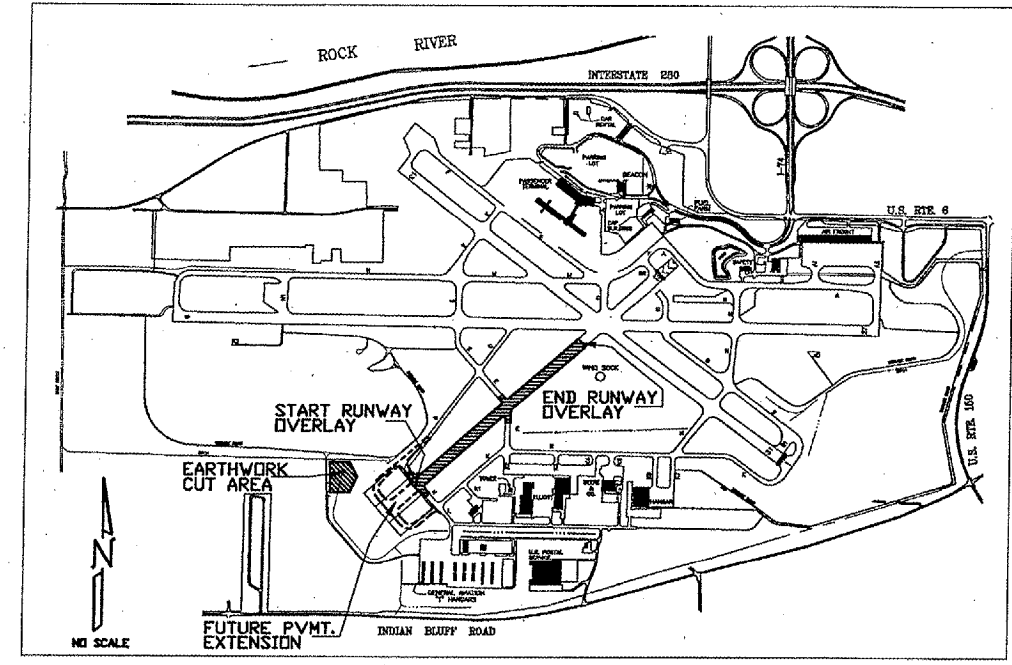
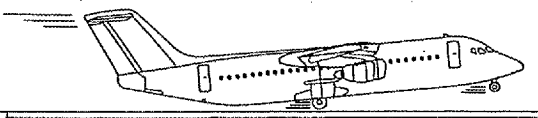


ITEM 12A ✓



STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF AERONAUTICS
 CONSTRUCTION PLANS
 FOR
**QUAD-CITY
 INTERNATIONAL
 AIRPORT**



PROJECT LOCATION MAP

ROCK ISLAND COUNTY, ILLINOIS

P.C. CONCRETE OVERLAY (3,090' X 150') ON BITUMINOUS
 RUNWAY 5-23 ALONG WITH TAXIWAY D, E, & K TIE-INS,
 SHOULDER EMBANKMENT, UTILITY ADJUSTMENTS, AND TURFING.

WARNING



CALL BEFORE
 YOU DIG

INDEX OF SHEETS	
SHEET NO.	TITLE
1	COVER SHEET AND INDEX OF SHEETS
2	SUMMARY OF QUANTITIES / GENERAL NOTES
3-7	PROPOSED SAFETY PLAN
8-12	TYPICAL SECTIONS
13-16	RUNWAY 5-23 PLAN & PROFILES
17	TERMINAL AREA PAVEMENT REMOVAL & REPLACEMENT PLAN
18	EARTHWORK CUT AREA PLAN
19-20	RUNWAY 5-23 JOINT PLANS
21	JOINT DETAILS
22-23	RUNWAY 5-23 STAKING PLANS
24-25	PROPOSED PAVEMENT MARKINGS
26-30	LIGHTING & GUIDANCE SIGN DETAILS
31	MISCELLANEOUS DETAILS
32-33	PROPOSED STORM WATER POLLUTION PREVENTION PLAN
34-51	PROPOSED RUNWAY 5-23 CROSS SECTIONS
52-54	PROPOSED EARTHWORK CUT AREA CROSS SECTIONS

ILLINOIS PROJECT MLI-3530
 A.I.P. PROJECT NO. 3-17-0068-XX
 AIRPORT CLASSIFICATION - AIR CARRIER

AIRCRAFT APPROACH CATEGORY - B
 AIRPLANE DESIGN GROUP - III
 LATITUDE 41°26'52.4", LONGITUDE 90°30'33.9", ELEVATION 589'M.S.L.

CONSULTING ENGINEERS MISSMAN, STANLEY & ASSOC. ENGINEER'S SIGNATURE <i>Dennis R. Martin</i> SEALED & SIGNED 1/23/06 DATE OF LICENSE EXPIRATION 11/30/07 	METROPOLITAN AIRPORT AUTHORITY OF ROCK ISLAND COUNTY, ILLINOIS <i>Bruce Carter</i> 1-23-06 DATE APPROVED
---	---

JANUARY 25, 2006
 OFFICIAL DATE OF PLANS
 PREPARED BY
 MISSMAN, STANLEY & ASSOCIATES
 Consulting Civil Engineers
 ROCK ISLAND, ILLINOIS

SUMMARY OF QUANTITIES

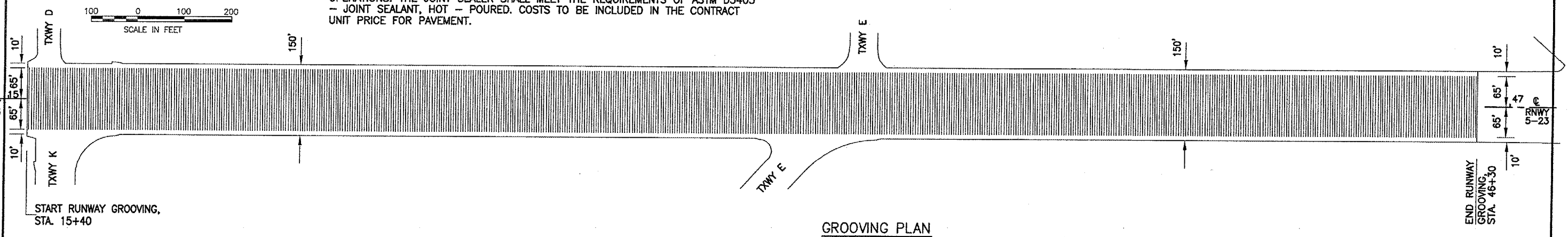
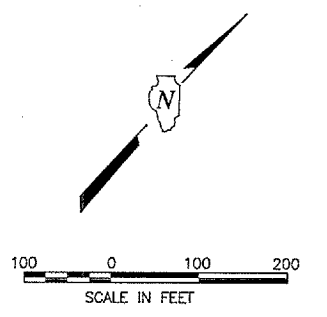
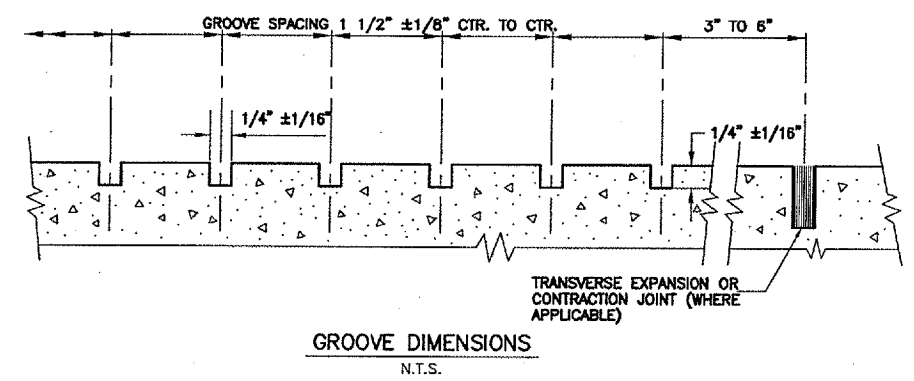
ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITIES			
			AS AWARDED		TOTAL	
			AIP GRANT NO. 3-17-0068-XX	AIP GRANT NO. 3-17-0068-XX		
AR108158	1/C #8 5KV UG CABLE IN UD	LF.	60		60	
AR108258	2/C #8 5KV UG CABLE IN UD	LF.	35		35	
AR125448	TAXI GUIDANCE SIGN, 8 CHARACTER	EACH	1		1	
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	1		1	
AR125942	ADJUST BASE MOUNTED LIGHT	EACH	56		56	
AR125962	RELOCATE BASE MOUNTED LIGHT	EACH	3		3	
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1		1	
AR150530	TRAFFIC MAINTENANCE	L.S.	1		1	
AR152410	UNCLASSIFIED EXCAVATION	C.Y.	4,035		4,035	
AR156500	TEMPORARY EROSION CONTROL	L.S.	1		1	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	1,825		1,825	
AR209511	CRUSHED AGGREGATE BASE, (CA-1)	TON	100		100	
AR209600	GEOTEXTILE FABRIC	S.Y.	3,025		3,025	
AR401610	BITUMINOUS SURFACE COURSE	TON	520		520	
AR401911	REMOVE & REPLACE BIT. SURFACE	S.Y.		4	4	
AR401650	BITUMINOUS PAVEMENT MILLING	S.Y.	9,800		9,800	
AR402620	POROUS FRICTION COURSE 5/8"	S.Y.	260		260	
AR501508	8" PCC PAVEMENT	S.Y.	51,650		51,650	
AR501510	10" PCC PAVEMENT	S.Y.	2,888		2,888	
AR501530	PCC TEST BATCH	EACH	1		1	
AR501540	PCC PAVEMENT GROOVING	S.Y.	45,080		45,080	
AR501900	REMOVE PCC PAVEMENT	S.Y.	2,845		2,845	
AR501910	REMOVE & REPLACE PCC PAVEMENT	S.Y.		225	225	
AR603510	BITUMINOUS TACK COAT	GAL.	250		250	
AR620510	PAVEMENT MARKING	S.F.	15,300		15,300	
AR620900	PAVEMENT MARKING REMOVAL	S.F.	950		950	
AR751944	ADJUST MANHOLE - PAVEMENT	EACH	1		1	
AR751945	ADJUST MANHOLE - NON PAVEMENT	EACH	8		8	
AR801605	REPLACE TAXI GUIDANCE SIGN PANEL	EACH	106		106	
AR801609	SURFACE SENSOR INSTALLATION	L.S.	1		1	
AR801616	TRAFFIC MAINTENANCE, NORTH APRON	L.S.		1	1	
AR801617	LIGHTED RUNWAY CLOSURE MARKER	EACH	2		2	
AR901510	SEEDING	ACRE	8		8	
AR908513	MULCHING - METHOD 3	ACRE	8		8	

GENERAL NOTES:

- MAXIMUM PAY WIDTH FOR 209 GRANULAR BASE SHALL BE AS SHOWN ON THE SECTIONS (AT THE PROPOSED EDGE OF PAVEMENT FOR RUNWAY 5-23 AND 12 INCHES BEYOND THE EDGE OF PAVEMENT FOR TAXIWAY E & K). IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVEMENT INSTALLATION, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.
- THE CONTRACTOR SHALL SALVAGE EXISTING AIRFIELD LIGHTING EQUIPMENT AS DETAILED IN THE CONSTRUCTION PLANS AND SPECIAL PROVISIONS PRIOR TO THE START OF EARTHWORK AND/OR PAVING ACTIVITIES. SALVAGED EQUIPMENT SHALL BE CLEANED AND REUSED OR DELIVERED TO THE METROPOLITAN AIRPORT AUTHORITY.
- CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS APPROVED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
- THE CONTRACTOR SHALL EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALES, AS REQUIRED BY THE ENGINEER, TO CONTROL STORM WATER RUN-OFF.
- THE CONTRACT 152 - EARTHWORK ITEM SHALL INCLUDE ALL COSTS ASSOCIATED WITH EXCAVATION OF SOILS, HAULING OF SOILS, STOCKPILING SOILS, INSTALLATION OF SOILS, COMPACTION OF SOILS, GRADING OF SOILS, INSTALLATION AND REMOVAL OF HAUL ROADS OR ROUTES, RESTORATION OF HAUL ROADS OR ROUTES, DISPOSAL OF WASTE SOILS, CLEANING OF PAVEMENTS, AND ALL OTHER ITEMS THAT ARE REQUIRED TO COMPLETE THE EARTHWORK. THIS ITEM SHALL BE PAID FOR BASED UPON THE CUBIC YARDS OF MATERIALS REMOVED AS ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- AFTER CONSTRUCTION HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEED AND HYDRAULIC MULCH ALL DISTURBED AREAS PER SPECIAL PROVISIONS 901 AND 908. ONLY SEED AND MULCHING AREAS WITHIN THE LIMITS OF CONSTRUCTION/SEEDING WILL BE ELIGIBLE FOR PAYMENT UNDER THESE CONTRACT PAY ITEMS. AREAS OUTSIDE OF THE LIMITS OF CONSTRUCTION/SEEDING SHALL BE SEED AND MULCHED BY THE CONTRACTOR PER SPECIAL PROVISION 901/908, BUT SHALL NOT BE MEASURED FOR PAYMENT.
- ITEM AR908513 MULCHING - METHOD 3 SHALL BE ACCOMPLISHED FOLLOWING THE METHODS AND PROCEDURES OUTLINED IN THE IDOT-DOA SUPPLEMENTAL SPECIFICATIONS AND IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 251, METHOD 3 (HYDRAULIC MULCH).

NOTES:

- GROOVING EXTENDS OUTWARD 65' FROM C OF RUNWAY UNLESS OTHERWISE NOTED.
- SUCCESSIVE PASSES OF THE GROOVING MACHINE SHALL NOT OVERLAP.
- CONTRACTOR SHALL NOT ALLOW ACCUMULATION OF CUTTINGS IN TURFED AREAS. THE GROOVING MACHINE SHALL BE EQUIPPED WITH AUTOMATIC CUTTING PICK-UP FEATURES TO PREVENT ACCUMULATION OF CUTTINGS IN THE TURFED AREAS.
- GROOVING SHALL BE COMPLETED PRIOR TO THE INSTALLATION OF NEOPRENE COMPRESSION SEALS IN THE LONGITUDINAL JOINTS.
- CONTRACTOR SHALL RE-SEAL ALL JOINTS DAMAGED BY THE PAVEMENT GROOVING OPERATIONS. THE JOINT SEALER SHALL MEET THE REQUIREMENTS OF ASTM D3405 - JOINT SEALANT, HOT - POURED. COSTS TO BE INCLUDED IN THE CONTRACT UNIT PRICE FOR PAVEMENT.



GROOVING PLAN

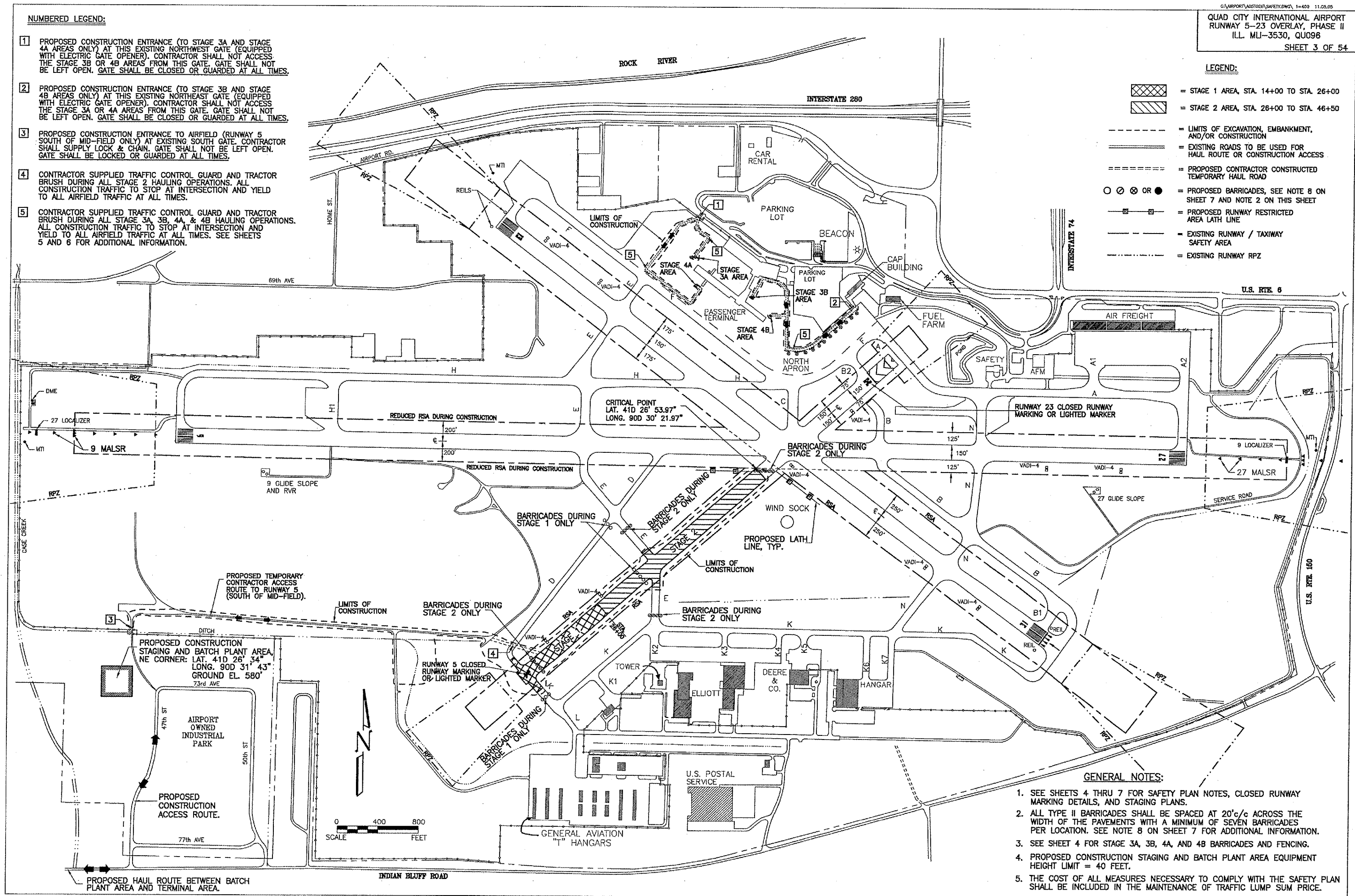
I:\airport\A05T026\sumquan.dwg, 1/30/2006 8:29:33 AM, jeffm

NUMBERED LEGEND:

- 1 PROPOSED CONSTRUCTION ENTRANCE (TO STAGE 3A AND STAGE 4A AREAS ONLY) AT THIS EXISTING NORTHWEST GATE (EQUIPPED WITH ELECTRIC GATE OPENER). CONTRACTOR SHALL NOT ACCESS THE STAGE 3B OR 4B AREAS FROM THIS GATE. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE CLOSED OR GUARDED AT ALL TIMES.
- 2 PROPOSED CONSTRUCTION ENTRANCE (TO STAGE 3B AND STAGE 4B AREAS ONLY) AT THIS EXISTING NORTHEAST GATE (EQUIPPED WITH ELECTRIC GATE OPENER). CONTRACTOR SHALL NOT ACCESS THE STAGE 3A OR 4A AREAS FROM THIS GATE. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE CLOSED OR GUARDED AT ALL TIMES.
- 3 PROPOSED CONSTRUCTION ENTRANCE TO AIRFIELD (RUNWAY 5 SOUTH OF MID-FIELD ONLY) AT EXISTING SOUTH GATE. CONTRACTOR SHALL SUPPLY LOCK & CHAIN. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE LOCKED OR GUARDED AT ALL TIMES.
- 4 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL STAGE 2 HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES.
- 5 CONTRACTOR SUPPLIED TRAFFIC CONTROL GUARD AND TRACTOR BRUSH DURING ALL STAGE 3A, 3B, 4A, & 4B HAULING OPERATIONS. ALL CONSTRUCTION TRAFFIC TO STOP AT INTERSECTION AND YIELD TO ALL AIRFIELD TRAFFIC AT ALL TIMES. SEE SHEETS 5 AND 6 FOR ADDITIONAL INFORMATION.

LEGEND:

- [Cross-hatched box] = STAGE 1 AREA, STA. 14+00 TO STA. 26+00
- [Diagonal hatched box] = STAGE 2 AREA, STA. 26+00 TO STA. 46+50
- [Dashed line] = LIMITS OF EXCAVATION, EMBANKMENT, AND/OR CONSTRUCTION
- [Solid line] = EXISTING ROADS TO BE USED FOR HAUL ROUTE OR CONSTRUCTION ACCESS
- [Dotted line] = PROPOSED CONTRACTOR CONSTRUCTED TEMPORARY HAUL ROAD
- [Circle with cross, circle with dot, circle with X, or circle with solid dot] = PROPOSED BARRICADES, SEE NOTE 8 ON SHEET 7 AND NOTE 2 ON THIS SHEET
- [Line with cross-ticks] = PROPOSED RUNWAY RESTRICTED AREA LATH LINE
- [Dashed line with cross-ticks] = EXISTING RUNWAY / TAXIWAY SAFETY AREA
- [Dashed line with cross-ticks] = EXISTING RUNWAY RPZ



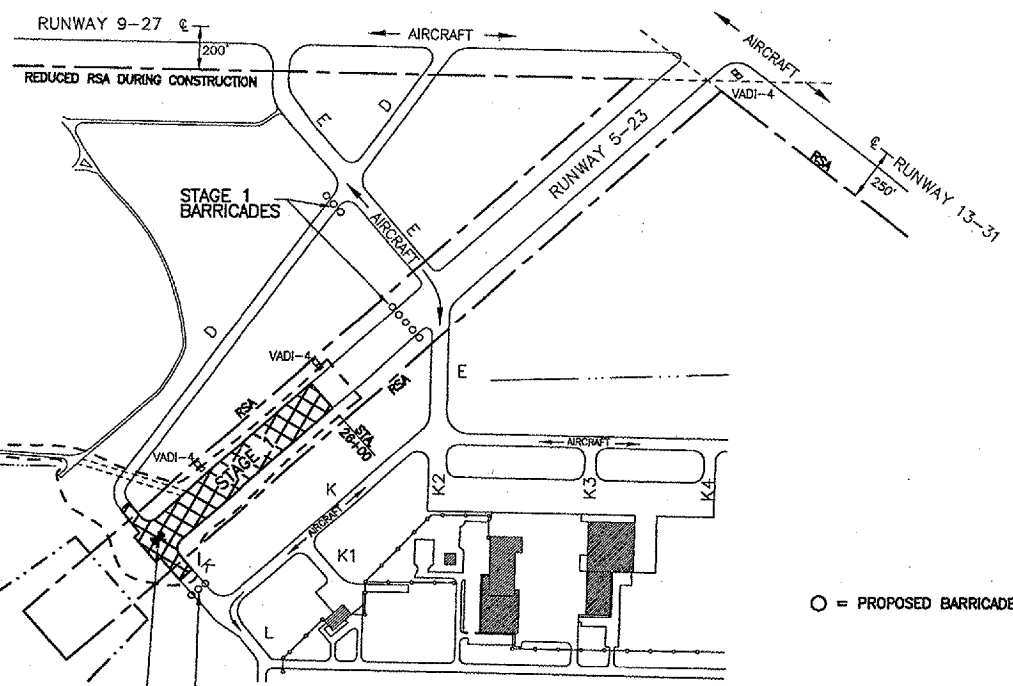
GENERAL NOTES:

- 1. SEE SHEETS 4 THRU 7 FOR SAFETY PLAN NOTES, CLOSED RUNWAY MARKING DETAILS, AND STAGING PLANS.
- 2. ALL TYPE II BARRICADES SHALL BE SPACED AT 20'c/c ACROSS THE WIDTH OF THE PAVEMENTS WITH A MINIMUM OF SEVEN BARRICADES PER LOCATION. SEE NOTE 8 ON SHEET 7 FOR ADDITIONAL INFORMATION.
- 3. SEE SHEET 4 FOR STAGE 3A, 3B, 4A, AND 4B BARRICADES AND FENCING.
- 4. PROPOSED CONSTRUCTION STAGING AND BATCH PLANT AREA EQUIPMENT HEIGHT LIMIT = 40 FEET.
- 5. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.

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STAGE 1 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. TAXIWAY D CLOSED BETWEEN TAXIWAY E AND RUNWAY 5-23.
3. TAXIWAY K CLOSED BETWEEN RUNWAY 5-23 AND TAXIWAY L.
4. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



RUNWAY 5 CLOSED RUNWAY MARKING OR LIGHTED MARKER, TYP. BOTH ENDS OF RWNY.

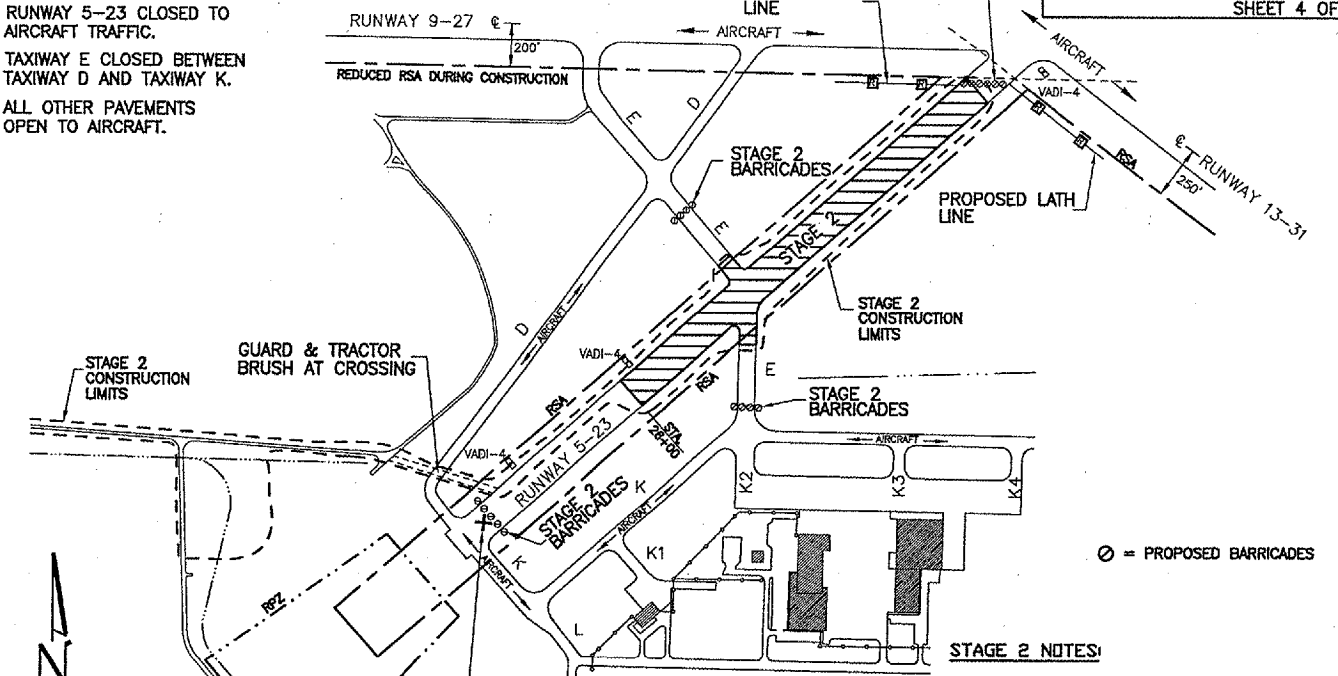
STAGE 1 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 1 NOTES:

1. COMPLETE STAGE 1 WORK AND OPEN TAXIWAYS D & K TO AIRCRAFT TRAFFIC PRIOR TO STARTING STAGE 2.
2. RUNWAY 5-23 CLOSED RUNWAY MARKINGS OR LIGHTED MARKERS IN PLACE WHEN CONTRACTOR IS IN THE R5-23 RSA.
3. ACCESS TO AIRFIELD AT SOUTH GATE ONLY.

STAGE 2 AIRFIELD STATUS

1. RUNWAY 5-23 CLOSED TO AIRCRAFT TRAFFIC.
2. TAXIWAY E CLOSED BETWEEN TAXIWAY D AND TAXIWAY K.
3. ALL OTHER PAVEMENTS OPEN TO AIRCRAFT.



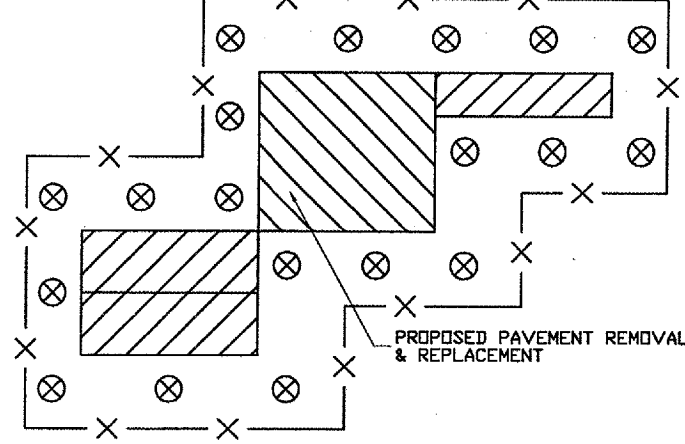
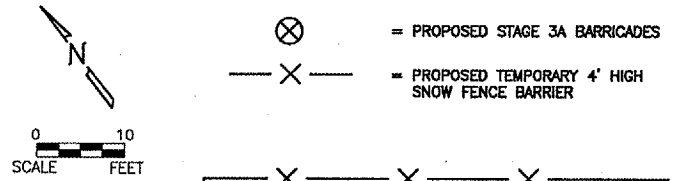
RUNWAY 5 CLOSED RUNWAY MARKING OR LIGHTED MARKER, TYP. BOTH ENDS OF RWNY.

STAGE 2 BARRICADES, LATH LINES, AND CONSTRUCTION OPERATION LIMITS

STAGE 2 NOTES:

1. COMPLETE STAGE 1 WORK AND OPEN TAXIWAYS D & K TO AIRCRAFT TRAFFIC PRIOR TO STARTING STAGE 2.
2. ACCESS TO AIRFIELD AT SOUTH GATE ONLY.
3. CONSTRUCTION IN THE STAGE 2 AREA SHALL NOT OCCUR SIMULTANEOUSLY WITH CONSTRUCTION IN THE 1 STAGE AREA.
4. RUNWAY 5-23 CLOSED RUNWAY MARKINGS OR LIGHTED MARKERS IN PLACE WHEN CONTRACTOR IS IN THE R5-23 RSA.

LEGEND:

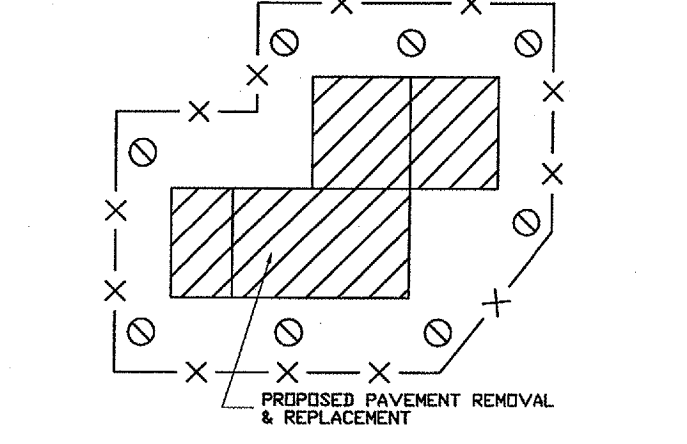
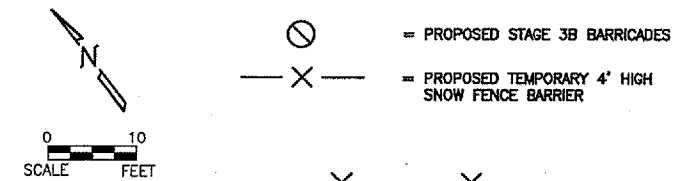


STAGE 3A NOTES:

1. COMPLETE THE STAGE 3A WORK AND OPEN THE STAGE 3A PAVEMENT TO AIRCRAFT TRAFFIC PRIOR TO STARTING WORK IN THE STAGE 4A AREA.
2. ACCESS TO THE STAGE 3A FROM THE NORTHWEST GATE ONLY.
3. NO CONSTRUCTION TRAFFIC ALLOWED ON THE NORTH APRON SOUTH OF CONCOURSE A & B.

STAGE 3A AREA BARRICADES AND TEMPORARY FENCING

LEGEND:

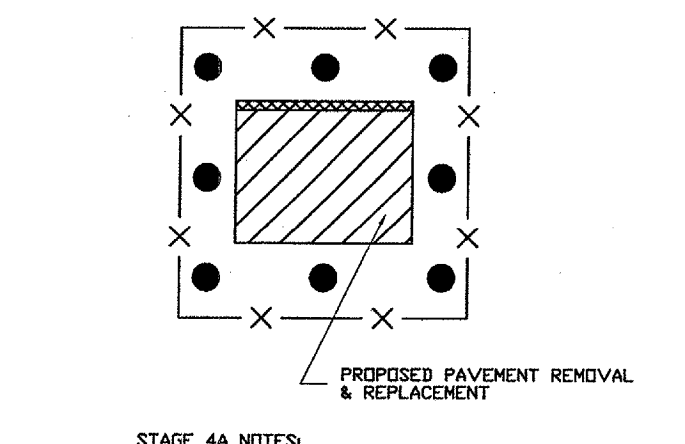
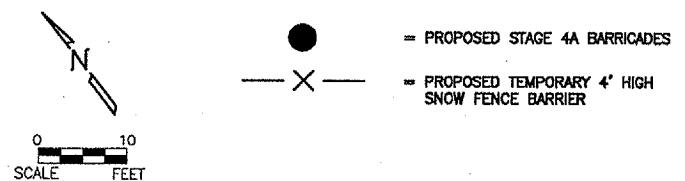


STAGE 3B NOTES:

1. COMPLETE THE STAGE 3B WORK AND OPEN THE STAGE 3B PAVEMENT TO AIRCRAFT TRAFFIC PRIOR TO STARTING WORK IN THE STAGE 4B AREA.
2. ACCESS TO THE STAGE 3B FROM THE NORTHEAST GATE ONLY.
3. NO CONSTRUCTION TRAFFIC ALLOWED ON THE NORTH APRON SOUTH OF CONCOURSE A & B.

STAGE 3B AREA BARRICADES AND TEMPORARY FENCING

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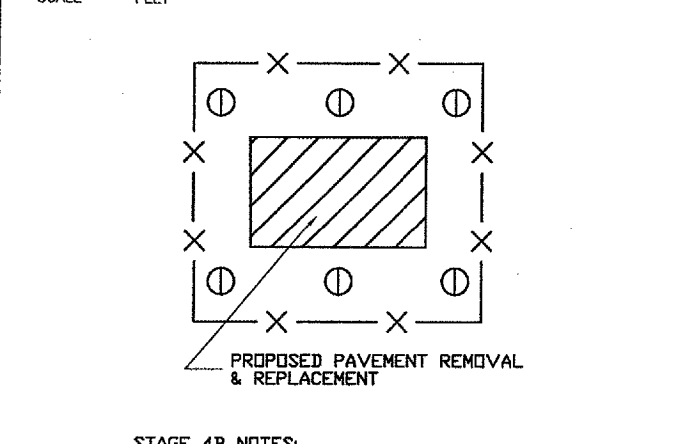
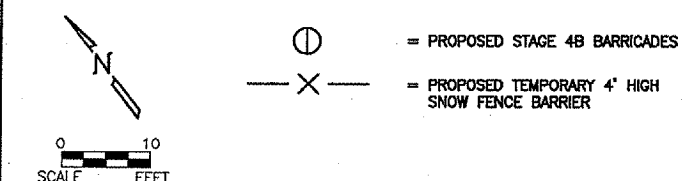


STAGE 4A NOTES:

1. CONSTRUCTION IN THE STAGE 4A AREA SHALL NOT OCCUR SIMULTANEOUSLY WITH CONSTRUCTION IN THE 3A STAGE AREA.
2. ACCESS TO THE STAGE 4A AREA FROM THE NORTHWEST GATE ONLY.
3. NO CONSTRUCTION TRAFFIC ALLOWED ON THE NORTH APRON SOUTHWEST OF THE STAGE 4A AREA.

STAGE 4A AREA BARRICADES AND TEMPORARY FENCING

LEGEND:



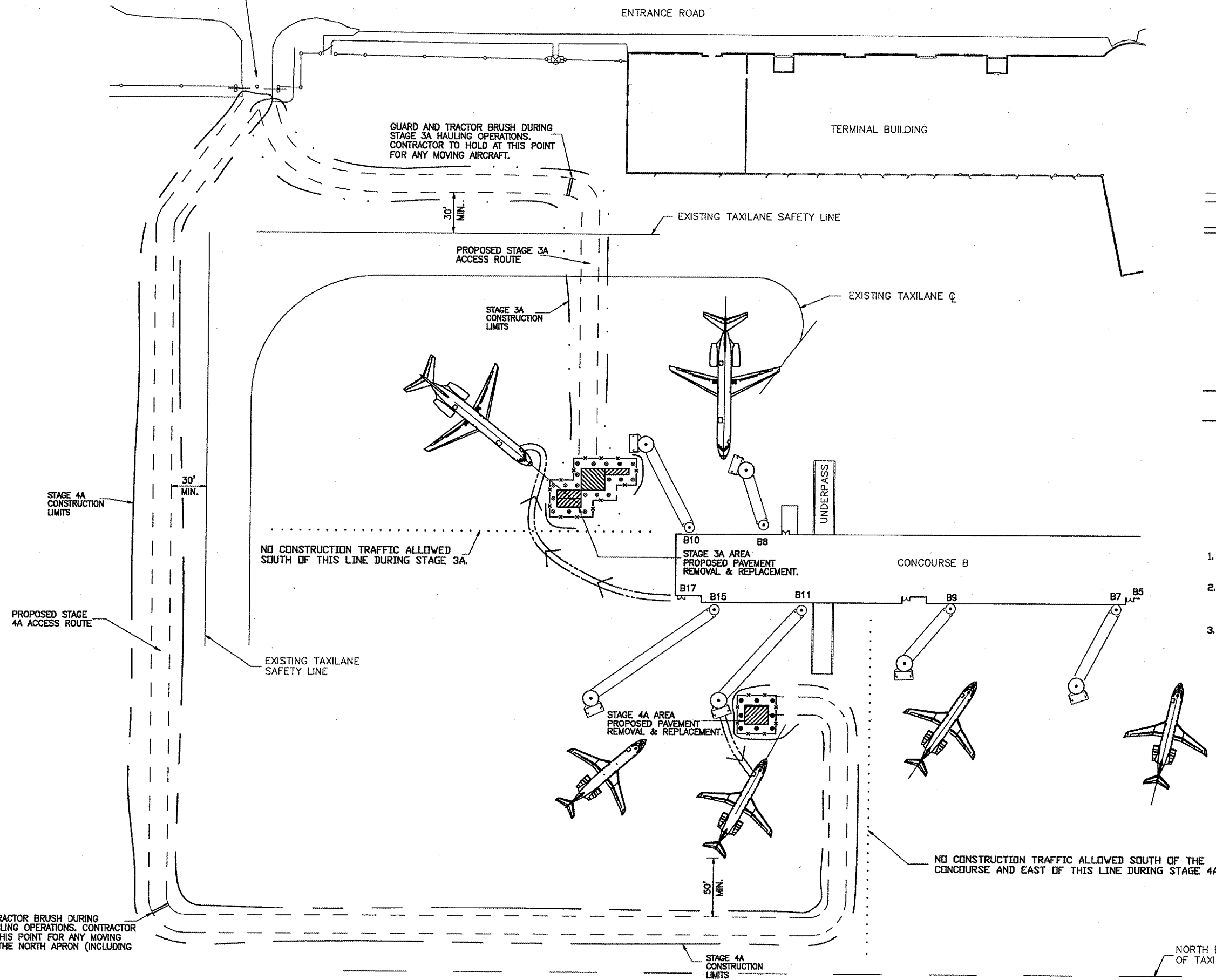
STAGE 4B NOTES:

1. CONSTRUCTION IN THE STAGE 4B AREA SHALL NOT OCCUR SIMULTANEOUSLY WITH CONSTRUCTION IN THE 3B STAGE AREA.
2. ACCESS TO THE STAGE 4B AREA FROM THE NORTHEAST GATE ONLY.
3. NO CONSTRUCTION TRAFFIC ALLOWED ON THE NORTH APRON NORTHWEST OF THE STAGE 4B AREA.

