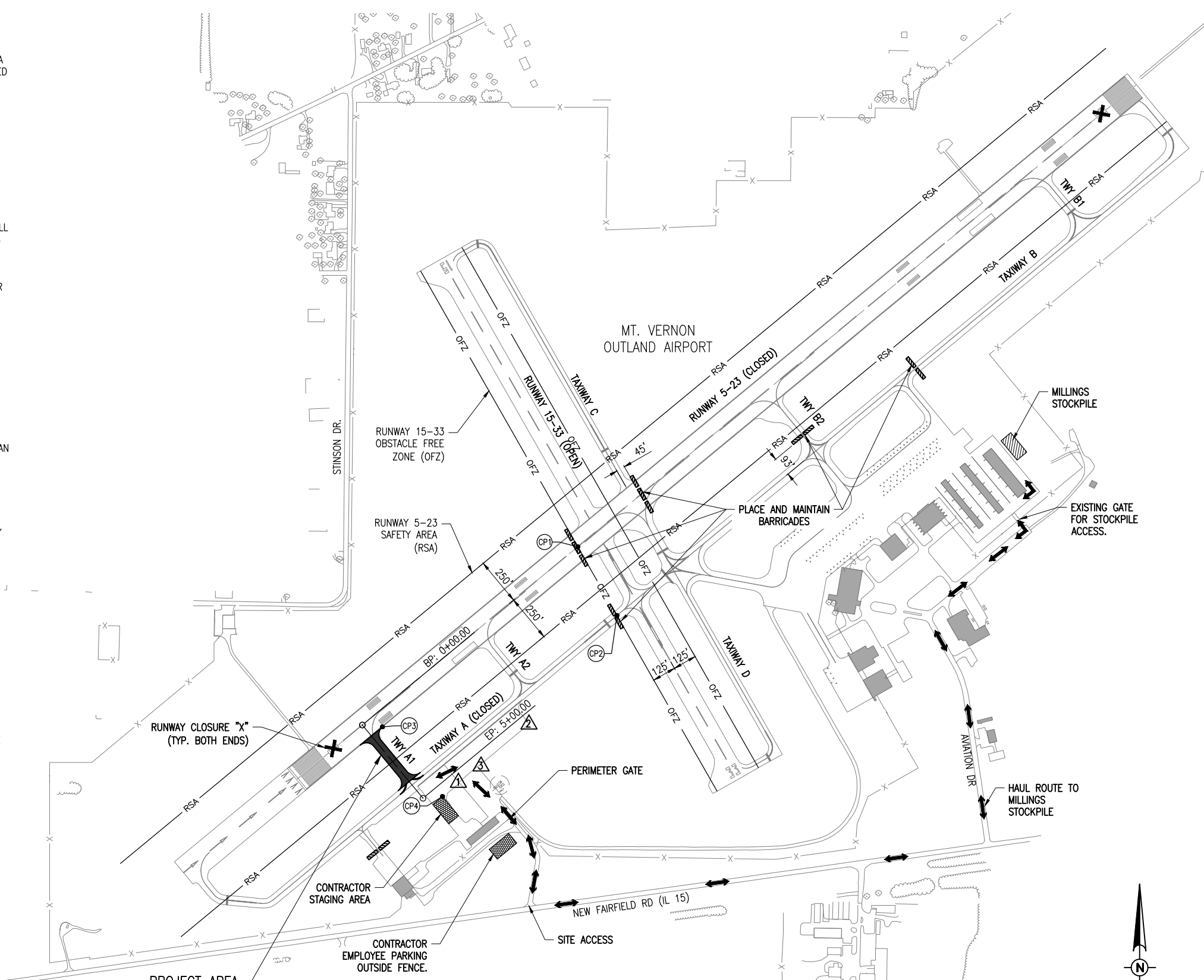


CONSTRUCTION SAFETY PLAN

- SCOPE OF WORK** – THE PROPOSED WORK WILL INCLUDE THE HMA MILLING, RESURFACING AND REMARKING OF TAXIWAY A1.
- GENERAL** – THE MT. VERNON OUTLAND AIRPORT IS A NON-TOWER CONTROLLED FAA PART 139 AIRPORT. IT IS COMPRISED OF TWO PAVED RUNWAYS AND THE ASSOCIATED TAXIWAY SYSTEM. THE PROPOSED CONSTRUCTION WILL NECESSITATE THE CLOSURE OF THE PRIMARY RUNWAY 5-23 FOR THE DURATION OF THE PROJECT. RUNWAY 15-33 SHALL REMAIN OPEN AT ALL TIMES.
- THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES/DRUMS AND SIGNS AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
- EXISTING AIRFIELD AIRPORT PAVEMENTS SHALL BE USED FOR THE CONSTRUCTION HAUL ROUTE AND STAGING AREA. AREAS SHALL BE PROTECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY, AND ANY DAMAGE REPAIRED AT THEIR EXPENSE.
- AIRFIELD SAFETY ASSURANCE** – AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FORTH IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR CURRENT ADDITION WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION FROM ENTERING THE AIR OPERATIONS AREA ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE). THE CONTRACTOR SHALL ALSO PROVIDE WORKERS WITH SOME TYPE OF TAG OR GARMENT TO IDENTIFY THE PERSON AS BEING PART OF THE CONSTRUCTION CREW.
- THE CONTRACTOR SHALL PROVIDE A SCHEDULE AT THE PRECONSTRUCTION MEETING DETAILING THE ANTICIPATED RUNWAY CLOSURE DATES AND DURATIONS. THE CONTRACTOR SHALL ALSO NOTIFY THE SPONSOR AND RESIDENT ENGINEER/TECHNICIAN A MINIMUM OF 10 DAYS PRIOR TO THE DESIRED CLOSURE DATE TO ALLOW FOR COORDINATION WITH THE FAA REGARDING DEACTIVATION OF FAA-OWNED NAV-AIDS.
- AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE DISRUPTION TO AIRPORT TRAFFIC.
- SAFETY PLAN COMPLIANCE DOCUMENT** – THE CONTRACTOR SHALL HAVE THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD), AS DETAILED IN THE SPECIAL PROVISIONS, SUBMITTED AND APPROVED PRIOR TO BEING ISSUED THE "NOTICE TO PROCEED".
- AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. ONLY CONTRACTOR EMPLOYEES SHALL BE ALLOWED WITHIN THE PROJECT LIMITS. GATES SHALL BE CLOSED AT ALL TIMES UNLESS THE CONTRACTOR IS IN A CONTINUOUS HAULING OPERATIONS, DURING WHICH TIME HE WILL PROVIDE A PERSON TO MONITOR THE GATE AREA.
- RADIO CONTROL** – THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT WITH THE AIRPORT UNICOM (123.00 MHz).
- ANOTHER CONTRACT (MV064-JAN. 18 IDOT LETTING) MAY BE WORKING ON THE AIRPORT AND AT THE SAME TIME AS THIS CONTRACT. COORDINATION BETWEEN THE CONTRACTS IS MANDATORY. NO TIME EXTENSIONS OR CHANGE ORDERS WILL BE PROCESSED DUE TO LACK OF COORDINATION BETWEEN CONTRACTS. ANY POTENTIAL DELAYS OR CONFLICTS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN.



