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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

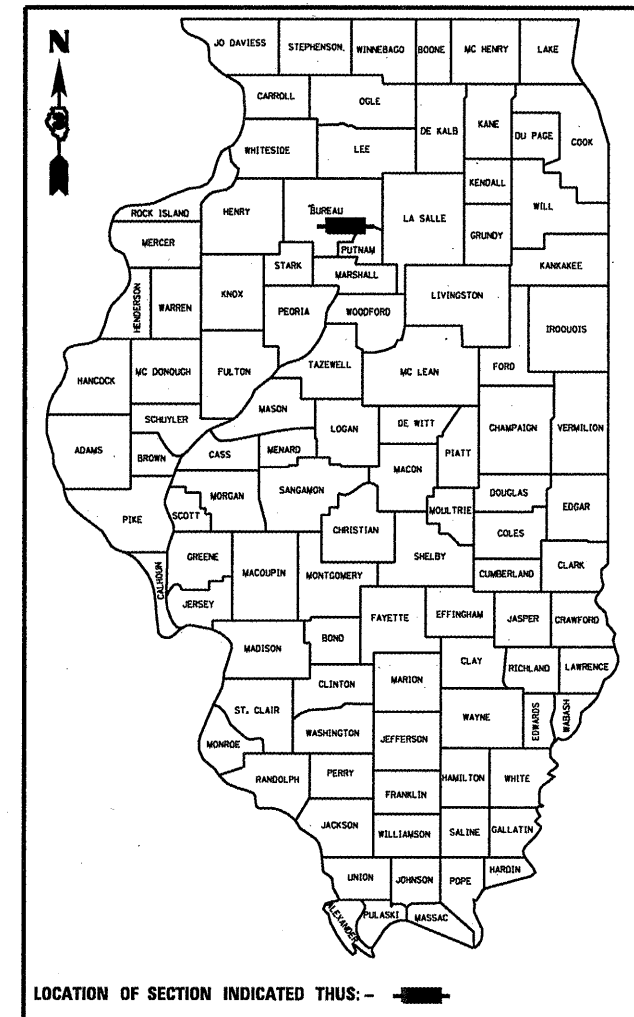
FAS 2247 (US 6)
SECTION (13C-BR)

BRIDGE REPAIRS
BUREAU COUNTY

C-93-114-10

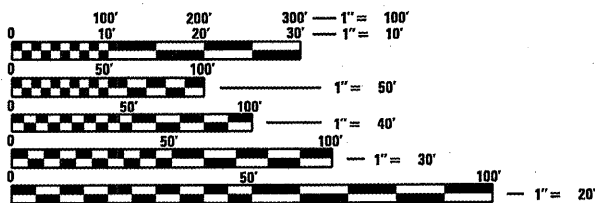
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2247	(13C-BR)	BUREAU	14	1
		ILLINOIS	CONTRACT NO. 66A29	

D-93-002-11



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

FUNCTIONAL CLASSIFICATION
RURAL - MAJOR ARTERIAL
 2009 ADT = 2600
 P.V. = 95.8%
 S.U. = 2.3%
 M.U. = 1.9%

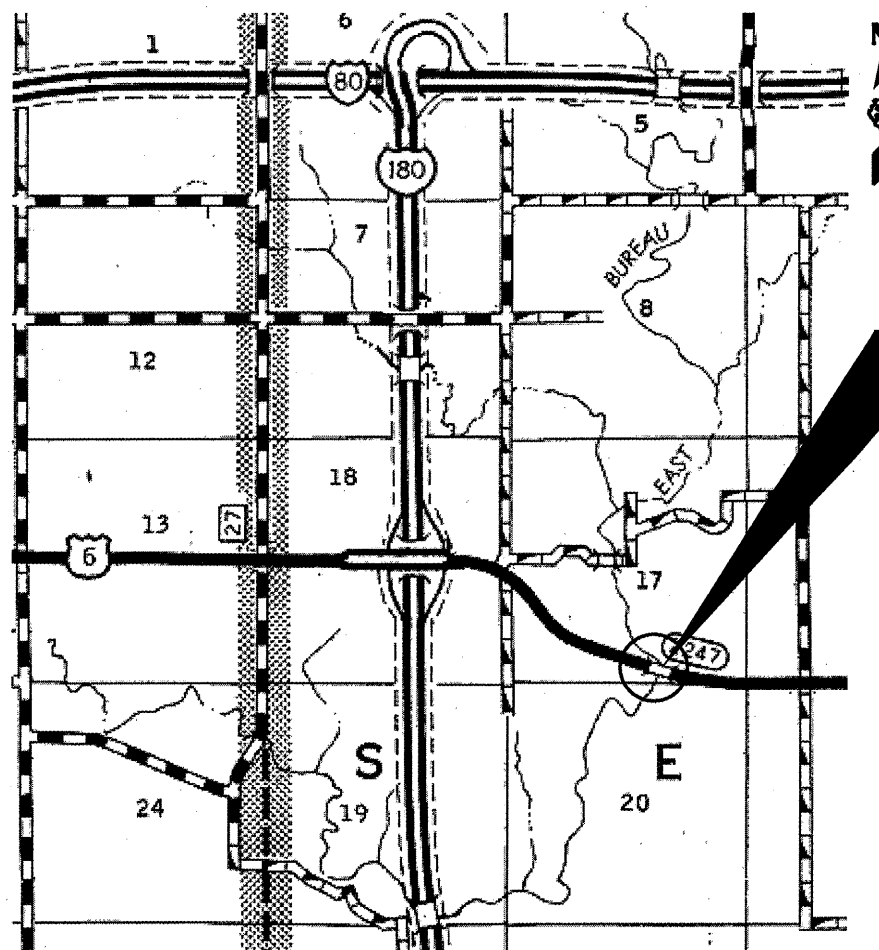


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: JOE KANNEL
UNIT CHIEF: RON WOODSHANK
TOWNSHIP: SELBY

CONTRACT NO. 66A29



POINT LOCATION

PROJECT LOCATION
 SN 006-0137
 CARRYING FAS 2247 (US 6) OVER
 EAST BUREAU CREEK
 1.3 MILES EAST OF I-180

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED DECEMBER 28 2010
George F. Ryan
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

February 4 2011
Scott E. Stitt, P.E.
 ENGINEER OF DESIGN AND ENVIRONMENT

February 4 2011
Christine M. Reed
 DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

THE HMA SURFACE OF ALL MAILBOX TURNOUTS, PRIVATE ENTRANCES, COMMERCIAL ENTRANCES, AND SIDE ROADS SHALL BE MADE NEATLY, IN A WORKMANLIKE MANNER, AND SHALL ACCURATELY CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. IF REQUIRED BY THE ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SAW CUT THE HMA SURFACE TO CONFORM TO THE SHAPES AND DIMENSIONS SHOWN ON THE PLAN DETAILS. THIS WORK SHALL BE INCLUDED IN THE COST OF THE HMA SURFACE.

THE HMA BASE COURSE WIDENING SHALL BE CARRIED THROUGH ALL ENTRANCES, SIDE ROADS, AND MAILBOX TURNOUTS. EXCEPTIONS WILL BE SHOWN ON THE PLANS.

THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.

FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

SEEDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

ON EXISTING SHOULDER WHICH MAY BE SUPERELEVATED, THE NEW HMA WIDENING SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.

ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

HMA RESURFACING	112	LBS / SQ YD / IN
-----------------	-----	------------------

COMMENTS

NONE. 1203/2010

HMA MIXTURE TABLE

	HMA WIDENING
PG GRADE	PG64-22
DESIGN AIR VOIDS	4.0% N50
MIXTURE COMPOSITION	IL 19.0
FRICTION AGGREGATE DENSITY CONTROL METHOD	CORES

DATE: 12-21-10

PREPARED BY: Dave Brownick
DISTRICT STUDIES & PLANS ENGINEER

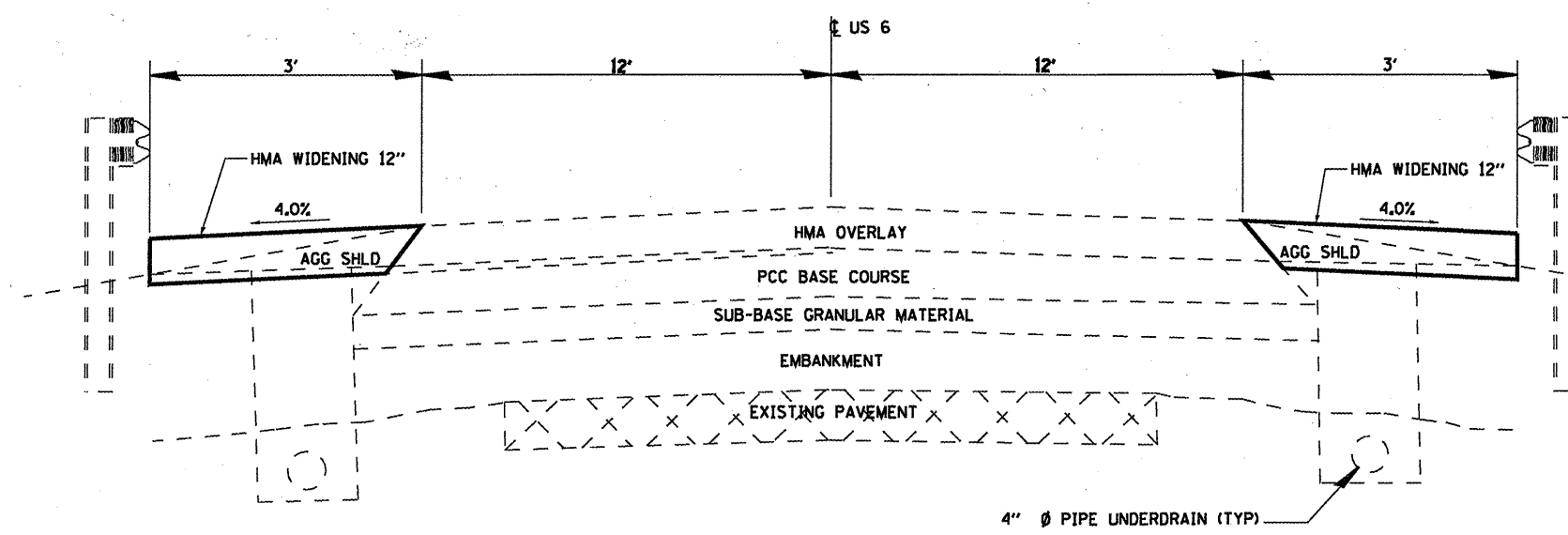
EXAMINED BY: Herbert Jiang
DISTRICT CONSTRUCTION ENGINEER

