

**POROUS FRICTION COURSE NOTES**

THE PROPOSED POROUS FRICTION COURSE WILL BE CONSTRUCTED IN ONE LAYER, HAVING A COMPACTED NOMINAL THICKNESS OF 0.10 FOOT.

POROUS FRICTION COURSE SHALL BE PLACED ON A CLEAN AND PREPARED SURFACE ONLY AFTER THE APPROVAL OF THE RESIDENT ENGINEER.

POROUS FRICTION COURSE WILL BE CONSTRUCTED IN THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS AND IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

THIS ITEM WILL BE PAID FOR UNDER ITEM:  
AR "POROUS FRICTION COURSE, 0.10'" 5,514 SQ. YDS.

**AR401--BITUMINOUS SURFACE COURSE NOTES**

THE BITUMINOUS SURFACE COURSE (401) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR401001 "BITUMINOUS SURFACE COURSE--METHOD 1" AS STATED ON PAGE 245 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THIS ITEM OF WORK SHALL CONSIST OF CONSTRUCTING TWO LIFTS OF BITUMINOUS SURFACE COURSE (1-1/2 INCH DEPTH, AVERAGE OVERLAY OF 3") ON THE PROPOSED POROUS FRICTION COURSE FOR THE PROPOSED APRON RECONSTRUCTION.

THE PROPOSED BITUMINOUS SURFACE COURSE WILL BE DESIGNED TO A MARSHALL DESIGN OF LESS THAN 60,000 POUNDS.

401-4.9 ADD THE FOLLOWING TO THIS SECTION:

ALL LANES OF THE BITUMINOUS SURFACE COURSE SHALL BE STARTED AT THE HIGH SIDE OF THE PAVEMENT WITH A TAUT STRINGLINE (GUIDE WIRE) SET TO GRADE AT BOTH SIDES OF THE PAVER. THE AUTOMATIC GRADE CONTROL SYSTEM OF THE PAVER SHALL BE USED TO CONTROL GRADE OF BOTH SIDES OF THE PAVER FROM THESE REFERENCE STRINGLINES. THE GRADE CONTROL FOR THE ADJACENT LANES OF PAVEMENT SHALL BE MAINTAINED BY USING A MATCHING SHOE WITH THE PREVIOUS LAID PAVEMENT AND A STRINGLINE ON THE OUTER EDGE OF THE NEXT LANE.

A STRINGLINE AND MATCHING SHOE SHALL BE USED TO PAVE ALL REMAINING LANES OF THE FIRST LIFT OF THE SURFACE COURSE. IF GRADE IS ESTABLISHED ON THE FIRST LIFT, THEN THE REMAINING LIFTS OF BITUMINOUS SURFACE COURSE SHALL BE LAID WITH A TRAVELING SKI ON BOTH SIDES OF THE PAVER FOR THE FIRST LANE AND A MATCHING SHOE AND TRAVELING SKI ON ADJACENT LANES. IF GRADE IS NOT ESTABLISHED ON THE FIRST LIFT, THE RESIDENT ENGINEER SHALL REQUIRE TAUT STRINGLINE REFERENCES UNTIL SATISFACTORY GRADE IS ESTABLISHED.

WHEN HAND SPREADING IS PERMITTED, THE MIXTURE WILL BE DISTRIBUTED AND SPREAD USING HAND TOOLS. WHEN THE WORK IS COMPLETED, THE LAYER WILL HAVE THE REQUIRED THICKNESS AND CONFORM TO THE GRADE AND SURFACE CONTOUR SHOWN ON THE PLANS.

401-4.12 SHAPING EDGES. ADD THE FOLLOWING TO THIS PARAGRAPH:

ALL PAVEMENT EDGES (LONGITUDINAL, RADIUS, AND PAVEMENT ENDS) MUST BE LEFT IN PROPER ALIGNMENT AS SHOWN ON THE PLANS. THIS MAY BE ACCOMPLISHED BY THE TRIMMING METHOD OUTLINED ABOVE OR AT THE CONTRACTOR'S OPTION BY SAWING AFTER THE PAVING HAS BEEN COMPLETED. NO ADDITIONAL COMPENSATION WILL BE MADE IF THE SAWING METHOD IS USED.

**603--BITUMINOUS TACK COAT NOTES:**

THE BITUMINOUS TACK COAT (603) SHALL BE PLACED IN ACCORDANCE WITH ITEM AR603 "BITUMINOUS TACK COAT" AS STATED ON PAGE 62 OF THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS, ADOPTED JULY 1, 2004.