HORIZONTAL AND VERTICAL CONTROL DATA				
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	MAG NAIL NW CORNER CONCRETE	832,652.45	602,132.97	463.38
2	MAG NAIL SE CORNER ASPHALT	833,023.28	602,440.11	463.73
3	CHISELED "X" ON F.H. CAP BOLT	832,772	602,221	464.66
	PROJECT ALIGNMENT STA. 5+00	832,464.53	602,049.33	
	PROJECT ALIGNMENT STA. 0+00	832,145.75	602,434.53	

SURVEY NOTES

- ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD 83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK.

CRITICAL POINTS					
POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)
CP1	BARRICADE	N038° 19' 21.185"	W088° 51' 34.445"	470	2'
CP2	BARRICADE	N038° 19' 17.626"	W088° 51' 31.838"	465	2'
CP3	CONST. EQUIP.	N038° 19' 11.766"	W088° 51' 47.263"	468	25'
CP4	STAGING	N038° 19' 08.167"	W088° 51' 43.272"	464	25'

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS

HALF SIZE SCALE: 1" = 600'
FULL SIZE SCALE: 1" = 300'

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MVA
MT. VERNON, ILLINOIS

100 AVIATION DRIVE
MT VERNON, IL 62864

EXP. 11/30/19

STATE OF ILLINOIS
BARRY S. STOLZ
062-057281
LICENSED PROFESSIONAL ENGINEER
1/11/2019

REHABILITATE TAXIWAY A1
IDA No: MVN-4684
Fed. No: 3-17-SBGP-144
Contract No. MV063

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 11, 2019
PROJECT NO: 18A0015
CAD FILE: G-101-SOW.DWG
DESIGN BY: KBS 12/12/2018
DRAWN BY: KBS 12/12/2018
REVIEWED BY: BSS 01/10/2019

SHEET TITLE

SCOPE OF WORK & SAFETY PLAN

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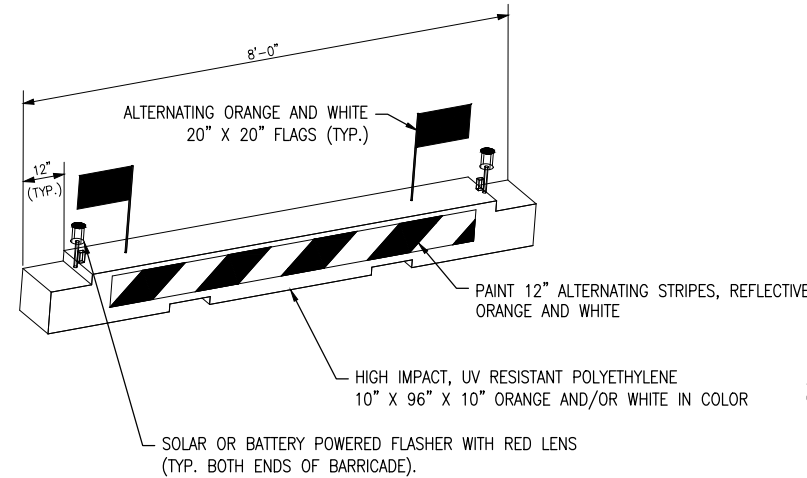
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DESIGN BY: MJD/KBS 12/13/2018
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REVIEWED BY: BSS 01/10/2019

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SAFETY NOTES & DETAILS

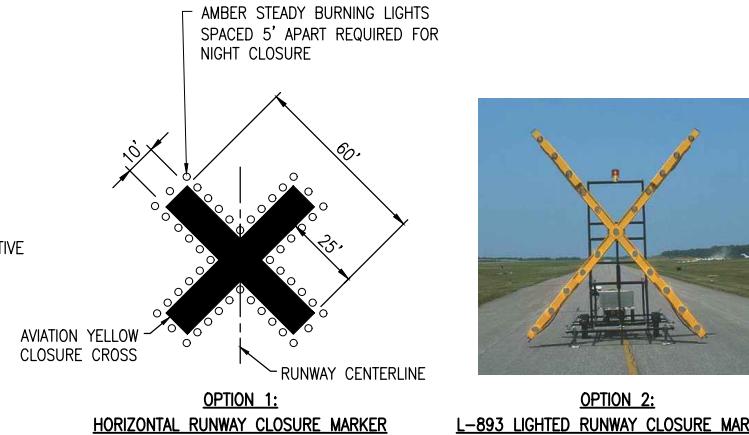
SAFETY NOTES

- THE FOLLOWING NOTES ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET AND THIS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2G (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRE-CONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5D, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA (EX. TAXIWAY), OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER/TECHNICIAN) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2G, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY TAXIWAY CENTERLINE WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE AIRPORT UNLESS PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRACKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