STAGE 4B AREA BARRICADES AND TEMPORARY FENCING

STAGE 3A AND STAGE 4A AIRFIELD ACCESS ROUTE

PROPOSED CONSTRUCTION ENTRANCE (TO STAGE 3A AND STAGE 4A AREAS ONLY) AT THIS EXISTING NORTHWEST GATE (EQUIPPED WITH ELECTRIC GATE OPENER). CONTRACTOR SHALL NOT ACCESS THE STAGE 3B OR 4B AREAS FROM THIS GATE. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE CLOSED OR GUARDED AT ALL TIMES.

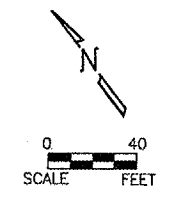


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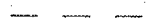
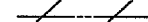
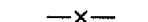





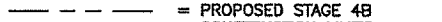

- = PROPOSED CONSTRUCTION ACCESS ROUTE
- <---> = PROPOSED AIRLINE PASSENGER GROUND LOADING ROUTE
- X- = PROPOSED TEMPORARY 4' HIGH SNOW FENCE BARRIER
- [Hatched Box] = PROPOSED PAVEMENT REMOVAL & REPLACEMENT AREA
- ⊗ = PROPOSED STAGE 3A BARRICADES
- = PROPOSED STAGE 4A BARRICADES
- . - . - = PROPOSED STAGE 3A CONSTRUCTION LIMITS
- - - - - = PROPOSED STAGE 4A CONSTRUCTION LIMITS

NOTES:

1. SEE SHEET 4 FOR ADDITIONAL INFORMATION ON BARRICADES AND FENCING.
2. AIRCRAFT WILL BE ARRIVING AND DEPARTING DURING CONSTRUCTION. CONTRACTOR SHALL CLEAR THE AREA FOR AIRCRAFT OPERATIONS.
3. AIRCRAFT PARKING POSITIONS ARE APPROXIMATE AND SUBJECT TO CHANGE.

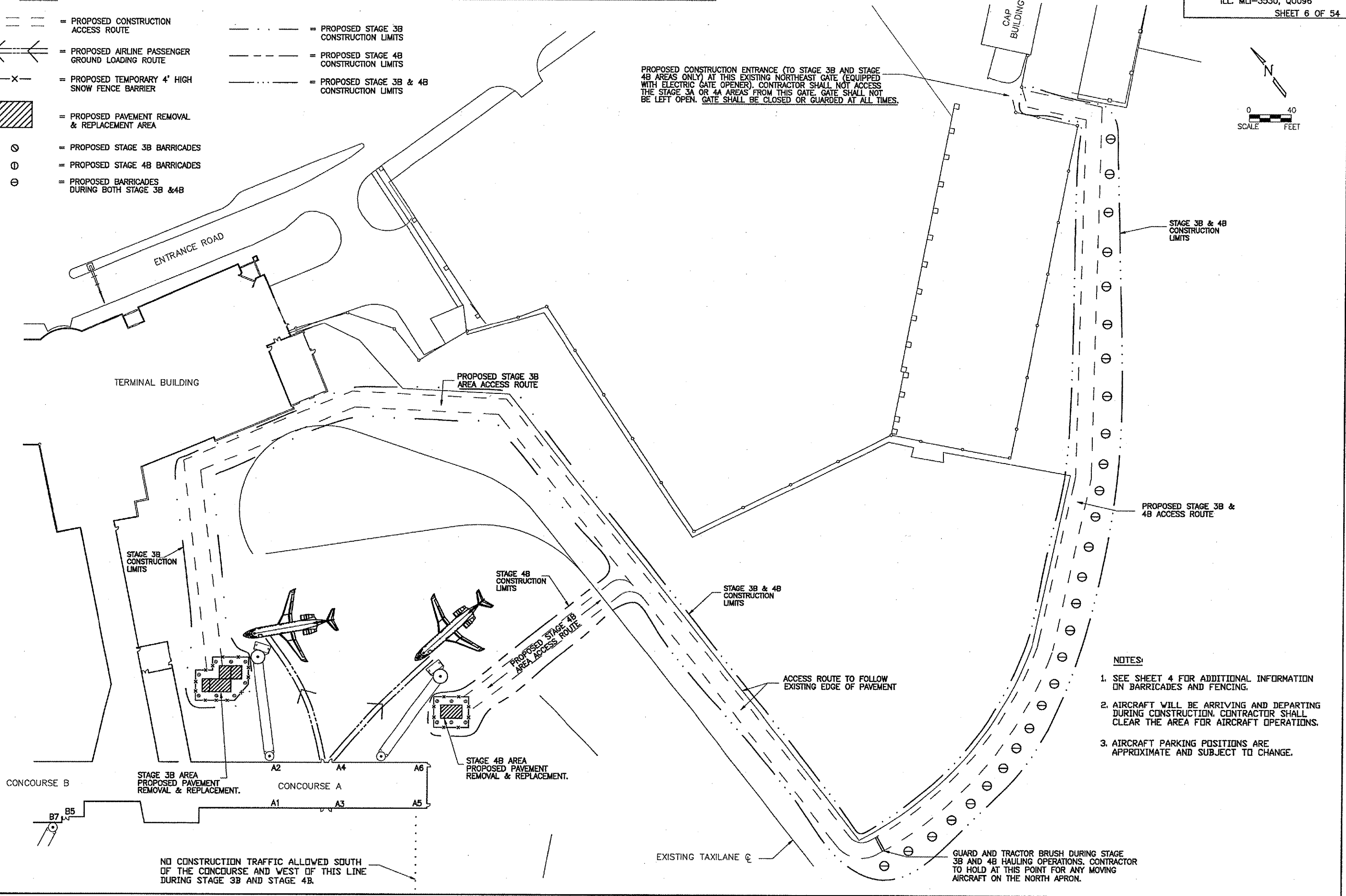
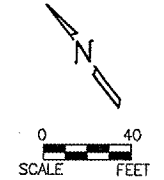


LEGEND:

-  = PROPOSED CONSTRUCTION ACCESS ROUTE
-  = PROPOSED AIRLINE PASSENGER GROUND LOADING ROUTE
-  = PROPOSED TEMPORARY 4' HIGH SNOW FENCE BARRIER
-  = PROPOSED PAVEMENT REMOVAL & REPLACEMENT AREA
-  = PROPOSED STAGE 3B BARRICADES
-  = PROPOSED STAGE 4B BARRICADES
-  = PROPOSED BARRICADES DURING BOTH STAGE 3B & 4B
-  = PROPOSED STAGE 3B CONSTRUCTION LIMITS
-  = PROPOSED STAGE 4B CONSTRUCTION LIMITS
-  = PROPOSED STAGE 3B & 4B CONSTRUCTION LIMITS

STAGE 3B AND 4B AIRFIELD ACCESS ROUTE

PROPOSED CONSTRUCTION ENTRANCE (TO STAGE 3B AND STAGE 4B AREAS ONLY) AT THIS EXISTING NORTHEAST GATE (EQUIPPED WITH ELECTRIC GATE OPENER). CONTRACTOR SHALL NOT ACCESS THE STAGE 3A OR 4A AREAS FROM THIS GATE. GATE SHALL NOT BE LEFT OPEN. GATE SHALL BE CLOSED OR GUARDED AT ALL TIMES.



NOTES:

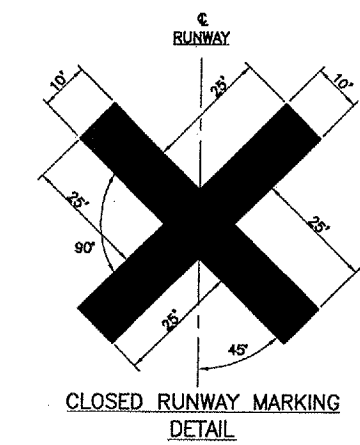
1. SEE SHEET 4 FOR ADDITIONAL INFORMATION ON BARRICADES AND FENCING.
2. AIRCRAFT WILL BE ARRIVING AND DEPARTING DURING CONSTRUCTION. CONTRACTOR SHALL CLEAR THE AREA FOR AIRCRAFT OPERATIONS.
3. AIRCRAFT PARKING POSITIONS ARE APPROXIMATE AND SUBJECT TO CHANGE.

NO CONSTRUCTION TRAFFIC ALLOWED SOUTH OF THE CONCOURSE AND WEST OF THIS LINE DURING STAGE 3B AND STAGE 4B.

GUARD AND TRACTOR BRUSH DURING STAGE 3B AND 4B HAULING OPERATIONS. CONTRACTOR TO HOLD AT THIS POINT FOR ANY MOVING AIRCRAFT ON THE NORTH APRON.

SAFETY PLAN NOTES:

1. THE METROPOLITAN AIRPORT AUTHORITY (MAA) SHALL DETERMINE WHEN AND WHERE THE CONTRACTOR WILL BE ALLOWED TO WORK. THE CONTRACTOR SHALL BE PREPARED TO EXIT THE RESTRICTED AREAS, SAFETY AREAS AND/OR AIR FIELD AT ALL TIMES WHEN DIRECTED TO DO SO BY THE MAA. ALL PAVEMENTS SHALL BE OPEN TO AIRCRAFT TRAFFIC UNLESS "NOTAMED" OTHERWISE DURING THE CONSTRUCTION OF THIS PROJECT. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION ON WORKING RESTRICTIONS AND CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF ALL BARRICADES AND LATH LINES AS SHOWN, PRIOR TO ANY EQUIPMENT ENTERING THE AIRFIELD OUTSIDE OF THE CONSTRUCTION STAGING AREA.
2. SOLID CLOSED RUNWAY MARKING CROSSES OR LIGHTED RUNWAY CLOSURE MARKERS ARE REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. SEE SPECIAL PROVISIONS FOR DETAILS OF CROSSES.
3. THE CONTRACTOR SHALL NOT TRAVEL IN OR THROUGH THE RESTRICTED AREAS AND/OR SAFETY AREAS UNLESS PERMISSION IS RECEIVED AND CONTACT HAS BEEN MADE WITH THE FAA CONTROL TOWER.
4. THE CONTRACTOR'S EMPLOYEES SHALL PARK IN THE PROPOSED CONSTRUCTION STAGING AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY CONTRACTOR'S MARKED VEHICLES AND EQUIPMENT SHALL BE ALLOWED ON THE AIRFIELD. ALL CONSTRUCTION VEHICLES AND EQUIPMENT, EXCEPT THE PAVING TRAIN, SHALL BE PARKED IN THE CONSTRUCTION STAGING AREA DURING ALL NON-WORKING HOUR. THE PAVING TRAIN MAY BE PARKED ON THE AIRFIELD OUTSIDE ALL RESTRICTED AREAS IN A LOCATION AUTHORIZED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL LOCATE HIS TRAILER, THE ENGINEERS FIELD OFFICE, AND ALL OTHER NECESSARY FACILITIES AND MATERIALS IN THE PROPOSED CONSTRUCTION STAGING AREA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONSTRUCT WHATEVER ACCESS ROAD HE DEEMS NECESSARY BETWEEN THE EXISTING ROADS AND THE CONSTRUCTION AREAS. OVERNIGHT PARKING OF EMPLOYEE VEHICLES IN THE CONSTRUCTION STAGING AREA WILL NOT BE ALLOWED UNLESS AUTHORIZED BY THE MAA.
5. THE CONTRACTOR SHALL PROCURE ENOUGH QUAD CITY AIRPORT SECURITY/IDENTIFICATION BADGES FOR HIS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES FROM THE AIRPORT AUTHORITY TO GUARANTEE AT LEAST ONE MEMBER OF EACH CONSTRUCTION CREW WILL HAVE A BADGE. ANY CONSTRUCTION CREW WITHOUT A BADGED MEMBER SHALL NOT BE ALLOWED ON THE AIRFIELD SITE. ALL INDIVIDUALS WHO ARE ISSUED SECURITY BADGES MUST CORRECTLY WEAR THEIR OWN BADGE WHILE ON THE AIRFIELD. BADGES MAY BE OBTAINED, AFTER MEETING SECURITY REQUIREMENTS, FROM THE SECURITY OFFICE AT THE QCI AIRPORT. A FIFTY DOLLAR (\$50.00) REFUNDABLE DEPOSIT IS REQUIRED FOR EACH BADGE.
6. THE CONTRACTOR SHALL USE THE DESIGNATED HAUL ROUTES, CONSTRUCTION STAGING AREA, AND ENTRANCE TO THE AIRFIELD AS SHOWN ON THE SAFETY PLAN SHEET. NO CONSTRUCTION TRAFFIC SHALL BE ALLOWED ON THE AIRFIELD OUTSIDE THE HAUL ROUTES AND WORK AREAS UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER. THE CONTRACTOR SHALL SUPPLY AND INSTALL TEMPORARY LOCKS ON EXISTING GATES AT THE PROPOSED CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL BE RESPONSIBLE TO KEEP THE CONSTRUCTION ENTRANCE GATE CLOSED AT ALL TIMES. VIOLATIONS ARE SUBJECT TO FINES/PENALTIES AND THE CONTRACTOR SHALL PAY ANY FINES INCURRED, INCLUDING FINES INCURRED BY THE ENGINEER AND/OR MAA DUE TO THE CONTRACTOR'S NEGLIGENCE. ALL VEHICULAR TRAFFIC SHALL BE KEPT TO A MINIMUM. ALL VEHICLES ON THE APRONS, RAMPS, TAXIWAYS, OR RUNWAYS REQUIRE THE APPROVAL OF THE ENGINEER / MAA.
7. THE CONTRACTOR AND EACH SUBCONTRACTOR SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WORK OR EQUIPMENT. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
8. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO INSTALL TYPE II BARRICADES EQUIPPED WITH FLASHING RED LIGHTS AND 20" X 20" ORANGE FLAGS AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR WILL FURNISH, MAINTAIN, AND MOVE THE BARRICADES AS REQUIRED BY THE RESIDENT ENGINEER. THE BARRICADES SHALL BE SUFFICIENTLY WEIGHTED WITH SANDBAGS OR OTHER APPROPRIATE METHOD TO WITHSTAND HIGH WINDS AND/OR JET BLAST WITHOUT DISLOCATION. BARRICADES SHALL BE CHECKED DAILY BY THE CONTRACTOR FOR PROPER PLACEMENT, ADEQUATE BALLAST, PROPER LIGHTING, PROPER FLAGGING AND WORKING BATTERIES. ALL DISCREPANCIES SHALL BE CORRECTED IMMEDIATELY. BARRICADES SHALL BE RELOCATED UPON THE COMPLETION OF EACH STAGE OF CONSTRUCTION. COST TO BE INCLUDED IN MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
9. WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRFIELD, THEY SHALL BE PROPERLY MARKED. THE MARKING SHALL CONSIST OF A THREE FOOT (3') SQUARE FLAG WITH A CHECKERED PATTERN OF INTERNATIONAL ORANGE AND WHITE SQUARES OF NOT LESS THAN ONE FOOT (1') ON EACH SIDE, DISPLAYED IN FULL VIEW ABOVE THE VEHICLE OR EQUIPMENT. EACH VEHICLE SHALL HAVE A FLASHING YELLOW LIGHT MOUNTED ON TOP OF THE ROOF.
10. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE THE CONSTRUCTION STAGING AREA, SERVICE ROADS, ACCESS ROADS, AND HAUL ROADS TO THEIR ORIGINAL CONDITIONS FOLLOWING COMPLETION OF CONSTRUCTION. THIS MAY INCLUDE, BUT NOT BE LIMITED TO, REGRADING, FERTILIZING, SEEDING AND MULCHING OF EARTH SURFACES AND/OR REGRADING, GRAVELING & SEAL COATING OF TREATED SURFACES, AS REQUIRED, TO THE SATISFACTION OF THE ENGINEER. ALL GROUND SURFACES, GRAVEL ROADS, PAVEMENTS, AND OTHER FACILITIES DAMAGED BY THE CONTRACTOR WHILE COMPLETING THE PROPOSED WORK SHALL BE REPAIRED OR RETURNED TO ITS ORIGINAL STATE. COST TO BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.
11. THE CONTRACTOR SHALL IMMEDIATELY SWEEP OR PICK UP ANY SOIL, DEBRIS, AGGREGATE CHIPS OR ROCK, OR LOOSE MATERIALS WHICH HAS BEEN DROPPED ONTO AIRPORT ROADS, RUNWAYS, TAXIWAYS, OR SODDED AREAS.
12. THE DISPOSAL OF ALL MATERIALS NOT TO BE INCORPORATED IN EMBANKMENTS ON THE PROJECT SHALL BE ACCOMPLISHED BY THE CONTRACTOR AT A LOCATION OFF AIRPORT PROPERTY.
13. THE SEQUENCE OF CONSTRUCTION OPERATIONS AND DESCRIPTION OF CONDITIONS ARE OUTLINED IN THE PROJECT SPECIAL PROVISIONS. THE CONTRACTOR SHALL GIVE THE MAA A 96 HOUR NOTICE PRIOR TO THE START OF ANY WORK REQUIRING THE CLOSING OF ANY PAVEMENTS TO AIRCRAFT TRAFFIC SO THAT A NOTAM CAN BE ISSUED.
14. WORKING HOURS OF THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL CONFORM TO ALL APPLICABLE LOCAL LAWS, INCLUDING ANY NOISE CONTROL.
15. NO MOUNDS OF DIRT OR IRREGULARITIES GREATER THAN 3" WHICH, IN THE OPINION OF THE RESIDENT ENGINEER, COULD INTERFERE WITH ANY AIRFIELD OPERATIONS WILL BE PERMITTED ON THE AIRFIELD. NO EXPOSED FACES IN EXCESS OF ONE AND ONE-HALF (1-1/2) INCHES IN HEIGHT AND 2:1 SLOPES ON ANY EXCAVATION WILL BE PERMITTED WITHIN THE RESTRICTED AREAS.
16. DUST ABATEMENT MEASURES WILL BE REQUIRED, WHEN IN THE OPINION OF THE RESIDENT ENGINEER, A HAZARD TO AIR TRAFFIC, LOCAL RESIDENCES, OR CONSTRUCTION PROJECT PERSONNEL EXISTS. PREVENTIVE MEASURES TO BE ACCOMPLISHED BY THE CONTRACTOR SHALL INCLUDE, BUT NOT BE LIMITED TO, WATERING AND TREATMENT WITH CALCIUM CHLORIDE.
17. BY THE END OF EACH WORK DAY AND PRIOR TO LEAVING THE AIRFIELD, THE CONTRACTOR SHALL HAVE THOROUGHLY SWEEPED THE AIR TRAFFIC CORRIDORS ADJACENT TO THE WORK AREAS TO REMOVE DUST AND DEBRIS. IN ADDITION, ALL AIR TRAFFIC AREAS USED BY CONSTRUCTION PERSONNEL AND EQUIPMENT MUST BE CONTINUOUSLY SWEEPED AND MAINTAINED FREE OF DEBRIS. SWEEPERS SHALL BE PROVIDED BY THE CONTRACTOR FOR THE ENTIRE LENGTH OF THE CONTRACT AND SHALL BE OF A TYPE CAPABLE OF REMOVING ALL DUST AND DEBRIS TO THE SATISFACTION OF THE MAA. SWEEPERS MUST BE COMMERCIAL QUALITY AND APPROVED BY THE RESIDENT ENGINEER AND MAA PRIOR TO THE START OF CONSTRUCTION.
18. THE CONTRACTOR SHALL INSTALL AND MAINTAIN LATH LINES DURING THE LENGTH OF THE PROJECT AS SHOWN OR DIRECTED BY THE RESIDENT ENGINEER. SEE PROJECT SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
19. NO CHANGES SHALL BE MADE IN ANY PROVISIONS OF THIS SAFETY PLAN UNLESS APPROVED IN WRITING BY THE METROPOLITAN AIRPORT AUTHORITY, THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, AND THE FEDERAL AVIATION ADMINISTRATION. THE COST OF ALL MEASURES NECESSARY TO COMPLY WITH THE SAFETY PLAN SHALL BE INCLUDED IN THE MAINTENANCE OF TRAFFIC LUMP SUM PRICE.



CLOSED RUNWAY MARKING
 DETAIL

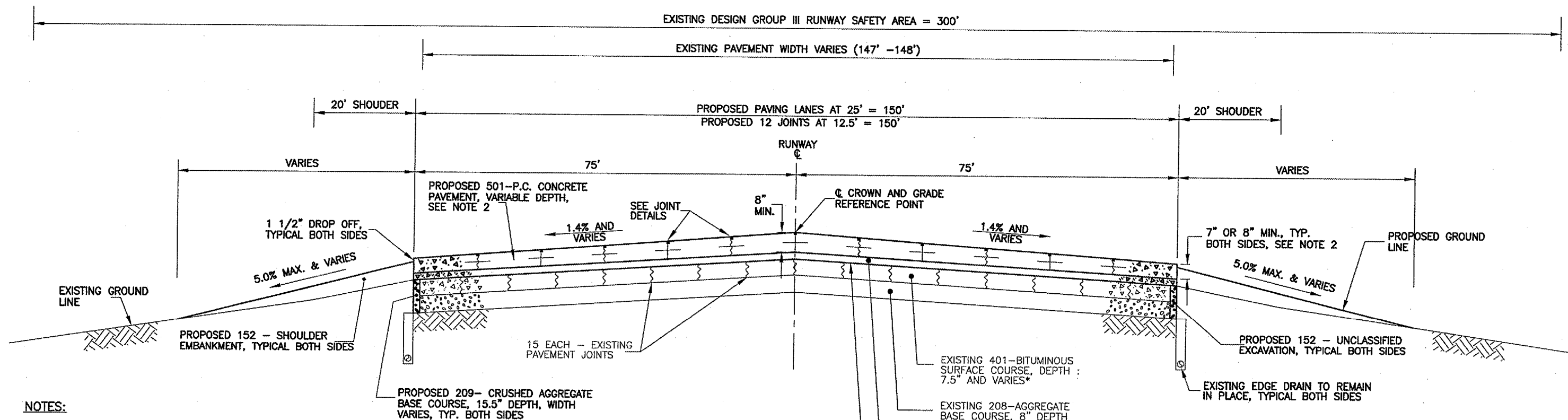
CLOSED RUNWAY MARKINGS NOTES:

1. A SOLID CROSS FOR CLOSED RUNWAY (OR A FULLY OPERATIONAL LIGHTED RUNWAY CLOSURE MARKER) IS REQUIRED AT EACH END OF THE RUNWAY DURING ALL RUNWAY CLOSURES. IF USED, SOLID CROSSES SHALL BE PLACED DIRECTLY OVER THE RUNWAY IDENTIFICATION NUMERALS.
2. THE CONTRACTOR SHALL INSTALL, REMOVE AND REINSTALL THE CROSSES AS REQUIRED BY WORKING CONDITIONS AND AS APPROVED BY THE RESIDENT ENGINEER.
3. COLOR OF ALL CROSSES SHALL BE AVIATION YELLOW.
4. SOLID CROSSES ARE TO BE CONSTRUCTED OF PLYWOOD, CANVAS, OR ANY OTHER APPROVED SOLID MATERIALS AND SHALL BE ADHERED TO THE RUNWAY IN A MANNER ACCEPTABLE TO THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE CROSSES IN EXCELLENT CONDITION.
5. LIGHTED RUNWAY CLOSURE MARKERS MAY BE USED INSTEAD OF SOLID CROSSES ON THE PAVEMENT TO MARK A CLOSE RUNWAY. IF USED, THE CONTRACTOR SHALL MAINTAIN THE LIGHTED RUNWAY CLOSURE MARKERS IN EXCELLENT CONDITION. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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AIRCRAFT APPROACH CATEGORY B
 AIRPLANE DESIGN GROUP III
 VISUAL RUNWAY

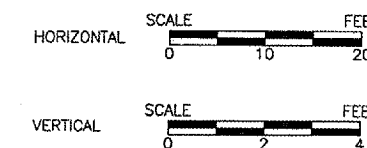
PROPOSED TYPICAL SECTION
 RUNWAY 5-23, STA. 15+40 TO STA. 44+35
 VARIABLE DEPTH P. C. CONCRETE OVERLAY



NOTES:

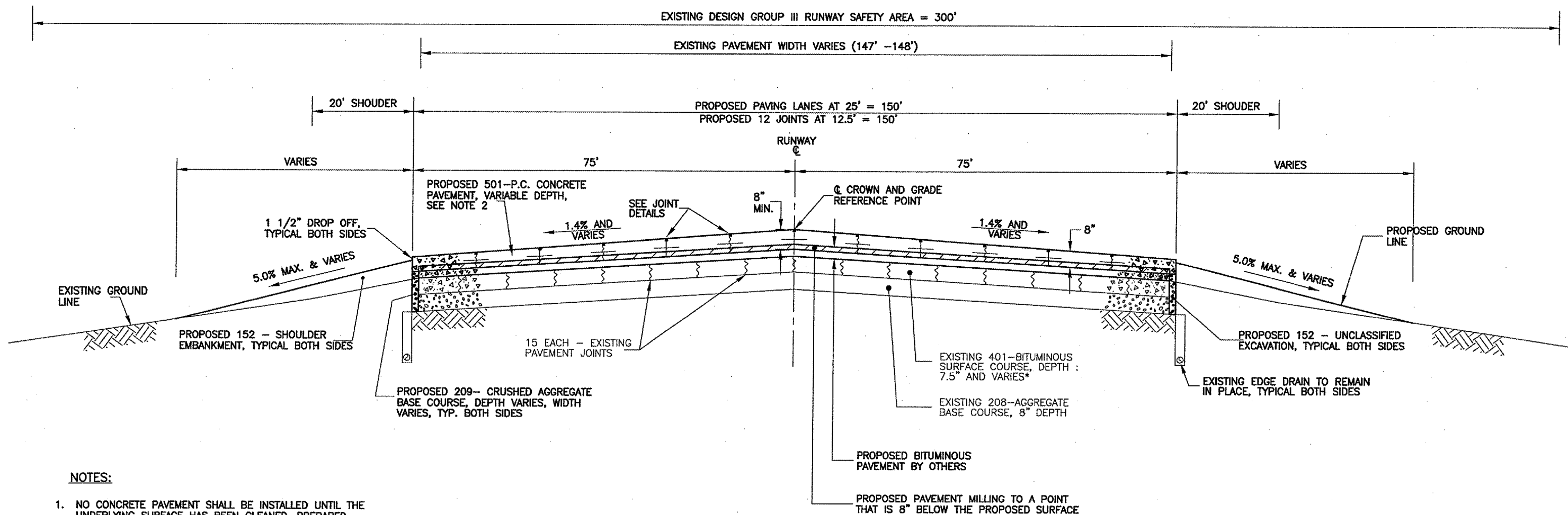
- NO CONCRETE PAVEMENT SHALL BE INSTALLED UNTIL THE UNDERLYING SURFACE HAS BEEN CLEANED, PREPARED, DAMPENED, AND ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- MINIMUM P.C.C. PAVEMENT DEPTH:
 STA. 15+40 TO STA. 18+40 AND
 STA. 31+18 TO STA. 35+18:
 RT. 75' TO LT. 75' = 8"
 STA. 18+40 TO STA. 31+18 AND
 STA. 35+18 TO STA. 46+30:
 RT. 75' TO RT. 25' = 7"
 RT. 25' TO LT. 25' = 8"
 LT. 25' TO LT. 75' = 7"
- * = EXISTING BIT. PVMT. DEPTH:
 STA. 15+40 TO STA. 45+55 : 7.5"
 STA. 45+55 TO STA. 49+06.4 : VARIES FROM 7.5" TO 16"
 STA. 49+06.4 TO STA. 51+29.1 : 16" (RUNWAY 9-27)
 STA. 51+29.1 TO STA. 53+80 : VARIES FROM 16" TO 7.5"
 STA. 53+80 TO STA. 63+81.46 : 7.5"
- MAXIMUM PAY WIDTH FOR 209 GRANULAR BASE SHALL BE AT THE EDGE OF THE PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVING, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.

SCALES:



AIRCRAFT APPROACH CATEGORY B
 AIRPLANE DESIGN GROUP III
 VISUAL RUNWAY

PROPOSED TYPICAL SECTION
 RUNWAY 5-23, STA. 44+35 TO STA. 46+30
 VARIABLE DEPTH P. C. CONCRETE OVERLAY



NOTES:

1. NO CONCRETE PAVEMENT SHALL BE INSTALLED UNTIL THE UNDERLYING SURFACE HAS BEEN CLEANED, PREPARED, DAMPENED, AND ACCEPTED BY THE ENGINEER. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
2. THE ACTUAL DEPTH OF THE PROPOSED P.C. CONCRETE PAVEMENT VARIES FROM APPROXIMATELY 0.58' TO 1.00'. THIS PAVEMENT SHALL BE PAID FOR UNDER CONTRACT ITEM AR501508, 8" PCC PAVEMENT PER SQUARE YARD REGARDLESS OF THE ACTUAL THICKNESS INSTALLED.
3. * = EXISTING BIT. PVMT. DEPTH:
 STA. 15+40 TO STA. 45+55 : 7.5"
 STA. 45+55 TO STA. 49+06.4 : VARIES FROM 7.5" TO 16"
 STA. 49+06.4 TO STA. 51+29.1 : 16" (RUNWAY 9-27)
 STA. 51+29.1 TO STA. 53+80 : VARIES FROM 16" TO 7.5"
 STA. 53+80 TO STA. 63+81.46 : 7.5"
4. MAXIMUM PAY WIDTH FOR 209 GRANULAR BASE SHALL BE AT THE EDGE OF THE PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVING, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL.

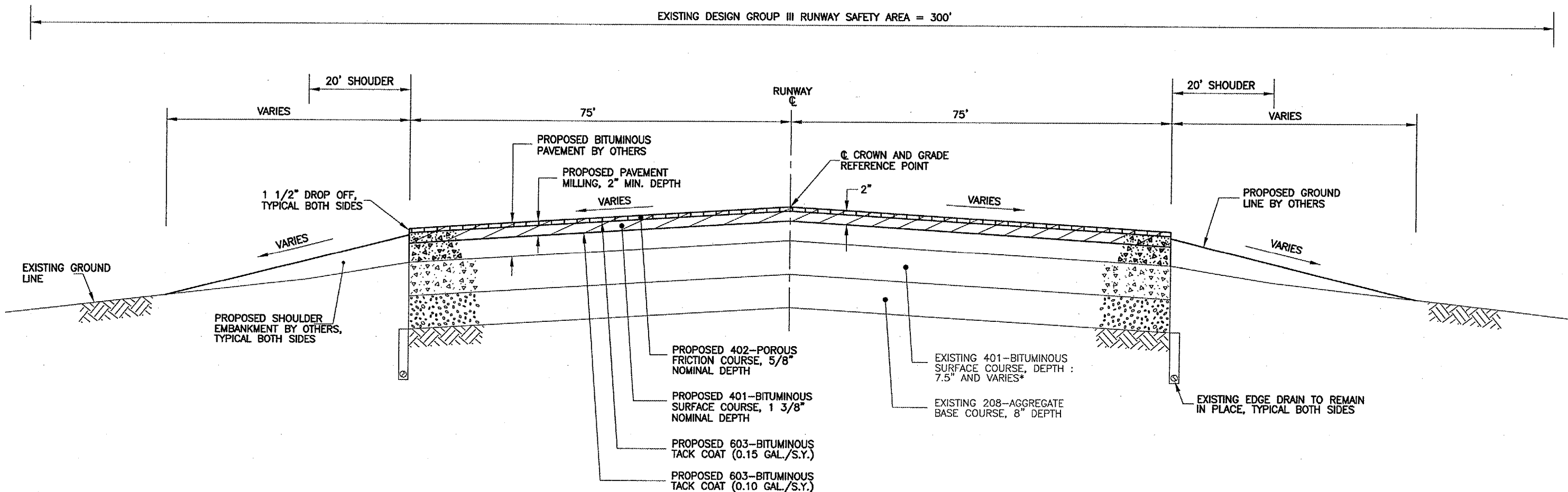
SCALES:



AIRCRAFT APPROACH CATEGORY B
 AIRPLANE DESIGN GROUP III
 VISUAL RUNWAY

PROPOSED TYPICAL SECTION
 RUNWAY 5-23, STA. 46+30 TO STA. 46+45
 BITUMINOUS INLAY

EXISTING DESIGN GROUP III RUNWAY SAFETY AREA = 300'



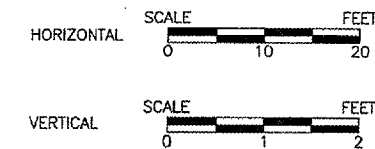
NOTES:

1. CONTRACTOR SHALL SPRAY A LIGHT COAT OF 603- BITUMINOUS TACK COAT MATERIALS BETWEEN ALL LAYERS OF BITUMINOUS AS DIRECTED BY THE ENGINEER.

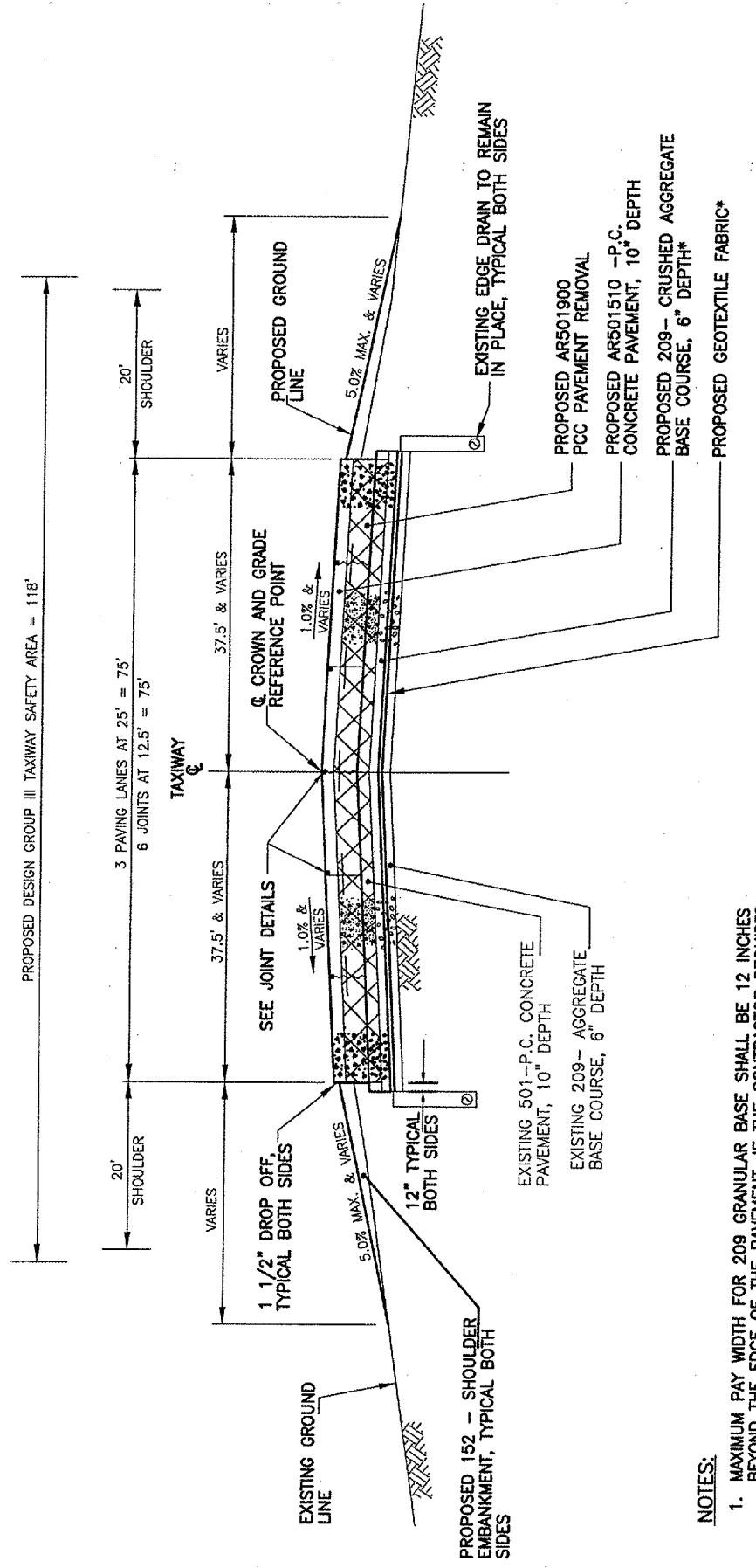
2. * = EXISTING BIT. PVMT. DEPTH:

- STA. 15+40 TO STA. 45+55 : 7.5"
- STA. 45+55 TO STA. 49+05 : VARIES FROM 7.5" TO 16"
- STA. 49+05 TO STA. 51+30 : 16" (RUNWAY 9-27)
- STA. 51+30 TO STA. 53+80 : VARIES FROM 16" TO 7.5"
- STA. 53+80 TO STA. 63+83.25 : 7.5"

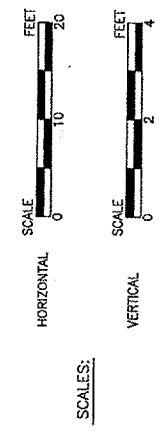
SCALES:



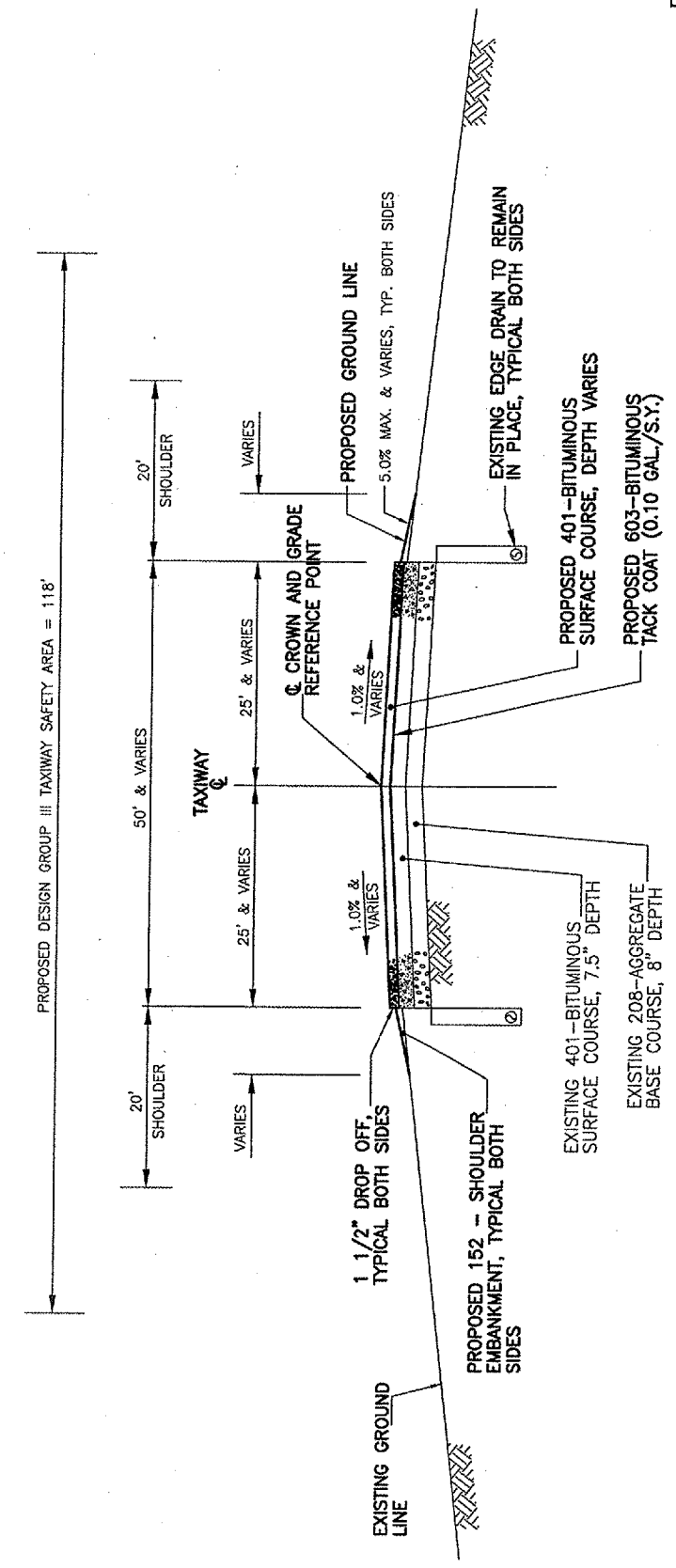
PROPOSED TYPICAL SECTION
 STA. 15+60 TO STA. 16+35 RIGHT (TAXIWAY K) &
 STA. 31+20 TO STA. 32+45 RIGHT (SOUTH TAXIWAY E)
 REMOVE AND REPLACE EXISTING PCC PAVEMENT



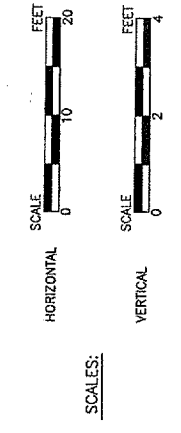
- NOTES:**
1. MAXIMUM PAY WIDTH FOR 209 GRANULAR BASE SHALL BE 12 INCHES BEYOND THE EDGE OF THE PAVEMENT. IF THE CONTRACTOR REQUIRES ADDITIONAL WIDTH FOR PAVING, THE ADDITIONAL MATERIALS SHALL MEET THE SAME SPECIFICATIONS, BUT WILL BE CONSIDERED INCIDENTAL
 2. TAXIWAY WIDTH = 75' WITHOUT RADIUS FILLETS OR TAPERS.
 3. * = IN LOCATIONS WHERE THE CONDITION OF THE EXISTING AGGREGATE BASE COURSE IS ACCEPTABLE TO THE ENGINEER, THE CONTRACTOR SHALL LEAVE THE EXISTING AGGREGATE BASE COURSE IN PLACE AND INSTALL NEW AGGREGATE BASE COURSE MATERIAL BETWEEN THE TOP OF THE EXISTING AGGREGATE BASE COURSE AND THE BOTTOM OF THE PROPOSED PAVEMENT. THE PROPOSED GEOTEXTILE FABRIC SHALL BE DELETED IN LOCATIONS WHERE NEW AGGREGATE IS PLACED ON THE EXISTING AGGREGATE.



