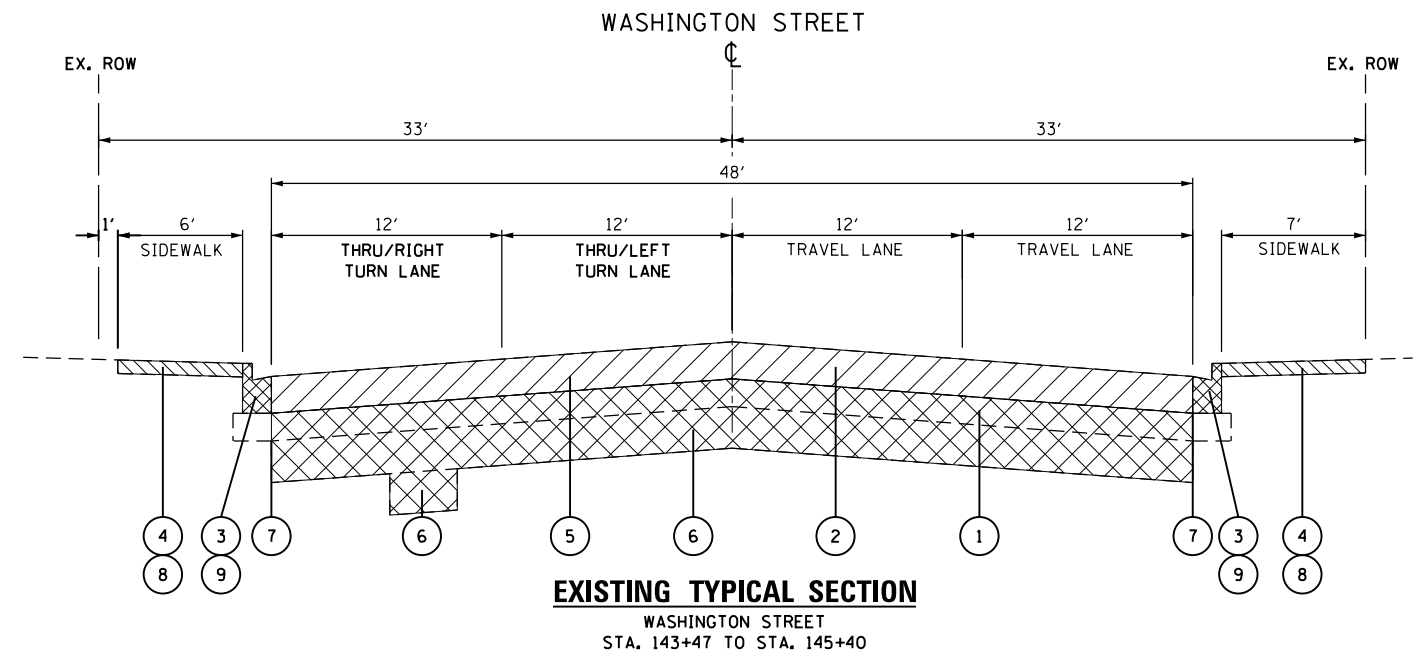
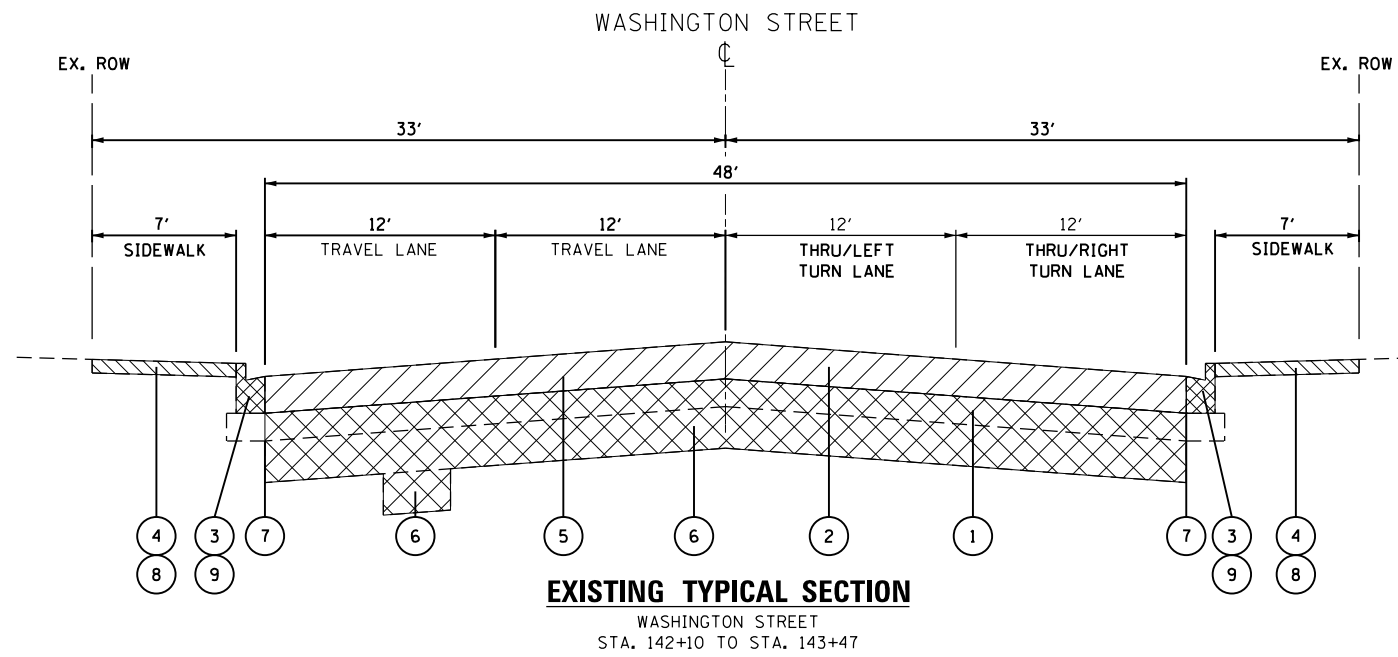
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	DRAWN -	REVISED -	
PLOT SCALE = 10.0000' / 1"	CHECKED -	REVISED -	
PLOT DATE = 12/21/2018	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
WASHINGTON STREET**

SCALE: N.T.S. SHEET 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	6
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



LEGEND

- ① EX. AGGREGATE SUB-GRADE, 4"
- ② EX. P.C.C. PAVEMENT, 9"
- ③ EX. CURB AND GUTTER, TYPE B-6.12
- ④ EX. SIDEWALK
- ⑤ PR. PAVEMENT REMOVAL
- ⑥ PR. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ⑦ PR. SAW CUTS
- ⑧ INTERMITTENT PCC SIDEWALK REPLACEMENT
- ⑨ INTERMITTENT CURB AND GUTTER REMOVAL AND REPLACEMENT
- ⑩ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑪ PR. HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 11-1/4"
- HOT-MIX ASPHALT SURFACE COURSE, N70, 2"
- HOT-MIX ASPHALT BINDER COURSE, N70, 9-1/4"
- ⑫ PR. AGGREGATE SUBGRADE IMPROVEMENT

NOTES:

1. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL, INTERMITTENT PCC SIDEWALK REPLACEMENT, AND INTERMITTENT CURB AND GUTTER REPLACEMENT SHALL BE DETERMINED IN THE FIELD DURING CONSTRUCTION PER THE DIRECTION OF THE ENGINEER.

FILE NAME = 04-07 sht-typical.dgn



USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
WASHINGTON STREET

SCALE: N.T.S. SHEET 4 OF 4 SHEETS STA. TO STA.

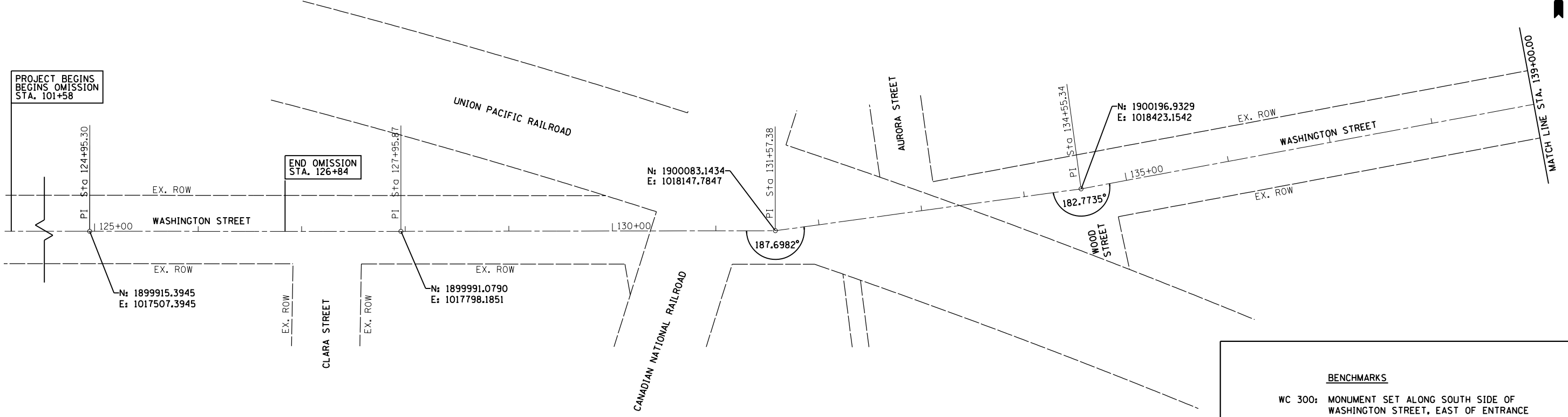
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	7
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



PROJECT BEGINS
BEGINS OMISSION
STA. 101+58

END OMISSION
STA. 126+84

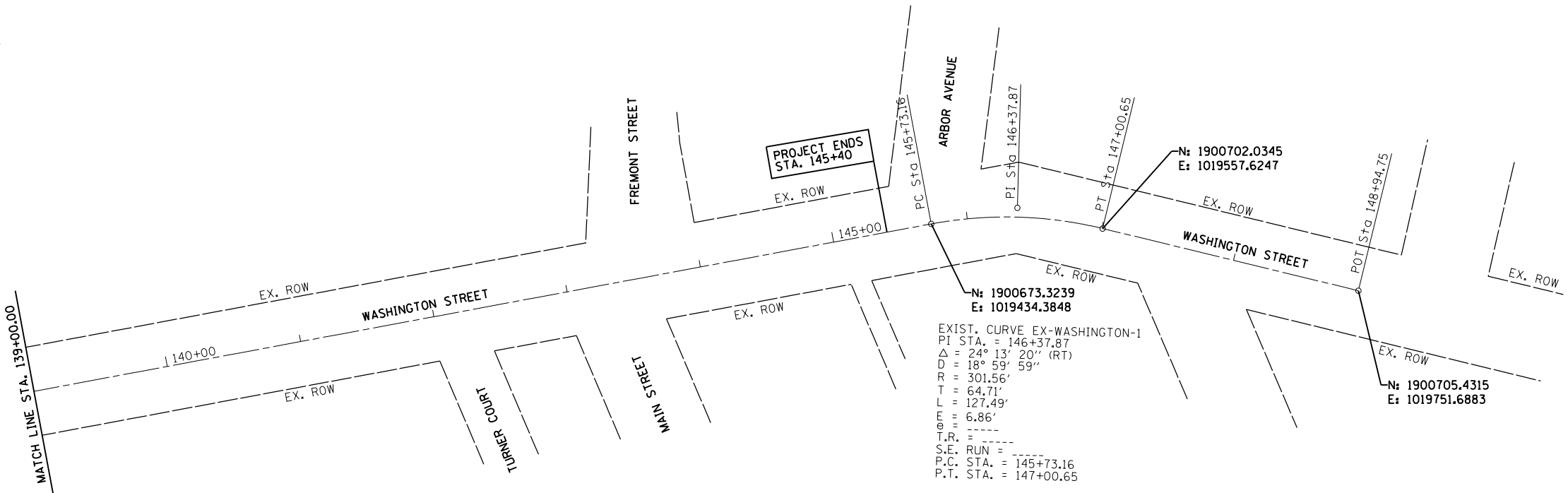
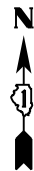
PROJECT ENDS
STA. 145+40



BENCHMARKS

WC 300: MONUMENT SET ALONG SOUTH SIDE OF WASHINGTON STREET, EAST OF ENTRANCE OF EGG YOLK CAFE ON IL ROUTE 59
ELEV: 801.58

WC 290: MONUMENT SET @ SE CORNER OF WASHINGTON STREET AND NORRIS STREET
ELEV: 760.46



FILE NAME = 08 Align_Ties.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT AND BENCHMARKS

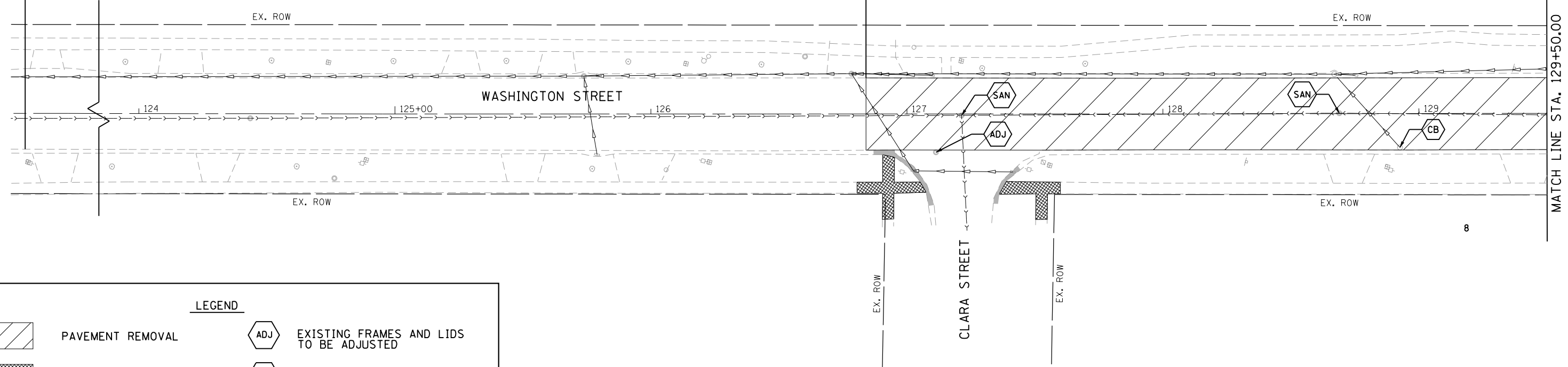
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	8
CONTRACT NO. 61F62			ILLINOIS FED. AID PROJECT	



PROJECT BEGINS,
BEGIN OMISSION
STA. 101+58

END OMISSION
STA. 126+84

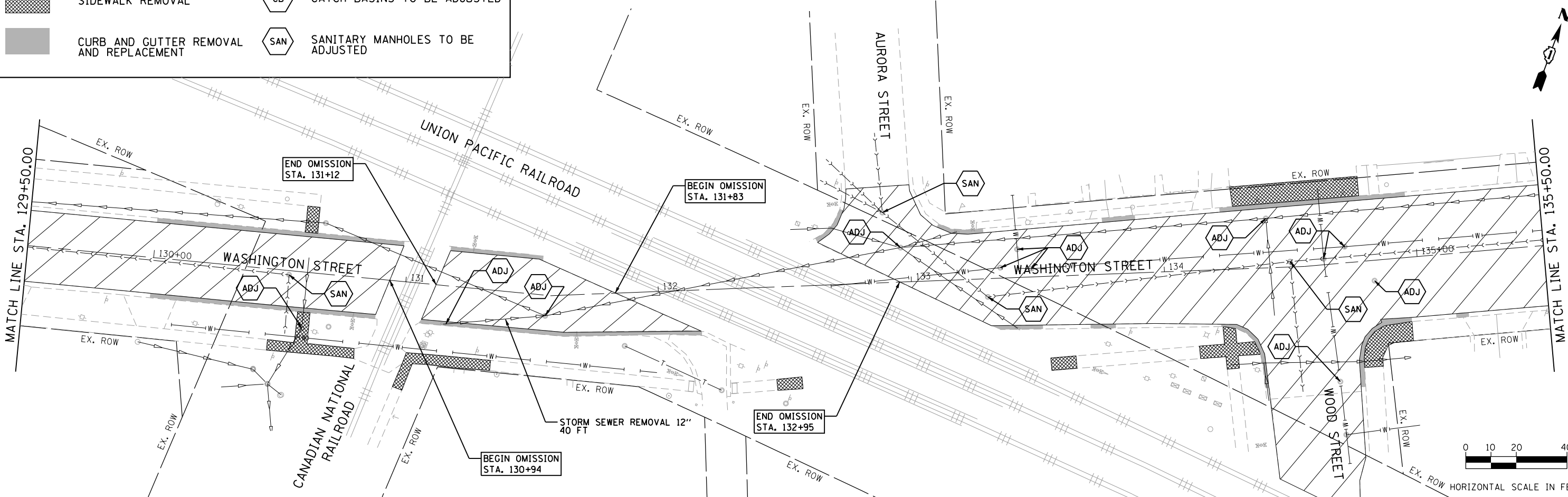


MATCH LINE STA. 129+50.00

8

LEGEND

- PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- CURB AND GUTTER REMOVAL AND REPLACEMENT
- EXISTING FRAMES AND LIDS TO BE ADJUSTED
- CATCH BASINS TO BE ADJUSTED
- SANITARY MANHOLES TO BE ADJUSTED



MATCH LINE STA. 129+50.00

MATCH LINE STA. 135+50.00



HORIZONTAL SCALE IN FEET

FILE NAME = 09 sht-Removal_01.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

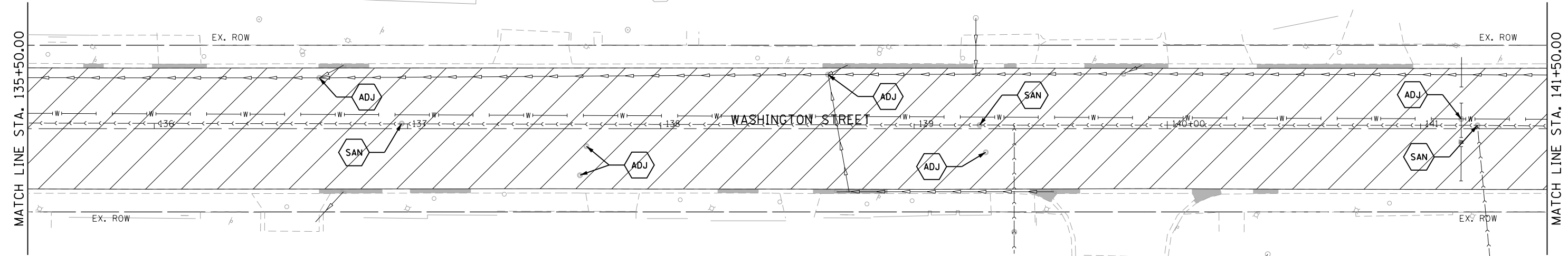
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND REMOVAL PLAN







SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

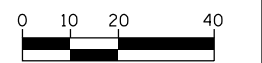
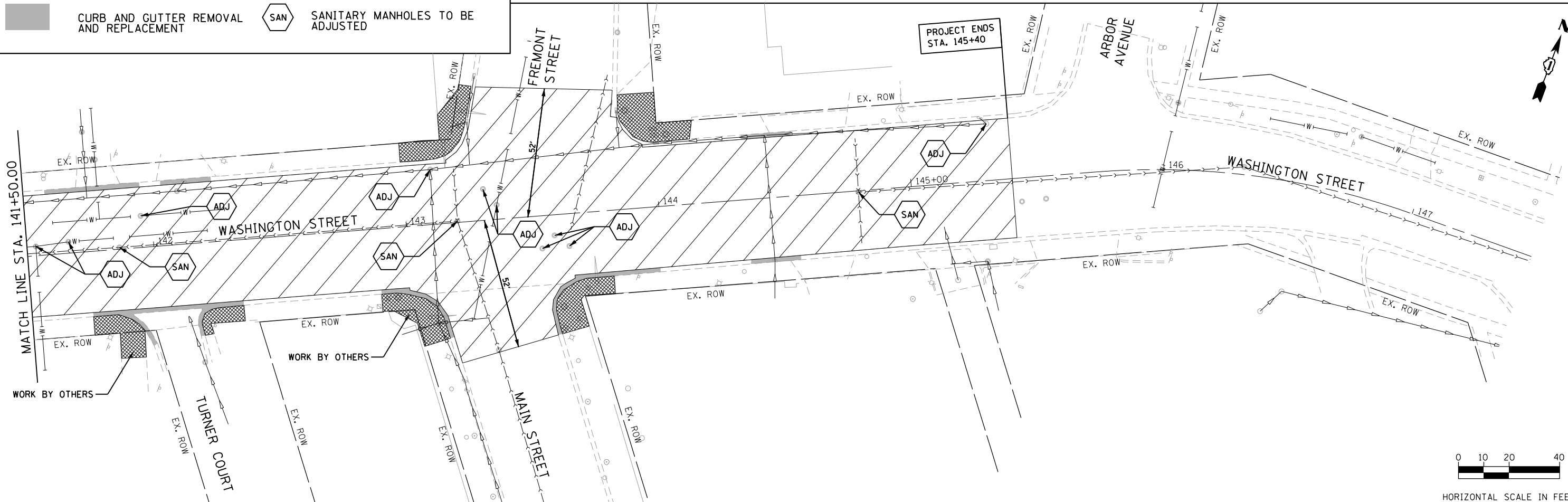
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	9
CONTRACT NO. 61F62				

ILLINOIS FED. AID PROJECT



LEGEND

	PAVEMENT REMOVAL		EXISTING FRAMES AND LIDS TO BE ADJUSTED
	SIDEWALK REMOVAL		CATCH BASINS TO BE ADJUSTED
	CURB AND GUTTER REMOVAL AND REPLACEMENT		SANITARY MANHOLES TO BE ADJUSTED



HORIZONTAL SCALE IN FEET

FILE NAME = 10 sht-Removal_02.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND REMOVAL PLANS

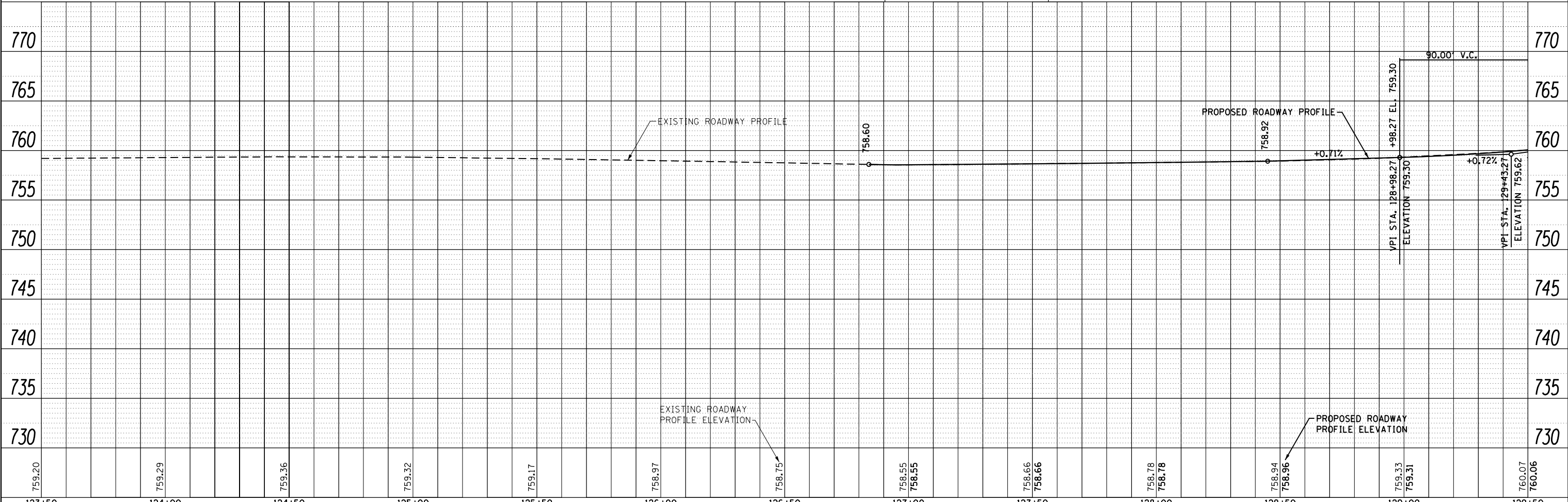
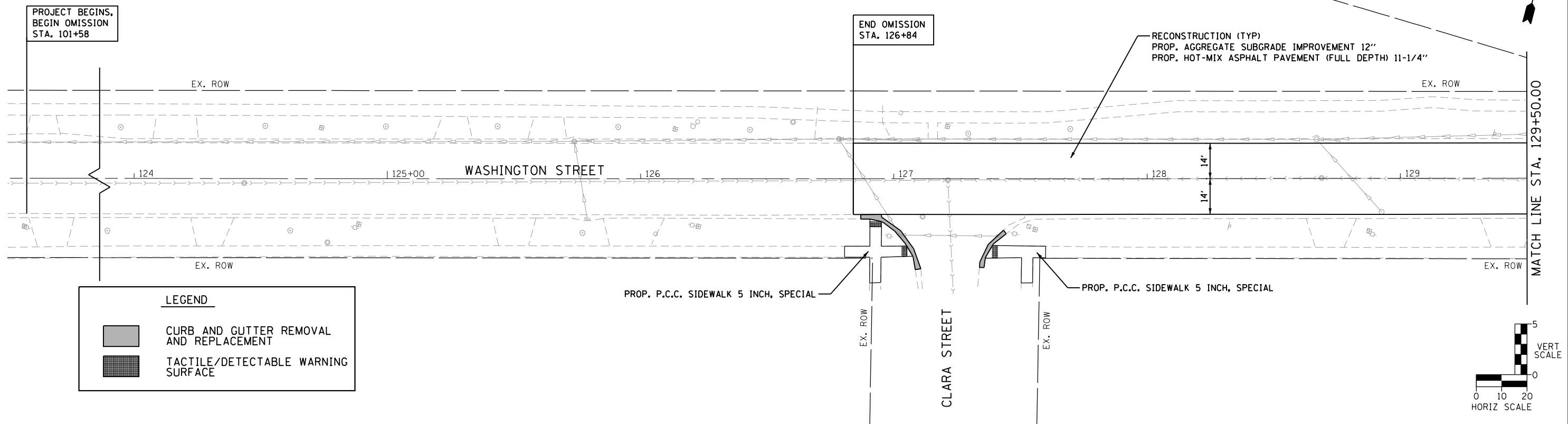
SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	10
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	CHECKED		
	FILE NAME		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE		
	NOTATIONS CHECKED		
	NO.		

