

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

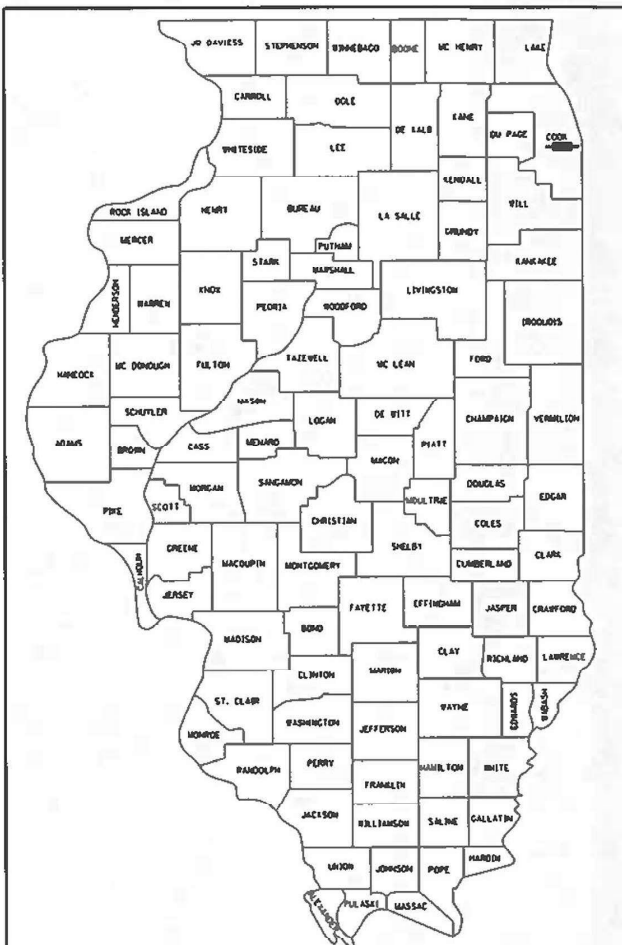
**PROPOSED
HIGHWAY PLANS**

ROUTE: FAI 90/94 (I-90/94)
SECTION: FAI 90/94 22 BJ
PROJECT NO. : NHPP-Q2KD(828)

BRIDGE DECK OVERLAY AND JOINT REPAIR

**COOK COUNTY
C-91-259-22**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	1
FED. ROAD DIST. NO. 1		ILLINOIS CONTRACT NO. 62R90		
D-91-206-22				



LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED DECEMBER 8 20 23
Gregory Geidener
DEPARTMENT ENGINEER

February 2, 2024
Gregory Geidener
ENGINEER OF DESIGN AND ENVIRONMENT

February 2, 2024
Stephen McLaughlin
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

SINGH
SINGH + ASSOCIATES INC.
CONSULTING ENGINEERS

Primera
100 S. WACKER DRIVE SUITE 700, CHICAGO, IL 60606 P.312.406.8115

Ardmore Roderick **MILLENNIA**
PROFESSIONAL SERVICES
www.millennia.pro

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OF THE STATE OF ILLINOIS**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN:
THE CITY OF CHICAGO

TRAFFIC DATA

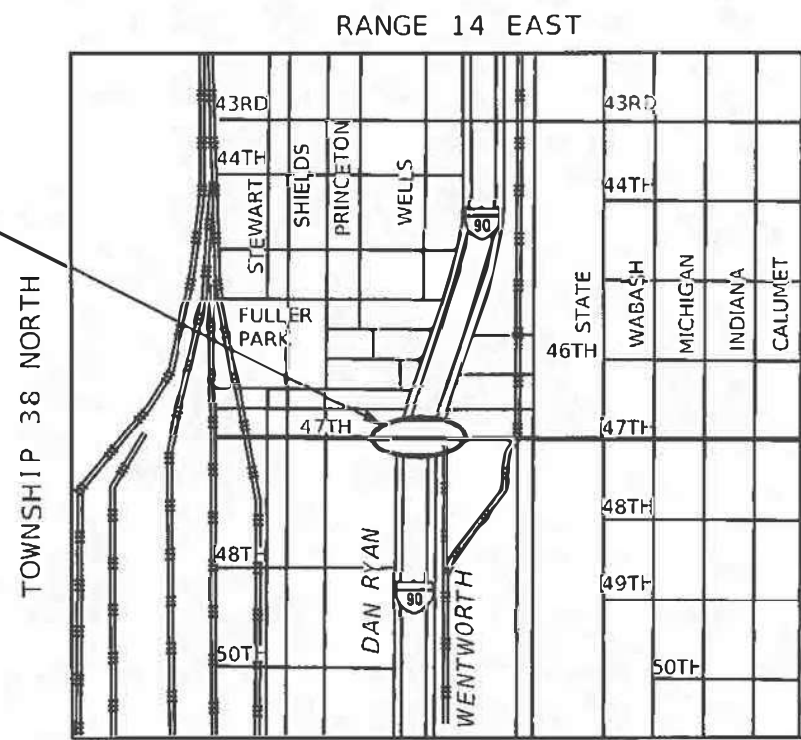
47TH STREET

2022 ADT = 13,300 (EXISTING)
SPEED LIMIT = 30 MPH

I-90/94

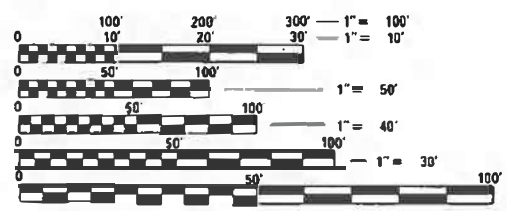
2022 ADT = 300,400 (EXISTING)
SPEED LIMIT = 55 MPH

PROJECT LOCATION
W 47TH STREET
BRIDGE
STRUCTURE NO:
016-1156



LOCATION MAP
NOT TO SCALE

GROSS LENGTH OF PROJECT = 412 FT. = 0.078 MILES
NET LENGTH OF PROJECT = 412 FT. = 0.078 MILES



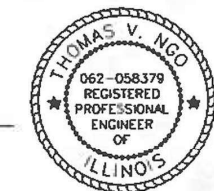
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

DIGGER - CHICAGO UTILITY AREA NETWORK (C.U.A.N.)
1-312-744-7000

PROJECT ENGINEER: PRAVEEN KAINI, P.E. (847) 705-4237
PROJECT MANAGER: J. ALAIN MIDY, P.E. (847) 221-3056

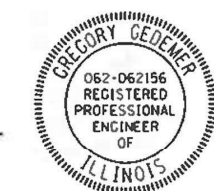
CONTRACT NO. 62R90

MILLENNIA PROFESSIONAL SERVICES
THOMAS V. NGO, P.E.
062-058379
EXPIRES 11/30/2025



DATE: 12/08/2023
SIGNATURE AND SEAL
APPLIES TO DRAWINGS: 1-13, 70-83

SINGH + ASSOCIATES, INC.
GREGORY GEIDENER, P.E.
062-062156
EXPIRES 11/30/2025



DATE: 12/08/2023
SIGNATURE AND SEAL
APPLIES TO DRAWINGS: 14-17

Chandani Bhagat, S.E.
Licensed Structural Engineer
IL. L.C. NO. 081-008090
EXPIRES 11/30/2024



SIGNED: CP Bhagat
DATE: 12/08/2023
SIGNATURE AND SEAL APPLIES TO DRWG.
S-01 TO S-21 (18-38)

INDEX OF SHEETS

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LIST OF HIGHWAY STANDARDS

000001-08 STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
 001001-02 AREAS OF REINFORCEMENT BARS
 001006 DECIMAL OF AN INCH AND OF A FOOT
 280001-07 TEMPORARY EROSION CONTROL SYSTEMS
 664001-02 CHAIN LINK FENCE
 701400-12 APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
 701401-13 LANE CLOSURE, FREEWAY/EXPRESSWAY
 701411-09 LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS > 45 MPH
 701427-05 LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS LESS/EQUAL TO 40 MPH
 701428-01 TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
 701446-11 TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
 701601-09 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
 701602-10 URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
 701606-10 URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
 701611-01 URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
 701701-10 URBAN LANE CLOSURE, MULTILANE INTERSECTION
 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
 701901-09 TRAFFIC CONTROL DEVICES
 704001-08 TEMPORARY CONCRETE BARRIER
 782006-01 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

COMMITMENTS

NO COMMITMENTS FOR THIS PROJECT

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER- CHICAGO UTILITY AREA NETWORK (C.U.A.N.) (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES (MINIMUM 48 HOURS NOTIFICATION IS REQUIRED.)
- THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS, BY THE CONTRACTOR, SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND CITY OF CHICAGO
- THE RESIDENT ENGINEER SHALL CONTACT THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR, MEADE ELECTRIC COMPANY, AT (773) 287-7672 PRIOR TO THE START OF CONSTRUCTION TO LOCATE ALL IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAH-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER AND CONTRACTOR SHALL VERIFY ALL EXISTING PAVEMENT MARKING PRIOR TO THE PROJECT STARTING. IF A DISCREPANCIES OCCURS BETWEEN EXISTING PAVEMENT MARKINGS AND THE PROPOSED PAVEMENT MARKINGS, THE RESIDENT ENGINEER SHALL CONTACT THE BUREAU OF TRAFFIC OPERATIONS.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- IN ADDITION TO FIELD AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS, OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS, SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL STAGES OF CONSTRUCTION.
- THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- POLLUTION CONTROL: THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE STATE REGULATIONS REGARDING AIR, WATER, AND NOISE POLLUTION. THIS WORK IS NOT TO BE PAID SEPARATELY AND SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- THE COST OF SAW CUTTING SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS REMOVAL PAY ITEMS.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24).
- THE CONTRACTOR SHALL USE CARE NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO THE EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRATOR'S OWN EXPENSE.
- DURING CONSTRUCTION OPERATION, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED THE MATERIAL SHALL BE REMOVED A THE CLOSE OF EACH WORKDAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE OF DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF CONTRACT.
- THE CONTRACTOR SHALL REFER TO THE SPECIAL PROVISION FOR STRUCTURE ASSESSMENT REPORTS FOR CONTRACTOR'S MEANS AND METOHDS WHILE FORMULATING A WORK PLAN THAT INVOLVES LOADS ON THE DECK.
- THE CONTRACTOR IS TO REFER TO THE SPECIAL PROVISION FOR SAW CUTTING CONTRACTION JOINTS IN P.C.C. SIDEWALKS, NO TOOLED JOINTS WILL BE ALLOWED.
- THE CONTRACTOR NEEDS TO SUBMIT COMPLETE CONSTRUCTION PROCESS (CPP'S) TO CTA FOR REVIEW AND COMMENTS PRIOR TO THE COMMENCEMENT OF ANY WORK.

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 USER NAME = jcovarrubias



USER NAME = jcovarrubias	DESIGNED - JC	REVISED -
	DRAWN - LDN	REVISED -
PLOT SCALE = 2.0000' / 1" =	CHECKED - TVN	REVISED -
PLOT DATE = 12/11/2023	DATE - 09/15/2023	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
 INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES**

F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	2
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: N/A SHEET NO. 1 OF 1 SHEETS STA. N/A TO STA. N/A

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PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
				BRIDGE SN: 016-1156	
				90% FED 10% STATE	
				0059	
28000510	INLET FILTERS	EACH	2	2	2
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	714	714	714
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	246	246	246
44000600	SIDEWALK REMOVAL	SQ FT	246	246	246
50102400	CONCRETE REMOVAL	CU YD	32.5	32.5	32.5
50157300	PROTECTIVE SHIELD	SQ YD	3,337	3,337	3,337
50300255	CONCRETE SUPERSTRUCTURE	CU YD	34	34	34
50300300	PROTECTIVE COAT	SQ YD	4,373	4,373	4,373
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	7,330	7,330	7,330
50800515	BAR SPLICERS	EACH	40	40	40
52000110	PREFORMED JOINT STRIP SEAL	FOOT	198	198	198
59000200	EPOXY CRACK INJECTION	FOOT	27	27	27
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2	2
67100100	MOBILIZATION	L SUM	1	1	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	14	14	14
70300100	SHORT TERM PAVEMENT MARKING	FOOT	7,836	7,836	7,836
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2,612	2,612	2,612

* Specialty Item

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PLOT DATE = 12/14/2023	DATE - 09/15/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
SUMMARY OF QUANTITIES**

SCALE: N/A SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	3
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
				BRIDGE SN: 016-1156	
				90% FED 10% STATE	
				0059	
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	254	254	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	5,988	5,988	
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	944	944	
70307140	TEMPORARY PAVEMENT MARKING - LINE 8" - TYPE IV TAPE	FOOT	672	672	
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	56	56	
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	212	212	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	388	388	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	363	363	
70600241	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
70600341	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	209	209	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,458	2,458	
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	397	397	
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	424	424	
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	483	483	
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	410	410	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1,025	1,025	

* Specialty Item

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PLOT DATE = 12/14/2023	DATE - 09/15/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
SUMMARY OF QUANTITIES**

SCALE: N/A SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	4
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
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				90% FED 10% STATE	
				0059	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,687	1,687	
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	799	799	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	277	277	
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	725	725	
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	10	10	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	3,650	3,650	
X5230146	BRIDGE DRAINAGE SYSTEM REPAIR	FOOT	6	6	
X4240800	DETECTABLE WARNINGS (SPECIAL)	SQ FT	40	40	
X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	2,939	2,939	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1	
X8900100	TEMPORARY TRAFFIC SIGNAL INSTALLATION (SPECIAL)	EACH	2	2	
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	25	25	
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	3,113	3,113	
Z0007400	BRIDGE SIDEWALK REPAIR (PARTIAL DEPTH)	SQ FT	17	17	
Z0010605	CLEANING DRAINAGE SYSTEM	L SUM	1	1	

* Specialty Item

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 DATE - 09/15/2023

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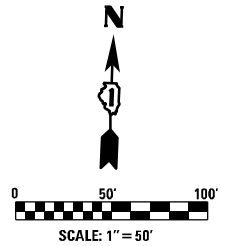
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FAI 90 / 94 (DAN RYAN) AT 47TH STREET
 SUMMARY OF QUANTITIES

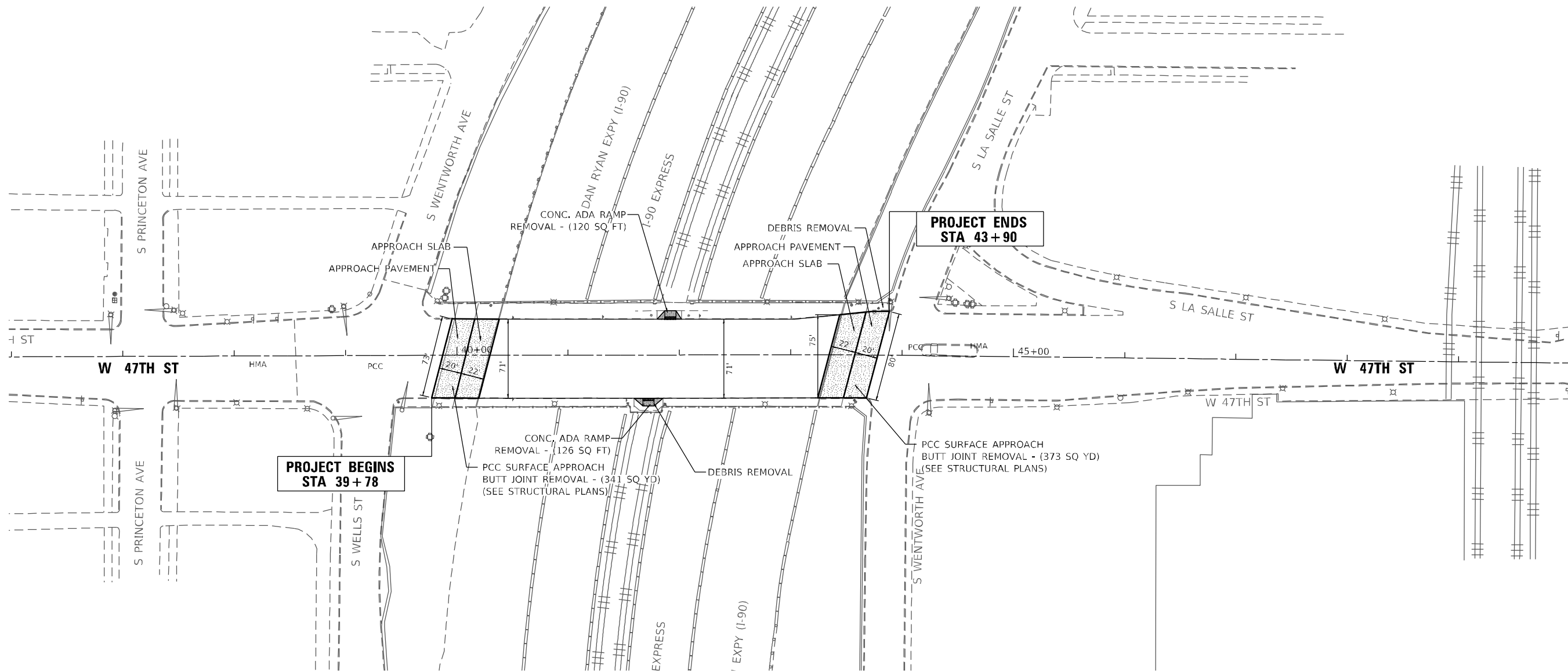
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	5
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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 SURFACE REMOVAL
 SEE APPROACH SLAB/PAVEMENT DETAILS (SHEET 8)



NOTE:
 DEBRIS REMOVAL SHALL BE APPLIED TO ALL CORNERS OF THE
 BRIDGE LIMITS AND THE CTA BUS SHELTER AREA.

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 USER NAME = lnguyen



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	DRAWN - LDN	REVISED -
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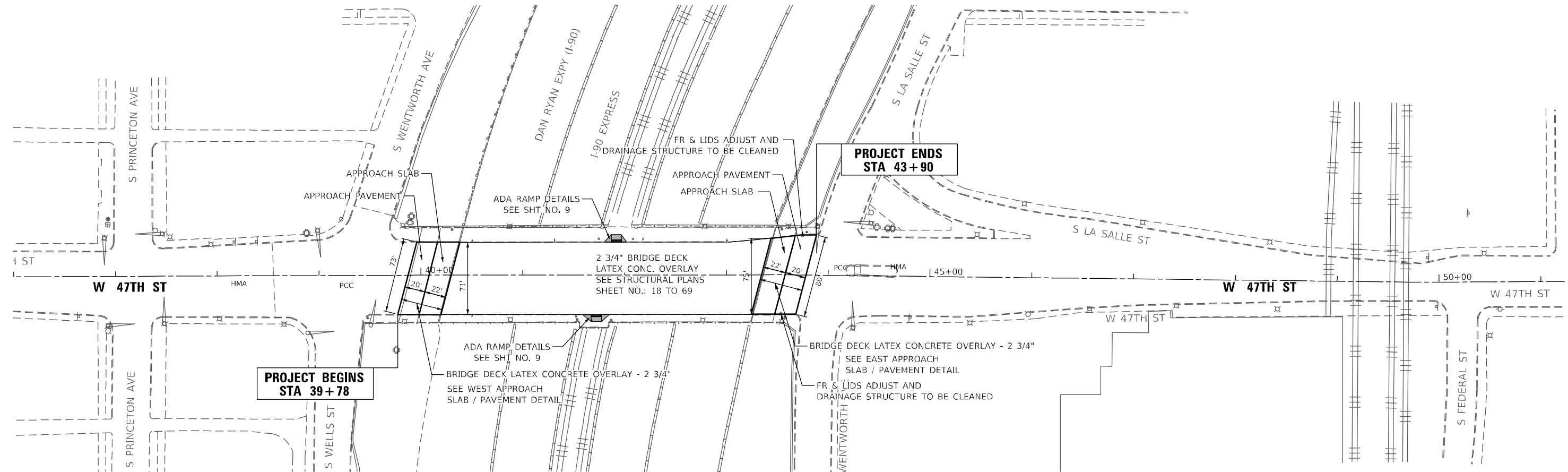
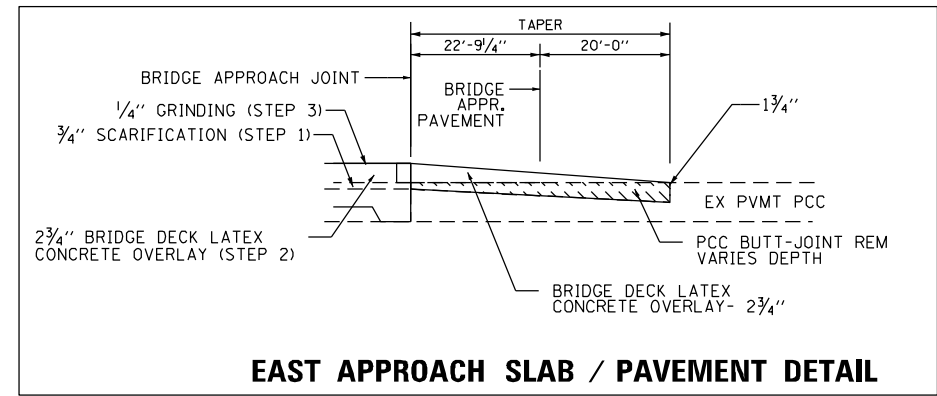
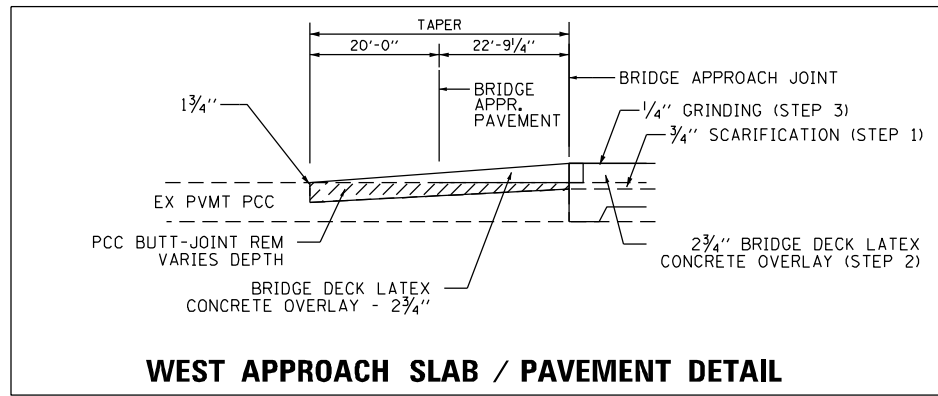
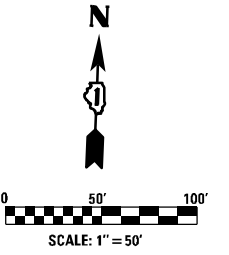
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI 90 /94 (DAN RYAN) AT 47TH STREET
EXISTING ROADWAY AND REMOVAL PLANS

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	7
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

P:\2023\ME23025 PTB 2005-03 01 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\07-D162R90-sht-ExRdy.dgn



FILE NAME = P:\2023\ME23025 PTB 205-03 DI Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\08-DIG2R90-sht-PrRdy.dgn
 PLOT SCALE = 100.0000' / in.
 USER NAME = Inguyen



USER NAME = inguyen	DESIGNED - JC	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN - LDN	REVISED -
PLOT DATE = 12/12/2023	CHECKED - TVN	REVISED -
	DATE - 12/07/2023	REVISED -

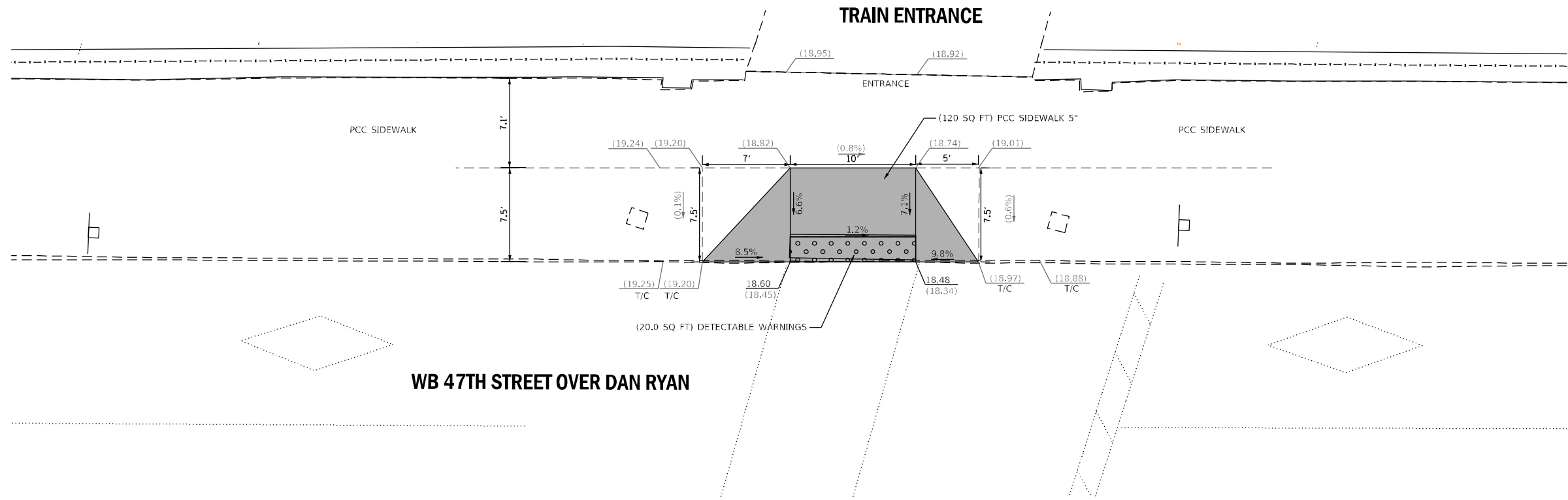
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
PROPOSED ROADWAY PLAN**

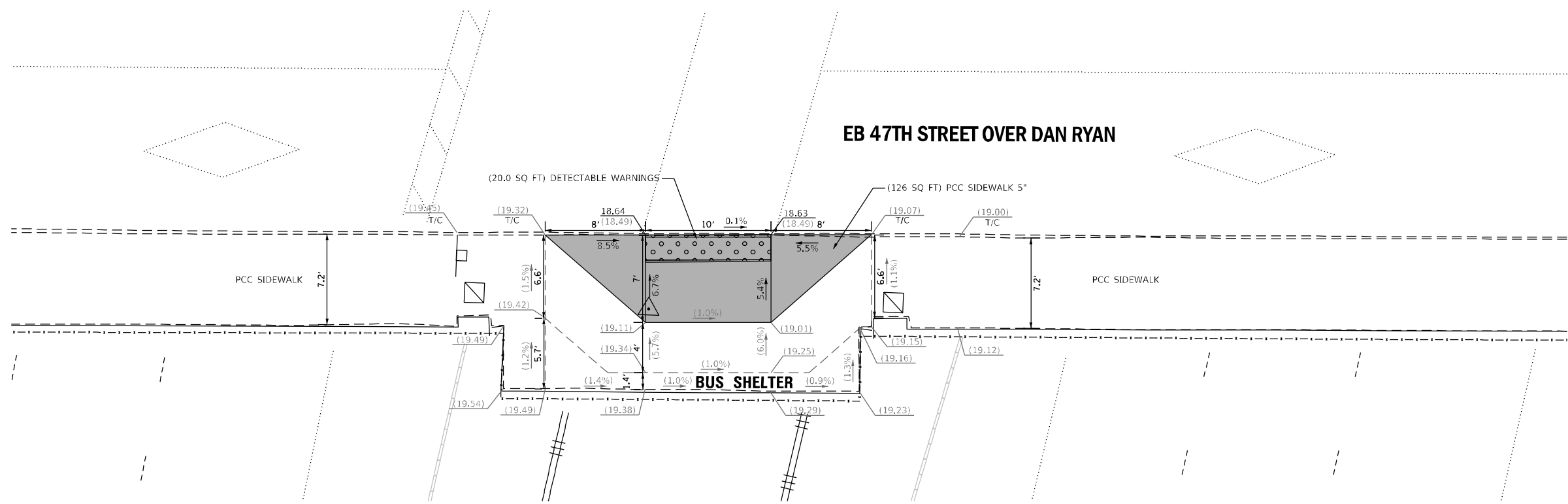
SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83	SHEET NO. 8
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

P:\2023\ME23025 PTB 205-03 DI Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\08-DIG2R90-sht-PrRdy.dgn



WB 47TH STREET OVER DAN RYAN



EB 47TH STREET OVER DAN RYAN

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL

REFERENCE BENCHMARK ELEV: 17.13
 BENCHMARK : SOUTHEAST BOLT IN CONCRETE LIGHT POLE BASE
 AT THE NORTHWEST CORNER OF W. 47TH STREET AND
 LOCATION : S. WENTWORTH AVE.

REFERENCE BENCHMARK ELEV: 19.95
 BENCHMARK : PK NAIL IN TOP OF CONCRETE ABUTMENT WALL ON THE
 SOUTH SIDE OF W. 47TH STREET, 3.0 FT WEST OF EAST
 LOCATION : END OF WALL

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
 ADA RAMP DETAIL PLAN**

USER NAME = inguyen	DESIGNED - JP	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - JP	REVISED -
PLOT DATE = 12/12/2023	CHECKED - TVN	REVISED -
	DATE - 09/15/2023	REVISED -

SCALE: 1"=5'	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	9
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62R90				

FILE NAME = P:\2023\ME23025 PTB 205-03 DI Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\09-DI62R90-sht-ADA.dgn
 PLOT SCALE = 10.0000' / in.
 USER NAME = inguyen



MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE TRAFFIC CONTROL DEPICTED IN THE PLANS ARE THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES, AS SPECIFIED IN THE HIGHWAY AND DISTRICT STANDARDS, AS SHOWN IN THE INDEX OF SHEETS, AND SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS, OR SPECIAL PROVISIONS.
2. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY, OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER.
3. ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
4. THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
5. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL GENERAL NOTES, PLANS, SPECIAL PROVISIONS, APPLICABLE HIGHWAY & DISTRICT STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER 72 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
6. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, LANDSCAPING, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED. IF THEY WERE REMOVED, DAMAGED, OR OTHERWISE AFFECTED BY CONSTRUCTION ACTIVITIES, THE COST TO REPAIR ANY DAMAGES WILL BE BORNE BY THE CONTRACTOR AND NOT THE RESPONSIBILITY OF THE DEPARTMENT.
7. NO WORK SHALL COMMENCE UNTILL TRAFFIC CONTROL REQUIREMENTS ARE MET.
8. THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
9. ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE PAVEMENT MARKING TAPE, TYPE IV (AND/OR MODIFIED URETHANE) USED FOR STAGING SHALL BE REMOVED. THIS WORK SHALL BE PAID FOR AS PAVEMENT MARKING REMOVAL-WATER BLASTING.
10. ALL EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS.
11. PAVEMENT MARKING TAPE, TYPE IV (AND/OR TYPE III) SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN MODIFIED URETHANE PAVEMENT MARKING AND WILL BE PAID FOR THEIR RESPECTIVE CONTRACT UNIT PRICE.
12. TEMPORARY INFORMATION SIGNS ON TEMPORARY SUPPORTS SHALL BE PROVIDED FOR ALL COMMERCIAL DRIVEWAYS THAT ARE LOCATED WITHIN A WORK AREA. THIS WORK SHALL BE PAID FOR PER DISTRICT 1 DETAIL TC-26. THESE SIGNS SHALL BE RELOCATED AS REQUIRED FOR EACH CONSTRUCTION STAGE AND SHALL BE PLACED AS DIRECTED BY THE ENGINEER. THIS SIGN RELOCATION WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR TEMPORARY INFORMATION SIGNING.
13. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.
14. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
15. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE DURING ALL PHASES OF CONSTRUCTION.

MAINTENANCE OF TRAFFIC PEDESTRIAN NOTES

1. THE CONTRACTOR MUST MAINTAIN A CONTINUOUS 5-FOOT PEDESTRIAN ZONE ALONG 47TH STREET. PEDESTRIAN TRAFFIC CAN BE DIRECTED TO THE NORTH SIDE FOR THE DURATION OF CONSTRUCTION STAGE I. THE SIDEWALK DETOUR MUST BE CLEARLY MARKED USING SIGNS AND BARRICADES NOTIFYING PEDESTRIANS OF THE CLOSURE.
2. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS (MIN 5') TO ADJACENT PROPERTIES (WHERE APPLICABLE) BY INSTALLING ADA COMPLIANT PLYWOOD WALKWAYS. PEDESTRIAN ACCESS TO ADJACENT PROPERTIES SHALL BE UNINTERRUPTED UNTILL THE WALKWAY IS FULLY RESTORED. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF SIGNAGE AND OTHER ITEMS TO ENSURE SAFE PEDESTRIAN ACCESS.
3. USE ONE "PEDESTRIAN WALKWAY (ARROW)" (BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND) SIGN AT EACH END OF EACH SIDEWALK SECTION BEING RECONSTRUCTED.
4. AT EACH POINT OF CLOSURE, SUFFICIENT NUMBERS OF BARRICADES MUST BE USED TO COMPLETELY CLOSE THE PATHWAY.
5. PEDESTRIAN WALKWAYS MUST BE MAINTAINED FREE OF ANY OBSTRUCTIONS AND HAZARDS SUCH AS HOLES, DEBRIS, MUD, CONSTRUCTION EQUIPMENT, STORED MATERIALS, ETC. AND MUST BE BROOM SWEEPED DAILY OR AS DIRECTED BY THE ENGINEER.
6. ALL HAZARDS NEAR OR ADJACENT TO WALKWAYS MUST BE CLEARLY DELINEATED.
7. THE CONTRACTOR MUST MAINTAIN DISABLED PERSON PEDESTRIAN ACCESS TO CROSSWALKS ACROSS 47TH STREET AND SIDE STREETS AT ALL TIMES VIA ADA COMPLIANT WOOD FRAME CONSTRUCTED WALKWAYS AND RAMPS THROUGH THE WORK ZONES. THESE ACCESSES MUST BE OBSERVED AND PROTECTED BY THE CONTRACTOR AT ALL TIMES.
8. THE CONTRACTOR SHALL MAINTAIN ACCESS TO AND FROM CTA STATION AT ALL TIMES DURING CONSTRUCTION.

FILE NAME = P:\2023\ME23025 PTB 205-03 D1 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\10-D162R90-sht-mot-gennotes.dgn
 PLOT SCALE = 2.0000' / 1" =
 USER NAME = lnguyen



USER NAME = lnguyen	DESIGNED - JC	REVISED -
DRAWN - LDN	REVISED -	
PLOT SCALE = 2.0000' / 1"	CHECKED - TVN	REVISED -
PLOT DATE = 12/12/2023	DATE - 12/07/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI 90 /94 (DAN RYAN) AT 47TH STREET
MAINTENANCE OF TRAFFIC GENERAL NOTES

SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA. N/A TO STA. N/A
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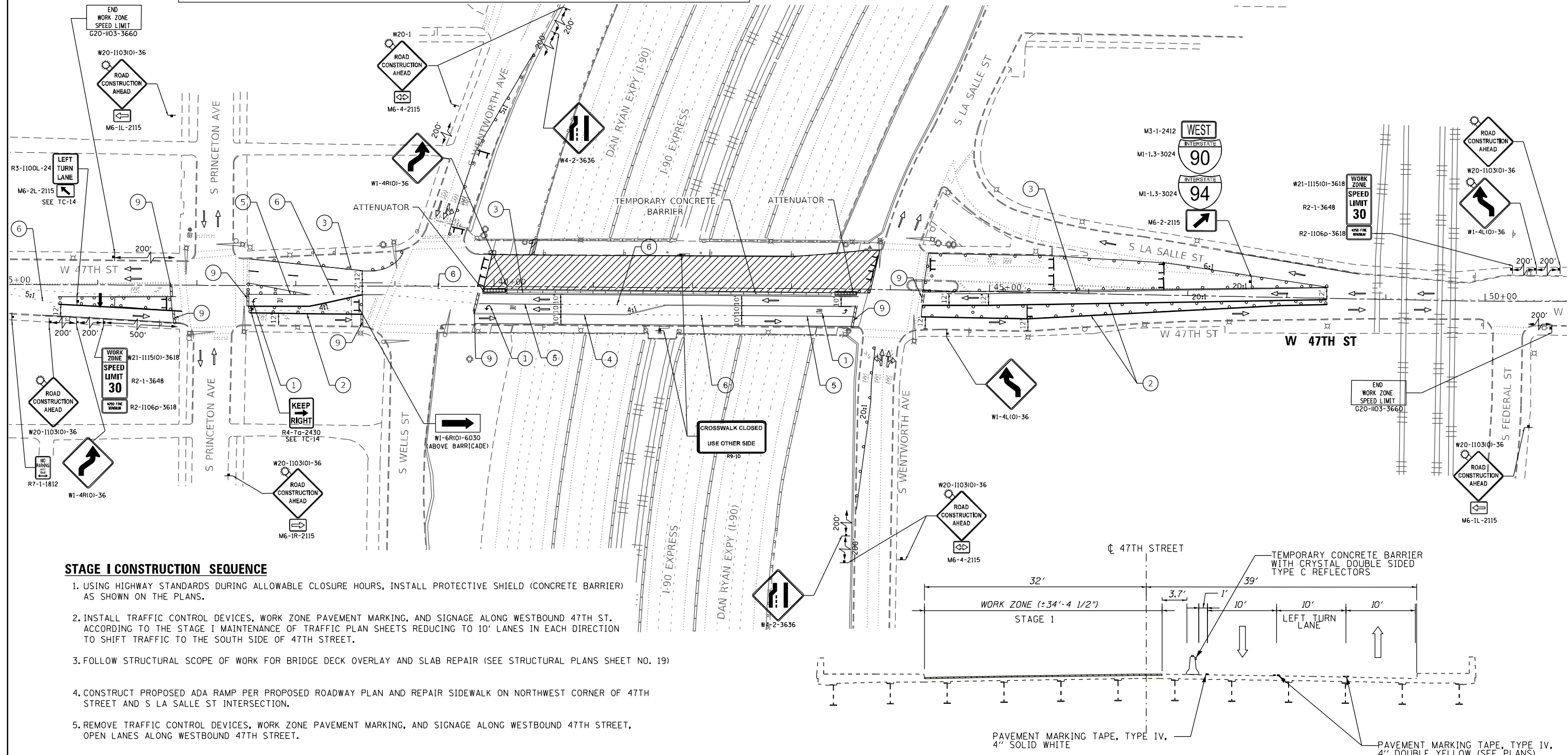
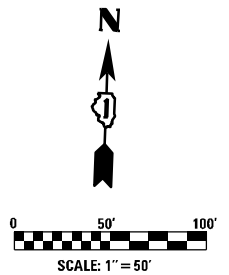
F.A.I RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	10
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LEGEND

- DRUMS @ 30 FT CENTERS ALONG TANGENT AND 20 FT CENTERS ALONG TAPERS
- ⊥ TYPE III BARRICADES WITH FLASHING BURNING LIGHTS
- ▨ WORK ZONE
- ⊣ SIGN
- ▬ IMPACT ATTENUATOR, TEMP NRN TL-2
- ▬ TEMPORARY CONCRETE BARRIER WALL WITH DOUBLE SIDED TYPE C CRYSTAL REFLECTORS
- ➔ DIRECTION OF TRAFFIC
- ➔ ARROW BOARD
- ① TEMPORARY LETTERS AND SYMBOLS TYPE IV TAPE
- ② TEMPORARY 4" SOLID YELLOW LINE TYPE IV TAPE
- ③ TEMPORARY 4" SOLID WHITE LINE TYPE IV TAPE
- ④ TEMPORARY DOUBLE YELLOW 4" SOLID LINES TYPE IV TAPE
- ⑤ TEMPORARY 6" SOLID WHITE LINE TYPE IV TAPE
- ⑥ TEMPORARY 6" WHITE DOTTED LINE (2" DASH - 6' SKIP) TYPE IV TAPE
- ⑦ TEMPORARY 8" SOLID WHITE LINE TYPE IV TAPE
- ⑧ TEMPORARY 12" SOLID WHITE LINE TYPE IV TAPE
- ⑨ TEMPORARY 24" SOLID WHITE LINE (STOP BAR) TYPE IV TAPE

NOTES

1. BEACONS SHOULD BE PLACED ON TRAFFIC SIDE OF SIGN
2. PROVIDE TEMPORARY CONCRETE BARRIER PER STD 704001
3. PROVIDE PAVEMENT MARKING PER STD TC-24
4. KEEP PEDESTRIAN ACCESS DURING CONSTRUCTION



STAGE I CONSTRUCTION SEQUENCE

1. USING HIGHWAY STANDARDS DURING ALLOWABLE CLOSURE HOURS, INSTALL PROTECTIVE SHIELD (CONCRETE BARRIER) AS SHOWN ON THE PLANS.
2. INSTALL TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG WESTBOUND 47TH ST. ACCORDING TO THE STAGE I MAINTENANCE/CONSTRUCTION OF TRAFFIC PLAN SHEETS REDUCING TO 10' LANES IN EACH DIRECTION TO SHIFT TRAFFIC TO THE SOUTH SIDE OF 47TH STREET.
3. FOLLOW STRUCTURAL SCOPE OF WORK FOR BRIDGE DECK OVERLAY AND SLAB REPAIR (SEE STRUCTURAL PLANS SHEET NO. 19)
4. CONSTRUCT PROPOSED ADA RAMP PER PROPOSED ROADWAY PLAN AND REPAIR SIDEWALK ON NORTHWEST CORNER OF 47TH STREET AND S LA SALLE ST INTERSECTION.
5. REMOVE TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG WESTBOUND 47TH STREET, OPEN LANES ALONG WESTBOUND 47TH STREET.

47TH ST STAGE 1 (LOOKING EAST)

FILE NAME = P:\2023\ME23025 PTB 205-03 DI Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\11-0162R90-sht-not-stage1.dgn
 PLOT SCALE = 100.0000 / in.
 USER NAME = inguyen



USER NAME = inguyen	DESIGNED - JC	REVISED - A
	DRAWN - LDN	REVISED -
PLOT SCALE = 100.0000 / in.	CHECKED - TVN	REVISED -
PLOT DATE = 1/24/2024	DATE - 12/07/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI 90 /94 (DAN RYAN) AT 47TH STREET
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL-STAGE I
 SCALE: 1"=50' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

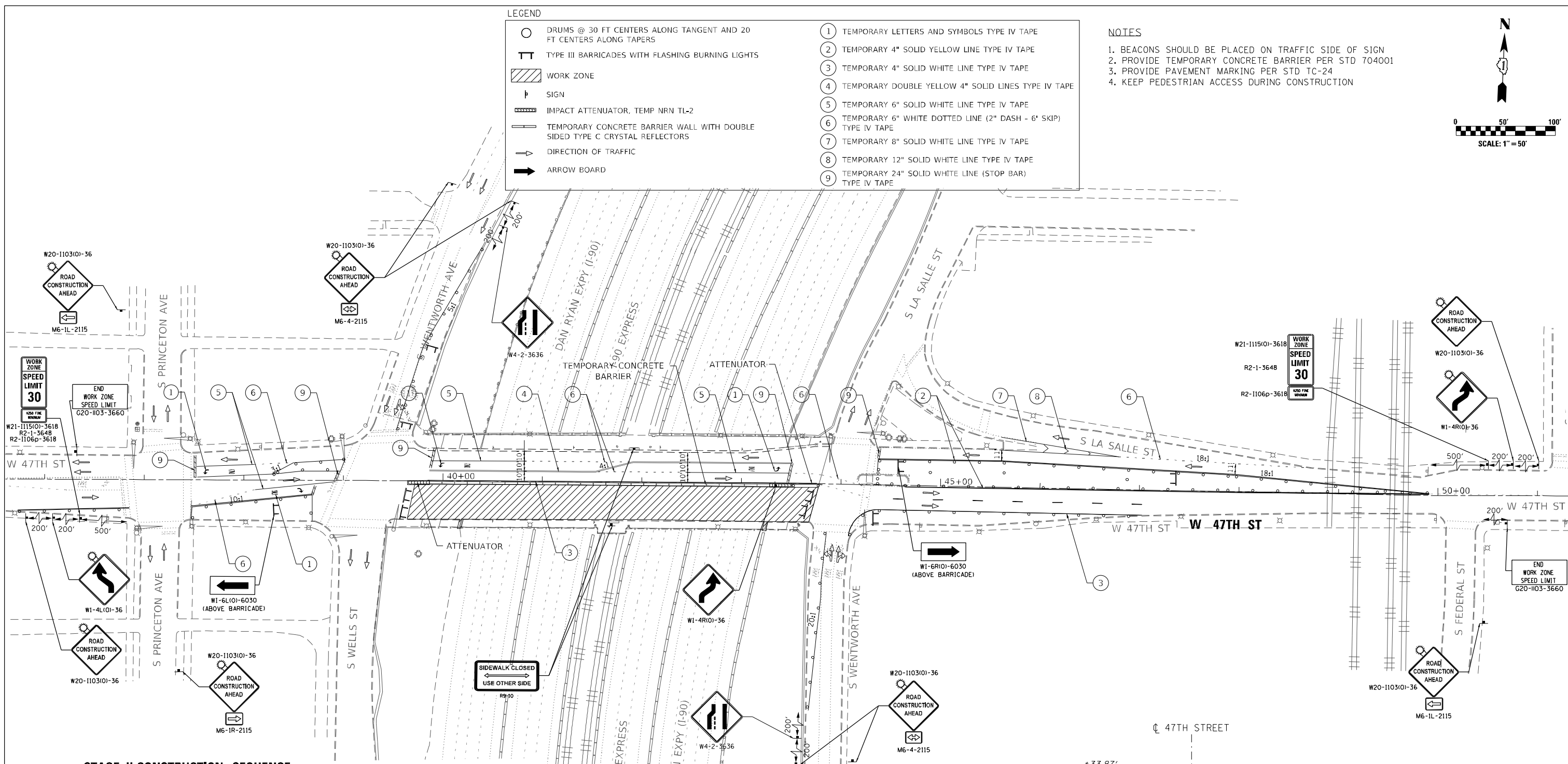
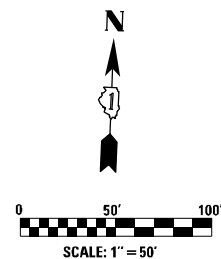
F.A.I. RTE. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83	SHEET NO. 11
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LEGEND

- DRUMS @ 30 FT CENTERS ALONG TANGENT AND 20 FT CENTERS ALONG TAPERS
- ⊥ TYPE III BARRICADES WITH FLASHING BURNING LIGHTS
- ▨ WORK ZONE
- ⊥ SIGN
- ▬ IMPACT ATTENUATOR, TEMP NRN TL-2
- ▬ TEMPORARY CONCRETE BARRIER WALL WITH DOUBLE SIDED TYPE C CRYSTAL REFLECTORS
- DIRECTION OF TRAFFIC
- ➔ ARROW BOARD
- ① TEMPORARY LETTERS AND SYMBOLS TYPE IV TAPE
- ② TEMPORARY 4" SOLID YELLOW LINE TYPE IV TAPE
- ③ TEMPORARY 4" SOLID WHITE LINE TYPE IV TAPE
- ④ TEMPORARY DOUBLE YELLOW 4" SOLID LINES TYPE IV TAPE
- ⑤ TEMPORARY 6" SOLID WHITE LINE TYPE IV TAPE
- ⑥ TEMPORARY 6" WHITE DOTTED LINE (2" DASH - 6' SKIP) TYPE IV TAPE
- ⑦ TEMPORARY 8" SOLID WHITE LINE TYPE IV TAPE
- ⑧ TEMPORARY 12" SOLID WHITE LINE TYPE IV TAPE
- ⑨ TEMPORARY 24" SOLID WHITE LINE (STOP BAR) TYPE IV TAPE

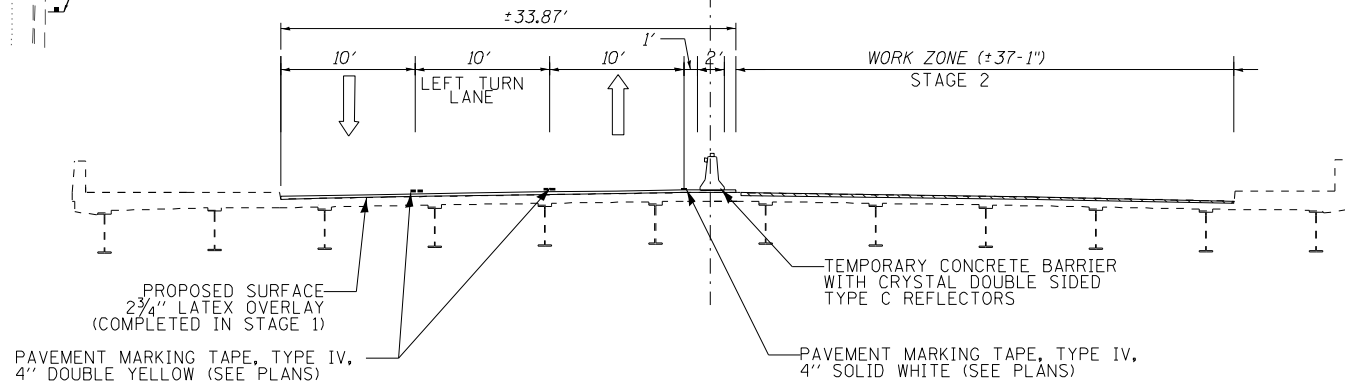
NOTES

1. BEACONS SHOULD BE PLACED ON TRAFFIC SIDE OF SIGN
2. PROVIDE TEMPORARY CONCRETE BARRIER PER STD 704001
3. PROVIDE PAVEMENT MARKING PER STD TC-24
4. KEEP PEDESTRIAN ACCESS DURING CONSTRUCTION



STAGE II CONSTRUCTION SEQUENCE

1. RELOCATE PROTECTIVE SHIELD AS SHOWN ON THE PLANS ACCORDING TO STAGE II.
 2. INSTALL TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG EASTBOUND 47TH STREET. ACCORDING TO THE STAGE II MAINTENANCE OF TRAFFIC PLAN SHEETS REDUCING TO 10' LANES IN EACH DIRECTION TO SHIFT TRAFFIC TO THE NORTH SIDE OF 47TH STREET.
 3. FOLLOW STRUCTURAL SCOPE OF WORK FOR BRIDGE DECK OVERLAY AND SLAB REPAIR (SEE STRUCTURAL PLANS SHEET NO.19)
 4. REMOVE TRAFFIC CONTROL DEVICES, WORK ZONE PAVEMENT MARKING, AND SIGNAGE ALONG EASTBOUND 47TH STREET, OPEN LANES ALONG EASTBOUND 47TH STREET.
- POST STAGE
1. INSTALL PERMANENT PAVEMENT MARKING ON DISTURBED PAVEMENT MARKING ALONG 47TH STREET.



47TH ST STAGE II (LOOKING EAST)

FILE NAME = P:\2023\ME23025 PTB 205-03 01 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\12-D162R90-sht-not-stage2.dgn
 PLOT SCALE = 100.0000' / in.
 USER NAME = Inguyen



USER NAME = inguyen	DESIGNED - JC	REVISED -
	DRAWN - LDN	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - TVN	REVISED -
PLOT DATE = 1/24/2024	DATE - 12/07/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 90 /94 (DAN RYAN) AT 47TH STREET
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL-STAGE II**

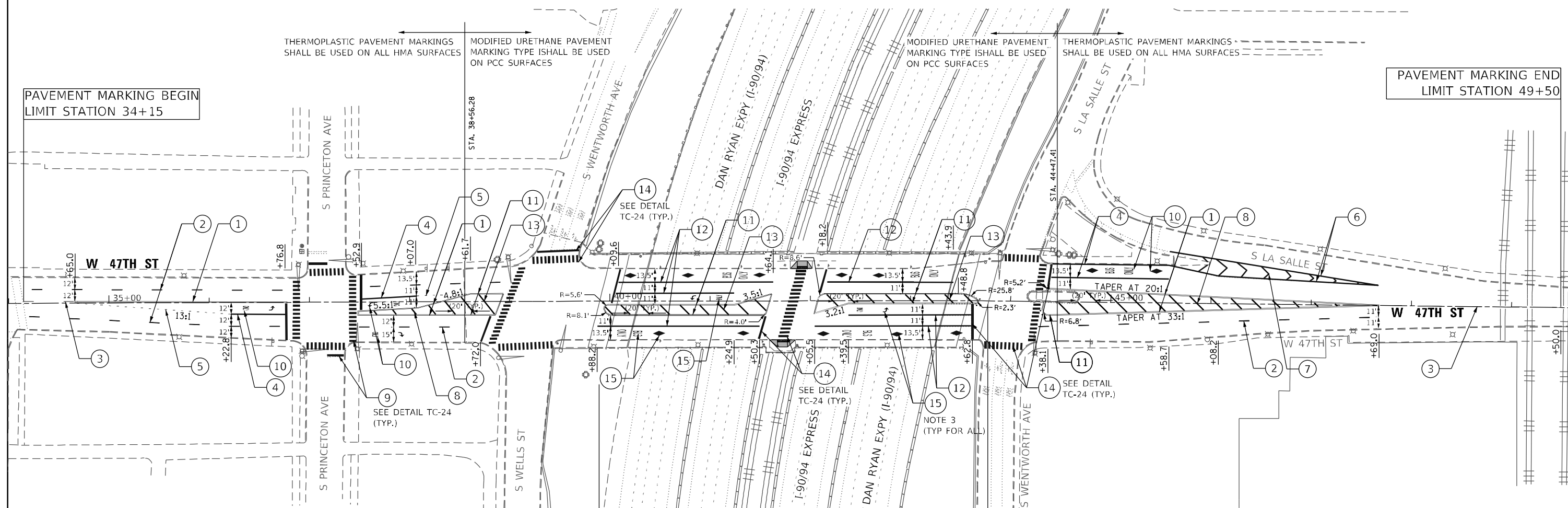
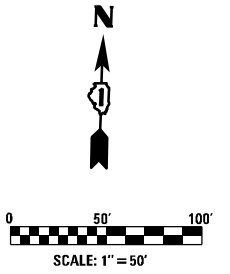
SCALE: 1"=50' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83	SHEET NO. 12
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				
CONTRACT NO. 62R90				

P:\2023\ME23025 PTB 205-03 01 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\12-D162R90-sht-not-stage2.dgn

PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE YELLOW LINE @ (11" C-C) | ⑪ MODIFIED URETHANE PAVEMENT MARKING, 4" DOUBLE YELLOW LINE @ (11" C-C) |
| ② THERMOPLASTIC PAVEMENT MARKING, 4" WHITE LANE LINE (6' DASH - 18' SKIP) | ⑫ MODIFIED URETHANE PAVEMENT MARKING, 6" SOLID WHITE LINE |
| ③ THERMOPLASTIC PAVEMENT MARKING, 4" YELLOW CENTERLINE (6' DASH - 18' SKIP) | ⑬ MODIFIED URETHANE PAVEMENT MARKING, 12" SOLID YELLOW DIAGONALS @ (20' C-C) |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE LINE | ⑭ MODIFIED URETHANE PAVEMENT MARKING, 24" SOLID WHITE LINE |
| ⑤ THERMOPLASTIC PAVEMENT MARKING, 6" WHITE LINE (2' DASH - 6' SKIP) | ⑮ MODIFIED URETHANE PAVEMENT MARKING, LETTERS AND SYMBOLS |
| ⑥ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE LINE | |
| ⑦ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID WHITE LINE | |
| ⑧ THERMOPLASTIC PAVEMENT MARKING, 12" SOLID YELLOW DIAGONALS @ (20' C-C) | |
| ⑨ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LINE | |
| ⑩ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS | |



- NOTES**
1. THERMOPLASTIC PAVEMENT MARKINGS ARE FOR HMA PAVEMENT.
 2. THE CONTRACTOR SHALL PLACE PROPOSED PAVEMENT MARKINGS IN ACCORDANCE WITH CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS DETAIL (TC-24).
 3. MODIFIED URETHANE PAVEMENT MARKING SHALL BE USED ON BRIDGE DECK
 4. ALL DRAINAGE STRUCTURES WITHIN THE PROJECT LIMITS SHALL BE CLEANED

FILE NAME = P:\2023\ME23025 PTB 205-03 01 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\13-0162R90-sht-PMK.dgn
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 USER NAME = Inguyen



USER NAME = inguyen	DESIGNED - JC	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN - LDN	REVISED -
PLOT DATE = 12/12/2023	CHECKED - TVN	REVISED -
	DATE - 12/07/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

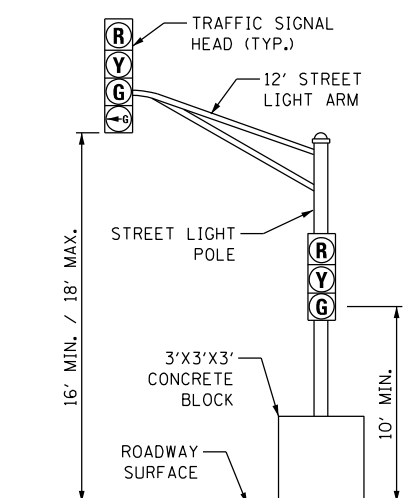
FAI 90 / 94 (DAN RYAN) AT 47TH STREET
PAVEMENT MARKING PLAN

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

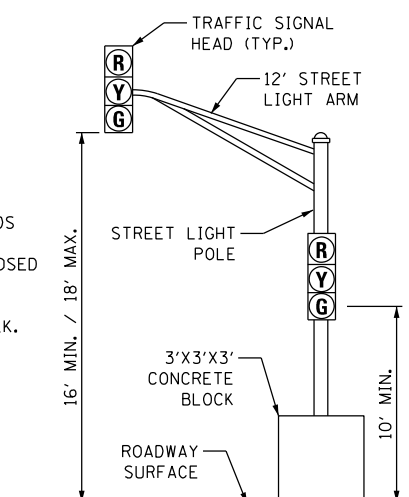
F.A.I. RTE. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83	SHEET NO. 13
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

P:\2023\ME23025 PTB 205-03 01 Ph II VV Roderick\W001_62R90_47th Street Over Dan Ryan\01-CADD\02-Shts\13-0162R90-sht-PMK.dgn

TEMPORARY TRAFFIC SIGNAL DETAIL



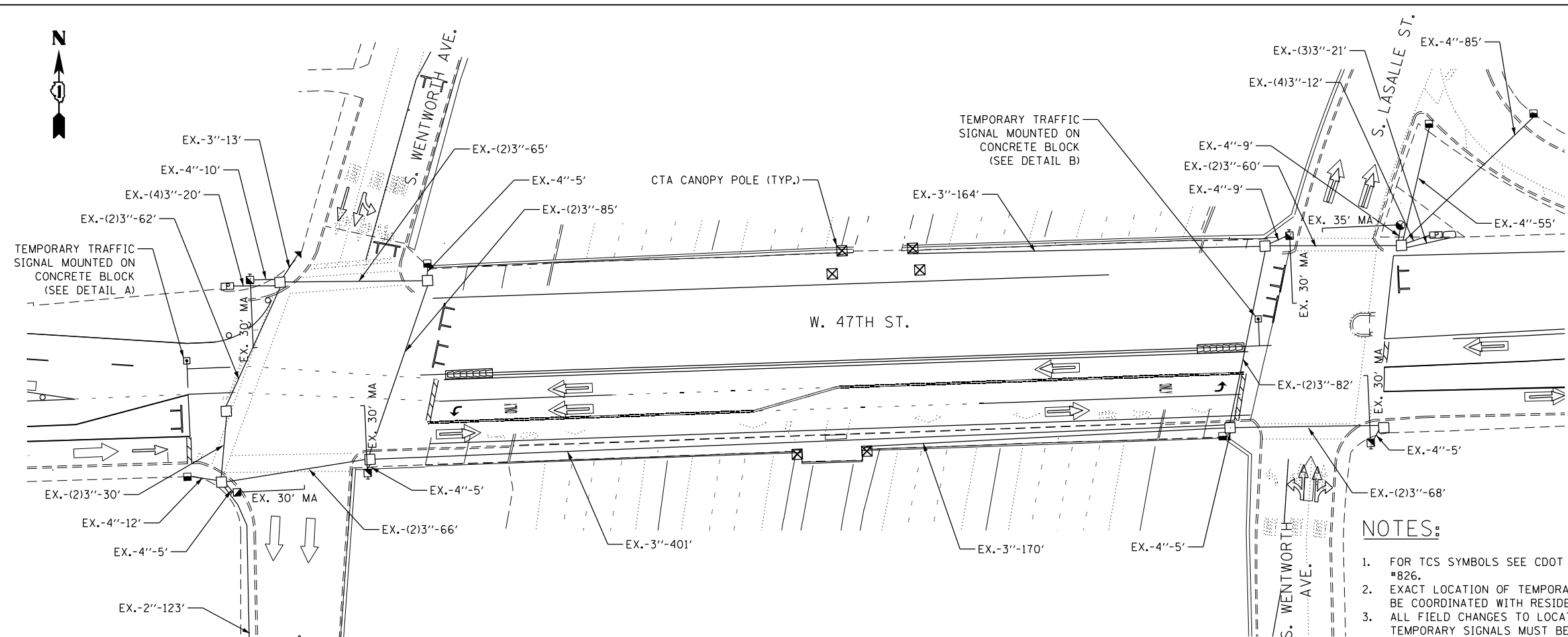
DETAIL A



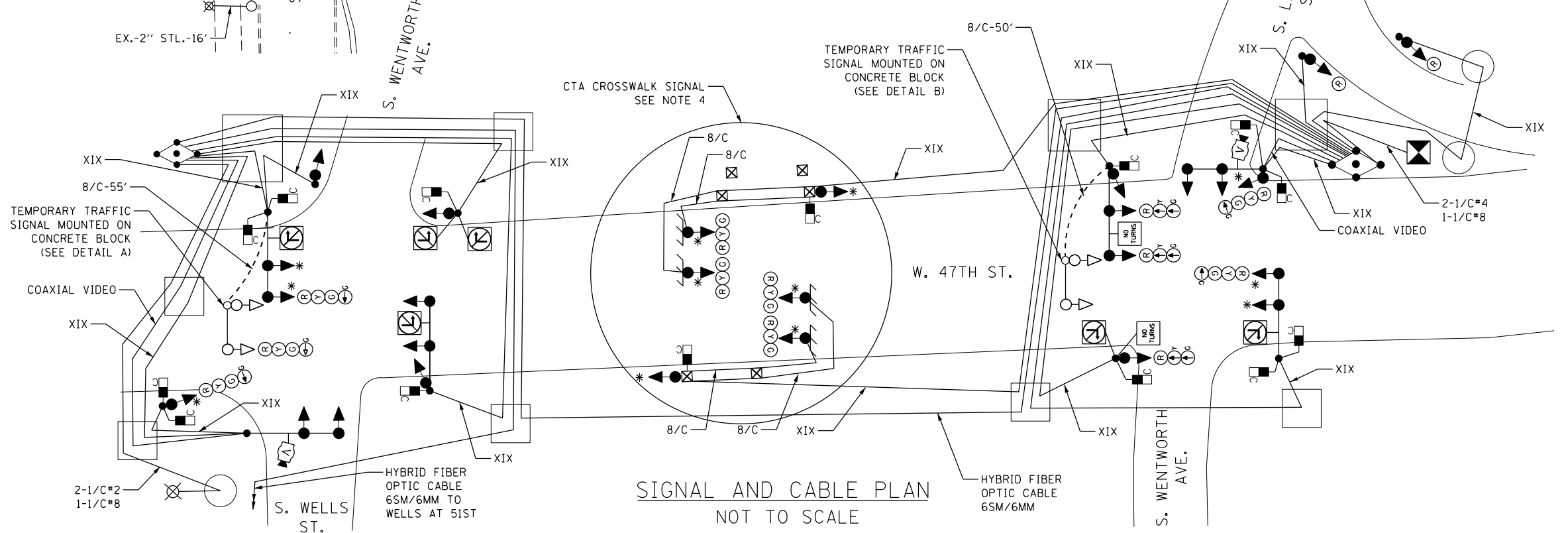
DETAIL B

NOTES:

1. FOR TCS SYMBOLS SEE CDOT DEO DRAWING #826.
2. EXACT LOCATION OF TEMPORARY SIGNALS TO BE COORDINATED WITH RESIDENT ENGINEER.
3. ALL FIELD CHANGES TO LOCATION OF THE TEMPORARY SIGNALS MUST BE COORDINATED WITH THE RESIDENT ENGINEER.
4. ALL SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS AT THE CTA CROSSWALK SHALL BE BAGGED AND DE-ENERGIZED WHILE THE CTA CROSSWALK IS CLOSED DURING CONSTRUCTION. ALL SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS MUST BE ENERGIZED PRIOR TO THE REOPENING OF THE CTA CROSSWALK.



FOUNDATION AND CONDUIT PLAN
SCALE: 1"=30'



SIGNAL AND CABLE PLAN
NOT TO SCALE

DATE		REVISION	
SUPERSEDES DWG. #			
TEMPORARY (MOT STAGE 1) TRAFFIC CONTROL SIGNALS S. WELLS ST. AT W. 47TH ST. S. WENTWORTH AVE. AT W. 47TH ST.			
CITY OF CHICAGO DEPT. OF TRANSPORTATION DIVISION OF ELECTRICAL OPERATIONS			
DRAFTSMAN: EEC	CHIEF DRAFTSMAN:	ENGINEER: EEC	
SUPERVISING ENGINEER: MG	ELEC. DESIGN ENGR.		DWG. NO.
ENGINEER OF ELECTRICITY:			14418
GEN'L SUPT. OF CONSTRUCTION:			
DEPUTY COMMISSIONER:			
SIZE:	SCALE: AS NOTED	DATE: 11/7/2023	

FILE NAME = 8FILES
USER NAME = 8USERNAME

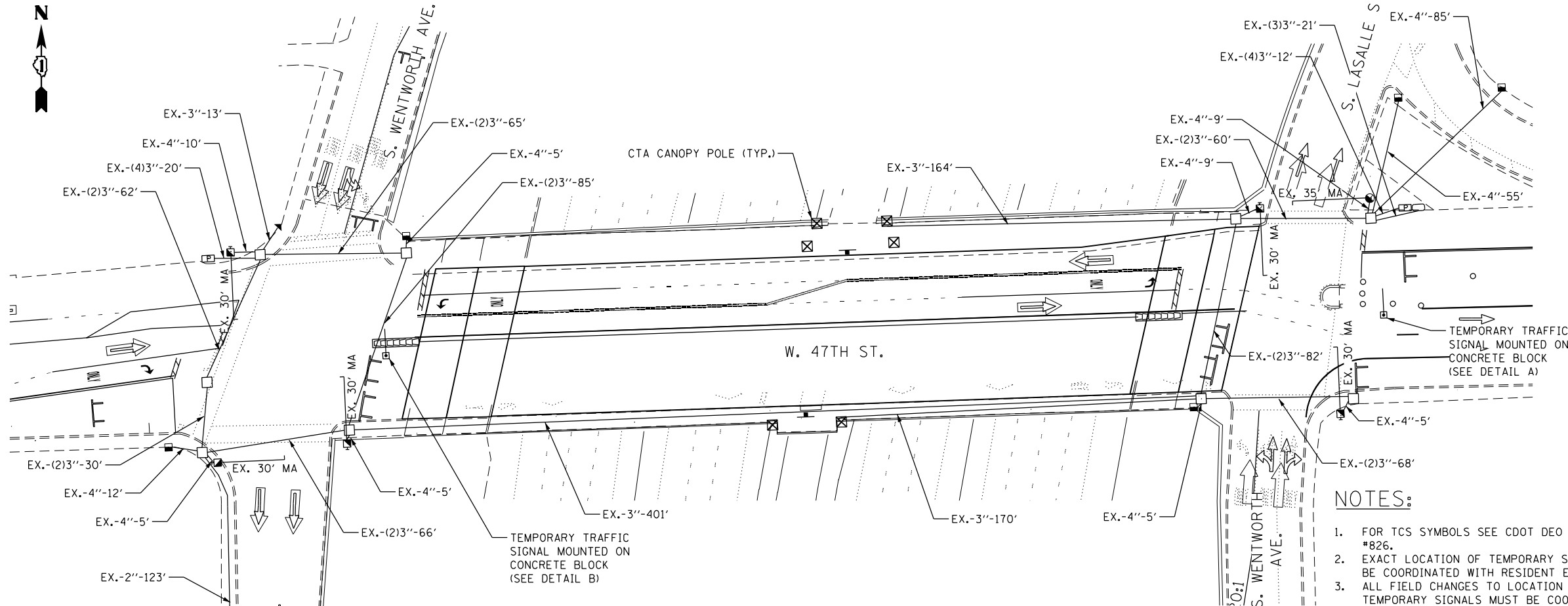


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PLOT DATE = 12/11/2023	CHECKED - EEC	REVISED -
	DATE - 11/7/2023	REVISED -

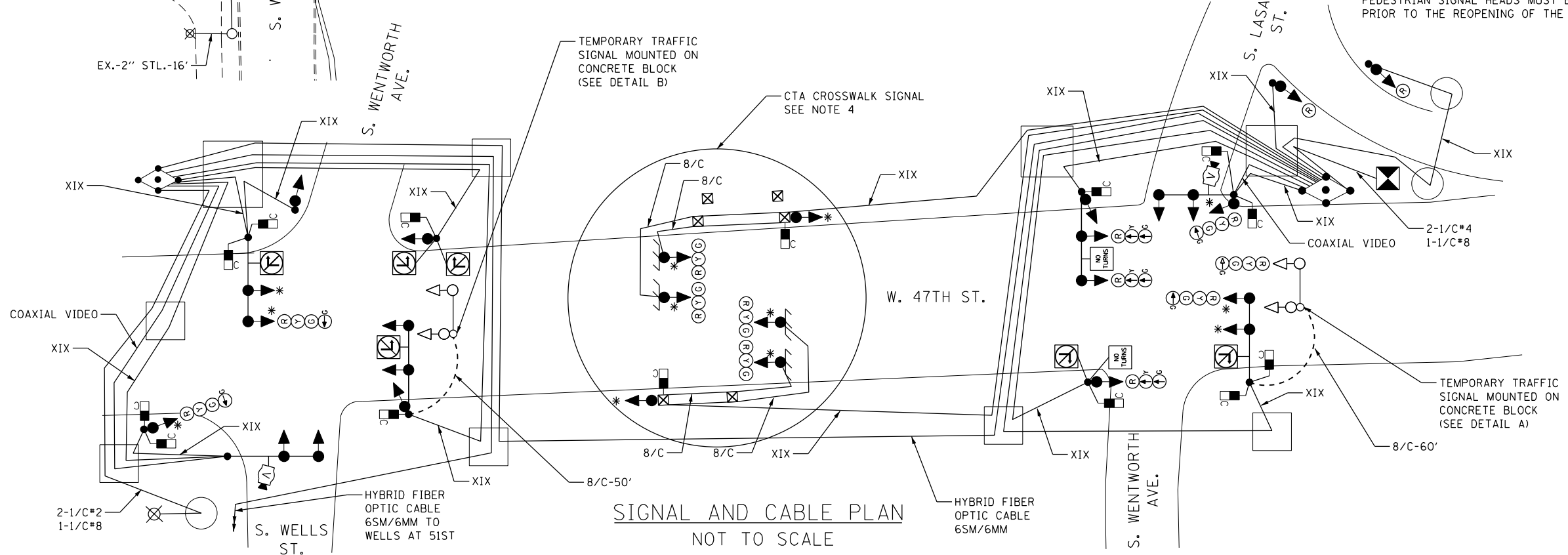
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FAI 9094 (DAN RYAN) AT 47TH STREET TEMPORARY TRAFFIC CONTROL SIGNALS (STAGE 1)			
SCALE: 1"=30'	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.I. R.T.E. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83	SHEET NO. 14
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

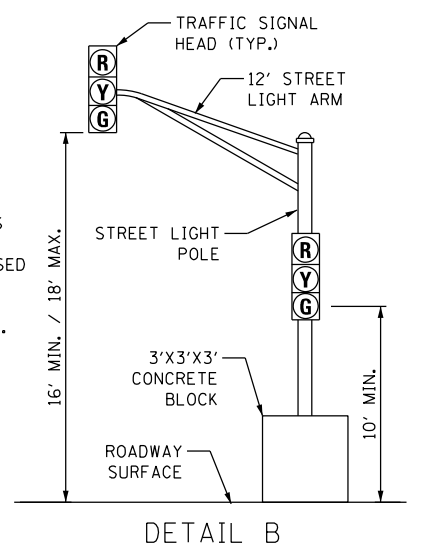
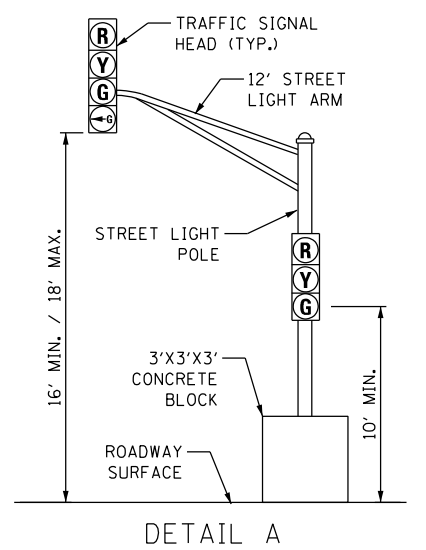


FOUNDATION AND CONDUIT PLAN
SCALE: 1"=30'



SIGNAL AND CABLE PLAN
NOT TO SCALE

TEMPORARY TRAFFIC SIGNAL DETAIL



- NOTES:**
1. FOR TCS SYMBOLS SEE CDOT DEO DRAWING #826.
 2. EXACT LOCATION OF TEMPORARY SIGNALS TO BE COORDINATED WITH RESIDENT ENGINEER.
 3. ALL FIELD CHANGES TO LOCATION OF THE TEMPORARY SIGNALS MUST BE COORDINATED WITH THE RESIDENT ENGINEER.
 4. ALL SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS AT THE CTA CROSSWALK SHALL BE BAGGED AND DE-ENERGIZED WHILE THE CTA CROSSWALK IS CLOSED DURING CONSTRUCTION. ALL SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS MUST BE ENERGIZED PRIOR TO THE REOPENING OF THE CTA CROSSWALK.

