

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF COUNTRY CLUB HILLS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

GLEN OAKS DRIVE FROM SUNSET RIDGE DRIVE TO 168TH STREET
BRIARGATE DRIVE FROM GLEN OAKS DRIVE TO 169TH STREET
GLEN OAKS COURT FROM GLEN OAKS DRIVE TO END
VIVIAN DRIVE FROM GLEN OAKS DRIVE TO END
RESURFACING AND STORM SEWER IMPROVEMENTS

PROJECT No.: HPD-897(008)
SECTION No.: 04-00027-03-PV
JOB No.: C-91-004-05
COOK COUNTY

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	1
STA.		TO STA.		
FED. ROAD DIST. NO		ILLINOIS	FED AID PROJECT	
C-91-004-05			HPD-897(008)	
CONTRACT No.: 63539				

PROJECT NUMBER: HPD-897(008)
GLEN OAKS COURT
END IMPROVEMENTS
STA. 2 + 74

PROJECT NUMBER: HPD-897(008)
GLEN OAKS DRIVE
END IMPROVEMENTS
STA. 30 + 74

PROJECT NUMBER: HPD-897(008)
BRIARGATE DRIVE
END IMPROVEMENTS
STA. 7 + 33

PROJECT NUMBER:
HPD-897(008)
VIVIAN DRIVE
BEGIN IMPROVEMENTS
STA. 1 + 00

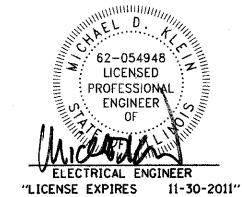
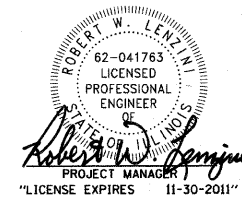
PROJECT NUMBER: HPD-897(008)
BRIARGATE DRIVE
BEGIN IMPROVEMENTS
STA. 1 + 00

PROJECT NUMBER:
HPD-897(008)
VIVIAN DRIVE
END IMPROVEMENTS
STA. 2 + 53

PROJECT NUMBER:
HPD-897(008)
GLEN OAKS COURT
BEGIN IMPROVEMENTS
STA. 1 + 00

PROJECT NUMBER: HPD-897(008)
GLEN OAKS DRIVE
BEGIN IMPROVEMENTS
STA. 9 + 45

BREMEN TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 3,089 LF OR 0.585 MILES
NET LENGTH OF IMPROVEMENT = 3,089 LF OR 0.585 MILES



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

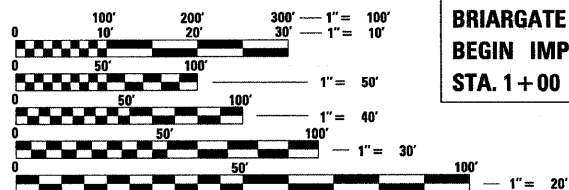
APPROVED 12-7-10
Daniel Barr
CITY OF COUNTRY CLUB HILLS, REPRESENTATIVE

PASSED JANUARY 4, 2011
Christina Barr
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED
REVIEW JANUARY 6, 2011
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

TRAFFIC DATA

GLEN OAKS DRIVE, GLEN OAKS COURT,
BRIARGATE DRIVE, VIVIAN DRIVE
POSTED & DESIGN SPEED LIMIT = 20 MPH
2010 ADT = 200 VPD
ROADWAY CLASSIFICATION: LOCAL ROAD



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. A1523040



CONTACT JULIE AT 811 OR 800-892-0123
WITH THE FOLLOWING:
COUNTY = COOK
CITY-TWNSHP. = COUNTRY CLUB HILLS-BREMEN
SEC. & 1/4 SEC. NO. = SEC.26 NW 1/4, T36N, R13E
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 63539

B&W PROJECT NO.: 041180 DATE: 12-01-10

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STATE OF ILLINOIS PROFESSIONAL ENGINEER
PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAMBERG, IL

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY OR SPRINKLER SYSTEM THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER IN ACCORDANCE WITH ARTICLES 105.07 AND 107.31.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE UTILITIES, AND SHALL REPAIR ANY UTILITIES AT HIS OR HER OWN EXPENSE IN ACCORDANCE WITH ARTICLES 105.07 AND 107.31. COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT A PRECONSTRUCTION CONFERENCE. THE CONTRACTOR IS RESPONSIBLE FOR THE COORDINATION OF ANY UTILITY RELOCATIONS REQUIRED.
5. THE CONTRACTOR SHALL NOTIFY THE CITY DIRECTOR OF PUBLIC WORKS AT 708-798-2616 AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN CITY UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. SPECIAL ATTENTION IS CALLED TO SECTION 107 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION. THE STORAGE OF EQUIPMENT AND/OR MATERIALS BEYOND LOCATIONS SHOWN IN THE PLANS SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.
6. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
7. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CITY RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF CITY WATER IF DEEMED NECESSARY. WATER IS INCLUDED IN THE COST OF MOBILIZATION.
8. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE ENGINEER TO RESIDENTS. EFFORT SHALL BE MADE TO NOTIFY PROPERTY OWNERS OF LIMITED ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN THE APPROPRIATE REPLACEMENT PAY ITEM.
9. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
10. ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL. INSTALLING AND REMOVING TEMPORARY RAMPS SHALL BE INCLUDED IN THE APPLICABLE AGGREGATE BASE COURSE PAY ITEM.
11. THE CONTRACTOR SHALL NOTIFY IDOT BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS PRIOR TO THE PLACEMENT OF HMA OR CONCRETE.
12. ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH ARTICLES 107.25 AND 107.20. THE COST OF THIS WORK IS INCLUDED IN THE APPROPRIATE REPLACEMENT PAY ITEM. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
13. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
14. IN AREAS WHERE THE EXISTING DRIVEWAY, SIDEWALK, OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED, THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE DRIVEWAY PAVEMENT REMOVAL, SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL PAY ITEMS.
15. THE CURB SHALL BE TAPERED TO THE GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE CURB AND GUTTER TERMINATES, WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER.
16. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
17. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
18. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
19. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 3/4" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5- FEET.
20. A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT PAY ITEM.
21. NOT USED
22. ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF CONSTRUCTION. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED OR SALVAGED BY EITHER THE PROPERTY OWNER OR CITY SHALL BE SALVAGED OR PROPERLY DISPOSED OF BY THE CONTRACTOR.
23. PRIOR TO CONSTRUCTION OF ANY PROPOSED UTILITIES, THE CONTRACTOR SHALL EXCAVATE AND LOCATE THE EXISTING UTILITIES TO VERIFY THEIR LOCATION, SIZE, AND DEPTH TO INSURE THAT GRADE CONFLICTS WILL NOT OCCUR. THE COST OF THIS EXPLORATION SHALL BE PAID FOR AS EXPLORATION TRENCH, SPECIAL.
24. CONNECTION OF PROPOSED STORM SEWER INTO EXISTING STORM SEWER OR EXISTING STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF STORM SEWERS.
25. CONNECTION OF EXISTING STORM SEWER INTO PROPOSED STORM SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE. ANY ADDITIONAL STORM SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE OF THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING STORM SEWER AND SHALL BE INCLUDED IN THE COST OF THE STORM SEWER STRUCTURE.
26. IF ANY STORM SEWER LATERALS ARE FOUND DURING CONSTRUCTION AND ARE NOT IDENTIFIED ON THE PLANS, THEY SHALL BE CONNECTED TO THE PROPOSED STORM SEWER SYSTEM AND INCLUDED IN THE COST OF THE STORM SEWER CONSTRUCTION.
27. STORM STRUCTURE OFFSET LOCATIONS ARE TO THE EDGE OF PAVEMENT IF THE STRUCTURE IS IN THE CURB LINE OR TO THE CENTER OF STRUCTURE IF THE STRUCTURE IS NOT IN THE CURBLINE.
28. IN ALL TRENCHES CROSSING DRIVEWAYS, SIDEWALKS, AND ALL PROPOSED AND EXISTING ROADWAYS, THE MATERIAL FOR THE TOP 12 INCHES SHALL BE CA-6 CRUSHED GRAVEL OR CRUSHED STONE AND BE INCLUDED IN THE PAY ITEM FOR TRENCH BACKFILL. THE BACKFILL SHALL EXTEND TO AND BE MEASURED FOR PAYMENT TO THE EXISTING GROUND OR SURFACE ELEVATION.
29. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF COST OF THE STRUCTURE.
30. SIGNS AND POSTS THAT ARE REPLACED, SHALL BE DELIVERED TO THE CITY PUBLIC WORKS FACILITY AT 4200 183RD STREET. THIS WORK SHALL BE INCLUDED IN THE REMOVE SIGN PANEL ASSEMBLY PAY ITEM.
31. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE MC-30 PRIME COAT APPLICATION RATE SHALL BE 0.3 GAL/SY.
32. FOR STEEL BARS CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT (847) 705-4337.
33. THE CONTRACTOR SHALL CLEAN EXISTING DRAINAGE STRUCTURES, AS DETERMINED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE STORM SEWER REMOVAL PAY ITEM.
34. THE DAY'S PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
35. DURING CONSTRUCTION, THE CONTRACTOR WILL BE PERMITTED TO LIMIT ON-STREET PARKING IN ORDER TO COMPLETE CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO COORDINATE WITH THE CITY AT 708-798-2616 A MINIMUM OF 48 HOURS IN ADVANCE. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PLACE ADVANCE SIGNS TO ALERT RESIDENTS AND COMMUTERS OF THE CONSTRUCTION WORK. THE PLACEMENT OF THESE SIGNS SHALL TAKE PLACE 48 HOURS IN ADVANCE TO ALLOW SUFFICIENT TIME FOR THE RESIDENTS AND GENERAL PUBLIC TO REVISE THEIR PARKING PATTERNS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE APPROPRIATE PAY ITEM REQUIRING THE PARKING LIMITATIONS.
36. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE "ARMOR-TILE" 24"x48" NOMINAL PANEL WIDTH AS MANUFACTURED BY "ENGINEERED PLASTICS, INC." (800) 682-2525 OF WILLIAMSVILLE, NY OR AN APPROVED EQUAL. THE PANEL SHALL BE A POLYMER COMPOSITE AND COMPLY WITH ADA REQUIREMENTS. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE PANEL COLOR SHALL BE RED. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
37. THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A TIME TO MINIMIZE CONGESTION. REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.
38. THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY THE CONTRACTOR'S OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE MOBILIZATION PAY ITEM.
39. TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED AS REQUIRED. WHERE PARKWAYS ARE DISTURBED, TEMPORARY EROSION CONTROL MEASURES SHALL BE ESTABLISHED WITHIN 14 DAYS OF THE COMPLETION OF DISTURBANCE AND MAINTAINED TO THE SATISFACTION OF THE ENGINEER. THIS WORK, INCLUDING MAINTENANCE, SHALL BE INCLUDED IN THE PERMANENT STABILIZATION PAY ITEMS UNLESS OTHERWISE NOTED.
40. FRAME AND GRATES OR LIDS THAT ARE REPLACED AS PART OF ADJUSTMENT OR RECONSTRUCTION, OR REMOVED AS PART OF STRUCTURE REPLACEMENT, SHALL BE DELIVERED TO THE CITY PUBLIC WORKS FACILITY AT 4200 183RD STREET. THIS WORK SHALL BE INCLUDED IN THE STRUCTURE ADJUSTMENT OR REMOVAL PAY ITEM.
41. CURB AND GUTTER AND DRIVEWAYS PROVIDING ACCESS SHALL BE REMOVED AND REPLACED WITHIN 3 DAYS.
42. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL.
43. NEW OR REPLACEMENT CLOSED LIDS SHALL BE STAMPED TO INDICATE THE STRUCTURE TYPE. STORM LIDS SHALL BE STAMPED WITH "STORM", SANITARY LIDS SHALL BE STAMPED WITH "SANITARY" AND WATER VALVE VAULT LIDS SHALL BE STAMPED WITH "WATER". STAMPING SHALL BE INCLUDED IN THE COST OF THE NEW LID.
44. ALL NEW TYPE 1 OPEN LIDS SHALL BE BICYCLE SAFE.

CONTRACT NO. 04-00027-03-PV
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00011 - EXPIRES 4/30/2011
 11/29/2010 10:05:55 AM



DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-GEN_NOTES.shx

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SCALE: NONE		STA. TO STA.		F.A. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				C-91-004-05		04-00027-03-PV	COOK	36	3
GENERAL NOTES				CONTRACT NO. 63539		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HP0-B97(008)			

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				RESURFACING 0005	SAFETY 0021	TRAINEES 0042
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	450	450	-	-
20800150	TRENCH BACKFILL	CU YD	927	927	-	-
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3,206	3,206	-	-
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	68	68	-	-
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	68	68	-	-
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	68	68	-	-
25200100	SODDING	SQ YD	3,206	3,206	-	-
25200200	SUPPLEMENTAL WATERING	UNIT	34	34	-	-
28000510	INLET FILTERS	EACH	25	25	-	-
35101400	AGGREGATE BASE COURSE, TYPE B	TON	938	938	-	-
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	1,223	1,223	-	-
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	1,618	1,618	-	-
35800100	PREPARATION OF BASE	SQ YD	9,684	9,684	-	-
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	4,015	4,015	-	-
40600300	AGGREGATE (PRIME COAT)	TON	41	41	-	-
40600895	CONSTRUCTING TEST STRIP	EACH	1	1	-	-
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	126	126	-	-
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,701	1,701	-	-
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1,485	1,485	-	-
42001300	PROTECTIVE COAT	SQ YD	1,363	1,363	-	-
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	200	200	-	-
42400800	DETECTABLE WARNINGS	SQ FT	272	272	-	-
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	346	346	-	-
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1,818	1,818	-	-
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3,762	3,762	-	-
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	431	431	-	-
550A0160	STORM SEWERS, CLASS A, TYPE 1 36"	FOOT	545	545	-	-
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	35	35	-	-
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	27	27	-	-
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	18	18	-	-

* INDICATES SPECIALTY ITEM

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 044-00027 - EXPIRES 4/30/2011
 458622 PM 11/23/2010



DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-500.sh1

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SUMMARY OF QUANTITIES

SCALE: NONE

STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	4
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPD-897008				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				RESURFACING 0005	SAFETY 0021	TRAINEES 0042
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	260	260	-	-
55100700	STORM SEWER REMOVAL 15"	FOOT	20	20	-	-
55101300	STORM SEWER REMOVAL 27"	FOOT	45	45	-	-
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	14	14	-	-
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	4	4	-	-
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	4	4	-	-
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5	5	-	-
60221000	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1	-	-
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3	-	-
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2	-	-
60250200	CATCH BASINS TO BE ADJUSTED	EACH	5	5	-	-
60255500	MANHOLES TO BE ADJUSTED	EACH	4	4	-	-
60260100	INLETS TO BE ADJUSTED	EACH	3	3	-	-
60266600	VALVE BOXES TO BE ADJUSTED	EACH	10	10	-	-
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	6	6	-	-
60500070	REMOVING MANHOLES TO MAINTAIN FLOW	EACH	3	3	-	-
60609600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12 (SPECIAL)	FOOT	3,682	3,682	-	-
67100100	MOBILIZATION	L SUM	1	1	-	-
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	-	-
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	-	-
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	316	316	-	-
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	53	53	-	-
72000100	SIGN PANEL - TYPE 1	SQ FT	16	16	-	-
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4	-	-
72900100	METAL POST - TYPE A	FOOT	52	52	-	-
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	84	84	-	-
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	60	60	-	-
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	52	52	-	-
* 80400100	ELECTRIC SERVICE INSTALLATION	EACH	4	-	4	-
* 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1	-	1	-

* INDICATES SPECIALTY ITEM

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DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-500.shd

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SUMMARY OF QUANTITIES

SCALE: _____ STA. _____ TO STA. _____

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	5
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPD-897(008)				

SUMMARY OF QUANTITIES

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION TYPE CODE		
				RESURFACING 0005	SAFETY 0021	TRAINEES 0042
* 81018700	CONDUIT PUSHED, 3" DIA, GALVANIZED STEEL	FOOT	680	-	680	-
* 81028020	CONDUIT, BORED AND PULLED, COILABLE NON-METALLIC CONDUIT, 3/4"	FOOT	1,418	-	1,418	-
* 81400730	HANDHOLE, COMPOSITE CONCRETE	EACH	3	-	3	-
* 81603000	UNIT DUCT, 600V, 2-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE	FOOT	685	-	685	-
* 81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1,283	-	1,283	-
* 82103250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL 250 WATT	EACH	12	-	12	-
* 83006300	LIGHT POLE, ALUMINUM, 30 FT. M.H., 8 FT. MAST ARM	EACH	9	-	9	-
* 83006500	LIGHT POLE, ALUMINUM, 30 FT. M.H., 12 FT. MAST ARM	EACH	3	-	3	-
* 83006550 83006552	LIGHT POLE FOUNDATION, METAL, 11 1/2" Bolt Circle, 8 5/8" x 4"	EACH	12	-	12	-
* 83800505	BREAKAWAY DEVICE, COUPLING, WITH ALUMINUM SKIRT	EACH	12	-	12	-
* 84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	5	-	5	-
Z0003300	BASE COURSE REMOVAL (SPECIAL)	SQ YD	991	991	-	-
Z0019600	DUST CONTROL WATERING	UNIT	87	87	-	-
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	182	182	-	-
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	72	72	-	-
Z0056608	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	FOOT	80	80	-	-
Z0076600	TRAINEES	HOUR	500	-	-	500
X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	100	100	-	-
X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	2,672	2,672	-	-
X4240440	PORTLAND CEMENT CONCRETE SIDEWALK 6 INCH, SPECIAL	SQ FT	405	405	-	-
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	9,684	9,684	-	-
X4404700	SIDEWALK REMOVAL (SPECIAL)	SQ FT	3,077	3,077	-	-
X5538400	STORM SEWERS TO BE CLEANED 30"	FOOT	200	200	-	-
X6022230	MANHOLES, TYPE A, 4'-DIAMETER, WITH SPECIAL FRAME AND GRATE	EACH	1	1	-	-
X6022930	MANHOLES, TYPE A, 5'-DIAMETER, WITH SPECIAL FRAME AND GRATE	EACH	1	1	-	-
X6023204	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID, SPECIAL	EACH	3	3	-	-
X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	5	5	-	-
* XX005049	POWER PEDESTALS	EACH	4	-	4	-

* INDICATES SPECIALTY ITEM

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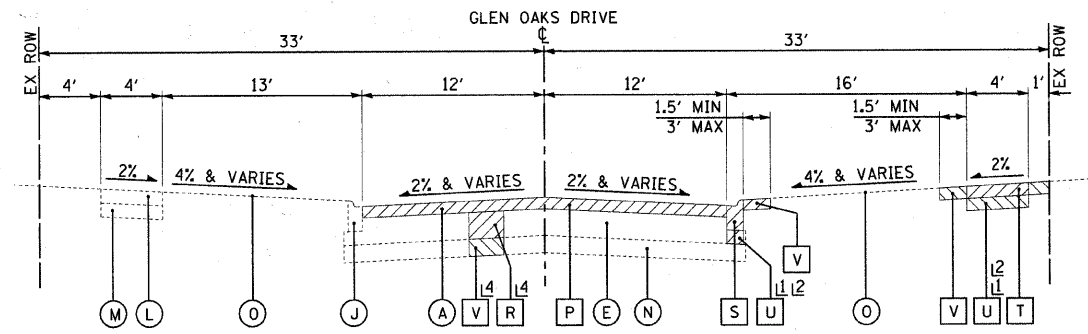
DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
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DATE - 10-01-10	FILE - 041180-500.sht

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SUMMARY OF QUANTITIES

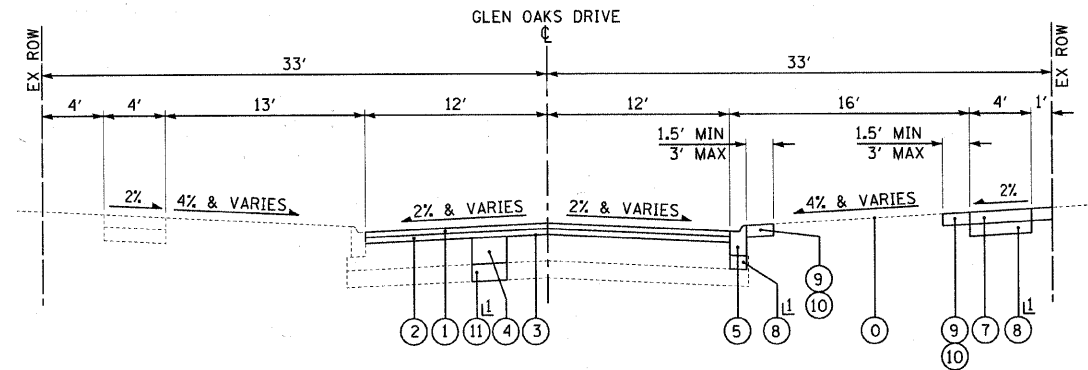
SCALE: _____ STA. _____ TO STA. _____

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	6
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				



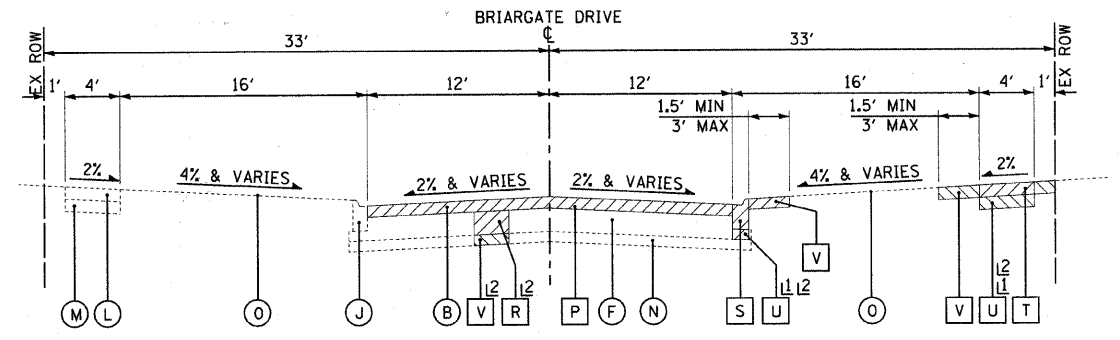
**EXISTING TYPICAL SECTION
GLEN OAKS DRIVE**
STA 9+45 TO STA 30+74

- 11 AGGREGATE BASE COURSE REMOVAL INCLUDED IN SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL
- 12 EXACT LOCATIONS DETERMINED BY THE ENGINEER



