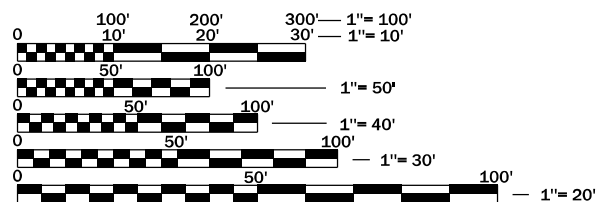


INDEX OF SHEETS

- 1 COVER SHEET
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- 113 - 130 DETAILS

SEE SHEET 2 FOR LIST OF HIGHWAY STANDARDS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

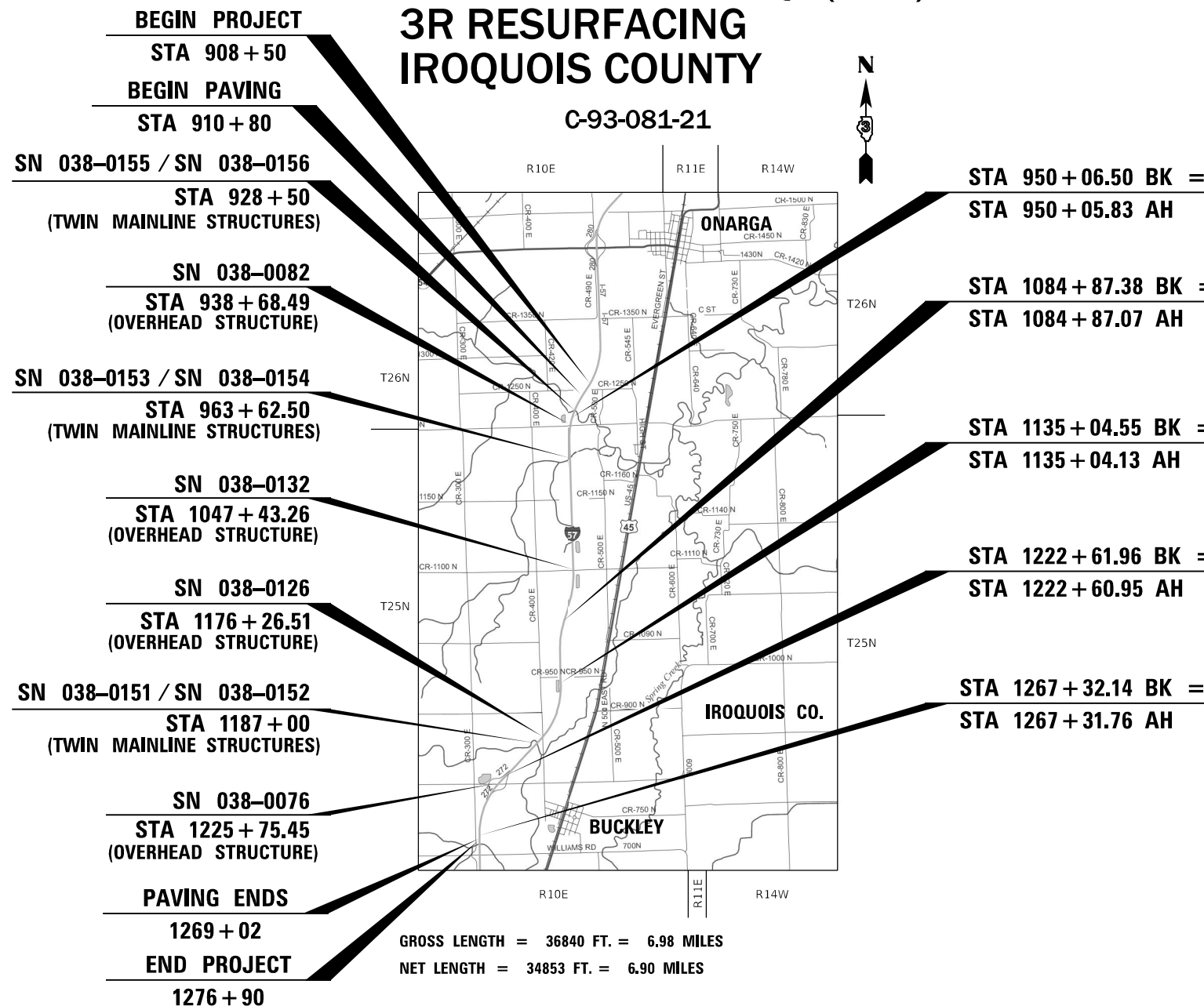
J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: BRAD DUNCAN, P.E.
UNIT CHIEF: LUIS CALDERON, P.E.
DISTRICT 3 NO. (815) 434-6131
CONTRACT NO. 66H63

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

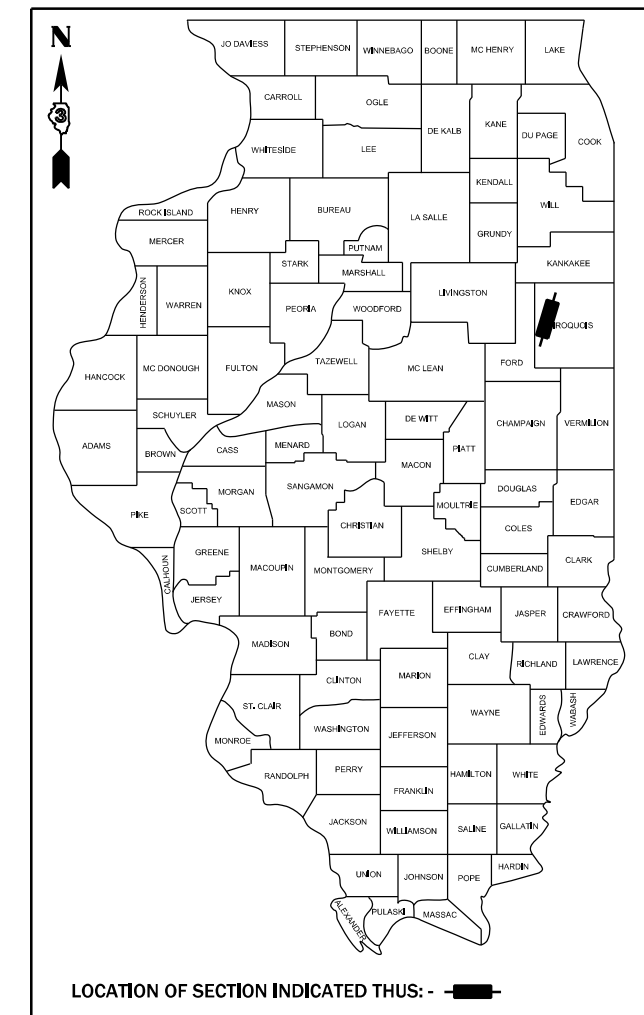
PROPOSED
HIGHWAY PLANS

FAI ROUTE 57 (I-57)
SECTION (38-7,38-6)RS
PROJECT NHPP-2DQR(624)
3R RESURFACING
IROQUOIS COUNTY



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	1
		ILLINOIS	CONTRACT NO. 66H63	

D-93-045-21



FUNCTIONAL CLASSIFICATION
INTERSTATE
 2019 ADT = 17505
 PV = 62.5% SU = 7.4% MU = 30.1%

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED December 17, 20 21

David Arnold
 REGIONAL ENGINEER

February 4, 20 22
[Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT

February 4, 20 22
Stephen M. Smith
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

- 1) EXCEPT AS NOTED ON THE PLANS, PAVEMENT GRADES SHOWN ARE AT THE TOP OF PAVEMENT SURFACES.
- 2) BEFORE ORDERING PIPE CULVERTS OR PIPE DRAINS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.
- 3) THE ENGINEER WILL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS HMA LIFTS.
- 4) THE FINISHED EARTHWORK SHALL HAVE A VEGETATION SUSTAINING SOIL COVERING THE TOP FOUR INCHES (100 MILLIMETERS) IN AREAS TO BE SEEDED OR SODDED. THE VEGETATION SUSTAINING SOIL REQUIRED WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.
- 5) ALL EXCAVATED MATERIAL, WHICH INCLUDES DIGGING OR GRADING OF ANY SOIL OR FILL MATERIAL, WITH THE EXCEPTION OF AGGREGATE FILLS, MUST BE INCORPORATED WITHIN THE IDOT RIGHT OF WAY.
- 6) SHORT TERM PAVEMENT MARKING SHALL BE USED TO OUTLINE EXIT AND ENTRANCE RAMPS FOR THE PRIME COAT APPLICATION AND EACH RESURFACING LIFT.
- 7) ON EXISTING PAVEMENT WHICH MAY BE SUPERELEVATED, THE NEW HMA PAVEMENT SHALL BE BUILT WITH THE SAME SUPERELEVATION UNLESS NEW SUPERELEVATION RATES ARE GIVEN ON THE PLANS.
- 8) ALL ELEVATIONS REFERRING TO U.S.G.S. MEAN SEA LEVEL DATUM.
- 9) THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05	TONS / CU YD
HMA RESURFACING	112	LBS / SQ YD / IN
SHORT TERM PAVEMENT MARKING	10	FT /100 FT OF APPLICATION
MIX FOR CRACKS, JTS & FLGWYS	0.0003	TONS / SQ YD
BINDER (HAND METHOD)	0.0005	TONS / SQ YD

- 10) THE WORK REQUIRED TO CONNECT ANY SEWER TO AN EXISTING DRAINAGE STRUCTURE OR PIPE WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE SEWER ITEMS.
- 11) MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

AMEREN	WINDSTREAM
AT&T	METRO COMMUNICATIONS
EASTERN ILLINI ELECTRIC	FRONTIER
CMS	MARATHON PIPELINE
- 12) NON-MEMBERS OF JULIE KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

IDOT (INTERCHANGE LIGHTING)
- 13) **TREE REMOVAL** - NO ENVIRONMENTAL RESTRICTIONS EXIST FOR TREE REMOVAL WITHIN PROJECT LIMITS.

COMMITMENTS

- 1) ENVIRONMENTAL COORDINATION
- 2) PROVIDE AS-BUILTS FOR UNDERDRAIN INSTALLATION & BRIDGE REPAIRS
- 3) DISTRICT OPERATIONS TO BE INFORMED 21 DAYS PRIOR BUCKLEY INTERCHANGE RAMP NIGHTLY CLOSURES
- 4) LEFTOVER EXCAVATED SOIL MATERIAL CLASSIFIED AS PER ARTICLE 669.05(a)(1) THAT CANNOT BE UTILIZED WITHIN ROW SHALL BE PLACED IN BUCKLEY INTERCHANGE INFIELDS

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

000001-08 001001-02 001006 280001-07 542301-03 601001-05 601101-02 606001-08 630001-12 631031-17 631032-09 635001-02 642001-03 643001-02 665001-02 701101-05 701106-02 701301-04 701400-11 701401-13 701402-12 701411-09 701426-09 701428-01 701451-05 701456-05 701901-08 704001-08 720001-01 720006-04 720011-01 725001-01 728001-01 780001-05 781001-04 782001-01 782006-01	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS AREAS OF REINFORCEMENT BARS DECIMAL OF AN INCH AND OF A FOOT TEMPORARY EROSION CONTROL SYSTEMS PRECAST REINFORCED CONCRETE FLARED END SECTION PIPE UNDERDRAINS CONCRETE HEADWALL FOR PIPE UNDERDRAIN CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER STEEL PLATE BEAM GUARDRAIL TRAFFIC BARRIER TERMINAL, TYPE 6 TRAFFIC BARRIER TERMINAL, TYPE 6A DELINEATORS SHOULDER RUMBLE STRIPS, 16 INCH SAND MODULE IMPACT ATTENUATORS WOVEN WIRE FENCE OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE OFF-ROAD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY LANE CLOSURE, FREEWAY/EXPRESSWAY LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH BARRIER LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP FOR SPEEDS ≥ 45 MPH LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≥ 45 MPH TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY RAMP CLOSURE FREEWAY/EXPRESSWAY PARTIAL EXIT RAMP CLOSURE FREEWAY/EXPRESSWAY TRAFFIC CONTROL DEVICES TEMPORARY CONCRETE BARRIER SIGN PANEL MOUNTING DETAILS SIGN PANEL ERECTION DETAILS METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS OBJECT AND TERMINAL MARKERS TELESCOPING STEEL SIGN SUPPORT TYPICAL PAVEMENT MARKINGS TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS PRISMATIC CURB REFLECTORS GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
--	--

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DISTRICT THREE
AS BUILT INFORMATION

SUPERVISING CONSTRUCTION FIELD ENGINEER

RESIDENT ENGINEER / TECHNICIAN

START & END DATES
OF CONSTRUCTION: _____

INSPECTORS: _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DISTRICT THREE

PREPARED BY: _____
DISTRICT STUDIES & PLANS ENGINEER

DATE: _____

EXAMINED BY: _____
DISTRICT CONSTRUCTION ENGINEER

DISTRICT MATERIALS ENGINEER

DISTRICT OPERATIONS ENGINEER

FILE NAME =	USER NAME = callderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES AND COMMITMENTS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw_work\pwwd\callderont\0521537\D366H63-ehb-cove.dgn		DRAWN -	REVISED -			57	(38-7,38-6)RS	IROQUOIS	130	2	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO



BEGIN PROJECT
 STA 909+00 (UNDERDRAIN)
 STA 910+80 (MAINLINE PAVEMENT)