Ray J. Kelly
DISTRICT MATERIALS ENGINEER

Bruce A. Wuehler
DISTRICT OPERATIONS ENGINEER

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE
				STATE FUNDS 100% STATE STRUCTURE 0014 RURAL
20200500	EARTH EXCAVATION (WIDENING)	CU YD	200	200
28100107	STONE RIPRAP, CLASS A4	SO YD	266	266
28200200	FILTER FABRIC	SO YD	347	347
28300400	AGGREGATE DITCH	TON	81	81
35600724	HOT-MIX ASPHALT BASE COURSE WIDENING, 12"	SO YD	598	598
50102400	CONCRETE REMOVAL	CU YD	11	11
50300225	CONCRETE STRUCTURES	CU YD	6.8	6.8
50300255	CONCRETE SUPERSTRUCTURE	CU YD	4.2	4.2
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2100	2100
50800515	BAR SPLICERS	EACH	26	26
52000110	PREFORMED JOINT STRIP SEAL	FOOT	86	86
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1
70400100	TEMPORARY CONCRETE BARRIER	FOOT	407	407
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	358	358
• 78001110	PAINT PAVEMENT MARKING - LINE 4 "	FOOT	707	707
• 78001130	PAINT PAVEMENT MARKING - LINE 6 "	FOOT	175	175
78300100	PAVEMENT MARKING REMOVAL	SO FT	582	582
X0323265	REMOVE EXISTING RIPRAP	SO YD	87	87
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1	1
Z0016200	DECK SLAB REPAIR (PARTIAL)	SO YD	50	50
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2	2

• SPECIALITY ITEMS



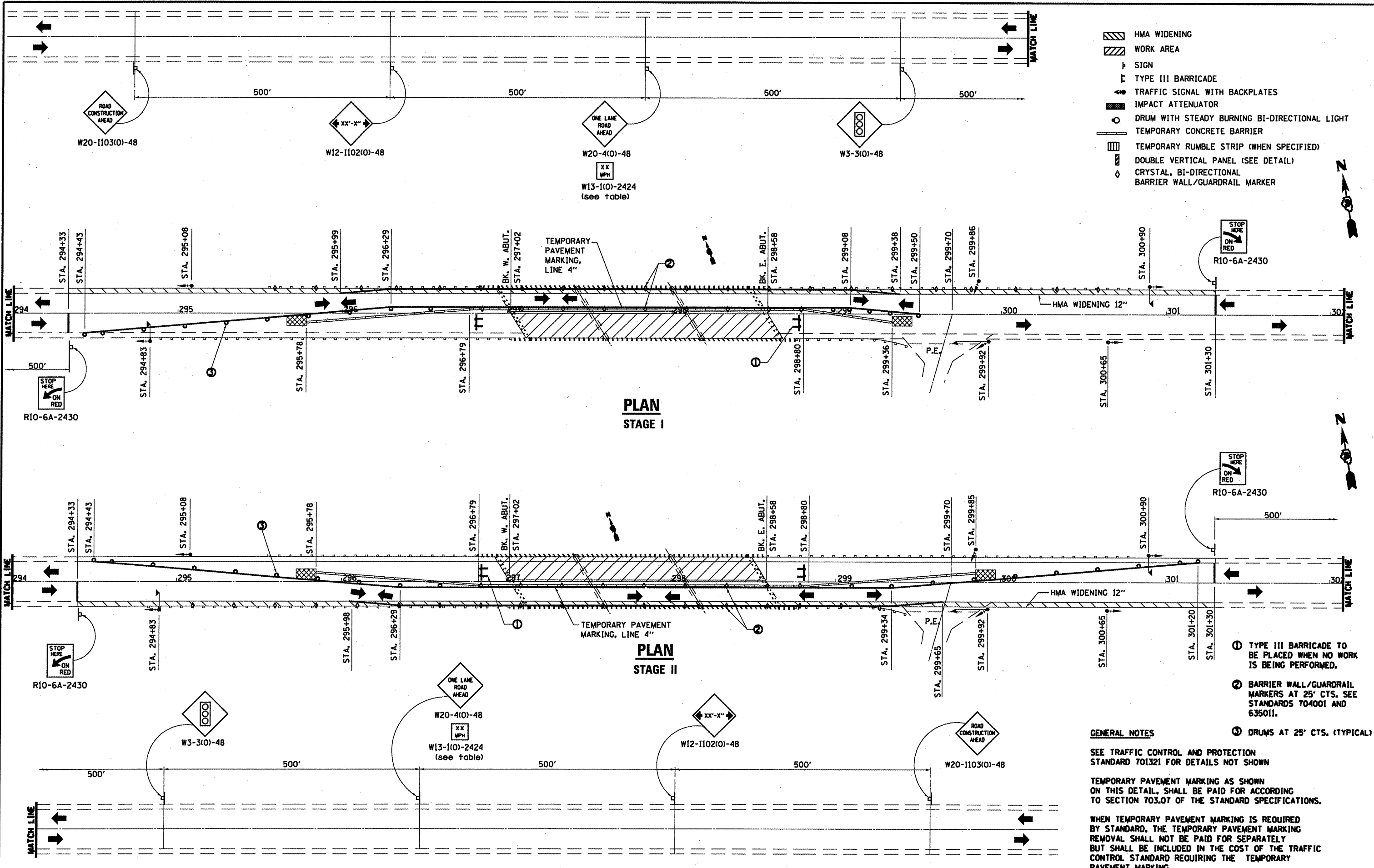
TYPICAL SECTION

** AS MEASURED ALONG E.O.P.

- ** STA. 294 + 33 TO STA. 269 + 95 LT.
- STA. 294 + 33 TO STA. 297 + 10 RT.
- STA. 298 + 50 TO STA. 301 + 30 LT.
- STA. 298 + 65 TO STA. 301 + 30 RT.

NOTE: SHOULDER WORK SHALL BE COMPLETED PRIOR TO BEGINNING STAGE I WORK.

FILE NAME = c:\pw\work\pwidot\woodshankr\1\d0143165	USER NAME = woodshankr1	DESIGNED - RLW	REVISED - -----	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS	F.A.S. RTE. 2247	SECTION (13C-BR1)	COUNTY BUREAU	TOTAL SHEETS 14	SHEET NO. 4			
	PLOT SCALE = 100.0000' / IN.	CHECKED - ---	REVISED - -----			SCALE: -----	SHEET NO. 1 OF 1 SHEETS	STA. ----- TO STA. -----	CONTRACT NO. 66A29				
	PLOT DATE = Dec 28, 2010 - 07:15:56 AM	DATE - -----	REVISED - -----			ILLINOIS FED. AID PROJECT							



- HMA WIDENING
- WORK AREA
- SIGN
- TYPE III BARRICADE
- TRAFFIC SIGNAL WITH BACKPLATES
- IMPACT ATTENUATOR
- DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
- TEMPORARY CONCRETE BARRIER
- TEMPORARY RUMBLE STRIP (WHEN SPECIFIED)
- DOUBLE VERTICAL PANEL (SEE DETAIL)
- CRYSTAL, BI-DIRECTIONAL BARRIER WALL/GUARDRAIL MARKER

**PLAN
STAGE I**

**PLAN
STAGE II**

- ① TYPE III BARRICADE TO BE PLACED WHEN NO WORK IS BEING PERFORMED.
- ② BARRIER WALL/GUARDRAIL MARKERS AT 25' CTS. SEE STANDARDS 704001 AND 635011.
- ③ DRUMS AT 25' CTS. (TYPICAL)

GENERAL NOTES

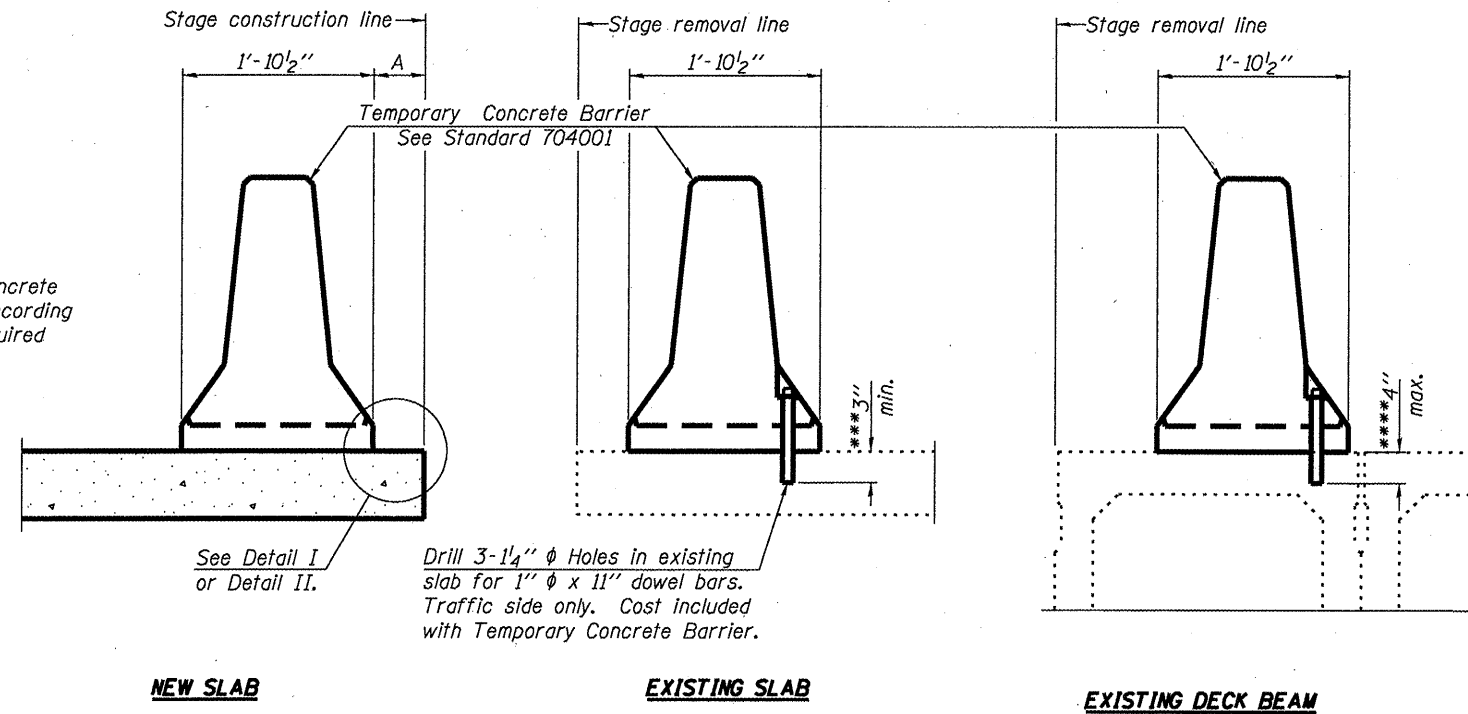
SEE TRAFFIC CONTROL AND PROTECTION STANDARD 701321 FOR DETAILS NOT SHOWN

TEMPORARY PAVEMENT MARKING AS SHOWN ON THIS DETAIL, SHALL BE PAID FOR ACCORDING TO SECTION 703.07 OF THE STANDARD SPECIFICATIONS.

