

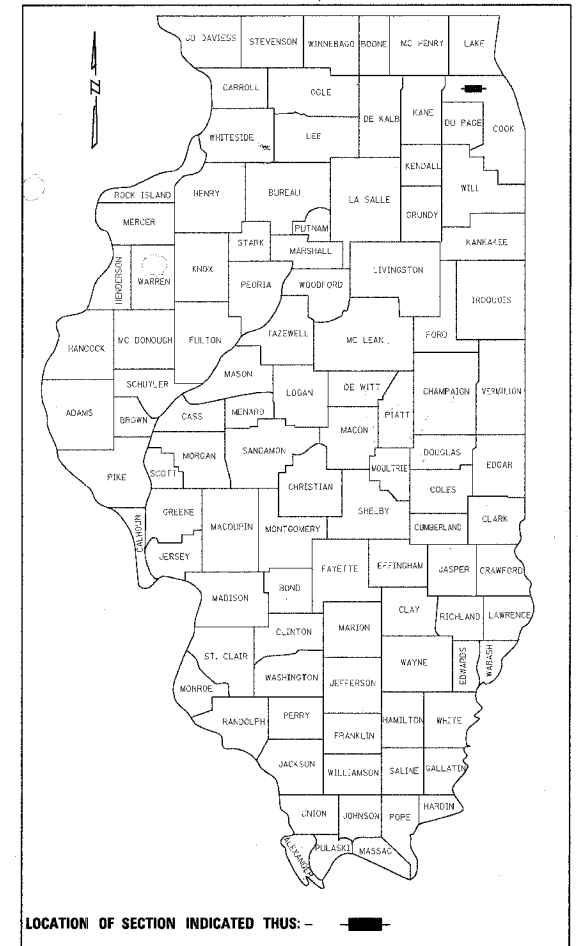
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	1
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83831				

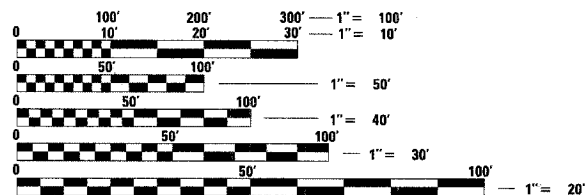
# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

(FAU 2582) OLD PLUM GROVE ROAD BIKE PATH  
COTTONWOOD PARK BIKE PATH TO  
KIRCHOFF ROAD BIKE PATH  
BIKE TRAIL CONSTRUCTION  
L.A. SECTION No. 03-00084-00-BT  
PROJECT No. CMM-8003 (266)  
C-91-042-03  
CITY OF ROLLING MEADOWS  
COOK COUNTY

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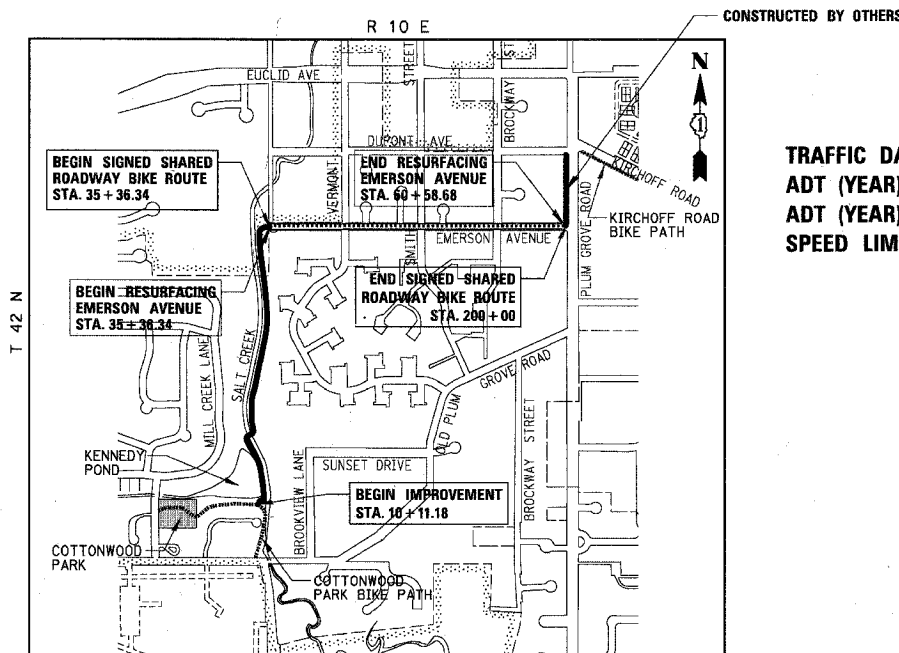
SCALES  
 PLAN 1:20  
 PROFILE HORIZ. 1:20  
 PROFILE VERT. 1:2  
 CROSS SECTIONS 1:2 VERT  
 1:5 HORIZ



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 83831



LOCATION MAP  
N.T.S.

GROSS LENGTH OF PROJECT = 5640 FEET (1.07 MI)  
NET LENGTH OF PROJECT = 5640 FEET (1.07 MI)

TRAFFIC DATA  
 ADT (YEAR) = N/A  
 ADT (YEAR) = 1500 (EMERSON AVENUE)  
 SPEED LIMIT = 25 MPH

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED November 1 2005  
*Edward A. Vogt*  
CITY OF ROLLING MEADOWS

APPROVED NOVEMBER 30 2005  
*C. Holt*  
LOCAL ROADS AND STREETS

APPROVED Dec 1 2005  
*Diane M. O'Keefe / cd*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER



**CB BURKE** CHRISTOPHER B. BURKE ENGINEERING LTD.  
9575 West Higgins Road, Suite 600  
Rosemont, Illinois 60018 (847) 823-0500

FEDERAL-AID PROGRAM ENGINEER: CHARLES RIDDLE, PE (847)-705-4406

CONTRACT NO. 83831

### GENERAL NOTES

#### UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE CITY IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE CITY MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE CITY. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

#### STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE CITY, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

#### WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE CITY.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS WITHOUT PRIOR AUTHORIZATION FROM THE CITY WATER DEPARTMENT. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

#### MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS (PRIVATE ENTRANCE).

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

THE THICKNESSES OF BITUMINOUS MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

TYPE "B" SIDEWALK RAMPS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS AND DRIVEWAYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001-01 INCLUDED IN THE SPECIFICATIONS).

DEPRESSED CURB: RAMPS OR DEPRESSED CURBS ACCESSIBLE TO THE HANDICAPPED SHALL BE PROVIDED AT ALL CROSSWALKS AND BICYCLE PATHS. NON-ADA DEPRESSED CURB SHALL BE PROVIDED AT ALL ALLEYS OR DRIVEWAYS.

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, THE WORK IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL SIGNS. ALL SIGNS AND POSTS SHALL BE REPLACED BY THE CITY AFTER CONSTRUCTION HAS BEEN COMPLETED. TEMPORARY STOP SIGNS, SPEED LIMIT SIGNS AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

ALL EROSION CONTROL BLANKET IS TO BE INSTALLED WITH SEEDING, CLASS 1 ON DISTURBED AREAS. THE PERIMETER EROSION BARRIER SHALL BE INSTALLED ALONG THE WEST SIDE OF THE PROPOSED BIKE PATH FROM STA. 10+00 TO STA. 35+00.

ALL CONSTRUCTION TRAFFIC FOR THE BIKE PATH AND BRIDGE CONSTRUCTION SHALL UTILIZE EMERSON AVENUE UNLESS OTHERWISE NOTIFIED BY THE ENGINEER OR THE VILLAGE.

### SUMMARY OF QUANTITIES

Y052

IDOT PAY ITEM	ITEM	DESCRIPTION	UNIT	QUANTITY
20100110	1	TREE REMOVAL ( 6 TO 15 UNITS DIAMETER)	UNIT	20
20100210	2	TREE REMOVAL ( OVER 15 UNITS DIAMETER)	UNIT	15
20101100	3	TREE TRUNK PROTECTION	EACH	10
20200100	4	EARTH EXCAVATION	CY	925
20201200	5	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CY	505
20700420	6	POROUS GRANULAR EMBANKMENT, SUBGRADE	CY	50
20800250	7	TRENCH BACKFILL, SPECIAL	CY	15
21101505	8	TOPSOIL EXCAVATION AND PLACEMENT	CY	215
25000100	9	SEEDING, CLASS 1	ACRE	0.5
25000400	10	NITROGEN FERTILIZER NUTRIENT	POUND	45
25000500	11	PHOSPHORUS FERTILIZER NUTRIENT	POUND	45
25000600	12	POTASSIUM FERTILIZER NUTRIENT	POUND	45
25100630	13	EROSION CONTROL BLANKET	SY	2000
25101504	14	WOOD CHIP MULCH 4"	SY	10050
28000400	15	PERIMETER EROSION BARRIER	FOOT	2500
28000510	16	INLET FILTERS	EACH	5
31101100	17	SUB-BASE GRANULAR MATERIAL, TYPE B	CY	0.5
35102000	18	AGGREGATE BASE COURSE, TYPE B 8"	SY	2810
40600100	19	BITUMINOUS MATERIALS (PRIME COAT)	GAL	850
40600300	20	AGGREGATE (PRIME COAT)	TON	17
42400200	21	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SF	20
42400800	22	DETECTABLE WARNINGS	SF	20
44000006	23	BITUMINOUS SURFACE REMOVAL 1 1/2"	SY	8460
44000500	24	COMBINATION CURB AND GUTTER REMOVAL	FOOT	20
50200100	25	STRUCTURE EXCAVATION	CY	38
50300225	26	CONCRETE STRUCTURES	CY	14
50800105	27	REINFORCEMENT BARS	POUND	1900
51201600	28	FURNISHING STEEL PILES HP12X53	FOOT	120
51202700	29	DRIVING STEEL PILES	FOOT	120
542A0220	30	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	91
54215550	31	METAL END SECTIONS 15"	EACH	6
60100080	32	FRENCH DRAINS	CY	8
60603800	33	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	20
67100100	34	MOBILIZATION	L. SUM	1
70102620	35	TRAFFIC CONTROL AND PROTECTION, STANDARD T01501	L. SUM	1
70102625	36	TRAFFIC CONTROL AND PROTECTION, STANDARD T01606	L. SUM	1
70102635	37	TRAFFIC CONTROL AND PROTECTION, STANDARD T01701	L. SUM	1
70102640	38	TRAFFIC CONTROL AND PROTECTION, STANDARD T01801	L. SUM	1
72000100	39	SIGN PANEL - TYPE 1	SF	55
72900100	40	METAL POST - TYPE A	FOOT	232
78000200	41	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2600
X0323426	42	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	6
X4066414	43	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50	TON	1235
XX001439	44	LOCKABLE REMOVABLE POST	EACH	1
XX003938	45	ERECTING AND ASSEMBLING PREFABRICATED BRIDGE SUPERSTRUCTURE	EACH	1
XX003939	46	FURNISHING PREFABRICATED BRIDGE SUPERSTRUCTURE	EACH	1
Z0013798	47	CONSTRUCTION LAYOUT	L. SUM	1
Z0076600	48	TRAINEES	HOURS	500
XX003940	49	LOW WATER CROSSING	L. SUM	1
XX001416	50	CLASS D PATCHES, SPECIAL, 6" SUPERPAVE	SY	425

\* SPECIALTY ITEMS

### IDOT STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
280001-02	TEMPORARY EROSION CONTROL SYSTEMS
424001-04	SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED
542401	METAL END SECTION FOR PIPE CULVERTS
606001-02	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701606-04	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE FOR SPEEDS 45 MPH
702001-05	TRAFFIC CONTROL DEVICES
720001	SIGN PANEL MOUNTING DETAILS
720006	SIGN PANEL ERECTION DETAILS
720011	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
780001-01	TYPICAL PAVEMENT MARKINGS
B.L.R. 17-3	TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (CASE LR-1) 2-LANE, 2-WAY, RURAL, OVER 1 DAY
B.L.R. 21-6	TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<p><b>SUMMARY OF QUANTITIES, GENERAL NOTES AND IDOT STANDARDS</b></p> <p>SCALE: VERT. N.T.S. HORIZ. N.T.S.</p> <p>DATE 11/14/2005</p> <p>DRAWN BY MYG CHECKED BY LMF</p>

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	3
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

**CONTRACT NO. 83831**

**EARTHWORK SCHEDULE**

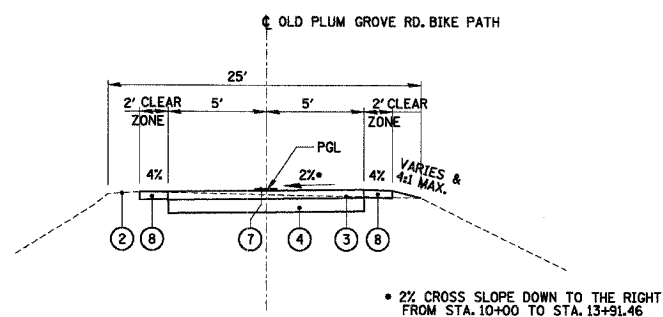
STATION	EARTH EXCAVATION CY	EARTH EXCAVATION ADJUSTED FOR SHRINKAGE (15%) CY	** EMBANKMENT CY	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) ** CY	TOPSOIL EXCAVATION AND PLACEMENT * CY	UNSUITABLE MATERIAL CY
10+50	12.0	10.2	0.9	9.3	2.8	7.4
11+00	11.1	9.4	0.9	8.5	1.9	7.4
11+50	9.3	7.9	1.9	6.0	2.8	8.3
12+00	7.4	6.3	3.7	2.6	2.8	8.3
12+50	8.3	7.1	2.8	4.3	1.9	8.3
13+00	83.3	70.8	0.9	69.9	13.0	9.3
13+50	70.6	60.1	31.8	28.3	17.5	6.5
13+85	28.9	24.6	59.0	-34.4	16.9	6.0
14+50	0.0	0.0	66.7	-66.7	5.6	4.6
15+00	0.0	0.0	75.9	-75.9	7.4	8.3
15+50	8.3	7.1	9.3	-2.2	3.7	7.4
16+00	13.0	11.0	0.9	10.1	1.9	8.3
16+50	7.4	6.3	1.9	4.4	0.0	9.3
17+00	7.4	6.3	1.9	4.4	0.0	9.3
17+50	10.2	8.7	1.9	6.8	0.0	9.3
18+00	18.5	15.7	9.3	6.5	10.2	9.3
18+50	21.3	18.1	8.3	9.8	10.2	9.3
19+00	39.8	33.8	0.0	33.8	5.6	8.3
19+50	61.1	51.9	0.0	51.9	12.0	7.4
20+00	55.6	47.2	5.6	41.7	17.6	7.4
20+50	27.8	23.6	7.4	16.2	12.0	8.3
21+00	10.2	8.7	1.9	6.8	5.6	8.3
21+50	24.1	20.5	0.0	20.5	10.2	8.3
22+00	21.3	18.1	0.0	18.1	7.4	9.3
22+50	9.3	7.9	0.9	6.9	1.9	9.3
23+00	9.3	7.9	1.9	6.0	0.9	9.3
23+50	11.1	9.4	1.9	7.6	0.9	9.3
24+00	17.6	15.0	1.9	13.1	5.6	8.3
24+50	21.3	18.1	1.9	16.3	11.1	7.4
25+00	13.0	11.0	1.9	9.2	6.5	7.4
25+50	10.2	8.7	0.9	7.7	0.9	8.3
26+00	13.0	11.0	0.0	11.0	0.0	9.3
26+50	9.3	7.9	1.9	6.0	0.0	9.3
27+00	8.3	7.1	1.9	5.2	0.0	9.3
27+50	13.9	11.8	0.0	11.8	1.9	9.3
28+00	13.9	11.8	0.9	10.9	1.9	9.3
28+50	10.2	8.7	1.9	6.8	0.0	9.3
29+00	11.1	9.4	1.9	7.6	0.9	8.3
29+50	11.1	9.4	1.9	7.6	0.9	8.3
30+00	8.3	7.1	1.9	5.2	0.0	9.3
30+50	5.6	4.7	3.7	1.0	0.9	9.3
31+00	3.7	3.1	5.6	-2.4	1.9	9.3
31+50	6.5	5.5	3.7	1.8	0.9	9.3
32+00	8.3	7.1	1.9	5.2	0.0	9.3
32+50	9.3	7.9	1.9	6.0	0.0	9.3
33+00	10.2	8.7	1.9	6.8	0.0	9.3
33+50	11.1	9.4	0.9	8.5	0.9	9.3
34+00	7.4	6.3	2.8	3.5	1.9	8.3
34+50	58.3	49.6	4.6	45.0	1.9	24.1
35+00	44.1	37.5	1.3	36.2	0.6	17.5
35+35	3.9	3.3	0.0	3.3	0.0	3.2
TOTALS=	916.0	778.6	343.9	434.7	210.9	452.7

\* PAID FOR AS TOPSOIL EXCAVATION AND PLACEMENT  
 \*\* PAID FOR AS EARTH EXCAVATION

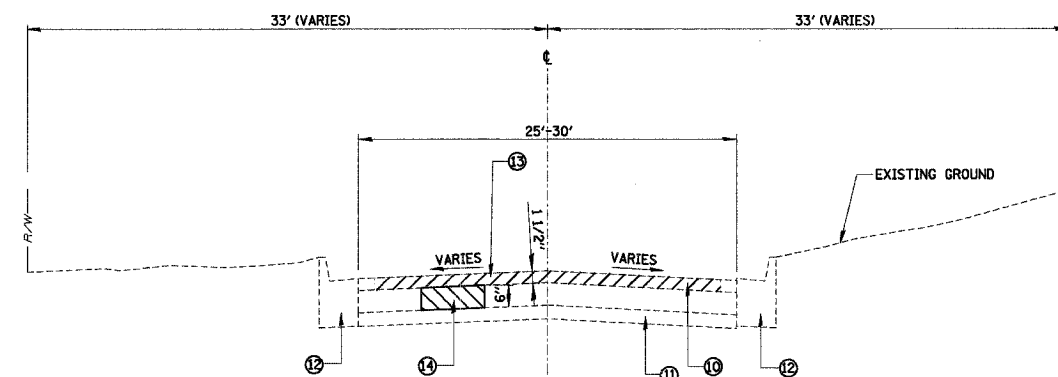
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	<b>EARTHWORK SCHEDULE</b>  SCALE: VERT. N.T.S. HORIZ. N.T.S. DATE 11/1/2005	
		SCALE: VERT. N.T.S.	DRAWN BY MYG
		DATE 11/1/2005	CHECKED BY LMF