24" RCCP CULVERT-#3905
 STA 915+00

24" RCCP CULVERT-#3904
 STA 925+50

24" RCCP CULVERT-#3903
 STA 931+50

24" RCCP CULVERT-#3900
 STA 941+00

24" RCCP CULVERT-#3899
 STA 961+25

24" RCCP CULVERT
 STA 965+75

24" RCCP CULVERT-#3898
 STA 975+00

E 1200 N

STA 950+06.50 BK =
 STA 950+05.83 AH

SN 038-0082

SN 038-0155 & SN 038-0156
 CL STA 928+50

24" RCCP CULVERT-#3893
 STA 1041+00

24" RCCP CULVERT-#3894
 STA 1023+00

36" RCCP CULVERT-#3895
 STA 1012+25

24" RCCP CULVERT-#3896
 STA 1005+00

24" RCCP CULVERT-#3897
 STA 992+00

SN 038-0153 & SN 038-0154
 CL STA 963+62.5

E 1100 N

24" RCCP CULVERT-#3890
 STA 1051+00

24" RCCP CULVERT-#3889
 STA 1060+00

36" RCCP CULVERT-#3888
 STA 1068+00

24" RCCP CULVERT-#3887
 STA 1074+00

24" RCCP CULVERT-#3886
 STA 1083+00

STA 1084+87.38 BK =
 STA 1084+87.07 AH

STA 1135+04.55 BK =
 STA 1135+04.13 AH

24" RCCP CULVERT-#3879
 STA 1164+00

24" RCCP CULVERT-#3878
 STA 1174+00

SN 038-0126

24" RCCP CULVERT-#3873
 STA 1217+00

STA 1222+61.96 BK =
 STA 1222+60.95 AH

24" RCCP CULVERT-#3871
 STA 01+50

36" RCCP CULVERT-#3870
 STA 19+20

SN 038-0076

24" RCCP CULVERT-#3866
 STA 1262+00

STA 1267+32.14 BK =
 STA 1267+31.76 AH



N 400 E

24" RCCP CULVERT-#3880
 STA 1135+00

24" RCCP CULVERT-#3881
 STA 1128+47

24" RCCP CULVERT-#3882
 STA 1115+00

24" RCCP CULVERT-#3883
 STA 1108+00

36" RCCP CULVERT-#3884
 STA 1102+27

48" RCCP CULVERT-#3885
 STA 1093+00

24" RCCP CULVERT-#3874
 STA 1205+00

24" RCCP CULVERT-#3875
 STA 1190+00

SN 038-0151 & SN 038-0152
 CL STA 1187+00

24" RCCP CULVERT-#3876
 STA 1181+29

24" RCCP CULVERT-#3867
 STA 1238+00

36" RCCP CULVERT-#3869
 STA 1+20

24" RCCP CULVERT-#3872
 STA 09+50

END PROJECT
 STA 1269+02 (MAINLINE)
 STA 1271+88 (UNDERDRAIN)

MODEL: \\MODELS\MH\MS
 FILE: \\MODELS\CON\work\shelton\calderon\0521537D\366H63-shr-cover.dwg

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

LOCATION MAP

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	3
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	NHPP	NHPP	NHPP	NHPP	NHPP	NHPP
				90%FED/10%STATE	90%FED/10%STATE	90%FED/10%STATE	90%FED/10%STATE	90%FED/10%STATE	90%FED/10%STATE
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE
				0005	0013	0013	0013	0013	0013
				RURAL	038-0132	038-0126	038-0076	038-0151	038-0152
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	60	60					
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	160	160					
20200100	EARTH EXCAVATION	CU YD	206	206					
20400800	FURNISHED EXCAVATION	CU YD	90	90					
21400100	GRADING AND SHAPING DITCHES	FOOT	13377	13377					
25000200	SEEDING, CLASS 2	ACRE	1.25	1.25					
25000210	SEEDING, CLASS 2A	ACRE	3	3					
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	390	390					
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	390	390					
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	390	390					
25100115	MULCH, METHOD 2	ACRE	1.25	1.25					
25100630	EROSION CONTROL BLANKET	SQ YD	14908	14908					
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	310	310					
28000305	TEMPORARY DITCH CHECKS	FOOT	1180	1180					

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005	0013	0013	0013	0013	0013	
				038-0132	038-0126	038-0076	038-0151	038-0152		
28000400	PERIMETER EROSION BARRIER	FOOT	1730	1730						
28100107	STONE RIPRAP, CLASS A4	SQ YD	96	96						
28200200	FILTER FABRIC	SQ YD	96	96						
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	269642	269642						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	60.4	60.4						
40600902	HOT-MIX ASPHALT BINDER COURSE (HAND METHOD), N90	TON	93	93						
40600907	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE (HAND METHOD), N90	TON	6	6						
40600990	TEMPORARY RAMP	SQ YD	1276	1276						
40602990	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N90	TON	16391	16391						
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	13597	13597						
40603209	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N90	TON	17211	17211						
40603235	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	1934	1934						
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	14164	14164						
40604162	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	774	774						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
40604164	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N90	TON	1079	1079						
40605034	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 12.5, MIX "E", N80	TON	20886	20886						
40733705	LONGITUDINAL JOINT SEALANT	FOOT	142061	142061						
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	36	36						
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	47139	47139						
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	434	434						
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	46950	46950						
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	8178	8178						
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	125319	125319						
44000169	HOT-MIX ASPHALT SURFACE REMOVAL, 5"	SQ YD	124928	124928						
44000171	HOT-MIX ASPHALT SURFACE REMOVAL, 5 1/2"	SQ YD	9207	9207						
44000300	CURB REMOVAL	FOOT	140	140						
44200583	CLASS A PATCHES, TYPE IV, 12 INCH	SQ YD	1656	1656						
44200610	CLASS A PATCHES, TYPE II, 13 INCH	SQ YD	307	307						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
44200612	CLASS A PATCHES, TYPE III, 13 INCH	SQ YD	54	54						
44213000	PATCHING REINFORCEMENT	SQ YD	2017	2017						
44213200	SAW CUTS	FOOT	3558	3558						
44213204	TIE BARS 3/4"	EACH	2239	2239						
48101200	AGGREGATE SHOULDERS, TYPE B	TON	872	872						
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	4834	4834						
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	467	467						
50102400	CONCRETE REMOVAL	CU YD	6.7					3.1	3.6	
50300225	CONCRETE STRUCTURES	CU YD	20.9		3.7	7.8	9.4			
50300300	PROTECTIVE COAT	SQ YD	85.6		12.5	31.9	41.2			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2384		362	924	1098			
50900200	STEEL RAILING, TYPE 2399	FOOT	184					91	93	
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 24"	EACH	3	3						
550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	32	32						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	9	9						
55101100	STORM SEWER REMOVAL 21"	FOOT	32	32						
55101200	STORM SEWER REMOVAL 24"	FOOT	9	9						
60100060	CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	305	305						
60100070	SHOULDER REMOVAL AND REPLACEMENT	FOOT	155073	155073						
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	5932	5932						
60108501	PIPE UNDERDRAINS, TYPE 3	FOOT	151980	151980						
60600605	CONCRETE CURB, TYPE B	FOOT	140	140						
60920036	PIPE CULVERTS TO BE CLEANED 36"	FOOT	518	518						
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	4300	4300						
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	16	16						
* 63100087	TRAFFIC BARRIER TERMINAL, TYPE 6A	EACH	2	2						
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	18	18						
63200310	GUARDRAIL REMOVAL	FOOT	6105	6105						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
63500105	DELINEATORS	EACH	286	286						
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	137829	137829						
66500105	WOVEN WIRE FENCE, 4'	FOOT	5000	5000						
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	100	100						
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1						
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1						
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1						
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	30	30						
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12						
67100100	MOBILIZATION	L SUM	1	1						
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	4	4						
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1						
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1						
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	50	50						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1						
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2142	2142						
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	476	476						
70302120	TEMPORARY PAVEMENT MARKING - LINE 4" - POLYUREA	FOOT	301957	301957						
70400100	TEMPORARY CONCRETE BARRIER	FOOT	2800	2800						
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2150	2150						
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	9	9						
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	7	7						
* 72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	14	14						
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	18	18						
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	210	210						
* 78004600	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LETTERS AND SYMBOLS	SQ FT	111	111						
* 78004630	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 6"	FOOT	17880	17880						
* 78004720	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 24"	FOOT	64	64						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005	0013	0013	0013	0013	0013	
				RURAL	038-0132	038-0126	038-0076	038-0151	038-0152	
* 78008310	POLYUREA PAVEMENT MARKING TYPE II - LINE 4"	FOOT	303400	303400						
* 78008340	POLYUREA PAVEMENT MARKING TYPE II - LINE 8"	FOOT	6938	6938						
* 78008350	POLYUREA PAVEMENT MARKING TYPE II - LINE 12"	FOOT	410	410						
* 78011000	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	62	62						
* 78011025	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	151700	151700						
* 78011035	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	17570	17570						
* 78011045	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	3469	3469						
* 78011125	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	64	64						
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2029	2029						
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	72	72						
* 78200020	CURB REFLECTORS	EACH	16	16						
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2029	2029						
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	103421	103421						
X0322936	REMOVE EXISTING FLARED END SECTION	EACH	3	3						

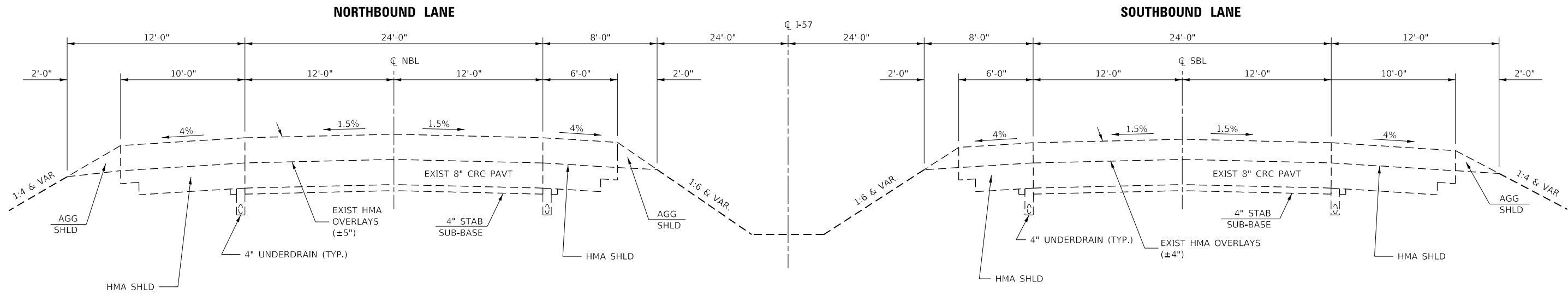
* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
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				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005	0013	0013	0013	0013	0013	
				RURAL	038-0132	038-0126	038-0076	038-0151	038-0152	
X0325893	CLEAN EXISTING END SECTION	EACH	36	36						
X0326649	LINEAR DELINEATOR PANELS, 6 INCH	EACH	38	38						
X0327809	LINEAR DELINEATOR PANELS, 4 INCH	EACH	70	70						
X0328004	REAL-TIME TRAFFIC CONTROL SENSOR UNIT	WEEK	112	112						
X0800001	SHOULDER REMOVAL (SPECIAL)	SQ YD	467	467						
X2111100	TOPSOIL EXCAVATION AND PLACEMENT, SPECIAL	CU YD	634	634						
X2700001	TEMPORARY RUMBLE STRIPS (SPECIAL)	EACH	4	4						
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	4666	4666						
X4810200	AGGREGATE SHOULDER REMOVAL	CU YD	434	434						
X6015000	REMOVE CONCRETE HEADWALLS FOR PIPE DRAINS	EACH	285	285						
X6350120	DELINEATOR REMOVAL	EACH	278	278						
X6431220	REMOVE AND REPLACE IMPACT ATTENUATOR SAND MODULE	EACH	16	16						
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1						
X7010225	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451, SPECIAL	L SUM	1	1						

* SPECIALTY ITEMS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE						
				NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	NHPP 90%FED/10%STATE	
				ROADWAY	BRIDGE	BRIDGE	BRIDGE	BRIDGE	BRIDGE	
				0005 RURAL	0013 038-0132	0013 038-0126	0013 038-0076	0013 038-0151	0013 038-0152	
X7010239	CHANGEABLE MESSAGE SIGN, SPECIAL	CAL WK	400	400						
X7260100	MILE POST MARKER ASSEMBLY (SPECIAL)	EACH	14	14						
X8570002	SMART TRAFFIC MONITORING SYSTEM	CAL WK	56	56						
X8570004	SMART TRAFFIC MONITORING DEVICE	CAL WK	280	280						
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	23.9			4.5	19.4			
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1						
Z0021700	EXPANSION JOINT REHABILITATION	FOOT	144	144						
Z0022800	FENCE REMOVAL	FOOT	5000	5000						
Z0034105	MATERIAL TRANSFER DEVICE	TON	55458	55458						
Z0040530	PIPE UNDERDRAIN REMOVAL	FOOT	157241	157241						
∅ Z0076600	TRAINEES	HOUR	2,000	2,000						
∅ Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	2,000	2,000						

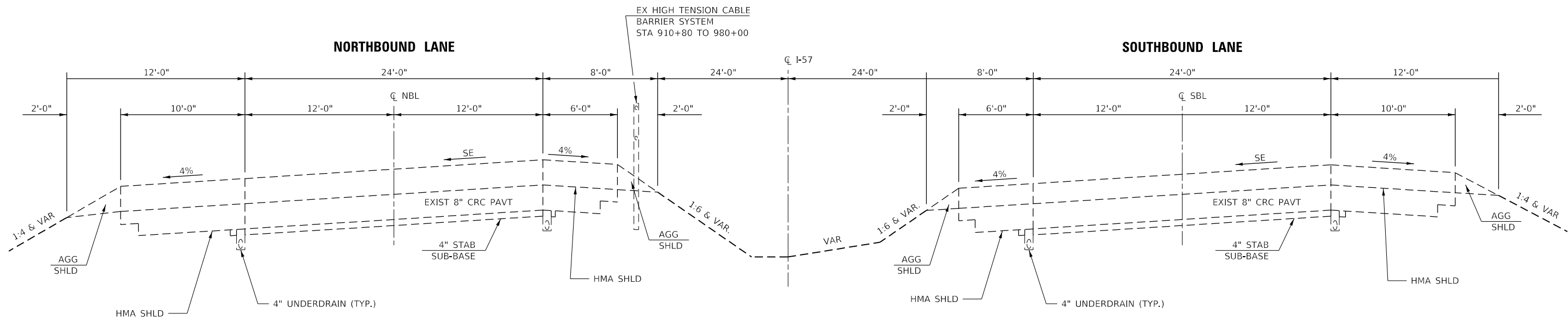
* SPECIALTY ITEMS ∅ 0042



EXISTING TYPICAL SECTION 1 - TANGENT

STA. 908+50 TO STA 910+80 (HMA SHOULDERS ONLY)
 STA. 910+80 TO STA 1269+02
 STA. 1269+02 TO STA 1276+90 (HMA SHOULDERS ONLY)
 (LOOKING SOUTH)

* EX THICKNESS VARIES AT TAPERS. (SEE TAPER DETAILS)



**EXISTING TYPICAL SECTION 2 - SUPERELEVATED
 (LOOKING SOUTH /LEFT HAND CURVE SHOWN)**

CURVE 16 - PI STA 933+44.69 **

PC STA 915+78.03
 PT STA 950+05.35
 S.E. = 2.8% (LEFT)

CURVE 17 - PI STA 1060+71.83

PC STA. 1047+75.84
 PT STA. 1073+43.38
 S.E. = 2.1% (RIGHT)

CURVE 18 - PI STA 1097+59.57

PC STA. 1084+87.19
 PT STA 1110+08.79
 S.E. = 2.1% (LEFT)

CURVE 19 - PI STA 1158+89.65

PC STA. 1135+03.80
 PT STA 1180+25.26
 S.E. = 2.8% (RIGHT)