LOW PROFILE AIRCRAFT BARRICADE DETAIL

NOT TO SCALE



RUNWAY CLOSURE CROSS MARKER DETAIL

NOT TO SCALE

BARRICADE NOTES

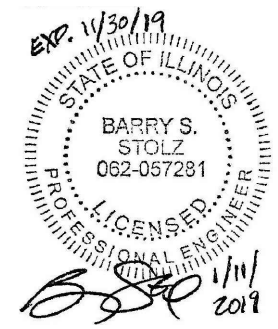
- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT WITH A MAXIMUM SPACING OF 10' BETWEEN ENDS. BARRICADES ARE TO BE SET BACK 250' FROM THE ACTIVE RUNWAY CENTERLINE, 93' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED IN THE MOBILIZATION ITEM.

CLOSURE CROSS NOTES

- RUNWAY CLOSURE CROSS MARKINGS SHALL BE LIGHTED DURING DARKNESS AND PERIODS OF REDUCED VISIBILITY. THE LIGHTED MARKERS SHALL BE PLACED OVER THE RUNWAY NUMERALS OR IMMEDIATELY OFF THE END OF THE RUNWAY ON THE EXTENDED CENTERLINE, AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- THE CONTRACTOR SHALL PROVIDE THE CLOSURE CROSSES BY ONE OF TWO OPTIONS:

OPTION 1: TEMPORARY CLOSURE CROSS MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.

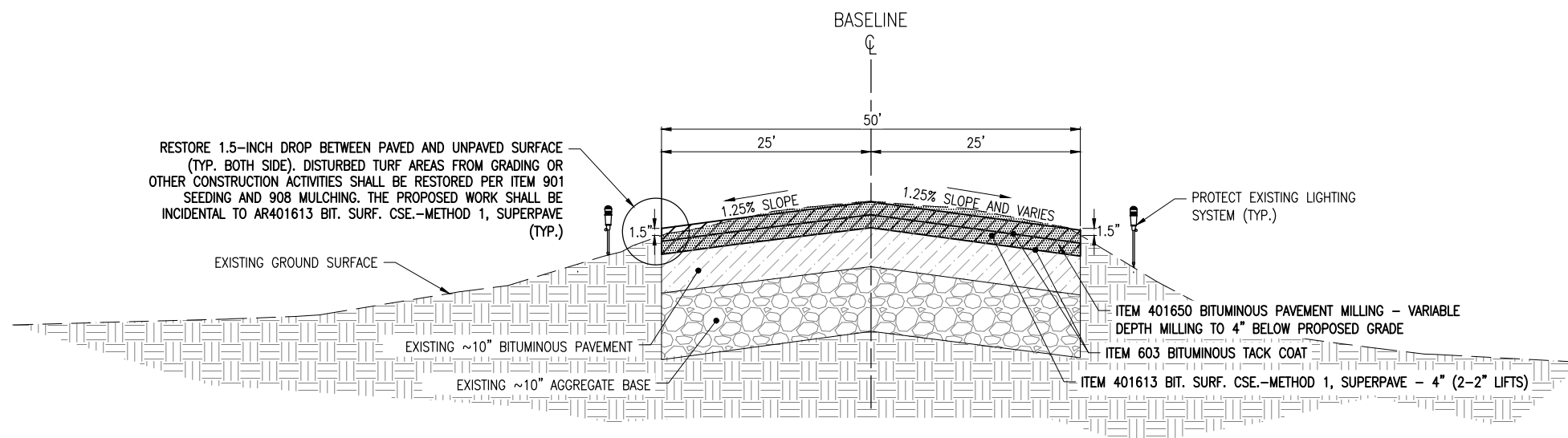
OPTION 2: THE CONTRACTOR SHALL PROVIDE TWO (2) L-893 LIGHTED RUNWAY CLOSURE MARKERS, MEETING THE REQUIREMENTS IN FAA ADVISORY CIRCULAR 150/5345-55 AND SHALL BE IN PLACE AND OPERATING WHENEVER THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED.
- THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE LIGHTED CROSSES AND MAKE PROMPT REPAIRS AS NECESSARY.
- THE CONTRACTOR SHALL BE ON-CALL FOR 24-HOUR EMERGENCY MAINTENANCE WHEN LIGHTED CROSSES ARE BEING USED.
- LIGHTED MARKERS SHALL BE SECURED FROM WIND EFFECTS BY THE CONTRACTOR AS RECOMMENDED BY THE MANUFACTURER.
- COST FOR PROVIDING, PLACING, OPERATING, MAINTAINING, RELOCATING AND REMOVING CLOSURE CROSSES SHALL BE INCLUDED IN THE COST OF THE MOBILIZATION ITEM.



REHABILITATE TAXIWAY A1

IDA No: MVN-4684
Fed. No: 3-17-SBGP-144


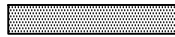


Contract No. MV063



TYPICAL SECTION A-A
NOT TO SCALE

NOTES:

1. THICKNESS OF EXISTING FEATURES SHOWN MAY VARY BY LOCATION. NO ADJUSTMENTS WILL BE MADE TO PAY ITEMS OR UNIT PRICES DUE TO THICKNESS VARIATIONS.