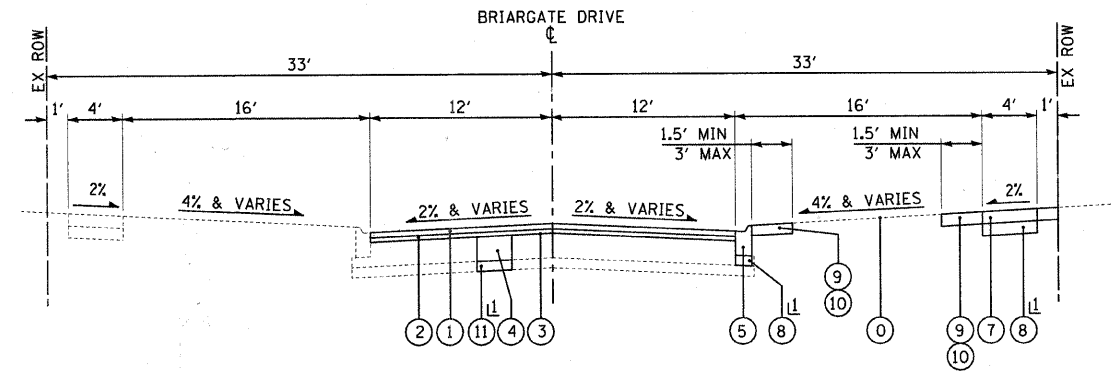
**PROPOSED TYPICAL SECTION
GLEN OAKS DRIVE**
STA 9+45 TO STA 30+74

- 1 AS REQUIRED



**EXISTING TYPICAL SECTION
BRIARGATE DRIVE**
STA 1+00 TO STA 7+33

- 11 AGGREGATE BASE COURSE REMOVAL INCLUDED IN SIDEWALK REMOVAL OR COMBINATION CURB AND GUTTER REMOVAL
- 12 EXACT LOCATIONS DETERMINED BY THE ENGINEER



**PROPOSED TYPICAL SECTION
BRIARGATE DRIVE**
STA 1+00 TO STA 7+33

- 1 AS REQUIRED

EXISTING LEGEND

- (A) HOT-MIX ASPHALT PAVEMENT, 3" - 6"
- (B) HOT-MIX ASPHALT PAVEMENT, 4 1/2"
- (C) HOT-MIX ASPHALT PAVEMENT, 2 1/2"
- (D) HOT-MIX ASPHALT PAVEMENT, 4 1/4"
- (E) AGGREGATE BASE COURSE, 5 1/2" - 8"
- (F) AGGREGATE BASE COURSE, 7" - 7 1/2"
- (G) AGGREGATE BASE COURSE, 7 3/4"
- (H) AGGREGATE BASE COURSE, 5 3/4"
- (J) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12
- (K) NOT USED
- (L) SIDEWALK
- (M) AGGREGATE BASE COURSE
- (N) SUB-GRADE
- (O) GROUND SURFACE
- (P) HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH
- (R) BASE COURSE REMOVAL (SPECIAL)
- (S) COMBINATION CURB AND GUTTER REMOVAL
- (T) SIDEWALK REMOVAL (SPECIAL)
- (U) AGGREGATE BASE COURSE REMOVAL (NOT PAID FOR SEPARATELY)
- (V) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- (W) HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
- 11 ITEM TO BE REMOVED

PROPOSED LEGEND

- 1 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 2"
- 2 HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N50 - 3"
- 3 PREPARATION OF BASE
- 4 AGGREGATE BASE COURSE, TYPE B
- 5 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12 (SPECIAL)
- 6 NOT USED
- 7 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, (SPECIAL)
- 8 AGGREGATE BASE COURSE, TYPE B 4"
- 9 TOPSOIL, FURNISH AND PLACE 4"
- 10 SODDING
- 11 POROUS GRANULAR EMBANKMENT, SUBGRADE **
- 12 HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 - 1 1/2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 1 1/2", 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BINDER COURSE, IL 19mm, N50; 3"	4% @ 50 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5mm); 3"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ. YD./IN
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

** POROUS GRANULAR EMBANKMENT, SUBGRADE (PGES) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

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DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-TypSec.sht

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

**TYPICAL SECTIONS AND
HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	7
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS]		FED. AID PROJECT HPD-897(008)		

SCALE: NONE STA. TO STA.

DRIVEWAYS

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)
GLEN OAKS DRIVE										
11+36.5 LT	HMA	PE	23	19	27	27	-	27	5	-
11+70 LT	HMA	PE	24	20	29	29	-	29	6	-
14+45 LT	HMA	PE	28	24	34	34	-	34	6	-
14+77 LT	HMA	PE	25	21	30	30	-	30	6	-
17+44 LT	PCC	PE	28	24	44	44	44	-	-	44
17+80.5 LT	HMA	PE	22	18	35	35	-	35	7	-
20+52 LT	HMA	PE	24	20	28	28	-	28	5	-
21+16 LT	HMA	PE	27	23	32	32	-	32	6	-
22+32 LT	HMA	PE	26	22	30	30	-	30	6	-
23+43 LT	HMA	PE	30	26	47	47	-	47	9	-
26+03.5 LT	HMA	PE	27	23	39	39	-	39	7	-
26+37 LT	HMA	PE	26	22	38	38	-	38	7	-
29+00.5 LT	HMA	PE	27	23	42	42	-	42	8	-
29+58.5 LT	HMA	PE	19	15	28	28	-	28	5	-
10+09 RT	PCC	PE	32	28	18	18	18	-	-	18
12+16.5 RT	HMA	PE	19	15	27	27	-	27	5	-
12+64.5 RT	HMA	PE	31	27	47	47	-	47	9	-
13+22 RT	HMA	PE	26	22	39	39	-	39	7	-
13+74 RT	HMA	PE	18	14	24	24	-	24	5	-
14+59 RT	HMA	PE	16	12	21	21	-	21	4	-
15+03.5 RT	HMA	PE	17	13	23	23	-	23	4	-
15+54 RT	HMA	PE	16	12	21	21	-	21	4	-
16+12 RT	HMA	PE	14	10	18	18	-	18	4	-
16+29 RT	PCC	PE	16	12	21	21	21	-	-	21
17+24 RT	HMA	PE	16	12	21	21	-	21	4	-
17+71 RT	HMA	PE	14	10	18	18	-	18	4	-
18+15.5 RT	HMA	PE	19	15	26	26	-	26	5	-
18+59 RT	HMA	PE	16	12	21	21	-	21	4	-
19+19.5 RT	HMA	PE	24	20	34	34	-	34	6	-
20+05.5 RT	HMA	PE	19	15	26	26	-	26	5	-
20+49 RT	HMA	PE	14	10	18	18	-	18	4	-
21+02 RT	HMA	PE	34	30	51	51	-	51	9	-
21+95.5 RT	HMA	PE	21	17	31	31	-	31	6	-
22+44.5 RT	HMA	PE	26	22	40	40	-	40	7	-
22+97.5 RT	HMA	PE	23	19	60	60	-	60	11	-
23+29 RT	HMA	PE	15	11	24	24	-	24	5	-
26+09.5 RT	PCC	PE	14	10	18	18	18	-	-	18
26+59.5 RT	HMA	PE	15	11	19	19	-	19	4	-
27+26.5 RT	HMA	PE	29	25	43	43	-	43	8	-
28+25 RT	HMA	PE	24	20	34	34	-	34	6	-
28+85 RT	HMA	PE	14	10	18	18	18	-	-	18
SUB-TOTAL						1,244	119	1,125	213	119

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)
168TH STREET										
25+63 LT	HMA	PE	14	10	18	18	-	18	4	-
26+18 LT	HMA	PE	15	11	19	19	-	19	4	-
26+75 LT	HMA	PE	15	11	19	19	-	19	4	-
SUB-TOTAL						56	0	56	12	0

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)
BRIARGATE DRIVE										
2+47 LT	HMA	PE	28	24	41	41	-	41	8	-
3+30 LT	HMA	PE	24	20	34	34	-	34	6	-
4+28 LT	HMA	PE	25	21	36	36	-	36	7	-
4+90 LT	HMA	PE	15	11	19	19	-	19	4	-
5+86.5 LT	HMA	PE	24	20	34	34	-	34	6	-
2+26.5 RT	HMA	PE	16	14	24	24	-	24	5	-
2+40.5 RT	PCC	PE	17	15	26	26	26	-	-	26
3+24 RT	HMA	PE	15	11	19	19	-	19	4	-
4+30 RT	HMA	PE	14	11	19	19	-	19	4	-
4+43.5 RT	PCC	PE	13	12	21	21	21	-	-	21
5+28 RT	PCC	PE	24	20	34	34	34	-	-	34
5+78 RT	HMA	PE	15	11	19	19	-	19	4	-
SUB-TOTAL						326	81	245	48	81

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)
GLEN OAKS COURT										
2+44 LT	HMA	PE	30	26	44	44	-	44	8	-
2+45 RT	HMA	PE	27	23	39	39	-	39	7	-
2+74	HMA	PE	24	20	34	34	-	34	6	-
SUB-TOTAL						117	0	117	21	0

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)
VIVIAN DRIVE										
2+24 LT	HMA	PE	22	18	31	31	-	31	6	-
2+35.5 RT	HMA	PE	30	26	44	44	-	44	8	-
SUB-TOTAL						75	0	75	14	0

STATION	EXIST DRIVEWAY APRON (TYPE)	PRIV. OR COMM.	WIDTH @ BACK OF CURB (FOOT)	WIDTH @ SIDEWALK (FOOT)	DRIVEWAY PAVEMENT AREA (SQ YD)	DRIVEWAY PAVEMENT REMOVAL (SQ YD)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)	HMA SURF. CRSE, MIX 'C' N50, 3" (TON)	PCC DW. PVT. 6" (SQ YD)	
GLEN OAKS DRIVE											
							1,244	119	1,125	213	119
168TH STREET											
							56	0	56	12	0
BRIARGATE DRIVE											
							326	81	245	48	81
GLEN OAKS COURT											
							117	0	117	21	0
VIVIAN DRIVE											
							75	0	75	14	0

TOTALS							1,818	200	1,618	308	200
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 LICENSE NO. 00021 - EXPIRES 12/31/2011
 12/29/2010



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DATE - 10-01-10	FILE - 041180-SOM.shx

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SCHEDULE OF QUANTITIES		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		-	04-00027-03-PV	COOK	36	9
STA. TO STA.		C-91-004-05		CONTRACT NO. 63539		
		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPD-897(O08)				

CURB & GUTTER

BEGIN STATION	END STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS DRIVE				
9+60 LT	11+85 LT	240	240	43
12+70 LT	1+33 LT	45	45	8
1+33 RT	13+62 LT	70	70	13
14+29 LT	15+40 LT	111	111	20
15+78 LT	1+33 LT	45	45	8
1+22 RT	16+46 LT	45	45	8
17+15 LT	17+96 LT	81	81	15
18+78 LT	1+28 LT	45	45	8
1+30 RT	19+49 LT	45	45	8
20+36 LT	23+65 LT	320	320	57
24+32 LT	0+65 RT	45	45	8
0+65 LT	25+01 LT	45	45	8
25+88 LT	26+60 LT	72	72	13
27+38 LT	25+35 RT	45	45	8
25+36 LT	28+06 LT	45	45	8
28+87 LT	29+16 LT	29	29	6
29+44 LT	29+73 LT	29	29	6
30+27 LT	25+74 RT	63	63	12
9+45 RT	11+76 RT	245	245	44
12+05 RT	12+31 RT	26	26	5
12+47 RT	12+87 RT	40	40	8
13+07 RT	13+97 RT	90	90	16
14+49 RT	14+76 RT	27	27	5
14+92 RT	15+17 RT	25	25	5
15+45 RT	15+63 RT	18	18	4
16+05 RT	16+58 RT	53	53	10
17+16 RT	17+34 RT	18	18	4
17+56 RT	17+80 RT	24	24	5
18+04 RT	18+26 RT	22	22	4
18+49 RT	18+76 RT	27	27	5
19+09 RT	19+37 RT	28	28	5
19+93 RT	20+18 RT	25	25	5
20+37 RT	24+32 RT	415	415	73
24+32 RT	1+35 RT	50	50	9
24+32 RT	25+01 RT	80	0	0
1+34 LT	25+25 RT	73	73	13
25+98 RT	26+20 RT	22	22	4
26+48 RT	26+69 RT	21	21	4
27+08 RT	27+50 RT	42	42	8
27+82 RT	27+92 RT	10	10	2
28+12 RT	28+42 RT	30	30	6
28+75 RT	28+98 RT	23	23	5
30+27 RT	26+58 RT	60	60	11
SUB-TOTAL		2,914	2,834	517

BEGIN STATION	END STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
168TH STREET				
25+55 LT	25+75 LT	20	20	4
26+10 LT	26+26 LT	16	16	3
26+66 LT	26+80 LT	20	20	4
SUB-TOTAL		56	56	11

BEGIN STATION	END STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
BRIARGATE DRIVE				
1+33 LT	1+60 LT	35	35	7
2+29 LT	2+63 LT	34	34	6
3+17 LT	3+42 LT	25	25	5
4+14 LT	4+48 LT	34	34	6
4+80 LT	5+07 LT	27	27	5
5+67 LT	6+08 LT	41	41	8
6+85 LT	17+80 RT	55	55	10
1+22 RT	1+32 RT	10	10	2
2+15 RT	2+61 RT	46	46	9
2+98 RT	3+39 RT	41	41	8
3+75 RT	4+53 RT	78	78	14
5+15 RT	5+41 RT	26	26	5
5+67 RT	5+92 RT	25	25	5
6+85 RT	18+51 RT	55	55	10
SUB-TOTAL		532	532	100

BEGIN STATION	END STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS COURT				
1+28 LT	1+33 LT	5	5	1
2+20 LT	2+10 RT	190	190	34
1+30 RT	1+35 RT	5	5	1
SUB-TOTAL		200	200	36

BEGIN STATION	END STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
VIVIAN DRIVE				
2+12 LT	2+36 LT	25	25	5
2+20 RT	2+53 RT	35	35	7
SUB-TOTAL		60	60	12

STATION	COMB CURB GUTTER REMOVAL (FOOT)	COMB CONC CURB GUTTER TYPE M-6.12 (SPECIAL) (FOOT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS DRIVE	2,914	2,834	517
168TH STREET	56	56	11
BRIARGATE DRIVE	532	532	100
GLEN OAKS COURT	200	200	36
VIVIAN DRIVE	60	60	12
TOTALS	3,762	3,682	676

SIDEWALK

STATION	SIDEWALK REMOVAL (SPECIAL) (SQ FT)	PCC SIDEWALK 5 INCH, SPECIAL (SQ FT)	PCC SIDEWALK 6 INCH, SPECIAL (SQ FT)	DETECTABLE WARNINGS (SQ FT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS DRIVE					
9+90 LT	80	80	-	8	9
10+30 LT	64	64	-	8	8
12+76 LT	80	80	-	8	9
12+85 LT	64	64	-	8	8
13+25 LT	80	80	-	8	9
13+35 LT	64	64	-	8	8
17+50 LT	405	-	405	-	45
24+36 LT	80	80	-	8	9
24+42 LT	80	80	-	8	9
24+88 LT	80	80	-	8	9
24+95 LT	80	80	-	8	9
27+42 LT	80	80	-	8	9
27+50 LT	80	80	-	8	9
27+95 LT	80	80	-	8	9
30+32 LT	80	80	-	8	9
30+45 LT	80	80	-	8	9
27+42 RT	80	80	-	8	9
30+32 RT	80	80	-	8	9
30+42 RT	80	80	-	8	9
SUB-TOTAL	1,797	1,392	405	144	204

STATION	SIDEWALK REMOVAL (SPECIAL) (SQ FT)	PCC SIDEWALK 5 INCH, SPECIAL (SQ FT)	PCC SIDEWALK 6 INCH, SPECIAL (SQ FT)	DETECTABLE WARNINGS (SQ FT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
BRIARGATE DRIVE					
1+20 LT	80	80	-	8	9
1+28 LT	80	80	-	8	8
6+90 LT	80	80	-	8	9
7+00 LT	80	80	-	8	9
1+20 RT	80	80	-	8	9
1+28 RT	80	80	-	8	9
6+90 RT	80	80	-	8	9
7+00 RT	80	80	-	8	9
SUB-TOTAL	640	640	0	64	71

STATION	SIDEWALK REMOVAL (SPECIAL) (SQ FT)	PCC SIDEWALK 5 INCH, SPECIAL (SQ FT)	PCC SIDEWALK 6 INCH, SPECIAL (SQ FT)	DETECTABLE WARNINGS (SQ FT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS COURT					
1+20 LT	80	80	-	8	9
1+28 LT	80	80	-	8	9
1+20 RT	80	80	-	8	9
1+28 RT	80	80	-	8	9
SUB-TOTAL	320	320	0	32	36

STATION	SIDEWALK REMOVAL (SPECIAL) (SQ FT)	PCC SIDEWALK 5 INCH, SPECIAL (SQ FT)	PCC SIDEWALK 6 INCH, SPECIAL (SQ FT)	DETECTABLE WARNINGS (SQ FT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
VIVIAN DRIVE					
1+20 LT	80	80	-	8	9
1+28 LT	80	80	-	8	9
1+20 RT	80	80	-	8	9
1+28 RT	80	80	-	8	9
SUB-TOTAL	320	320	0	32	36

STATION	SIDEWALK REMOVAL (SPECIAL) (SQ FT)	PCC SIDEWALK 5 INCH, SPECIAL (SQ FT)	PCC SIDEWALK 6 INCH, SPECIAL (SQ FT)	DETECTABLE WARNINGS (SQ FT)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)
GLEN OAKS DRIVE	1,797	1,392	405	144	204
BRIARGATE DRIVE	640	640	0	64	71
GLEN OAKS COURT	320	320	0	32	36
VIVIAN DRIVE	320	320	0	32	36
TOTALS	3,077	2,672	405	272	347

AGGREGATE BASE COURSE

	AGGREGATE BASE COURSE, TYPE B (TON)	AGGREGATE BASE COURSE, TYPE B 4" (SQ YD)	AGGREGATE BASE COURSE, TYPE B 8" (SQ YD)
FROM PAVEMENT SCHEDULE			
PREP BASE	564	-	-
BASE REPAIR	374	-	-
FROM SIDEWALK SCHEDULE			
	-	347	-
FROM CURB AND GUTTER SCHEDULE			
	-	676	-
FROM DRIVEWAY SCHEDULE			
	-	200	1,618
TOTALS	938	1,223	1,618

DRAWING NO. 200 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00021 - EXPIRES 4/30/2011
 503 E. 11th St., Springfield, IL 62761-3000
 TEL: 217-223-1100 FAX: 217-223-1101



DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-SOM.shd

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SCALE: STA. TO STA.

SCHEDULE OF QUANTITIES

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	10
C-91-004-05	CONTRACT NO. 63539			
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				

HOT-MIX ASPHALT SURFACE

	HMA SURFACE COURSE MIX "C" N50 (TON)
FROM PAVEMENT SCHEDULE	1,177
FROM DRIVEWAY SCHEDULE	308
TOTALS	1,485

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS

BEGIN STATION	END STATION	REMOVAL DISPOSAL UNSUITABLE MATERIALS (CU YD)
FOR PARKWAY RESTORATION		
GLEN OAKS DRIVE		
9+45	30+74	230
SUB-TOTAL		230
BRIARGATE DRIVE		
1+00	7+33	101
SUB-TOTAL		101
GLEN OAKS COURT		
1+00	2+74	16
SUB-TOTAL		16
VIVIAN DRIVE		
1+00	2+53	31
SUB-TOTAL		31
FROM PAVEMENT SCHEDULE		72
TOTALS		450

PAVEMENT

BEGIN STATION	END STATION	PAVEMENT AREA (SQ YD)	HMA SURFACE REMOVAL, VARIABLE DEPTH (SQ YD)	HMA SURFACE REMOVAL, 1 1/2" (SQ YD)	HMA SURFACE REMOVAL, BUTT JOINT (SQ YD)	PREPARATION OF BASE (SQ YD)	AGGREGATE BASE COURSE, TYPE B (TON)	BASE REPAIRS		REMOVAL DISPOSAL UNSUITABLE MATERIALS (CU YD)	POROUS GRANULAR EMBANKMENT, SUBGRADE (CU YD)	HMA BINDER COURSE IL-19.0 N50 (TON)	HMA SURFACE COURSE MIX "C" N50 (TON)
								BASE COURSE REMOVAL (SPECIAL) (SQ YD)	AGGREGATE BASE COURSE, TYPE B (TON)				
GLEN OAKS DRIVE													
9+45	13+07	1,351	1,351	-	28	1,351	78	137	52	10	10	237	160
13+07	16+12	825	825	-	0	825	48	85	32	6	6	145	98
16+12	19+14	820	820	-	0	820	48	82	31	6	6	144	97
19+14	24+66.5	1,717	1,717	-	14	1,717	100	175	66	12	12	302	203
24+66.5	27+73	980	980	-	14	980	57	101	38	7	7	172	116
27+73	30+74	1,217	1,217	-	28	1,217	71	125	47	9	9	214	144
SUB-TOTAL		6,910	6,910	0	84	6,910	402	705	266	50	50	1,214	818
BRIARGATE DRIVE													
1+00	7+33	2,000	2,000	-	28	2,000	116	201	76	14	14	351	236
SUB-TOTAL		2,000	2,000	0	28	2,000	116	201	76	14	14	351	236
GLEN OAKS COURT													
1+00	2+74	660	660	-		660	39	67	25	6	6	116	78
SUB-TOTAL		660	660	0	0	660	39	67	25	6	6	116	78
VIVIAN DRIVE													
1+00	2+53	460	114	346	14	114	7	18	7	2	2	20	45
SUB-TOTAL		460	114	346	14	114	7	18	7	2	2	20	45
TOTALS		10,030	9,684	346	126	9,684	564	991	374	72	72	1,701	1,177

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00027 - EXPIRES 4/30/2011
 WOODMAN Consulting Engineers