CURVE 20 - PI STA 1246+17.35

PC STA. 1222+60.95
 PT STA 1267+32.13
 S.E. = 2.8% (LEFT)

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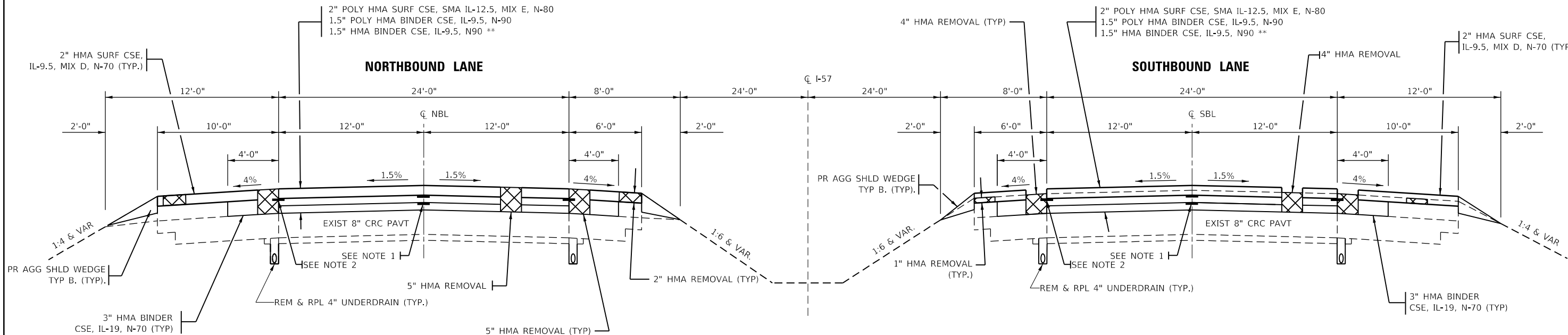
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	14
CONTRACT NO. 66H63			ILLINOIS FED. AID PROJECT	

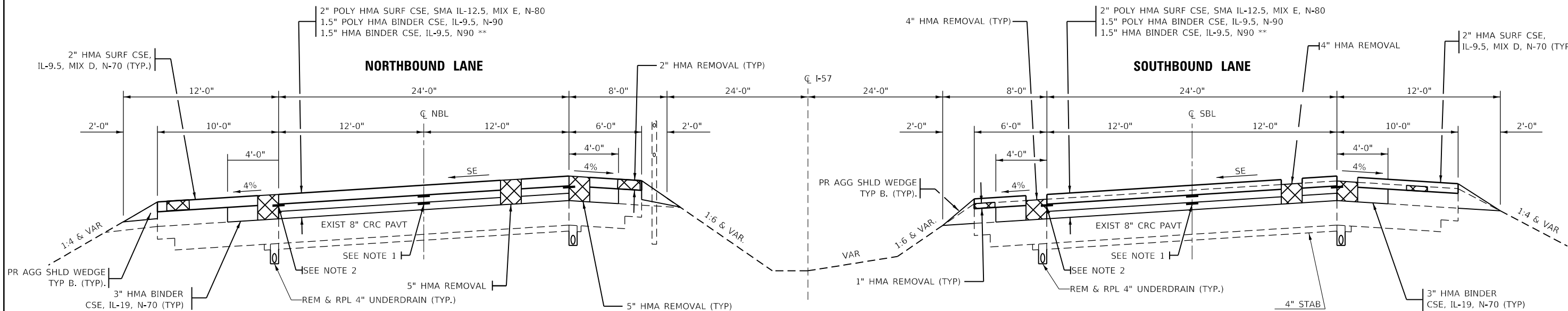


PROPOSED TYPICAL SECTION 1 - TANGENT

STA. 908+50 TO STA 910+80 (HMA SHOULDERS ONLY)
 STA. 910+80 TO STA 1269+02
 STA. 1269+02 TO STA 1276+90 (HMA SHOULDERS ONLY)
 (LOOKING SOUTH)

NOTES:

- 1) LONGITUDINAL JOINT SEALANT TO BE APPLIED UNDER SURFACE AND 2ND BINDER LIFT.
- 2) LONGITUDINAL JOINT SEALANT (EDGE) - TO BE APPLIED UNDER SURFACE LIFT. FINAL LOCATION TO BE DETERMINED BY THE ENGINEER.
- 3) REMOVAL OF HMA UNDER MAINLINE PAVEMENT SHALL BE TO ORIGINAL PCC PAVEMENT



PROPOSED TYPICAL SECTION 2 - SUPERELEVATED
 (LOOKING SOUTH /LEFT HAND CURVE SHOWN)

** ADDITIONAL BINDER CSE INCLUDED FOR SLOPE CORRECTIONS

CURVE 16 - PI STA 933+44.69 **

PC STA 915+78.03
 PT STA 950+05.35
 S.E. = 2.8% (LEFT)

CURVE 17 - PI STA 1060+71.83

PC STA. 1047+75.84
 PT STA. 1073+43.38
 S.E. = 2.1% (RIGHT)

CURVE 18 - PI STA 1097+59.57

PC STA. 1084+87.19
 PT STA 1110+08.79
 S.E. = 2.1% (LEFT)

CURVE 19 - PI STA 1158+89.65

PC STA. 1135+03.80
 PT STA 1180+25.26
 S.E. = 2.8% (RIGHT)

CURVE 20 - PI STA 1246+17.35

PC STA. 1222+60.95
 PT STA 1267+32.13
 S.E. = 2.8% (LEFT)

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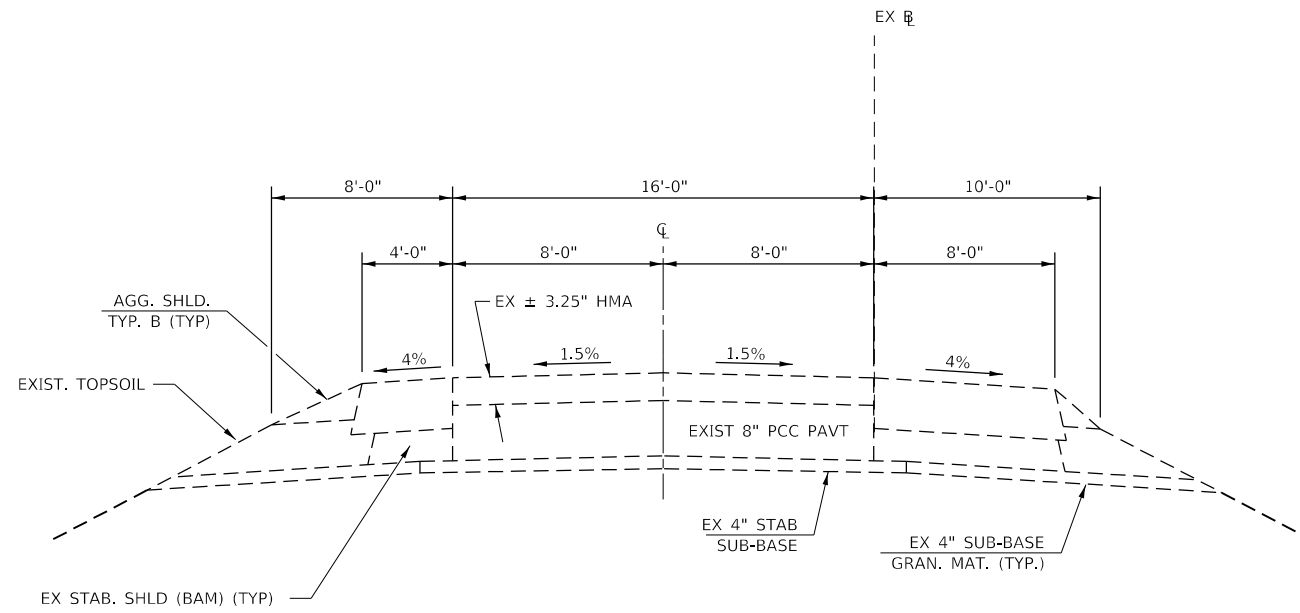
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

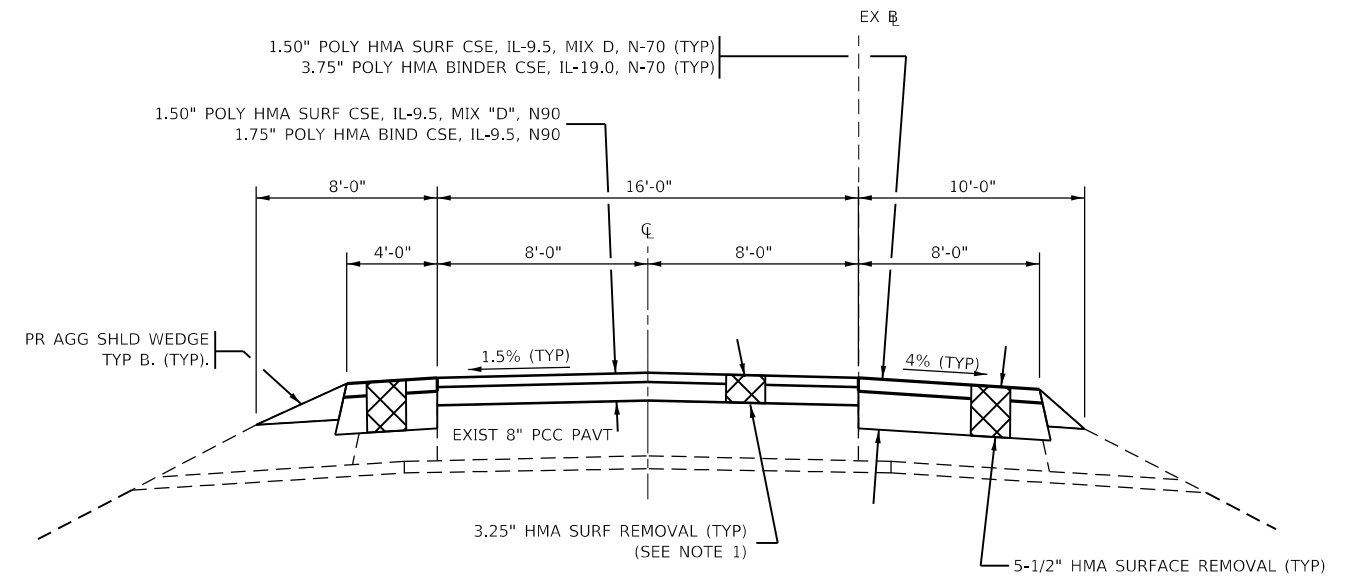
TYPICAL SECTIONS				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.I. RTE. 57	SECTION (38-7,38-6)RS	COUNTY IROQUOIS	TOTAL SHEETS 130	SHEET NO. 15
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 66H63



**EXISTING TYPICAL SECTION 3
BUCKLEY INTERCHANGE CH 9
RAMPS A, B, C, D**



**PROPOSED TYPICAL SECTION 3
BUCKLEY INTERCHANGE CH 9
RAMPS A, B, C, D**

NOTES:

1) REMOVAL OF HMA UNDER MAINLINE PAVEMENT (BUCKLEY RAMPS) SHALL BE TO ORIGINAL PCC PAVEMENT

HMA MIXTURE REQUIREMENT TABLE

LOCATIONS:	ENTIRE PROJECT			I-57		BUCKLEY INTERCHANGE				CROSSOVERS		ENTIRE PROJECT
MIXTURE USE(S):	SMA SURFACE	HMA BINDER	HMA BINDER	HMA SHOULDER BOTTOM LIFT(S)	HMA SHOULDER TOP LIFT	HMA SHOULDER BOTTOM LIFT(S)	HMA SHOULDER TOP LIFT	HMA RAMP BINDER	HMA RAMP SURFACE	HMA 8" SHOULDER BOTTOM LIFT(S)	HMA 8" SHOULDER TOP LIFT	INCIDENTAL HMA
BINDER GRADE (PG):	SBS PG 76-28	SBS PG 70-28	PG 64-22	PG 64-22	PG 64-22	SBS PG 70-28	SBS PG 70-28	SBS PG 70-28	SBS PG 70-28	PG 64-22	PG 64-22	PG 64-22
DESIGN AIR VOIDS:	4.0% @ N80	4.0% @ N90	4.0% @ N90	4.0% @ N70	4.0% @ N70	4.0% @ N70	4.0% @ N70	4.0% @ N90	4.0% @ N90	4.0% @ N50	4.0% @ N50	4.0% @ N50
MIXTURE COMPOSITION: (MIXTURE GRADATION)	IL 12.5	IL 9.5	IL 9.5	IL 19.0	IL 9.5	IL 19.0	IL 9.5	IL 9.5	IL 9.5	IL 19.0	IL 9.5	IL 9.5
FRICION AGGREGATE:	MIXTURE E				MIXTURE D		MIXTURE D		MIXTURE D			MIXTURE C
MIXTURE WEIGHT:	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN	112.0 LB/SY/IN
QUALITY MANAGEMENT PROGRAM:	PPF	PPF	PPF	QCQA	QCQA	QCQA	QCQA	QCQA	QCQA	QCQA	QCQA	QCQA
SUBLOT SIZE:	1000 TONS	1000 TONS	1000 TONS	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
DENSITY TEST METHOD:	CORES	CORES	CORES	CORES	CORES	CORES	CORES	CORES	CORES	CORES	CORES	SATISFACTION OF ENGINEER
MATERIAL TRANSFER DEVICE (REQUIRED):	YES	YES	YES	NO	NO	NO	NO	YES	YES	NO	NO	NO

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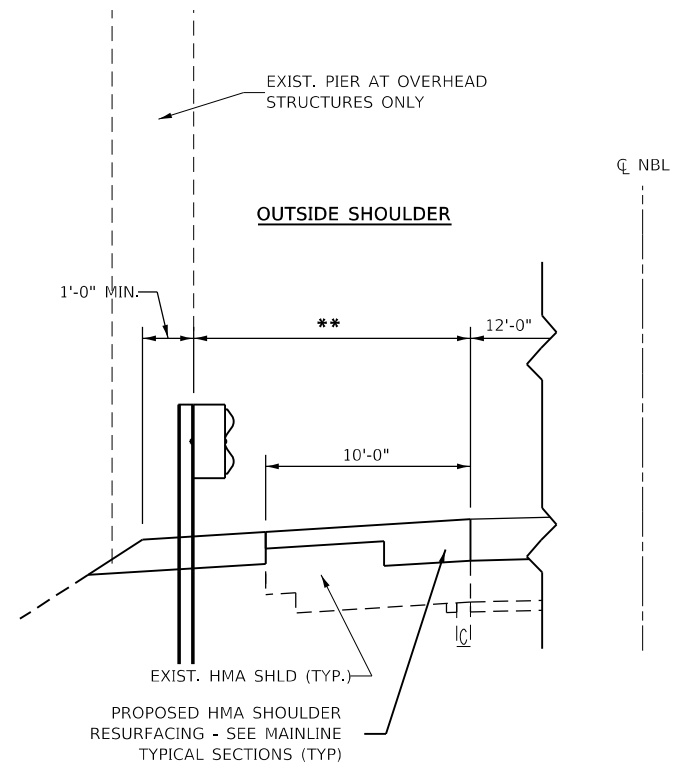
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