DATE		REVISION	
SUPERSEDES DWG. #			
TEMPORARY (MOT STAGE 2) TRAFFIC CONTROL SIGNALS S. WELLS ST. AT W. 47TH ST. S. WENTWORTH AVE. AT W. 47TH ST.			
CITY OF CHICAGO DEPT. OF TRANSPORTATION DIVISION OF ELECTRICAL OPERATIONS			
DRAFTSMAN: EEC	CHIEF DRAFTSMAN:	ENGINEER: EEC	DWG. NO. 14418
SUPERVISING ENGINEER: MG	ELEC. DESIGN ENGR.		
ENGINEER OF ELECTRICITY:			
GEN'L SUPT. OF CONSTRUCTION:			
DEPUTY COMMISSIONER:			
SIZE:	SCALE: AS NOTED	DATE: 11/7/2023	
F.A.I. R.T.E. 90/94	SECTION FAI 90/94 22 BJ	COUNTY COOK	TOTAL SHEETS 83
FED. ROAD DIST. NO. 1 ILLINOIS		FED. AID PROJECT	SHEET NO. 15

FILE NAME = 8FILES
DRAWING NO. = 14418
USER NAME = GGDENNER



USER NAME = ggdenner	DESIGNED - GJG	REVISED -
PLOT SCALE = 60,0000' / in.	DRAWN - GJG	REVISED -
PLOT DATE = 12/11/2023	CHECKED - EEC	REVISED -
	DATE - 11/7/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 9094 (DAN RYAN) AT 47TH STREET
TEMPORARY TRAFFIC CONTROL SIGNALS (STAGE 2)**
SCALE: 1"=30' SHEET NO. OF SHEETS STA. TO STA.

PROPOSED	PRESENT	
		SIGNAL, TRAFFIC 3 SECTION 1-WAY ADJUSTABLE, 12" OR AS NOTED
		SIGNAL, TRAFFIC 3 SECTION 2-WAY ADJUSTABLE, 12" OR AS NOTED
		SIGNAL OPTICALLY PROGRAMMED
		SIGNAL, PEDESTRIAN, COUNTDOWN
		SIGNAL, PEDESTRIAN, DON'T WALK/WALK
		SIGNAL FACE ARROW, 12" COLOR AS NOTED
		SIGNAL FACE, 1 SECTION YELLOW/GREEN ARROW DUAL INDICATION
		PUSH BUTTON, PEDESTRIAN
		SIGN, ILLUMINATED, WITH MESSAGE OR SYMBOL AS INDICATED
		MAST ARM, MONOTUBE, STEEL. SIZE AS INDICATED (SEE DWG. #870)
		MAST ARM, TRUSS, ALUMINUM. SIZE AS INDICATED
		CONTROLLER, TRAFFIC SIGNAL. PEDESTAL OR BASE MOUNTED AS INDICATED
		CONTROLLER, STREET LIGHTING. PEDESTAL OR BASE MOUNTED. (DWG. 876 or 880)
		CONTROLLER, STREET LIGHTING. POLE MOUNTED (DWG. #11940)
		POLE, WOOD. COMMONWEALTH EDISON COMPANY, SERVICE
		POLE, CITY STEEL, ANCHOR BASE, 34'-6", 7 GA. 10" DI A. AND 15" B.C. 24"x7" FND. W/1 1/4" ANCHOR RODS DRG. #818.
		POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3 GA. 10" DIA. AND 15" B.C. 24"x9" FND. W/1 1/4" ANCHOR RODS DRG. #818 (16', 20' or 26' M.A.)
		POLE, CITY STEEL, ANCHOR BASE, 34'-6", 3GA., 11" DIA. AND 17 3/4" B.C. 30"x9" FND. W/1 1/4" ANCHOR RODS DRG. #816. (30' M.A.)
		POLE, CITY STEEL, ANCHOR BASE 34'-6", 3 GA. 12 1/2" DIA. AND 16 1/2" B.C. 30"x11" FND. W/1 1/2" ANCHOR RODS DRG.#817. (35', 40' or 44' M.A.)
		POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA. 10" DIA., WITH 3 GA. BAL. HSG. BASE AND 17 1/4" B. C. ON 30"x9' FND. W/ 11/4" ANCHOR RODS DRG. #816.
		POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 7 GA. WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DRG. #716.
		POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6", 3 GA., WITH STEEL BAL. HSG. BASE AND FND. W/10" D. B.C. AND 1" ANCHOR RODS DWG.#719.
		POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 7 GA., AND ALUMINUM RESIDENTIAL DAVIT AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DWG.#565 (CONCRETE) OR DWG.#936 (HELIX).
		POLE, CITY STEEL, ANCHOR BASE, 20', 27'-6", 29'-6" 3 GA., AND FND. WITH 10" B.C. AND 1" ANCHOR RODS DWG. #565 (CONCRETE) OR DWG.#936 (HELIX).
		POLE, CITY STEEL, ANCHOR BASE, 32'-6", 7 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DWG. #753.
		POLE, CITY STEEL, ANCHR BASE, 32'-6", 3 GA., AND FND. WITH 11 1/2" B.C. AND 1" ANCHOR RODS DWG. #753.
		POLE, CITY STEEL, ANCHOR BASE, 32'-6" 7 GA., ALUM. BHB AND FND. WITH 15" B.C.-24"x7" WITH 1" ANCHOR RODS DRG. #691.
		POLE, CITY STEEL, ANCHOR BASE, 32'-6", 3 GA., ALUM. BHB AND FND. WITH 15" B.C. 24"x7" WITH 1" ANCHOR RODS DWG. #691.
		POLE, CITY ALUMINUM, WITH ROUND BAL. HSG. BASE, 25', 28', or 30' ON FND. WITH 14" B.C., ACQUIRED FROM CHICAGO PARK DISTRICT.
		POLE, CITY STEEL, EMBEDDED, 4"x 9"x 35' 7 GA., TAPERED TUBULAR. (DWG. #658)
		POLE, CITY STEEL, EMBEDDED, 4"x 9"x 35' 3 GA., TAPERED TUBULAR. (DWG. #658)
		POLE, CITY STEEL, EMBEDDED. (ACQUIRED FROM CTA)
		COLUMN, ELEVATED STRUCTURE
		POLE, WOOD. (SIZE AS NOTED)
		POLE, FOUNDATION WITH ELBOWS AS INDICATED. (SIZE AS NOTED)
		POLE, ORNAMENTAL OR OTHER, AS INDICATED ON THE PLANS
		RESIDENTIAL STREET LIGHTING CONTROLLER

PROPOSED	PRESENT	
		MANHOLE, 3'X4'X4' 24" F & C (DWG.#730) (A) 30" F & C (DWG#729) (B)
		MANHOLE, 4'X6'X6' 24" F & C (DWG.#732) (C) 30" F & C (DWG#733) (D)
		HANDHOLE, HEAVY DUTY, 36" I.D. (DWG.#866) 24" F & C (E). (DWG#871) 30" F & C (F)
		HANDHOLE, CIRCULAR WITH 24" FRAME & COVER, 30" I.D. (#867) (G)
		FOUNDATION, CONTROLLER OR PEDESTAL, 13" B.C., 20"X5' (DWG. #709)
		FOUNDATION, TRAFFIC CONTROLLER DWG. #854. F.A. TERMINAL FND. DWG. #11972
		FOUNDATION, TRAFFIC TYPE "P", BASE MOUNT. (DWG. #888)
		FOUNDATION, CONTROLLER STREET LIGHT ,SPECIAL, 100A & 200A. (DWG.#876 & # 880)
		FOUNDATION, TRANSCLOSURE; TRANSCLOSURE HOUSING. (DWG.# 583 & #891)
		CONTROLLER, UNDERPASS LIGHTING 120V. & 240V. (DWG. #860 & #861)
		MANHOLE, UTILITY, E=COMMONWEALTH EDISON; T=ILL.BELL TEL.; G=PEOPLES GAS; W=CITY WATER; P=CHGO PARK DISTRICT; CTA=C.T.A.; S= SEWER JUNCTION BOX, IN PAVEMENT (DWG. #815)
		DETECTOR LOOP IN PAVEMENT
		CONDUIT or P.V.C., NUMBER, SIZE & TYPE. (AS NOTED)
		CONDUIT or P.V.C. ENCASED IN CONCRETE. (SECTION or NUMBER OF CONDUIT INDICATED)
		LUMINAIRE, H.P.S.V. 400W LAMP, 240V, SEMI-CUTOFF
		LUMINAIRE, H.P.S.V. 400W LAMP, 240V, CUTOFF
		LUMINAIRE, H.P.S.V. 310W LAMP, 240V
		LUMINAIRE, H.P.S.V. 310W LAMP 240V, CUTOFF
		LUMINAIRE, H.P.S.V. 150W LAMP, 240V
		LUMINAIRE, H.P.S.V. 150W LAMP, 120V
		LUMINAIRE, H.P.S.V. 250W LAMP, 120V, (ALLEY LIGHT)
		LUMINAIRE, H.P.S.V. 250W LAMP, 120V
		LUMINAIRE, H.P.S.V. 400W LAMP, 240V, (FLOOD LIGHT)
		TERMINAL, CABINET F.A. & P.C.
		FIRE ALARM BOX, MOUNTED
		FIRE ALARM BOX, POLE MOUNTED
		CABLE, TRAFFIC SIGNAL, COMMUNICATION, 1-PAIR #14 SHIELDED, IN CONDUIT
		CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C- #4, 600 V. EPR. IN CONDUIT
		CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2 1/C-#2 or #1/0 600V. EPR IN CONDUIT
		CABLE, TRAFFIC SIGNAL POWER SUPPLY, 2/C-#10 or #6, 600V NSRI, IN CONDUIT
		CABLE, TRAFFIC SIGNAL, 7/C-#12 or #14, 600V. EPR IN CONDUIT
		CABLE, TRAFFIC SIGNAL, 10/C-#12 600V. EPR IN CONDUIT
		CABLE, TRAFFIC SIGNAL, 14/C-#14, 600V. EPR IN CONDUIT
		CABLE, TRAFFIC SIGNAL, 19/C-#12 600V, EPR IN CONDUIT
		CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN PARKWAY
		CABLE, STREET LIGHT, 2 1/C-#6, 600V. RINS IN CONDUIT
		CABLE, STREET LIGHT, 2 1/C-#6 EPRN 600V. & 1 1/C-#8 GREEN, TRIPLEXED, IN CONDUIT
		CABLE, STREET LIGHT, 3 1/C-#1/0, or #2/0, or #4, 600V. EPR IN CONDUIT
		WIRE, STREET LIGHT, 2 1/C-#6, HDNS. AERIAL
		WIRE, STREET LIGHT, 2 1/C-#6 & 1 1/C #8, HDNS. AERIAL
		CABLE, STREET LIGHT AERIAL, 3 1/C-#4 or #2 SELF SUPPORTING, 600V EPR
		WIRE, F.A. & P.C. AERIAL, 1/C-#10, NUMERAL DENOTES QUANTITY
		CABLE, F.A. & P.C. AERIAL, W/ MESSENGER #19-(NUMBER OF PAIRS AS INDICATED)
		CABLE, F.A. & P.C. AERIAL, SELF SUPPORTING, #19-(NUMBER OF PAIRS AS INDICATED)
		CABLE, F.A. & P.C., IN CONDUIT, #19-(NUMBER OF PAIRS AS INDICATED)
		DOWNLIGHT ASSEMBLY. (DWG. #850)
		LIGHT, TRAFFIC SAFETY ISLAND
		FLASHING BEACON & DOWNLIGHT

C.M.H. LUMINAIRES		
PROPOSED	PRESENT	
		LUMINAIRE, C.M.H. 315W LAMP, 240V
		LUMINAIRE, C.M.H. 315W LAMP, 240V, (FLOOD)
		LUMINAIRE, C.M.H. 210W LAMP, 240V
		LUMINAIRE, C.M.H. 140W LAMP, 240V
		LUMINAIRE, C.M.H. 140W LAMP, 120V, (ALLEY)
		LUMINAIRE, C.M.H. 90W LAMP, 240V
		LUMINAIRE, C.M.H. 90W LAMP, 240V (ACORN)
		LUMINAIRE, C.M.H. 60W LAMP, 240V (ACORN)

H.P.S.V. ORNAMENTAL LUMINAIRES			
PROPOSED	PRESENT		
		310W PENDANT	(240V)
		400W PENDANT	(240V)
		250W PENDANT	(240V)
		150W ACORN	(120V)
		150W ACORN	(240V)
		50W ACORN	(240V)
		100W ACORN	(240V)
		150W GLOBE	(240V)
		100W GLOBE	(240V)
		50W GLOBE	(240V)

L.E.D. LUMINAIRES		
PROPOSED	PRESENT	
		(400W HPSV EQUIVALENT), 240V
		(100W HPSV EQUIVALENT), 240V, ACORN
		(310W HPSV EQUIVALENT), 240V
		(100/150W HPSV EQUIVALENT), 240V ACORN
		(250W HPSV EQUIVALENT), 240V
		(50W HPSV EQUIVALENT), 240V, ACORN

F 01-08-14	ADDED LED LUMINAIRES	A.VIEW
E 09-19-13	ADDED CMH LUMINAIRES	A.VIEW
D 02-06-04	REVISED/REDRAW	R.POOL/B.I.
C 04-01-02	REVISED/REDRAW	R.POOL/B.I.
B 12-4-01	ADDED ORNAMENTAL SYMBOLS	
A 8-6-96	REDRAWN	
DATE	REVISION	
SUPERSEDES DWG. #		
WORK ORDER NO. _____ DATE _____		
COST ALLOCATION ACCOUNT _____		
APPROPRIATION ACCOUNT (MATERIAL _____ LABOR _____)		
STANDARD CODE FOR TRAFFIC SIGNALS/ STREET LIGHTING		
CITY OF CHICAGO SEPT. OF TRANSPORTATION DIVISION OF ENGINEERING ELECTRICAL SECTION		
DRAFTSMAN: R. IVY	CHIEF DRAFTSMAN: R. CARTER	ENGINEER: R. POOL/R.C./W.T.
SUPERVISING ENGINEER:	ELEC. DESIGN ENGR.	DWG. NO.
ENGINEER OF ELECTRICITY:		826
GEN'L SUPT. OF CONSTRUCTION:		
DEPUTY COMMISSIONER:		
SIZE: 22" 36"	SCALE: NONE	DATE: 09-19-13

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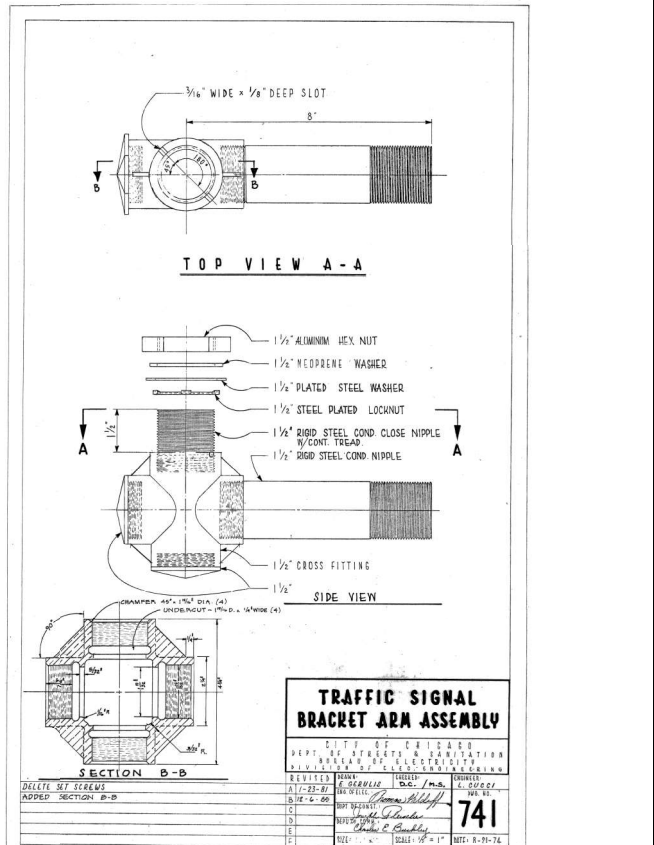
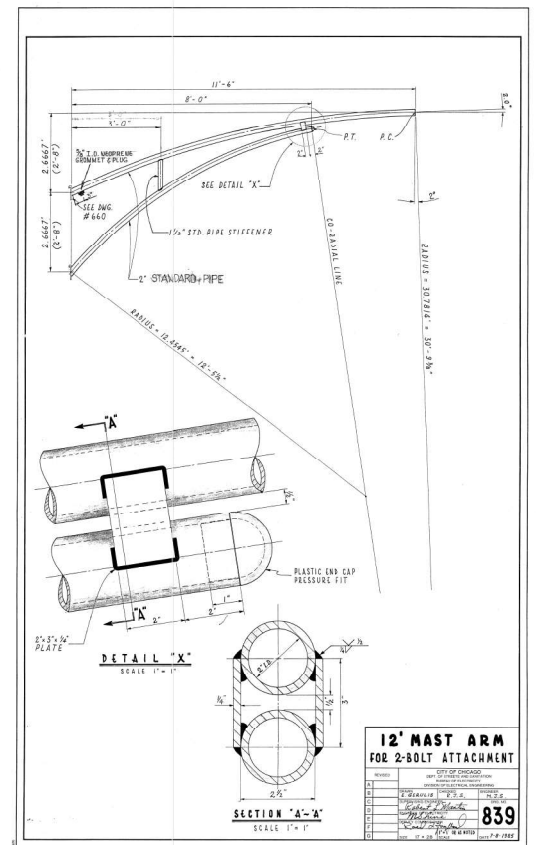
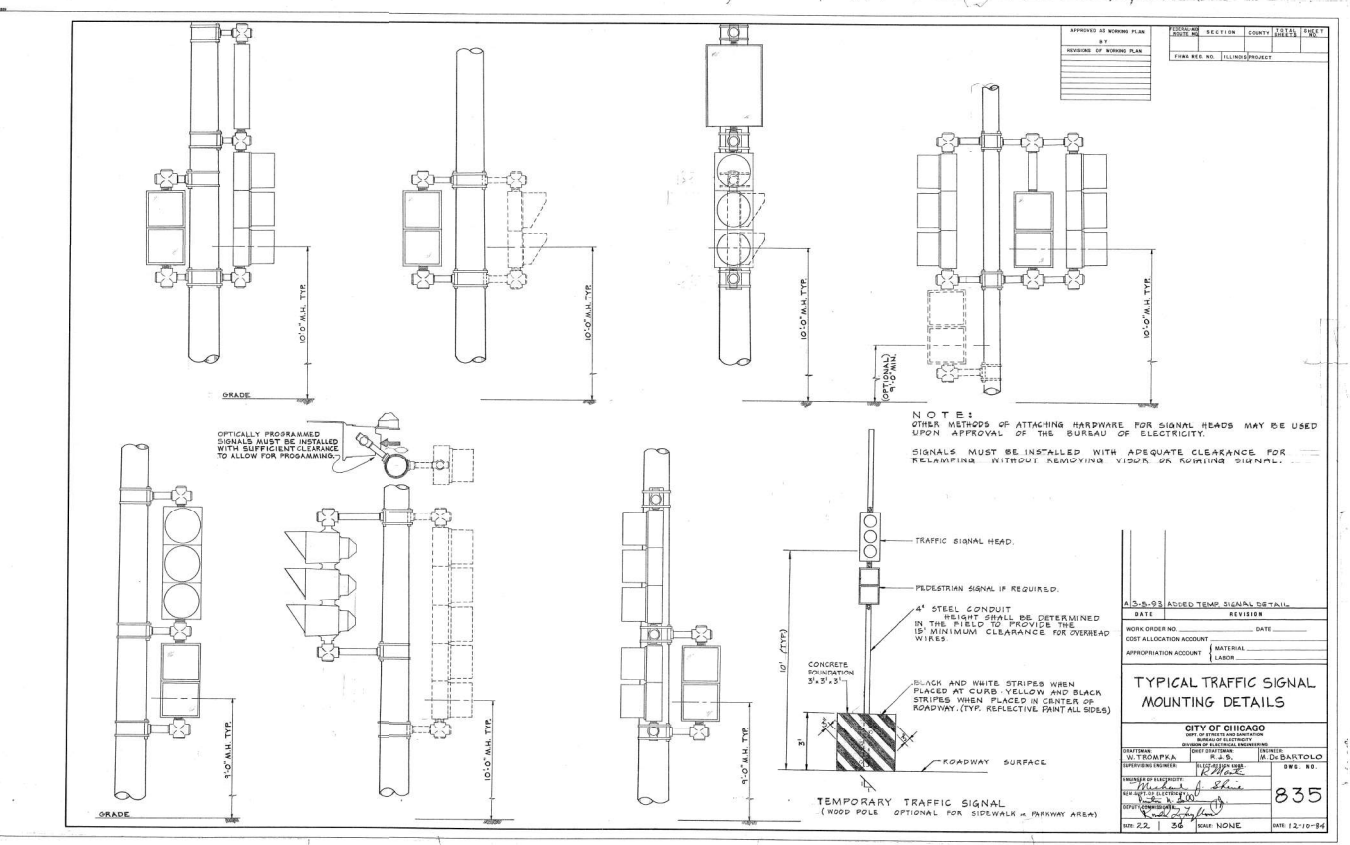
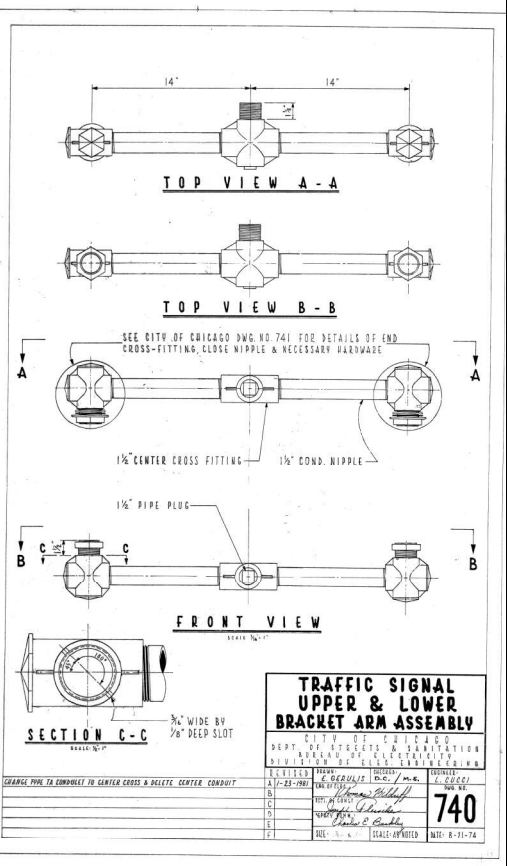
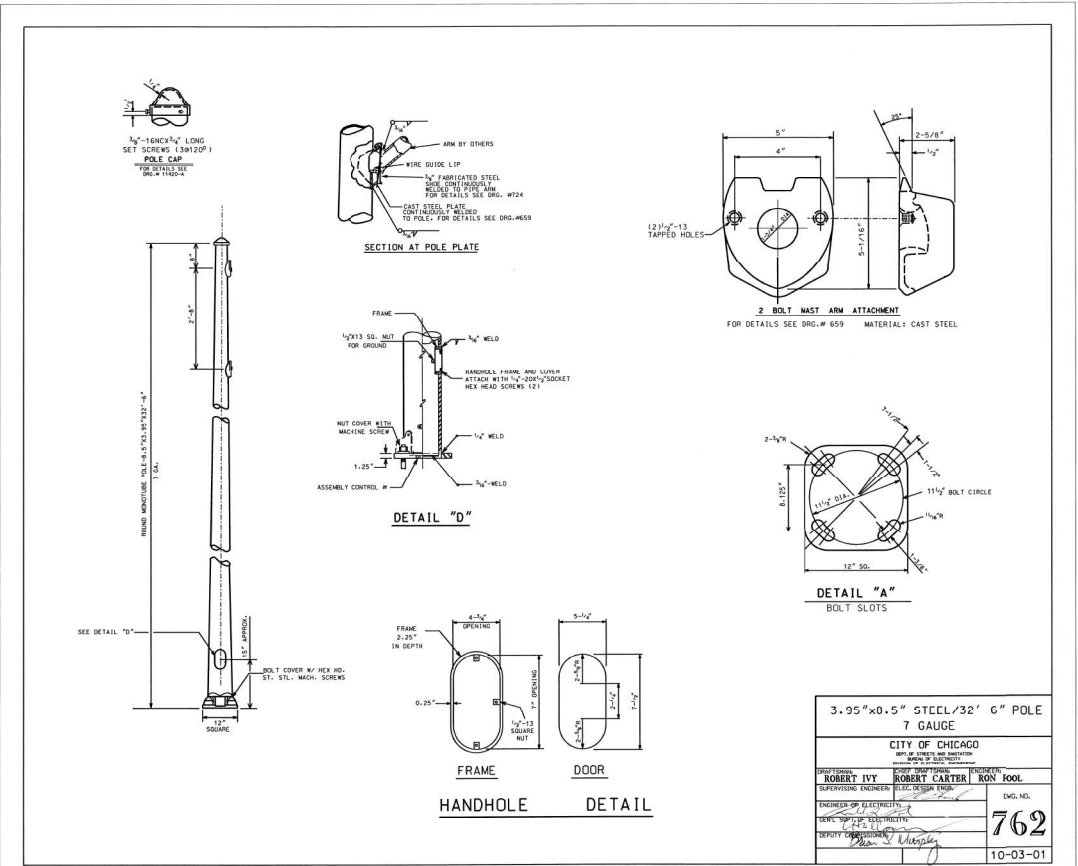
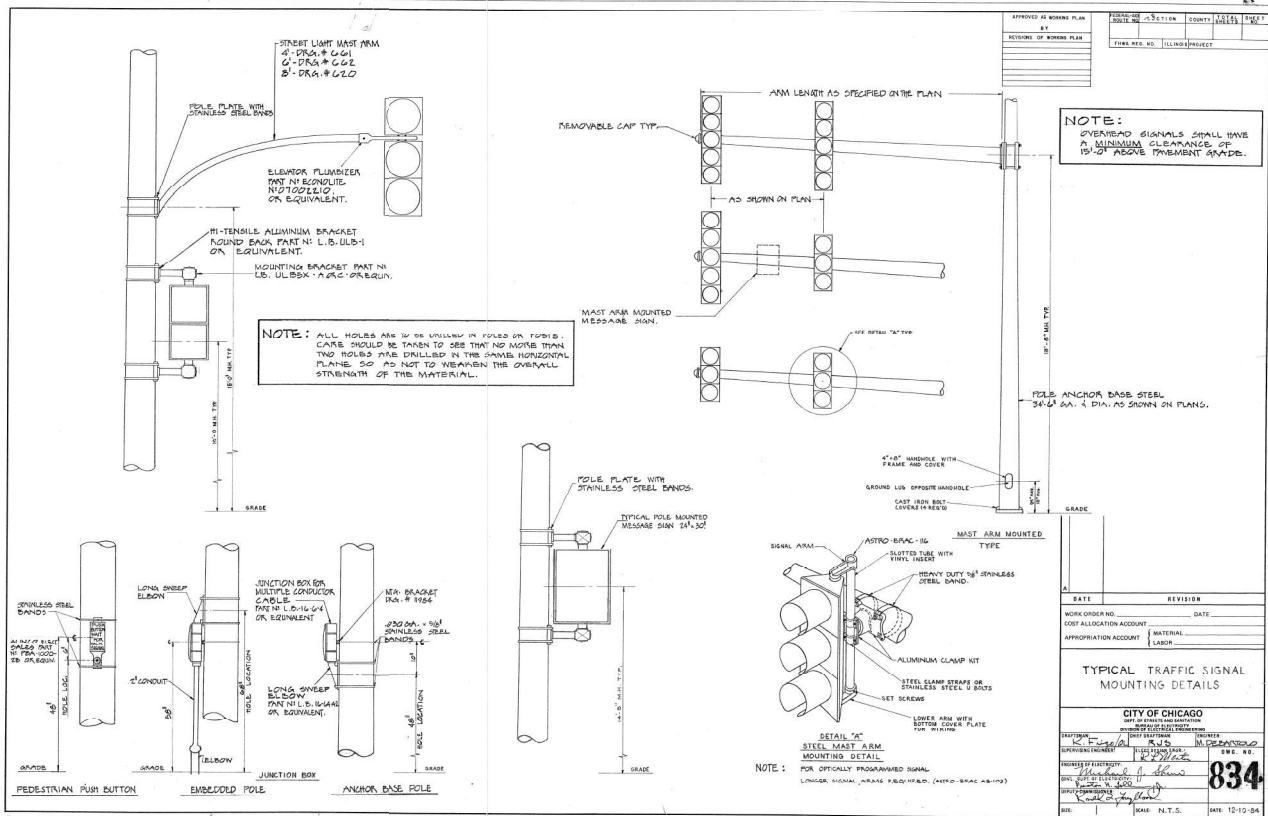


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PLOT SCALE = 60,0000' / in.	DRAWN - GJG	REVISED -
PLOT DATE = 12/11/2023	CHECKED - EEC	REVISED -
	DATE - 11/7/2023	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI 9094 (DAN RYAN) AT 47TH STREET
CDOT-DEO STANDARD CODE FOR TRAFFIC SIGNALS /STREET LIGHTING**

F.A.I. R/E:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	16
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



FILE NAME = SINGH
USER NAME = SINGH
USER NAME = SINGH

SINGH
SINGH & ASSOCIATES, INC. | CONSULTING ENGINEERS

USER NAME	= ggedemer
DESIGNED	- GJG
REVISION	-
DRAWN	- GJG
REVISION	-
CHECKED	- EEC
REVISION	-
DATE	- 11/7/2023
REVISION	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI 9094 (DAN RYAN) AT 47TH STREET
CDOT-DEO STANDARD DRAWINGS FOR TRAFFIC SIGNALS

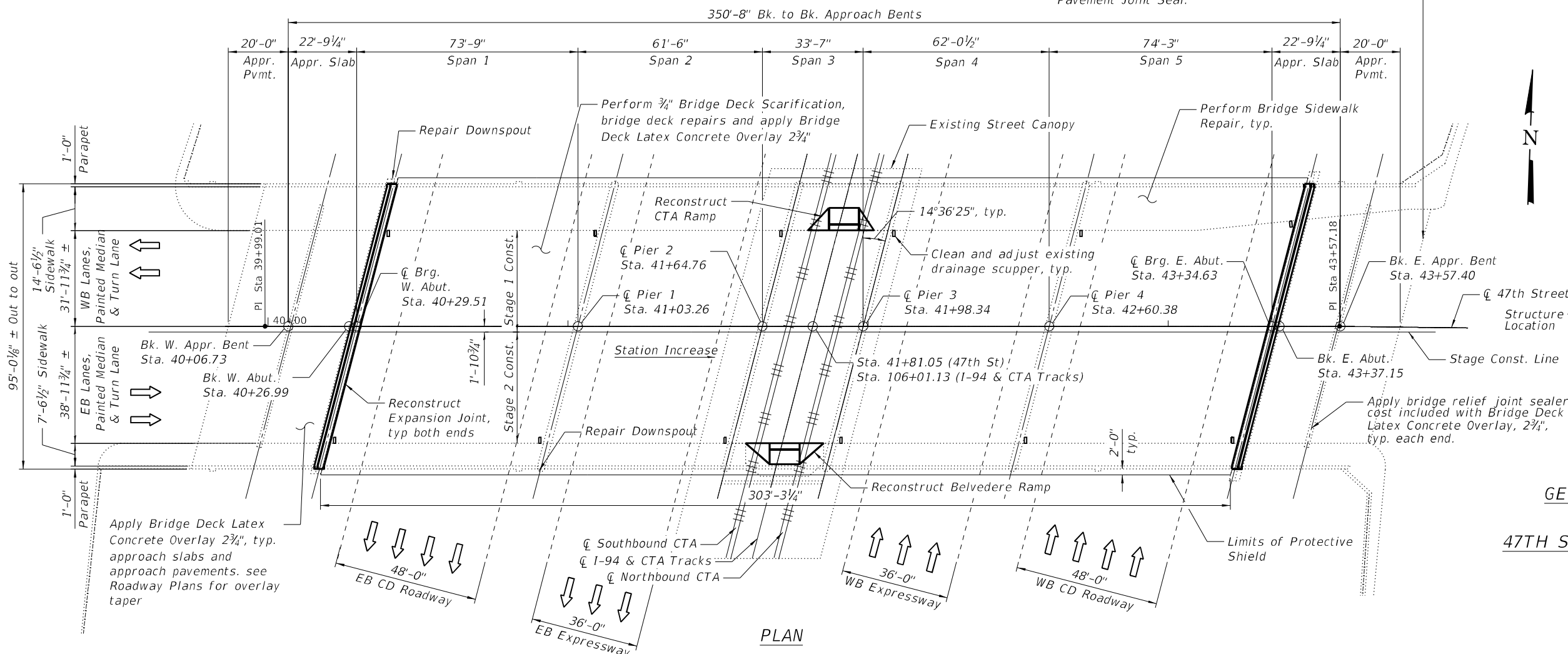
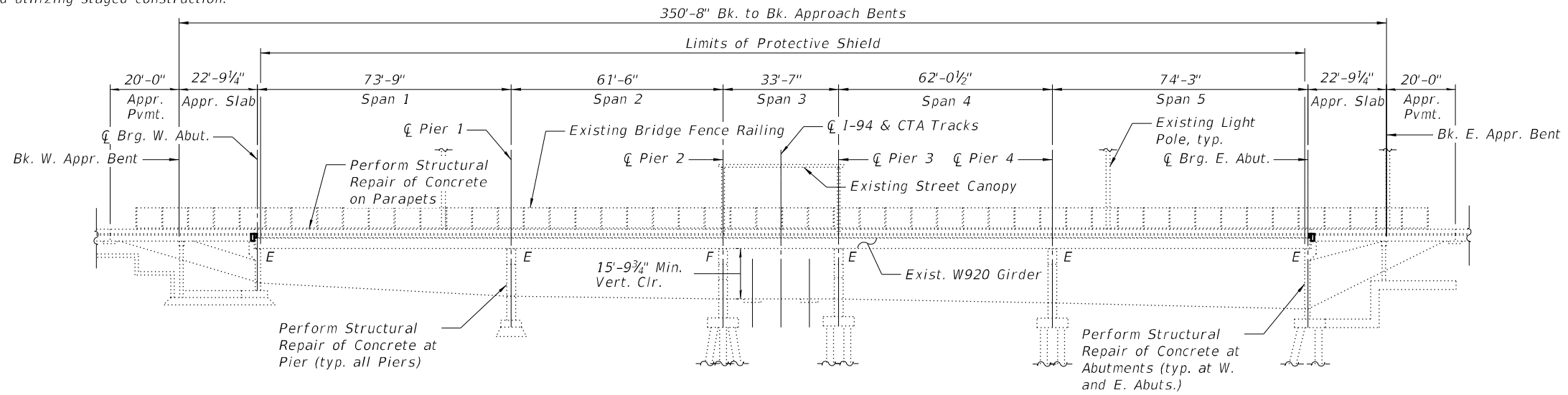
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	17
				CONTRACT NO. 62R90
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

Existing Structure: Structure No. 016-1156 was originally constructed in 1960. In 1999, rehabilitation was performed and consisted of superstructure replacement, east abutment replacement and west abutment partial replacement. In 2005, aesthetic improvements were performed. A street canopy was erected over the middle span of the bridge, the north sidewalk was widened by 7'-0" and new expansion joints were added between the bridge and the station houses. The current bridge is a five-span structure (73'-9", 61'-6", 33'-7", 62'-0½", 74'-3" respectively from west to east along centerline of 47th Street) with total length of 350'-8" (back-to-back approach bents) and an overall width of 95'-0½" ± (out to out deck). The superstructure consists of a 7½"-thick reinforced concrete deck on continuous steel W920 girders. The substructure consists of reinforced concrete abutments, wingwalls, and multi-column piers. Piers 2, 3, and 4 and the east abutment and east abutment wingwalls are supported by footings with 24" diameter caissons from the original construction. The pier 1 is supported by the spread footing. The west abutment wingwalls are supported by creosoted piles from the original construction.

Traffic is to be maintained utilizing staged construction.

No salvage.

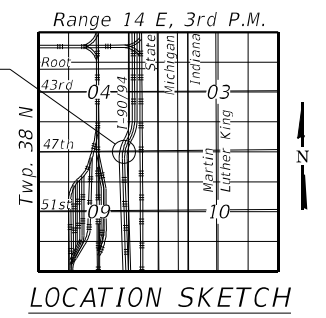


DESIGN SPECIFICATIONS
 2002 AASHTO Standard Specification for Highway Bridges, 17th Edition
RECONSTRUCTION (1999)
 AASHTO Standard Specification for Highway Bridges 1996 and 1997 Interim
RECONSTRUCTION (2005)
 2002 AASHTO Standard Specification for Highway Bridges, 17th Edition

DESIGN STRESSES
FIELD UNITS
 f'c = 4,000 psi (Superstructure)
 fy = 60,000 psi (Reinforcement)
ORIGINAL CONSTRUCTION (1960)
 f'c = 3,500 psi
 f'c = 1,400 psi (Superstructure)
 f'c = 1,000 psi (Substructure)
 fy = 60,000 psi (Reinforcement)
RECONSTRUCTION (1999)
 f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)
 fy = 50,000 psi (M270 M Grade 345)
RECONSTRUCTION (2005)
 f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)
 fy = 50,000 psi (M270 M Grade 50) (Exist. Steel)



CP Bhagat 11/7/2023
 Chandani Bhagat Date
 Licensed Structural Engineer
 State of Illinois 081-008090
 Expires 11/30/2024
 Applies to Drawings S-01 to S-21



GENERAL PLAN AND ELEVATION
F.A.I. ROUTE 90/94
47TH STREET OVER DAN RYAN (I-90/94)
SECTION 22 BJ
COOK COUNTY
STATION 41+81.05
STRUCTURE NO. 016-1156



USER NAME = \$USERS\$	DESIGNED - K W Y	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - C P B	REVISED -
PLOT DATE = \$DATES\$	DRAWN - K W Y	REVISED -
	CHECKED - C P B	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-1156
 SHEET S-01 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	18
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES

- Prior to pouring the new concrete deck for expansion joint reconstruction and deck slab repairs, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete (SSPC - SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1/4 in. deep shall be identified and reported to the Bureau of Bridges & Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas, and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.
- Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost incidental to "Concrete Removal".
- Protective coat shall be applied to the top and inside faces of parapets, sidewalks, reconstructed joint area, and surface of the new overlay.
- Fasteners shall be ASTM F 3125 Grade A325 Type 1. Fasteners shall be hot dip galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel." Bolts 3/4" diameter, holes 13/16", Unless otherwise noted.
- All new structural steel shall be galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel."
- No field welding is permitted except as specified in contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Up to 1/4 inch to be ground off the bridge deck and the bridge approach slabs.
- The Contractor shall take the necessary precautions for the protection of passing vehicles, bicycles, and pedestrians from falling objects and/or materials until completion of work.
- Bars indicated thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bar per line.
- All exposed concrete edges shall have a 3/4"x45° chamfer, except where shown otherwise.
- Hand finish of overlay material is required around the floor drains. The cost is included with Bridge Deck Latex Concrete Overlay, 2 3/4".
- Stage Construction shall be utilized to maintain traffic during construction.
- See Butt Joint and HMA Taper Detail for Approach Slab Transition details on Roadway Plans.

INDEX OF SHEETS

- S-01 General Plan & Elevation
- S-02 General Data
- S-03 Stage Construction Details
- S-04 Deck Overlay & Slab Repair Plan Details
- S-05 CTA Ramp Details
- S-06 Belvedere Ramp Details
- S-07 Expansion Joint Removal Plan & Reconstruction Plan
- S-08 Expansion Joint Details I
- S-09 Expansion Joint Details II
- S-10 Preformed Joint Strip Seal I
- S-11 Preformed Joint Strip Seal II
- S-12 Scupper Details
- S-13 Drainage System Repair Details
- S-14 Abutment Repair Details
- S-15 Pier Repair Details I
- S-16 Pier Repair Details II
- S-17 Pier Repair Details III
- S-18 Pier Repair Details IV
- S-19 Parapet & Railing Repair Details
- S-20 Joint Repair Details
- S-21 Bar Splicer Details
- S-22 through S-52 Existing Plans (Information Only)

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	32.5		32.5
Protective Shield	Sq Yd	3,337		3,337
Concrete Superstructure	Cu Yd	34.0		34.0
Protective Coat	Sq Yd	4,373		4,373
Reinforcement Bars, Epoxy Coated	Pound	7,330		7,330
Bar Splicers	Each	40		40
Preformed Joint Strip Seal	Foot	198		198
Epoxy Crack Injection	Foot	27		27
Bridge Drainage System Repair	Foot	6		6
Bridge Deck Grooving (Longitudinal)	Sq Yd	2,939		2,939
Approach Slab Repair (Full Depth)	Sq Yd	25		25
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq Yd	3,113		3,113
Cleaning Drainage System	L Sum	1		1
Bridge Deck Scarification 3/4"	Sq Yd	2,425		2,425
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft		1,288	1,288
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	Sq Ft		21	21
Deck Slab Repair (Full Depth, Type I)	Sq Yd	1		1
Deck Slab Repair (Full Depth, Type II)	Sq Yd	9		9
Bridge Sidewalk Repair (Partial Depth)	Sq Ft	17		17
Drainage Scuppers To Be Adjusted	Each	12		12
Diamond Grinding (Bridge Section)	Sq Yd	2,939		2,939
Railroad Protective Liability Insurance	L Sum	1		1

SCOPE OF WORK

- Provide Protective Shield within limits indicated on plans.
- Perform 3/4" Bridge Deck Scarification on deck. See Roadway Plans for the PCC Variable Surface Depth Removal on approach slab details, pay item and quantity.
- Perform Deck Slab Repairs, Bridge Sidewalk Repairs, and Approach Slab Repairs as required.
- Remove and reconstruct bridge deck expansion joints at the East and West abutments and install new preformed joint strip seal.
- Clean and adjust drainage scuppers.
- Apply 2 3/4" Bridge Deck Latex Concrete Overlay on bridge deck, approach slab and approach pavement.
- Reconstruct CTA and Belvedere Ramps.
- Perform 1/4" Diamond Grinding over the entire bridge and approach length and over a width that extends to within 2 ft of the curbs.
- Perform parapet repairs.
- Perform Bridge Deck Grooving (Longitudinal) over the entire bridge and approach length and over a width that extends to within 2 ft of the curbs.
- Apply bridge relief joint sealer between approach pavement and approach slab.
- Replace the existing preformed joint seal with preformed pavement seal between the approach pavement and PCC connector.
- Perform Structural Repair of Concrete to the abutments and piers as noted on the plans.
- Perform Bridge Drainage System Repairs.
- Apply Protective Coat to the top and inside faces of parapets, sidewalks, reconstructed joint area, and surface of the new overlay.

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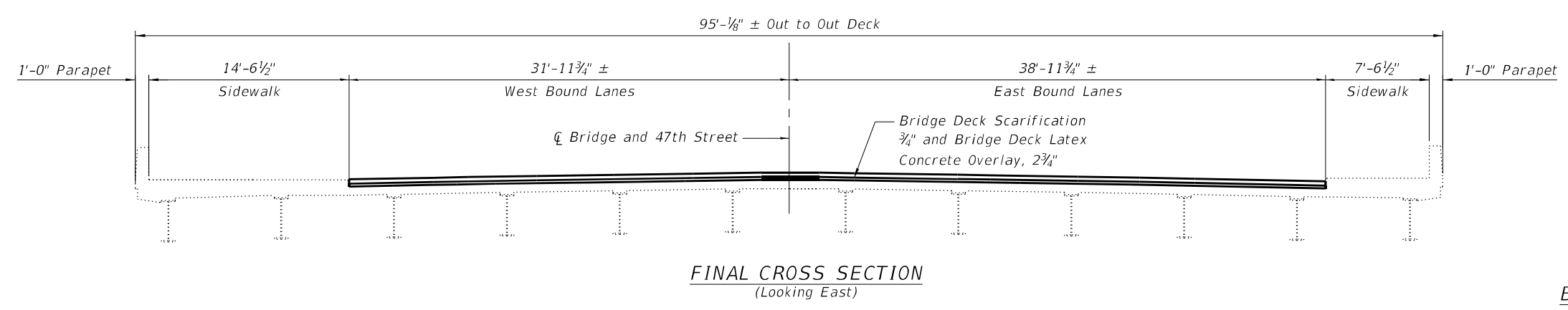
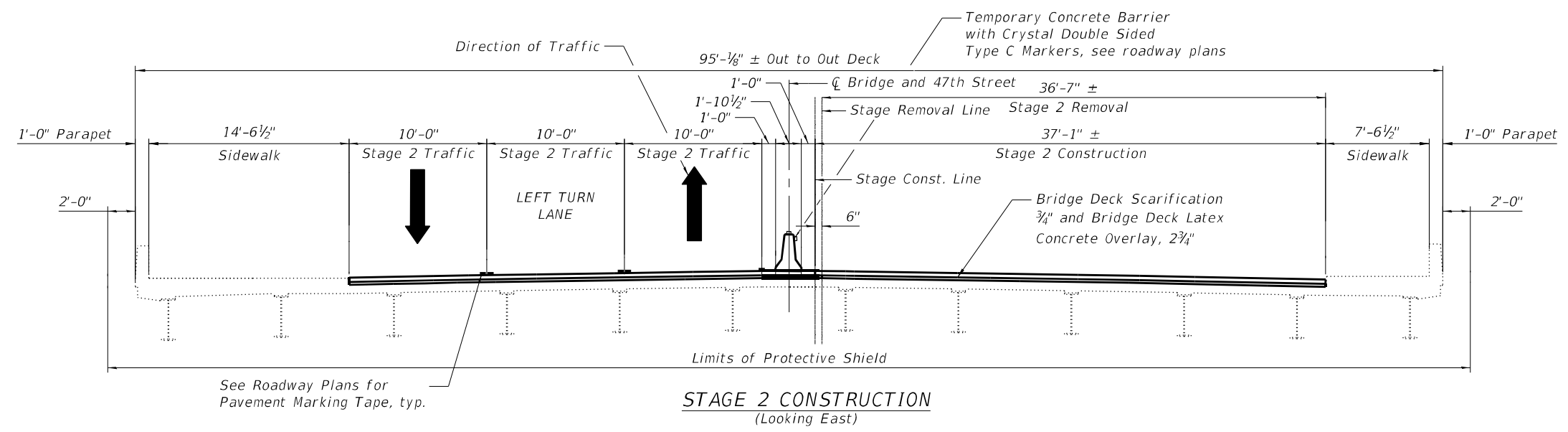
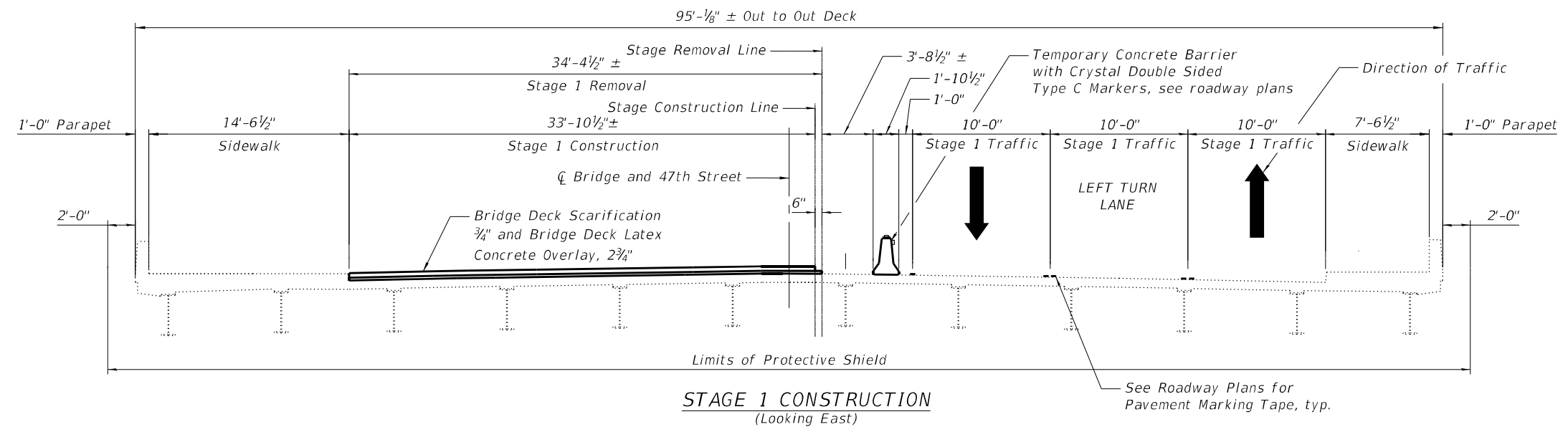
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA
STRUCTURE NO. 016-1156**

SHEET S-02 OF S-52 SHEETS

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90/94	FAI 90/94 22 BJ	COOK	83	19
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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BILL OF MATERIAL

ITEM	UNIT	TOTAL
Protective Shield	Sq. Yd.	3,337



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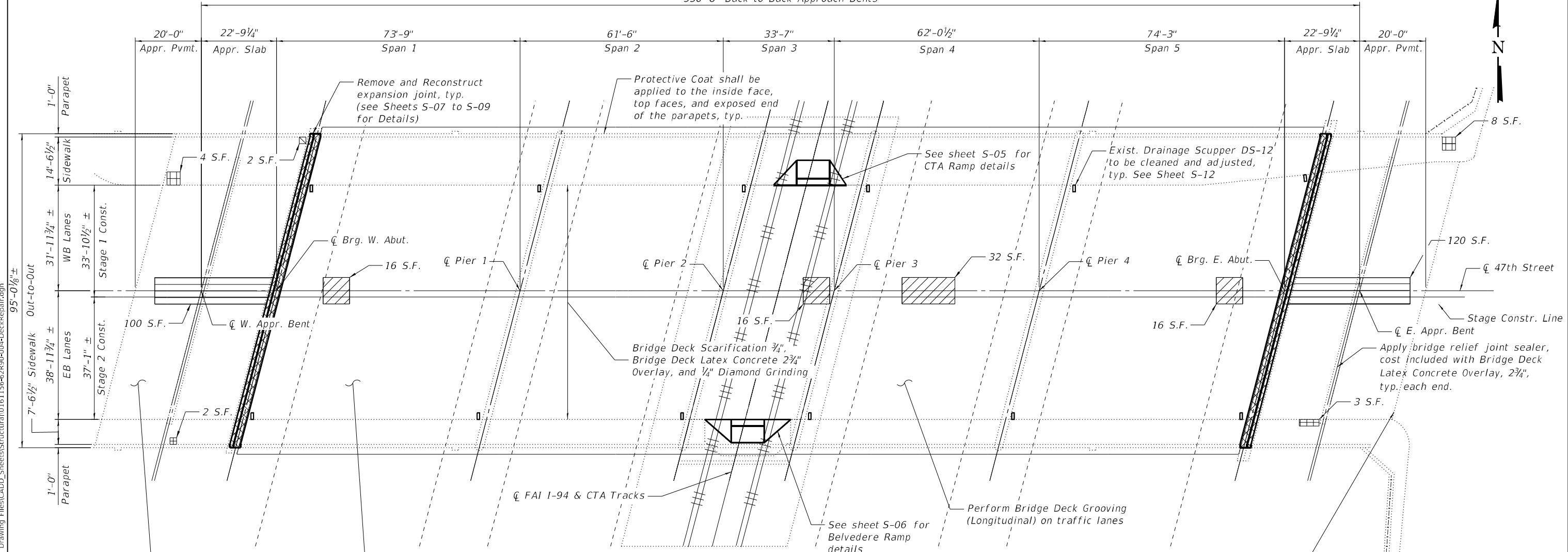
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BRIDGE TYPICAL SECTION
 STRUCTURE NO. 016-1156**

SHEET S-03 OF S-52 SHEETS

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CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

350'-8" Back to Back Approach Bents



DECK PLAN

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Deck Grooving (Longitudinal)	Sq Yd	2,939
Protective Coat	Sq Yd	4,373
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq Yd	3,113
Bridge Deck Scarification 3/4"	Sq Yd	2,425
Bridge Sidewalk Repair (Partial Depth)	Sq Ft	17
Deck Slab Repair (Full Depth, Type I)	Sq Yd	1
Deck Slab Repair (Full Depth, Type II)	Sq Yd	9
Approach Slab Repair (Full Depth)	Sq Yd	25
Diamond Grinding (Bridge Section)	Sq Yd	2,939

LEGEND

- Concrete Removal
- Bridge Sidewalk Repair (Partial Depth)
- Deck Slab Repair (Full Depth, Type I)
- Deck Slab Repair (Full Depth, Type II)
- Approach Slab Repair (Full Depth)
- S.F. Square Foot

NOTES:

- Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.
- For West and East expansion joint removal and reconstruction, see Sheets S-07 thru S-09.
- For Parapet Repairs, see Sheet S-19.
- Perform 1/4" Diamond Grinding and Bridge Deck Grooving (Longitudinal) over the entire bridge and approach length and over a width that extends to within 2 ft of the curbs.
- Protective Coat shall be applied to the top of reconstructed transverse joints, top and inside face of parapets and sidewalks and top of latex concrete overlay.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. The cost of repair or replacement shall be included in the cost of Concrete Removal.
- The Contractor shall exercise extreme caution during concrete removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer at no cost to the Department.

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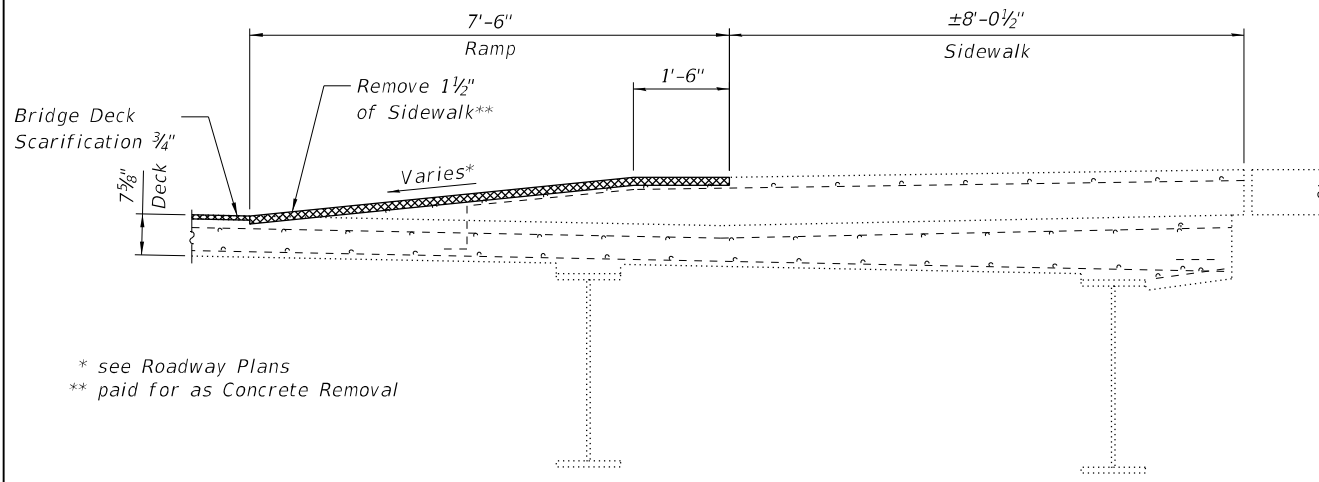
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK OVERLAY AND SLAB REPAIR PLAN AND DETAILS
STRUCTURE NO. 016-1156

SHEET S-04 OF S-52 SHEETS

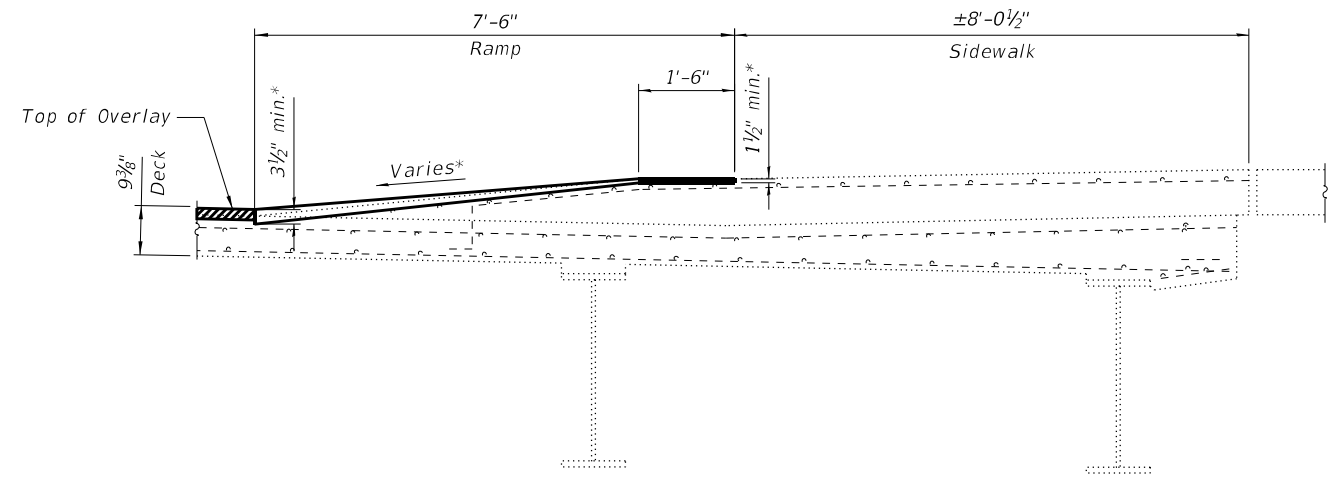
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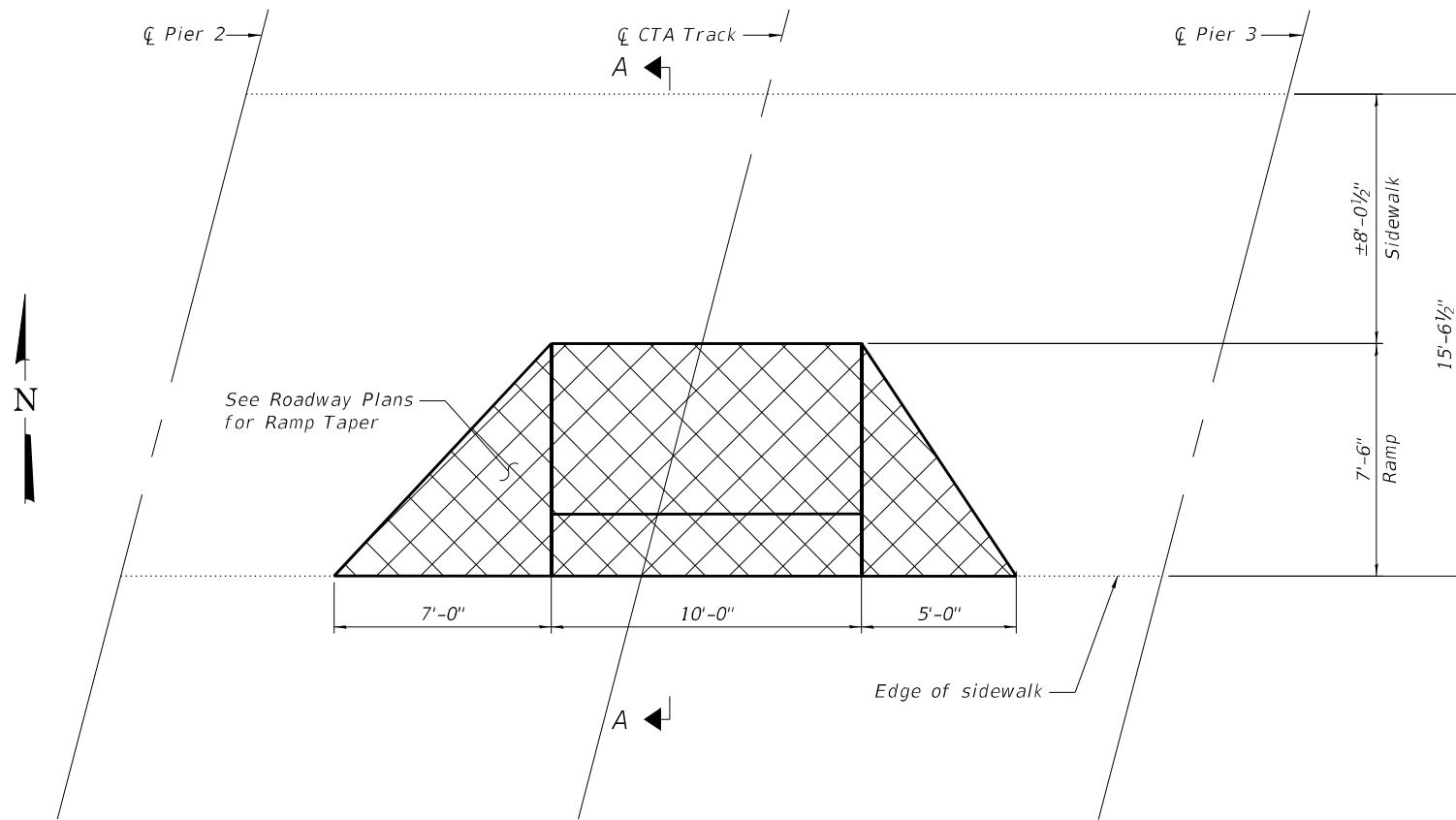


* see Roadway Plans
 ** paid for as Concrete Removal

SECTION A-A
 (Showing Removal)



SECTION A-A
 (Showing Reconstruction)



REMOVAL AND RECONSTRUCTION PLAN

LEGEND

- Concrete Removal
- Bridge Deck Latex Concrete Overlay, 2 3/4"

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	0.6
Concrete Superstructure	Cu Yd	1.3



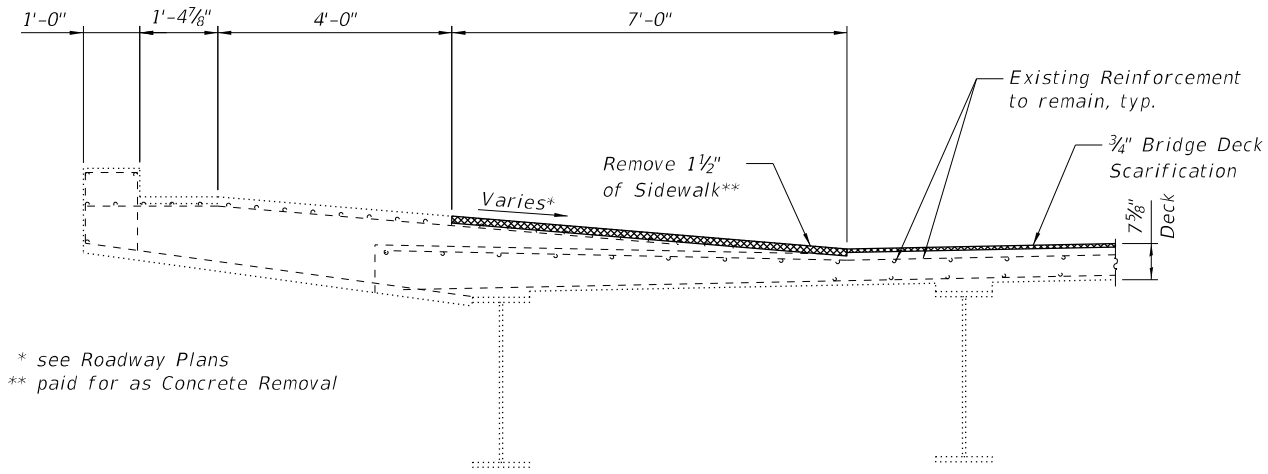
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

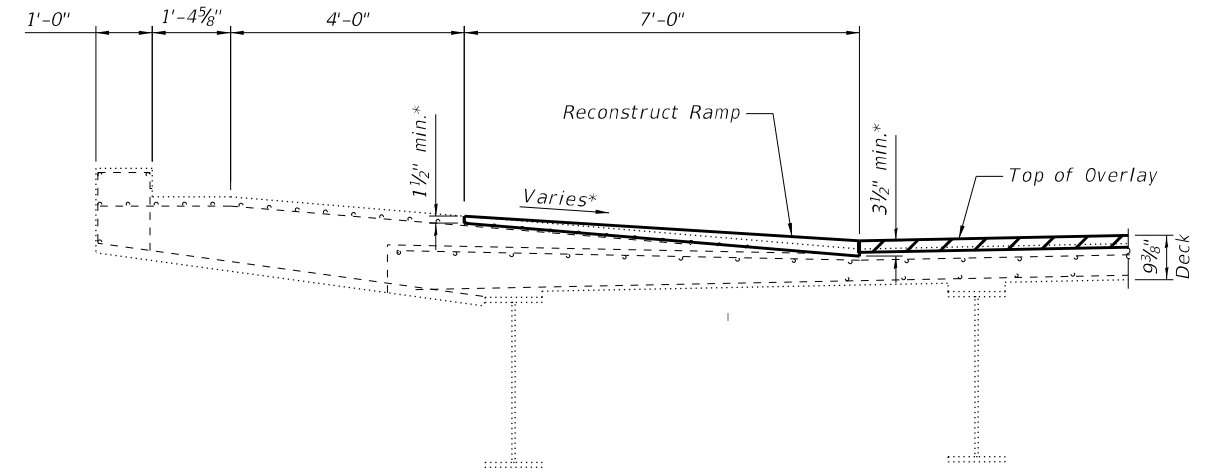
CTA RAMP DETAILS
 STRUCTURE NO. 016-1156