PROPOSED TYPICAL SECTION
 STA. 15+60 TO STA. 16+10 LEFT (TAXIWAY D) &
 STA. 32+93 TO STA. 33+43 LEFT (NORTH TAXIWAY E)
 VARIABLE DEPTH BITUMINOUS OVERLAY

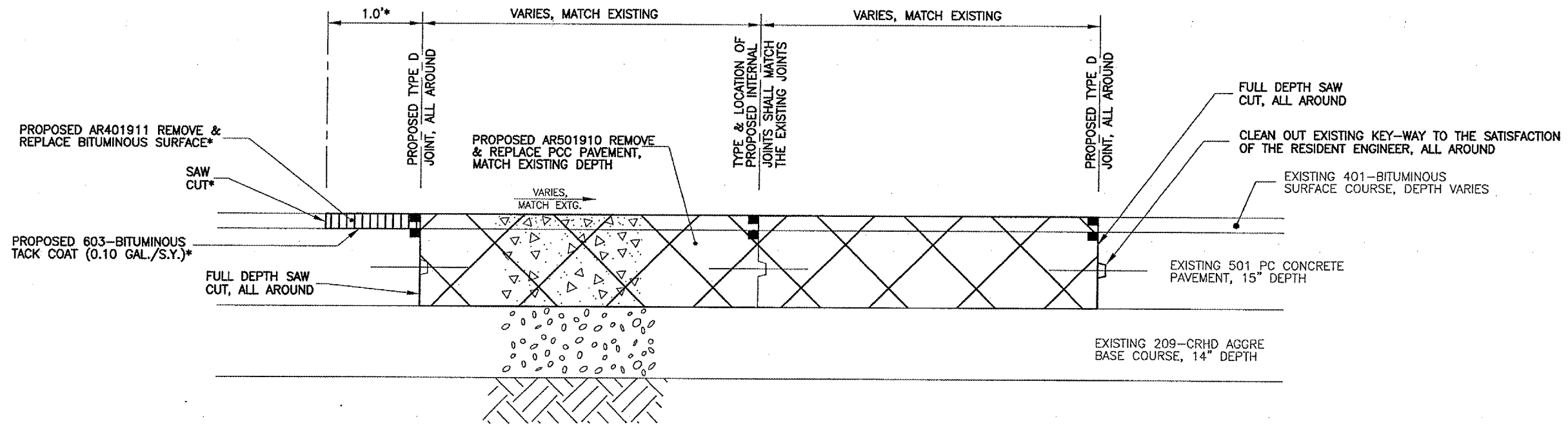


- NOTES:**
1. CONTRACTOR SHALL SPRAY A LIGHT COAT OF 603-BITUMINOUS TACK COAT MATERIALS BETWEEN ALL LAYERS OF BITUMINOUS AS DIRECTED BY THE ENGINEER.
 2. LONGITUDINAL JOINTS IN ONE LAYER SHALL OFFSET THE LONGITUDINAL JOINTS IN THE LAYER IMMEDIATELY BELOW BY AT LEAST ONE FOOT. IN THE TOP LAYER THERE SHALL BE A LONGITUDINAL JOINT AT THE CENTERLINE OF THE PAVEMENT.
 3. TRANSVERSE JOINTS IN ONE LAYER SHALL BE OFFSET BY AT LEAST TWO FEET FROM TRANSVERSE JOINTS IN THE PREVIOUS LAYER. TRANSVERSE JOINTS IN ADJACENT LANES SHALL BE OFFSET A MINIMUM OF TEN FEET.

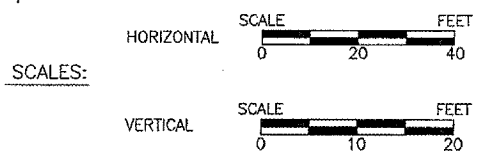
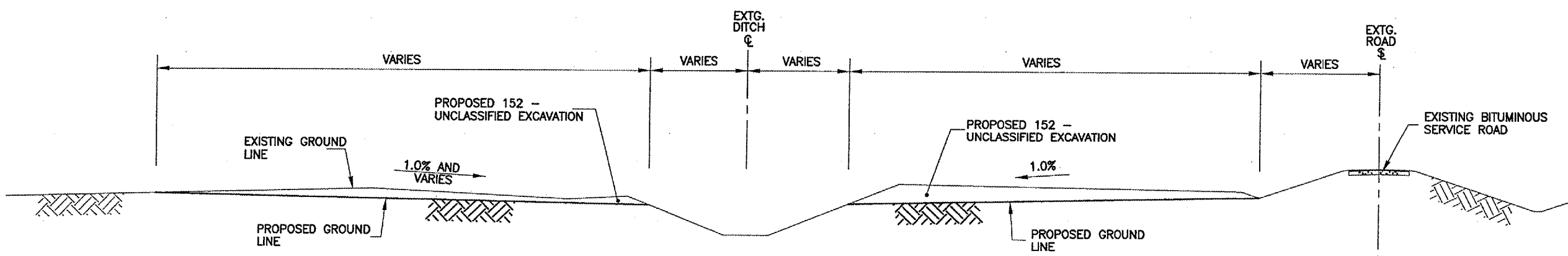


PROPOSED TYPICAL SECTION
TERMINAL AREA PAVEMENT REMOVAL & REPLACEMENT
 (BITUMINOUS PAVEMENT RUT REPAIR)

* = AR401911 REMOVE & REPLACE BITUMINOUS SURFACE REQUIRED ONLY WHERE SHOWN ON PLAN VIEW AND IN LOCATIONS SELECTED BY THE RESIDENT ENGINEER.

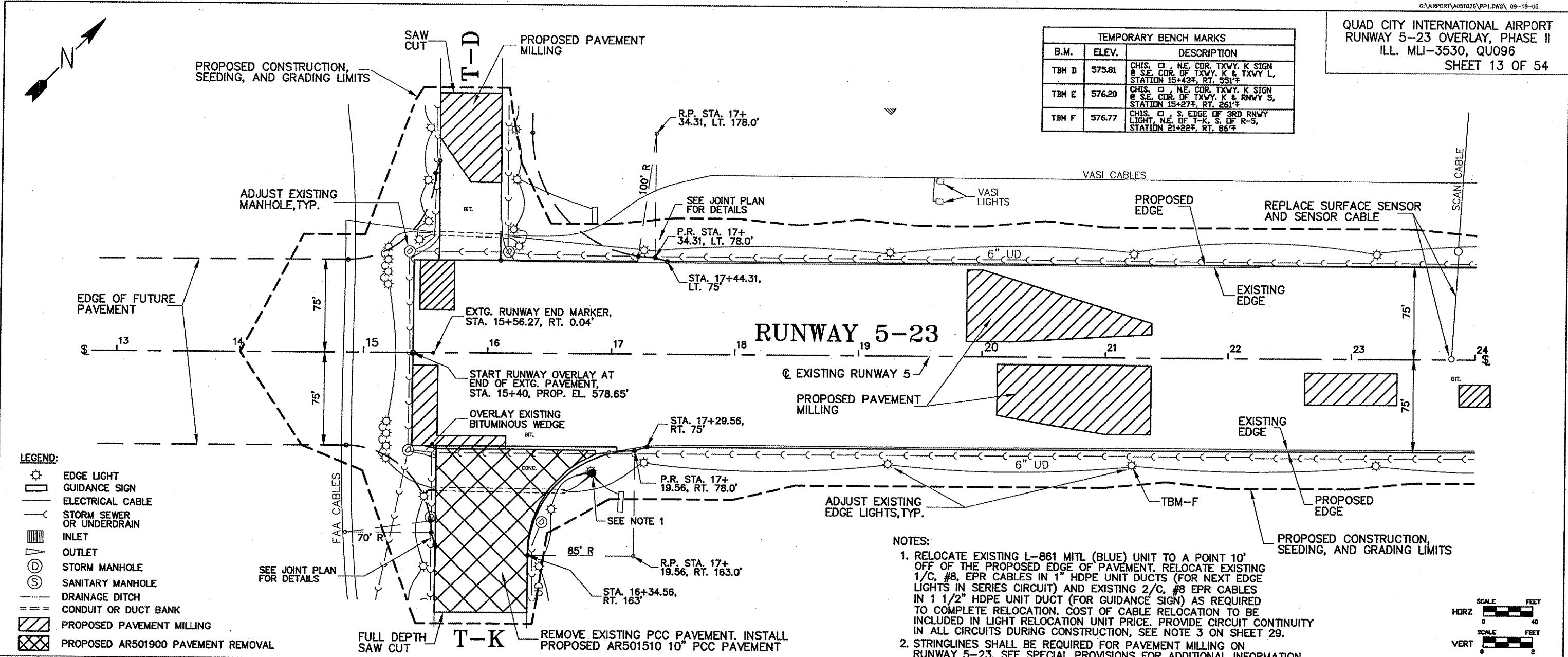


PROPOSED TYPICAL SECTION
EARTHWORK CUT AREA, STA. 107+80 TO STA. 111+50



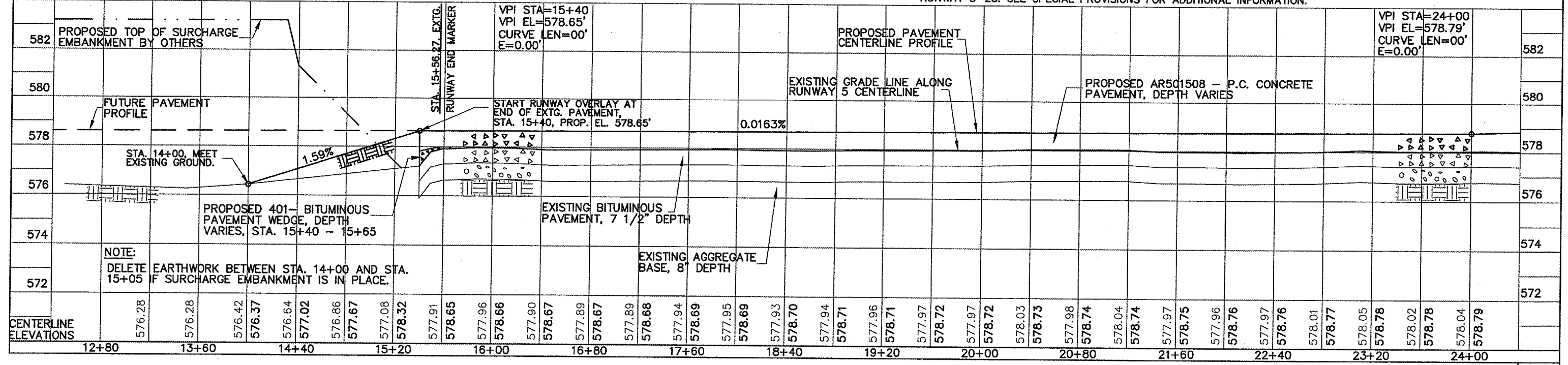
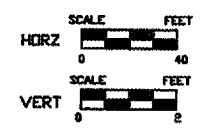
QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 5-23 OVERLAY, PHASE II
 ILL. MLI-3530, QU096
 SHEET 13 OF 54

B.M.	ELEV.	DESCRIPTION
TBM D	575.81	CHIS. □ NE COR. TAXWY. K SIGN @ S.E. COR. OF TAXWY. K & TAXWY L, STATION 15+437, RT. 551'±
TBM E	576.20	CHIS. □ NE COR. TAXWY. K SIGN @ S.E. COR. OF TAXWY. K & RWY 5, STATION 15+277, RT. 261'±
TBM F	576.77	CHIS. □ S. EDGE OF 3RD RWY LIGHT, N.E. OF T-K, S. OF R-S, STATION 21+227, RT. 867'±



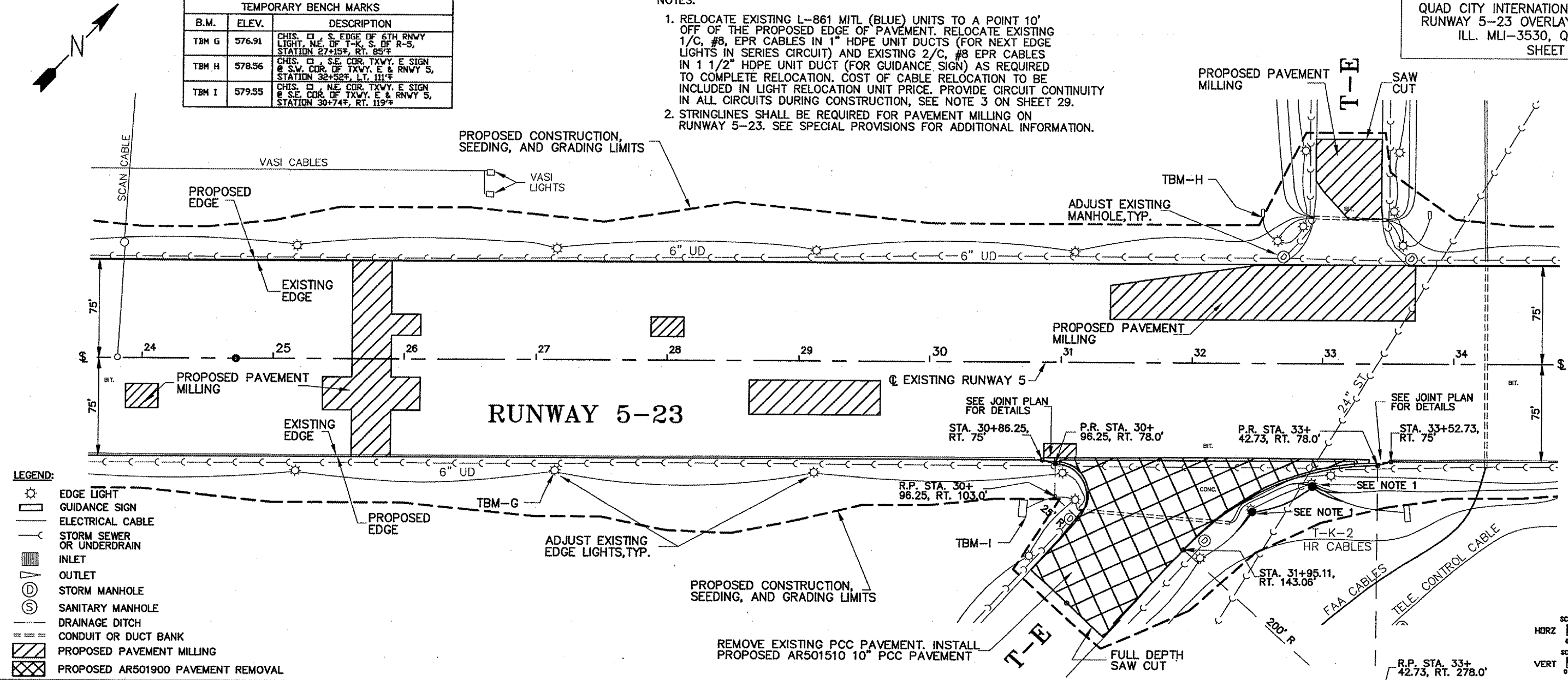
- LEGEND:**
- ☉ EDGE LIGHT
 - ▭ GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - ▭ INLET
 - ▽ OUTLET
 - ⊙ STORM MANHOLE
 - ⊙ SANITARY MANHOLE
 - DRAINAGE DITCH
 - ▭ CONDUIT OR DUCT BANK
 - ▨ PROPOSED PAVEMENT MILLING
 - ▩ PROPOSED AR501900 PAVEMENT REMOVAL

- NOTES:**
- RELOCATE EXISTING L-861 MITL (BLUE) UNIT TO A POINT 10' OFF OF THE PROPOSED EDGE OF PAVEMENT. RELOCATE EXISTING 1/C, #8, EPR CABLES IN 1" HDPE UNIT DUCTS (FOR NEXT EDGE LIGHTS IN SERIES CIRCUIT) AND EXISTING 2/C, #8 EPR CABLES IN 1 1/2" HDPE UNIT DUCT (FOR GUIDANCE SIGN) AS REQUIRED TO COMPLETE RELOCATION. COST OF CABLE RELOCATION TO BE INCLUDED IN LIGHT RELOCATION UNIT PRICE. PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS DURING CONSTRUCTION, SEE NOTE 3 ON SHEET 29.
 - STRINGLINES SHALL BE REQUIRED FOR PAVEMENT MILLING ON RUNWAY 5-23. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

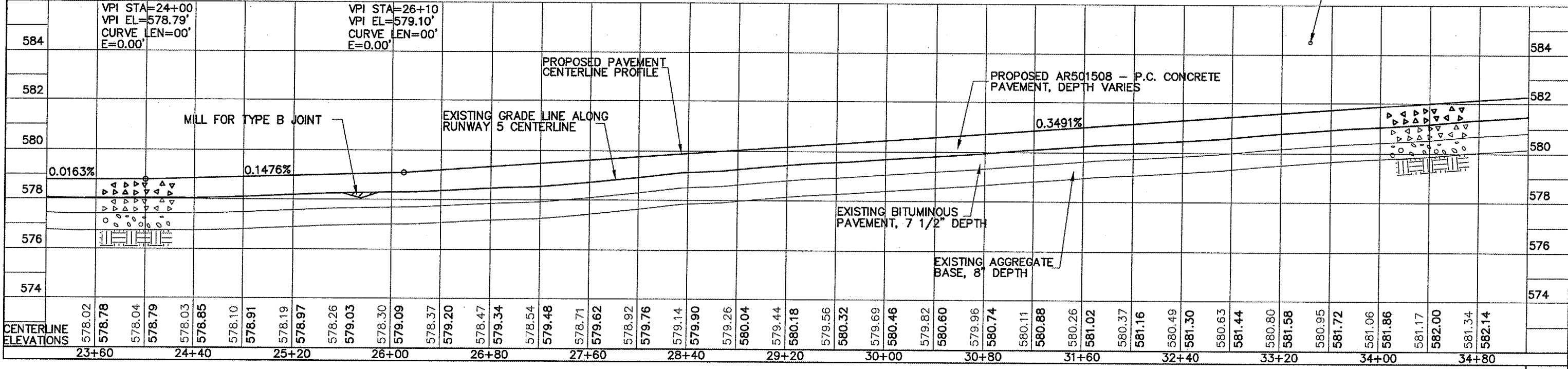
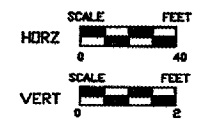


TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM G	576.91	CHIS. □, S. EDGE OF 6TH RWY LIGHT, N.E. OF T-K, S. OF R-S, STATION 27+15.7, RT. 85.7
TBM H	578.56	CHIS. □, S.E. COR. TXWY. E SIGN @ S.W. COR. OF TXWY. E & RWY 5, STATION 32+52.7, LT. 111.7
TBM I	579.55	CHIS. □, N.E. COR. TXWY. E SIGN @ S.E. COR. OF TXWY. E & RWY 5, STATION 30+74.7, RT. 119.7

- NOTES:
1. RELOCATE EXISTING L-861 MITL (BLUE) UNITS TO A POINT 10' OFF OF THE PROPOSED EDGE OF PAVEMENT. RELOCATE EXISTING 1/C, #8, EPR CABLES IN 1" HDPE UNIT DUCTS (FOR NEXT EDGE LIGHTS IN SERIES CIRCUIT) AND EXISTING 2/C, #8 EPR CABLES IN 1 1/2" HDPE UNIT DUCT (FOR GUIDANCE SIGN) AS REQUIRED TO COMPLETE RELOCATION. COST OF CABLE RELOCATION TO BE INCLUDED IN LIGHT RELOCATION UNIT PRICE. PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS DURING CONSTRUCTION, SEE NOTE 3 ON SHEET 29.
 2. STRINGLINES SHALL BE REQUIRED FOR PAVEMENT MILLING ON RUNWAY 5-23. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.



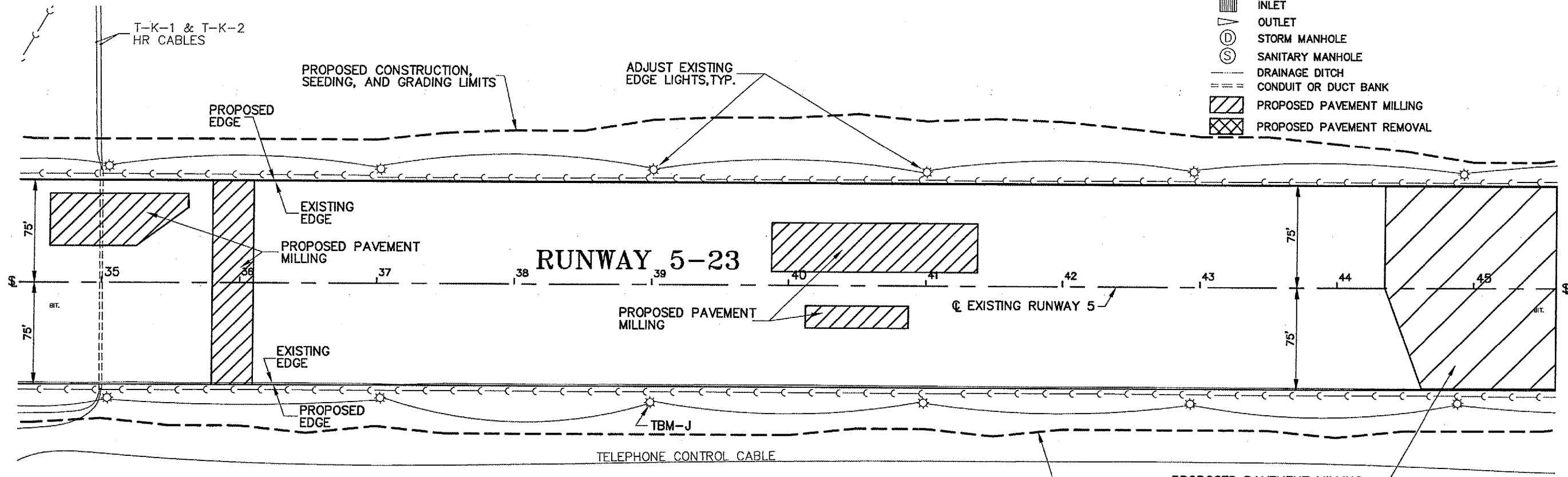
- LEGEND:
- ☉ EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - ▨ INLET
 - ▽ OUTLET
 - ⊙ STORM MANHOLE
 - ⊙ SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - ▨ PROPOSED PAVEMENT MILLING
 - ▨ PROPOSED AR501900 PAVEMENT REMOVAL



QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 5-23 OVERLAY, PHASE II
 ILL. MLI-3530, QU096
 SHEET 15 OF 54

NOTE:
 1. STRINGLINES SHALL BE REQUIRED FOR PAVEMENT MILLING ON
 RUNWAY 5-23. SEE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

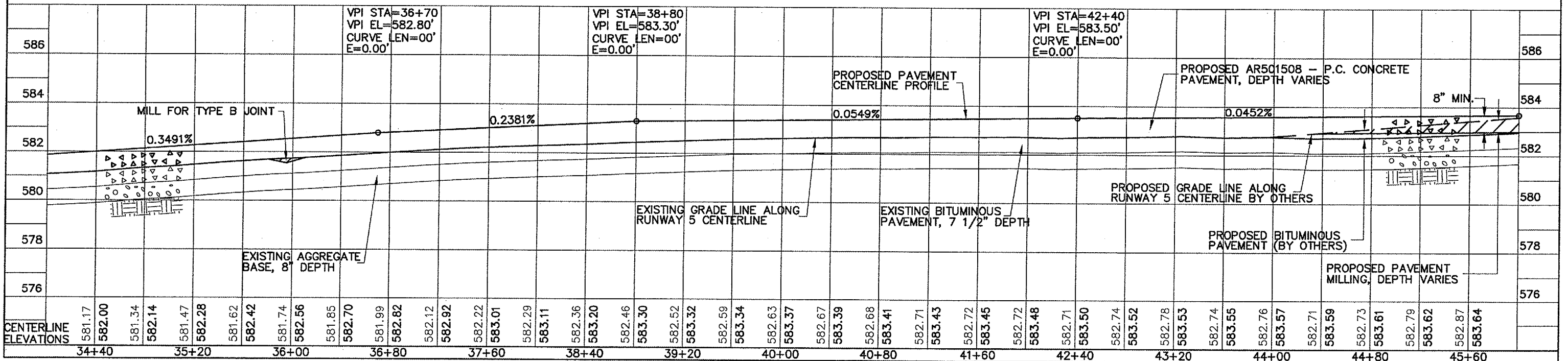
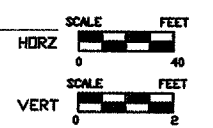
- LEGEND:
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - PROPOSED PAVEMENT MILLING
 - PROPOSED PAVEMENT REMOVAL



TEMPORARY BENCH MARKS		
B.M.	ELEV.	DESCRIPTION
TBM J	581.20	CHIS. □ S. EDGE OF 4TH RNVY LIGHT, N.E. OF T-E, S. OF R-5, STATION 39+00, RT. 87°
TBM K	581.38	CHIS. □ N.E. COR. R-5 HOLD SIGN, S. SIDE OF RNVY 5 @ MID-FIELD STATION 45+70, RT. 127°

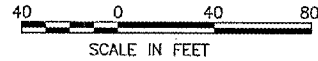
PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

PROPOSED PAVEMENT MILLING TO A POINT THAT IS 8" BELOW THE PROPOSED SURFACE



RUNWAY 5-23 PLAN & PROFILE STA 34+60 TO STA 45+60 15/54

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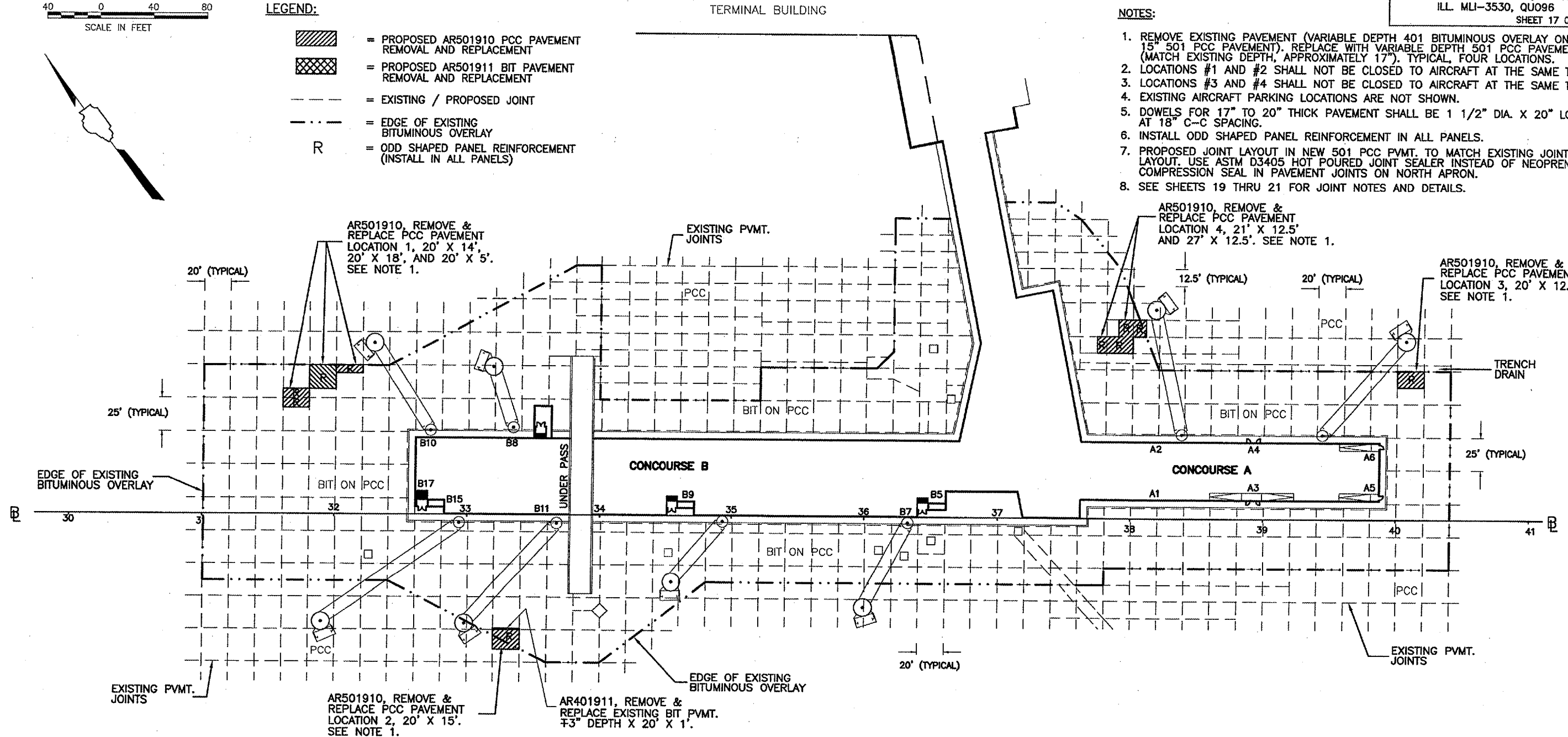


LEGEND:

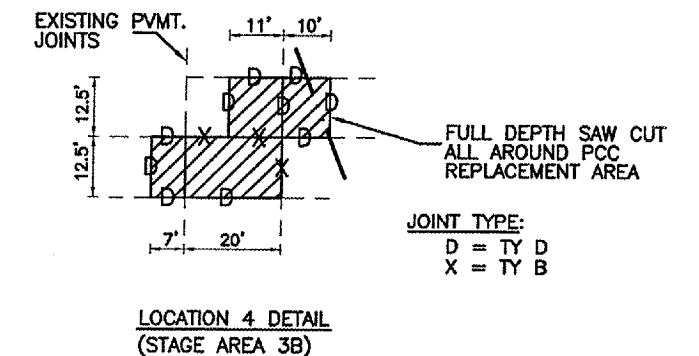
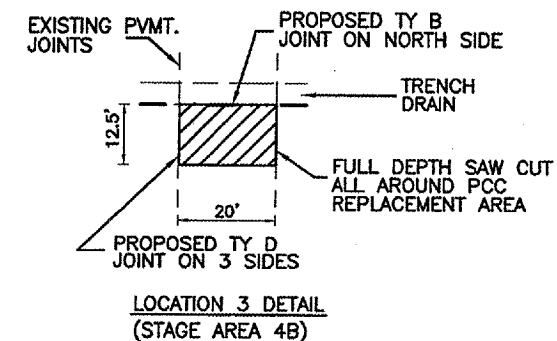
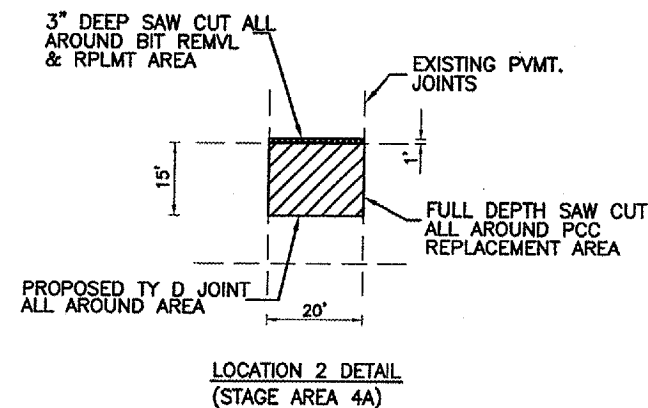
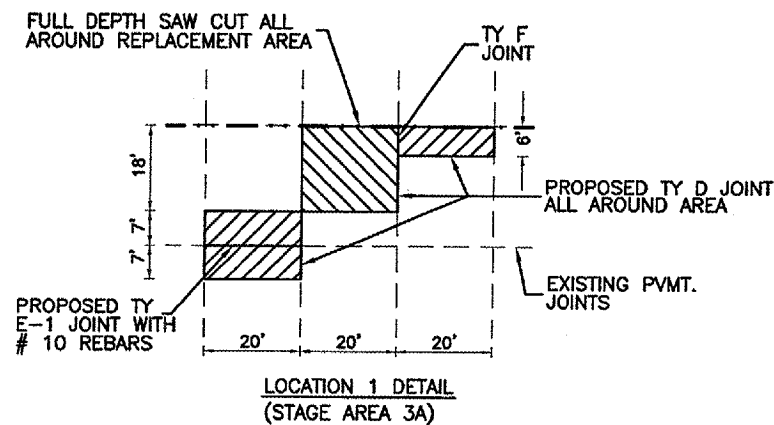
- = PROPOSED AR501910 PCC PAVEMENT REMOVAL AND REPLACEMENT
- = PROPOSED AR501911 BIT PAVEMENT REMOVAL AND REPLACEMENT
- = EXISTING / PROPOSED JOINT
- = EDGE OF EXISTING BITUMINOUS OVERLAY
- = ODD SHAPED PANEL REINFORCEMENT (INSTALL IN ALL PANELS)

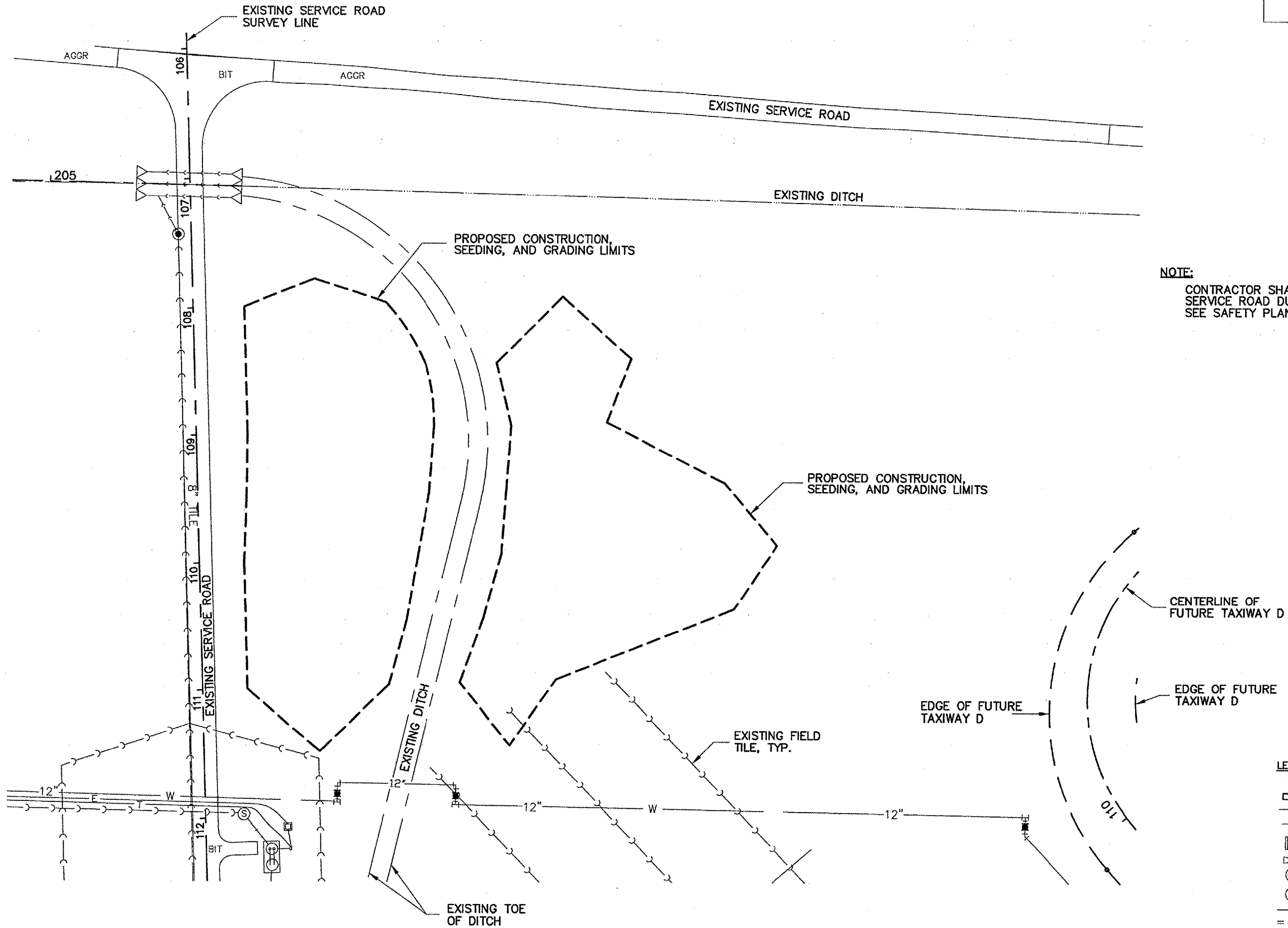
NOTES:

1. REMOVE EXISTING PAVEMENT (VARIABLE DEPTH 401 BITUMINOUS OVERLAY ON 15" 501 PCC PAVEMENT). REPLACE WITH VARIABLE DEPTH 501 PCC PAVEMENT (MATCH EXISTING DEPTH, APPROXIMATELY 17"). TYPICAL, FOUR LOCATIONS.
2. LOCATIONS #1 AND #2 SHALL NOT BE CLOSED TO AIRCRAFT AT THE SAME TIME.
3. LOCATIONS #3 AND #4 SHALL NOT BE CLOSED TO AIRCRAFT AT THE SAME TIME.
4. EXISTING AIRCRAFT PARKING LOCATIONS ARE NOT SHOWN.
5. DOWELS FOR 17" TO 20" THICK PAVEMENT SHALL BE 1 1/2" DIA. X 20" LONG AT 18" C-C SPACING.
6. INSTALL ODD SHAPED PANEL REINFORCEMENT IN ALL PANELS.
7. PROPOSED JOINT LAYOUT IN NEW 501 PCC PVMT. TO MATCH EXISTING JOINT LAYOUT. USE ASTM D3405 HOT POURED JOINT SEALER INSTEAD OF NEOPRENE COMPRESSION SEAL IN PAVEMENT JOINTS ON NORTH APRON.
8. SEE SHEETS 19 THRU 21 FOR JOINT NOTES AND DETAILS.