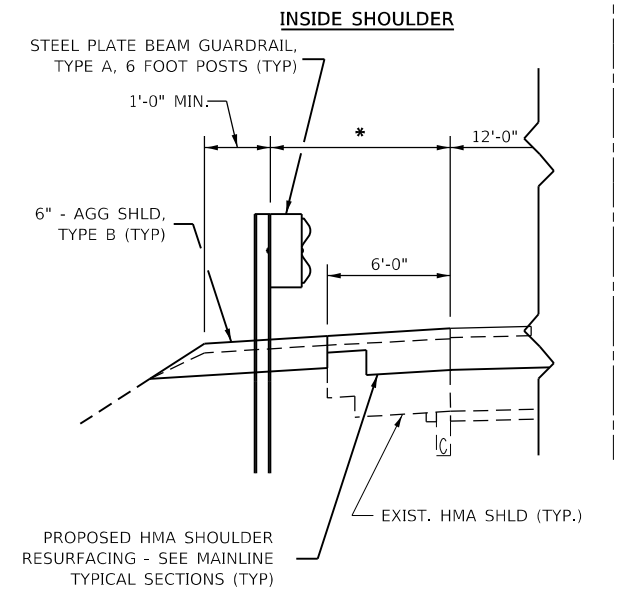
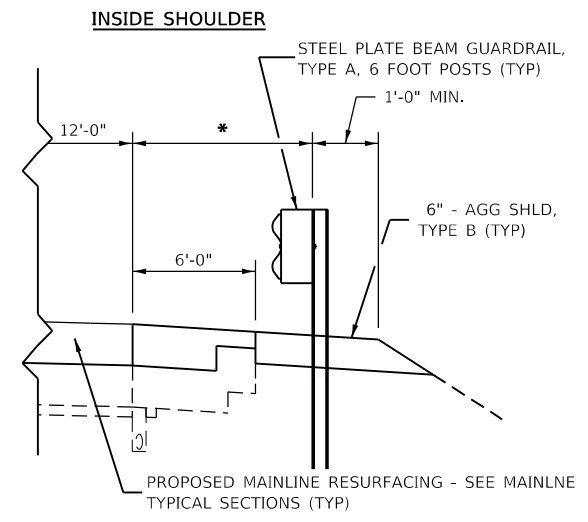
TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

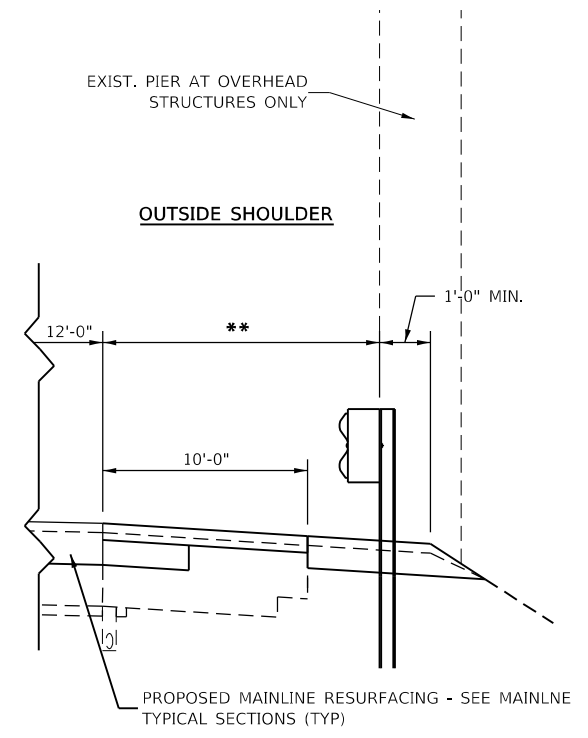
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	16
CONTRACT NO. 66H63			ILLINOIS FED. AID PROJECT	



☐ NBL



☐ SBL



PROPOSED TYPICAL SECTION 4

GUARDRAIL OFFSETS

- | | |
|---|--|
| <p>OUTSIDE SHOULDER **</p> <ul style="list-style-type: none"> 12' O/S EOP @ 038-0156 (NB) 12' O/S EOP @ 038-0154 (NB) 12' O/S EOP @ 038-0152 (NB) 12' O/S EOP @ 038-0082 (NB OVERHEAD STR) 12' O/S EOP @ 038-0076 (NB OVERHEAD STR) 12' O/S EOP @ 038-0126 (NB OVERHEAD STR) | <p>INSIDE SHOULDER *</p> <ul style="list-style-type: none"> 8' O/S EOP @ 038-0156 (NB) 8' O/S EOP @ 038-0154 (NB) 8' O/S EOP @ 038-0152 (NB) |
|---|--|

NOTES:

REFER TO GUARDRAIL SCHEDULES FOR APPROPRIATE GUARDRAIL TERMINALS
 NEWER GUARDRAIL INSTALLATION TO MATCH EXISTING OFFSETS.

PROPOSED TYPICAL SECTION 5

GUARDRAIL OFFSETS

- | | |
|--|---|
| <p>* INSIDE SHOULDER</p> <ul style="list-style-type: none"> 8' O/S EOP @ 038-0155 (SB) 8' O/S EOP @ 038-0153 (SB) 7' O/S EOP @ 038-0151 (SB) | <p>** OUTSIDE SHOULDER</p> <ul style="list-style-type: none"> 12' O/S EOP @ 038-0155 (SB) 11' O/S EOP @ 038-0153 (SB) 12' O/S EOP @ 038-0151 (SB) 12' O/S EOP @ 038-0082 (SB OVERHEAD STR) 12' O/S EOP @ 038-0076 (SB OVERHEAD STR) 11' O/S EOP @ 038-0126 (SB OVERHEAD STR) |
|--|---|

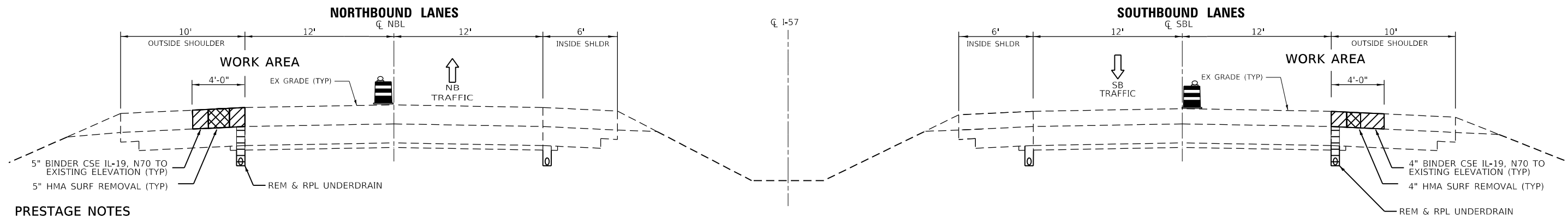
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

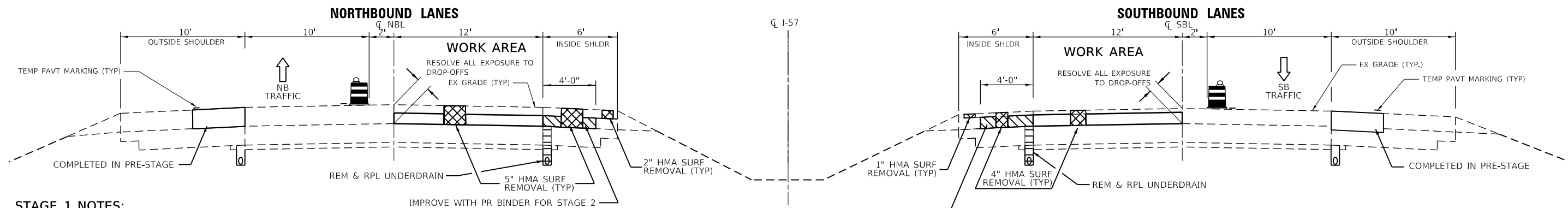
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	17
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				



PRE-STAGE CONFIGURATION

(LOOKING SOUTH)

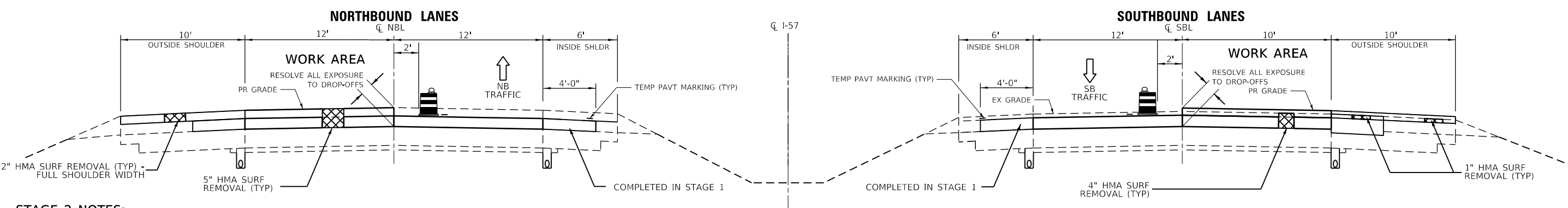
PRESTAGE NOTES
 *NORTHBOUND & SOUTHBOUND OUTSIDE SHOULDERS: MILL THE INNER 4' OF THESE SHOULDERS AND PLACE BINDER MIX TO TO EXISTING GRADE TO SUPPORT STAGE 1 TRAFFIC.
 *UNDERDRAIN REMOVAL & REPLACEMENT AT OUTSIDE SHOULDER
 *UTILIZE HWY STND 701401, 701411 FOR TRAFFIC CONTROL AND SMART TRAFFIC CONTROL FOR ADVANCE SIGNING
 *RESOLVE ALL EXPOSURE TO DROP-OFFS PER SPECIAL PROVISIONS.
 *APPLY TEMPORARY PAVEMENT MARKING FOR STAGE 1



STAGE 1

(LOOKING SOUTH)

STAGE 1 NOTES:
 *IMPROVE ALL MEDIAN STRUCTURAL ITEMS, PATCHING UNDER OVERHEAD STRUCTURES, GUARDRAIL UPGRADES, UNDERDRAIN UNDER INSIDE SHOULDER.
 *USE TRAFFIC CONTROL SPECIAL DETAIL, HWY STND 701411 AND SMART WORKZONE ADVANCE SIGNING.
 IMPROVE INSIDE SHOULDER (BINDER) FOR STAGE 2.
 *MILL AND RESURFARCE MAINLINE PAVEMENT (PROPOSED BINDER MIXES ONLY).
 *EXPOSURE TO DROP-OFFS SHALL BE RESOLVED PER SPECIAL PROVISIONS. IN ADDITION, ALL DROP-OFFS TO BE RESOLVED BY END OF WORKDAY ON FRIDAY, PRIOR HOLIDAYS AND END OF STAGE 1.
 *APPLY TEMPORARY PAVEMENT MARKING FOR STAGE 2



STAGE 2

(LOOKING SOUTH)

STAGE 2 NOTES:
 *IMPROVE ALL OUTSIDE STRUCTURAL ITEMS, PATCHING UNDER OVERHEAD STRUCTURES, GUARDRAIL UPGRADES, UNDERDRAIN UNDER OUTSIDE SHOULDER.
 *USE TRAFFIC CONTROL SPECIAL DETAIL, HWY STND 701411 AND SMART WORKZONE ADVANCE SIGNING.
 *MILL AND RESURFARCE MAINLINE PAVEMENT & SHOULDER (FINAL GRADE)
 *EXPOSURE TO DROP-OFFS SHALL BE RESOLVED PER SPECIAL PROVISION. IN ADDITION, ALL DROP-OFFS TO BE RESOLVED BY END OF WORKDAY ON FRIDAY, PRIOR -HOLIDAYS AND END OF STAGE 2

STAGE 3 NOTES:

(SETUP SIMILAR TO PRE-STAGE)
 *IMPROVE ALL INSIDE LANES AND SHOULDER WITH FINAL HMA SURFACE & ADDITIONAL MINOR IMPROVEMENTS
 *UTILIZE HWY STND 701401, 701411 FOR TRAFFIC CONTROL AND SMART TRAFFIC CONTROL FOR ADVANCE SIGNING
 *MILL & RESURFACE BUCKLEY RAMPS (NIGHT CLOSURES)

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS FOR STAGING

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	18
			CONTRACT NO. 66H63	
		ILLINOIS	FED. AID PROJECT	