LEGEND	
	AR401613 PROPOSED BIT. SUR. CSE.-METHOD 1, SUPERPAVE (4" - 2" LIFTS)
	AR401650 PROPOSED BITUMINOUS PAVEMENT MILLING (VARIABLE DEPTH)
	EXISTING BITUMINOUS PAVEMENT
	EXISTING AGGREGATE BASE/SUBBASE COURSE

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

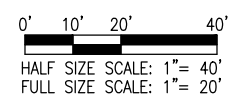
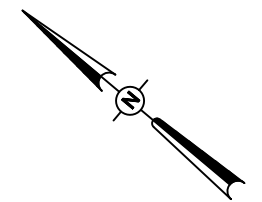
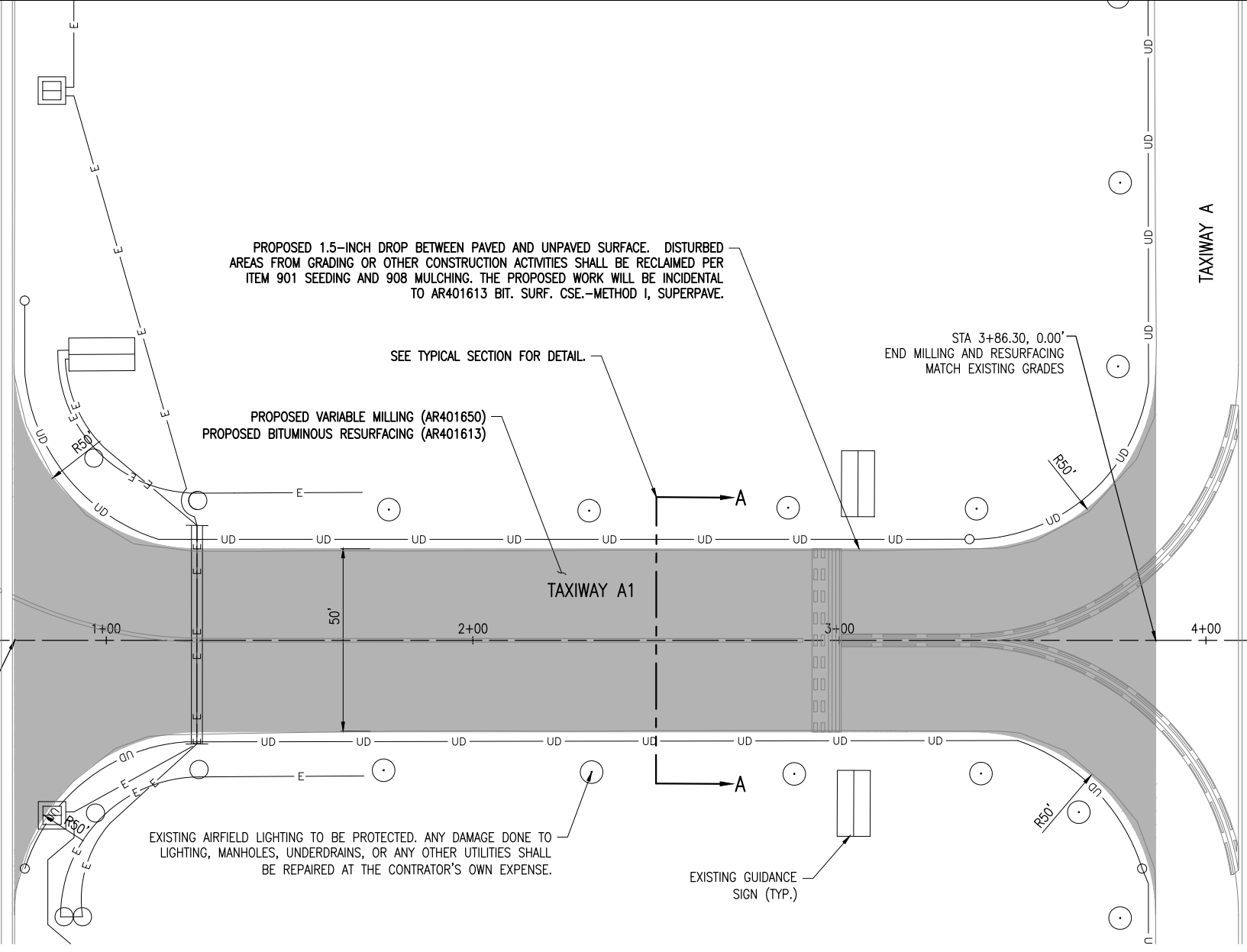
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REVIEWED BY: BSS 01/10/2019

SHEET TITLE

TYPICAL SECTION

NOTES:

1. CONTRACTOR SHALL PROTECT ALL ADJACENT PAVEMENT EDGES DURING MILLING AND PAVING OPERATIONS. ANY DAMAGE DONE TO EXISTING PAVEMENT OR MARKINGS SHALL BE REPAIRED OR REMARKED AT THE CONTRACTOR'S EXPENSE.
2. ALL MILLED MATERIAL SHALL BE HAULED TO THE ON-SITE MILLINGS STOCKPILE (SEE SHEET 3) UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
3. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER/DIRECTOR AND RESIDENT ENGINEER/TECHNICIAN.
4. CONTRACTOR SHALL PROVIDE PROTECTIVE MEASURES FOR AIRFIELD LIGHTING, SIGNS, NAVAIDS, AND OTHER DEVICES TO AVOID DAMAGE DURING CONSTRUCTION.
5. CONTRACTOR SHALL EXAMINE THE SITE TO CONFIRM AND FIELD VERIFY EXISTING SITE CONDITIONS.