WHEN TEMPORARY PAVEMENT MARKING IS REQUIRED BY STANDARD, THE TEMPORARY PAVEMENT MARKING REMOVAL SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL STANDARD REQUIRING THE TEMPORARY PAVEMENT MARKING.

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RLW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	F.A.S. RTE. 2247	SECTION (13C-BRI)	COUNTY BUREAU	TOTAL SHEETS 14	SHEET NO. 5		
MAIN1-0366A29-details.dgn	DRAWN - RLW	CHECKED -	REVISED -			SCALE: _____	SHEET NO. 1 OF 2 SHEETS	STA. _____ TO STA. _____	CONTRACT NO. 66A29			
PLOT SCALE = 1/80.0000" / IN.	DATE -	REVISED -	REVISED -			ILLINOIS FED. AID PROJECT						
PLOT DATE = Dec 28, 2010 - 07:16:26 AM	DATE -	REVISED -	REVISED -									

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4" ϕ Holes in existing slab for 1" ϕ x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:
Connect one (1) 1" x 7" x "W" steel \bar{P} to the top layer of couplers with 2-5/8" ϕ bolts screwed to coupler at approximate \bar{C} of each barrier panel.

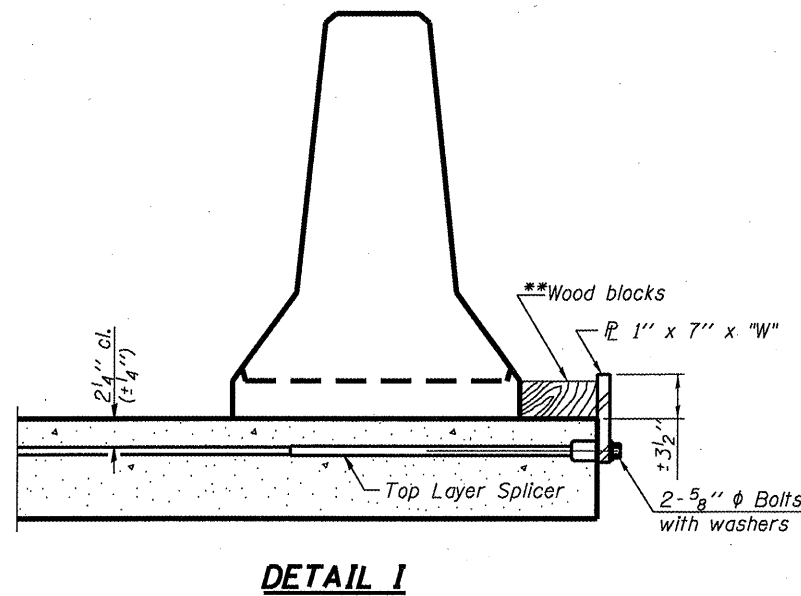
Detail II - With Extended Reinforcement Bars:
Connect one (1) 1" x 7" x "W" steel \bar{P} to the concrete slab or concrete wearing surface with 2-5/8" ϕ Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate \bar{C} of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

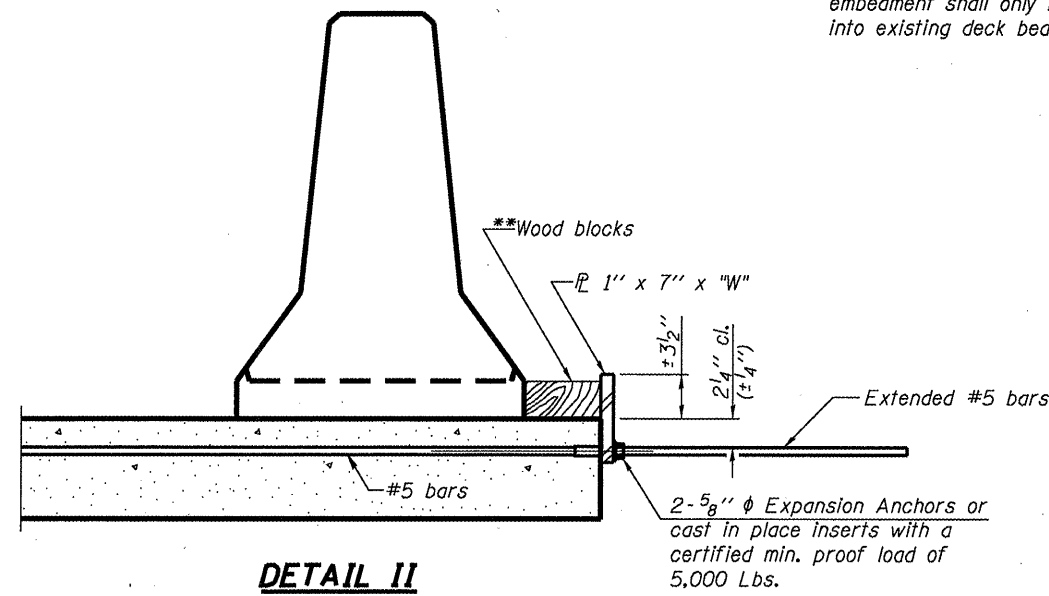
SECTIONS THRU SLAB OR DECK BEAM

*** Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

**** If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



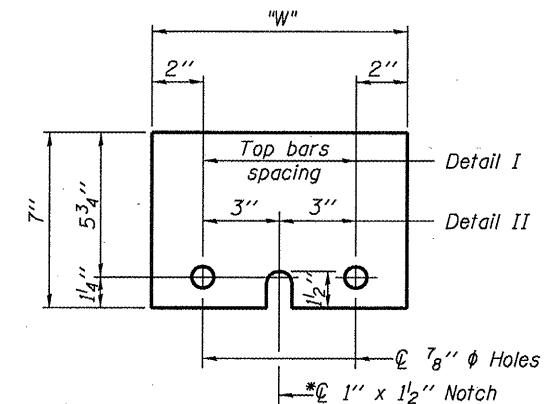
DETAIL I



DETAIL II

** Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"



STEEL RETAINER \bar{P} 1" x 7" x "W"

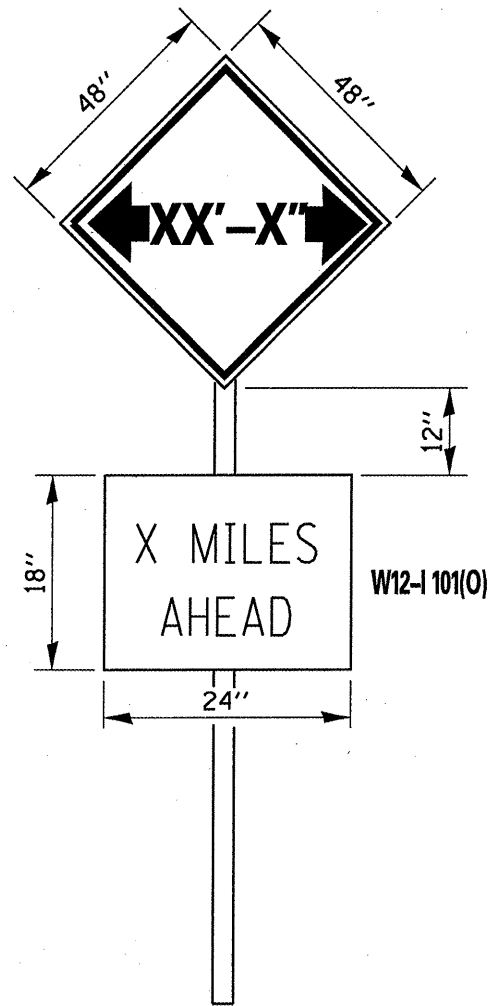
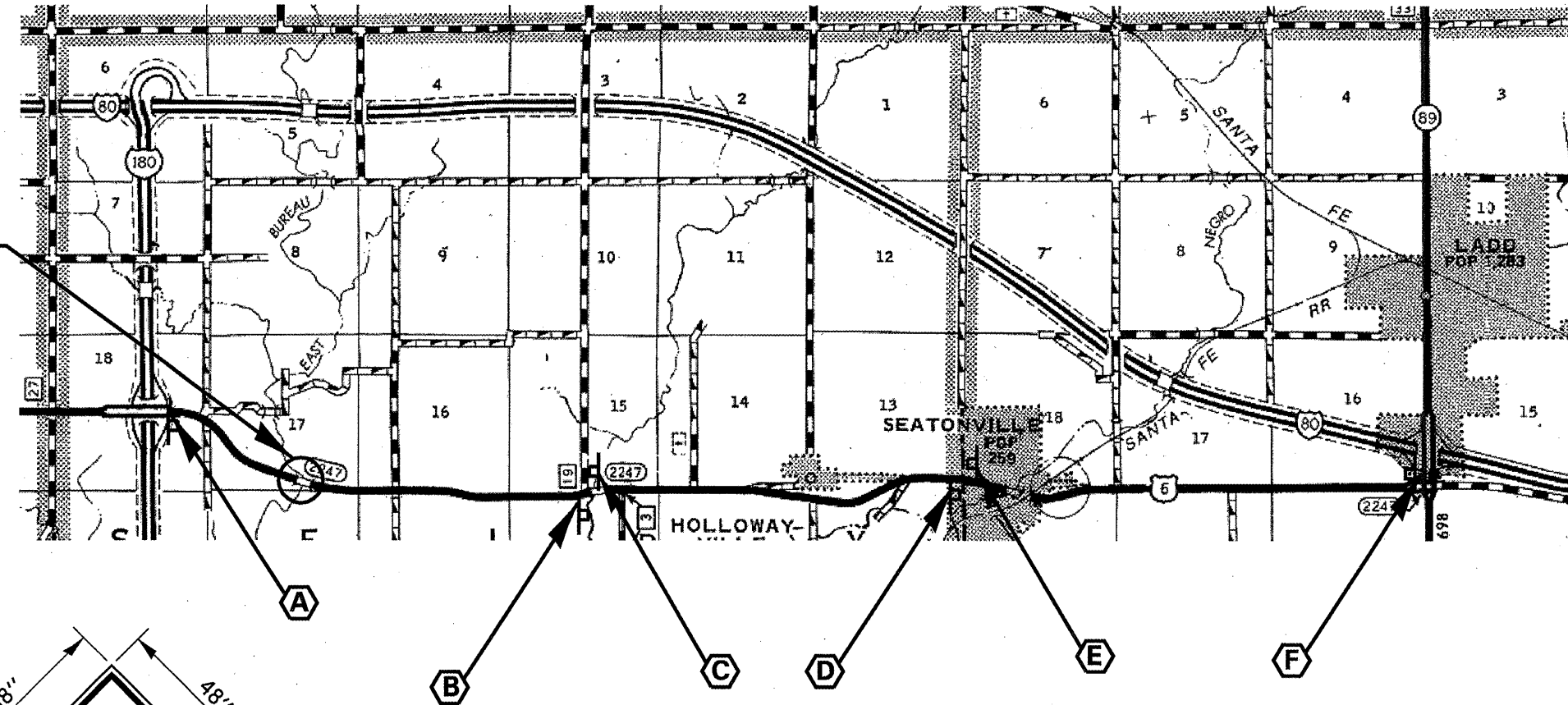
* Required only with Detail II