CONTRACT NO. 83831

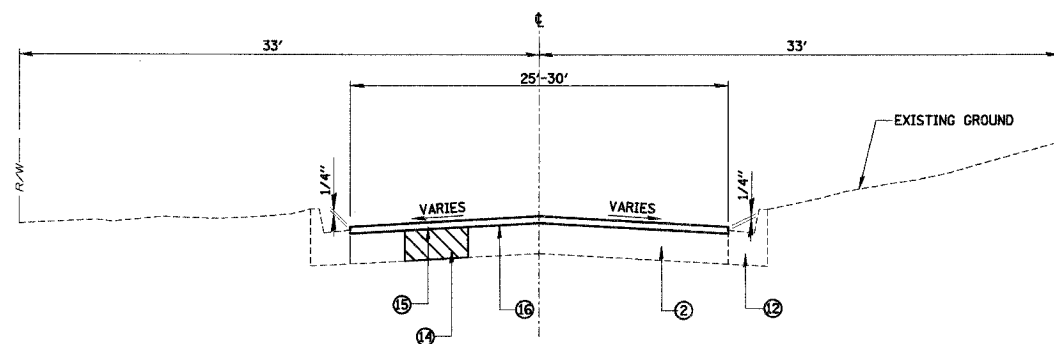
- LEGEND:
- 1 EXISTING COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.24
  - 2 EXISTING GRADE
  - 3 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C N50, 3"
  - 4 AGGREGATE BASE COURSE TYPE B - 8"
  - 5 TOPSOIL EXCAVATION AND PLACEMENT - 4"
  - 6 SODDING, SALT TOLERANT
  - 7 THERMOPLASTIC PAVEMENT MARKING- LINE 4" WHITE
  - 8 WOOD CHIPS, 4"
  - 9 EXISTING SIDEWALK
  - 10 EXISTING BITUMINOUS PAVEMENT, (±3" TO 12") SEE BORINGS
  - 11 EXISTING AGGREGATE BASE (±0-14")
  - 12 EXISTING CURB AND GUTTER
  - 13 BITUMINOUS SURFACE REMOVAL, 1 1/2"
  - 14 CLASS D PATCHES, SPECIAL, 6" SUPERPAVE (QC/QA)
  - 15 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "C" N50 (QC/QA) - 1 1/2"
  - 16 BITUMINOUS MATERIAL (PRIME COAT) AND AGGREGATE (PRIME COAT)



PROPOSED TYPICAL SECTION  
 STA. 10+11.18 TO STA. 13+91.46  
 STA. 14+81.38 TO STA. 35+36.34



EXISTING TYPICAL SECTION  
 EMERSON AVENUE  
 STA. 35+36.34 TO STA. 60+58.68



PROPOSED TYPICAL SECTION  
 EMERSON AVENUE  
 STA. 35+36.34 TO STA. 60+58.68

BITUMINOUS MIXTURE REQUIREMENTS			
ITEM	AC-TYPE	VOIDS	RAP %
BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50	PG 64-22	4%±50GYR.	15
CLASS D PATCHES, 6" SUPERPAVE	PG 64-22	4%±70GYR.	15

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL SECTIONS

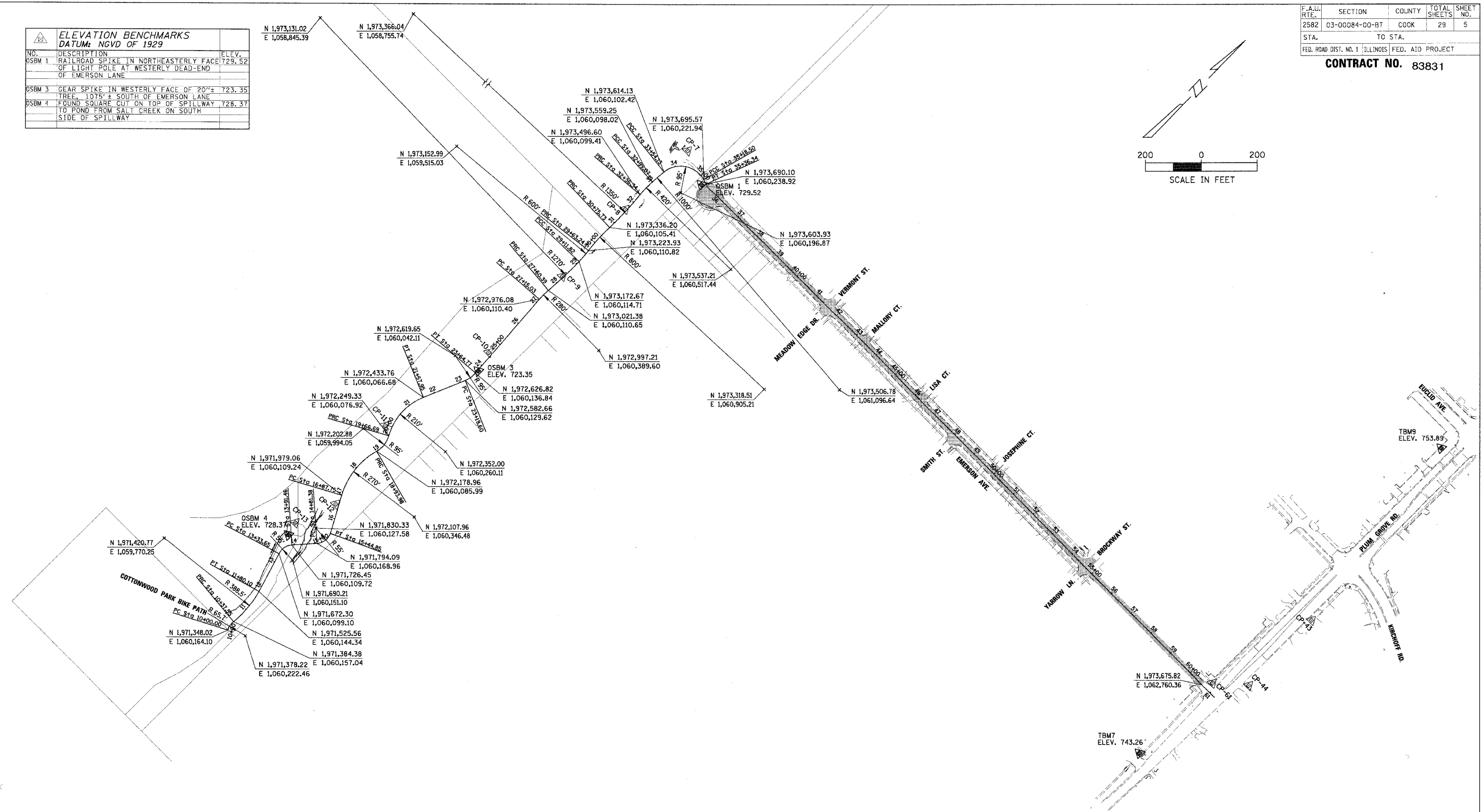
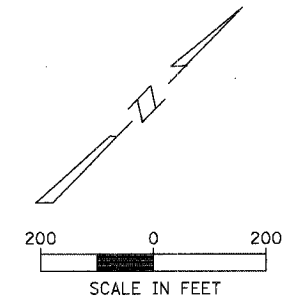
SCALE: VERT. N.T.S.  
 HORIZ. N.T.S.  
 DATE 11/4/2005

DRAWN BY MYG  
 CHECKED BY LMF

F.A.U. SECTION COUNTY TOTAL SHEET SHEETS NO.
2582 03-00084-00-BT COOK 29 5
STA. TO STA.
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

CONTRACT NO. 83831

ELEVATION BENCHMARKS DATUM: NGVD OF 1929		
NO.	DESCRIPTION	ELEV.
OSBM 1	RAILROAD SPIKE IN NORTHEASTERLY FACE OF LIGHT POLE AT WESTERLY DEAD-END OF EMERSON LANE	729.52
OSBM 3	GEAR SPIKE IN WESTERLY FACE OF 20'± TREE, 1075'± SOUTH OF EMERSON LANE	723.35
OSBM 4	FOUND SQUARE CUT ON TOP OF SPILLWAY TO POND FROM SALT CREEK ON SOUTH SIDE OF SPILLWAY	728.37



HORIZONTAL CONTROL						
NO.	NORTHING	EASTING	ELEV.	DESCR.	STATION	OFFSET
CP-7	1,973,727.51	1,060,108.42	721.91	CP-IP	34+34.12	57.0 LT
CP-8	1,973,422.10	1,060,098.43	722.55	CP-IP	31+61.91	6.13 LT
CP-9	1,973,092.57	1,060,110.79	722.66	CP-IP	28+31.58	4.02 LT
CP-10	1,972,713.34	1,060,117.79	723.31	CP-IP	24+52.48	12.46 LT
CP-11	1,972,265.55	1,060,041.41	726.81	CP-IP	19+94.91	25.16 LT
CP-12	1,971,941.11	1,060,116.86	721.56	CP-IP	16+50.17	11.42 LT
CP-13	1,971,791.92	1,060,061.51	726.60	CP-IP	14+08.95	79.41 LT
CP-43	1,974,115.67	1,062,875.44	748.71	CP-XCUT	204+10.65	92.7 RT
CP-44	1,973,792.83	1,062,880.66	743.16	CP-IP	200+90.10	95.2 RT
CP-61	1,973,706.91	1,062,783.22	741.61	CP-XCUT	200+04.07	2.4 LT

ELEVATION BENCHMARKS DATUM: NGVD 1929		
TBM 7	ON NE BOLT OF FIRE HYD. @ NORTH SIDE OF ENTERANCE OF ROLLING MEADOWS FIRE DEPT. BUILDING #2455	743.26
TBM 9	SQUARE CUT @ TOP OF LIGHT POLE CONC. FOOTING ±27 FEET FROM CURB ALONG WESTSIDE OF PLUM GROVE RD. NEAR SW. JUNCTION OF EUCLID AV.	753.89

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

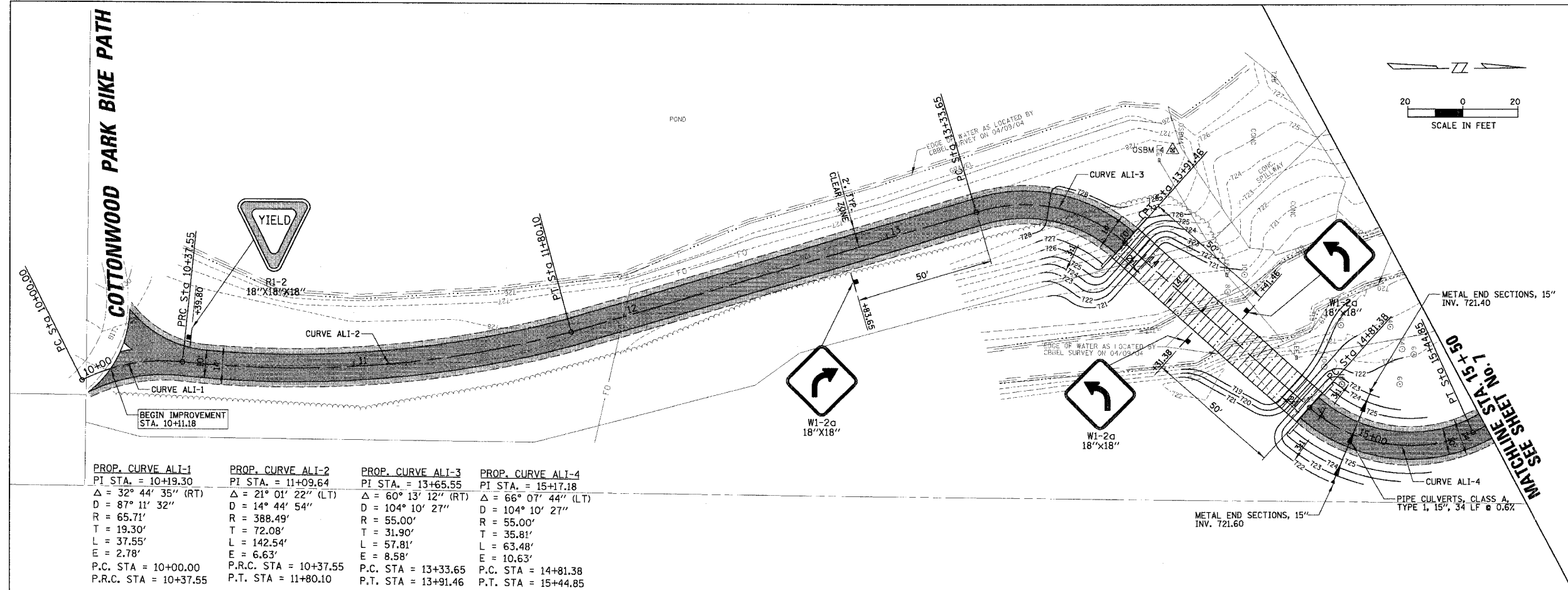
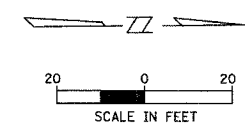
**ALIGNMENT, TIES AND BENCHMARKS**

SCALE: VERT. NOT TO SCALE  
HORIZ. DATE 11/1/2005

DRAWN BY MYG  
CHECKED BY LMF

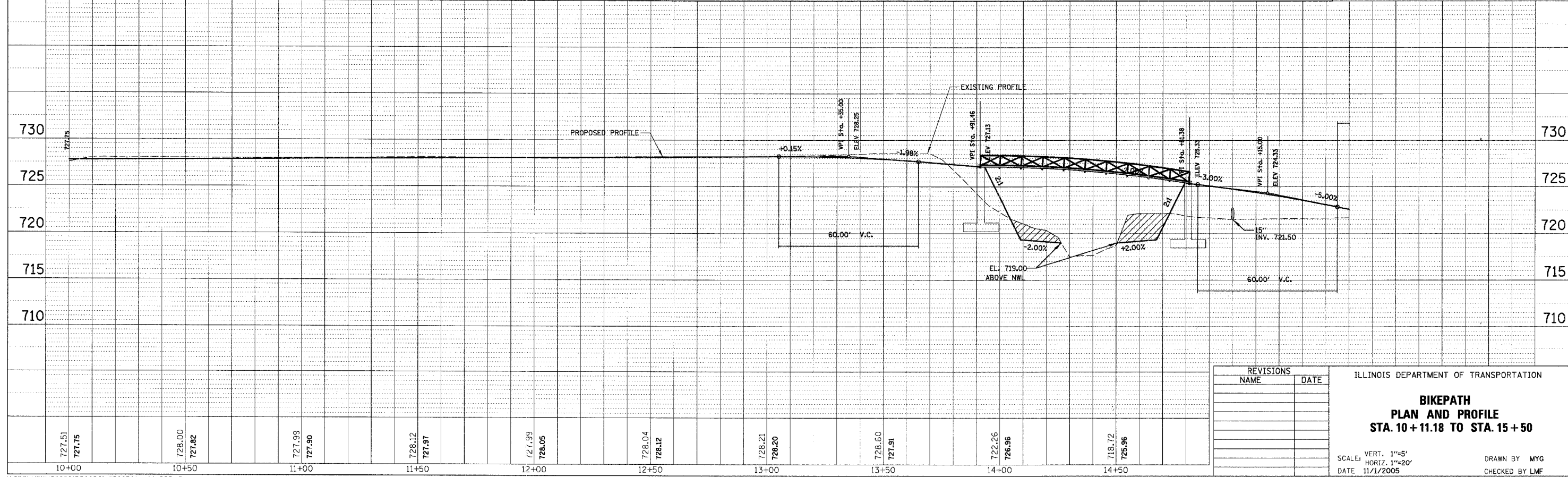
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET NO.
2582	03-00084-00-BT	COOK	29
STA.	10+11.18	TO STA.	15+50
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

CONTRACT NO. 83831



PROP. CURVE ALI-1	PROP. CURVE ALI-2	PROP. CURVE ALI-3	PROP. CURVE ALI-4
PI STA. = 10+19.30	PI STA. = 11+09.64	PI STA. = 13+65.55	PI STA. = 15+17.18
$\Delta = 32^\circ 44' 35''$ (RT)	$\Delta = 21^\circ 01' 22''$ (LT)	$\Delta = 60^\circ 13' 12''$ (RT)	$\Delta = 66^\circ 07' 44''$ (LT)
D = 87° 11' 32"	D = 14° 44' 54"	D = 104° 10' 27"	D = 104° 10' 27"
R = 65.71'	R = 388.49'	R = 55.00'	R = 55.00'
T = 19.30'	T = 72.08'	T = 31.90'	T = 35.81'
L = 37.55'	L = 142.54'	L = 57.81'	L = 63.48'
E = 2.78'	E = 6.63'	E = 8.58'	E = 10.63'
P.C. STA = 10+00.00	P.R.C. STA = 10+37.55	P.C. STA = 13+33.65	P.C. STA = 14+81.38
P.R.C. STA = 10+37.55	P.T. STA = 11+80.10	P.T. STA = 13+91.46	P.T. STA = 15+44.85