THE PROPOSED BITUMINOUS TACK COAT SHALL BE PLACED ON THE EXISTING AND THE PROPOSED BITUMINOUS PAVEMENT PRIOR TO THE PLACEMENT OF THE NEXT LIFT OF PROPOSED BITUMINOUS SURFACE COURSE.

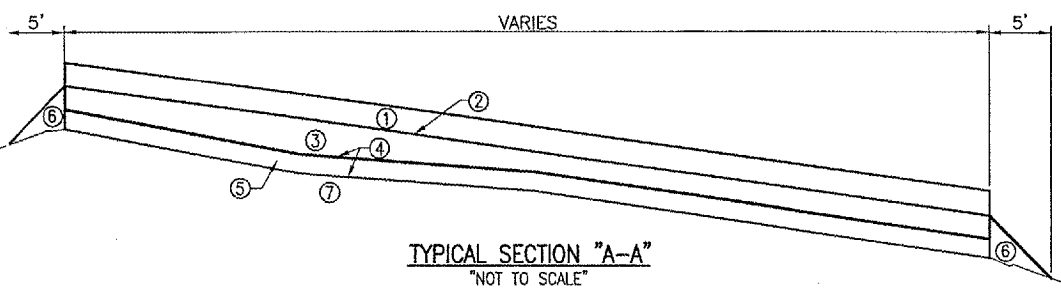
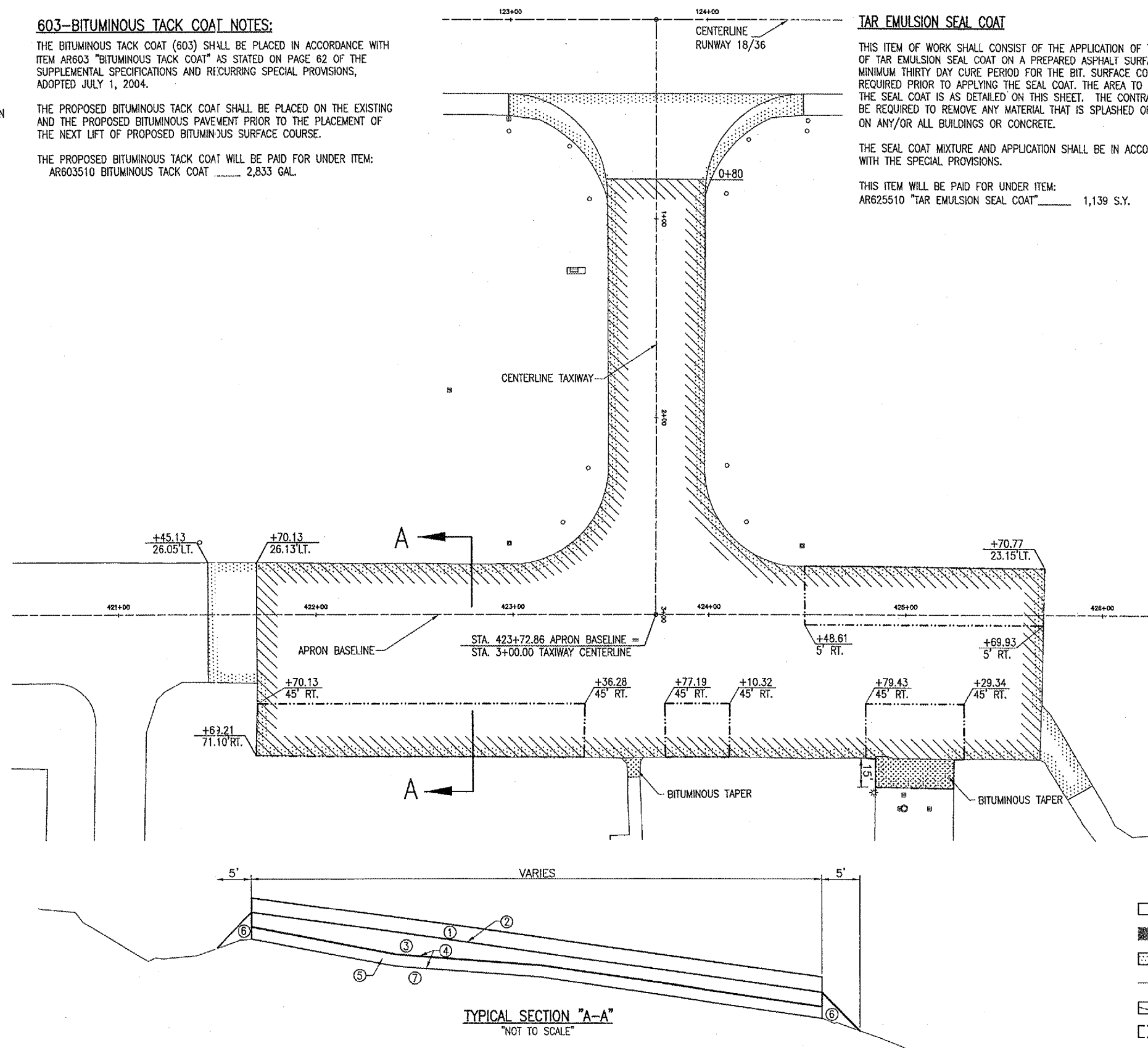
THE PROPOSED BITUMINOUS TACK COAT WILL BE PAID FOR UNDER ITEM:  
AR603510 BITUMINOUS TACK COAT 2,833 GAL.