SHEET S-05 OF S-52 SHEETS

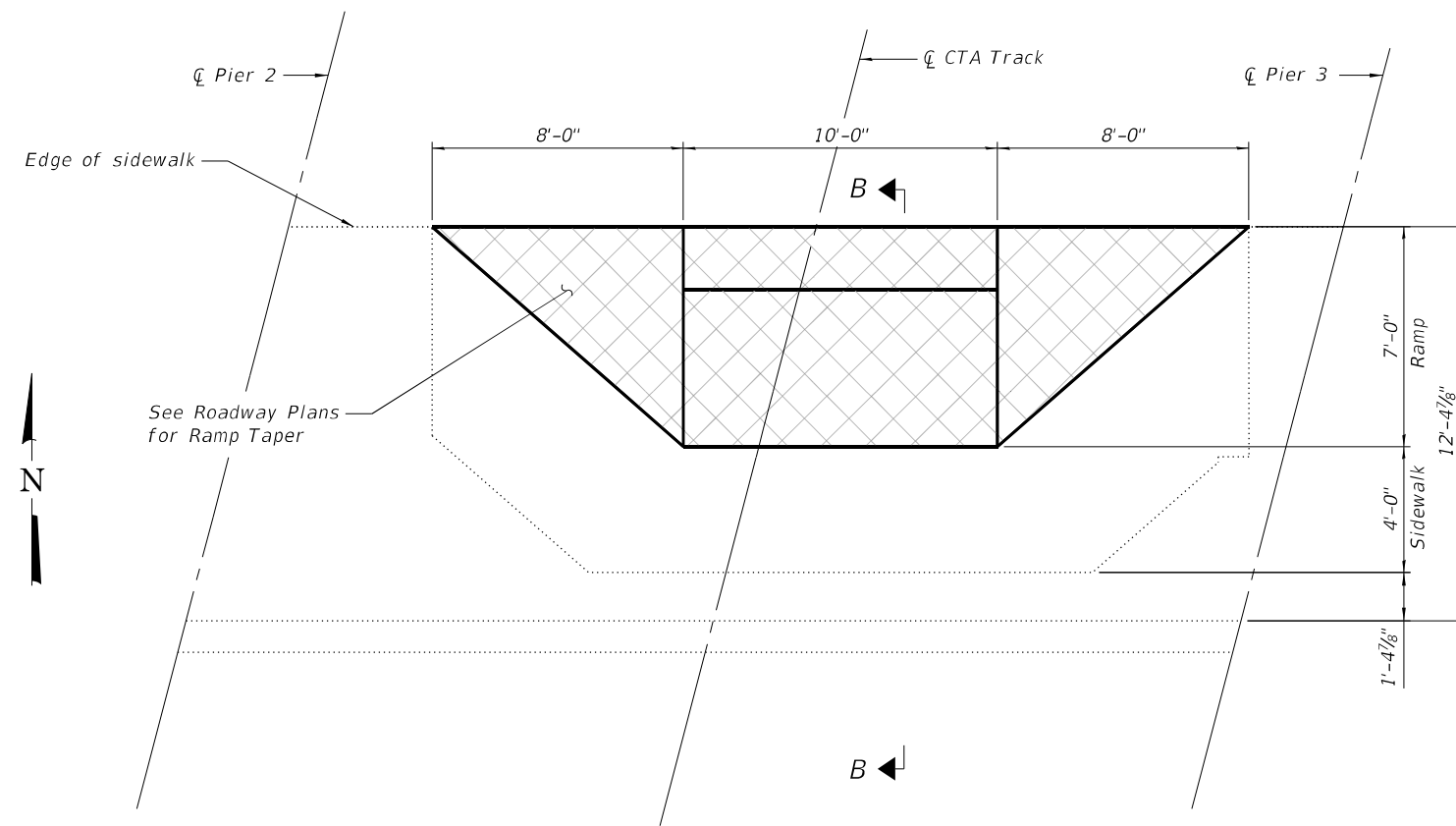
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CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



SECTION B-B
(Showing Removal)



SECTION B-B
(Showing Reconstruction)



REMOVAL AND RECONSTRUCTION PLAN

LEGEND

- Concrete Removal
- Bridge Deck Latex Concrete Overlay, 2 3/4"

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	0.6
Concrete Superstructure	Cu Yd	1.4

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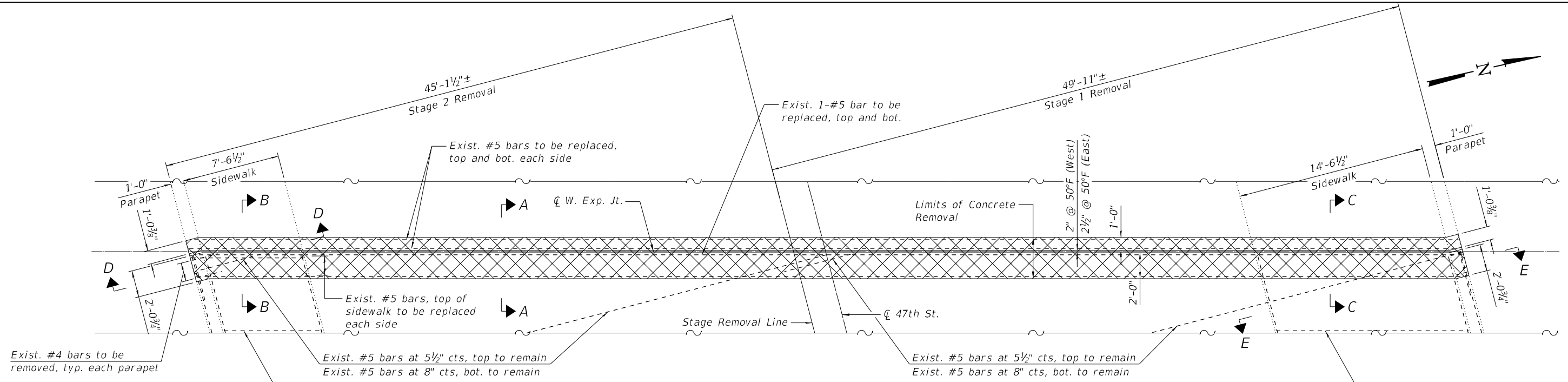
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DEPARTMENT OF TRANSPORTATION

BELVEDERE RAMP
STRUCTURE NO. 016-1156

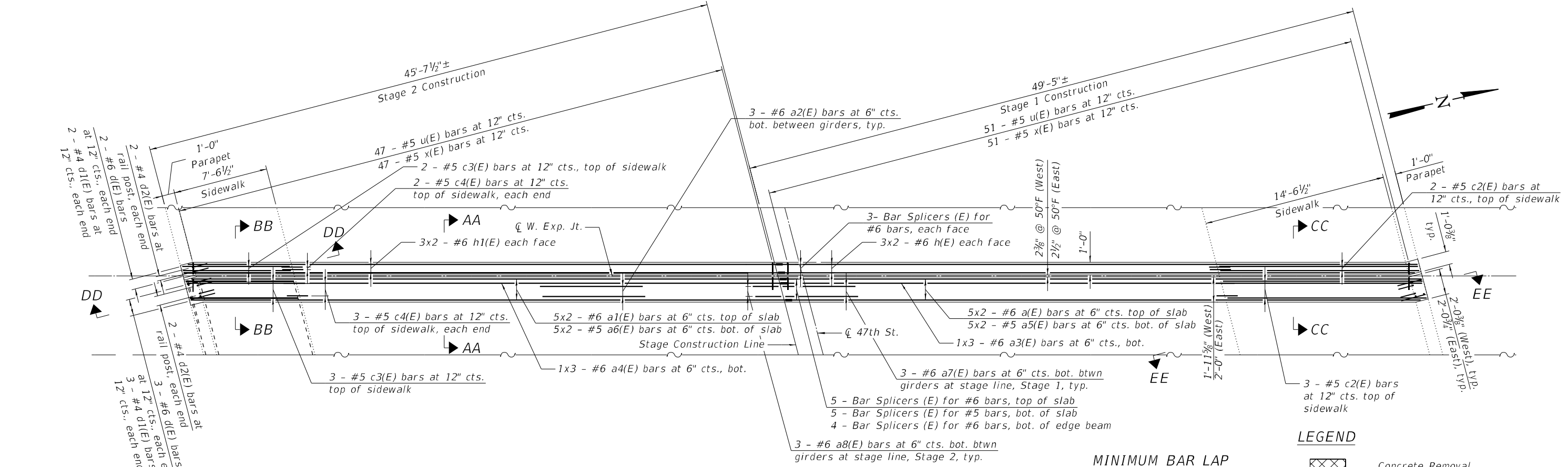
SHEET S-06 OF S-52 SHEETS

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CONTRACT NO. 62R90				
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ABUTMENT JOINT REMOVAL PLAN
 (West Abutment Joint shown, East similar)



ABUTMENT JOINT RECONSTRUCTION PLAN
 (West Abutment Joint shown, East similar)

MINIMUM BAR LAP
 #5 bar = 3'-6"
 #6 bar = 5'-6"

LEGEND
 Concrete Removal



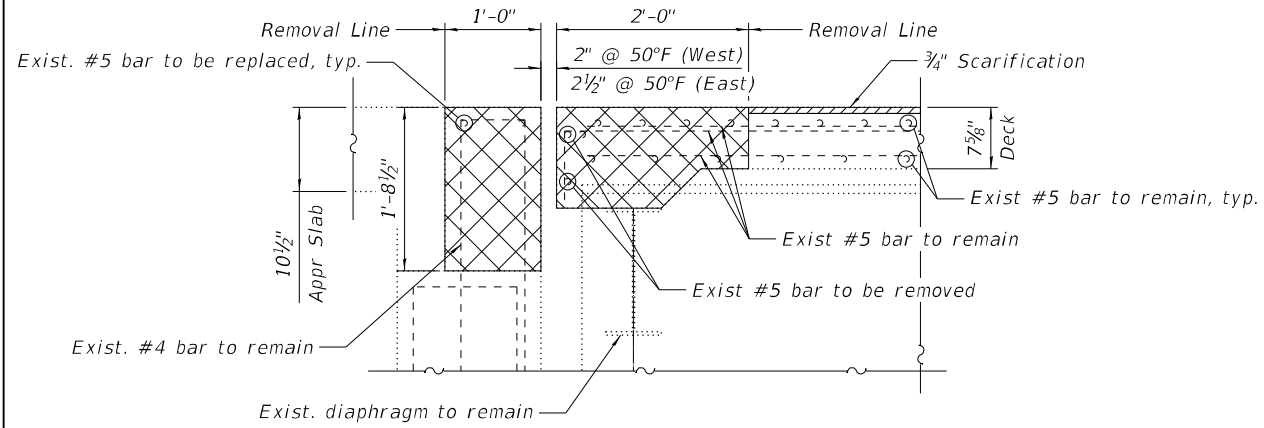
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

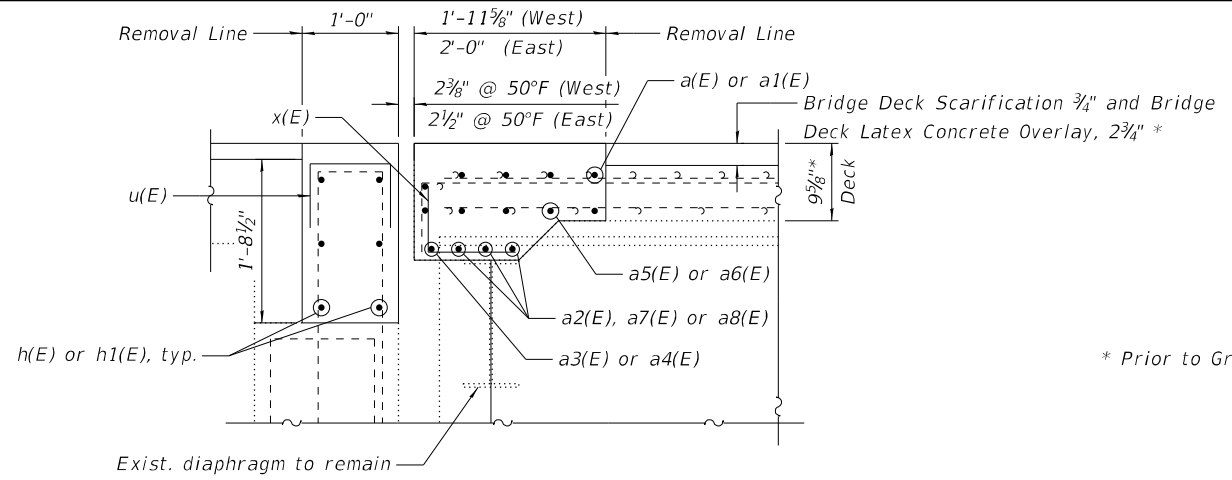
EXPANSION JOINT REMOVAL PLAN AND RECONSTRUCTION PLAN
STRUCTURE NO. 016-1156
 SHEET S-07 OF S-52 SHEETS

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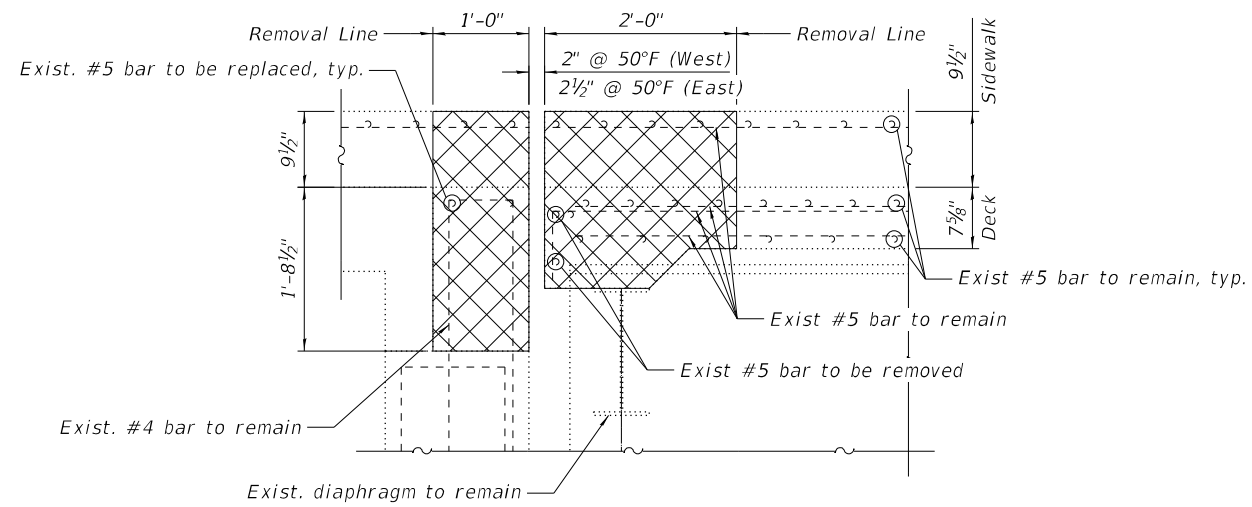


SECTION A-A

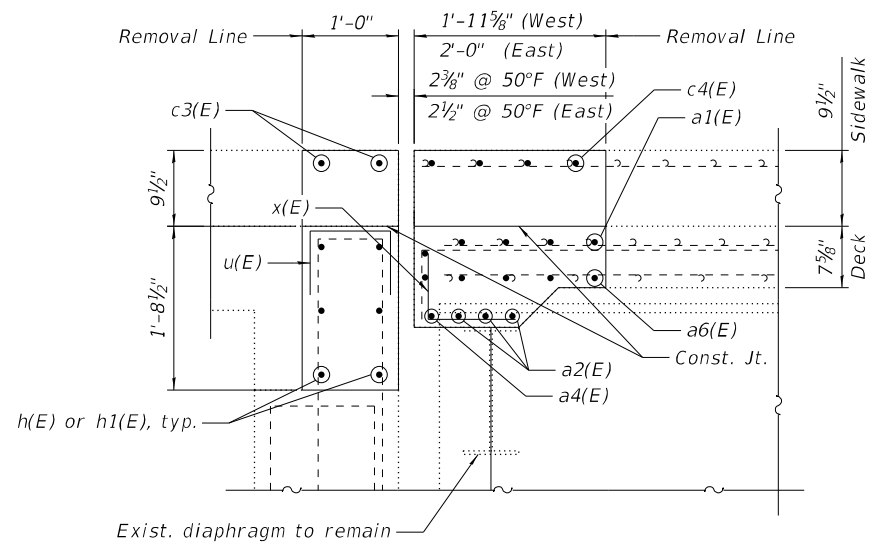


SECTION AA-AA

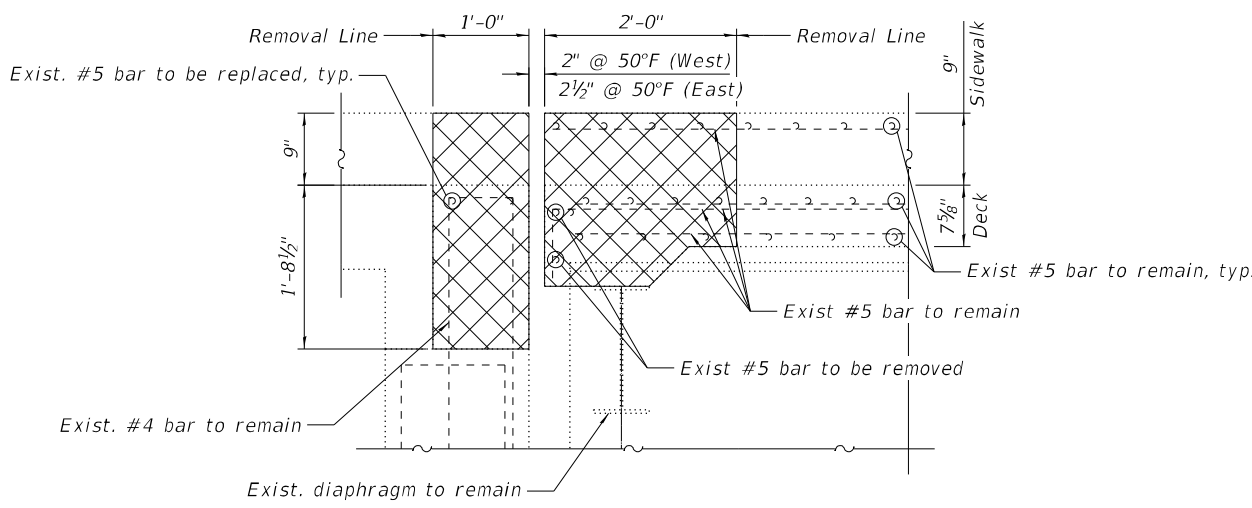
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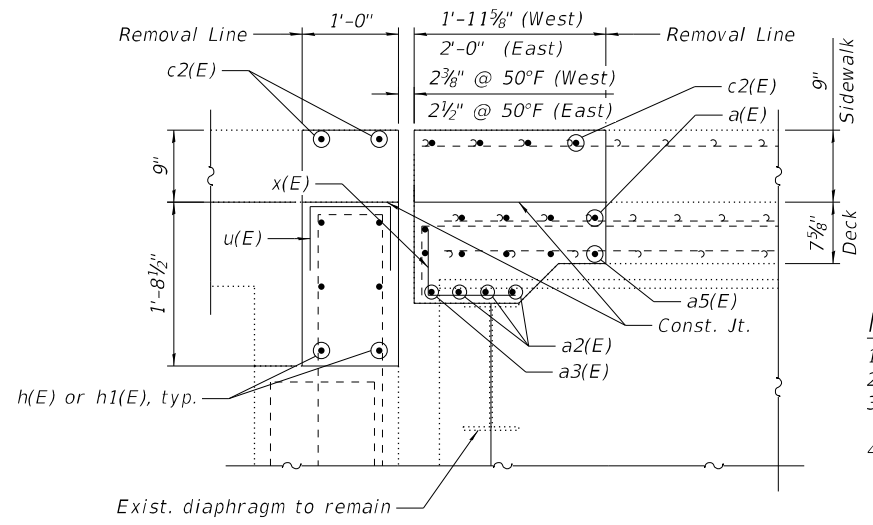
SECTION B-B



SECTION BB-BB





SECTION C-C



SECTION CC-CC

LEGEND

	Concrete Removal
	Bridge Deck Scarification 3/4"

- NOTES:**
1. For preformed joint strip seal details, see Sheets S-10 and S-11.
 2. For bar splicer assembly details, see Sheet S-21.
 3. Removal and disposal of existing expansion joint shall be included with the cost of Concrete Removal.
 4. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
 5. The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the existing steel beams and diaphragms to remain. Any damage to the existing beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.



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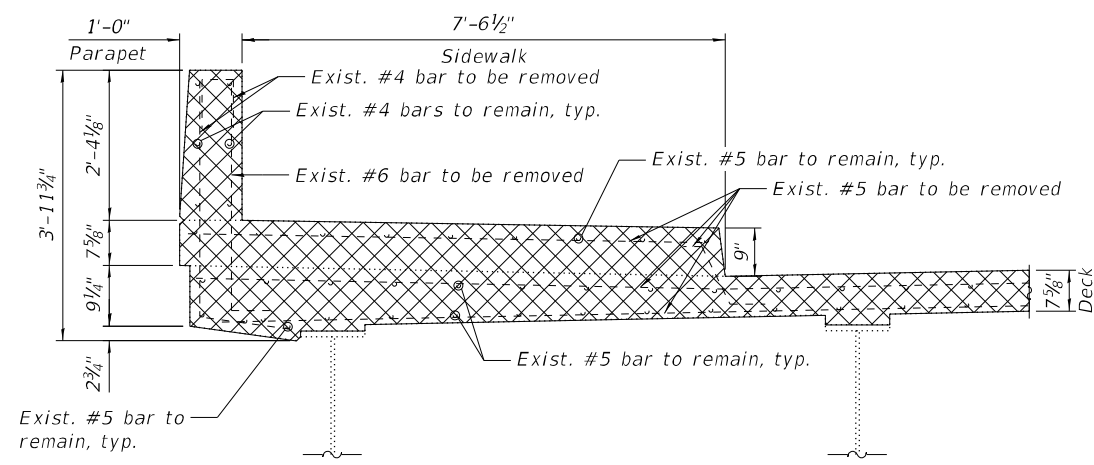
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT DETAILS I
 STRUCTURE NO. 016-1156**

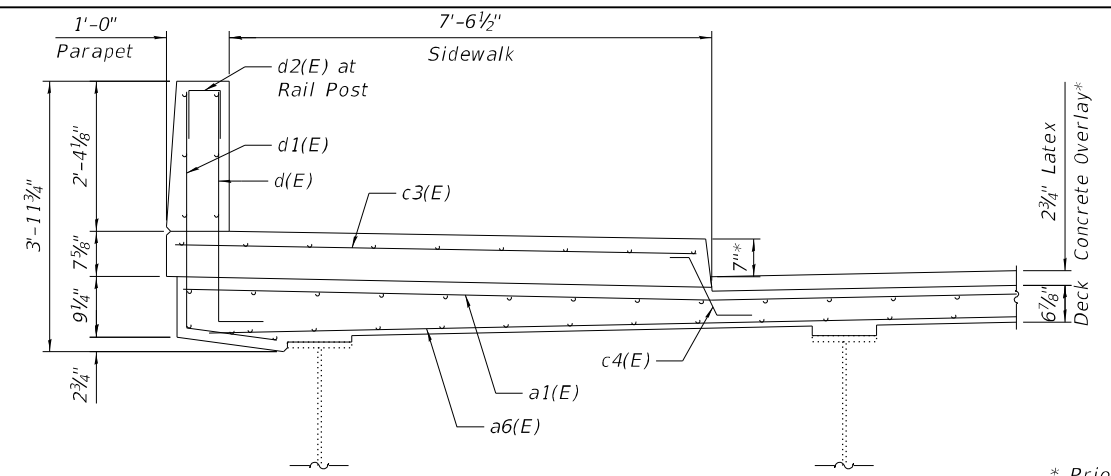
SHEET S-08 OF S-52 SHEETS

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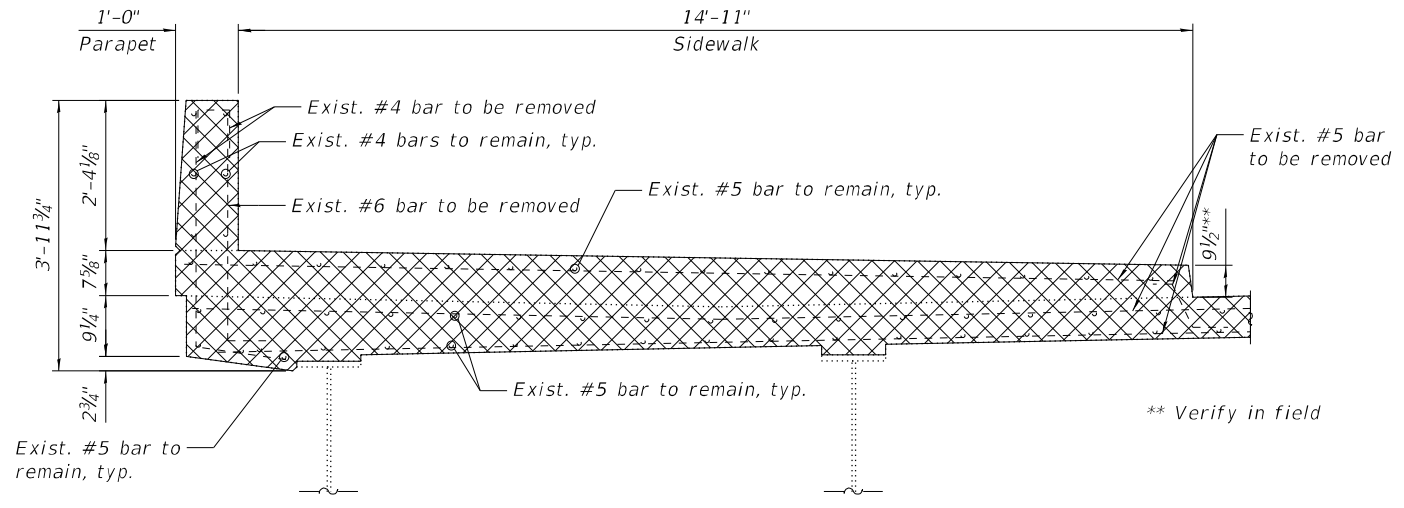


SECTION D-D



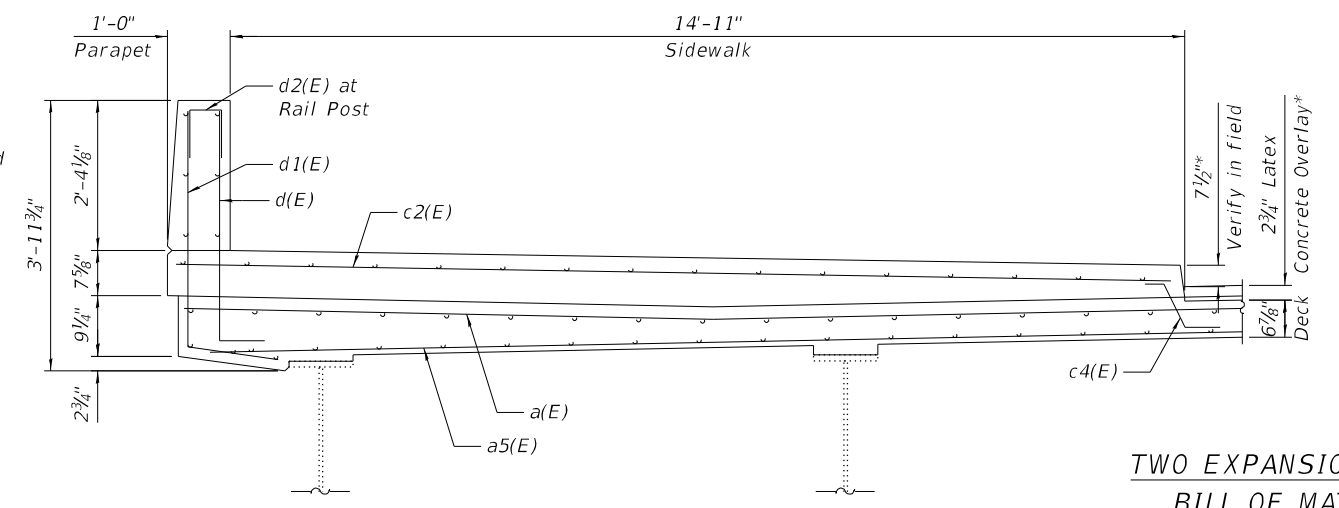
SECTION DD-DD

* Prior to grinding



SECTION E-E

** Verify in field



SECTION EE-EE

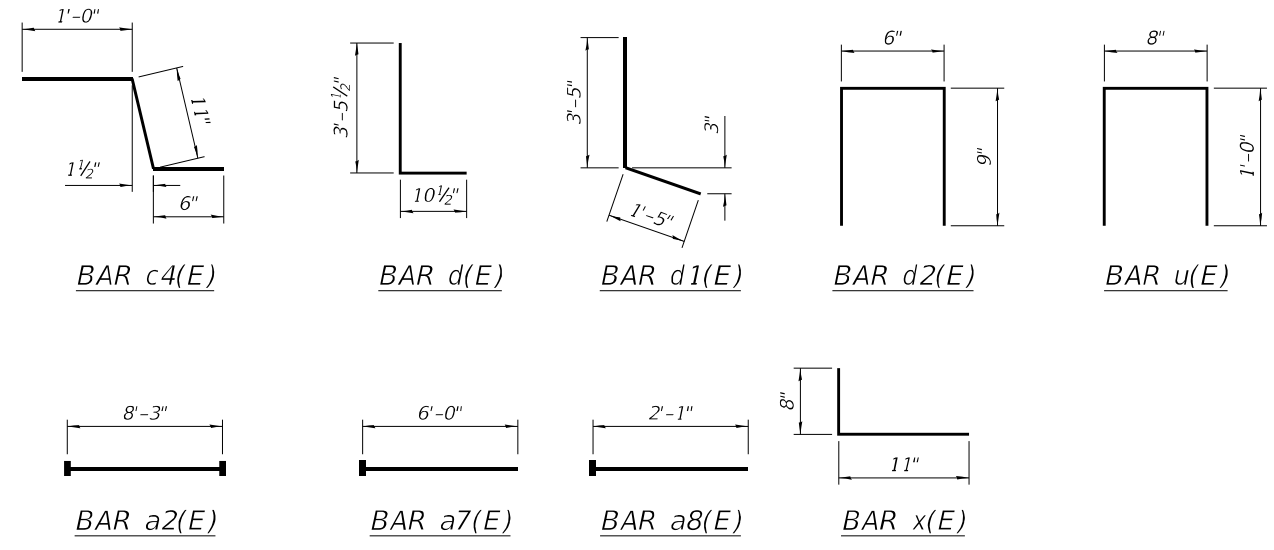
TWO EXPANSION JOINTS
 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	20	#6	28'-2"	—
a1(E)	20	#6	26'-2"	—
a2(E)	60	#6	8'-3"	—
a3(E)	6	#6	20'-7"	—
a4(E)	6	#6	19'-4"	—
a5(E)	20	#5	27'-2"	—
a6(E)	20	#5	25'-2"	—
a7(E)	6	#6	6'-0"	—
a8(E)	6	#6	2'-1"	—
c2(E)	10	#5	15'-7"	—
c3(E)	10	#5	8'-5"	—
c4(E)	20	#5	2'-5"	—
d(E)	20	#6	4'-4"	L
d1(E)	20	#6	4'-10"	L
d2(E)	16	#4	2'-0"	□
h(E)	24	#6	28'-2"	—
h1(E)	24	#6	26'-2"	—
u(E)	196	#5	2'-8"	□
x(E)	196	#5	1'-7"	L
Reinforcement Bars, Epoxy Coated			Lbs	7,330
Concrete Superstructure			Cu Yds	31.3
Concrete Removal			Cu Yds	31.3

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.

NOTES:

- For preformed joint strip seal details, see Sheets S-10 and S-11.
- For bar splicer assembly details, see Sheet S-21.
- Existing reinforcement bars extending into the removal areas shall be cleaned, straightened and incorporated into the new construction.
- The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the existing steel beams and diaphragms to remain. Any damage to the existing beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.



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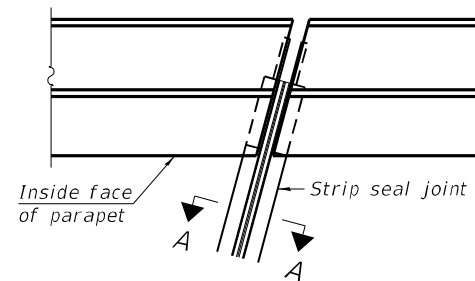
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 DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT DETAILS II
 STRUCTURE NO. 016-1156

SHEET S-09 OF S-52 SHEETS

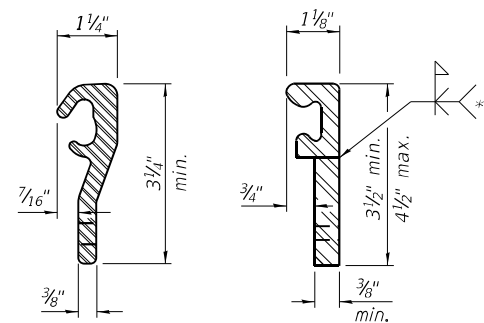
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FOR SKEWS $\leq 30^\circ$

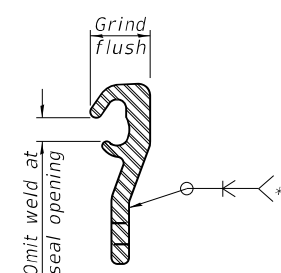
PLAN AT PARAPET



ROLLED (EXTRUDED) RAIL
 WELDED RAIL

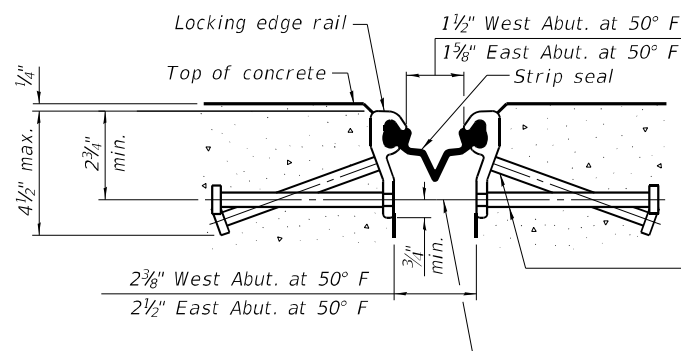
LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

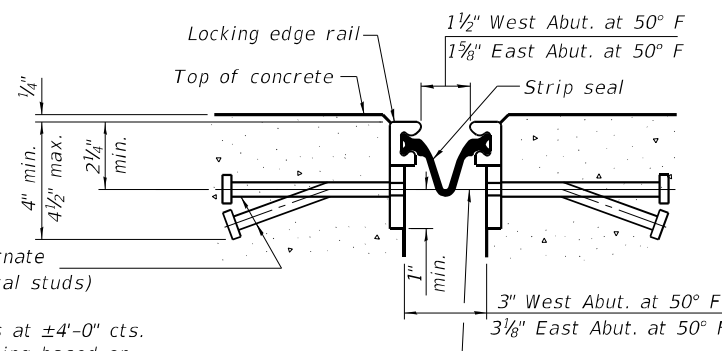


LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
 Rolled rail shown, welded rail similar.



SHOWING ROLLED RAIL JOINT



SHOWING WELDED RAIL JOINT

* 3/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" ϕ threaded rods in 7/16" ϕ holes at $\pm 4'-0"$ cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	198

(Sheet 1 of 2)



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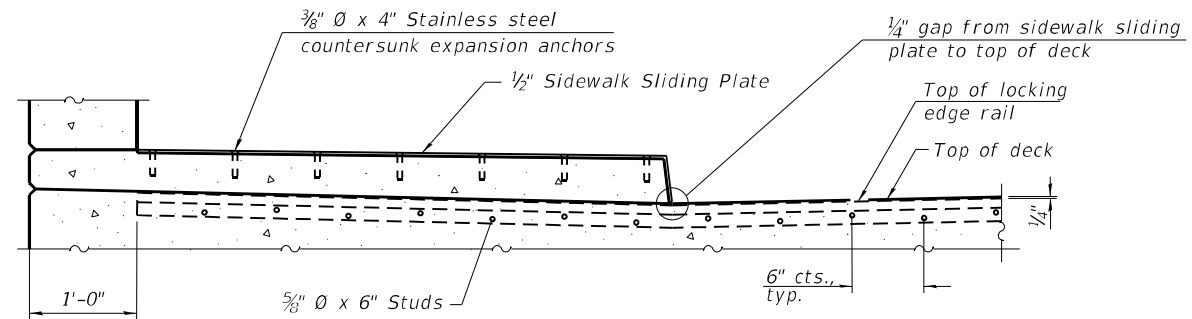
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL I
 STRUCTURE NO. 016-1156

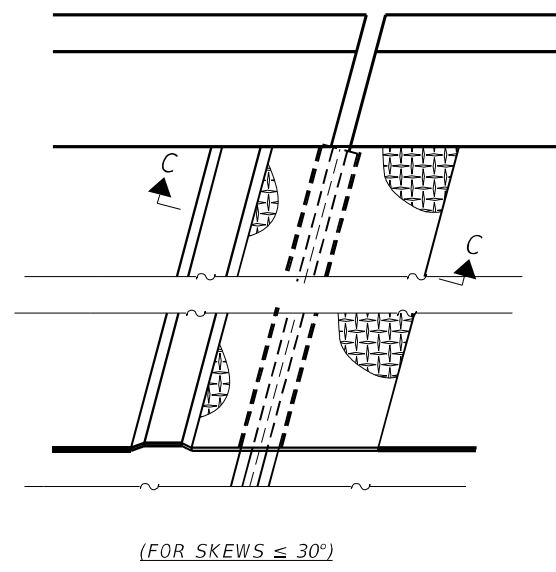
SHEET S-10 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

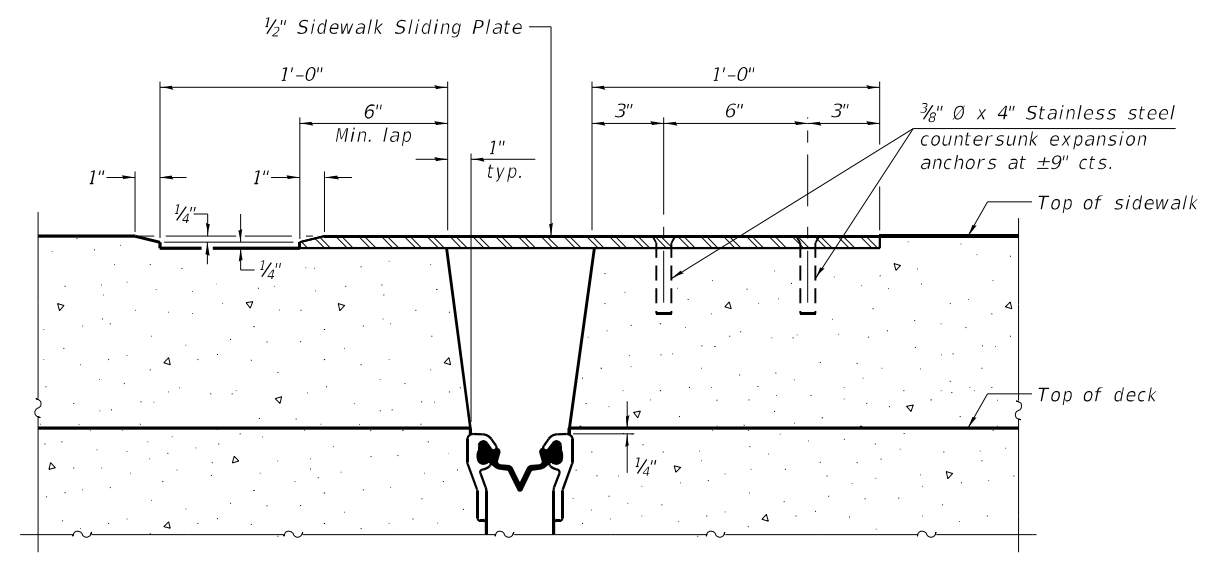
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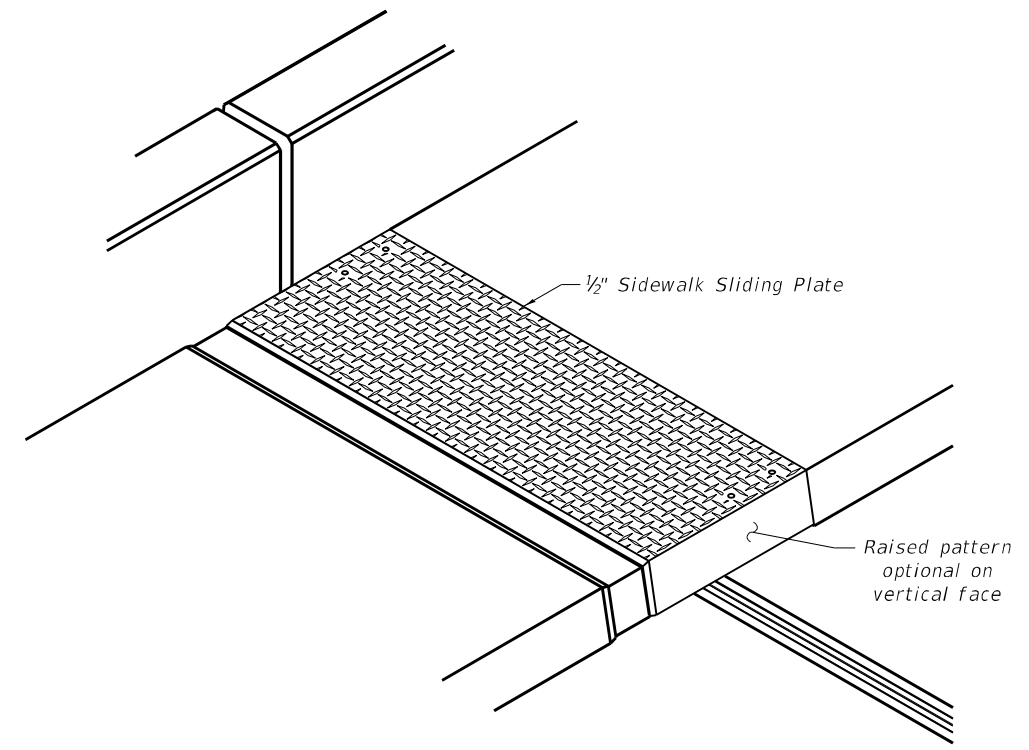
SECTION AT RAISED SIDEWALK



PLAN AT RAISED SIDEWALK
 (FOR SKEWS ≤ 30°)



SECTION C-C



TRIMETRIC VIEW

(Sheet 2 of 2)



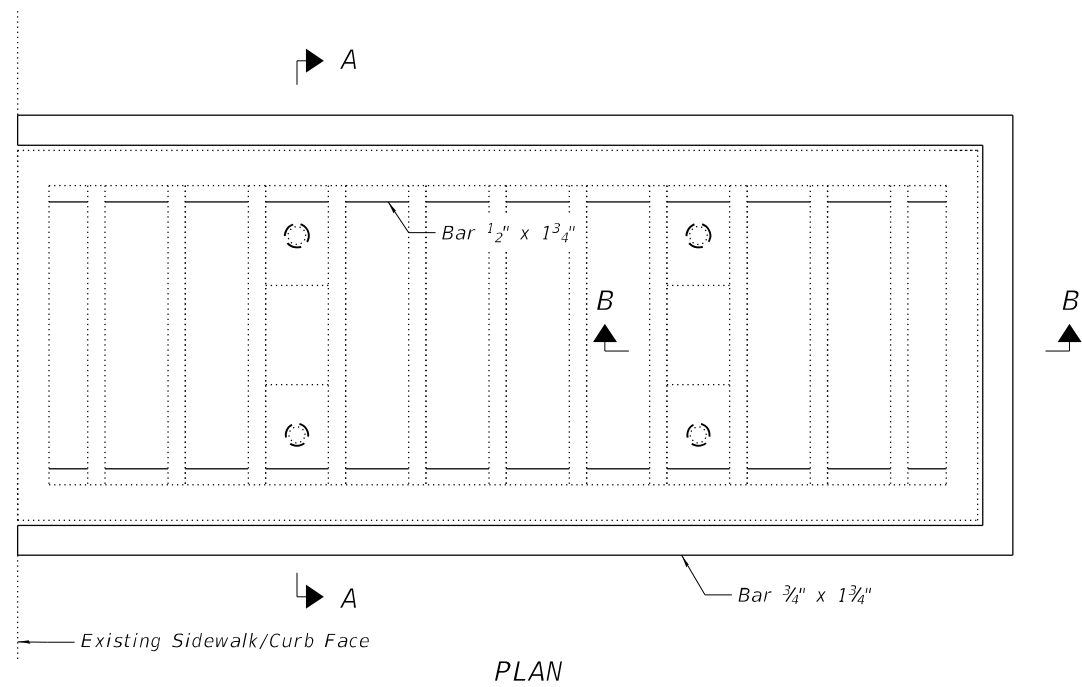
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL II
 STRUCTURE NO. 016-1156

SHEET S-11 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



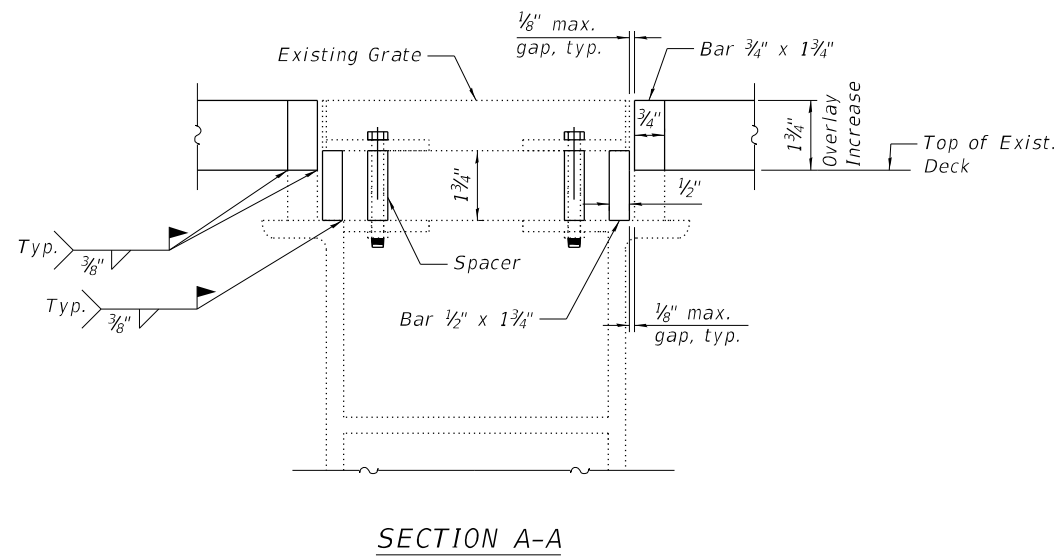
PLAN

NOTES:

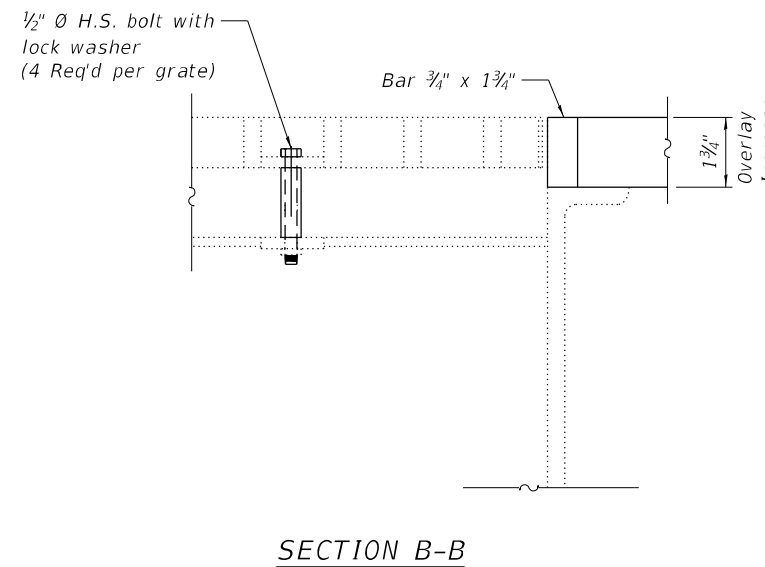
- Existing drainage scupper gates shall be adjusted so that the top of the existing grate is flush with the top of the bridge deck overlay after diamond surface grinding.
- Proposed structural steel for adjusting the scupper gates shall conform to AASHTO Classification M-270 Gr. 36. All proposed components adjusting the scupper gates shall be hot dipped galvanized.
- Bolts shall be 1/2" \varnothing ASTM F3125 Grade a325 Type 1, mechanically galvanized. Spacer may be fabricated from round steel pipe.
- The Contractor shall ensure that no damage is done to the existing gates to be reused.
- Shop plans for the proposed scupper adjustment ring shall be submitted to the Engineer for approval prior to fabrication.
- See existing scupper plans for details and dimensions not shown. The Contractor shall field verify the type of scupper present and the scupper dimensions.
- Galvanizing for field welded areas shall be repaired per ASTM A 780.
- Cost of all labor and materials necessary to remove existing gates, clean existing scuppers and downspouts, furnish and install scupper adjustment ring, and reinstalling the gates is included in the cost per unit each for Drainage Scupper to be Adjusted.
- Clean existing scuppers and downspouts.

BILL OF MATERIAL

Item	Unit	Quantity
Drainage Scupper to be Adjusted	Each	12
Cleaning Drainage System	L Sum	1



SECTION A-A



SECTION B-B

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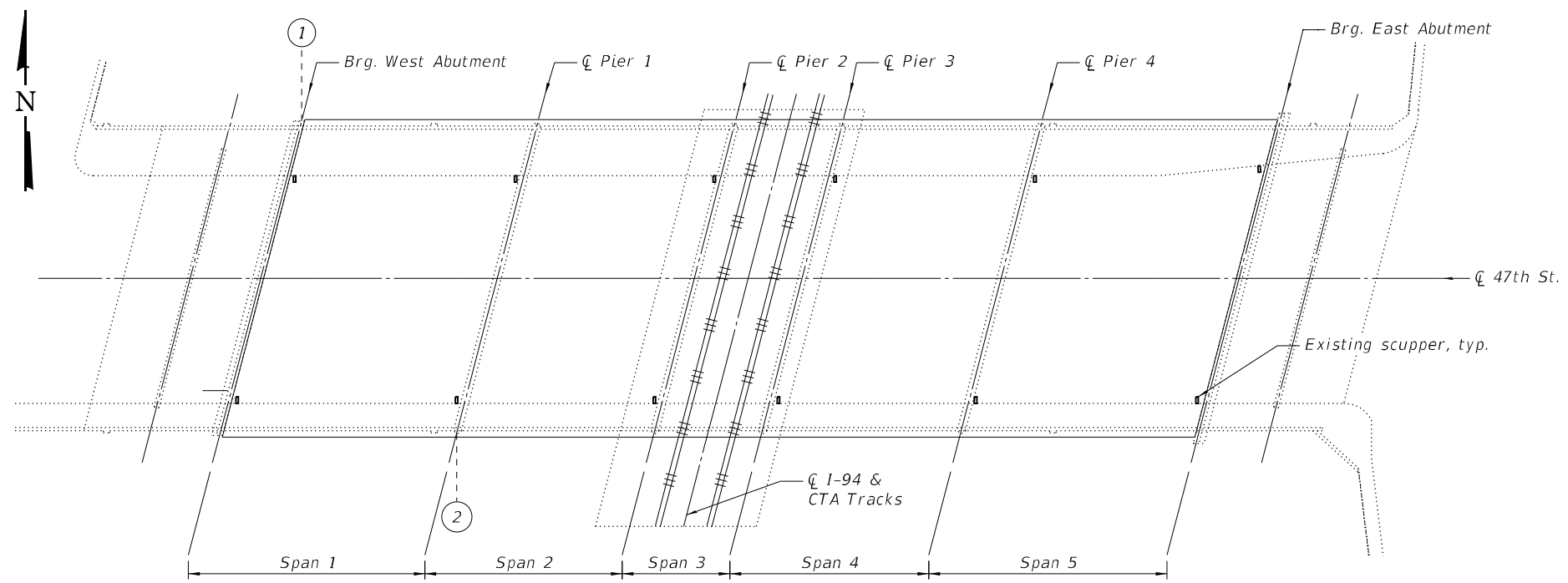
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SCUPPER DETAILS
 STRUCTURE NO. 016-1156

SHEET S-12 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	29
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



DRAINAGE REPAIR PLAN

DRAINAGE REPAIR NOTES

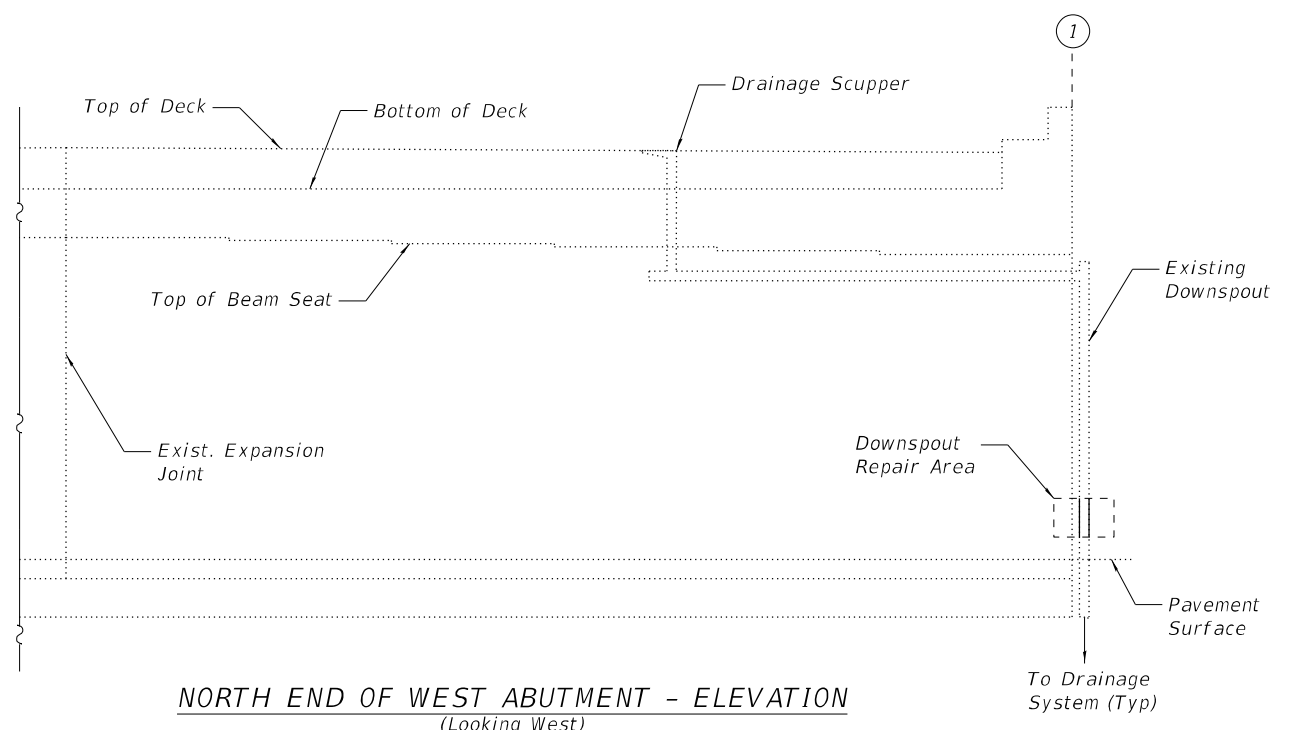
- The locations shown identify the general areas of drainage deficiencies that require repairs. The drainage system is shown schematically. It shall be the Contractor's responsibility to verify the repair locations and details prior to ordering the material. Such variations shall not be cause for additional compensation for a change in the scope of work, however the Contractor be paid for the quantity actually furnished at the unit price bid for the work.
- The Contractor shall provide all piping, brackets, hangers, fittings, hardware, and other accessories required to return the existing drainage system to a fully functional leak free system. See Special Provisions. Pipe color shall match existing.
- When existing drain pipes are wrapped with tape, the tape shall be removed, if the pipe is broken or missing a connection, the pipe shall be replaced or otherwise new tape shall be provided. The type of tape shall be approved by the Engineer. The cost shall be included in Bridge Drainage System Repairs and the length measured shall be the length of the pipe replaced or length of pipe re-taped.
- The Contractor shall wrap any joints that are leaking as directed by the Engineer. The cost shall be included in Bridge Drainage System Repairs.

REPAIR SUMMARY TABLE

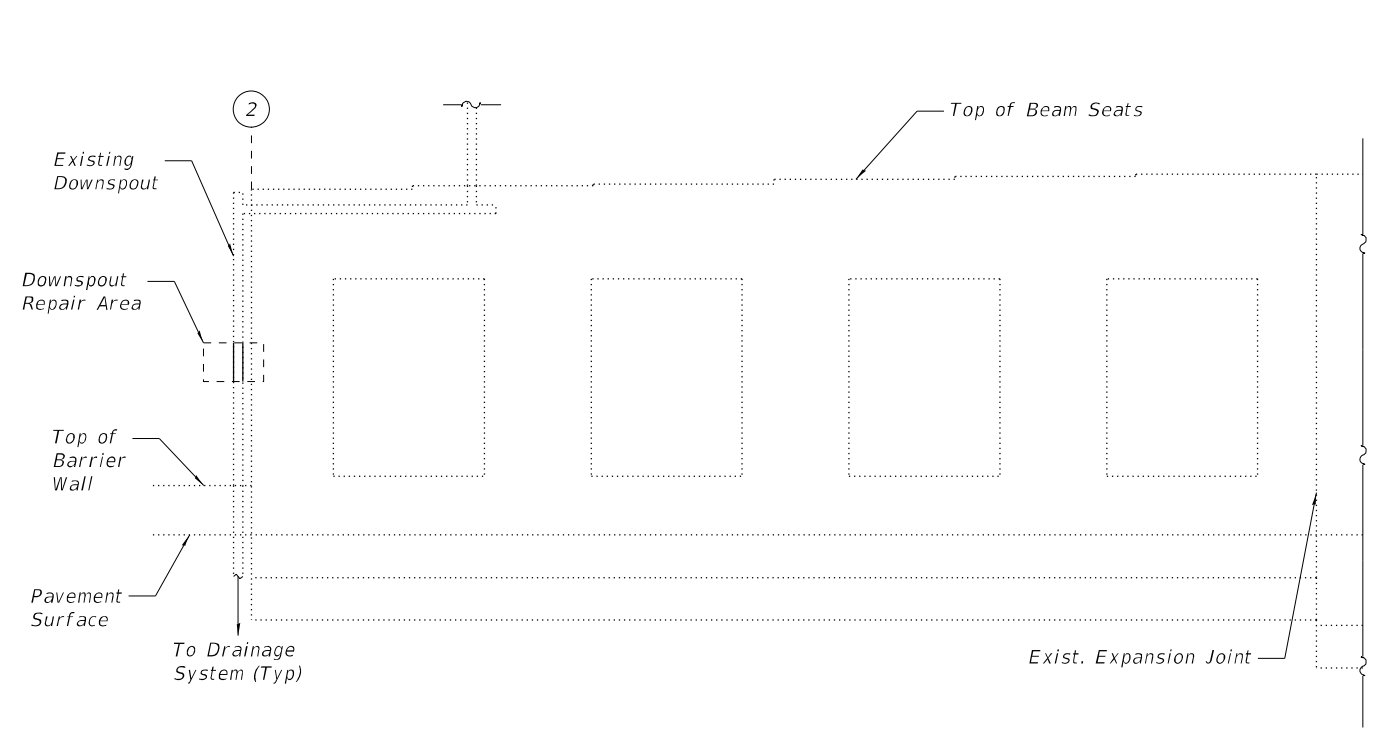
Repair Number	Location	Repair	Unit	Quantity
1	North Face of West Abutment	Downspout repairs	Foot	3
2	South Face of Pier 1	Downspout repairs	Foot	3

BILL OF MATERIAL

Item	Unit	Quantity
Bridge Drainage System Repairs	Foot	6



NORTH END OF WEST ABUTMENT - ELEVATION
(Looking West)



SOUTH END OF PIER 1 - ELEVATION
(Looking West)

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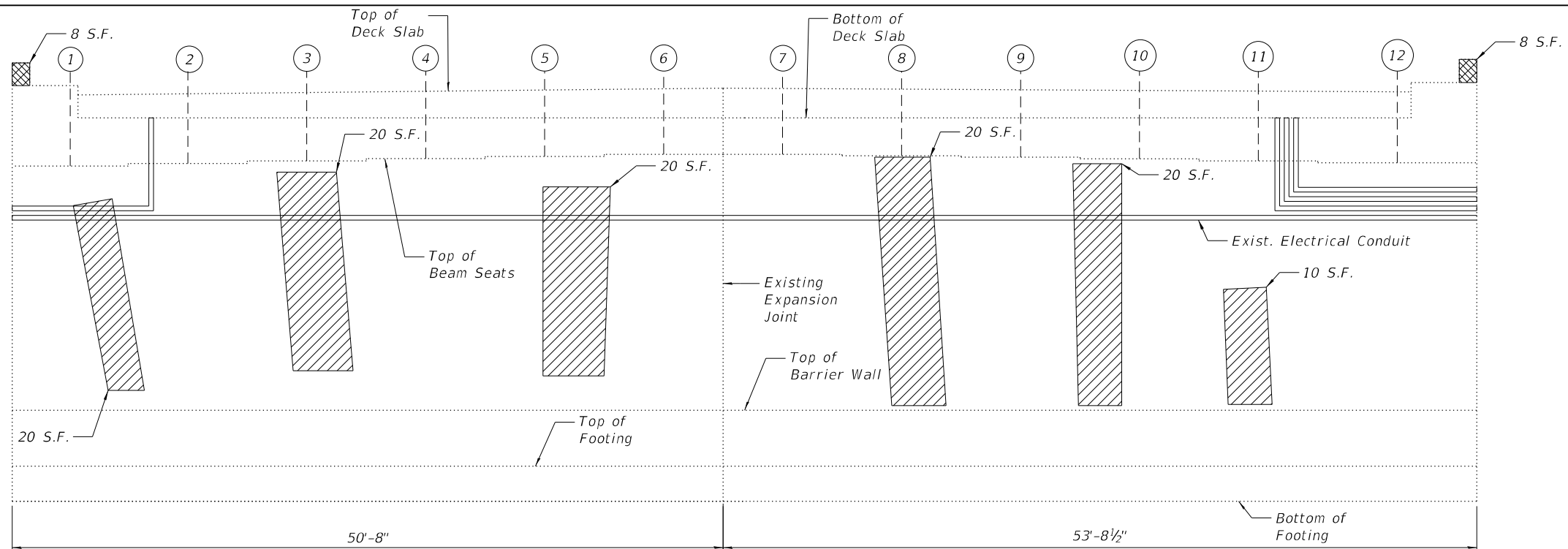
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE SYSTEM REPAIR DETAILS
STRUCTURE NO. 016-1156**

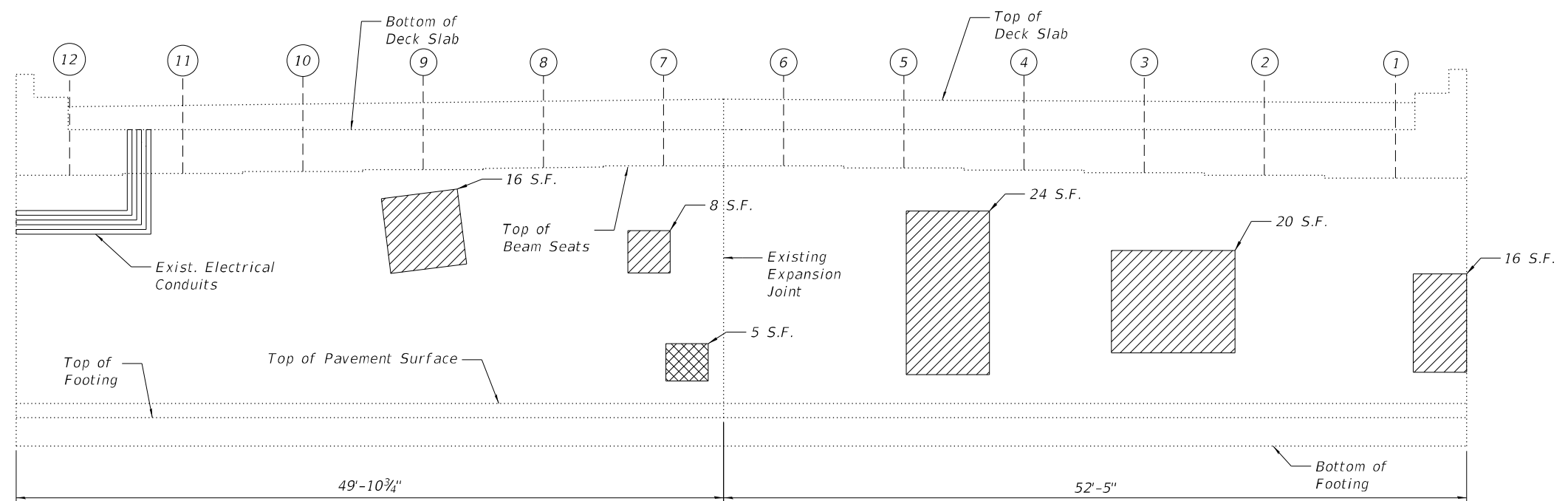
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ELEVATION - EAST ABUTMENT
 (Looking East)



ELEVATION - WEST ABUTMENT
 (Looking West)

NOTE:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

LEGEND

- Structural Repair of Concrete (Depth equal to or less than 5 Inches)
- Structural Repair of Concrete (Depth greater than 5 Inches)
- S.F. Square Foot

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth equal to or less than 5 Inches)	Sq Ft	194
Structural Repair of Concrete (Depth greater than 5 Inches)	Sq Ft	21



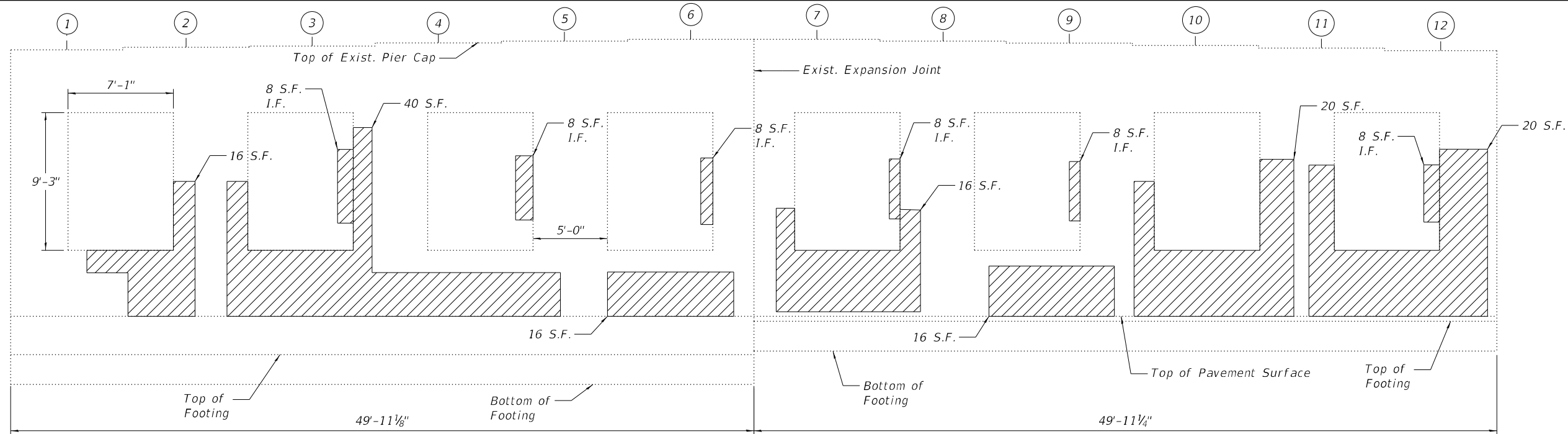
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

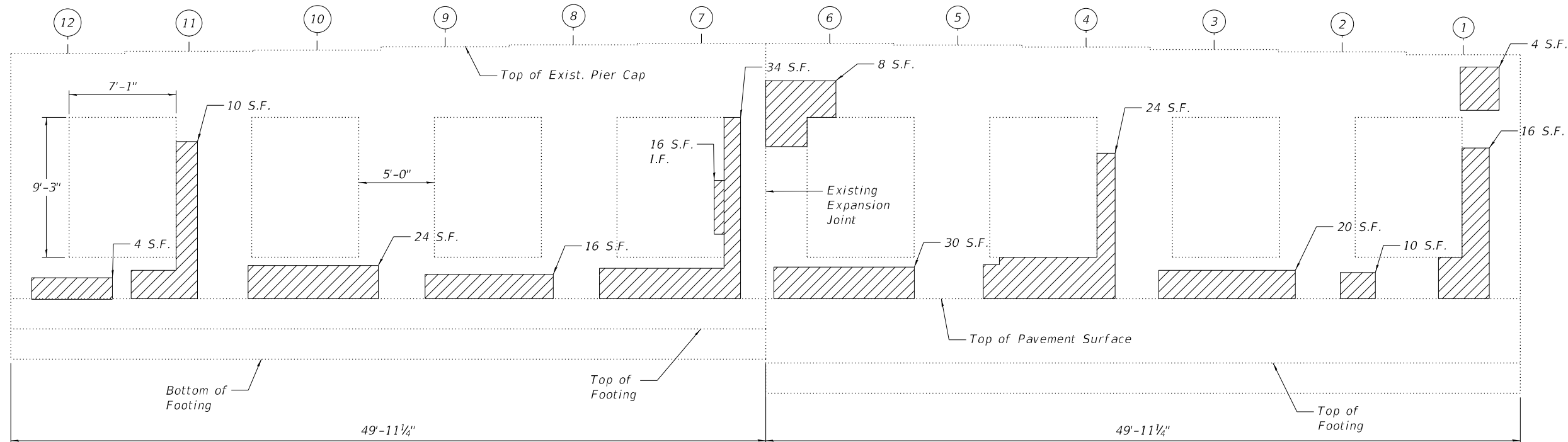
**ABUTMENT REPAIR DETAILS
 STRUCTURE NO. 016-1156**

SHEET S-14 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	31
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



ELEVATION - PIER 1
(Looking East)



ELEVATION - PIER 1
(Looking West)

NOTE:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

LEGEND

- Structural Repair of Concrete (Depth equal to or less than 5 inches)
- S.F. Square Foot
- I.F. Inside Face

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth equal to or less than 5 Inches)	Sq Ft	408

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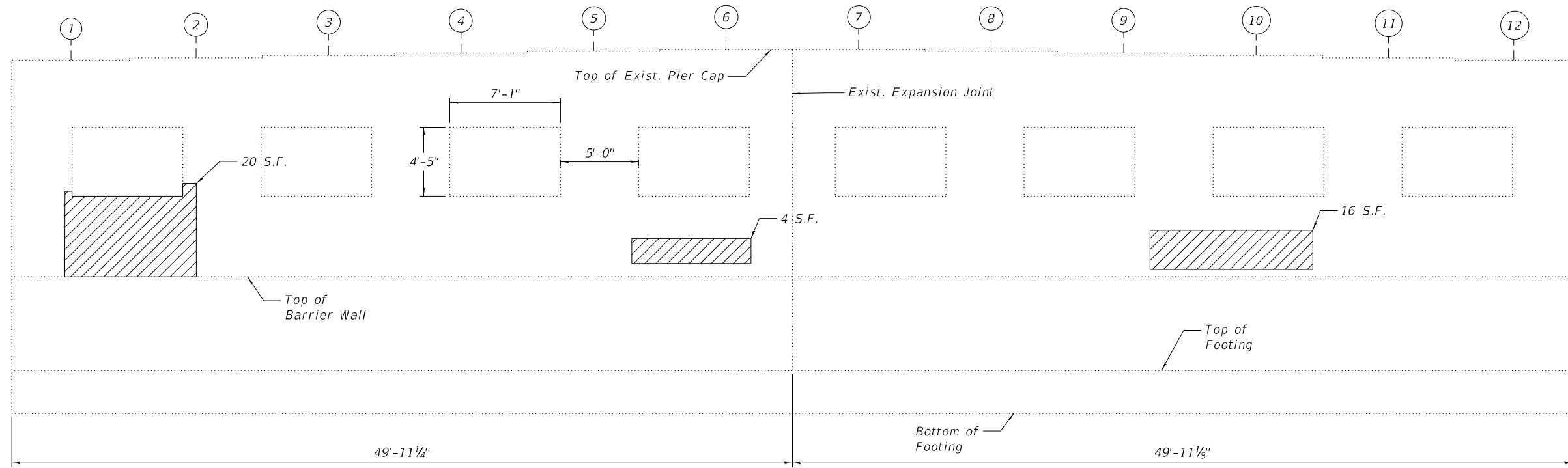
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER 1 REPAIR DETAILS
STRUCTURE NO. 016-1156**

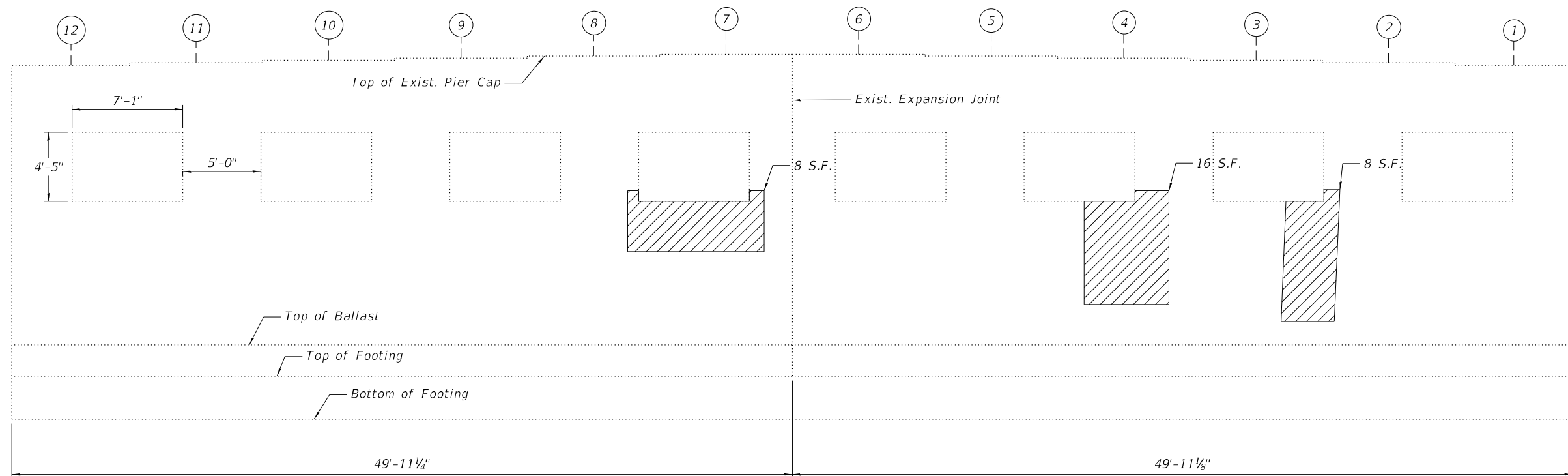
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CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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ELEVATION - PIER 2
 (Looking East)



ELEVATION - PIER 2
 (Looking West)

LEGEND

- Structural Repair of Concrete (Depth equal to or less than 5 inches)
- S.F. Square Foot

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth equal to or less than 5 inches)	Sq Ft	72

NOTE:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.