R-27

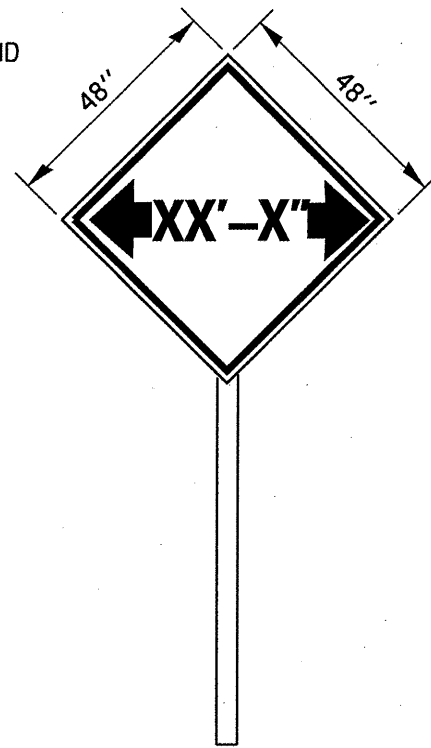
7-1-10

FILE NAME =	USER NAME = woodshenkr1	DESIGNED - RLW	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY CONCRETE BARRIER	F.A.S. RTE. 2247	SECTION 113C-BRII	COUNTY BUREAU	TOTAL SHEETS 14	SHEET NO. 7		
ci:\pw_work\p1dot\woodshenkr1\d0143186	MAIN1-0366A29-detail1.dgn	DRAWN - RLW	REVISED -			SCALE: _____	SHEET NO. 1 OF 1 SHEETS	STA. _____	TO STA. _____	CONTRACT NO. 66A29		
	PLOT SCALE = 1/8" = 1' IN.	CHECKED -	REVISED -			ILLINOIS FED. AID PROJECT						
	PLOT DATE = Dec 28, 2010 - 07:16:35 AM	DATE -	REVISED -									

**WIDTH RESTRICTION
SN 006-0137**



12" D BLACK LETTERING
WITH ORANGE BACKGROUND

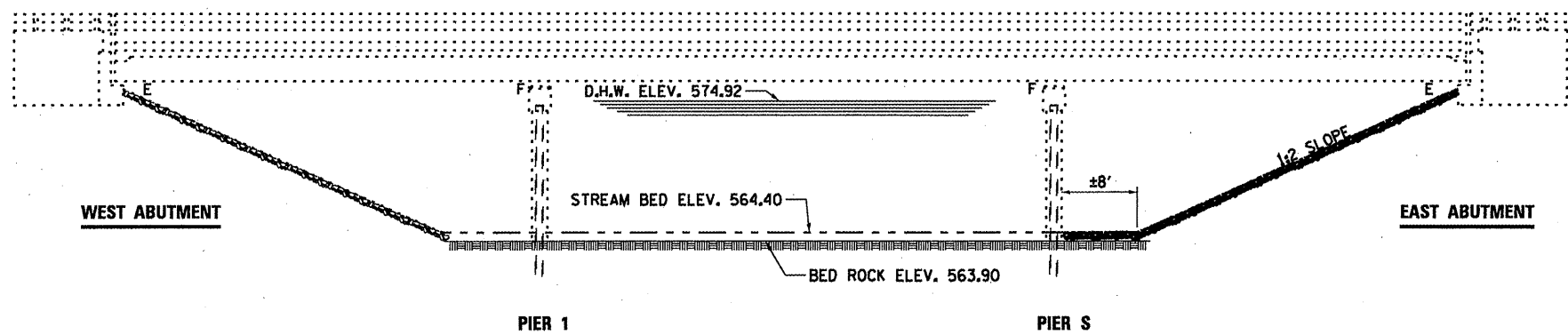


TO BE POST MOUNTED AS SHOWN ELSEWHERE IN THE PLANS.

THE ENGINEER WILL NOTIFY DISTRICT 3 BUREAU OF OPERATIONS 14 CALENDAR DAYS PRIOR TO INSTALLING ANY TRAFFIC CONTROL DEVICES THAT WILL RESTRICT THE PAVEMENT WIDTH.

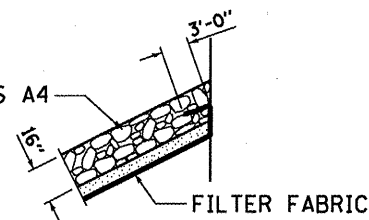
THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE ENGINEER TO MEET THIS REQUIREMENT.

WIDTH RESTRICTION SIGNING TABLE				
NO. OF SIGNS	TYPE OF SIGN	SIGN DESIGNATION	LOCATION	WIDTH RESTRICTION & DISTANCE
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	A	ON US 6 AT TOP OF I-180 EB OFF RAMP (FOR US 6 EB TRAFFIC)	10'-0" 1.1 MILES
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	B	ON US 6 SW CORNER AT 2625 E ST. (FOR US 6 EB TRAFFIC)	10'-0" 1.6 MILE
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	C	ON US 6 NE CORNER AT 2625 E ST. (FOR US 6 EB TRAFFIC)	10'-0" 1.6 MILE
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	D	ON US 6 SW CORNER AT CR 3000 E (FOR US 6 EB TRAFFIC)	10'-0" 4.4 MILE
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	E	ON US 6 NE CORNER AT CR 3000 E (FOR US 6 EB TRAFFIC)	10'-0" 4.4 MILE
1	WIDTH RESTRICTION W12-1-101-(0) M6-3	F	ON US 6 NW CORNER AT IL RTE. 89 FOR US 6 WB TRAFFIC)	10'-0" 7.4 MILE

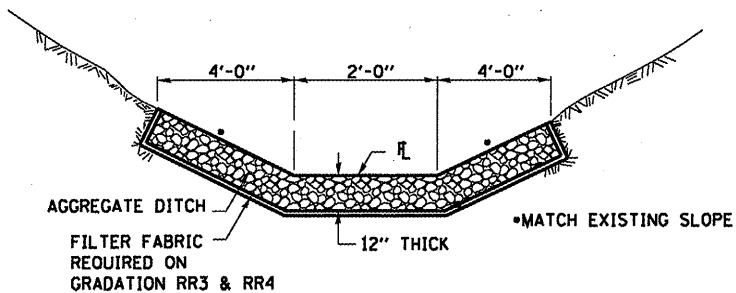


ELEVATION

STONE RIPRAP, CLASS A4



SECTION AT ABUTMENT



AGGREGATE DITCH SECTION
STA. 298+40 TO STA. 300+00 LT.

- (A)** REMOVE EXISTING RIPRAP, FILTER FABRIC AND REGRADE SLOPE, REPLACE FILTER FABRIC, AND RIPRAP W/BEDDING.
- (B)** GRADE EXISTING SLOPE, PLACE FILTER FABRIC AND RIPRAP W/BEDDING.

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC Deck planks.

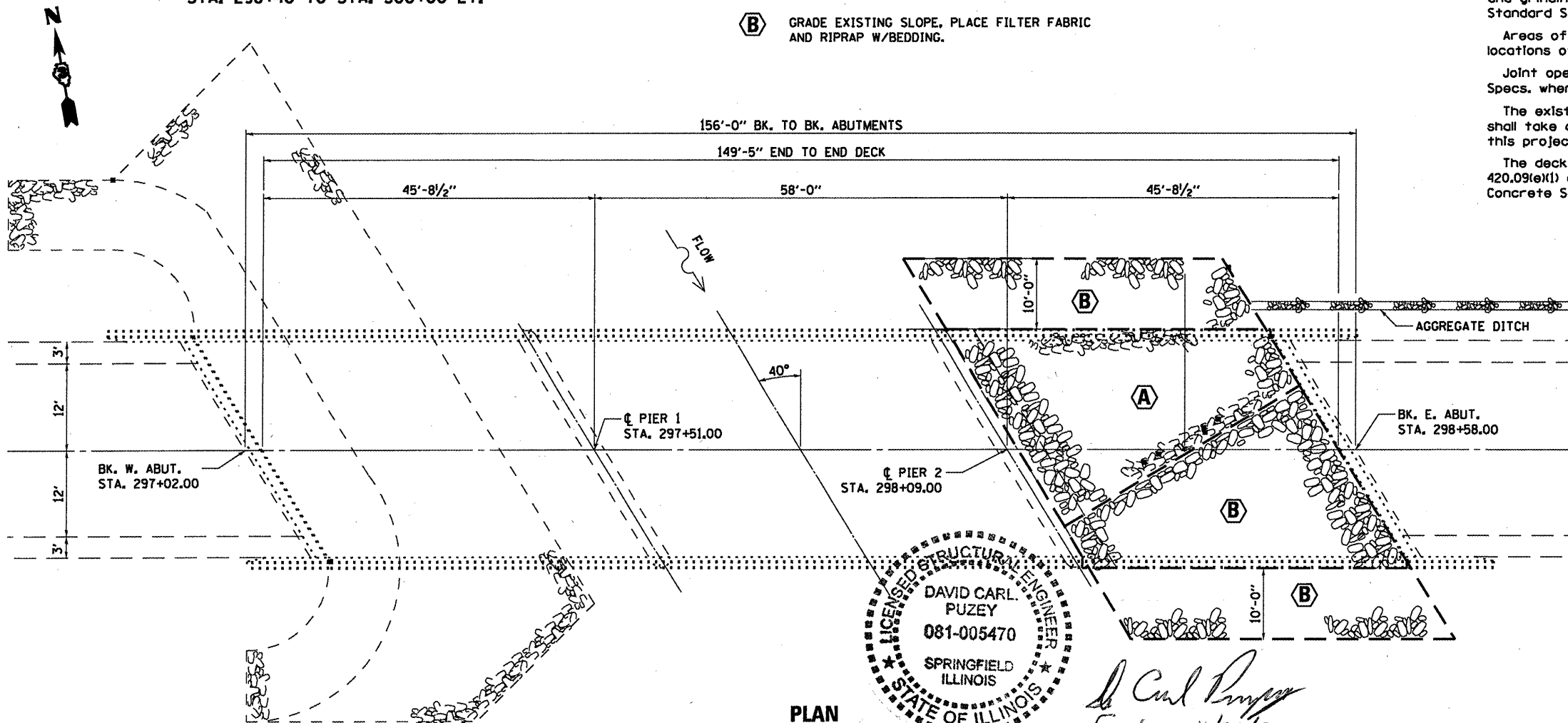
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that can not be removed by grinding 1/4" inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on As-built Plans.

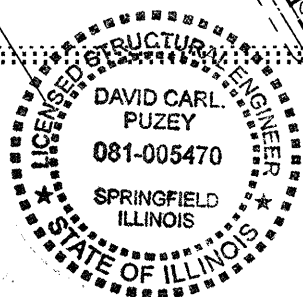
Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50° F.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete Superstructures.