LEGEND

- AR401650 BIT. PAVEMENT MILLING & AR401613 BITUMINOUS OVERLAY
- EXISTING ELECTRIC
- EXISTING UNDERDRAIN
- EXISTING STORM INLET/MAHOLE
- EXISTING AIRFIELD LIGHTING

RUNWAY 5-23

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E: 832145.75

N: 602049.33
E: 832464.53



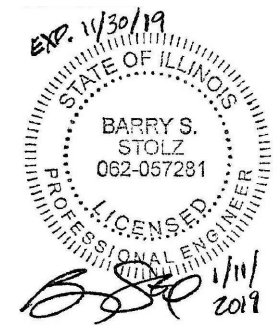
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REHABILITATE TAXIWAY A1

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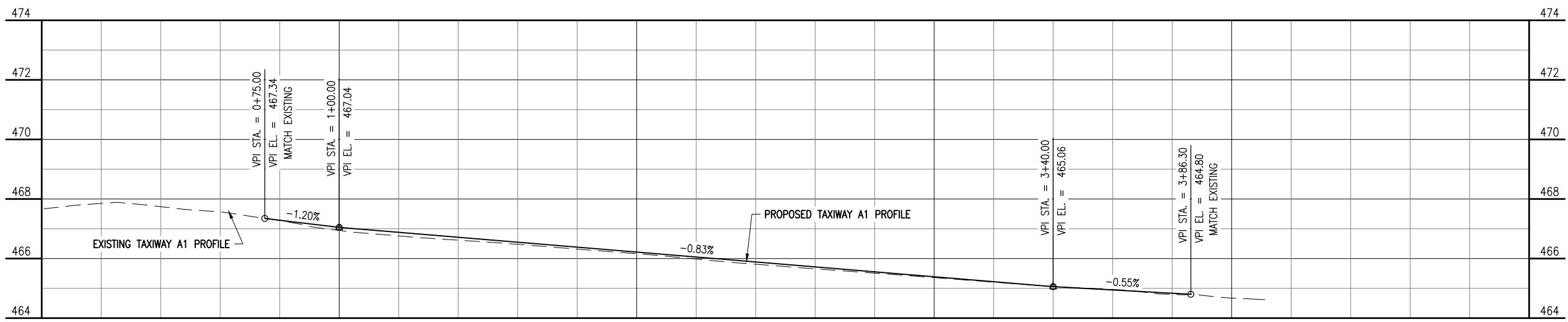
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ISSUE: JANUARY 11, 2019
PROJECT NO: 18A0015
CAD FILE: C-102-PNP.DWG
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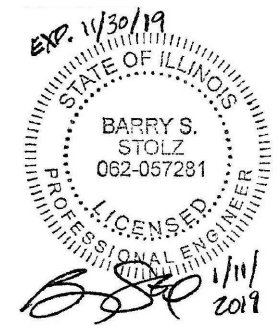
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PLAN AND PROFILE



EX. 467.85	PR.	EX. 467.74	PR.	EX. 467.57	PR.	EX. 467.27	PR.	EX. 467.28	PR.	EX. 466.94	PR.	EX. 467.04	PR.	EX. 466.76	PR.	EX. 466.88	PR.	EX. 466.62	PR.	EX. 466.71	PR.	EX. 466.48	PR.	EX. 466.55	PR.	EX. 466.32	PR.	EX. 466.38	PR.	EX. 466.16	PR.	EX. 466.22	PR.	EX. 465.98	PR.	EX. 466.05	PR.	EX. 465.81	PR.	EX. 465.88	PR.	EX. 465.65	PR.	EX. 465.72	PR.	EX. 465.51	PR.	EX. 465.55	PR.	EX. 465.36	PR.	EX. 465.39	PR.	EX. 465.21	PR.	EX. 465.22	PR.	EX. 465.06	PR.	EX. 465.06	PR.	EX. 464.96	PR.	EX. 464.95	PR.	EX. 464.80	PR.	EX. 464.84	PR.	EX. 464.67	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.	EX.	PR.
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REHABILITATE TAXIWAY A1

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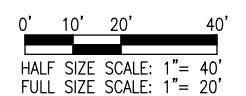
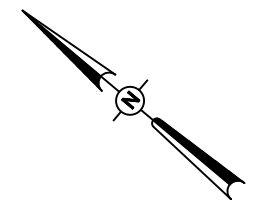
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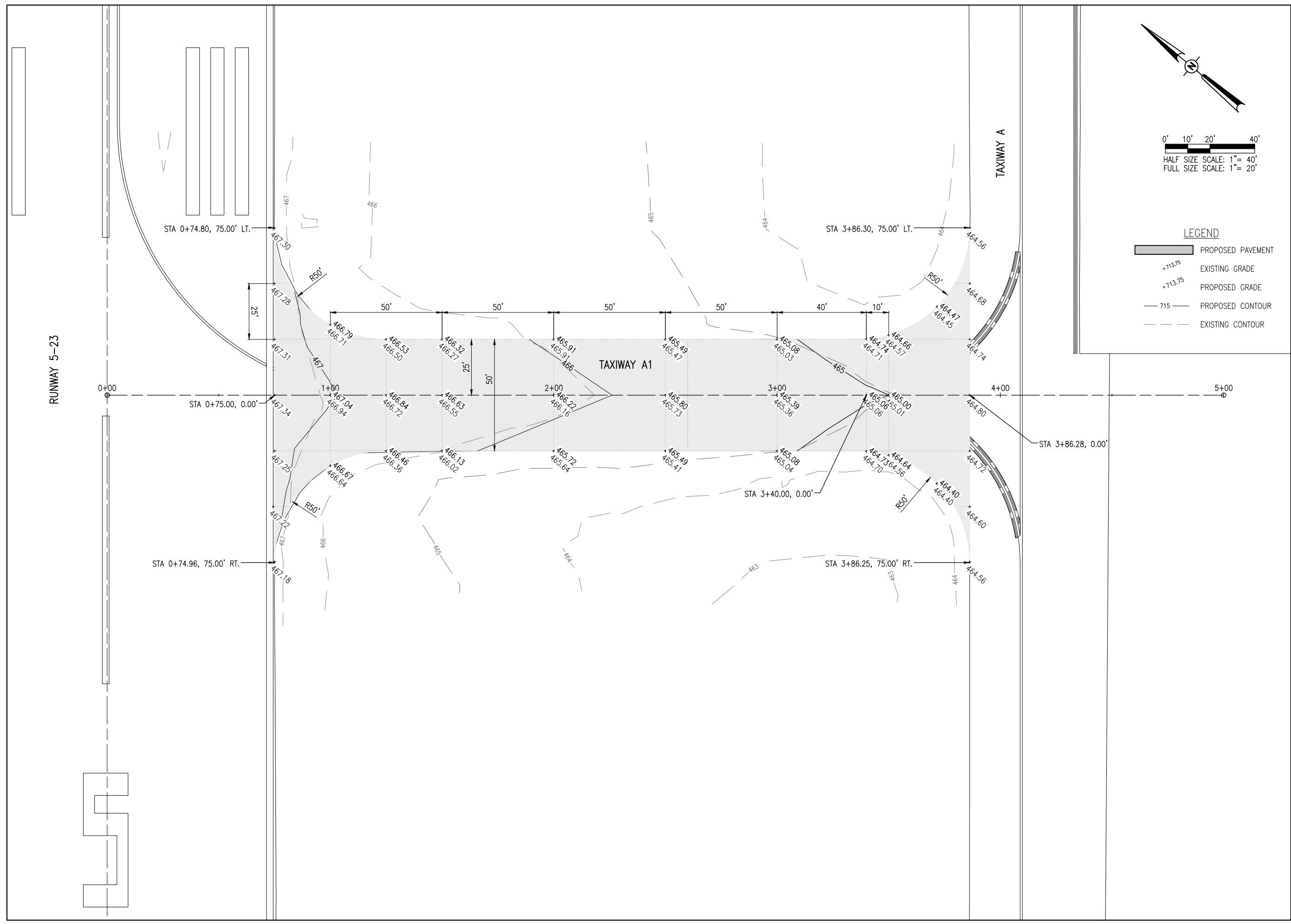
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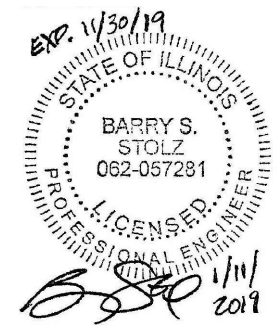
SHEET TITLE