PAVEMENT REMOVAL & REPLACEMENT PLAN

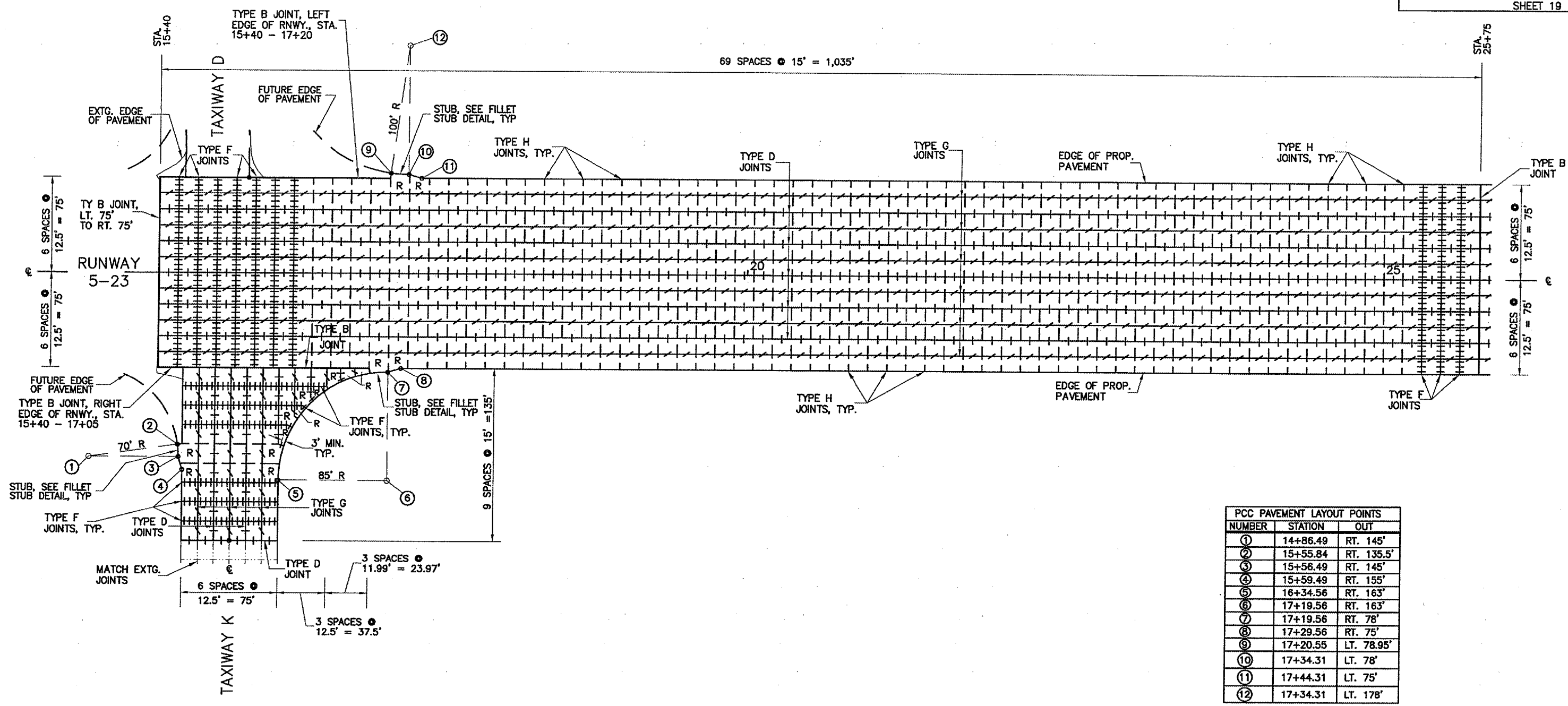




NOTE:
CONTRACTOR SHALL PROTECT EXISTING SERVICE ROAD DURING HAULING OPERATIONS. SEE SAFETY PLAN FOR ADDITIONAL INFORMATION.

- LEGEND:**
- EDGE LIGHT
 - GUIDANCE SIGN
 - ELECTRICAL CABLE
 - STORM SEWER OR UNDERDRAIN
 - INLET
 - OUTLET
 - STORM MANHOLE
 - SANITARY MANHOLE
 - DRAINAGE DITCH
 - CONDUIT OR DUCT BANK
 - FENCE LINE
 - POWER POLE
 - PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS

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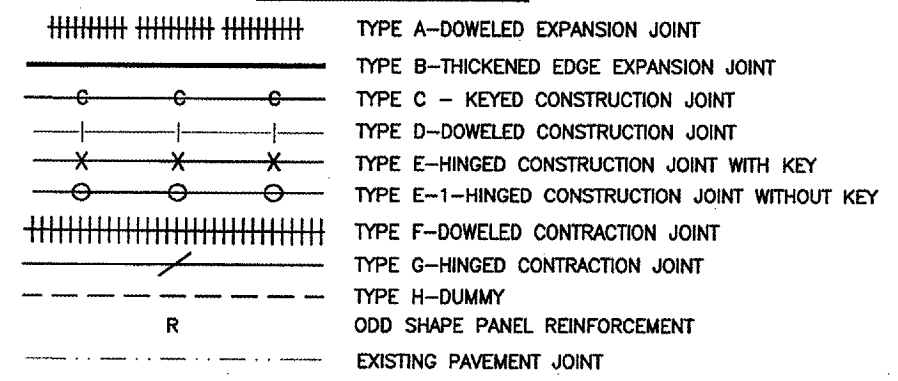


PCC PAVEMENT LAYOUT POINTS		
NUMBER	STATION	OUT
①	14+86.49	RT. 145'
②	15+55.84	RT. 135.5'
③	15+56.49	RT. 145'
④	15+59.49	RT. 155'
⑤	16+34.56	RT. 163'
⑥	17+19.56	RT. 163'
⑦	17+19.56	RT. 78'
⑧	17+29.56	RT. 75'
⑨	17+20.55	LT. 78.95'
⑩	17+34.31	LT. 78'
⑪	17+44.31	LT. 75'
⑫	17+34.31	LT. 178'

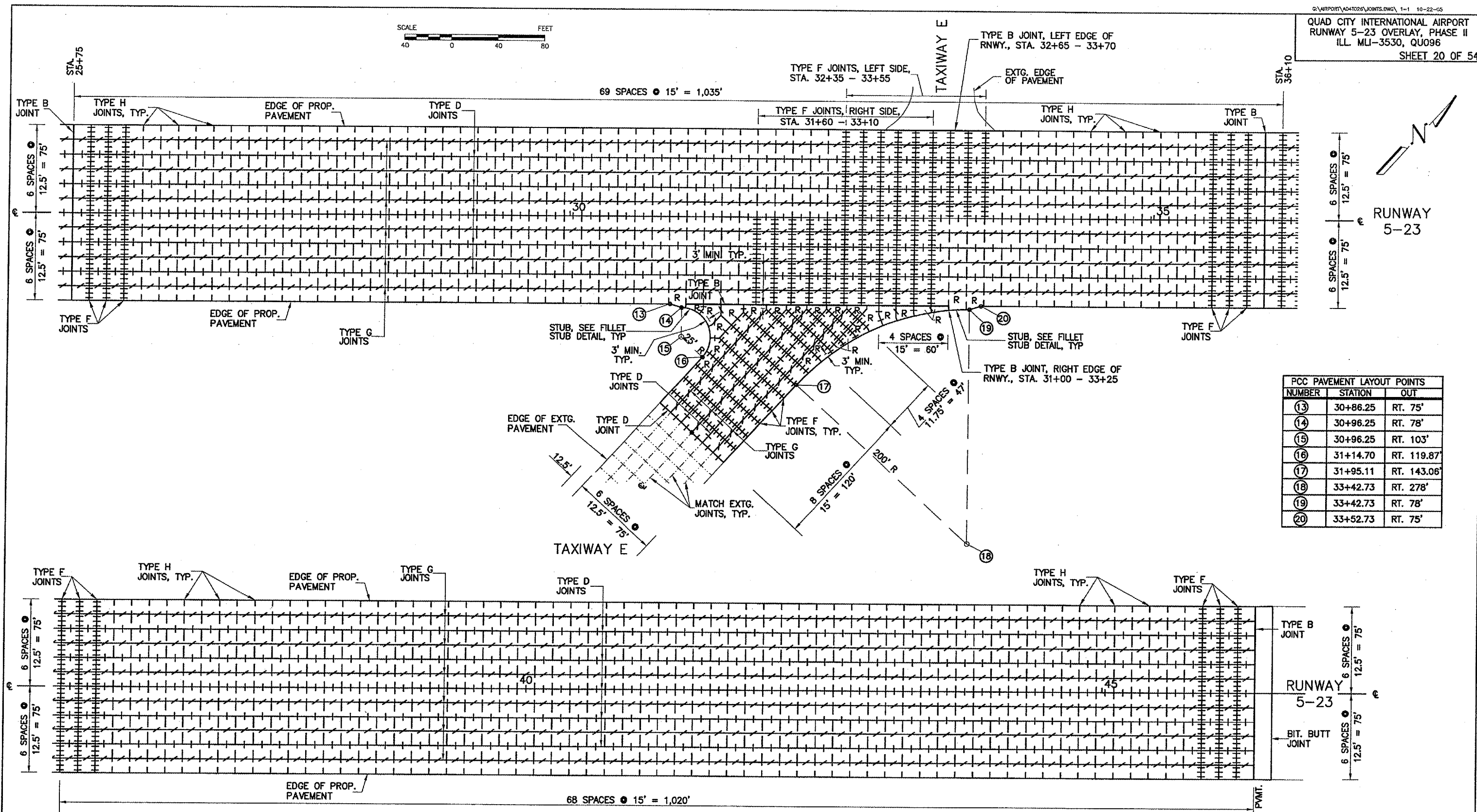
JOINT NOTES:

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- METAL FORMS USED FOR KEYED JOINTS MAY BE LEFT IN PLACE.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH AASHTO M137.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO DIMENSIONS OF THE SECOND SAWCUT WILL BE ALLOWED.
- SEE SHEET 21 FOR JOINT DETAILS, FILLET STUB DETAILS, FILLET TAPER DETAILS, AND ODD SHAPE PANEL REINFORCEMENT DETAILS.
- TAXIWAY E / TAXIWAY K EXISTING JOINT LOCATIONS AND SPACING BASED ON ORIGINAL PLANS AND/OR AERIAL PHOTOS. ACTUAL EXISTING JOINT LOCATIONS AND SPACING MAY VARY.

JOINT SYMBOL LEGEND



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PCC PAVEMENT LAYOUT POINTS		
NUMBER	STATION	OUT
13	30+86.25	RT. 75'
14	30+96.25	RT. 78'
15	30+96.25	RT. 103'
16	31+14.70	RT. 119.87'
17	31+95.11	RT. 143.06'
18	33+42.73	RT. 278'
19	33+42.73	RT. 78'
20	33+52.73	RT. 75'

JOINT SYMBOL LEGEND

- | | | | |
|-----|---|-------|--|
| | TYPE A-DOWELED EXPANSION JOINT | ○—○—○ | TYPE E-1-HINGED CONSTRUCTION JOINT WITHOUT KEY |
| — | TYPE B-THICKENED EDGE EXPANSION JOINT | | TYPE F-DOWELED CONTRACTION JOINT |
| —○— | TYPE C-KEYED CONSTRUCTION JOINT | —/— | TYPE G-HINGED CONTRACTION JOINT |
| — — | TYPE D-DOWELED CONSTRUCTION JOINT | --- | TYPE H-DUMMY |
| —X— | TYPE E-HINGED CONSTRUCTION JOINT WITH KEY | R | ODD SHAPE PANEL REINFORCEMENT |
| | | --- | EXISTING PAVEMENT JOINT |

- NOTES:**
- SEE SHEET 19 FOR JOINT NOTES.
 - SEE SHEET 21 FOR JOINT DETAILS.

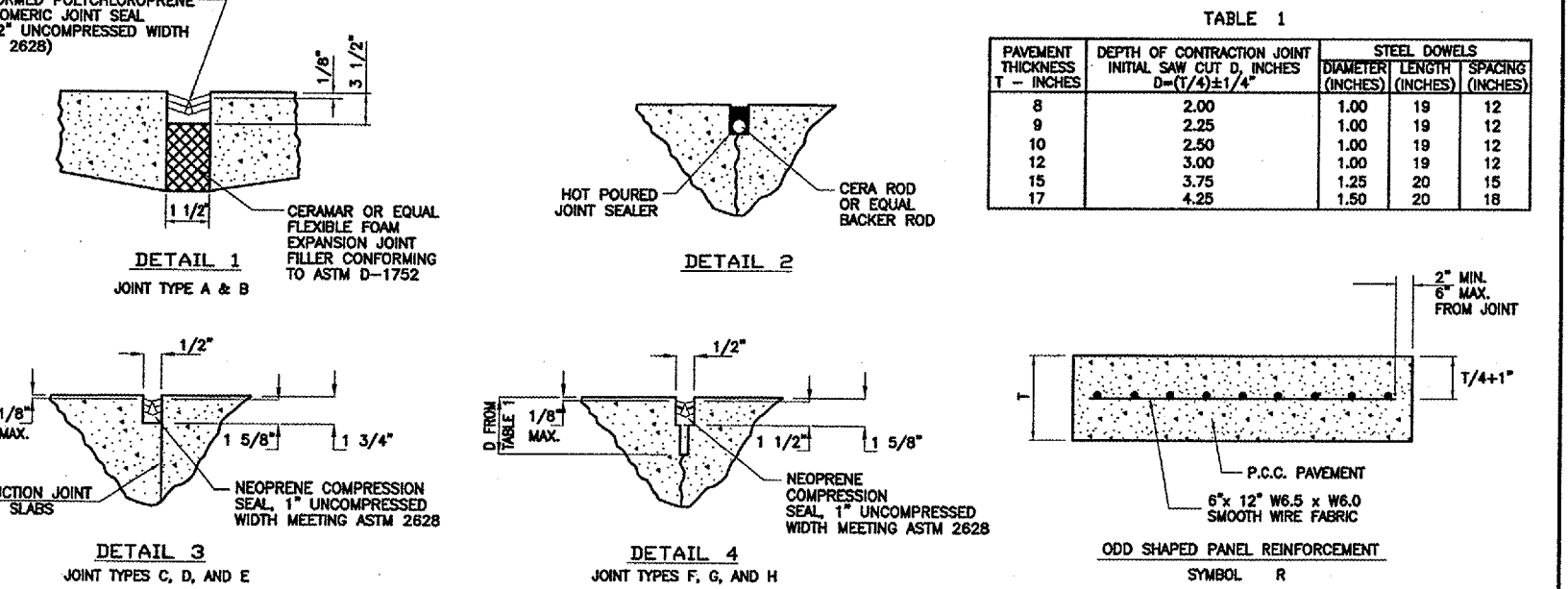
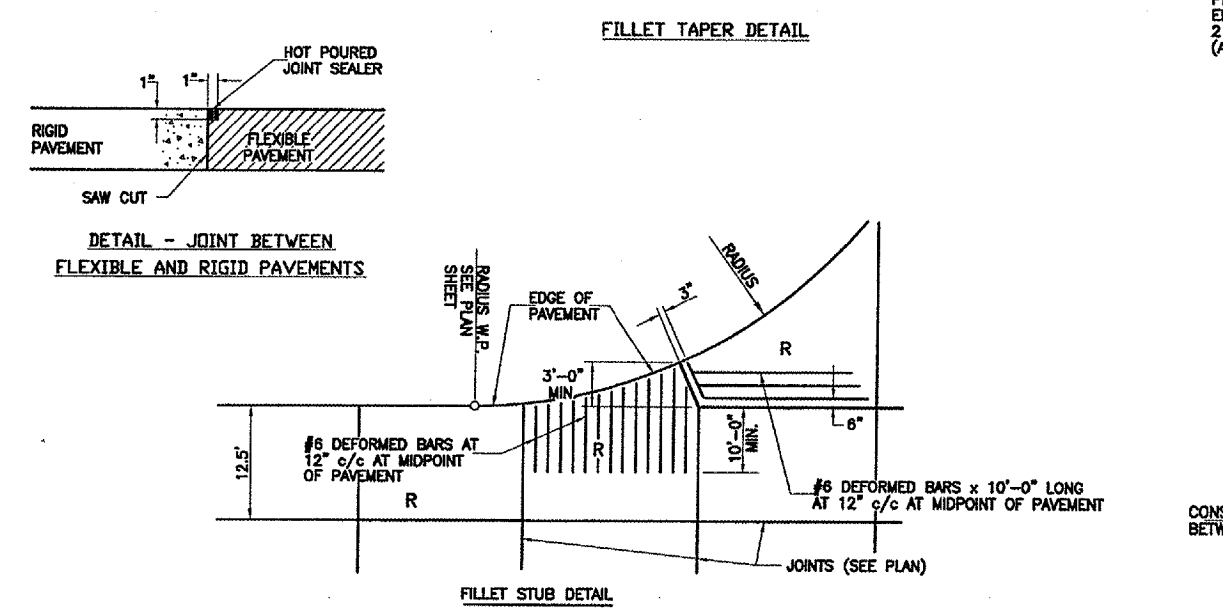
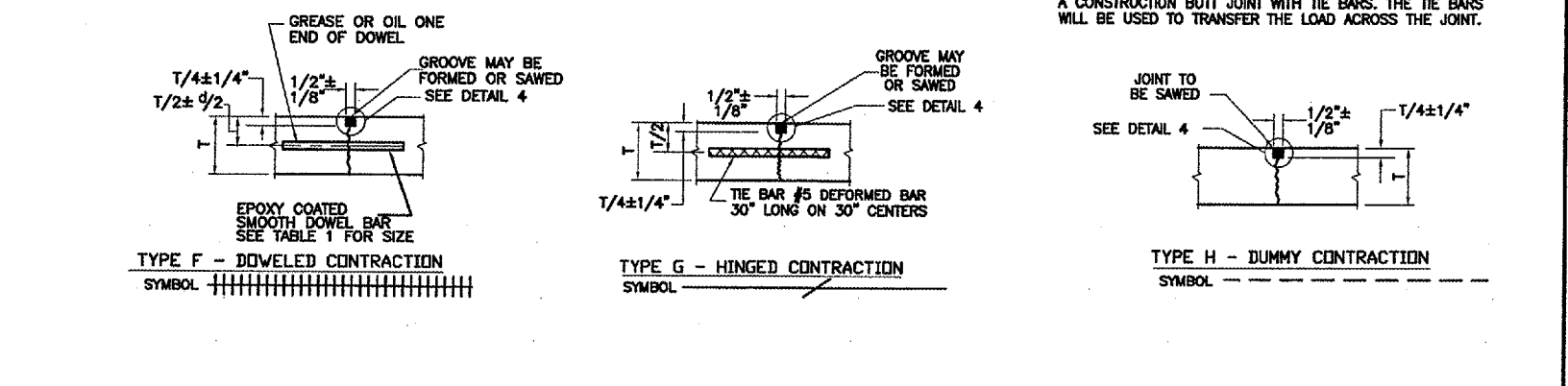
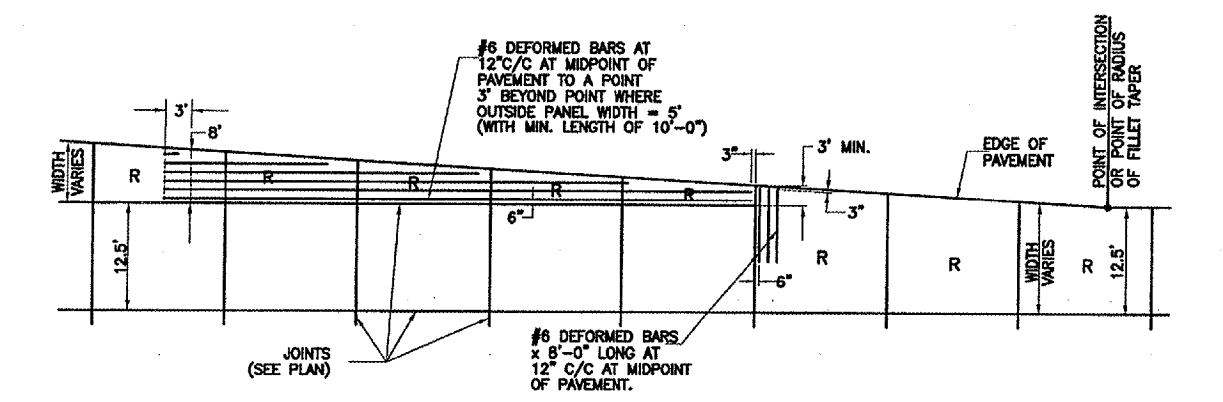
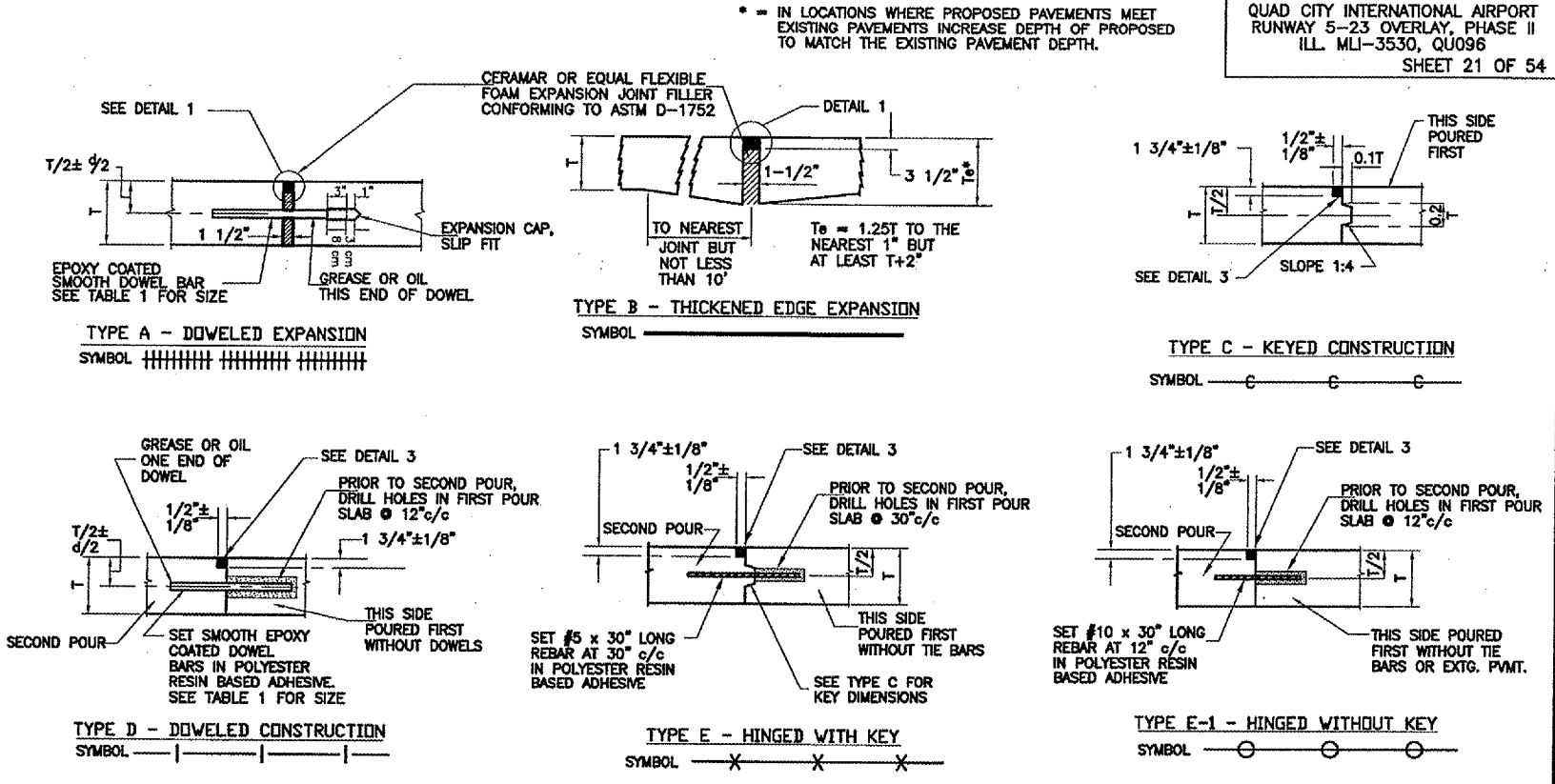
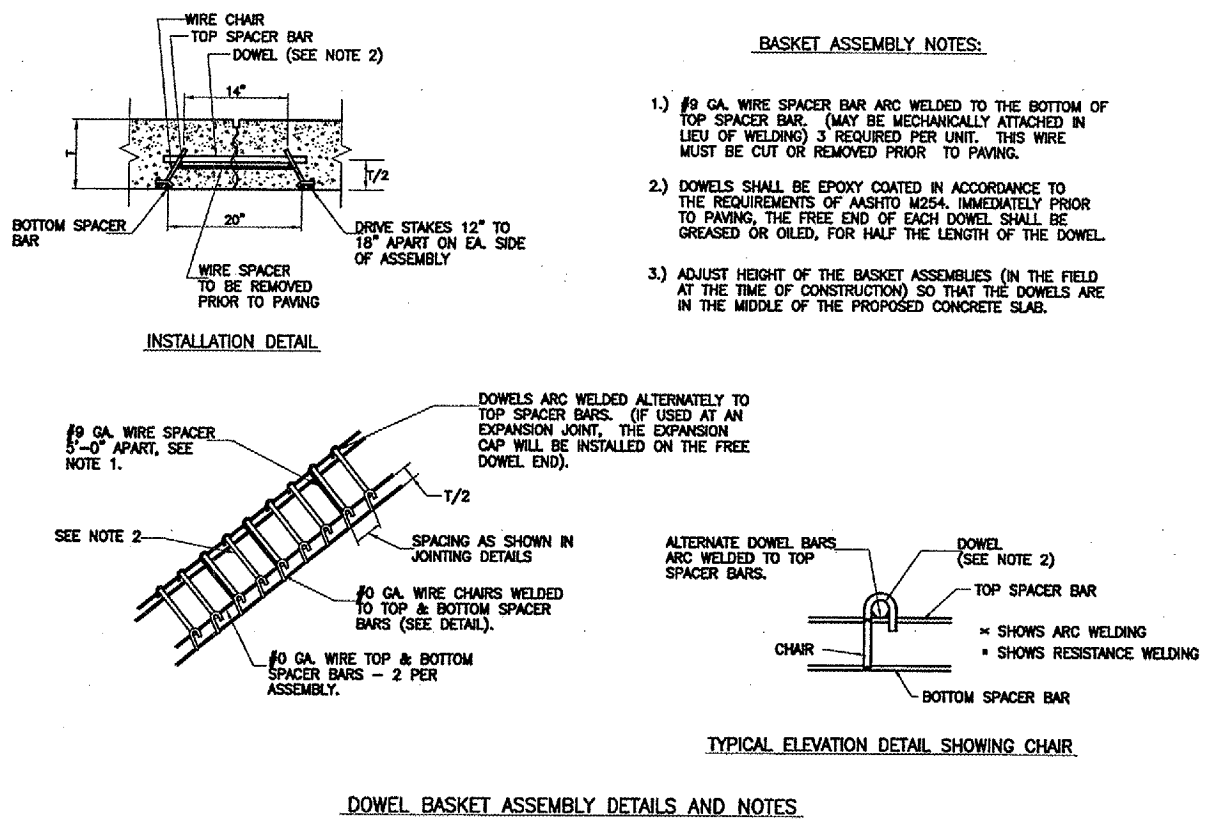
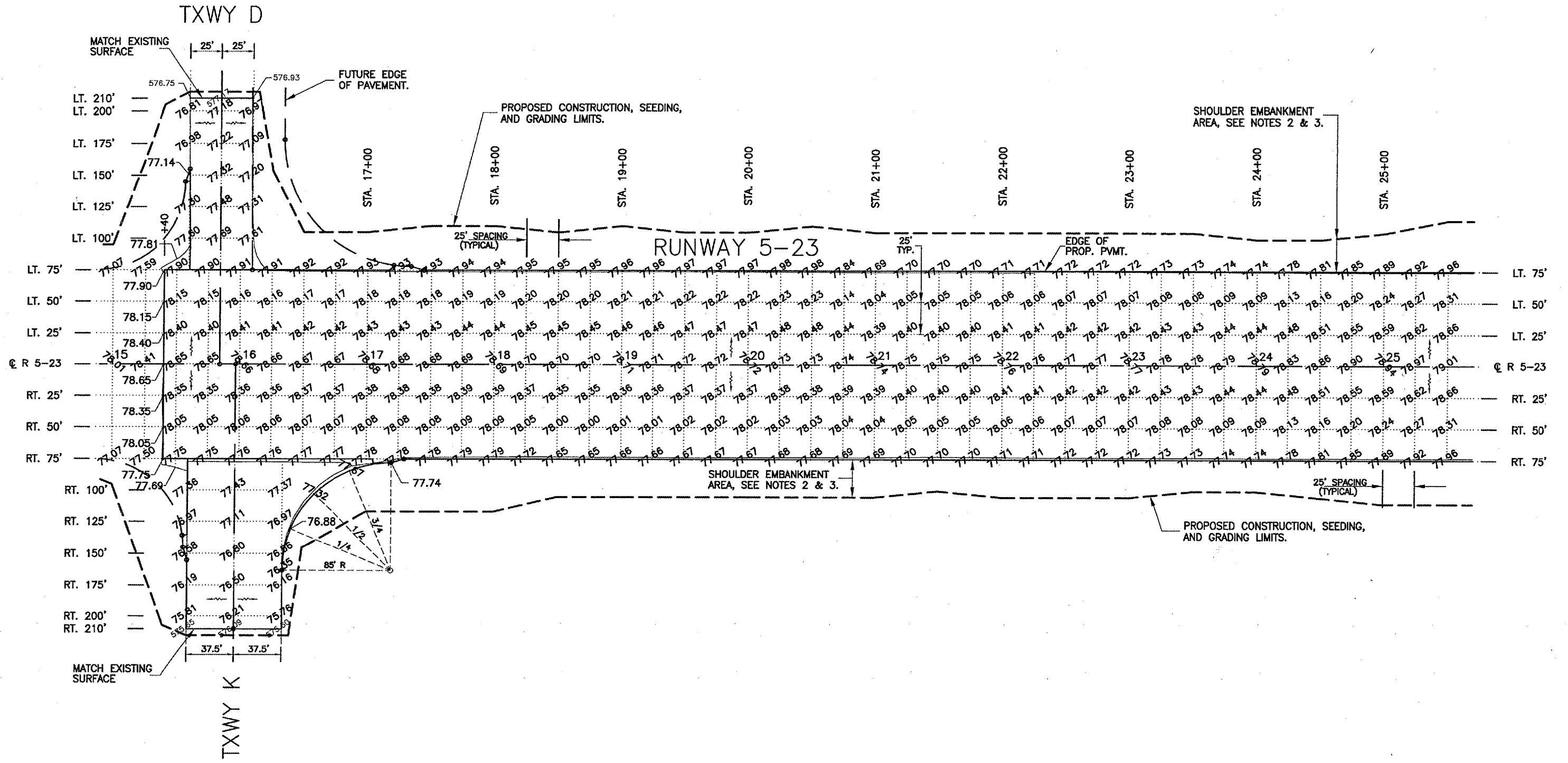
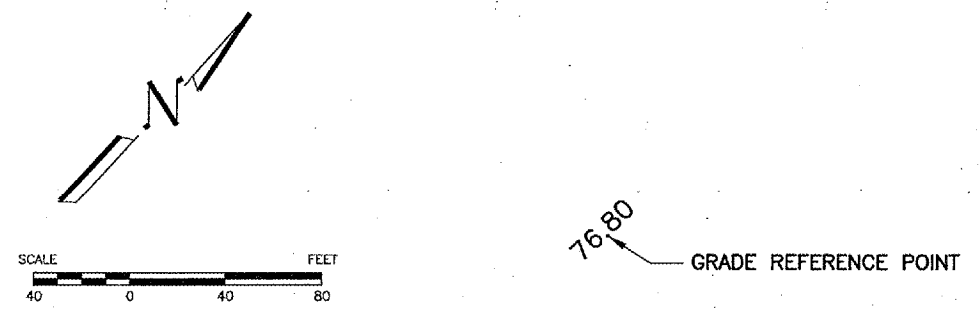


TABLE 1

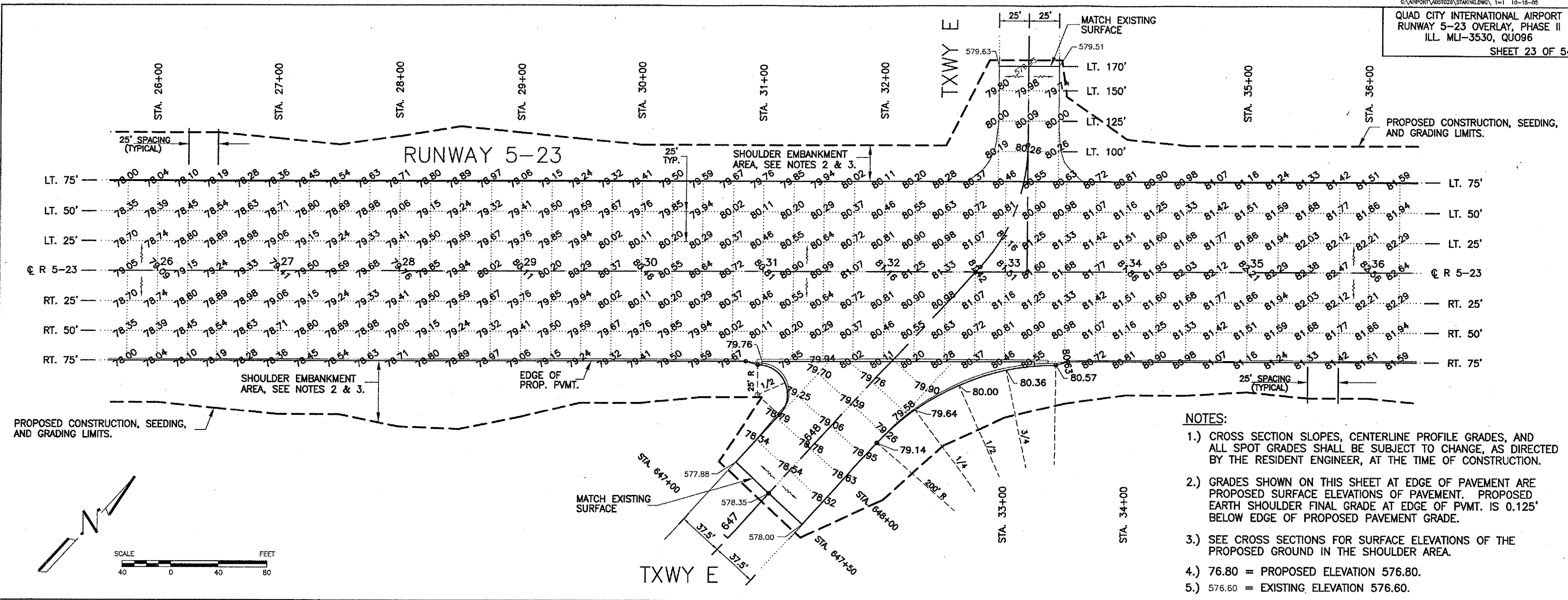
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT D, INCHES D=(T/4)±1/4"	STEEL DOWELS		
		DIAMETER (INCHES)	LENGTH (INCHES)	SPACING (INCHES)
8	2.00	1.00	19	12
9	2.25	1.00	19	12
10	2.50	1.00	19	12
12	3.00	1.00	19	12
15	3.75	1.25	20	15
17	4.25	1.50	20	18



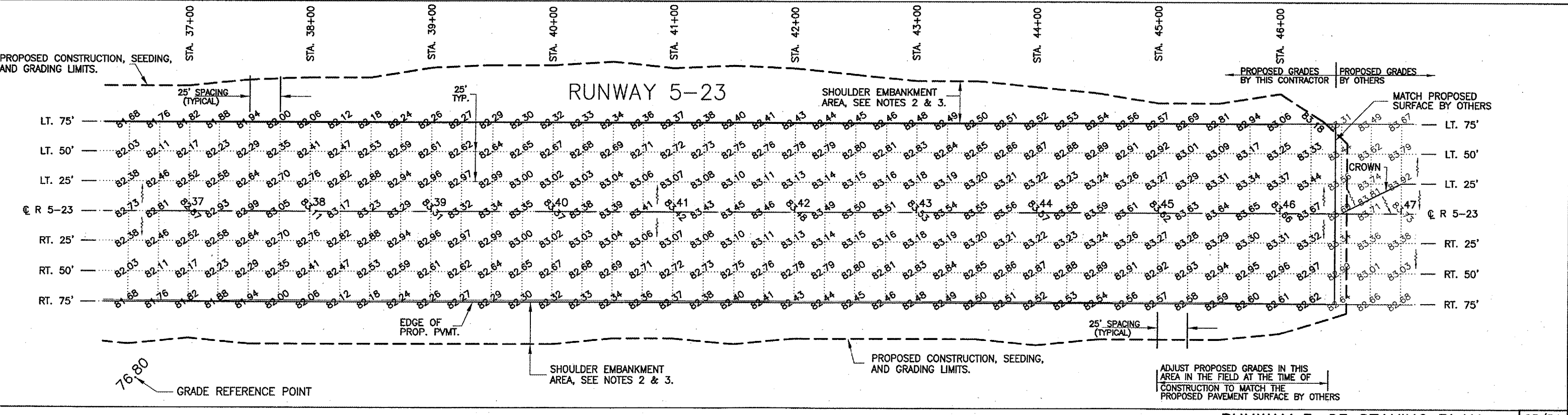
- NOTES:**
- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
 - 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PAVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
 - 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
 - 4.) 76.80 = PROPOSED ELEVATION 576.80.
 - 5.) 576.60 = EXISTING ELEVATION 576.60.



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- NOTES:**
- 1.) CROSS SECTION SLOPES, CENTERLINE PROFILE GRADES, AND ALL SPOT GRADES SHALL BE SUBJECT TO CHANGE, AS DIRECTED BY THE RESIDENT ENGINEER, AT THE TIME OF CONSTRUCTION.
 - 2.) GRADES SHOWN ON THIS SHEET AT EDGE OF PAVEMENT ARE PROPOSED SURFACE ELEVATIONS OF PAVEMENT. PROPOSED EARTH SHOULDER FINAL GRADE AT EDGE OF PVMT. IS 0.125' BELOW EDGE OF PROPOSED PAVEMENT GRADE.
 - 3.) SEE CROSS SECTIONS FOR SURFACE ELEVATIONS OF THE PROPOSED GROUND IN THE SHOULDER AREA.
 - 4.) 76.80 = PROPOSED ELEVATION 576.80.
 - 5.) 576.60 = EXISTING ELEVATION 576.60.