- LEGEND:**
- TREE REMOVAL
  - BITUMINOUS BIKE PATH
  - BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50 (NON OC/OA), 3"
  - AGGREGATE BASE COURSE, TYPE B, 8"



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**BIKEPATH  
 PLAN AND PROFILE  
 STA. 10+11.18 TO STA. 15+50**

SCALE: VERT. 1"=5'  
 HORIZ. 1"=20'  
 DATE 11/1/2005  
 DRAWN BY MYG  
 CHECKED BY LMF

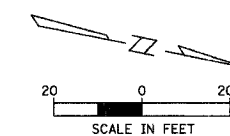
DATE	BY

DATE	BY

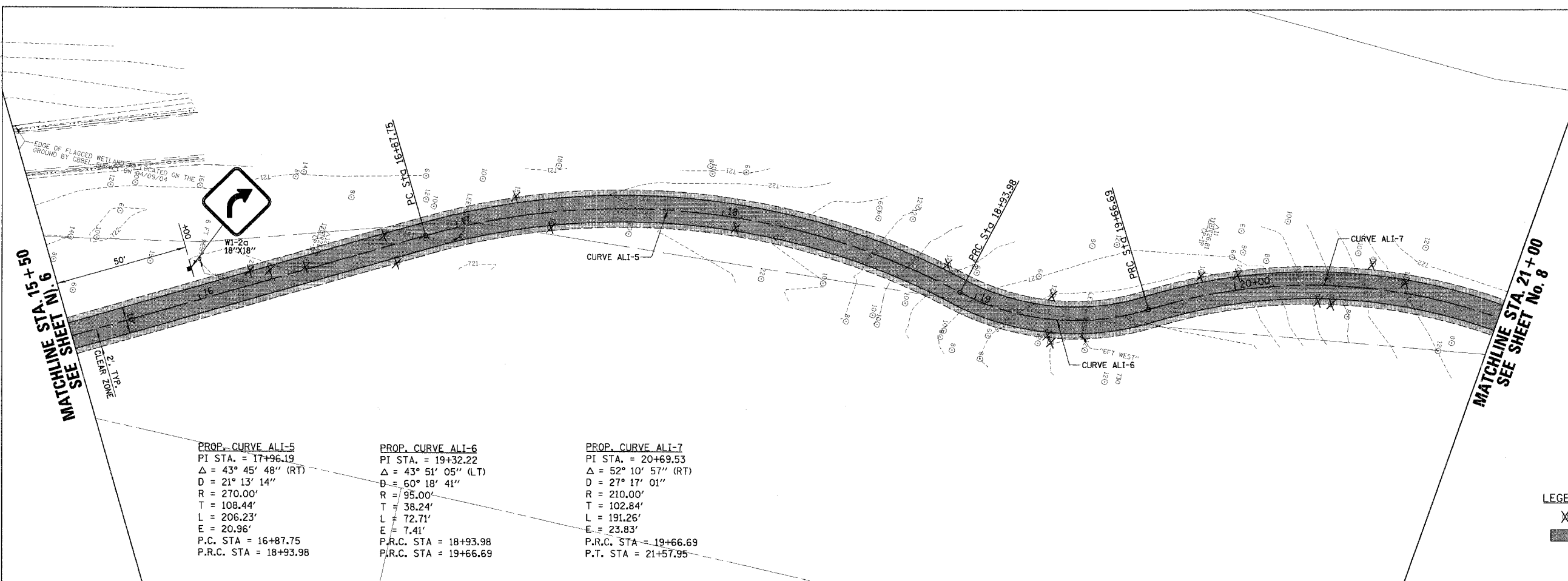
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F.A.U. RT#	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	7
STA. 15+50	TO STA. 21+00			
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

CONTRACT NO. 83831



PI	AN	DATE
SUBMITTED	BY	
REVISION	BY	DATE
NO.		



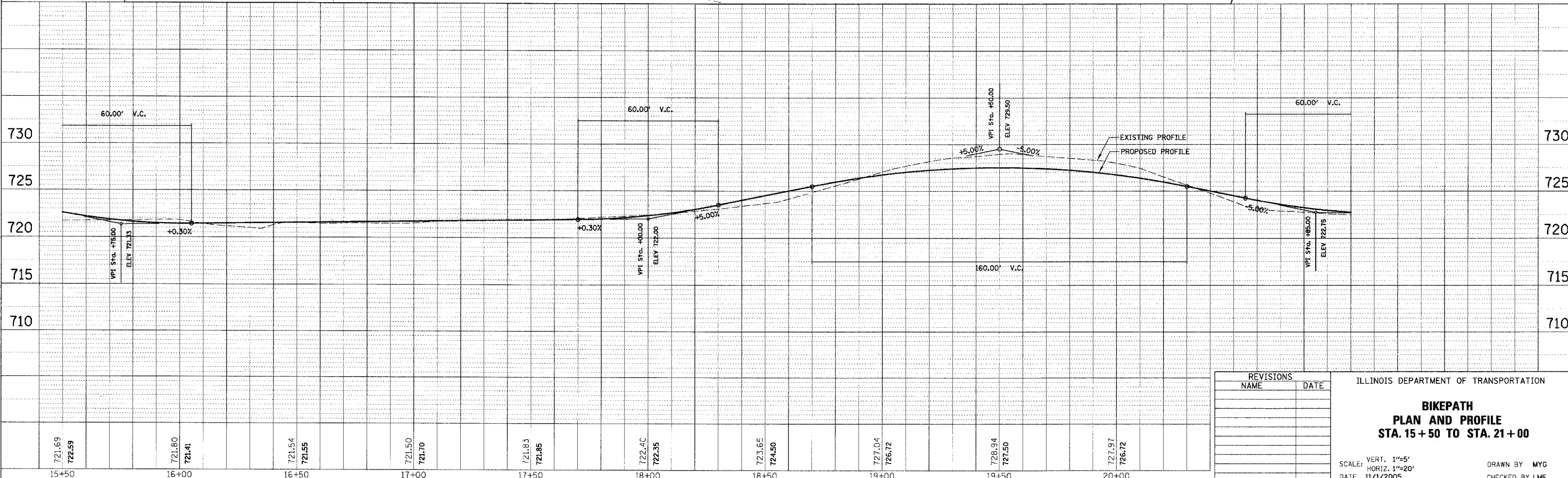
PROP. CURVE ALI-5  
 PI STA. = 17+96.19  
 $\Delta = 43^\circ 45' 48''$  (RT)  
 $D = 21^\circ 13' 14''$   
 $R = 270.00'$   
 $T = 108.44'$   
 $L = 206.23'$   
 $E = 20.96'$   
 P.C. STA = 16+87.75  
 P.R.C. STA = 18+93.98

PROP. CURVE ALI-6  
 PI STA. = 19+32.22  
 $\Delta = 43^\circ 51' 05''$  (LT)  
 $D = 60^\circ 18' 41''$   
 $R = 95.00'$   
 $T = 38.24'$   
 $L = 72.71'$   
 $E = 7.41'$   
 P.R.C. STA = 18+93.98  
 P.R.C. STA = 19+66.69

PROP. CURVE ALI-7  
 PI STA. = 20+69.53  
 $\Delta = 52^\circ 10' 57''$  (RT)  
 $D = 27^\circ 17' 01''$   
 $R = 210.00'$   
 $T = 102.84'$   
 $L = 191.26'$   
 $E = 23.83'$   
 P.R.C. STA = 19+66.69  
 P.T. STA = 21+57.95

LEGEND:  
 X TREE REMOVAL  
 BITUMINOUS BIKE PATH  
 BITUMINOUS CONCRETE SURFACE COURSE,  
 SUPERPAVE, MIX C, N50 (NON OC/OA), 3"  
 AGGREGATE BASE COURSE, TYPE B, 8"

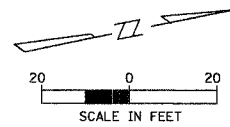
PROFILE	DATE
SUBMITTED	BY
REVISION	BY
NO.	DATE



REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**BIKEPATH**  
**PLAN AND PROFILE**  
**STA. 15+50 TO STA. 21+00**  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=20'  
 DATE 11/1/2005  
 DRAWN BY MYG  
 CHECKED BY LMF

F.A.D. RTE. 1	SECTION	COUNTY	TOTAL SHEET SHEETS, NO.
2582	03-00084-00-BT	COOK	29 8
STA. 21+00	TO STA. 27+00		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



CONTRACT NO. 83831

MATCHLINE STA. 27+00  
SEE SHEET NO. 9

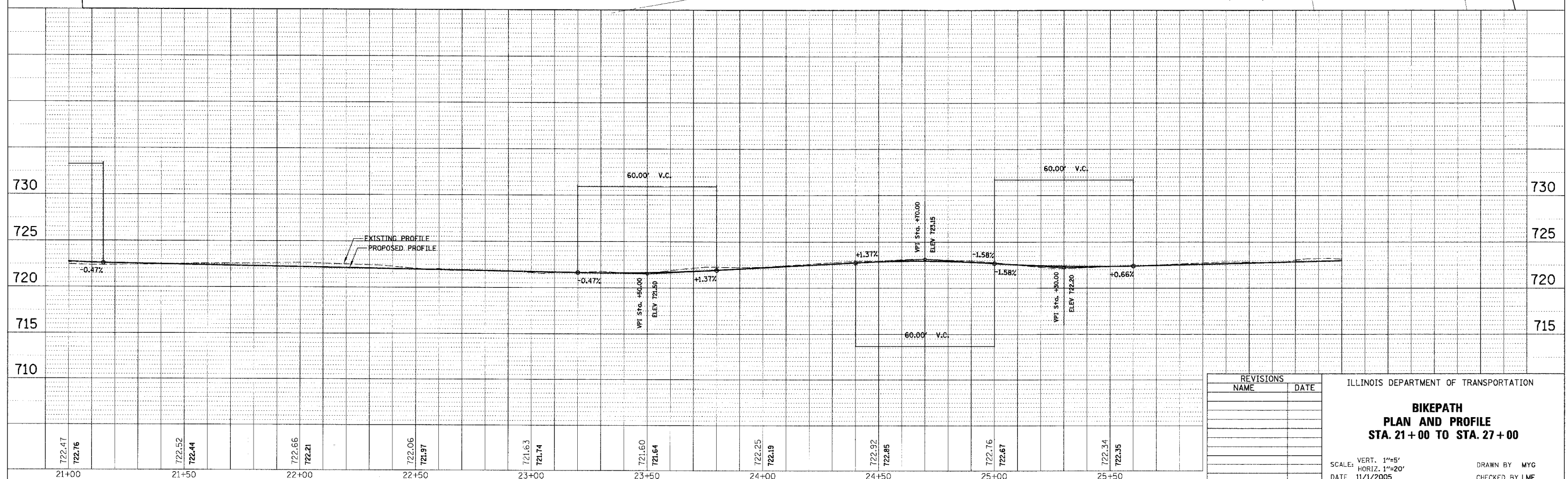
MATCHLINE STA. 21+00  
SEE SHEET NO. 7

PLAN	REVISED	BY	DATE
NOTE BOOK	NO.		
ALIGNMENT CHECKED			
FILE NAME			

**PROP. CURVE ALI-7**  
 PI STA. = 20+69.53  
 $\Delta = 52^\circ 10' 57''$  (RT)  
 $D = 27^\circ 17' 01''$   
 $R = 210.00'$   
 $T = 102.84'$   
 $L = 191.26'$   
 $E = 23.83'$   
 P.R.C. STA = 19+66.69  
 P.T. STA = 21+57.95

**PROP. CURVE ALI-8**  
 PI STA. = 23+42.62  
 $\Delta = 27^\circ 14' 34''$  (LT)  
 $D = 60^\circ 18' 41''$   
 $R = 95.00'$   
 $T = 23.02'$   
 $L = 45.17'$   
 $E = 2.75'$   
 P.C. STA = 23+19.60  
 P.T. STA = 23+64.77

**LEGEND:**  
 X TREE REMOVAL  
 BITUMINOUS BIKE PATH  
 BITUMINOUS CONCRETE SURFACE COURSE,  
 SUPERPAVE, MIX C, N50 (NON OC/OA), 3"  
 AGGREGATE BASE COURSE, TYPE B, 8"



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BIKEPATH  
 PLAN AND PROFILE  
 STA. 21+00 TO STA. 27+00**

SCALE: VERT. 1"=5'  
 HORIZ. 1"=20'

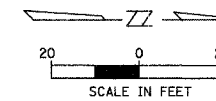
DATE 11/1/2005

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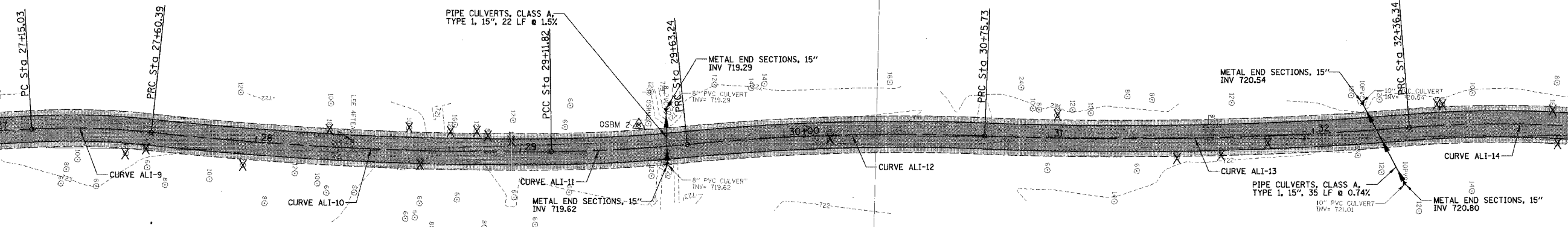
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	9
STA.	27+00	TO STA.	33+00	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 83831



REVISION	BY	DATE

MATCHLINE STA. 27+00  
SHEET NO. 8

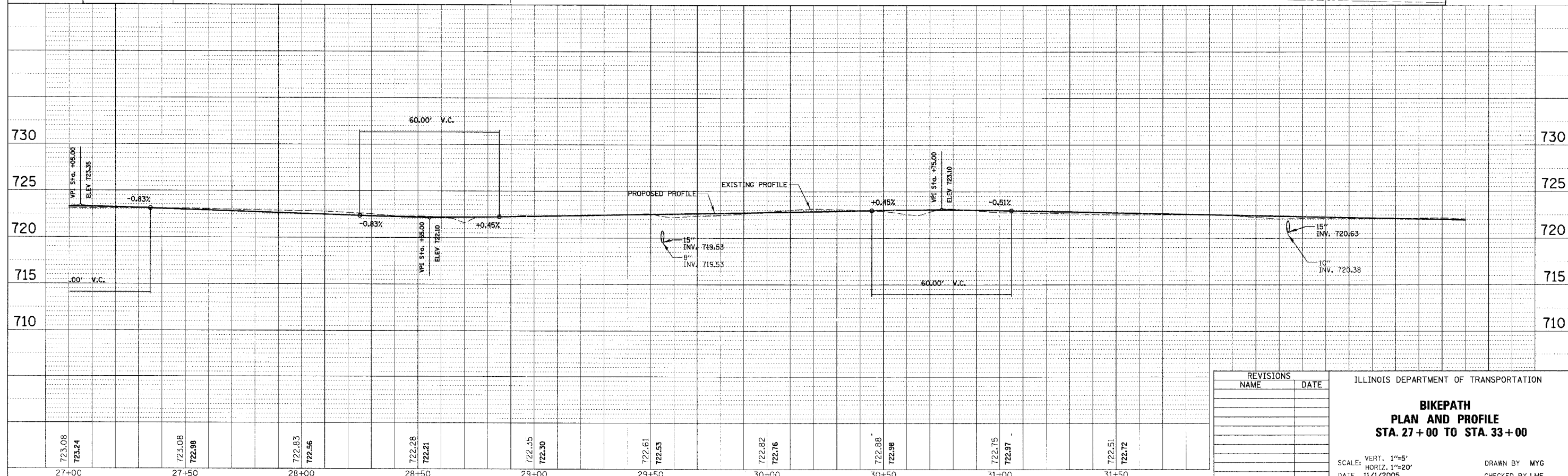


PROP. CURVE ALI-9	PROP. CURVE ALI-10	PROP. CURVE ALI-11	PROP. CURVE ALI-12	PROP. CURVE ALI-13	PROP. CURVE ALI-14
PI STA. = 27+37.76	PI STA. = 28+36.19	PI STA. = 29+37.54	PI STA. = 30+19.58	PI STA. = 31+56.13	PI STA. = 32+67.76
$\Delta = 9^\circ 16' 54''$ (RT)	$\Delta = 6^\circ 49' 54''$ (LT)	$\Delta = 4^\circ 54' 38''$ (LT)	$\Delta = 8^\circ 03' 23''$ (RT)	$\Delta = 6^\circ 48' 59''$ (LT)	$\Delta = 8^\circ 33' 28''$ (RT)
D = 20° 27' 46"	D = 4° 30' 41"	D = 9° 32' 57"	D = 7° 09' 43"	D = 4° 14' 39"	D = 13° 38' 31"
R = 280.00'	R = 1,270.00'	R = 600.00'	R = 800.00'	R = 1,350.00'	R = 420.00'
T = 22.73'	T = 75.80'	T = 25.73'	T = 56.34'	T = 80.40'	T = 31.42'
L = 45.36'	L = 151.43'	L = 51.42'	L = 112.49'	L = 160.61'	L = 62.73'
E = 0.92'	E = 2.26'	E = 0.55'	E = 1.98'	E = 2.39'	E = 1.17'
P.C. STA = 27+15.03	P.R.C. STA = 27+60.39	P.R.C. STA = 29+11.82	P.R.C. STA = 29+63.24	P.R.C. STA = 30+75.73	P.C.C. STA = 32+36.34
P.R.C. STA = 27+60.39	P.R.C. STA = 29+11.82	P.R.C. STA = 29+63.24	P.R.C. STA = 30+75.73	P.R.C. STA = 32+36.34	P.C.C. STA = 32+99.07