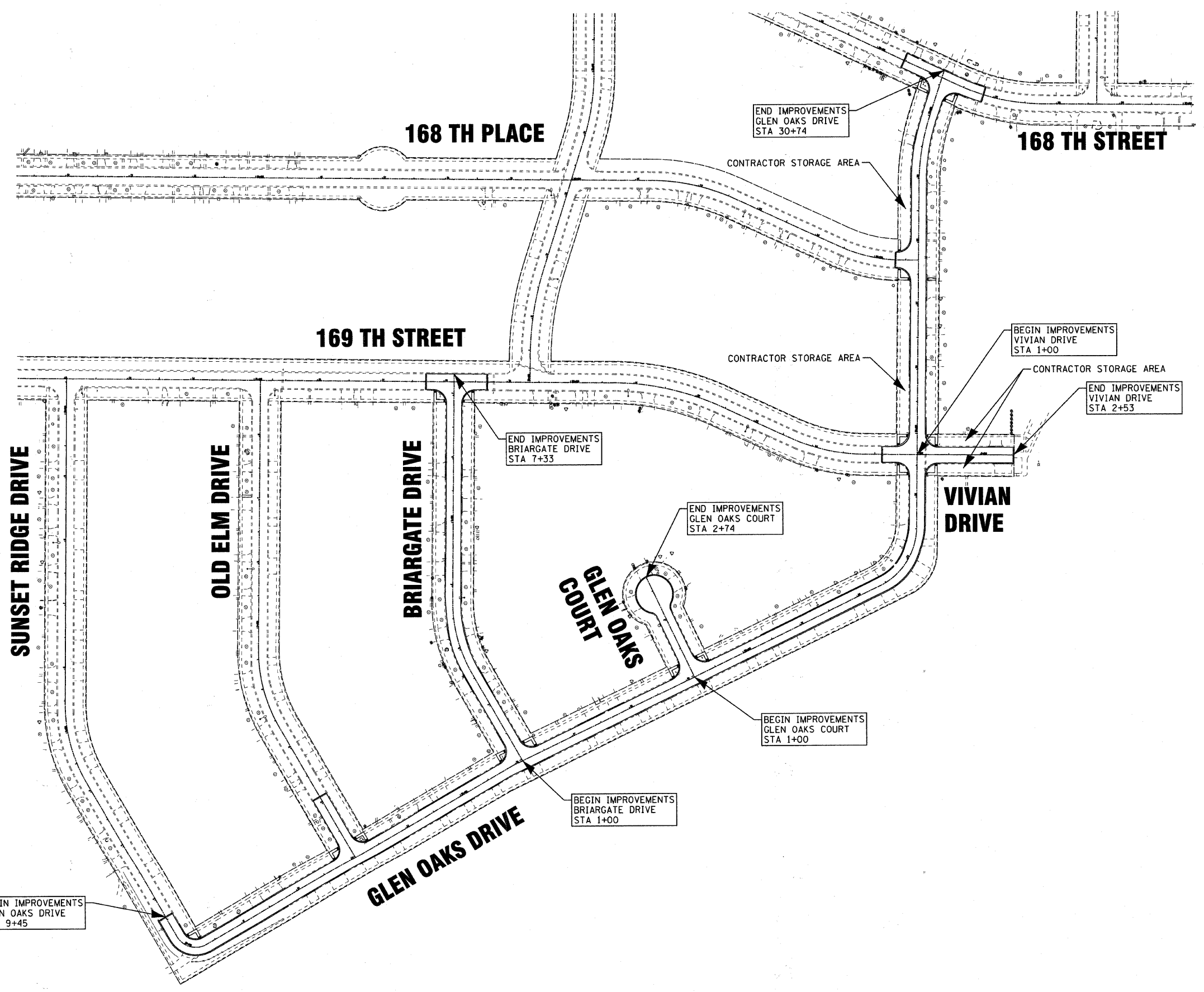
DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-SOM.shp

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

SCHEDULE OF QUANTITIES

SCALE: STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	11
C-91-004-05			CONTRACT NO. 63539	
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				



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DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-LAYOUT.sht

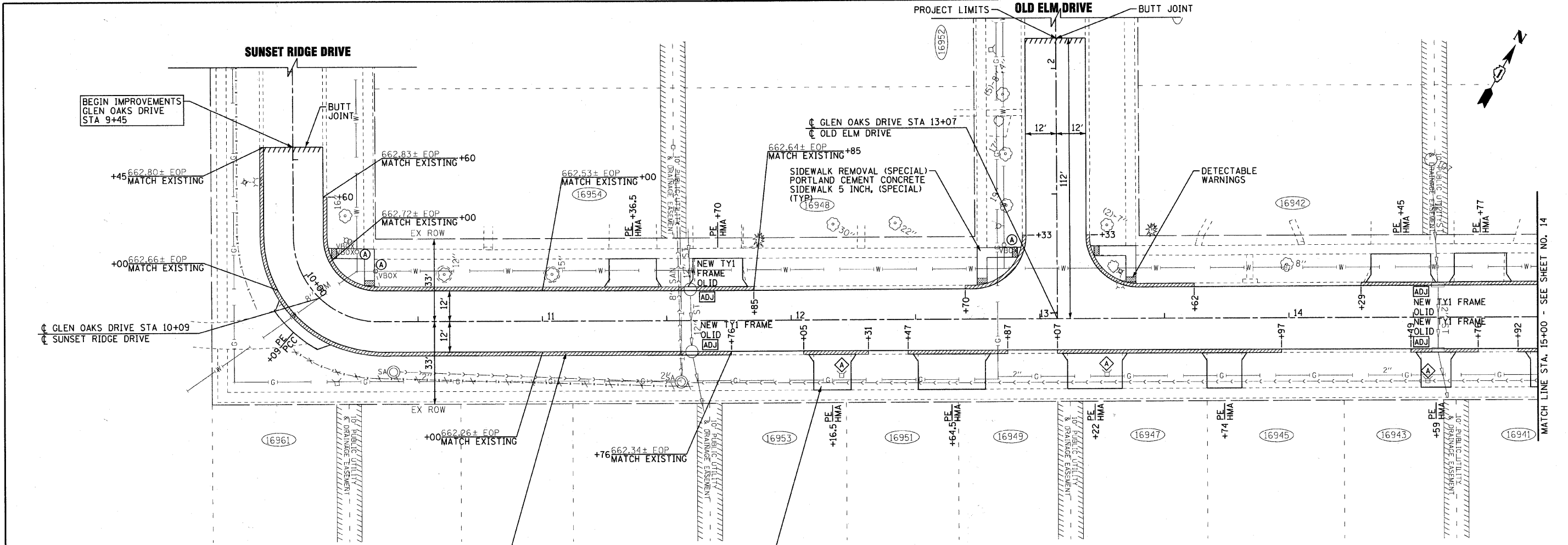
CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

OVERALL LAYOUT

SCALE: 1" = 80'

STA. TO STA.

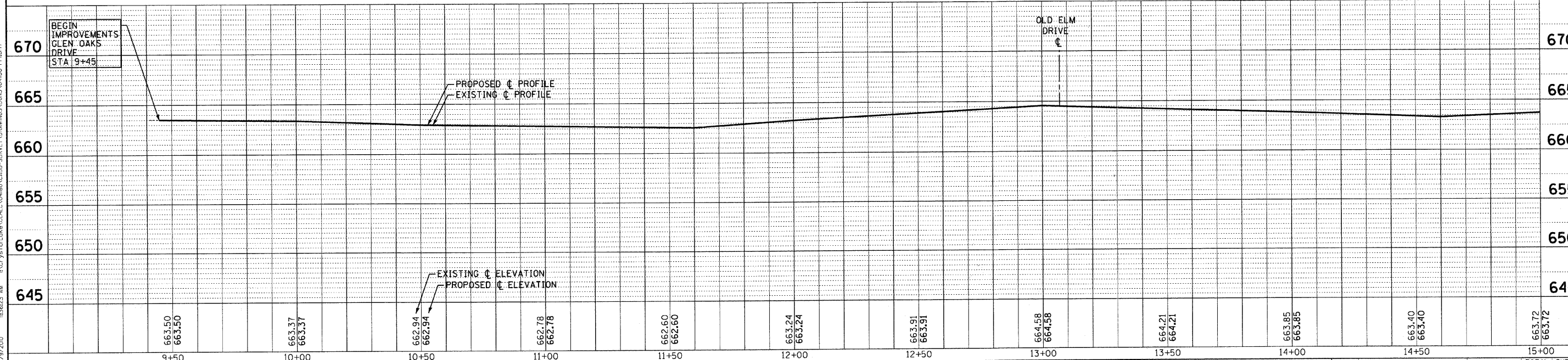
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	12
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				



GLEN OAKS DRIVE

COMBINATION CURB AND GUTTER REMOVAL
COMBINATION CONCRETE CURB AND GUTTER,
TYPE M-6.12 (SPECIAL)
(TYP)

DRIVEWAY PAVEMENT REMOVAL
AGGREGATE BASE COURSE,
TYPE B 8"
HOT-MIX ASPHALT SURFACE
COURSE, MIX "C", N50 - 3"
(TYP)

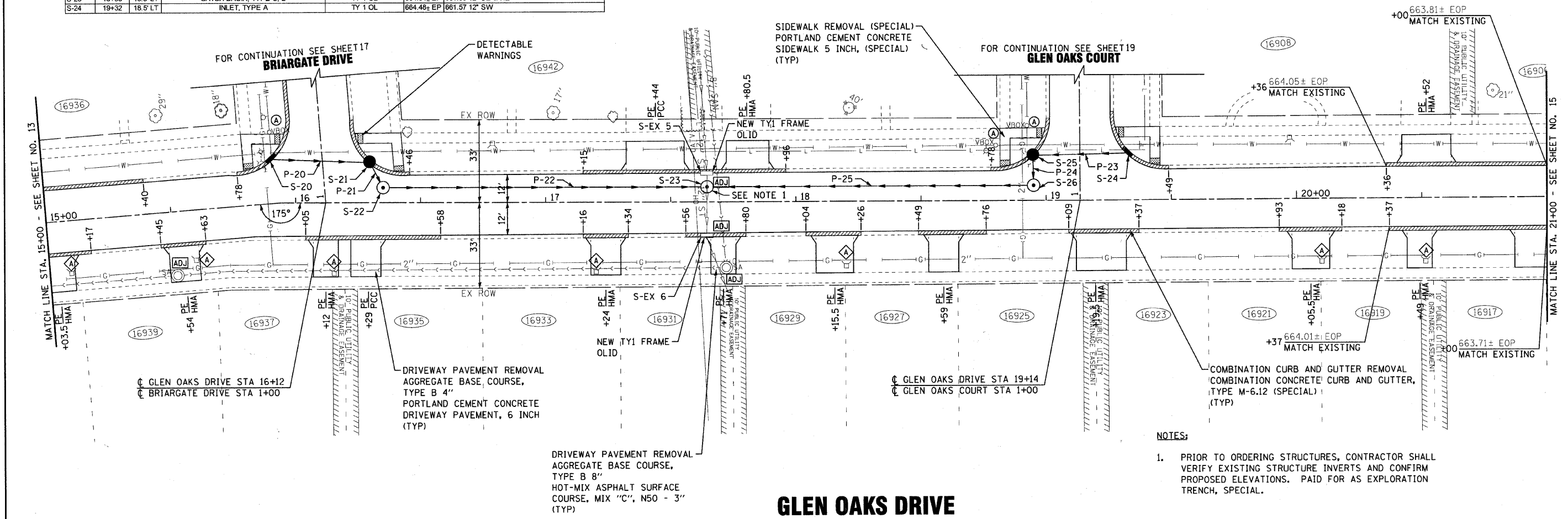


	DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW	CITY OF COUNTRY CLUB HILLS, ILLINOIS GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS	PLAN AND PROFILE GLEN OAKS DRIVE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	DRAWN - KAR	REVISED -		SCALE: H: 1"=20' V: 1"=5'		C-91-004-05	04-00027-03-PV	COOK	36	13	
	CHECKED - RWL	REVISED -		STA. 9+40 TO STA. 15+00		FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT HPD-B97(008)		CONTRACT NO. 63539	
	DATE - 10-01-10	FILE - 041180-PP1.sh+									

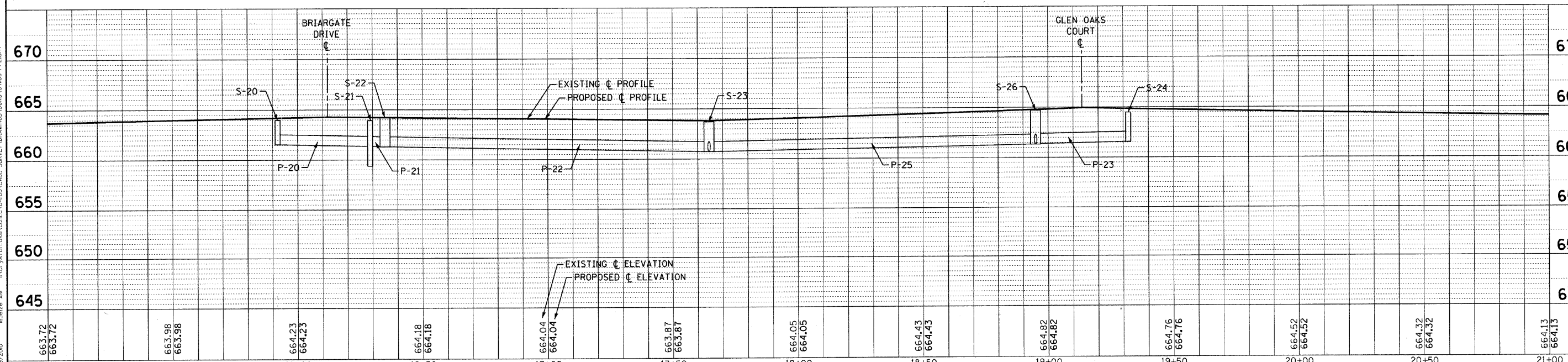
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 PROJECT: COUNTRY CLUB HILLS, ILLINOIS
 DRAWING: GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS
 SHEET NO.: 36 OF 36
 DATE: 10-01-10

NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
S-20	15+92	17.5' LT	INLET, TYPE A	TY 1 OL	663.98 EP	661.55 12" NE
S-21	16+29	18' LT	CATCH BASIN, TYPE C, 2'	TY 1 OL	663.93 EP	661.37 12" SW & SE
S-22	16+36	6' LT	MANHOLE, TYPE A, 4'	TY 1 CL	664.21 RIM	661.32 12" NW & NE
S-23	17+64.5	6' LT	MANHOLE, TYPE A, 4' (BUILD OVER EXISTING)	TY 1 CL	663.70 RIM	EX 660.75± 12" NW & SE, 660.75 12" NE & SW
S-EX 5	17+64.5	12' LT	EXISTING INLET	EXISTING OL	663.22 RIM	EX 660.75± 12" NW & SE
S-EX 6	17+64.5	12' RT	EXISTING INLET	EXISTING OL	663.16 RIM	EX 660.81± 12" NW, EX 660.51± 12" SE
S-26	18+95	6' LT	MANHOLE, TYPE A, 4'	TY 1 CL	664.70 RIM	661.33 12" NW & SW
S-25	18+95	18.5' LT	CATCH BASIN, TYPE C, 2'	TY 1 OL	664.34± EP	661.39 12" SE & NE
S-24	19+32	18.5' LT	INLET, TYPE A	TY 1 OL	664.48± EP	661.57 12" SW

STORM PIPES						
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)	
P-20	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	40	12	0.44%	10	
P-21	STORM SEWERS, CLASS A, TYPE 1 12"	12	12	0.44%	3	
P-22	STORM SEWERS, CLASS A, TYPE 1 12"	130	12	0.44%	34	
P-23	STORM SEWER (WATER MAIN REQUIREMENTS) 12 INCH	40	12	0.44%	10	
P-24	STORM SEWERS, CLASS A, TYPE 1 12"	14	12	0.44%	4	
P-25	STORM SEWERS, CLASS A, TYPE 1 12"	131	12	0.44%	40	



- NOTES:**
- PRIOR TO ORDERING STRUCTURES, CONTRACTOR SHALL VERIFY EXISTING STRUCTURE INVERTS AND CONFIRM PROPOSED ELEVATIONS. PAID FOR AS EXPLORATION TRENCH, SPECIAL.



COMPILED BY: BAXTER & WOODMAN, INC.
 PROJECT: C-91-004-05
 DRAWING: 04-0027-03-PV
 DATE: 12/9/2010
 LICENSE NO.: 184-0019 - EXPIRES 4/30/2011
 ILLINOIS PROFESSIONAL ENGINEER LICENSE NO. 125676 AM



DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW
DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-PP2.shx

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

**PLAN AND PROFILE
GLEN OAKS DRIVE**

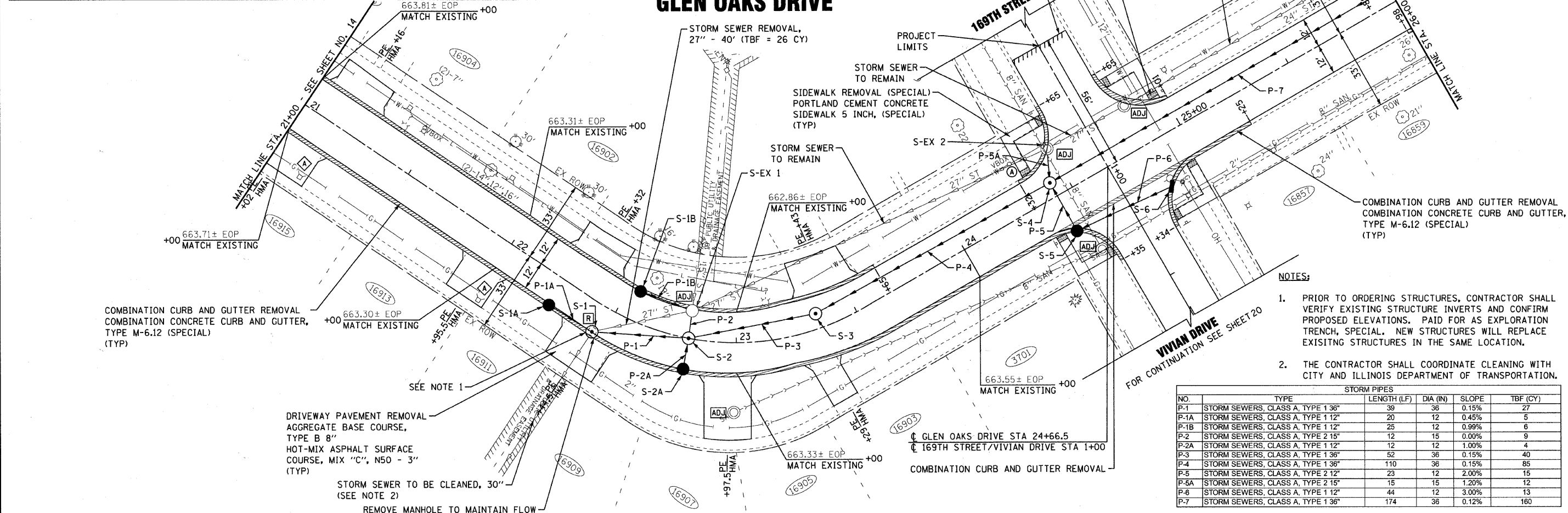
SCALE: H: 1"=20' V: 1"=5'

STA. 15+00 TO STA. 21+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-0027-03-PV	COOK	36	14
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPD-897(00B)		

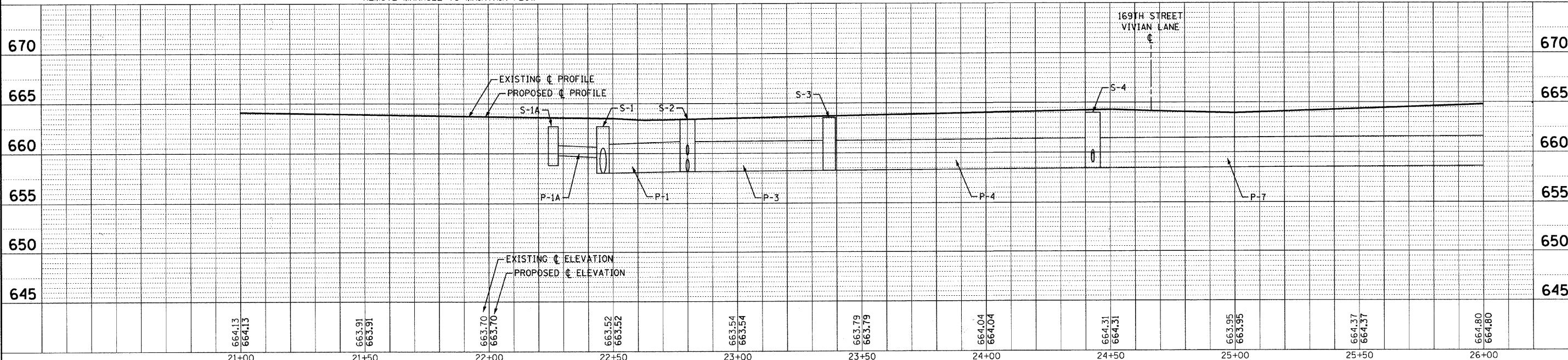
NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
S-1	22+46	12' RT	MANHOLE, TYPE A, 5'	TY 1 OL (SPECIAL 4")	662.72 EP	658.12 36" NE, 659.71 12" W, EX 658.12± 30" S
S-1A	22+26	12' RT	CATCH BASIN, TYPE A, 4'	TY 1 OL	662.72 EP	659.80 12" E
S-1B	22+55	12' LT	CATCH BASIN, TYPE A, 4'	TY 1 OL	662.76 EP	659.86 12" E
S-2	22+80	0' RT	MANHOLE, TYPE A, 6'	TY 1 CL	663.43 RIM	658.18 36" NE & SW, 658.18 15" N, 659.86 12" SE
S-2A	22+80	12' RT	CATCH BASIN, TYPE A, 4'	TY 1 OL	663.15 EP	659.98 12" NW
S-EX1	22+81	12' RT	EXISTING CATCH BASIN	EXISTING OL	662.78 RIM	658.18 15" SE, EX 658.18± 15" NW, EX 658.23± 27" NE & SW
S-3	23+37	6' LT	MANHOLE, TYPE A, 5'	TY 1 CL	663.58 RIM	658.26 36" NE & SW
S-4	24+43	6' LT	MANHOLE, TYPE A, 6'	TY 1 CL	664.12 RIM	658.43 36" NE & SW, 659.00 15" NW, 659.00 12" SE
S-5	24+43	16' RT	CATCH BASIN, TYPE A, 4'	TY 1 OL	663.50 EP	659.46 12" NW & NE
S-6	24+84	16' RT	INLET, TYPE A	TY 1 OL	663.60 EP	660.78 12" SW
S-EX2	24+49	18.5' LT	EXISTING MANHOLE	EXISTING OL	663.36 RIM	659.18 15" SE, EX 659.18± 27" NE, EX 659.39± 27" SW

GLEN OAKS DRIVE



- NOTES:**
1. PRIOR TO ORDERING STRUCTURES, CONTRACTOR SHALL VERIFY EXISTING STRUCTURE INVERTS AND CONFIRM PROPOSED ELEVATIONS. PAID FOR AS EXPLORATION TRENCH, SPECIAL. NEW STRUCTURES WILL REPLACE EXISTING STRUCTURES IN THE SAME LOCATION.
 2. THE CONTRACTOR SHALL COORDINATE CLEANING WITH CITY AND ILLINOIS DEPARTMENT OF TRANSPORTATION.