FILE NAME = 11_mvt_eplan_p_r_01.dgn



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123+50	124+00	124+50	125+00	125+50	126+00	126+50	127+00	127+50	128+00	128+50	129+00	129+50						



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	DRAWN -	REVISED -
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PLOT DATE = 12/21/2018	DATE -	REVISED -

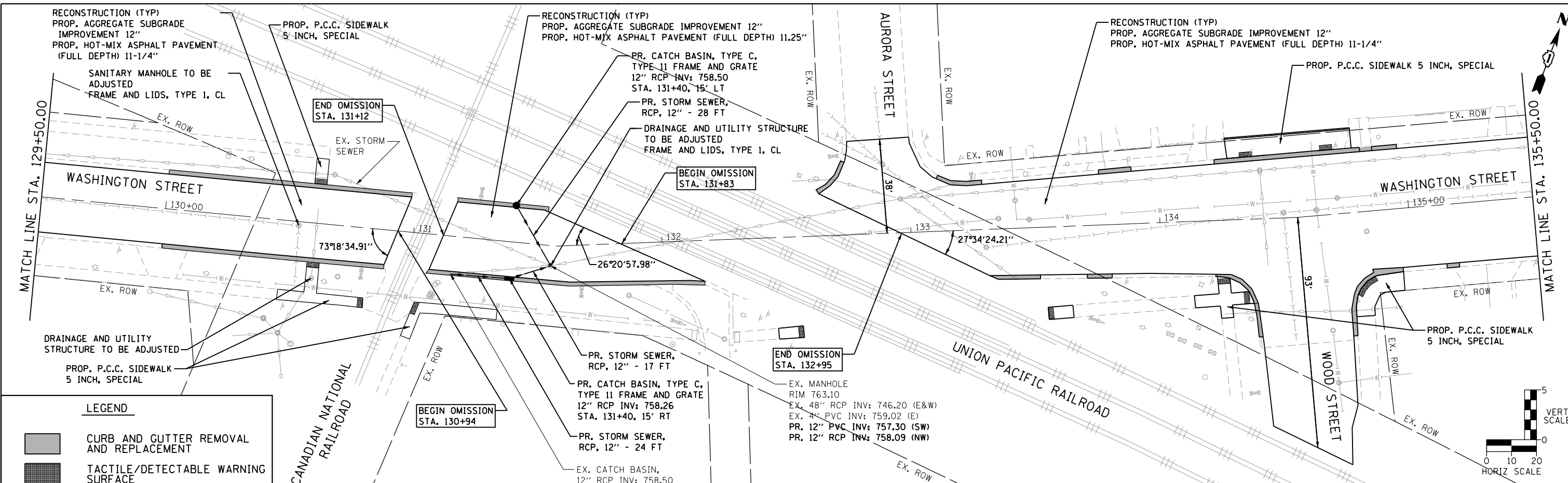
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
WASHINGTON STREET
SCALE: 1"=20' SHEET 1 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV		44	11
CONTRACT NO. 61F62			ILLINOIS FED. AID PROJECT	

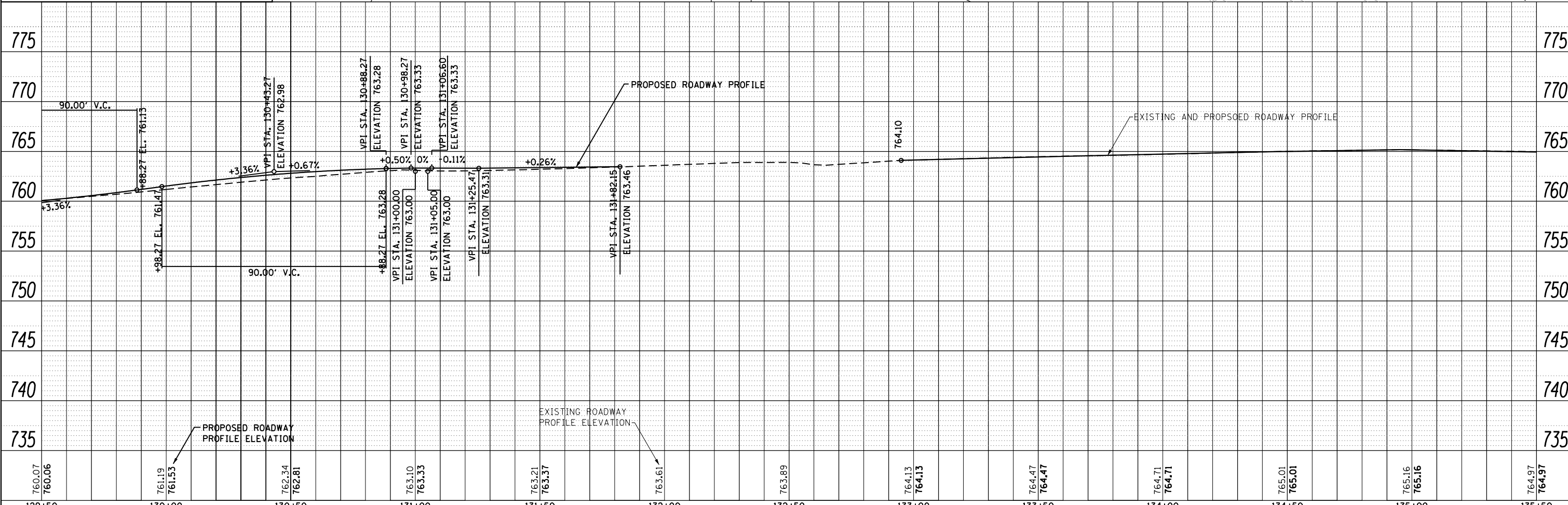
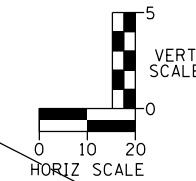
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	PLOTTED	
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	DESIGNED	
	FILE NAME	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES CHECKED	
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LEGEND

- CURB AND GUTTER REMOVAL AND REPLACEMENT
- TACTILE/DETECTABLE WARNING SURFACE



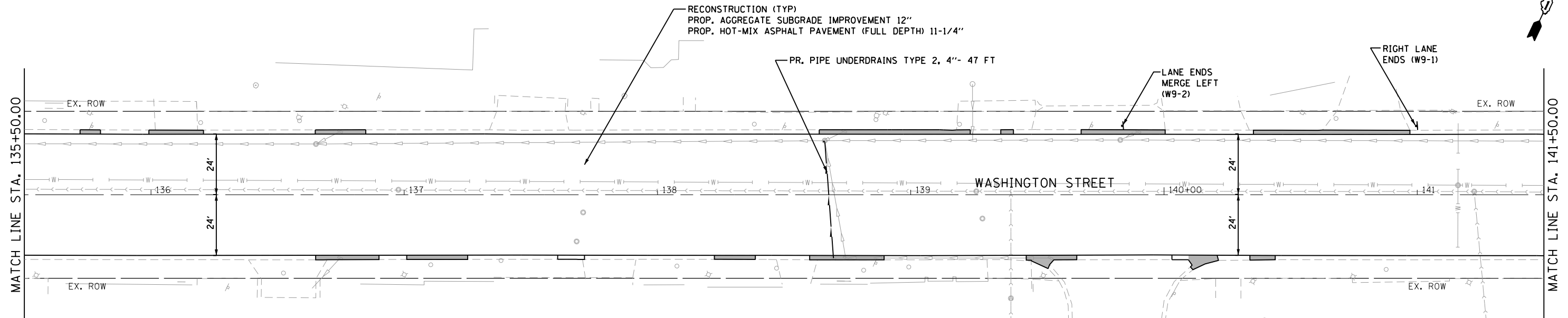
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	DESIGNED -	REVISER -	1397			16-00079-00-PV	ILLINOIS	44	12	
PLOT SCALE = 48.0000' / in.	CHECKED -	REVISER -		SCALE: 1"=20'	SHEET 2 OF 4 SHEETS	STA.	TO STA.	CONTRACT NO. 61F62		
PLOT DATE = 12/21/2018	DATE -	REVISER -		ILLINOIS FED. AID PROJECT						

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTE BOOK	
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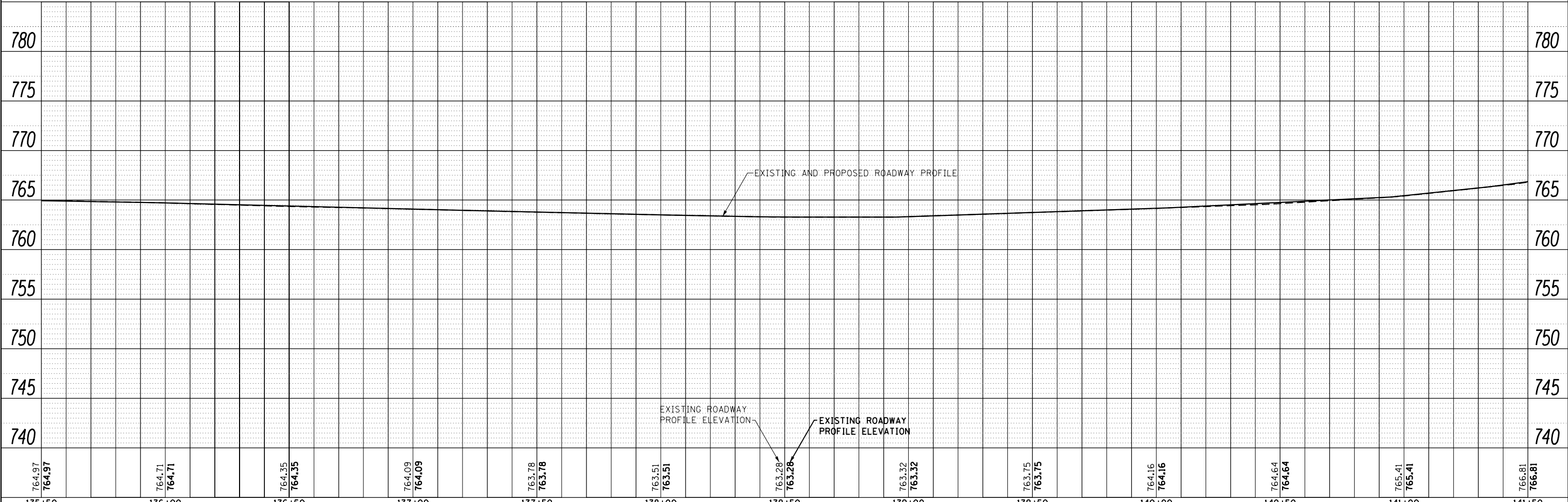
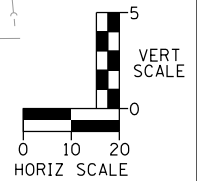
PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTE BOOK	
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FILE NAME = 13_ah+plnprf.03.dgn



LEGEND

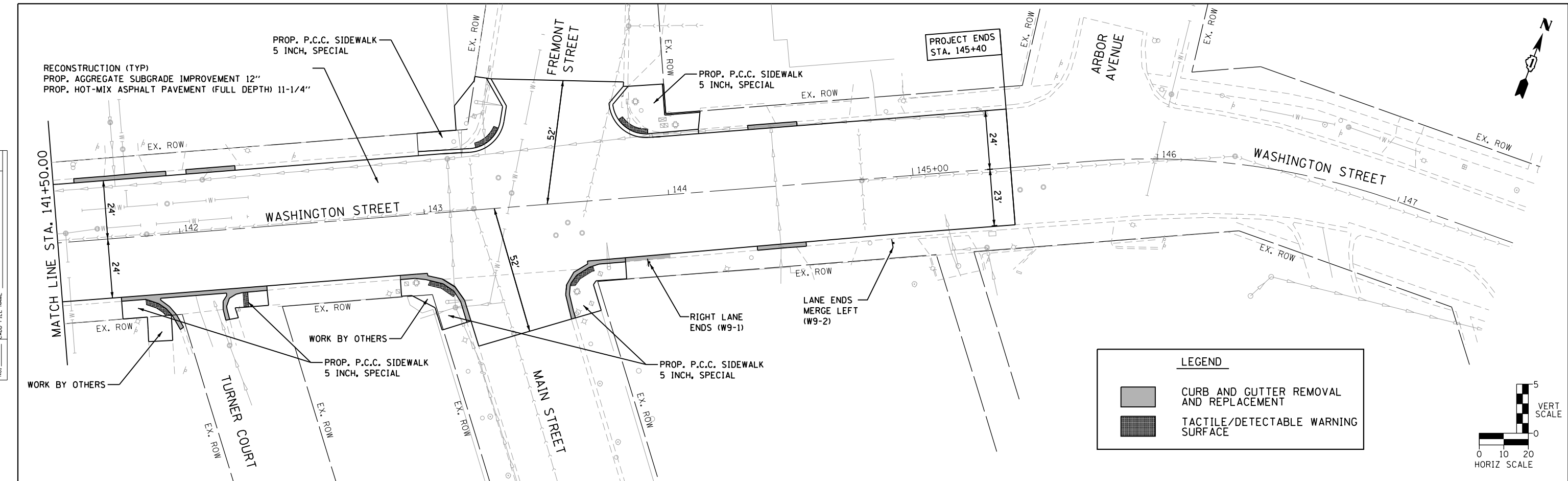
- CURB AND GUTTER REMOVAL AND REPLACEMENT
- TACTILE/DETECTABLE WARNING SURFACE



	USER NAME = DonN	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PLAN AND PROFILE WASHINGTON STREET		F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 12/21/2018	CHECKED -	REVISED -	SCALE: 1"=20'		SHEET 3 OF 4 SHEETS	STA.	CONTRACT NO. 61F62				
	DATE -	REVISED -	TO STA.		ILLINOIS FED. AID PROJECT						

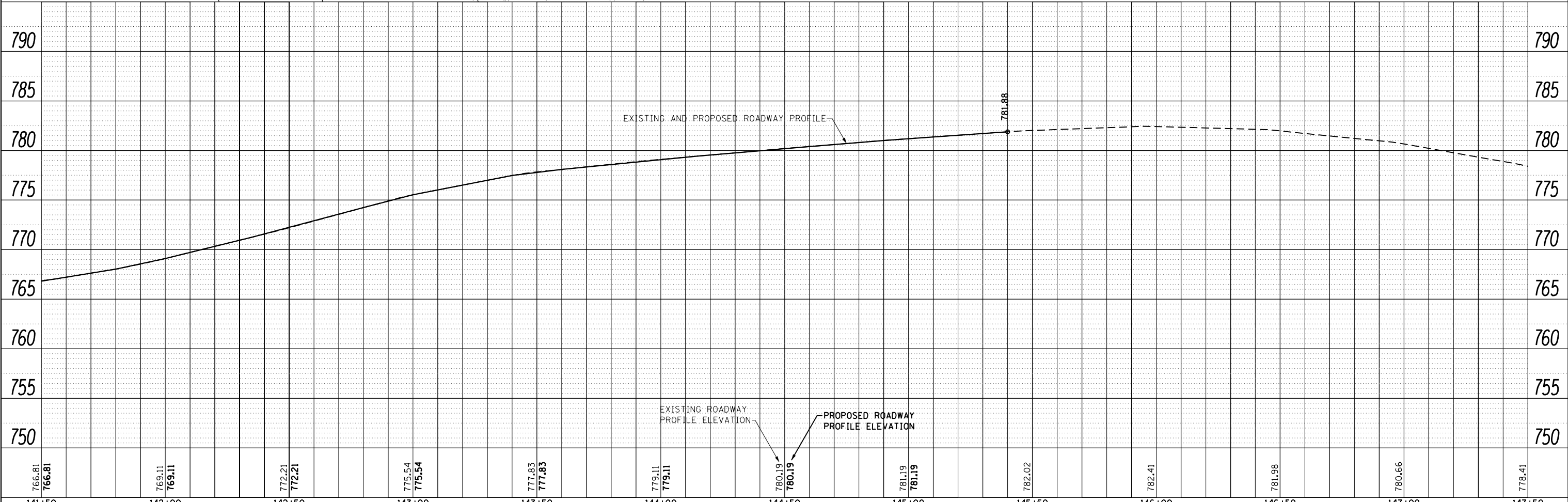
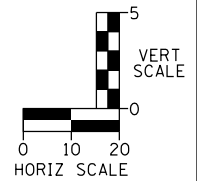
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	PLOTTED	
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	FILED	
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PROFILE	SURVEYED	DATE
	PLOTTED	
	GRADES	
	CHECKED	
	STRUCTURE	
	NOTATIONS	
	CHKD	
	NO.	



LEGEND

- CURB AND GUTTER REMOVAL AND REPLACEMENT
- TACTILE/DETECTABLE WARNING SURFACE



FILE NAME = 14_sht-plnpr-f_04.dgn



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	DRAWN -	REVISED -
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PLOT DATE = 12/21/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

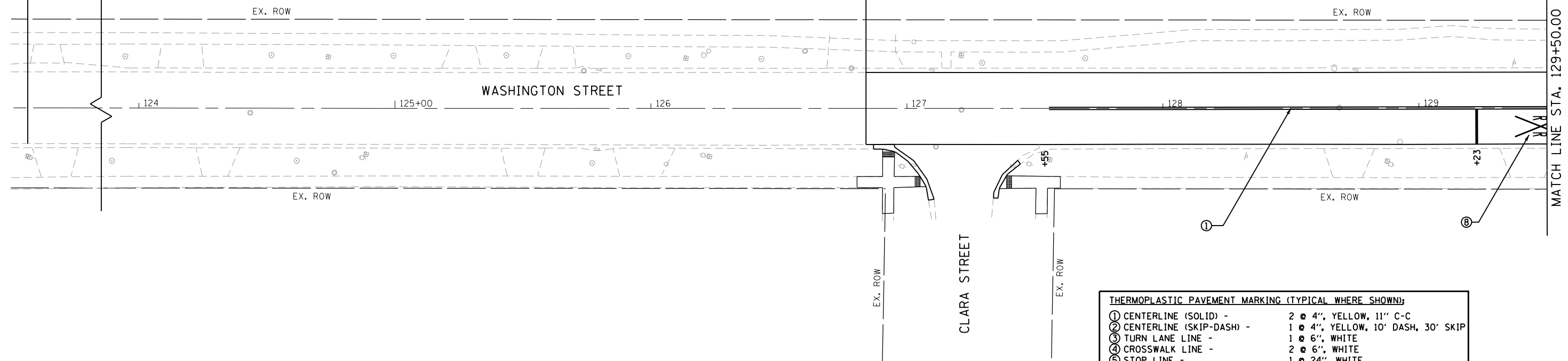
PLAN AND PROFILE	
WASHINGTON STREET	
SCALE: 1"=20'	SHEET 4 OF 4 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61F62	



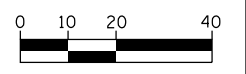
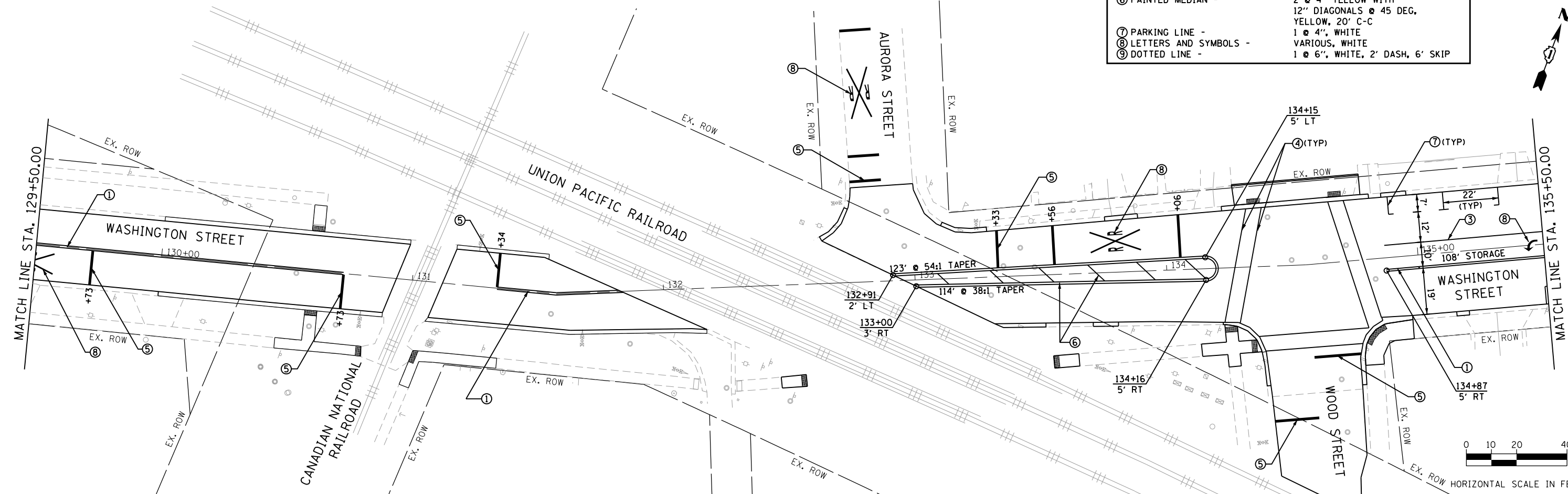
PROJECT BEGINS,
BEGIN OMISSION
STA. 101+58

END OMISSION
STA. 126+84

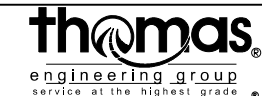


THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN):

① CENTERLINE (SOLID) -	2 @ 4", YELLOW, 11" C-C
② CENTERLINE (SKIP-DASH) -	1 @ 4", YELLOW, 10' DASH, 30' SKIP
③ TURN LANE LINE -	1 @ 6", WHITE
④ CROSSWALK LINE -	2 @ 6", WHITE
⑤ STOP LINE -	1 @ 24", WHITE
⑥ PAINTED MEDIAN -	2 @ 4" YELLOW WITH 12" DIAGONALS @ 45 DEG, YELLOW, 20' C-C
⑦ PARKING LINE -	1 @ 4", WHITE
⑧ LETTERS AND SYMBOLS -	VARIOUS, WHITE
⑨ DOTTED LINE -	1 @ 6", WHITE, 2' DASH, 6' SKIP



FILE NAME = 15 sh1t-pmk_01.dgn



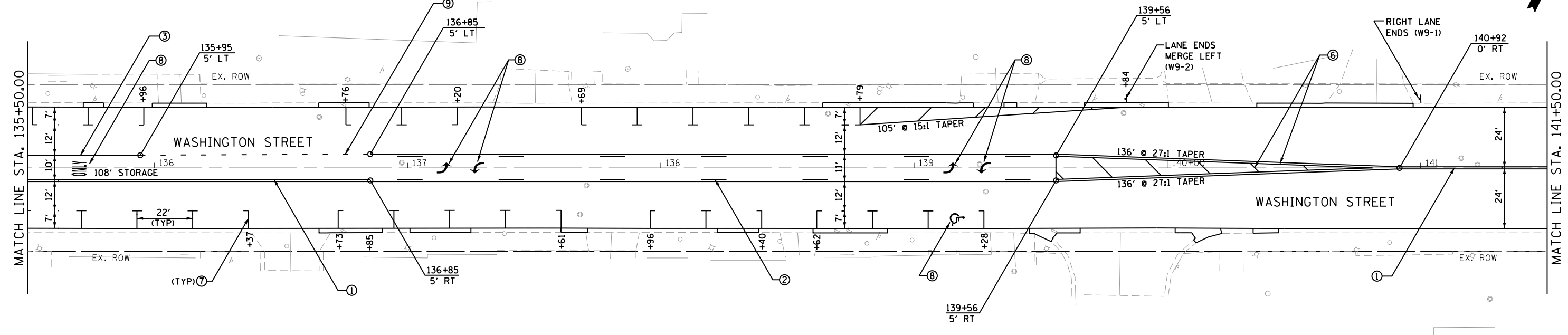
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PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN

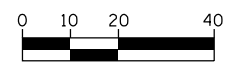
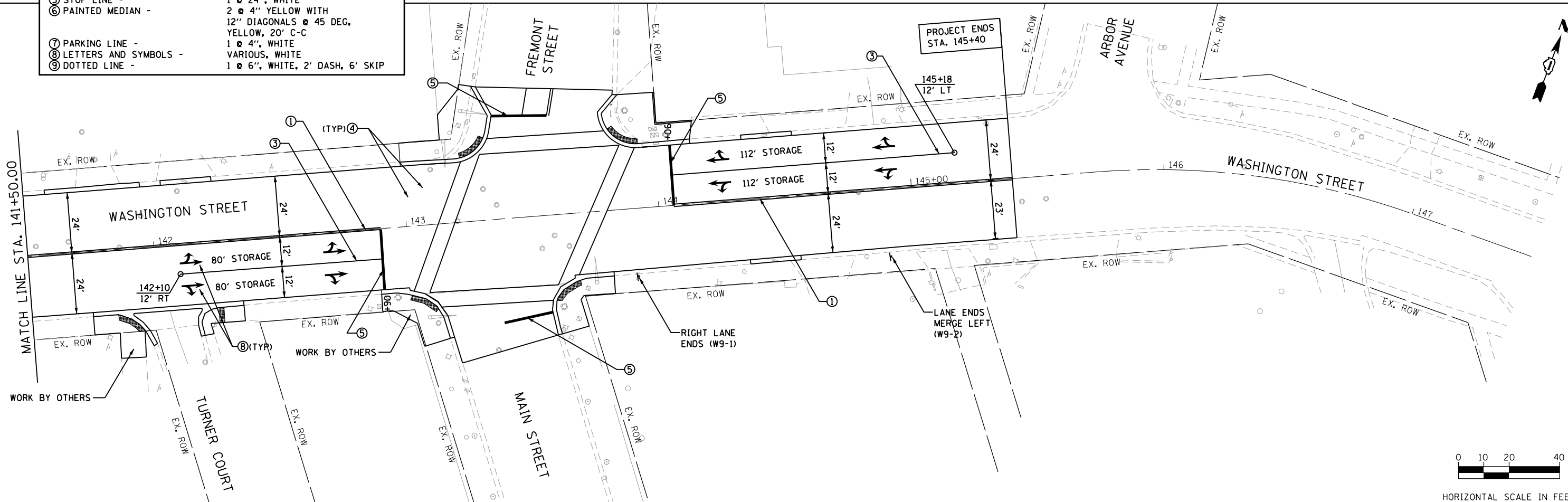
SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	15
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



THERMOPLASTIC PAVEMENT MARKING (TYPICAL WHERE SHOWN):

① CENTERLINE (SOLID) -	2 @ 4", YELLOW, 11" C-C
② CENTERLINE (SKIP-DASH) -	1 @ 4", YELLOW, 10' DASH, 30' SKIP
③ TURN LANE LINE -	1 @ 6", WHITE
④ CROSSWALK LINE -	2 @ 6", WHITE
⑤ STOP LINE -	1 @ 24", WHITE
⑥ PAINTED MEDIAN -	2 @ 4" YELLOW WITH 12" DIAGONALS @ 45 DEG, YELLOW, 20' C-C
⑦ PARKING LINE -	1 @ 4", WHITE
⑧ LETTERS AND SYMBOLS -	VARIOUS, WHITE
⑨ DOTTED LINE -	1 @ 6", WHITE, 2' DASH, 6' SKIP



HORIZONTAL SCALE IN FEET

FILE NAME = 16 sh1t-pmk_02.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	16
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES:

GENERAL

1. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OR CHANNELIZATIONS SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND WITHIN 30 MINUTES FROM THE TIME OF NOTIFICATION BY THE ENGINEER TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION, IMPROVEMENTS, OR MODIFICATION OF THE MAINTENANCE OF TRAFFIC CONTROL DEVICES. FAILURE TO RESPOND WITHIN THE TIME LIMIT ABOVE WILL RESULT IN PENALTIES AS DESCRIBED IN SECTIONS 105 AND 701 OF THE STANDARD SPECIFICATION S.
2. LONGITUDINAL DIMENSIONS SHOWN ON THESE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
3. CONSTRUCTION EQUIPMENT SHALL BE PARKED WITHIN 25 FT BEHIND THE TYPE III BARRICADES. IN ANY EVENT, ARTICLE 701.04 OF THE STANDARD SPECIFICATIONS SHALL APPLY.
4. TEMPORARY ACCESS SHALL BE MAINTAINED AT ALL TIMES THROUGH THE USE OF PLACED STONE). COMBINATION CC&G REPLACEMENT AT DRIVEWAYS NOT SHOWN TO BE CLOSED SHALL BE CONSTRUCTED IN STAGES, ONE HALF AT A TIME. THE CONTRACTOR WILL COORDINATE ANY CLOSURE OF DRIVEWAYS WITH THE ENGINEER AND THE BUSINESS OR RESIDENT THAT USES THE DRIVEWAY TO BE CLOSED.
5. ONLY WORK PERTAINING TO THE ITEMS DESCRIBED IN EACH STAGE WILL BE WORKED ON DURING EACH RESPECTIVE STAGE. ALL WORK PERTAINING TO SUBSEQUENT STAGES WILL BE STARTED ONLY AT THE COMPLETION OF THE CURRENT STAGE AS DETERMINED BY THE ENGINEER.
6. THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC OF THIS PROJECT WITH OTHER FACILITIES AND PROJECTS IN ADJACENT SECTIONS.