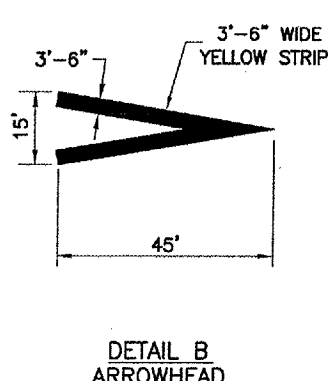
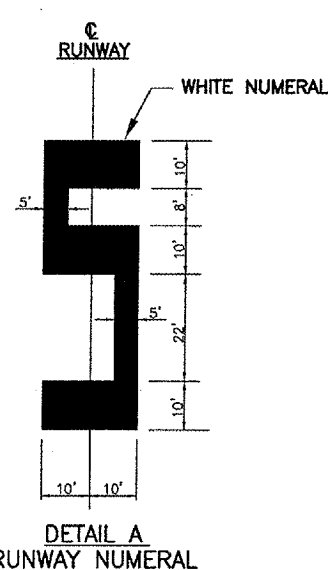
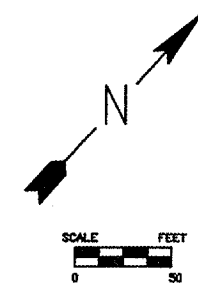
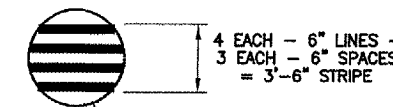
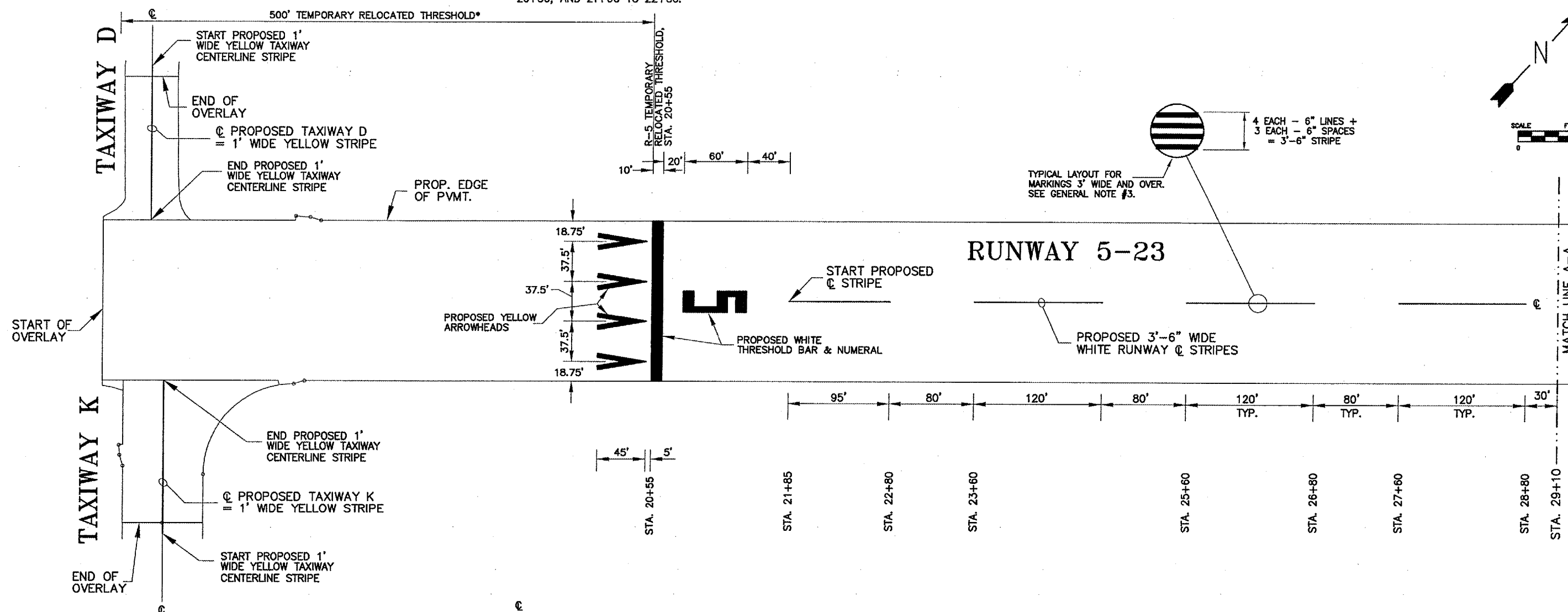


ADJUST PROPOSED GRADES IN THIS AREA IN THE FIELD AT THE TIME OF CONSTRUCTION TO MATCH THE PROPOSED PAVEMENT SURFACE BY OTHERS

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* = IF THE TEMPORARY RELOCATED THRESHOLD (BY OTHERS) HAS NOT BEEN INSTALLED, THEN DELETE THE YELLOW ARROWHEADS AND WHITE THRESHOLD BAR. INSTALL THE PROPOSED WHITE RUNWAY NUMERAL BETWEEN STATION 15+75.5 AND STATION 16+35.5. ADD CENTERLINE DASHES BETWEEN THE FOLLOWING STATIONS: 16+55.5 TO 16+80, 17+60 TO 18+80, 19+60 TO 20+80, AND 21+60 TO 22+80.

VISUAL RUNWAY MARKINGS



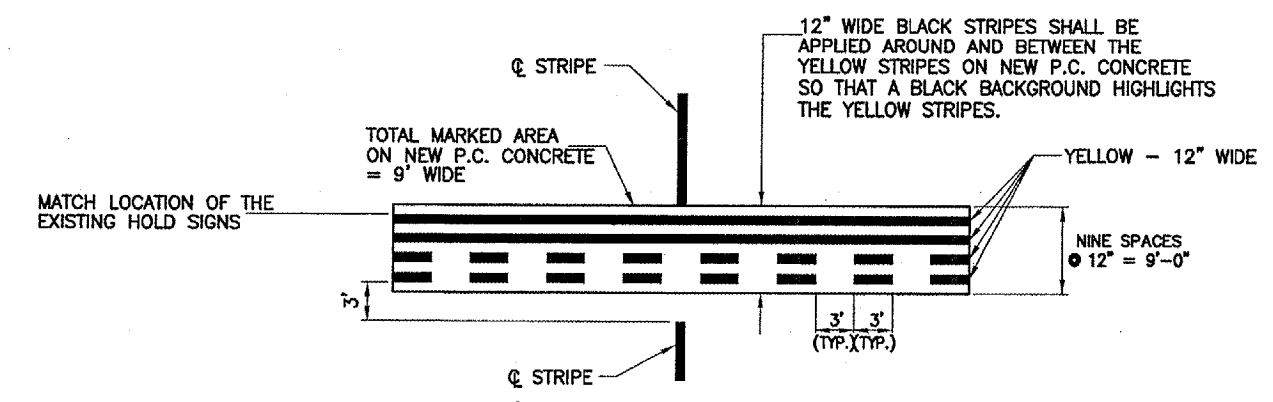
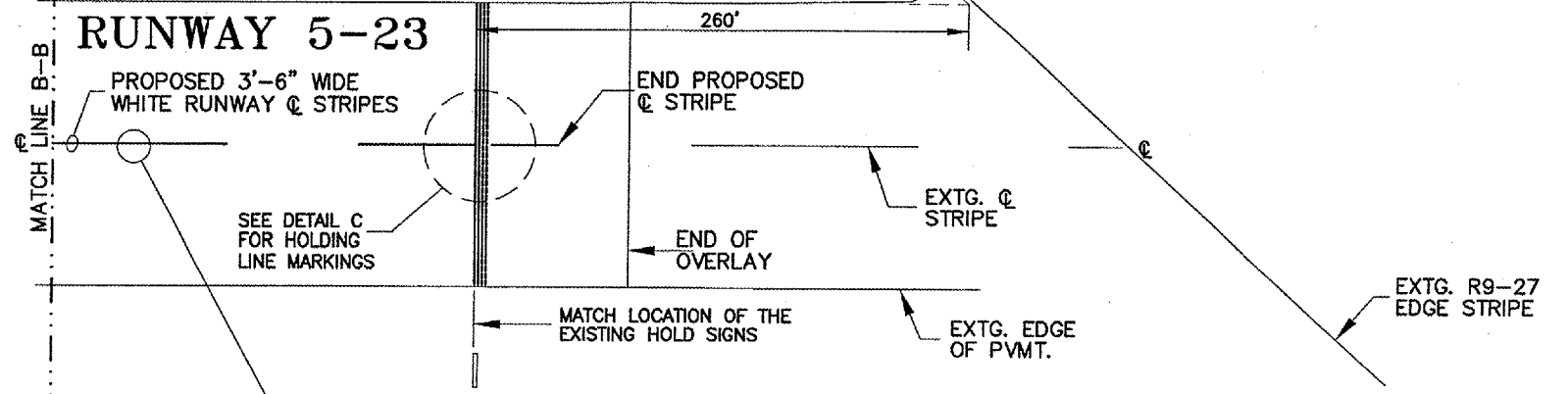
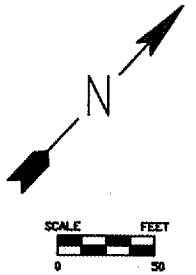
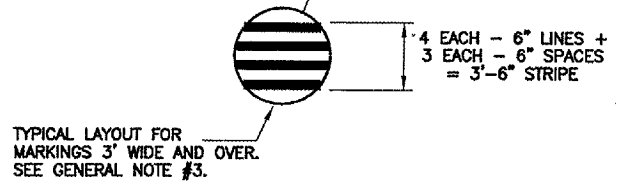
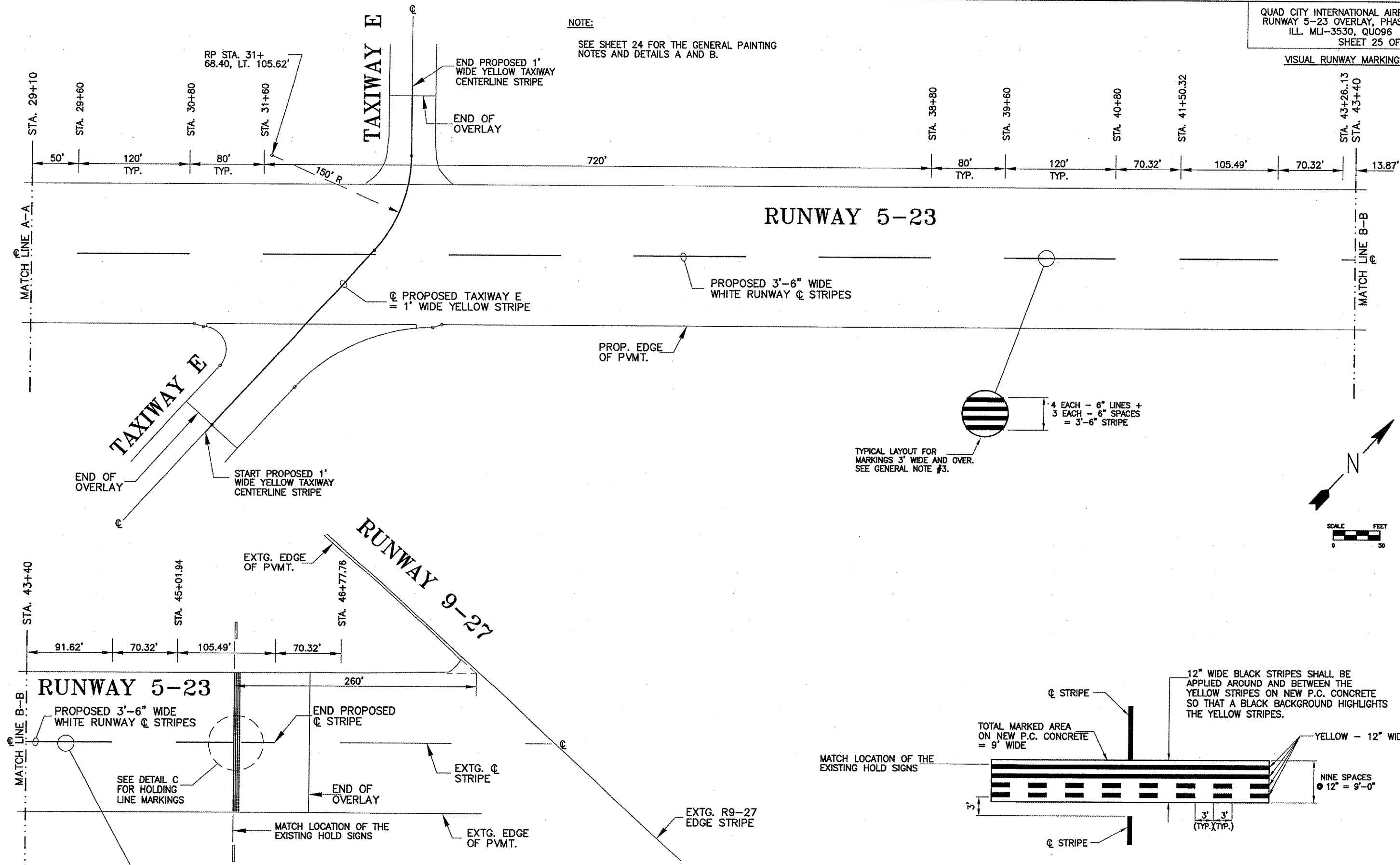
GENERAL NOTES - PAINTING:

1. ALL RUNWAY MARKINGS SHALL BE WHITE UNLESS NOTED OTHERWISE.
2. ALL TAXIWAY MARKINGS SHALL BE YELLOW UNLESS NOTED OTHERWISE.
3. RUNWAY MARKINGS 3' FEET WIDE AND OVER SHALL CONSIST OF A SERIES OF LONGITUDINAL STRIPES 6" WIDE WITH EQUAL WIDTH SPACING BETWEEN THE STRIPES (STRIATED MARKINGS.)
4. ALL MARKINGS SHALL HAVE A REFLECTIVE MEDIA APPLIED IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
5. TAXIWAY EDGE STRIPES AND TAXIWAY CENTERLINE STRIPES SHALL BE SOLID TO MATCH THE EXISTING TAXIWAY MARKINGS. RUNWAY EDGE STRIPES SHALL BE STRIATED TO MATCH THE EXISTING RUNWAY MARKINGS.
6. EXISTING TAXIWAY AND RUNWAY MARKINGS DAMAGED BY CONSTRUCTION OF THE PROJECT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
7. MARKING LAYOUT BY CONTRACTOR, SEE SPECIAL PROVISIONS.
8. ALL NEW PAVEMENT MARKINGS SHALL BE OUTLINED WITH A SIX INCH (6") WIDE BLACK BORDER. FOR HOLD LINES, THE BLACK BORDER WIDTH SHALL BE INCREASE TO TWELVE INCHES (12").
9. THE MAA RESERVES AND SHALL HAVE THE RIGHT TO DELETE THE PAVEMENT MARKING PAY ITEMS FROM THE CONTRACT WORK. THE SPONSOR MAY OPT TO COMPLETE THIS WORK WITH THEIR OWN WORK FORCE.
10. THIS OVERLAY PROJECT DOES NOT INCLUDE THE REINSTALLATION OF THE RUNWAY 5 AIMING POINT MARKERS. THE RUNWAY 5 AIMING POINT MARKERS WILL BE REINSTALLED AS PART OF THE FUTURE RUNWAY 5 PAVEMENT EXTENSION PROJECT (WHEN THE RUNWAY 5 THRESHOLD IS MOVED TO STATION 10+40).

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NOTE:
 SEE SHEET 24 FOR THE GENERAL PAINTING
 NOTES AND DETAILS A AND B.

VISUAL RUNWAY MARKINGS



TYPICAL LAYOUT FOR MARKINGS 3' WIDE AND OVER. SEE GENERAL NOTE #3.

DETAIL C
 HOLDING LINE MARKINGS

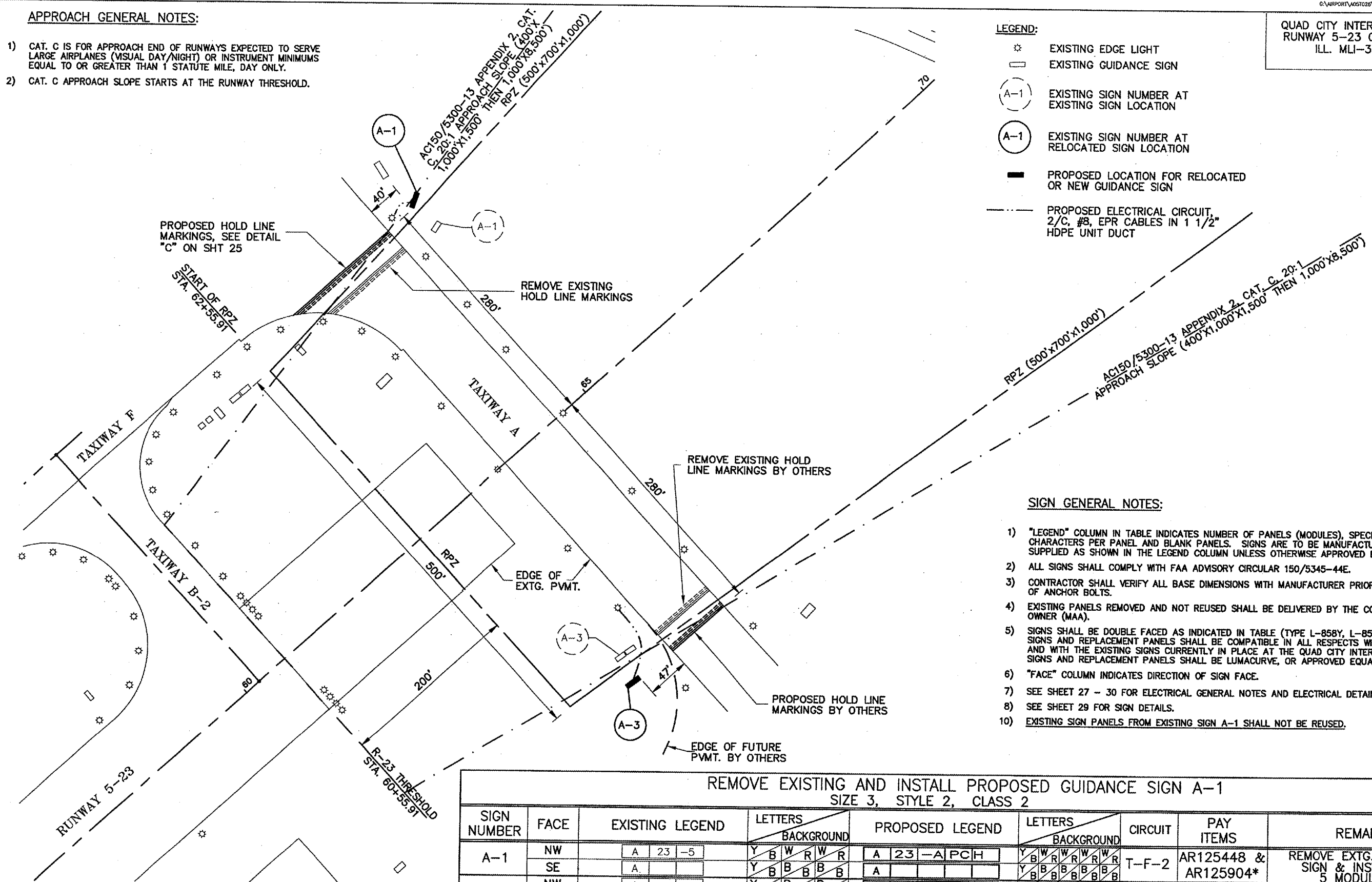
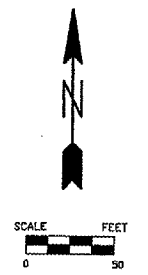
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APPROACH GENERAL NOTES:

- CAT. C IS FOR APPROACH END OF RUNWAYS EXPECTED TO SERVE LARGE AIRPLANES (VISUAL DAY/NIGHT) OR INSTRUMENT MINIMUMS EQUAL TO OR GREATER THAN 1 STATUTE MILE, DAY ONLY.
- CAT. C APPROACH SLOPE STARTS AT THE RUNWAY THRESHOLD.

LEGEND:

- EXISTING EDGE LIGHT
- EXISTING GUIDANCE SIGN
- EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION
- EXISTING SIGN NUMBER AT RELOCATED SIGN LOCATION
- PROPOSED LOCATION FOR RELOCATED OR NEW GUIDANCE SIGN
- PROPOSED ELECTRICAL CIRCUIT, 2/C, #8, EPR CABLES IN 1 1/2" HDPE UNIT DUCT



SIGN GENERAL NOTES:

- "LEGEND" COLUMN IN TABLE INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
- ALL SIGNS SHALL COMPLY WITH FAA ADVISORY CIRCULAR 150/5345-44E.
- CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
- EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
- SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, OR L-969L). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE LUMACURVE, OR APPROVED EQUAL.
- "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
- SEE SHEET 27 - 30 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
- SEE SHEET 29 FOR SIGN DETAILS.
- EXISTING SIGN PANELS FROM EXISTING SIGN A-1 SHALL NOT BE REUSED.

REMOVE EXISTING AND INSTALL PROPOSED GUIDANCE SIGN A-1
 SIZE 3, STYLE 2, CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND		LETTERS		PROPOSED LEGEND		LETTERS		CIRCUIT	PAY ITEMS	REMARK
		A	23-5	BACKGROUND	BACKGROUND	A	23-A PCH	BACKGROUND	BACKGROUND			
A-1	NW	A	23-5	Y	B W R W R	A	23-A PCH	Y	B W R W R	T-F-2	AR125448 & AR125904*	REMOVE EXTG. 3 MODULE SIGN & INSTALL PROP. 5 MODULE SIGN
	SE	A		Y	B B B B B	A		Y	B B B B B			
A-3	NW	A		Y	B B B B B	A		Y	B B B B B	T-A	NONE	BY OTHERS
	SE	A	5-23	Y	B W R W R	A	23-A PCH	Y	B W R W R			

* = THE AR125904 - REMOVE TAXI GUIDANCE SIGN CONTRACT UNIT PRICE SHALL INCLUDE THE COSTS TO REMOVAL AND DISPOSAL OF THE EXISTING TAXI GUIDANCE SIGN BEING REPLACED.

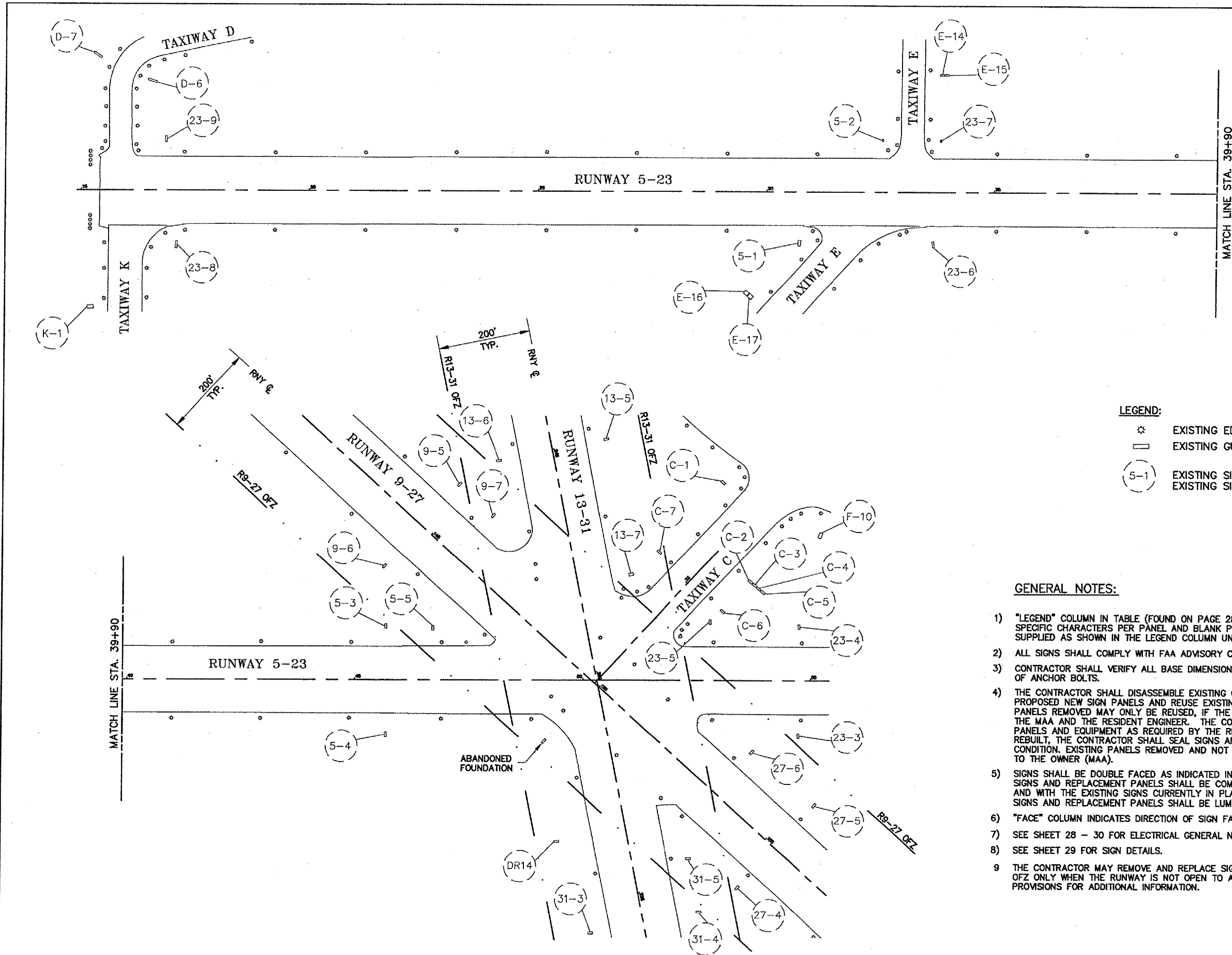
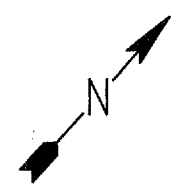
B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
 Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
 B/B = BLACK BLANK PANEL
 Y/Y = YELLOW BLANK PANEL
 W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)

N = NORTH
 S = SOUTH
 E = EAST
 W = WEST

NW = NORTHWEST
 SE = SOUTHEAST
 NE = NORTHEAST
 SW = SOUTHWEST

REPLACE / RELOCATE RUNWAY 23 APPROACH AREA HOLD SIGN A-1

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LEGEND:

- ⊛ EXISTING EDGE LIGHT
- ▭ EXISTING GUIDANCE SIGN
- 5-1 EXISTING SIGN NUMBER AT EXISTING SIGN LOCATION

GENERAL NOTES:

- 1) "LEGEND" COLUMN IN TABLE (FOUND ON PAGE 28) INDICATES NUMBER OF PANELS (MODULES), SPECIFIC CHARACTERS PER PANEL AND BLANK PANELS. SIGNS ARE TO BE MANUFACTURED AND SUPPLIED AS SHOWN IN THE LEGEND COLUMN UNLESS OTHERWISE APPROVED BY THE MAA.
- 2) ALL SIGNS SHALL COMPLY WITH FAA ADVISORY CIRCULAR 150/5345-44E.
- 3) CONTRACTOR SHALL VERIFY ALL BASE DIMENSIONS WITH MANUFACTURER PRIOR TO INSTALLATION OF ANCHOR BOLTS.
- 4) THE CONTRACTOR SHALL DISASSEMBLE EXISTING GUIDANCE SIGNS AS REQUIRED TO INSTALL PROPOSED NEW SIGN PANELS AND REUSE EXISTING PANELS AS INDICATED IN TABLE. EXISTING PANELS REMOVED MAY ONLY BE REUSED, IF THE CONDITION OF THE PANEL IS ACCEPTABLE TO THE MAA AND THE RESIDENT ENGINEER. THE CONTRACTOR SHALL REPLACE ALL DAMAGED EXISTING PANELS AND EQUIPMENT AS REQUIRED BY THE RESIDENT ENGINEER. ONCE THE SIGNS HAVE BEEN REBUILT, THE CONTRACTOR SHALL SEAL SIGNS AND RETURN THE EXISTING SIGNS TO THEIR ORIGINAL CONDITION. EXISTING PANELS REMOVED AND NOT REUSED SHALL BE DELIVERED BY THE CONTRACTOR TO THE OWNER (MAA).
- 5) SIGNS SHALL BE DOUBLE FACED AS INDICATED IN TABLE (TYPE L-858Y, L-858R, OR L-969L). SIGNS AND REPLACEMENT PANELS SHALL BE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND WITH THE EXISTING SIGNS CURRENTLY IN PLACE AT THE QUAD CITY INTERNATIONAL AIRPORT. SIGNS AND REPLACEMENT PANELS SHALL BE LUMACURVE, OR APPROVED EQUAL.
- 6) "FACE" COLUMN INDICATES DIRECTION OF SIGN FACE.
- 7) SEE SHEET 28 - 30 FOR ELECTRICAL GENERAL NOTES AND ELECTRICAL DETAILS.
- 8) SEE SHEET 29 FOR SIGN DETAILS.
- 9) THE CONTRACTOR MAY REMOVE AND REPLACE SIGN PANELS ON SIGNS LOCATED WITH THE RUNWAY OFZ ONLY WHEN THE RUNWAY IS NOT OPEN TO AIRCRAFT TRAFFIC. SEE SAFETY PLAN AND SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.

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QUAD CITY INTERNATIONAL AIRPORT
 RUNWAY 5-23 OVERLAY, PHASE II
 ILL. MLI-3530, QU096
 SHEET 28 OF 54

REMOVE EXISTING AND INSTALL PROPOSED GUIDANCE SIGN PANELS
 SIZE 3, STYLE 2, CLASS 2

SIGN NUMBER	FACE	EXISTING LEGEND	LETTERS BACKGROUND	PROPOSED LEGEND ON REPLACEMENT PANELS	LETTERS BACKGROUND	CIRCUIT	PAY ITEMS	REMARK
5-1	NE		B B		B B	K-1	AR801605	REPLACE 2 PANELS
	SW	E ↘	B Y	E ↘	B Y			
5-2	NE		B B		B B	E	AR801605	REPLACE 2 PANELS
	SW	← E	B Y	← E	B Y			
5-3	NE		B B B B B B B B		B B B B B B B B	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SW	9 - 27 13 - 31	W R W R W R W R	9 - 27 13 - 31	W R W R W R W R			
5-4	NE		B B B B B B B B		B B B B B B B B	SIGN	NONE	PANEL REPLACEMENT BY OTHERS
	SW	9 - 27 13 - 31	W R W R W R W R					
5-5	NE		B B		B B	SIGN	AR801605	REPLACE 2 PANELS
	SW	↖ C	B Y	↖ C	B Y			
9-5	W	13 - 31 23 - 5	W R W R W R W R	13 - 31 23 - 5	W R W R W R W R	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	E		B B B B B B B B		B B B B B B B B			
9-6	W	13 - 31 23 - 5	W R W R W R W R	13 - 31 23 - 5	W R W R W R W R	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	E		B B B B B B B B		B B B B B B B B			
9-7	W	← C	B Y	← C	B Y	SIGN	AR801605	REPLACE 2 PANELS
	E		B B		B B			
13-5	NW	23 - 5 27 - 9	W R W R W R W R	23 - 5 27 - 9	W R W R W R W R	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SE		B B B B B B B B		B B B B B B B B			
13-6	NW	23 - 5 27 - 9	W R W R W R W R	23 - 5 27 - 9	W R W R W R W R	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SE		B B B B B B B B		B B B B B B B B			
13-7	NW	↖ C	B Y	↖ C	B Y	SIGN	AR801605	REPLACE 2 PANELS
	SE		B B		B B			
23-3	NE	27 - 9 31 - 13	W R W R W R W R			SIGN	NONE	PANEL REPLACEMENT BY OTHERS
	SW		B B B B B B B B					
23-4	NE	27 - 9 31 - 13	W R W R W R W R	27 - 9 31 - 13	W R W R W R W R	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SW		B B B B B B B B		B B B B B B B B			
23-5	NE	C ↘	B Y	C ↘	B Y	SIGN	AR801605	REPLACE 2 PANELS
	SW		B B		B B			
23-6	NE	↖ E	B Y	↖ E	B Y	K-1	AR801605	REPLACE 2 PANELS
	SW		B B		B B			
23-7	NE	E →	B Y	E →	B Y	E	AR801605	REPLACE 2 PANELS
	SW		B B		B B			
23-8	NE	← K	B Y	← K	B Y	K-1	AR801605	REPLACE 2 PANELS
	SW		B B		B B			
23-9	NE	D →	B Y	D →	B Y	D	AR801605	REPLACE 2 PANELS
	SW		B B		B B			
31-3	NW		B B B B B B B B		B B B B B B B B	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SE	5- 23 9- 27	W R W R W R W R	5- 23 9- 27	W R W R W R W R			
31-4	NW		B B B B B B B B		B B B B B B B B	SIGN	AR801605	REPLACE 8 PANELS REPLACE MESSAGE DIVIDER, SEE NOTE 1
	SE	5- 23 9- 27	W R W R W R W R	5- 23 9- 27	W R W R W R W R			
D-6	NW	D 5	Y B W R	D 5	Y B W R	D	AR801605	REPLACE 4 PANELS
	SE	D	Y B B B	D	Y B B B			
D-7	NW	5 D	W R Y B	5 D	W R Y B	D	AR801605	REPLACE 4 PANELS
	SE		B B B B		B B B B			
E-14 & E-15	NW	E 23 -5	Y B W R W R	E 23 -5	Y B W R W R	E	AR801605	REPLACE 6 PANELS
	SE	E	Y B B B B B	E	Y B B B B B			
E-16 & E-17	N	E	Y B B B B B	E	Y B B B B B	K-1	AR801605	REPLACE 4 PANELS
	S	E 5 -23	Y B W R W R	E	Y B NO CHANGE			
K-1	NW		B B B B		B B B B	K-1	AR801605	REPLACE 4 PANELS
	SE	K 5	Y B W R	K 5	Y B W R			

LEGEND:

B/Y = BLACK LETTERS ON YELLOW BACKGROUND (TYPE L-858Y)
 Y/B = YELLOW LETTERS ON BLACK BACKGROUND (TYPE L-858L)
 B/B = BLACK BLANK PANEL
 Y/Y = YELLOW BLANK PANEL
 W/R = WHITE LETTERS ON RED BACKGROUND (TYPE L-858R)

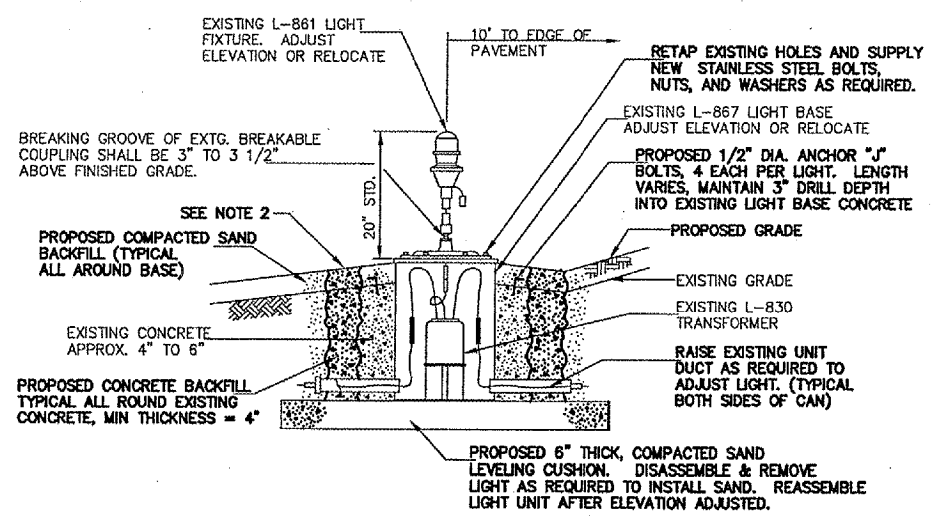
N = NORTH NW = NORTHWEST
 S = SOUTH SE = SOUTHEAST
 E = EAST NE = NORTHEAST
 W = WEST SW = SOUTHWEST

NOTE:

1. REPLACE THE EXISTING BLACK MESSAGE DIVIDER / FACE INSERT CHANNEL WITH A PROPOSED WHITE MESSAGE DIVIDER / FACE INSERT CHANNEL ON THE TYPE L-858R SIDE OF THESE SIGNS. INCLUDE COSTS FOR THIS WORK IN THE AR801605 CONTRACT UNIT PRICE.

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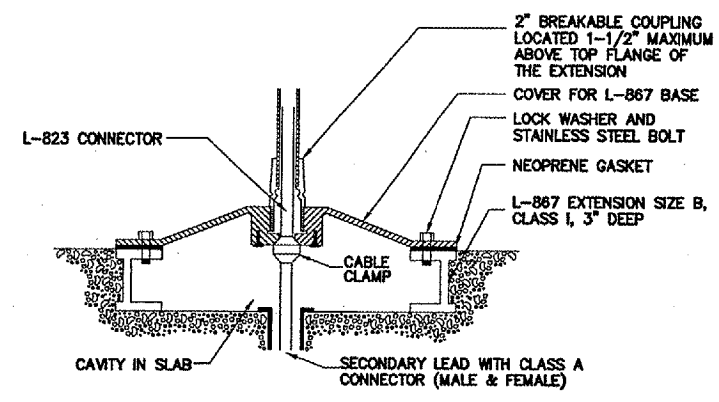
PROPOSED GUIDANCE SIGNS



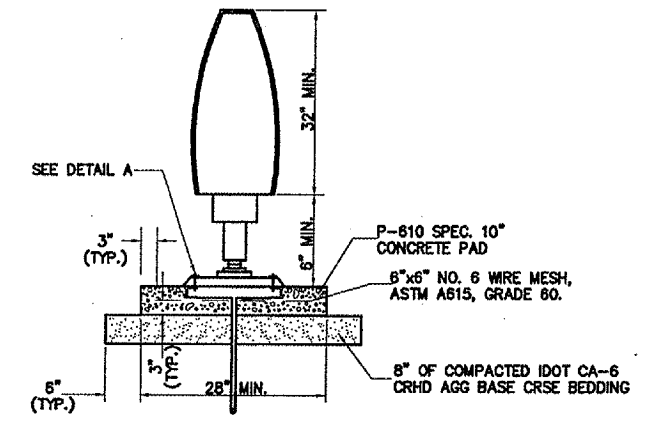
TYPICAL DETAIL L-861
 EDGE LIGHTS ADJUSTMENT OR RELOCATION - IN TURF

NOTES:

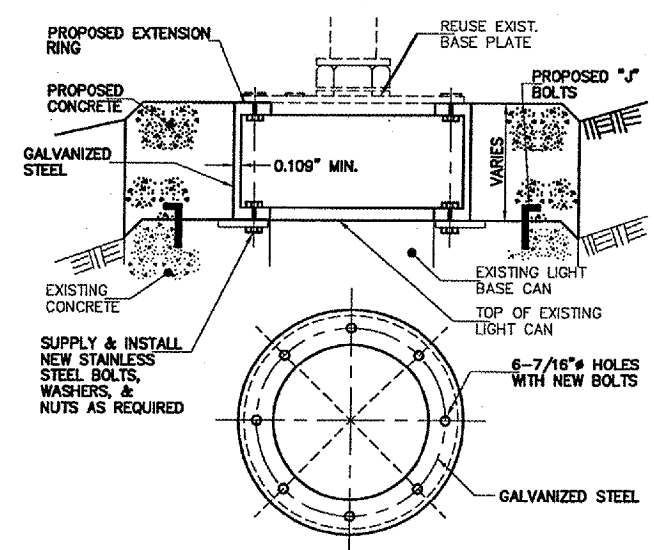
1. AT THE CONTRACTOR'S OPTION, LIGHT BASE CAN EXTENSION RINGS MAY BE USED TO ADJUST LIGHT FIXTURE. CONTRACTOR TO EXTEND EXISTING CONCRETE APRON AROUND LIGHT BASE TO PROPOSED GRADE ELEVATION IF BASE CAN EXTENSION RINGS ARE USED. CONTRACTOR TO DETERMINE HEIGHT OF BASE CAN EXTENSION RINGS IN THE FIELD AT THE TIME OF CONSTRUCTION.
2. GROUND SURFACE DRAINAGE TO FLOW AWAY FROM EDGE OF PAVEMENT AND LIGHT BASE CANS. NO WATER PONDING OR LOW SPOTS SHALL BE PERMITTED AROUND LIGHT BASE CANS.
3. ALL EXISTING ELECTRICAL CIRCUITS TO REMAIN ACTIVE AT ALL TIMES. CONTRACTOR TO PROVIDE, INSTALL, AND MAINTAIN TEMPORARY ABOVE GROUND JUMPER CABLES AS REQUIRED TO PROVIDE CIRCUIT CONTINUITY IN ALL CIRCUITS AT ALL TIMES DURING CONSTRUCTION. COSTS TO BE INCLUDED IN THE LIGHTING CONTRACT UNIT PRICES. LENGTH OF JUMPER CABLES SHALL NOT BE MEASURED FOR PAYMENT AND / OR PAID FOR.



DETAIL A

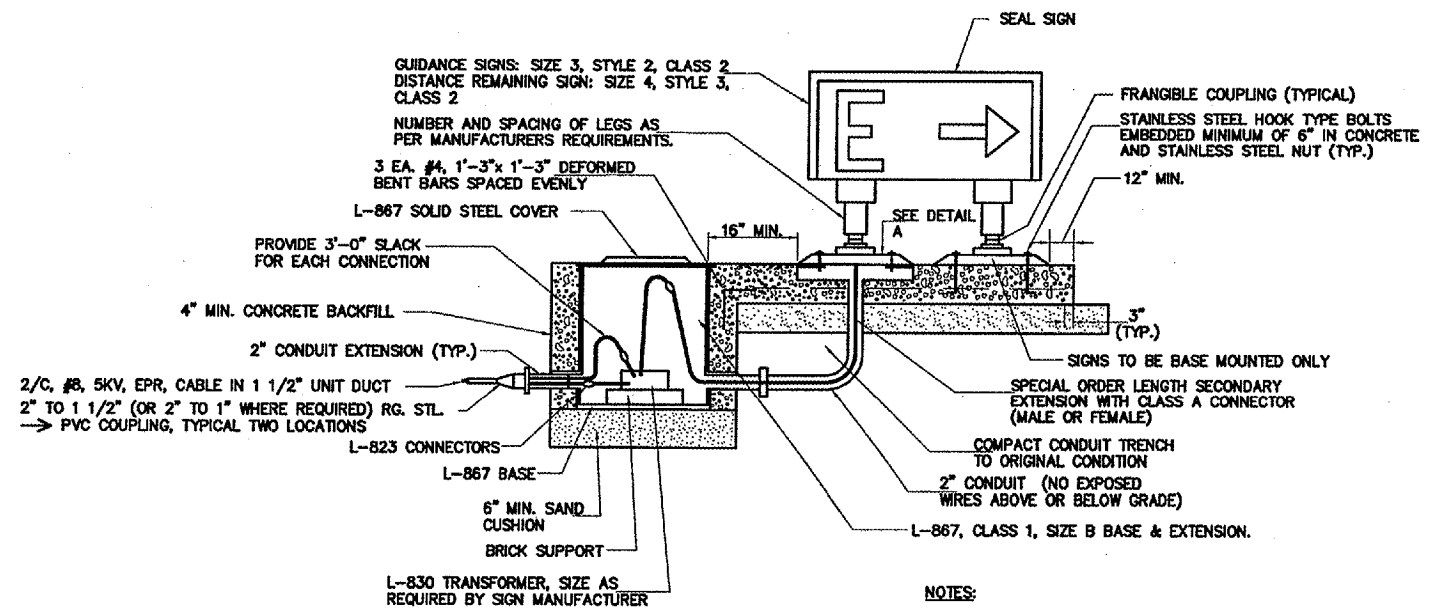


SIDE VIEW



OPTIONAL EXTENSION RING DETAIL
 N.T.S.