MAINLINE SCHEDULE

LOCATION	LENGTH	MAINLINE WIDTH	HMA SURF REM 4" ****	BIT MATLS TACK CT	HMA BC IL-9.5 N90 *	POLY HMA BC IL 9.5 N90	POLY HMA SURF SMA IL 12.5 MX E N80	HMA BC HM N90	MIX CR JTS & FLANGE-WAYS	LONGITUDINAL JOINT SEAL		TEMP RAMP	MATL TRANSF DEVICE	
										MAIN **	(EDGE) ***			
STA - STA	FOOT	FOOT	SQ YD	POUND	TON	TON	TON	TON	TON	TON	FOOT	FOOT	SQ YD	TON
SOUTHBOUND DIRECTION (STATIONS INCREASING SOUTH)														
910+80 - 911+20	40	24	106.7	96	9.0	9.0	12.0	0.06	0.04	80		36		
911+20 - 914+48	328	24	874.7	787	73.5	73.5	98.0	0.44	0.27	656				
914+48 - 916+20	172	24	458.7	413	42.4	38.5	51.4	0.23	0.14	344				1,284
916+20 - 926+20	1,000	24	2,666.7	2,400	246.4	224.0	298.7	1.34	0.81	2,000				
926+20 - 927+60	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12	280		13		
927+60 - 929+40	180		SN 038-0155 (SB) BRIDGE OMISSION											
929+40 - 930+80	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12	280		13		675
930+80 - 938+17	737	24	1,965.3	1,769	181.6	165.1	220.1	0.99	0.59	1,474		36		
938+17 - 939+37	120		SEE PATCHING SCHEDULE - PR PATCH UNDER 038-0082 OVERHEAD STR											
939+37 - 950+06.60	1,070	24	2,852.3	2,567	263.6	239.6	319.5	1.43	0.86	2,139		36		823
STA EQUATION - 950+06.50 BK = STA 950+05.83 AH														
950+05.83 - 951+35	129	24	344.5	310	31.8	28.9	38.6	0.18	0.11	258				
951+35 - 961+60	1,025	24	2,733.3	2,460	229.6	229.6	306.1	1.37	0.82	2,050				969
961+60 - 963+00	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12	280		13		
963+00 - 964+54	154		SN 038-0153 (SB) BRIDGE OMISSION											
964+54 - 965+94	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12	280		13		
965+94 - 1046+60	8,066	24	21,509.3	19,358	1,806.8	1,806.8	2,409.0	10.76	6.46	16,132				6,145
1046+60 - 1046+83	23	24	61.3	55	5.7	5.1	6.9	0.04	0.02	46		36		
1046+83 - 1048+03	120		SEE PATCHING SCHEDULE - PR PATCH UNDER 038-0132 OVERHEAD STR											
1048+03 - 1048+08	5	24	13.3	12	1.2	1.1	1.5	0.01	0.01	10		36		
1048+08 - 1073+12	2,504	24	6,677.3	6,010	617.0	560.9	747.9	3.34	2.01	5,008				2,813
1073+12 - 1074+60	148	24	394.7	355	36.5	33.2	39.4	0.20	0.12	296				
1074+60 - 1083+71	911	24	2,429.3	2,186	204.1	204.1	272.1	1.22	0.73	1,822				
1083+71 - 1084+87.38	116	24	310.3	279	28.7	26.1	34.8	0.16	0.10	233				
STA EQUATION - STA 1084+87.38 BK = STA 1084+87.07 AH														
1084+87.07 - 1085+19	32	24	85.1	77	7.9	7.1	9.5	0.05	0.03	64				
1085+19 - 1109+77	2,458	24	6,554.7	5,899	605.7	550.6	734.1	3.28	1.97	4,916				3,809
1109+77 - 1111+25	148	24	394.7	355	36.5	33.2	39.4	0.20	0.12	296				
1111+25 - 1133+74	2,249	24	5,997.3	5,398	503.8	503.8	671.7	3.00	1.80	4,498				
1133+74 - 1135+04.55	131	24	348.1	313	32.2	29.2	39.0	0.18	0.11	261				
STA EQUATION - STA 1135+04.55 BK = STA 1135+04.13 AH														
1135+04.13 - 1135+46	42	24	111.7	101	10.3	9.4	12.5	0.06	0.04	84				
1135+46 - 1173+24	3,778	24	10,074.7	9,067	930.9	846.3	1,128.4	5.04	3.03	7,556				3,046
1173+24 - 1174+64	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12	280		13		
1174+64 - 1177+14	250		GAP EX. PATCH UNDER 038-0126 OVERHEAD STR											
1177+14 - 1178+54	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12	280		13		
1178+54 - 1179+83	129	24	344.0	310	31.8	28.9	38.5	0.18	0.11	258				
1179+83 - 1181+55	172	24	458.7	413	42.4	38.5	51.4	0.23	0.14	344				697
1181+55 - 1184+94	339	24	904.0	814	75.9	75.9	101.2	0.46	0.28	678				
1184+94 - 1186+34	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12	280				
1186+34 - 1187+82	148		SN 038-0151 (SB) BRIDGE (SEE BRIDGE HMA OVERLAY SCHEDULE)											
1187+82 - 1189+22	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12	280				
1189+22 - 1221+31	3,209	24	8,557.3	7,702	718.8	718.8	958.4	4.28	2.57	6,418				2,601
1221+31 - 1222+61.96	131	24	349.2	314	32.3	29.3	39.1	0.18	0.11	262				
STA EQUATION - STA 1222+61.96 BK = STA 1222+60.95 BK														
1222+60.95 - 1223+03	42	24	112.0	101	10.3	9.4	12.5	0.06	0.04	84				
1223+03 - 1266+90	4,387	24	11,698.8	10,529	1081.0	982.7	1,310.3	5.85	3.51	8,774				3,439
1266+90 - 1267+32.14	42	24	112.4	101	10.4	9.4	12.6	0.06	0.04	84				
STA EQUATION - 1267+32.14 BK=1267+31.76 AH														
1267+31.76 - 1268+62	130	24	347.3	313	32.1	29.2	38.9	0.18	0.11	260				130
1268+62 - 1269+02	40	24	106.7	96	9.0	9.0	12.0	0.06	0.04	80		36		
SOUTHBOUND TOTALS	34,853		92,941	83,648	8,213	7,807	10,410	47	28	69,706		294		26,431

NOTES:

- * 1ST BINDER LIFT QUANTITIES INCREASED BY 10% TO ACCOUNT FOR SLOPE CORRECTION AT CURVES
- ** LONGITUDINAL JOINT SEALANT TO BE APPLIED UNDER SURFACE AND 2ND BINDER LIFT.
- *** LONGITUDINAL JOINT SEALANT (EDGE) TO BE APPLIED BENEATH THE SURFACE LIFT. FINAL LOCATION OF LONGITUDINAL JOINT SEALANT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- **** MAINLINE HMA SHALL BE REMOVED TO THE ORIGINAL PCC PAVEMENT AND SHALL REQUIRE ADJUSTING MILLING DEPTHS TO COMPLETE THE REMOVAL TO PREVENT SCABBING.

MAINLINE SCHEDULE (CONTINUED)														
LOCATION	LENGTH	MAINLINE WIDTH	HMA SURF REM 5" ****	BIT MATLS TACK CT	HMA BC IL-9.5 N90 *	POLY HMA BC IL 9.5 N90	POLY HMA SURF SMA IL 12.5 MX E N80	HMA BC HM N90	MIX CR JTS & FLANGE-WAYS	LONGITUDINAL JOINT SEAL		TEMP RAMP	MATL TRANSF DEVICE	
										MAIN **	(EDGE) ***			
STA - STA	FOOT	FOOT	SQ YD	POUND	TON	TON	TON	TON	TON	TON	FOOT	FOOT	SQ YD	TON
NORTHBOUND DIRECTION (STATIONS INCREASING SOUTH)														
910+80 - 911+20	40	24	106.7	96	9.0	9.0	12.0	0.06	0.04			44		
911+20 - 914+48	328	24	874.7	787	73.5	73.5	98.0	0.44	0.27					1,283
914+48 - 916+20	172	24	458.7	413	42.4	38.5	51.4	0.23	0.14					
916+20 - 926+19	999	24	2,664.0	2,398	246.2	223.8	298.4	1.34	0.80					
926+19 - 927+59	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12			13		
927+59 - 929+41	182	SN 038-0156 (NB) BRIDGE OMISSION												
929+41 - 930+81	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12			13		1,589
930+81 - 950+06.60	1,926	24	5,134.9	4,621	474.5	431.3	575.1	2.57	1.55					
STA EQUATION - 950+06.50 BK = STA 950+05.83 AH														
950+05.83 - 951+35	129	24	344.5	310	31.8	28.9	38.6	0.18	0.11					948
951+35 - 961+60	1,025	24	2,733.3	2,460	229.6	229.6	306.1	1.37	0.82					
961+60 - 962+72	112	24	298.7	269	25.1	25.1	33.5	0.15	0.09			13		
962+72 - 964+27	155	SN 038-0154 (NB) BRIDGE OMISSION												
964+27 - 965+93	166	24	442.7	398	37.2	37.2	49.6	0.23	0.14			13		
965+93 - 1046+60	8,067	24	21,512.0	19,361	1,807.0	1,807.0	2,409.3	10.76	6.46					6,165
1046+60 - 1046+83	23	24	61.3	55	5.7	5.1	6.9	0.04	0.02			44		
1046+83 - 1048+03	120	SEE PATCHING SCHEDULE - PR PATCH UNDER 038-0132 OVERHEAD STR												
1048+03 - 1048+08	5	24	13.3	12	1.2	1.1	1.5	0.01	0.01			44		
1048+08 - 1073+12	2,504	24	6,677.3	6,010	617.0	560.9	747.9	3.34	2.01					2,813
1073+12 - 1074+60	148	24	394.7	355	36.5	33.2	44.2	0.20	0.12					
1074+60 - 1083+71	911	24	2,429.3	2,186	204.1	204.1	272.1	1.22	0.73					
1083+71 - 1084+87.38	116	24	310.3	279	28.7	26.1	34.8	0.16	0.10					
STA EQUATION - STA 1084+87.38 BK = STA 1084+87.07 AH														
1084+87.07 - 1085+19	32	24	85.1	77	7.9	7.1	9.5	0.05	0.03					
1085+19 - 1109+77	2,458	24	6,554.7	5,899	605.7	550.6	734.1	3.28	1.97					3,809
1109+77 - 1111+25	148	24	394.7	355	36.5	33.2	44.2	0.20	0.12					
1111+25 - 1133+74	2,249	24	5,997.3	5,398	503.8	503.8	671.7	3.00	1.80					
1133+74 - 1135+04.55	131	24	348.1	313	32.2	29.2	39.0	0.18	0.11					
STA EQUATION - STA 1135+04.55 BK = STA 1135+04.13 AH														
1135+04.13 - 1135+46	42	24	111.7	101	10.3	9.4	12.5	0.06	0.04					
1135+46 - 1173+99	3,853	24	10,274.7	9,247	949.4	863.1	1,150.8	5.14	3.09					3,103
1173+99 - 1175+39	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12			13		
1175+39 - 1177+89	250	GAP EX. PATCH UNDER 038-0126 OVERHEAD STR												
1177+89 - 1179+29	140	24	373.3	336	34.5	31.4	41.8	0.19	0.12			13		
1179+29 - 1179+83	54	24	144.7	130	13.4	12.2	16.2	0.08	0.05					
1179+83 - 1181+55	172	24	458.0	412	42.3	38.5	51.3	0.23	0.14					628
1181+55 - 1184+79	324	24	864.0	778	72.6	72.6	96.8	0.44	0.26					
1184+79 - 1186+19	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12					
1186+19 - 1187+66	147	SN 038-0152 (NB) BRIDGE (SEE BRIDGE HMA OVERLAY SCHEDULE)												
1187+66 - 1189+06	140	24	373.3	336	31.4	31.4	41.8	0.19	0.12					
1189+06 - 1221+31	3,225	24	8,600.0	7,740	722.4	722.4	963.2	4.30	2.58					2,613
1221+31 - 1222+61.96	131	24	349.2	314	32.3	29.3	39.1	0.18	0.11					
STA EQUATION - STA 1222+61.96 BK = STA 1222+60.95 BK														
1222+60.95 - 1223+03	42	24	112.0	101	10.3	9.4	12.5	0.06	0.04					137
1223+03 - 1224+39	136	24	362.8	327	33.5	30.5	40.6	0.19	0.11			44		
1224+39 - 1227+00	261	SEE PATCHING SCHEDULE - PR PATCH UNDER 038-0076 OVERHEAD STR												
1227+00 - 1266+90.13	3,990	24	10,640.3	9,576	983.2	893.8	1,191.7	5.33	3.20			44		3,101
1266+90 - 1267+32.14	42	24	112.0	101	10.3	9.4	12.5	0.06	0.04					
STA EQUATION - 1267+32.14 BK=1267+31.76 AH														
1267+31.76 - 1268+62	130	24	347.3	313	32.1	29.2	38.9	0.18	0.11					130
1268+62 - 1269+02	40	24	106.7	96	9.0	9.0	12.0	0.06	0.04			44		
							0.0							
NORTHBOUND TOTALS	34,710		92,560	83,304	8,178	7,776	10,367	46	28	69,420	2,640	342	26,320	
GRAND TOTALS	69,563			166,952	16,391	15,583	20,776	93	56	141,766	636	52,751		

NOTES:

* 1ST BINDER LIFT QUANTITIES INCREASED BY 10% TO ACCOUNT FOR SLOPE CORRECTION AT CURVES

** LONGITUDINAL JOINT SEALANT TO BE APPLIED UNDER SURFACE AND 2ND BINDER LIFT.

*** LONGITUDINAL JOINT SEALANT (EDGE) TO BE APPLIED BENEATH THE SURFACE LIFT. FINAL LOCATION OF LONGITUDINAL JOINT SEALANT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

**** MAINLINE HMA SHALL BE REMOVED TO THE ORIGINAL PCC PAVEMENT AND SHALL REQUIRE ADJUSTING MILLING DEPTHS TO COMPLETE THE REMOVAL TO PREVENT SCABBING.

BUCKLEY INTERCHANGE - HMA MAINLINE RAMP SCHEDULE												
LOCATION		LENGTH	RAMP WIDTH	HMA** SURF REM VAR DEPTH	HMA** SURF REM 3.75"	BIT MATLS TACK CT	POLY HMA BC IL-9.5 N90	POLY HMA SC IL-9.5 N90, MX D	POLY HMA BC (HM) N90	MIX CR JTS & FLANGE-WAYS	TEMP RAMP	MATL TRANSF DEVICE
STA	- STA	FOOT	FOOT	SQ YD	SQ YD	POUND	TON	TON	TON	TON	SQ YD	TON
RAMP A - SE RAMP (NB I-57 ENTRANCE RAMP)												
0+14	- 0+44	30	VAR	193.3		130	19	16	0.10	0.06	111	638
0+44	- 1+10	66	VAR		149.4	101	15	13	0.07	0.05	22	
1+10	- 8+68	758	16		1,347.6	910	132	113	0.67	0.41		
* 8+68	- 11+28	260	16	462.2		312	91	39	0.23	0.14	30	
* 11+28	- 18+88	760	VAR	718.0		485	141	60	0.36	0.22		
RAMP B - SE RAMP (NB I-57 EXIT RAMP)												
* 0+00	- 4+39	439	VAR	279.9		252	55	24	0.14	0.09		675
* 4+39	- 6+00	161	16	286.2		258	56	24	0.14	0.09	30	
6+00	- 19+72	1,372	16		2,439.1	2,195	239	205	1.22	0.74		
19+72	- 20+29	57	VAR		139.5	126	14	12	0.07	0.05	22	
20+29	- 20+72	43	VAR	258.8		233	25	22	0.13	0.08	111	
RAMP C - NW RAMP (SB I-57 EXIT RAMP)												
* 0+00	- 2+82	282	VAR	266.7		180	52	22	0.13	0.09		738
* 2+82	- 6+00	318	16	565.3		382	111	48	0.28	0.17	24	
6+00	- 19+43	1,343	16		2,387.6	1,612	234	201	1.19	0.72		
19+43	- 20+00	57	VAR		135.4	91	13	11	0.07	0.05	22	
20+00	- 20+42	42	VAR	252.4	-	170	25	21	0.13	0.08	111	
RAMP D - SW RAMP (SB I-57 ENTRANCE RAMP)												
0+12	- 0+42	30	VAR	194.6		131	19	16	0.10	0.06	111	656
0+42	- 0+97	55	VAR		113.0	76	11	10	0.06	0.04	22	
0+97	- 9+22	825	16		1,466.7	990	144	123	0.73	0.45		
* 9+22	- 11+92	270	16	480.0		324	94	40	0.24	0.15	24	
* 11+92	- 19+43	751	VAR	708.2		478	139	60	0.35	0.22		
HMA RAMP TOTALS												
		7,919		4,666	8,178	9,436	1,628	1,079	6	4	640	2,707

* ADDITIONAL BINDER ADDED TO TRANSITION TO I-57 MAINLINE HMA THICKNESS

** HMA SHALL BE REMOVED TO THE ORIGINAL PCC PAVEMENT AND SHALL REQUIRE ADJUSTING MILLING DEPTHS TO COMPLETE THE REMOVAL TO PREVENT SCABBING.