DETOUR

7. THE PENALTY FOR EXCEEDING THE TIME LIMIT AS SPECIFIED HEREIN SHALL BE EQUAL THE CHARGE FOR TRAFFIC CONTROL DEFICIENCY DEDUCTION PER DAY IN ACCORDANCE WITH ARTICLE 105.03 OF STANDARD SPECIFICATIONS.
8. THE DURATION'S OF THE DETOURS SHALL NOT EXCEED THE NUMBER OF WORKING DAYS LISTED BELOW. THE CONTRACTOR SHALL PROCEED WITH THE WORK IN AN EXPEDIENT MANNER TO REDUCE THE LENGTH OF TIME THAT THE DETOURS NEED TO BE IN EFFECT.
 - A) WASHINGTON STREET - EASTBOUND AND WESTBOUND DETOUR (STAGE 1): 40 WORKING DAYS
 - B) WASHINGTON STREET - WESTBOUND DETOUR (STAGE 2): 30 WORKING DAYS
9. THE ENGINEER SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE ENGINEER SHALL DETERMINE THE HOUR OF CLOSURE. THE ENGINEER WILL NOTIFY THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
10. IF DEEMED NECESSARY BY THE ENGINEER, A PRE- CONSTRUCTION MEETING WITH THE CONTRACTOR SHALL BE HELD AT LEAST TWO WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT.
11. THE CONTRACTOR SHALL SUPPLY THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF HIS/HER REPRESENTATIVES ON THE CONSTRUCTION SITE AND HIS/HER REPRESENTATIVE RESPONSIBLE FOR THE DETOUR SIGNING PRIOR TO THE START OF THE WORK
12. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING IS ERECTED IN ACCORDANCE WITH THE DETOUR PLAN AND APPROVED BY THE ENGINEER.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THE DETOUR IS IN EFFECT.
14. THE TRAFFIC CONTROL SHOWN ON THE DETOUR PLANS IS THE MINIMUM REQUIRED TO IMPLEMENT THE ROAD CLOSURE. THE CONTRACTOR SHALL MAKE ALL CHANGES IN TRAFFIC CONTROL THAT ARE DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION.
15. AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED FOR THESE DETOURS SHALL MEET THE REQUIREMENTS FOR TYPE A-LOW INTENSITY FLASHING LIGHTS IN ARTICLE 1084.01 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING THE HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION SHALL BE USED.
16. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE DETOUR IS IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
17. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1084.02 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR LIKE NEW CONDITION OF THE SIGNS. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
18. PRIOR AND SUBSEQUENT TO THE OPERATION OF THE DETOUR ROUTE, ALL STREETS SHALL BE OPEN FOR ALL TRAFFIC AT ALL TIME WITH THE FOLLOWING EXCEPTION: ANY SHORT -TERM ACTIVITY THAT REQUIRES ENCROACHMENT ON THE LANES OPEN TO TRAFFIC SHALL BE RESTRICTED TO WITHIN THE HOURS FROM 9:00 A.M. TO 3:00 P.M. LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE APPLICABLE ILLINOIS DEPARTMENT OF TRANSPORTATION TRAFFIC CONTROL STANDARDS.
19. ALL LOADED CONSTRUCTION VEHICLES MUST ENTER AND LEAVE THE JOB SITE FROM IL ROUTE 38 DURING THE WESTBOUND CLOSURE AND FROM IL ROUTE 59 (AT MAIN STREET) DURING THE EASTBOUND CLOSURE.
20. DETOUR SIGNS SHALL BE INSTALLED PROGRESSING FROM THE END OF THE DETOUR ROUTE TO THE BEGINNING.

SIGNS

21. THE DIMENSIONS OF ALL SIGNS AND ASSOCIATED TEXT NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
22. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES" ADOPTED 2004, THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", AND THE SPECIAL PROVISIONS.
23. TRAILER MOUNTED FULL MATRIX PORTABLE CHANGEABLE MESSAGE SIGNS (70106800) ARE TO BE LOCATED WITHIN CONTRACT LIMITS AS DIRECTED BY ENGINEER. COMPENSATION WILL BE MADE FOR THE ACTUAL DURATION IN WHICH THE SIGNS WERE ACCEPTABLY FURNISHED AND OPERATED AS REQUESTED BY THE ENGINEER.
24. THE CONTRACTOR SHALL REMOVE OR COVER ALL CONFLICTING EXISTING SIGNS FOR THE DURATION OF CONSTRUCTION. THE COST IS INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION ITEMS.
25. THE FOLLOWING SHALL APPLY TO CONSTRUCTION SIGNS:
 - A) THE CONTRACTOR SHALL FURNISH ALL SIGNS.
 - B) THE CONTRACTOR SHALL BE RESPONSIBLE FOR AND REPLACE ANY SIGNS THAT ARE SUPPLIED BY OTHERS AND DAMAGED BY THE CONTRACTOR'S OR SUBCONTRACTOR'S WORK FORCE DURING RELOCATION OR CONSTRUCTION OPERATIONS.
 - C) ALL SIGNS AND ASSEMBLIES SHALL BE CERTIFIED BY THE CONTRACTOR AS MEETING THE APPLICABLE REQUIREMENTS OF NCHRP REPORT 350, TEST LEVEL 3.
 - D) ALL CONSTRUCTION SIGNS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED TRAFFIC CONTROL AND PROTECTION PAY ITEMS.
26. THE "ROAD CLOSED" (R11-2), THE "ROAD CLOSED XX MILES AHEAD LOCAL TRAFFIC ONLY" (R11-3), AND THE "ROAD CLOSED TO THRU TRAFFIC" (R11-4) SIGNS SHALL BE MOUNTED ABOVE THE TOP OF THE BARRICADE. ALL TYPE III BARRICADES SHALL HAVE TWO (2) AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINE OF THE SUPPORTS.
27. ROAD NAME SIGNS SHALL HAVE A BLACK LEGEND ON FLUORESCENT ORANGE REFLECTIVE SHEETING. THE SIGN BLANK SHALL BE A 9" X VARIABLE OR A 12" X VARIABLE WITH DESIGNS SERIES C LETTERS. THE CAPITAL LETTERS SHALL BE 6", AND THE LOWER CASE SHALL BE 5".
28. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNING INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER.
29. FOR DETAILS OF STANDARD PROTECTIVE DEVICES AND CONSTRUCTION SIGNS, SEE HIGHWAY STANDARD 701901.
30. WHEN REQUIRED, THE MINIMUM DIMENSIONS OF THE ORANGE WARNING FLAGS SHOWN IN THESE PLANS ARE 18" X 18".

BARRICADES

31. ALL BARRICADES SHALL HAVE REFLECTIVE STRIPING ON BOTH SIDES OF THE BARRICADES. THE TYPE III BARRICADES USED AT POINTS OF CLOSURE TO THRU TRAFFIC SHALL NOT EXCEED 8 FEET IN WIDTH EACH FOR A SINGLE APPROACH LANE.
32. DURING NON-WORKING HOURS AT POINTS OF ROAD CLOSURE TO ALL TRAFFIC, THE CONTRACTOR SHALL PROVIDE A MEANS TO RESTRAIN THE BARRICADES FROM EASY MOVEMENT BY VANDALS. THE CHOSEN METHOD SHALL BE APPROVED BY THE ENGINEER.
33. TYPE I BARRICADE, TYPE II BARRICADE, AND DRUM MINIMUM SPACING: 50' C-C ON TANGENTS, 20' C-C ON TAPERS, AND 12.5' C-C AROUND CORNERS.

PAVEMENT MARKINGS

34. THE CONTRACTOR SHALL REMOVE STRIPING WHICH CONFLICTS WITH THE NEXT STAGE OR FINAL STRIPING. EPOXY PAVEMENT MARKINGS ON PERMANENT PAVEMENT SHALL BE REMOVED BY WATERBLASTING. GRINDING THE PAVEMENT WILL NOT BE ALLOWED. REMOVAL OF ALL EXISTING PAVEMENT MARKINGS SHALL BE INCLUDED IN THE PAY ITEM PAVEMENT MARKING REMOVAL.
35. TEMPORARY PAVEMENT MARKING - LINE 4" (70300220) SHALL BE USED FOR TEMPORARY STRIPING ON EXISTING CONCRETE . PAVEMENT MARKING TAPE, TYPE III 4" (70300520) SHALL BE USED FOR TEMPORARY STRIPING ON FINISHED SURFACES UNTIL PERMANENT STRIPING IS COMPLETE.

NOTIFICATION

36. THE ENGINEER SHALL BE NOTIFIED AT LEAST TWO (2) HOURS BEFORE THE ROAD IS TO BE OPEN TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
37. THE CONTRACTOR SHALL NOTIFY THE ENGINEER TEN (10) DAYS PRIOR TO THE ESTIMATED DATE THAT THE ROADWAY WILL BE READY FOR THE APPLICATION OF PERMANENT PAVEMENT MARKINGS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE ROADWAY CLEANED OF ANY DIRT, GRAVEL, OIL, ETC. ON THE DAY PAVEMENT MARKINGS ARE APPLIED.
38. IF REQUESTED BY THE CONTRACTOR IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT, THE ENGINEER WILL FIELD LOCATE THE POSITIONS OF ANY SIGNS.
39. THE CONTRACTOR SHALL CONTACT THE ENGINEER AT LEAST 72 HOURS IN ADVANCE OF INSTALLING DETOUR SIGNS.

FILE NAME = 17 sh1c-MDT General Notes.dgn



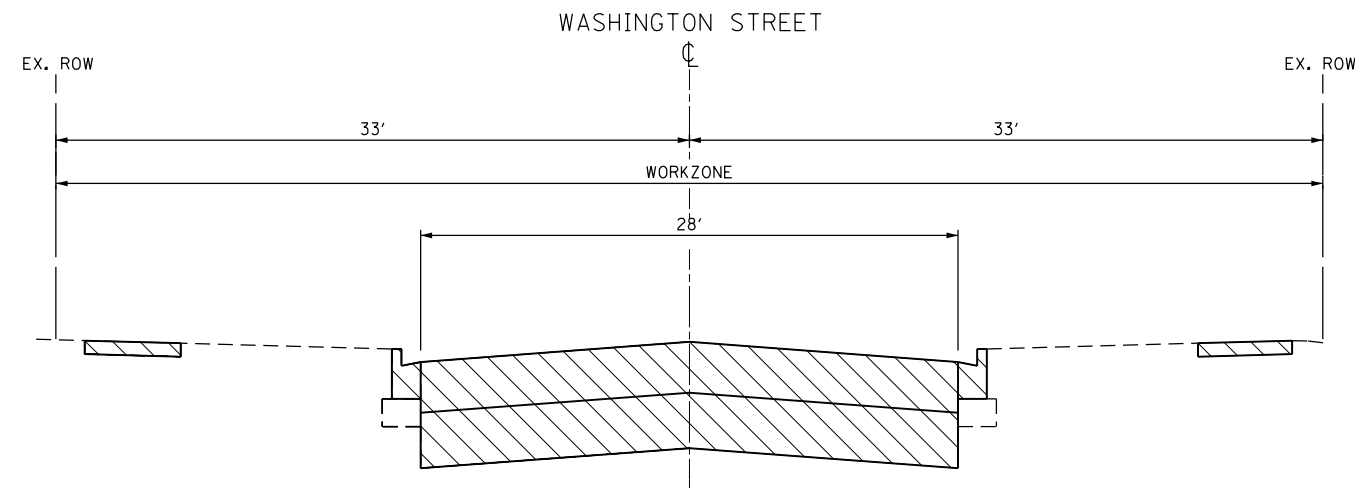
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
GENERAL NOTES**

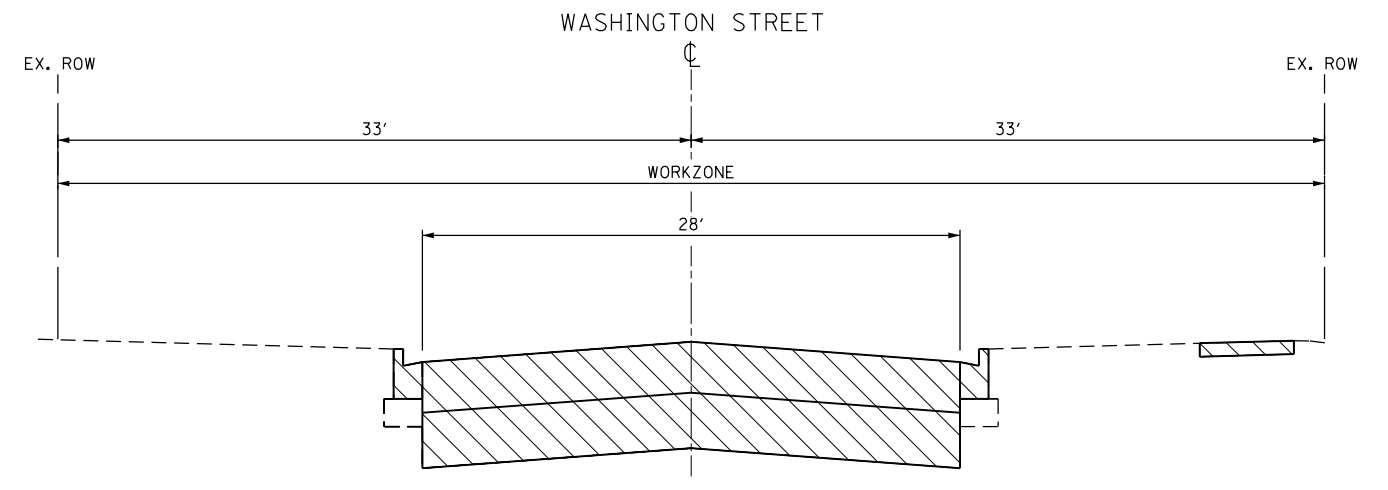
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CONTRACT NO. 61F62			ILLINOIS FED. AID PROJECT	



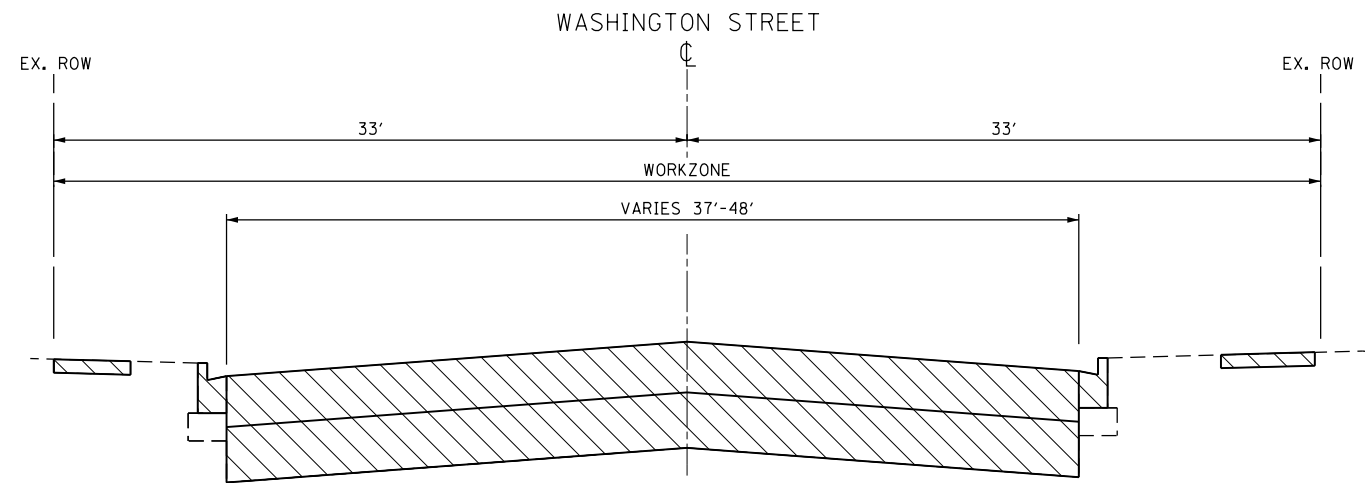
PROPOSED STAGE 1 TYPICAL SECTION

STA. 126+84 TO STA. 130+64



PROPOSED STAGE 1 TYPICAL SECTION

STA. 130+64 TO STA. 132+96



PROPOSED STAGE 1 TYPICAL SECTION

STA. 132+96 TO STA. 134+11

LEGEND

➔ DIRECTION OF TRAFFIC

▬ TYPE II BARRICADES OR DRUMS @ 50' CENTERS UNLESS OTHERWISE SHOWN

FILE NAME = 18-19 sht-MOT typical.dgn



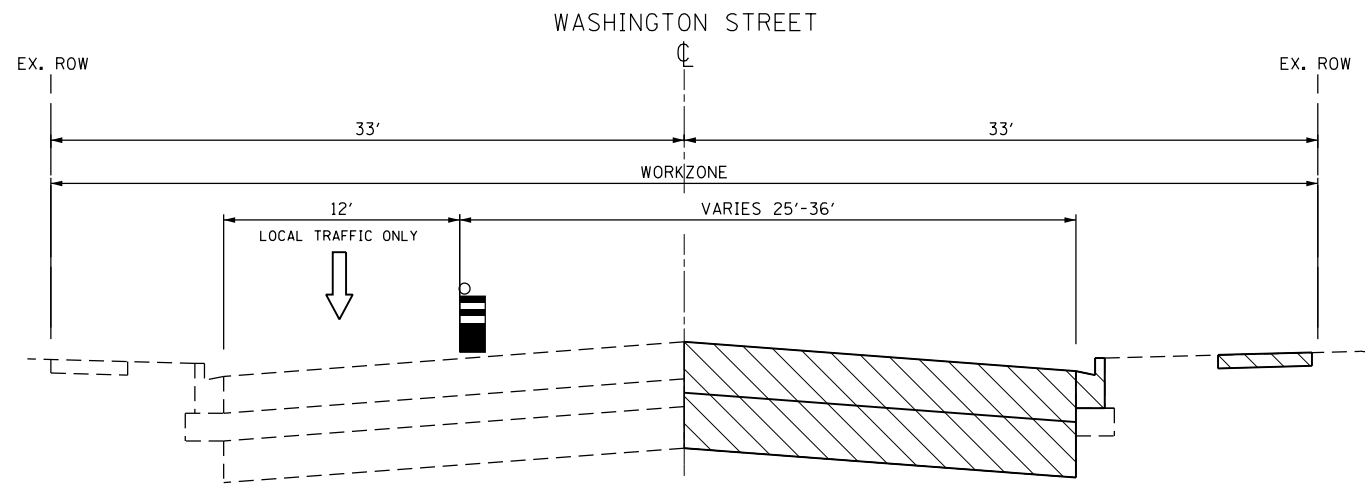
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

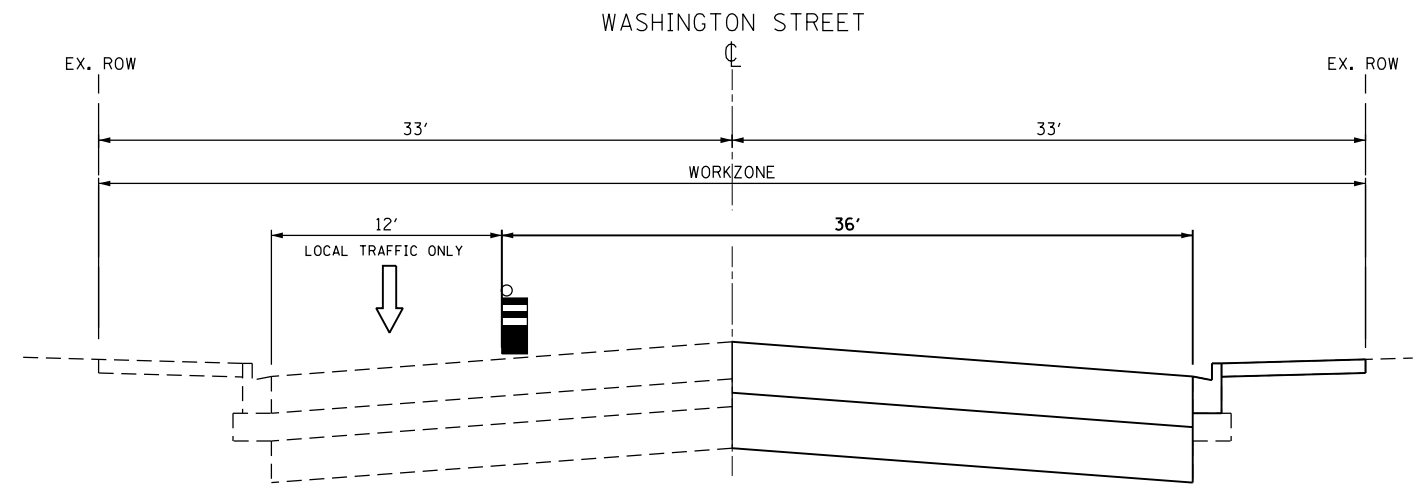
**MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
WASHINGTON STREET**

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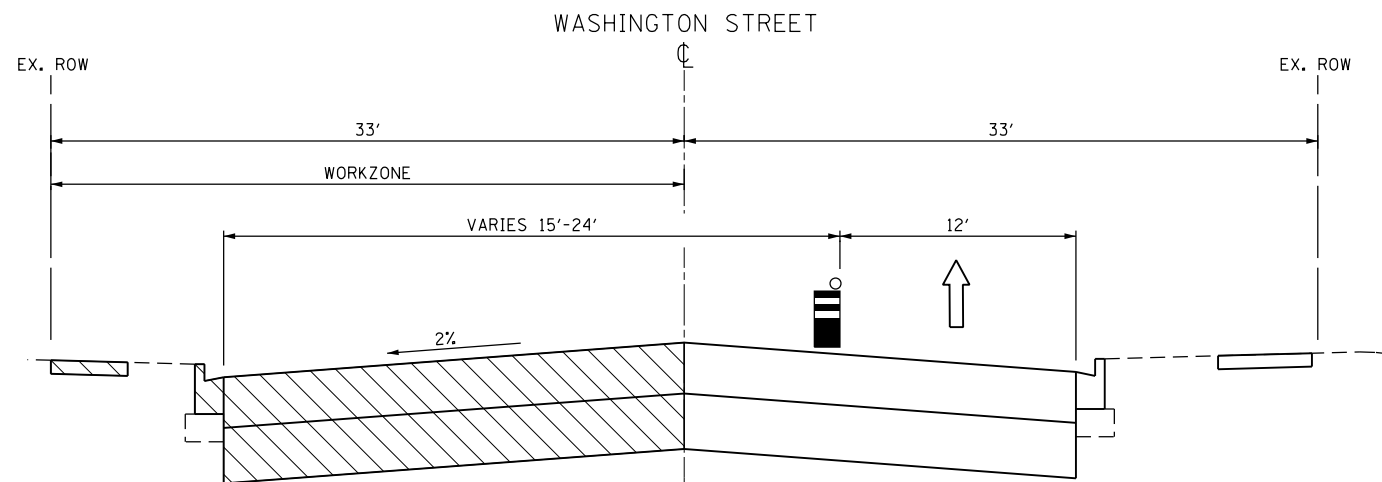
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CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