**LEGEND:**  
 X TREE REMOVAL  
 BITUMINOUS BIKE PATH  
 BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N50 (NON QC/QA), 3"  
 AGGREGATE BASE COURSE, TYPE B, 8"

MATCHLINE STA. 33+00  
SEE SHEET NO. 10

REVISION	BY	DATE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**BIKEPATH  
 PLAN AND PROFILE  
 STA. 27+00 TO STA. 33+00**  
 SCALE: VERT. 1"=5'  
 HORIZ. 1"=20'  
 DATE 11/1/2005  
 DRAWN BY MYG  
 CHECKED BY LMF



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	11
STA.	35+90	TO STA.	40+00	
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

**CONTRACT NO. 83831**



MATCHLINE STA. 35+90  
SEE SHEET No. 10

MATCHLINE STA. 40+00  
SEE SHEET No. 12

**EMERSON AVE.**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BIKEPATH PLAN  
STA. 35+90 TO STA. 40+00**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 11/1/2005

DRAWN BY MYG  
CHECKED BY LMF

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	12
STA.	40+00	TO STA.	46+63	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83831

MATCHLINE STA. 40+00  
SEE SHEET No. 11

MATCHLINE STA. 46+63  
SEE SHEET No. 13



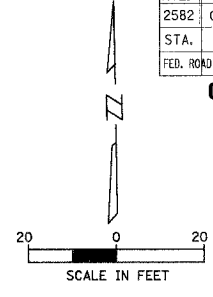
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE		

**BIKEPATH PLAN**  
**STA. 40+00 TO STA. 46+63**

VERT. SCALE:     DATE: 11/1/2005  
 HORIZ. SCALE:     DRAWN BY: MYG  
 DATE: 11/1/2005     CHECKED BY: LMF

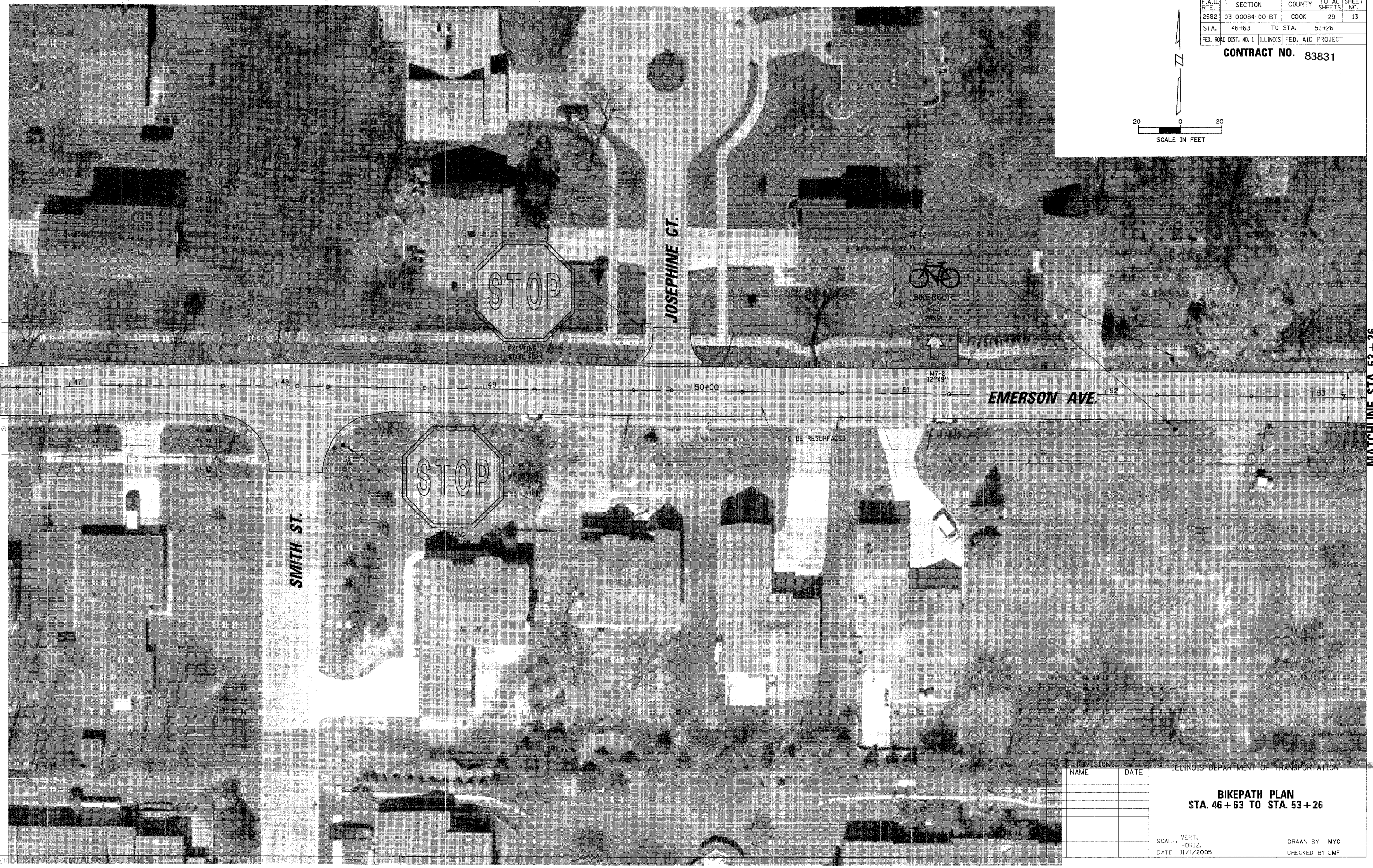
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	13
STA.	46+63	TO STA.	53+26	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

**CONTRACT NO. 83831**



MATCHLINE STA. 46+63  
SEE SHEET No. 12

MATCHLINE STA. 53+26  
SEE SHEET No. 14



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

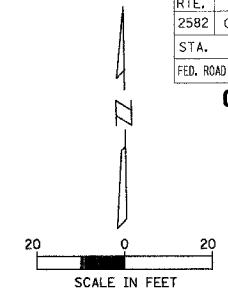
**BIKEPATH PLAN**  
**STA. 46+63 TO STA. 53+26**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 11/1/2005

DRAWN BY MYG  
CHECKED BY LMF

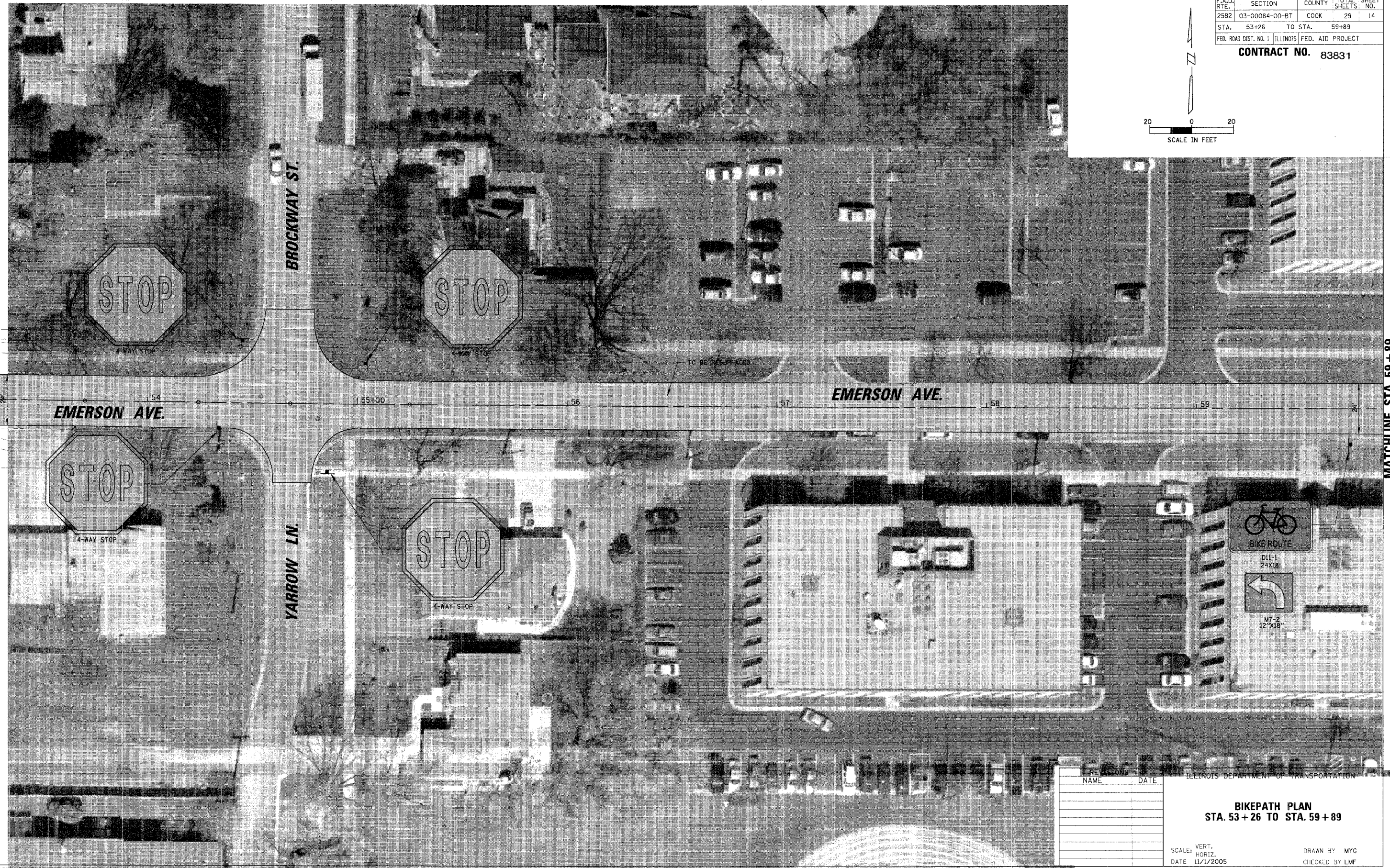
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEET SHEETS: NO.
2582	03-00084-00-BT	COOK	29 14
STA. 53+26	TO STA. 59+89		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

**CONTRACT NO. 83831**



MATCHLINE STA. 53+26  
SEE SHEET No. 13

MATCHLINE STA. 59+89  
SEE SHEET No. 15



REVISIONS	NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**BIKEPATH PLAN**  
STA. 53+26 TO STA. 59+89

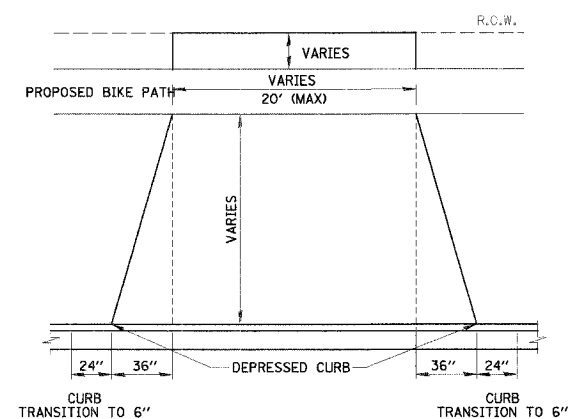
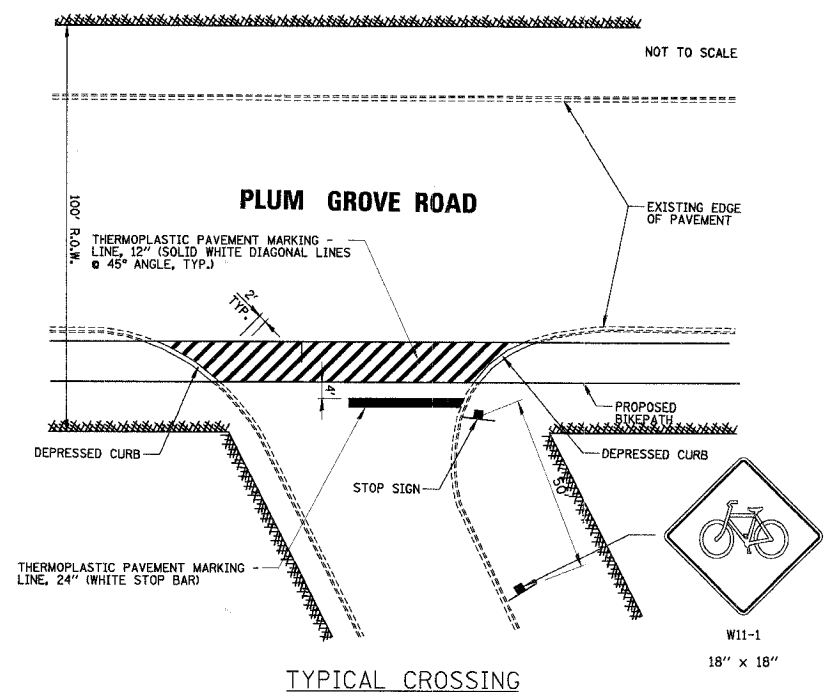
VERT. SCALE: 1"=20'  
HORIZ. SCALE: 1"=40'  
DATE: 11/1/2005

DRAWN BY: MYG  
CHECKED BY: LMF



F.A.U. / RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	16
STA. —		TO STA. —		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**CONTRACT NO. 83831**



ASPHALT DRIVE - 3" BIT. CONC. SURF. COURSE, SUPERPAVE, MIX "C", N50 (QC/QA)  
 6" AGGREGATE BASE COURSE, TYPE B  
 NOTE: BIKE PATH THROUGH DRIVEWAY SHALL HAVE SAME TYPICAL SECTION AS THE DRIVEWAY

**DETAIL OF DRIVEWAY**  
 THIS TYPICAL DRIVE LAYOUT IS FOR ASPHALT DRIVES

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**CONSTRUCTION DETAILS**

SCALE: VERT. N.T.S.  
 HORIZ. N.T.S.  
 DATE 11/1/2005

DRAWN BY MYG  
 CHECKED BY LMF



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	17
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

**CONTRACT NO. 83831**

**I GENERAL NOTES**

- Standard Specifications, Construction Plans And Subsequent Details Referenced And Presented On The Plans Are All To Be Considered As Part Of The Contract. Incidental Items Or Accessories Necessary To Complete This Work May Not Be Specifically Noted But Are Considered A Part Of This Contract.
- No Construction Plans Shall Be Used For Construction Unless Specifically Marked For Construction. Prior To Commencement Of Construction, The Contractor Shall Verify All Dimensions And Conditions Affecting The Work With The Actual Conditions. If There Are Discrepancies Between The Job Site And What Is Shown On The Construction Plans, He Must Immediately Report To Engineer Before Doing Any Work, Otherwise The Contractor Shall Assume Full Responsibility. In The Event Of Disagreement And/or Special Details, The Contractor Shall Secure Written Instruction From The Engineer Prior To Proceeding With Any Part Of The Work Affected By Omissions Or Discrepancies. In Failing To Secure Such Instruction, The Contractor Will Be Considered To Have Proceeded At His Own Risk And Expense. In The Event Of Any Doubt Or Questions Arising With Respect To The True Meaning Of The Construction Plans Or Specifications, The Decision Of The Engineer Shall Be Final And Conclusive.
- The Design For The Foundation Is Based On The Soil Report Prepared By Testing Service Corporation. File No. L-62,651 Dated March 16, 2005.
- The Contractor Is Responsible For Design, Installation And Removal Of All Excavation Support System.
- The Excavation And Work Area Shall Be Properly Drained At All Times During Construction. All Wet, Loose, Frozen Or Other Unsuitable Material Shall Be Removed Prior To Placement Of Concrete Or Compacted Backfill.
- Contractor Shall Verify All Topographic Information And Grade Elevations Adjacent To Bridge Prior To Proceeding And Inform Engineer Of Any Variation.

**II CAST-IN-PLACE CONCRETE**

- All Cast-In-Place Concrete Work Shall Be In Accordance With Section 503 Of The IDOT Standard Specifications For Road And Bridge Construction, Adopted January 1, 2002, And Supplemental Specifications And Recurring Special Provisions And As Noted Below.
- An Approved Testing Laboratory Shall Prepare Concrete Mix Designs. The Concrete Mix Design Shall Be Submitted To The Engineer For Approval A Minimum Of Seven Days Prior To Ordering Of The Concrete.
- Concrete Testing Shall Be The Responsibility Of The Contractor According To Article 1020.09 Of The IDOT Standard Specifications. Four Concrete Test Cylinders Shall Be Taken For Every Concrete Pour. Test Results Shall Be Determined By A Testing Laboratory And Provided To The Owner, Engineer And Contractor At The 7-day, 14-day And 28-day Breaks.
- All C.I.P. Concrete Shall Be Class SI Concrete And Shall Have A Minimum Compressive Strength Of 4,000 Psi @ 28 Days.
- Cover From The Face Of Concrete To Face Of Reinforcement Bars Shall Be 3" For Surfaces Formed Against Earth And 2" For All Other Surfaces Unless Otherwise Shown.
- All Reinforcing Steel Work Shall Be In Accordance With Section 508 Of The Standard Specifications.
- Reinforcement Bars Shall Conform To The Requirements Of AASTHO M-31, M-42 Or M-53 Grade 60. Field Bending Or Cutting Shall Not Be Permitted.
- Reinforcing Bar Bending Dimensions Are Out To Out.
- Reinforcing Bar Bending Details Shall Be In Accordance With The ACI "Manual Of Concrete Practice For Details And Detailing Of Concrete Reinforcement", ACI 315, Latest Edition. Shop Bending And Placement Drawings Shall Be Submitted To The Engineer For Review And Approval Prior To Fabrication.
- All Exposed Concrete Edges Shall Be Beveled 3/4".
- All Walking Surfaces Shall Receive a "Broom" Finish.