STORM PIPES					
NO.	TYPE	LENGTH (LF)	DIA (IN)	SLOPE	TBF (CY)
P-1	STORM SEWERS, CLASS A, TYPE 1 3/8"	39	36	0.15%	27
P-1A	STORM SEWERS, CLASS A, TYPE 1 1/2"	20	12	0.45%	5
P-1B	STORM SEWERS, CLASS A, TYPE 1 1/2"	25	12	0.99%	6
P-2	STORM SEWERS, CLASS A, TYPE 2 1/5"	12	15	0.00%	9
P-2A	STORM SEWERS, CLASS A, TYPE 1 1/2"	12	12	1.00%	4
P-3	STORM SEWERS, CLASS A, TYPE 1 3/8"	52	36	0.15%	40
P-4	STORM SEWERS, CLASS A, TYPE 1 3/8"	110	36	0.15%	85
P-5	STORM SEWERS, CLASS A, TYPE 2 1/2"	23	12	2.00%	15
P-5A	STORM SEWERS, CLASS A, TYPE 2 1/2"	15	15	1.20%	12
P-6	STORM SEWERS, CLASS A, TYPE 1 1/2"	44	12	3.00%	13
P-7	STORM SEWERS, CLASS A, TYPE 1 3/8"	174	36	0.12%	160

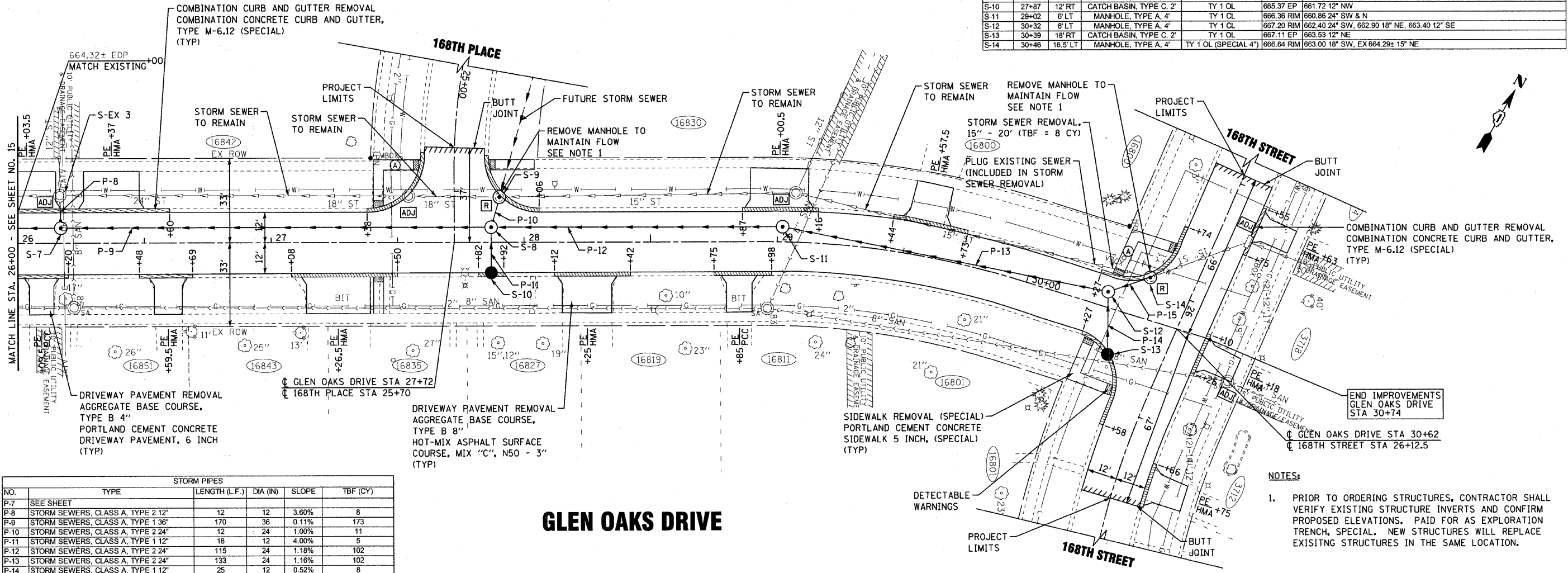


	DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW	CITY OF COUNTRY CLUB HILLS, ILLINOIS GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS	PLAN AND PROFILE GLEN OAKS DRIVE	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - KAR	REVISED -			-	04-00027-03-PV	COOK	36	15
	CHECKED - RWL	REVISED -			C-91-004-05	CONTRACT NO. 63539			
	DATE - 10-01-10	FILE - 041180-PP3.sht			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				

SCALE: H: 1"=20' V: 1"=5' STA. 21+00 TO STA. 26+00

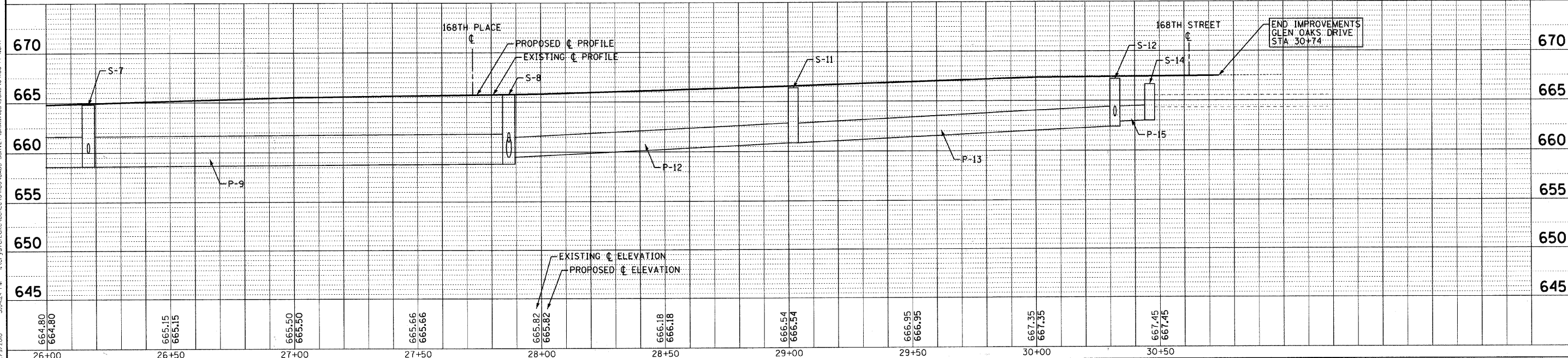
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 LICENSE NO. 184-0021 - EXPIRES 4/30/2011
 1100 S. WASHINGTON ST., SUITE 200, CHICAGO, IL 60605

NO.	STATION	OFFSET	TYPE	FRAME	RIM / EP	INVERT
S-7	26+17	6' LT	MANHOLE, TYPE A, 5'	TY 1 CL	664.80 RIM	658.63 36" NE & SW, 660.00 12" NW
S-EX3	26+17	18.5' LT	EXISTING MANHOLE	EXISTING CL	664.94 RIM	660.44 12" SE, EX 660.44± 24" SW, EX 660.54± 24" NE, EX 660.59± 12" NW
S-8	27+87	6' LT	MANHOLE, TYPE A, 5'	TY 1 CL	665.63 RIM	658.82 36" SW, 659.50 24" N & NE, 661.00 12" SE
S-9	27+90	18.5' LT	MANHOLE, TYPE A, 5'	TY 1 OL	665.03 RIM	659.62 24" S, EX 661.83± 15" NE, EX 661.73± 18" SW, FUTURE 659.62± 24" N
S-10	27+87	12' RT	CATCH BASIN, TYPE C, 2'	TY 1 OL	665.37 EP	661.72 12" NW
S-11	29+02	6' LT	MANHOLE, TYPE A, 4'	TY 1 CL	666.36 RIM	660.86 24" SW & N
S-12	30+32	6' LT	MANHOLE, TYPE A, 4'	TY 1 CL	667.20 RIM	662.40 24" SW, 662.90 18" NE, 663.40 12" SE
S-13	30+39	10' RT	CATCH BASIN, TYPE C, 2'	TY 1 OL	667.11 EP	663.53 12" NE
S-14	30+46	16.5' LT	MANHOLE, TYPE A, 4'	TY 1 OL (SPECIAL 4')	666.64 RIM	663.00 18" SW, EX 664.29± 15" NE



STORM PIPES					
NO.	TYPE	LENGTH (L.F.)	DIA (IN)	SLOPE	TBF (CY)
P-7	SEE SHEET				
P-8	STORM SEWERS, CLASS A, TYPE 1 12"	12	12	3.60%	8
P-9	STORM SEWERS, CLASS A, TYPE 1 36"	170	36	0.11%	173
P-10	STORM SEWERS, CLASS A, TYPE 2 24"	12	24	1.00%	11
P-11	STORM SEWERS, CLASS A, TYPE 1 12"	18	12	4.00%	5
P-12	STORM SEWERS, CLASS A, TYPE 2 24"	115	24	1.18%	102
P-13	STORM SEWERS, CLASS A, TYPE 2 24"	133	24	1.16%	102
P-14	STORM SEWERS, CLASS A, TYPE 1 12"	25	12	0.52%	8
P-15	STORM SEWERS, CLASS A, TYPE 1 18"	18	18	0.56%	7

GLEN OAKS DRIVE



- NOTES:**
- PRIOR TO ORDERING STRUCTURES, CONTRACTOR SHALL VERIFY EXISTING STRUCTURE INVERTS AND CONFIRM PROPOSED ELEVATIONS. PAID FOR AS EXPLORATION TRENCH, SPECIAL. NEW STRUCTURES WILL REPLACE EXISTING STRUCTURES IN THE SAME LOCATION.

CONTRACT NO. 04-00027-03-PV
 COUNTY COOK
 TOTAL SHEETS 36
 SHEET NO. 16
 C-91-004-05
 CONTRACT NO. 63539
 FEDERAL AID PROJECT HPD-097(009)

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DRAWN - KAR	REVISED -
CHECKED - RWL	REVISED -
DATE - 10-01-10	FILE - 041180-PP4.sh+

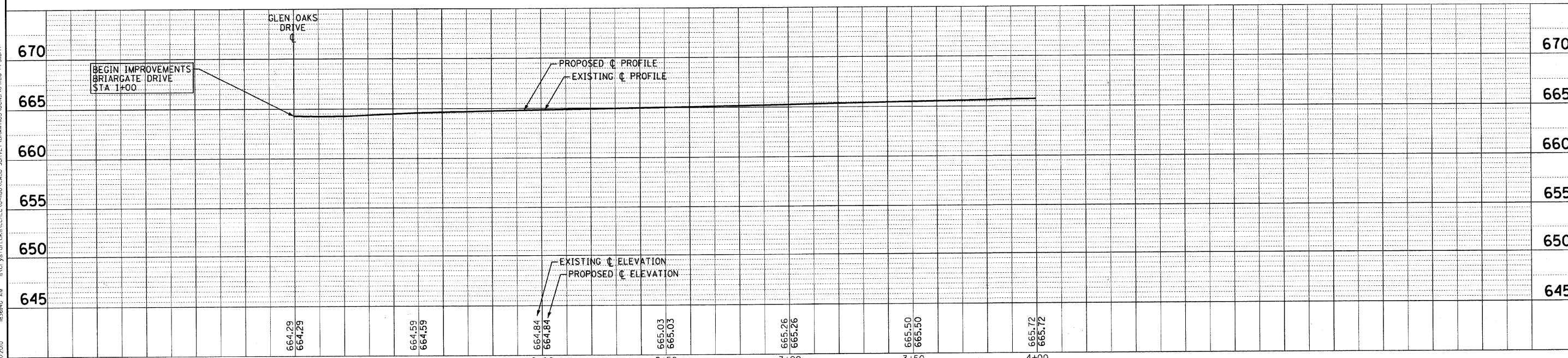
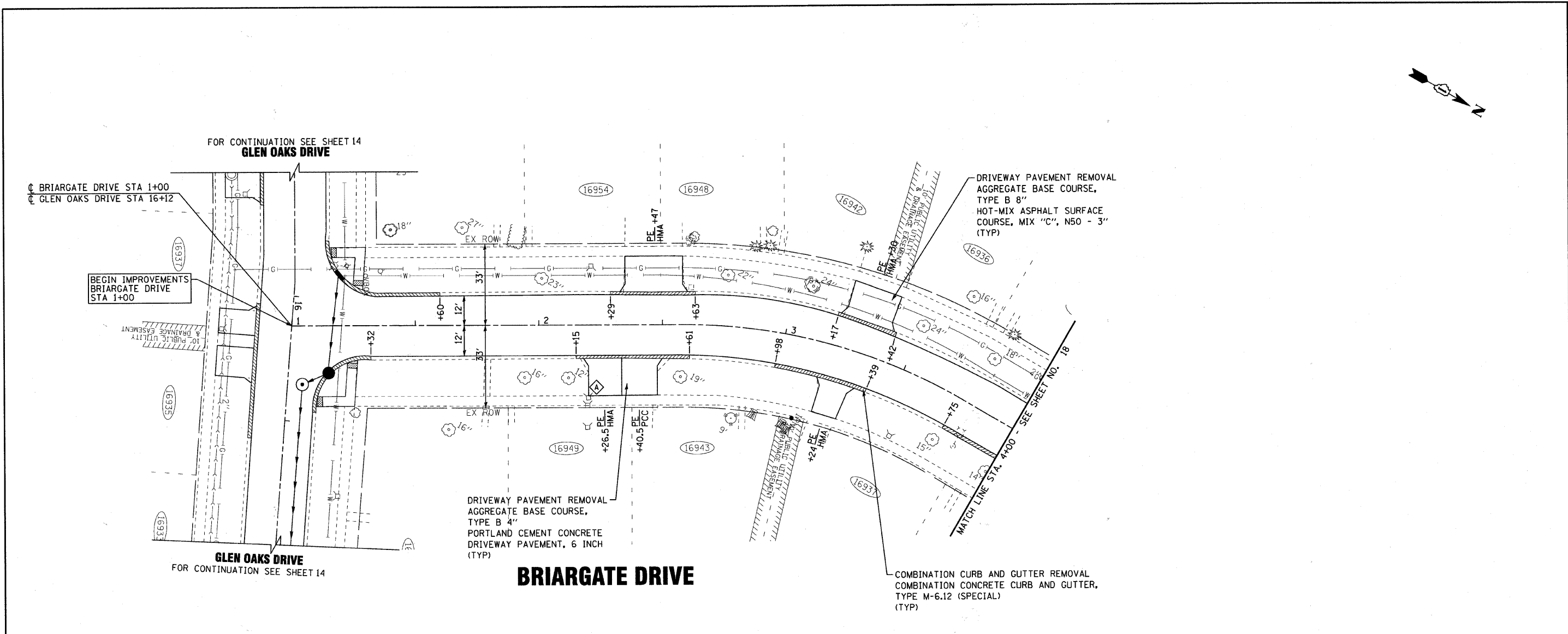
**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

**PLAN AND PROFILE
GLEN OAKS DRIVE**

SCALE: H: 1"=20' V: 1"=5'

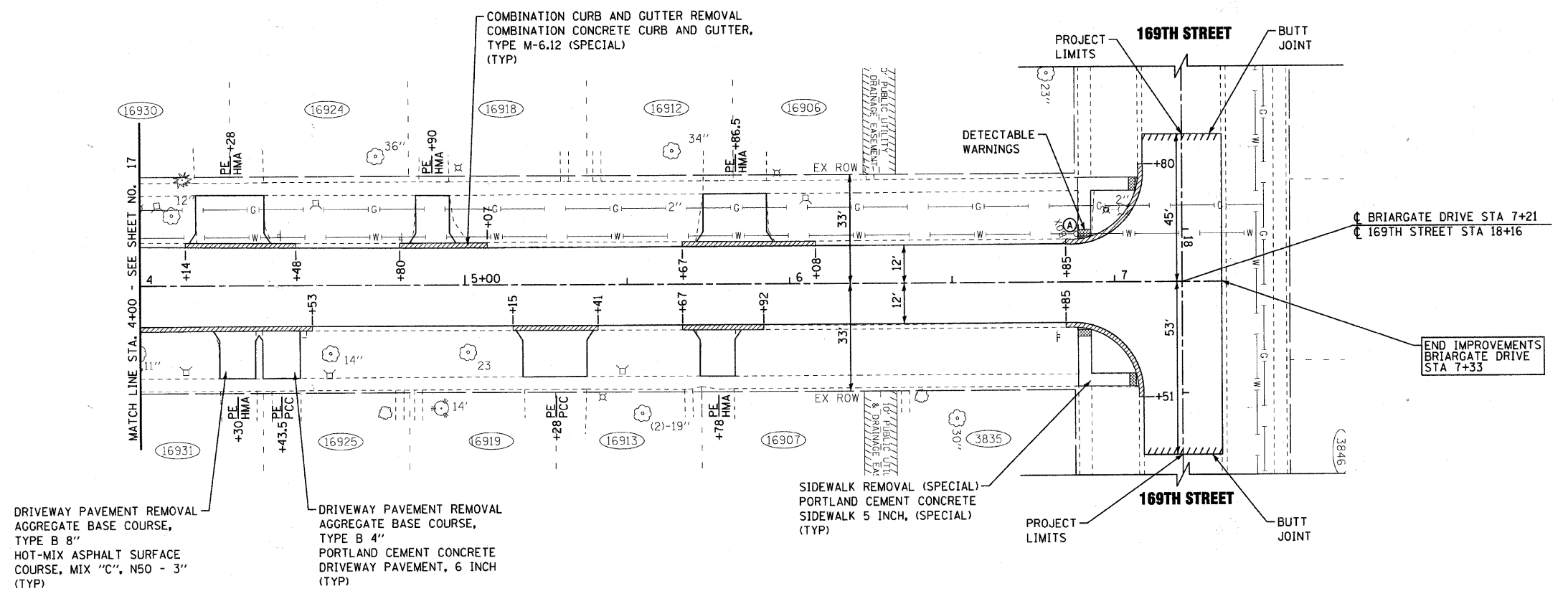
STA. 26+00 TO STA. 30+62

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	16
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. (ILLINOIS) FED. AID PROJECT HPD-097(009)				

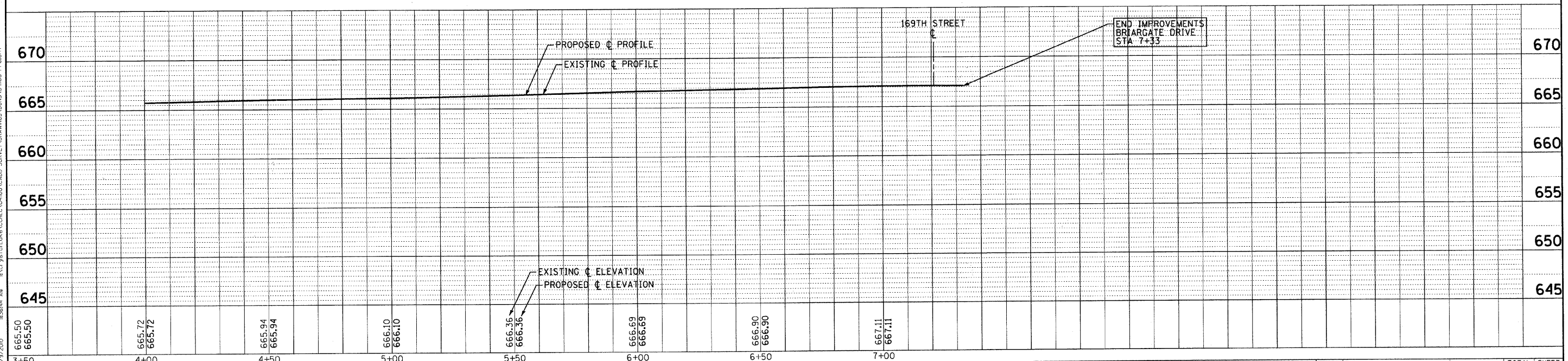


	DESIGNED - MWP	REVISED - 12-01-10 IDOT REVIEW	<p align="center">CITY OF COUNTRY CLUB HILLS, ILLINOIS</p> <p align="center">GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS</p>	<p align="center">PLAN AND PROFILE</p> <p align="center">BRIARGATE DRIVE</p>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - KAR	REVISED -			-	04-00027-03-PV	COOK	36	17
	CHECKED - RWL	REVISED -			C-91-004-05	CONTRACT NO. 63539			
	DATE - 10-01-10	FILE - 041180-PP5.shd			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT HPD-897(008)			

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BRIARGATE DRIVE



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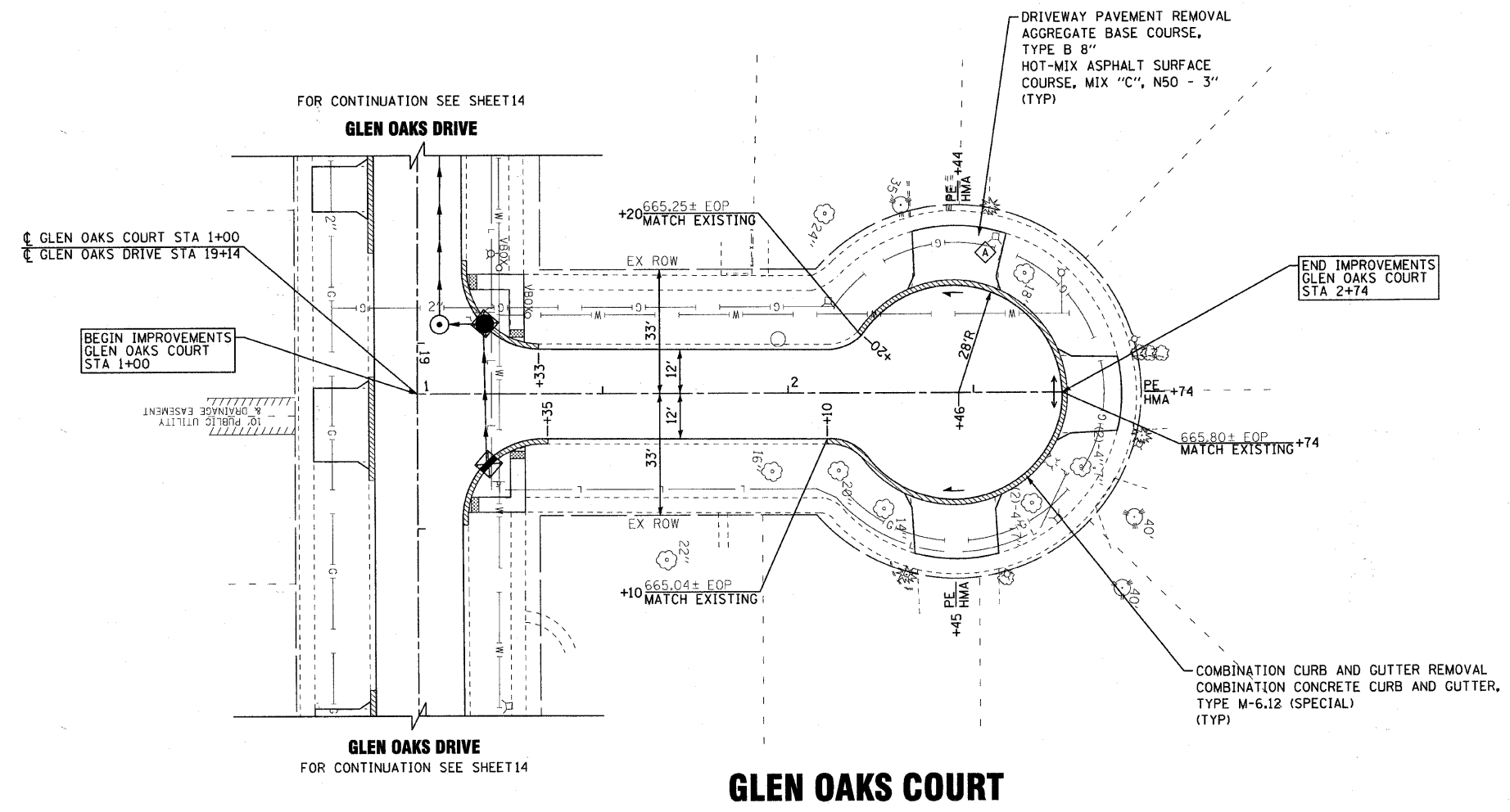
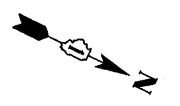
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CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