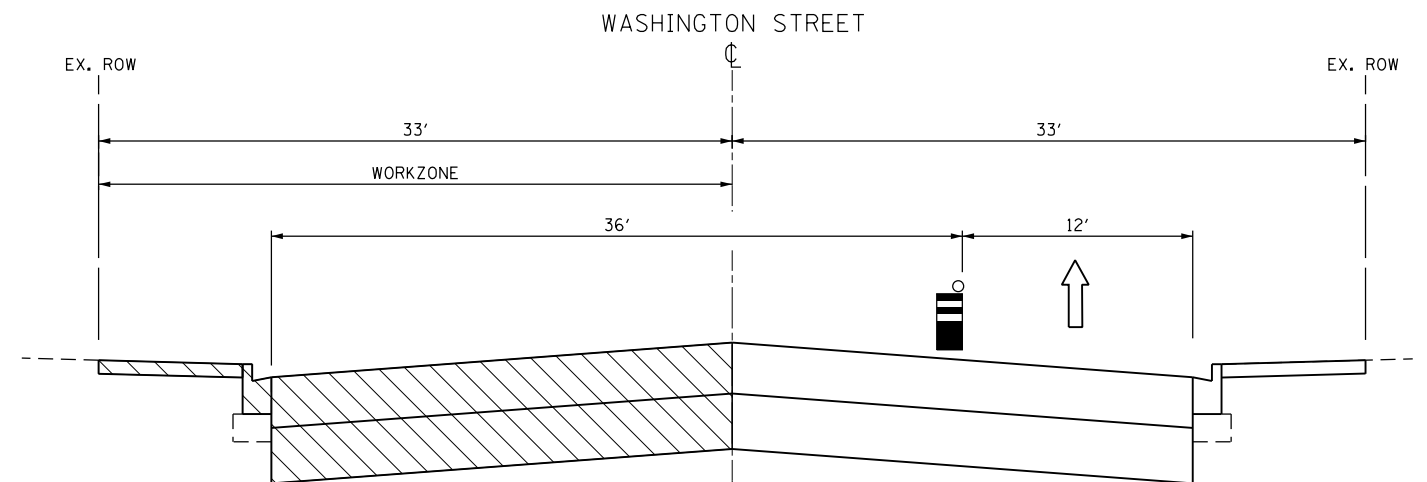
PROPOSED STAGE 1 TYPICAL SECTION
STA. 134+11 TO STA. 134+50



PROPOSED STAGE 1 TYPICAL SECTION
STA. 134+50 TO STA. 145+40



PROPOSED STAGE 2 TYPICAL SECTION
STA. 134+11 TO STA. 134+50



PROPOSED STAGE 2 TYPICAL SECTION
STA. 134+50 TO STA. 145+40

LEGEND

➔ DIRECTION OF TRAFFIC

▬ TYPE II BARRICADES OR DRUMS @ 50' CENTERS UNLESS OTHERWISE SHOWN

FILE NAME = 18-19 sht-MOT typical.dgn



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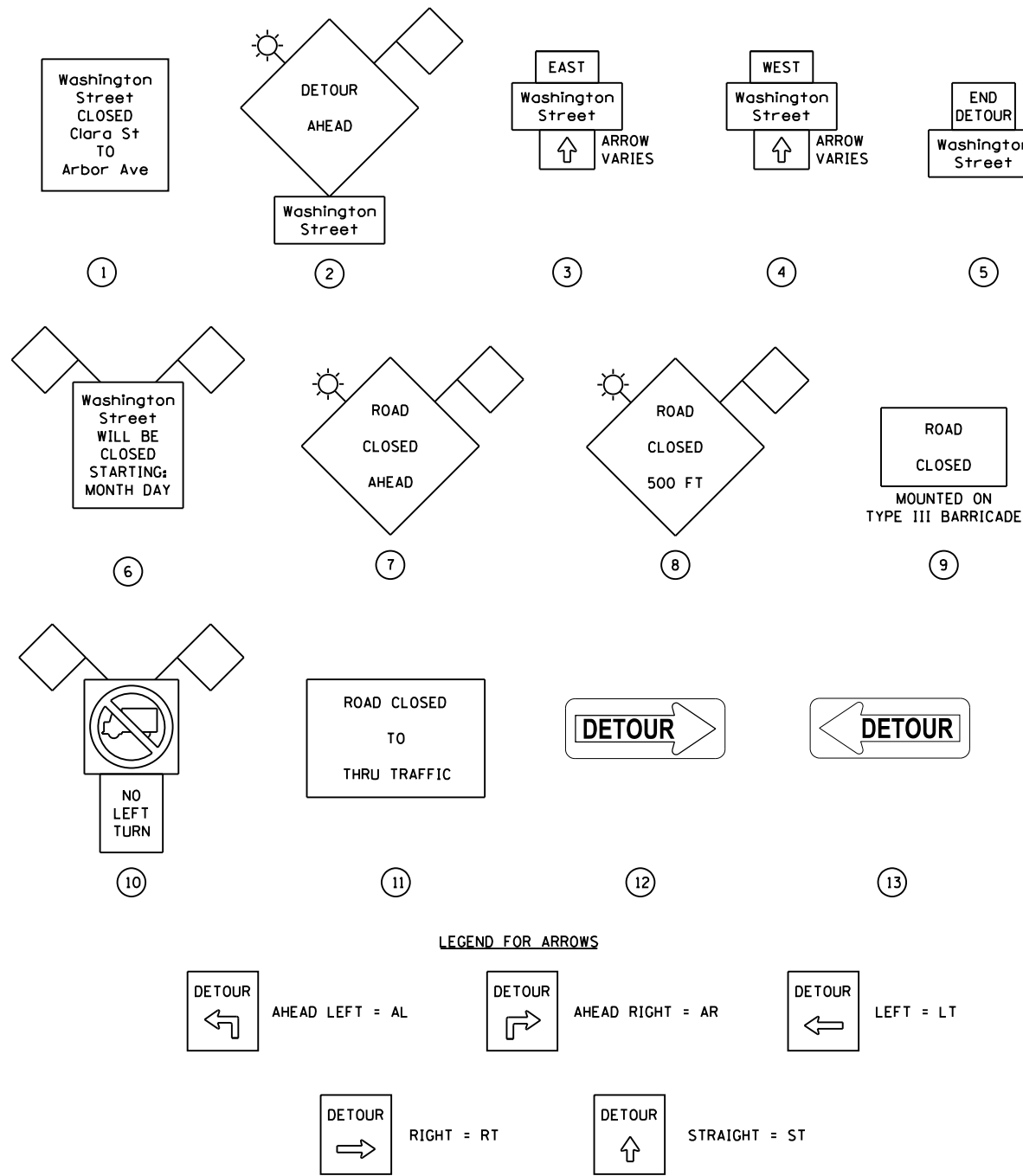
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
WASHINGTON STREET**

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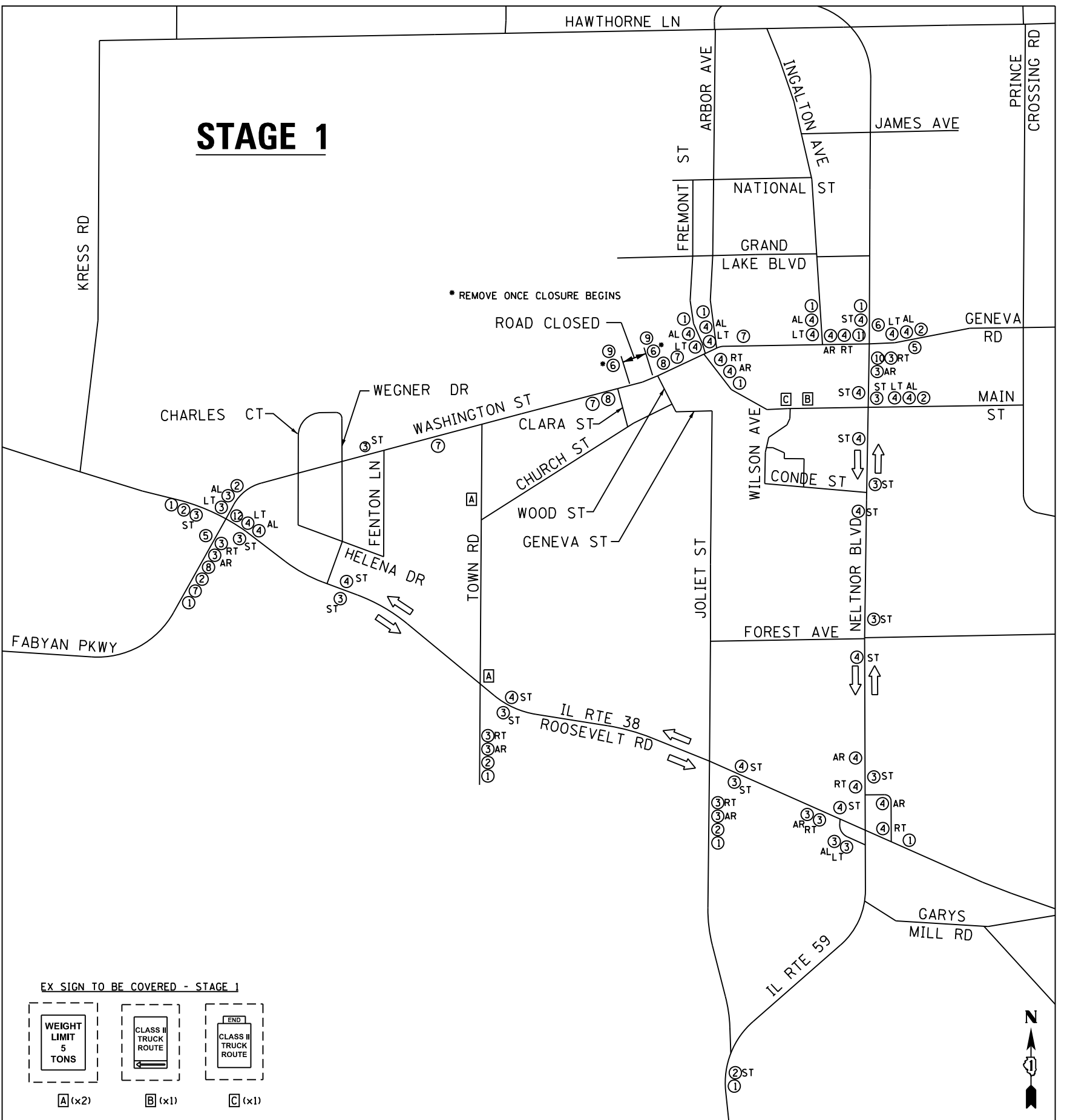
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1397	16-00079-00-PV	DUPAGE	44	19
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

SIGN LEGEND FOR DETOUR PLAN - STAGE 1

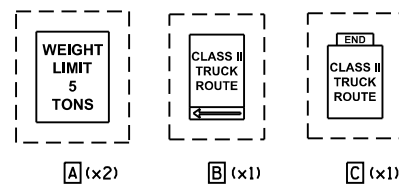


NOTES:

- DURING STAGE 1, EASTBOUND WASHINGTON STREET IS CLOSED.
- SIGN NUMBERS 7 AND 8 SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO THE CLOSURE OF THE ROAD AND SHALL BE REMOVED WHEN THE DETOUR ROUTE IS IN OPERATION. THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR.
- SEE MAINTENANCE OF TRAFFIC PLAN SHEETS FOR THE APPROPRIATE STAGE FOR TEMPORARY TRAFFIC CONTROL DEVICES, PAVEMENT MARKINGS, AND SIGNING ON WASHINGTON STREET (I.E. WITHIN THE WORK ZONE) AND ITS SIDE STREETS.
- SEE DISTRICT 1 DETAILS AND IDOT STATEWIDE STANDARDS FOR ADDITIONAL INFORMATION ON SIGN PLACEMENT AND SPACING.



EX SIGN TO BE COVERED - STAGE 1



FILE NAME = 20 sht-detour-Stage 01.dgn



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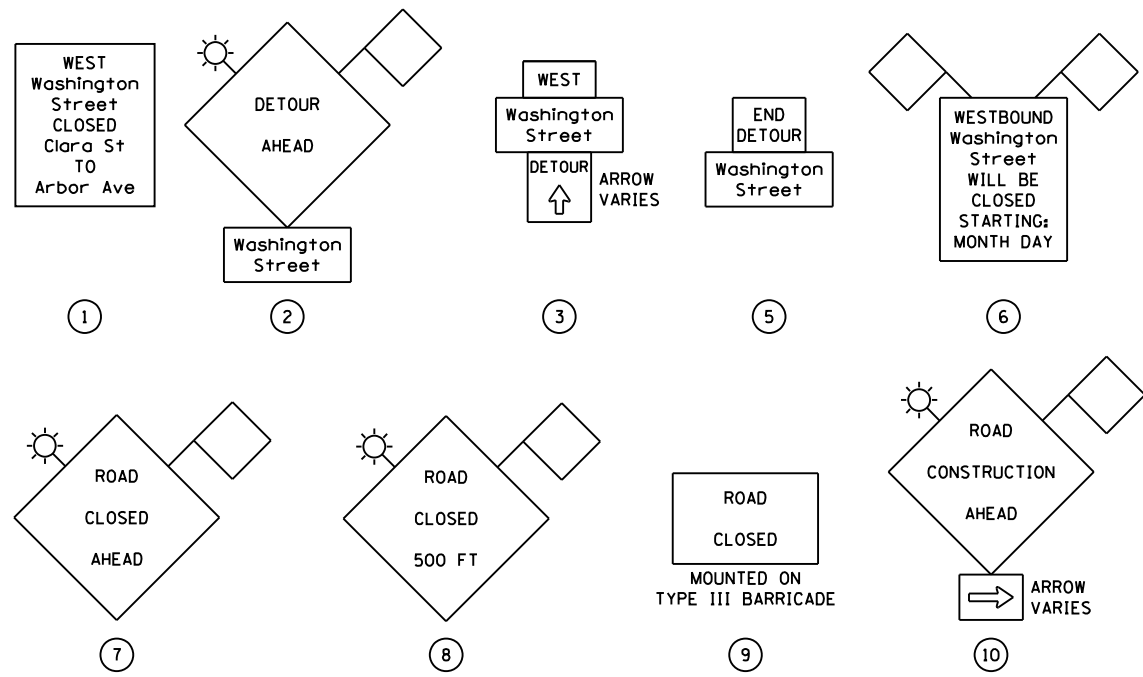
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC DETOUR PLAN SHEET - STAGE 1
WASHINGTON STREET**

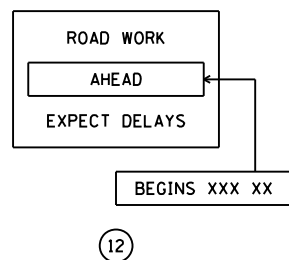
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CONTRACT NO. 61F62				ILLINOIS FED. AID PROJECT

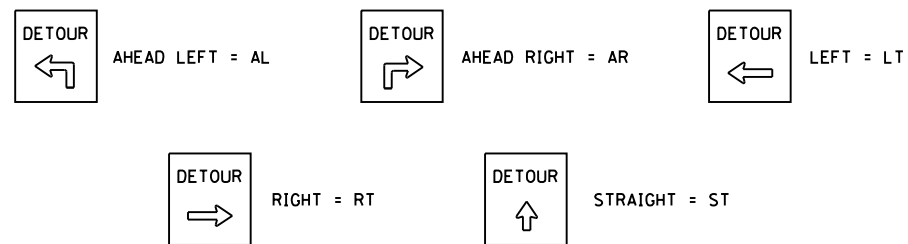
SIGN LEGEND FOR DETOUR PLAN - STAGE 2



SEE DISTRICT 1 DETAIL TC-22

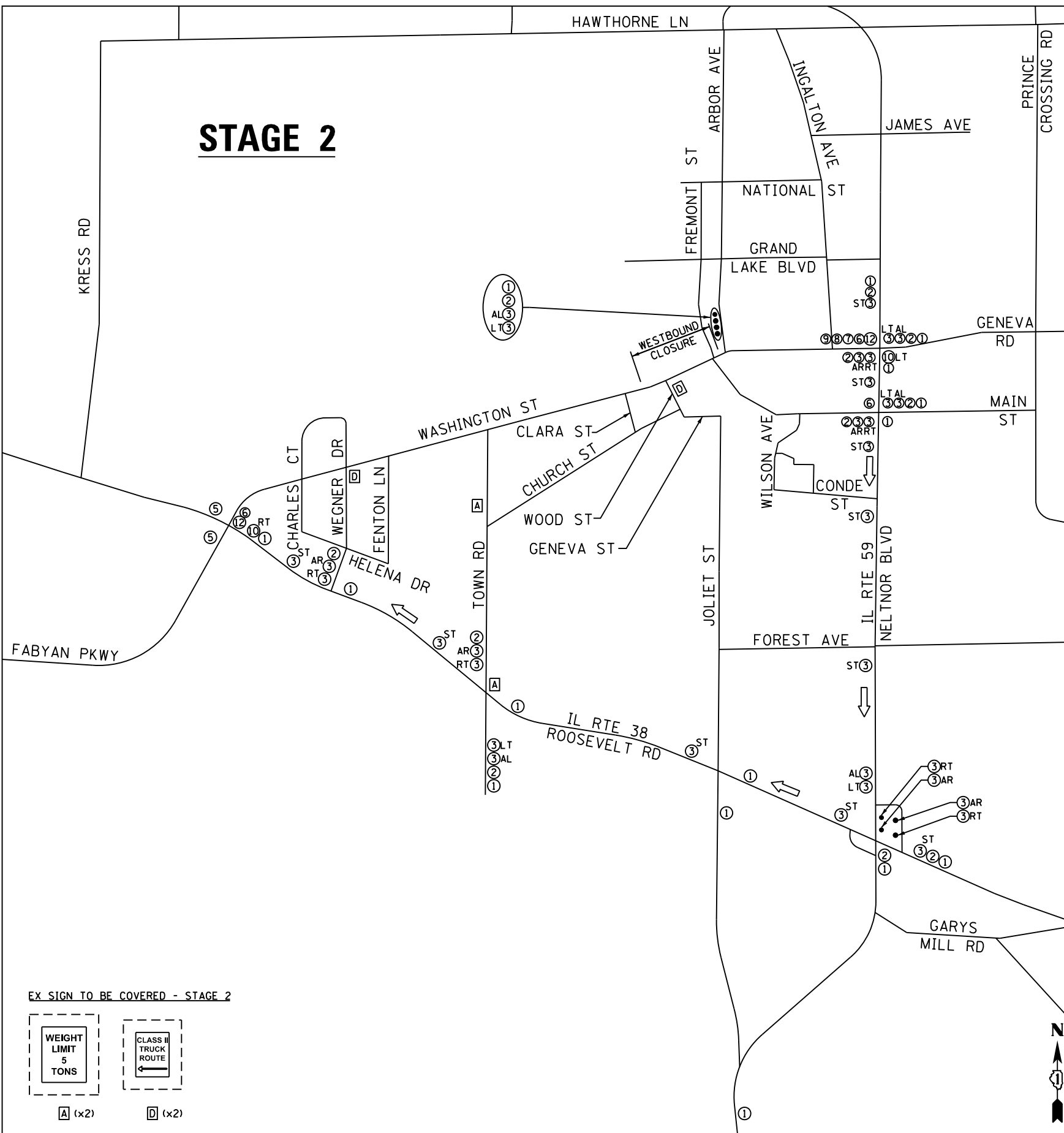


LEGEND FOR ARROWS



NOTES:

1. DURING STAGE 2, WESTBOUND WASHINGTON STREET IS CLOSED.
2. SIGN NUMBER 6 SHALL BE ERECTED AT LEAST 10 DAYS PRIOR TO THE CLOSURE OF THE ROAD AND SHALL BE REMOVED WHEN THE DETOUR ROUTE IS IN OPERATION. THE COST SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR.
3. SEE MAINTENANCE OF TRAFFIC PLAN SHEETS FOR THE APPROPRIATE STAGE FOR TEMPORARY TRAFFIC CONTROL DEVICES, PAVEMENT MARKINGS, AND SIGNING ON WASHINGTON STREET (I.E. WITHIN THE WORK ZONE) AND ITS SIDE STREETS.
4. SEE DISTRICT 1 DETAILS AND IDOT STATEWIDE STANDARDS FOR ADDITIONAL INFORMATION ON SIGN PLACEMENT AND SPACING.



FILE NAME = 21-ahf-detour-Stage 02.dgn



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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC DETOUR PLAN SHEET - STAGE 2
WASHINGTON STREET**

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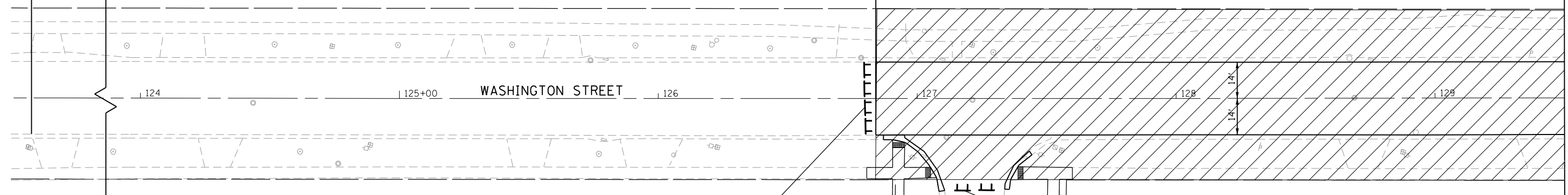
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1397	16-00079-00-PV	DUPAGE	44	21
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



PROJECT BEGINS,
BEGIN OMISSION
STA. 101+58

END OMISSION
STA. 126+84

MATCH LINE STA. 129+50.00

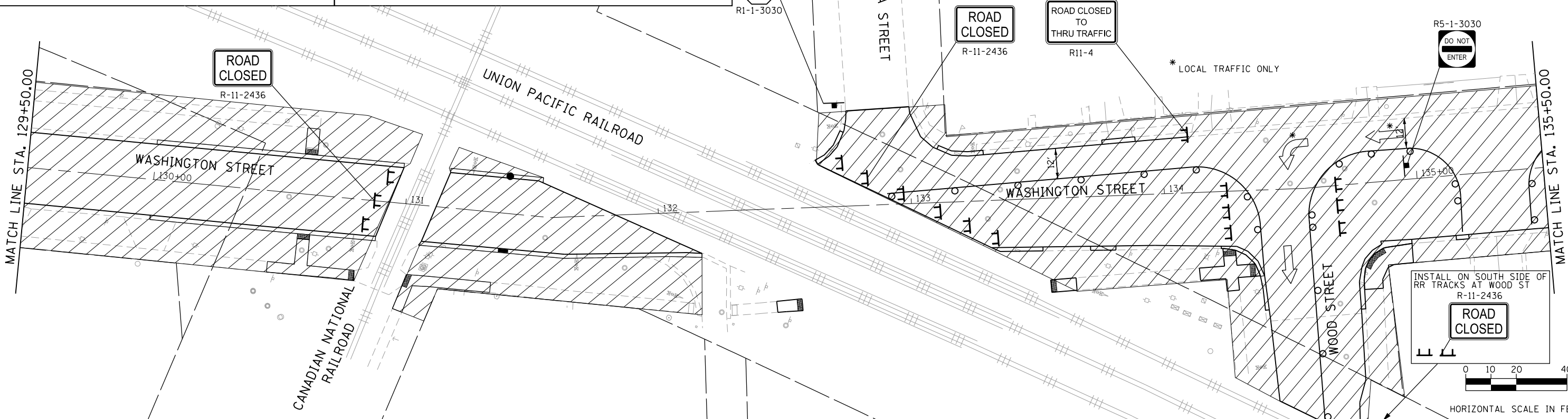


LEGEND

- WORK ZONE
- DRUM (AT 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS)
- TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- DIRECTIONAL OF TRAVEL

NOTES:

1. SEE DETOUR PLANS FOR DETOUR SIGNING NOT SHOWN ON THESE SHEETS.
2. ALL PAVEMENT MARKINGS OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE TAPE.
3. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
4. THE TRAFFIC CONTROL AND PROTECTION SHOWN ON THE PLANS IS THE MINIMUM REQUIRED TO IMPLEMENT THE ROAD CLOSURE. LOCAL DETOUR SIGNAGE INCLUDING, BUT NOT LIMITED TO TYPE III BARRICADES AND SIGNS M-4 (DETOUR), R-11 (ROAD CLOSED), R-6 (ONE-WAY) WILL BE REQUIRED ON SIDE STREETS TO DETOUR LOCAL TRAFFIC DURING SHORT-TERM INTERSECTION CLOSURES SUCH AS WOOD STREET, TURNER COURT, AND MAIN STREET, AS DIRECTED BY THE ENGINEER.