NOTE: THE CONTRACTOR SHALL FIELD VERIFY EXISTING LIGHT BASE TYPES (MOST ARE 12" L-867) AND REQUIRED HEIGHT ADJUSTMENT DIMENSIONS. NO ADDITIONAL PAYMENT WILL BE MADE FOR ALTERNATE LIGHT CAN TYPES. REDRILL AND RETAP EXISTING HOLES AS REQUIRED. COSTS SHALL BE INCLUDED IN THE UNIT PRICE FOR LIGHT ADJUSTMENT.



FRONT VIEW

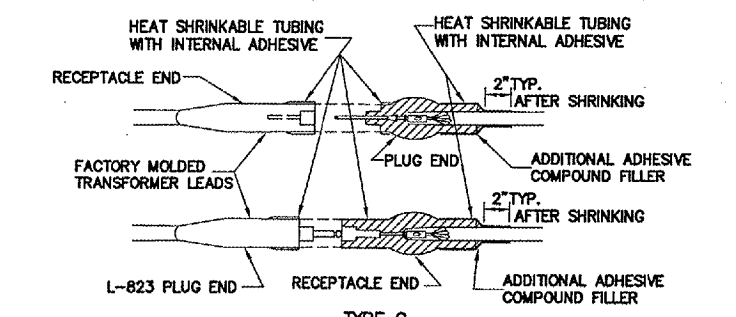
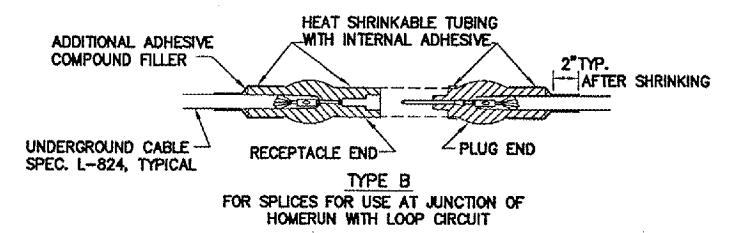
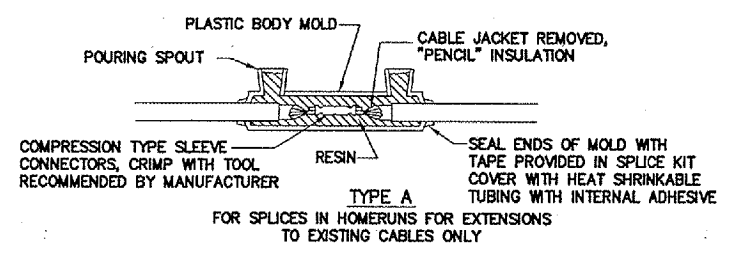
- NOTES:
1. COLOR CODE TAPE FOR WIRE IDENTIFICATION 6" BEFORE L-823 CONNECTORS.
 2. WHERE PROPOSED SIGNS ARE INSTALLED BESIDE EXISTING SIGNS PROVIDE 1' SPACE BETWEEN SIGNS.

TAXI GUIDANCE SIGNS ISOLATION TRANSFORMER DATA*	
NUMBER OF MODULES	TRANSFORMER WATTAGE
1	100
2	300
3	500
4	500

TRANSFORMERS SHALL BE 8.8/8.8 AMP.
 * - OR AS REQUIRED BY SIGN MANUFACTURER.

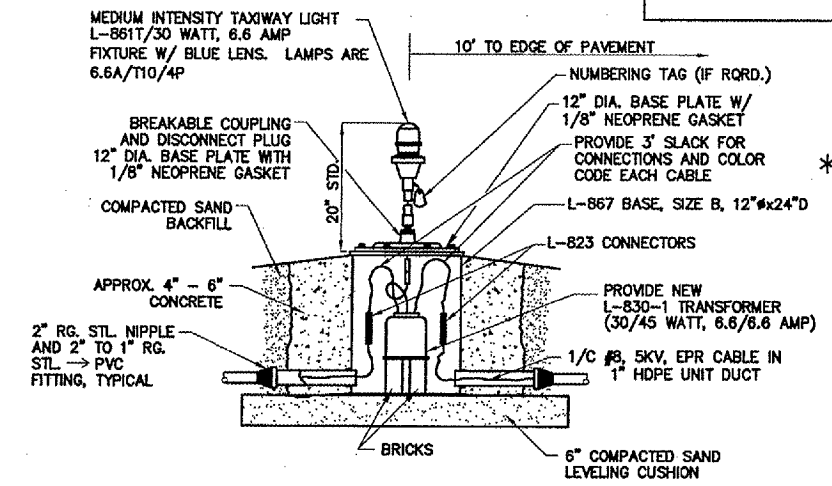
GENERAL ELECTRICAL NOTES:

1. ALL EXCAVATION SHALL BE DONE VERY CAREFULLY. EXCAVATION BY HAND DIGGING SHALL BE REQUIRED AROUND ALL EXISTING DUCT BANKS, SPLICE CANS, MANHOLES, AND EXISTING CABLES. MANY EXISTING ACTIVE UNDERGROUND CABLES, WHOSE EXACT LOCATIONS CANNOT BE DETERMINED, ARE FOUND IN THE PROJECT AREA. IN ORDER TO AVOID EXISTING UNDERGROUND CABLES, THE CONTRACTOR SHALL CONNECT A THUMPER TO ALL EXISTING CIRCUITS AFTER WHICH THEY SHALL BE STAKED IN ALL AREAS REQUIRING TRENCHING OR EXCAVATION. CONTRACTOR SHALL ALSO NOTE THAT LOW VOLTAGE, FAA CABLES ALSO RUN UNDERGROUND THROUGHOUT THESE AREAS. ANY CABLE DAMAGED SHALL BE REPAIRED OR REPLACED TO ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. A MINIMUM OF 3 FEET OF SLACK SHALL BE PROVIDED IN THE CABLES AT EACH TRANSFORMER, CONNECTOR, OR SPLICE POINT. ALL CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS OR SPLICE CANS, UNLESS NOTED OTHERWISE.
3. THE ELECTRICAL INSTALLATION, AS A MINIMUM, SHALL MEET THE NATIONAL ELECTRICAL CODE AND LOCAL REGULATIONS.
4. ALL MANUFACTURERS FOR SUPPLYING AIRPORT LIGHTING EQUIPMENT SHALL APPEAR ON THE CURRENT FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-53B. THE EQUIPMENT SHALL COMPLY WITH THE APPLICABLE CURRENT FAA ADVISORY CIRCULAR LISTED IN THE FAA "APPROVED AIRPORT EQUIPMENT" LIST FOUND IN AC 150/5345-2 (AIRPORTS ELECTRONIC BULLETIN BOARD NUMBER 14).
5. THE CONTRACTOR SHALL ASCERTAIN THAT ALL LIGHTING SYSTEM COMPONENTS FURNISHED BY HIM (INCLUDING FAA APPROVED EQUIPMENT) ARE COMPATIBLE IN ALL RESPECTS WITH EACH OTHER AND THE REMAINDER OF THE NEW/EXISTING SYSTEM. ANY NONCOMPATIBLE COMPONENTS FURNISHED BY THIS CONTRACTOR SHALL BE REPLACED BY HIM AT NO ADDITIONAL COST TO THE AIRPORT SPONSOR WITH A SIMILAR UNIT, APPROVED BY THE ENGINEER (DIFFERENT MODEL OR DIFFERENT MANUFACTURER) THAT IS COMPATIBLE WITH THE REMAINDER OF THE AIRPORT LIGHTING SYSTEM.
6. IF THE CONTRACTOR SELECTS TO FURNISH AND INSTALL AIRPORT LIGHTING EQUIPMENT WHICH REQUIRES ADDITIONAL WIRING, TRANSFORMERS, ADAPTERS, MOUNTINGS, ETC., BEYOND THAT SHOWN ON THE DRAWINGS AND/OR LISTED IN THE SPECIFICATIONS, THEN THE COST FOR THE ADDITIONAL ITEMS SHALL BE INCLUDED TO THE CONTRACT UNIT PRICES.
7. THE CONTRACTOR INSTALLED EQUIPMENT (INCLUDING FAA APPROVED) SHALL NOT GENERATE ANY ELECTROMAGNETIC INTERFERENCE IN THE EXISTING AND/OR NEW COMMUNICATIONS, WEATHER, AIR NAVIGATION, AND AIR TRAFFIC CONTROL EQUIPMENT. ANY EQUIPMENT GENERATING SUCH INTERFERENCE SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST WITH THE EQUIPMENT MEETING THE APPLICABLE SPECIFICATIONS AND NOT GENERATING ANY INTERFERENCE.
8. WHEN A SPECIFIC TYPE, STYLE, CLASS, ETC. OF FAA APPROVED EQUIPMENT IS SPECIFIED ONLY THAT TYPE, STYLE, CLASS, WILL BE ACCEPTABLE, EVEN THOUGH EQUIPMENT OF OTHER TYPES, STYLES, CLASSES, ETC. MAY BE FAA APPROVED.
9. ALL CONCRETE FOR ELECTRICAL EQUIPMENT SHALL COMPLY WITH SPECIFICATION 610-STRUCTURAL PC CONCRETE 3500 PSI AT 28 DAYS, AIR ENTRAINED CONCRETE MIX SHALL BE USED.
10. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL HAVE A 1/4" DIAMETER OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE CONNECTOR INTO THE BASE.
11. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1 1/2" ABOVE THE EDGE OF THE COVER IN THE CASE OF A BASE MOUNTED COUPLING.
12. ALL PERMANENT CABLE SPLICES SHALL OCCUR IN MANHOLES, LIGHT WELLS, OR SPLICE CANS, UNLESS NOTED OTHERWISE.
13. MIMIC PANEL COLORS : TAXIWAY R5-23 CIRCUIT = WHITE, TAXIWAY D CIRCUIT = ORANGE, TAXIWAY E CIRCUIT = YELLOW, TAXIWAY F-2 CIRCUIT = LIME, TAXIWAY T-K-1 CIRCUIT = RED AND SIGN CIRCUIT = WHITE.



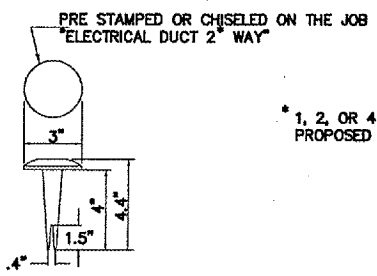
- NOTES :
1. SEE LIGHTING LAYOUT SHEET(S) FOR SPLICE TYPE
 2. INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE

CABLE SPLICES
(NOT TO SCALE)

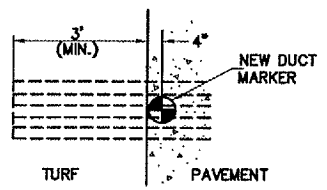


L-861T EDGE LIGHT INSTALLATION / RELOCATION DETAILS
MEDIUM INTENSITY TAXIWAY LIGHT (MITL)
BASE MOUNTED, 6.6 AMP SERIES CIRCUIT
NOT TO SCALE

- NOTES :
1. BREAKING GROOVE OF BREAKABLE COUPLING SHALL BE 3" TO 3 1/2" ABOVE FINISHED GRADE.
 2. * ORDER L-867 BASE WITH ADDITIONAL 2" CONDUIT HUB APPROXIMATELY 90' FROM MAIN ENTRANCE HUB WHERE INDICATED ON PLAN SHEETS.
 3. IF THE CONDITION OF THE EXISTING LIGHTING EQUIPMENT TO BE REMOVED IS ACCEPTABLE TO THE RESIDENT ENGINEER, THE CONTRACTOR MAY REUSE THE REMOVED LIGHT FIXTURES, BASE PLATES, CANS, AND TRANSFORMERS AT THE PROPOSED LIGHT LOCATIONS. IF THE EXISTING EDGE LIGHT EQUIPMENT IS REUSED, THEN THE CONTRACTOR SHALL SUPPLY AND INSTALL 6" COMPACTED SAND LEVELING CUSHION, EXTEND THE EXISTING STEEL CONDUIT TO A POINT OUTSIDE OF THE PROPOSED CONCRETE BACKFILL, ADDITIONAL P.C. CONCRETE BACKFILL ALL AROUND (MIN. 4" THICKNESS), COMPACTED SAND BACKFILL, NEW 1/8" NEOPRENE GASKETS, RETAP BASE HOLES AS REQ., NEW STAINLESS STEEL BOLTS, NUTS, & WASHERS, AND NEW LENS & LAMPS AS REQUIRED. CONTRACTOR SHALL REPLACE ANY DAMAGED EQUIPMENT AS DIRECTED BY THE RESIDENT ENGINEER. CONTRACTOR SHALL SUPPLY AND INSTALL NEW EDGE LIGHT EQUIPMENT AS REQUIRED.

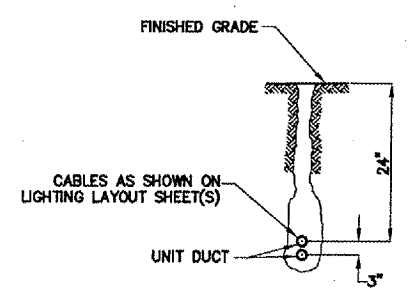


* 1, 2, OR 4 AS APPROPRIATE FOR PROPOSED / EXISTING DUCT BANK

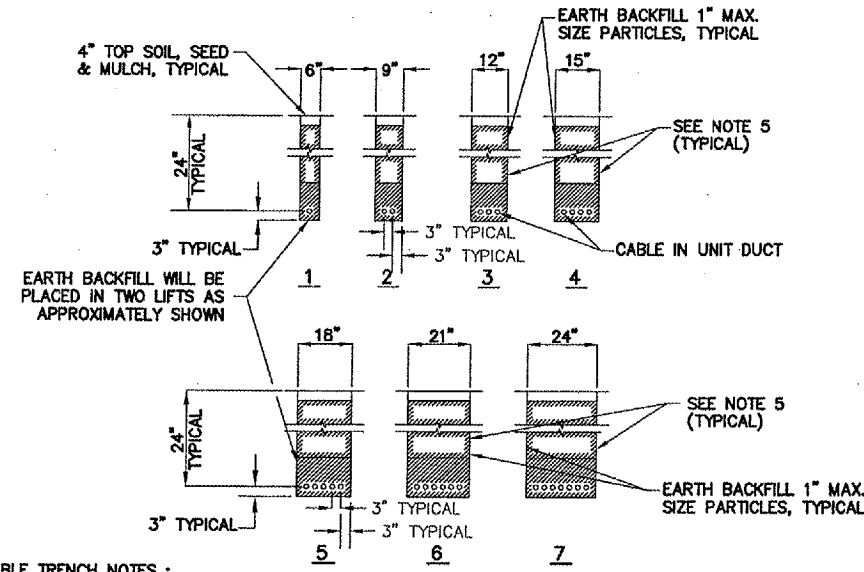


IN-PAVEMENT BRASS DUCT MARKER DETAIL

NOTE: PAVING CONTRACTOR SHALL INSTALL NEW BRASS DUCT MARKERS IN THE PROPOSED PAVEMENT AT ALL LOCATIONS WHERE THE PROPOSED PAVEMENT CROSSES EXISTING OR PROPOSED ELECTRICAL DUCTS. COST OF DUCT MARKERS SHALL BE INCLUDED IN THE 401 AND / OR 501 CONTRACT UNIT PRICES.



PLOWED CABLE
(NOT TO SCALE)

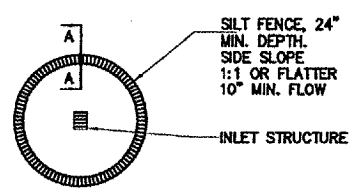


CABLE TRENCH NOTES :

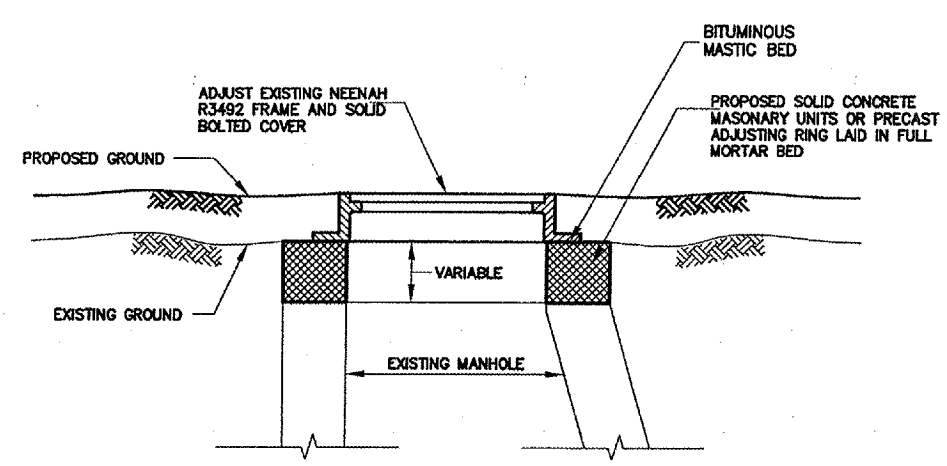
1. DETAIL NUMBERS INDICATE NO. OF CABLES.
2. TRENCHES WITH MORE THAN 7 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE; IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
3. DEPTH OF TRENCHES SHALL BE AS SHOWN ABOVE UNLESS OTHERWISE SPECIFIED ON THE PLANS.
4. ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL TO TRENCH. RETURNING MATERIALS AND RATES MAY BE SHOWN ON THE PLANS.
5. INSTALL YELLOW PLASTIC WARNING RIBBON IN TRENCH 9" ABOVE CABLES (TYPICAL ALL TRENCHES).

CABLE TRENCHES
(NOT TO SCALE)

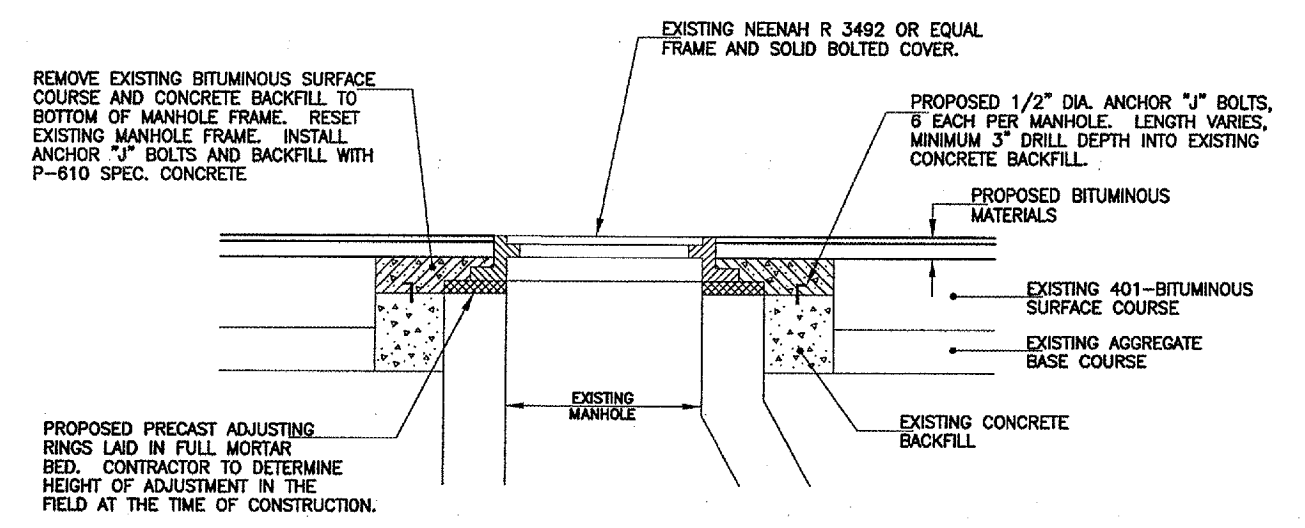
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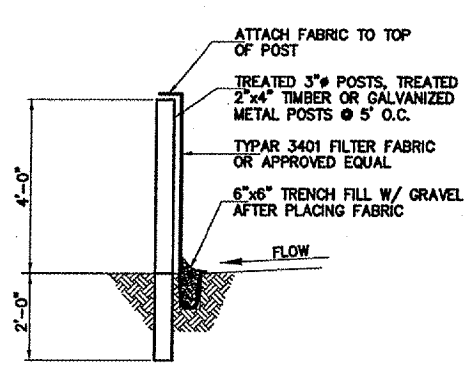
TEMPORARY SEDIMENT TRAP
 AT ALL INLET STRUCTURES



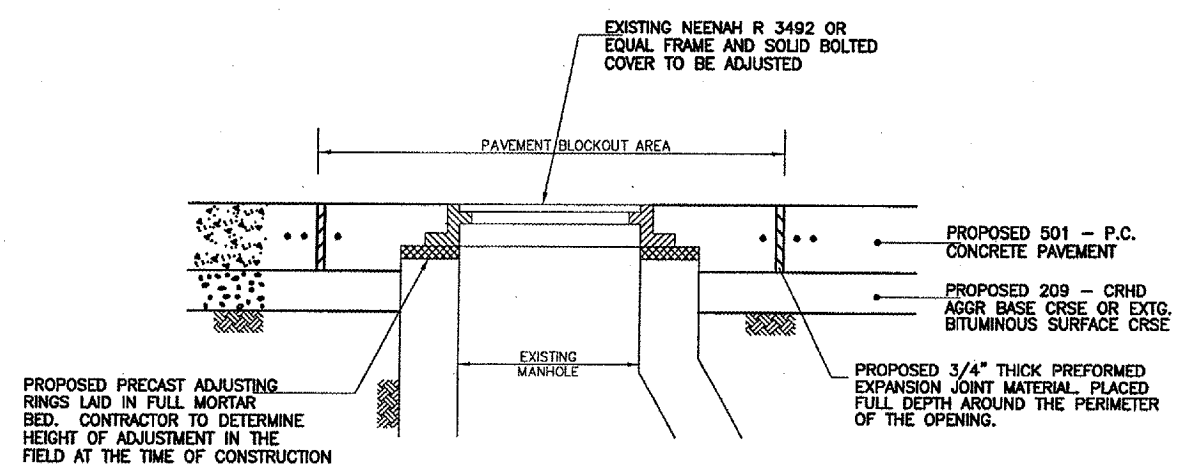
TYPICAL DETAIL - MANHOLE
 ADJUST - IN TURF



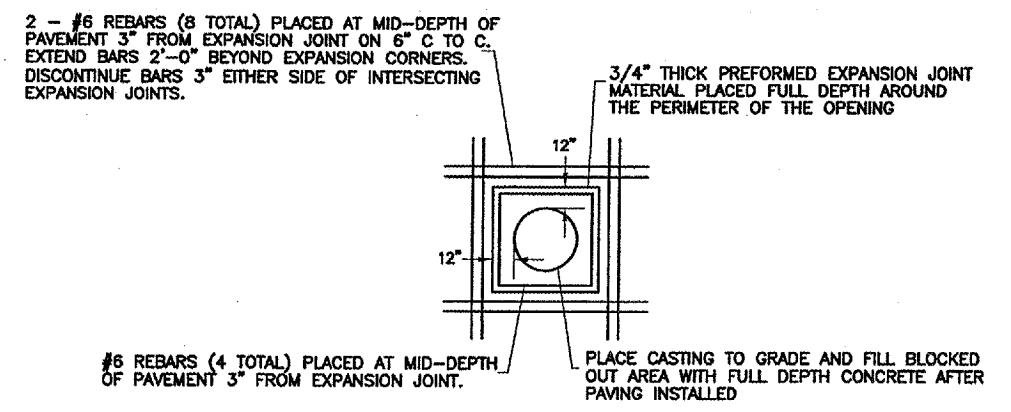
TYPICAL DETAIL - MANHOLE
 ADJUSTMENT IN BIT. PAVEMENT



SECTION A-A
 SILT FENCE DETAIL
 NOT TO SCALE

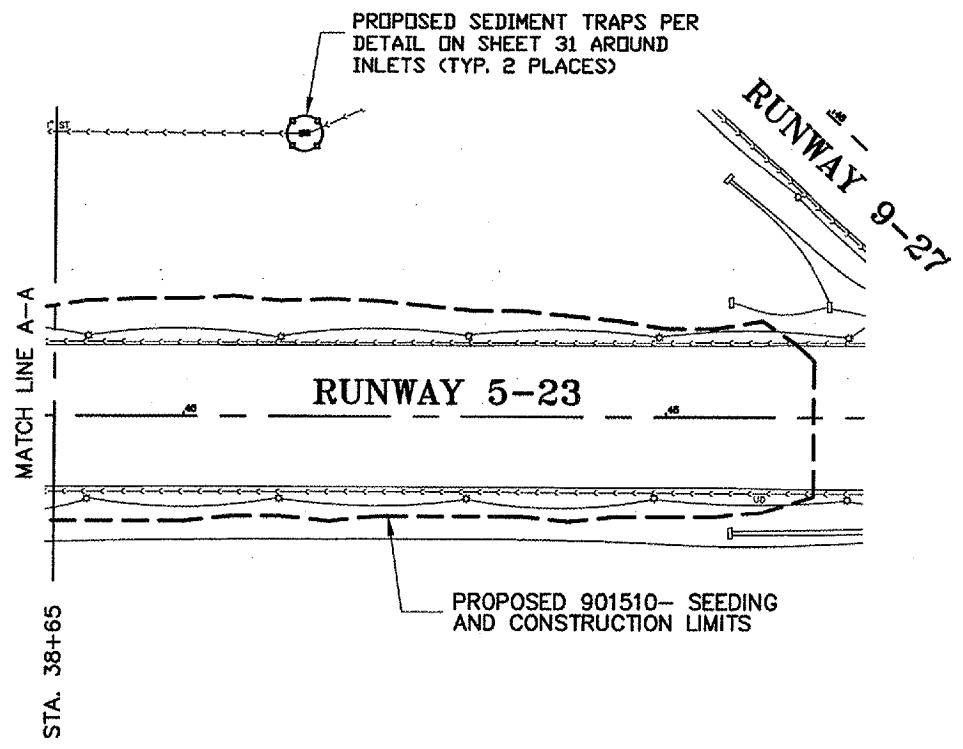
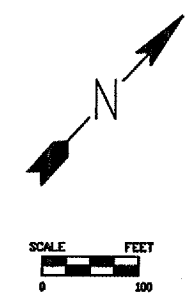
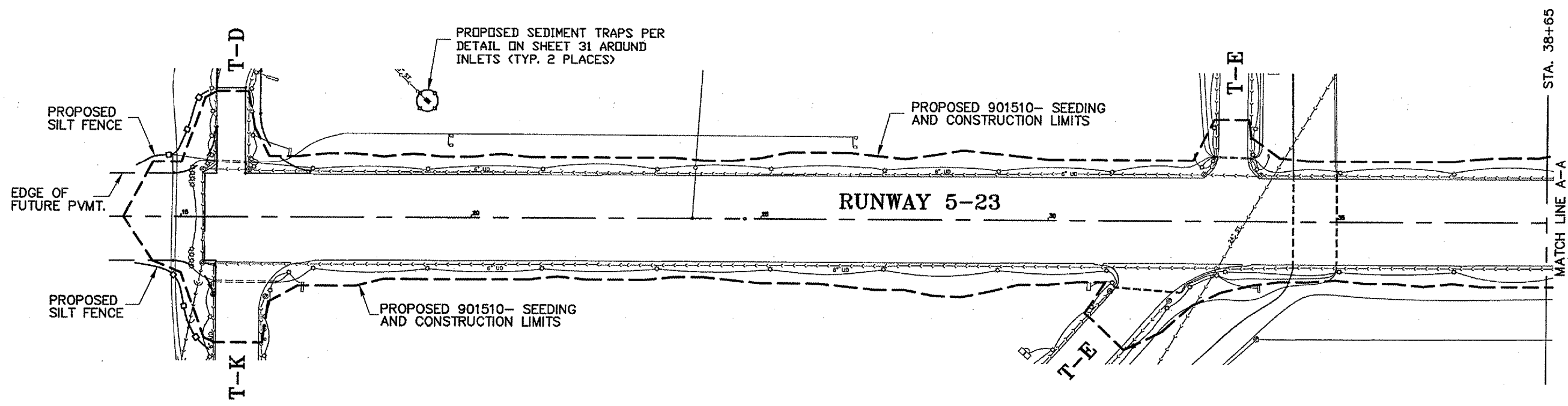


TYPICAL DETAIL - MANHOLE
 ADJUSTMENT IN P.C. PAVEMENT
 NO SCALE



PAVEMENT SQUARE BLOCKOUT
 FOR CIRCULAR CASTING
 NO SCALE

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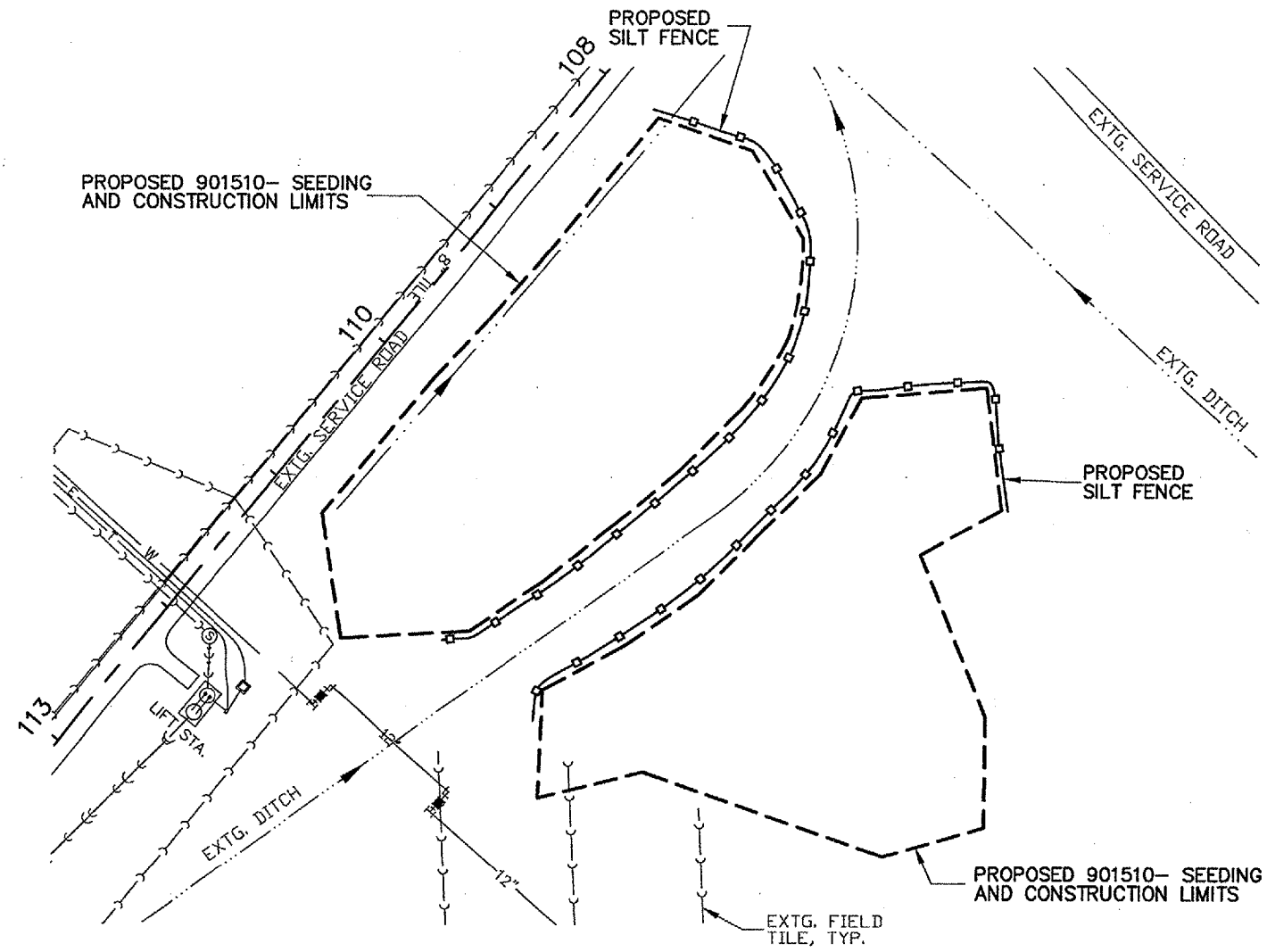
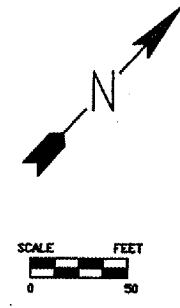


LEGEND:

	EXTG. EDGE LIGHT
	EXTG. GUIDANCE SIGN
	EXTG. ELECTRICAL CABLE
	EXTG. STORM SEWER OR UNDERDRAIN
	EXTG. INLET
	EXTG. OUTLET
	EXTG. MANHOLE
	EXTG. DRAINAGE DITCH
	PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
	EXISTING FENCE
	EXISTING OR PROPOSED DRAINAGE SWALE
	PROPOSED SILT FENCE
	PROPOSED TEMPORARY STRAW BALE DITCH CHECK
	PROPOSED SEDIMENT TRAP

GENERAL NOTES:

1. TEMPORARY DITCH CHECKS - TWO BALES HIGH WITH SILT FENCING SHALL BE REQUIRED PER IDOT STANDARD 280001 - TEMPORARY EROSION SITE CONTROL SYSTEM DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING, MAINTAINING, AND REMOVING DITCH CHECKS, SILT FENCE, AND SEDIMENT TRAPS TO THE SATISFACTION OF THE ENGINEER. THIS INCLUDES, BUT IS NOT LIMITED TO, CLEANING EROSION SOILS AS REQUIRED.
2. LOCATION OF THE DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHOWN ARE APPROXIMATE. ACTUAL LOCATIONS TO BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION.
3. ONCE CONSTRUCTION HAS BEEN COMPLETED, OR TEMPORARILY SUSPENDED FOR LONGER THAN 21 DAYS (SUCH AS A WINTER SHUTDOWN), THE CONTRACTOR SHALL SEED ALL AREAS DISTURBED IN ACCORDANCE WITH ITEM 901510 WITHIN 14 DAYS OF THE LAST DISTURBANCE. DITCH CHECKS, SILT FENCES, AND SEDIMENT TRAPS SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL THE CONTRACTOR ESTABLISHES A GOOD STAND OF GRASS OF UNIFORM COLOR AND DENSITY TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR AND EACH SUBCONTRACTOR RESPONSIBLE FOR WATER POLLUTION CONTROL SHALL DESIGNATE, PRIOR TO BEGINNING CONSTRUCTION, A PERSON OR PERSONS WHO CAN BE CONTACTED IN AN EMERGENCY INVOLVING THEIR WATER POLLUTION CONTROL ITEMS. THESE DESIGNATED PEOPLE SHALL BE AVAILABLE TO REPAIR AND MAINTAIN WATER POLLUTION CONTROL DEVICES ON A 24-HOUR / 7 DAYS PER WEEK BASIS.
5. CONTRACTOR TO EXCAVATE TEMPORARY EROSION CONTROL DRAINAGE SWALE AS REQUIRED TO PREVENT RAIN WATER PONDING AND TO CONTROL STORM WATER RUN-OFF.
6. CONTRACTOR SHALL ADHERE TO THE CITY OF MOLINE'S EROSION AND SEDIMENT CONTROL REGULATIONS AND THE ILLINOIS MANUAL ON EROSION AND SEDIMENT CONTROL.
7. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY CONSTRUCTION THAT DISTURBS EXISTING STORM WATER RUN-OFF CONDITIONS AND/OR GROUND VEGETATION.
8. EROSION CONTROL MEASURES SHALL BE INSPECTED WEEKLY AND AFTER EACH PRECIPITATION EVENT AND REPLACED OR REPAIRED AS NECESSARY.
9. RESIDENT ENGINEER SHALL CHECK THAT ALL FILL AREAS ARE TO A MINIMUM COMPACTION OF 95% OF THE MATERIALS STANDARD PROCTOR MAXIMUM DRY DENSITY.
10. SILT FENCE, SEDIMENT TRAPS, AND HAY BALES SHALL BE CLEANED OR REPLACED WHEN SILT BUILDS UP TO WITHIN ONE FOOT OF THE TOP OF THE SILT FENCE OR HAY BALES.
11. ADDITIONAL EROSION CONTROL DEVICES SHALL BE USED AS REQUIRED. THE COSTS OF ALL MEASURES NECESSARY TO COMPLY WITH THIS STORM WATER POLLUTION PREVENTION PLAN (INCLUDING TEMPORARY SEEDING FOR ANY TEMPORARY PROJECT SUSPENSION) SHALL BE INCLUDED IN THE ITEM 156500 - TEMPORARY EROSION CONTROL LUMP SUM PRICES.
12. SEE SHEET 33 FOR STORM WATER MANAGERS SIGNATURE CHART, ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE, AND EROSION / SEDIMENT CONTROL MEASURES LIST.



EARTHWORK CUT AREA PLAN

- LEGEND:**
- ⊙ EXTG. EDGE LIGHT
 - EXTG. GUIDANCE SIGN
 - EXTG. ELECTRICAL CABLE
 - (—) EXTG. STORM SEWER OR UNDERDRAIN
 - ▨ EXTG. INLET
 - ▽ EXTG. OUTLET
 - ⊙ EXTG. MANHOLE
 - (—) EXTG. DRAINAGE DITCH
 - (—) PROPOSED CONSTRUCTION, SEEDING, AND GRADING LIMITS
 - (—) EXISTING FENCE
 - (—) EXISTING OR PROPOSED DRAINAGE SWALE
 - (—) PROPOSED SILT FENCE
 - ▨▨▨▨ PROPOSED TEMPORARY STRAW BALE DITCH CHECK
 - ⊙ PROPOSED SEDIMENT TRAP

NOTE:
 SEE SHEET 32 FOR STORM WATER POLLUTION PREVENTION GENERAL NOTES.