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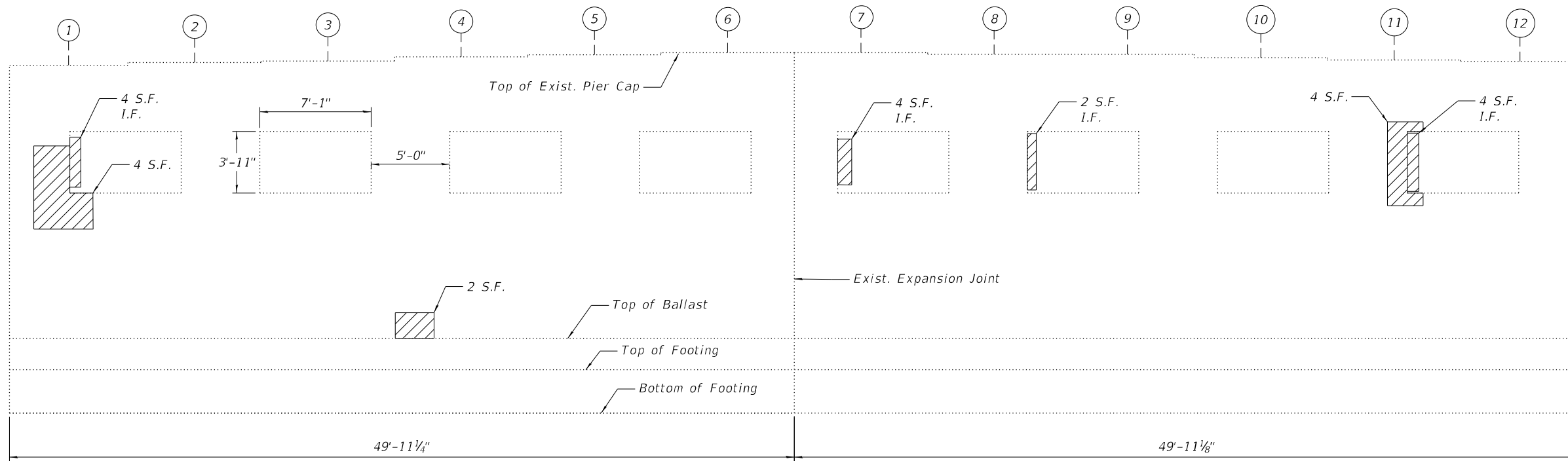
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 REPAIR DETAILS
STRUCTURE NO. 016-1156

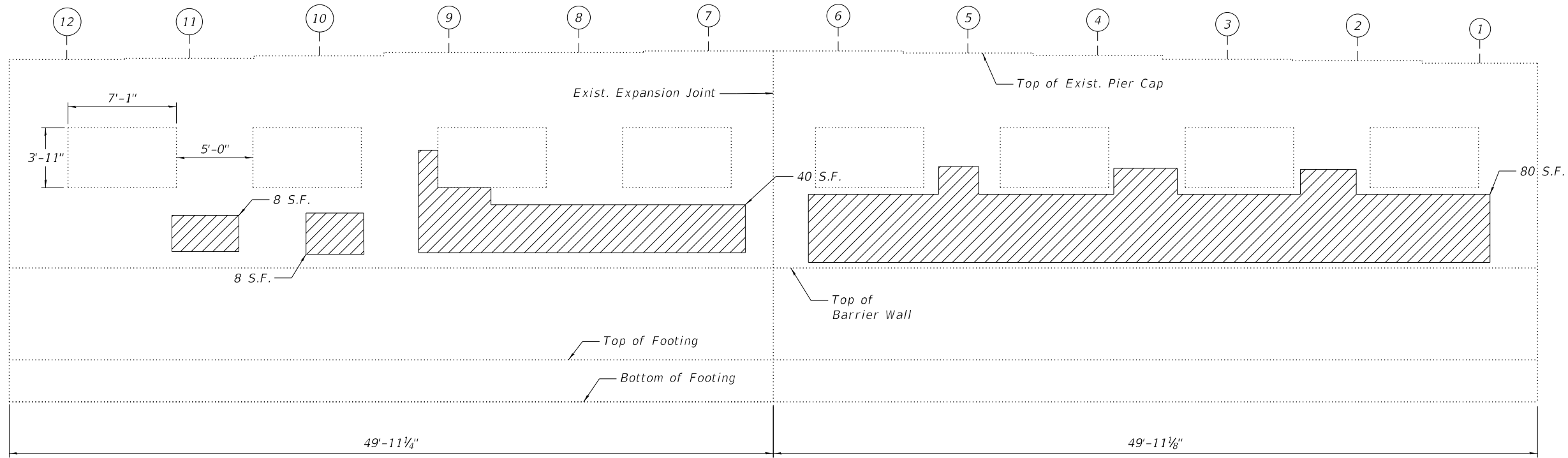
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CONTRACT NO. 62R90				
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ELEVATION - PIER 3
 (Looking East)



ELEVATION - PIER 3
 (Looking West)

NOTE:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

LEGEND

- Structural Repair of Concrete (Depth equal to or less than 5 Inches)
- S.F. Square Foot
- I.F. Inside Face

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth equal to or less than 5 Inches)	Sq Ft	160



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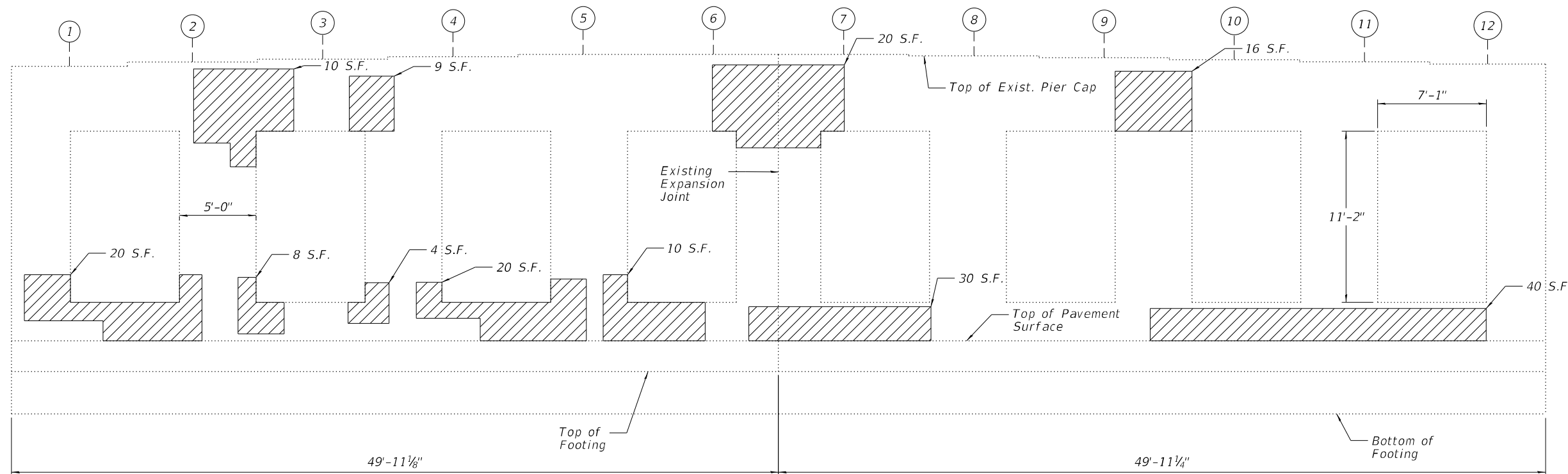
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DEPARTMENT OF TRANSPORTATION

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STRUCTURE NO. 016-1156

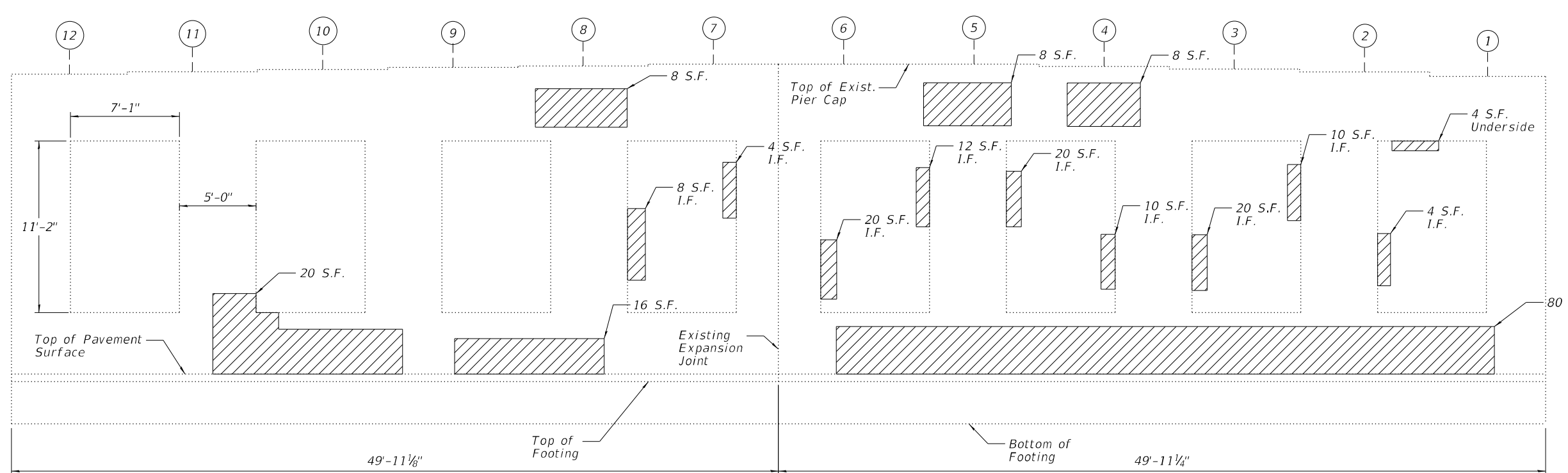
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90/94	FAI 90/94 22 BJ	COOK	83	34
CONTRACT NO. 62R90				
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ELEVATION - PIER 4
 (Looking East)



ELEVATION - PIER 4
 (Looking West)

NOTE:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

LEGEND

- Structural Repair of Concrete (Depth equal to or less than 5 Inches)
- S.F. Square Foot
- I.F. Inside Face

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth equal to or less than 5 Inches)	Sq Ft	439



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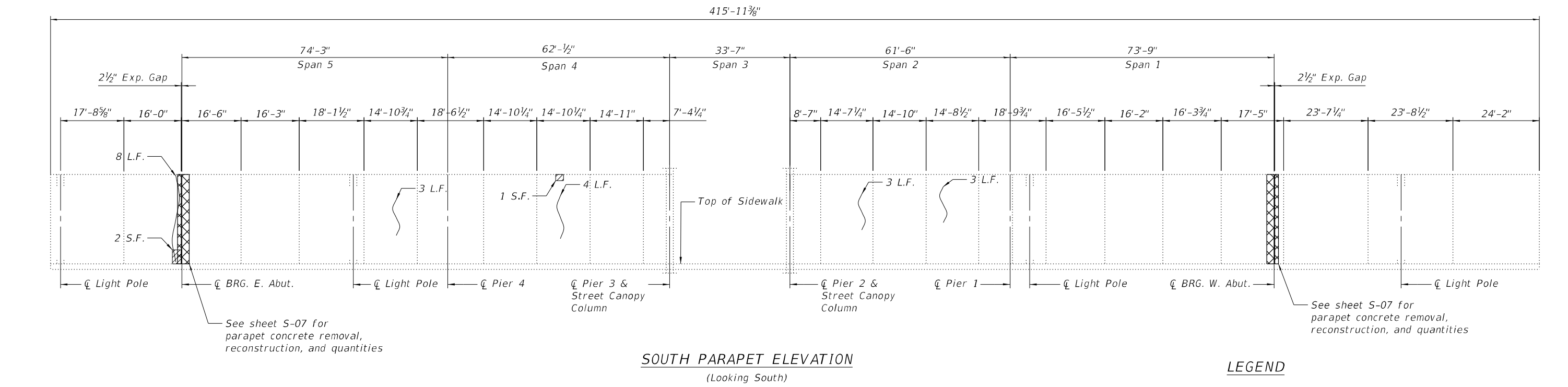
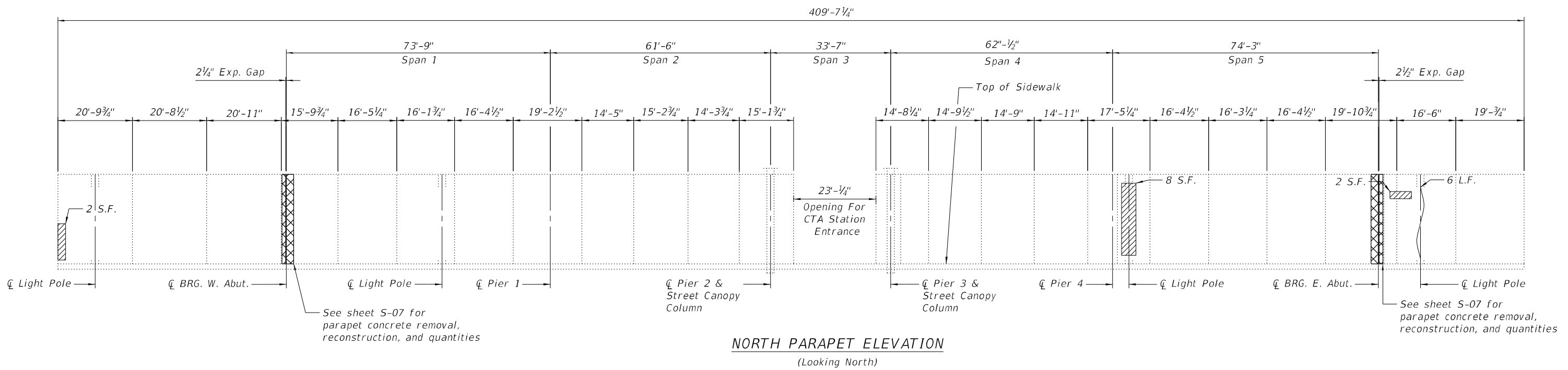
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 4 REPAIR DETAILS
STRUCTURE NO. 016-1156

SHEET S-18 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BILL OF MATERIAL

ITEM	UNIT	TOTAL
Epoxy Crack Injection	LF	27
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	15

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)
- Concrete Removal
- Epoxy Crack Injection
- S.F. Square Feet
- L.F. Linear Feet



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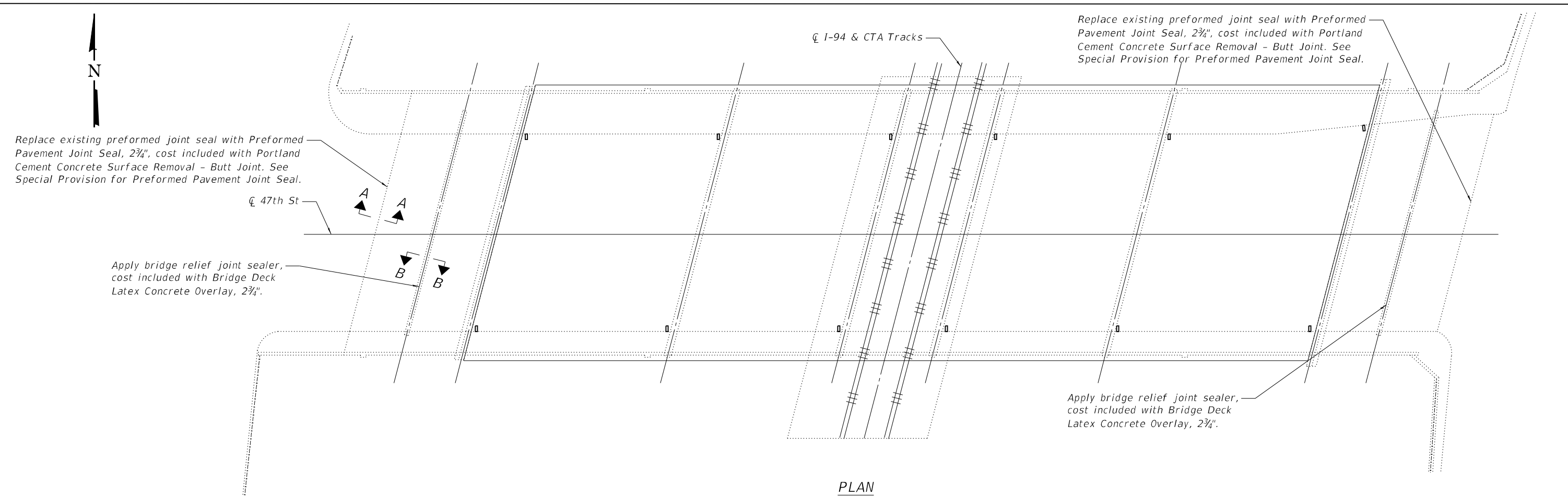
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARAPET & RAILING REPAIR DETAILS
STRUCTURE NO. 016-1156

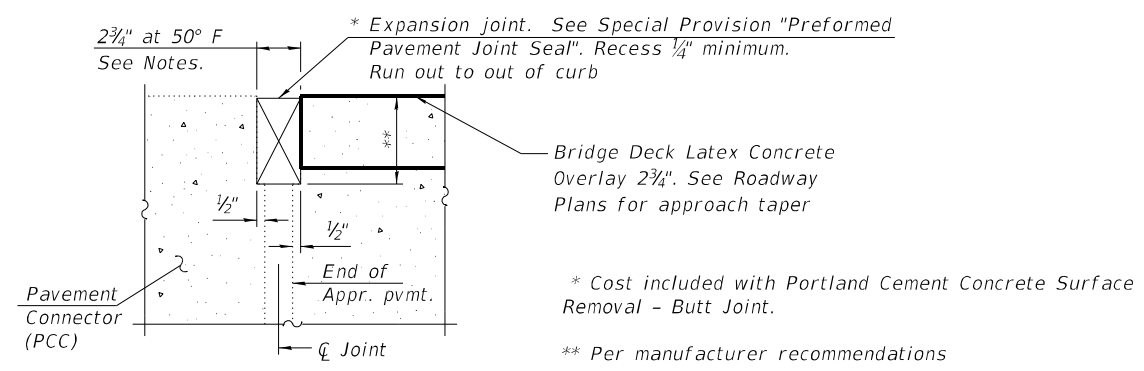
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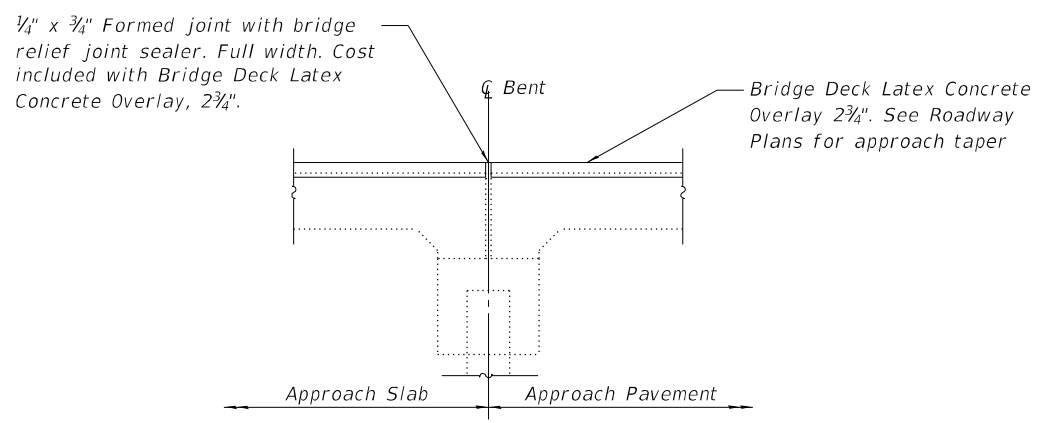
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PLAN



SECTION A-A



SECTION B-B

Notes:
 The joint opening shall be adjusted for temperature per Article 520.04 of the Standard Specifications.
 Joint opening of 2 3/4" at 50° F per record plans. Contractor to verify dimension in field prior to ordering materials.



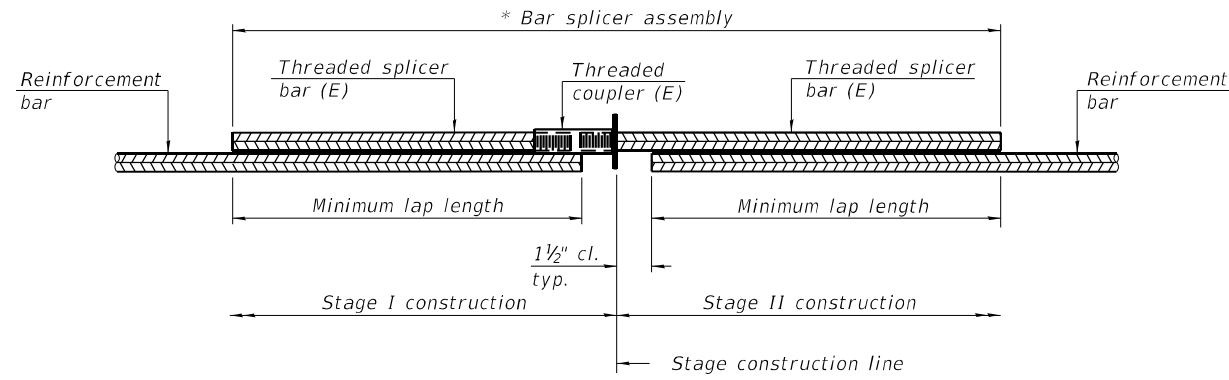
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PLOT SCALE = \$SCALES	CHECKED - CPB	REVISED -
PLOT DATE = \$DATES	DRAWN - KWY	REVISED -
	CHECKED - CPB	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

JOINT REPAIR DETAILS
 STRUCTURE NO. 016-1156

SHEET S-20 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	37
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



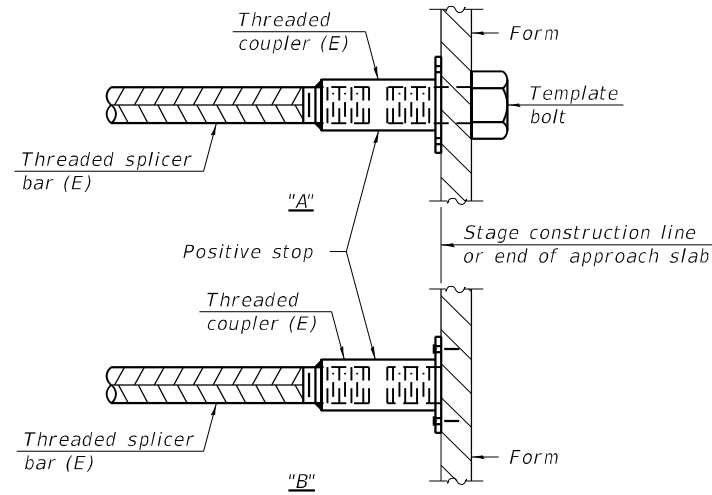
STANDARD BAR SPLICER ASSEMBLY PLAN

Only bar splicer assemblies as presented on the approved QPL list may be used.

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
West Exp. Jt.	#6	15	5'-6"
East Exp. Jt.	#6	15	5'-6"
West Exp. Jt.	#5	5	3'-6"
East Exp. Jt.	#5	5	3'-6"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
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BSD-1

2-1-2023



USER NAME = \$USERS\$	DESIGNED - K W Y	REVISED -
	CHECKED - C P B	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN - K W Y	REVISED -
PLOT DATE = \$DATES\$	CHECKED - C P B	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-1156**

SHEET S-21 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	38
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

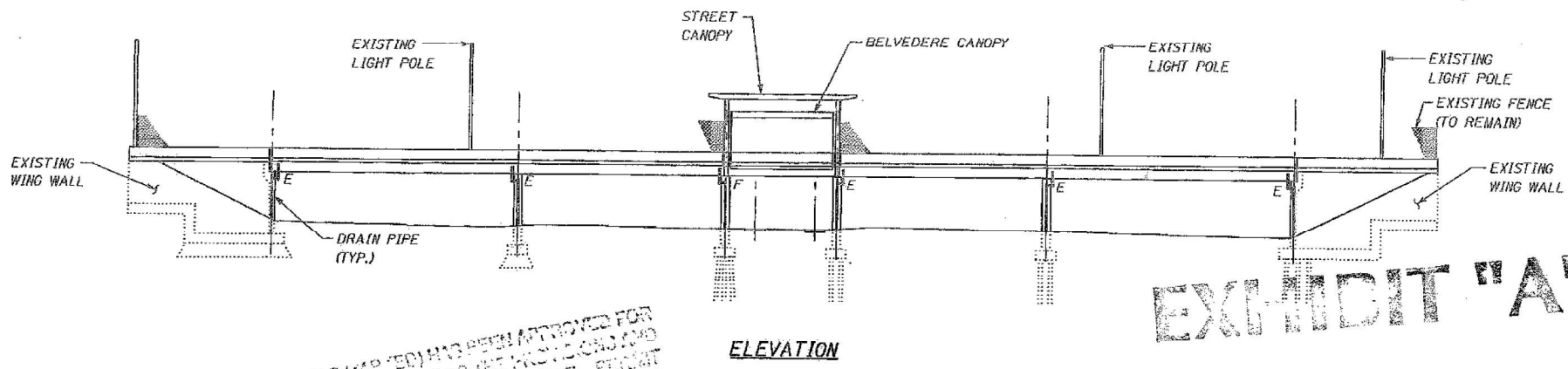
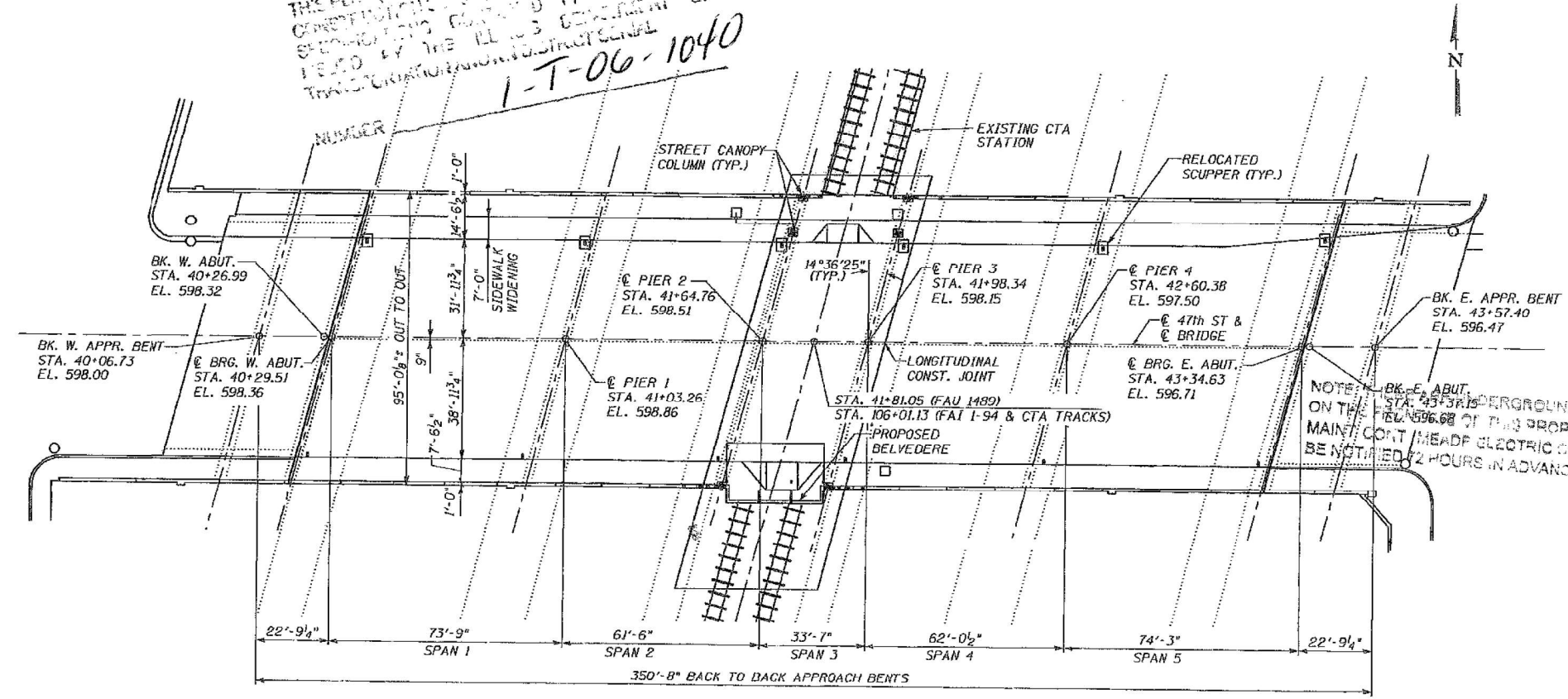


EXHIBIT "A"



INDEX OF DRAWINGS

- S47S-701 GENERAL PLAN AND ELEVATION
- S47S-702 GENERAL NOTES
- S47S-703 DEMOLITION PLAN
- S47S-704 DEMOLITION DETAILS I
- S47S-705 DEMOLITION DETAILS II
- S47S-706 DECK PLAN
- S47S-707 SUPERSTRUCTURE DETAILS I
- S47S-708 SUPERSTRUCTURE DETAILS II
- S47S-709 SUPERSTRUCTURE DETAILS III
- S47S-710 EXPANSION JOINT DETAILS
- S47S-711 PARAPET ELEVATION AND DETAILS
- S47S-712 DRAINAGE MODIFICATIONS
- S47S-713 PIER CAP MODIFICATIONS
- S47S-714 STEEL FRAMING MODIFICATIONS
- S47S-715 BRIDGE FENCE RAILING DETAILS
- S47S-716 BAR SPLICER ASSEMBLY DETAILS
- S47S-717 MAINTENANCE OF TRAFFIC TYPICALS & NOTES
- S47S-718 MAINTENANCE OF TRAFFIC STAGE 1
- S47S-719 MAINTENANCE OF TRAFFIC STAGE 2
- S47S-720 MAINTENANCE OF TRAFFIC STAGE 3
- S47S-721 MAINTENANCE OF TRAFFIC STAGE 4
- S47S-722 MAINTENANCE OF TRAFFIC STAGE 5
- S47S-723 PAVEMENT MARKINGS
- S47S-724 PROPOSED TRAFFIC SIGNAL PLAN

LOADING HS20-44

ALLOW 25 #/SD. FT. FOR FUTURE WEARING SURFACE

DESIGN SPECIFICATIONS

AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 2002

DESIGN STRESSES

FIELD UNITS
 $f_c = 3,500$ PSI
 $f_y = 60,000$ PSI (REINF.)
 $f_y = 50,000$ PSI (A270 GRADE 50) (EXIST. STEEL)

SEISMIC DATA

SEISMIC PERFORMANCE CATEGORY (SPC) - A
 BEDROCK ACCELERATION COEFFICIENT (A) = 0.04g
 SITE COEFFICIENT (S) = 1.0

SCOPE OF WORK

1. ADD A STREET CANOPY OVER THE MIDDLE SPAN OF THE BRIDGE AT THE CTA STATION ENTRANCE.
2. EXTEND THE NORTH SIDEWALK ON THE BRIDGE 7'-0" TO THE SOUTH.
3. ADD THE BELVEDERE AND CANOPY TO THE SOUTH SIDE OF THE BRIDGE (AS SHOWN)
4. REMOVE AND RELOCATE EXISTING SCUPPERS ON THE NORTH SIDE OF THE BRIDGE FROM THE EXISTING CURB LINE TO THE PROPOSED CURB LINE.
5. ADD NEW EXPANSION JOINTS BETWEEN THE BRIDGE AND THE STATION HOUSES ON THE NORTH AND SOUTH SIDES OF THE BRIDGE.
6. RELOCATE TRAFFIC SIGNALS ON THE BRIDGE TO THE CROSSWALK CANOPY.
7. REMOVE EXISTING LIGHTPOLE AT THE PROPOSED BELVEDERE LOCATION.
8. APPLY PAVEMENT MARKINGS ON THE BRIDGE.

DATE: 3/22/2006
 TIME: 4:39:54 PM

USER: evaysman
 FILE: g:\CHDS\0019\bridge\47thGP.dgn

THIS PLAN (AS MARKED) HAS BEEN APPROVED FOR CONSTRUCTION BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION PERMIT NO. 1-T-06-1040

NOTE: BK. E. ABUT. STA. 43+34.63 ON THE MAIN CONT. HEADP. ELECTRIC CO. (773-287-7600) MUST BE NOTIFIED 2 HOURS IN ADVANCE OF CONSTRUCTION.



Kevin M. Placzek
 KEVIN M. PLACZEK
 TRANSYSTEMS CORPORATION
 KEVIN M. PLACZEK, P.E., S.E.
 NO. 081-005460
 EXP. DATE 11/30/06
 S47S-701 TO S47S-723

In Charge: RJM
 Designed By: KMP
 Drawn By: CWE
 Checked By: KMP
 Approved By: RLP



30 W. Madison
 Suite 400
 Chicago, IL 60603
 Telephone: 312.456.0173
 Facsimile: 312.456.0129

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 1051 Perimeter Drive, Suite 1025
 Schaumburg, Illinois 60173
 (847) 605-9500

No.	Date	Revision	App'd
5	03/23/06	ISSUED TO DOT	
3	12/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR DOT APPROVAL	
1	06/24/05	IN PROGRESS	

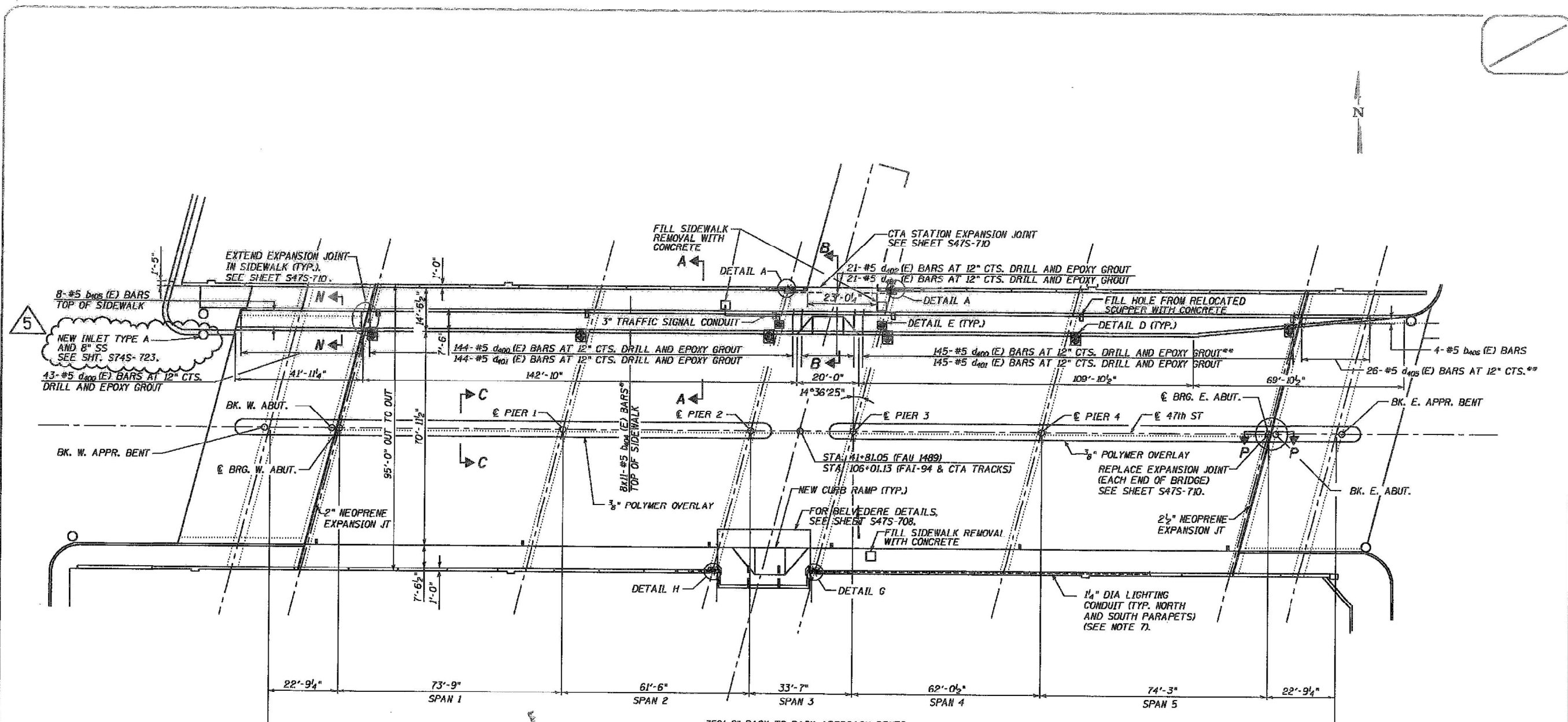


CTA RED LINE (DAN RYAN)
 AESTHETIC IMPROVEMENTS
 47TH STREET OVER I-94 (S.N. 016-1156)
 GENERAL PLAN AND ELEVATION
 Scale: NO SCALE Date: 03/23/06 J.L.O. No.:

Sheet No.
 S47S-701

USER NAME =	DESIGNED -	REVISIONS -
SUSERS	KWY	
	CPB	
	PWB	
	KWY	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	39
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT



PLAN

* CUT d_{400} (E) BARS AS REQUIRED FOR CURB RAMP
 ** CUT d_{400} (E) AND d_{405} (E) BARS AS REQUIRED FOR SIDEWALK EXTENSION

- NOTES:**
1. FOR SECTIONS AND DETAILS, SEE SHEETS S47S-707 THRU S47S-710.
 2. FOR JOINT DETAILS, SEE SHEET S47S-710.
 3. FOR BILL OF MATERIAL, SEE SHEET S47S-709.
 4. REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
 4. BARS INDICATED THUS 4x7-#5 ETC. INDICATES 4 LINES OF BARS WITH 7 LENGTHS PER LINE.
 5. EXTEND EXISTING 1" SAW CUTS IN THE NORTH SIDEWALK INTO THE WIDENED SIDEWALK.
 6. FOR BRIDGE FENCE RAILING LAYOUTS AT BELVEDERE AND STATION ENTRANCES SEE SHEET S47-709.
 7. CONTRACTOR SHALL MAINTAIN EXISTING LIGHTING CONDUIT. EXISTING LIGHTING CABLE SHALL BE PULLED AND REPLACED WITH ELECTRICAL CABLE IN CONDUIT TRIPLEX, 2-1/C WITHIN LIMITS OF PARAPET.

DATE: 3/24/2006
 TIME: 9:47:02 AM

USER: kmplaczek
 P.L.E.: g:\CH05\0019\Bridges\47thDeck.dgn

In Charge: RJM
 Designed By: CWE
 Drawn By: CWE
 Checked By: KMP
 Approved By: RLP



30 W. Monroe
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 Chicago, IL 60603
 Telephone 312.456.0123
 Facsimile 312.456.0124



No.	Date	Revision	App'd
5	03/23/06	ISSUED TO IDOT	
3	02/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR IDOT APPROVAL	
1	06/24/05	IN PROGRESS	



CTA RED LINE (DAN RYAN)
 AESTHETIC IMPROVEMENTS
 47TH STREET OVER I-94 (S.N. 016-1156)
 DECK PLAN

Sheet No.
 S47S-706

Scale: 1"=5'
 Date: 03/23/06
 J.O. No.:



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156

SHEET S-23 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	40
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

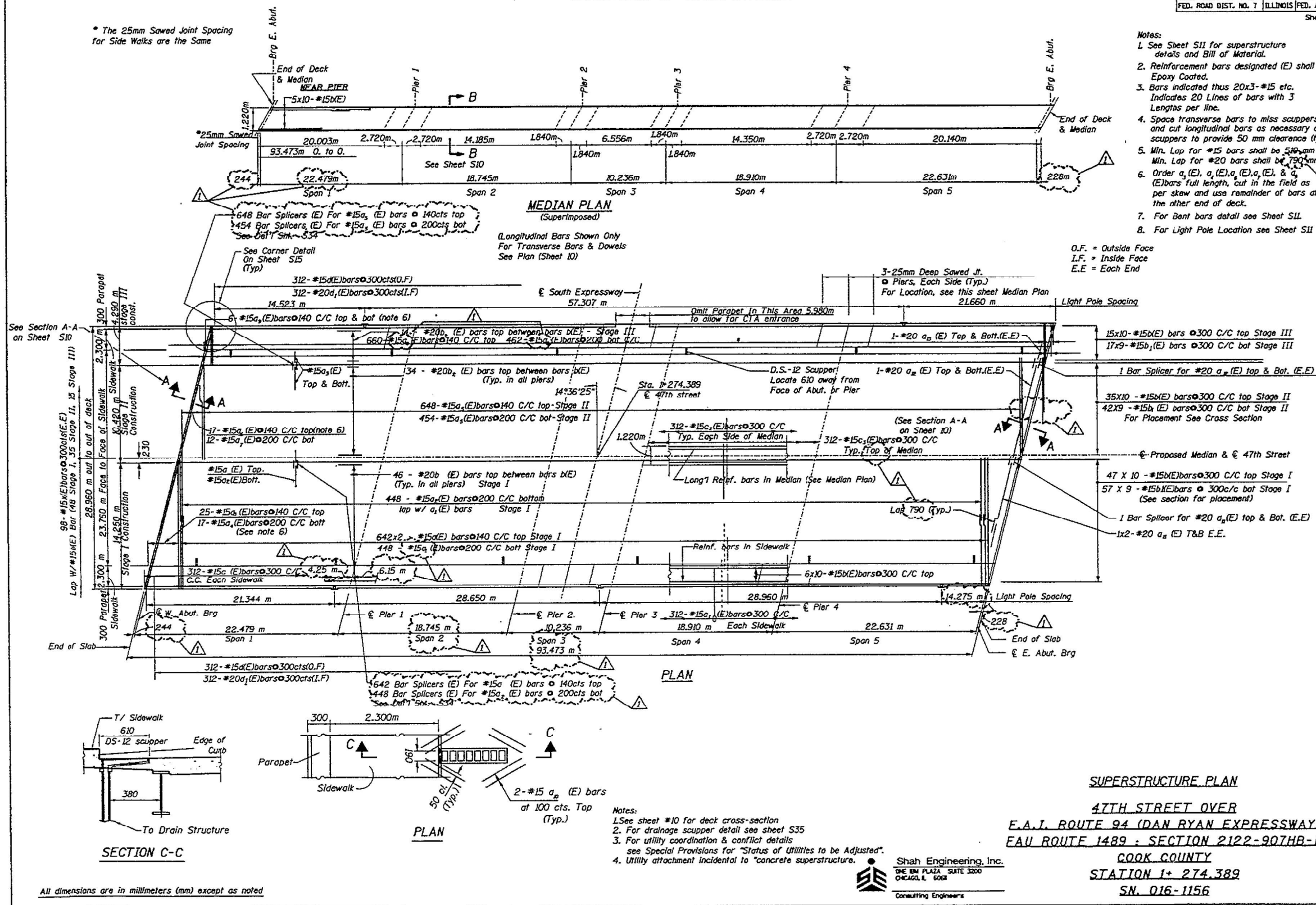
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1489	2122-907HB-R	COOK	83	32
STA. TO STA.				
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT	

Sheet 59 of 39

* The 25mm Sawn Joint Spacing for Side Walks are the Same

- Notes:
- See Sheet S11 for superstructure details and Bill of Material.
 - Reinforcement bars designated (E) shall Be Epoxy Coated.
 - Bars indicated thus 20x3-#15 etc. Indicates 20 Lines of bars with 3 Lengths per line.
 - Space transverse bars to miss scuppers and cut longitudinal bars as necessary at scuppers to provide 50 mm clearance (typ.)
 - Min. Lap for #15 bars shall be 510mm. Min. Lap for #20 bars shall be 790mm.
 - Order $a_1(E)$, $a_2(E)$, $a_3(E)$, $a_4(E)$, & $a_5(E)$ bars full length, cut in the field as per skew and use remainder of bars at the other end of deck.
 - For Bent bars detail see Sheet S11.
 - For Light Pole Location see Sheet S11.

O.F. = Outside Face
I.F. = Inside Face
E.E. = Each End



SUPERSTRUCTURE PLAN
47TH STREET OVER
E.A.J. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

- Notes:
- See sheet #10 for deck cross-section
 - For drainage scupper detail see sheet S35
 - For utility coordination & conflict details see Special Provisions for "Status of Utilities to be Adjusted".
 - Utility attachment incidental to "concrete superstructure".

Shah Engineering, Inc.
ONE EM PLAZA SUITE 3200
CHICAGO, IL 60604
Consulting Engineers

Revision 7/15/99 MM

All dimensions are in millimeters (mm) except as noted

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

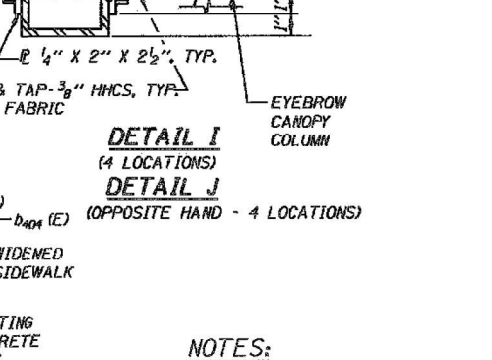
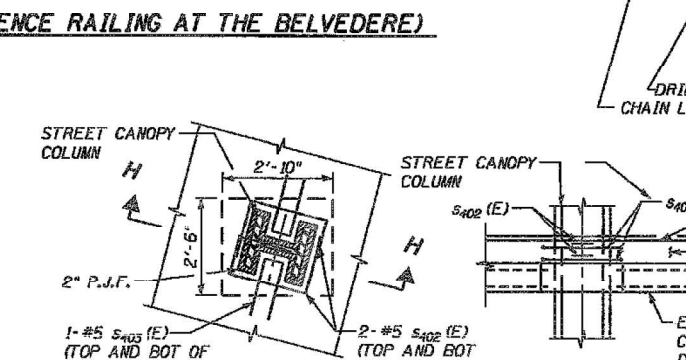
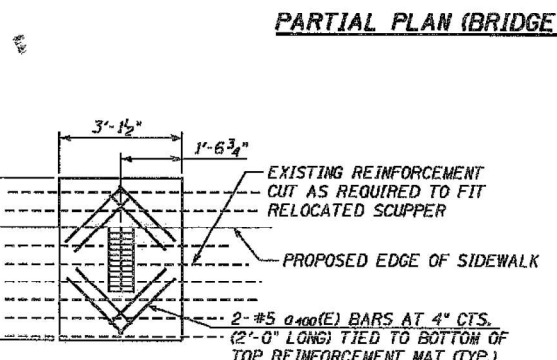
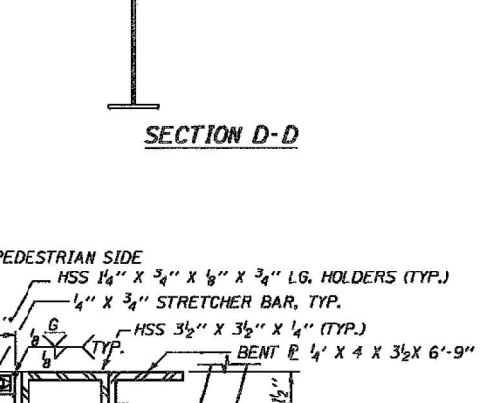
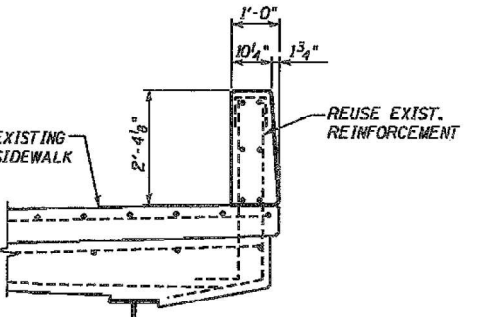
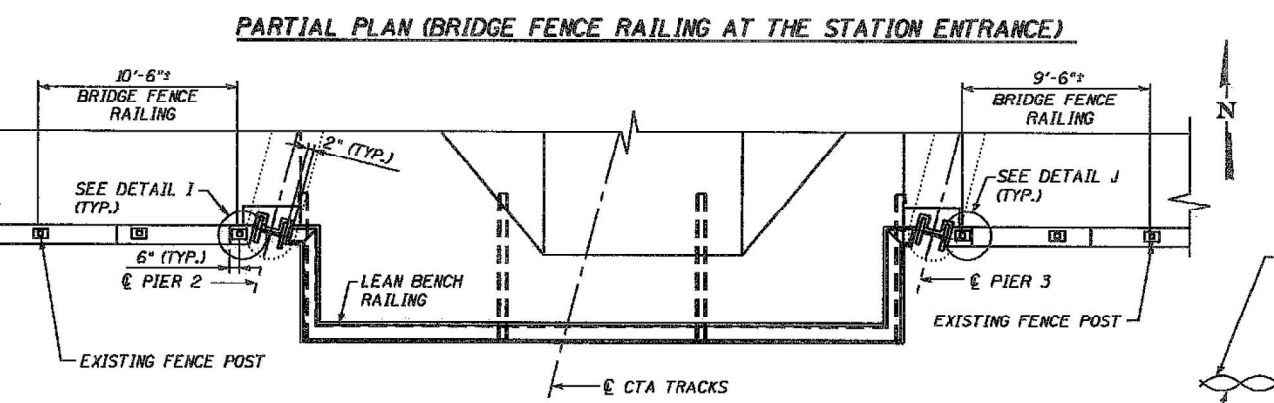
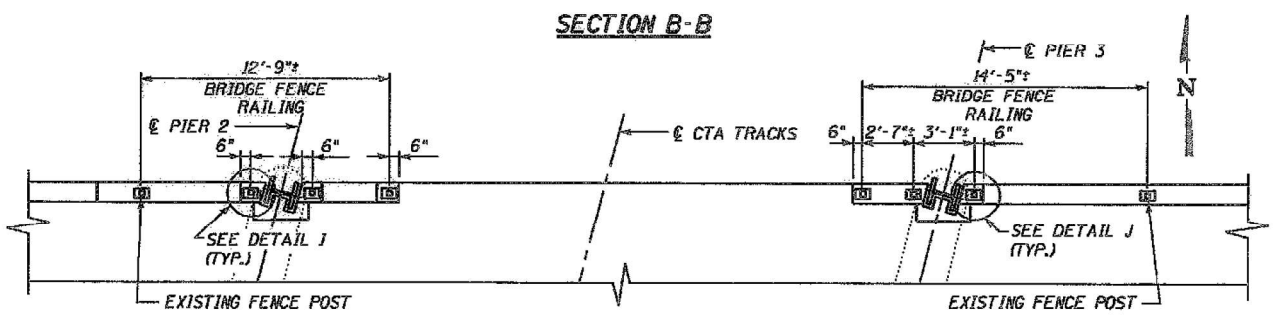
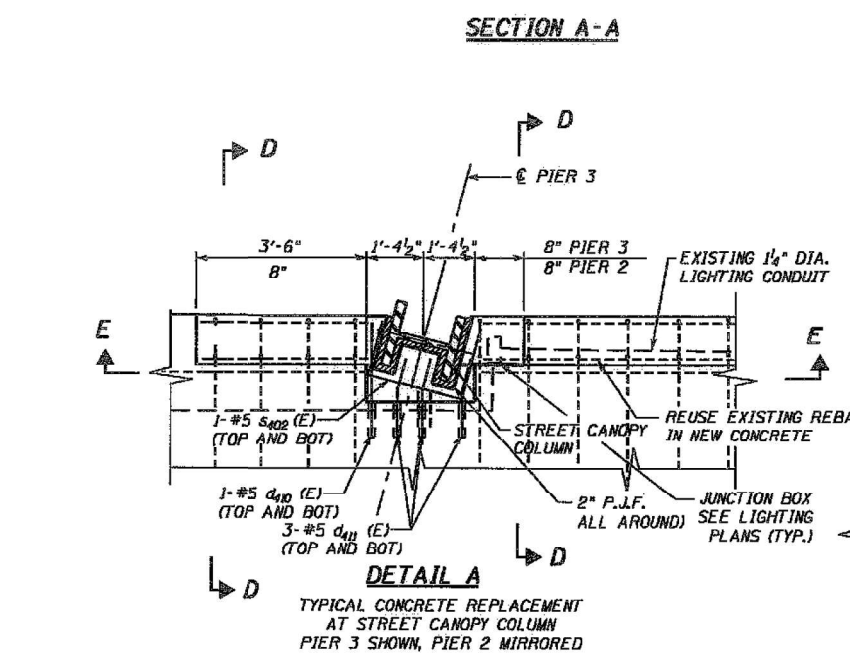
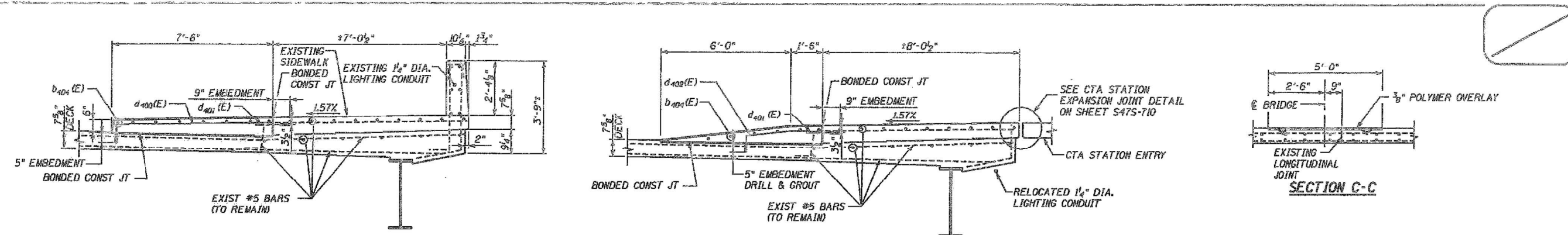
EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	41
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	



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PLOT DATE = \$DATE\$	DRAWN - PWB	REVISED -
	CHECKED - KWY	REVISED -

SHEET S-24 OF S-52 SHEETS



NOTES:
 1. WORK THIS SHEET WITH SHEETS S47S-706 AND S47S-711.
 2. SEE SHEET S47S-709 FOR BILL OF MATERIAL.

DATE: 3/22/2006
 TIME: 4:45:38 PM

USER: evayman
 FILE: c:\CH05\0019\Bridg\47thSuper2.dgn

In Charge: RLP
 Designed By: CWE
 Drawn By: CWE
 Checked By: KMP
 Approved By: RLP



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TRANSYSTEMS CORPORATION
 1051 Parimeter Drive, Suite 1025
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 (847) 805-1800

No.	Date	Revision	App'd
3	12/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR IDOT APPROVAL	
1	06/24/05	IN PROGRESS	



Chicago Transit Authority
 Engineering Department

CTA RED LINE (DAN RYAN)
 AESTHETIC IMPROVEMENTS

47TH STREET OVER I-94 (S.N. 016-1156)
 SUPERSTRUCTURE DETAILS I

Scale: NO SCALE Date: 03/23/06 J.P. No. 1

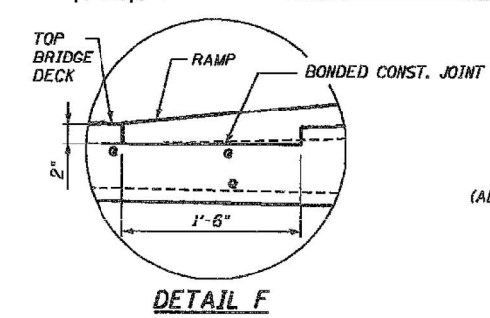
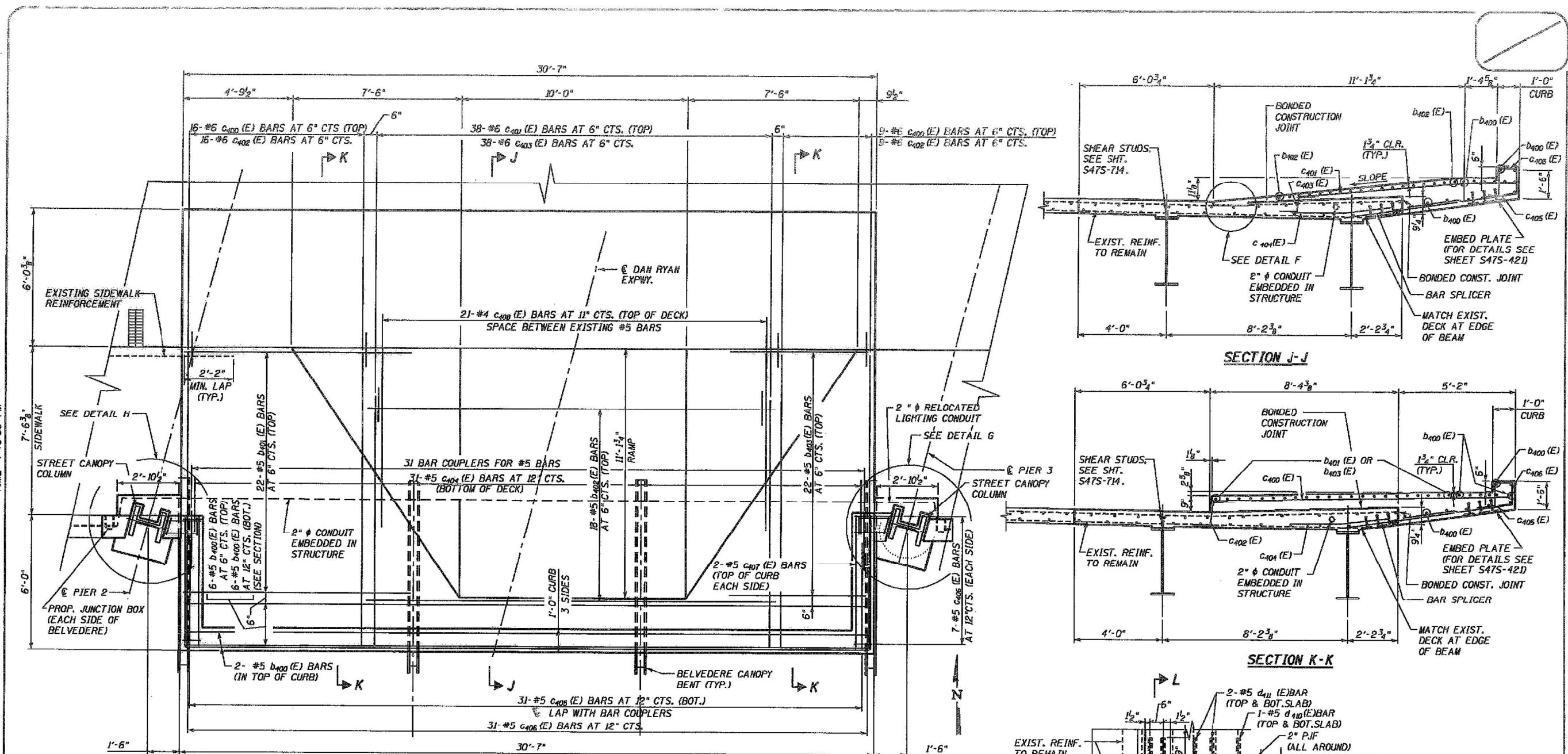
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	OPB	-
	PWB	-
	KWY	-

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	42
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

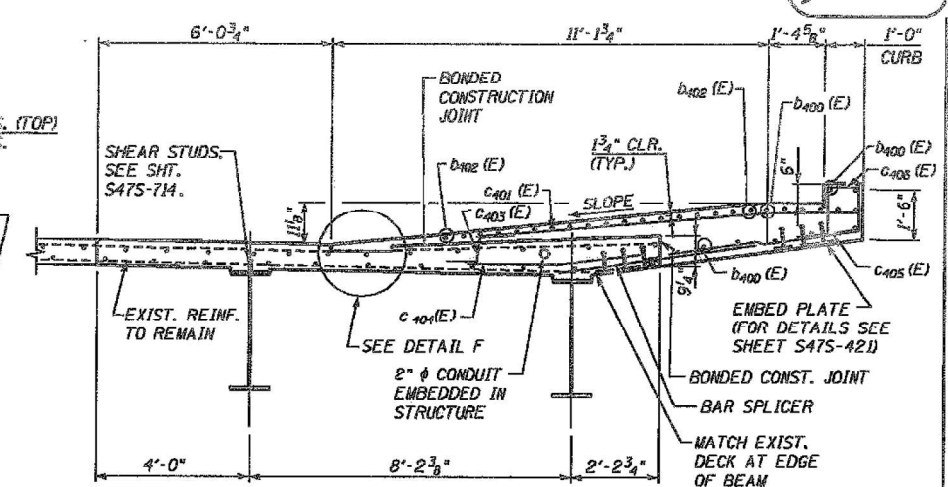
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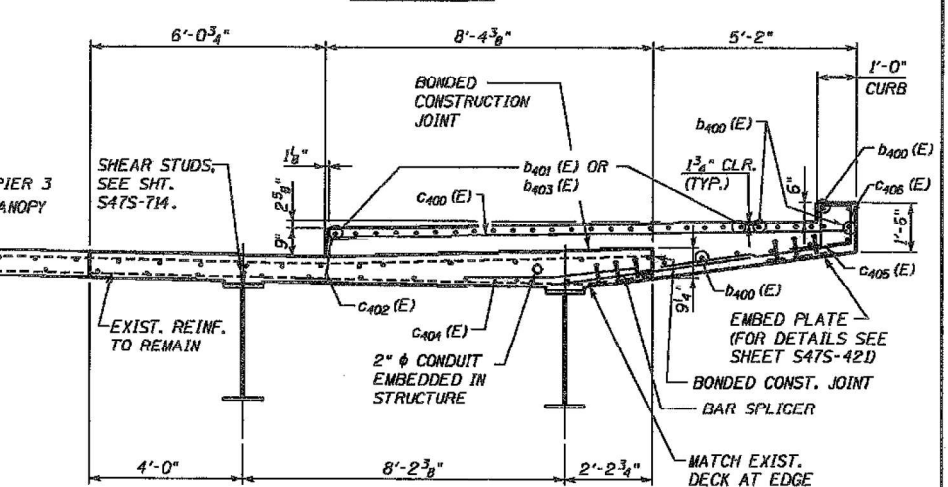


PLAN - BELVEDERE
(ALL EXISTING REINFORCEMENT NOT SHOWN)

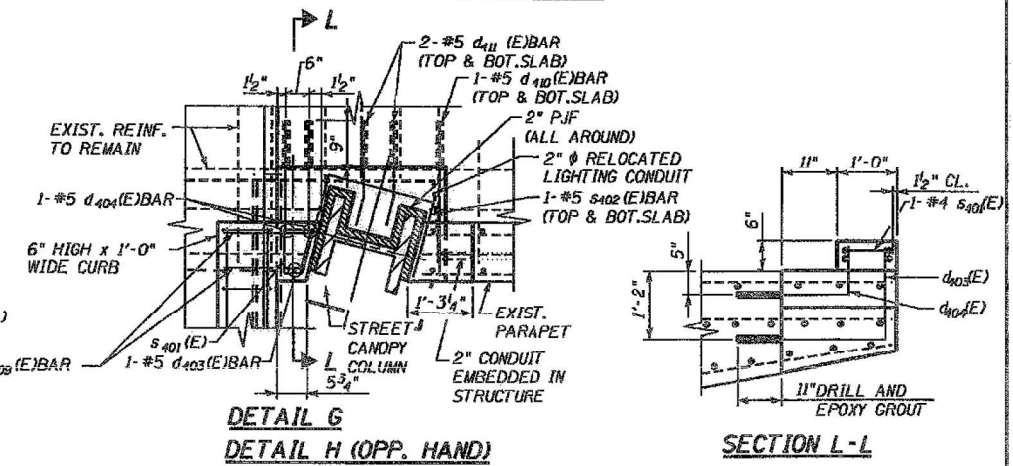
- NOTES:**
- SEE SHEET S475-421 FOR THE BELVEDERE CANOPY CONNECTION DETAILS TO THE BRIDGE DECK.
 - THE EXISTING REINFORCEMENT IN THE BRIDGE DECK IS TO BE CLEANED AND REUSED.
 - THE EXISTING TRANSVERSE REINFORCEMENT IN THE SIDEWALK IS TO BE REMOVED. THE EXISTING LONGITUDINAL REINFORCEMENT IN THE SIDEWALK IS TO BE REMOVED BUT SHALL BE LAPPED (2'-2" MIN.) WITH THE NEW REINFORCEMENT.
 - ALL JOINTS BETWEEN EXISTING AND NEW CONCRETE SHALL BE BONDED CONSTRUCTION JOINTS.
 - FOR REINFORCEMENT DETAILS, SEE SHEET S475-710.
 - THE FOLLOWING MINIMUM LAP LENGTHS SHOULD BE USED:
2'-2" FOR #5 BARS, 2'-7" FOR #6 BARS.
 - SEE SHEET S475-715 FOR BRIDGE FENCE RAILING AND LEAN BENCH RAILING DETAILS.