STAKING PLAN



- LEGEND**
- PROPOSED PAVEMENT
 - EXISTING GRADE
 - PROPOSED GRADE
 - PROPOSED CONTOUR
 - EXISTING CONTOUR





REHABILITATE TAXIWAY A1

IDA No: MVN-4684
Fed. No: 3-17-SBGP-144

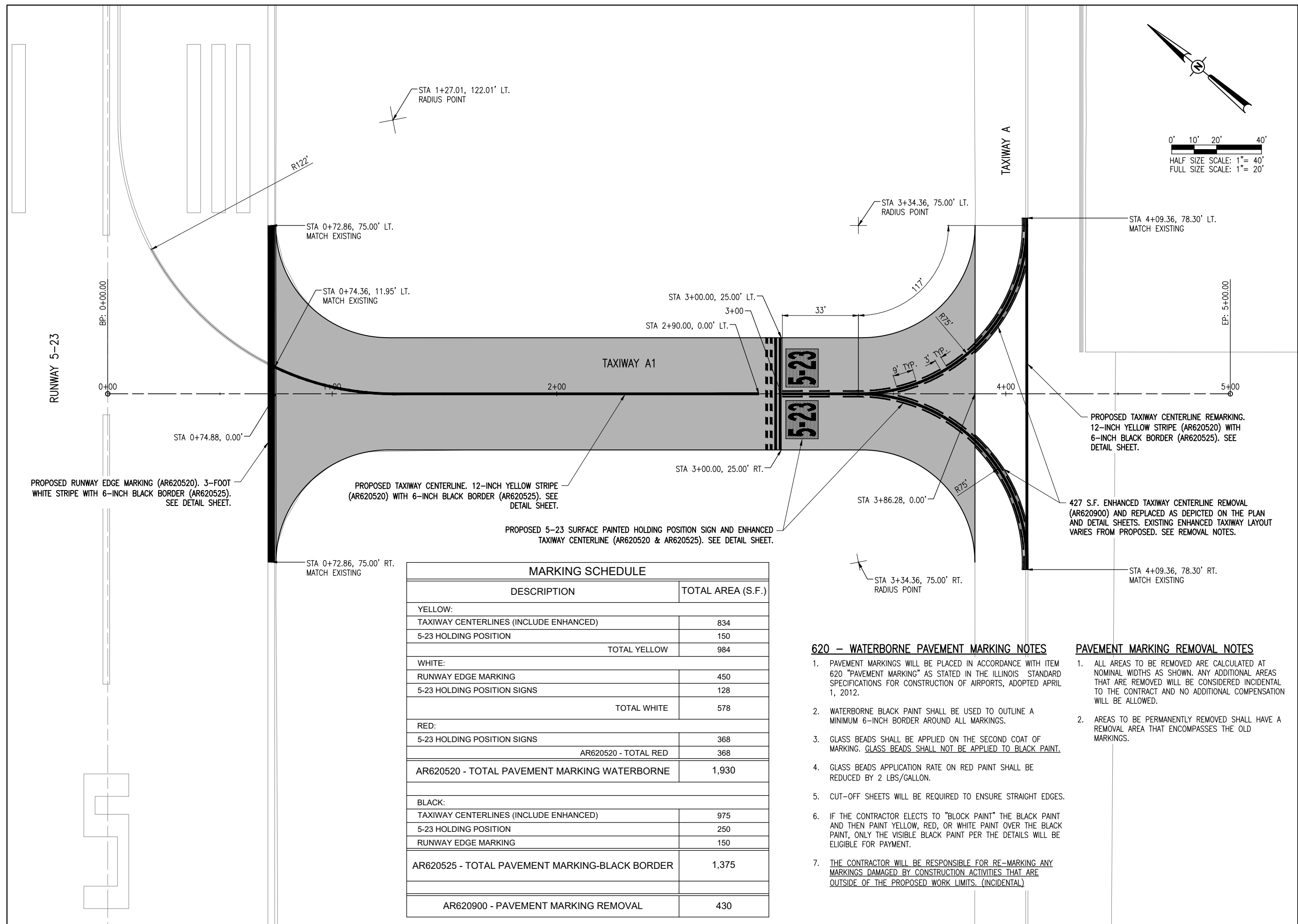
Contract No. MV063

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 11, 2019
PROJECT NO: 18A0015
CAD FILE: C-104-MRK.DWG
DESIGN BY: MJD/KBS 12/12/2018
DRAWN BY: MJD/KBS 12/12/2018
REVIEWED BY: BSS 01/10/2019