**III CONSTRUCTION**

- All Work And Materials Shall Be In Accordance With Illinois Department Of Transportation (IDOT) Standard Specifications For Road And Bridge Construction, Adopted January 1, 2002, And Latest Supplemental Specifications And Recurring Special Provisions, Unless Noted Otherwise.
- Do Not Scale Dimensions For Construction. Scale, If Shown, Applies Only To Full Size Drawings.
- No Construction Joints Will Be Allowed Unless Directed By The Engineer.
- Any Information Concerning Type Or Location Of Underground And Other Utilities Is Not Guaranteed To Be Accurate Or All Inclusive. The Contractor Is Responsible For Making His Own Determinations As To The Type And Location Of The Utilities As May Be Necessary To Avoid Damage Thereof. Contractor Shall Call J.U.L.I.E. Prior To Excavation.
- Shop, Working Or Layout Drawings (Including Reinforcement Bending And Placing Drawings) Pertaining To The Construction Of The Work, As May Be Required, Shall Be Submitted To The Engineer For Approval Prior To The Start Of Construction.
- Upon Completion, The Contractor Shall Collect And Remove All Construction Debris And Excess Material From The Site. Damaged Trees, Shrubs, And Other Landscape Features Resulting From Construction Activities Shall Be Replaced Or Repaired.
- All Bearing Surfaces Must Be True And Level.
- Contractor Must Coordinate With Bridge Manufacturer To Ensure Proper Placement Of Cast-In-Place Anchors. If The Contractor Elects To Use Post-Installed Anchors In Lieu Of Cast-In-Place Anchors, He Must Coordinate The Plate Dimensions, Bolt Spacing And Bolt Quantity With The Bridge Manufacturer Prior To Construction.

**IV PREFABRICATED PEDESTRIAN BRIDGE**

The Bridge Shall Be Designed And Fabricated In Accordance With The American Association Of The State Highway And Transportation Officials (AASHTO) Standard Specifications For Highway Bridges, 17th Edition With Current Interims, The American Welding Society (AWS) Structural Welding Code D1.1 And Bridge Welding Code D1.5, And Conform To The Rules And Standards Of The AASHTO Guide Specifications For Design Of Pedestrian Bridges. The Bridge Manufacturer Shall Have Been In The Business Of Design And Fabrication Of Bridges For A Minimum Of Five Years.

- Style: Pratt Truss Or Approved Equal.
- Bridge Shall Be Fabricated To The Length And Width Shown On The Plans.
- Loading: Dead Load Of The Bridge Plus 85 Pounds Per Square Foot Evenly Distributed Live Load (Reduced Where Applicable Per AASHTO Guide Specification For Design Of Pedestrian Bridges) Or A 10,000 Pound Concentrated Vehicle Load. Vehicle Impact Is Not Required.
- Wind Loading: The Bridges Shall Be Designed For A Wind Load Of 35 Pounds Per Square Foot On The Full Vertical Projected Area Of The Bridge As If Enclosed. The Wind Load Shall Be Applied Horizontally At Right Angles To The Longitudinal Axis Of The Structure. The Wind Load Shall Be Considered In The Design Of The Lateral Load Bracing System And In The Design Of The Truss Vertical Members, Floor Beams And Their Connections. Wind Loads Shall Also Be Considered In Top Chord Stability Per 1.3.6 Of The AASHTO Guide Specifications For Design Of Pedestrian Bridges. A Wind Overturning Force Shall Be Applied According To Article 3.15.3 Of The AASHTO Standard Specification For Bridges, 2002.
- Camber Overall Bridge Profile 2 1/2% Of The Bridge Length But At No Point Along The Bridge Shall The Deck Slope Be Greater Than 5%. Taking Into Account The Difference In Bearing Elevations. In Addition, All Truss Verticals Shall Be Plumb.
- Railings: The Top Chord Of The Trusses Shall Be Considered A Railing And Shall Be A Minimum Of 54" Above The Bridge Deck. Safety Rails Shall Be Placed On The Outside Of The Bridge With A Maximum Clear Spacing Of 4". Rub Rails Shall Be 42" From The Top Of The Deck, A Minimum Of 5" Tall And Located On The Inside Surface Of The Truss. All Railings Shall Have A Smooth Surface With No Depressions Or Protrusions Greater Than 3/8" As Per AASHTO 2.7.1.4. All Exposed Members, Railings, And Sharp Corners Shall Be Ground Smooth.
- Materials: No Structural Material Shall Be Less Than 3/16" Thick (0.1875").

- Bridge Shall Be Fabricated From Weathering Steel Conforming To The Following: Plates And Shapes: ASTM A588 Or A242 Gr.50 Or Equal  
HSS: ASTM A847 Gr.50 Or Equal  
Structural Fasteners: Astm A325- Type 3  
Weld Electrode Low Hydrogen E70XX  
  
Decking: Timber Decking Shall Be IPE Hardwood Or Approved Equal. The Wood Deck Shall Be Designed For A Minimum 100 Psf Local Loading Condition In Addition To Wheel Loads Associated With A 10,000 Lb Vehicle. Floor Planks Shall Be Attached To Supporting Members With At Least Two Plated Fasteners Per Support Point. Manufacturer Must Provide A 15 Year Warranty On Decking Material And Fasteners.
- Finishes: All Steel Shall Be Unpainted Weathering Steel Conforming To The Material Requirements Listed Above. A Minimum Corrosion Index of 6.0 Is Required. All Exposed Surfaces Of Steel Shall Be Blast Cleaned In Accordance With Steel Structures Painting Council Surface Preparation Specifications No. 7 Brush-off Blast Cleaning, SSPC- SP7 Latest Edition. Setting Plates And Anchor Bolts Shall Be Galvanized.
- Quality: The Bridge Manufacturer Shall Maintain Proper Records Assuring That All Steel, Bolts, And Materials Used Are In Accordance With Materials Specified. The Bridge Shall Be Identified And Marked With A Permanent Nameplate Showing The Manufacturer's Name, Location, Date Of Manufacture, And Load Carrying Capacity. Structural Material Shall Be Traceable To Each Bridge. All Welders Shall Be Qualified In Accordance With AWS D1.5:2002 Bridge Welding Code And AWS D1.1:2002 Structural Welding Code. All Workmanship Shall Be In Compliance With AASHTO And AISC Standard Practice. Full Penetration Weld Details Used In Shop Splices Shall Be Submitted To The Engineer To Determine Testing Required (If Any).
- Certified Copies Of All Material Certifications Shall Be Provided. The Bridge Manufacturer Shall Provide Certified Copies Of Shop Welding Procedure Specifications And Certified Copies Of Welder Qualifications I.A.W. AWS D1.5:2002 And AWS D1.1:2002.
- Bridge Manufacturer Shall Determine Sign Support Requirements And Method Of Attachment To Bridge.
- Delivery: Bridges Shall Be Delivered By Truck To A Location Nearest The Site Accessible By Roads.
- Four Sets Of Plans And Calculations Shall Be Submitted To The Engineer For Review. Submittal Shall Be Stamped And Sealed By A Structural Engineer Licensed In The State Of Illinois.

**PILE DATA (2 ABUTMENTS)**

Type	Steel HP 12 x 53
Capacity	25 Tons
Estimated Length	30 Feet
Number Required	4

Piles Shall Be Furnished And Driven In Accordance With Section 512 Of The Standard Specifications.

I Certify That To The Best Of My Knowledge, Information And Belief, This Bridge Design Is Structurally Adequate For The Design Loading Shown On The Plans. The Design Is An Economical One For The Syle Of Structure And Complies With Requirements Of The Current "AASHTO Standard Specification For Highway And Bridges".



DECEMBER 21, 2005

John P. Riley  
**JOHN P. RILEY**

ILLINOIS REGISTRATION No. 081-004427 STRUCTURAL ENGINEER  
EXPIRATION DATE: 11/30/06  
STRUCTURAL SHEETS

**BILL OF MATERIAL**

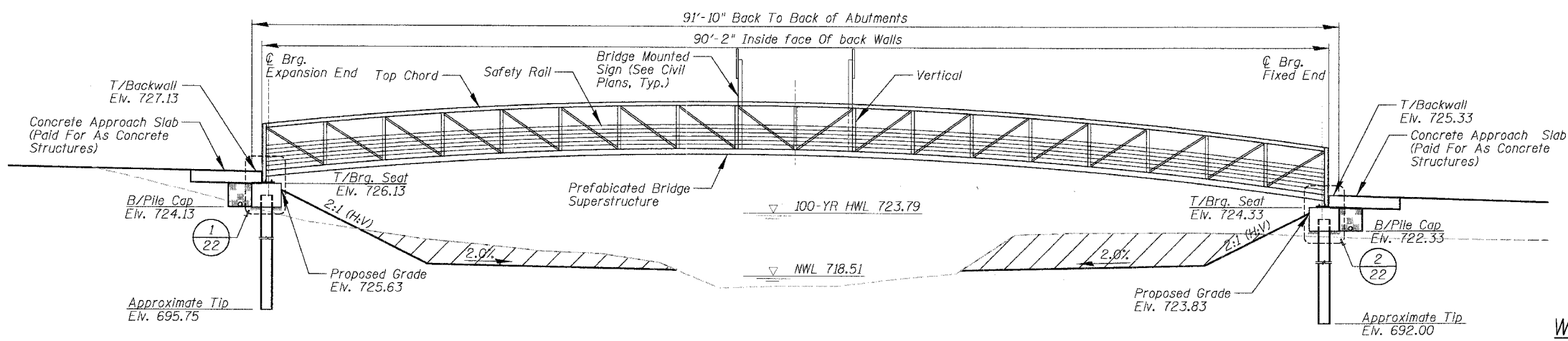
Structure Excavation	Cu. Yd.	38
Concrete Structures	Cu. Yd.	14
Reinforcement Bars	Lbs.	1900
Furnishing Steel Piles HP 12x53	Ft.	120
Driving Steel Piles	Ft.	120
Furnishing Prefabricated Bridge Superstructure	Each	1
Erecting And Assembling Prefabricated Bridge Superstructure	Each	1
French Drains	Cu. Yd.	8

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<p align="center"><b>GENERAL NOTES</b></p> <p>SCALE: VERT.      DRAWN BY PDR HORIZ.              CHECKED BY WJS DATE: 12/21/2005</p>

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-0084-00-BT	COOK	29	18
STA.	TO STA.			
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 83831

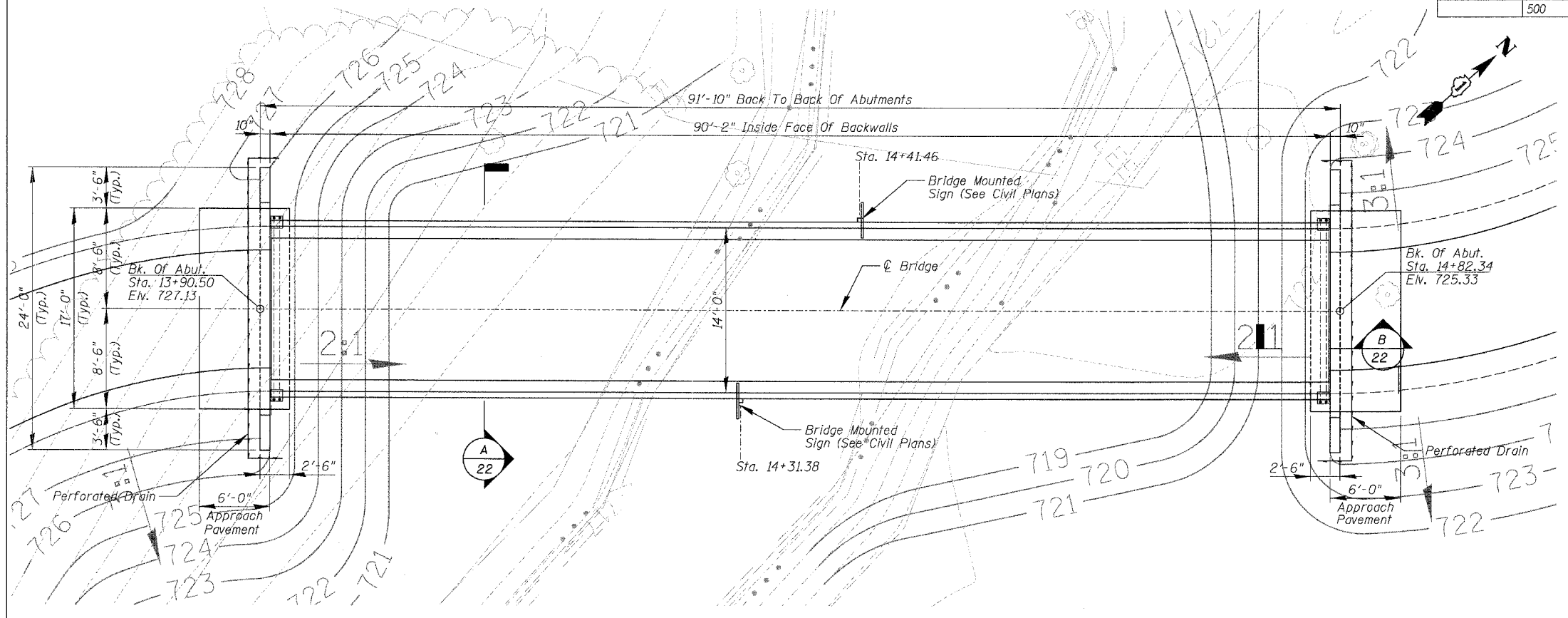
NOTE: Camber Overall Bridge Profile 2 1/2% Of The Bridge Length But At No Point Along The Bridge Shall The Deck Slope Be Greater Than 5%. Taking Into Account The Difference In Bearing Elevations. In Addition, All Truss Verticals Shall Be Plumb.



BRIDGE ELEVATION  
Scale: 3/16" = 1'-0"

WATERWAY INFORMATION TABLE

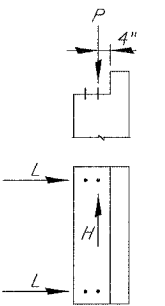
FLOOD	FREQUENCY (YR.)	DISCHARGE (CFS.)	WATER OPENING		OVERTOPPING EL. = 725.33		HEADWATER ELEVATION		
			EXISTING	PROPOSED	NATURAL H.W.E.	CREATED HEAD	EXISTING	PROPOSED	
DESIGN	10	353	N/A	195.00	722.83	N/A	0.00	N/A	722.83
BASE	50	545	N/A	238.80	723.48	N/A	0.02	N/A	723.50
OVERTOPPING	100	649	N/A	261.00	723.79	N/A	0.04	N/A	723.83
	500	940	N/A	307.30	724.58	N/A	0.13	N/A	724.71



BRIDGE PLAN  
Scale: 3/16" = 1'-0"

BRIDGE REACTION TABLE

ITEM	P (LBS) BRG.	H (LBS) ABUTMENT	L (LBS)
DEAD LOAD	13,000	---	---
UNI. LIVE LOAD	22,680	---	---
VEHICLE LOAD	5,000	---	---
UPLIFT WIND 20 PSF	-10,125	---	---
WIND	±2,155	8,915	---
THERMAL	---	---	1300



All Footings Have Been Designed Based On The Bridge Reactions Shown  
 "P"- Vertical Load Per Base Plate  
 "H"- Horizontal Load Per Footing  
 "L"- Longitudinal Load Per Base Plate

REVISIONS	NAME	DATE	ILLINOIS DEPARTMENT OF TRANSPORTATION

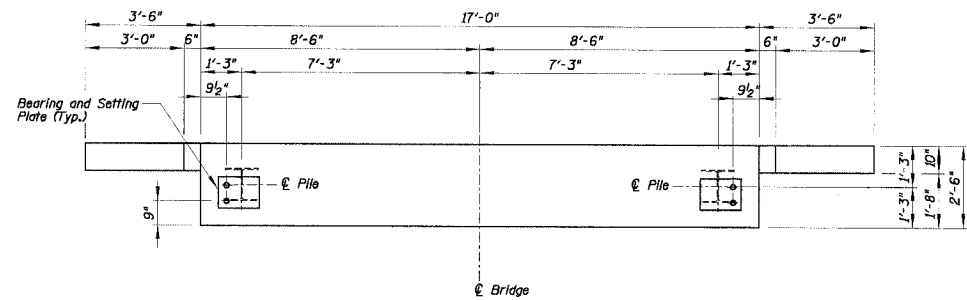
PEDESTRIAN BRIDGE  
PLAN AND ELEVATION

SCALE: VERT. 1/8" = 1'-0"  
 HORIZ. 1/4" = 1'-0"  
 DATE 11/1/2005  
 DRAWN BY PDR  
 CHECKED BY WJS

N:\R01\MDWS\83831\361R60\361R60-S2.PLN

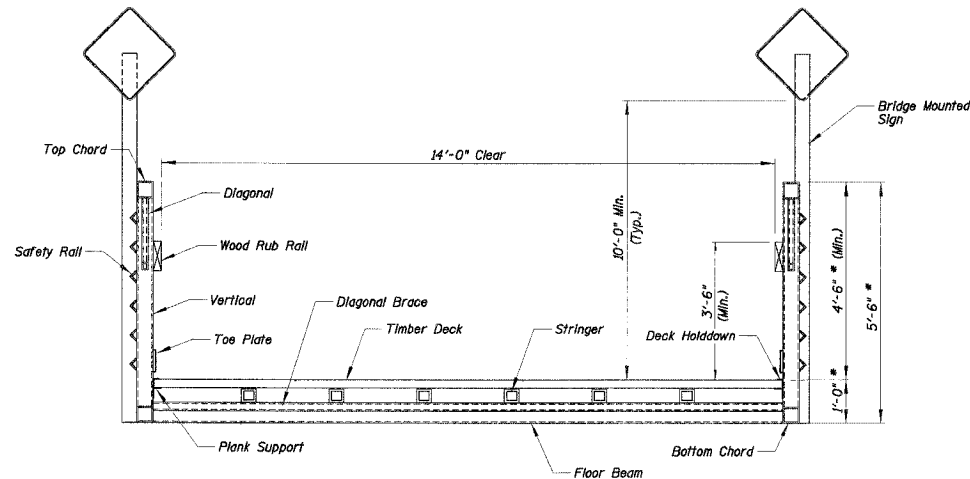
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	19
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 83831