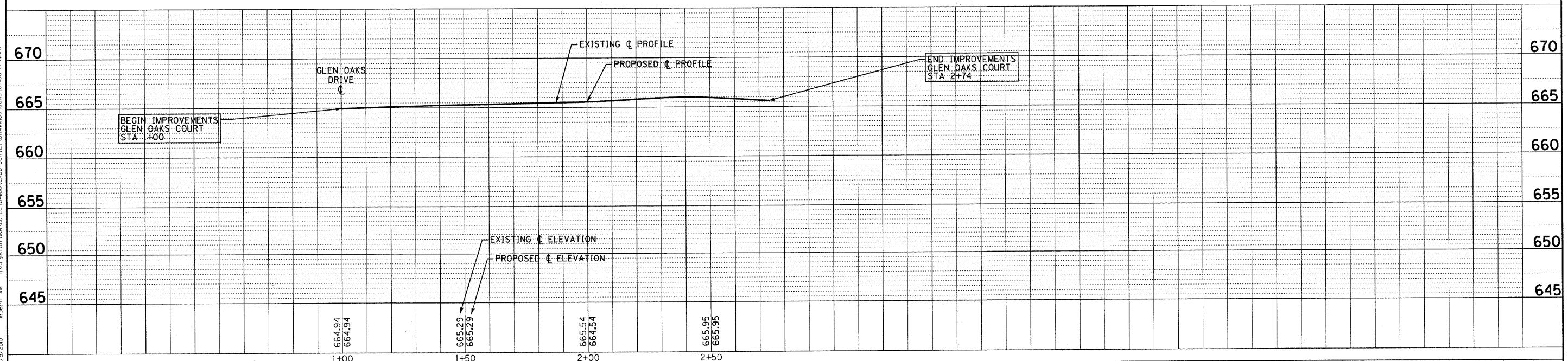
PLAN AND PROFILE
BRIARGATE DRIVE

SCALE: H: 1"=20' V: 1"=5'
 STA. 4+00 TO STA. 7+21

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	18
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPD-097(008)		



GLEN OAKS COURT



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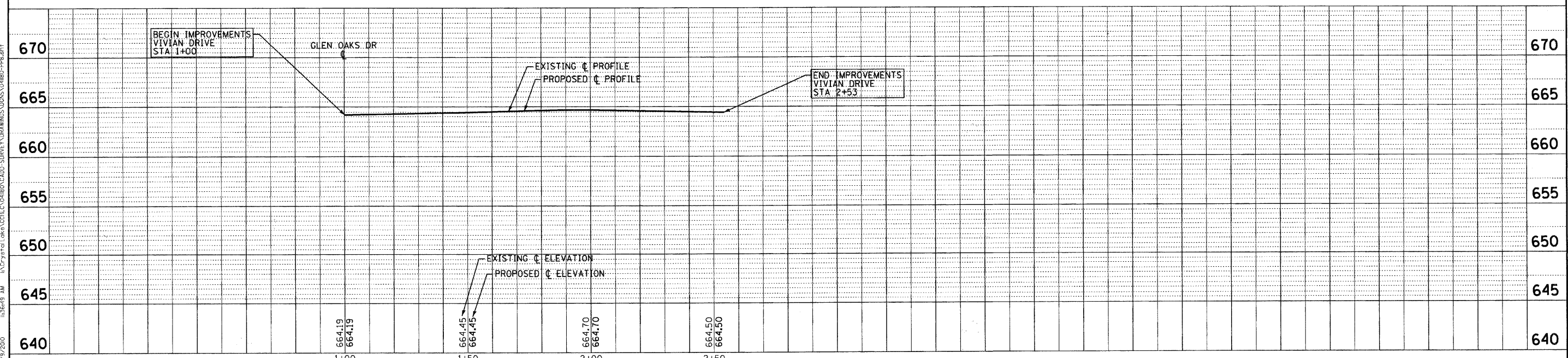
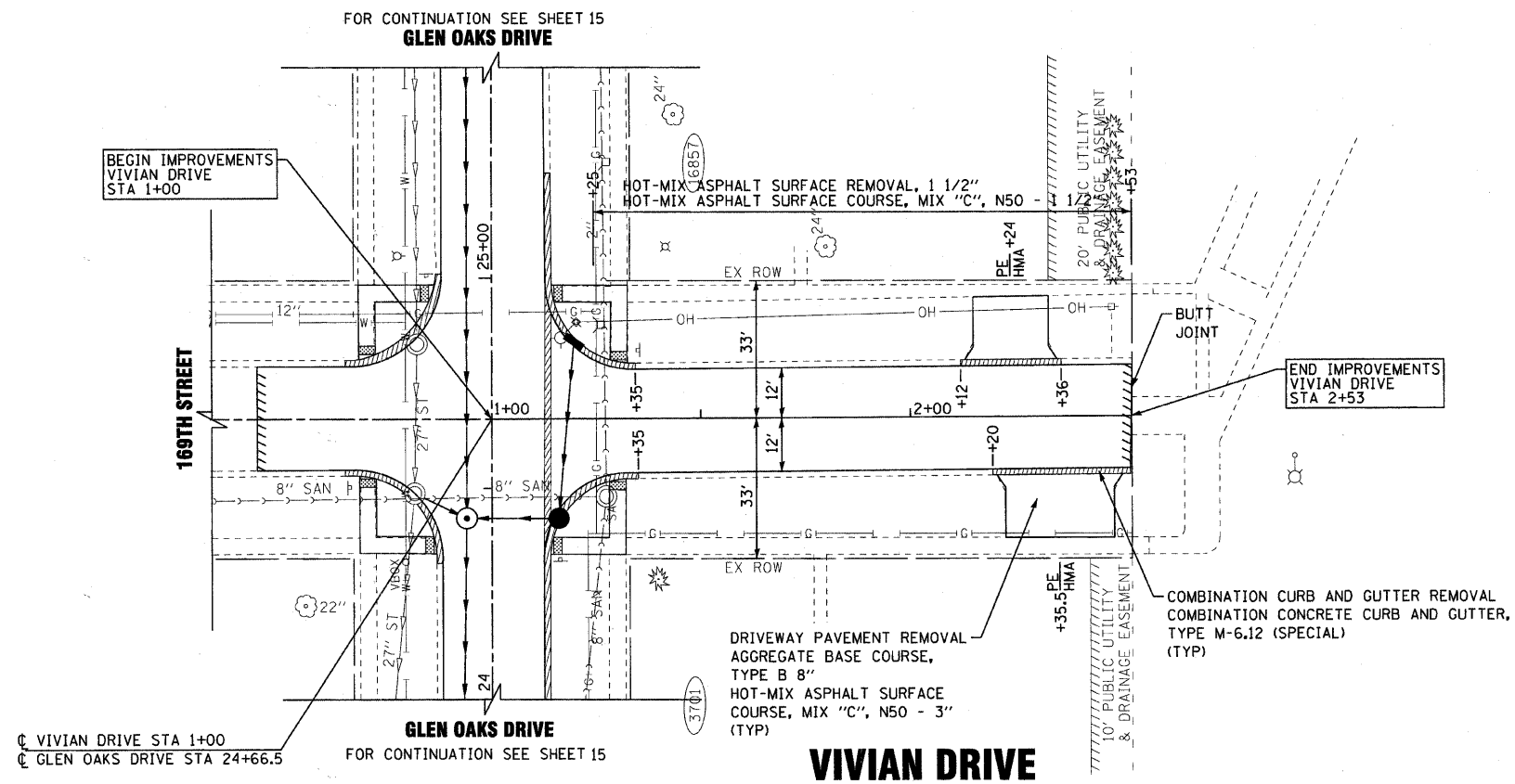


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CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

PLAN AND PROFILE
GLEN OAKS COURT
 SCALE: H: 1"=20' V: 1"=5'
 STA. 1+00 TO STA. 3+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	19
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				



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CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

PLAN AND PROFILE VIVIAN DRIVE		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: H: 1"=20' V: 1"=5'		-	04-00027-03-PV	COOK	36	20
STA. 1+00 TO STA. 3+00		C-91-004-05		CONTRACT NO. 63539		
		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPD-897008				

SOIL EROSION AND SEDIMENT CONTROL NOTES

1. SEDIMENTATION AND EROSION CONTROL STANDARDS

THE FOLLOWING STANDARDS MUST BE SATISFIED:

- A. ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF A DEVELOPMENT SITE SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
- B. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO OR CONCURRENTLY WITH THE START OF DISTURBANCE.
- C. PERMANENT STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 14 DAYS AFTER THE CONSTRUCTION IN THE AREA TEMPORARILY OR PERMANENTLY CEASES.

2. SEDIMENTATION AND EROSION CONTROL METHODS

THE FOLLOWING SEDIMENTATION AND EROSION CONTROL METHODS MUST BE INSTALLED AND MAINTAINED:

- A. ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER PIPING SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
- B. DISCHARGES FROM DEWATERING OPERATIONS SHALL ENTER OR BE ROUTED TO A SEDIMENT AND EROSION CONTROL SYSTEM OR DEVICE.
- C. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.

3. MAINTENANCE

ALL TEMPORARY MEASURES AND PERMANENT EROSION AND SEDIMENT CONTROL MUST BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION AS IDENTIFIED BY REQUIRED INSPECTIONS. THIS INCLUDES, BUT IS NOT LIMITED TO, THE FOLLOWING:

- A. REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A SINGULAR OR CUMULATIVE RAINFALL EVENT(S) OF 0.5 INCH OR MORE OVER A TWENTY-FOUR-HOUR PERIOD.
- B. MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
- C. ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE ENGINEER.

4. INSPECTIONS

- A. THESE PLANS SHALL BE MAINTAINED AT THE SITE DURING CONSTRUCTION OPERATIONS. THE ENGINEER SHALL MAKE INSPECTIONS AND MAINTAIN ON -SITE RECORDS OF SUCH INSPECTIONS AT THE INTERVALS SPECIFIED BELOW:
 - I. UPON COMPLETION OF INSTALLATION OF SEDIMENT AND RUNOFF CONTROL MEASURES, PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING;
 - II. AFTER CURB AND GUTTER, SIDEWALK, STREET LIGHTING AND STORM SEWER INSTALLATION;
 - III. AFTER FINAL GRADING; AND
 - IV. WEEKLY AND AFTER EACH RAINFALL EVENT OF 0.5 INCH OR MORE OVER A TWENTY-FOUR-HOUR PERIOD.
- B. ANY NECESSARY REPAIRS TO SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MADE AND REPORTED IN THE ON-SITE INSPECTION RECORDS.

5. SOIL EROSION AND SEDIMENT CONTROL

- A. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL PERMANENT RESTORATION IS PERFORMED.
- B. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DISPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DCONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.

C. TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED AS REQUIRED. WHERE PARKWAYS ARE DISTURBED, TEMPORARY EROSION CONTROL MEASURES SHALL BE ESTABLISHED WITHIN 14 DAYS OF THE COMPLETION OF DISTURBANCE AND MAINTAINED TO THE SATISFACTION OF THE CITY. THIS WORK, INCLUDING MAINTENANCE, SHALL BE INCLUDED IN THE PERMANENT STABILIZATION PAY ITEMS UNLESS OTHERWISE NOTED.

SCHEDULE OF EROSION CONTROL QUANTITIES

INLET FILTERS (INCLUDES MAINTENANCE)	EACH	25
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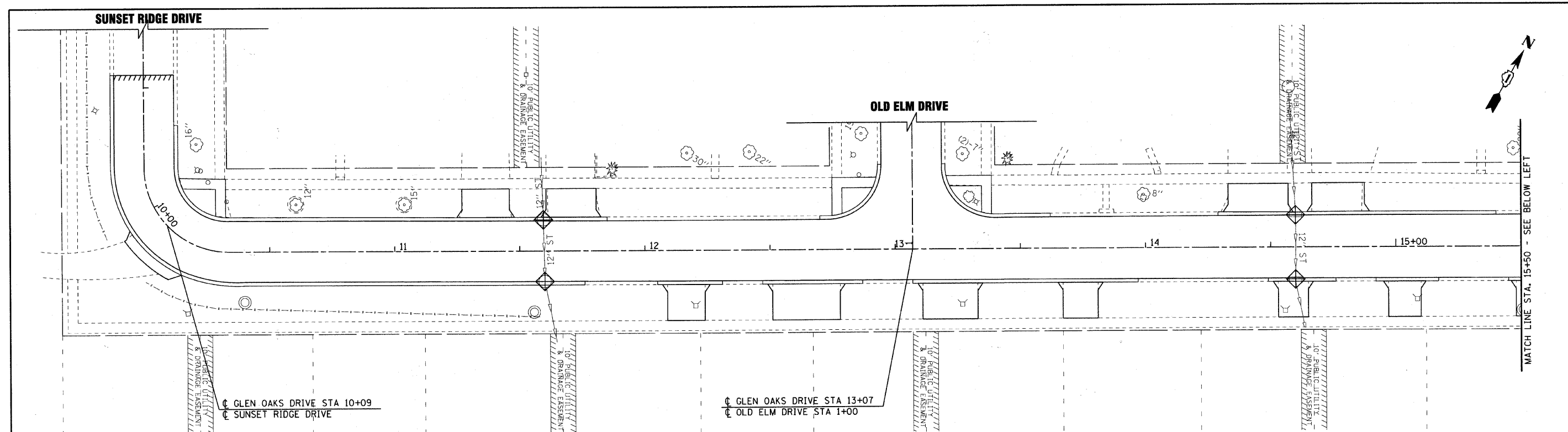
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**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

EROSION AND SEDIMENT CONTROL NOTES

SCALE: NONE STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	21
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897008				



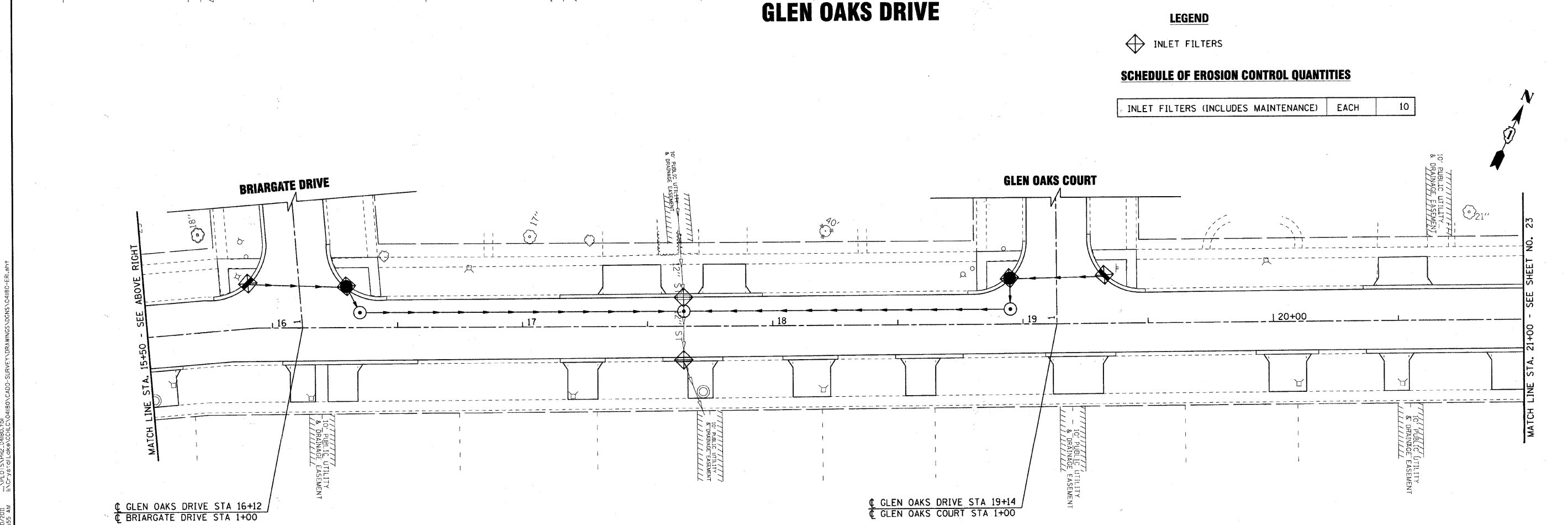
GLEN OAKS DRIVE

LEGEND

◆ INLET FILTERS

SCHEDULE OF EROSION CONTROL QUANTITIES

INLET FILTERS (INCLUDES MAINTENANCE)	EACH	10
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GLEN OAKS DRIVE

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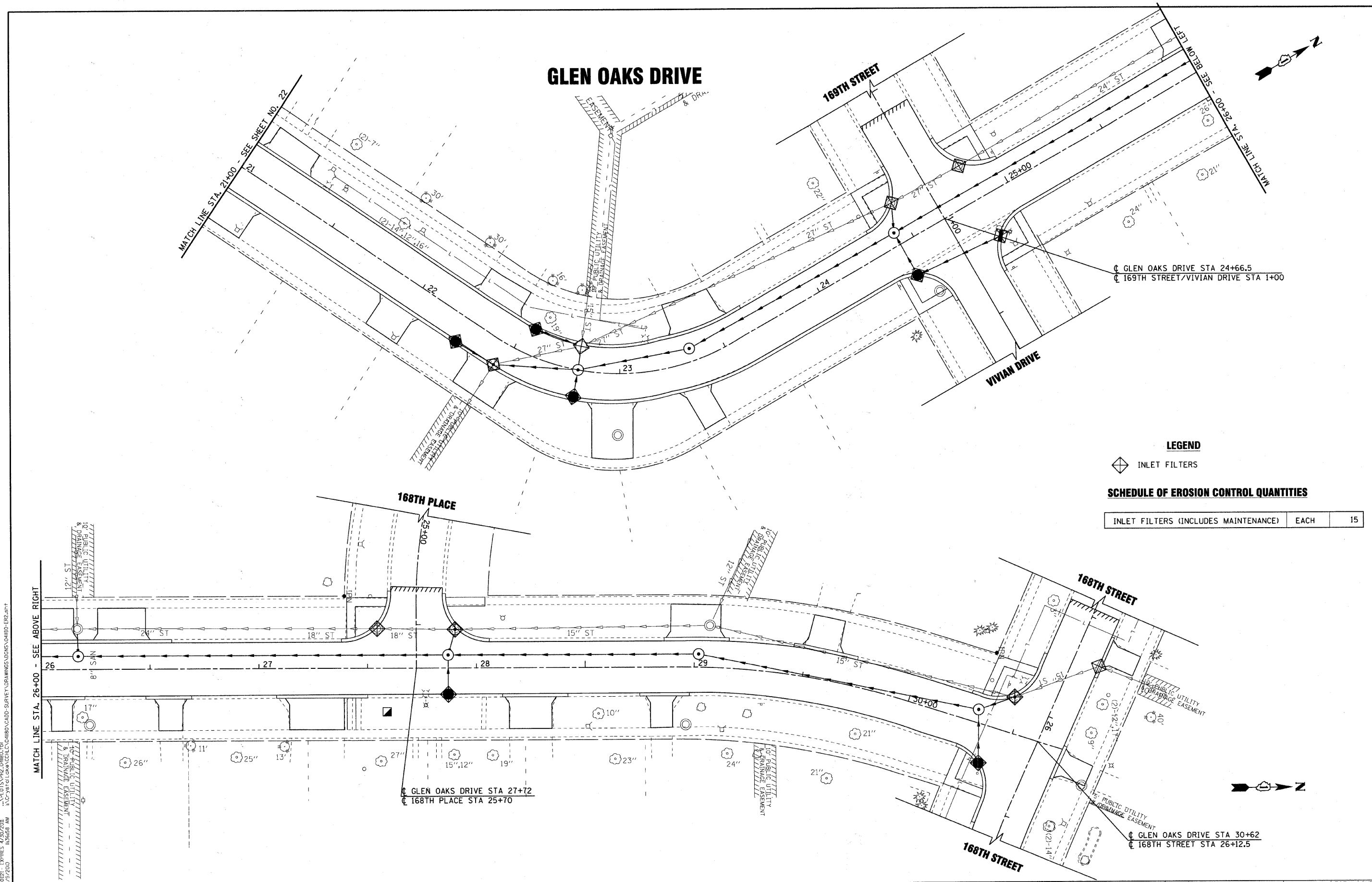
**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

**EROSION & SEDIMENT CONTROL PLAN
GLEN OAKS DRIVE**

SCALE: 1" = 20'
STA. 9+40 TO STA. 21+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	22
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				

GLEN OAKS DRIVE



GLEN OAKS DRIVE STA 24+66.5
 169TH STREET/VIVIAN DRIVE STA 1+00

GLEN OAKS DRIVE STA 27+72
 168TH PLACE STA 25+70

GLEN OAKS DRIVE STA 30+62
 168TH STREET STA 26+12.5

LEGEND

INLET FILTERS

SCHEDULE OF EROSION CONTROL QUANTITIES

INLET FILTERS (INCLUDES MAINTENANCE)	EACH	15
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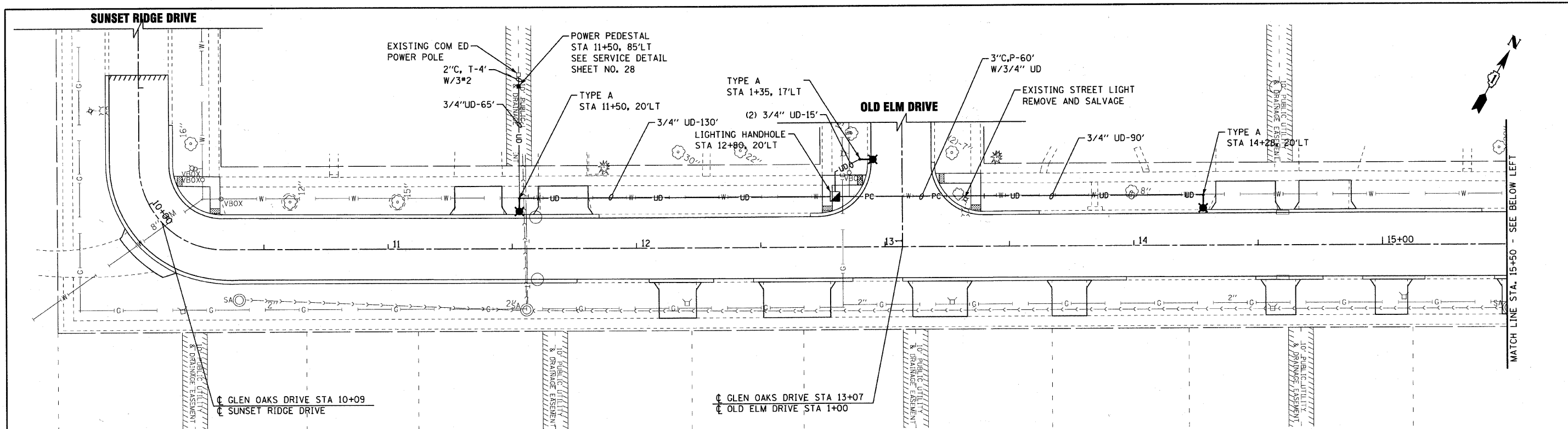
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CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