FILE NAME = 22 sht-MOT Stage 1.dgn



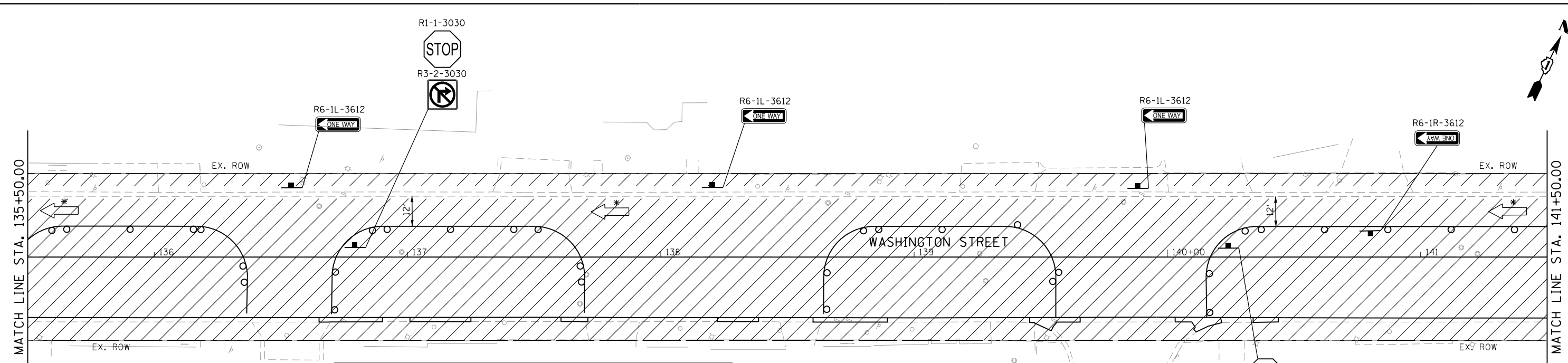
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
STAGE 1**

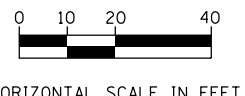
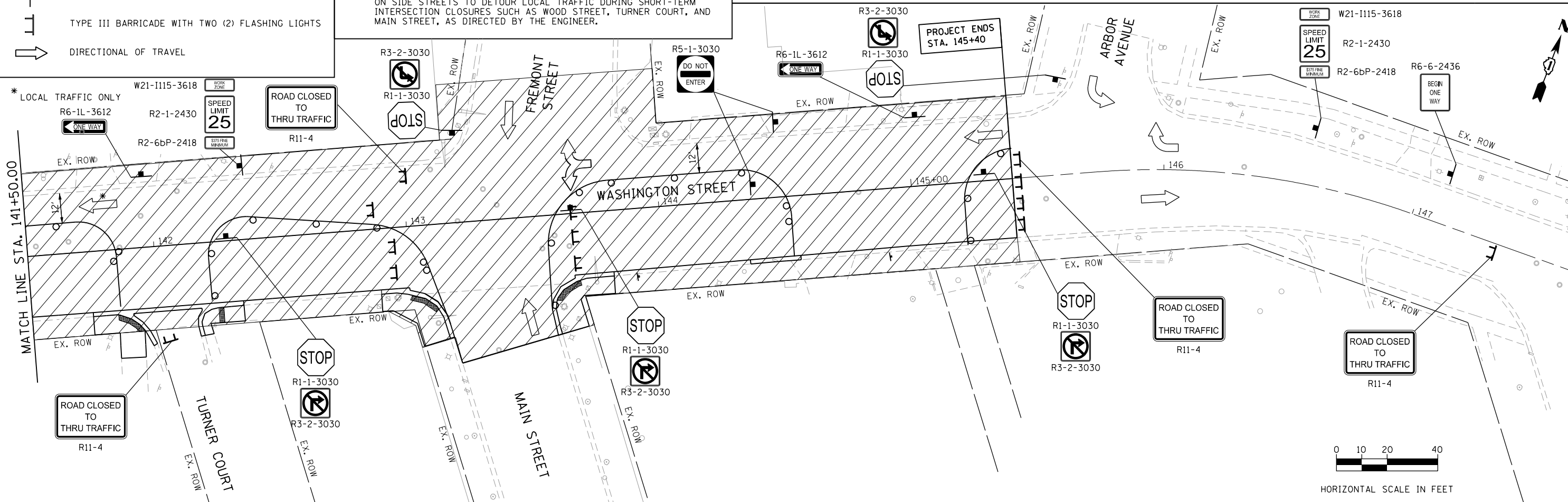
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	22
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



- NOTES:**
1. SEE DETOUR PLANS FOR DETOUR SIGNING NOT SHOWN ON THESE SHEETS.
 2. ALL PAVEMENT MARKINGS OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE TAPE.
 3. DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
 4. THE TRAFFIC CONTROL AND PROTECTION SHOWN ON THE PLANS IS THE MINIMUM REQUIRED TO IMPLEMENT THE ROAD CLOSURE. LOCAL DETOUR SIGNAGE INCLUDING, BUT NOT LIMITED TO TYPE III BARRICADES AND SIGNS M-4 (DETOUR), R-11 (ROAD CLOSED), R-6 (ONE-WAY) WILL BE REQUIRED ON SIDE STREETS TO DETOUR LOCAL TRAFFIC DURING SHORT-TERM INTERSECTION CLOSURES SUCH AS WOOD STREET, TURNER COURT, AND MAIN STREET, AS DIRECTED BY THE ENGINEER.

- LEGEND**
- WORK ZONE
 - DRUM (AT 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS)
 - TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
 - DIRECTIONAL OF TRAVEL



FILE NAME = 23 sht-MOT Stage 1.02.dgn



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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

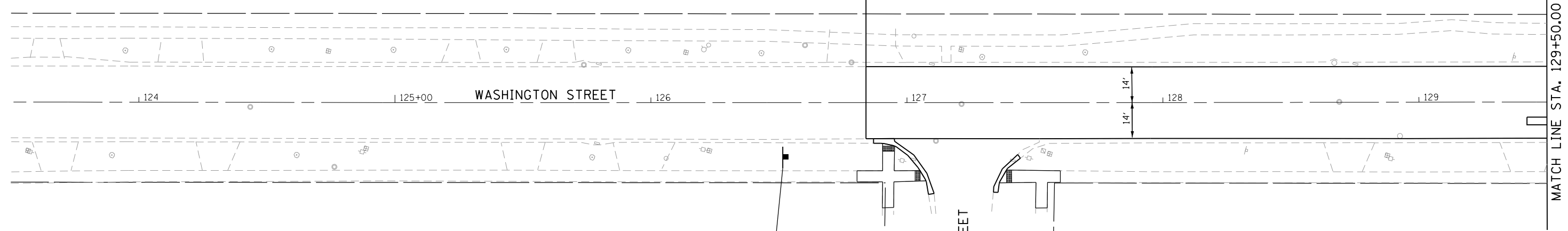
**MAINTENANCE OF TRAFFIC
STAGE 2**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

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1397	16-00079-00-PV	DUPAGE	44	23
				CONTRACT NO. 61F62
ILLINOIS FED. AID PROJECT				



PROJECT BEGINS,
STA. 126+84



LEGEND

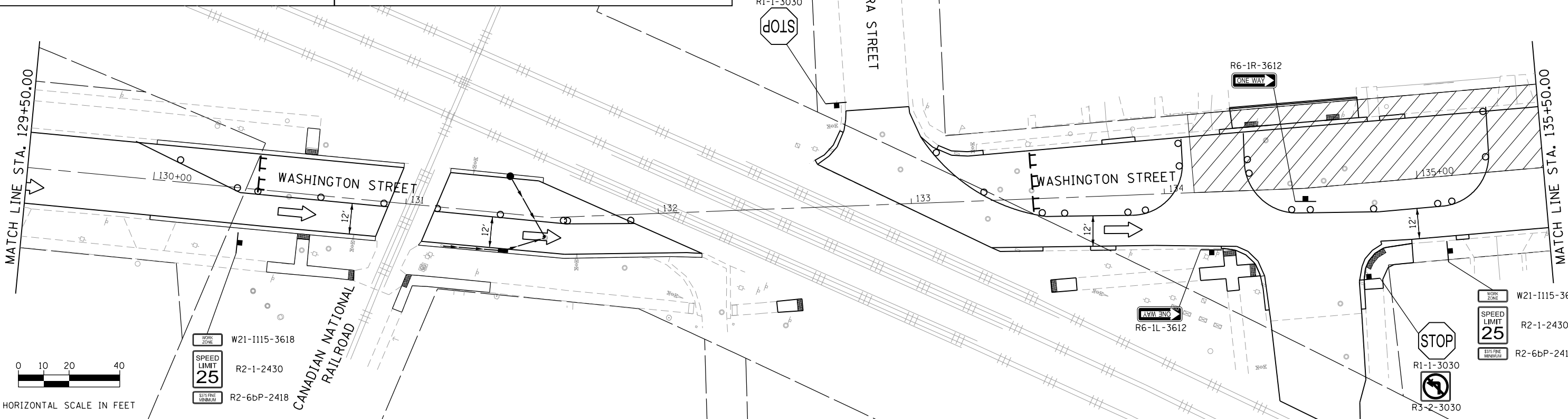
- WORK ZONE
- DRUM (AT 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS)
- TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- DIRECTIONAL OF TRAVEL

NOTES:

- SEE DETOUR PLANS FOR DETOUR SIGNING NOT SHOWN ON THESE SHEETS.
- ALL PAVEMENT MARKINGS OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE TAPE.
- DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION SHOWN ON THE PLANS IS THE MINIMUM REQUIRED TO IMPLEMENT THE ROAD CLOSURE. LOCAL DETOUR SIGNAGE INCLUDING, BUT NOT LIMITED TYPE III BARRICADES AND SIGNS M-4 (DETOUR), R-11 (ROAD CLOSED), R-6 (ONE-WAY) WILL BE REQUIRED ON SIDE STREETS TO DETOUR LOCAL TRAFFIC DURING SHORT-TERM INTERSECTION CLOSURES SUCH AS WOOD STREET, TURNER COURT, AND MAIN STREET, AS DIRECTED BY THE ENGINEER.

WORK ZONE W21-1115-3618
 SPEED LIMIT 25 R2-1-2430
 SIX FINE MINIMUM R2-6bP-2418

CLARA STREET



WORK ZONE W21-1115-3618
 SPEED LIMIT 25 R2-1-2430
 SIX FINE MINIMUM R2-6bP-2418

WORK ZONE W21-1115-3618
 SPEED LIMIT 25 R2-1-2430
 SIX FINE MINIMUM R2-6bP-2418

FILE NAME = 24 sht-MOT Stage 2.01.dgn



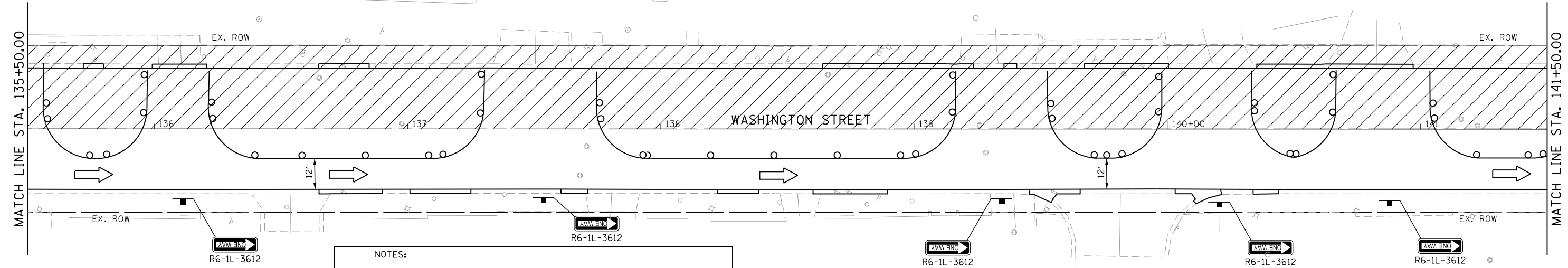
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PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE 1

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	24
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

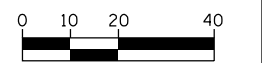
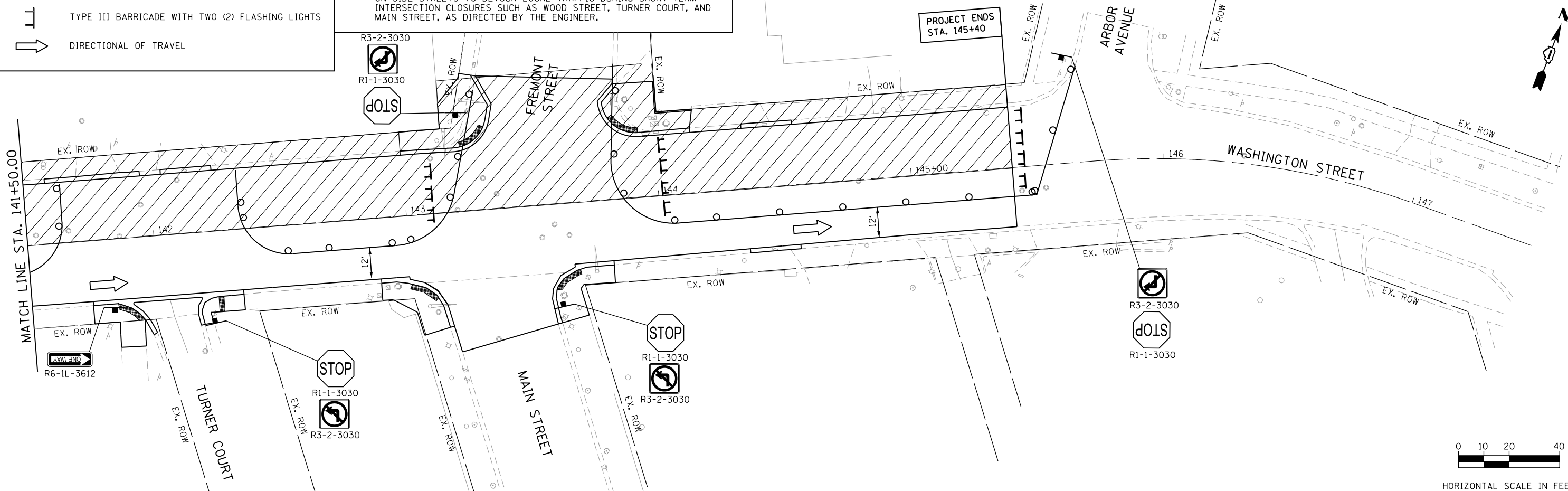


LEGEND

- WORK ZONE
- DRUM (AT 50' C-C (TYP), 25' C-C IN CURVES AND RADII, AND 20' C-C IN TAPERS)
- TEMPORARY TRAFFIC SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE WITH TWO (2) FLASHING LIGHTS
- DIRECTIONAL OF TRAVEL

NOTES:

- SEE DETOUR PLANS FOR DETOUR SIGNING NOT SHOWN ON THESE SHEETS.
- ALL PAVEMENT MARKINGS OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE TAPE.
- DRIVEWAY ENTRANCE SIGNS (TC-26) SHALL BE PLACED AS DIRECTED BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION SHOWN ON THE PLANS IS THE MINIMUM REQUIRED TO IMPLEMENT THE ROAD CLOSURE. LOCAL DETOUR SIGNAGE INCLUDING, BUT NOT LIMITED TO TYPE III BARRICADES AND SIGNS M-4 (DETOUR), R-11 (ROAD CLOSED), R-6 (ONE-WAY) WILL BE REQUIRED ON SIDE STREETS TO DETOUR LOCAL TRAFFIC DURING SHORT-TERM INTERSECTION CLOSURES SUCH AS WOOD STREET, TURNER COURT, AND MAIN STREET, AS DIRECTED BY THE ENGINEER.



HORIZONTAL SCALE IN FEET

FILE NAME = 25 sht-MOT Stage 2.dgn



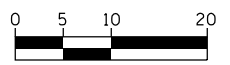
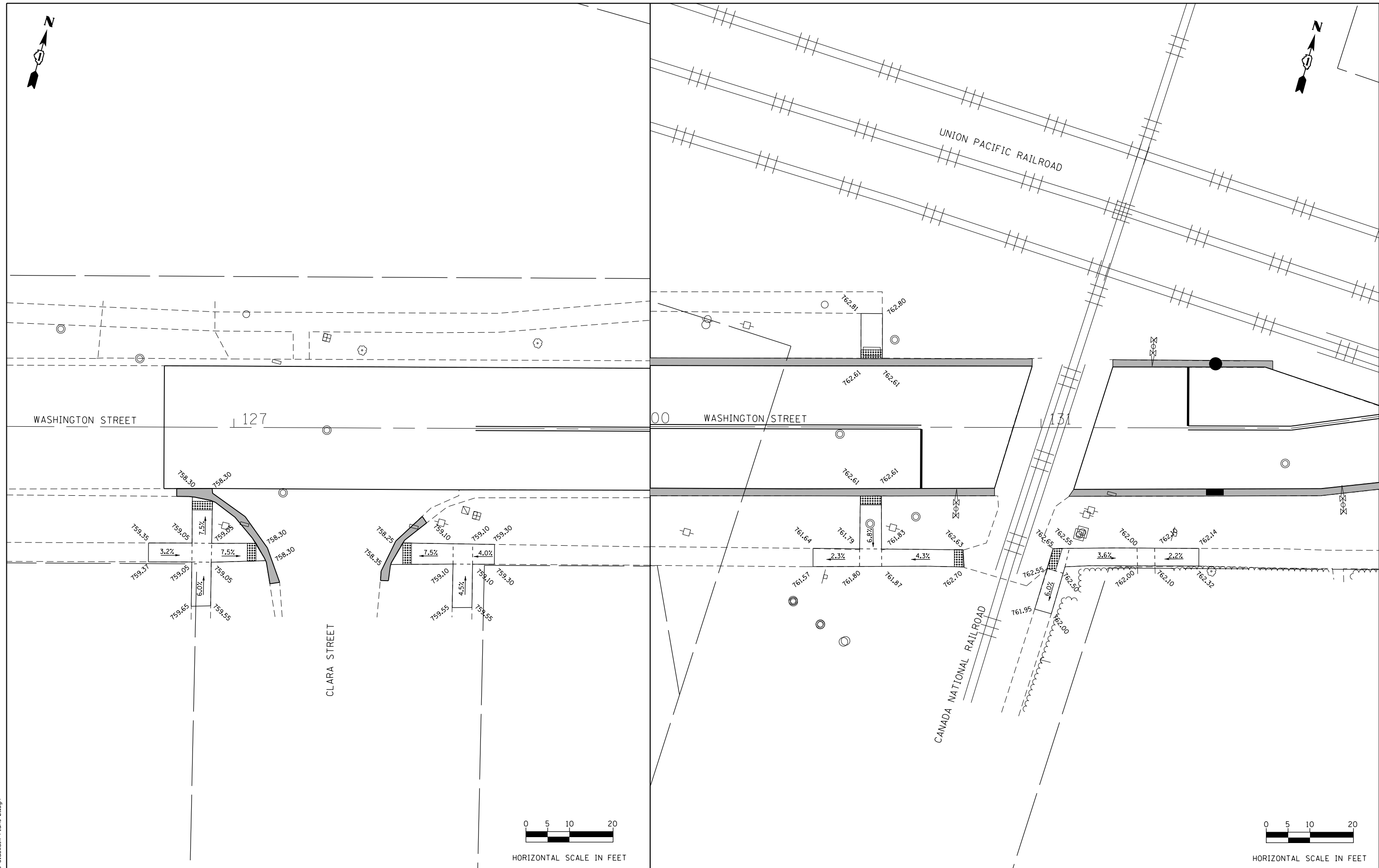
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

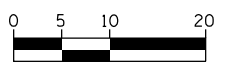
**MAINTENANCE OF TRAFFIC
STAGE 2**

SCALE: 1"=20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	25
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



HORIZONTAL SCALE IN FEET



HORIZONTAL SCALE IN FEET

FILE NAME = 26 Sidewalk Plans 01.dgn



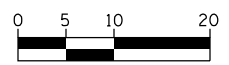
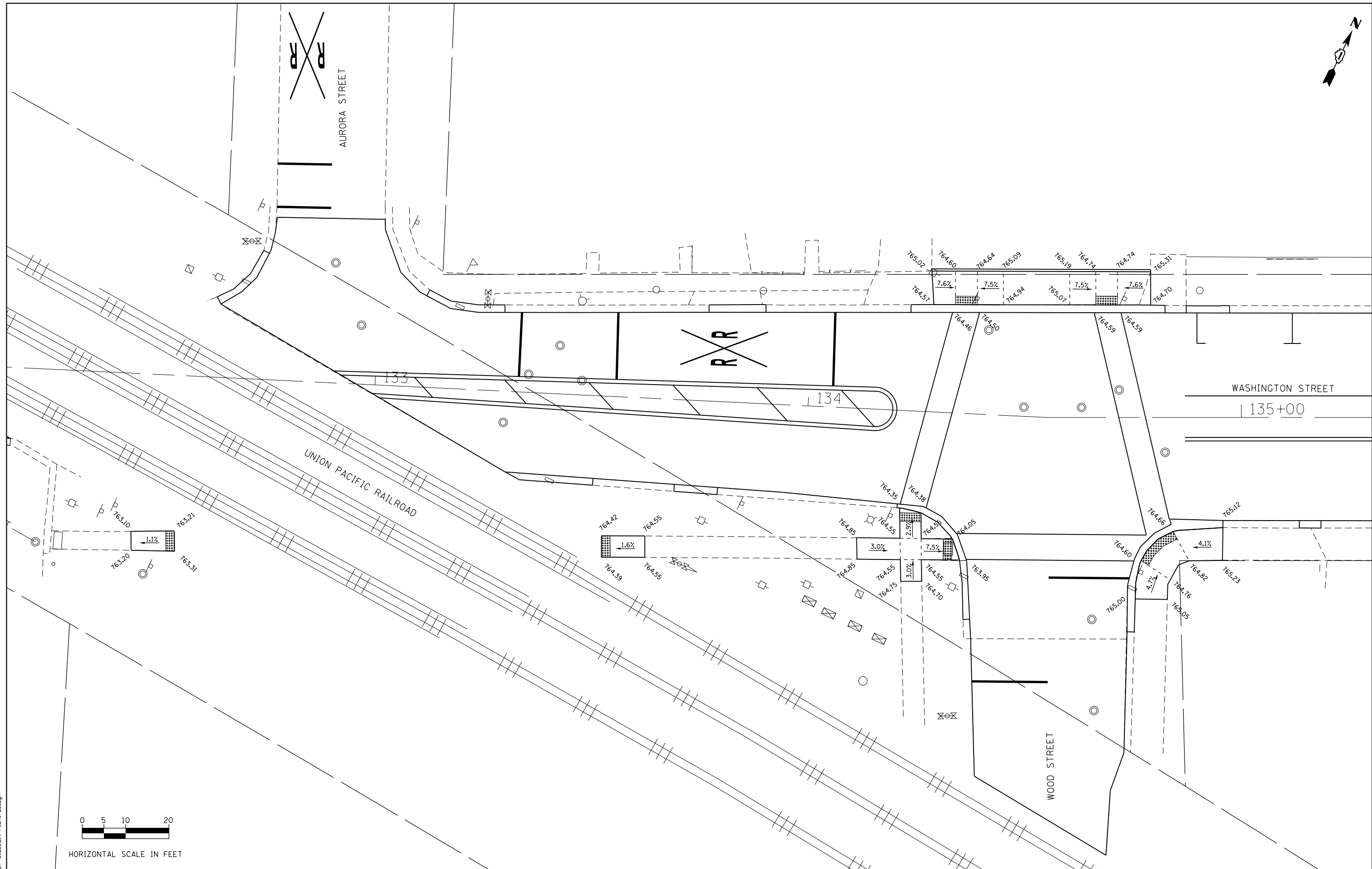
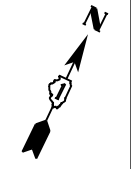
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	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	26
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



HORIZONTAL SCALE IN FEET

FILE NAME = 27 Sidewalk Plans 02.dgn



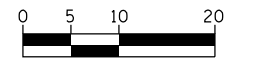
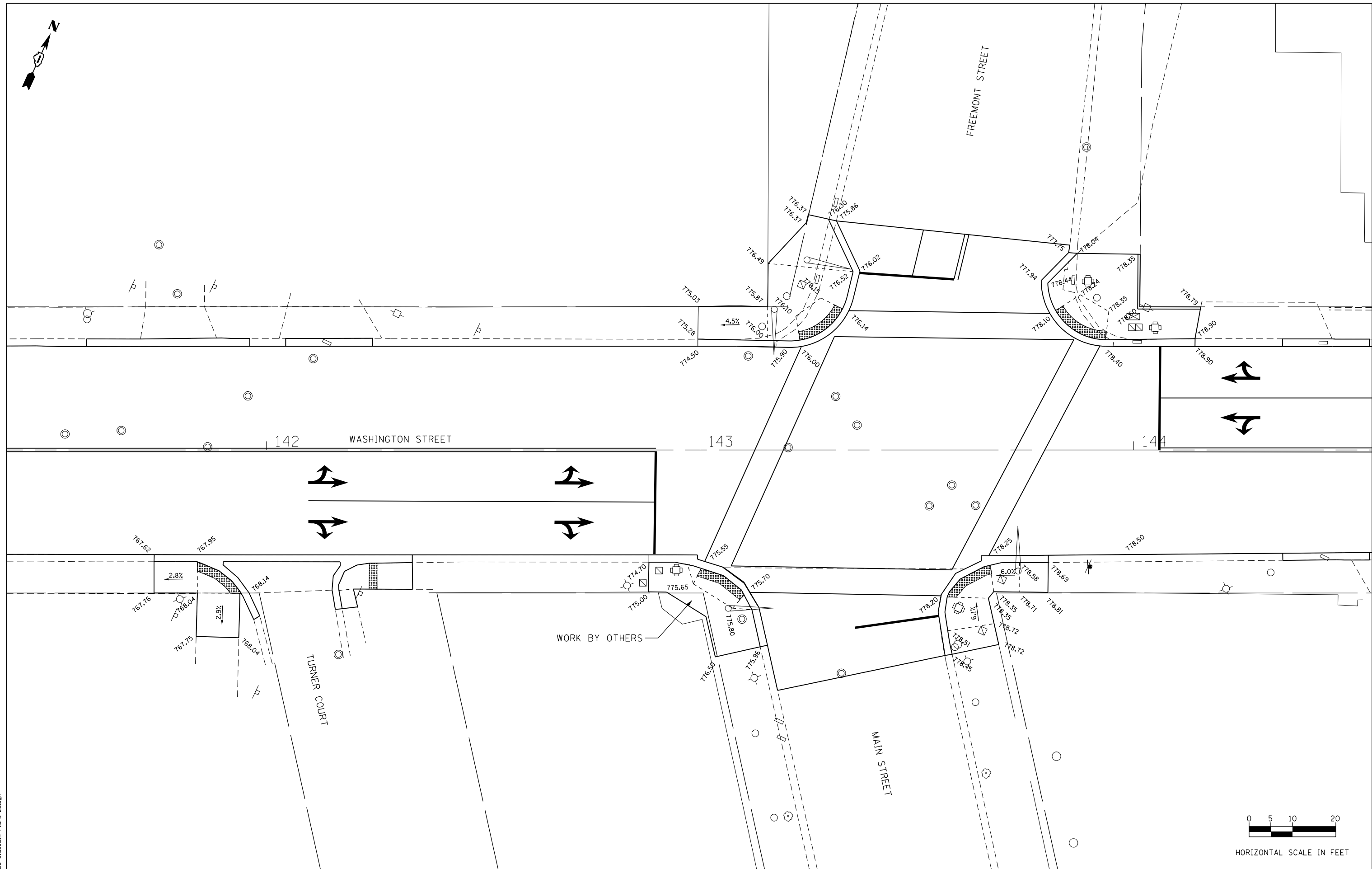
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	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	27
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



HORIZONTAL SCALE IN FEET

FILE NAME = 28 Sidewalk Plans 03.dgn



USER NAME = DonN	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIDEWALK PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	28
CONTRACT NO. 61F62				

ILLINOIS FED. AID PROJECT

NOTES:

DETECTABLE WARNING TILES SHALL ALIGN WITH THE CROSSWALK STRIPES OR STREET CROSSING. IF FIELD CONDITIONS PROHIBIT THIS, TILE ALIGNMENT SHALL BE AT THE DISCRETION OF THE ENGINEER.