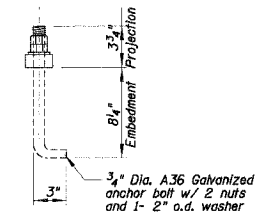
**TYPICAL ABUTMENT PLAN**

SCALE: 3/8" = 1'-0"  
(South Abutment Shown, North Is Similar)  
(Approach Slab Omitted For Clarity)



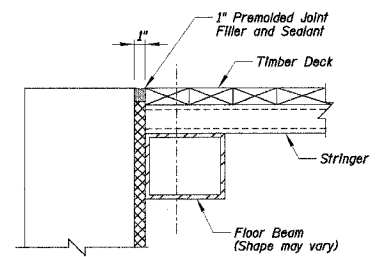
**SECTION THRU PREFABRICATED BRIDGE SUPERSTRUCTURE**

SCALE: 1/2" = 1'-0"



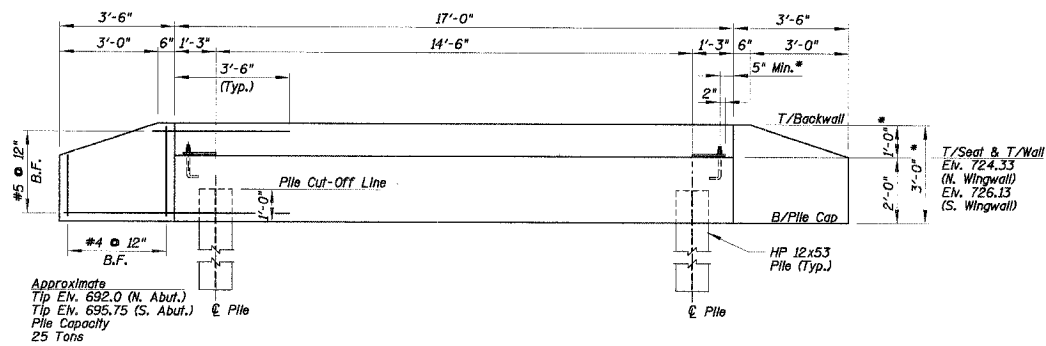
**ANCHOR BOLT DETAIL**

SCALE: 1 1/2" = 1'-0"



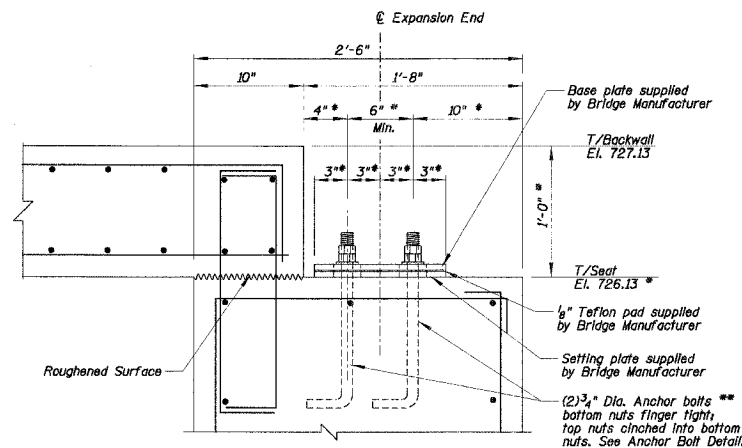
**JOINT SEAL AT ABUTMENT**

SCALE: 1 1/2" = 1'-0"



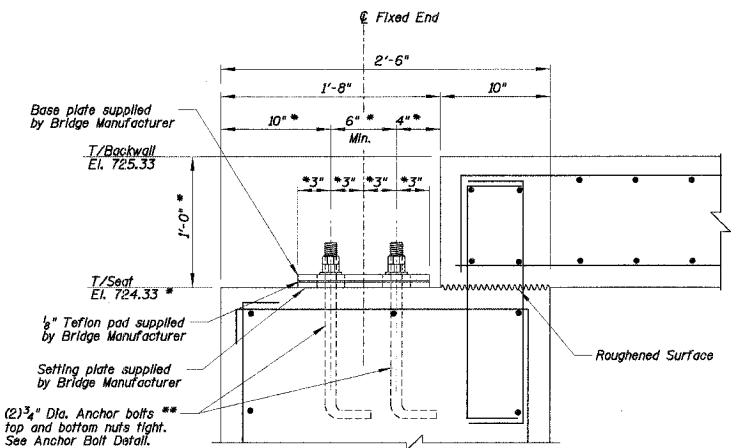
**ABUTMENT ELEVATION**

SCALE: 3/8" = 1'-0"



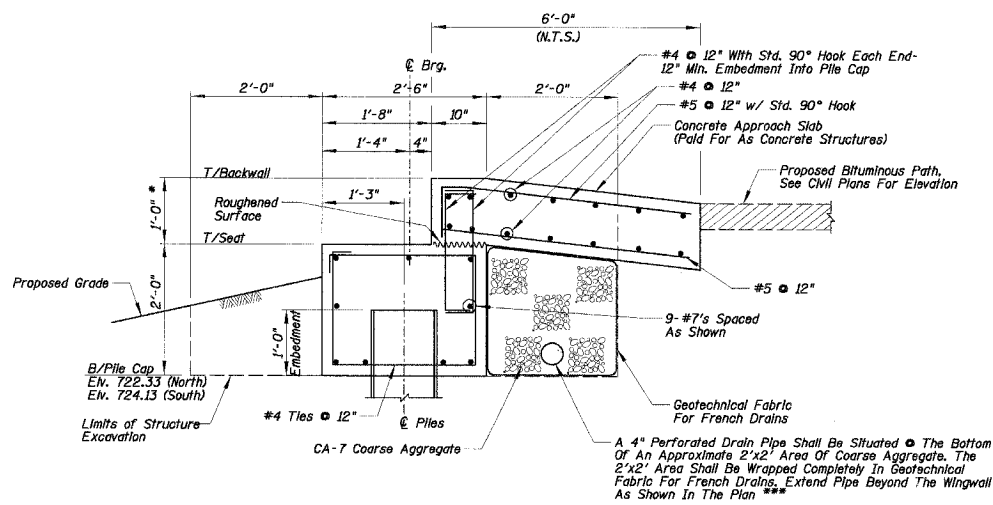
**1 EXPANSION BEARING DETAIL AT SOUTH ABUTMENT**

SCALE: 1 1/2" = 1'-0"  
(Shown Perpendicular To Abutment)



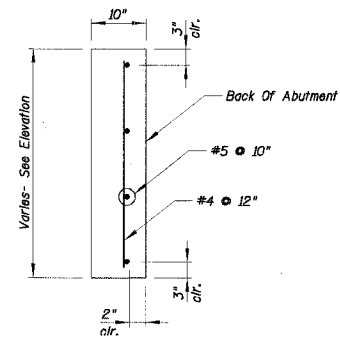
**2 FIXED BEARING DETAIL AT NORTH ABUTMENT**

SCALE: 1 1/2" = 1'-0"  
(Shown Perpendicular To Abutment)



**B TYPICAL SECTION - ABUTMENT**

SCALE: 3/4" = 1'-0"



**TYPICAL SECTION - WINGWALL**

SCALE: 3/4" = 1'-0"

**NOTES:**

- \* Contractor shall coordinate this dimension with Bridge Manufacturer prior to construction.
- \*\* Contractor has the option of substituting anchor bolts with 4-3/4" HILTI HAS-EE AISI 304 SS Bolts embedded 6 5/8" into HIT HY 150 Injection adhesive. Bolts shall not be placed less than 5" from the edge of the structure or less than 6" apart. Contractor shall coordinate plate dimensions, bolt spacing and bolt quantity with Bridge Manufacturer prior to construction.
- \*\*\* Included in the cost of French Drains.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

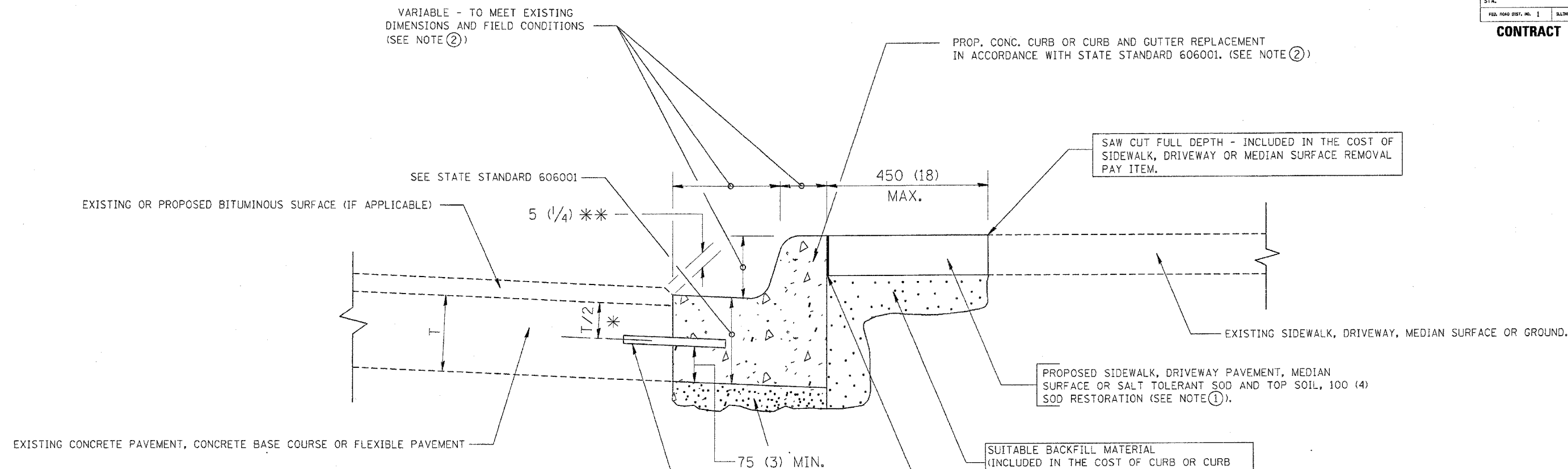
**PEDESTRIAN BRIDGE SECTIONS AND DETAILS**

SCALE: VERT. HORIZ. DATE 11/1/2005

DRAWN BY PDR CHECKED BY WJS

F. & U. ETC.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	20
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ALTERN.	FED. AID PROJECT		

CONTRACT NO. 83831



\* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSATURABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 100 (4) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 100 (4) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED NO. 20 (NO. 6) EPOXY COATED TIE BARS 600 (24) LONG AT 600 (24) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

**BASIS OF PAYMENT:**

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

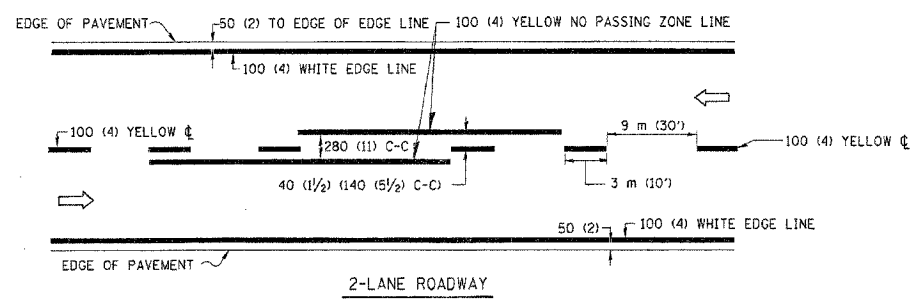
REVISIONS	
NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**CURB OR  
CURB AND GUTTER  
REMOVAL AND REPLACEMENT**

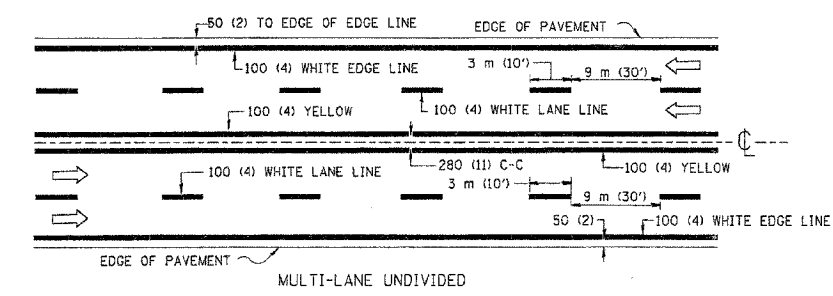
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DATE 11/20/01

DRAWN BY  
CHECKED BY

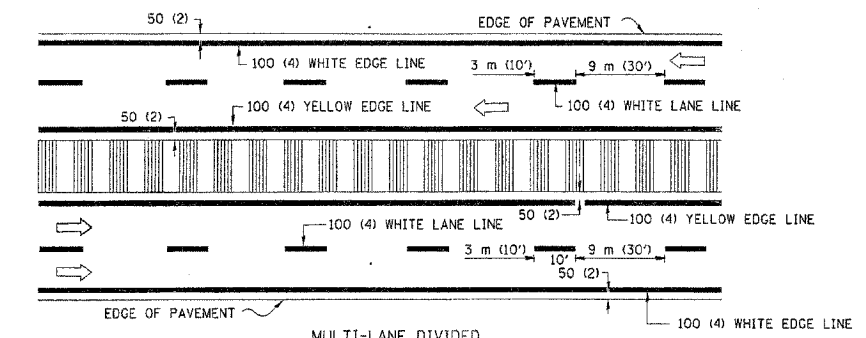
**CONTRACT NO. 83831**



**2-LANE ROADWAY**



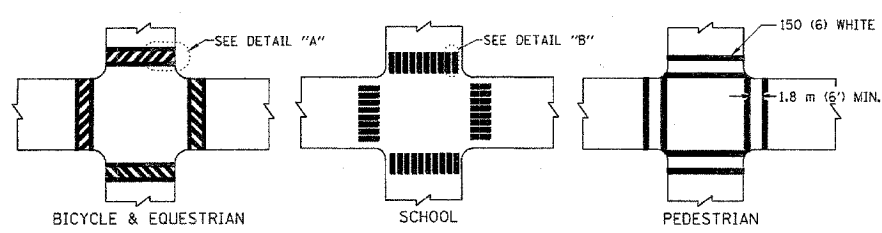
**MULTI-LANE UNDIVIDED**



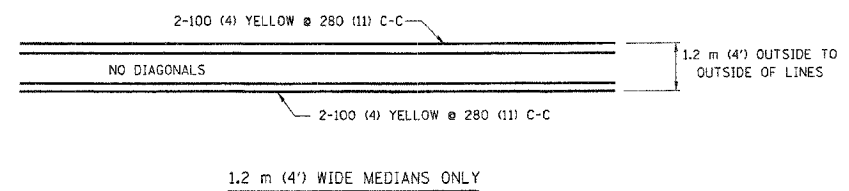
**MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN**

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

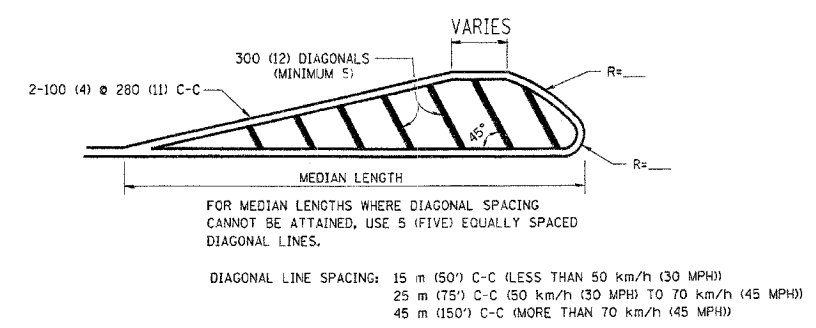
**TYPICAL LANE AND EDGE LINE MARKING**



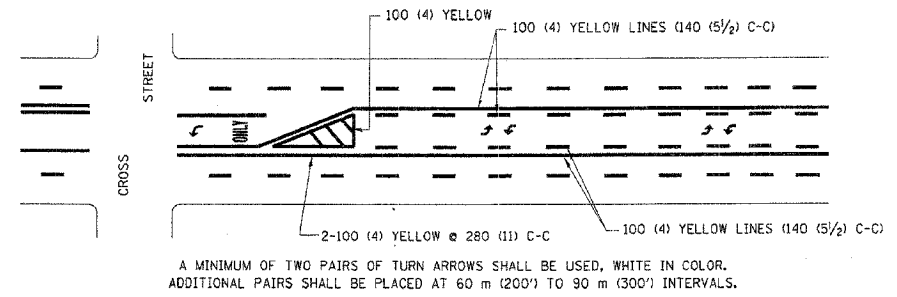
**TYPICAL CROSSWALK MARKING**



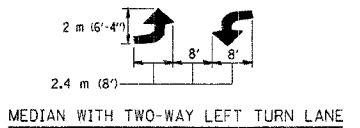
**1.2 m (4') WIDE MEDIANS ONLY**



**MEDIANS OVER 1.2 m (4') WIDE**

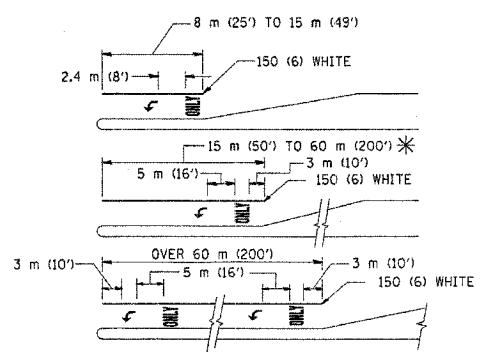


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

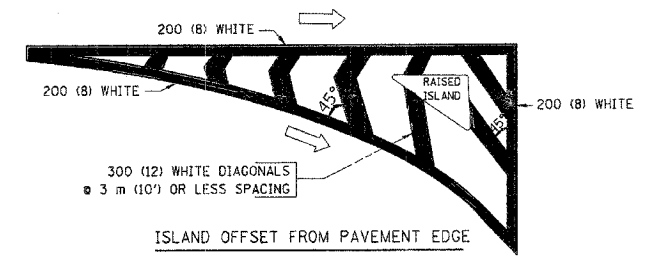


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED. AREA = 1.5 m<sup>2</sup> (15.6 SQ. FT.) ONLY AREA = 1.9 m<sup>2</sup> (20.8 SQ. FT.)