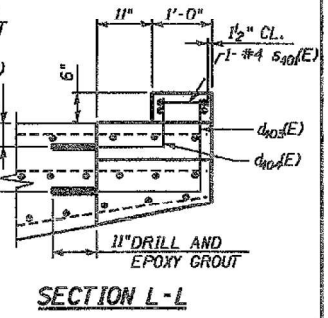
SECTION J-J



SECTION K-K



DETAIL G
DETAIL H (OPP. HAND)



SECTION L-L

In Charge	RLP
Designed By	KMP
Drawn By	KMP/CWE
Checked By	KMP/CWE
Approved By	RLP



30 W. Monroe
Suite 400
Chicago, IL 60603
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TRANSYSTEMS CORPORATION
1054 Parkmer Drive, Suite 1025
Schaumburg, Illinois 60173
(317) 605-9500

No.	Date	Revisor	App'd
3	12/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR IDOT APPROVAL	
1	06/24/05	IN PROGRESS	



Chicago Transit Authority
Engineering Department

CTA RED LINE (DAN RYAN)
AESTHETIC IMPROVEMENTS

47TH STREET OVER I-94 (S.N. 016-1156)
SUPERSTRUCTURE DETAILS II

Scale: NO SCALE Date: 03/23/08 J.D. No.:

Sheet No. S475-708



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-26 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	43

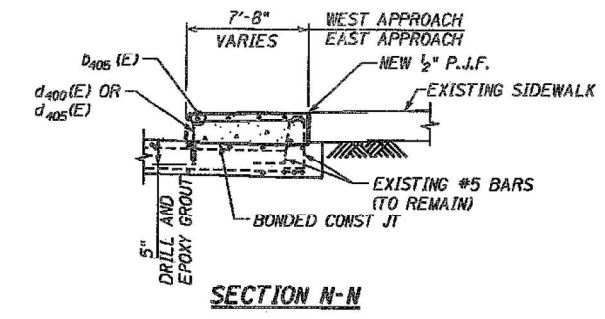
CONTRACT NO. 62R90
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

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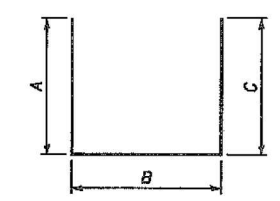
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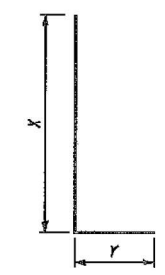
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b401 (E)	22	# 5	10' - 7"	—
b402 (E)	18	# 5	18' - 8"	—
b403 (E)	22	# 5	6' - 6"	—
b404 (E)	88	# 5	29' - 10"	—
b405 (E)	8	# 5	41' - 9"	—
b406 (E)	4	# 5	22' - 5"	—
c400 (E)	25	# 6	13' - 2"	—
c401 (E)	38	# 6	10' - 4"	—
c402 (E)	25	# 6	5' - 0"	—
c403 (E)	38	# 6	4' - 8"	—
c404 (E)	31	# 5	4' - 10"	—
c405 (E)	31	# 5	4' - 8"	—
c406 (E)	45	# 5	7' - 4"	—
c407 (E)	4	# 5	5' - 8"	—
c408 (E)	21	# 4	9' - 6"	—
c409 (E)	4	# 5	1' - 3"	—
d400 (E)	332	# 5	7' - 11"	L
d401 (E)	310	# 5	1' - 9"	—
d402 (E)	21	# 5	5' - 0"	—
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d404 (E)	4	# 5	2' - 7"	L
d405 (E)	26	# 5	4' - 4"	L
d410 (E)	4	# 5	2' - 6"	—
d411 (E)	20	# 5	1' - 7"	—
s401 (E)	2	# 4	3' - 0"	U
s402 (E)	24	# 5	4' - 6"	L
s403 (E)	8	# 5	3' - 6"	L
PAY ITEM				
CONCRETE SUPERSTRUCTURE		CU. YD.	95	
REINF. BARS, EPOXY COATED		POUND	10,720	

BAR	A	B	C
s402 (E)	1'-0"	2'-0"	1'-6"
s403 (E)	1'-6"	6"	1'-6"

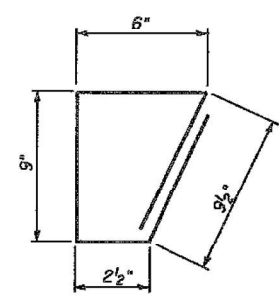


**BARS s402 (E), s402 (E)
 AND s403 (E)**

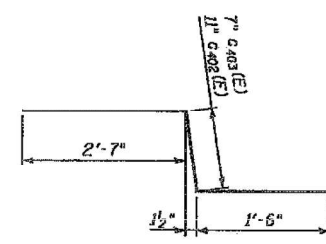
BAR	X	Y
d400 (E)	7'-2"	9"
d402 (E)	4'-4"	8"
d403 (E)	1'-6"	2'-8"
d404 (E)	7'	2'-0"
d405 (E)	3'-5"	11"



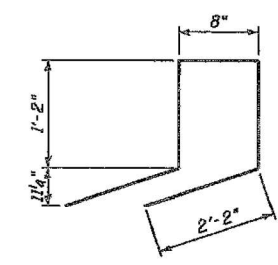
BAR BEND DETAIL



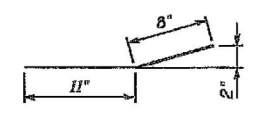
BAR s401 (E)



**BARS c402 (E)
 AND c403 (E)**



BAR c406 (E)



BAR d411 (E)

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

DRILL AND EPOXY GROUT BARS ACCORDING TO ARTICLE 5B4 OF THE IDOT STANDARD SPECIFICATIONS.

NOTES:
 1. WORK THIS SHEET WITH SHEETS S475-706 AND S475-711.

In Charge	RJM
Designed By	SLC
Drawn By	SLC
Checked By	KMP
Approved By	RLP



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No.	Date	Revision	App'd
3	12/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR IDOT APPROVAL	
1	06/24/05	IN PROGRESS	



CTA RED LINE (DAN RYAN) AESTHETIC IMPROVEMENTS	
47TH STREET OVER I-94 (S.N. 016-1156) SUPERSTRUCTURE DETAILS III	
Scale: NO SCALE	Date: 03/23/06

Sheet No.
 S475-709



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PLOT SCALE = \$SCALE\$	CHECKED - OPB	REVISED -
PLOT DATE = \$DATES	DRAWN - PWB	REVISED -
	CHECKED - KKY	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156

SHEET S-27 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	44
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

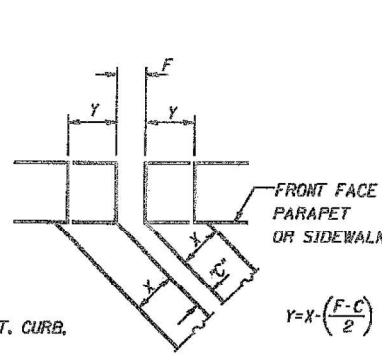
Joint Size	"C" at 50°F	"D" at 50°F
2"	2"	1 1/2" Min.
2 1/2"	2 1/2"	1 3/4" Min.
3"	3"	2 1/4" Min.

INSTALLATION NOTES

- INSTALL CONTINUOUS SEAL IN ROADWAY, PARAPET, CURB, AND SIDEWALK.
- INSTALL ANCHOR BLOCKS AS INDICATED.

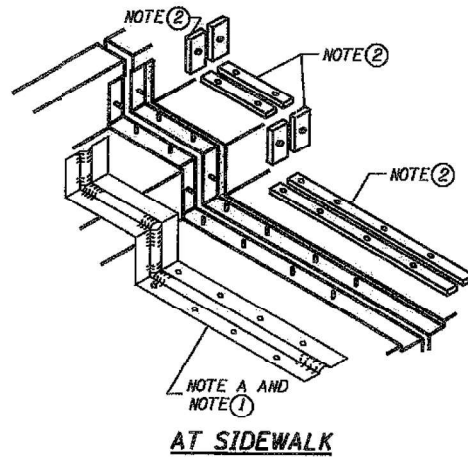
NOTE A: MAXIMUM SPACING OF ANCHOR BOLTS SHALL BE 12" CENTERS.

FORMING BLOCKOUT SKETCH

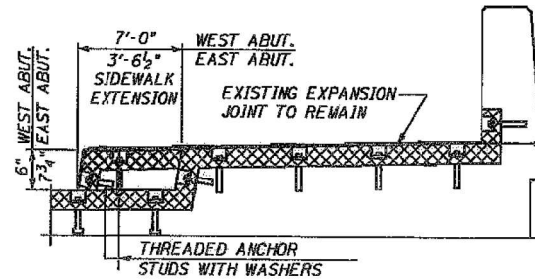


SKIEW LIMITATIONS

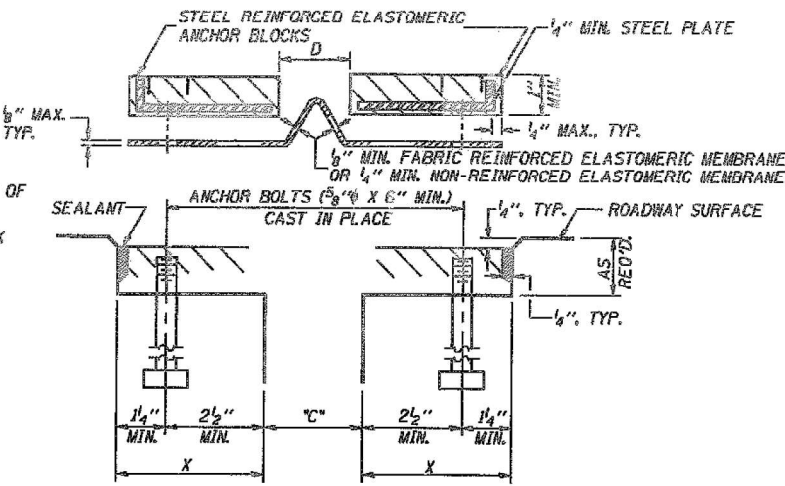
THE DETAILS OF THE ANCHOR BLOCKS AND THE ELASTOMERIC MEMBRANE IN THE PARAPET, AS SHOWN, ARE FOR UP TO 50° SKEWS. FOR SKEWS GREATER THAN 50°, THE ANCHOR BLOCKS AND THE ELASTOMERIC MEMBRANE, INSTALLED ACCORDING TO DIMENSION "D", MIGHT REQUIRE MODIFICATIONS TO INSURE A MINIMUM CLEARANCE OF 1/2" FROM CENTERLINE OF ANCHOR STUDS TO EDGE OF PARAPET OPENING. THE ANCHOR BLOCKS AND THE ELASTOMERIC MEMBRANE SHALL ALSO BE INSTALLED TO THE TOP OF THE PARAPET WITH THE ANCHOR STUDS SPACED AT ±12" CTS.



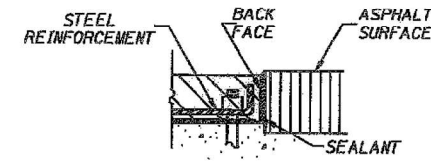
AT SIDEWALK



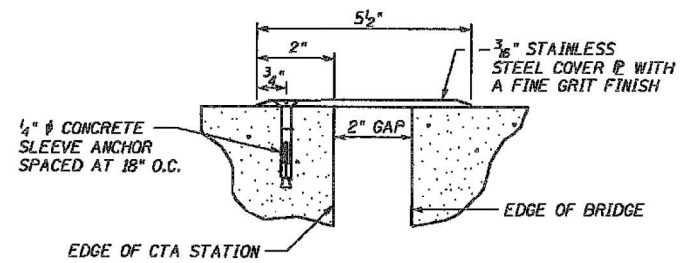
AT SIDEWALK



CROSS SECTION

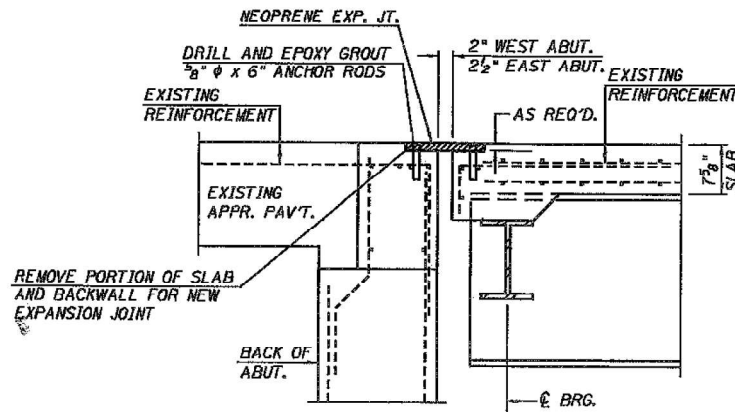


ANCHOR BLOCK WITH ASPHALT SURFACE



CTA STATION EXPANSION JOINT DETAIL

WABO FAST FLOOR MODEL FJF-200 WITH STAINLESS STEEL COVER (FINE GRIT FINISH) (INSTALLED PER MANUFACTURER'S RECOMMENDATIONS)



SECTION P-P EXPANSION JOINT REPLACEMENT WHERE EXISTING MEDIAN IS REMOVED

LEGEND:
 CONCRETE REMOVAL

BILL OF MATERIAL

ITEM	UNIT	TOTAL
NEOPRENE EXPANSION JOINT 2"	FOOT	6
NEOPRENE EXPANSION JOINT 2 1/2"	FOOT	6
CTA EXPANSION JOINT	FOOT	23

DATE: 3/22/2006
TIME: 4:14:56 PM

JSER: evaystmcn
FILE: g:\CH05\0019\Bridg\47thSuper3.dgn

In Charge: RJM
 Designed By: SLC
 Drawn By: SLC
 Checked By: KMP
 Approved By: RLP



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No.	Date	Revision	App'd
3	12/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR IDOT APPROVAL	
1	06/24/05	IN PROGRESS	

Chicago Transit Authority
 Engineering Department

CTA RED LINE (DAN RYAN)
 AESTHETIC IMPROVEMENTS
 47TH STREET OVER I-94 (S.N. 016-1156)
 EXPANSION JOINT DETAILS
 Scale: NO SCALE Date: 03/23/06 J.O. No.:

Sheet No.
S47S-710



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PLOT SCALE = \$SCALE\$	CHECKED - CPB	REVISED -
PLOT DATE = \$DATES	DRAWN - PWB	REVISED -
	CHECKED - KWY	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156

SHEET S-28 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	45
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	35
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

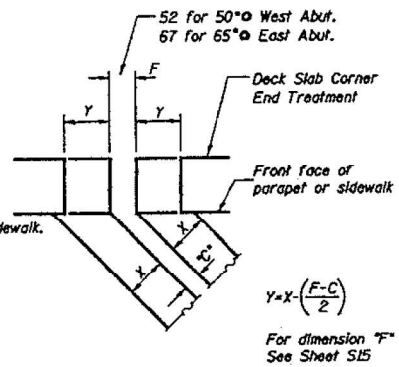
Sheet 512 of 39

Joint Size	°C at 10 °C	°D at 10 °C
50	50	40 Mil.
65	65	45 Mil.

INSTALLATION NOTES

- Install continuous seal in roadway, parapet, curb, and sidewalk.
- Install anchor blocks as indicated.

NOTE A: Maximum spacing of anchor bolts shall be 300 centers.



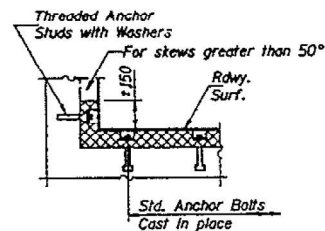
$$Y = X \left(\frac{F-C}{2} \right)$$

For dimension "F" See Sheet S15

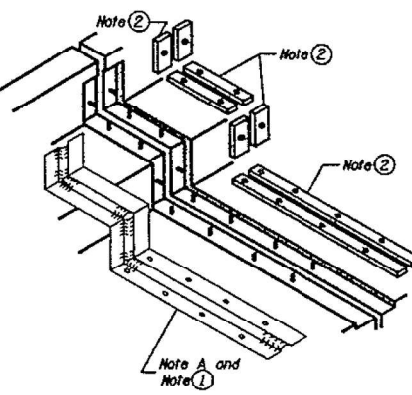
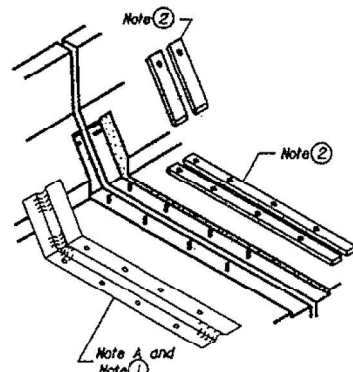
FORMING BLOCKOUT SKETCH

SKEW LIMITATIONS

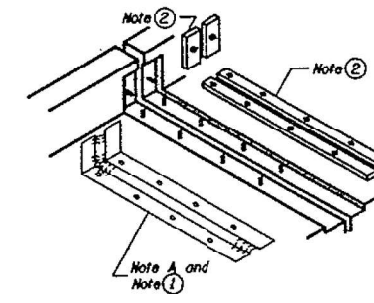
The details of the anchor blocks and the elastomeric membrane in the parapet, as shown, are for up to 50° skews. For skews greater than 50°, the anchor blocks and the elastomeric membrane, installed according to dimension "D", might require modifications to insure a minimum clearance of 40 mm from centerline of anchor studs to edge of parapet opening. The anchor blocks and the elastomeric membrane shall also be installed to the top of the parapet with the anchor studs spaced at ±300 cts.



AT WALL

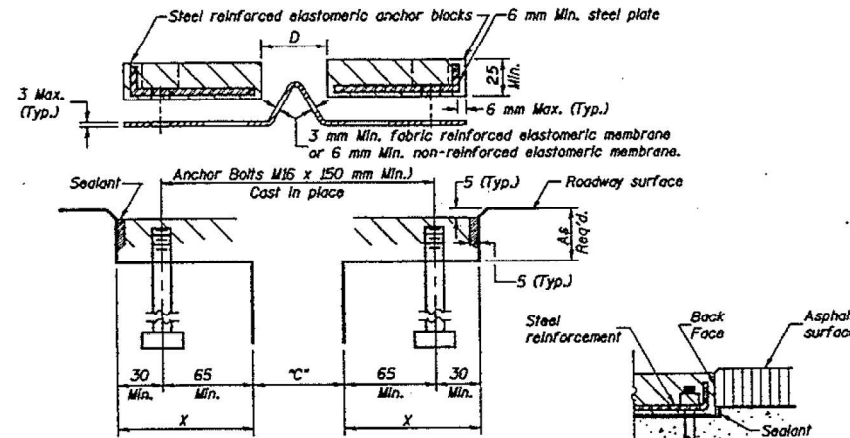


AT SIDEWALK OR MEDIAN



AT WALL

In Per IDOT Comment See Sheet 9



CROSS SECTION

Deck Slab Corner End Treatment

ANCHOR BLOCK WITH ASPHALT SURFACE

GENERAL NOTES

Continuous Seal Neoprene Expansion Joint shall consist of molded anchor blocks of elastomer and steel, field assembled over continuous lengths of elastomeric membrane.

The elastomeric membrane shall be pre-molded with a single or a double upward convolution that will have a "memory" to return to its molded position upon joint closure.

The convolution length shall be such that the extended length will not be greater than the manufactured length when the joint is fully expanded in its design range and will not protrude above the anchor blocks when the joint is fully compressed.

Joint openings shall be adjusted according to Article 503.10(c) of the Standard Specifications when the deck is poured at an ambient temperature other than 10 °C.

The parapet and roadway membrane shall be made continuous by an approved vulcanizing process. Lapping will not be permitted.

All dimensions are in millimeters (mm) except as noted.

**NEOPRENE EXPANSION JOINT DETAILS
47TH STREET OVER**

**E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
FAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156**

Shah Engineering, Inc.
ONE 884 PLAZA SUITE 3200
CHICAGO, IL 60682
Consulting Engineers

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

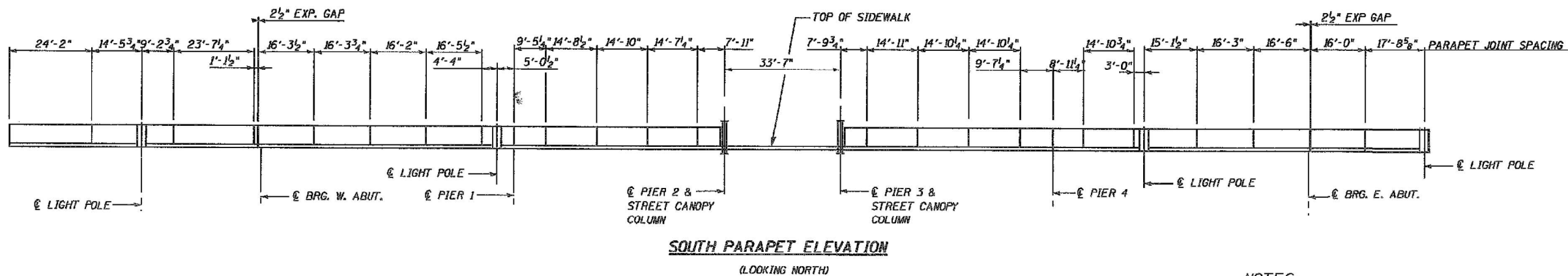
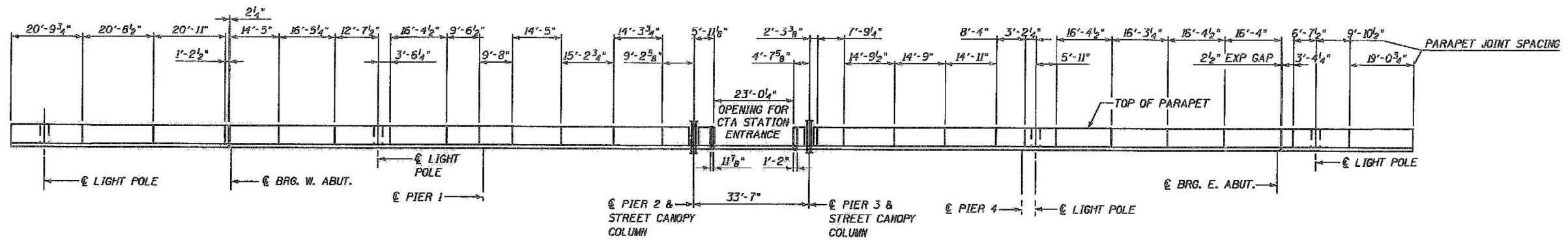
SHEET S-29 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	46
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

Primera

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		CHECKED -	OPB	REVISED -	
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NOTES:
 1. SEE SHEET S47S-703 AND S47S-704 FOR CONCRETE REMOVAL DETAILS.
 2. SEE SHEETS S47S-706 AND S47S-708 FOR PARAPET MODIFICATIONS.

DATE: 3/22/2006
 TIME: 4:48:21 PM

JSR: evoyzman
 FILE: g:\CHDS\0019\Bridg\47thParapet.dgn

In Charge: RJM
 Designed By: KMP
 Drawn By: JRS
 Checked By: KMP
 Approved By: RLP



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No.	Date	Revision	App'd
3	12/23/05	REVISED SCOPE	
2	07/04/05	ISSUED FOR BID APPROVAL	
1	06/24/05	IN PROGRESS	

Chicago Transit Authority
 Engineering Department

CTA RED LINE (DAN RYAN)
 AESTHETIC IMPROVEMENTS
 47TH STREET OVER I-94 (S.N. 016-1156)
 PARAPET ELEVATION AND DETAILS

Sheet No.
 S47S-711



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PLOT DATE =	\$\$DATE\$	DRAWN -	PWB	REVISED -	
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156

SHEET S-30 OF S-52 SHEETS

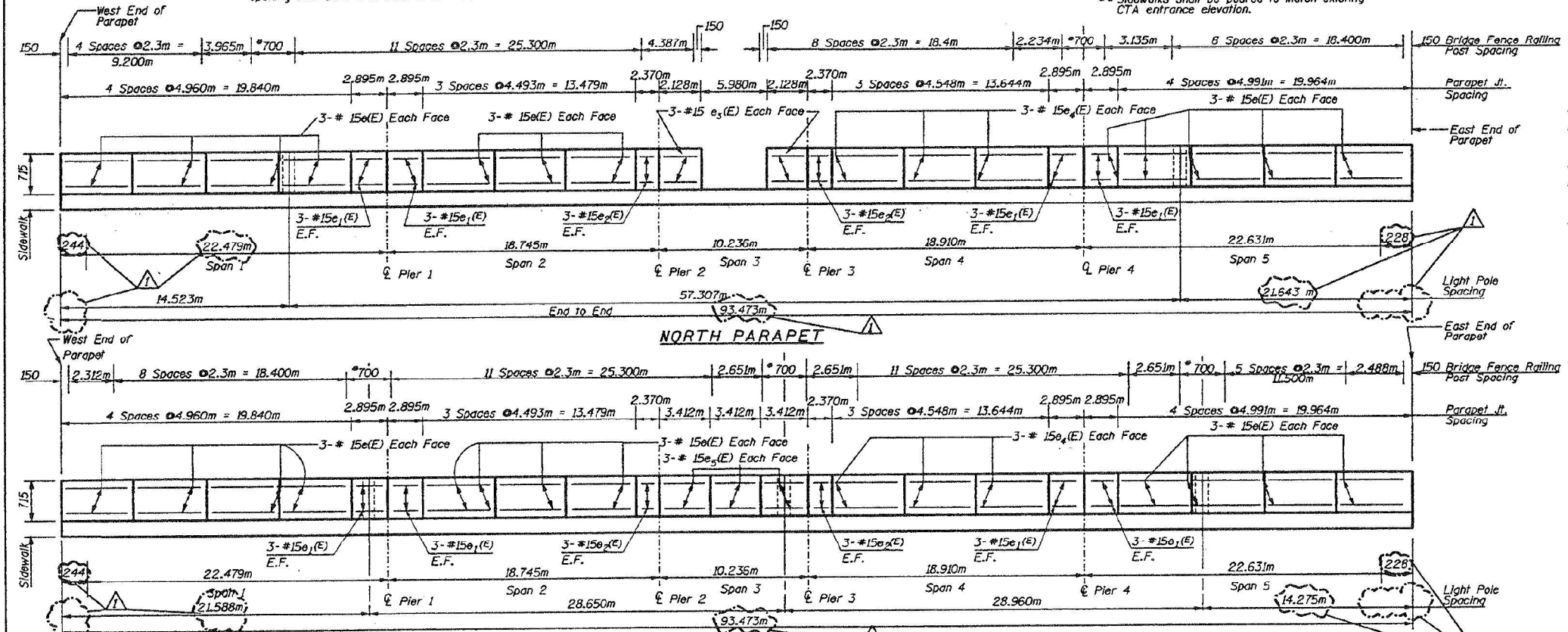
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90/94	FAI 90/94 22 BJ	COOK	83	47
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	34
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		Sheet 511 of 39

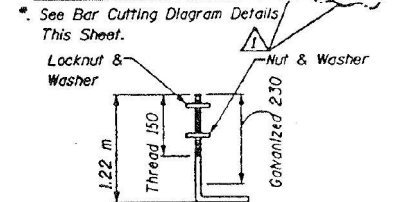
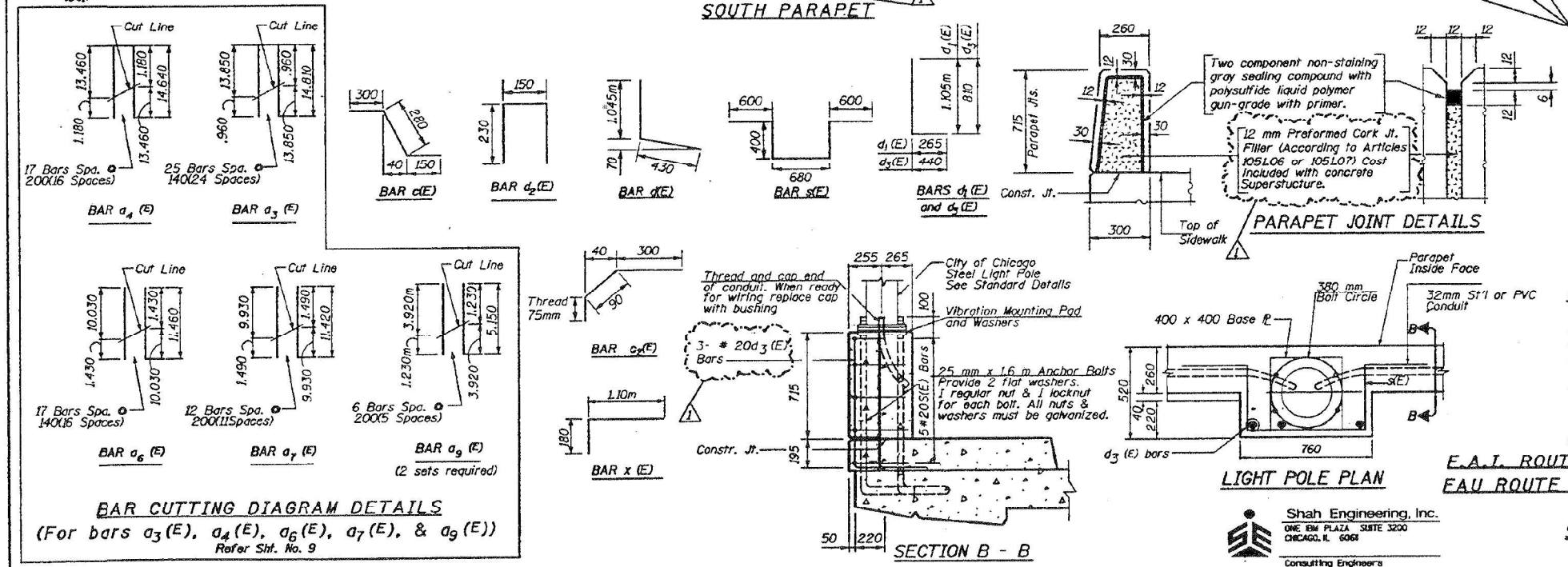
* Opening in Bridge Fence Railing to accommodate light standards. Provide end post on either side of opening with Bent L's. See Sheet #35.

** Sidewalks shall be poured to match existing CTA entrance elevation.



BILL OF MATERIAL

BAR	No.	Size	Length (m)	Shape
a ₁ (E)	1284	#15	7.290	—
a ₂ (E)	448	#15	8.180	—
a ₃ (E)	448	#15	6.400	—
a ₄ (E)	25	#15	14.81	—
a ₅ (E)	17	#15	14.64	—
a ₆ (E)	1102	#15	10.380	—
a ₇ (E)	17	#15	11.46	—
a ₈ (E)	12	#15	11.42	—
a ₉ (E)	1122	#15	4.20	—
a ₁₀ (E)	12	#15	5.15	—
a ₁₁ (E)	96	#15	1.200	—
a ₁₂ (E)	8	#20	7.05	△
a ₁₃ (E)	4	#20	10.65	—
a ₁₄ (E)	4	#20	4.30	—
a ₁₅ (E)	1020	#15	7.71	—
b ₁ (E)	1044	#15	8.320	—
b ₂ (E)	376	#20	10.40	△
c ₁ (E)	624	#15	0.730	—
c ₂ (E)	624	#15	2.470	—
c ₃ (E)	624	#15	0.465	—
c ₄ (E)	312	#15	1.120	—
c ₅ (E)	624	#15	1.54	△
d ₁ (E)	624	#20	1.370	—
d ₂ (E)	164	#15	0.610	—
d ₃ (E)	15	#20	1.25	—
e ₁ (E)	96	#15	4.860	—
e ₂ (E)	48	#15	2.795	—
e ₃ (E)	24	#15	2.270	—
e ₄ (E)	12	#15	2.028	—
e ₅ (E)	36	#15	4.448	—
e ₆ (E)	18	#15	3.312	—
f ₁ (E)	25	#20	2.68	—
x ₁ (E)	194	#15	1.280	—
Concrete Superstructure	m ³		836.3	
Reinforcement Bars Epoxy Coated	kg		94,660	
Protective Coat	m ²		2880	
Bridge Deck Grooving	m ²		2222	



Notes:
 1. Reinforcement Bars Designed (E) Shall Be Epoxy Coated.
 2. Work This Sheet With Sheet 9.
 3. For Bridge Fence Railing See Sheet 36. Cost of 25 mm Anchor Bolts Shall Be Included With Concrete Superstructures.
 4. For additional Light pole Location See Sheet 519.

PARAPET DETAILS
 47TH STREET OVER
 E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
 EAU ROUTE 1489 - SECTION 2122-907HB-R
 COOK COUNTY
 STATION 1+ 274.389
 SN. 016-1156

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 Consulting Engineers

Revision 7/16/99 MM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

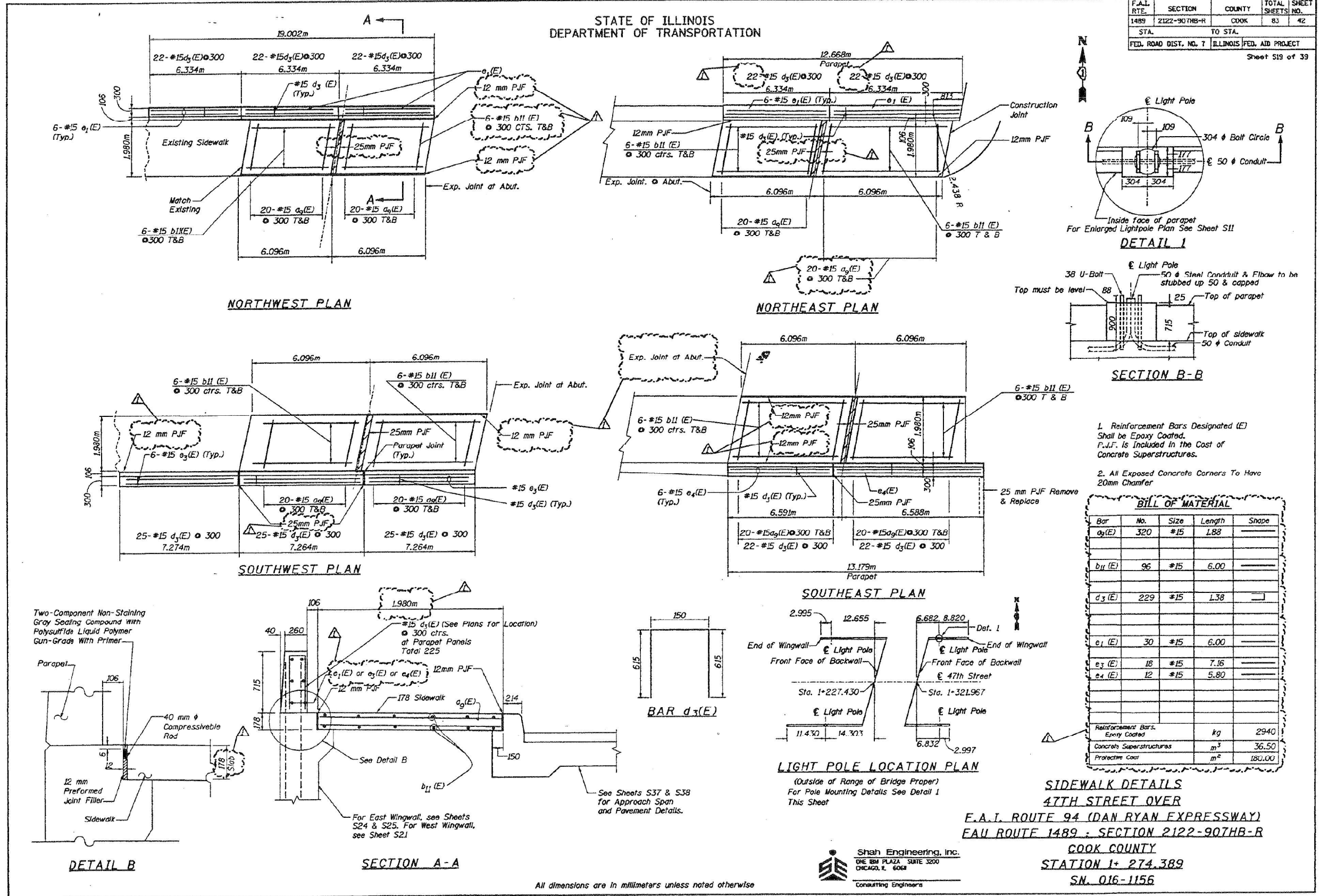
EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-31 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	48
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	42
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT		Sheet S19 of 39	



All dimensions are in millimeters unless noted otherwise

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Revision 7/16/99 MM

STATE OF ILLINOIS
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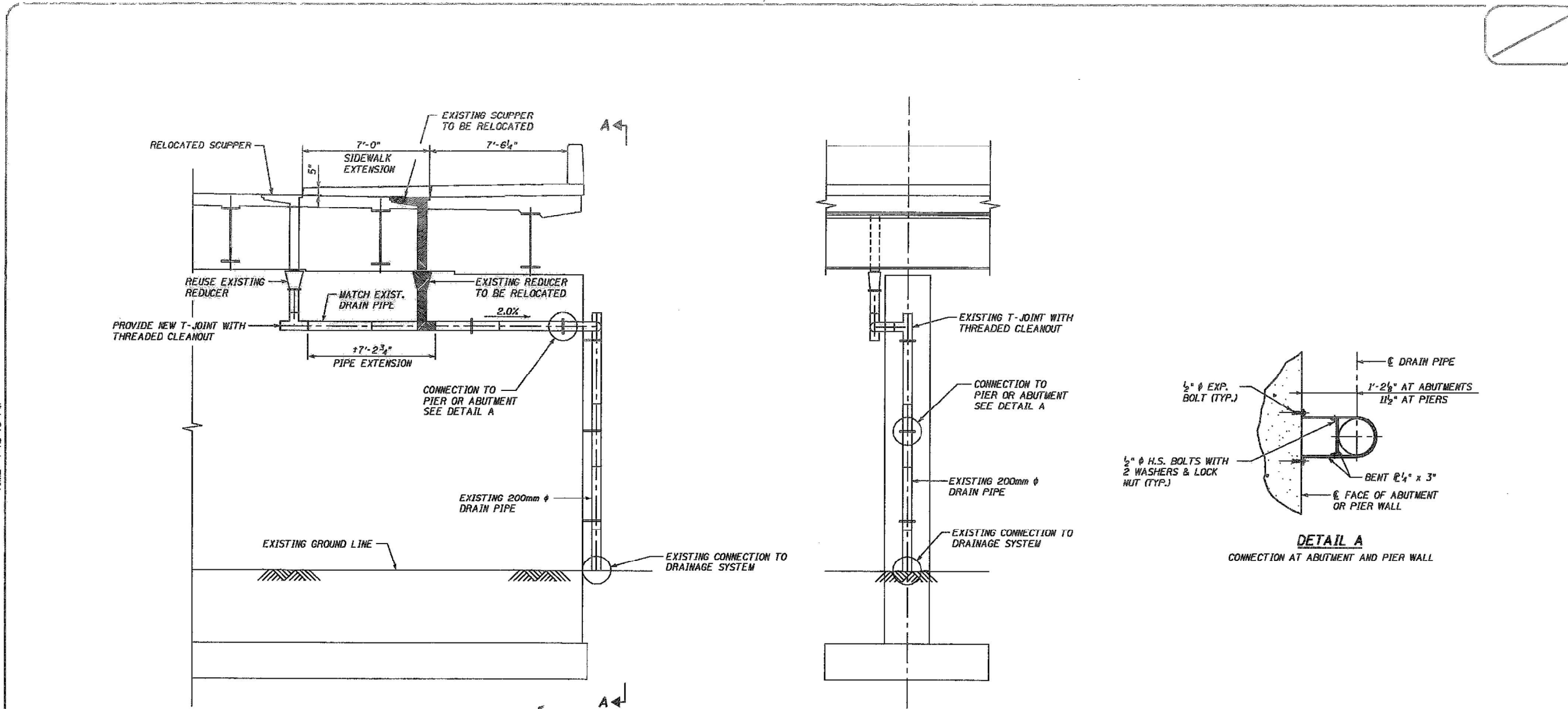
EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-32 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	49
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT			

Primera

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PLOT DATE =	\$\$DATE\$	DRAWN -	PWB	REVISED -	
		CHECKED -	KWY	REVISED -	



TYPICAL DRAIN PIPE MODIFICATION
(SCUPPER RELOCATION AT PIER SHOWN, ABUTMENTS SIMILAR)

VIEW A-A

DETAIL A
CONNECTION AT ABUTMENT AND PIER WALL

NOTE:
1. THE EXISTING DRAINAGE SYSTEM SHALL BE MODIFIED AS SHOWN IN THE PLANS TO ACCOMMODATE THE RELOCATED SCUPPER. THE DRAINAGE SYSTEM MODIFICATIONS SHALL FOLLOW THE REQUIREMENTS OF THE DRAINAGE SYSTEM SPECIAL PROVISION.

DATE: 3/23/2006
TIME: 4:48:46 PM

USER: evoyseman
FILE: g:\CH05\0019\Bridg\4-7\ndrain.dgn

In Charge: RJM
Designed By: CWE
Drawn By: CWE
Checked By: KMP
Approved By: RLP



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No.	Date	Revision	App'd
3	12/23/05	REVISED SCOPE	
2	07/08/05	ISSUED FOR DOT APPROVAL	
1	06/24/05	IN PROGRESS	



CTA RED LINE (DAN RYAN)
AESTHETIC IMPROVEMENTS
47TH STREET OVER I-94 (S.N. 016-1156)
DRAINAGE MODIFICATIONS

Sheet No.
S47S-712



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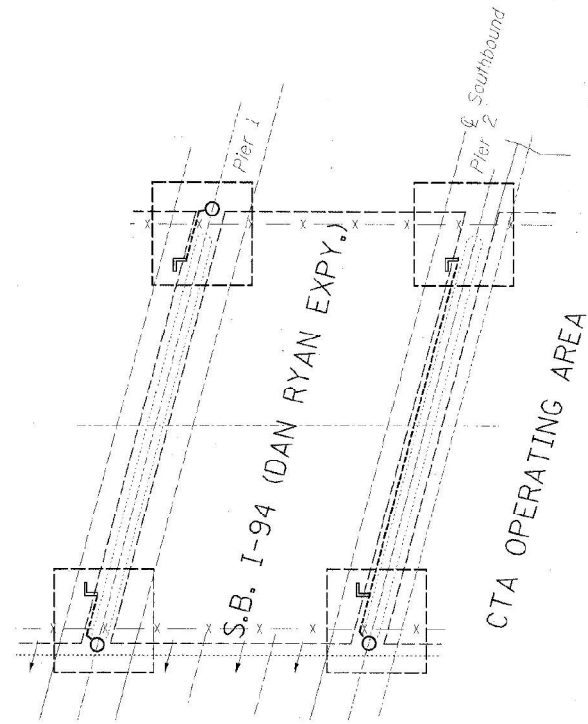
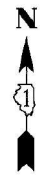
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

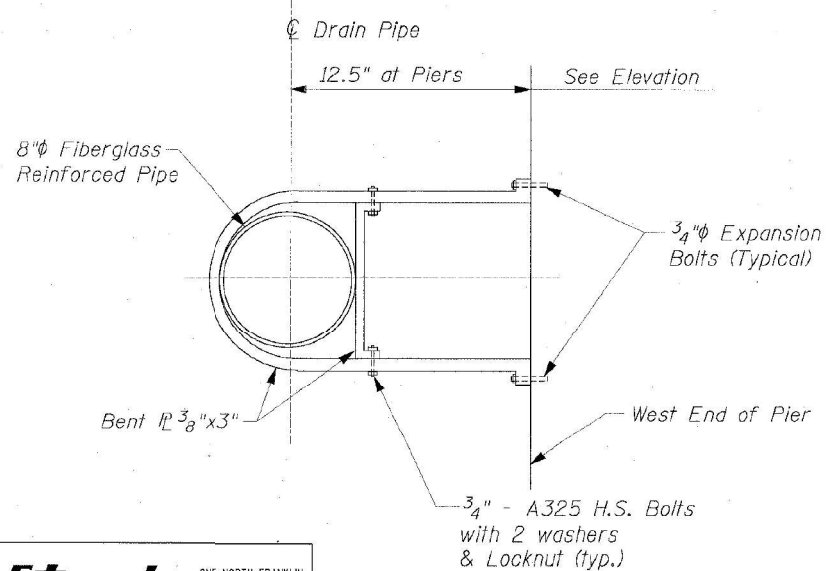
SHEET S-33 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	50
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94/90		COOK	598	226A
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
62302	• (1818, ETC, 2324.6-IPR-9			



PLAN

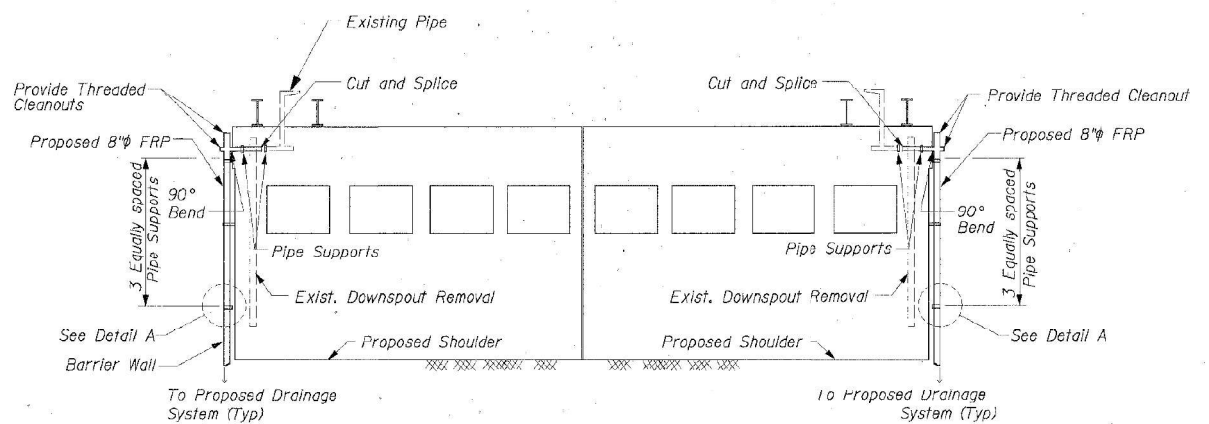


DETAIL A

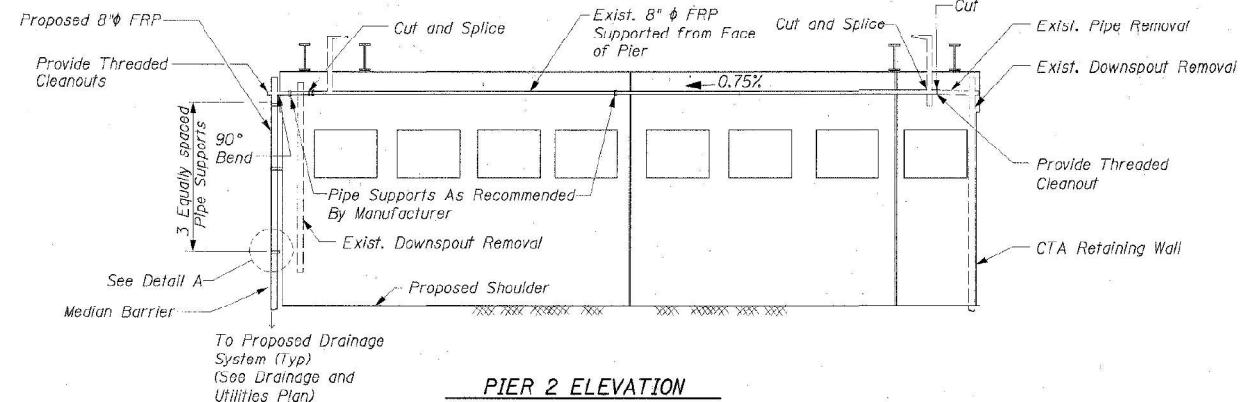
Edwards AND Kelcey
 ONE NORTH FRANKLIN
 CHICAGO, IL 60606
 PHONE: (312) 291-3000
 FAX: (312) 291-3015
 WEB: WWW.EKCORP.COM

NOTES:

1. This work paid for as 'Drainage System' Lump Sum.
2. All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 30.
3. Cost for Connection of Drainage Downspouts @ Piers to the proposed drainage structure is included with 'Drainage System'.
4. Cost for Supporting 8" ϕ FRP to be included with "Drainage System".



PIER 1 ELEVATION



PIER 2 ELEVATION

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94/90 (DAN RYAN EXPRESSWAY)
 31ST STREET TO 71ST STREET
 SB EXPRESS LANE RECONSTRUCTION
 47TH ST. BRIDGE DOWNSPOUT RELOCATIONS

SCALE: N.T.S. DRAWN BY: LPN
 DATE: 7/1/05 CHECKED BY: PJM

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PLOT DATE =	\$\$DATE\$	DRAWN -	PWB	REVISED -	
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156

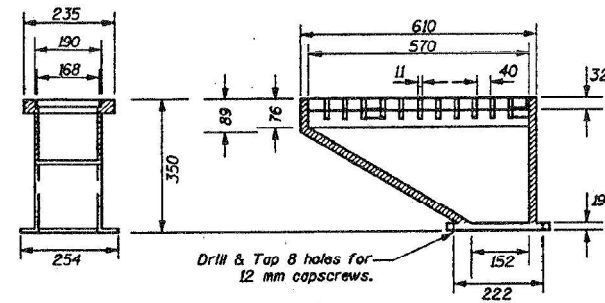
SHEET S-34 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	51
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

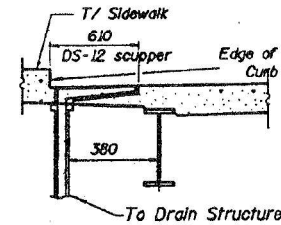
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	52
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

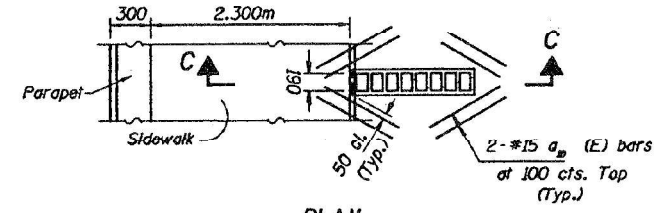
Sheet 535 of 39



DS-12 SCUPPER DETAILS



SECTION C-C



PLAN

Cut longitudinal reinforcement to clear drainage scuppers.

Notes:
All cast iron parts shall be gray iron conforming to the requirements of AASHTO M 105, Class 30.
Bolts, washers and nuts shall be galvanized according to AASHTO M 232.
Cost of the Main Grate, Cleanout Grate, Frame, Downspout, Bolts, Washers, Nuts including complete installation of Scupper shall be paid for at the unit bid price each for "DRAINAGE SCUPPERS".
All dimensions are in millimeters unless noted otherwise.

STEEL DRAINAGE SCUPPER DETAILS
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
ONE EM PLAZA SUITE 3200
CHICAGO, IL 60611
Consulting Engineers

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-35 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	52
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

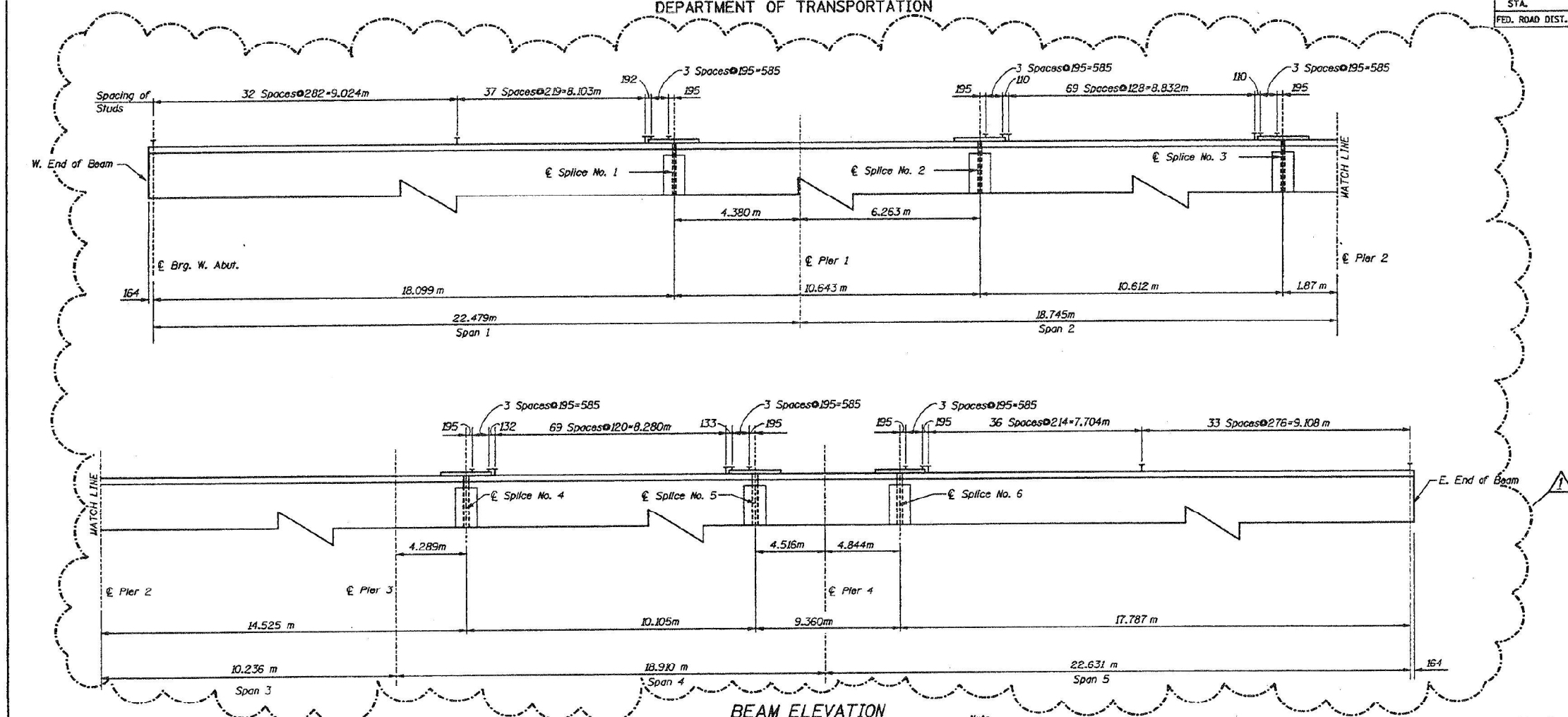
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Primera
100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 - P.312-405-8900 F.312-406-4105

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PLOT DATE =	\$\$DATE\$	DRAWN -	PWB	REVISED -	
		CHECKED -	KWY	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
38	2122-907HB-R	COOK	83	37
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				
Sheet 514 of 39				



**BEAM ELEVATION
(SPACING OF STUDS)**

**MOMENT TABLE - Symmetrical Composite 5 Span
(Composite in positive moment area only)**

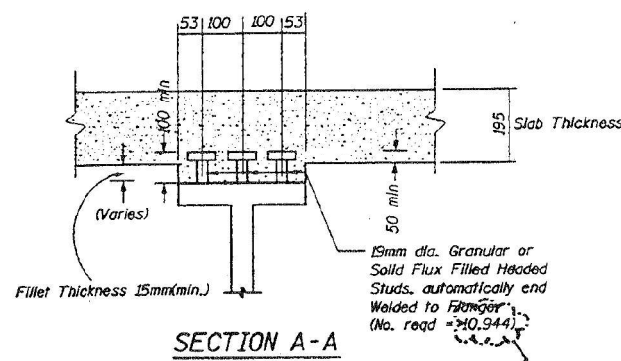
	0.4 Sp. #1 or 0.6 Sp. #5	Pier 1 or Pier 4	0.6 Sp. #2 or 0.4 Sp. #4	Pier 2 or Pier 3	0.5 Sp. #3
I_s (10^6 mm^4)	4312	4312	4312	4312	4312
I_c (n) (10^6 mm^4)	10980	-	10980	-	-
I_c (sn) (10^6 mm^4)	8108	-	8108	-	-
S_s (10^3 mm^3)	9400	9400	9400	9400	9400
S_c (n) (10^3 mm^3)	13732	-	13732	-	-
S_c (sn) (10^3 mm^3)	12459	-	12459	-	-
W (kN/m)	14.59	24.37	14.59	24.37	14.59
M_E (kN-m)	597	1142	202	334	-13
S_E (kN-m)	9.78	-	9.78	-	9.78
M_{SE} (kN-m)	438	-	187	-	2
M_L (kN-m)	901	468	568	304	222
M (Imp) (kN-m)	225	122	148	90	66
$S_2[M_L + M(\text{Imp})]$ (kN-m)	1877	983	1893	657	480
M_a (kN-m)	3785	2763	2057	1288	610
f_s non-comp (MPa)	63	121	21	36	-1
f_s comp (MPa)	35	-	15	-	-
f_s^s (k) (MPa)	137	105	87	70	35
f_s^s (t) (MPa)	235	226	123	106	34
f_s (Overload) (MPa)	306	294	160	137	44
f_s (Total) (MPa)	273	-	294	-	272

Note:
For splice plate detail see sheet #S13.
For stud spacing on flange plate, see sheet #S13.

	W. Abut.	Pier 1 or Pier 2 or Pier 4	Pier 3	E. Abut.
R_E & R_{SE} (kN)	214	580	300	215
R_L (kN)	221	276	256	222
$Imp.$ (kN)	51	60	66	51
R (Total) (kN)	486	916	622	488

I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s (Total & Overload).
 I_{cs} and S_{cs} are the moment of inertia and section modulus of the composite section used in computing stresses due to Live Load.
 I_{cs} and S_{cs} are the moment of inertia and section modulus of the composite section used in computing stresses due to superimposed dead loads. (see AASHTO 10.38)
 VR is the maximum Live Load + Impact shear range in span.

M_a (Applied Moment) = $1.3(M_R + M_S + S_2(M_L + M_{Imp}))$.
The Plastic Moment capacity (M_u) is computed according to AASHTO 10.48.1 and 10.50.11.
 f_s (Overload) is the sum of the stresses due to $M_E + M_{SE} + S_2(M_L + M)$.
 f_s (Total) (Non-compact section) is the sum of the stresses due to $1.3(M_R + M_S + S_2(M_L + M_{Imp}))$.



SECTION A-A

**BEAM DETAILS 1
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
FAU ROUTE 1489 : SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SM. 016-1156**

Shah Engineering, Inc.
ONE BIN PLAZA SUITE 3000
CHICAGO, IL 60611
Consulting Engineers

Revision 7/14/99 MM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

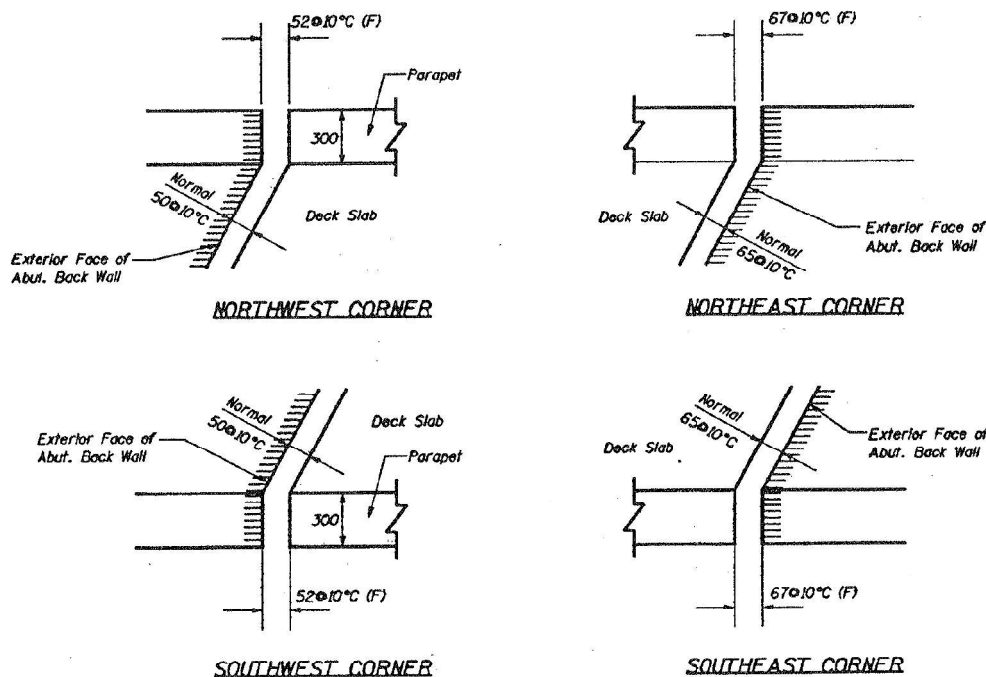
SHEET S-36 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	53
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	38
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

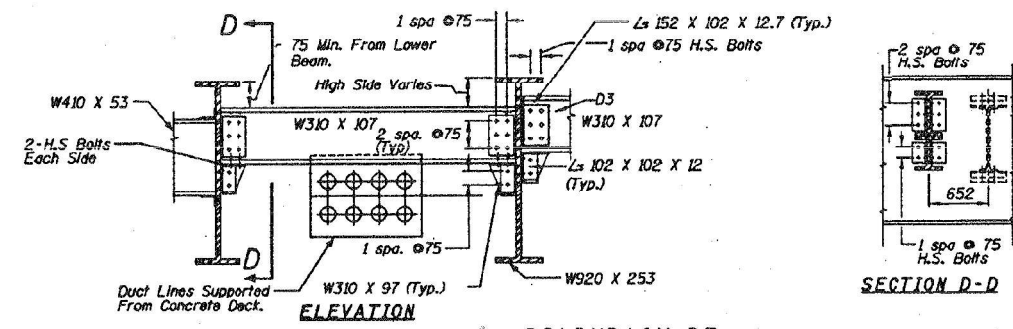
Sheet 315 of 39



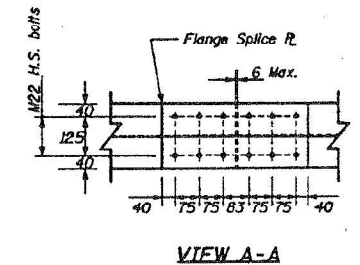
DECK SLAB TREATMENT
AT END CORNERS
(Reference Dwg. No. S9)

TOP OF BEAM ELEVATIONS (For Blocking Diagram)

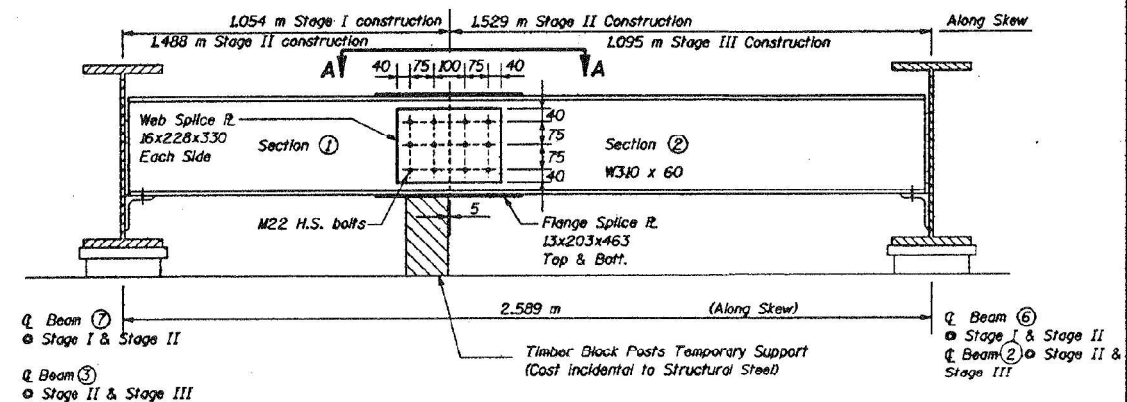
BM	1	2	3	4	5	6	7	8	9	10	11	12
W. Abut.	181.967	182.009	182.051	182.088	182.117	182.146	182.137	182.091	182.044	181.989	181.929	181.869
Splice #1	181.946	181.963	181.990	182.053	182.087	182.097	182.118	182.077	182.036	181.987	181.933	181.879
Pier 1	181.903	181.952	181.975	182.044	182.080	182.085	182.113	182.074	182.034	181.981	181.934	181.881
Splice #2	181.886	181.936	181.954	182.032	182.053	182.068	182.089	182.051	182.013	181.967	181.916	181.864
Splice #3	181.856	181.909	181.918	182.011	182.007	182.040	182.048	182.013	181.977	181.933	181.884	181.836
Pier 2	181.850	181.904	181.911	182.007	181.999	182.035	182.041	182.006	181.970	181.927	181.879	181.831
Pier 3	181.767	181.819	181.846	181.926	181.969	181.964	181.963	181.928	181.916	181.852	181.804	181.757
Splice #4	181.727	181.787	181.821	181.895	181.937	181.936	181.932	181.898	181.881	181.823	181.776	181.729
Splice #5	181.633	181.712	181.762	181.820	181.864	181.870	181.860	181.828	181.800	181.755	181.709	181.663
Pier 4	181.592	181.678	181.735	181.787	181.831	181.840	181.828	181.796	181.763	181.724	181.679	181.634
Splice #6	181.547	181.627	181.684	181.736	181.780	181.796	181.789	181.757	181.724	181.686	181.641	181.596
E. Abut.	181.382	181.438	181.495	181.547	181.591	181.636	181.643	181.612	181.581	181.544	181.501	181.457



DIAPHRAGM D3
(45 Req'd)



VIEW A-A



DIAPHRAGM D1A
34 Required (Looking West)

DIAPHRAGM D1A CONSTRUCTION SEQUENCE (Similar for Beams 2 & 3) @ Stage II & Stage III

- 1.) Order Diaphragm D1A.
- 2.) Attach section 1 to Beam 7 along with top flange splice R.
- 3.) Place Timber Block Posts between section 1 of diaphragm and abutment bearing seat.
- 4.) Attach section 2 of diaphragm to section 1 and Beam 6 during Stage II Construction.
- 5.) Attach web splice plates to sections 1 and 2 of diaphragms.
- 6.) Remove Timber Block Posts.
- 7.) Attach bottom flange splice plate to sections 1 and 2 of diaphragms.

BEAM DETAILS 2

47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 ; SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
ONE BNA PLAZA SUITE 3200
CHICAGO, IL 60604
Consulting Engineers

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-37 OF S-52 SHEETS

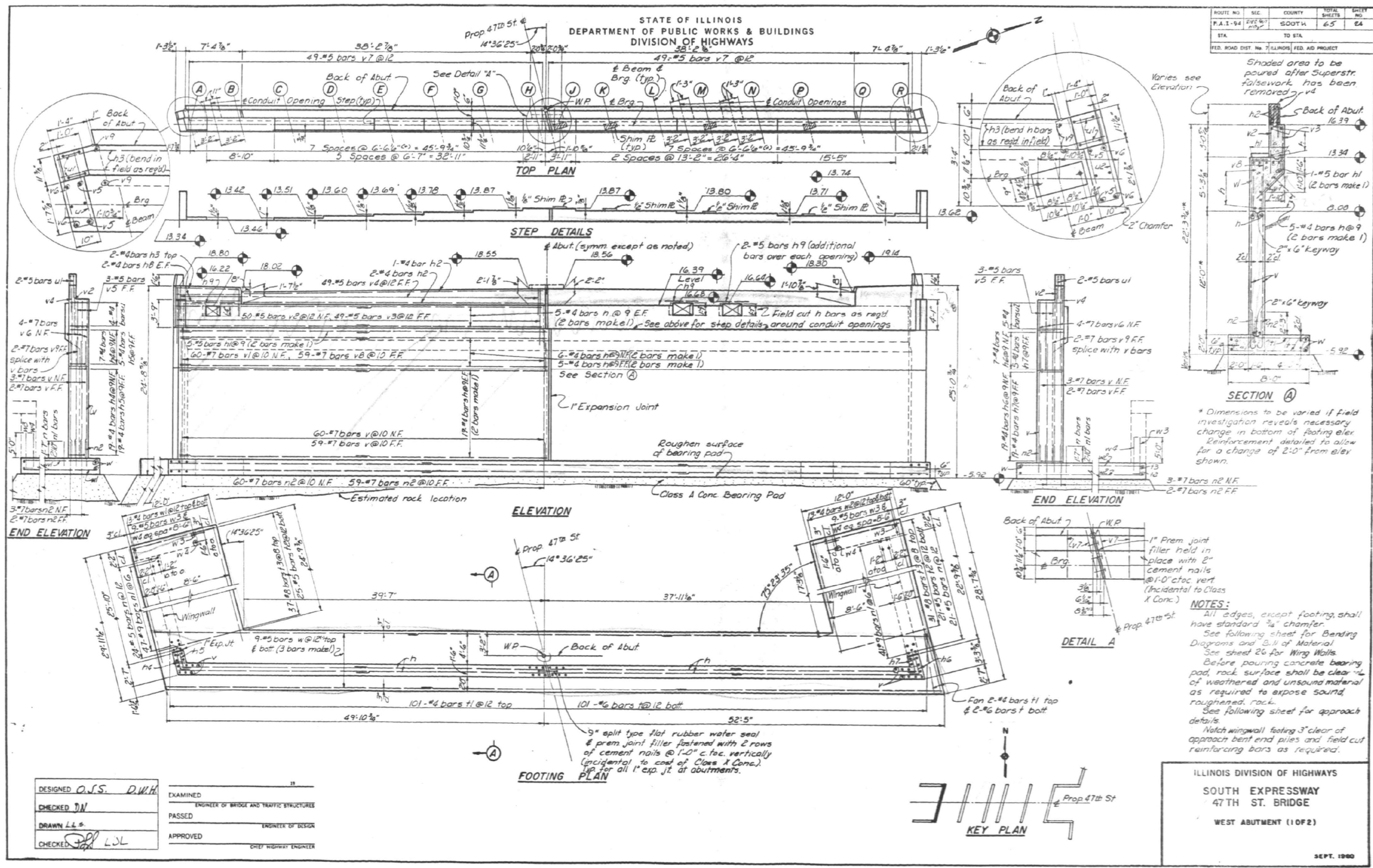
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90/94	FAI 90/94 22 BJ	COOK	83	54
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



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ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 94	22 BJ	COOK	83	55
STA.	TO STA.			
FED. ROAD DIST. No. 7	ILLINOIS	FED. AID PROJECT		



DESIGNED <i>O.S.S. D.W.H.</i>	EXAMINED
CHECKED <i>D.W.</i>	ENGINEER OF BRIDGE AND TRAFFIC STRUCTURES
DRAWN <i>L.L.S.</i>	PASSED
CHECKED <i>L.L.S.</i>	ENGINEER OF DESIGN
	APPROVED
	CHIEF HIGHWAY ENGINEER

NOTES:
 All edges, except footing, shall have standard $\frac{3}{8}$ " chamfer.
 See following sheet for Bending Diagrams and B.I.I of Material.
 See sheet 26 for Wing Walls.
 Before pouring concrete bearing pad, rock surface shall be clear of weathered and unsound material as required to expose sound, roughened, rock.
 See following sheet for approach details.
 Watch wingwall footing 3' clear of approach bent end piles and field cut reinforcing bars as required.