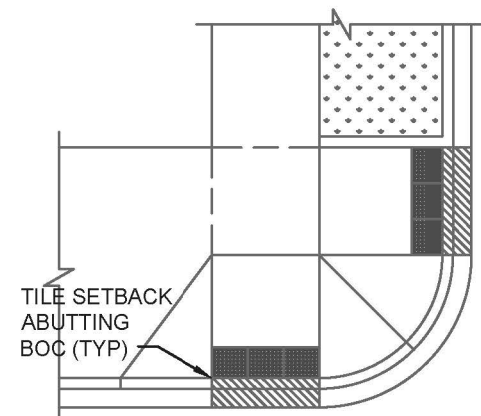
CURB RAMP LAYOUT B-1-2 IS PREFERRED WHENEVER POSSIBLE. WHERE RAMPS ARE LOCATED IN THE CORNER RADIUS, LAYOUT B-1-3 SHALL BE USED.

CURB RAMP PLACEMENT SHALL BE COORDINATED AS REQUIRED TO ALLOW FOR A 4' MINIMUM WIDTH SIDEWALK AROUND EACH CORNER OF INTERSECTION. SIDEWALK NOT TO BE OBSTRUCTED BY CURB RAMPS OR OTHER BARRIERS AND SHALL HAVE A CROSS SLOPE OF 1:50 MAXIMUM (2%).

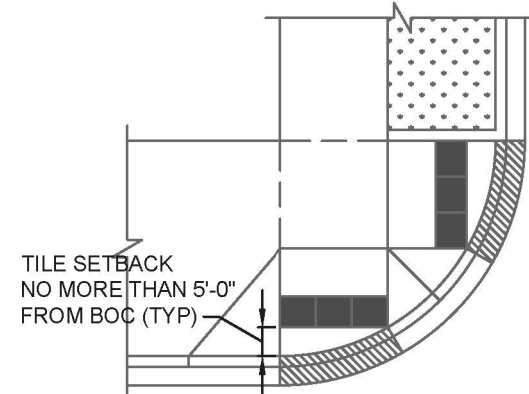
THE BLENDED TRANSITION LAYOUT B-1-7 (AND SIMILAR) MAY BE USED WHERE TWO RAMPS ARE NOT POSSIBLE DUE TO GEOMETRIC CONSTRAINTS SUCH AS LIMITED SIDEWALK WIDTH OR GRADE ELEVATIONS. THE BLENDED TRANSITION SHALL NOT BE USED IF ACCESS TO AN EXISTING FACILITY WOULD BE REDUCED.

THE SHARED PERPENDICULAR RAMP AT CORNER LAYOUT B-1-10 IS NOT PREFERRED AND MAY ONLY BE USED WITH PERMISSION FROM THE ENGINEER.

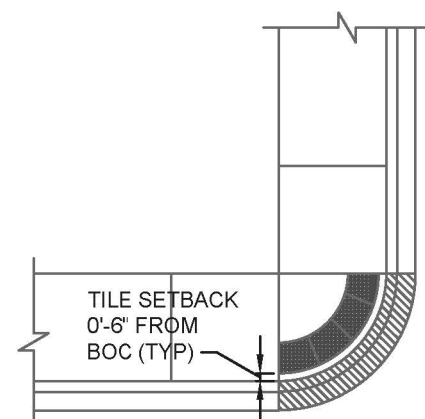
WHERE APPLICABLE, RADIAL TILES MAY BE REQUIRED. IF USING RADIAL TILES, THE CONTRACTOR SHALL VERIFY THAT THE CURB RADIUS MATCHES THE AVAILABLE TILE RADII WITH THE TILE MANUFACTURER. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN THE FIELD.



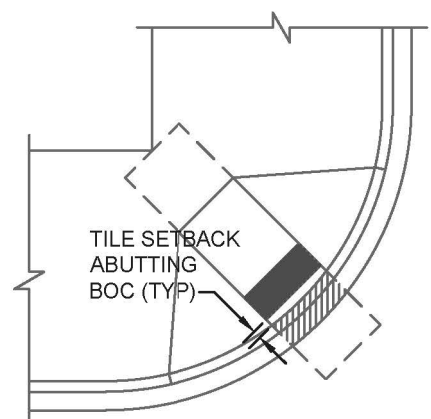
LAYOUT B-1-2: 2 PERPENDICULAR RAMPS



LAYOUT B-1-3: 2 RAMPS IN RADIUS



LAYOUT B-1-7: BLENDED TRANSITION



LAYOUT B-1-10: SHARED PERPENDICULAR RAMP AT CORNER

FILE NAME = 29 Sidewalk Construction Details.dgn



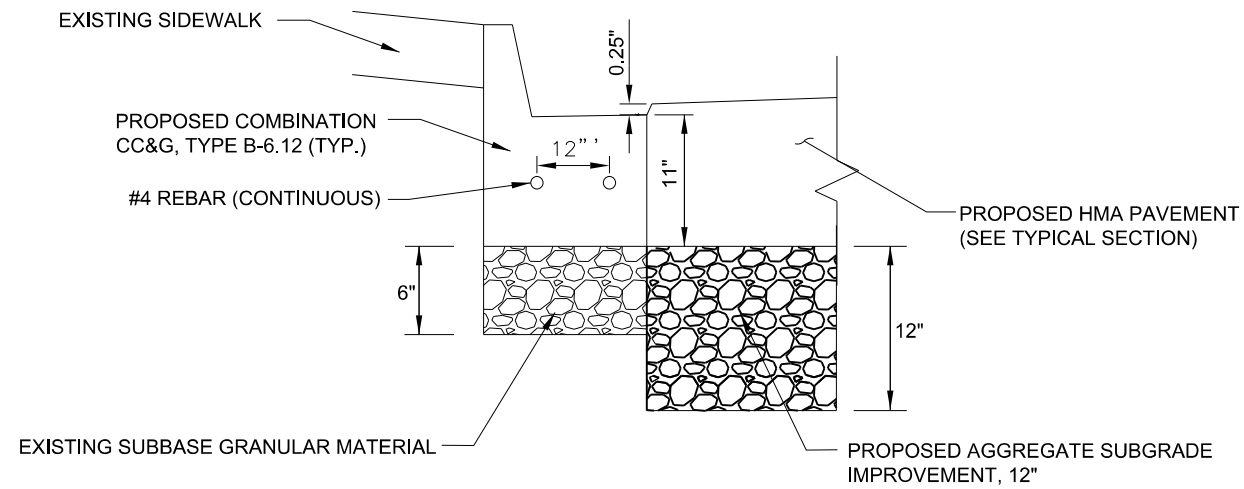
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	DRAWN -	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/21/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	29
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

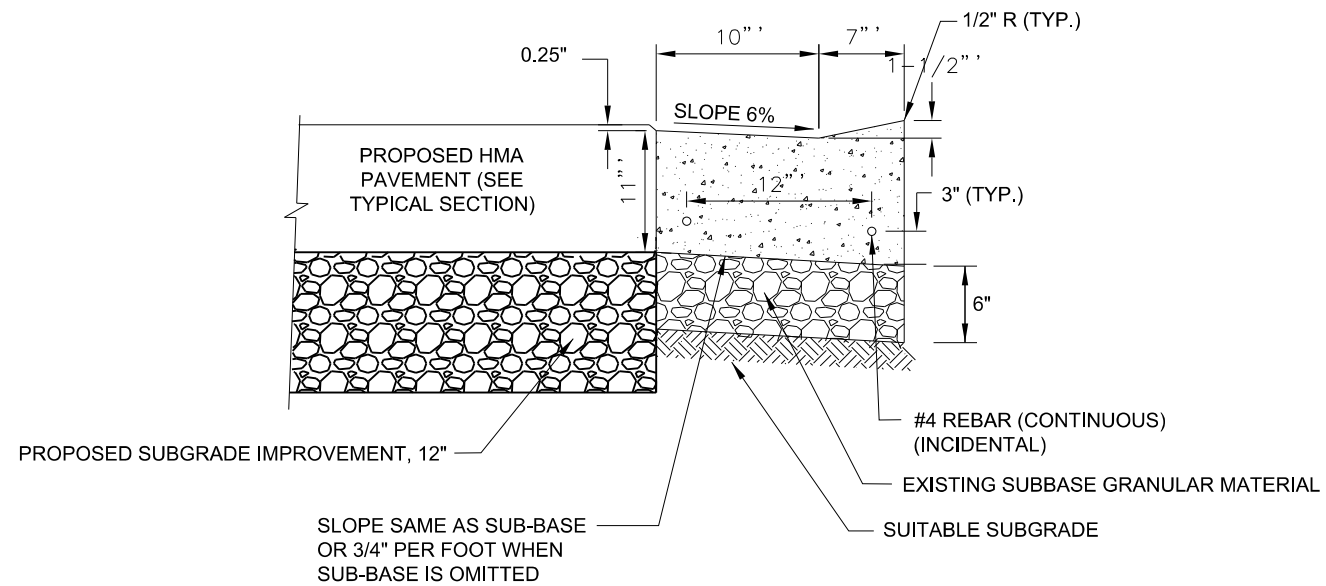


DETAIL A: COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

NOT TO SCALE

COMBINATION CONCRETE CURB AND GUTTER REQUIREMENTS

1. TWO NO. 4 REINFORCEMENT BARS SHALL BE INSTALLED CONTINUOUSLY IN ALL CURB AND GUTTER.
2. WHEN CURB AND GUTTER IS CONSTRUCTED ADJACENT TO EXISTING PAVEMENT, THE VERTICAL THICKNESS OF THE GUTTER FLAG SHALL BE 9" OR EQUAL TO THE THICKNESS OF THE ADJACENT PAVEMENT, WHICHEVER IS GREATER. ALSO, TIE BARS SHALL BE OMITTED.
3. DRAINAGE OPENINGS - AT ALL LOCATIONS WHERE METAL CASTINGS ARE TO BE INCORPORATED IN THE CURB AND GUTTER, A 1" THICK PREFORMED JOINT FILLER, CONFORMING TO THE CROSS SECTIONS OF THE CURB AND GUTTER, SHALL BE INSTALLED IN THE CURB AND GUTTER A DISTANCE OF 5FT FROM EACH SIDE OF THE METAL CASTING.
4. TRANSITIONS - THE TRANSITION FROM FULL HEIGHT CURB TO DEPRESSED CURB SHALL BE MADE AT THE RATE OF 2" PER FOOT OF LENGTH OR FLATTER.
5. JOINTS - IN ADDITION TO THE REQUIREMENT OF HIGHWAY STANDARD 606001 AND ARTICLE 606.06 OF THE STANDARD SPECIFICATIONS, JOINTS SHALL BE CONSTRUCTED AS FOLLOWS:
 CONSTRUCTION JOINTS AND EXPANSION JOINTS SHALL BE INSTALLED IN THE CURB AND GUTTER IN PROLONGATION WITH JOINTS IN ADJACENT P.C.C. PAVEMENT OR BASE COURSE.
 ALL EXPANSION JOINTS SHALL BE PROVIDED WITH A 1 1/4" DIA. X 18" COATED SMOOTH DOWEL BAR CONFORMING TO ARTICLE 1006.11(b) OF THE STANDARD SPECIFICATIONS. THE DOWEL BAR SHALL BE FITTED WITH A CAP HAVING A PINCHED STOP THAT WILL PROVIDE 1" OF EXPANSION.



NOTES:

1. THE TOP OF CURB SHALL BE DEPRESSED WHERE THE CURB AND GUTTER IS CONSTRUCTED ACROSS ALLEYS, DRIVEWAY, AND SIDEWALKS AS DETAILED IN THE PLANS OR WHERE DIRECTED BY THE ENGINEER OR PUBLIC WORKS DEPARTMENT.

DETAIL B: DEPRESSED CURB FOR INTERSECTIONS

NOT TO SCALE

FILE NAME = 30 Construction Details.dgn



USER NAME = DonN	DESIGNED -	REVISED -
PLOT DATE = 12/21/2018	DRAWN -	REVISED -
	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CONSTRUCTION DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV		44	30
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

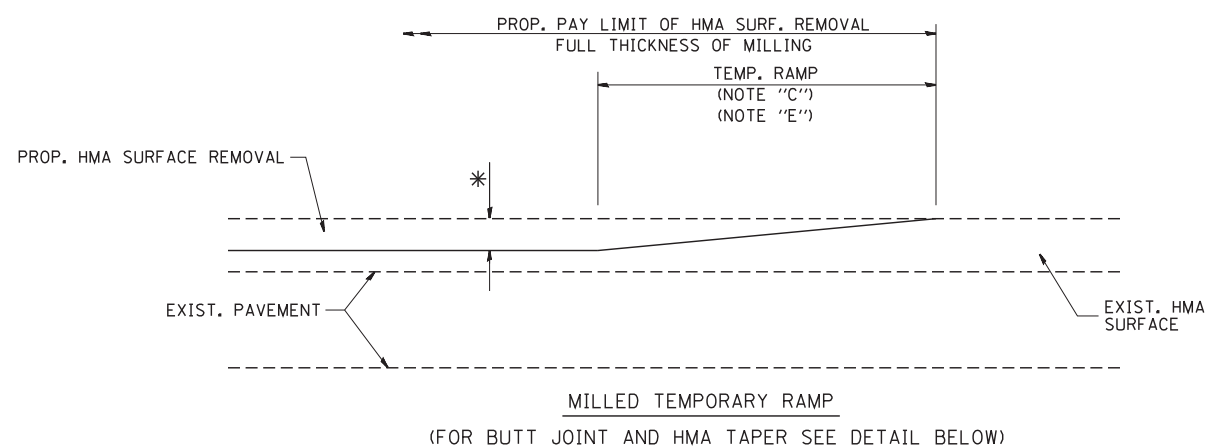
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01
	PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

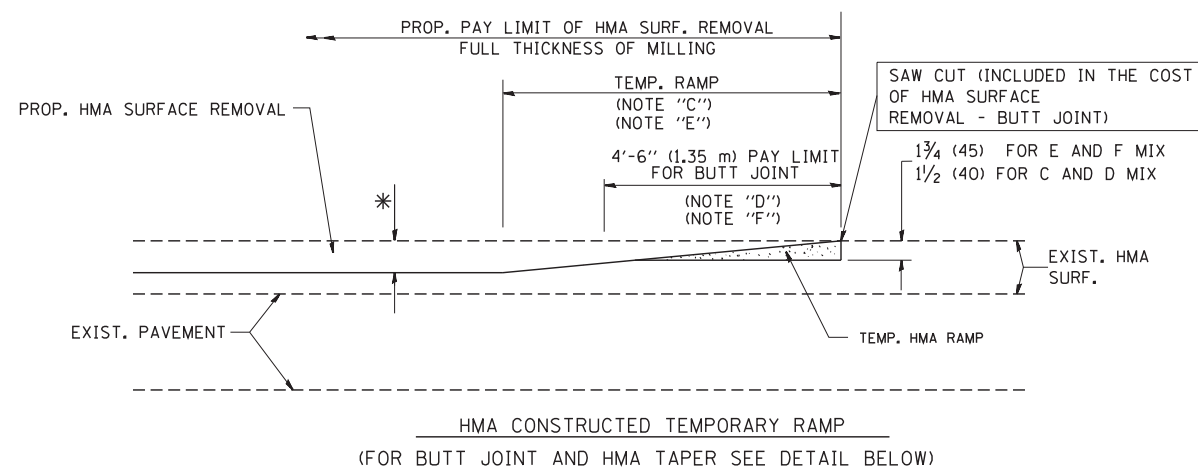
**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RE. 1397	SECTION 16-00079-00-PV	COUNTY DuPage	TOTAL SHEETS 44	SHEET NO. 31
BD600-06 (BD-24)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

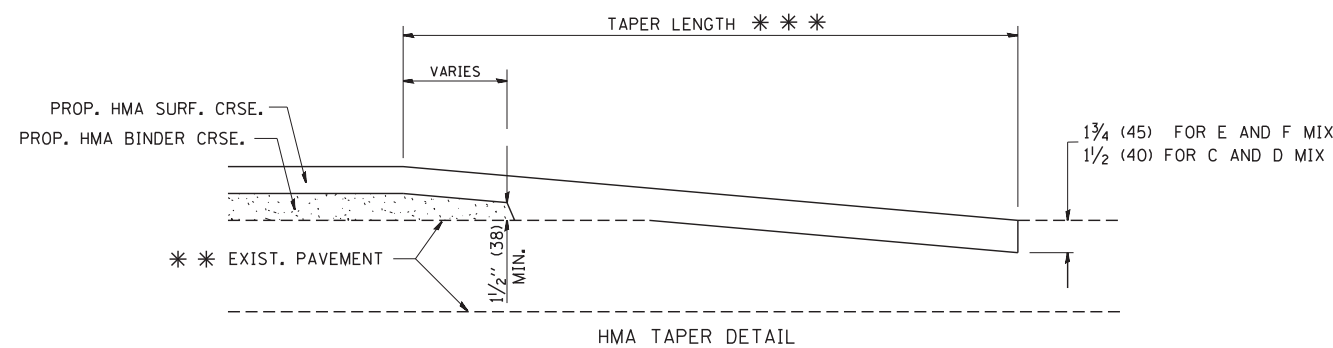
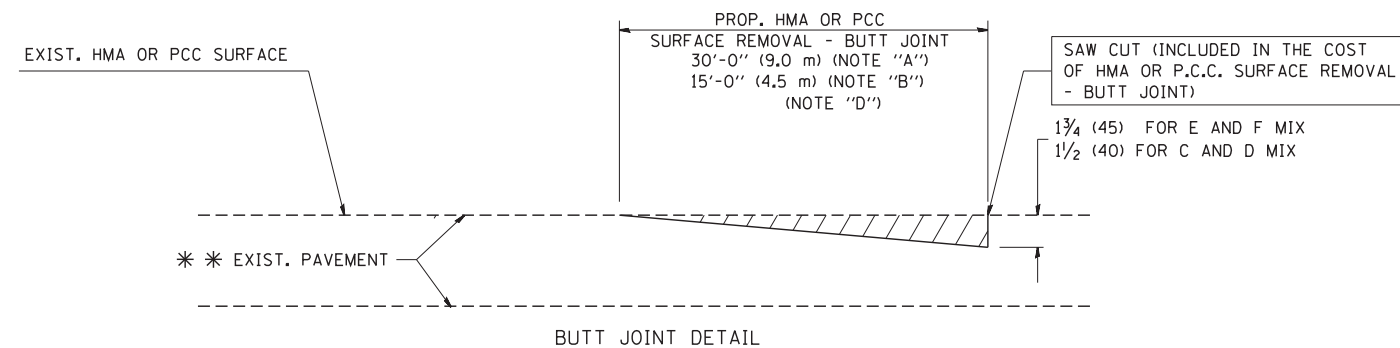


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

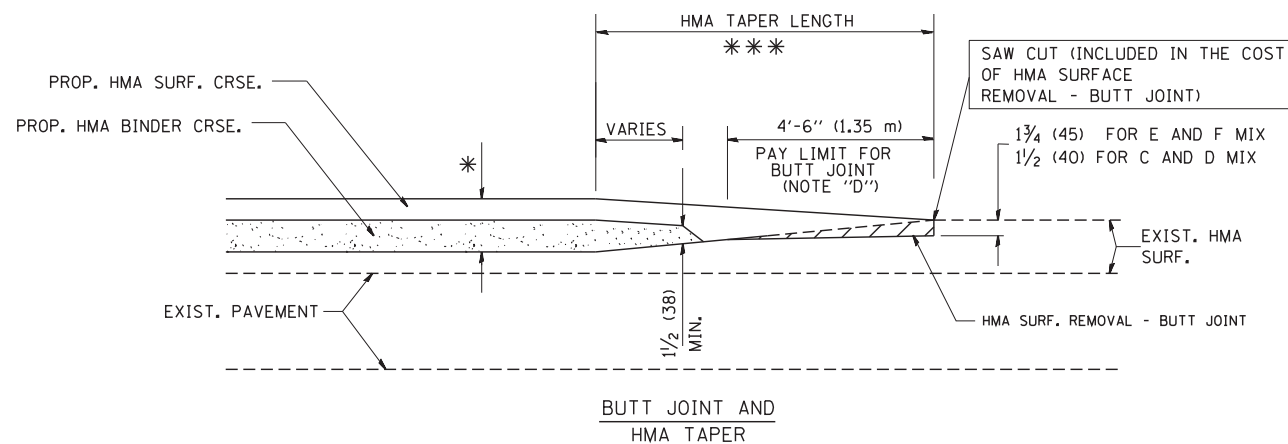
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

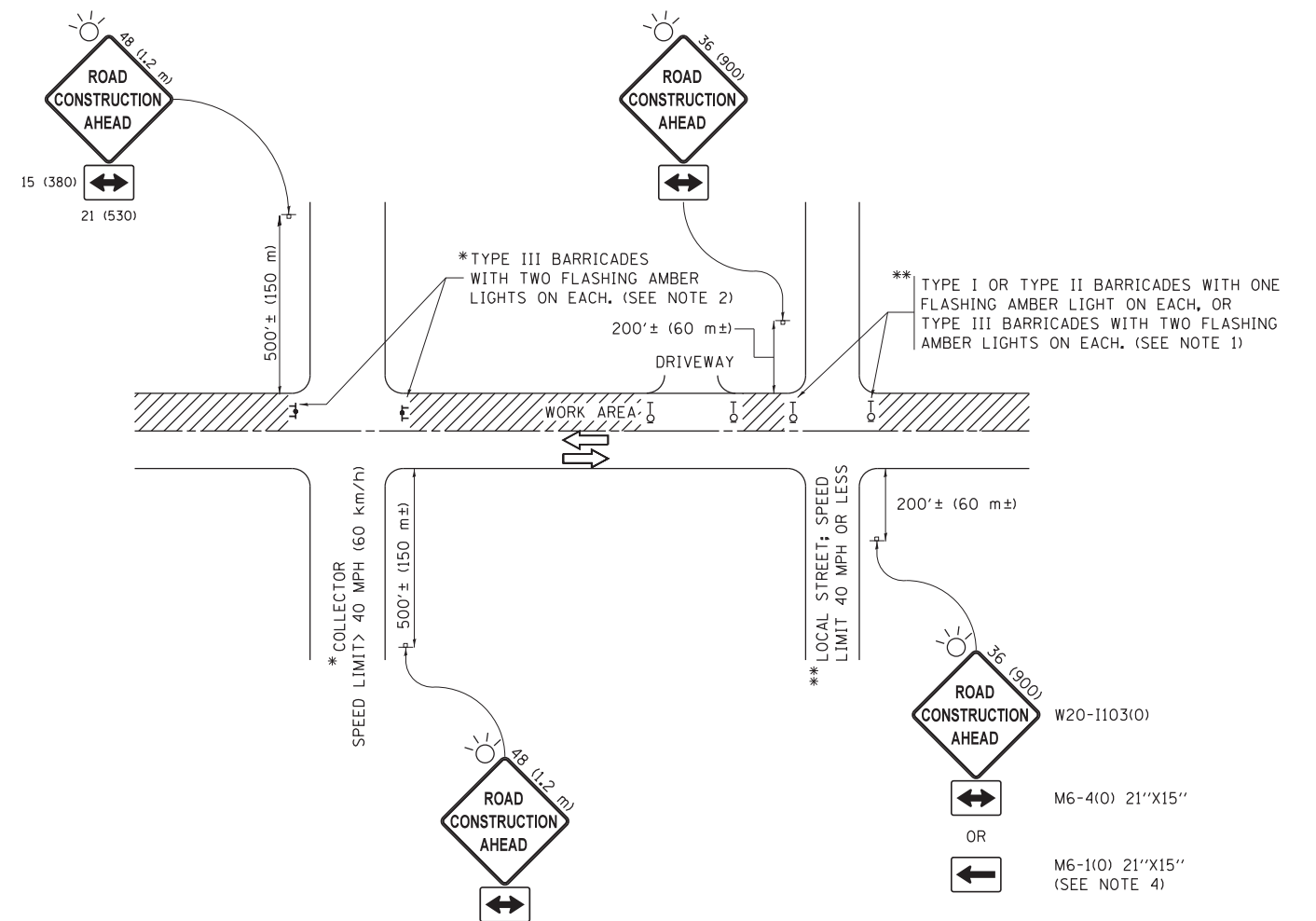
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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DuPage	44	32
BD400-05 BD32		CONTRACT NO. 61F62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