PLAN



David Carl Puzey
Expires 11/30/2012

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
DECK SLAB REPAIR (PARTIAL)	SQ YD	50		50
CONCRETE REMOVAL	CU YD	11		11
CONCRETE SUPERSTRUCTURE	CU YD	4.2		4.2
CONCRETE STRUCTURE	CU YD	6.8		6.8
REINFORCEMENT BARS EPOXY COATED	POUND	2100		2100
PREFORMED JOINT STRIP SEAL	FOOT	86		86
BAR SPLICERS	EACH	26		26
STONE RIPRAP, CLASS A4	SQ YD	266		266
FILTER FABRIC	SQ YD	347		347

GENERAL PLAN AND ELEVATION
F.A.S. 2246 (US 6) SECTION (13C-BR1)
BUREAU COUNTY
STA. 297+80.00
S.N. 006-0137

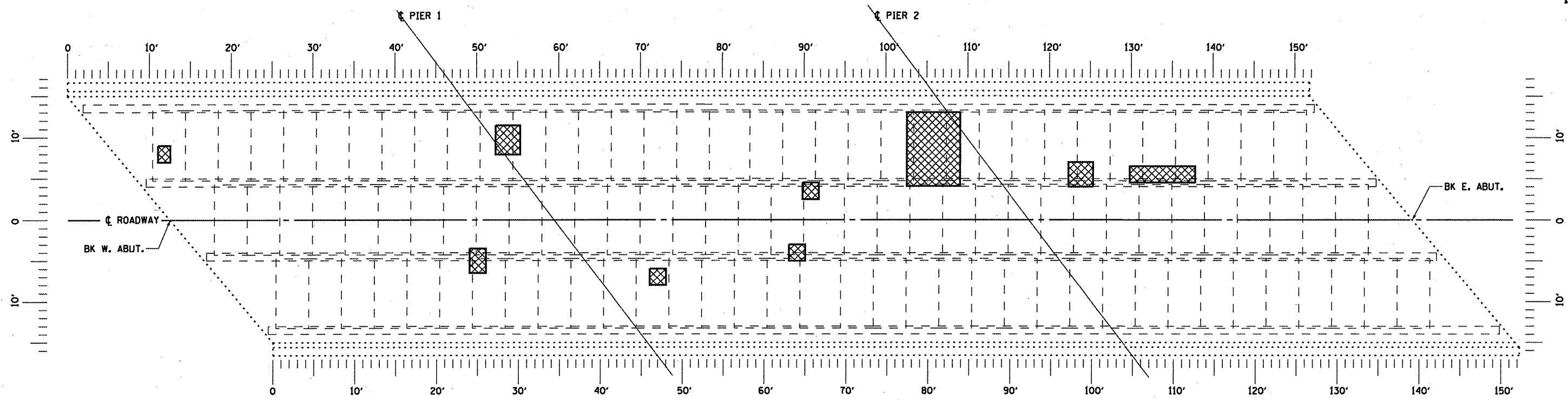
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MAIN11-0366A29-details.dgn		DRAWN - RLW	REVISED -
PLOT SCALE = 100.0000' / 1" IN.		CHECKED -	REVISED -
PLOT DATE = Dec 28, 2010 - 07:17:13 AM		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

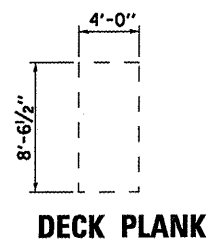
TYPICAL SECTIONS

SCALE: _____ SHEET NO. 1 OF 6 SHEETS STA. _____ TO STA. _____

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2247	(13C-BR1)	BUREAU	14	9
			CONTRACT NO. 66A29	
ILLINOIS FED. AID PROJECT				



PLAN

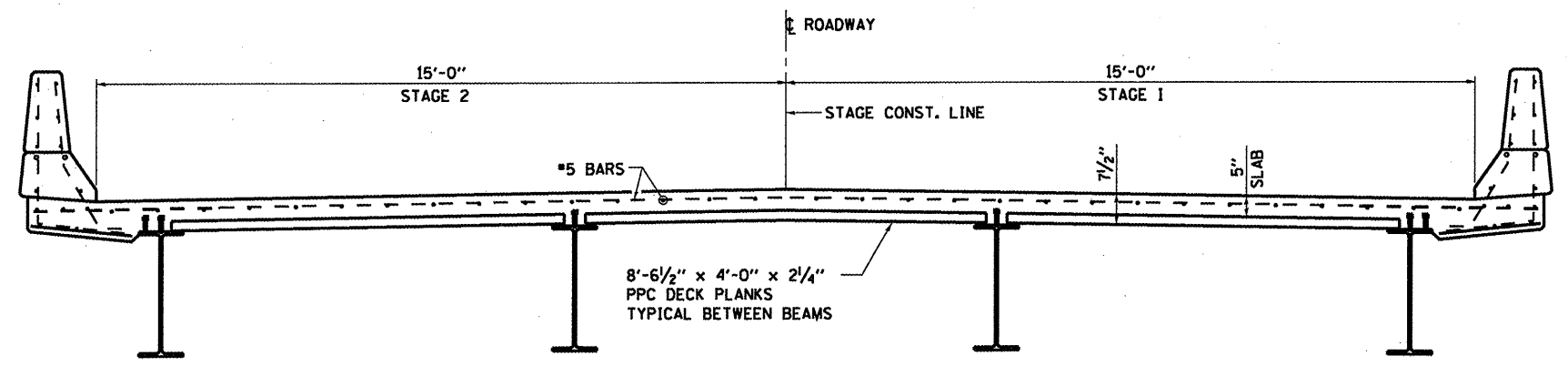


NOTES:

CROSSED HATCHED AREAS REPRESENTS "DECK SLAB REPAIR (PARTIAL)" AREAS.

DECK SLAB REPAIR (PARTIAL) AREAS ARE ESTIMATED. THE ENGINEER SHALL SOUND THE ENTIRE DECK SURFACE TO ESTABLISH EXACT LOCATIONS AND AREAS PRIOR TO BEGINNING REMOVAL OPERATIONS. THE QUANTITY OF PATCHING FOUND SHALL BE RECORDED ON THE "AS BUILT" SET OF PLANS.

THE CONTRACTOR SHALL EXERCISE EXTREME CARE DURING CONCRETE REMOVAL OPERATIONS SO THAT THE EXISTING PPC DECK PLANKS ARE NOT DAMAGED. IF THE CONTRACTOR'S REMOVAL OPERATIONS CAUSES DAMAGED TO THE DECK PLANKS, THE PLANKS SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.