SHEET TITLE

MARKING PLAN



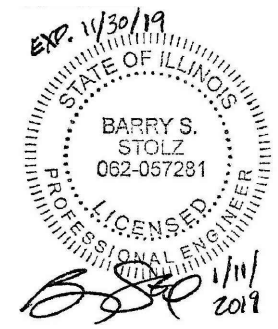
MARKING SCHEDULE	
DESCRIPTION	TOTAL AREA (S.F.)
YELLOW:	
TAXIWAY CENTERLINES (INCLUDE ENHANCED)	834
5-23 HOLDING POSITION	150
TOTAL YELLOW	984
WHITE:	
RUNWAY EDGE MARKING	450
5-23 HOLDING POSITION SIGNS	128
TOTAL WHITE	578
RED:	
5-23 HOLDING POSITION SIGNS	368
AR620520 - TOTAL RED	368
AR620520 - TOTAL PAVEMENT MARKING WATERBORNE	1,930
BLACK:	
TAXIWAY CENTERLINES (INCLUDE ENHANCED)	975
5-23 HOLDING POSITION	250
RUNWAY EDGE MARKING	150
AR620525 - TOTAL PAVEMENT MARKING-BLACK BORDER	1,375
AR620900 - PAVEMENT MARKING REMOVAL	430

620 - WATERBORNE PAVEMENT MARKING NOTES

- PAVEMENT MARKINGS WILL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED IN THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.
- WATERBORNE BLACK PAINT SHALL BE USED TO OUTLINE A MINIMUM 6-INCH BORDER AROUND ALL MARKINGS.
- GLASS BEADS SHALL BE APPLIED ON THE SECOND COAT OF MARKING. GLASS BEADS SHALL NOT BE APPLIED TO BLACK PAINT.
- GLASS BEADS APPLICATION RATE ON RED PAINT SHALL BE REDUCED BY 2 LBS/GALLON.
- CUT-OFF SHEETS WILL BE REQUIRED TO ENSURE STRAIGHT EDGES.
- IF THE CONTRACTOR ELECTS TO "BLOCK PAINT" THE BLACK PAINT AND THEN PAINT YELLOW, RED, OR WHITE PAINT OVER THE BLACK PAINT, ONLY THE VISIBLE BLACK PAINT PER THE DETAILS WILL BE ELIGIBLE FOR PAYMENT.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR RE-MARKING ANY MARKINGS DAMAGED BY CONSTRUCTION ACTIVITIES THAT ARE OUTSIDE OF THE PROPOSED WORK LIMITS. (INCIDENTAL)

PAVEMENT MARKING REMOVAL NOTES

- ALL AREAS TO BE REMOVED ARE CALCULATED AT NOMINAL WIDTHS AS SHOWN. ANY ADDITIONAL AREAS THAT ARE REMOVED WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- AREAS TO BE PERMANENTLY REMOVED SHALL HAVE A REMOVAL AREA THAT ENCOMPASSES THE OLD MARKINGS.