EXPANSION JOINT REHABILITATION		
LOCATION	LANE	LENGTH
		FOOT
962+95	SBL	24
1130+31	NBL	24
1184+60	NBL	24
1184+60	SBL	24
1189+20	SBL	24
1241+20	SBL	24
TOTAL		144

STATIONS ARE APPROXIMATE

BRIDGE RESURFACING SCHEDULE							
STA. TO STA.		LENGTH	WIDTH	POLY HMA SURF SMA IL 12.5 MX E N80 (1.5")	LONG JOINT SEAL (1 APP)	MIX JTS, CRACKS & FLGWYS	BIT MAT'L TACK COAT
FROM	TO	FT	FT	TONS	FOOT	TON	POUND
SOUTHBOUND - SN 038-0151 BRIDGE							
1186+34	1187+82	148	40	55.3	148	0.20	296.0
NORTHBOUND - SN 038-0152 BRIDGE							
1186+19	1187+66	147	40	54.9	147	0.20	294.0
BRIDGE RESURF TOTALS		295		110	295	0.4	590

I-57 HMA SHOULDER SCHEDULE - SOUTHBOUND

LOCATION		LENGTH	PRE-STAGE			STAGE 1 (INSIDE SHLDR)				STAGE 2 (OUTSIDE SHLDR)			STAGE 3 (INSIDE SHLDR)	
			HMA SURF REM 4"	BIT MATLS TACK CT	HMA BC IL-19 N70	HMA SURF REM 1"	HMA SURF REM 4"	BIT MATLS TACK CT	HMA BC IL-19 N70	HMA SURF REM 1"	BIT MATLS TACK CT	HMA SC IL-9.5 MIX D N70	BIT MATLS TACK CT	HMA SC IL-9.5 MIX D N70
STA	- STA	FOOT	SQ YD	POUND	TON	SQ YD	SQ YD	POUND	TON	SQ YD	POUND	TON	POUND	TON
SOUTHBOUND DIRECTION (STATIONS INCREASING SOUTH)														
904+00	- 910+80	680	302.2	136.0	67.7	151.1	302.2	136.0	50.8	755.5	340.0	84.6	204.0	50.8
910+80	- 914+48	368	163.6	73.6	36.6	81.8	163.6	73.6	27.5	408.9	184.0	45.8	110.4	27.5
914+48	- 927+60	1,312	583.1	262.4	130.6	291.6	583.1	262.4	98.0	1,457.8	656.0	163.3	393.6	98.0
927+60	- 929+40	180	SN 038-0155 (SB) BRIDGE OMISSION											
929+40	- 950+06.60	2,067	918.5	413.3	205.7	459.2	919	413.3	154.3	2,296.2	1,033.3	257	620.0	154
STA EQUATION - 950+06.50 BK = STA 950+05.83 AH														
950+05.83	- 951+35	129	57.4	25.8	12.9	28.7	57.4	25.8	9.6	143.5	64.6	16.1	38.7	9.6
951+35	- 963+25	1,190	528.9	238.0	118.5	264.4	528.9	238.0	88.9	1,322.2	595.0	148.1	357.0	88.8
963+25	- 964+26	101	SN 038-0153 (SB) BRIDGE OMISSION											
964+26	- 1046+60	8,234	3,659.6	1,646.8	819.8	1,829.8	3,659.6	1,646.8	614.8	9,148.9	4,117.0	1,024.7	2,470.2	614.8
1046+60	- 1074+60	2,800	1,244.4	560.0	278.7	622.2	1,244.4	560.0	209.1	3,111.1	1,400.0	348.4	840.0	209.1
1074+60	- 1083+71	911	404.9	182.2	90.7	202.4	404.9	182.2	68.0	1,012.2	455.5	113.4	273.3	68.0
1083+71	- 1084+87.38	116	51.7	23.3	11.6	25.9	51.7	23.3	8.7	129.3	58.2	14.5	34.9	8.7
STA EQUATION - STA 1084+87.38 BK = STA 1084+87.07 AH														
1084+87.07	- 1111+25	2,638	1,172.4	527.6	262.6	586.2	1,172.4	527.6	197.0	2,931.0	1,319.0	328.3	791.4	197.0
1111+25	- 1133+74	2,249	999.6	449.8	223.9	499.8	999.6	449.8	167.9	2,498.9	1,124.5	279.9	674.7	167.9
1133+74	- 1135+04.55	131	58.0	26.1	13.0	29.0	58.0	26.1	9.7	145.0	65.3	16.2	39.2	9.7
STA EQUATION - STA 1135+04.55 BK = STA 1135+04.13 AH														
1135+04.13	- 1181+55	4,651	2,067.1	930.2	463.0	1,033.5	2,067.1	930.2	347.3	5,167.7	2,325.5	578.8	1,395.3	347.3
1181+55	- 1186+34	479	212.9	95.8	47.7	106.4	212.9	95.8	35.8	532.2	239.5	59.6	143.7	35.8
1186+34	- 1187+82	148	SN 038-0151 (SB) BRIDGE OMISSION											
1187+82	- 1211+46	2,364	1,050.7	472.8	235.4	525.3	1,050.7	472.8	176.5	2,626.7	1,182.0	294.2	709.2	176.5
1211+46	- 1215+05	359	159.6	71.8	35.8	79.8	159.6	71.8	26.8	0.0	0.0	0.0	107.7	26.8
1215+05	- 1218+31	326	144.9	65.2	32.5	72.4	144.9	65.2	24.3	398.2	179.2	44.6	97.8	24.3
1218+31	- 1222+61.96	431	191.5	86.2	42.9	95.8	191.5	86.2	32.2	478.8	215.5	53.6	129.3	32.2
STA EQUATION - STA 1222+61.96 BK = STA 1222+60.95 BK														
1222+60.95	- 1236+88	1,427	634.2	285.4	142.1	317.1	634.2	285.4	106.5	1,585.6	713.5	177.6	428.1	106.5
1236+88	- 1238+13	125	55.6	25.0	12.5	27.8	55.6	25.0	9.3	121.0	54.5	13.6	37.5	9.3
1238+13	- 1247+54	941	418.2	188.2	93.7	209.1	418.2	188.2	70.3	0.0	0.0	0.0	282.3	70.3
1247+54	- 1267+32.14	1,978	879.2	395.6	196.9	439.6	879.2	395.6	147.7	2,198.0	989.1	246.2	593.5	147.7
STA EQUATION - 1267+32.14 BK=1267+31.76 AH														
1267+31.76	- 1268+62	130	57.9	26.1	13.0	28.9	57.9	26.1	9.7	144.7	65.1	16.2	39.1	9.7
1268+62	- 1269+02	40	17.8	8.0	4.0	8.9	17.8	8.0	3.0	44.5	20.0	5.0	12.0	3.0
1269+02	- 1272+50	348	154.7	69.6	34.7	77.3	154.7	69.6	26.0	386.7	174.0	43.3	104.4	26.0
														0.0
SOUTHBOUND TOTALS	36,853	16,189	7,285	3,626	8,094	16,189	7,285	2,720	39,045	17,570	4,373	10,927	2,720	

NOTES
 PRE-STAGE - OUTSIDE SHOULDER (4 FT) TO BE PREPARED FOR STAGE TRAFFIC. USE BINDER TO REACH EXISTING ELEVATION.
 STAGE 1 - IMPROVE INSIDE SHOULDER FOR STAGE 2 TRAFFIC. USE BINDER TO REACH PROPOSED BINDER ELEVATION.
 STAGE 2 - REMOVE 1" OF HMA OUTSIDE SHOULDER, IMPROVE OUTSIDE SHOULDER WITH FINAL SURFACE COURSE MIX
 STAGE 3 - IMPROVE INSIDE SHOULDER WITH FINAL SURFACE COURSE MIX
 HMA SURFACE & BINDER MIXES TO BE PROTECTED FROM ALL CONSTRUCTION ACTIVITIES (WEAR AND TEAR). DAMAGES TO BE CORRECTED BY CONTRACTOR.

I-57 HMA SHOULDER SCHEDULE - NORTHBOUND

LOCATION	LENGTH	PRE-STAGE			STAGE 1 (INSIDE SHLDR)				STAGE 2 (OUTSIDE SHLDR)			STAGE 3 (INSIDE SHLDR)	
		HMA SURF REM 5"	BIT MATLS TACK CT	HMA BC IL-19 N70	HMA SURF REM 2"	HMA SURF REM 5"	BIT MATLS TACK CT	HMA BC IL-19 N70	HMA SURF REM 2"	BIT MATLS TACK CT	HMA SC IL-9.5 MIX D N70	BIT MATLS TACK CT	HMA SC IL-9.5 MIX D N70
STA - STA	FOOT	SQ YD	POUND	TON	SQ YD	SQ YD	POUND	TON	SQ YD	POUND	TON	POUND	TON
908+50 - 910+80	230	102.2	46.0	28.6	51.1	102.2	46.0	17.2	255.5	115.0	28.6	69.0	17.2
910+80 - 911+20	40	17.8	8.0	5.0	8.9	17.8	8.0	3.0	44.5	20.0	5.0	12.0	3.0
911+20 - 914+48	328	145.8	65.6	40.8	72.9	145.8	65.6	24.5	364.5	164.0	40.8	98.4	24.5
914+48 - 927+59	1,311	582.7	262.2	163.2	291.3	582.7	262.2	97.9	1,456.7	655.5	163.2	393.3	97.9
927+59 - 929+41	182	SN 038-0156 (NB) BRIDGE OMISSION											
929+41 - 950+06.60	2,066	918.0	413.1	257.0	459.0	918.0	413.1	154.2	2,295.1	1,032.8	257.1	619.7	154.2
STA EQUATION - 950+06.50 BK = STA 950+05.83 AH													
950+05.83 - 951+35	129	57.4	25.8	16.1	28.7	57.4	25.8	9.6	143.5	64.6	16.1	38.7	9.6
951+35 - 962+99	1,164	517.3	232.8	144.8	258.7	517.3	232.8	86.9	1,293.3	582.0	144.8	349.2	86.9
962+99 - 964+00	101	SN 038-0154 (NB) BRIDGE OMISSION											
964+00 - 1046+60	8,260	3,671.1	1,652.0	1,027.9	1,835.6	3,671.1	1,652.0	616.7	9,177.8	4,130.0	1,027.9	2,478.0	616.8
1046+60 - 1046+83	23	10.2	4.6	2.9	5.1	10.2	4.6	1.7	25.5	11.5	2.9	6.9	1.7
1046+83 - 1074+60	2,777	1,234.2	555.4	345.6	617.1	1,234.2	555.4	207.3	3,085.5	1,388.5	345.6	833.1	207.3
1074+60 - 1083+71	911	404.9	182.2	113.4	202.4	404.9	182.2	68.0	1,012.2	455.5	113.4	273.3	68.0
1083+71 - 1084+87.38	116	51.7	23.3	14.5	25.9	51.7	23.3	8.7	129.3	58.2	14.5	34.9	8.7
STA EQUATION - STA 1084+87.38 BK = STA 1084+87.07 AH													
1084+87.07 - 1111+25	2,638	1,172.4	527.6	328.3	586.2	1,172.4	527.6	197.0	2,931.0	1,319.0	328.3	791.4	197.0
1111+25 - 1133+74	2,249	999.6	449.8	279.9	499.8	999.6	449.8	167.9	2,498.9	1,124.5	279.9	674.7	167.9
1133+74 - 1135+04.55	131	58.0	26.1	16.2	29.0	58.0	26.1	9.7	145.0	65.3	16.2	39.2	9.7
STA EQUATION - STA 1135+04.55 BK = STA 1135+04.13 AH													
1135+04.13 - 1181+55	4,651	2,067.1	930.2	578.8	1,033.5	2,067.1	930.2	347.3	5,167.7	2,325.5	578.8	1,395.3	347.3
1181+55 - 1186+19	464	206.2	92.8	57.7	103.1	206.2	92.8	34.6	515.5	232.0	57.7	139.2	34.6
1186+19 - 1187+66	147	SN 038-0152 (NB) BRIDGE OMISSION											
1187+66 - 1203+38	1,572	698.7	314.4	195.6	349.3	698.7	314.4	117.4	1,746.7	786.0	195.6	471.6	117.4
1203+38 - 1212+98	960	426.7	192.0	119.5	213.3	426.7	192.0	71.7	0.0	0.0	0.0	288.0	71.7
1212+98 - 1213+95	97	43.1	19.4	12.1	21.6	43.1	19.4	7.2	85.0	38.3	9.5	29.1	7.2
1213+95 - 1222+61.96	867	385.3	173.4	107.9	192.7	385.3	173.4	64.7	963.3	433.5	107.9	260.1	64.7
STA EQUATION - STA 1222+61.96 BK = STA 1222+60.95 BK													
1222+60.95 - 1234+03	1,142	507.6	228.4	142.1	253.8	507.6	228.4	85.3	1,269.0	571.1	142.1	342.6	85.3
1234+03 - 1236+15	212	94.2	42.4	26.4	47.1	94.2	42.4	15.8	275.5	124.0	30.9	63.6	15.8
1236+15 - 1241+11	496	220.4	99.2	61.7	110.2	220.4	99.2	37.0	0.0	0.0	0.0	148.8	37.0
1241+11 - 1267+32.14	2,621	1,165.0	524.3	326.2	582.5	1,165.0	524.3	195.7	2,912.4	1,310.6	326.2	786.4	195.7
STA EQUATION - 1267+32.14 BK=1267+31.76 AH													
1267+31.76 - 1268+62	130	57.9	26.1	16.2	28.9	57.9	26.1	9.7	144.7	65.1	16.2	39.1	9.7
1268+62 - 1269+02	40	17.8	8.0	5.0	8.9	17.8	8.0	3.0	44.5	20.0	5.0	12.0	3.0
1269+02 - 1276+90	788	350.2	157.6	98.1	175.1	350.2	157.6	58.8	875.5	394.0	98.1	236.4	58.8
NORTHBOUND TOTALS	36,843	16,184	7,283	4,532	8,092	16,184	7,283	2,719	38,858	17,487	4,352	10,924	2,719

NOTES

- PRE-STAGE - OUTSIDE SHOULDER (4 FT) TO BE PREPARED FOR STAGE TRAFFIC. USE BINDER TO REACH EXISTING ELEVATION.
- STAGE 1 - IMPROVE INSIDE SHOULDER FOR STAGE 2 TRAFFIC. USE BINDER TO REACH PROPOSED BINDER ELEVATION.
- STAGE 2 - REMOVE 2" OF HMA OUTSIDE SHOULDER, IMPROVE OUTSIDE SHOULDER WITH FINAL SURFACE COURSE MIX
- STAGE 3 - IMPROVE INSIDE SHOULDER WITH FINAL SURFACE COURSE MIX
- HMA SURFACE & BINDER MIXES TO BE PROTECTED FROM ALL CONSTRUCTION ACTIVITIES (WEAR AND TEAR). DAMAGES TO BE CORRECTED BY CONTRACTOR.