ILLINOIS DIVISION OF HIGHWAYS
 SOUTH EXPRESSWAY
 47TH ST. BRIDGE
 WEST ABUTMENT (10F2)
 SEPT. 1980



USER NAME =	SUSERS	DESIGNED -	KWY	REVISED -	
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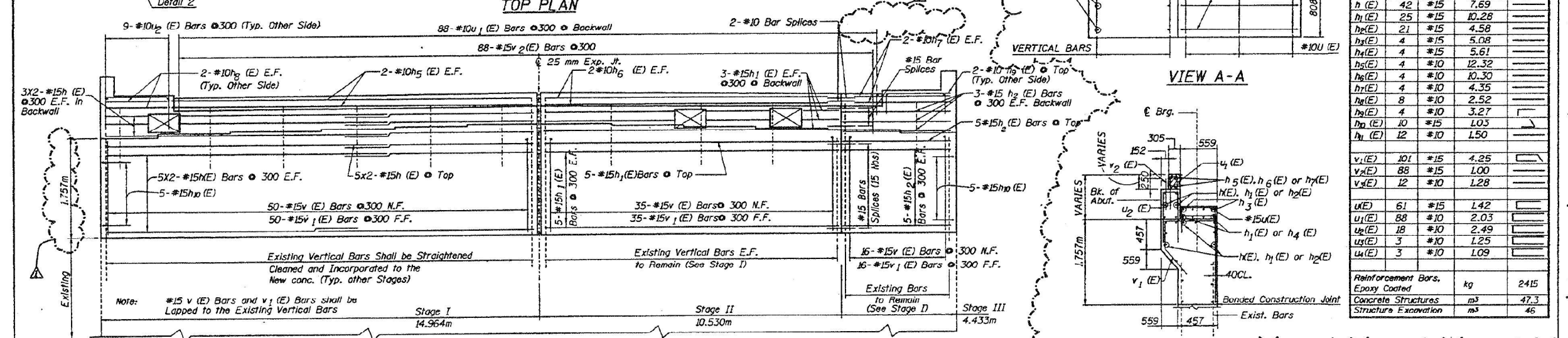
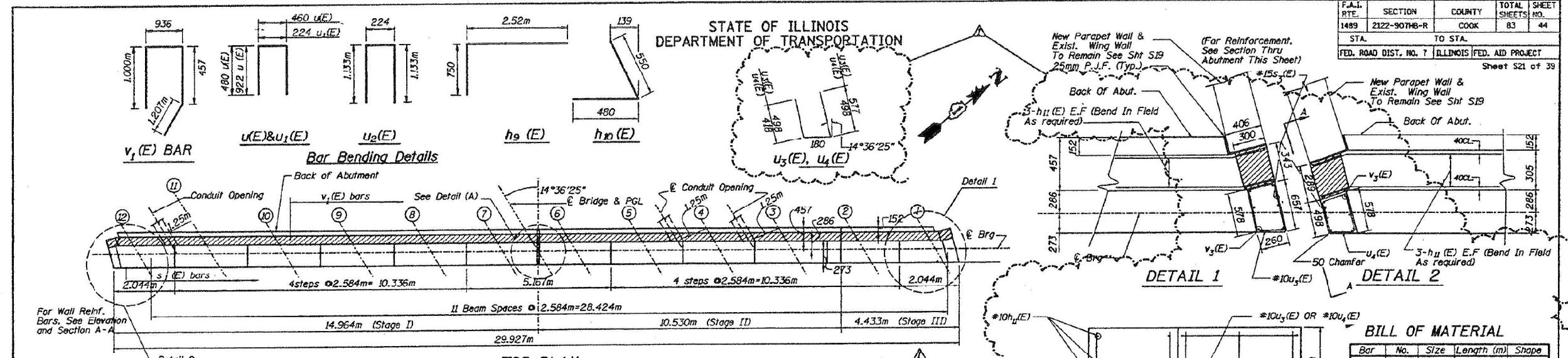
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
 STRUCTURE NO. 016-1156
 SHEET S-38 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	55
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

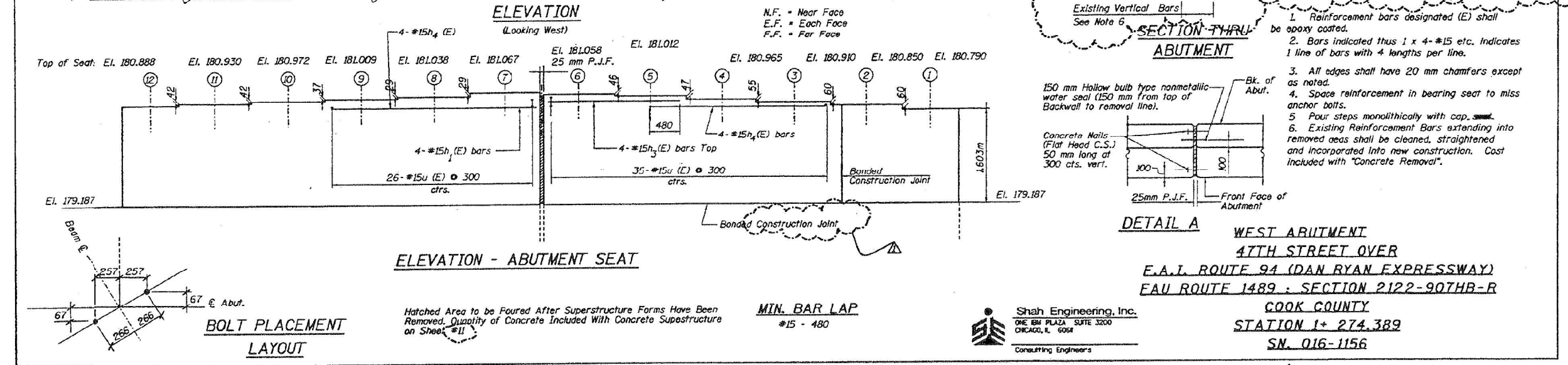
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	44
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT		Sheet S21 of 39	



BILL OF MATERIAL

Bar	No.	Size	Length (m)	Shape
h (E)	42	#15	7.69	
h1 (E)	25	#15	10.28	
h2 (E)	21	#15	4.58	
h3 (E)	4	#15	5.08	
h4 (E)	4	#15	5.61	
h5 (E)	4	#10	12.32	
h6 (E)	4	#10	10.30	
h7 (E)	4	#10	4.35	
h8 (E)	8	#10	2.52	
h9 (E)	4	#10	3.27	
h10 (E)	10	#15	1.03	
h11 (E)	12	#10	1.50	
v1 (E)	101	#15	4.25	
v2 (E)	88	#15	1.00	
v3 (E)	12	#10	1.28	
u1 (E)	61	#15	1.42	
u2 (E)	88	#10	2.03	
u3 (E)	18	#10	2.49	
u4 (E)	3	#10	1.25	
u5 (E)	3	#10	1.09	
Reinforcement Bars, Epoxy Coated		kg	2415	
Concrete Structures		m ³	47.3	
Structure Excavation		m ³	46	



- SECTION THRU ABUTMENT**
- Reinforcement bars designated (E) shall be epoxy coated.
 - Bars indicated thus 1 x 4-#15 etc. Indicates 1 line of bars with 4 lengths per line.
 - All edges shall have 20 mm chamfers except as noted.
 - Space reinforcement in bearing seat to miss anchor bolts.
 - Four steps monolithically with cap.
 - Existing Reinforcement Bars extending into removed areas shall be cleaned, straightened and incorporated into new construction. Cost included with "Concrete Removal".

DETAIL A WEST ABUTMENT
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
 ONE EMB PLAZA SUITE 3200
 CHICAGO, IL 60604
 Consulting Engineers

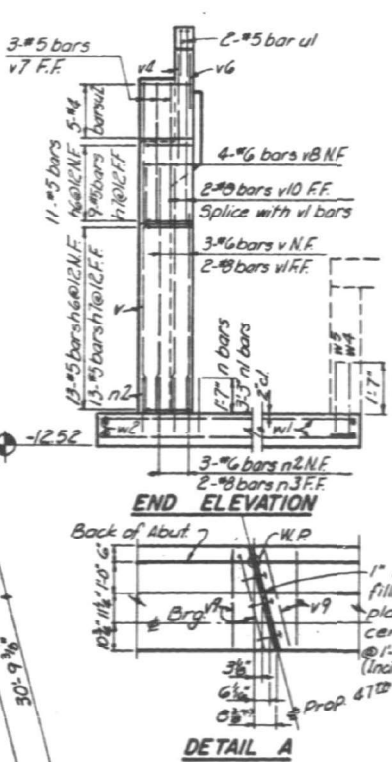
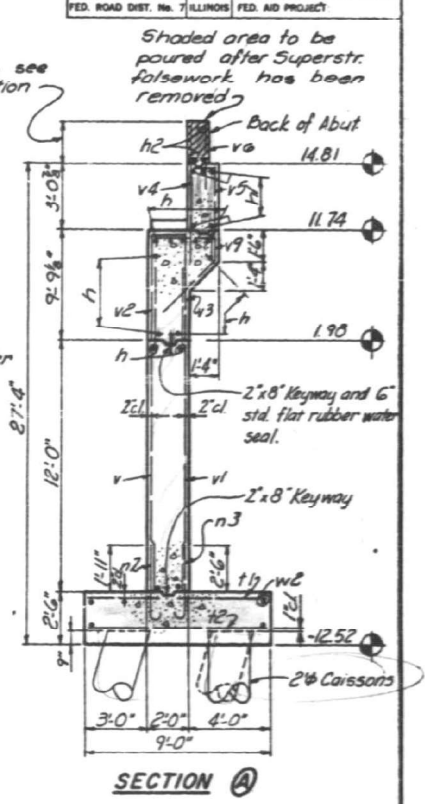
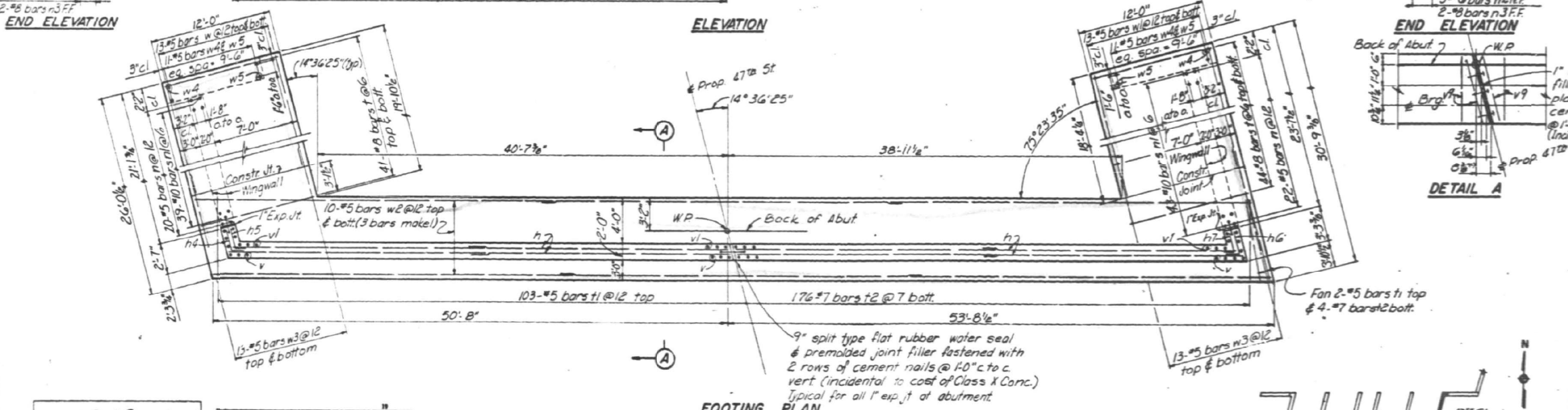
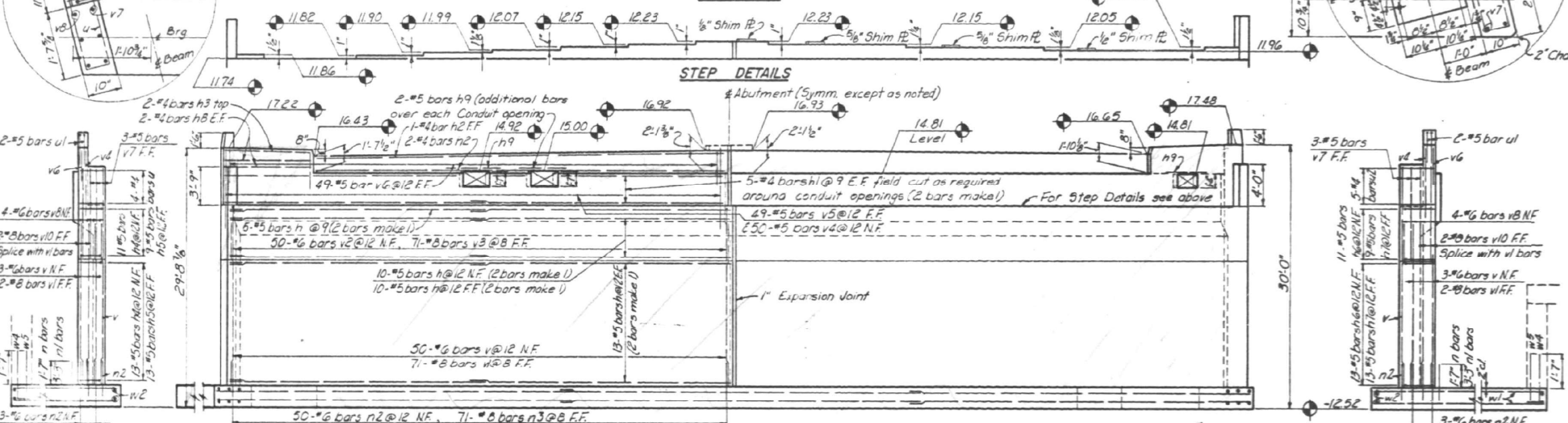
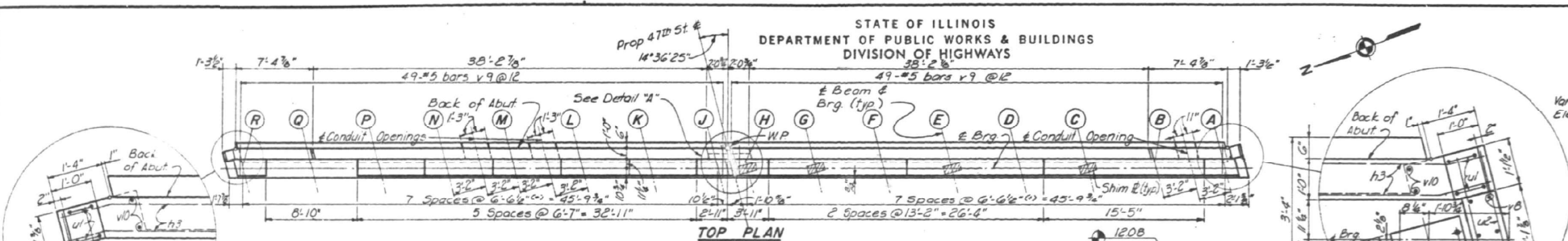
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 100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 - 312-405-8919 F311-006-0105
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STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SEC.	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.I. 94	641-67	COOK	65	29
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



NOTES:
All edges, except footing, shall have standard 3/8" chamfer.
See following sheet for Bending Diagrams and Bill of Material.
See sheet 31 for Wing Walls.
See sheet 19 for Coisson Layout and Details.
See sheets 6 & 7 for Construction Staging and Sheet Pile Layout.
Notch wingwall footing 3" clear of approach bent end piles and field cut reinforcing bars as required.

DESIGNED	O.J.S. D.W.H.	EXAMINED	
CHECKED	D.N.	PASSED	
DRAWN	L.L.S.	APPROVED	
CHECKED	L.J.L.		



ILLINOIS DIVISION OF HIGHWAYS
SOUTH EXPRESSWAY
47TH ST. BRIDGE
EAST ABUTMENT (1 OF 2)
SEPT. 1960

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PLOT DATE =	\$\$DATE\$	DRAWN -	PWB	REVISED -	
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

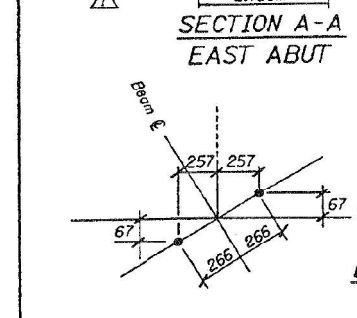
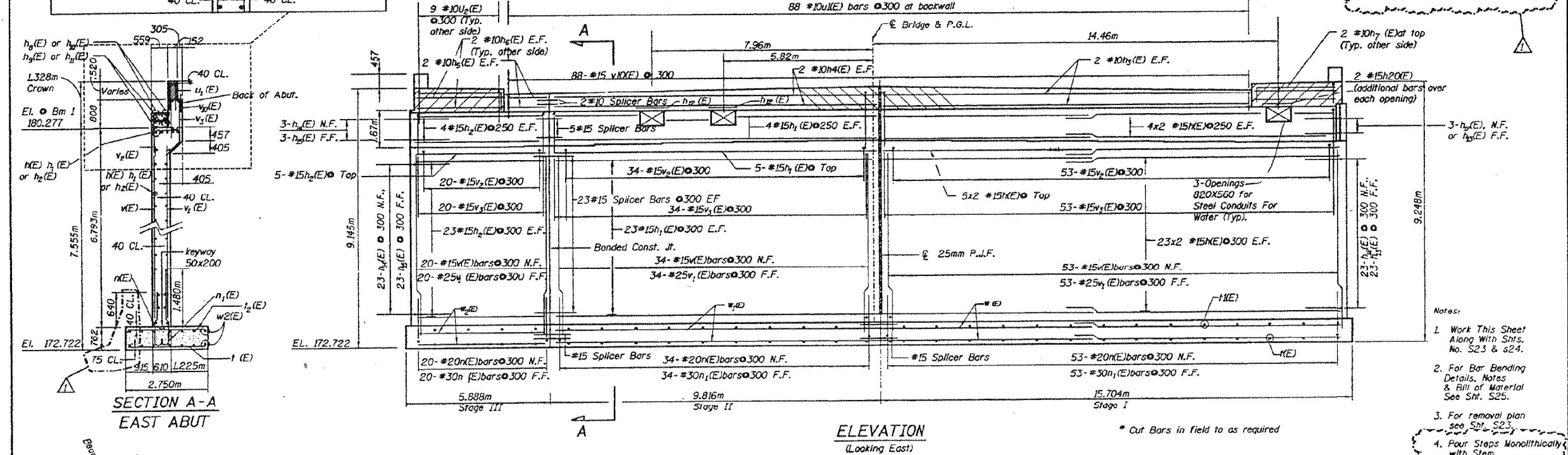
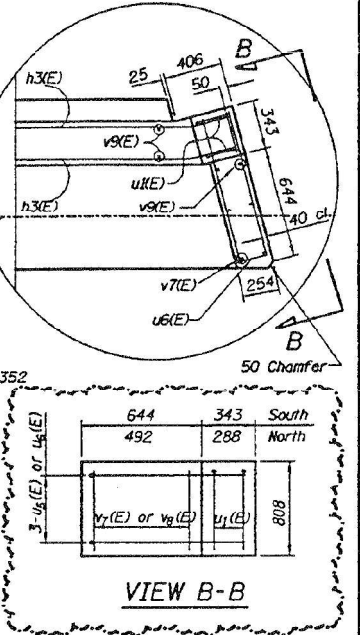
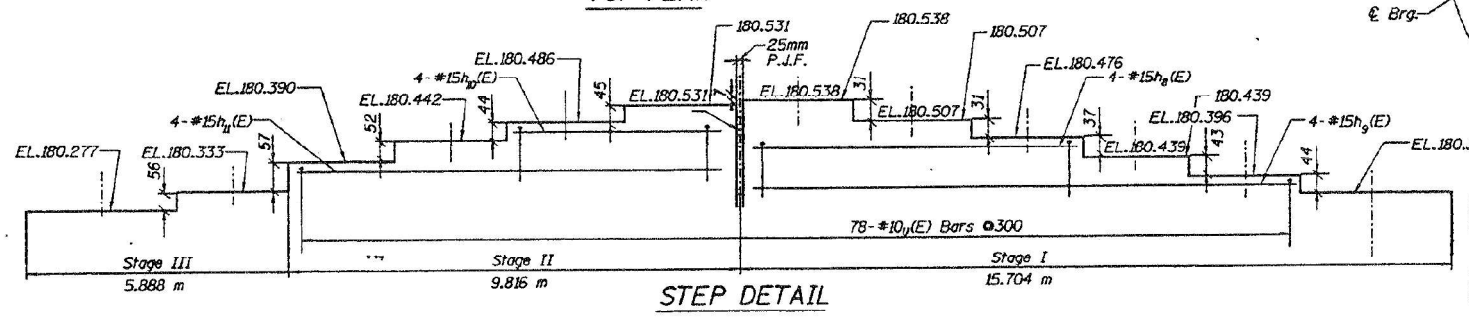
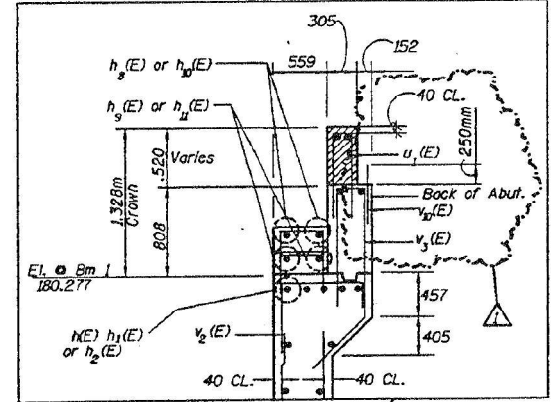
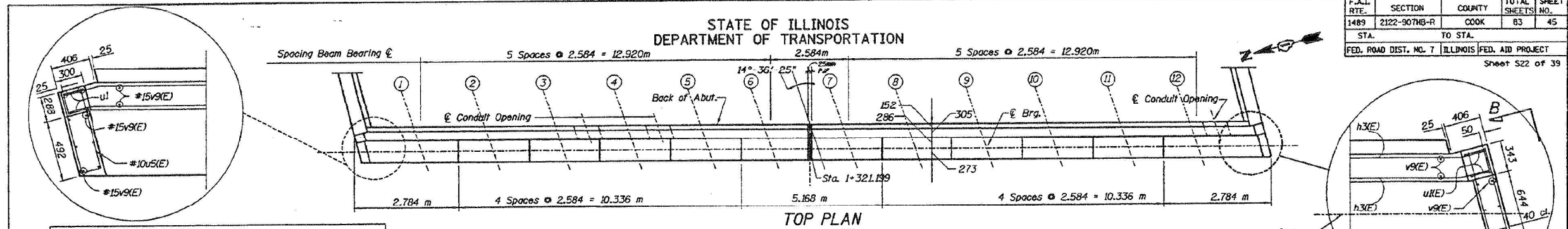
SHEET S-40 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	57
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HR-R	COOK	83	45
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				

Sheet S22 of 39



- Notes:
1. Work This Sheet Along With Shts. No. S23 & S24.
 2. For Bar Bending Details, Notes & Bill of Material See Sht. S25.
 3. For removal plan see Sht. S23.
 4. Pour Steps Monolithically with Stem.

EAST ABUTMENT
47TH STREET OVER
E.A.T. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HR-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
ONE BAY PLAZA SUITE 3200
CHICAGO, IL 60604
Consulting Engineers

Revision 7/16/99 MM

06-18-99

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-41 OF S-52 SHEETS

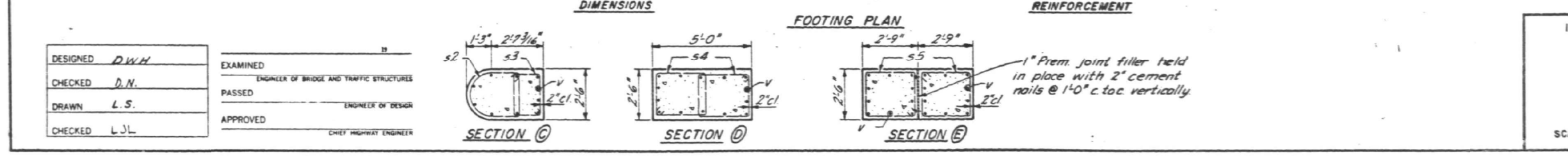
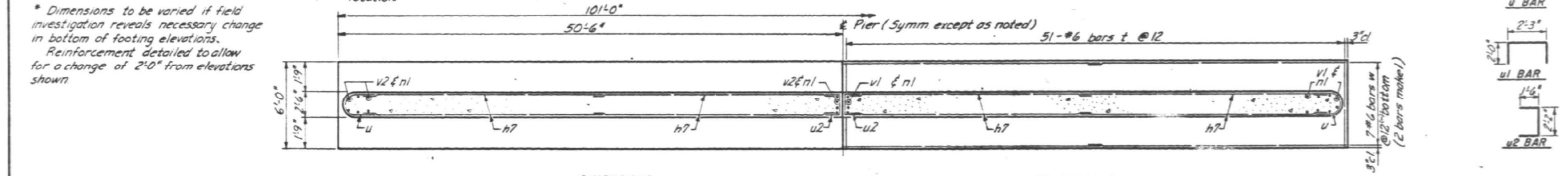
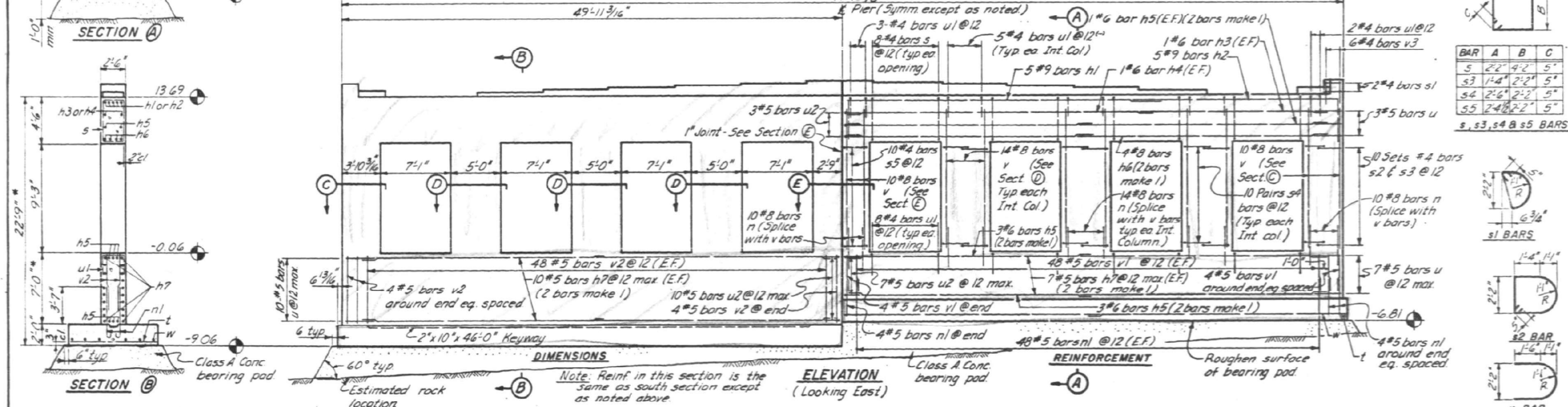
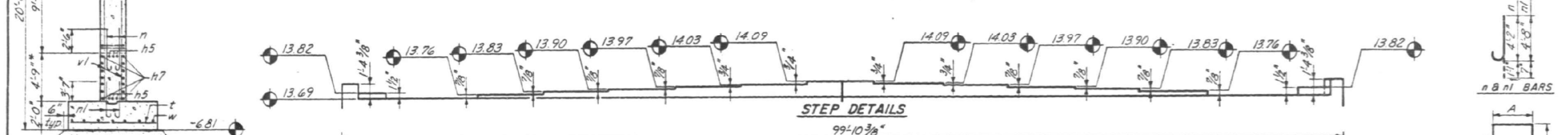
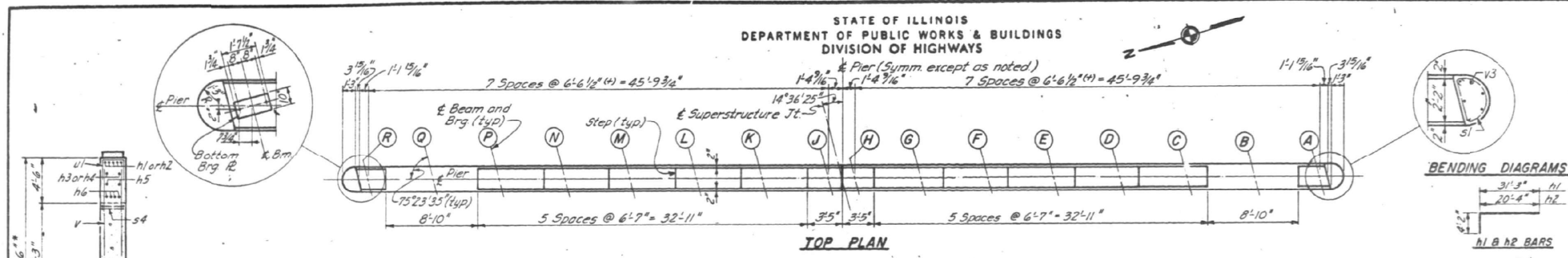
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90/94	FAI 90/94 22 BJ	COOK	83	58
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SEC.	EXPRESSWAY	TOTAL SHEETS	SHEET NO.
FAI-94	22 BJ	SOUTH	65	20
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS		FED. AID PROJ.	

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1	10	#9	35'-5"	
h2	10	#9	24'-6"	
h3	4	#6	18'-0"	
h4	4	#6	31'-0"	
h5	32	#6	25'-0"	
h6	16	#8	25'-3"	
h7	68	#5	24'-10"	
n	124	#8	5'-3"	
n1	208	#5	5'-3"	
s	64	#4	13'-6"	
s1	4	#4	6'-3"	
s2	20	#4	9'-1"	
s3	20	#4	7'-10"	
s4	120	#4	10'-2"	
s5	20	#4	9'-11"	
t	102	#6	5'-6"	
u	23	#5	6'-5"	
u1	104	#4	6'-3"	
u2	23	#5	5'-2"	
v	124	#8	12'-6"	
v1	104	#5	4'-6"	
v2	104	#5	6'-9"	
v3	12	#4	2'-6"	
w	28	#6	25'-9"	
CLASS X CONCRETE		CU YDS	178.5	
REINFORCEMENT BARS		LBS	19,039	
CL A EXCAV FOR STRUCT		CU YDS	262	
ROCK EXCAV FOR STRUCT		CU YDS	19	
CLASS A CONCRETE		CU YDS	83.7	



DESIGNED	DWH	EXAMINED	
CHECKED	D.N.	PASSED	
DRAWN	L.S.	APPROVED	
CHECKED	L.J.L.		

NOTES:
All edges, except footing, shall have standard 3/8" chamfer.
Before pouring concrete bearing pad, rock surface shall be cleared of weathered and unsound material as required to expose sound, roughened, rock.

ILLINOIS DIVISION OF HIGHWAYS
SOUTH EXPRESSWAY
47TH ST. BRIDGE
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

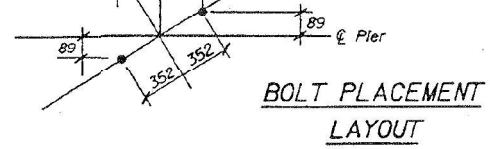
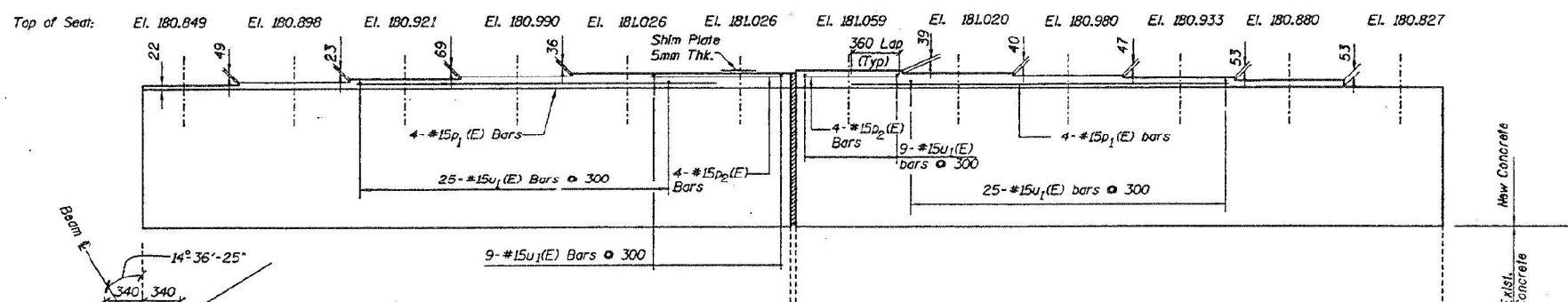
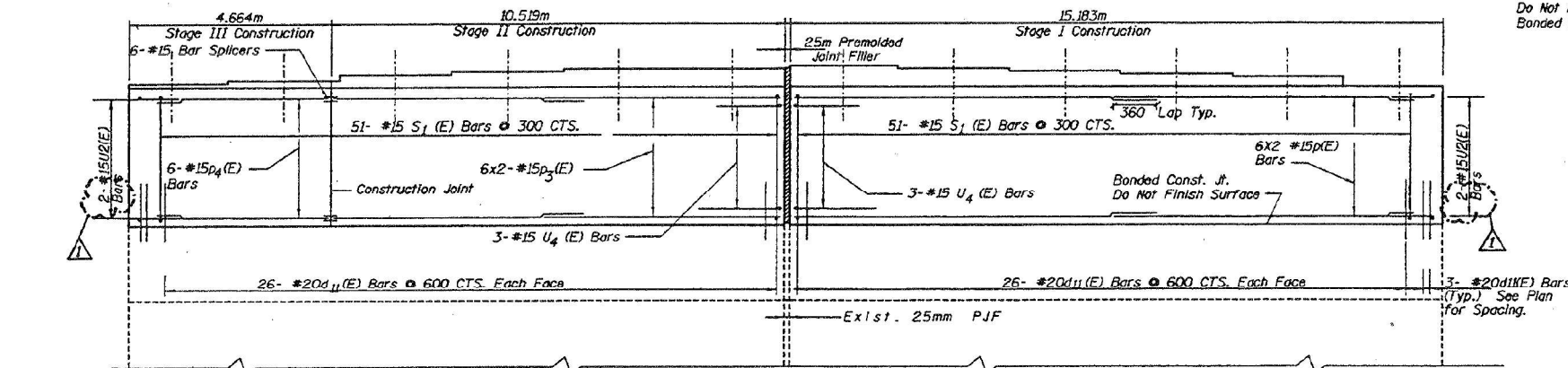
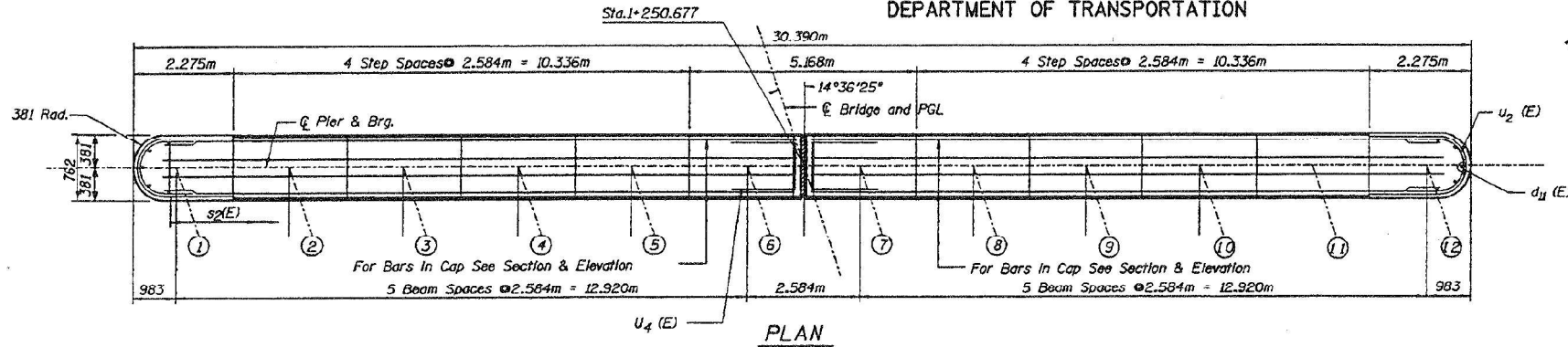
EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156
SHEET S-42 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	59
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

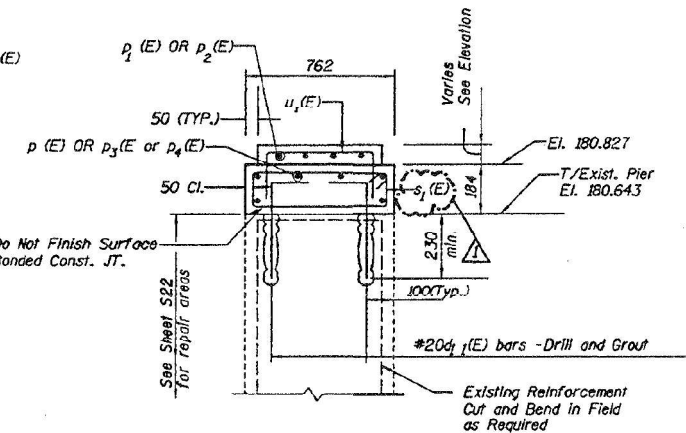
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1489	2122-907HB-R	COOK	83	50
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT			

Sheet S27 of 39



Notes:
Existing vertical reinforcement bars encountered during concrete removal shall be cleaned and straightened for incorporation into the new construction. Drill and grout #20 d11 (E) bars in 30 mm diameter drilled holes with a min. depth of 230 mm according to Art. 584 of the Std. Specs.
Space reinforcement in cap to miss anchor bolts.
All edges shall have 20 mm chamfers except as noted.
Four steps monolithically with cap.

All dimensions are in millimeters unless noted otherwise.



BILL OF MATERIALS				
Bar	No.	Size	Length (m)	Shape
d ₁₁ (E)	110	#20	0.70	L
p(E)	12	#15	7.65	—
p ₁ (E)	8	#15	8.23	—
p ₂ (E)	8	#15	2.50	—
p ₃ (E)	12	#15	5.50	—
p ₄ (E)	6	#15	4.20	—
s ₁ (E)	102	#15	1.77	—
u ₁ (E)	68	#15	1.44	U
u ₂ (E)	4	#15	2.31	U
u ₄ (E)	6	#15	1.94	U
Concrete Structures			m ³	7.0
Reinforcement Bars, Epoxy Coated			kg	1042

Reinforcement Bars, designated (E) shall be Epoxy coated.
Bars indicated thus 1 x 4-#15 etc. indicates 1 line of bars with 4 lengths per line.

PIER 1
47TH STREET OVER
F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
ONE EMB PLAZA SUITE 3200
CHICAGO, IL 60601
Consulting Engineers

Revision 7/15/99 MW

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CHECKED -	OPB	REVISIONS -			
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

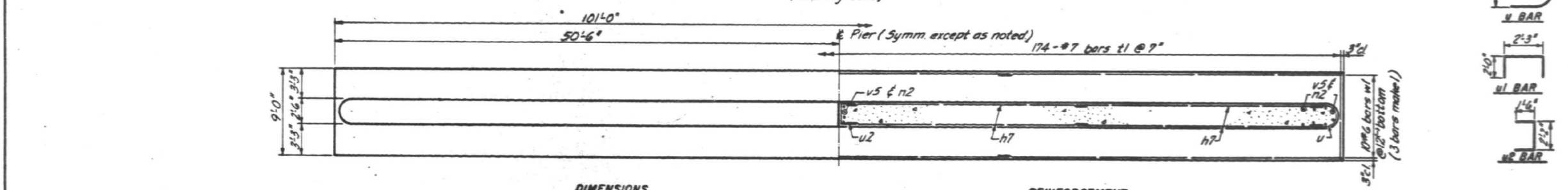
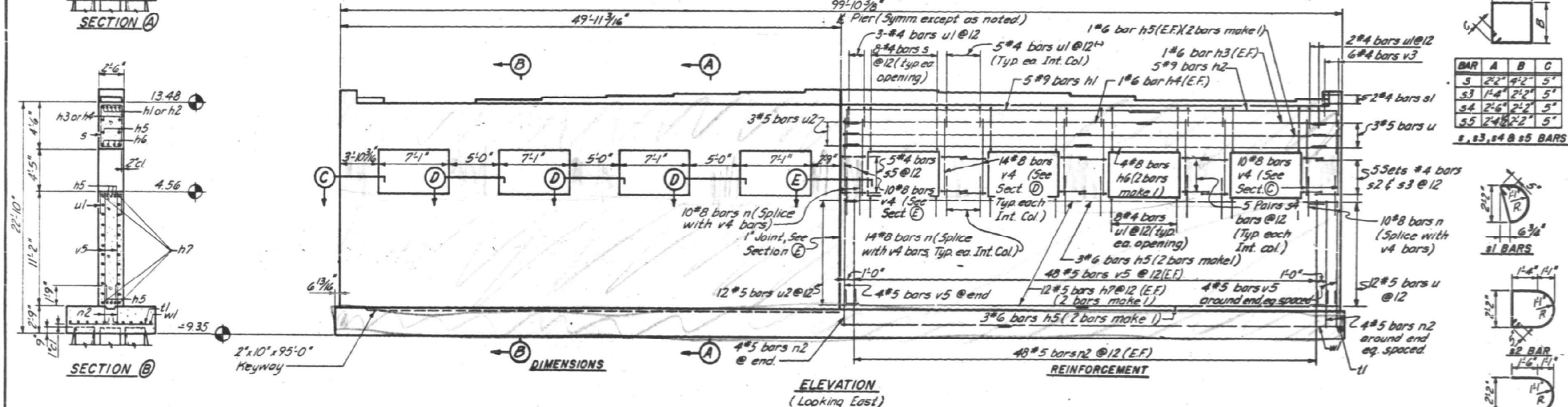
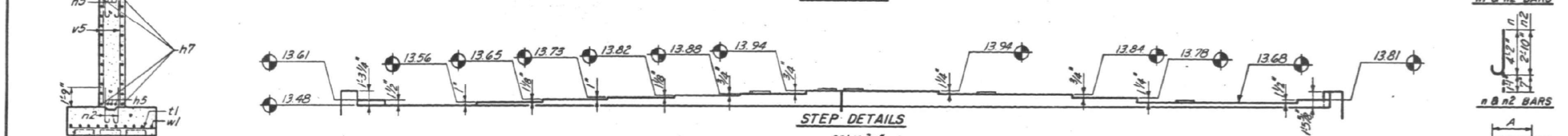
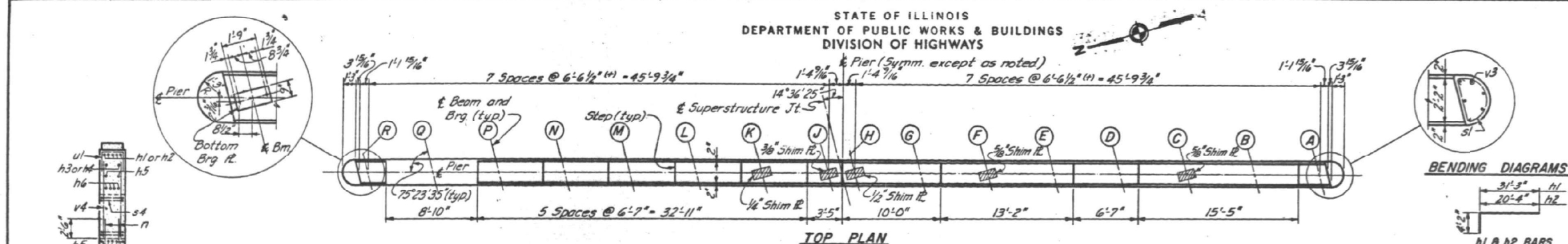
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STRUCTURE NO. 016-1156

SHEET S-43 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	60
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SEC.	EXPRESSWAYS	TOTAL SHEETS	SHEET NO.
FAI-94	202-207	SOUTH	65	21
STA.	TO STA.			
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT.				

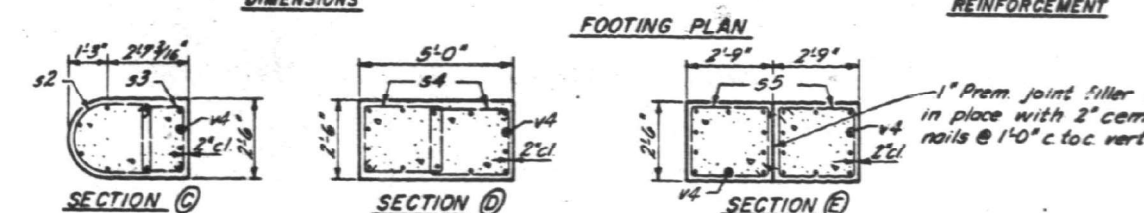


BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1	10	#9	35'-5"	
h2	10	#9	24'-6"	
h3	4	#6	18'-9"	
h4	4	#6	31'-0"	
h5	32	#6	25'-0"	
h6	16	#8	25'-3"	
h7	96	#5	24'-10"	
n	124	#8	5'-3"	
n2	208	#5	3'-5"	
s	64	#4	13'-6"	
s1	4	#4	6'-3"	
s2	10	#4	9'-1"	
s3	10	#4	7'-10"	
s4	60	#4	10'-2"	
s5	10	#4	9'-11"	
t1	174	#7	8'-6"	
u	30	#5	6'-5"	
u1	104	#4	6'-3"	
u2	30	#5	5'-2"	
v3	12	#4	2'-6"	
v4	124	#8	7'-6"	
v5	208	#5	10'-9"	
w1	30	#6	34'-6"	
CLASS I CONCRETE			CU YDS	253.9
REINFORCEMENT BARS			LBS	20,973
CL. A EXCAV FOR STRUCT.			CU YDS	226

NOTES:
All edges, except footing, shall have standard 3/8" chamfer.
See sht. 19 for concrete caisson layout and data.

DESIGNED	D.W.H.	EXAMINED	
CHECKED	D.N.	ENGINEER OF BRIDGE AND TRAFFIC STRUCTURES	
DRAWN	L.S.	PASSED	
CHECKED	L.J.L.	ENGINEER OF DESIGN	
		APPROVED	
		CHIEF HIGHWAY ENGINEER	

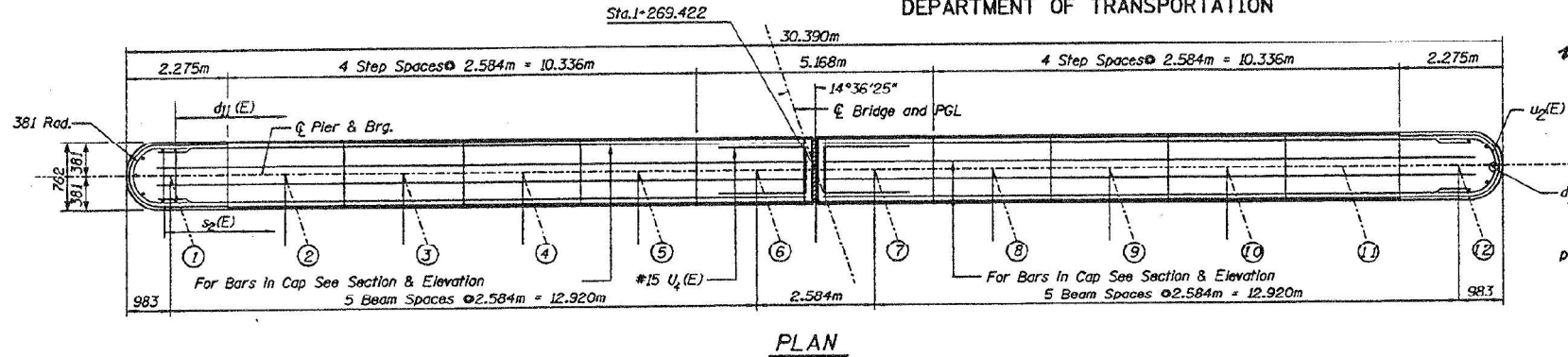


ILLINOIS DIVISION OF HIGHWAYS
SOUTH EXPRESSWAY
47TH. ST. BRIDGE
PIER NO. 2
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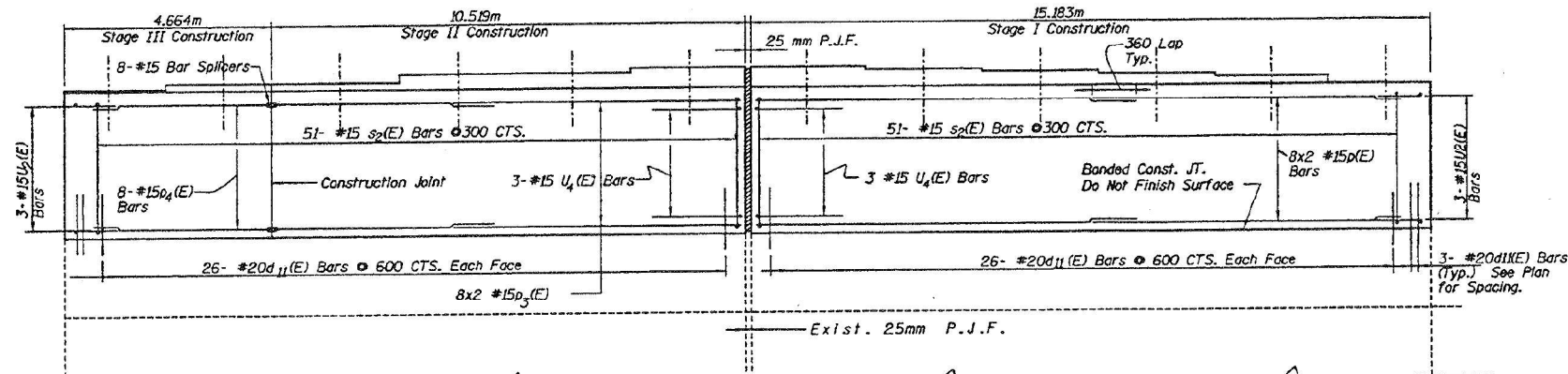
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

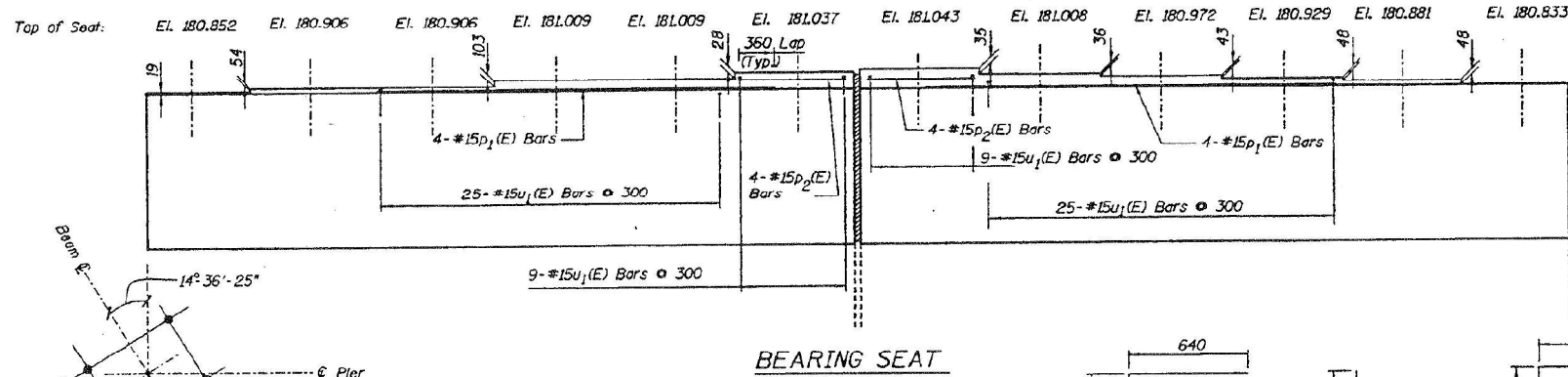
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1489	2122-907HB-R	COOK	83	52
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT			
Sheet S29 of 39				



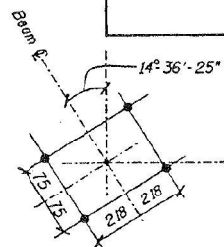
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(Looking East)

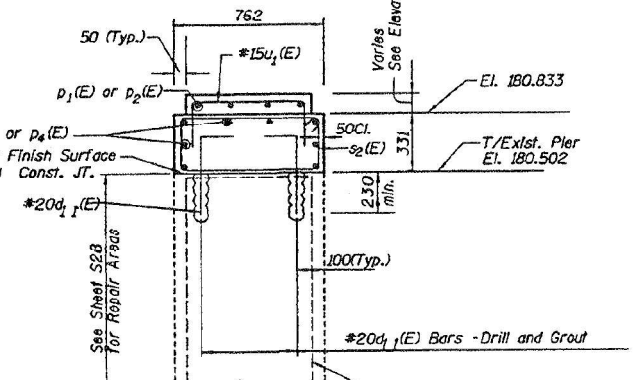


BEARING SEAT



BOLT PLACEMENT
LAYOUT

Notes:
Existing vertical reinforcement bars encountered during concrete removal shall be cleaned and straightened for incorporation into the new construction. Drill and grout #20 d11 (E) bars in 30 mm ϕ drilled holes with a min. depth of 230 mm according to Art. 584 of the Std. Specs.
Space reinforcement in cap to miss anchor bolts.
All edges shall have 20 mm chamfers except as noted.
Four steps monolithically with cap.



SECTION THRU PIER CAP

BILL OF MATERIALS				
Bar	No.	Size	Length (m)	Shape
d11 (E)	110	#20	0.70	—
p(E)	16	#15	7.65	—
p1(E)	8	#15	8.23	—
p2(E)	8	#15	2.50	—
p3(E)	16	#15	5.50	—
p4(E)	8	#15	4.20	—
sp(E)	102	#15	2.07	□
U1(E)	68	#15	1.344	┌
U2(E)	6	#15	2.31	┌
U4(E)	6	#15	1.94	┌
Concrete Structures			m ³	10.0
Reinforcement Bars, Epoxy Coated			kg	1214

Reinforcement Bars, designated (E) shall be Epoxy coated.
Bars indicated thus 1 x 4-#15 etc. indicates 1 line of bars with 4 lengths per line.

PIER 2
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
FAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

All dimensions are in millimeters unless noted otherwise.

Shah Engineering, Inc.
ONE ERM PLAZA, SUITE 2000
CHICAGO, IL 60681
Consulting Engineers

Peristal 7/16/99

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-45 OF S-52 SHEETS

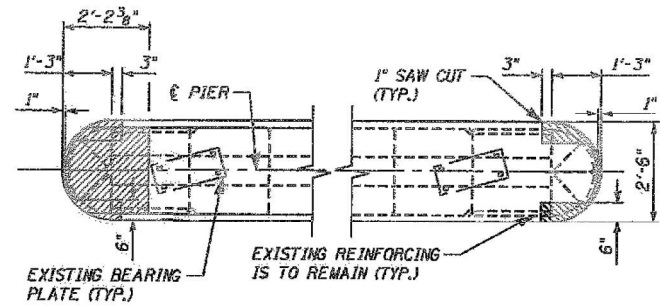
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	62
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS		FED. AID PROJECT	

Primera

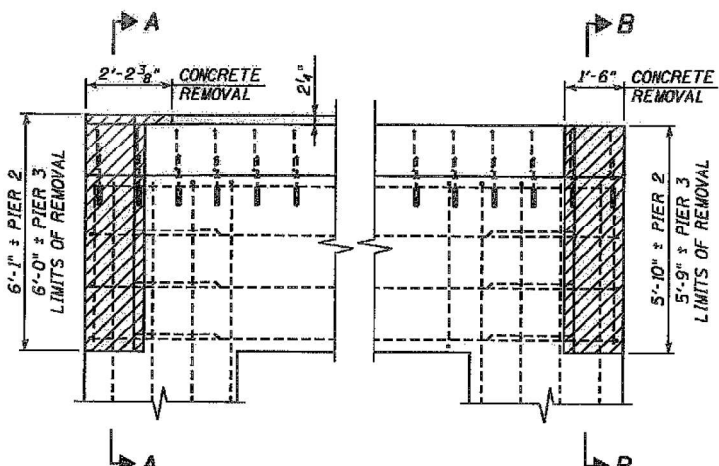
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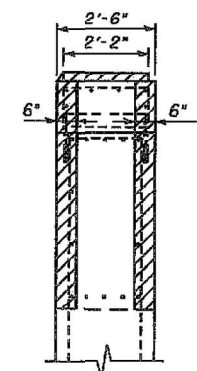
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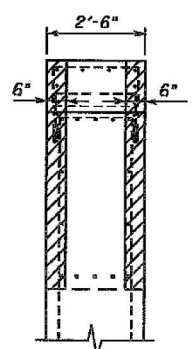
PLAN VIEW - CONCRETE REMOVAL



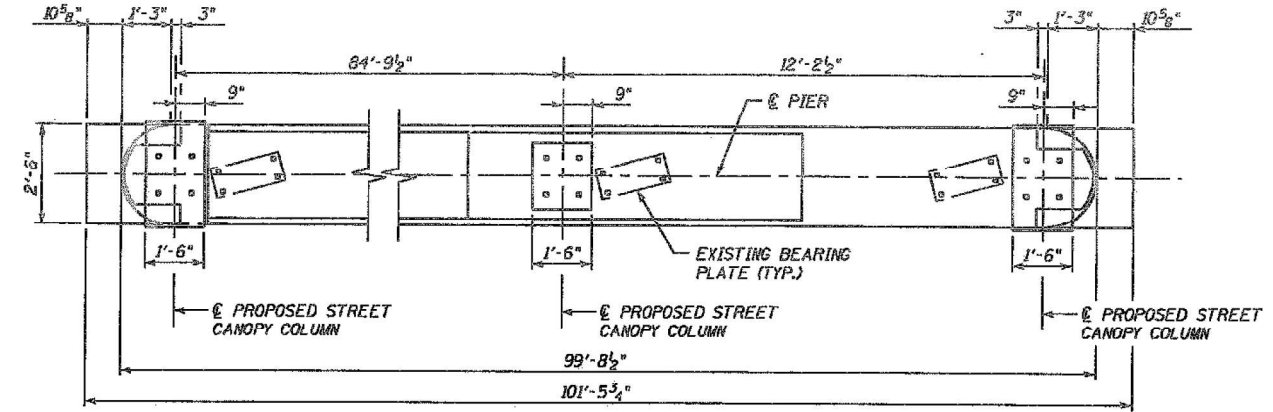
ELEVATION - CONCRETE REMOVAL



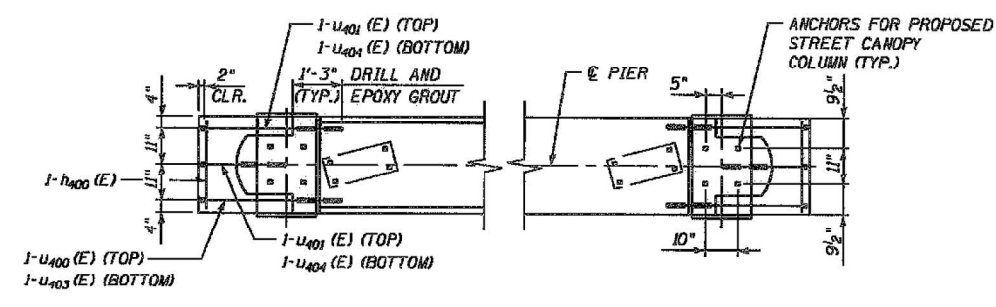
SECTION A-A



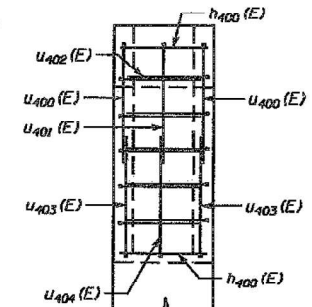
SECTION B-B



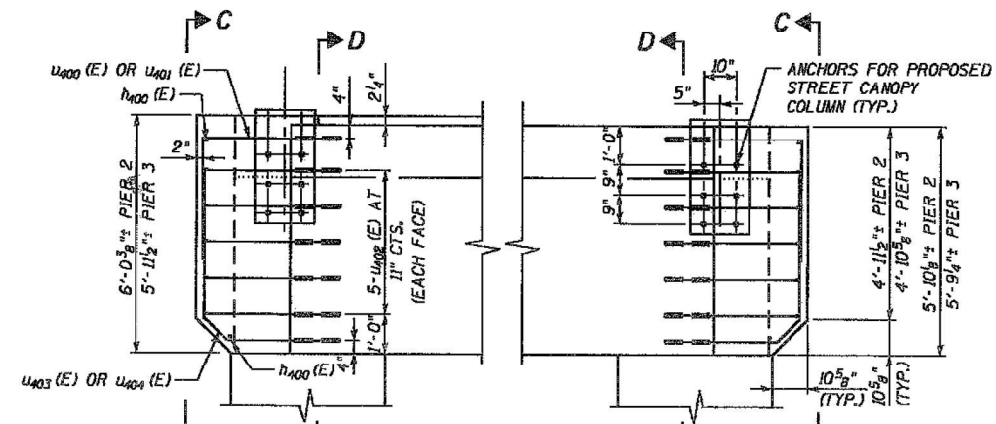
PLAN VIEW - PIER 2
(PIER 3 SIMILAR)



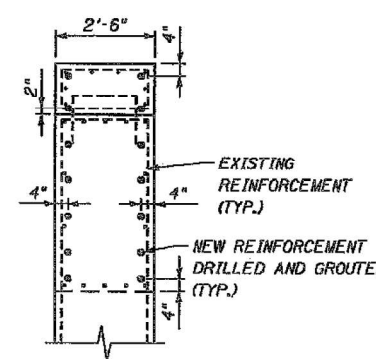
PLAN VIEW - REINFORCING



VIEW C-C



ELEVATION - REINFORCING



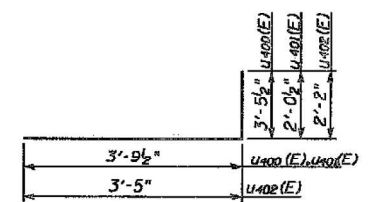
SECTION D-D

BILL OF MATERIAL
(FOR PIERS 2 AND 3)

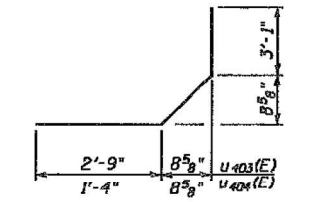
BAR	NO.	SIZE	LENGTH	SHAPE
U400 (E)	8	# 5	2' - 2"	
U401 (E)	8	# 5	7' - 3"	L
U402 (E)	4	# 5	5' - 7"	L
U403 (E)	40	# 5	5' - 7"	L
U403 (E)	8	# 5	6' - 10"	L
U404 (E)	4	# 5	5' - 5"	L

PAY ITEM	UNIT	TOTAL
CONCRETE REMOVAL	CU. YD.	1.0
CONCRETE STRUCTURES	CU. YD.	3.5
REINF. BARS, EPOXY COATED	POUND	420
BRIDGE SEAT SEALER	SQ. FT.	24

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY-COATED.



U400 (E), U401 (E), and U402 (E)



U403 (E) and U404 (E)

NOTES:

- BRIDGE SEAT SEALER SHALL BE APPLIED TO THE TOP SURFACE OF THE PIER CAP EXTENSION.
- SEE SHEETS S475-511 TO S475-512 FOR PROPOSED CANOPY COLUMNS AND CONNECTIONS TO THE PIER.
- ALL JOINTS BETWEEN NEW AND EXISTING CONCRETE SHALL BE BONDED CONSTRUCTION JOINTS.

LEGEND:

- CONCRETE TO BE REMOVED

DATE: 3/22/2006
TIME: 4:49:48 PM

USER: svayzman
FILE: g:\CH05\0019\Bridg\47thPier.dgn

In Charge: RJM
Designed By: CWE
Drawn By: CWE
Checked By: KMP
Approved By: RLP



38 W. Monroe
Suite 400
Chicago, IL 60603
Telephone 312.456.8123
Facsimile 312.456.8124

TRANSYSTEMS CORPORATION
1051 Perimeter Drive, Suite 1025
Schaumburg, Illinois 60173
(847) 805-9800

No.	Date	Revision	App'd
5	03/23/06	ISSUED TO DOT	
3	02/23/05	REVISED SCOPE	
2	07/01/05	ISSUED FOR DOT APPROVAL	
1	06/24/05	IN PROGRESS	

Chicago Transit Authority
Engineering Department

CTA RED LINE (DAN RYAN)
AESTHETIC IMPROVEMENTS
47TH STREET OVER I-94 (S.N. 016-1156)
PIER CAP MODIFICATIONS
Scale: NG SCALE Date: 03/23/06 J.D. No.:

Sheet No. S475-713



USER NAME	DESIGNED	REVISION
SUSERS	KWY	-
	OPB	-
	PWB	-
	KWY	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-46 OF S-52 SHEETS

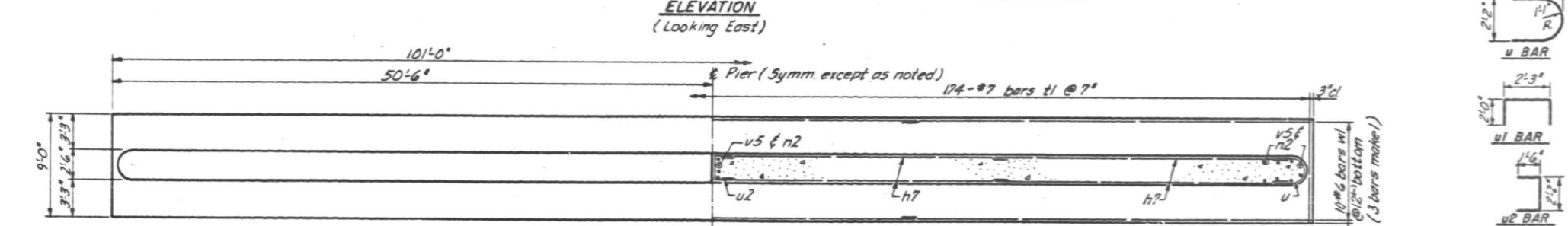
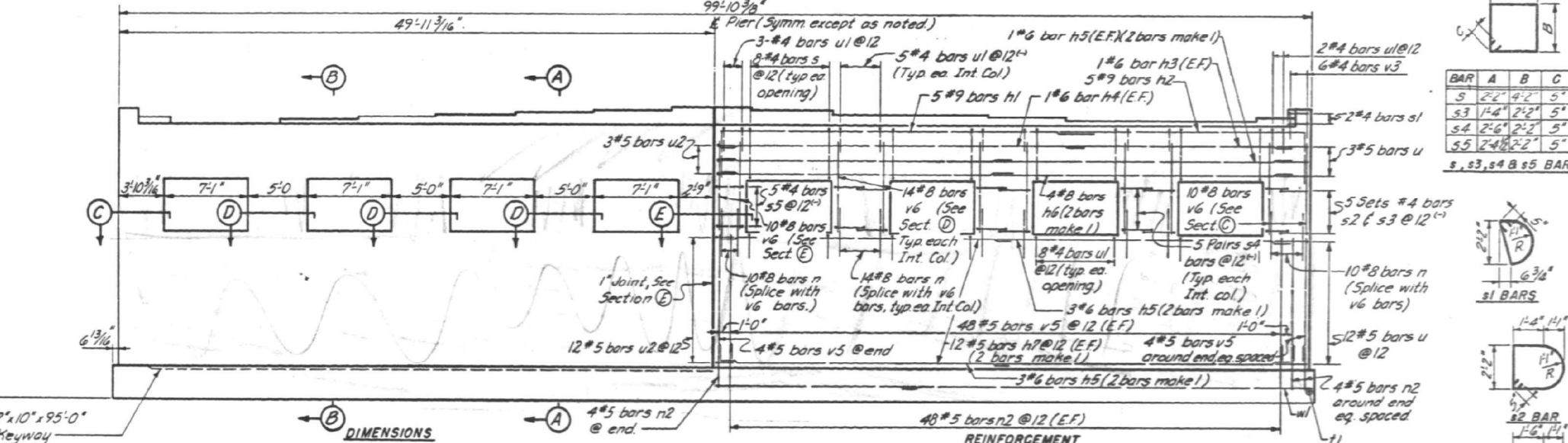
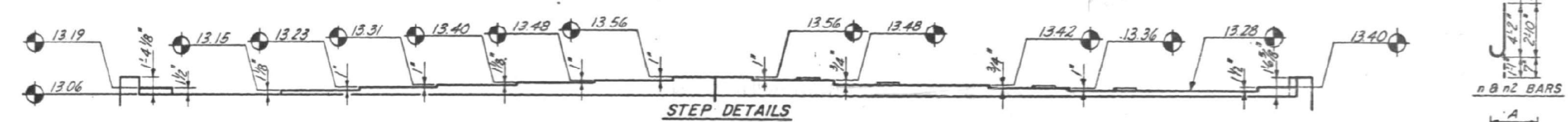
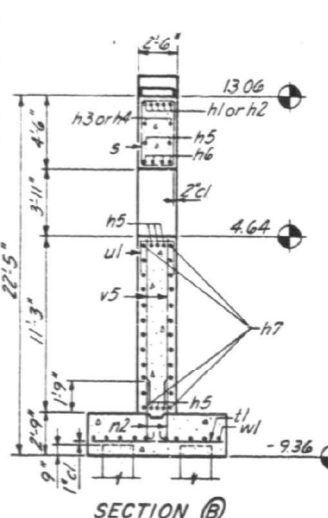
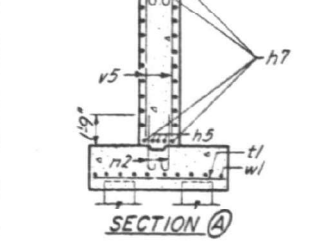
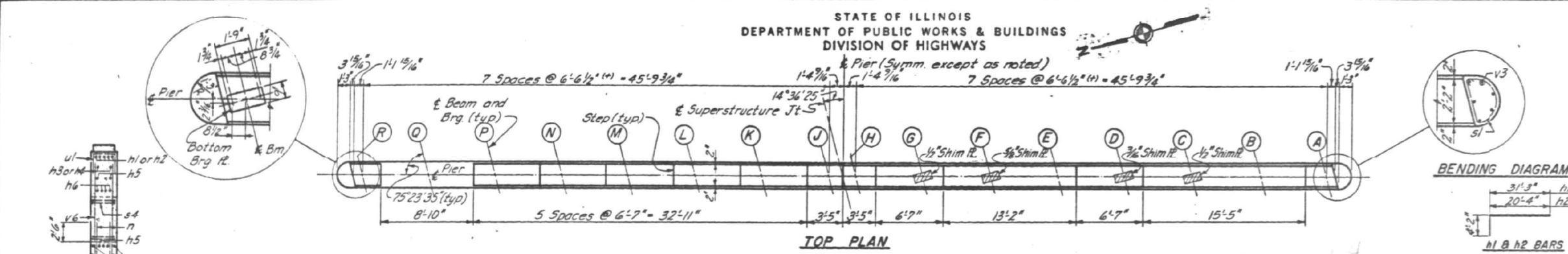
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	63

CONTRACT NO. 62R90

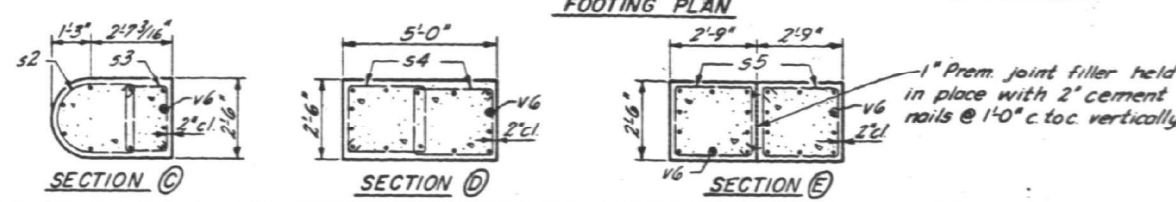
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SEC.	EXPRESSWAY	TOTAL SHEETS	SHEET NO.
FAI-94	22	SOUTH	65	22
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



DESIGNED	D.W.H.	EXAMINED	
CHECKED	D.N.	PASSED	
DRAWN	L.S.	APPROVED	
CHECKED	L.J.L.		