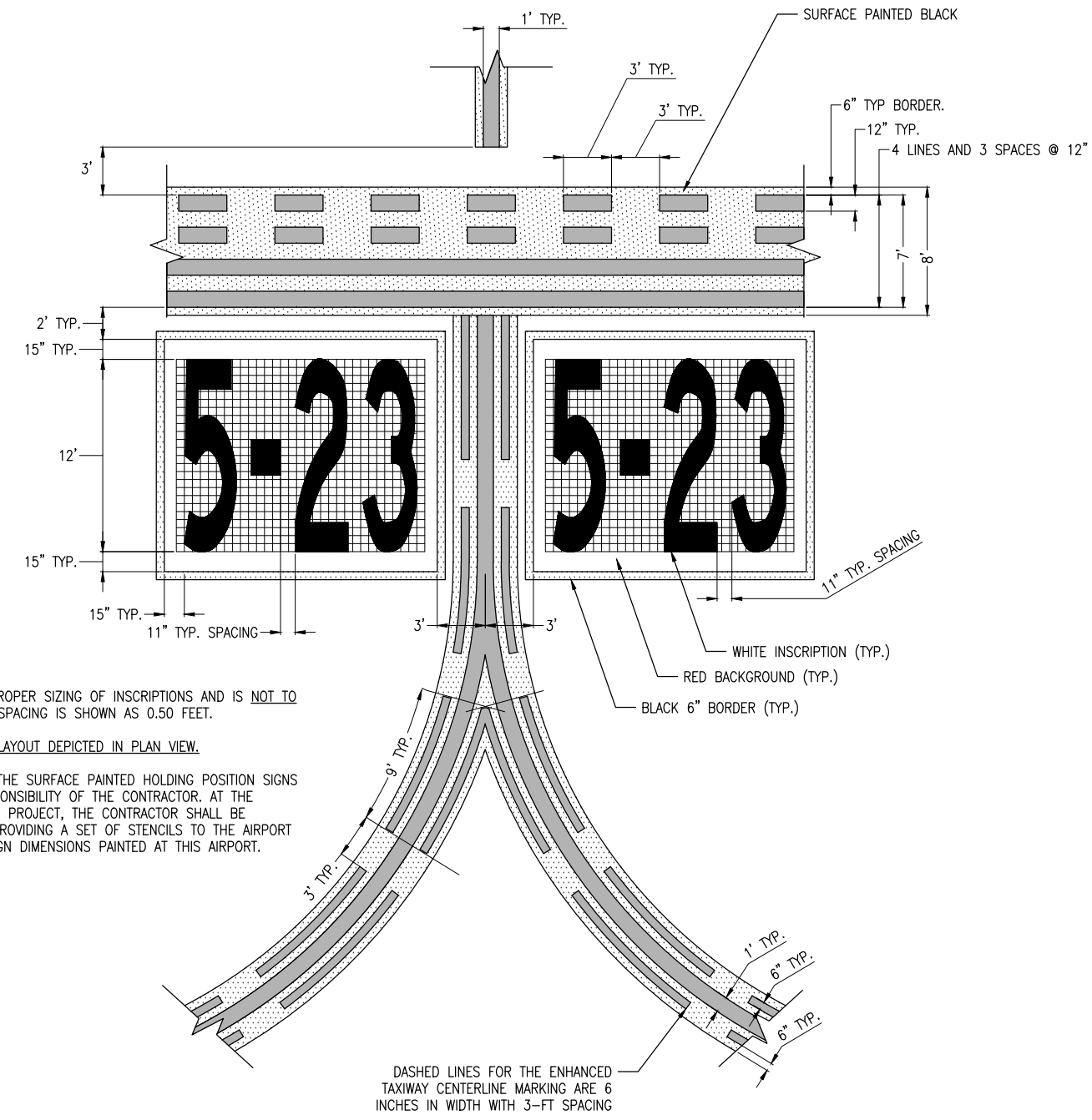


NO.	DATE	DESCRIPTION		
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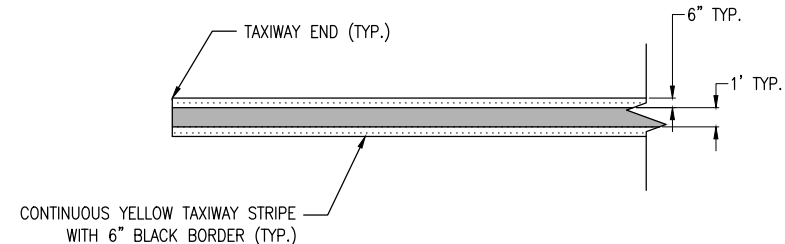
ISSUE: JANUARY 11, 2019
PROJECT NO: 18A0015
CAD FILE: C-501-MRK.DWG
DESIGN BY: MJD 12/13/2018
DRAWN BY: MJD 12/13/2018
REVIEWED BY: BSS 01/10/2019

SHEET TITLE

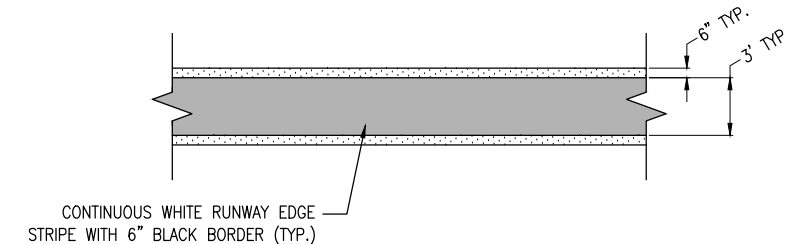
MARKING DETAILS



5-23 HOLDING POSITION AND ENHANCED TAXIWAY CENTERLINE DETAIL
NOT TO SCALE



TAXIWAY CENTERLINE DETAIL
NOT TO SCALE



RUNWAY EDGE STRIPE DETAIL
NOT TO SCALE

NOTES

1. GRID SHOWN FOR PROPER SIZING OF INSCRIPTIONS AND IS NOT TO BE PAINTED. GRID SPACING IS SHOWN AS 0.50 FEET.
2. ENHANCED TAXIWAY LAYOUT DEPICTED IN PLAN VIEW.
3. THE STENCILS FOR THE SURFACE PAINTED HOLDING POSITION SIGNS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. AT THE CONCLUSION OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A SET OF STENCILS TO THE AIRPORT THAT MATCH THE SIGN DIMENSIONS PAINTED AT THIS AIRPORT.

ENHANCED TAXIWAY CENTERLINE MARKINGS

1. TAXIWAY CENTERLINE MARKINGS SHALL BE YELLOW IN COLOR AND OUTLINED IN BLACK.
2. TAXIWAY CENTERLINE MARKINGS SHALL BE ENHANCED FOR 150-FT PRIOR TO A RUNWAY HOLDING POSITION MARKING UNLESS OTHERWISE NOTED. FOR A CURVED TAXIWAY CENTERLINE, MEASUREMENTS SHALL BE TAKEN ALONG THE CENTERLINE.
3. DASHES ON EITHER SIDE OF THE TAXIWAY CENTERLINE MUST BE ALIGNED, STARTING AND STOPPING WITH DASHES ON THE OPPOSITE SIDE OF THE CENTERLINE. TO ALIGN DASHES ON A CURVED TAXIWAY CENTERLINES, THE MEASUREMENTS FOR THE DASHES AND GAPS SHALL BE MADE AT THE CENTERLINE AND EXTENDED PERPENDICULAR FROM THE CENTERLINE TO OBTAIN DASH LOCATIONS.
4. DASHED LINES FOR TAXIWAY CENTERLINE MARKINGS ARE 6 INCHES IN WIDTH AND SEPARATED 6 INCHES FROM THE TAXIWAY CENTERLINE.
5. AS DEPICTED, V-SHAPED INNER DASHES START AND STOP WITH THE OUTSIDE 9-FOOT DASHES. PARTIAL INNER DASHED LINES LESS THAN 5 FEET AT THE POINT OF CONVERGENCE MAY BE OMITTED.

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