**TAR EMULSION SEAL COAT**

THIS ITEM OF WORK SHALL CONSIST OF THE APPLICATION OF TWO COATS OF TAR EMULSION SEAL COAT ON A PREPARED ASPHALT SURFACE. A MINIMUM THIRTY DAY CURE PERIOD FOR THE BIT. SURFACE COURSE IS REQUIRED PRIOR TO APPLYING THE SEAL COAT. THE AREA TO RECEIVE THE SEAL COAT IS AS DETAILED ON THIS SHEET. THE CONTRACTOR WILL BE REQUIRED TO REMOVE ANY MATERIAL THAT IS SPLASHED OR SPRAYED ON ANY/OR ALL BUILDINGS OR CONCRETE.

THE SEAL COAT MIXTURE AND APPLICATION SHALL BE IN ACCORDANCE WITH THE SPECIAL PROVISIONS.

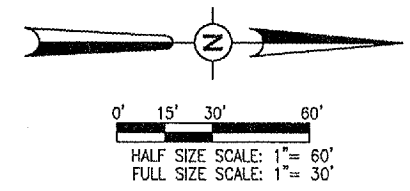
THIS ITEM WILL BE PAID FOR UNDER ITEM:  
AR625510 "TAR EMULSION SEAL COAT" 1,139 S.Y.



**LEGEND FOR TYPICAL SECTION "A-A"**

- ① 401 - BITUMINOUS SURFACE COURSE (1-1/2" DEPTH)
- ② 603 - BITUMINOUS TACK COAT (0.15 GAL./S.Y.)
- ③ 401 - BITUMINOUS SURFACE COURSE (VAR. DEPTH)
- ④ 603 - BITUMINOUS TACK COAT (0.25 GAL./S.Y.)
- ⑤ 402 - BITUMINOUS POROUS FRICTION COURSE (0.10')
- ⑥ TOPSOILING (FROM OFF SITE)
- ⑦ EXISTING PAVEMENT

- LEGEND**
- [Hatched Box] EXISTING IMPROVEMENTS
  - [Solid Black Box] EXISTING BUILDINGS
  - [Dotted Box] PROPOSED IMPROVEMENTS
  - [Line with X] EXISTING FENCE
  - [Diagonal Lines] PROPOSED PFC AREA
  - [Dashed Box] TAR EMULSION SEAL COAT



PAVING QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR401610	BITUMINOUS SURFACE COURSE	TON	1,079
AR402622	POROUS FRICTION COURSE, 0.10'	S.Y.	5,514
AR603510	BITUMINOUS TACK COAT	GAL.	2,833
AR625510	TAR EMULSION SEAL COAT	S.Y.	1,139

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DATE	REVISION	BY	<b>METROPOLIS MUNICIPAL AIRPORT METROPOLIS, ILLINOIS</b>	A.I.P. PROJ.: 3-17-0067-B7 ILL. PROJ.: M30-3540
ILL. Project No. 818-06APND R-121CON.DWG Name: N/A Scale: 09/26/05	LAYOUT: JEO 09/26/05 DRAWN: JEO 09/26/05 REVIEWED: CAH 12/12/05	 Hanson Professional Services Inc. 1525 South State Street Springfield, Illinois 62703-2886 Offices Nationwide		
<b>PROPOSED APRON REHABILITATION</b>		<b>PROPOSED CONSTRUCTION PLAN</b>		
4				
- 4 of 25 sheets				