EROSION & SEDIMENT CONTROL PLAN
GLEN OAKS DRIVE

SCALE: 1" = 20' STA. 21+00 TO STA. 30+74

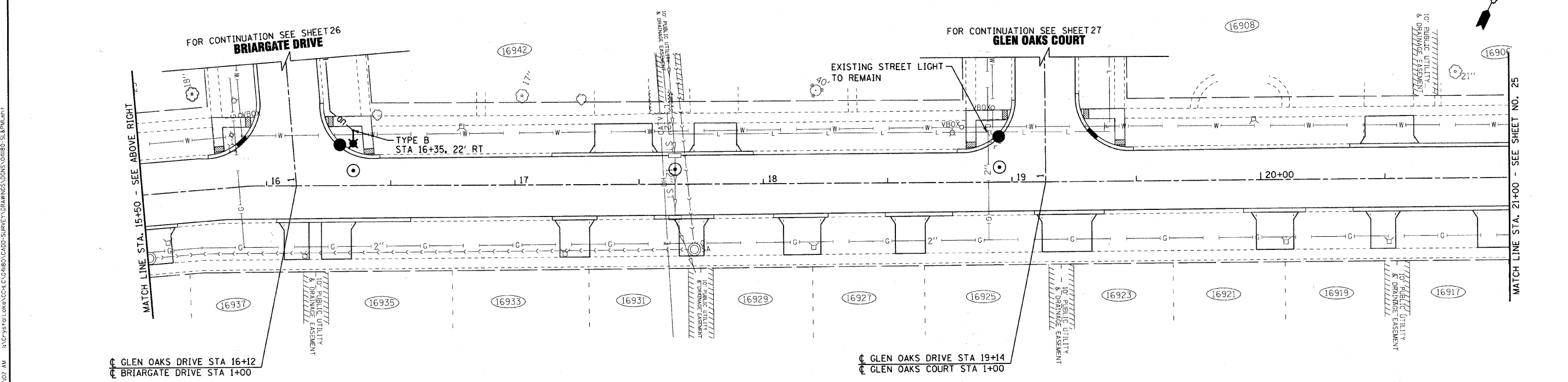
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-	04-00027-03-PV	COOK	36	23
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPD-097(008)		



GLEN OAKS DRIVE

LEGEND

□	EXISTING COM ED POWER POLE	—UD—	3/4" UNIT DUCT W/2*8 & 1*8 GROUND
■	POWER PEDESTAL	PC—	2" CONDUIT, PUSHED 8' LONG WITH 3/4" UNIT DUCT, OR AS NOTED
◻	LIGHTING HANDHOLE	—U—	2" CONDUIT, TRENCHED 8' LONG WITH 3*2, OR AS NOTED
—	EXISTING UNDERGROUND CONDUIT	—	PROPOSED STREET LIGHT
—OH—	EXISTING OVERHEAD POWER LINE	—	EXISTING STREET LIGHT - TYPE A; 8' ARM, 250W HPS, 240V
			- TYPE B; 12' ARM, 250W HPS, 240V



GLEN OAKS DRIVE

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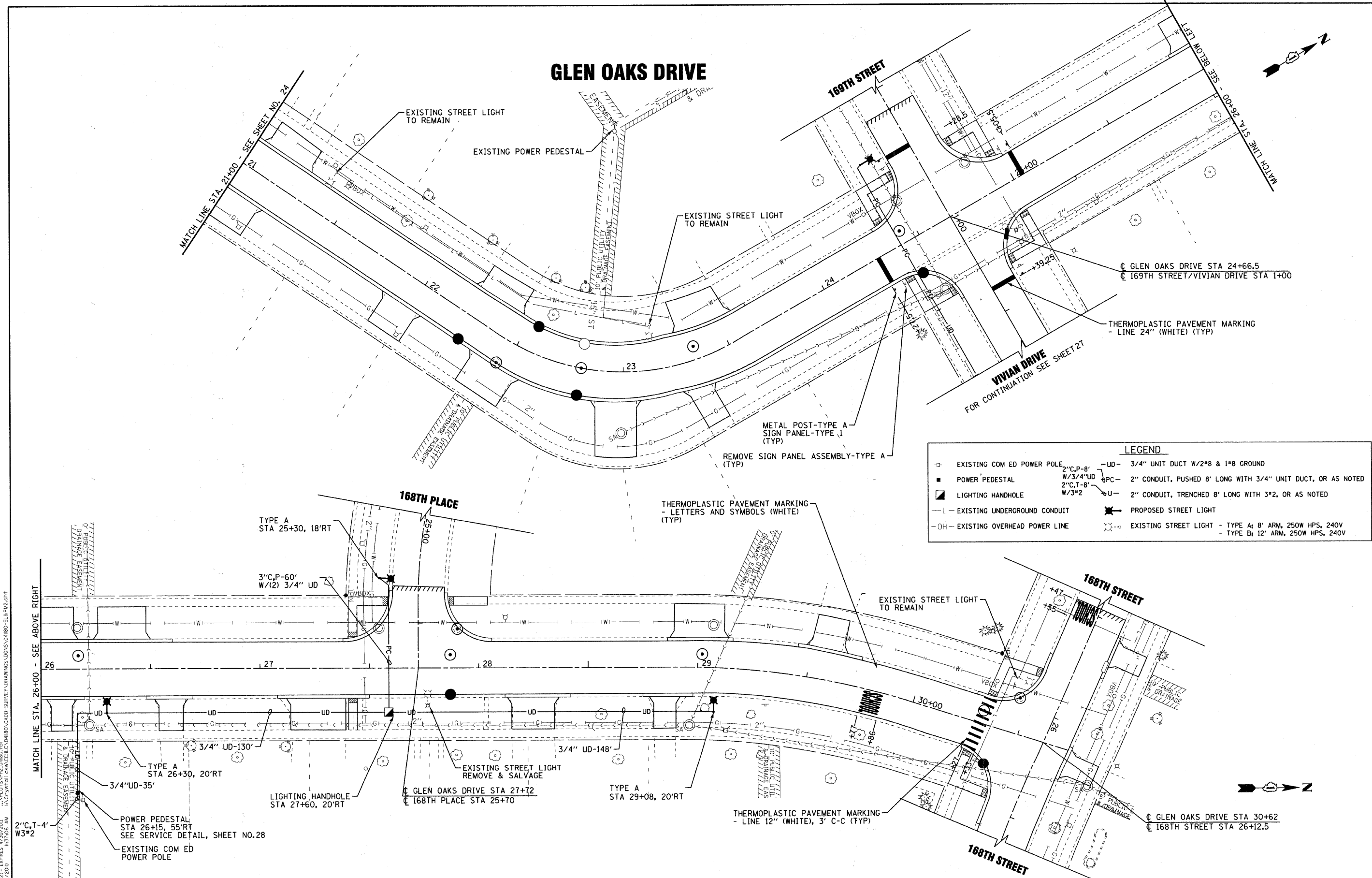
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**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

**STREET LIGHTING AND PAVEMENT MARKING PLAN
GLEN OAKS DRIVE**

SCALE: 1" = 20' STA. 9+40 TO STA. 21+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	24
	C-91-004-05		CONTRACT NO. 63539	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	HPD-897(08)	



LEGEND

⊕	EXISTING COM ED POWER POLE	—UD—	3/4" UNIT DUCT W/2"8 & 1"8 GROUND
■	POWER PEDESTAL	—PC—	2" CONDUIT, PUSHED 8' LONG WITH 3/4" UNIT DUCT, OR AS NOTED
⬢	LIGHTING HANDHOLE	—U—	2" CONDUIT, TRENCHED 8' LONG WITH 3"2, OR AS NOTED
—L—	EXISTING UNDERGROUND CONDUIT	⊙	PROPOSED STREET LIGHT
—OH—	EXISTING OVERHEAD POWER LINE	⊙	EXISTING STREET LIGHT - TYPE A: 8' ARM, 250W HPS, 240V
		⊙	- TYPE B: 12' ARM, 250W HPS, 240V

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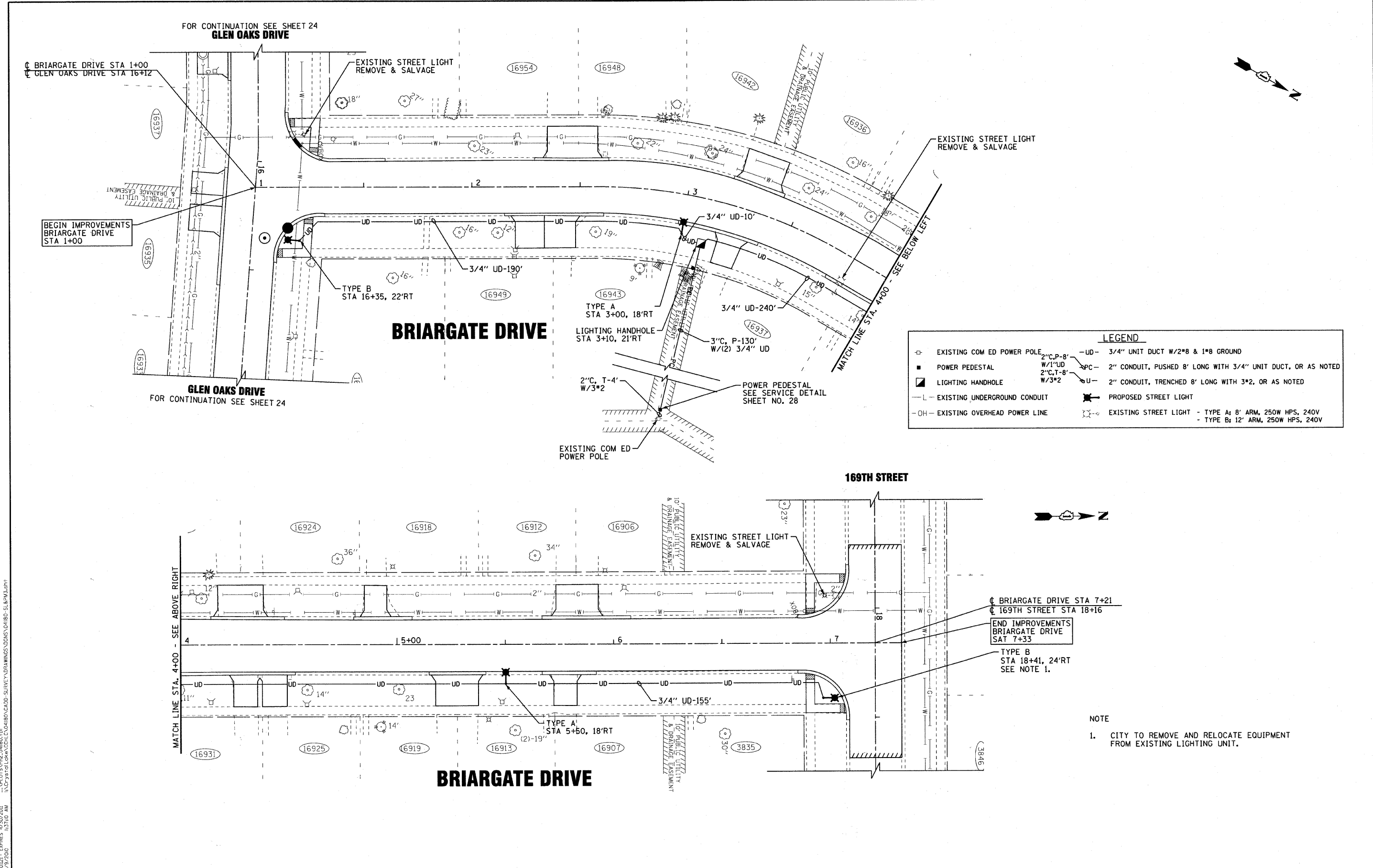
CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

STREET LIGHTING AND PAVEMENT MARKING PLAN
GLEN OAKS DRIVE

SCALE: 1" = 20'

STA. 21+00 TO STA. 30+74

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	25
	C-91-004-05		CONTRACT NO. 635339	
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT WPD-897(008)				



LEGEND

⊕	EXISTING COM ED POWER POLE	—UD—	3/4" UNIT DUCT W/2"Ø & 1"Ø GROUND
■	POWER PEDESTAL	—SPC—	2" CONDUIT, PUSHED 8' LONG WITH 3/4" UNIT DUCT, OR AS NOTED
⊠	LIGHTING HANDHOLE	—U—	2" CONDUIT, TRENCHED 8' LONG WITH 3"Ø, OR AS NOTED
—L—	EXISTING UNDERGROUND CONDUIT	⊠	PROPOSED STREET LIGHT
—OH—	EXISTING OVERHEAD POWER LINE	⊙	EXISTING STREET LIGHT - TYPE A: 8' ARM, 250W HPS, 240V - TYPE B: 12' ARM, 250W HPS, 240V

MATCH LINE STA. 4+00 - SEE ABOVE RIGHT

⊕ BRIARGATE DRIVE STA 7+21
 ⊕ 169TH STREET STA 18+16
 END IMPROVEMENTS
 BRIARGATE DRIVE
 STA 7+33
 TYPE B
 STA 18+41, 24'RT
 SEE NOTE 1.

NOTE
 1. CITY TO REMOVE AND RELOCATE EQUIPMENT FROM EXISTING LIGHTING UNIT.

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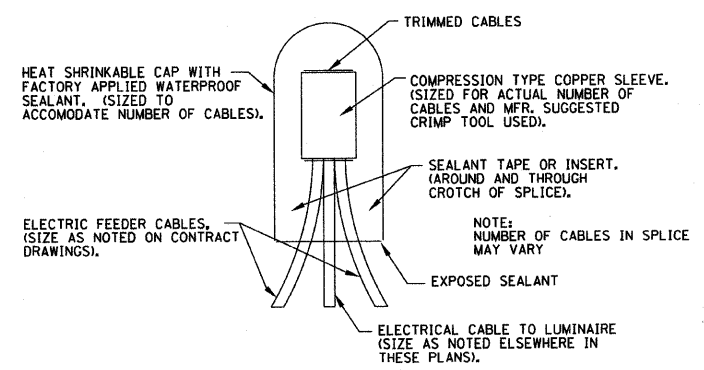
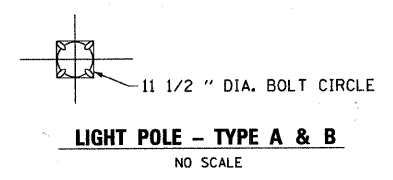
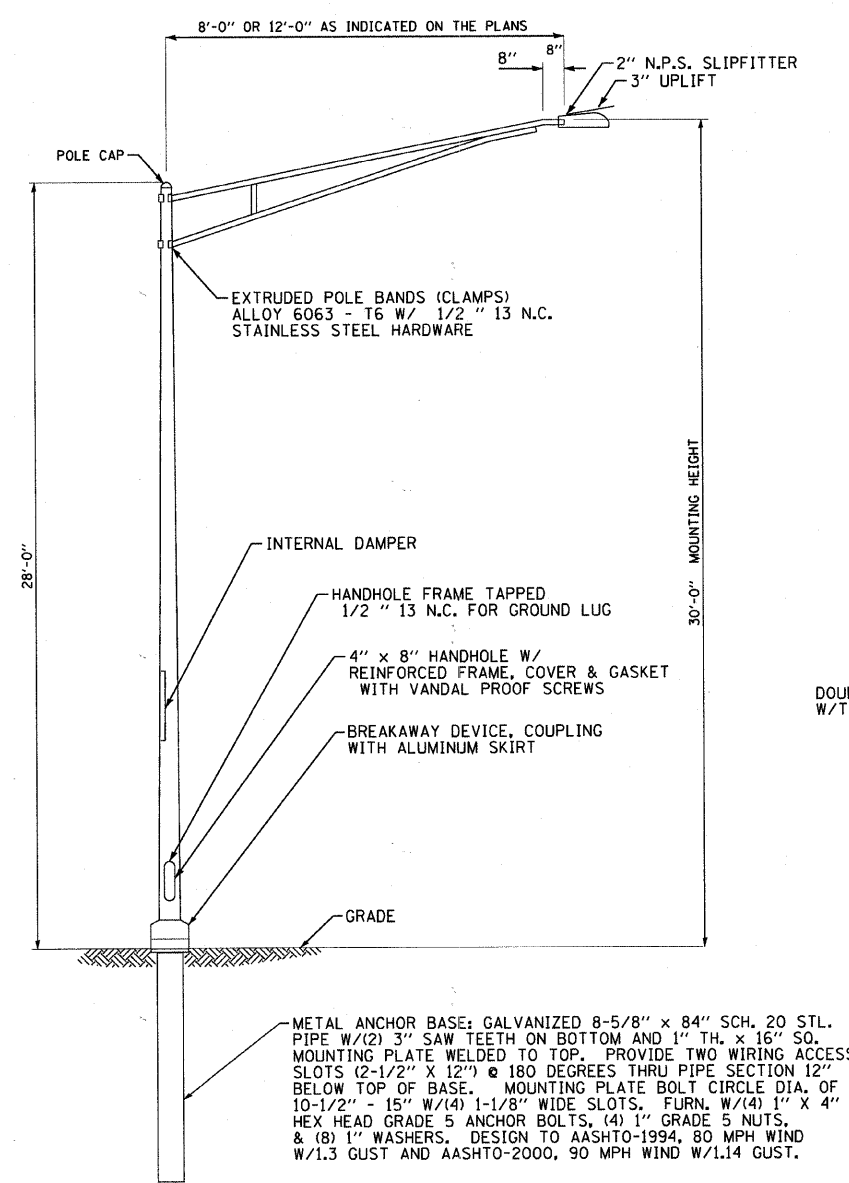
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**CITY OF COUNTRY CLUB HILLS, ILLINOIS
 GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

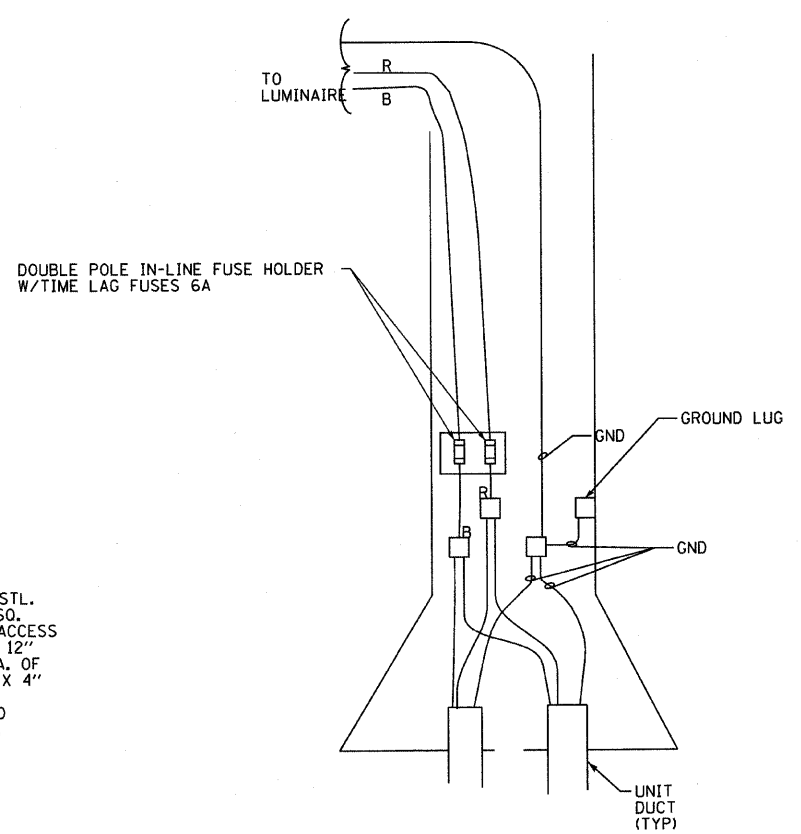
**STREET LIGHTING AND PAVEMENT MARKING PLAN
 BRIARGATE DRIVE**

SCALE: 1" = 20'
 STA. 1+00 TO STA. 7+21

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	26
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				

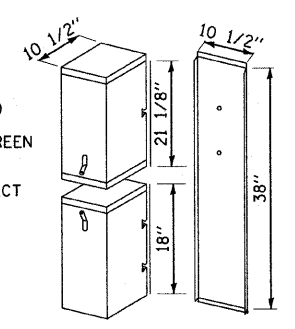


SPLICING ELECTRICAL CABLES
BASIC MATERIALS AND METHODS
 NO SCALE

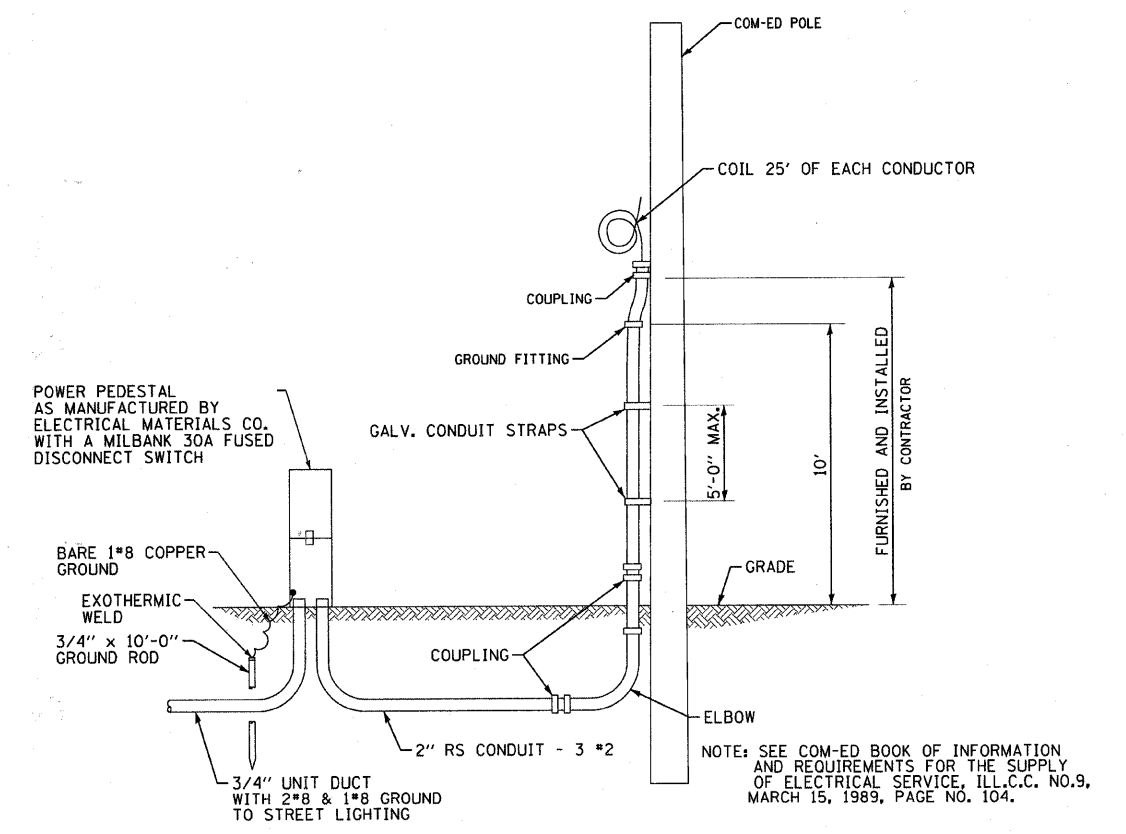


POLE WIRING DETAIL
 NO SCALE

- FEATURES :
- 14 GAUGE 235 MILL GALVANIZED STEEL WITH 12 GAUGE HASP
 - POWDER COATED PAINT DARK GREEN
 - ROUNDED FRONT
 - RECESSED PENTELOCK FEATURE
 - 30A, 2 POLE FUSIBLE DISCONNECT