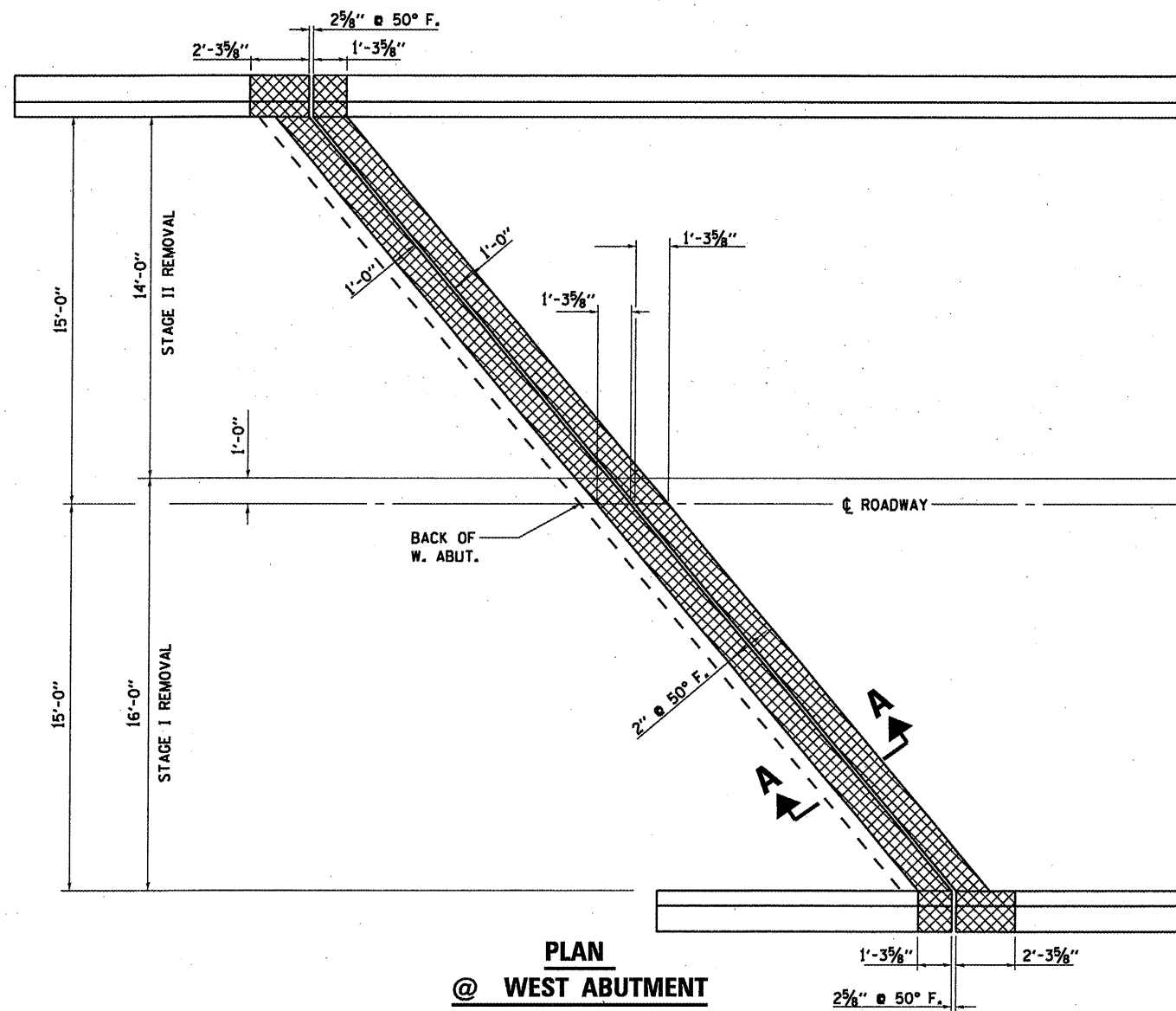


CROSS SECTION
LOOKING EAST

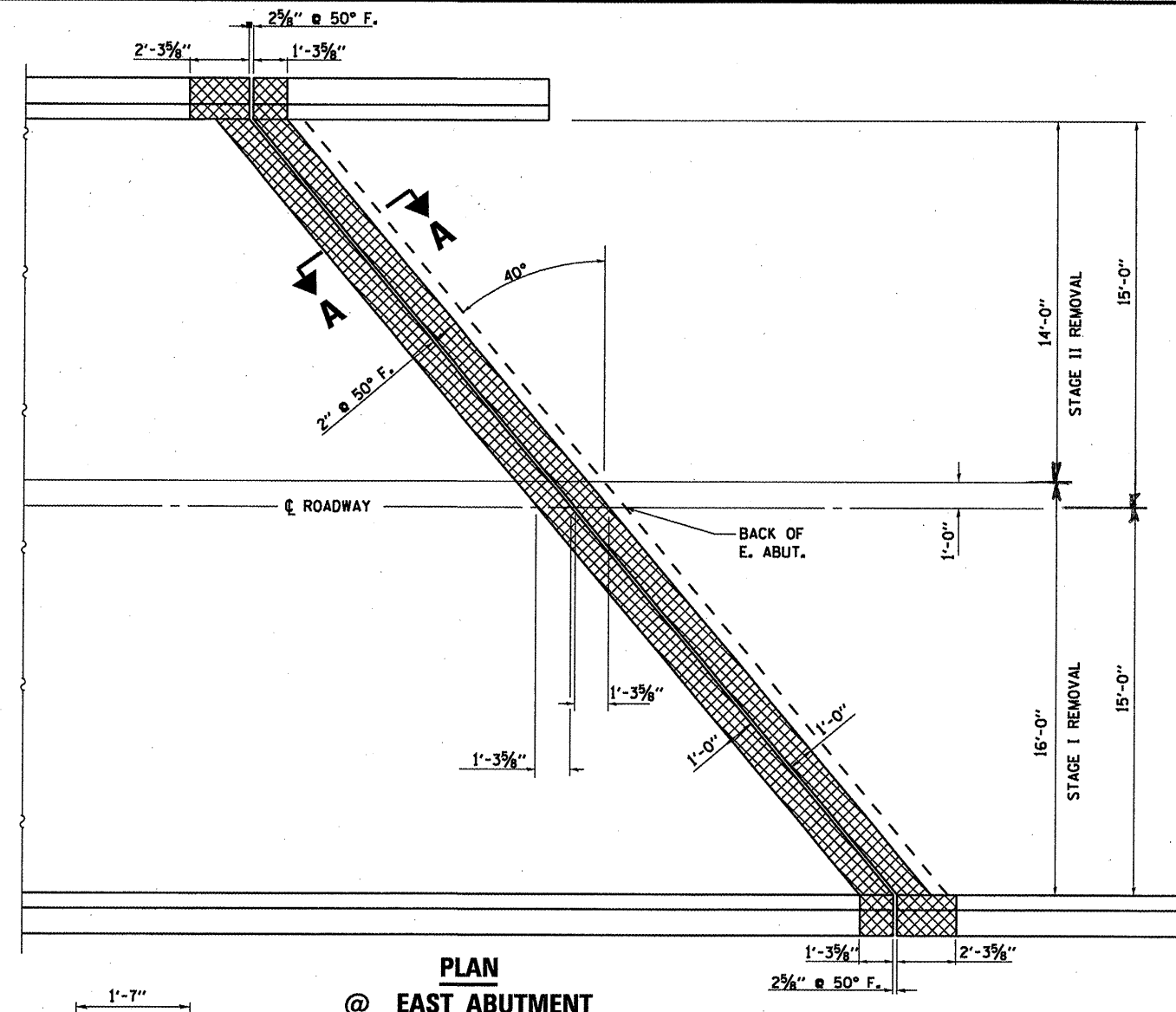
BILL OF MATERIAL

ITEM	UNIT	TOTAL
DECK SLAB REPAIR (PARTIAL)	50 YD	50

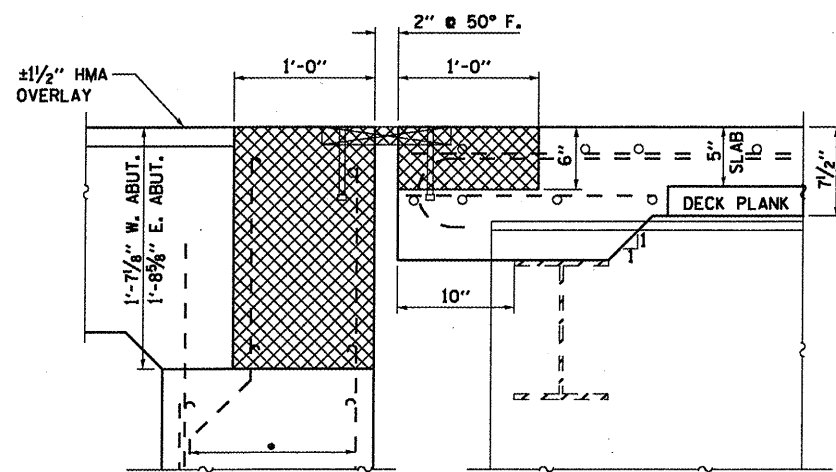
DECK SLAB REPAIR (PARTIAL)
F.A.S. 2246 (US 6) SECTION (13C-BR)I
BUREAU COUNTY
STA. 297+80.00
S.N. 006-0137



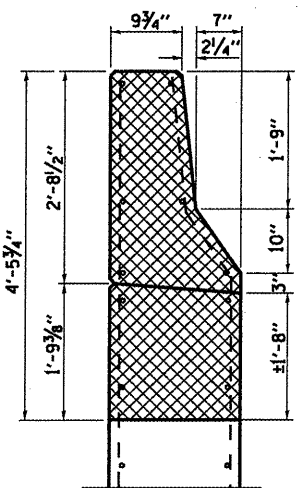
PLAN @ WEST ABUTMENT



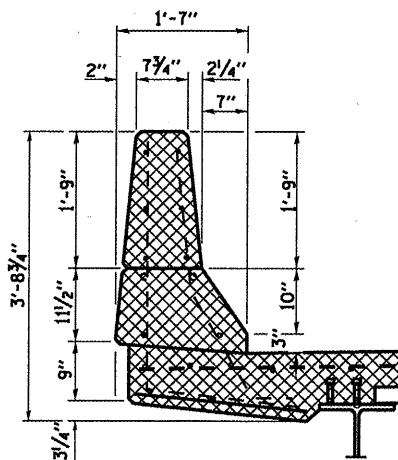
PLAN @ EAST ABUTMENT



SECTION A-A AT RT. L'S



WING WALL SECTION AT RT. L'S



PARAPET WALL SECTION AT RT. L'S

NOTE:
 CROSSED HATCHED AREA REPRESENTS CONCRETE REMOVAL AND REPLACEMENT AREAS.
 THE CONTRACTOR SHALL EXERCISE EXTREME CARE WHEN REMOVING AND REPLACING CONCRETE SO THAT THE EXISTING PPC DECK PLANKS ARE NOT DAMAGED. IF THE EXISTING DECK PLANKS ARE DAMAGED DUE TO THE CONTRACTOR'S OPERATION, THE DAMAGED PLANK SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
 COST FOR REMOVAL OF EXISTING EXPANSION JOINT MATERIALS SHALL BE INCLUDED WITH "CONCRETE REMOVAL"
 EXISTING SHEAR STUDS SHALL REMAIN IN PLACE WHEN PRESENT, IF THE SHEAR STUDS ARE DAMAGED OR WELDS CRACKED DUE TO THE CONTRACTOR'S REMOVAL OPERATIONS, DAMAGED STUDS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
CONCRETE REMOVAL	CU YD	11

EXPANSION JOINT REMOVAL DETAILS
 F.A.S. 2246 (US 6) SECTION (13C-BR1)
 BUREAU COUNTY
 STA. 297+80.00
 S.N. 006-0137

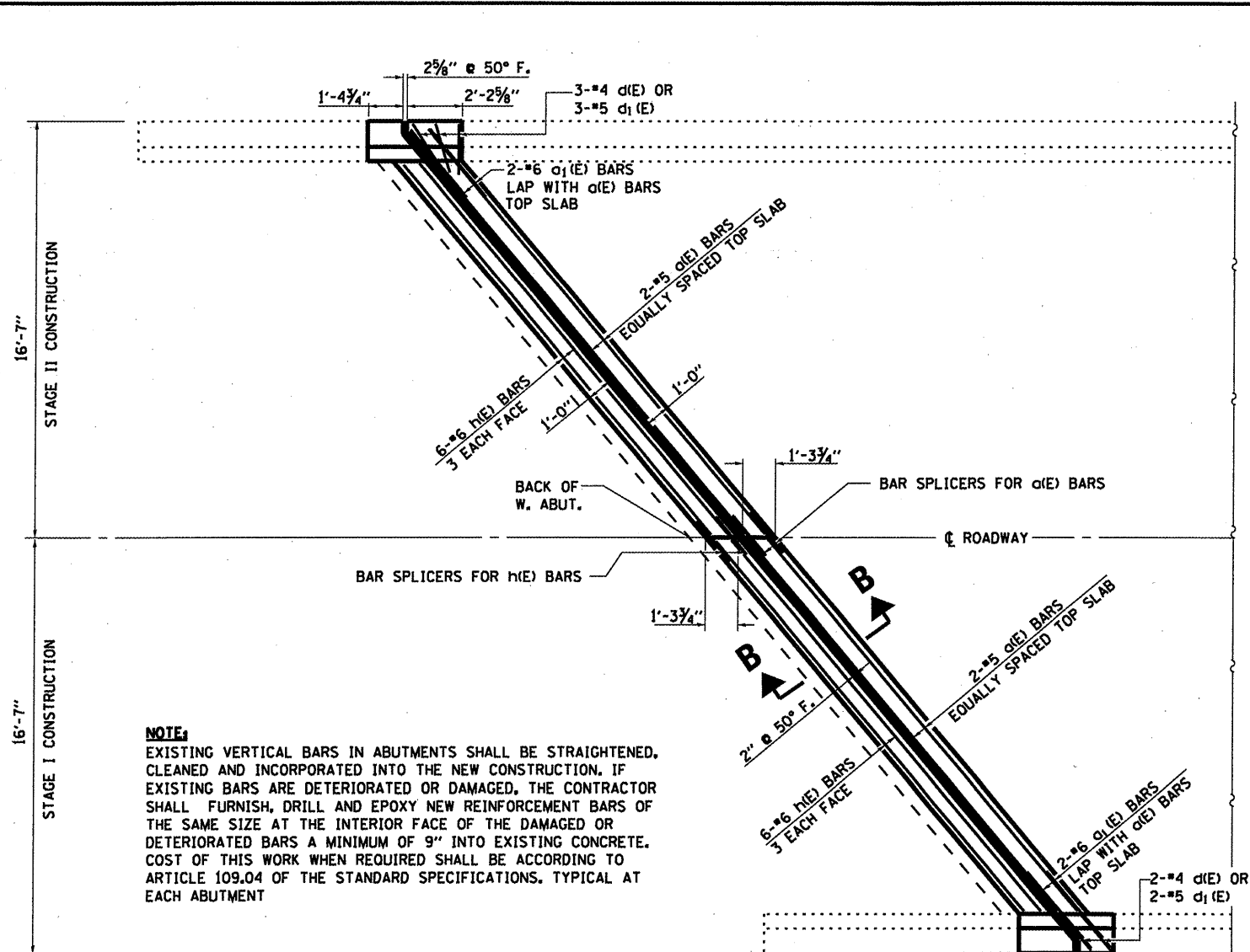
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXPANSION JOINT REMOVAL