BUCKLEY INTERCHANGE - HMA SHOULDER SCHEDULE						
LOCATION	LENGTH	HMA SURF REM 5.5"	BIT MATLS TACK CT	POLY HMA BC IL-19 N70	POLY HMA SC IL-9.5 N70, MX D	
STA - STA	FOOT	SQ YD	POUND	TON	TON	
RAMP A - SE RAMP (NB I-57 ENTRANCE RAMP)						
0+14 - 1+10	96	151.5	102	32	13	
1+10 - 8+34	724	965.4	652	203	81	
8+34 - 18+88	1,054	936.9	632	197	79	
RAMP B - SE RAMP (NB I-57 EXIT RAMP)						
0+00 - 6+00	600	533.3	360	112	45	
6+00 - 19+72	1,372	1,829.4	1,235	384	154	
19+72 - 20+72	100	155.1	105	33	13	
RAMP C - NW RAMP (SB I-57 EXIT RAMP)						
0+00 - 5+46	546	485.3	328	102	41	
5+46 - 19+43	1,397	1,862.7	1,257	391	157	
19+43 - 20+42	99	152.4	103	32	13	
RAMP D - SW RAMP (SB I-57 ENTRANCE RAMP)						
0+12 - 0+97	85	140.4	95	30	12	
0+97 - 8+94	797	1,062.6	717	223	89	
8+94 - 19+43	1,049	932.4	629	196	78	
HMA SHOULDER TOTALS	7,919	9,207	6,215	1,934	774	

PERMANENT SURVEY MARKER SCHEDULE					
MONUMENT NUMBER	DESCRIPTION	EXISTING MONUMENT TYPE	PROPOSED MONUMENT TYPE	MONUMENT RECORD TO BE RECORDED	RESPONSIBILITY
N/A	NO SURVEY CONTROL OR SECTION CORNER PERMANENT MARKERS REQUIRED.	N/A	N/A	N/A	N/A
NO KNOWN SURVEY OR CENTERLINE CONTROL MONUMENTS ARE ON THE ROAD SURFACE THAT WILL BE DISTURBED BY THE RESURFACING WORK. ONLY THE ORIGINAL SCOPE OF THIS JOB HAS BEEN SEARCHED FOR MONUMENTS. THE R.E. MUST INFORM PLATS AND PLANS TO SEARCH FOR ADDITIONAL MONUMENTS PRIOR TO MILLING IF AN ADDENDUM HAS BEEN ADDED TO THIS JOB. NO MONUMENT RECORDS ARE REQUIRED FOR THIS JOB.					

MAINTENANCE CROSSOVER SCHEDULE								
LOCATION	LENGTH	SIDE	SHLR REM SPL	BIT MATLS TACK CT	HMA SHLDRS 8"	HMA SURF REM 1.5"	INC HMA SURF	DELINEATORS
STA - STA	FOOT	FOOT	SQ YD	POUND	SQ YD	SQ YD	TON	EACH
* 1022+77		SB PL						1
1028+73 - 1030+73	200	NB PL	66.7	30.0	66.7			
** 1028+73 - 1030+73	200	SB PL	66.7	30.0	66.7			1
1030+77 - 1031+12	35	X-OVER		97.7		217.2	18.2	
** 1031+20 - 1033+20	200	NB PL	0.0	0.0	0.0			1
1031+20 - 1033+20	200	SB PL	66.7	30.0	66.7			
* 1039+12		NB PL						1
* 1254+07		SB PL						1
1260+02 - 1262+02	200	NB PL	66.7	30.0	66.7			
** 1260+02 - 1262+02	200	SB PL	66.7	30.0	66.7			1
1262+07 - 1262+42	35	X-OVER		97.7		217.2	18.2	
** 1262+47 - 1264+47	200	NB PL	66.7	30.0	66.7			1
1262+47 - 1264+47	200	SB PL	66.7	30.0	66.7			
* 1270+42		NB PL						1
TOTALS	1,670		467	405	467	434	36	8

NOTE:
 *INSTALL DELINEATOR POST WITH **3 AMBER** REFLECTORS ALONG INSIDE SHOULDER **800 FT** BEFORE THE MAINTENANCE CROSSOVER
 INSTALL DELINEATOR POST WITH **2 AMBER REFLECTORS ALONG INSIDE SHOULDER **100 FT** BEFORE THE MAINTENANCE CROSSOVER

AGG SHOULDER & RUMBLE STRIP SCHEDULE						
LOCATION			LENGTH	SIDE	AGG WEDGE SHLDR TY B	SHOULDER RUMBLE STRIP 16"
STA	TO	STA	FOOT	LT/RT	TON	FOOT
MAINLINE - SOUTHBOUND						
910+80	TO	927+60	1,680	LT & RT	109	3,360
STRUCTURE OMISSION SN 038-0155						
929+40	TO	950+06.50	2,067	LT & RT	134	4,133
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH						
950+05.83	TO	963+25	1,319	LT & RT	86	2,638
STRUCTURE OMISSION SN 038-0153						
964+26	TO	1084+87.34	12,061	LT & RT	782	24,123
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH						
1084+87.07	TO	1135+04.55	5,017	LT & RT	325	10,035
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH						
1135+04.13	TO	1186+34	5,130	LT & RT	333	10,260
STRUCTURE OMISSION SN 038-0151						
1187+82	TO	1211+45	2,363	LT & RT	153	4,726
1211+45	TO	1214+29	284	LT	9	284
1214+29	TO	1218+83	454	LT	15	454
1218+83	TO	1222+61.96	379	LT & RT	25	758
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK						
1222+60.95	TO	1236+35	1,374	LT & RT	89	2,748
1236+35	TO	1240+11	376	LT	12	376
1240+11	TO	1247+54	743	LT	24	743
1247+54	TO	1267+32.14	1,978	LT & RT	128	3,956
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH						
1267+31.76	TO	1269+02	170	LT & RT	11	340
SOUTHBOUND TOTAL			35,396		2,234.1	68,934.0
MAINLINE - NORTHBOUND						
910+80	TO	927+59	1,679	LT & RT	109	3,358
STRUCTURE OMISSION SN 038-0156						
929+41	TO	950+06.50	2,066	LT & RT	134	4,131
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH						
950+05.83	TO	962+99	1,293	LT & RT	84	2,586
STRUCTURE OMISSION SN 038-0154						
964+00	TO	1084+87.34	12,087	LT & RT	783	24,175
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH						
1084+87.07	TO	1135+04.55	5,017	LT & RT	325	10,035
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH						
1135+04.13	TO	1186+19	5,115	LT & RT	332	10,230
STRUCTURE OMISSION SN 038-0152						
1187+66	TO	1203+37	1,571	LT & RT	102	3,142
1203+37	TO	1210+98	761	LT	25	761
1210+98	TO	1214+76	378	LT	12	378
1214+76	TO	1222+61.96	786	LT & RT	51	1,572
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK						
1222+60.95	TO	1233+54	1,093	LT & RT	71	2,186
1233+54	TO	1236+67	313	LT	10	313
1236+67	TO	1241+09	442	LT	14	442
1241+09	TO	1267+32.14	2,623	LT & RT	170	5,246
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH						
1267+31.76	TO	1269+02	170	LT & RT	11	340
NORTHBOUND TOTAL			35,395		2,232.5	68,895.0

AGG SHOULDER & RUMBLE STRIP SCHEDULE (CONT.)						
LOCATION			LENGTH	SIDE	AGG WEDGE SHLDR TY B	SHOULDER RUMBLE STRIP 16"
STA	TO	STA	FOOT	LT/RT	TON	FOOT
RAMP A - SE RAMP (NB I-57 ENTRANCE RAMP)						
0+14	TO	8+15	801	LT & RT	49	
8+15	TO	18+88	1,073	RT	40	
RAMP B - SE RAMP (NB I-57 EXIT RAMP)						
0+00	TO	7+00	700	LT & RT	43	
7+00	TO	20+72	1,372	RT	51	
RAMP C - NW RAMP (SB I-57 EXIT RAMP)						
0+00	TO	6+90	690	LT & RT	42	
6+90	TO	20+42	1,352	RT	50	
RAMP D - SW RAMP (SB I-57 ENTRANCE RAMP)						
0+12	TO	8+65	853	LT & RT	52	
8+65	TO	19+43	1,078	RT	40	
RAMPS TOTAL			7,919		367.5	0
GRAND TOTAL			78,710		4,834	137,829

GUARDRAIL SCHEDULE

SN NO.	QUAD	SIDE	DIRECTION OF TRAFFIC	STATION	LENGTH	LENGTH OF NEED	GR REM	SPBGR TY A 6 FT POSTS	TRAFFIC BARRIER			TERMINAL MARKER DIRECT APPLIED	GR REF TYPE A	LINEAR DELIN PANELS 4"		AGG SHLD REM (6")	AGG SHLD TYP B	CURB REM	CONC CURB TY B
									TERM TY 6	TERM TY 6A	TERM TY 1 SPL TANGENT			WHITE	YELLOW				
									FOOT	FOOT	FOOT			EACH	EACH				
038-0155	NW	DL	SB APPR	924+13 TO 927+88	375	362.5	378	287.5	1		1	1	4	5	28	56	35	35	
	NE	PL	SB APPR	924+63 TO 927+88	325	312.5	390	237.5	1		1	1	4	4	24	48	35	35	
038-0156	SE	DL	NB APPR	929+11 TO 932+86	375	362.5	366	287.5	1		1	1	4	5	28	56	35	35	
	SW	PL	NB APPR	929+12 TO 932+99	387	375	390	300	1		1	1	4	4	29	57	35	35	
038-0082	NW	DL	SB APPR	935+99 TO 938+74	275	262.5	270	187.5	1		1	1	4	3	20	41			
	SE	DL	NB APPR	938+62 TO 941+37	275	262.5	265	187.5	1		1	1	4	3	20	41			
038-0153	NW	DL	SB APPR	960+09 TO 963+34	325	312.5	325	237.5	1		1	1	4	4	24	48			
	NE	PL	SB APPR	959+97 TO 963+22	325	312.5	385	237.5	1		1	1	4	4	24	48			
038-0154	SE	DL	NB APPR	963+92 TO 967+17	325	312.5	352	237.5	1		1	1	4	4	24	48			
	SW	PL	NB APPR	964+04 TO 967+91	387	375	390	300	1		1	1	4	4	29	57			
038-0126	NW	DL	SB APPR	1172+61 TO 1175+36	275	262.5	268	187.5	1		1	1	4	3	20	41			
	SE	DL	NB APPR	1177+14 TO 1179+89	275	262.5	270	187.5	1		1	1	4	3	20	41			
038-0151	NW	DL	SB APPR	1182+91 TO 1186+66	375	362.5	371	287.5	1		1	1	4	5	28	56			
	NE	PL	SB APPR	1183+35 TO 1186+60	325	312.5	390	237.5		1	1	1	4	4	24	48			
038-0152	SE	DL	NB APPR	1187+34 TO 1191+09	375	362.5	371	287.5	1		1	1	4	5	28	56			
	SW	PL	NB APPR	1187+42 TO 1190+67	325	312.5	390	237.5		1	1	1	4	4	24	48			
038-0076	NW	DL	SB APPR	1223+44 TO 1226+19	275	262.5	267	187.5	1		1	1	4	3	20	41			
	SE	DL	NB APPR	1225+31 TO 1228+06	275	262.5	267	187.5	1		1	1	4	3	20	41			
TOTAL					5,874	5,650	6,105	4,300	16	2	18	18	72	70	434	872	140	140	

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULES			
SCALE:	SHEET	OF	SHEETS
		STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	26
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

CLASS A PATCHING SCHEDULE														
SOUTHBOUND PASSING LANE							SOUTHBOUND DRIVING LANE							
MILE MARKER	PATCH LENGTH	PATCH WIDTH	CLASS A TYPE II, 13"	CLASS A TYPE III, 13"	PATCH REINF	SAW CUTS	TIE BARS 3/4"	PATCH LENGTH	PATCH WIDTH	CLASS A TYPE II, 13"	CLASS A TYPE III, 13"	PATCH REINF	SAW CUTS	TIE BARS 3/4"
	FOOT	FOOT	SQ YD	SQ YD	SQ YD	FOOT	EACH	FOOT	FOOT	SQ YD	SQ YD	SQ YD	FOOT	EACH
278.40								6	12	8		8	36	42
277.72								6	12	8		8	36	42
277.50	12	12		16	16	48	42							
276.60								6	12	8		8	36	42
276.50								6	12	8		8	36	42
276.40								6	12	8		8	36	42
276.20								6	12	8		8	36	42
275.90	6	12	8		8	36	42	6	12	8		8	36	42
275.89								6	12	8		8	36	42
275.85								6	12	8		8	36	42
275.60	6	12	8		8	36	42	6	12	8		8	36	42
275.57								6	12	8		8	36	42
275.56								6	12	8		8	36	42
275.54								6	12	8		8	36	42
275.50								6	12	8		8	36	42
275.20								6	12	8		8	36	42
275.00								6	12	8		8	36	42
275.00								6	12	8		8	36	42
274.95								6	12	8		8	36	42
274.90	6	12	8		8	36	42	6	12	8		8	36	42
274.86								7	12	9		9	38	42
274.80								6	12	8		8	36	42
273.02								14.5	12		19	19	53	42
272.95								7.5	12	10		10	39	42
272.82	6	12	8		8	36	42							
SUBTOTAL	36	60	32	16	48	192	210	149	276	179	19	198	850	966
TOTAL	185	336	211	35	246	1042	1176							