ANTICIPATED PROBABLE CONSTRUCTION ACTIVITIES SCHEDULE		WEEK															
NO.	ITEM DESCRIPTION	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
1	INSTALL SEDIMENT/EROSION CONTROLS	█															
2	CLEARING AND GRADING		█														
3	EXCAVATION AND EMBANKMENT			█	█												
4	TURFING																
5	MAINTAIN SEDIMENT/EROSION CONTROL	█	█	█	█	█	█	█	█	█	█	█	█	█	█	█	█
6	PAVING																
7	CLEAN-UP																█

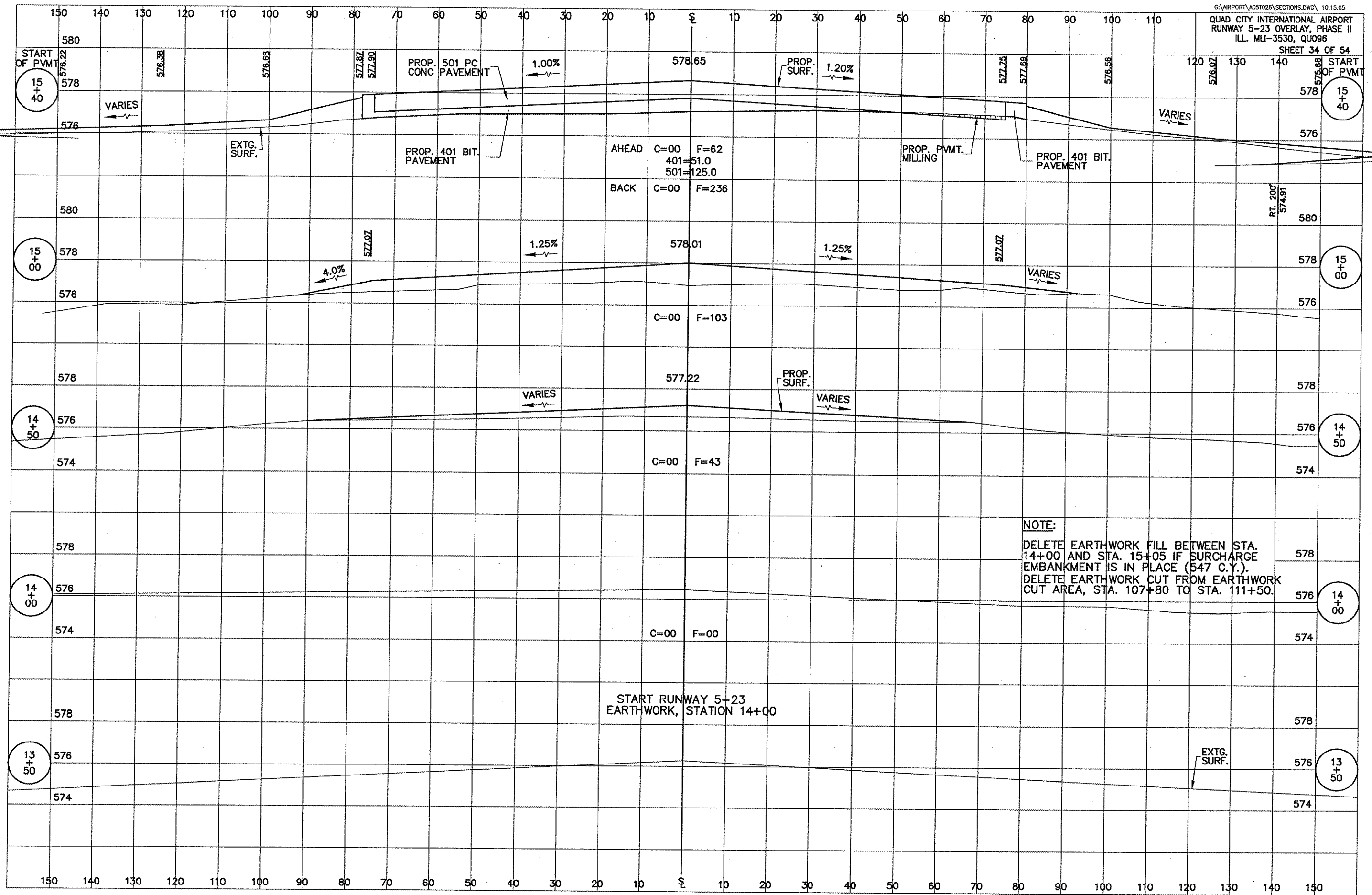
STORM WATER MANAGERS					
	NAME	ADDRESS	TELEPHONE NUMBER		SIGNATURE
			WORK	HOME	
CONTRACTOR					
SUBCONTRACTOR					
SUBCONTRACTOR					

EROSION AND SEDIMENT CONTROL MEASURES		
ITEM	LOCATION	SPECIAL PROVISION/PAY ITEM
TEMPORARY DITCH CHECKS	NONE REQUIRED	156500
TEMPORARY SILT FENCE	AT ALL SEDIMENT TRAPS; AT ALL DITCH CHECKS; WEST OF THE EXTG. DITCH, CUT AREA STA. 107+75 TO STA. 111+17, LT.; EAST OF THE EXTG. DITCH, CUT AREA STA. 107+96 TO STA. 111+17, LT.; AND RWY 5 STA. 14+20 TO STA. 15+25, LT. & RT.	156500
TEMPORARY SEDIMENT TRAPS	RUNWAY 5-23, STA. 19+21, LT. AND RUNWAY 5-23, STA. 41+22, LT	156500

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LT. 180' LAP LINE
576.06

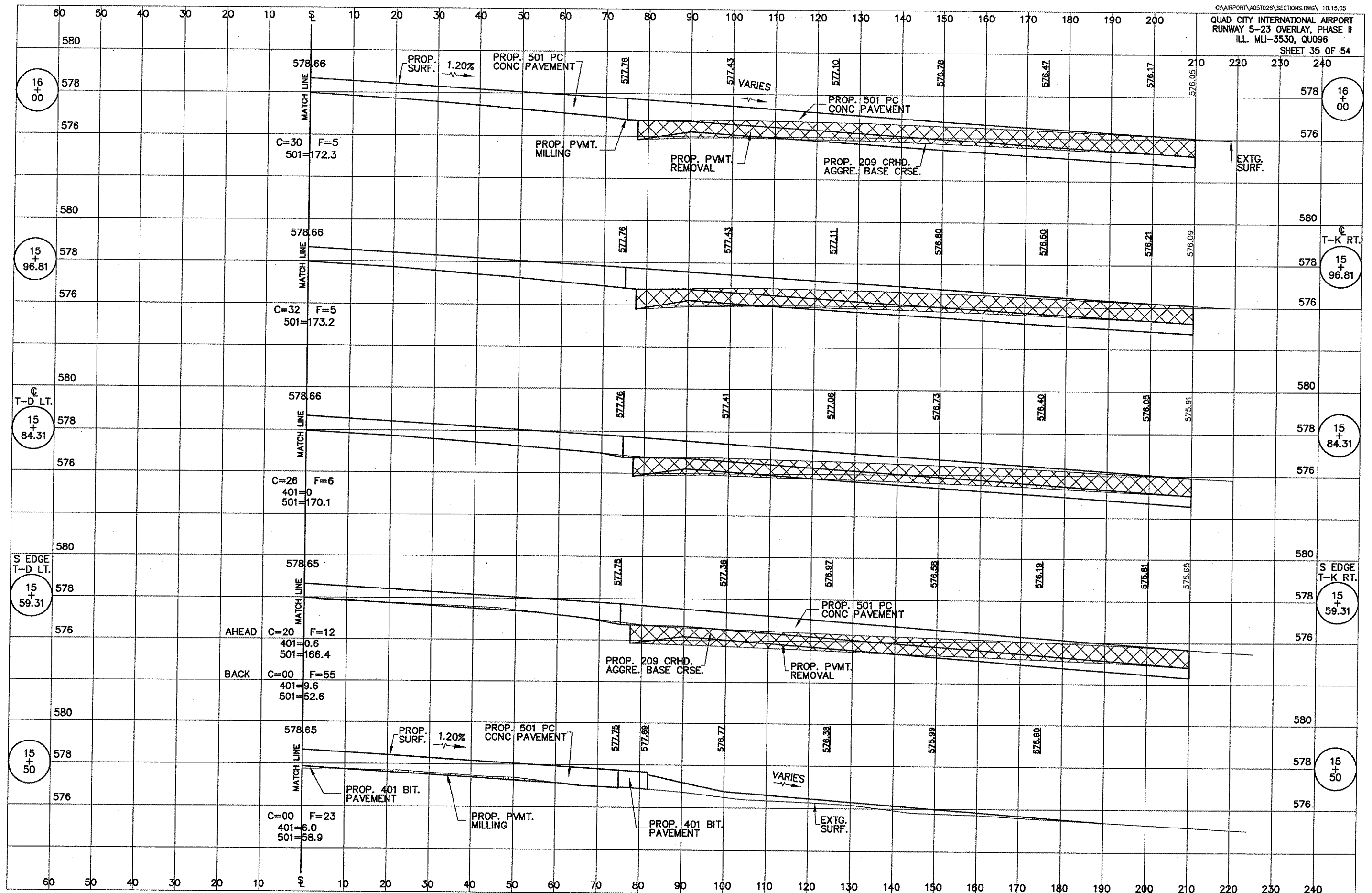
RT. 170' LAP LINE
576.29



START RUNWAY 5-23
EARTHWORK, STATION 14+00

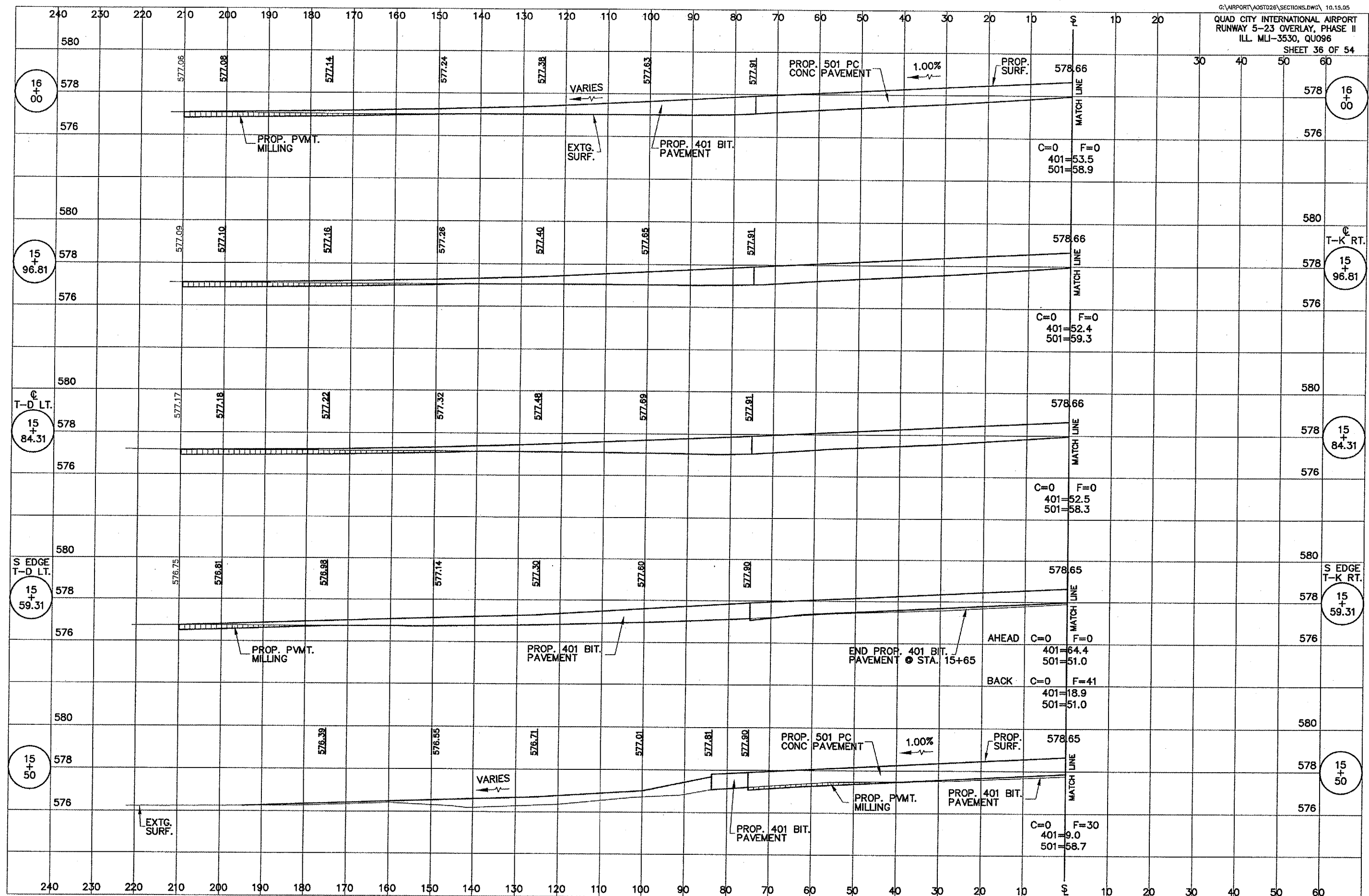
NOTE:
DELETE EARTHWORK FILL BETWEEN STA.
14+00 AND STA. 15+05 IF SURCHARGE
EMBANKMENT IS IN PLACE (547 C.Y.).
DELETE EARTHWORK CUT FROM EARTHWORK
CUT AREA, STA. 107+80 TO STA. 111+50.

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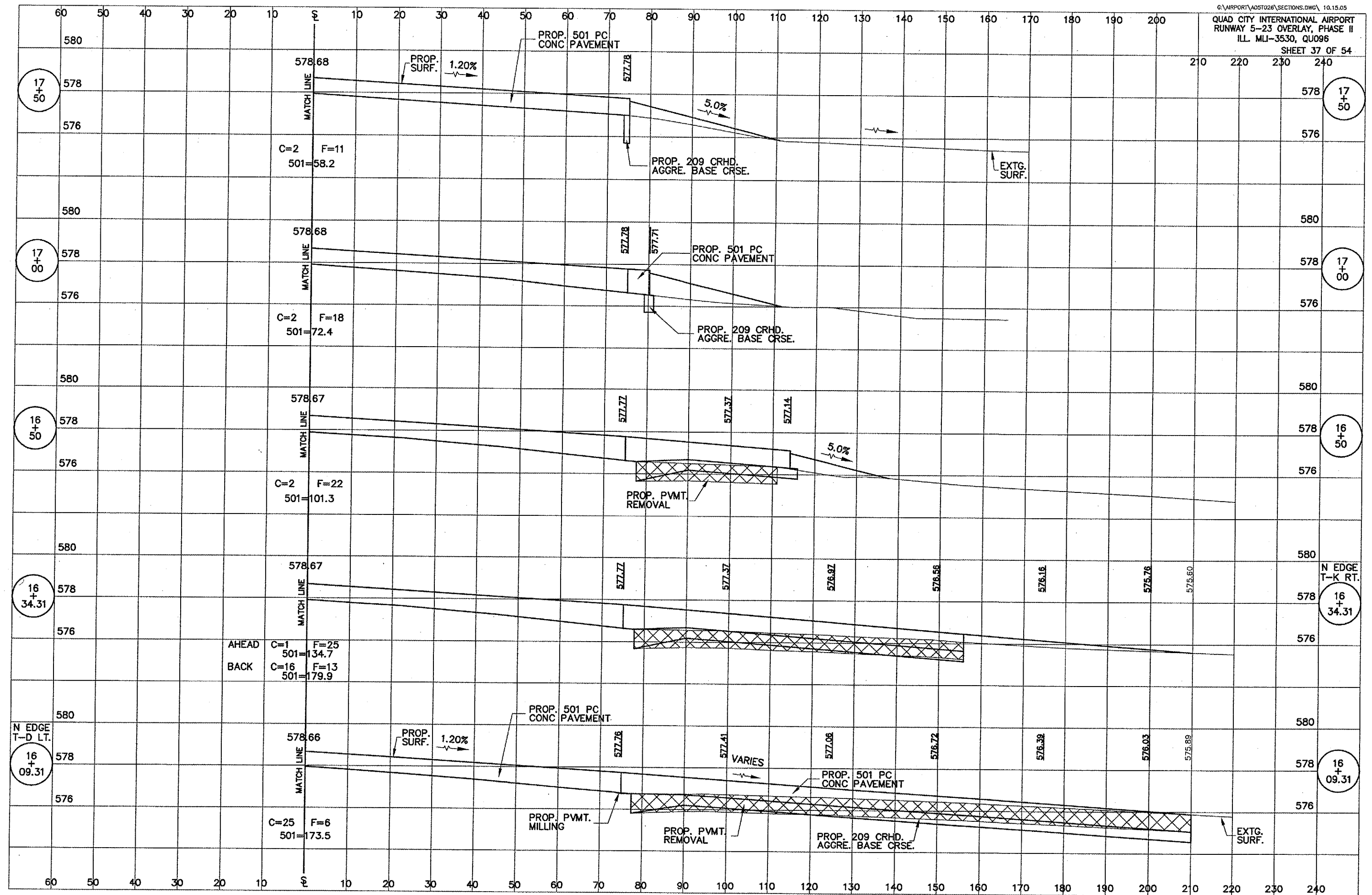
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 15+50 TO STA 16+00, RIGHT

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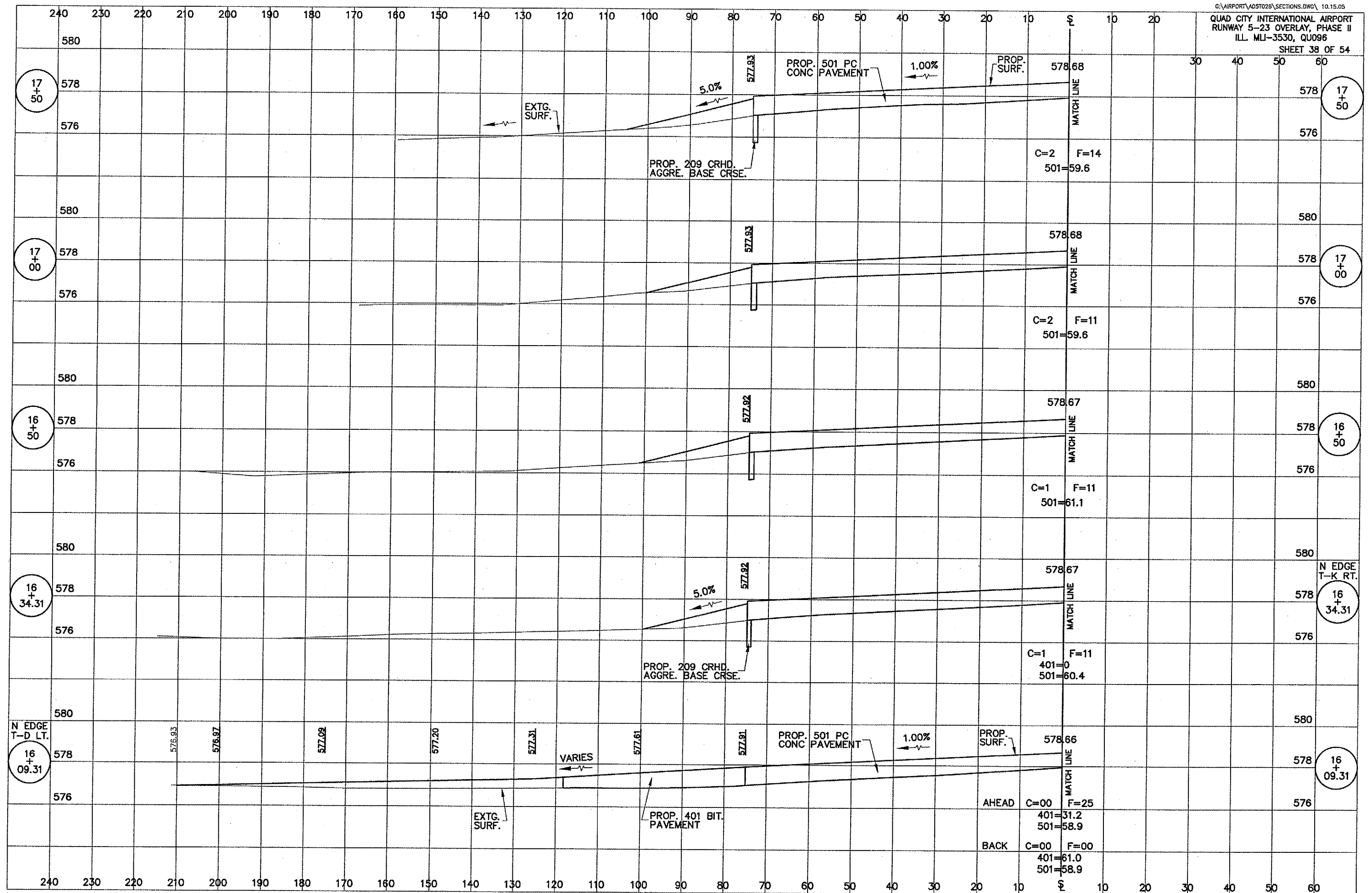
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 15+50 TO STA 16+00, LEFT 36/54

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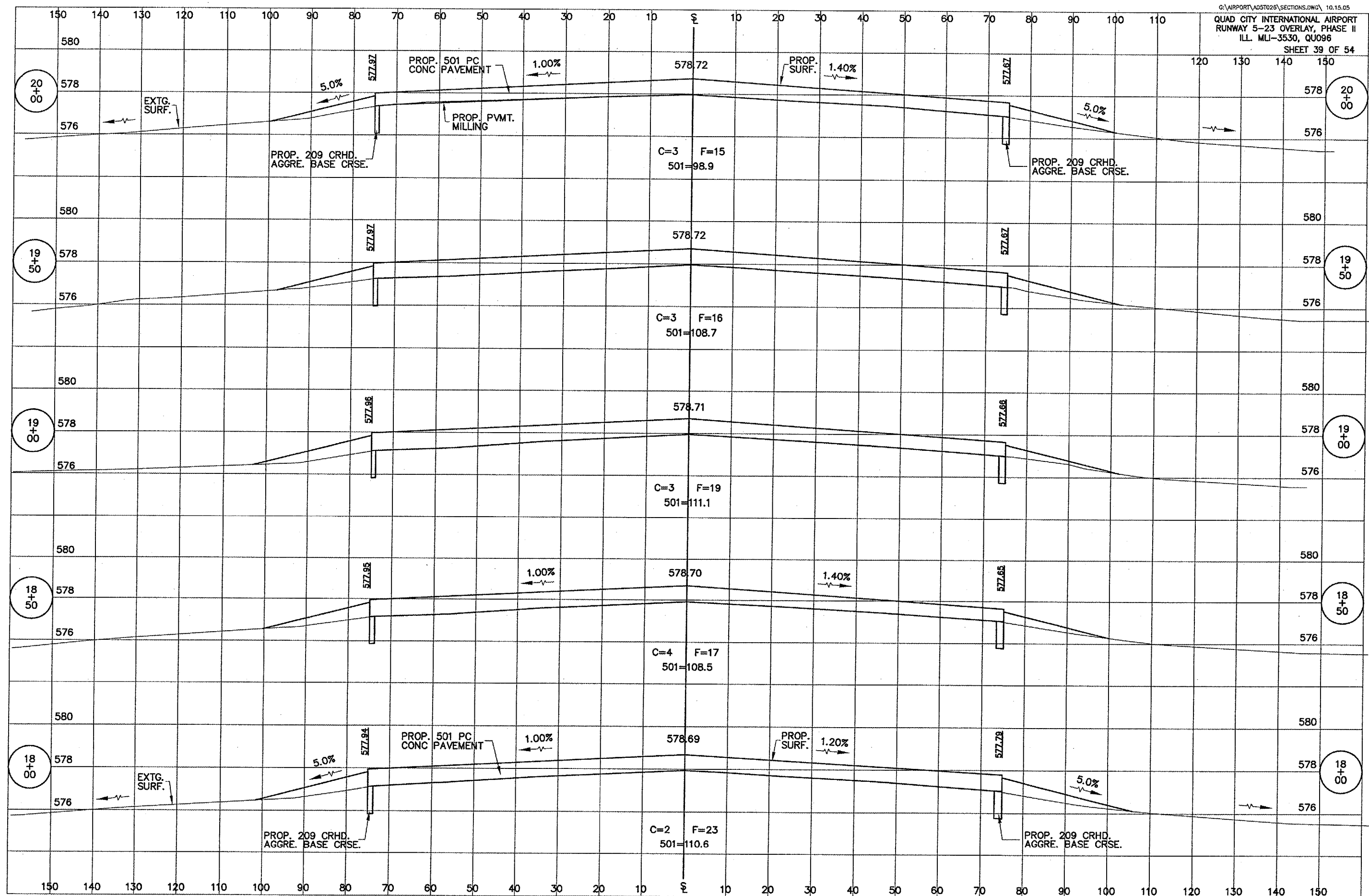
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 16+09.31 TO STA 17+50, RIGHT

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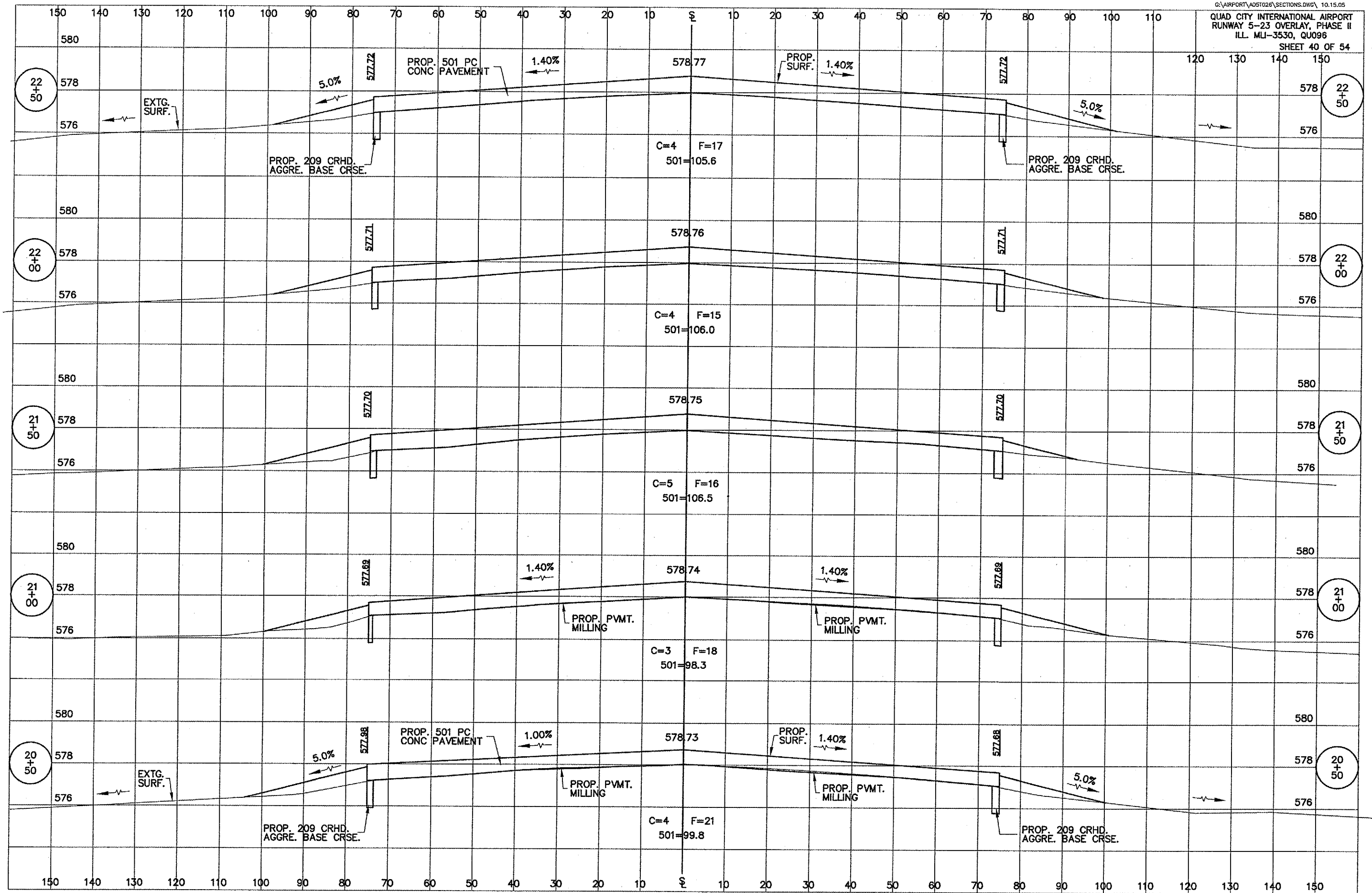
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 16+09.31 TO STA 17+50, LEFT

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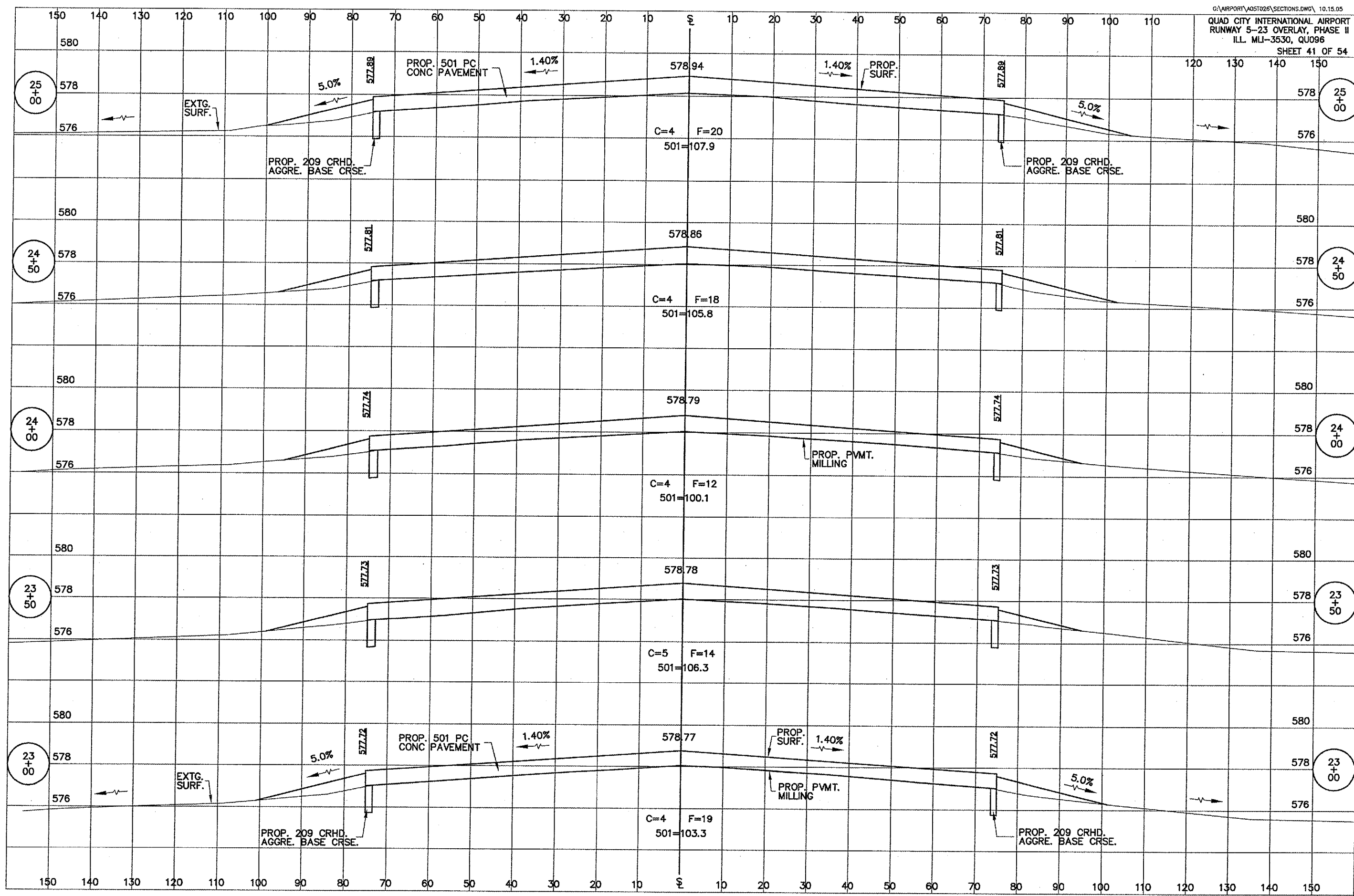
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 18+00 TO STA 20+00

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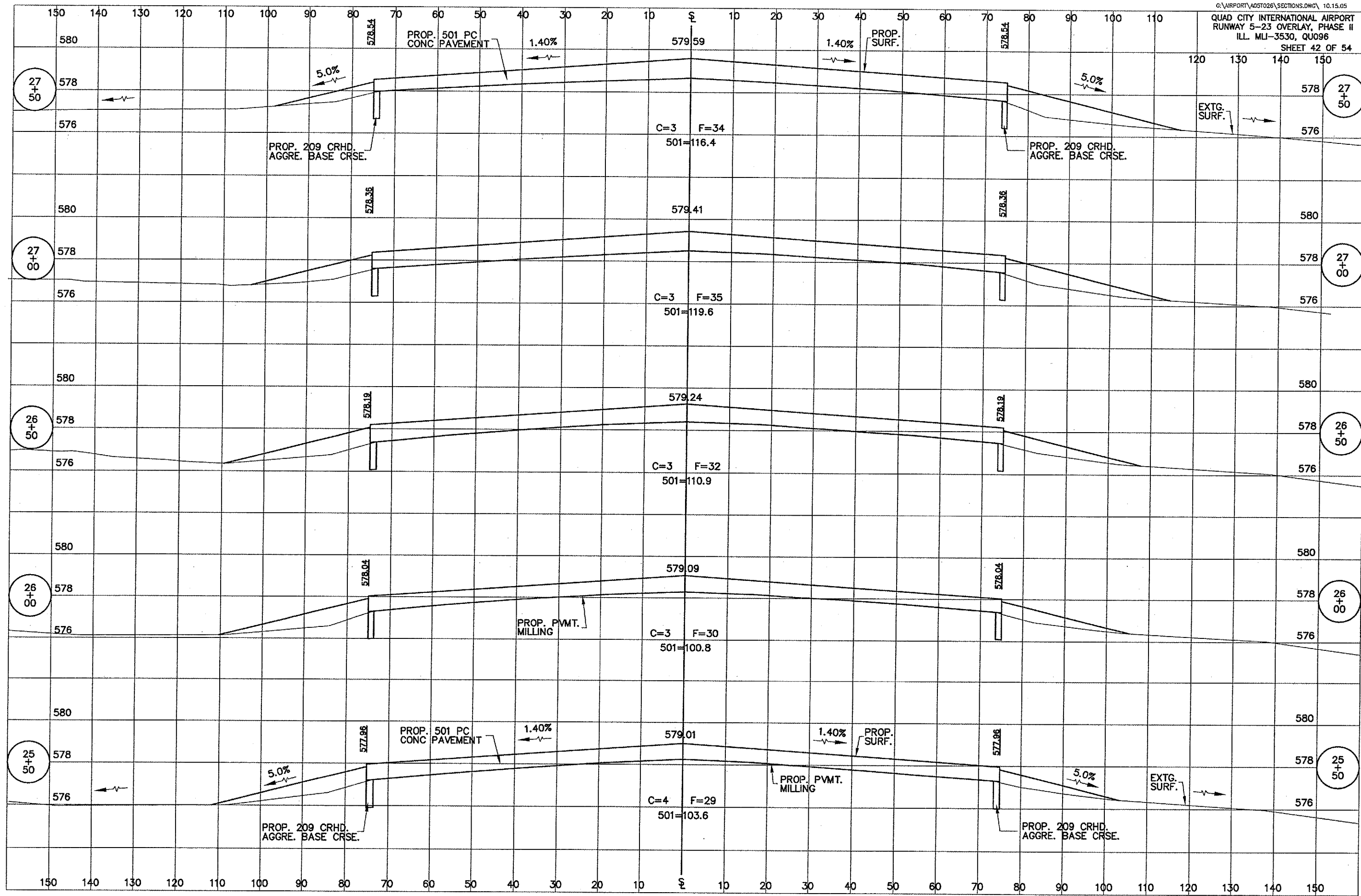
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 20+50 TO STA 22+50

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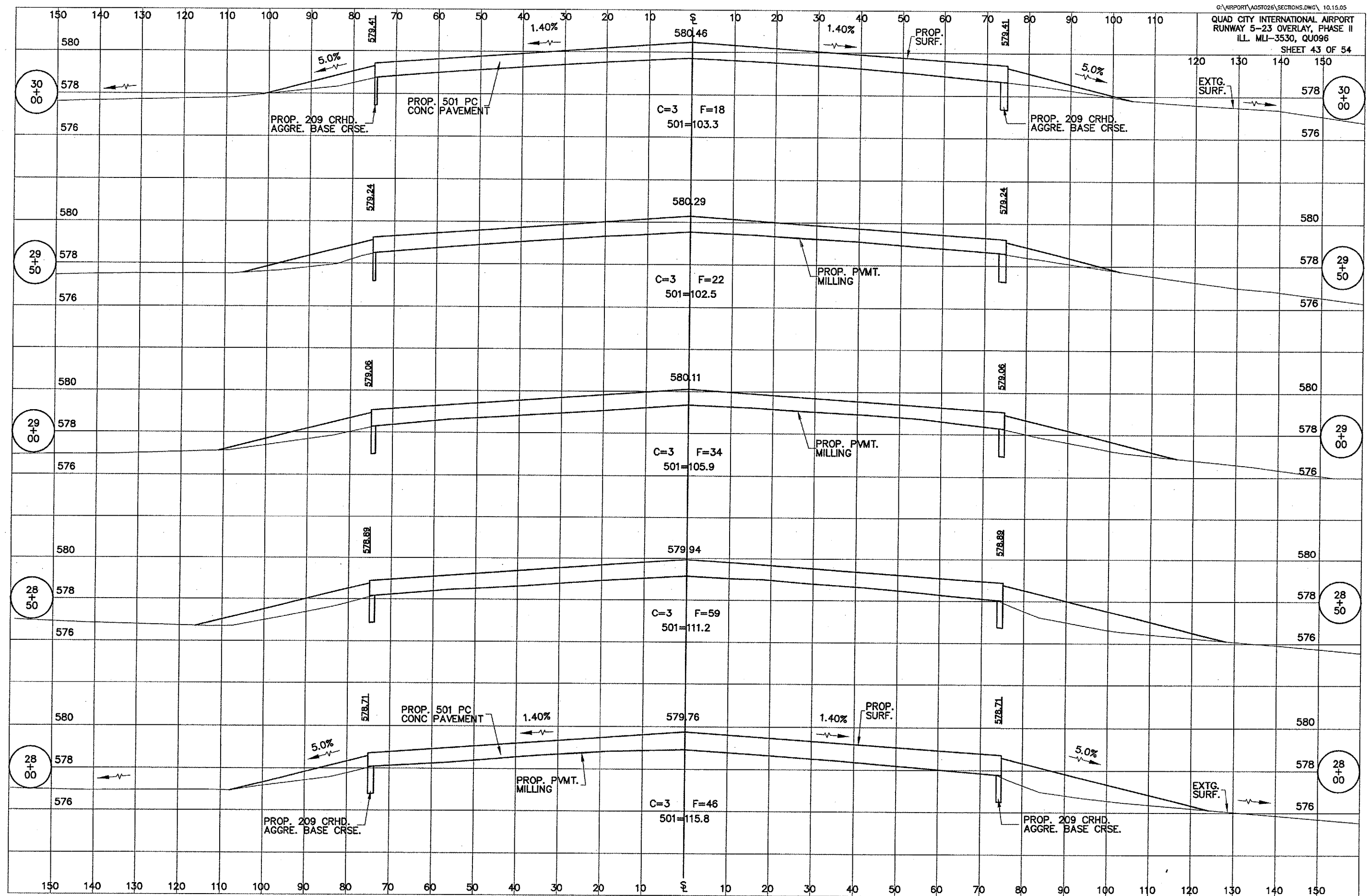
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 23+00 TO STA 25+00