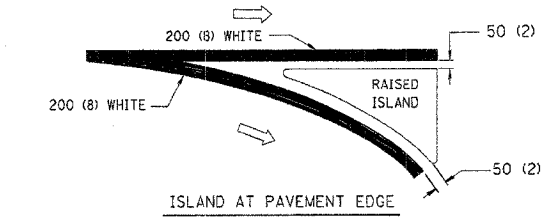
\* TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5 1/2) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINES; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5 1/2) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33m <sup>2</sup> (3.6 SQ. FT.) EACH "X"=5.0 m <sup>2</sup> (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

ILLINOIS DEPARTMENT OF TRANSPORTATION

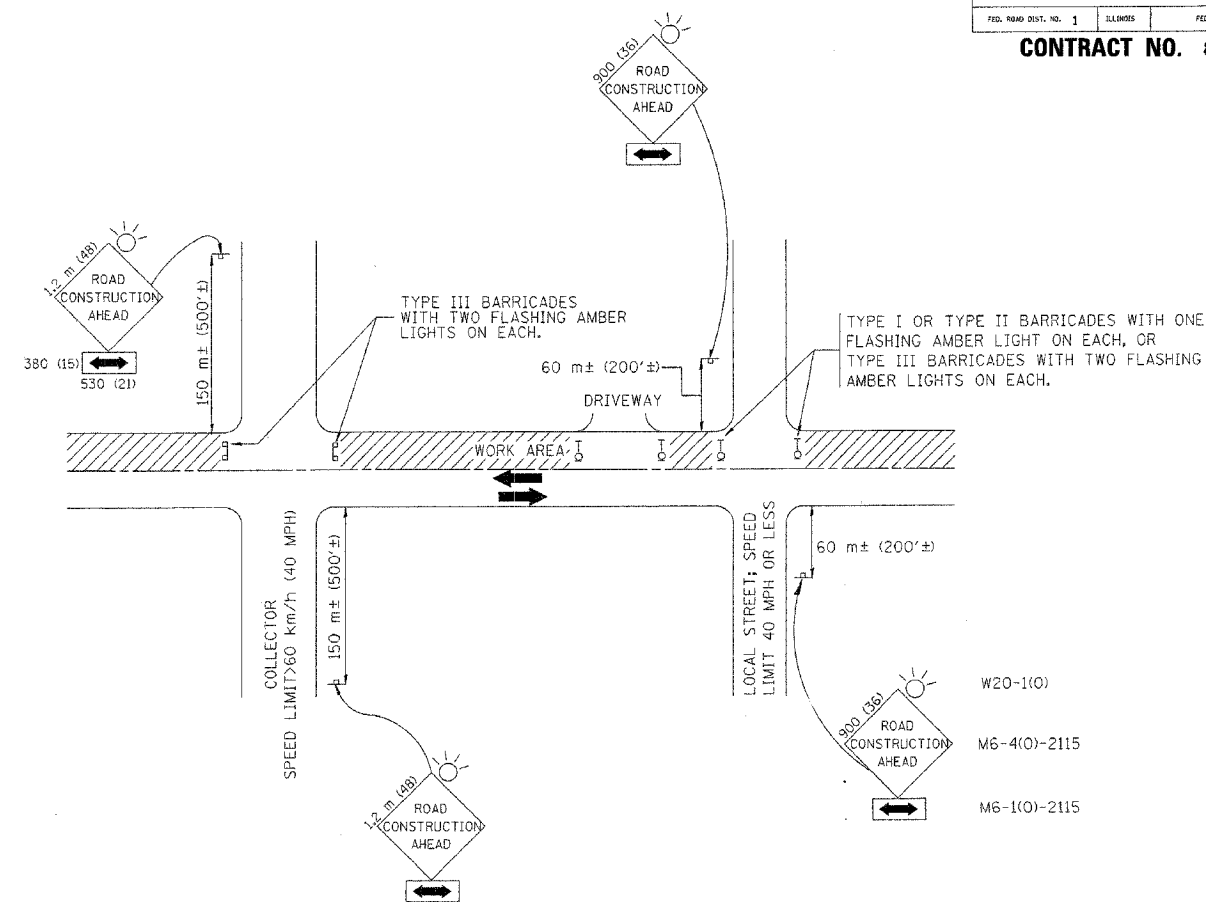
DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

SCALE: NONE  
DATE 11/20/01  
DRAWN BY CADD  
CHECKED BY

F. & U. SITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	22
STA.		TO STA.		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83831



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
  1. SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAYS:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION  
TRAFFIC CONTROL AND PROTECTION  
FOR  
SIDE ROADS, INTERSECTIONS, AND  
DRIVEWAYS

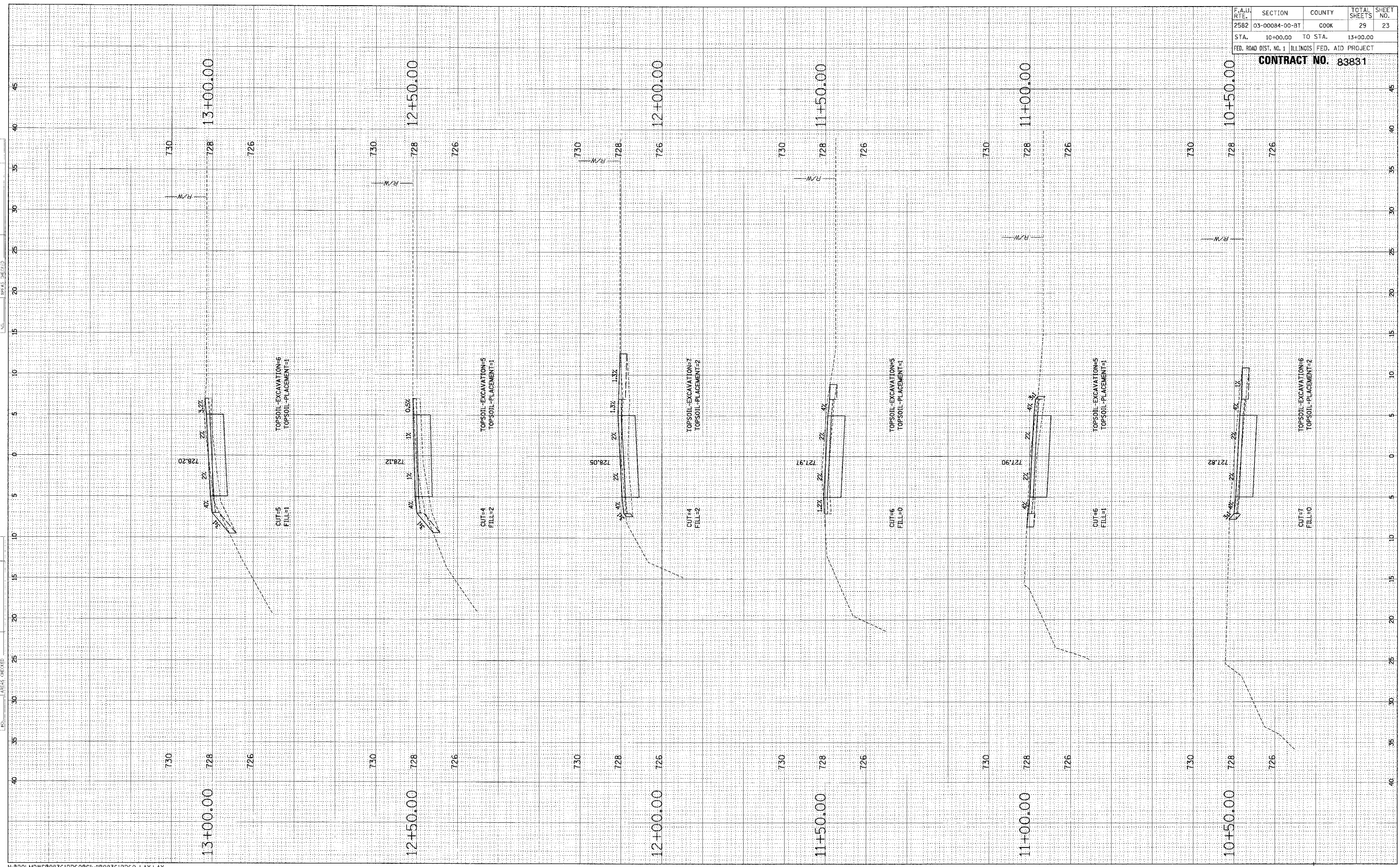
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DATE: 11/20/01  
DRAWN BY  
CHECKED BY

FINAL	SURVEYED	DATE
SHEET	PLOTTED	BY
NO. / E. BOOK	DATE	
NO.	AREAS CHECKED	

ORIGINAL	SURVEYED	DATE
SHEET	PLOTTED	BY
NO. / E. BOOK	DATE	
NO.	AREAS CHECKED	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	23
STA.	10+00.00	TO STA.	13+00.00	
FEB. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 83831

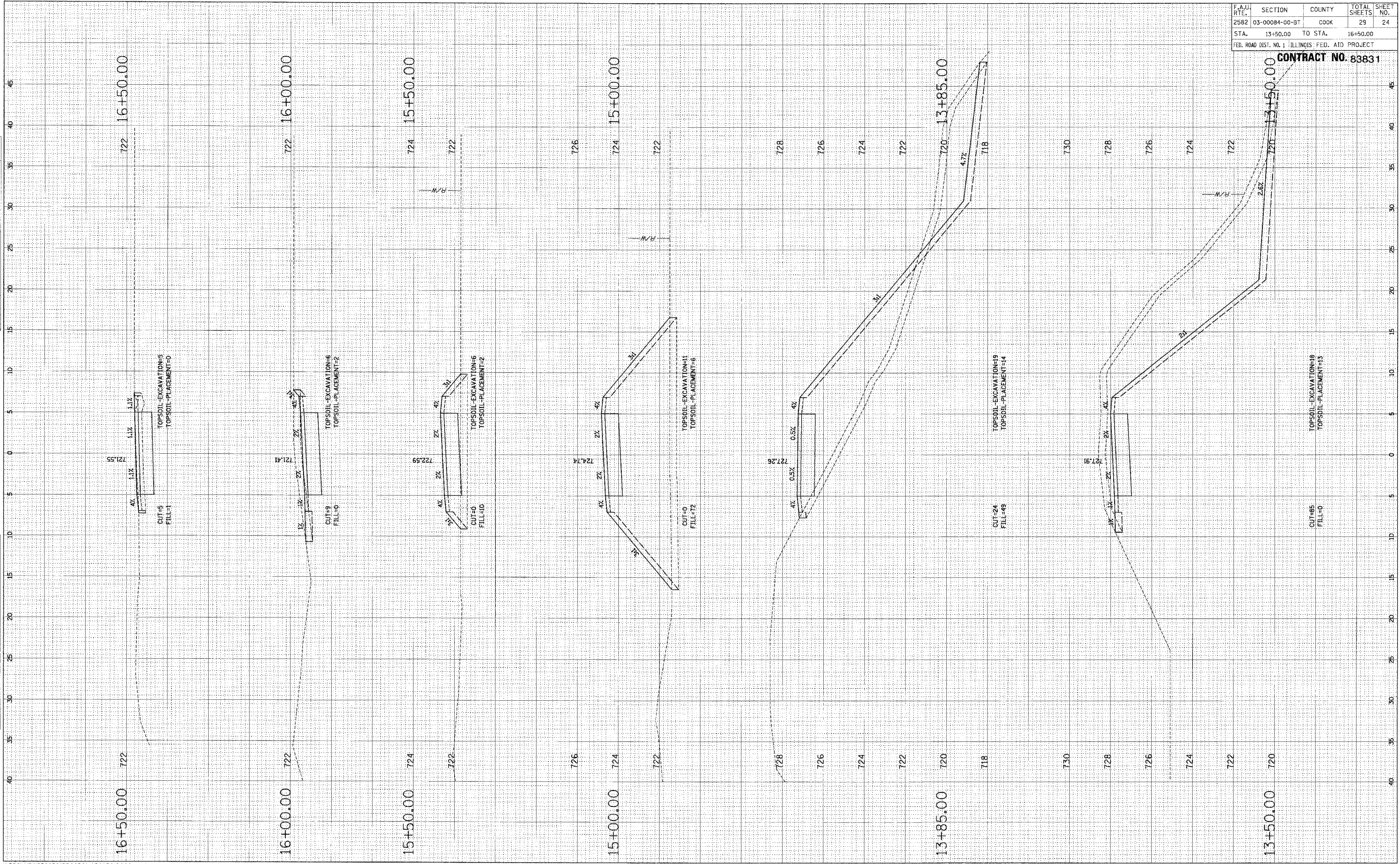


FINAL SURVEY SUBMITTED BY DATE  
 NOTE BOOK PLOTTED BY DATE  
 TEMPLATE AREA CHECKED

ORIGINAL SURVEY SUBMITTED BY DATE  
 NOTE BOOK PLOTTED BY DATE  
 TEMPLATE AREA CHECKED

F.A.U. RT.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	24
STA.	13+50.00	TO STA.	16+50.00	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