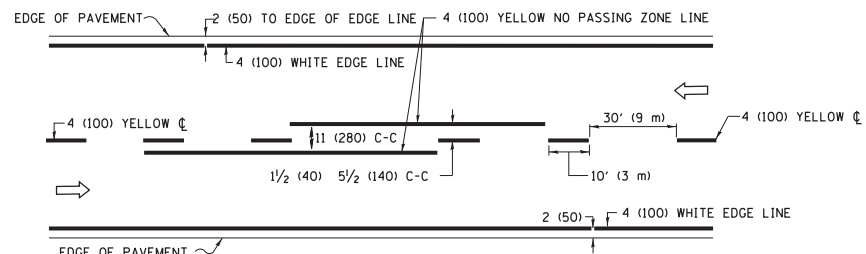
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	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

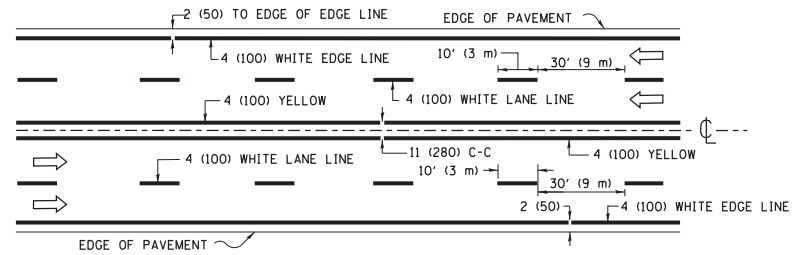
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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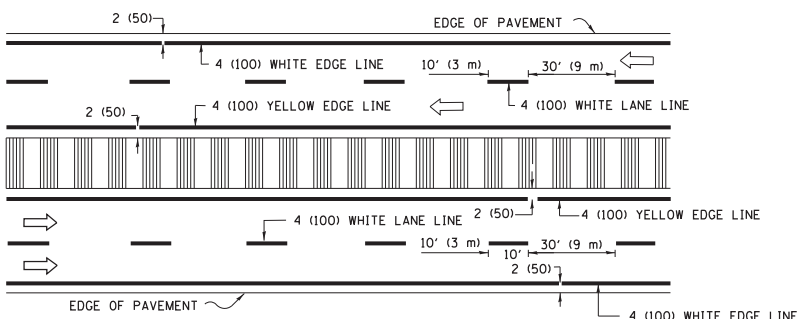
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1397	16-00079-00-PV	DuPage	44	33
TC-10			CONTRACT NO. 61F62	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

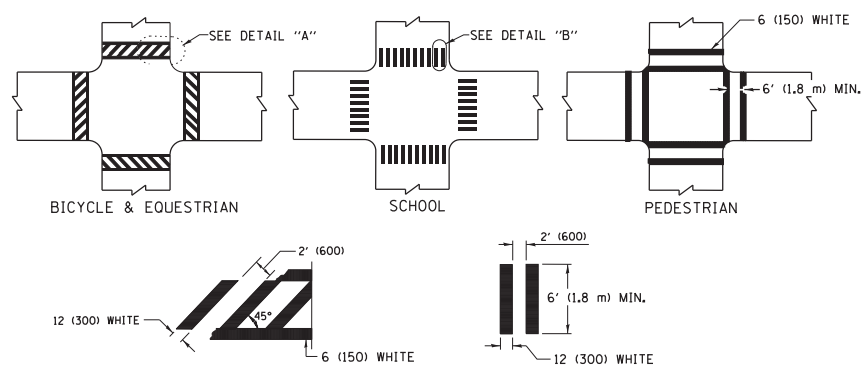


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

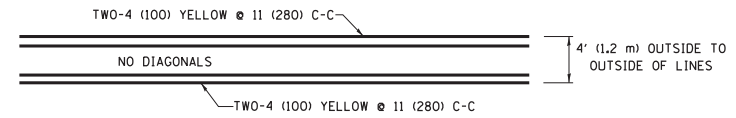


DETAIL "A"

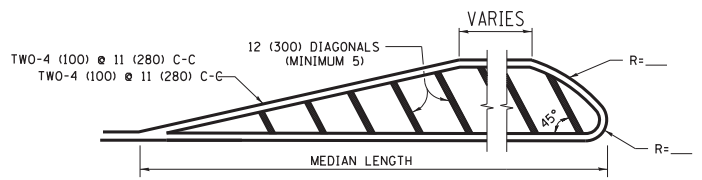
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

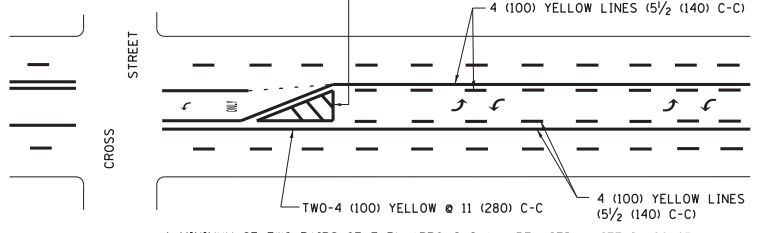


4' (1.2 m) WIDE MEDIANS ONLY

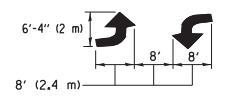


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

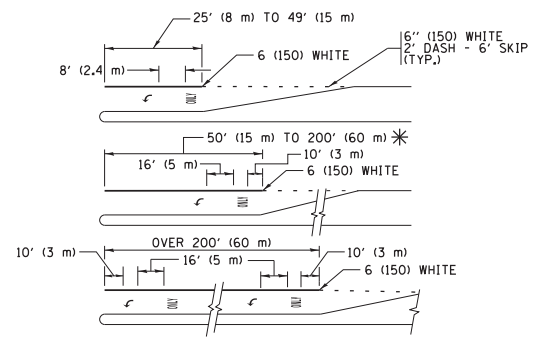


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

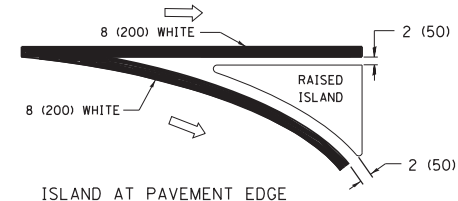
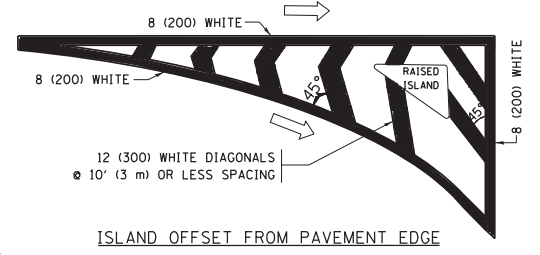


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

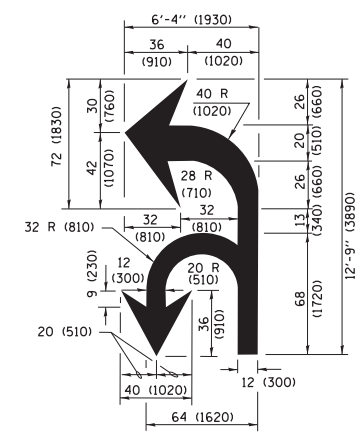
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

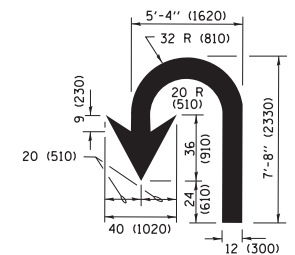
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

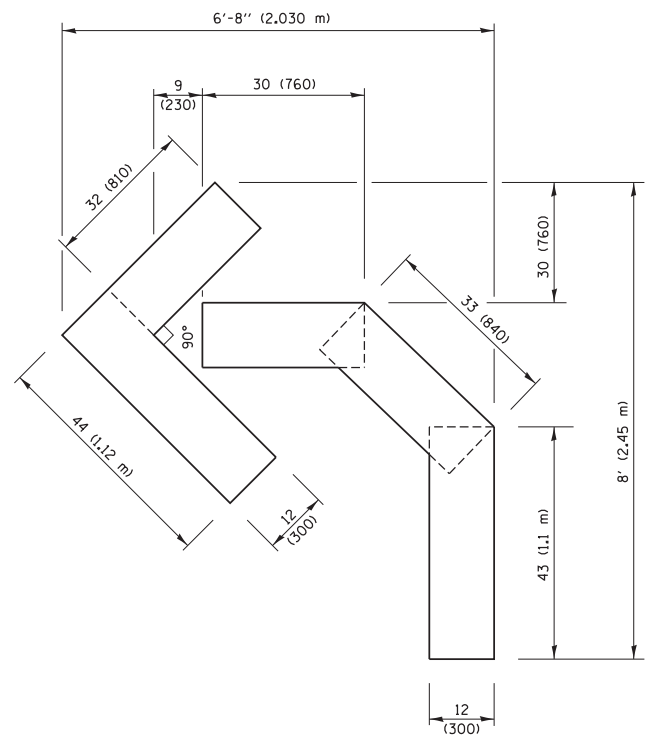
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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

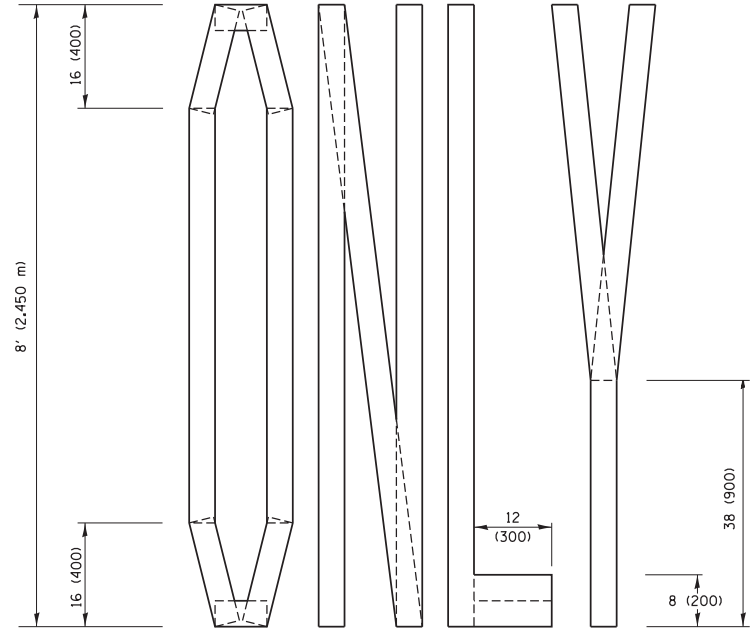
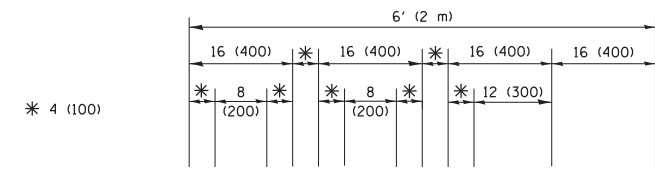
DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

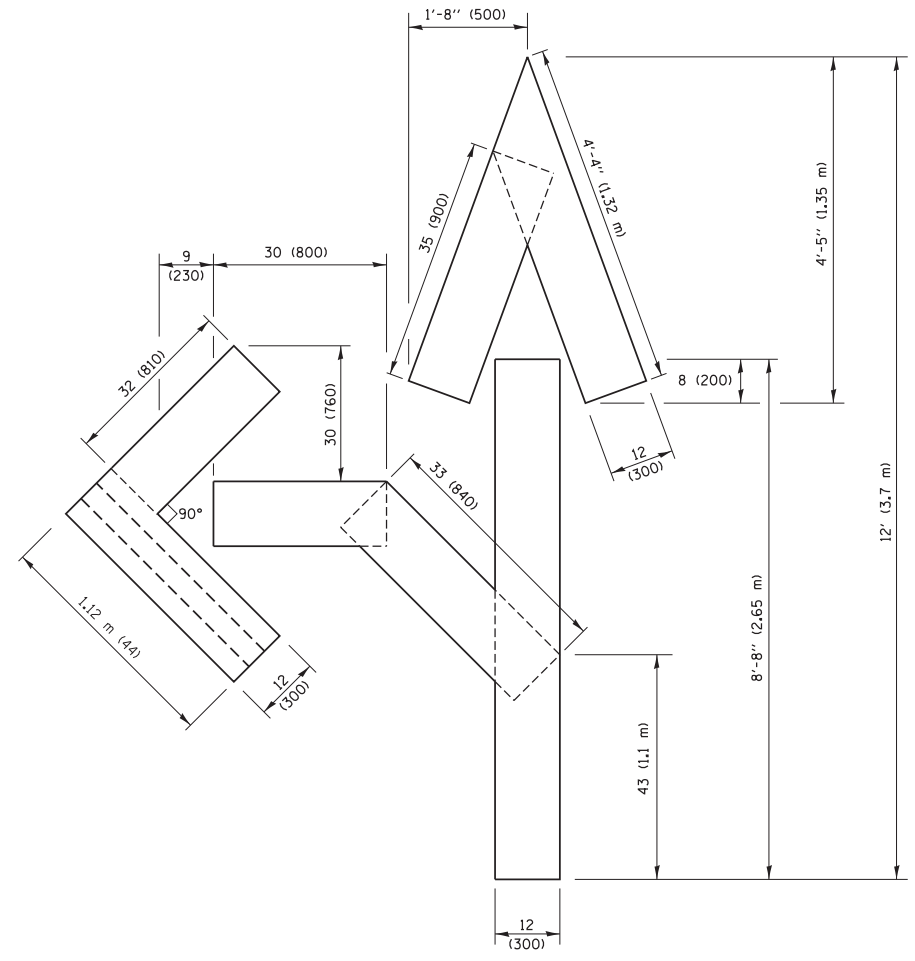
F.A.U R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DuPage	44	34
	TC-13	CONTRACT NO.	61F62	
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

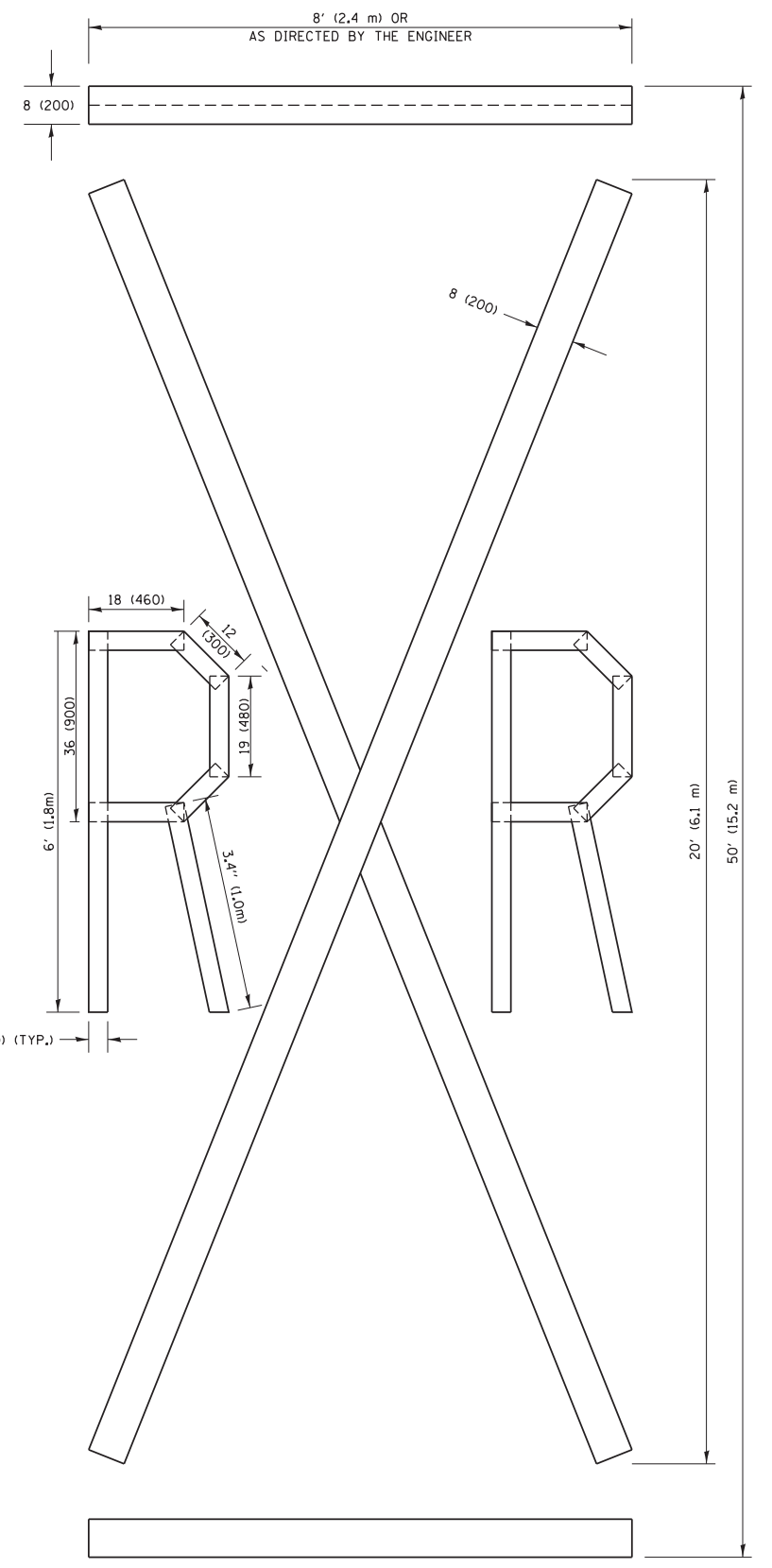


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
 IN LINEAR FEET OF 4" LINES TO MATCH THE
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters)
 unless otherwise shown.


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		DATE -	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16


**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**


SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DuPage	44	35
TC-16		CONTRACT NO.	61F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				






ROUTE MARKERS

 FOR U.S. ROUTES
M1-40-2424






 FOR ILLINOIS ROUTES
M1-50-2424

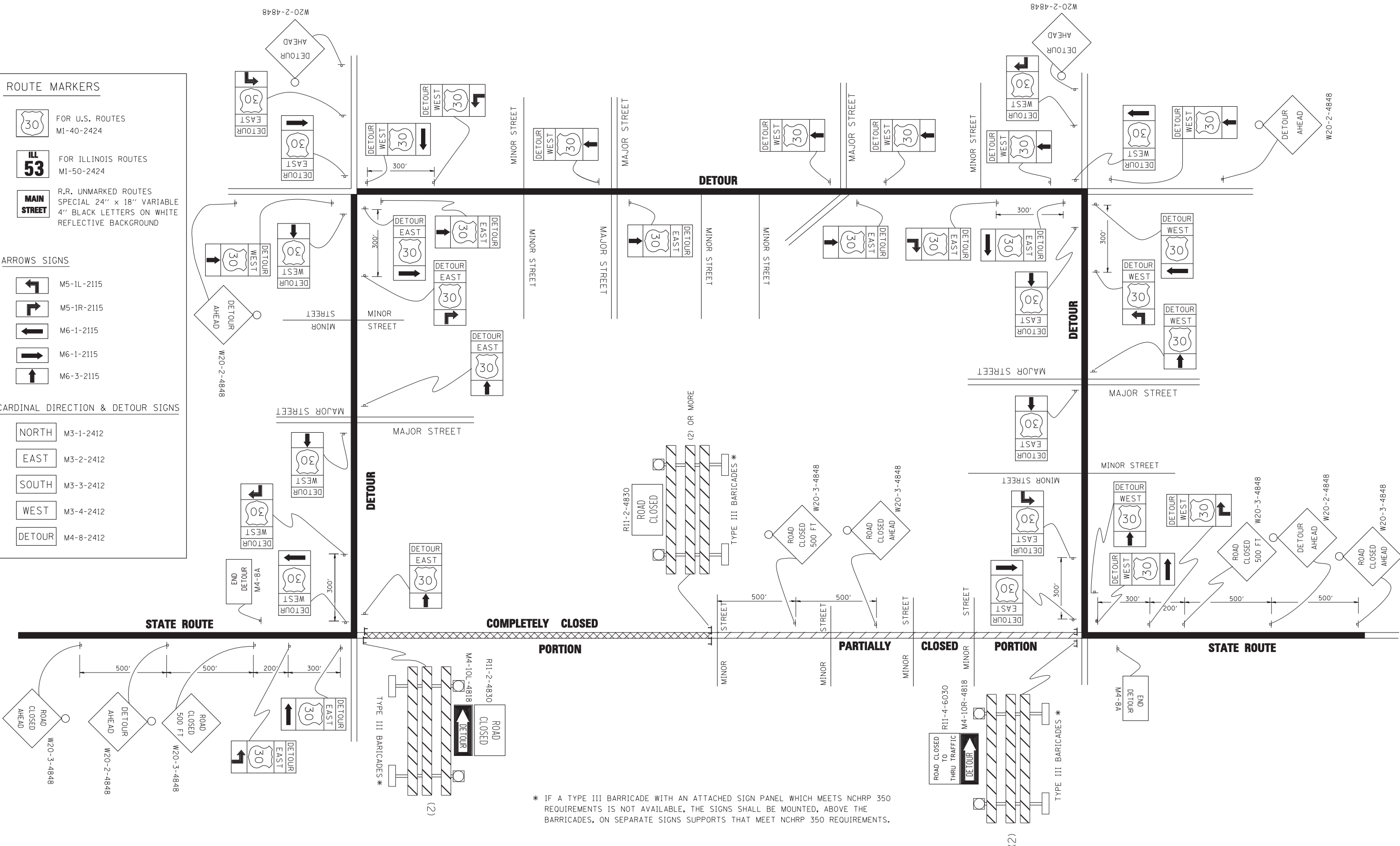
 R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

ARROWS SIGNS

-  M5-1L-2115
-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

CARDINAL DIRECTION & DETOUR SIGNS

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

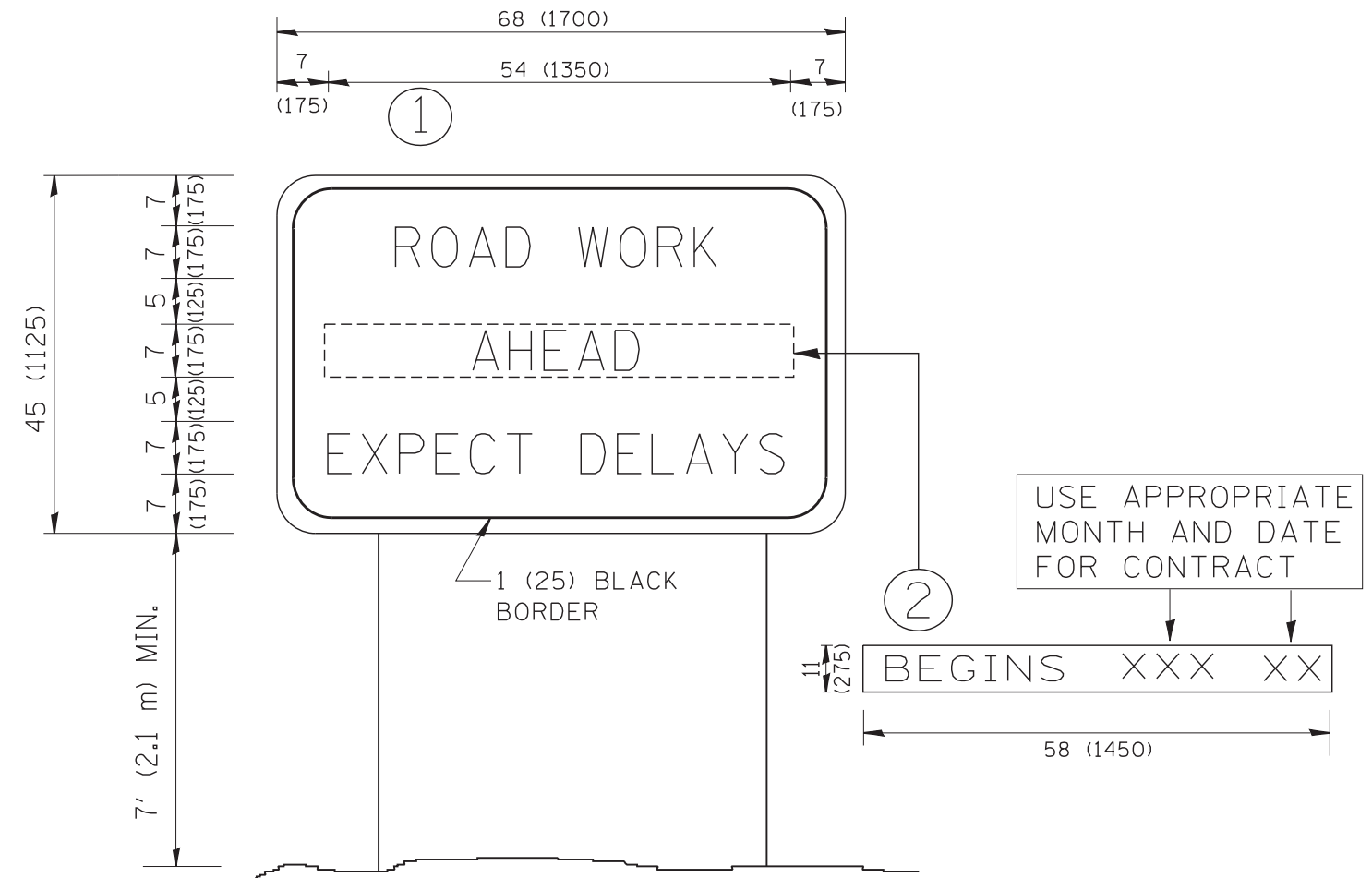
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DuPage	44	36
TC-21			CONTRACT NO. 61F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

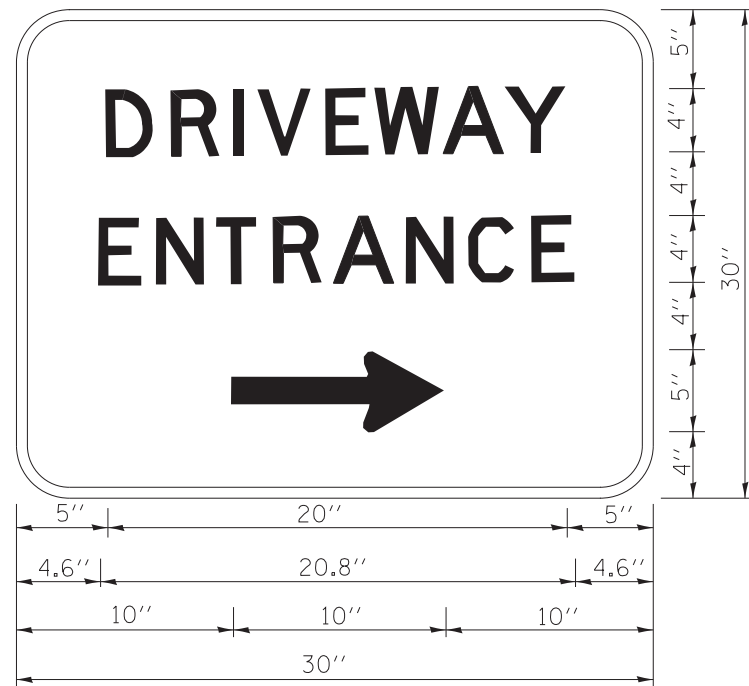
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		DESIGNED - CHECKED -	REVISED - REVISED -
		DESIGNED - DATE -	REVISED - REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 1397	SECTION 16-00079-00-PV	COUNTY DuPage	TOTAL SHEETS 44	SHEET NO. 37
TC-22			CONTRACT NO. 61F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gegl1enobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ct:\pwork\pwork\gagl1enobt\d0108315\to26.dgn		DRAWN -	REVISED -
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	PLOT DATE = 12/13/2012	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DuPage	44	38
TC-26			CONTRACT NO. 61F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

BY		DATE	
FINAL SURVEY	SURVEYED		
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		
	NO.		