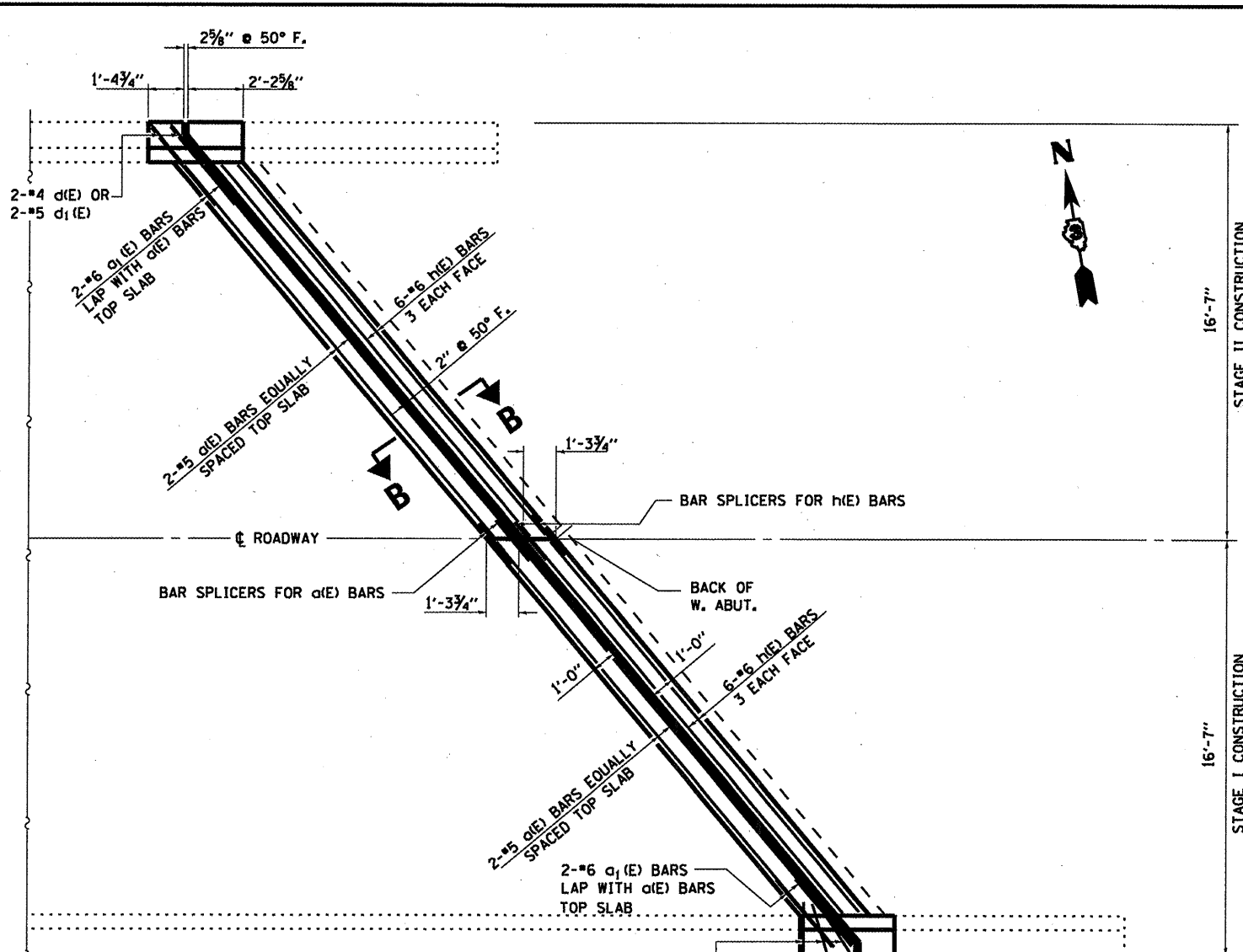
SCALE: SHEET NO. 3 OF 6 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 66A29				
ILLINOIS FED. AID PROJECT				

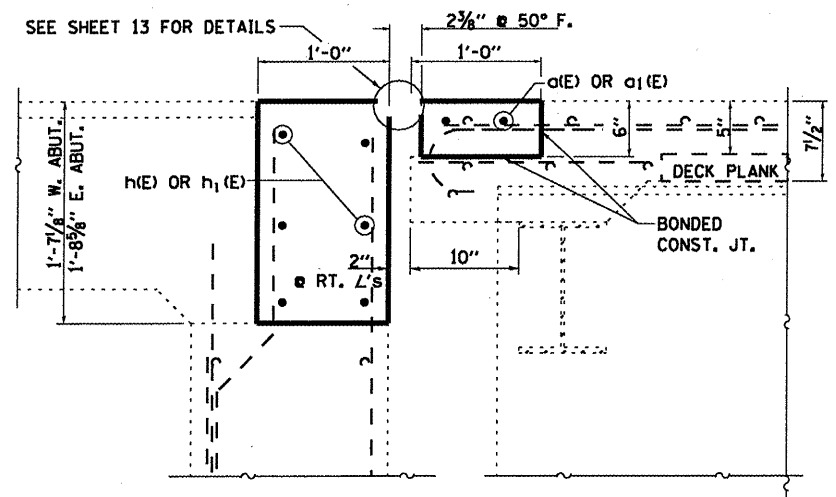


NOTE:
 EXISTING VERTICAL BARS IN ABUTMENTS SHALL BE STRAIGHTENED, CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION, IF EXISTING BARS ARE DETERIORATED OR DAMAGED, THE CONTRACTOR SHALL FURNISH, DRILL AND EPOXY NEW REINFORCEMENT BARS OF THE SAME SIZE AT THE INTERIOR FACE OF THE DAMAGED OR DETERIORATED BARS A MINIMUM OF 9" INTO EXISTING CONCRETE. COST OF THIS WORK WHEN REQUIRED SHALL BE ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, TYPICAL AT EACH ABUTMENT

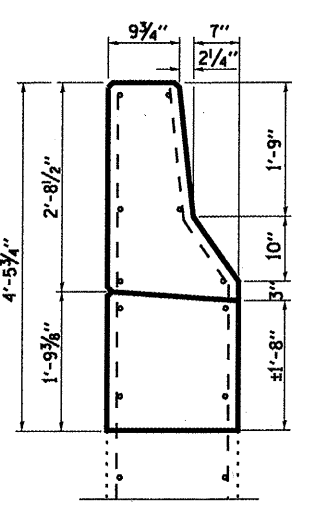
PLAN @ WEST ABUTMENT



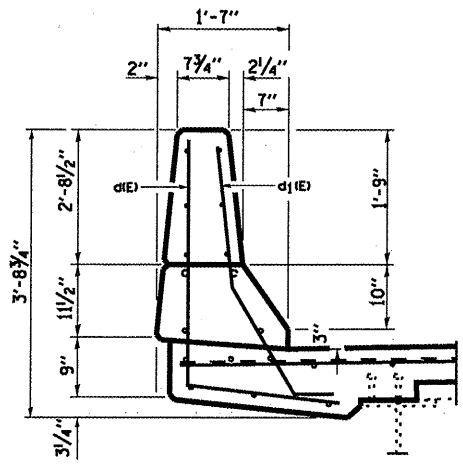
PLAN @ EAST ABUTMENT



SECTION B-B AT RT. L'S



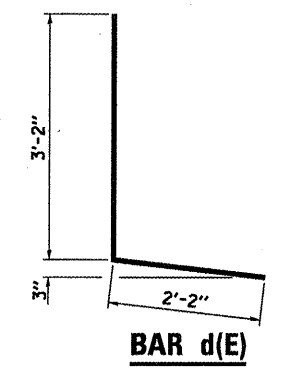
WING WALL SECTION AT RT. L'S



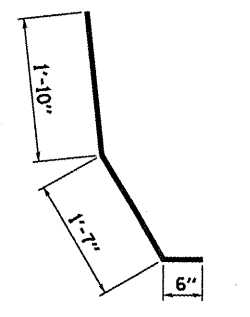
PARAPET WALL SECTION AT RT. L'S

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
α(E)	8	#5	20'-11"	—
α1(E)	8	#6	4'-0"	—
d(E)	10	#4	5'-4"	—
d1(E)	10	#5	3'-11"	—
h(E)	24	#6	19'-3"	—
Reinforcement Bars, Epoxy Coated		Pound	2100	
Concrete Superstructure		Cu. Yds.	4.2	
Concrete Structure		Cu. Yds.	6.8	

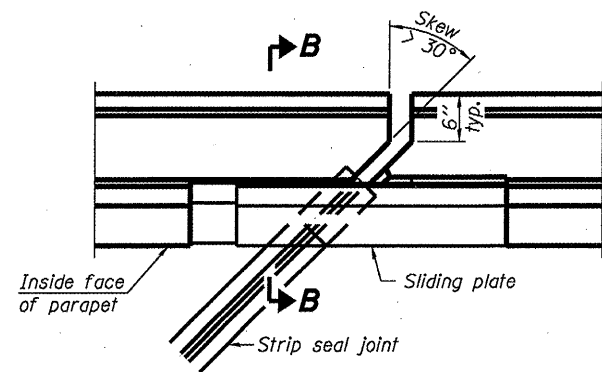


BAR d(E)

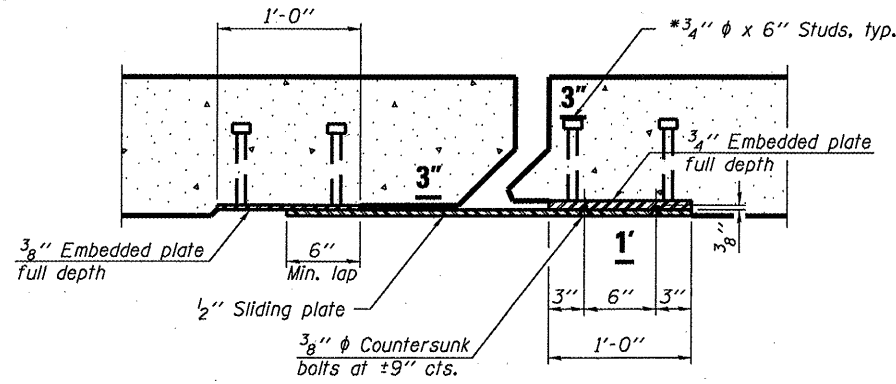


BAR d1(E)