POWER PEDESTAL DETAIL
 NO SCALE



COM-ED OVERHEAD CONNECTION WHERE REQUIRED
 NO SCALE

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 LICENSE NO. 184-0001 - EXPIRES 12/31/2011
 12/29/2010



DESIGNED - MWH	REVISED - 12-01-10 IDOT REVIEW
DRAWN - CJC	REVISED -
CHECKED - MDK	REVISED -
DATE - 10-01-10	FILE - 041180-SL-DETAILS.sh+

CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

LIGHTING DETAILS	
SCALE: NONE	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	28
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT HPD-897(008)		

GENERAL NOTES

- ALL WORK SHALL CONFORM TO N.E.C. AND ANY APPLICABLE AND/OR ALL SECTIONS OF "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" DATED JANUARY 1, 2007 BY ILLINOIS DEPARTMENT OF TRANSPORTATION, ALSO ANY AND ALL APPLICABLE "SPECIAL PROVISION" ISSUED BY I.D.O.T. TO DATE.
- THE CONTRACTOR SHALL PROVIDE WARRANTIES OR GUARANTEES PROVIDING FOR SATISFACTORY IN-SERVICE OPERATION OF THE MECHANICAL AND ELECTRICAL EQUIPMENT AND RELATED COMPONENTS AS PER I.D.O.T. STANDARDS.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR GROUNDING. GROUNDING CONNECTIONS AT THE FOUNDATION SHALL BE EXOTHERMICALLY WELDED, AS SPECIFIED, AND SHALL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO BACKFILLING, AS APPLICABLE.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENT FOR BURIED WARNING TAPE, SPECIFIED AS PART OF "TRENCH AND BACKFILL FOR ELECTRICAL WORK". THE INSTALLATION OF THE TAPE SHALL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO BACKFILLING OR DURING PLOWING OPERATIONS, AS APPLICABLE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ESTABLISHMENT OF FINISHED GRADE. THE ENGINEER MAY ASSIST THE CONTRACTOR, AS APPLICABLE, BUT THE RESPONSIBILITY FOR COORDINATING THE FINISHED GRADE ELEVATION WITH THE TOP OF FOUNDATION HEIGHTS AND THE LIGHT SHALL REMAIN WITH THE CONTRACTOR.
- THE CONTRACTOR SHALL MAKE SPECIAL NOTE OF THE REQUIREMENTS FOR WIRE MARKERS AND SHALL TAG ALL WIRE MARKERS AND SHALL TAG ALL WIRING ACCORDINGLY.
- THE CITY OF COUNTRY CLUB HILLS AND ENGINEER SHALL BE NOTIFIED 48 HOURS PRIOR TO COMMENCING EACH CONSTRUCTION PHASE.
- THE CONTRACTOR WILL BE REIMBURSED BY THIS CONTRACT TO THE EXACT AMOUNT OF MONEY AS BILLED BY COM-ED FOR THEIR SERVICES UNDER THE ELECTRIC UTILITY SERVICE CONNECTION PAY ITEM. NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR ANY INCIDENTAL MATERIALS AND LABOR REQUIRED TO FULFILL THE REQUIREMENTS AS SHOWN ON THE PLANS AND SPECIFIED HEREIN. SEE SECTION 109.05 "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION".
- ALL ELECTRICAL DEVICES AND MATERIALS SHALL BE U/L LISTED WHERE APPLICABLE.
- PARKWAYS ARE TO BE DISTURBED AS LITTLE AS POSSIBLE. CONTRACTOR SHALL IDENTIFY AREAS REQUIRED TO BE DISTURBED TO THE ENGINEER AND RECEIVE APPROVAL PRIOR TO ANY CONSTRUCTION. ANY AREAS DISTURBED WITHOUT PRIOR APPROVAL FROM THE ENGINEER SHALL BE REPAIRED AND RESTORED AT THE CONTRACTORS EXPENSE.
- ALL WIRING TO BE INSTALLED BY DIRECTIONAL DRILLING METHOD UNLESS OTHERWISE NOTED ON THE PLANS OR DETERMINED BY THE ENGINEER.
- ANY EXISTING TURF AND/OR SOIL DISTURBED IN AREAS APPROVED BY THE ENGINEER THAT CANNOT REMAIN OR BE RE-USED SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE INCLUDED IN THE COST OF THE BORING, OR TRENCHING PAY ITEM.

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
80400100	ELECTRIC SERVICE INSTALLATION	EACH	4
80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1
81018700	CONDUIT PUSHED, 3" DIA., GALVANIZED STEEL	FOOT	680
81028020	CONDUIT, BORED AND PULLED, COILABLE NON-METALLIC CONDUIT, 3/4"	FOOT	1418
81400730	HANDHOLE, COMPOSITE CONCRETE	EACH	3
81603000	UNIT DUCT, 600V, 2-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 3/4" DIA. POLYETHYLENE	FOOT	685
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1283
82103250	LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, PHOTO-CELL CONTROL 250 WATT	EACH	12
83006300	LIGHT POLE, ALUMINUM, 30 FT. M.H., 8 FT. MAST ARM	EACH	9
83006500	LIGHT POLE, ALUMINUM, 30 FT. M.H., 12 FT. MAST ARM	EACH	3
83600405	POLE FOUNDATION, STEEL	EACH	12
83800505	BREAKAWAY DEVICE, COUPLING, WITH ALUMINUM SKIRT	EACH	12
84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	5
XX005049	POWER PEDESTALS	EACH	4

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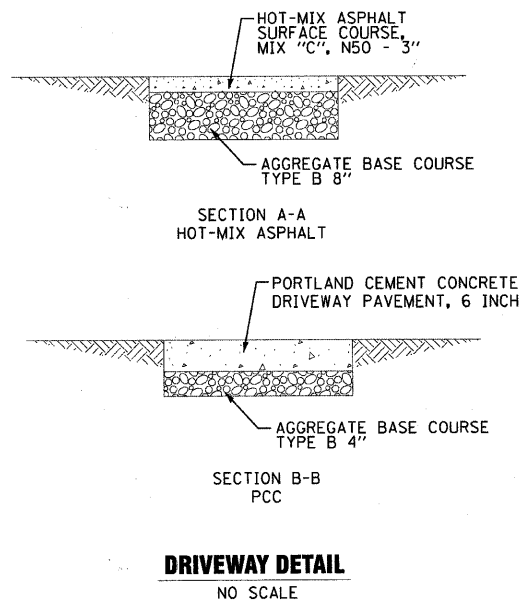
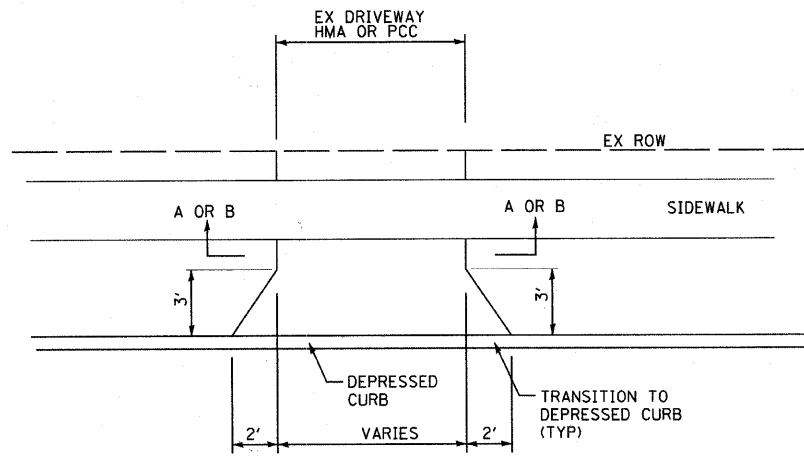
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DRAWN - CJC	REVISED -
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DATE - 10-01-10	FILE - 041180-SL-DETAILS.shp

**CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS**

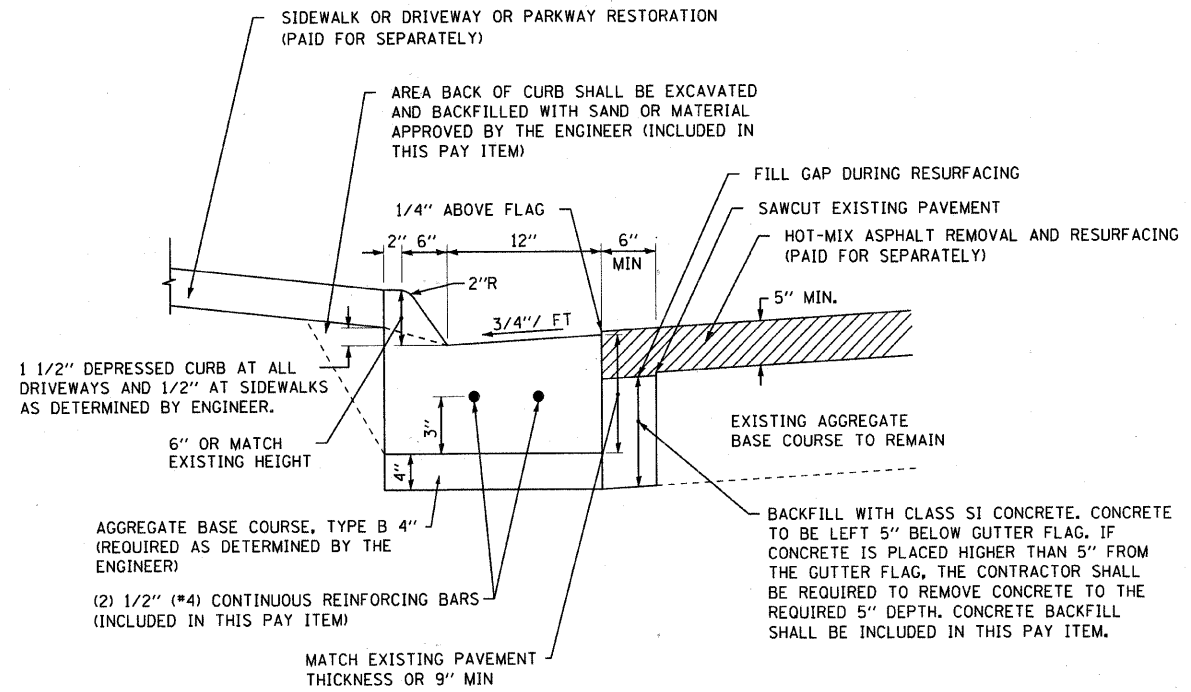
LIGHTING GENERAL NOTES AND SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

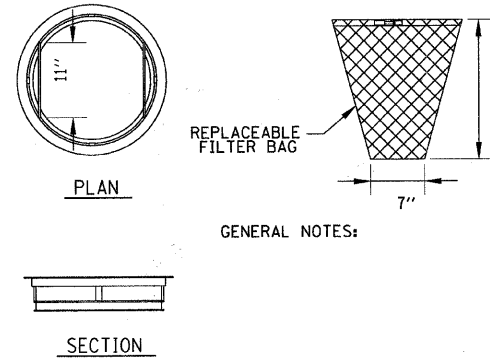
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	29
C-91-004-05		CONTRACT NO. 63539		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT HPD-897(008)				



DRIVEWAY DETAIL
NO SCALE



COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.12 (SPECIAL)
(NOT TO SCALE)



GENERAL NOTES:
FRAME: TOP RING CONSTRUCTED FROM 1 1/4" x 1 1/4" x 1/8" ANGLE. BASE RING CONSTRUCTED OF 1 1/2" x 1/2" x 1/8" CHANNEL. HANDLES & SUSPENSION BRACKETS CONSTRUCTED FROM 1/4" x 1 1/4" FLAT. ALL STEEL CONFORMING TO ASTM-A36.
REPLACEABLE BAG: CONSTRUCTED FROM 4 OZ./SQ. YD. NON-WOVEN POLYPROPYLENE GEOTEXTILE REINFORCED WITH POLYESTER MESH. CONNECTED TO BASE RING WITH STAINLESS STEEL STRAP & LOCK.

INLET FILTER
NO SCALE

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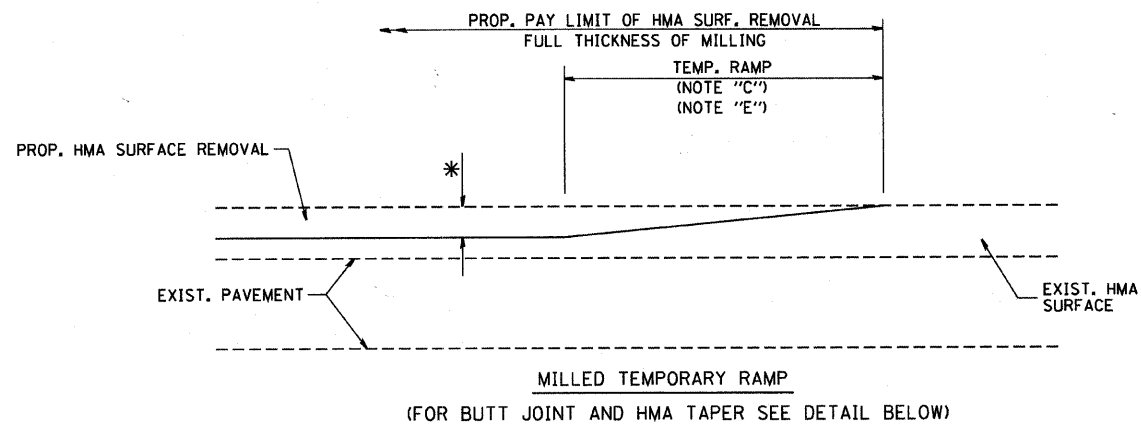
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CITY OF COUNTRY CLUB HILLS, ILLINOIS
GLEN OAKS DRIVE INFRASTRUCTURE IMPROVEMENTS

MISCELLANEOUS DETAILS

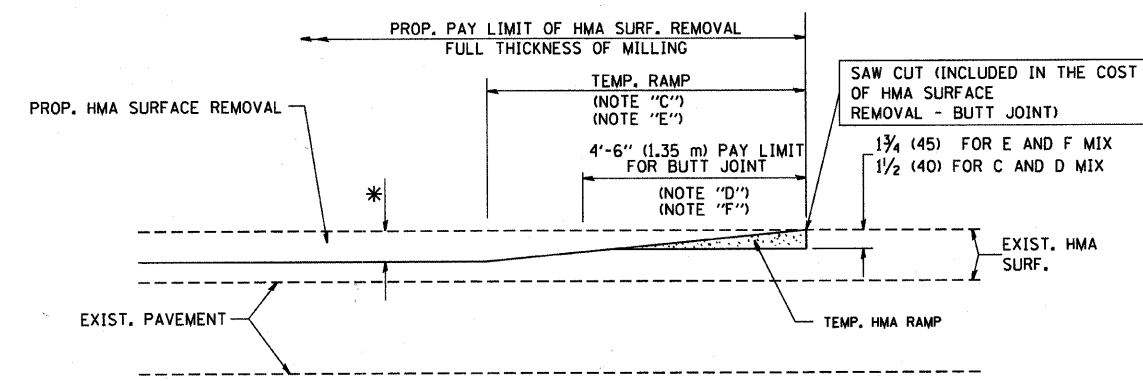
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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C-91-004-05			CONTRACT NO. 63539	
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT HPD-897(008)				



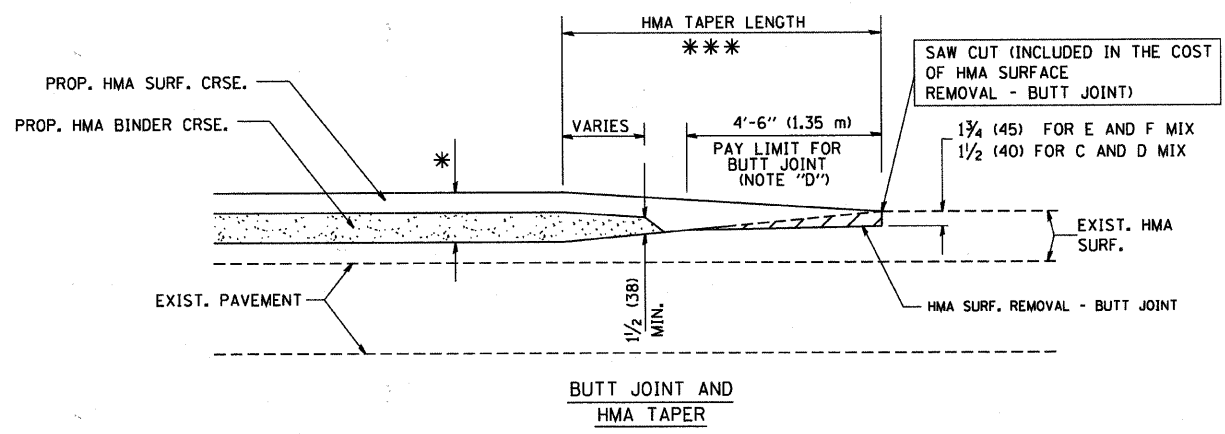
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

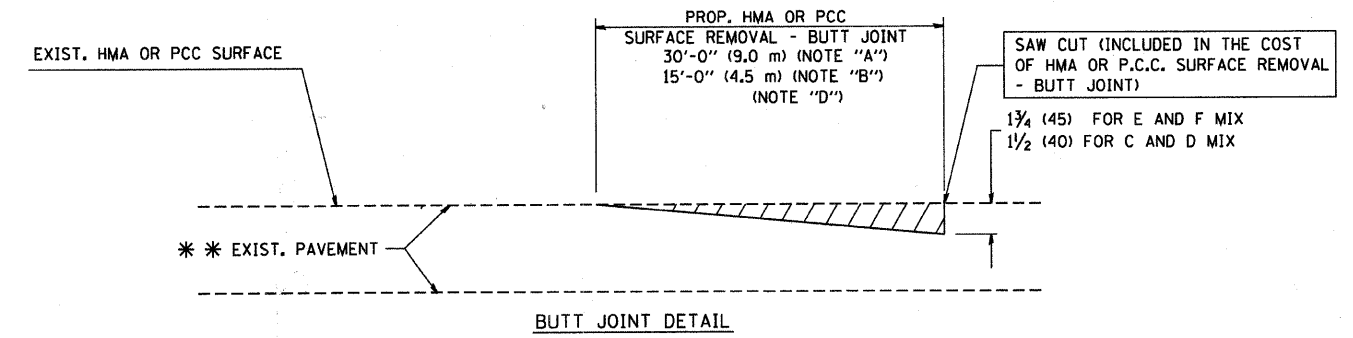


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

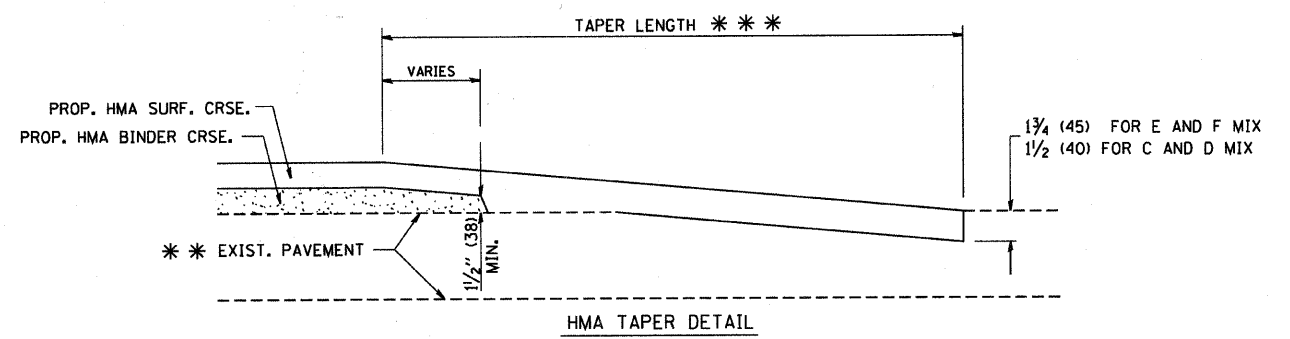
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