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1	10	#9	35'-5"	
h2	10	#9	24'-6"	
h3	4	#6	18'-9"	
h4	4	#6	31'-0"	
h5	32	#6	25'-0"	
h6	16	#8	25'-3"	
h7	96	#5	24'-10"	
n	124	#8	5'-3"	
n2	208	#5	3'-5"	
s	64	#4	13'-6"	
s1	4	#4	6'-3"	
s2	10	#4	9'-1"	
s3	10	#4	7'-10"	
s4	60	#4	10'-2"	
s5	10	#4	9'-11"	
t1	174	#7	8'-6"	
u	30	#5	6'-5"	
u1	104	#4	6'-3"	
u2	30	#5	5'-2"	
v3	12	#4	2'-6"	
v5	208	#5	10'-9"	
v6	124	#8	7'-1"	
w1	30	#6	34'-6"	

NOTES:
All edges, except footing, shall have standard 3/8" chamfer.
See sht. 19 for concrete caisson layout and data.

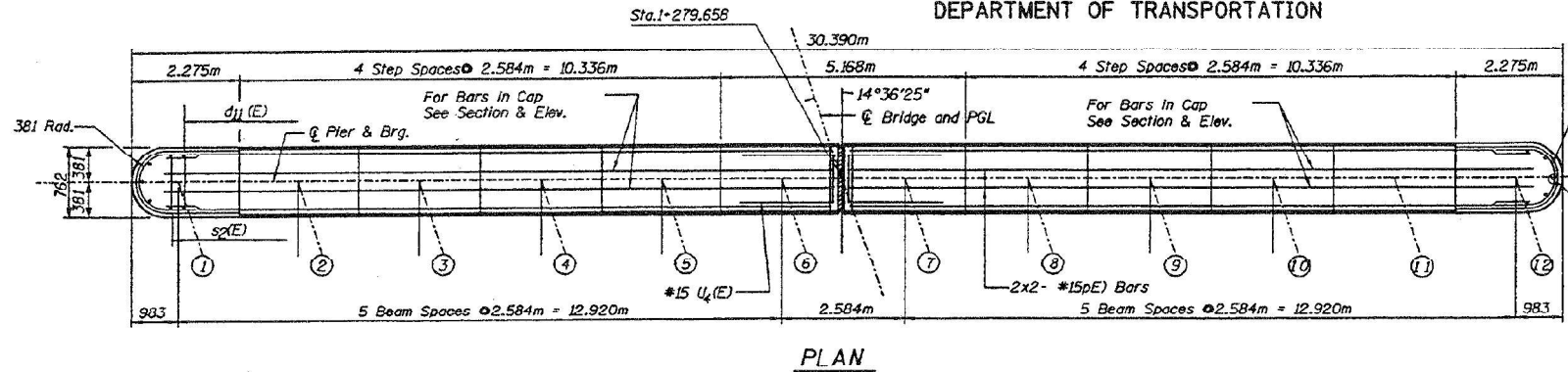
ILLINOIS DIVISION OF HIGHWAYS
SOUTH EXPRESSWAY
47TH. ST. BRIDGE
PIER NO. 3
SCALE: NONE DATE: SEPT. 1960

MODEL: Default
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100 S. WABLER DRIVE SUITE 700, CHICAGO, IL 60606 - P.312-405-8919 F.312-406-4105
12/7/2023 2:18:06 PM

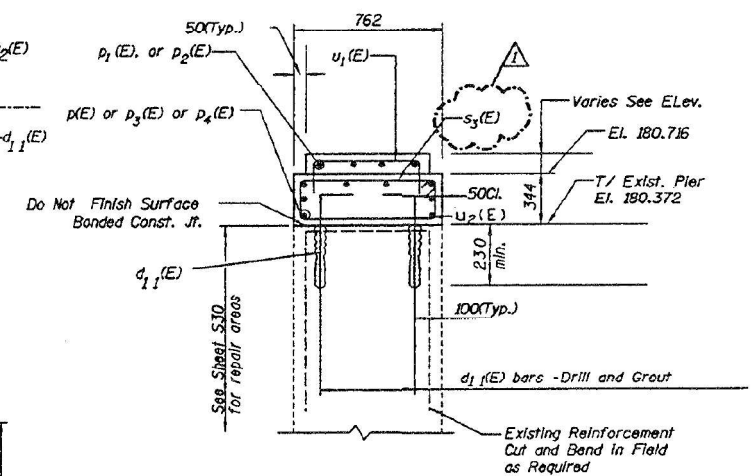
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	54
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS FED. AID PROJECT		

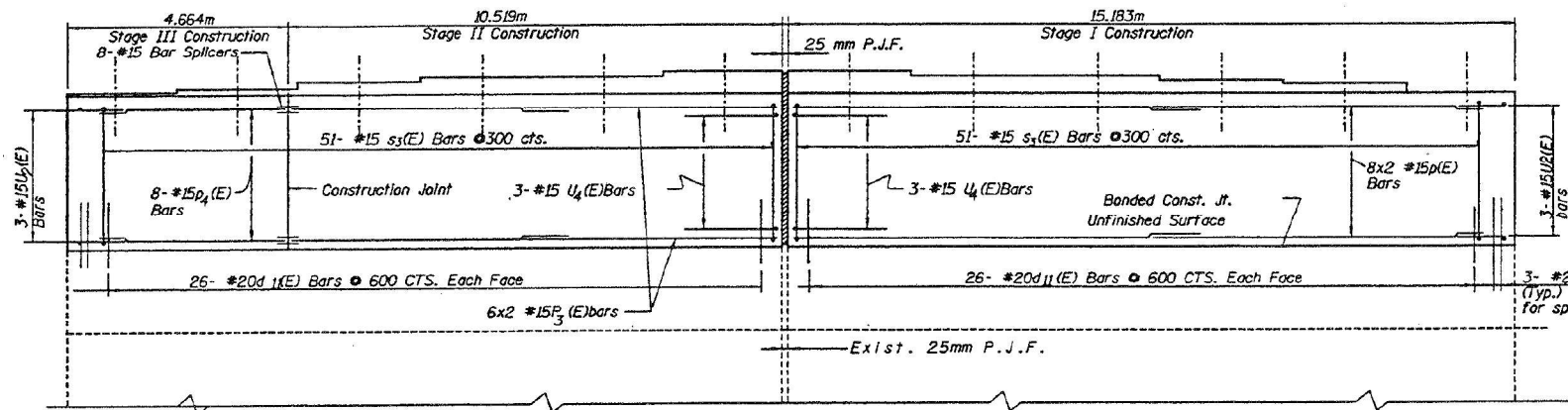
Sheet S31 of 39



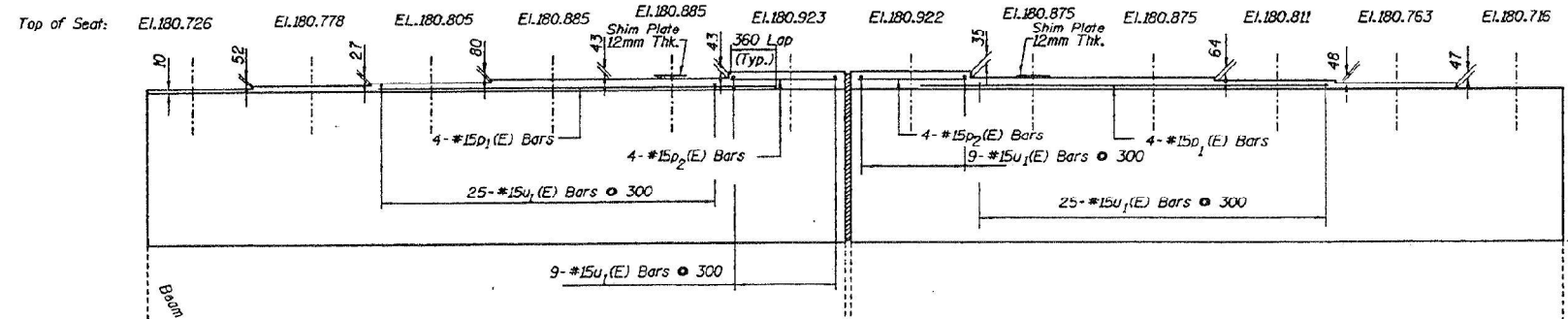
PLAN



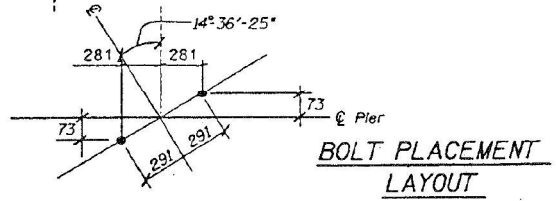
SECTION THRU PIER CAP



ELEVATION
(Looking East)



BEARING SEAT



BOLT PLACEMENT LAYOUT

Notes:
Existing vertical reinforcement bars encountered during concrete removal shall be cleaned and straightened for incorporation into the new construction. Drill and grout #20 d11 (E) bars in 30 mm φ drilled holes with a min. depth of 230 mm according to Art. 584 of the Std. Specs.
Space reinforcement in cap to miss anchor bolts.
All edges shall have 20 mm chamfers except as noted.
Pour steps monolithically with cap.

All dimensions are in millimeters unless noted otherwise.

BILL OF MATERIALS				
Bar	No.	Size	Length (m)	Shape
d11 (E)	110	#20	0.700	—
p(E)	16	#15	7.65	—
p1(E)	8	#15	8.23	—
p2(E)	8	#15	2.50	—
p3(E)	16	#15	5.50	—
p4(E)	8	#15	4.20	—
s3(E)	102	#15	2.09	□
UX(E)	68	#15	1.362	U
U2(E)	6	#15	2.31	U
U4(E)	6	#15	1.94	U
Concrete Structures			m ³	10.0
Reinforcement Bars - Epoxy Coated			kg	1219

Reinforcement Bars, designated (E) shall be Epoxy coated.
Bars indicated thus 1 x 4-#15 etc. Indicates 1 line of bars with 4 lengths per line.

PIER 3
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Revision 7/16/99 MM

Shah Engineering, Inc.
ONE IBM PLAZA SUITE 3200
CHICAGO, IL 60611
Consulting Engineers

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-48 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	65
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT

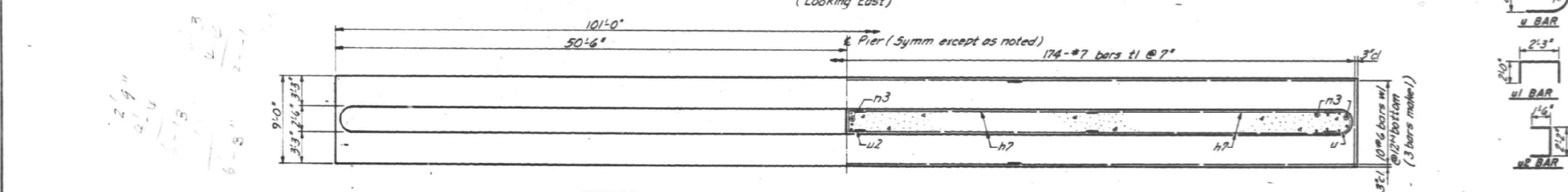
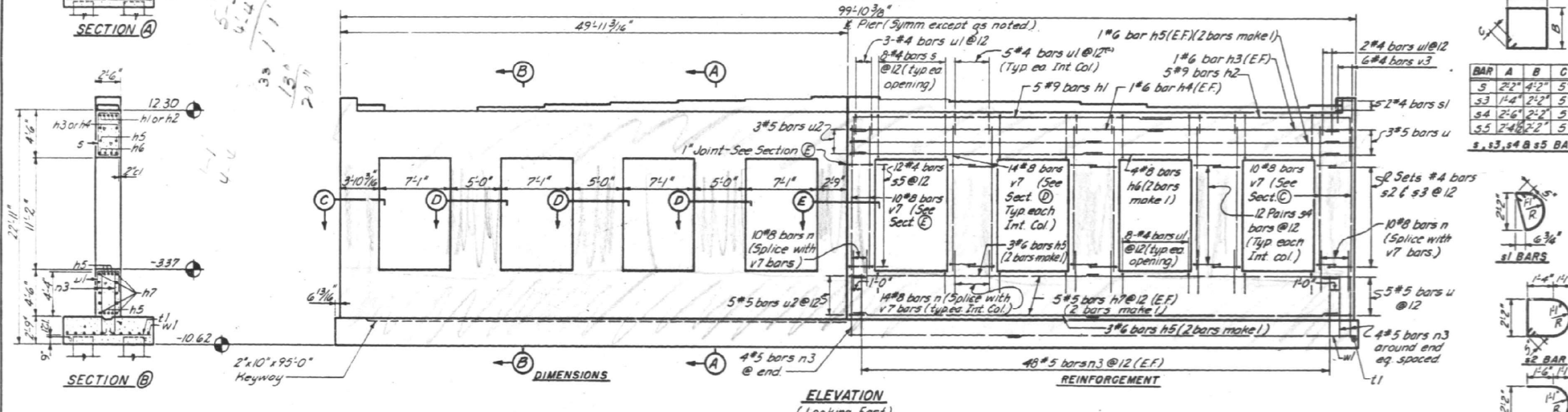
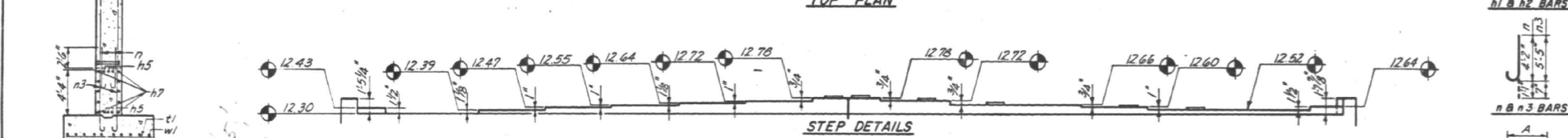
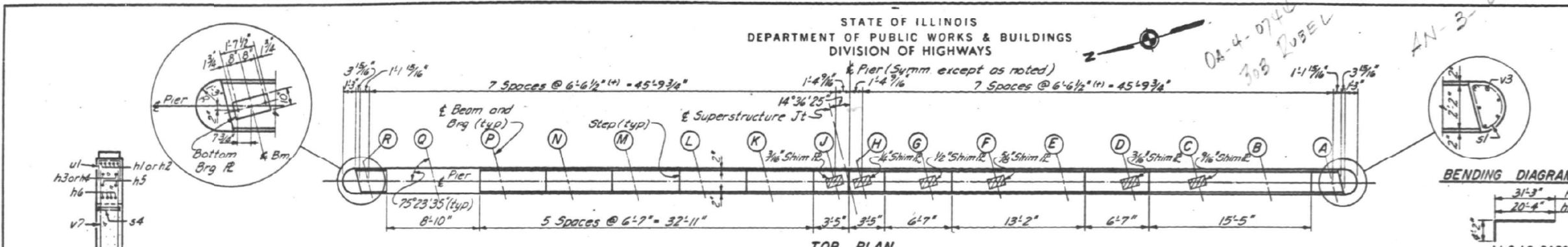


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CHECKED -	OPB	REVISIONS -			
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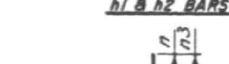
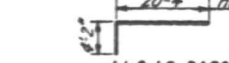
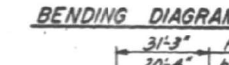
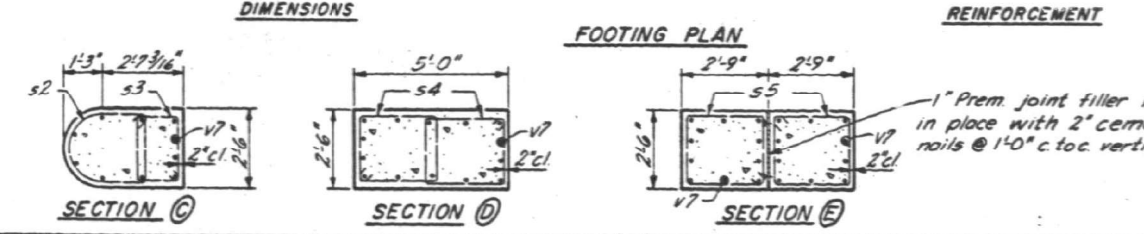
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 100 S. WABLER DRIVE SUITE 700, CHICAGO, IL 60606 - P.312-405-8910 F.312-406-4105

STATE OF ILLINOIS
 DEPARTMENT OF PUBLIC WORKS & BUILDINGS
 DIVISION OF HIGHWAYS

ROUTE NO.	SEC.	EXPRESSWAYS	TOTAL SHEETS	SHEET NO.
FAI-94	207-AD	SOUTH	65	23
STA.	TO STA.			
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT				



DESIGNED	D.W.H.	EXAMINED	
CHECKED	D.N.	ENGINEER OF BRIDGE AND TRAFFIC STRUCTURES	
DRAWN	L.S.	PASSED	
CHECKED	L.J.L.	APPROVED	



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h1	10	#9	35'-5"	
h2	10	#9	24'-6"	
h3	4	#6	18'-9"	
h4	4	#6	31'-0"	
h5	32	#6	25'-0"	
h6	16	#8	25'-5"	
h7	40	#5	24'-10"	
n1	124	#8	5'-3"	
n2	208	#5	6'-0"	
s1	64	#4	13'-6"	
s2	4	#4	6'-3"	
s3	24	#4	9'-1"	
s4	24	#4	7'-10"	
s5	144	#4	10'-2"	
s5	24	#4	9'-11"	
t1	174	#7	8'-6"	
u1	16	#5	6'-5"	
u2	104	#4	6'-3"	
u2	16	#5	5'-2"	
v3	12	#4	2'-6"	
v7	124	#8	14'-4"	
w1	30	#6	34'-6"	

CLASS X CONCRETE CU YDS 220
 REINFORCEMENT BARS LBS 20,665
 CL. A EXCAV FOR STRUCT CU YDS 196

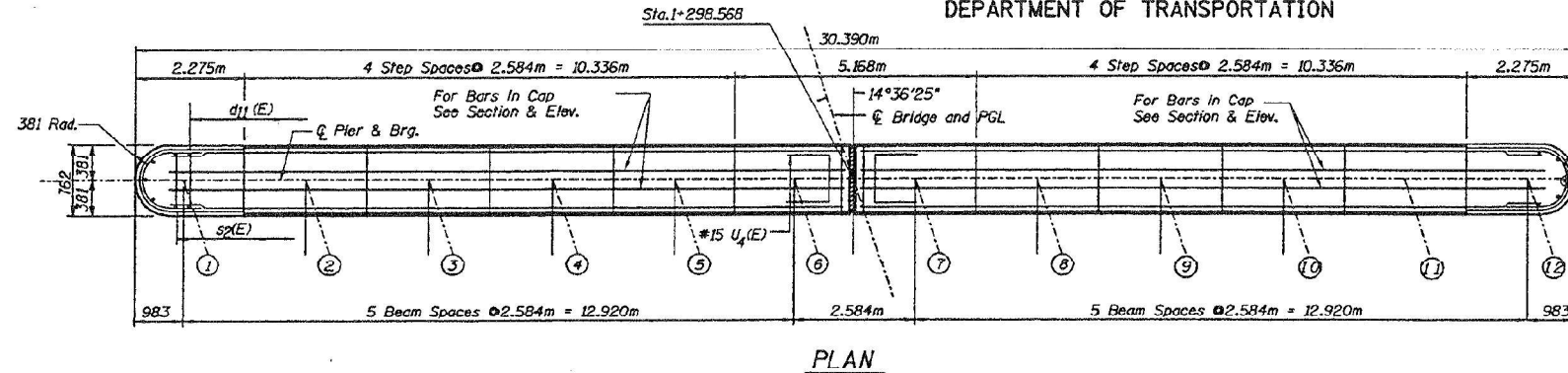
NOTES:
 All edges, except footing, shall have standard 3/4" chamfer
 See sht 19 for concrete caisson layout and data

ILLINOIS DIVISION OF HIGHWAYS
 SOUTH EXPRESSWAY
 47TH. ST. BRIDGE
 PIER NO. 4
 SCALE: NONE DATE: SEPT 1960

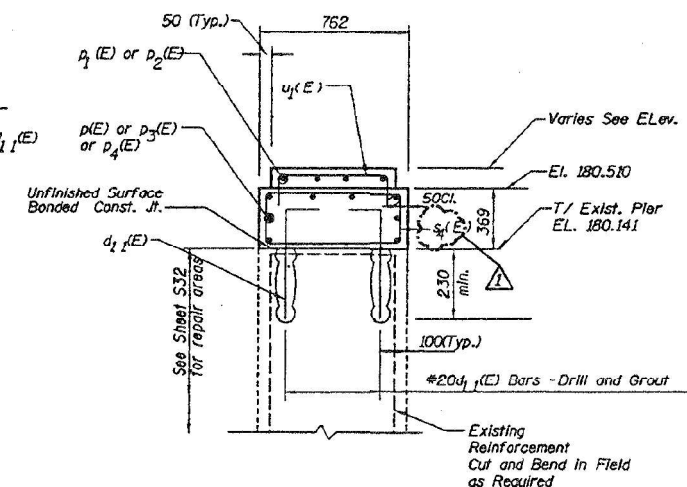
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1489	2122-907HB-R	COOK	83	56
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT			

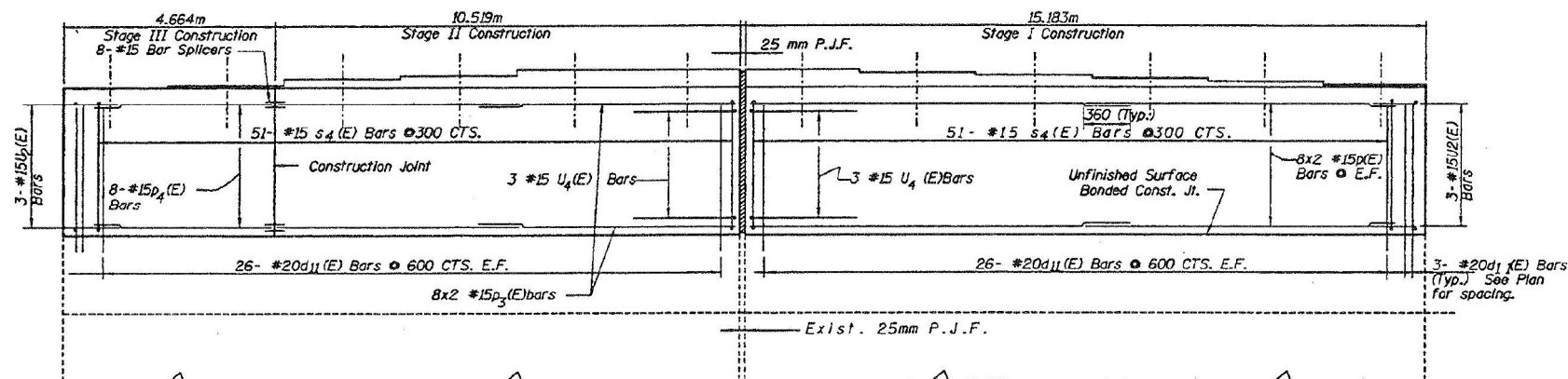
Sheet S33 of 39



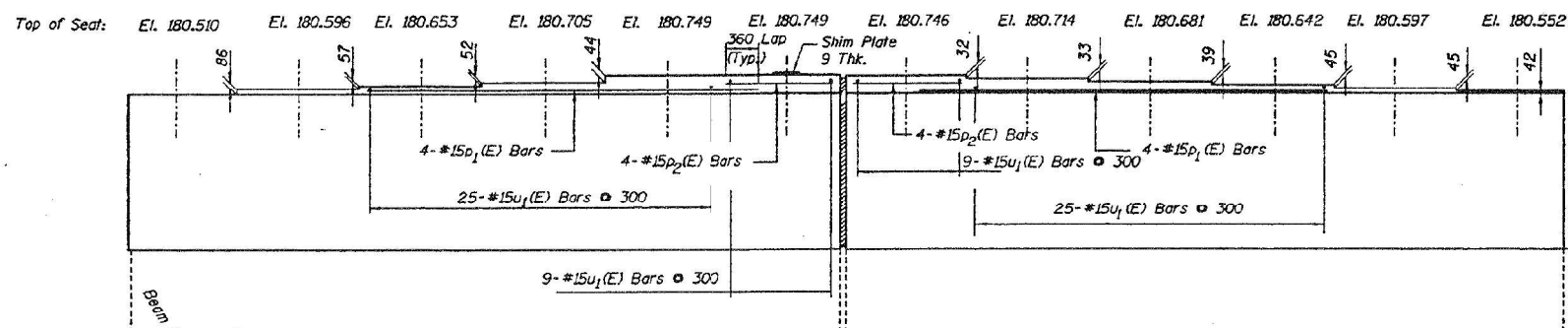
PLAN



SECTION THRU PIER CAP



ELEVATION
(Looking East)



BEARING SEAT

BILL OF MATERIALS				
Bar	No.	Size	Length (m)	Shape
d11(E)	110	#20	0.70	┌
p(E)	16	#15	7.65	—
p1(E)	8	#15	8.23	—
p2(E)	8	#15	2.50	—
p3(E)	16	#15	5.50	—
p4(E)	8	#15	4.20	—
s4(E)	102	#15	2.14	□
U1(E)	68	#15	1.382	┌
U2(E)	6	#15	2.31	┌
U4(E)	6	#15	1.94	┌
Concrete Structures		m ³	11.0	
Reinforcement - Bars, Epoxy Coated		kg	1230	

Reinforcement Bars, designated (E) shall be Epoxy coated.
Bars indicated thus 1 x 4-#15 etc. indicates 1 line of bars with 4 lengths per line.

PIER 4
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
FAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Notes:
Existing vertical reinforcement bars encountered during concrete removal shall be cleaned and straightened for incorporation into the new construction. Drill and grout #20 d11(E) bars in 30 mm diameter drilled holes with a min. depth of 230 mm according to Art. 584 of the Std. Specs.
Space reinforcement in cap to miss anchor bolts. All edges shall have 20 mm chamfers except as noted. Pour steps monolithically with cap.

All dimensions are in millimeters unless noted otherwise.

Shah Engineering, Inc.
ONE BEM PLAZA SUITE 3200
CHICAGO, IL 60611
Consulting Engineers

Revision 7/16/99 MM

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100 S. WABLER DRIVE SUITE 700, CHICAGO, IL 60606 - P.312-405-8919 F.312-406-4145



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CHECKED -	OPB	REVISOR -		REVISED -	
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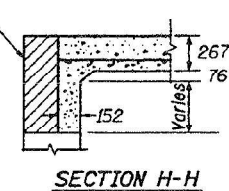
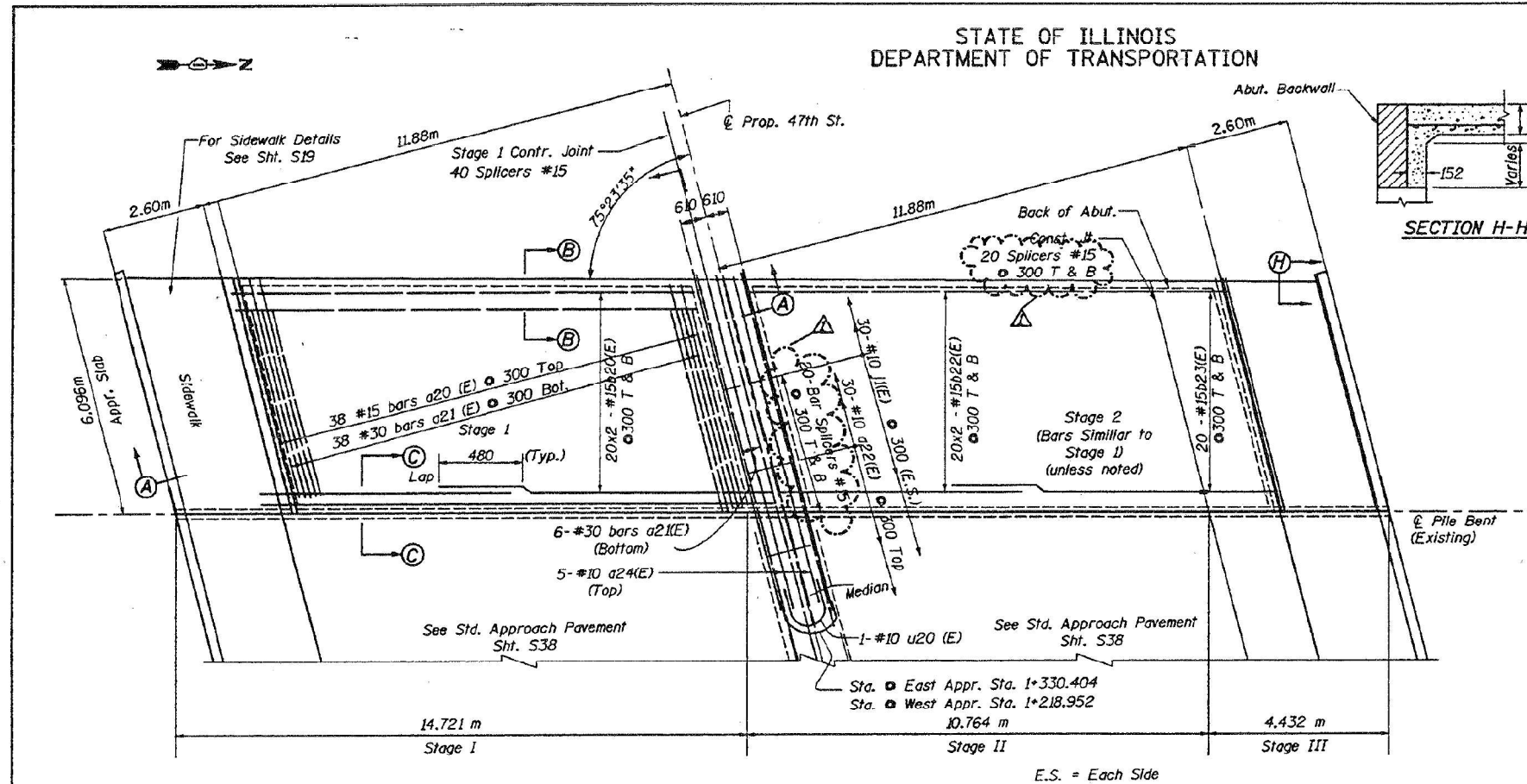
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156
SHEET S-50 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	67
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2122-907HB-R	COOK	83	60
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT		Sheet S37 of 39	

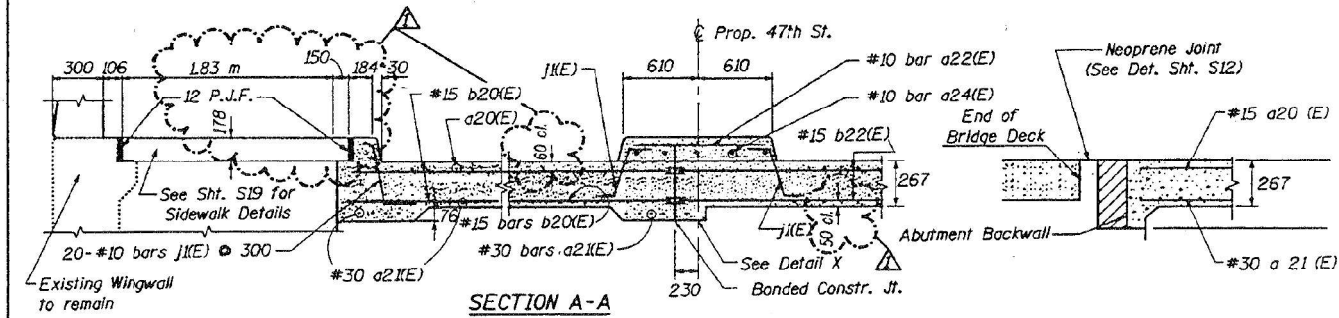


BILL OF MATERIAL FOR
EAST & WEST APPROACH SLABS

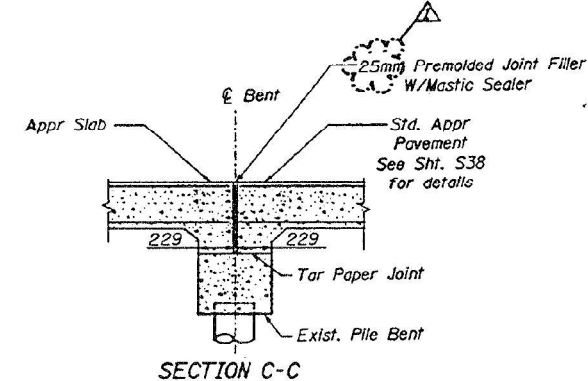
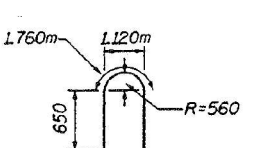
Bar	No.	Size	Length(m)	Shape
a20(E)	153	#15	6.00	—
a21(E)	164	#30	6.00	—
a22(E)	60	#10	1.10	—
a24(E)	10	#10	7.90	—
b20(E)	160	#15	6.38	—
b22(E)	160	#15	5.50	—
b23(E)	88	#15	1.71	—
j1(E)	200	#10	0.72	L
u20(E)	2	#10	3.06	C

Reinforcement Bars, Epoxy Coated: 9598 kg
Concrete Superstructures: 78 m³
Protective Coat: 380 m²
Bridge Deck Grooving: 290 m²
Reinforcement Bars Designated (E) Shall Be Epoxy Coated

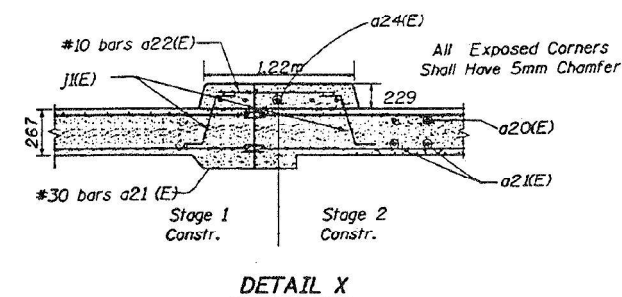
PLAN OF APPROACH SLABS
(Similar For East & West Approach Slabs)



SECTION B-B



SECTION C-C



DETAIL X

APPROACH SLAB
(EAST & WEST)
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
FAU ROUTE 1489 : SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN 016-1156

Shah Engineering, Inc.
ONE ERI PLAZA SUITE 3200
CHICAGO, IL 60611
Consulting Engineers

Revision 7/16/99 MM

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100 S. WACKER DRIVE SUITE 700, CHICAGO IL 60606 - P.312-405-8910 F.312-406-4145
12/7/2023 2:19:15 PM



USER NAME =	SUSERS	DESIGNED -	KWY	REVISED -	
CHECKED -	OPB	REVISOR -		REVISION -	
PLOT SCALE =	\$\$CALE\$\$	DRAWN -	PWB	REVISION -	
PLOT DATE =	\$\$DATE\$\$	CHECKED -	KWY	REVISION -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

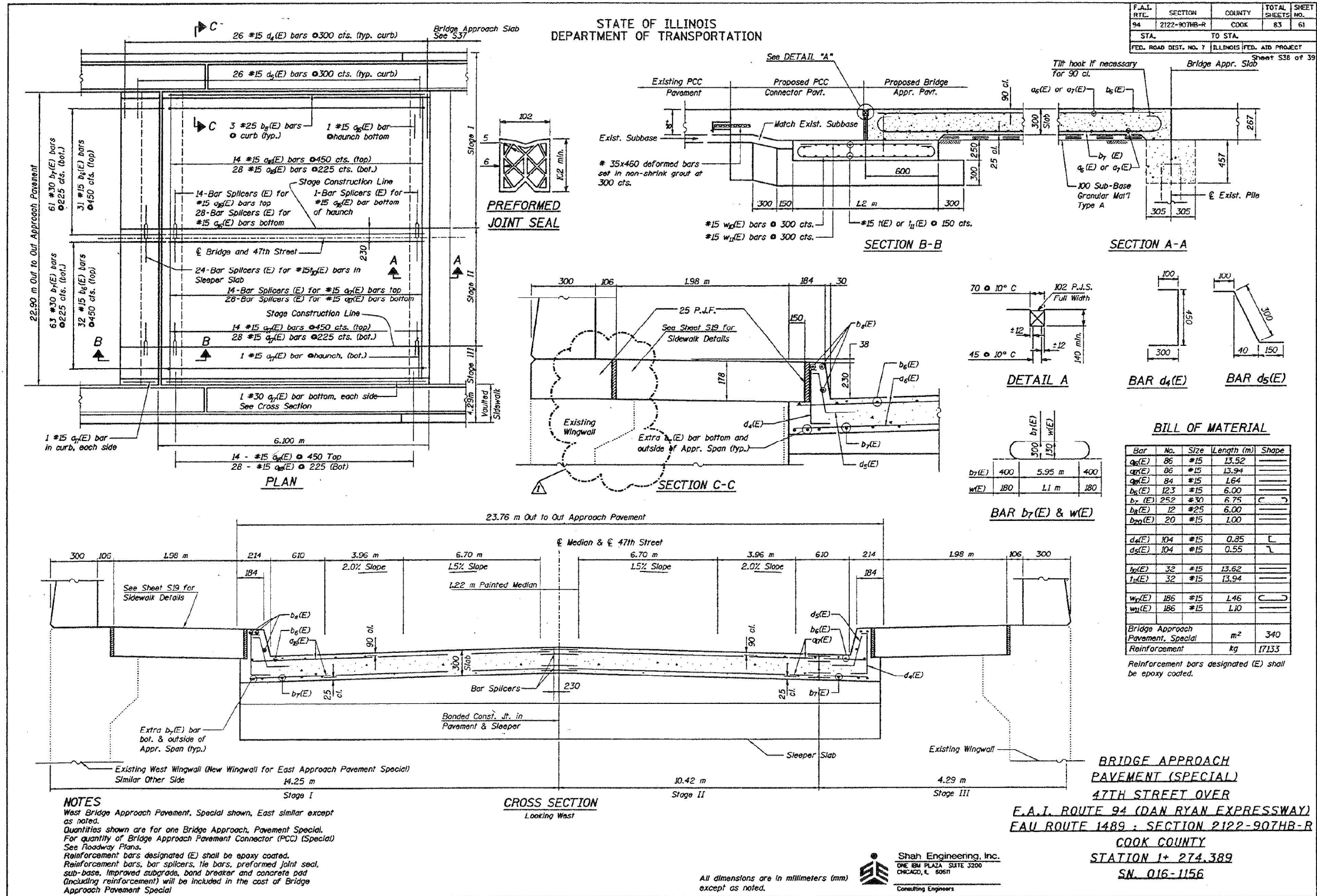
EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-51 OF S-52 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	68
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.	SHEET NO.
94	2122-907HB-R	COOK	83	61
STA.	TO STA.			
FED. ROAD DIST. NO. 7	ILLINOIS FED. AID PROJECT		Sheet 538 of 39	



BILL OF MATERIAL

Bar	No.	Size	Length (m)	Shape
d4(E)	86	#15	13.52	
d5(E)	86	#15	13.94	
b7(E)	84	#15	1.64	
w(E)	123	#15	6.00	
b7(E)	252	#30	6.75	
w(E)	12	#25	6.00	
b7(E)	20	#15	1.00	
d4(E)	104	#15	0.85	
d5(E)	104	#15	0.55	
b7(E)	32	#15	13.62	
w(E)	32	#15	13.94	
w(E)	186	#15	1.46	
w(E)	186	#15	1.10	
Bridge Approach Pavement, Special		m ²	340	
Reinforcement		kg	17133	

Reinforcement bars designated (E) shall be epoxy coated.

BRIDGE APPROACH PAVEMENT (SPECIAL)
47TH STREET OVER
E.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
EAU ROUTE 1489 - SECTION 2122-907HB-R
COOK COUNTY
STATION 1+ 274.389
SN. 016-1156

Shah Engineering, Inc.
ONE EM PLAZA, SUITE 3200
CHICAGO, IL 60651
Consulting Engineers

All dimensions are in millimeters (mm) except as noted.

NOTES
West Bridge Approach Pavement, Special shown, East similar except as noted.
Quantities shown are for one Bridge Approach, Pavement Special.
For quantity of Bridge Approach Pavement Connector (PCC) (Special) See Roadway Plans.
Reinforcement bars designated (E) shall be epoxy coated.
Reinforcement bars, bar splicers, tie bars, preformed joint seal, sub-base, improved subgrade, bond breaker and concrete pad (including reinforcement) will be included in the cost of Bridge Approach Pavement Special

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING PLANS (INFORMATION ONLY)
STRUCTURE NO. 016-1156

SHEET S-52 OF S-52 SHEETS

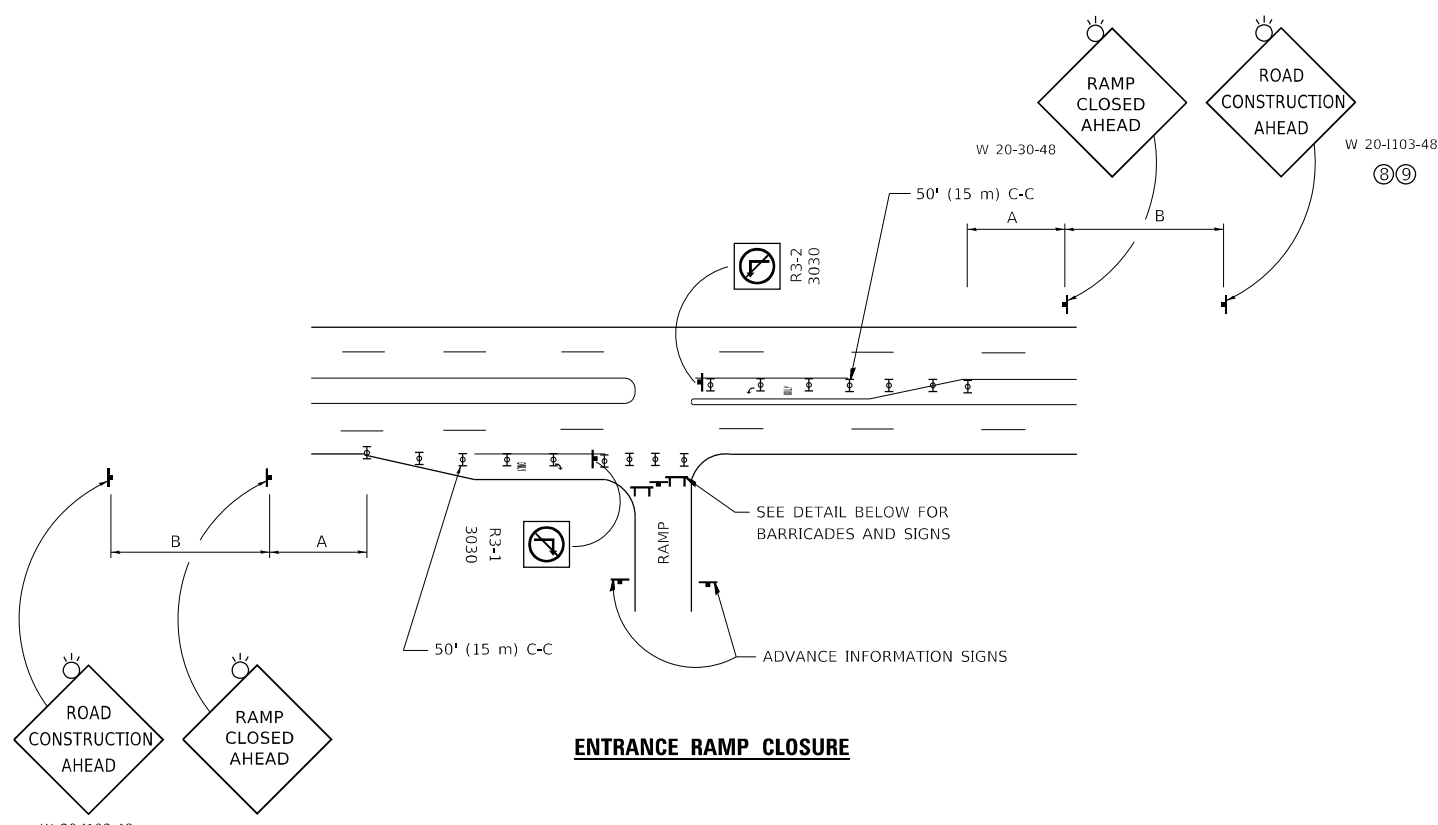
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90/94	FAI 90/94 22 BJ	COOK	83	69
CONTRACT NO. 62R90				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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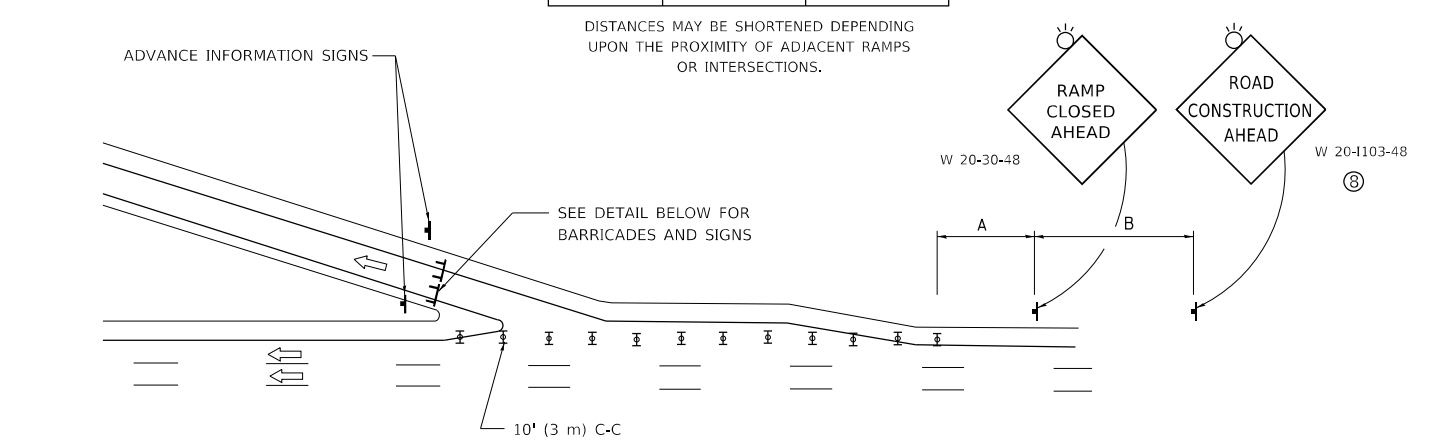


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

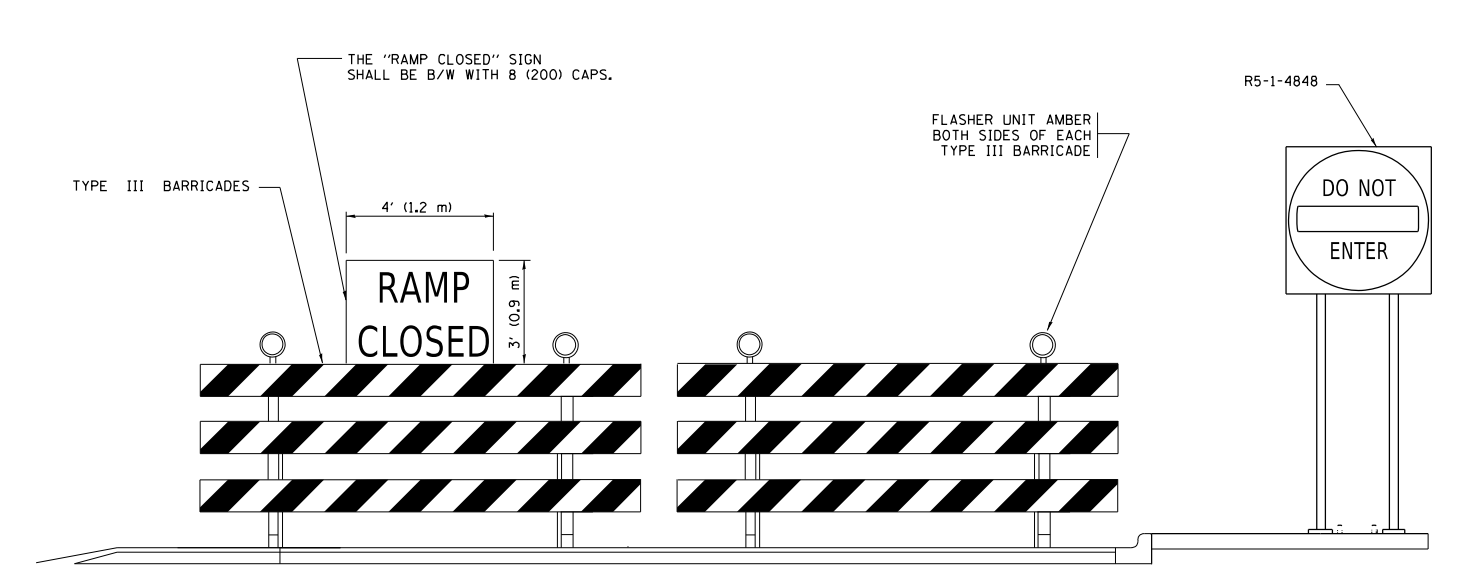
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

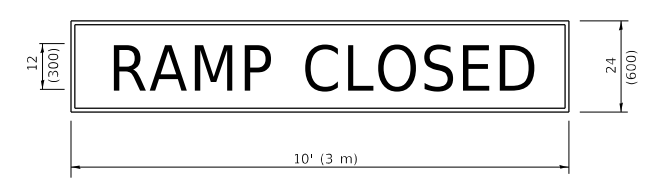
SYMBOLS

- ☐ TYPE II BARRICADE OR DRUM
- ☐ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

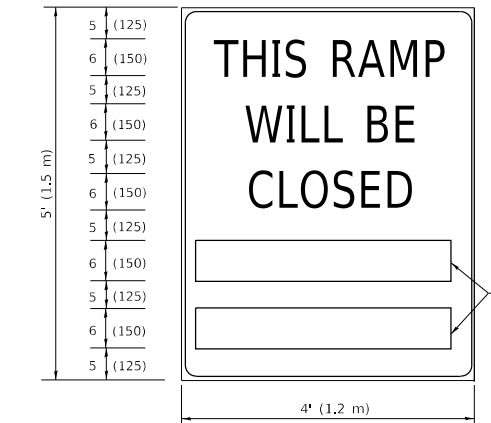
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
 E MOD FONT
 1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND
 1/2 (12) BORDER
 E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.
 THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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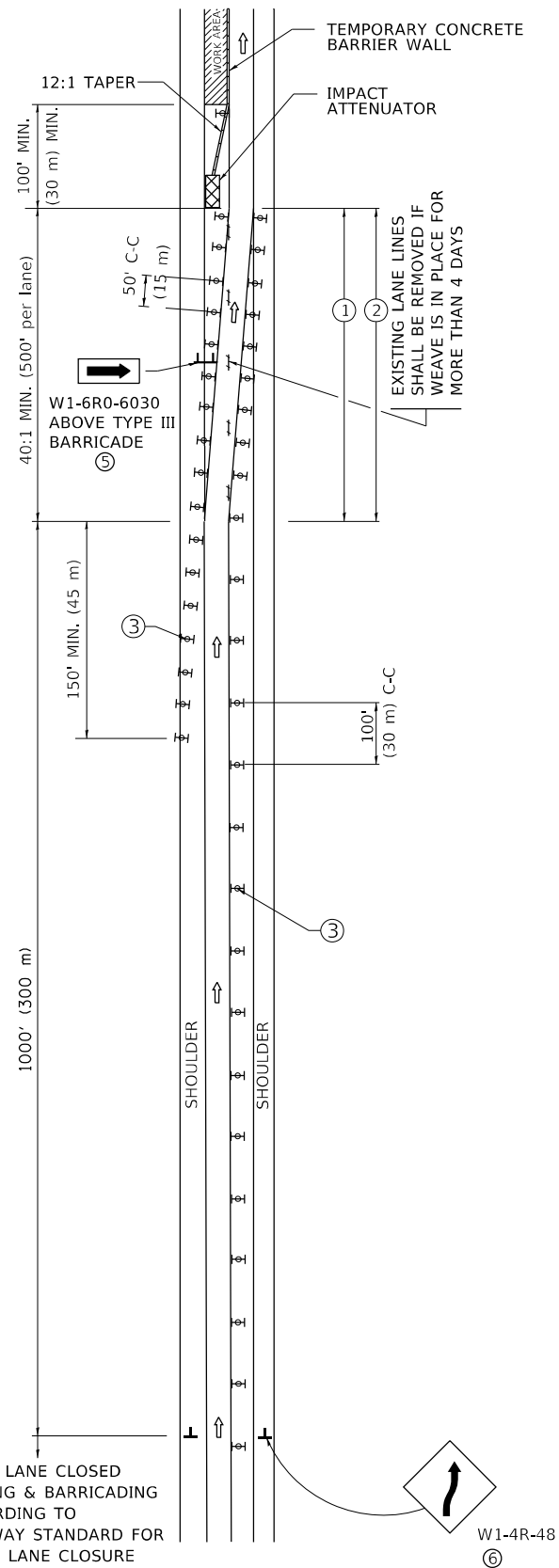
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ENTRANCE_AND_EXIT_RAMP
 CLOSURE_DETAILS**

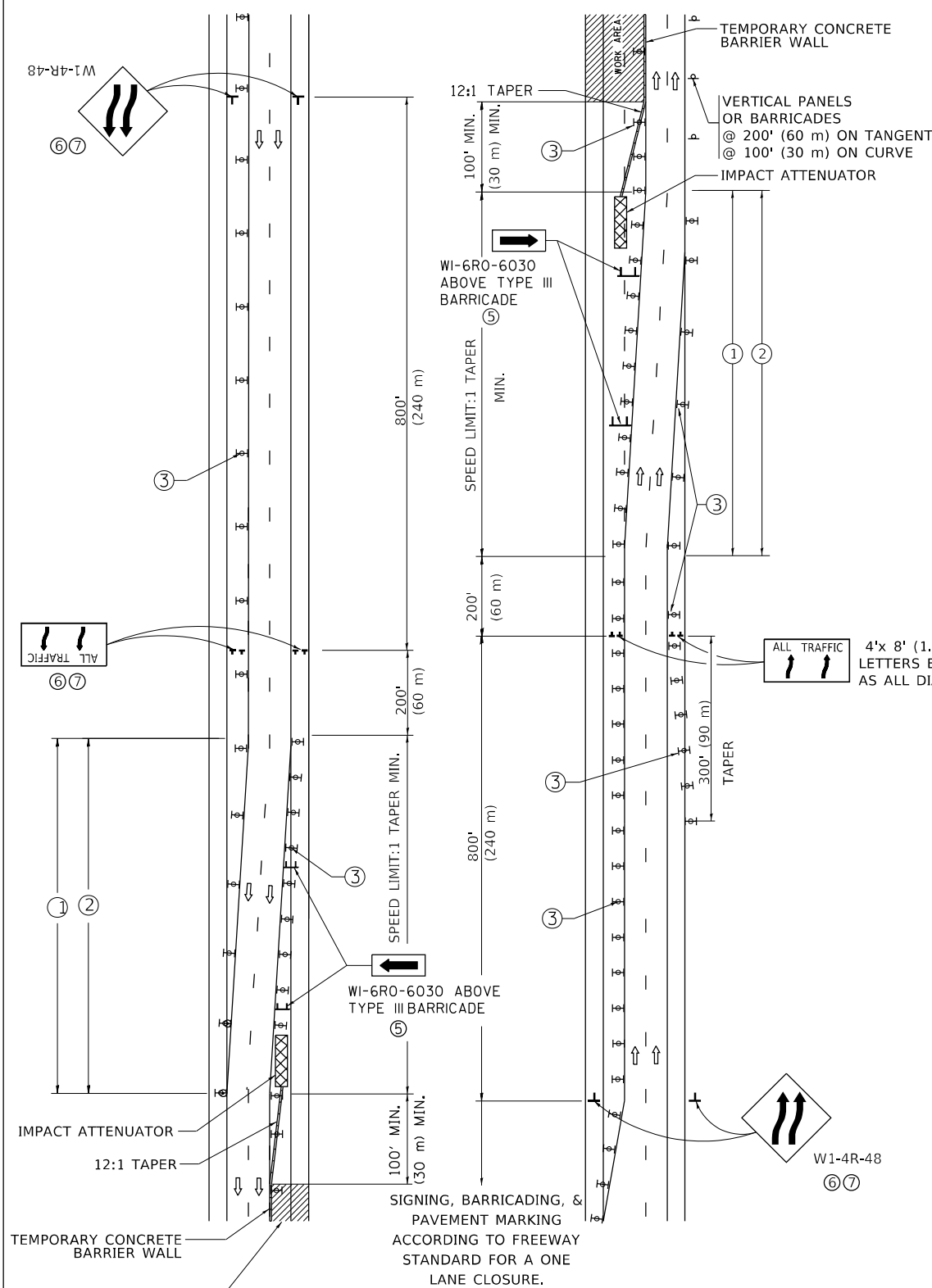
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22 BJ	COOK	83	70
TC-08		CONTRACT NO. 62R90		
ILLINOIS		FED. AID PROJECT		

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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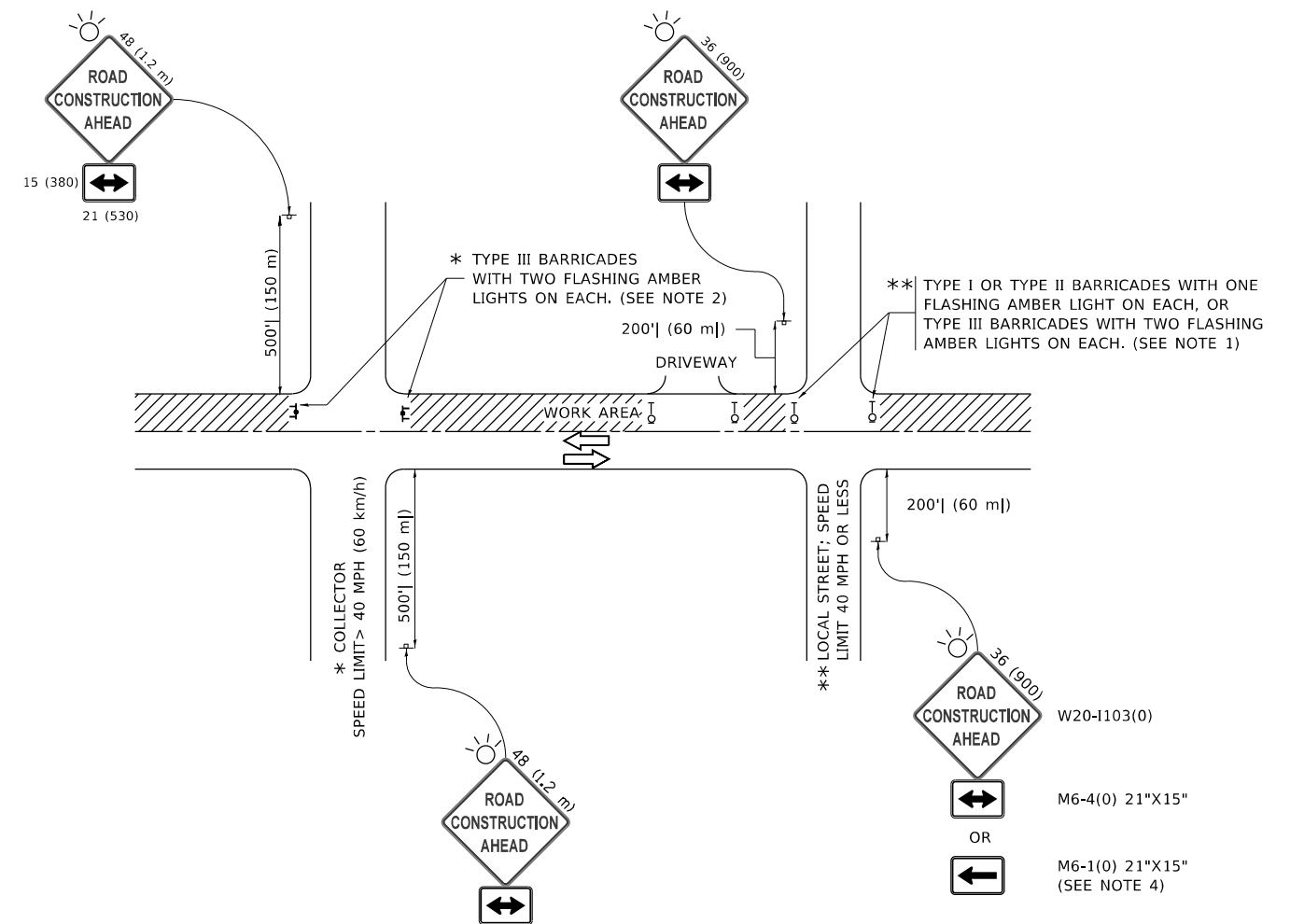
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22	COOK	83	71
TC-09		CONTRACT NO. 62R90		
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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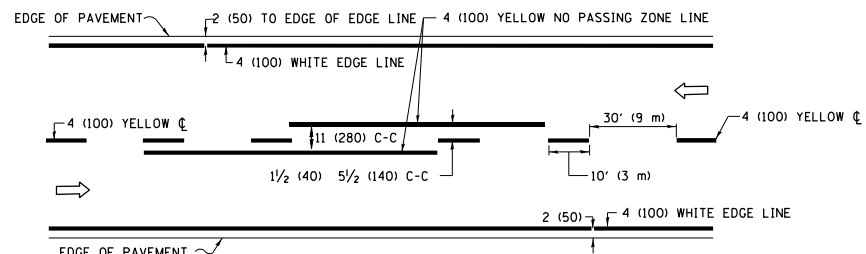
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

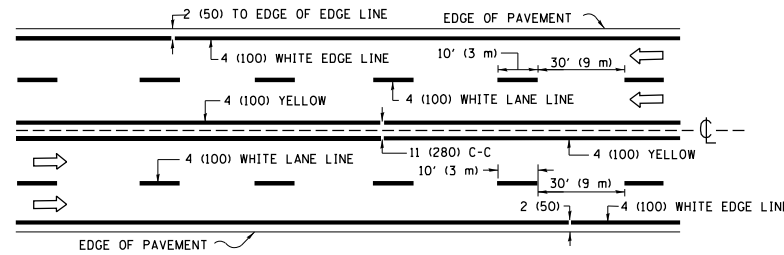
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

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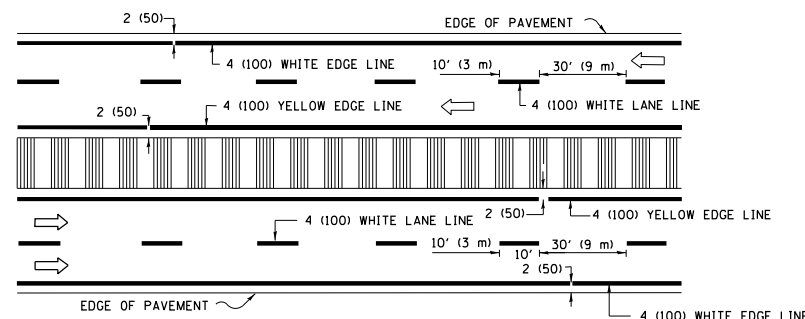
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TC-10			CONTRACT NO. 62R90	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

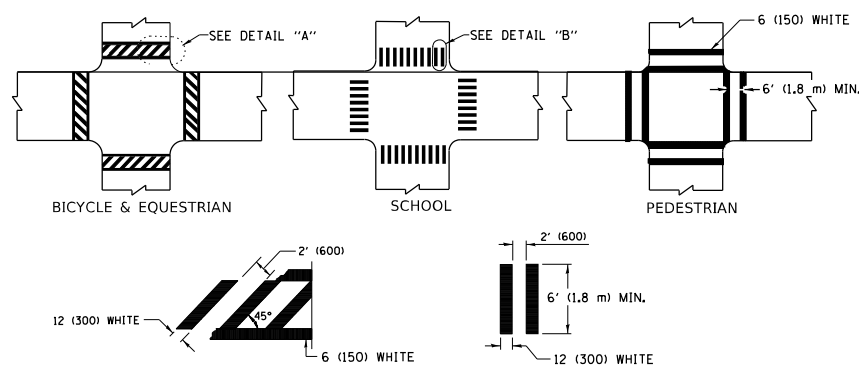


MULTI-LANE UNDIVIDED



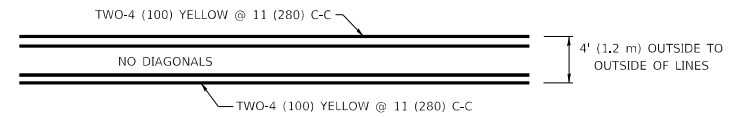
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

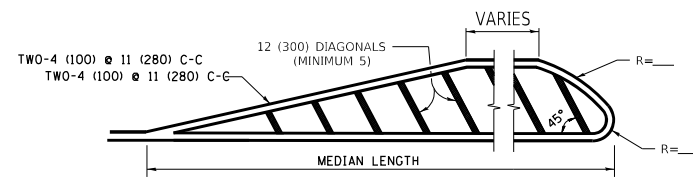


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



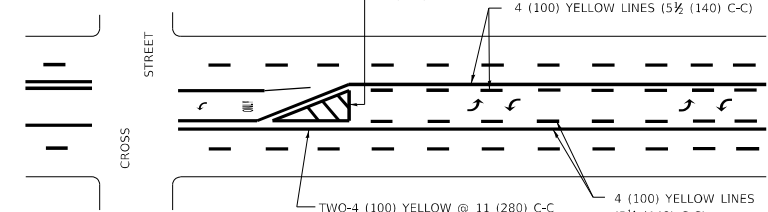
4' (1.2 m) WIDE MEDIANS ONLY



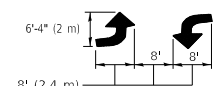
MEDIANS OVER 4' (1.2 m) WIDE

FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

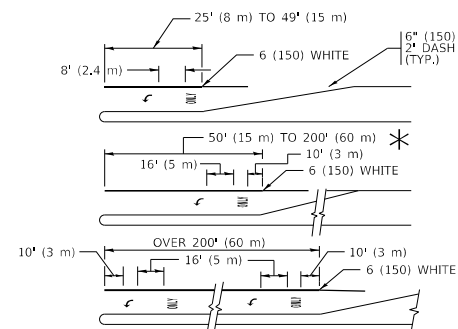


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

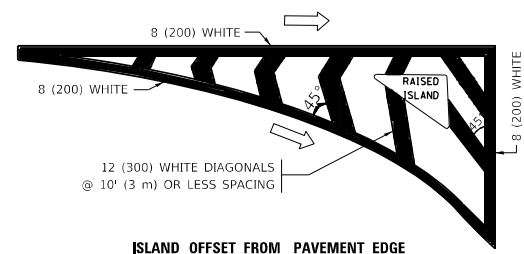


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

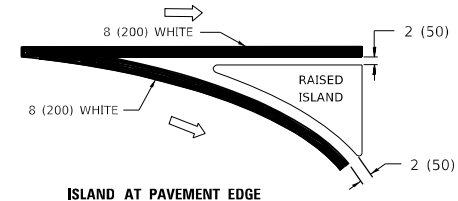
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