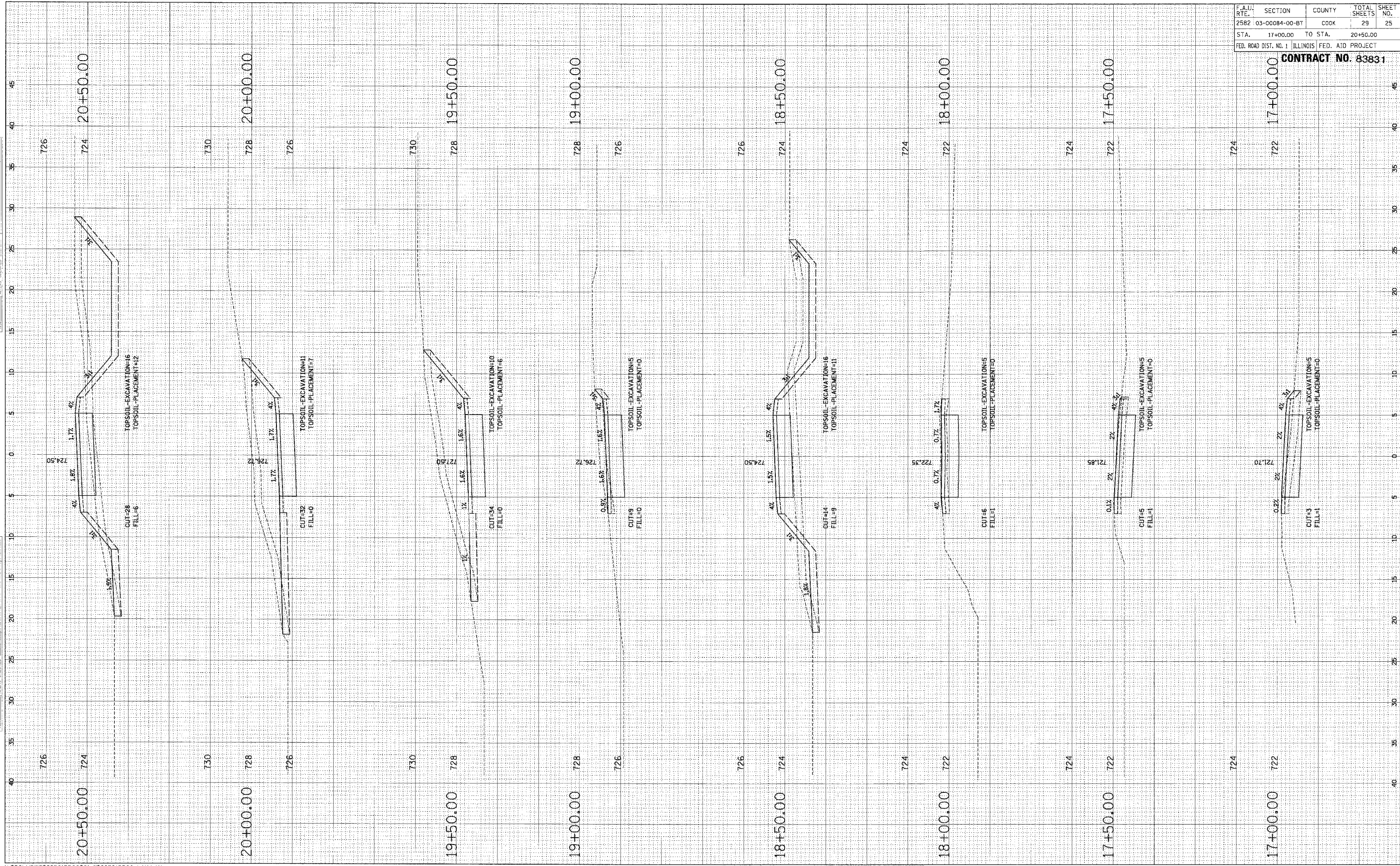
CONTRACT NO. 83831





FINAL SURVEYED  
 SURVEY PLOTTED  
 NOTE BOOK  
 AREAS CHECKED

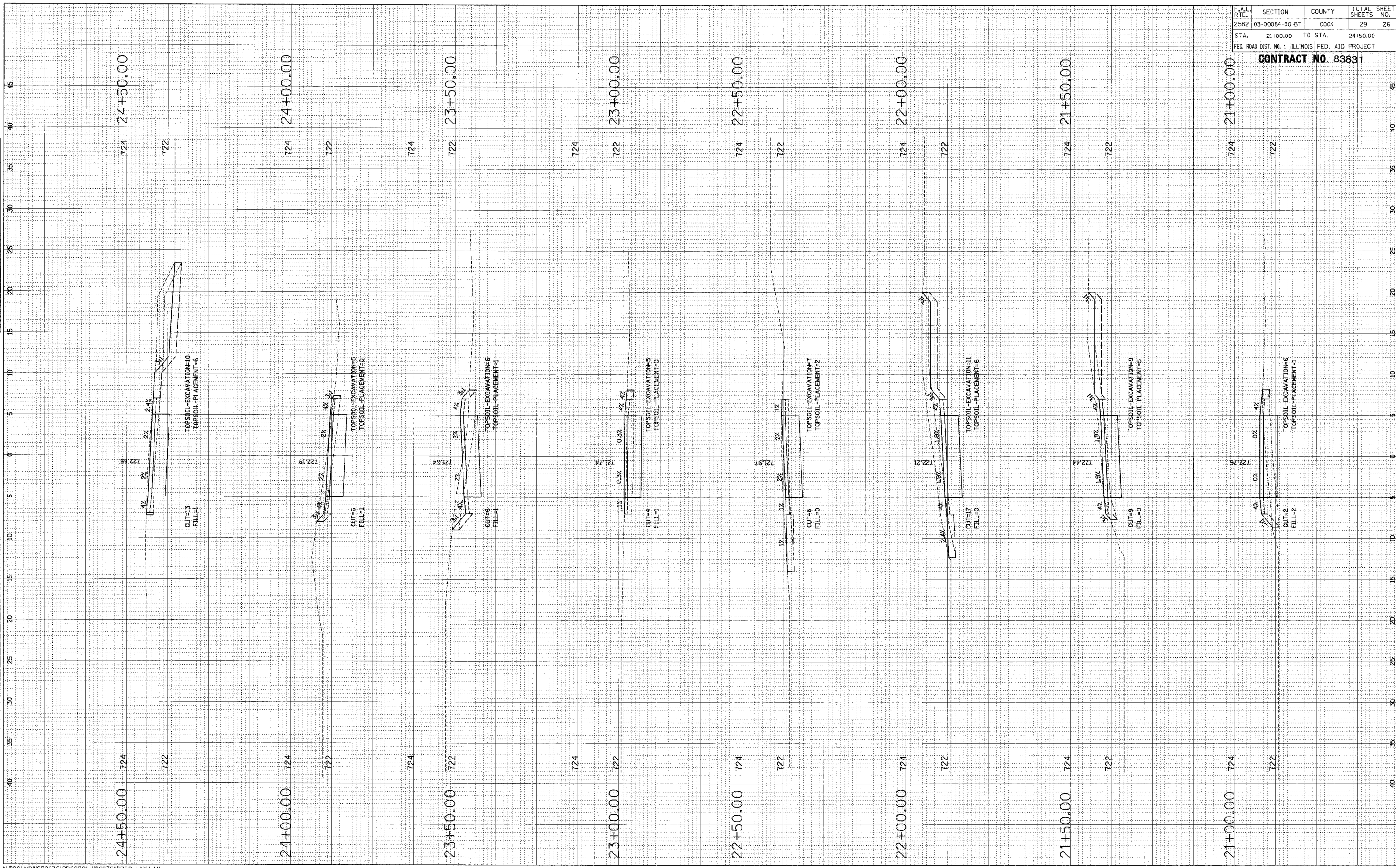
ORIGINAL SURVEY  
 SURVEY PLOTTED  
 NOTE BOOK  
 AREAS CHECKED



F.A.U. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	25
STA.	17+00.00	TO STA.	20+50.00	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
<b>CONTRACT NO. 83831</b>				

FINAL SURVEY SUBMITTED TO DATE  
 NOTED BOOK PLOTTED BY  
 NO. AREAS CHECKED

ORIGINAL SURVEY SUBMITTED TO DATE  
 NOTED BOOK PLOTTED BY  
 NO. AREAS CHECKED

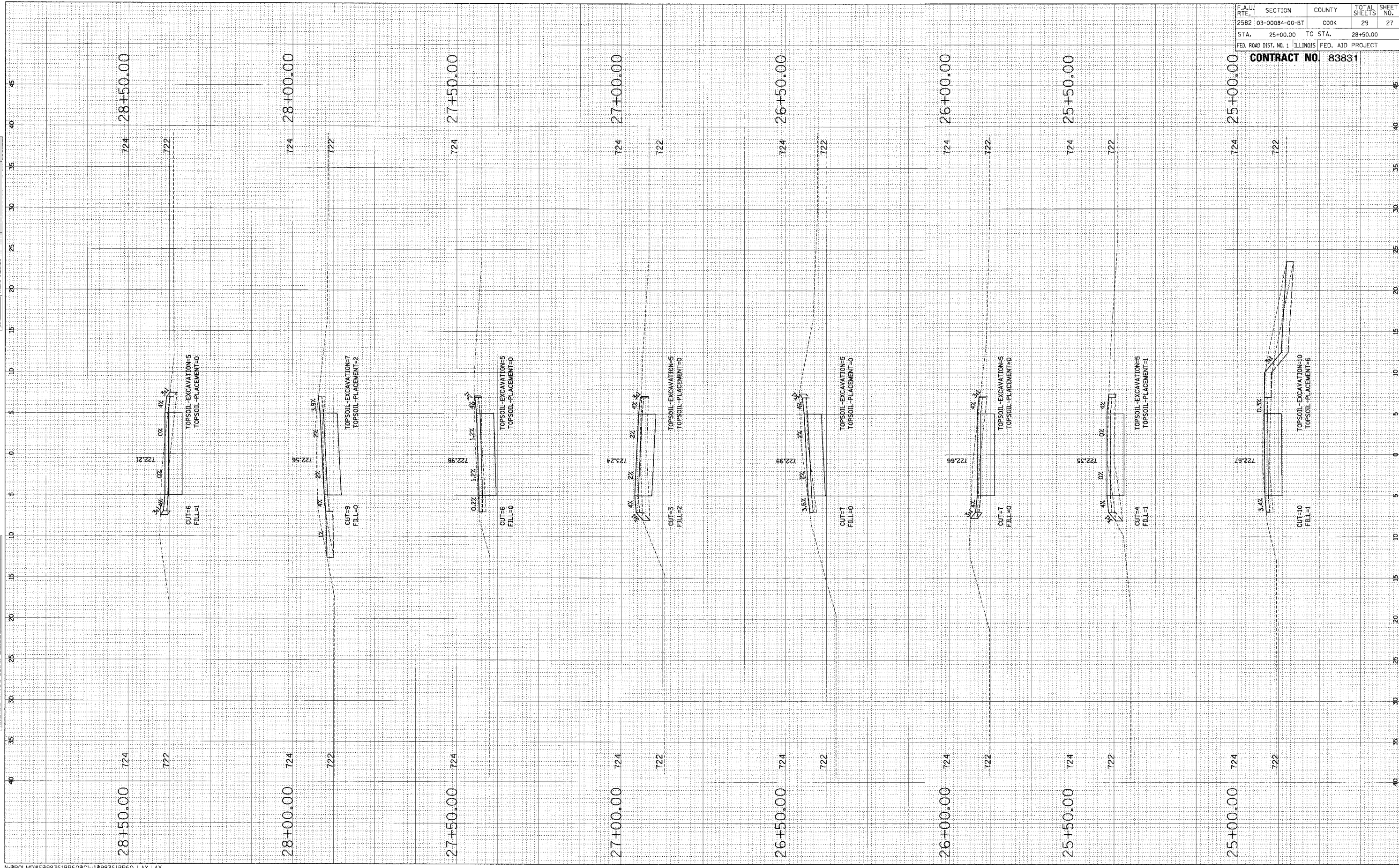


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	26
STA.	21+00.00	TO STA.	24+50.00	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

**CONTRACT NO. 83831**

ORIGINAL SURVEY	DATE
SKETCHED	BY
PLOTTED	
TEMPLATE	
NOTE BOOK	
NO.	
AREAS CHECKED	

FINAL SURVEY	DATE
SKETCHED	BY
PLOTTED	
TEMPLATE	
NOTE BOOK	
NO.	
AREAS CHECKED	

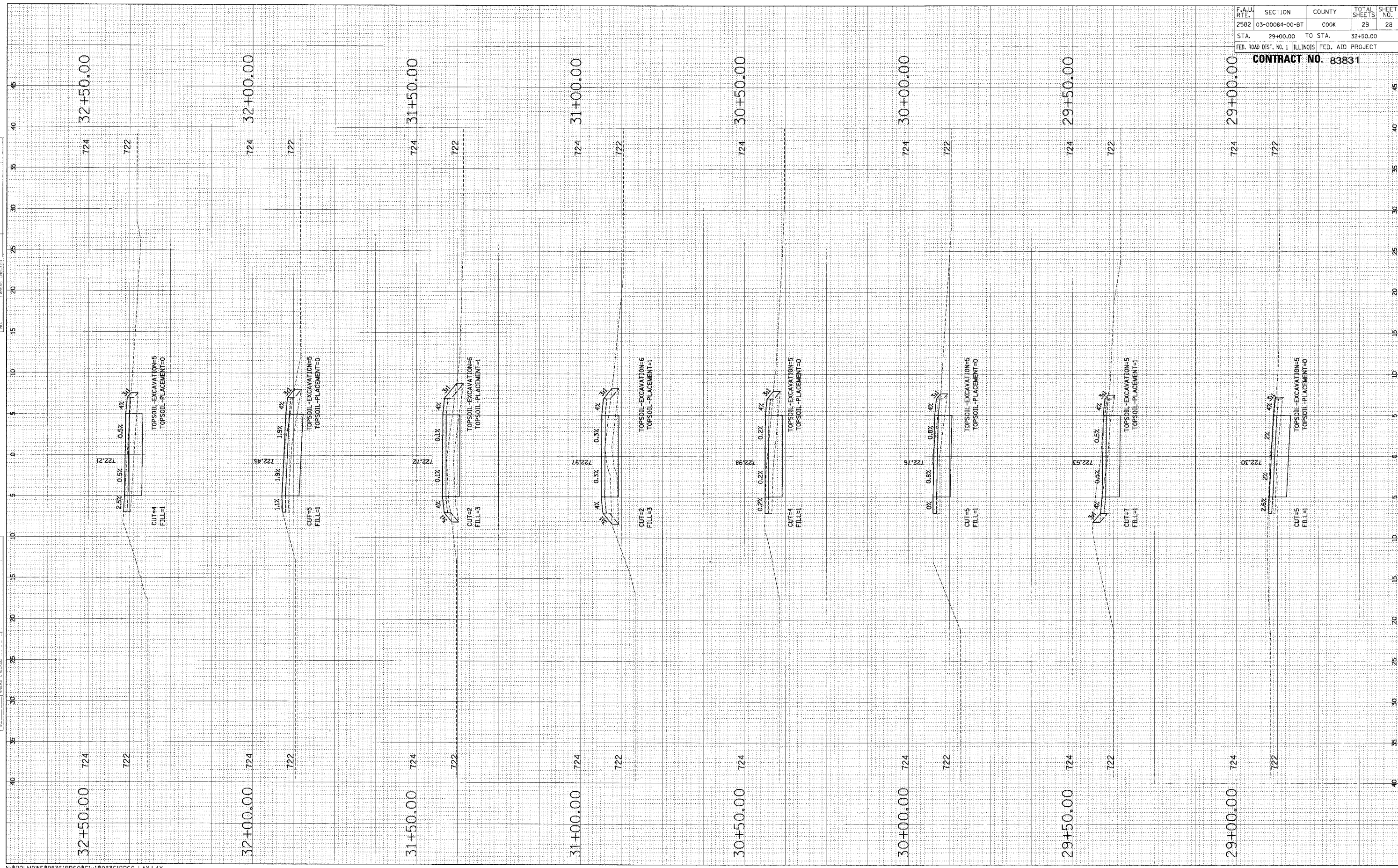


F.A.U. SECTION COUNTY TOTAL SHEETS SHEET NO.
2582 03-00084-00-BT COOK 29 27
STA. 25+00.00 TO STA. 28+50.00
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

CONTRACT NO. 83831

ORIGINAL SURVEY PLOTTED DATE BY  
 NOT BECK PLATE RE-DATE  
 NO. AREAS CHECKED

FINAL SURVEY PLOTTED DATE BY  
 NOT BECK PLATE RE-DATE  
 NO. AREAS CHECKED

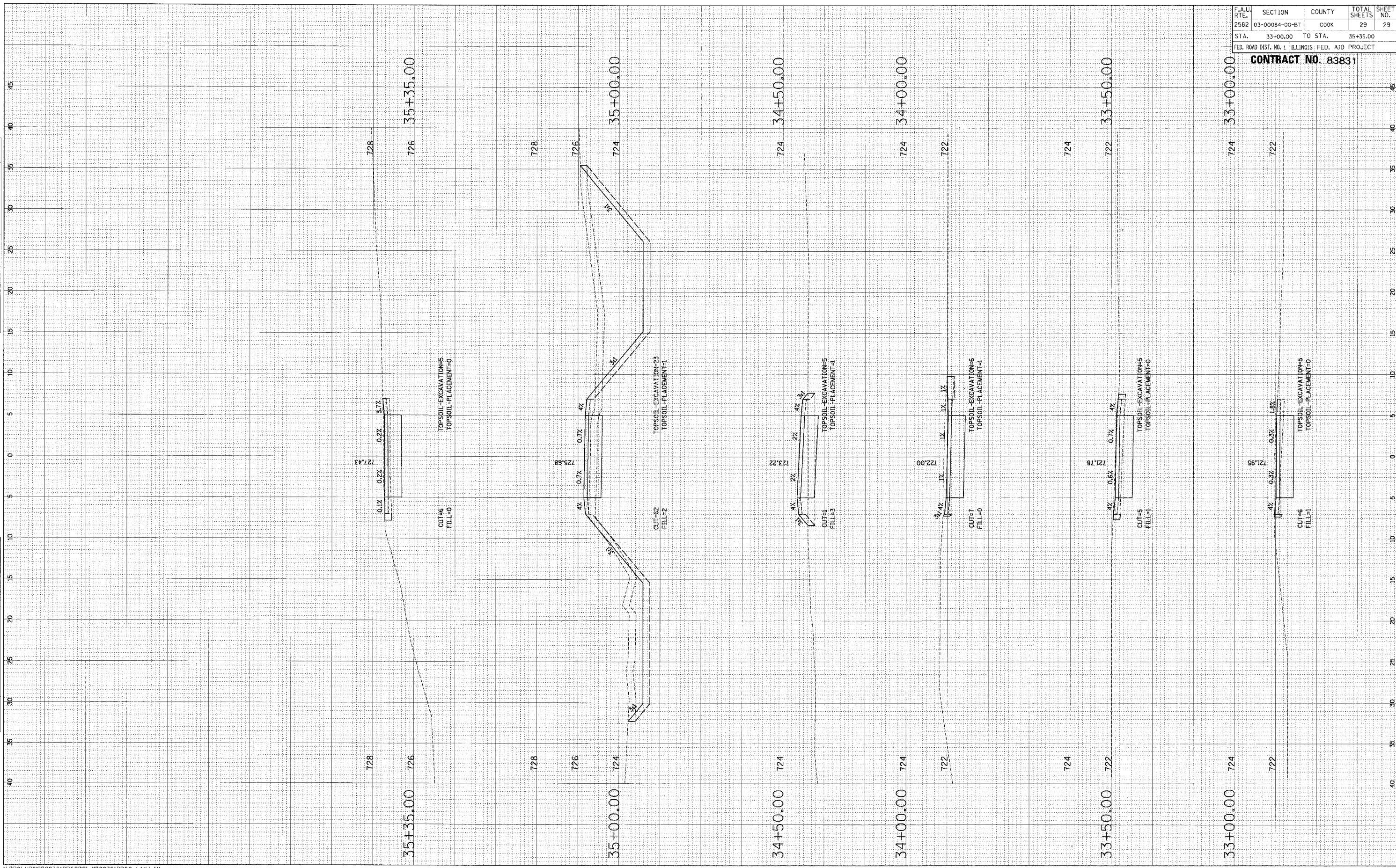


F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	28
STA. 29+00.00 TO STA. 32+50.00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

CONTRACT NO. 83831

FINAL SURVEY PLotted  
 SURVEY NOTE BOOK  
 DATE

ORIGINAL SURVEY PLotted  
 SURVEY NOTE BOOK  
 DATE



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2582	03-00084-00-BT	COOK	29	29
STA. 33+00.00 TO STA. 35+35.00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

CONTRACT NO. 83831