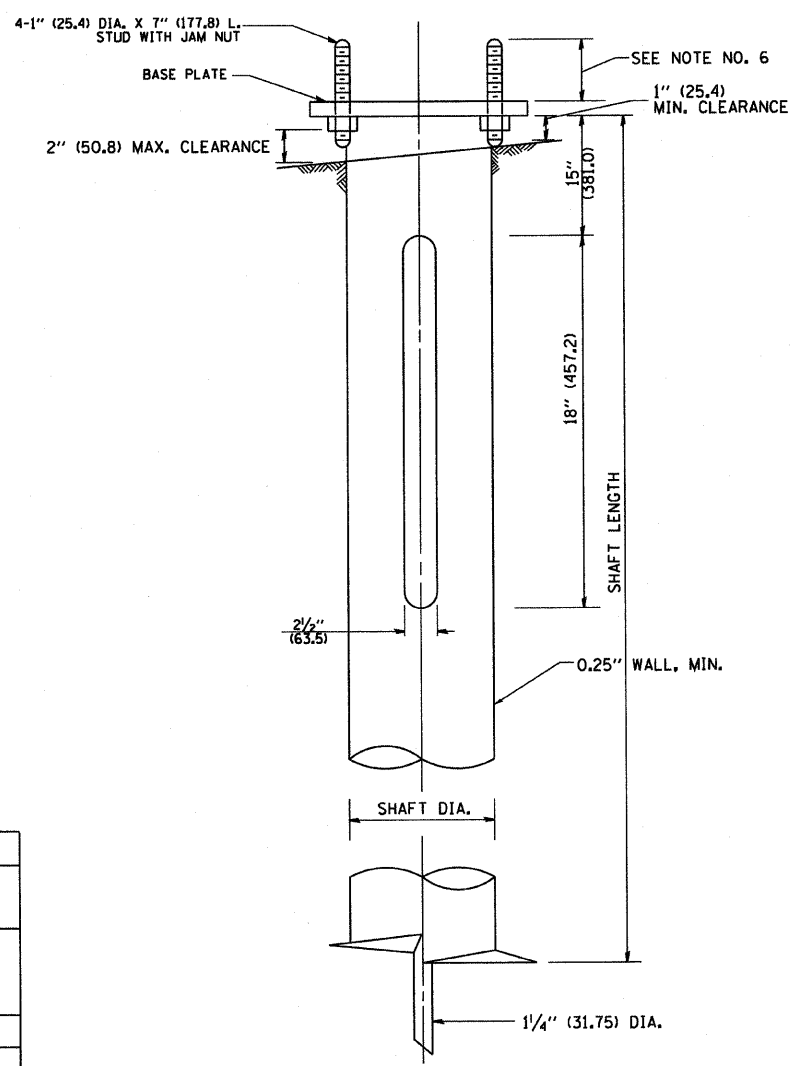
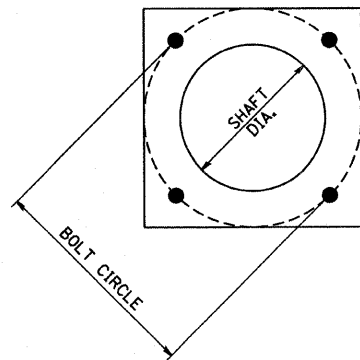
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 "/ IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-0027-03-PV	COOK	36	31
BD400-05 BD32		CONTRACT NO. 63539		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPD-897(008)				



HELIX FOUNDATION SIZE

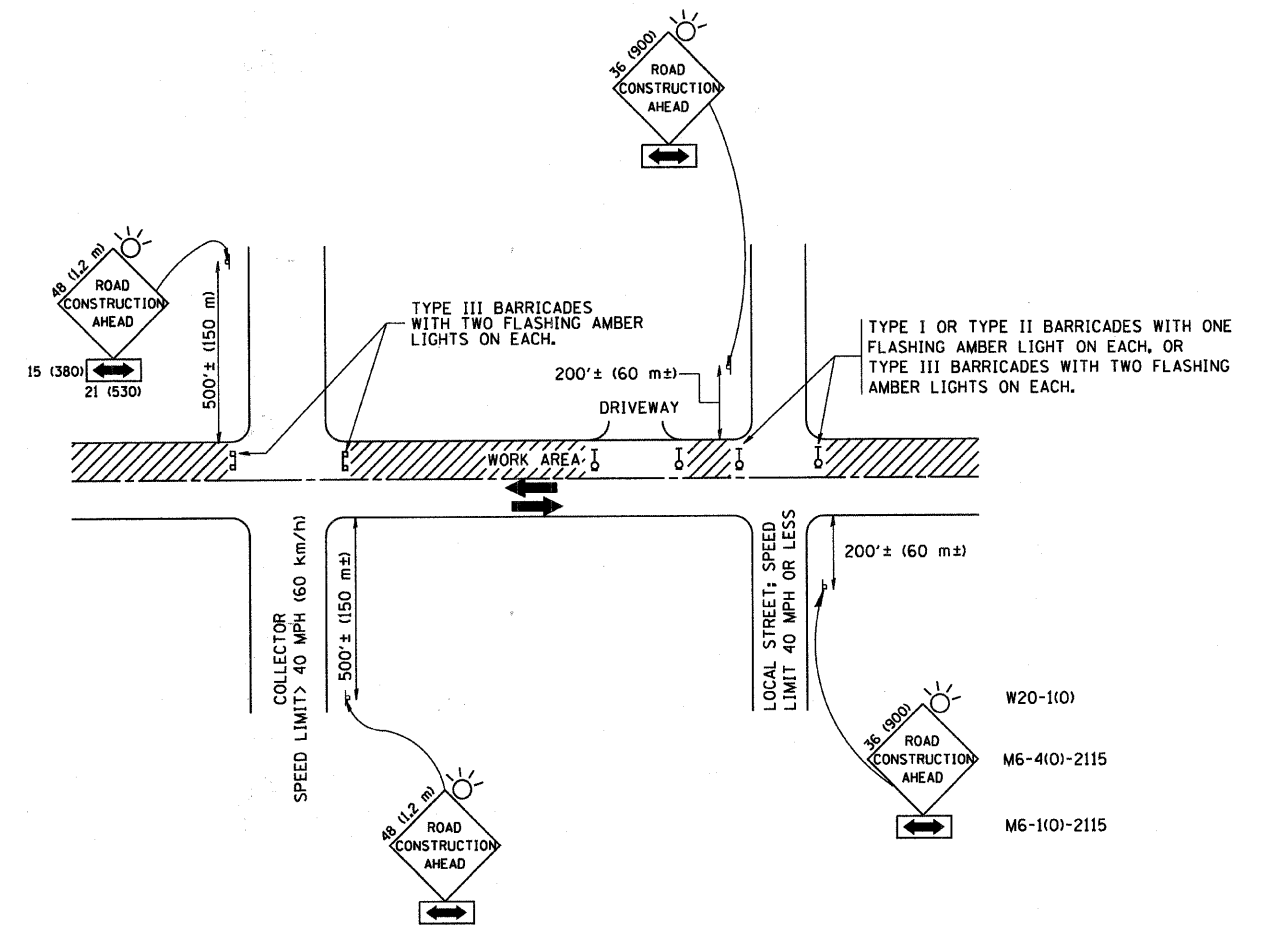
POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
31 FT.-35 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
36 FT.-40 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
41 FT.-45 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1 1/4"

METAL HELIX FOUNDATION MATERIALS

ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)

NOTES:

- ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
- ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1/4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
- THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
- ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
- METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDATION IS NOT ALLOWED.
- THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
- THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ($\pm 1^\circ$) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC (± 0.188) TO THE SHAFT AXIS.
- THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC (± 0.125) AND IN LINE ($\pm 2^\circ$).
- THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

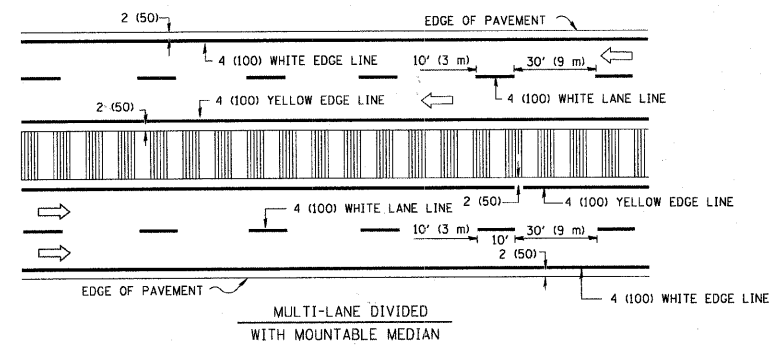
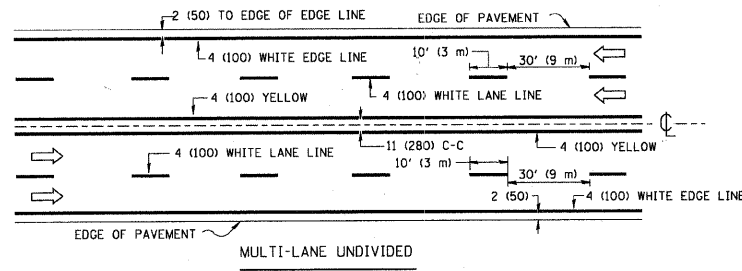
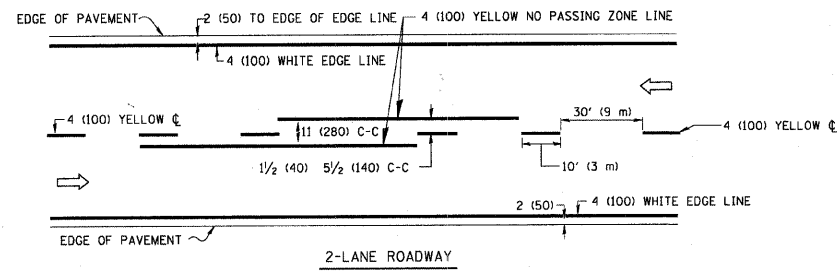
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

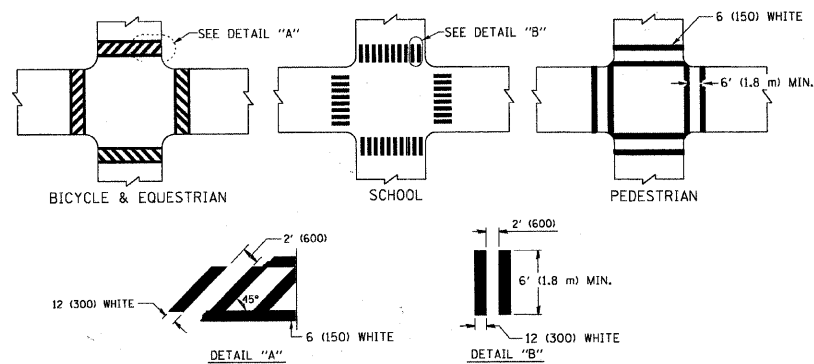
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-2V	COOK	36	33
TC-10			CONTRACT NO. 63539	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT HFD-897(008)				

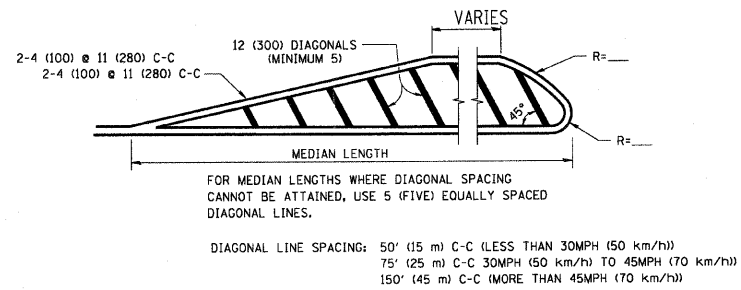
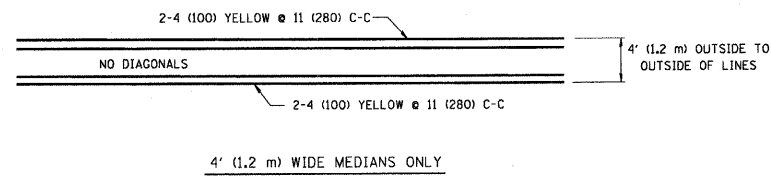


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

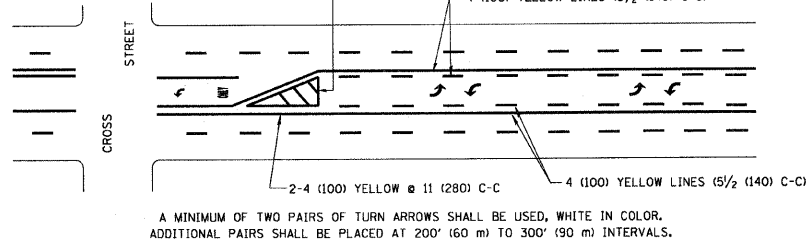
TYPICAL LANE AND EDGE LINE MARKING



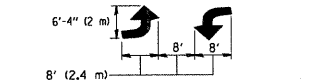
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

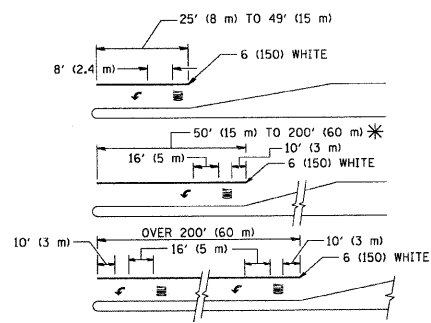


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

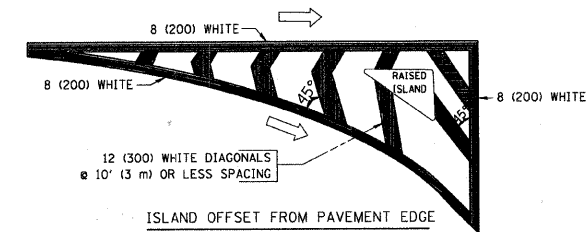


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

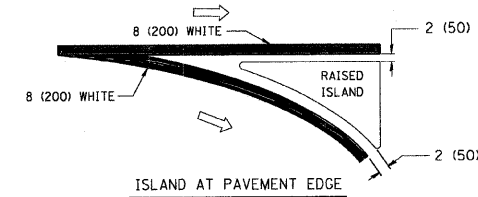
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100)	SOLID	YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
c:\p\work\pavdot\drivakosgn\d0108315\to3.dgn		DRAWN -	REVISED -C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

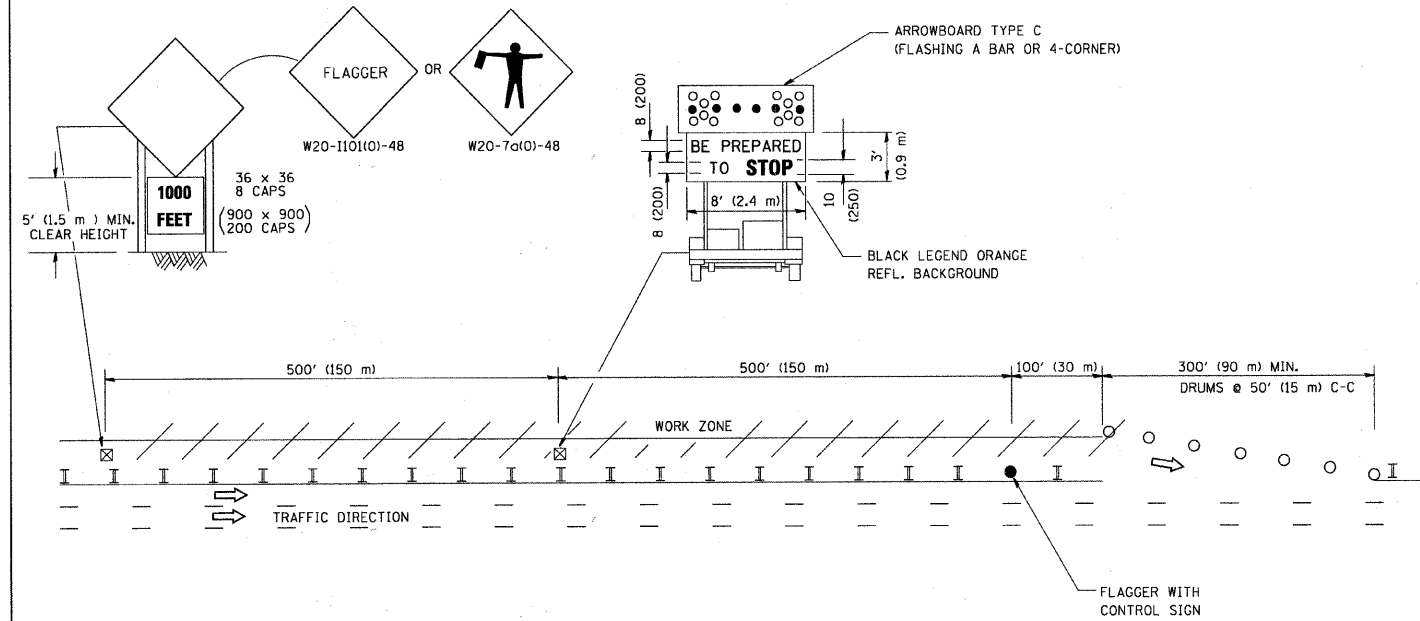
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

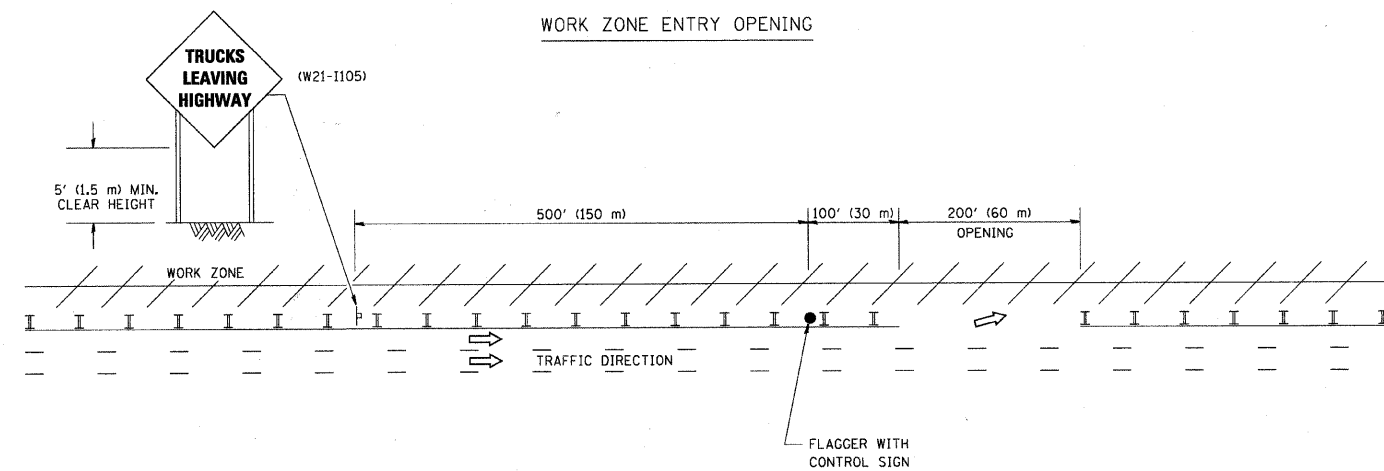
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-0027-03-PV	COOK	36	34
TC-13			CONTRACT NO. 63539	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT HPD-897(008)				

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. THE ARROWBOARD, THE FLAGGER AHEAD SIGN AND THE TRUCKS LEAVING HIGHWAY SIGN SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE EXIT OPENINGS SHOULD BE A MINIMUM OF ONE HALF MILE APART.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

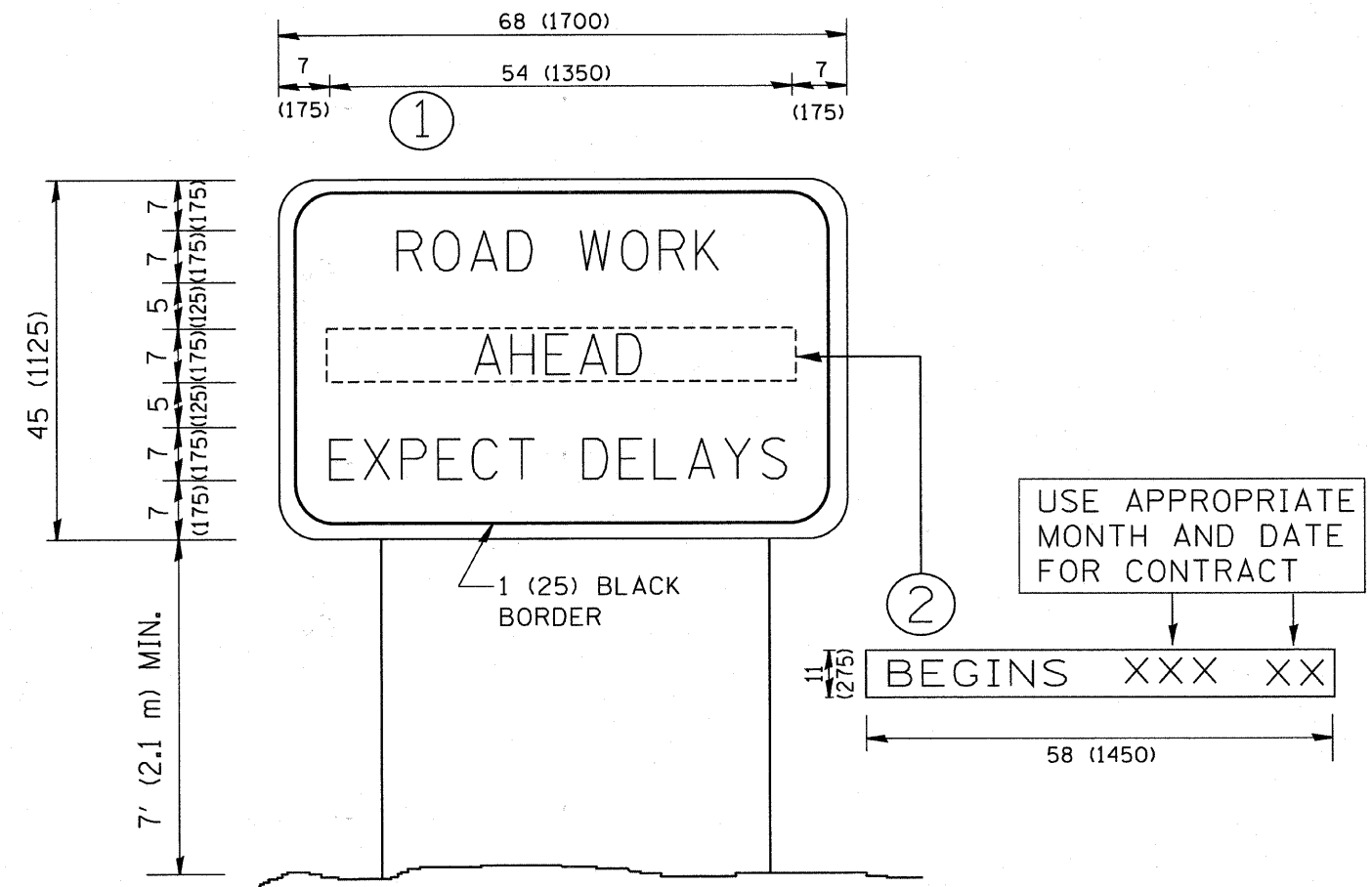
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		DRAWN -	REVISED - J.A.F. 02-06
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED - S.P.B. 01-07
	PLOT DATE = 1/26/2010	DATE -	REVISED - S.P.B. 12-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	04-00027-03-PV	COOK	36	35
TC-18			CONTRACT NO. 63539	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT HPD-897(008)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\distatd\22x34\to22.dgn	USER NAME = goglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	04-00027-03-PV	COOK	36	36
		PLOT SCALE = 50,000' / IN.	CHECKED -		REVISED - T. RAMMACHER 02-02-99				TC-22	CONTRACT NO. 63539		
		PLOT DATE = 1/4/2008	DATE -		REVISED - C. JUCIUS 01-31-07				FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT HPD-897 (008)			