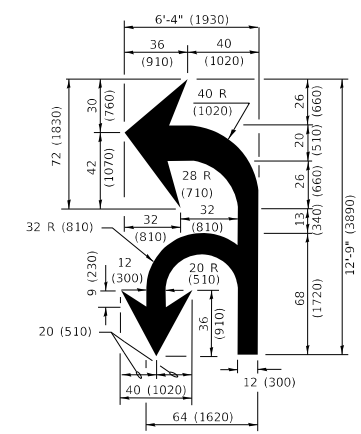


ISLAND OFFSET FROM PAVEMENT EDGE

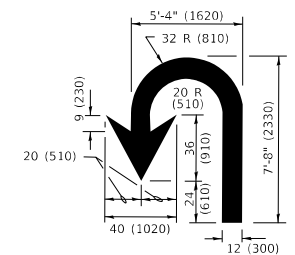


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES "RR" IS 8' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9094	FAI 90/94 22	COOK	83	73
TC-13		CONTRACT NO. 62R90		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

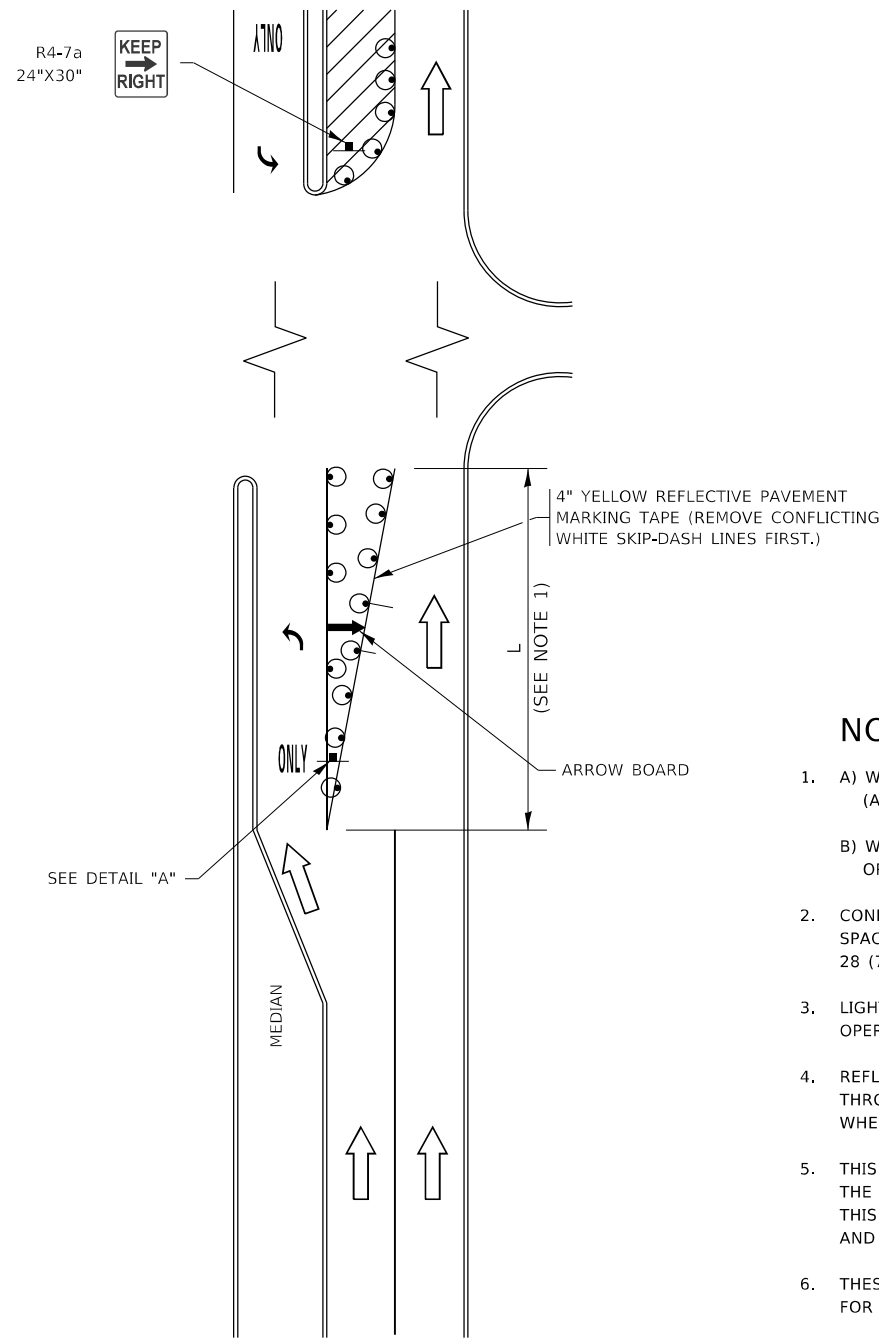


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

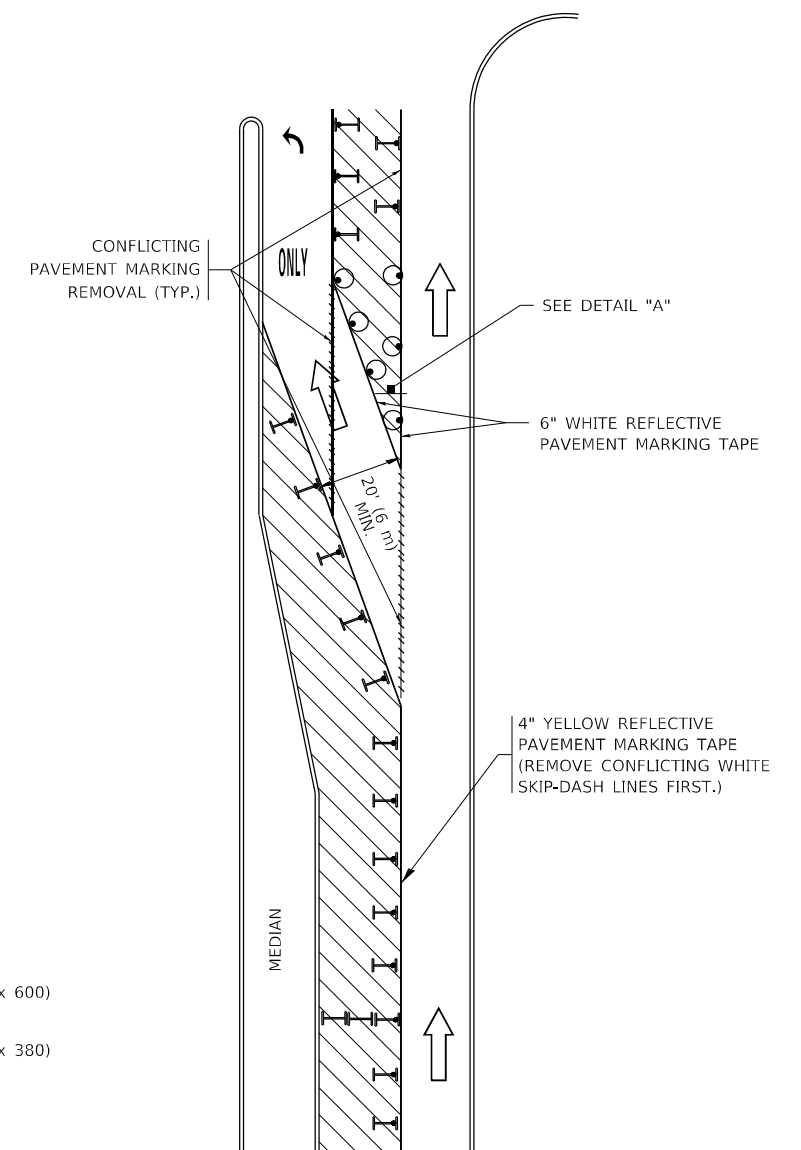
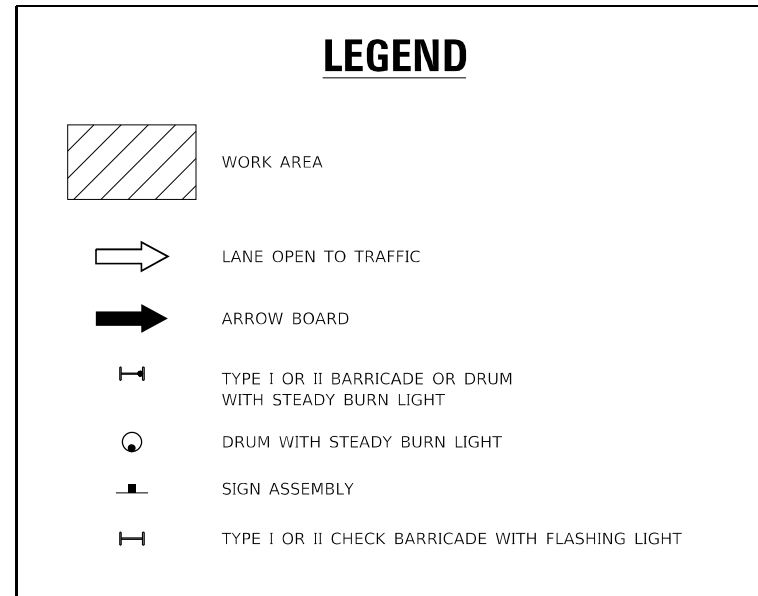
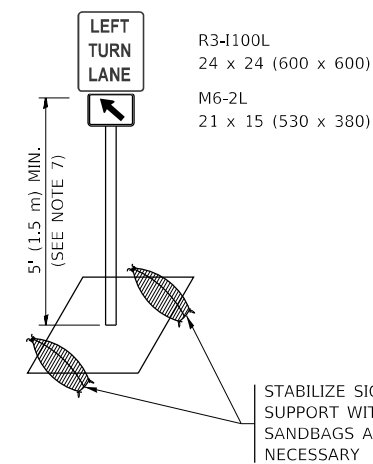


FIGURE 2



NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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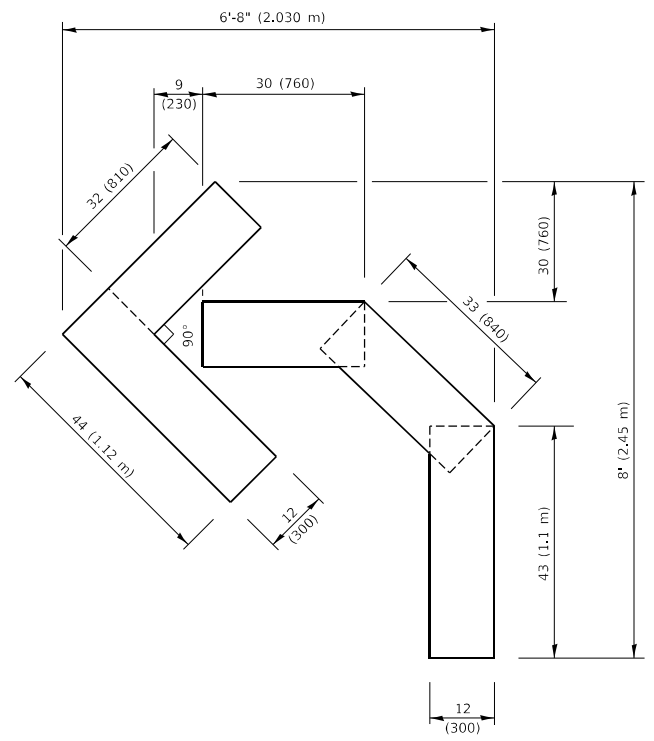
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

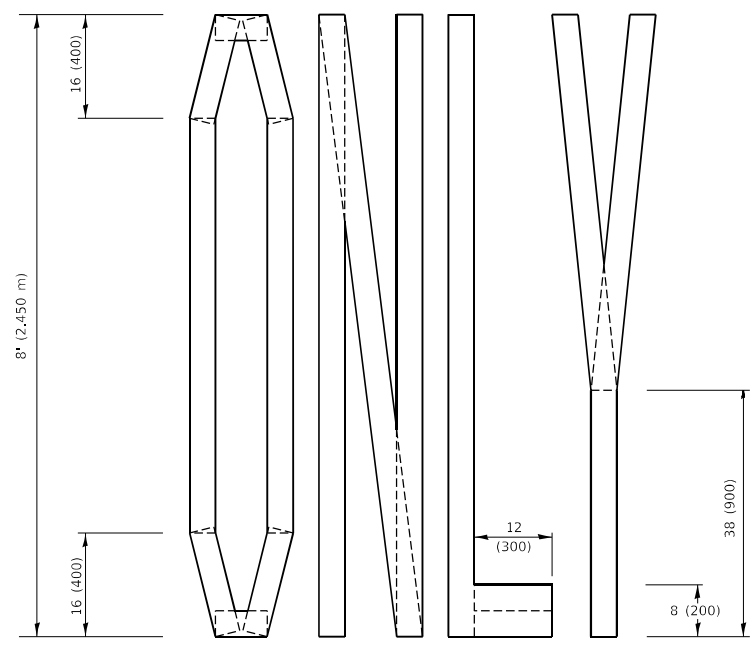
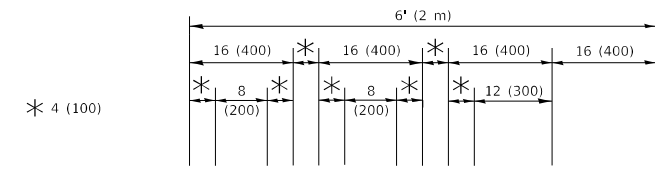
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

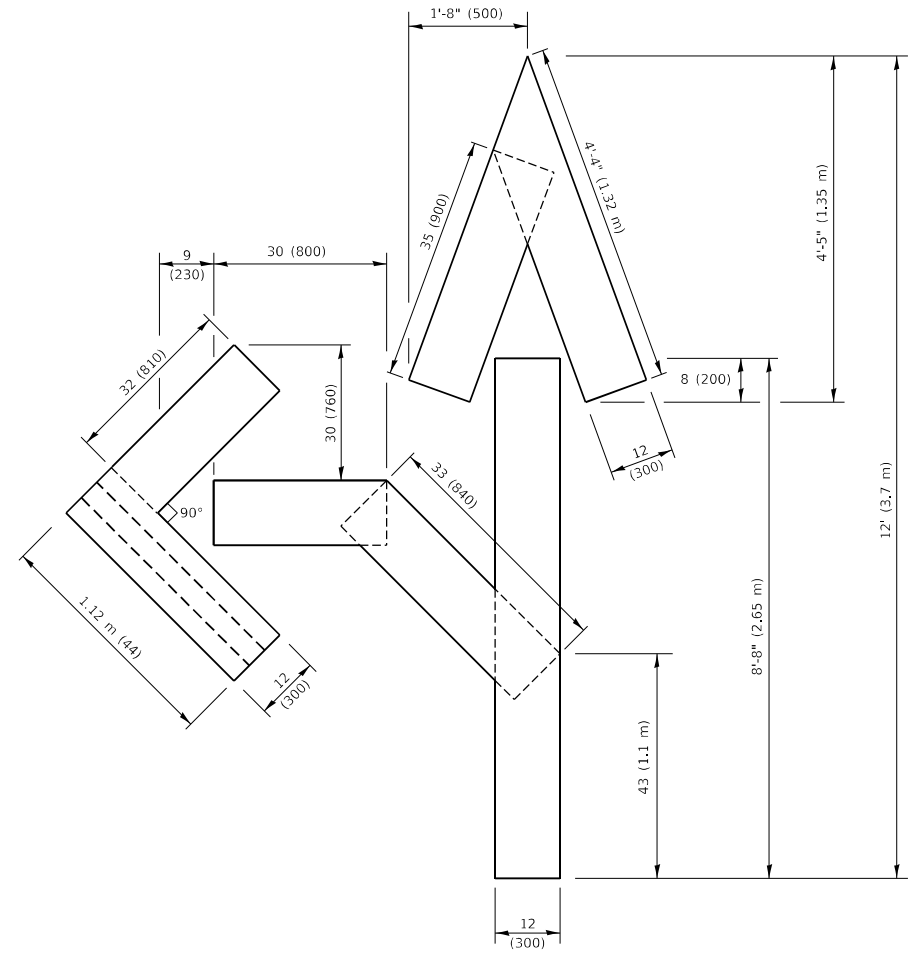
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22	COOK	83	74
TC-14		CONTRACT NO. 62R90		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

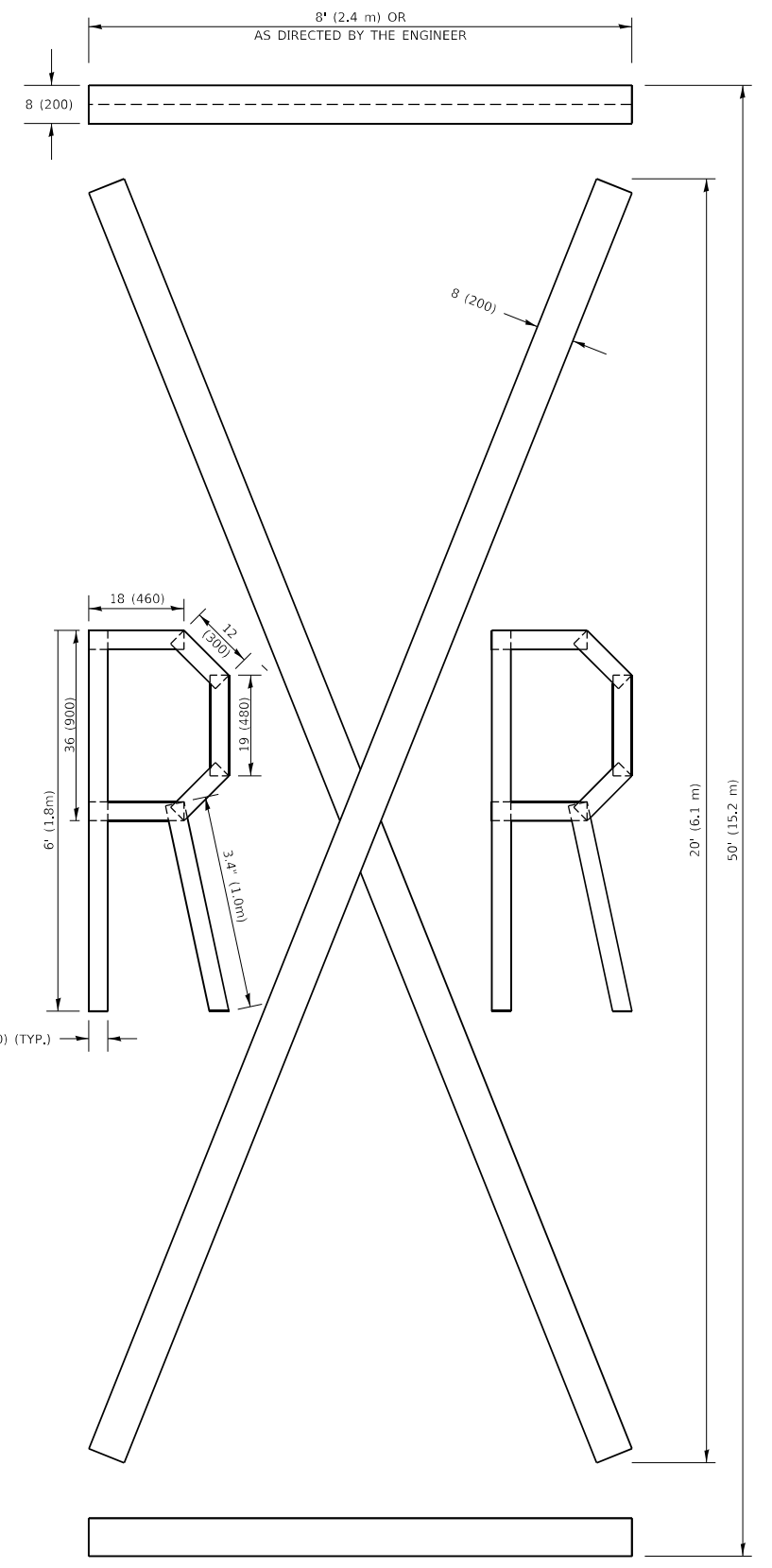


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/4/2019	CHECKED -	REVISED - E. GOMEZ 08-28-00
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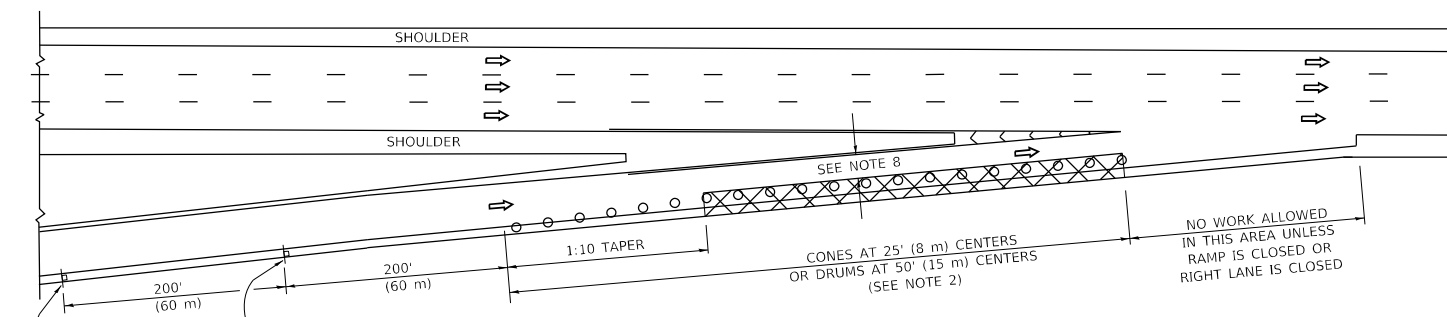
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

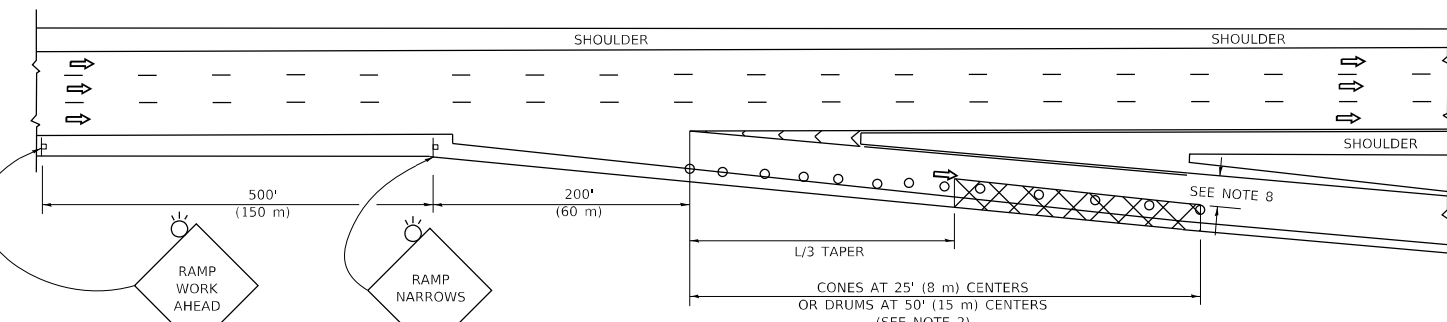
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16		CONTRACT NO. 62R90		
ILLINOIS FED. AID PROJECT				

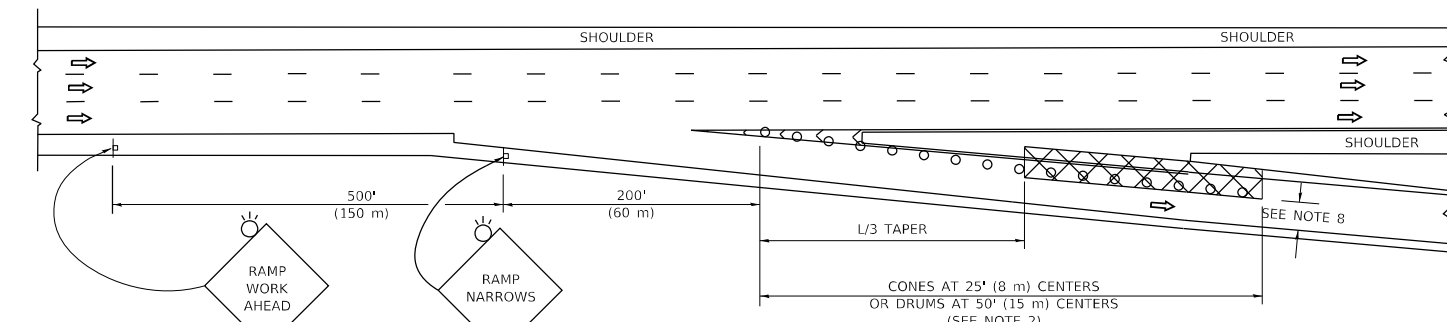
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

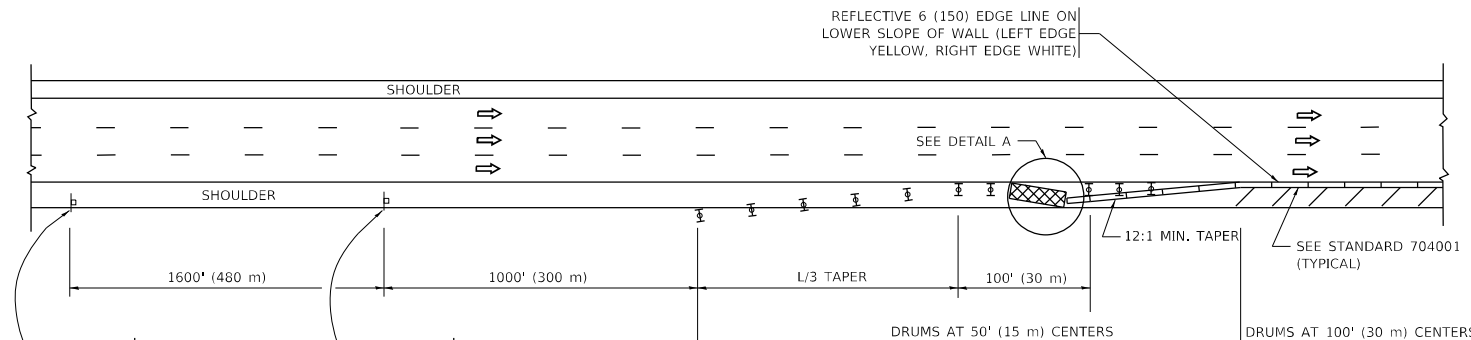
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

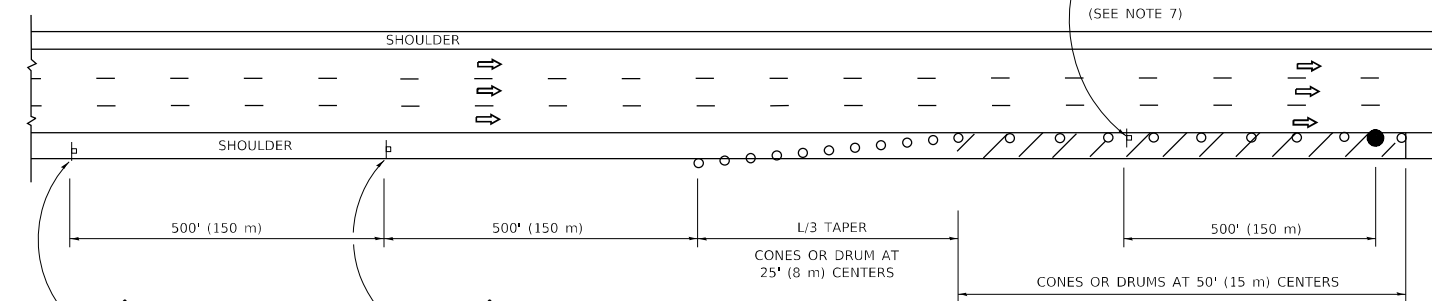
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h)	METRIC ENGLISH
OR GREATER:	$L=0.65(W)(S)$ $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

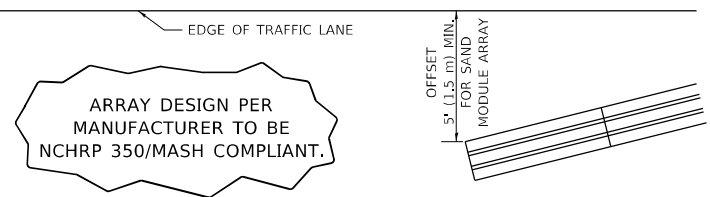


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCR OACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK AVTIVITY REQUIRES FREQUENT ENCR OACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

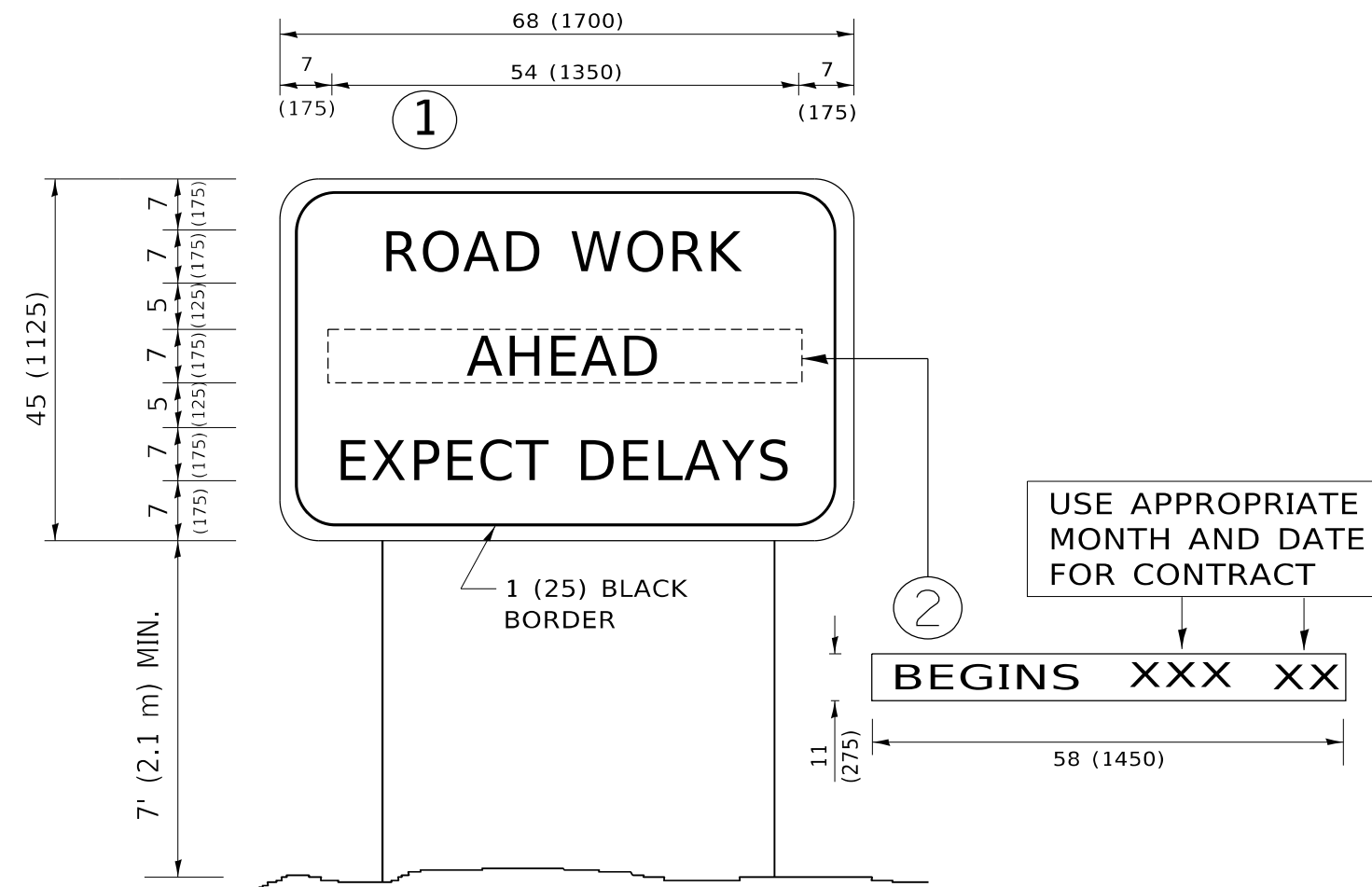
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

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PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - M.D. 06-13
PLOT DATE = 3/4/2019	DATE - 11-96	REVISED - M.D. 01-18

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22	COOK	83	76
TC-17		CONTRACT NO. 62R90		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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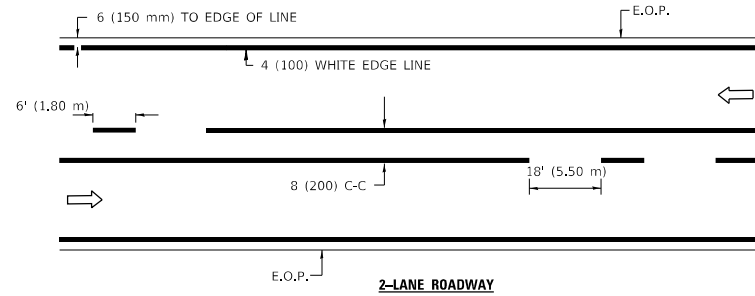
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PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

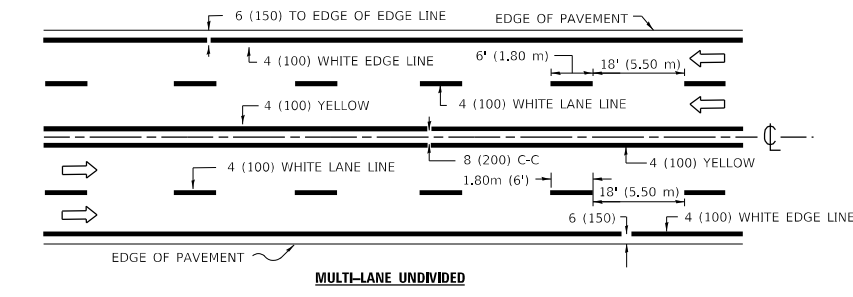
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

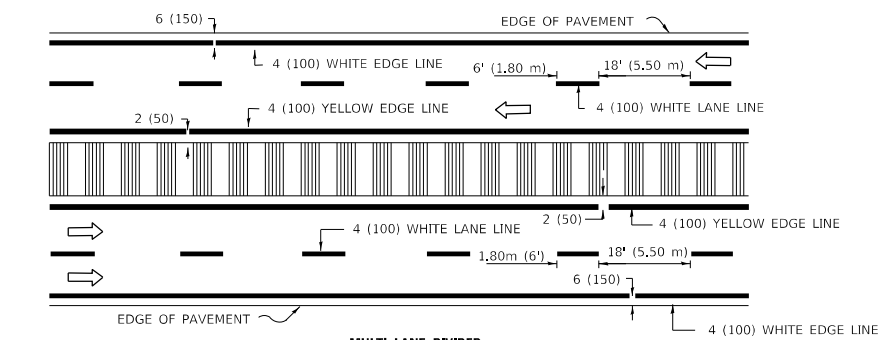
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22	COOK	83	77
TC-22			CONTRACT NO. 62R90	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



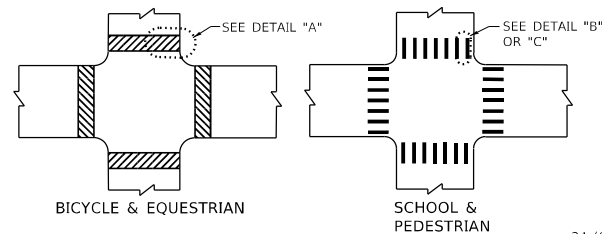
MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

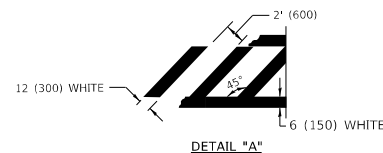
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

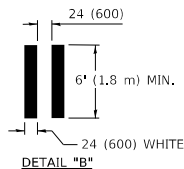


BICYCLE & EQUESTRIAN

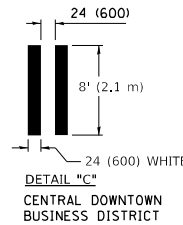
SCHOOL & PEDESTRIAN



DETAIL "A"

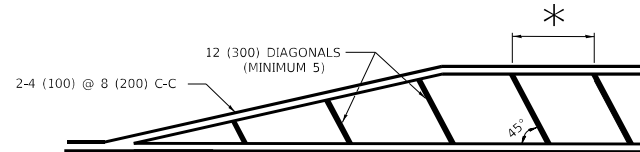


DETAIL "B"



DETAIL "C"

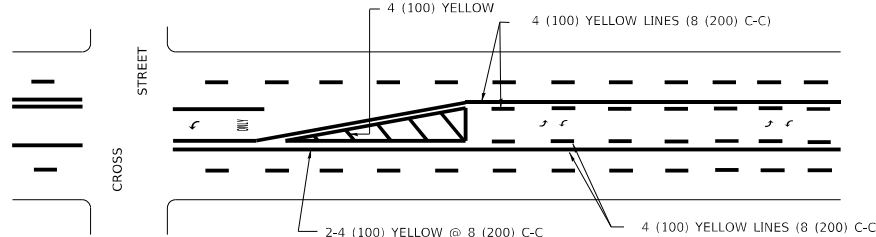
TYPICAL CROSSWALK MARKING



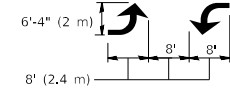
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

PAINTED MEDIANS

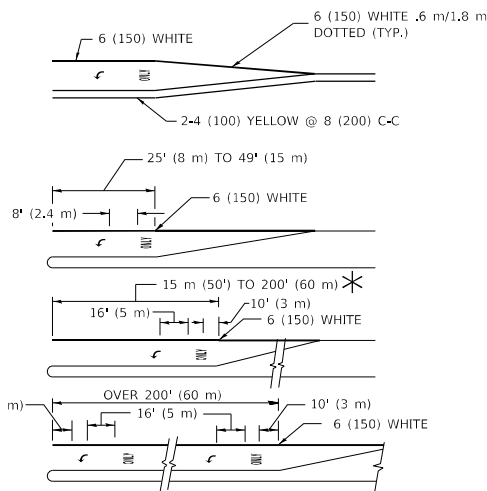


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

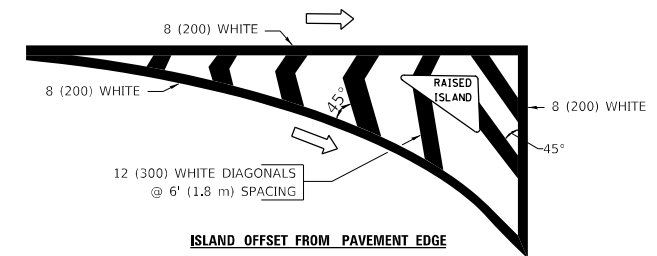


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)

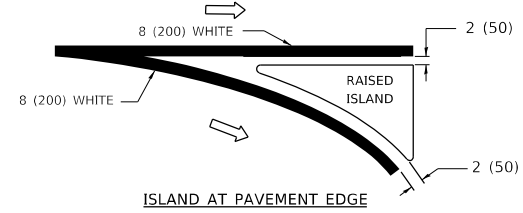
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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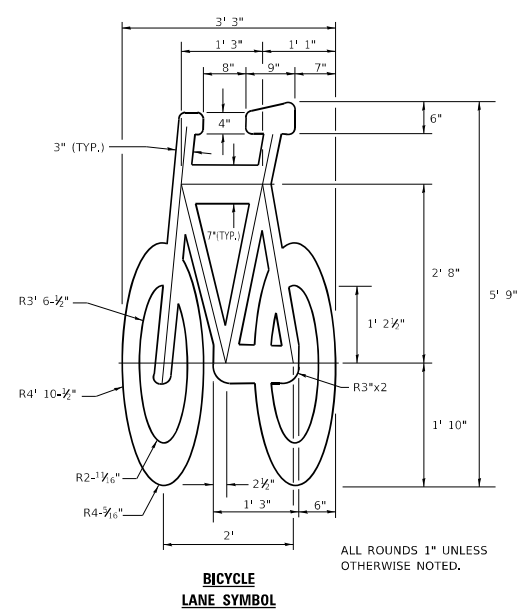
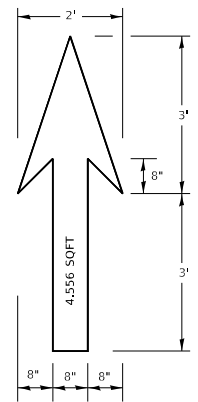
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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS**

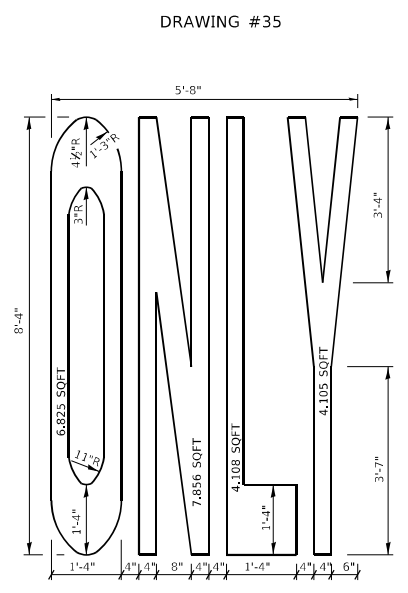
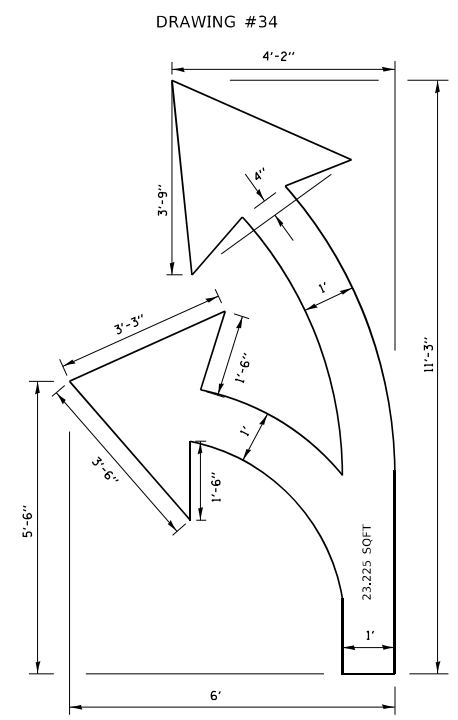
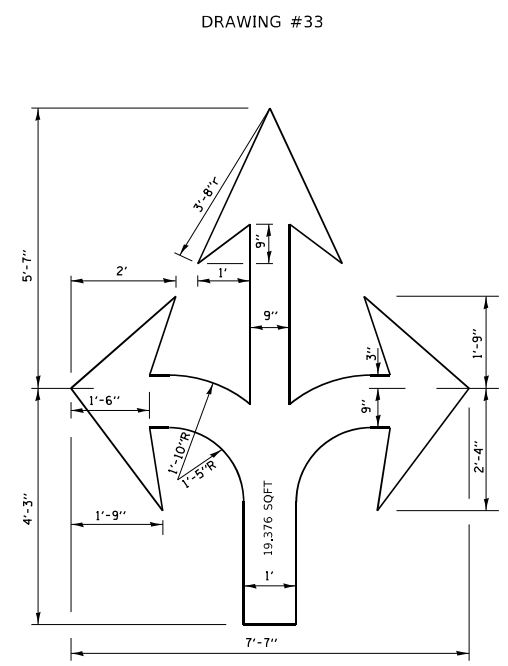
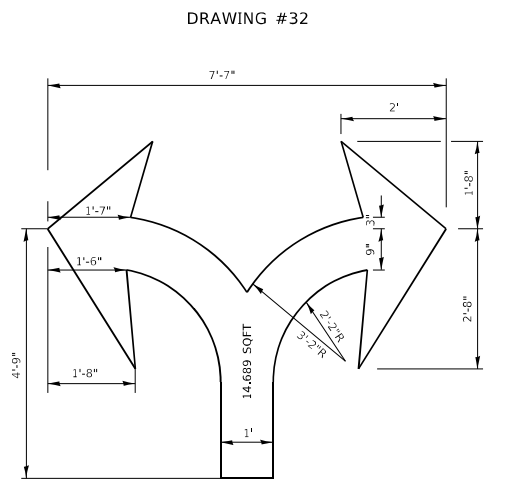
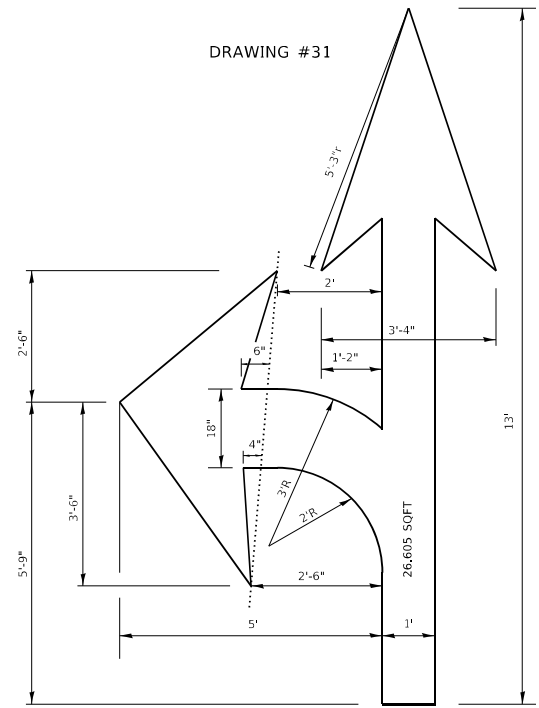
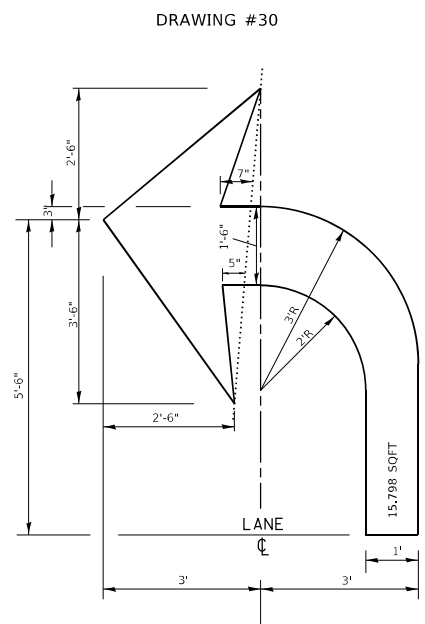
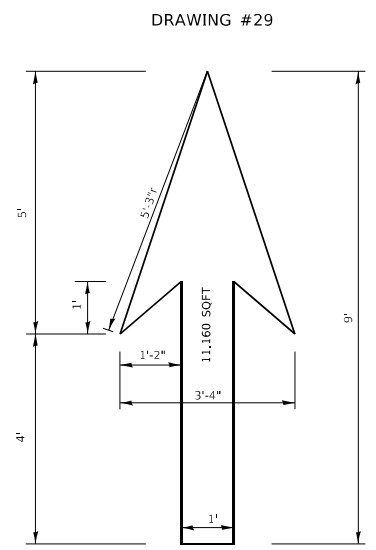
SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9094	FAI 90/94 22	COOK	83	78
TC-24		CONTRACT NO. 62R90		
ILLINOIS		FED. AID PROJECT		



- NOTE:**
1. FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 2. THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

MODEL: Default
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PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

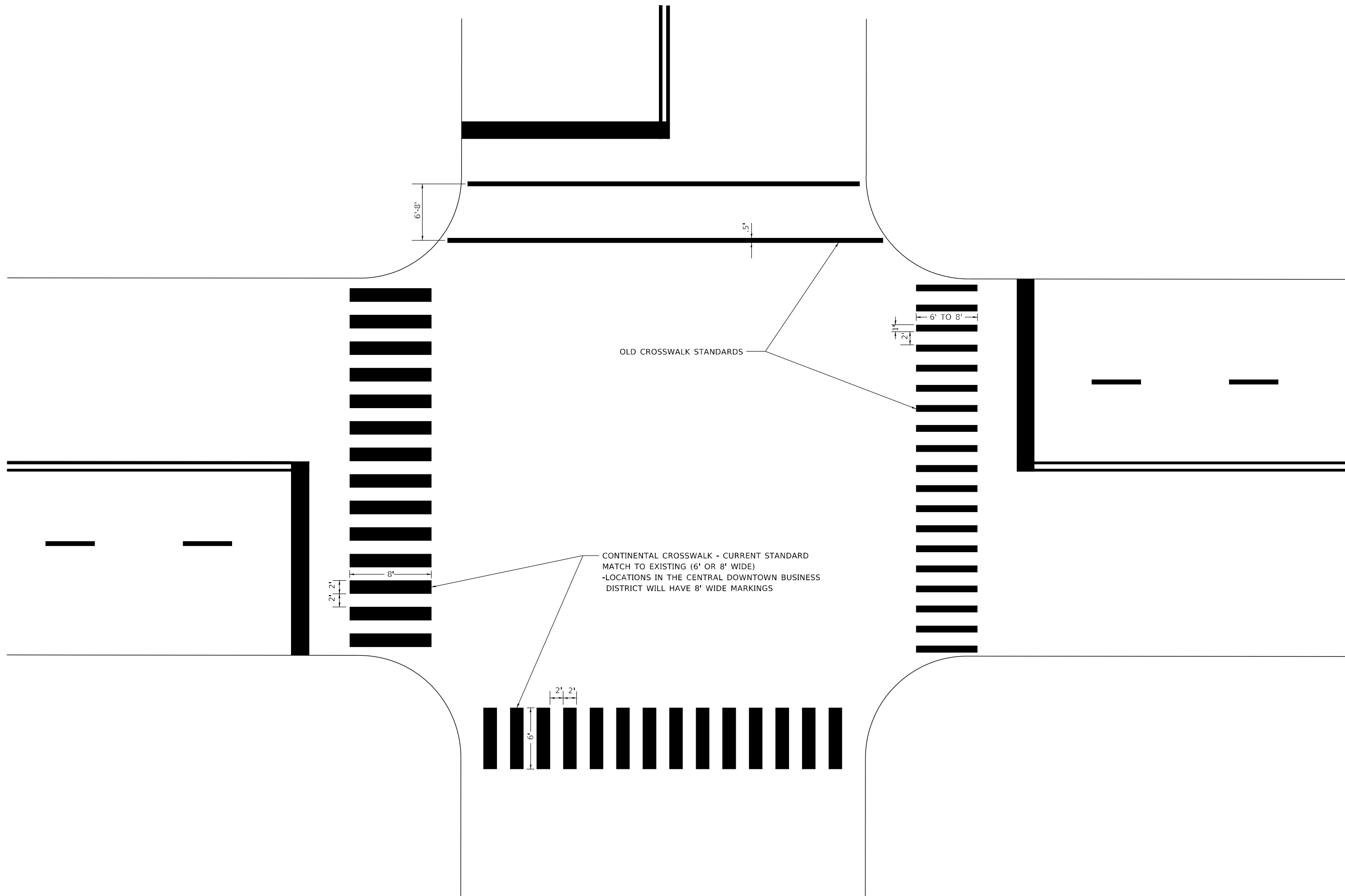
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9094	FAI 90/94 22	COOK	83	79
TC-24		CONTRACT NO. 62R90		
ILLINOIS		FED. AID PROJECT		

MODEL: Default
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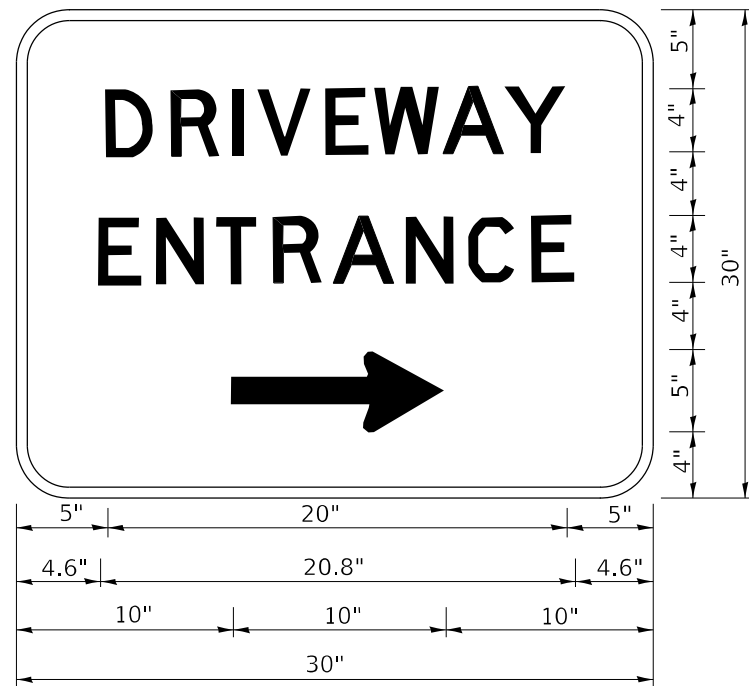
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	DRAWN -	REVISED -
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PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
 TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9094	FAI 90/94 22	COOK	83	80
TC-24			CONTRACT NO. 62R90	
ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default
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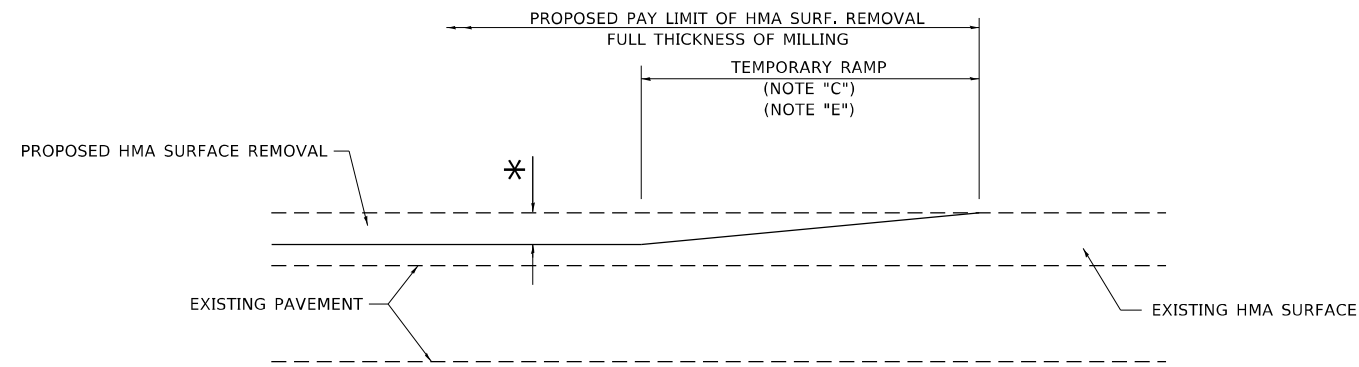
USER NAME = footemj	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

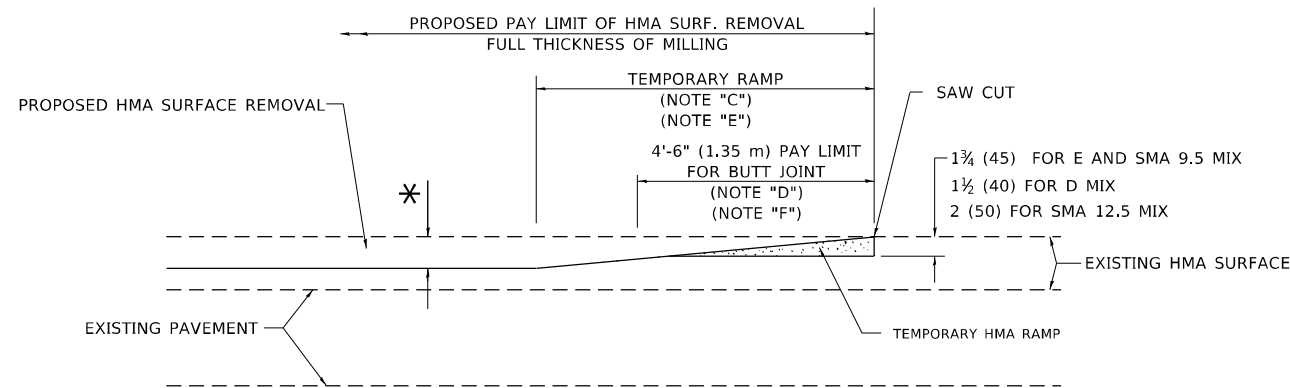
SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9094	FAI 90/94 22	COOK	83	81
TC-26		CONTRACT NO. 62R90		
		ILLINOIS	FED. AID PROJECT	



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

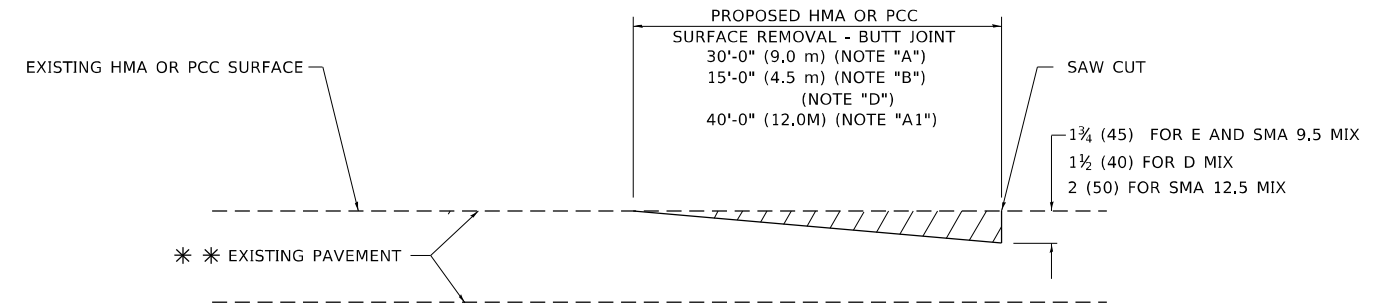
OPTION 1



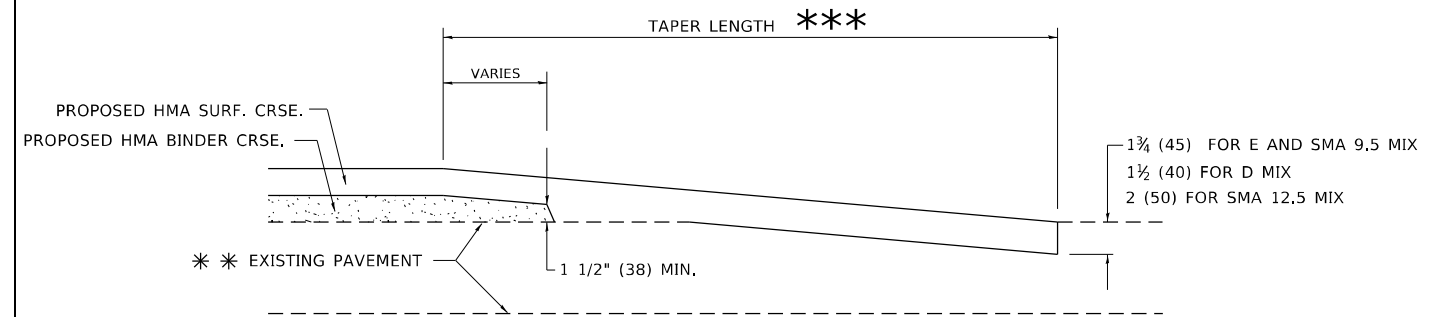
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

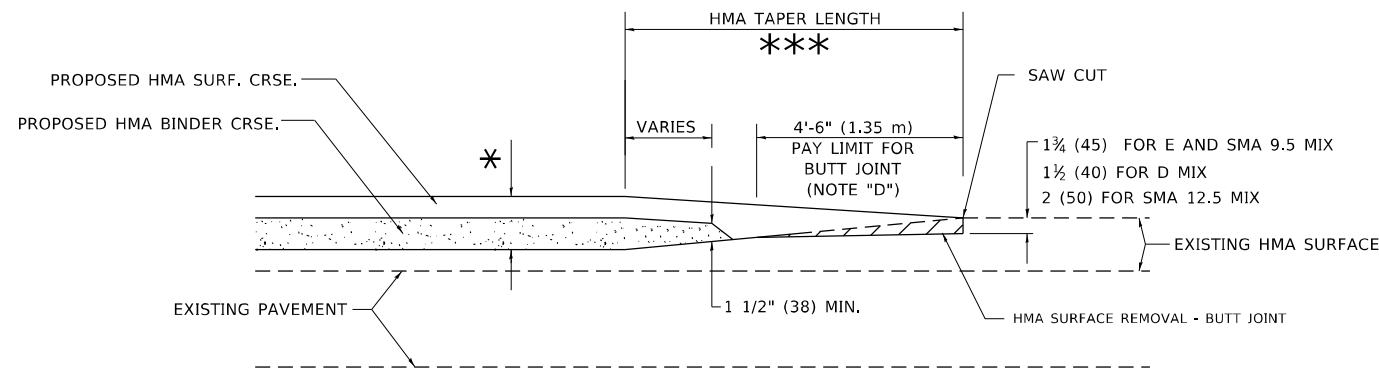
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: D:\p\h\ FILE NAME: W:\distr\022524\bd32.dgn

USER NAME = demanchell	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BORO 01-01-07
PLOT DATE = 2/2/2022	DATE - 06-13-90	REVISED - K. SMITH 02-01-22

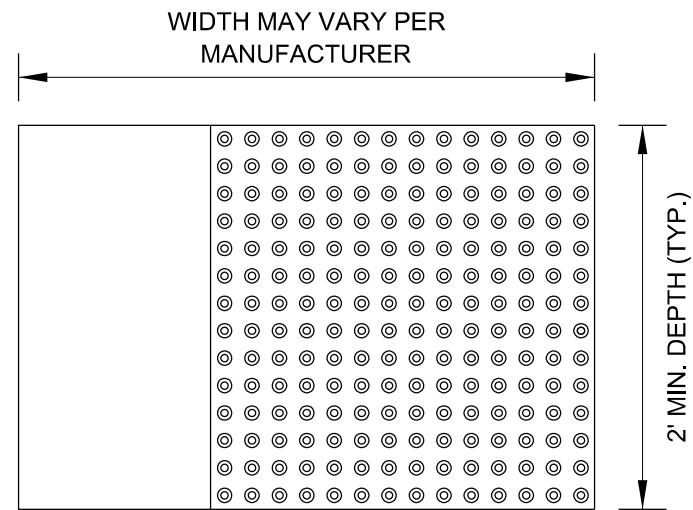
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

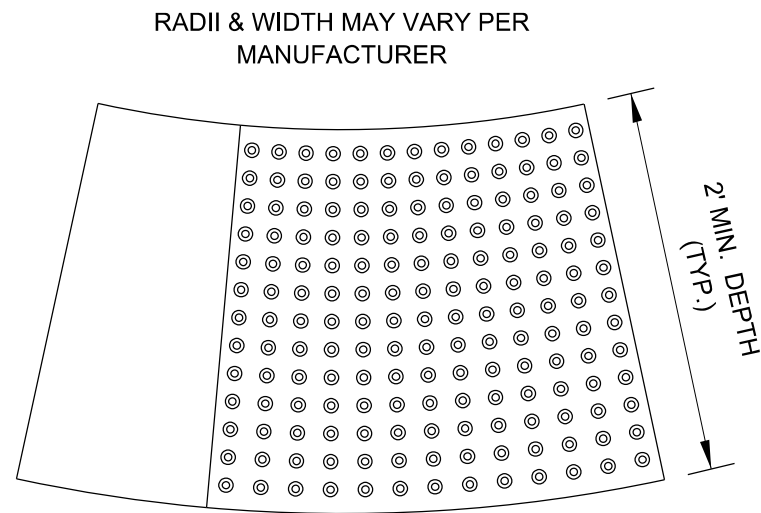
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94	FAI 90/94 22	COOK	83	82
BD400-05 BD-32		CONTRACT NO. 62R90		
ILLINOIS FED. AID PROJECT				

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS

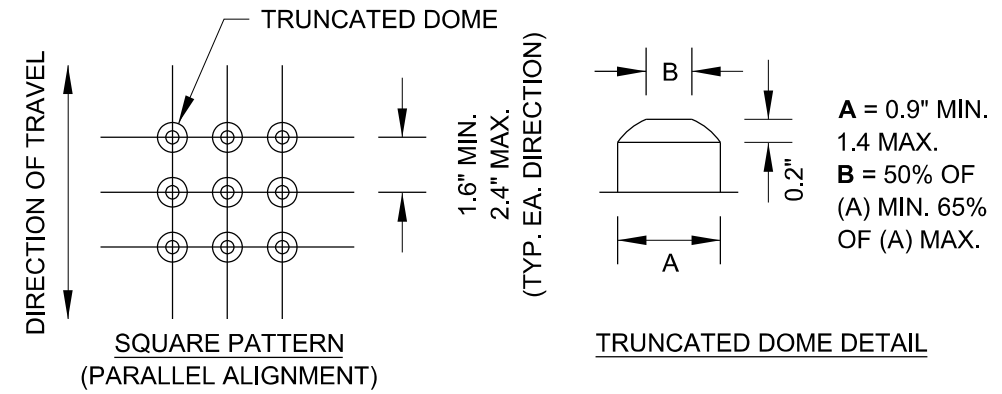


DETECTABLE WARNING UNIT SIZES

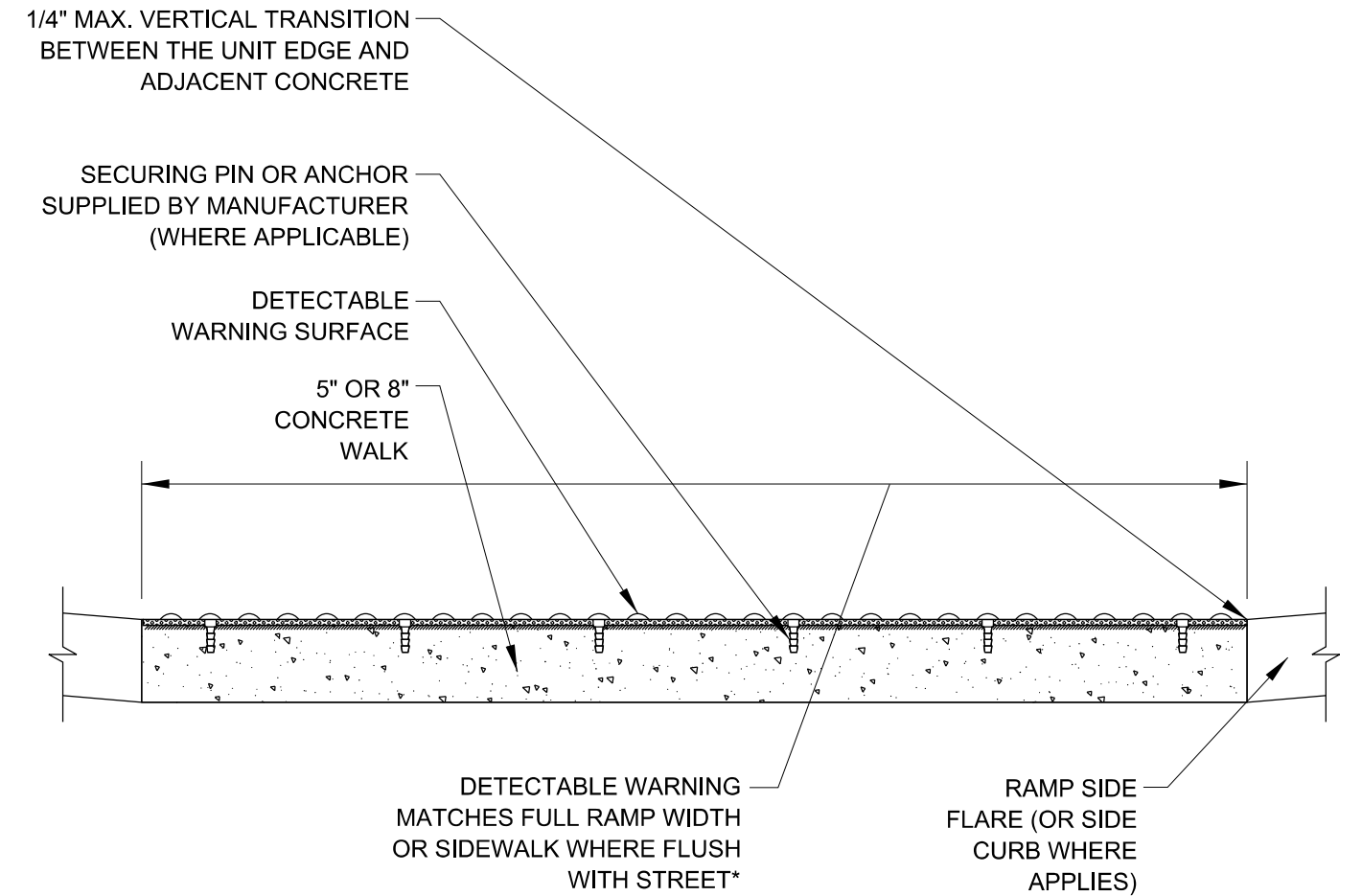
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



UNIT PATTERN & DOME DETAIL



*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

DETECTABLE WARNING UNIT SECTION

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PROJECT: I:\Projects\DH5022\3A\CADD\Drawings\Drawings\58.dwg

USER NAME = footej	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CITY OF CHICAGO DETECTABLE WARNINGS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 50.0000 ' / in.	DRAWN -	REVISED -			9094	FAI 90/94 22	COOK	83	83	
PLOT DATE = 10/8/2019	CHECKED -	REVISED -			BD 58		CONTRACT NO. 62R90			
	DATE - 06-20-2017	REVISED -			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.		