BY		DATE	
ORIGINAL SURVEY	SURVEYED		
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		
	NO.		

FILE NAME = 39:44 sht-ash-190



USER NAME = DonN
DESIGNED -
DRAWN -
PLOT SCALE = 20.0000' / in.
CHECKED -
DATE - 12/21/2018

REVISER -
REVISION -
REVISION -
REVISION -
REVISION -

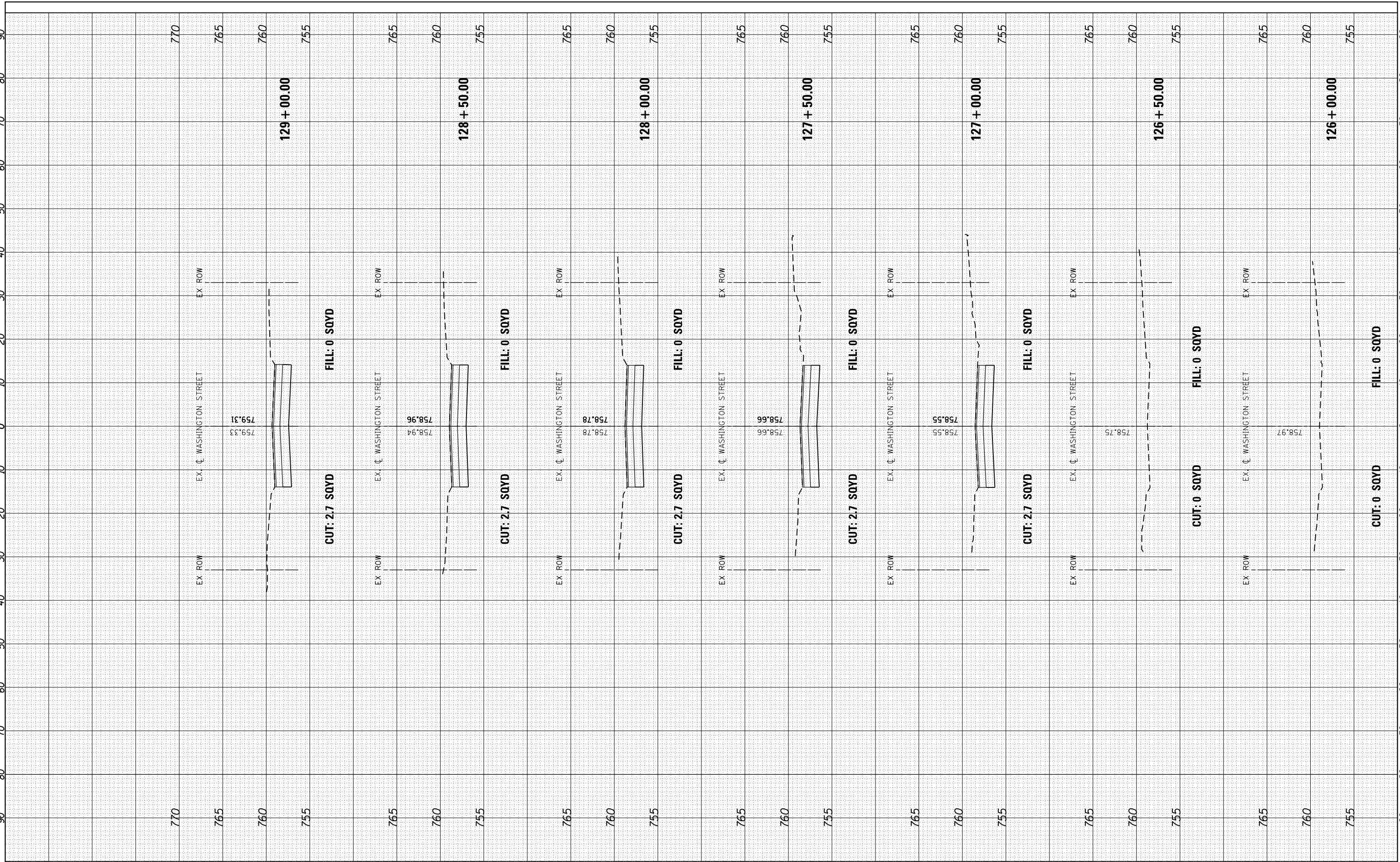
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
WASHINGTON STREET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	39
CONTRACT NO. 61F62				

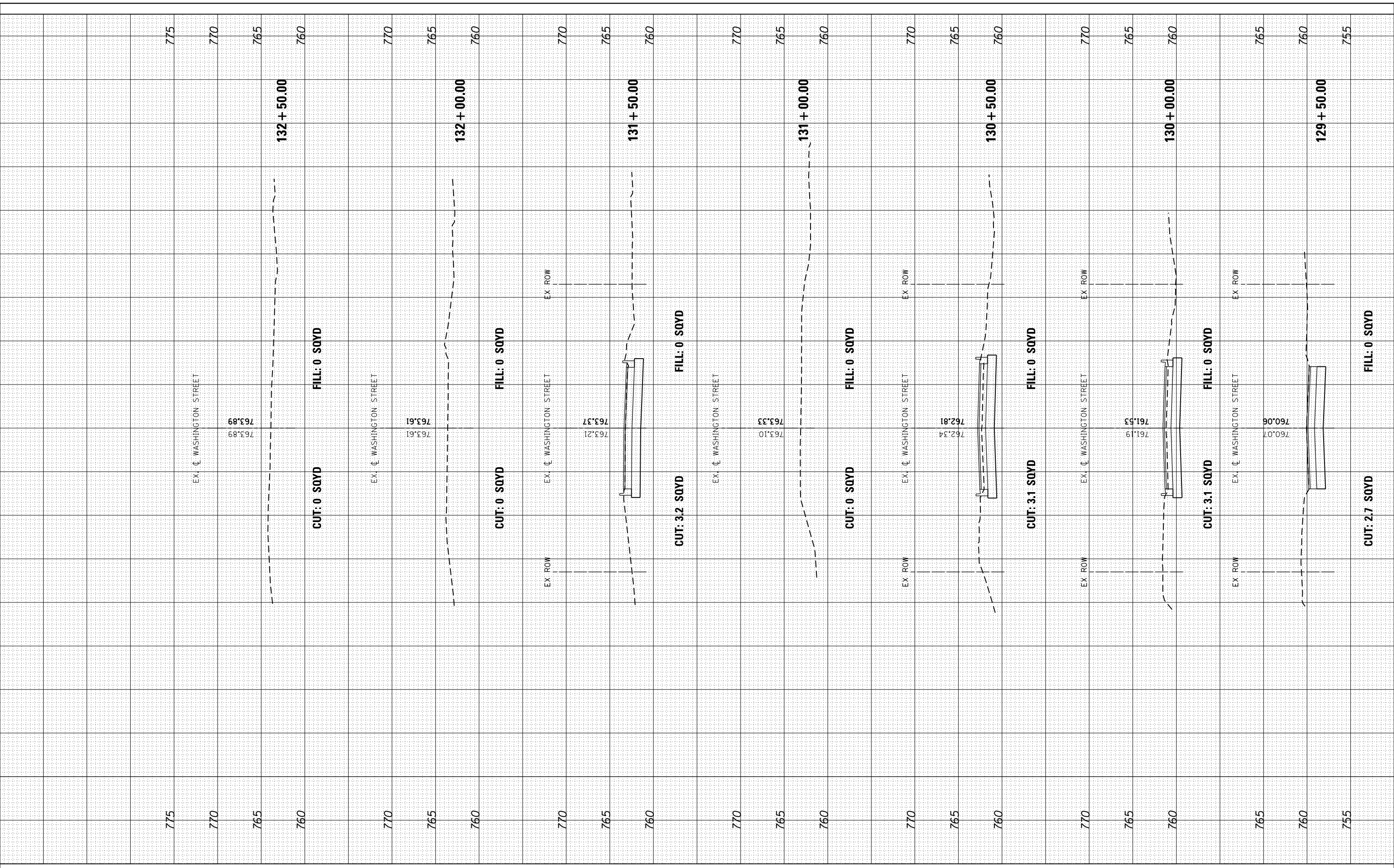
ILLINOIS FED. AID PROJECT



FINL SURVEY	SURVEYED	BY	DATE
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NO.	AREAS CHECKED		
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ORIGINAL SURVEY	SURVEYED	BY	DATE
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	AREAS CHECKED		

FILE NAME = 39-44 sht-ash-90



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PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS			
WASHINGTON STREET			
SCALE:	SHEET	OF SHEETS	STA. TO STA.
			129+50.00 132+50.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	40
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

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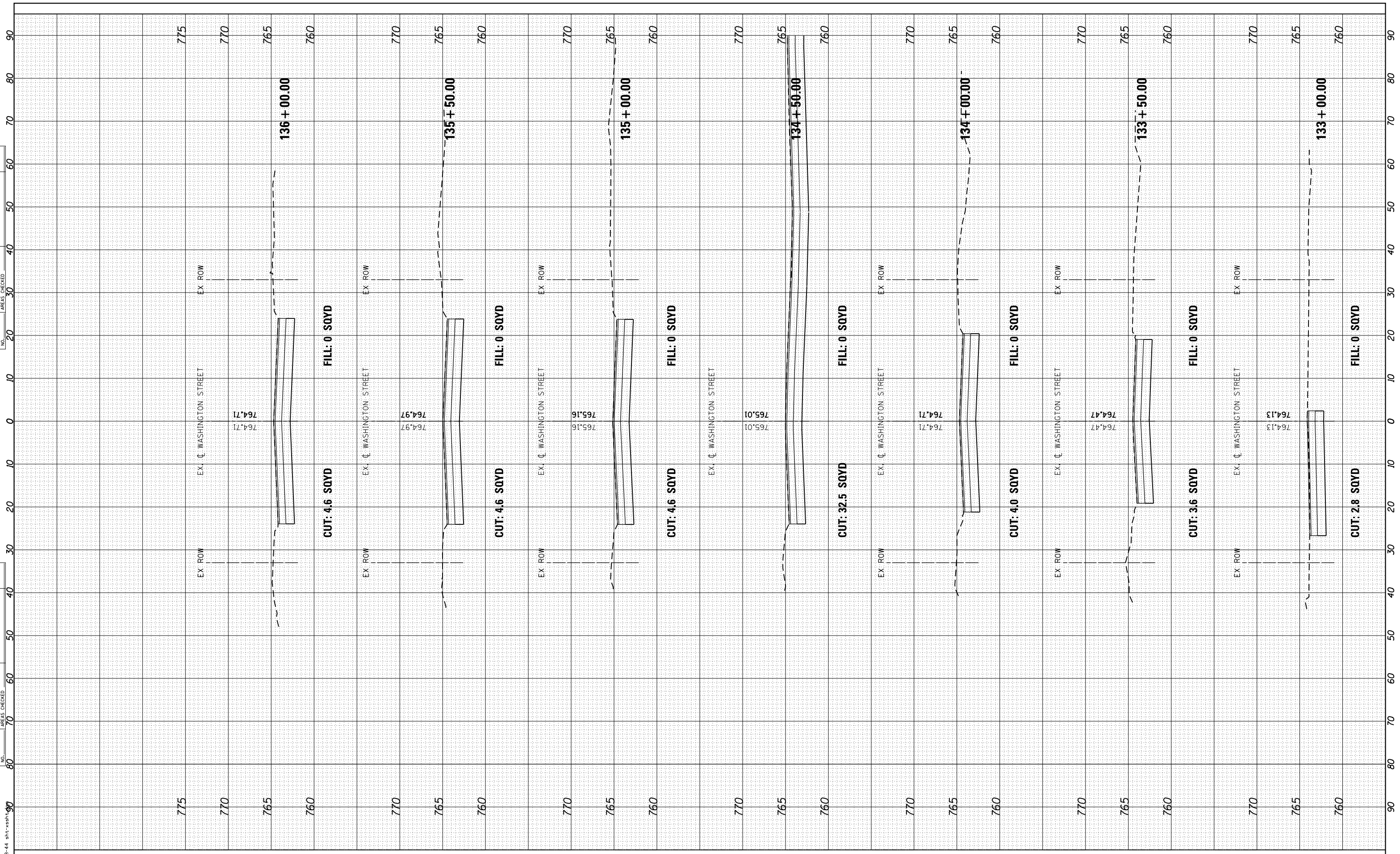


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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

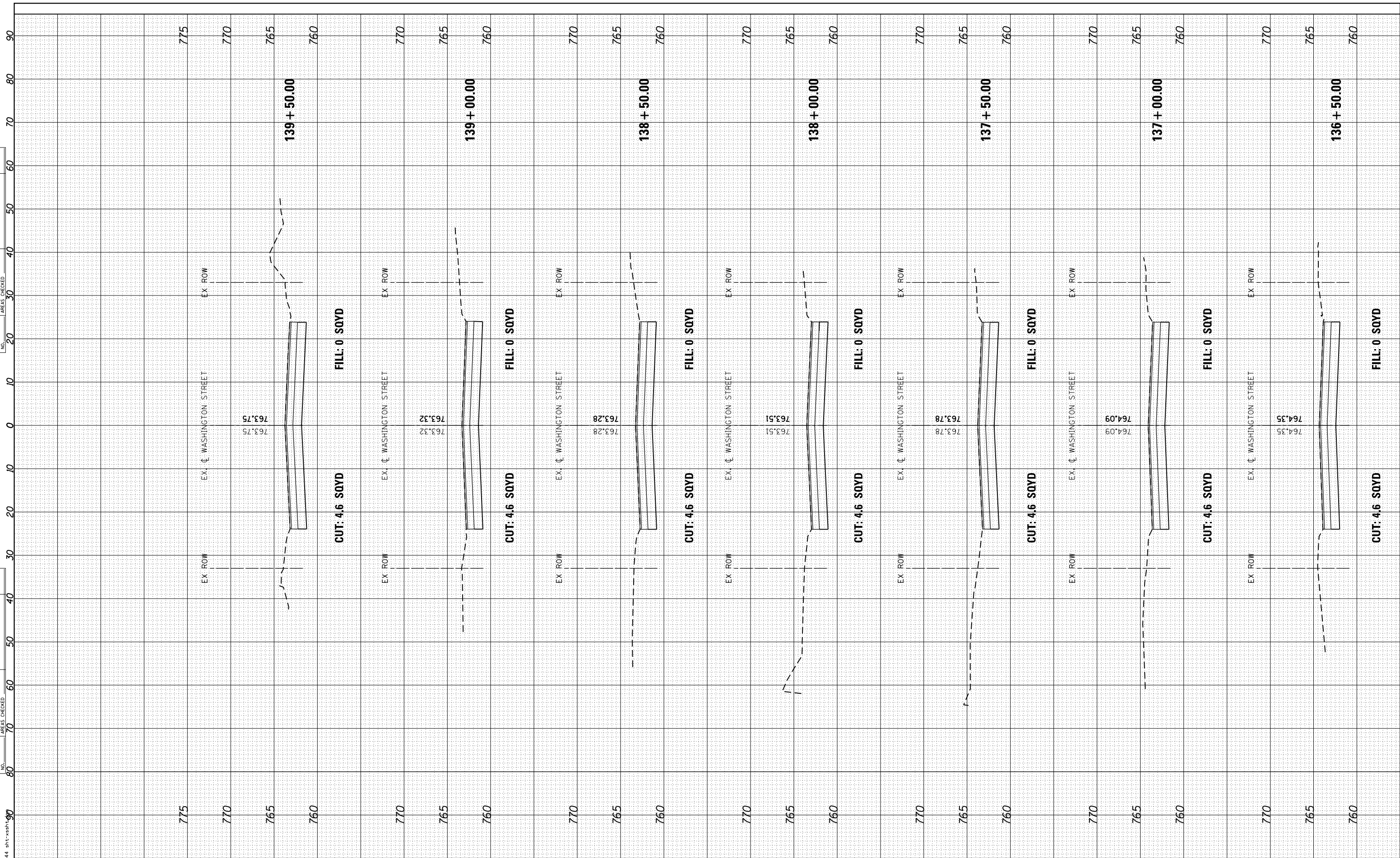
CROSS SECTIONS			
WASHINGTON STREET			
SCALE:		SHEET OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	41
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				



BY	DATE

BY	DATE



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS WASHINGTON STREET	
SCALE:	SHEET OF SHEETS STA. TO STA.

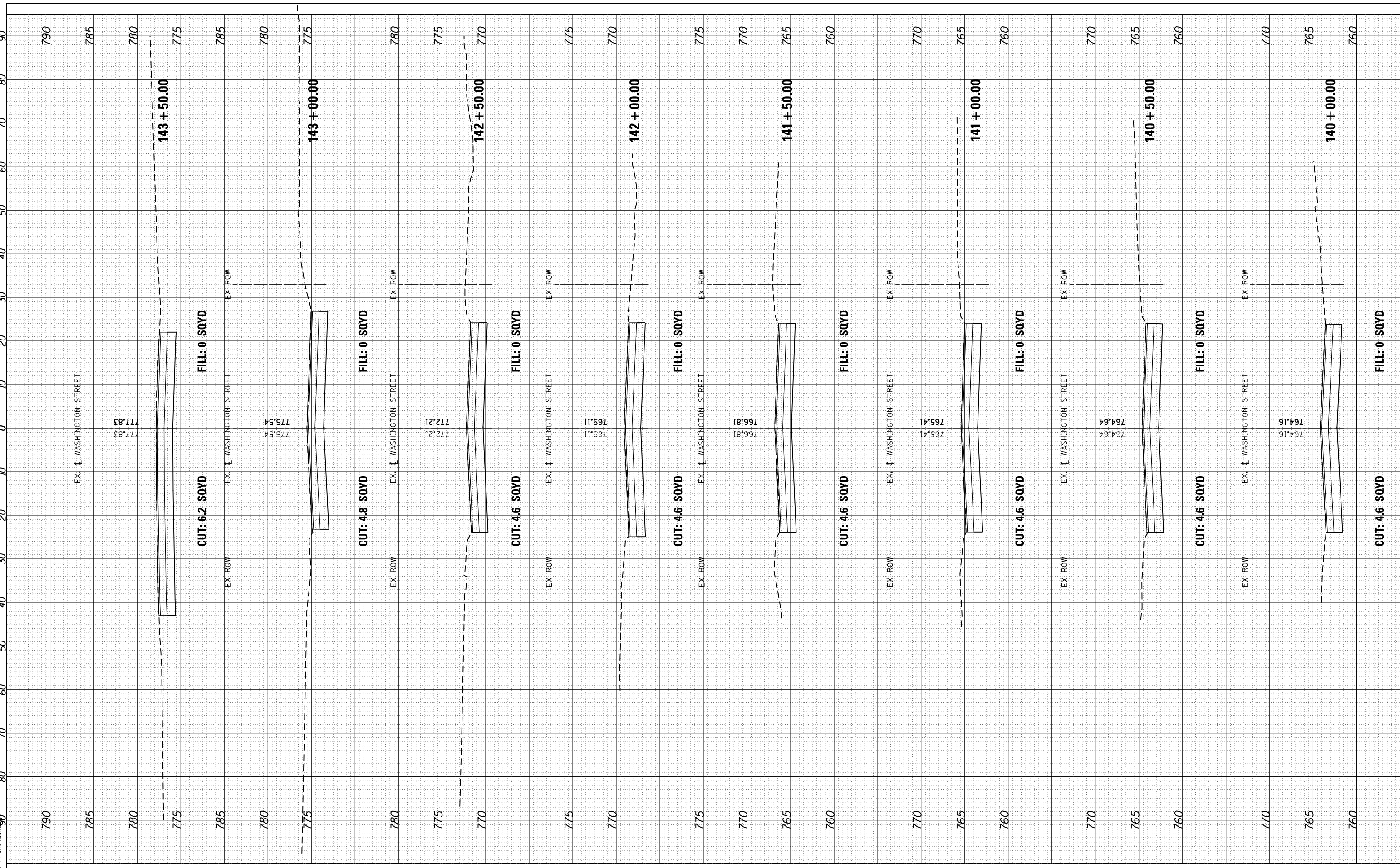
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1397	16-00079-00-PV	DUPAGE	44	42
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

FILE NAME = 39:44 sht-ash-139

BY	DATE

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
	TEMPLATE		
	AREAS		
	CHECKED		

FILE NAME = 39:44 sht-ash-90



USER NAME = DonN
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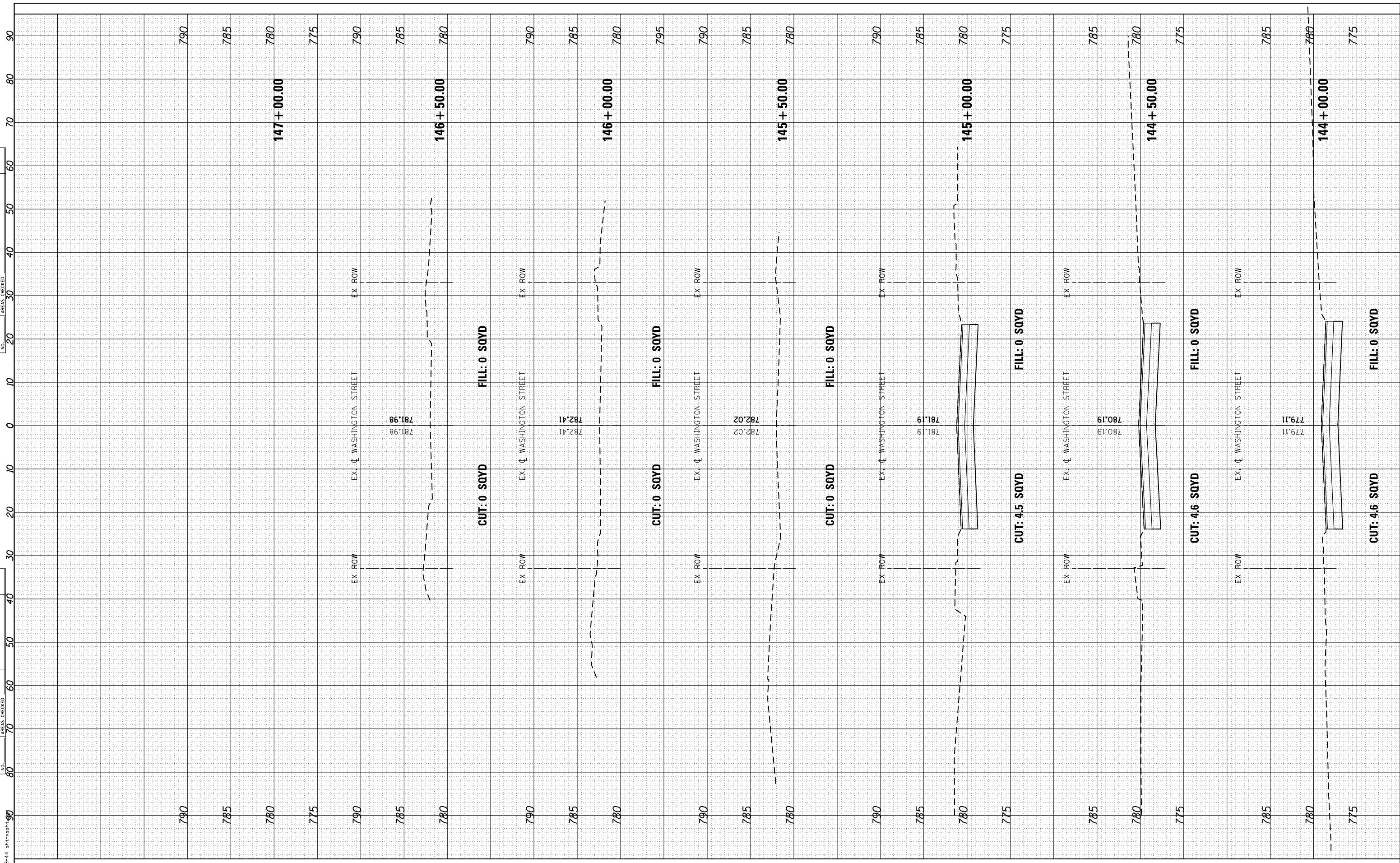
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
WASHINGTON STREET
 SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	43
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	
NO.	TEMPLATE	
	AREAS	
	CHECKED	



USER NAME = DonN	DESIGNED -	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/21/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS	
WASHINGTON STREET	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1397	16-00079-00-PV	DUPAGE	44	44
CONTRACT NO. 61F62				
ILLINOIS FED. AID PROJECT				