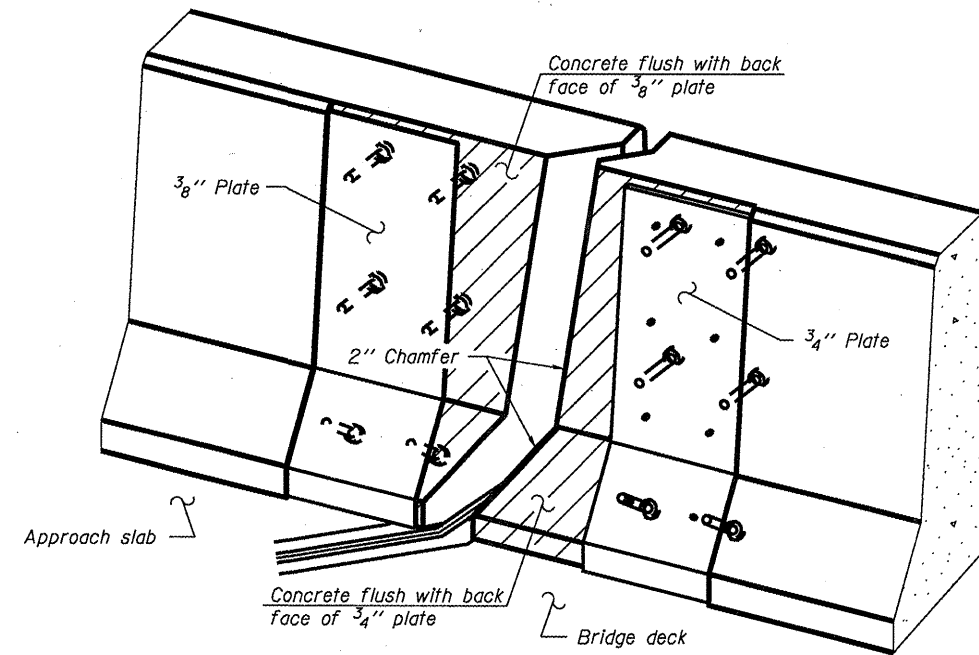
EXPANSION JOINT REPLACEMENT DETAILS
 F.A.S. 2246 (US 6) SECTION (13C-BR1)
 BUREAU COUNTY
 STA. 297+80.00
 S.N. 006-0137



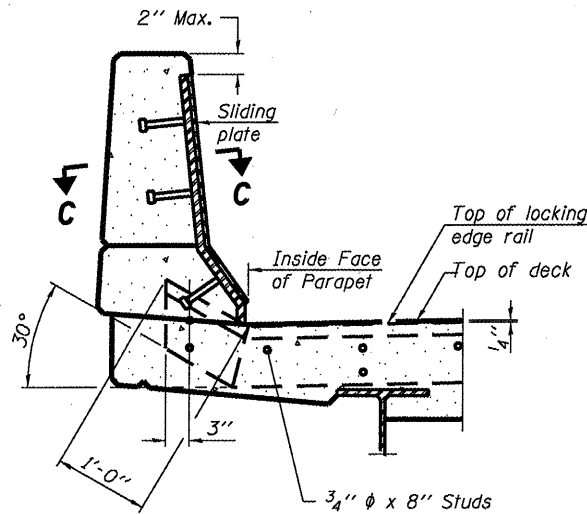
PLAN
(For skews > 30°)
Showing point block



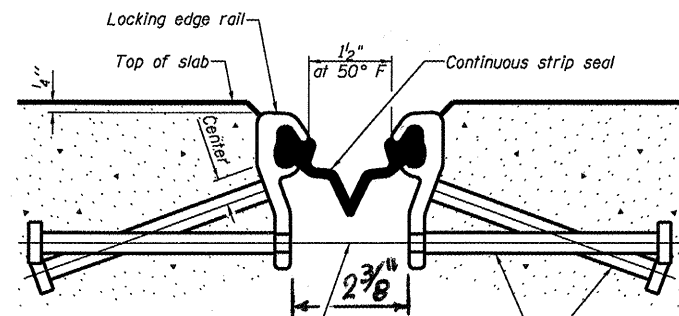
SECTION C-C



TRIMETRIC VIEW
(Showing back plates only)



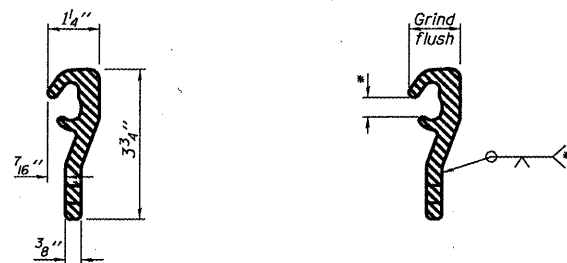
SECTION B-B



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" ϕ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

SECTION THRU STRIP SEAL JOINT FOR OVERLAY OVER DECK BEAMS



LOCKING EDGE RAIL

LOCKING EDGE RAIL SPLICE

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
PREFORMED JOINT STRIP SEAL	FOOT	86

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MAIN1-0366A29-details.dgn	
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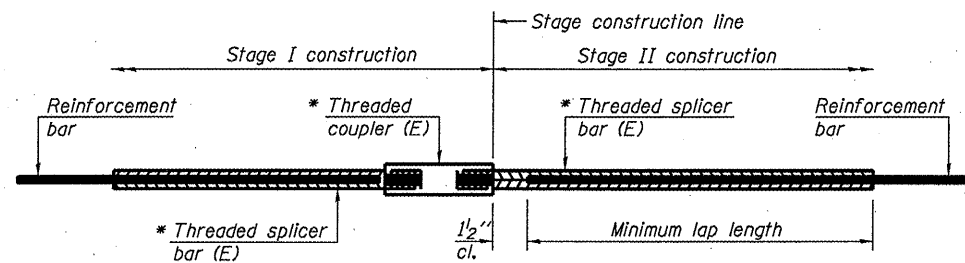
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DRAWN - RLW	REVISIONS
CHECKED -	REVISIONS
DATE -	REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS

SCALE: SHEET NO. 5 OF 6 SHEETS STA. TO STA.

F.A.S. RTE. 2247	SECTION (13C-BR1)	COUNTY BUREAU	TOTAL SHEETS 14	SHEET NO. 13
CONTRACT NO. 66A29				
ILLINOIS FED. AID PROJECT				



STANDARD BAR SPLICER ASSEMBLY

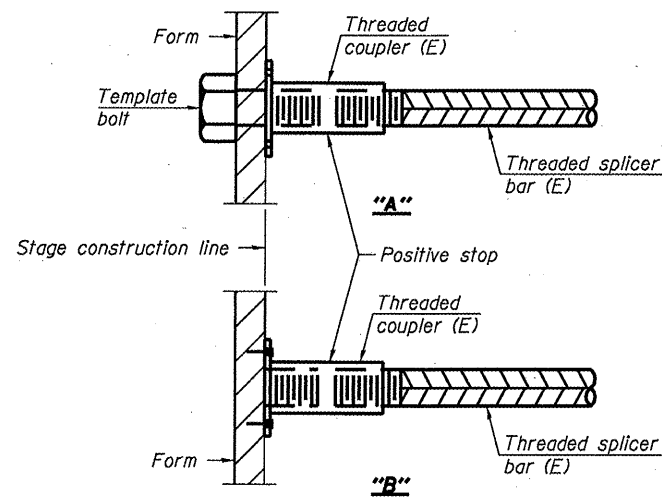
Bar size to be spliced	Minimum Lap Lengths				
	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

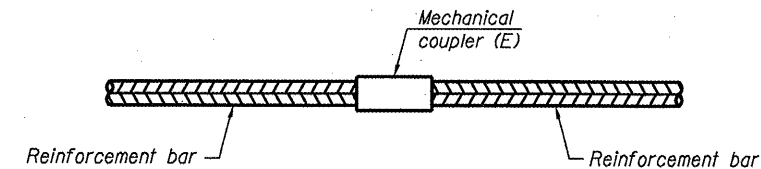
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length
W. ABUT. DECK	#5	7	3
W. ABUT. END DAM	#6	6	3
E. ABUT. DECK	#5	7	3
E. ABUT. END DAM	#6	6	3
TOTAL		26	



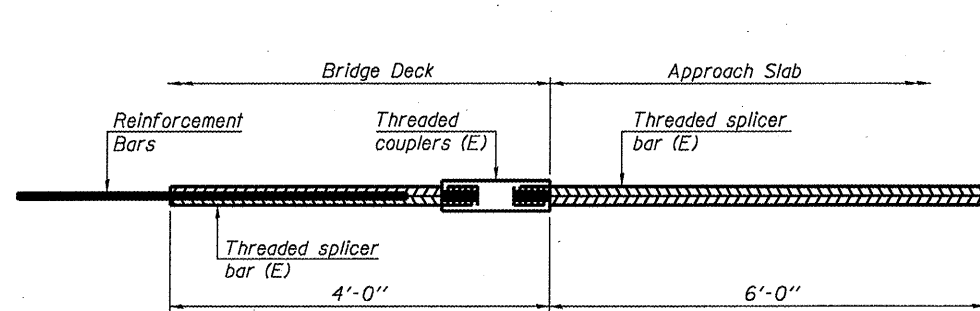
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



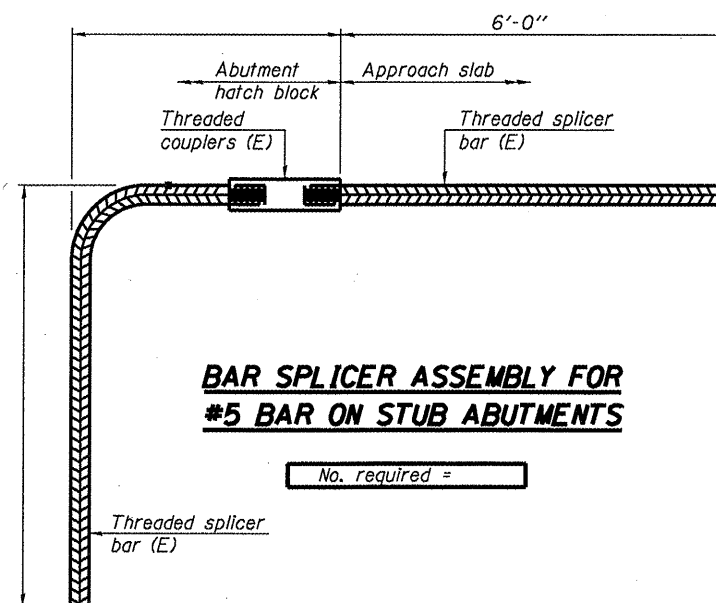
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

FILE NAME =	USER NAME = woodshankr1	DESIGNED - RLW	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS

SCALE: SHEET NO. 6 OF 6 SHEETS STA. TO STA.

F.A.S. RTE. 2247	SECTION (13C-BR1)	COUNTY BUREAU	TOTAL SHEETS 14	SHEET NO. 14
			CONTRACT NO. 66A29	
ILLINOIS FED. AID PROJECT				