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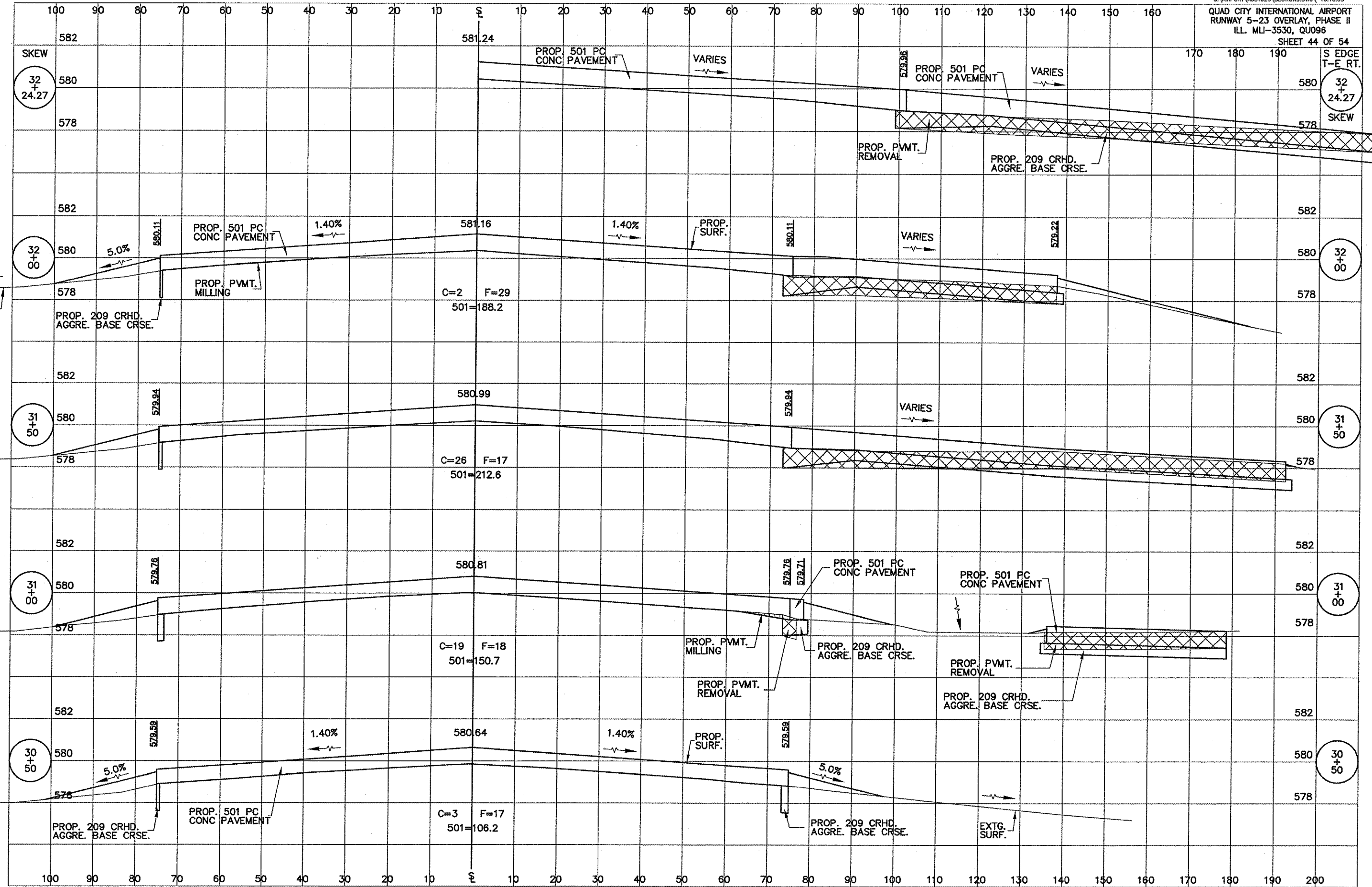
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 25+50 TO STA 27+50

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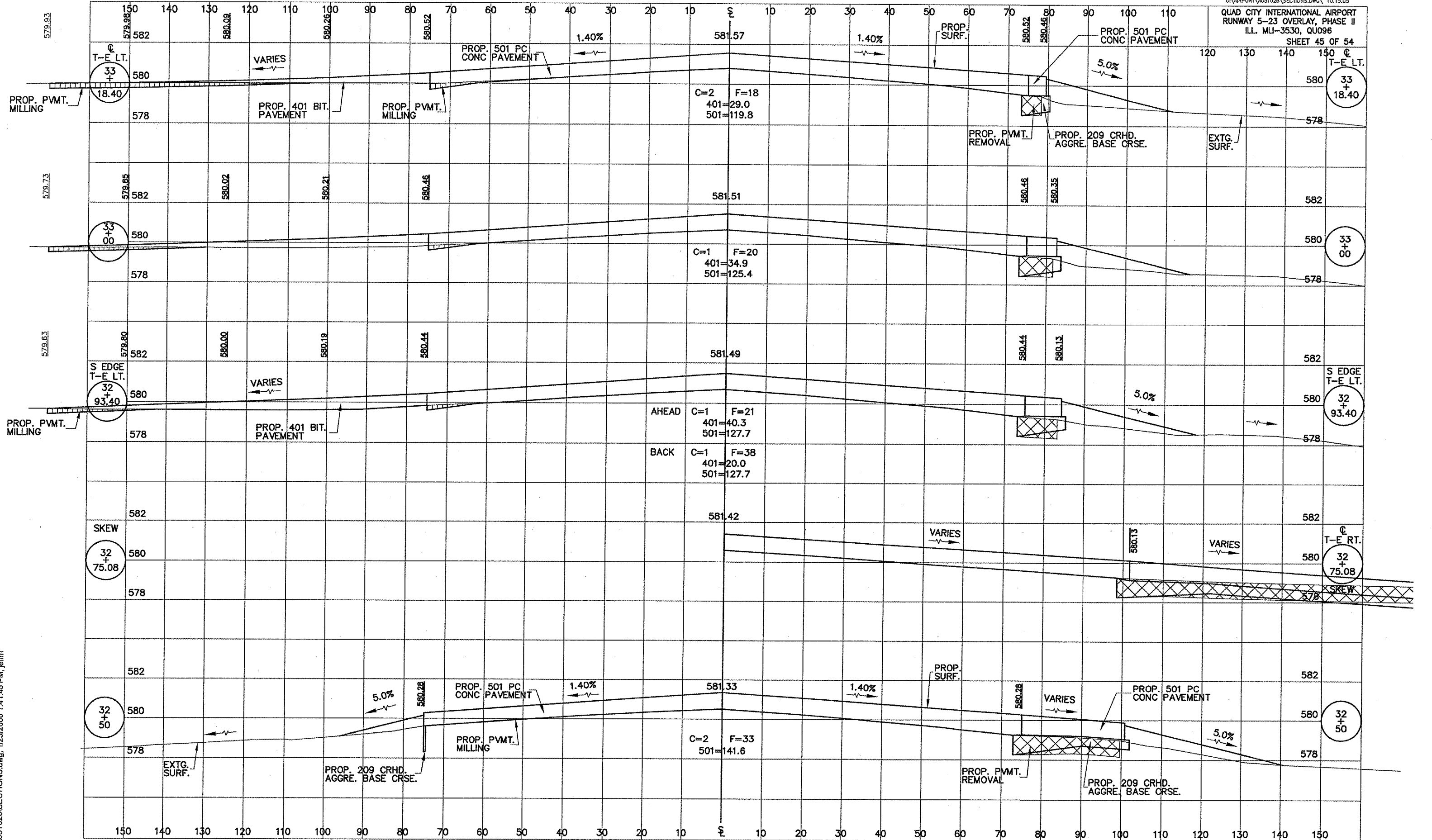
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 28+00 TO STA 30+00

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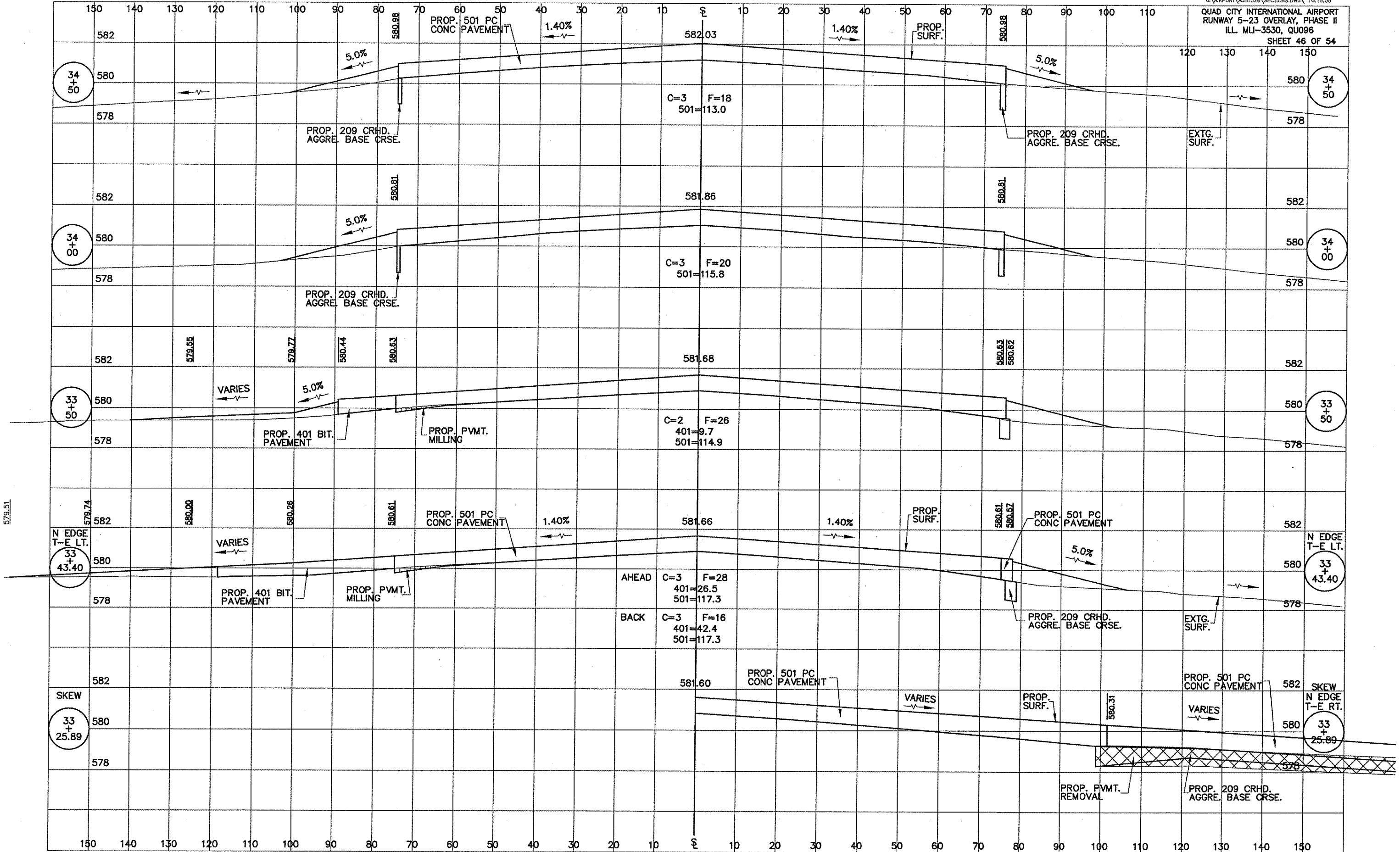
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 30+50 TO STA 32+24.27

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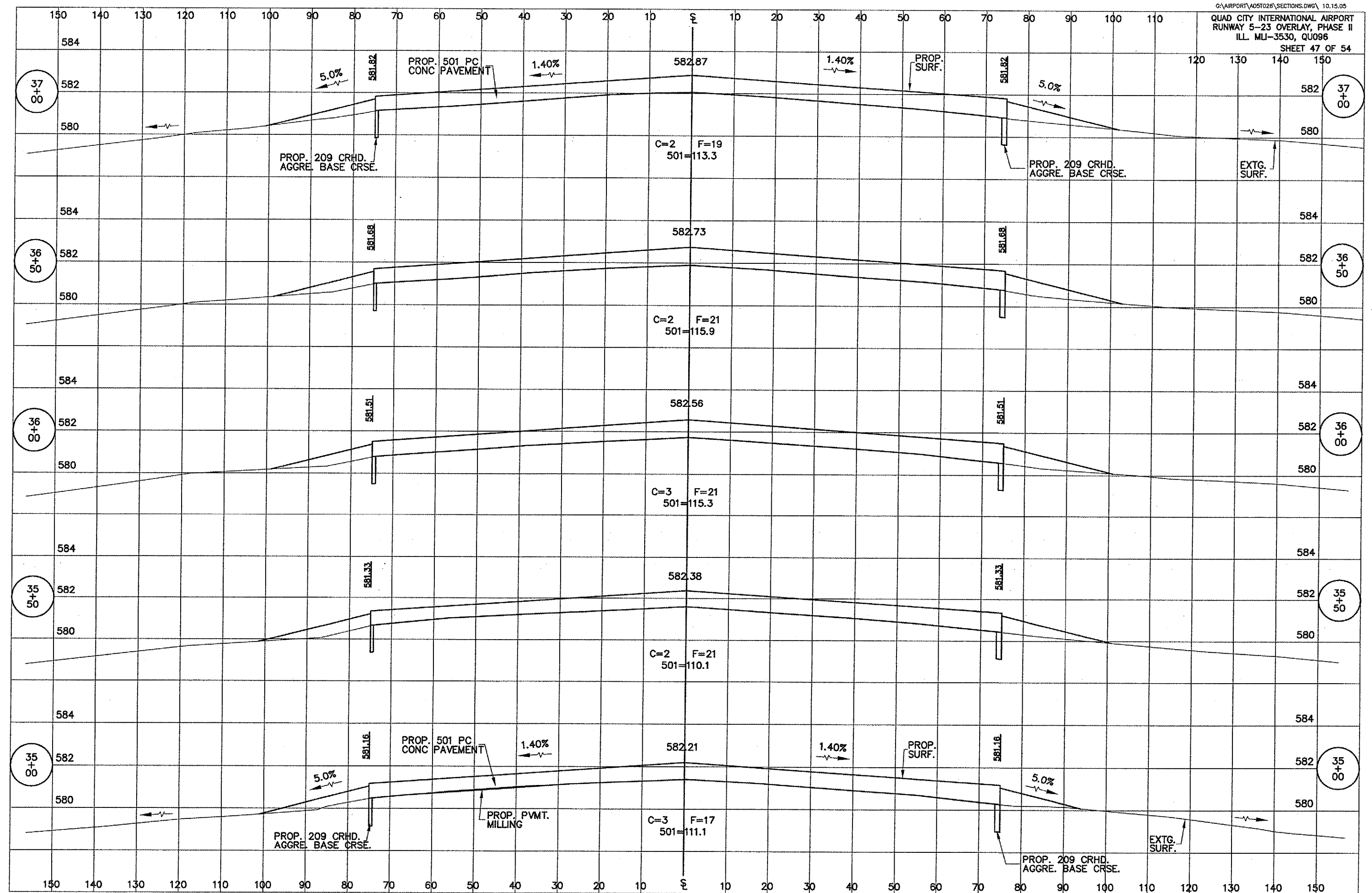
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 32+50 TO STA 33+18.40

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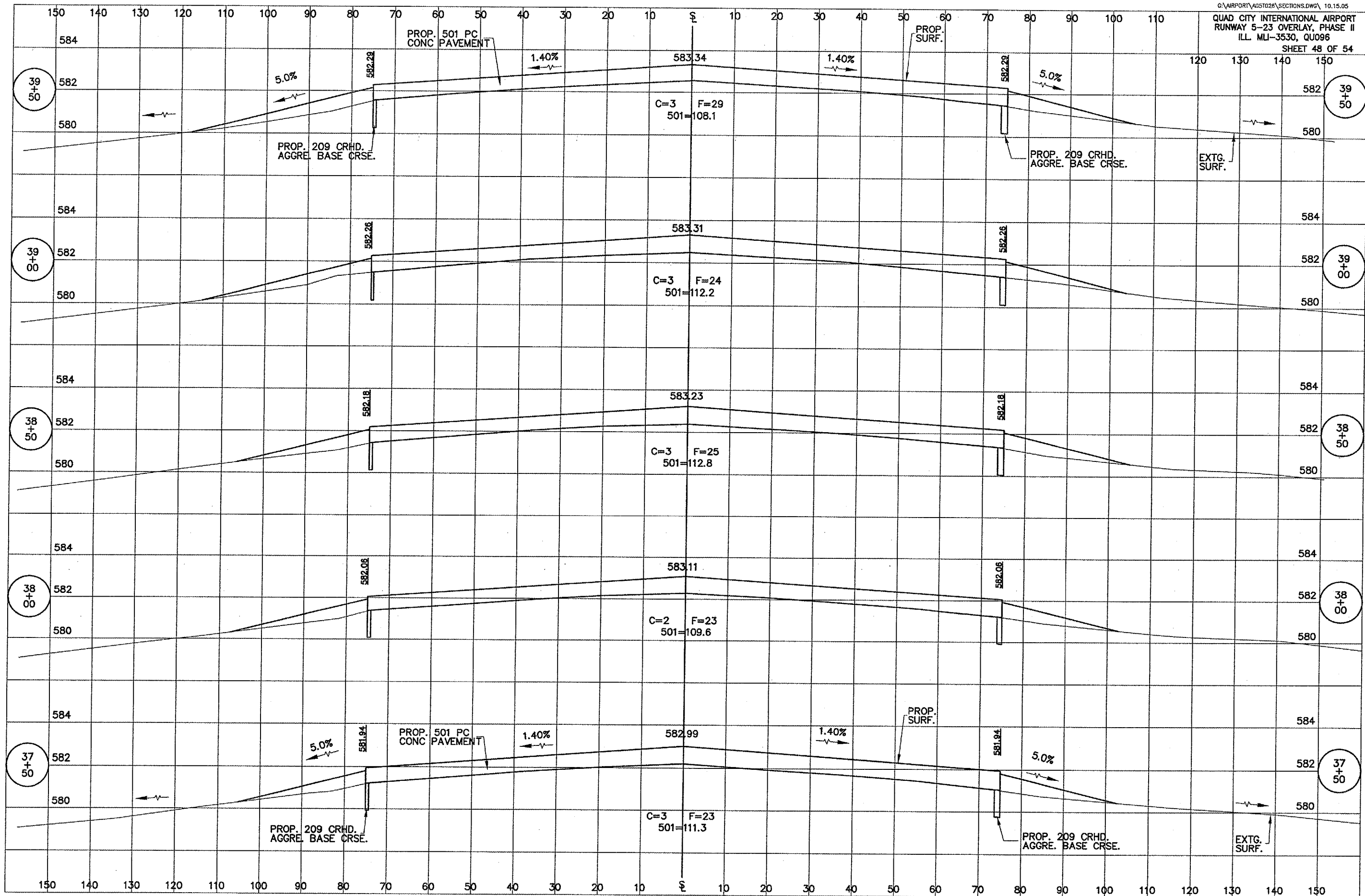
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 33+25.89 TO STA 34+50

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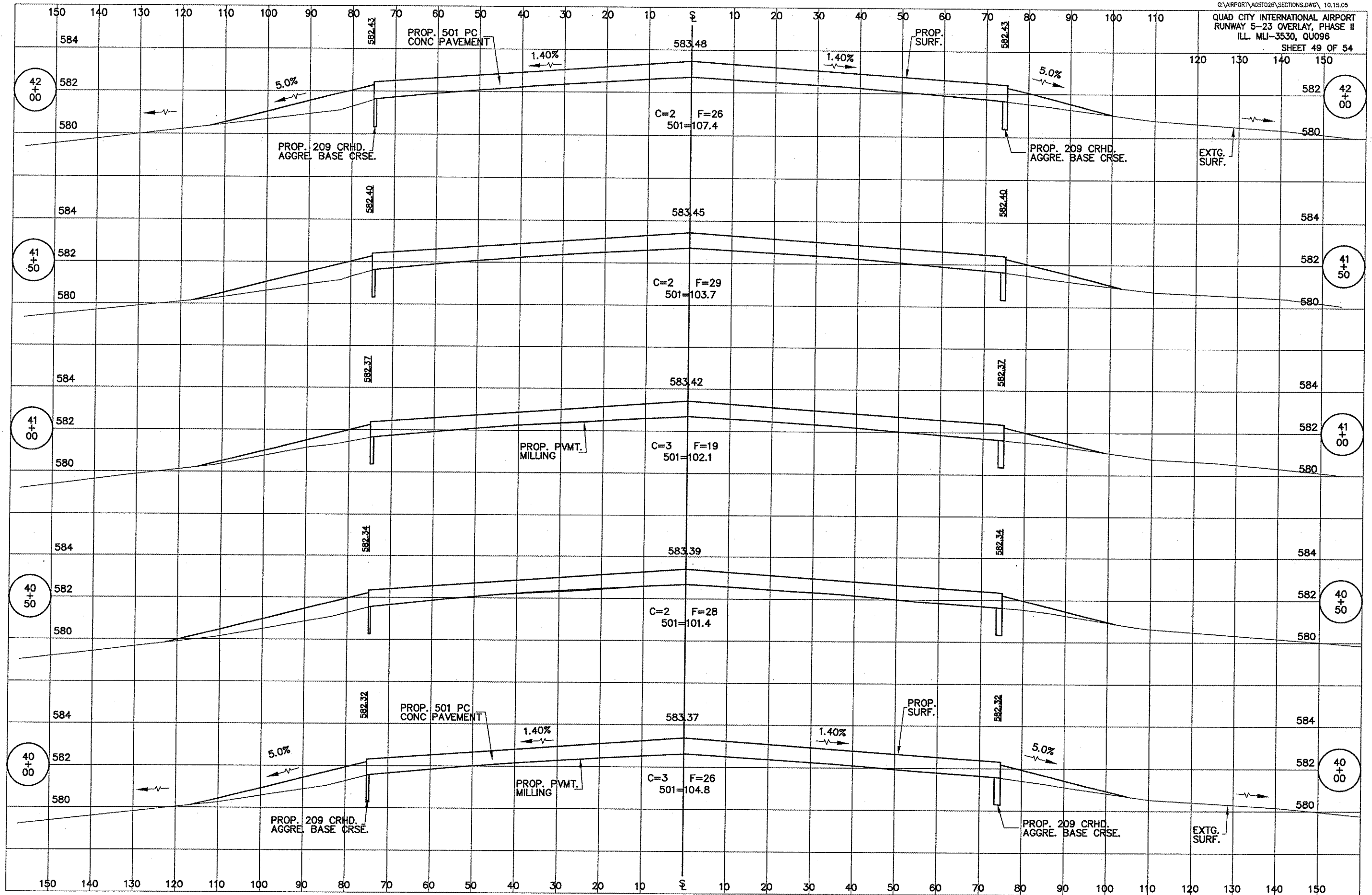
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 35+00 TO STA 37+00

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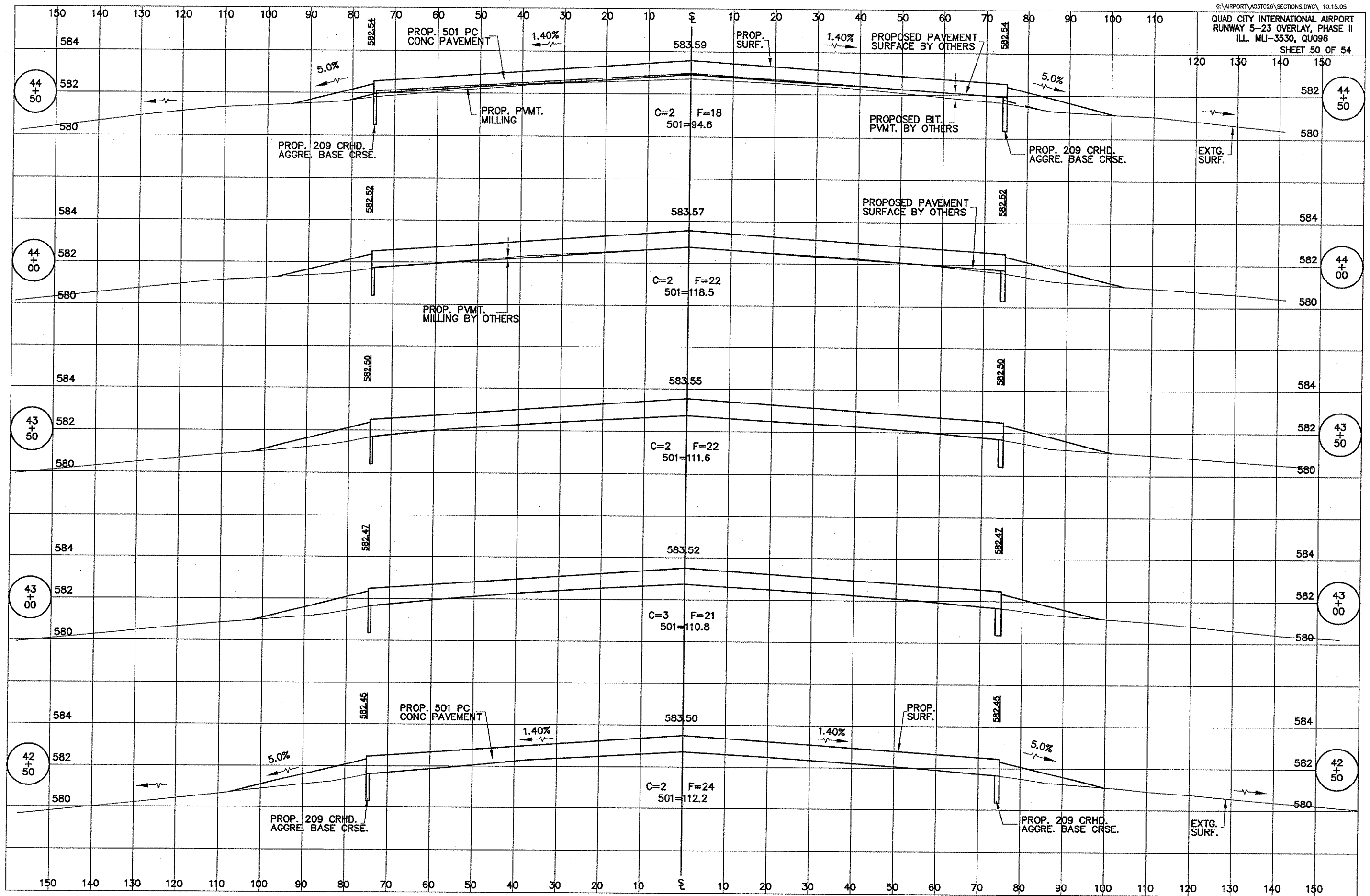
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 37+50 TO STA 39+50

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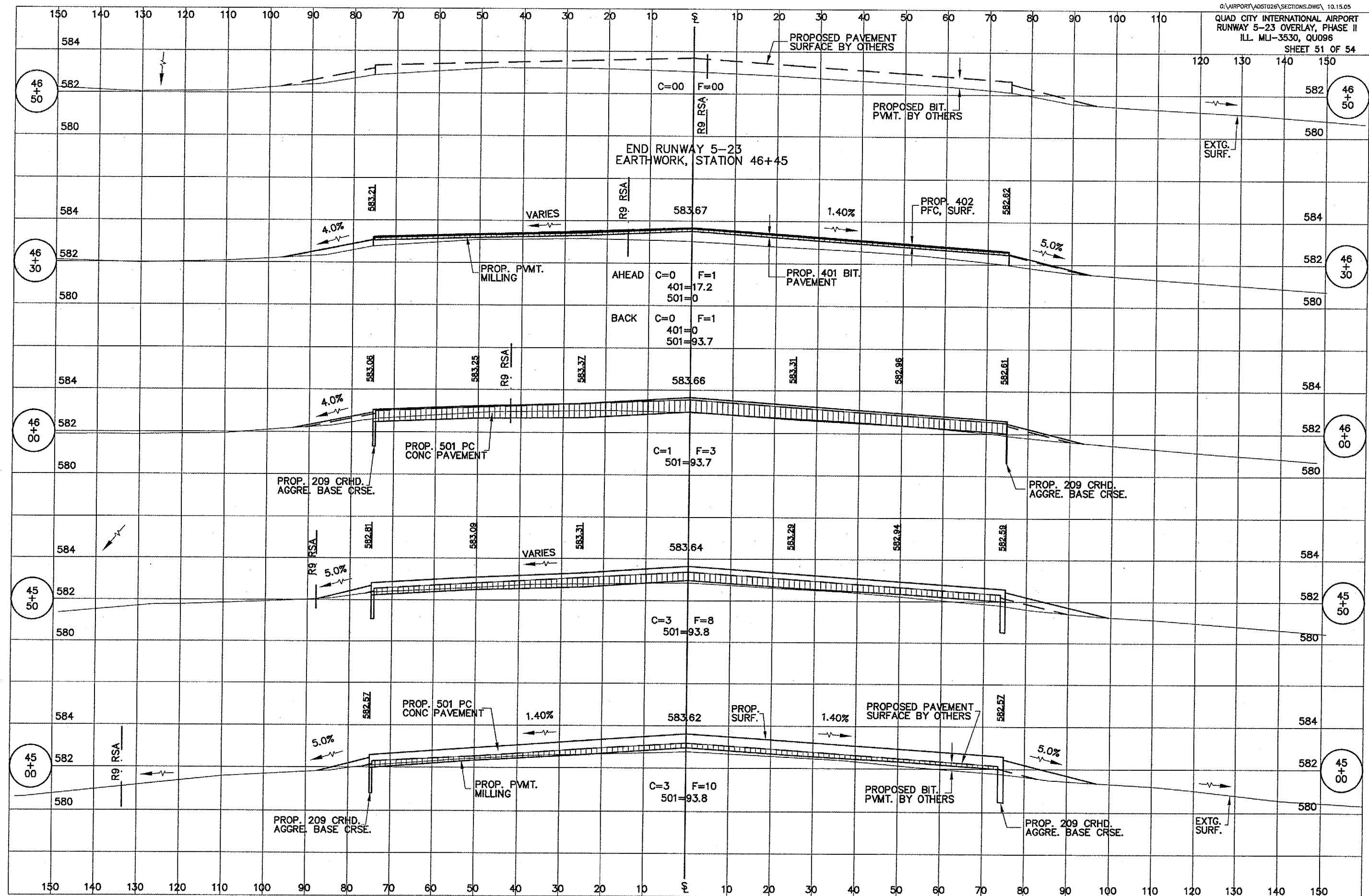
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 40+00 TO STA 42+00

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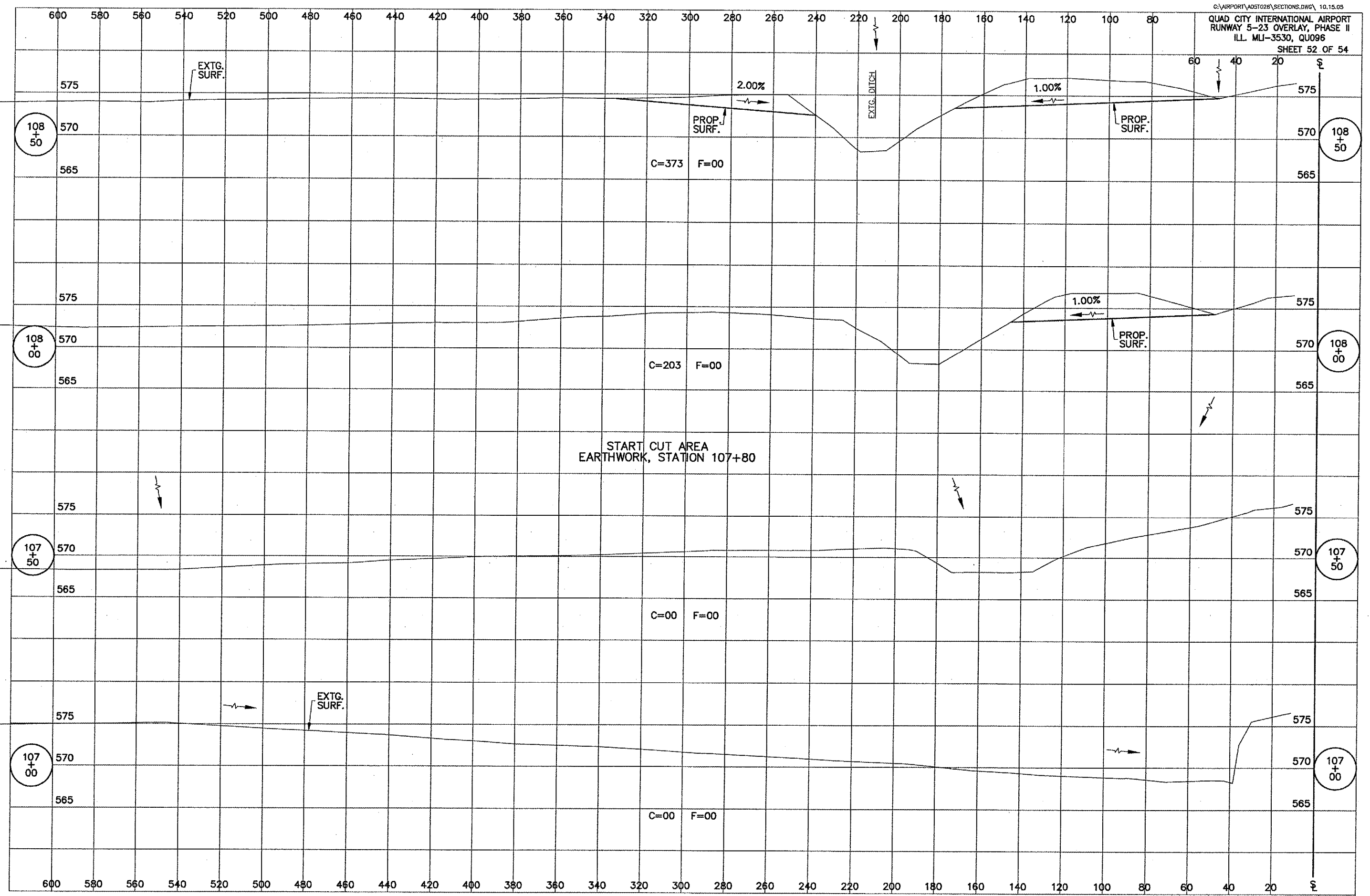
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 42+50 TO STA 44+50

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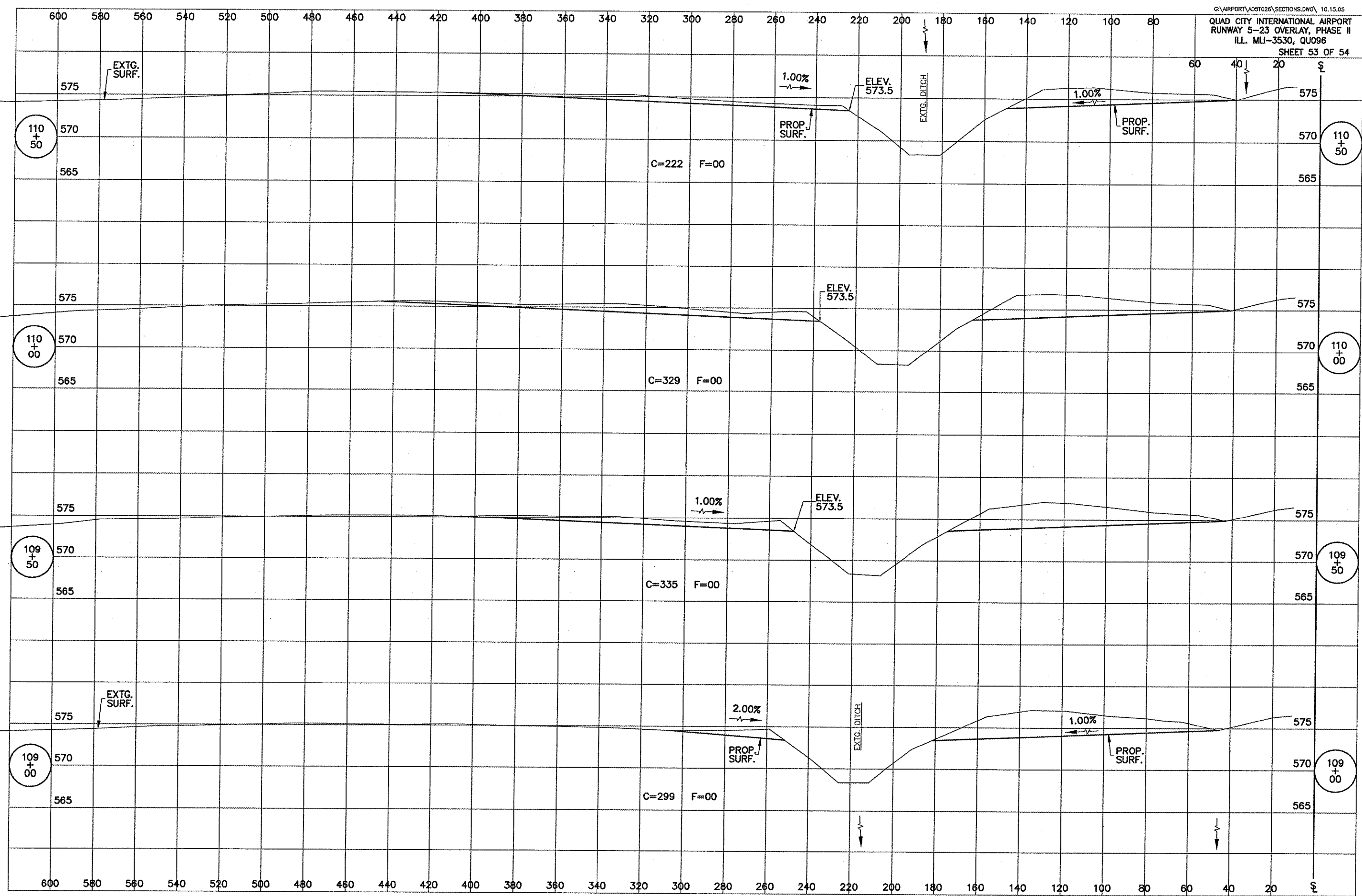
RUNWAY 5-23 OVERLAY CROSS SECTIONS, STA 45+00 TO STA 46+50

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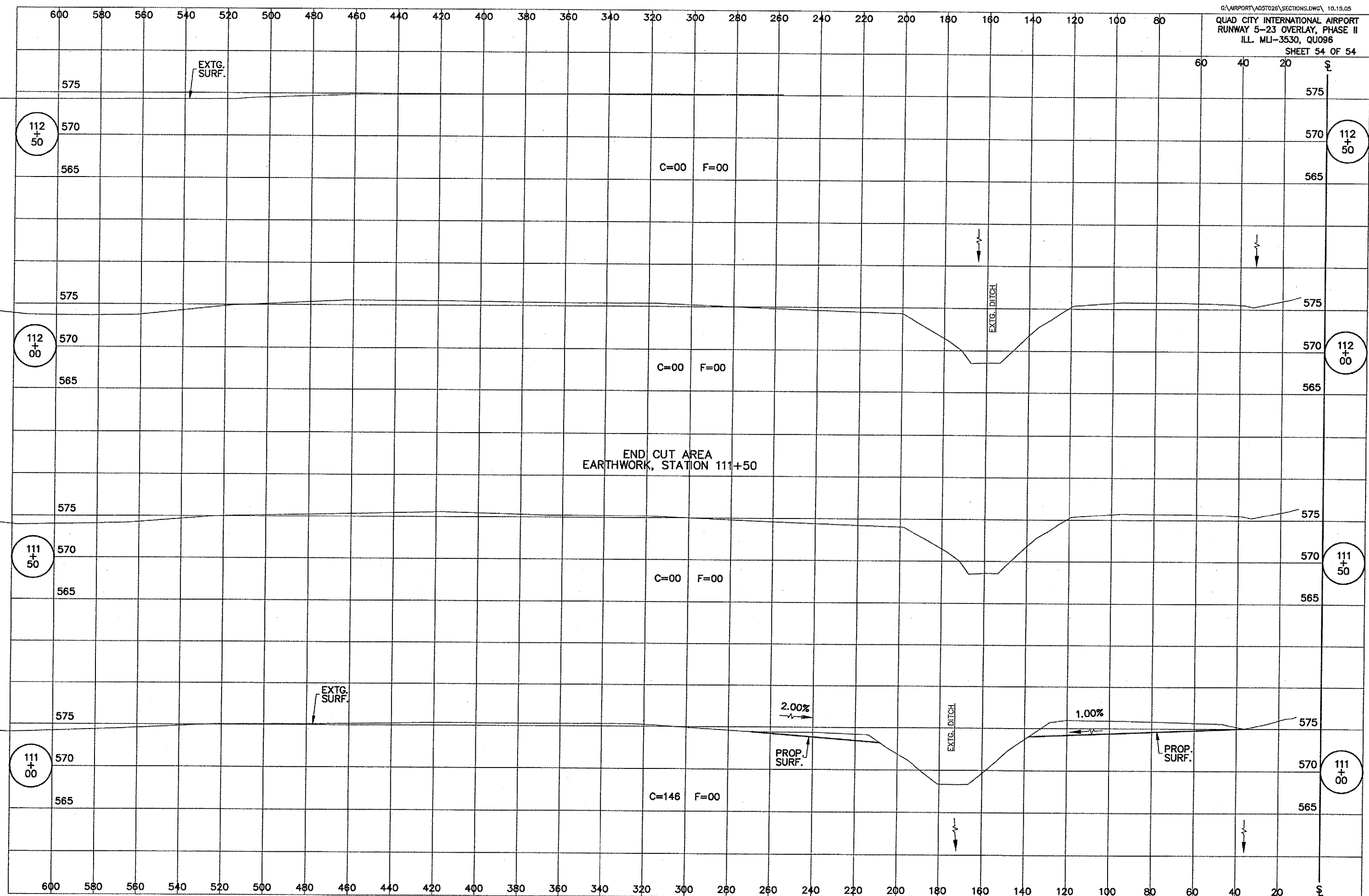
EARTHWORK CUT AREA CROSS SECTIONS, STA 107+00 TO STA 108+50

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EARTHWORK CUT AREA CROSS SECTIONS, STA 109+00 TO STA 110+50

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END CUT AREA
EARTHWORK, STATION 111+50

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