CLASS A PATCHING SCHEDULE (CONTINUED)														
SOUTHBOUND PASSING LANE							SOUTHBOUND DRIVING LANE							
MILE MARKER	PATCH LENGTH	PATCH WIDTH	CLASS A TYPE II, 13"	CLASS A TYPE III, 13"	PATCH REINF	SAW CUTS	TIE BARS 3/4"	PATCH LENGTH	PATCH WIDTH	CLASS A TYPE II, 13"	CLASS A TYPE III, 13"	PATCH REINF	SAW CUTS	TIE BARS 3/4"
	FOOT	FOOT	SQ YD	SQ YD	SQ YD	FOOT	EACH	FOOT	FOOT	SQ YD	SQ YD	SQ YD	FOOT	EACH
271.50	6	12	8		8	36	42							
271.60	6	12	8		8	36	42							
272.74	6	12	8		8	36	42							
272.75								6	12	8		8	36	42
277.30	6	12	8		8	36	42							
277.31	6	12	8		8	36	42							
277.33	6	12	8		8	36	42							
277.35								6	12	8		8	36	42
277.40								10	12	13		13	44	42
277.60	15	12		19	19	53	42							
277.60	6	12	8		8	36	42							
278.00	8	12	11		11	40	42							
SUBTOTAL	65	108	67	19	86	345	378	22	36	29	0	29	116	126
TOTAL	87	144	96	19	115	461	504							

CLASS A PATCHING SCHEDULE (UNDER STRUCTURES)							
LOCATION	LANE	PATCH LENGTH	CLASS A	EARTH	PATCH	SAW	TIE
			PATCH TY IV 12"	EX (2)	REINF	CUTS	BARS 3/4"
		FOOT	SQ YD	CU YD	SQ YD	FOOT	EACH
SOUTHBOUND							
STRUCTURE 038-0082 (OVERHEAD)							
938+17 - 939+37	PL	120	160	13.3	160	264	84
938+17 - 939+37	DL	120	160	13.3	160	144	44
STRUCTURE 038-0132 (OVERHEAD)							
1046+83 - 1048+03	PL	120	160	13.3	160	264	84
1046+83 - 1048+03	DL	120	160	13.3	160	144	44
			0				
NORTHBOUND							
STRUCTURE 038-0132 (OVERHEAD)							
1046+83 - 1048+03	PL	120	160	13.3	160	264	84
1046+83 - 1048+03	DL	120	160	13.3	160	144	44
STRUCTURE 038-0076 (OVERHEAD AT BUCKLEY)							
1224+39 - 1227+00	PL	261	348	13.3	348	546	131
1224+39 - 1227+00	DL	261	348	13.3	348	285	44
TOTAL		1,242	1,656	106	1,656	2,055	559

NOTE:
1) FULL LANE WIDTH PATCHES SHALL BE ACCOMPLISHED IN TWO STAGES
2) IF FIELD CONDITIONS PERMIT, EXCAVATE TO MAINTAIN PROPOSED PATCH THICKNESSES AND ATTAIN VERTICAL CLEARANCES

TEMPORARY CONCRETE BARRIER SCHEDULE							
LOCATION	LENGTH	SIDE	TEMPORARY CONCRETE BARRIER		IMPACT ATTENUATOR NON-REDIRECTIVE TL 3		
			INSTALL	RELOCATE	INSTALL	RELOCATE	
			FOOT	FOOT	EACH	EACH	
STAGE 1 - PASSING LANE CLOSURE							
936+15.00 - 939+40.00	325.0	SB	325.0		1		
1044+80.00 - 1048+05.00	325.0	SB	325.0		1		
1173+99.00 - 1176+36.50	237.5	SB	237.5		1		
1184+55.00 - 1187+80.00	325.0	SB	325.0		1		
1223+61.00 - 1225+98.50	237.5	SB	237.5		1		
1046+78.00 - 1050+03.00	325.0	NB	325.0		1		
1176+15.50 - 1178+53.00	237.5	NB	237.5		1		
1186+17.00 - 1189+42.00	325.0	NB	325.0		1		
1224+37.50 - 1229+00.00	462.5	NB	462.5		1		
STAGE 2 - DRIVING LANE CLOSURE							
936+15.00 - 939+40.00	325.0	SB		325.0			1
1044+80.00 - 1048+05.00	325.0	SB		325.0			1
1173+35.00 - 1175+72.50	237.5	SB		237.5			1
1224+20.00 - 1226+57.50	237.5	SB		237.5			1
1046+78.00 - 1050+03.00	325.0	NB		325.0			1
1176+77.50 - 1179+15.00	237.5	NB		237.5			1
1224+37.50 - 1229+00.00	462.5	NB		462.5			1
TOTALS			2,800	2,150	9	7	

SHORT TERM PAVEMENT MARKING SCHEDULE			
LOCATION	LENGTH	SHORT TERM PAVT MARKING	
		INSTALL	REMOVAL
STA	FOOT	FOOT	SQ FT
BUCKLEY RAMPS			
RAMP A	900	432	96
RAMP B	1,350	648	144
RAMP C	1,400	672	150
RAMP D	815	390	86
TOTAL		2,142	476

SMART TRAFFIC CONTROL SCHEDULE		
DESCRIPTION	UNIT	QTY
REAL-TIME TRAFFIC CONTROL SENSOR UNIT	WEEK	112
*CHANGEABLE MESSAGE SIGN, SPECIAL	CAL WK	400
SMART TRAFFIC MONITORING SYSTEM	CAL WK	56
SMART TRAFFIC MONITORING DEVICE	CAL WK	280
TEMPORARY RUMBLE STRIPS (SPECIAL)	EACH	4

*ADDITIONAL CHANGEABLE MESSAGE SIGNS ADDED FOR BUCKLEY INTERCHANGE CLOSURE

TEMPORARY PAVEMENT MARKING SCHEDULE										
LOCATION	LENGTH	PRE-STAGE	STAGE 1				STAGE 2			
			PMK REM WATER BLAST *	TEMP PMK POLYUREA		PMK REM WATER BLAST **	TEMP PMK POLYUREA		PMK REM WATER BLAST ***	
				4" EOP			4" EOP			
				W	Y		W	Y		
STA	TO	STA	FOOT	SQ FT	FOOT	FOOT	SQ FT	FOOT	FOOT	SQ FT
SOUTHBOUND LANES (STATIONS INCREASING SOUTH)										
894+00	TO	904+00.00	1,000	333	0	1,000	333	1,000	0	333
904+00	TO	910+80.00	680	227	680	680	453	680	680	453
910+80	TO	950+06.50	3,927	60	3,927	3,927	2,618	3,927	3,927	2,618
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH										
950+05.83	TO	1084+87.34	13,482	51	13,482	13,482	8,988	13,482	13,482	8,988
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH										
1084+87.07	TO	1135+04.55	5,017		5,017	5,017	3,345	5,017	5,017	3,345
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH										
1135+04.13	TO	1222+61.96	8,758	349	8,958	8,758	5,905	9,208	8,758	5,989
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK										
1222+60.95	TO	1267+32.14	4,471	267	4,671	4,471	3,047	5,296	4,471	3,256
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH										
1267+31.76	TO	1269+02	170		170	170	113	170	170	113
1269+02.00	TO	1272+50	348	116	348	348	232	348	348	232
SOUTHBOUND TOTAL			37,853	1,403	75,106	25,034	75,981	25,327		
NORTHBOUND LANES (STATIONS INCREASING SOUTH)										
908+50	TO	910+80.00	230	77	230	230	153	230	230	153
910+80	TO	950+06.50	3,927	61	3,927	3,927	2,618	3,927	3,927	2,618
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH										
950+05.83	TO	1084+87.34	13,482	52	13,482	13,482	8,988	13,482	13,482	8,988
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH										
1084+87.07	TO	1135+04.55	5,017		5,017	5,017	3,345	5,017	5,017	3,345
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH										
1135+04.13	TO	1222+61.96	8,758	316	8,858	8,758	5,872	9,558	8,758	6,105
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK										
1222+60.95	TO	1267+32.14	4,471	267	4,671	4,471	3,047	4,871	4,471	3,114
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH										
1267+31.76	TO	1269+02	170		170	170	113	170	170	113
1269+02.00	TO	1276+90	788	263	788	788	525	788	788	525
1276+90.00	TO	1286+90	1,000	333	0	1,000	333	1,000	0	333
NORTHBOUND TOTAL			37,843	1,369	74,986	24,994	75,886	25,294		
GRAND TOTAL			75,696	2,772	150,091	50,028	151,866	50,621		

* REMOVE ANY CONFLICTING PAVEMENT MARKING PRIOR STAGE 1
 ** REMOVE ANY CONFLICTING PAVEMENT MARKING PRIOR STAGE 2
 *** REMOVE ANY CONFLICTING PAVEMENT MARKING PRIOR STAGE 3

MAINLINE PAVEMENT MARKING / RRP M SCHEDULE												
LOCATION			LENGTH	POLYUREA PMK TY 2 (2 APPS)**		PREF PL PMK TY D	GRV RCSD PVT MRKG			RAISED REFLECTIVE PAVEMENT MARKERS		
				4"		8"	6"				INSTALL*	REMOVAL
EOP		GORE		CL SKIPS				ONE-WAY				
WHITE	YELLOW	WHITE		WHITE	5"	7"	9"	CRYSTAL				
STA	TO	STA	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH
SOUTHBOUND LANES (STATIONS INCREASING SOUTH)												
910+80	TO	950+06.50	3,927	7,853	7,853		980	7,853	890		98	98
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH												
950+05.83	TO	1084+87.34	13,482	26,963	26,963		3,370	26,963	3,320		337	337
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH												
1084+87.07	TO	1135+04.55	5,017	10,035	10,035		1,250	10,035	1,250		125	125
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH												
1135+04.13	TO	1211+45	7,641	15,282	15,282		1,910	15,282	1,910		191	191
1211+45	TO	1214+29	284	0	568	144	70	284	70	72	7	7
1214+29	TO	1218+83	454	0	908	908	110	454	110	454	22	22
1218+83	TO	1222+61.96	379	758	758		90	758	90		9	9
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK												
1222+60.95	TO	1236+39	1,378	2,756	2,756		340	2,756	340		34	34
1236+39	TO	1240+11	372	0	744	744	90	372	90	372	18	18
1240+11	TO	1247+54	743	0	1,486		190	743	190		19	19
1247+54	TO	1267+32.14	1,978	3,956	3,956		490	3,956	490		49	49
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH												
1267+31.76	TO	1269+02	170	340	340		40	340	40		4	4
SOUTHBOUND TOTAL			35,825	67,944	71,650	1,796	8,930	69,797	8,790	898	913	913
NORTHBOUND LANES (STATIONS INCREASING SOUTH)												
910+80	TO	950+06.50	3,927	7,853	7,853		980	7,853	890		98	98
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH												
950+05.83	TO	1084+87.34	13,482	26,963	26,963		3,370	26,963	3,290		337	337
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH												
1084+87.07	TO	1135+04.55	5,017	10,035	10,035		1,250	10,035	1,250		125	125
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH												
1135+04.13	TO	1203+37	6,833	13,666	13,666		1,710	13,666	1,710		171	171
1203+37	TO	1210+98	761	0	1,522		190	761	190		19	19
1210+98	TO	1214+45	347	0	694	694	90	347	90	347	18	18
1214+45	TO	1222+61.96	817	1,634	1,634		200	1,634	200		20	20
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK												
1222+60.95	TO	1233+52	1,091	2,182	2,182		270	2,182	270		27	27
1233+52	TO	1236+67	315	0	630	630	80	315	80	315	16	16
1236+67	TO	1241+10	443	0	886	220	110	443	110	110	11	11
1241+10	TO	1267+32.14	2,622	5,244	5,244		660	5,244	660		66	66
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH												
1267+31.76	TO	1269+02	170	340	340		40	340	40		4	4
NORTHBOUND TOTAL			35,825	67,918	71,650	1,544	8,950	69,784	8,780	772	912	912
GRAND TOTAL			71,650	279,160	3,340	17,880	139,580	17,570	1,670	1,825	1,825	

NOTES:
 *REFER TO PLAN DETAIL FOR RRP M APPLICATIONS AT INTERSTATE
 **POLYUREA PAVEMENT MARKING SHALL REQUIRE TWO APPLICATIONS WITH 7 DAYS BETWEEN APPLICATIONS (SEE SPECIAL PROVISION)

BUCKLEY INTERCHANGE RAMP PAVEMENT MARKING / RRPV SCHEDULE															
LOCATION			LENGTH	POLYUREA PMK TY 2 (2 APPS)**			PREF PL PMK TY D		GRV RCSD PVT MRKG				RAISED REFLECTIVE PAVEMENT MARKER*		
				4"		8"	12"	24"	LTR & SYMB				LTR & SYMB	INSTALL	
EOP				ISL. DIAG.	STOP BAR						CRYSTAL/ RED	AMBER/ RED			
WHITE	YELLOW	WHITE		WHITE	WHITE	WHITE	5"	9"	25"	WHITE					
STA	TO	STA	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	FOOT	FOOT	FOOT	SQ FT	EACH	EACH	EACH
RAMP A - SE RAMP (NB I-57 ENTRANCE RAMP)															
0+14	TO	7+84	770	1,540	1,540				1,540						
7+84	TO	11+28	344	688		688			344	344					
11+28	TO	18+88	760	1,520					760						
RAMP B - SE RAMP (NB I-57 EXIT RAMP)															
0+00	TO	4+39	439	878					439				22		22
4+39	TO	7+51	312	624		624			312	312			24		24
7+51	TO	12+29	478	956	956				956				12	12	24
12+29	TO	14+58	229	458	458				458				3	3	6
14+58	TO	18+52	394	788	788				788				10	10	20
18+52	TO	19+72	120	240	240			24,3	240				3	3	6
19+72	TO	20+72	100	200	200	312	208	32	31,2	200	156	32	31,2		
RAMP C - NW RAMP (SB I-57 EXIT RAMP)															
0+00	TO	2+82	282	564	0				282				14		14
2+82	TO	7+36	454	908	0	908			454	454			34		34
7+36	TO	11+30	394	788	788				788				10	10	20
11+30	TO	13+88	258	516	516				516				3	3	6
13+88	TO	18+23	435	870	870				870				11	11	22
18+23	TO	19+43	120	240	240			24,3	240				3	3	6
19+43	TO	20+42	99	198	198	314	202	32	31,2	198	157	32	31,2		
RAMP D - SW RAMP (SB I-57 ENTRANCE RAMP)															
0+12	TO	8+16	804	1,608	1,608				1,608						
8+16	TO	11+92	376	752	0	752			376	376					
11+92	TO	19+43	751	1,502	0				751						
TOTAL			7,919	24,240	3,598	410	64	111	12,120	1,799	64	62	204	204	

NOTES:
 *REFER TO PLAN DETAIL FOR RRPV APPLICATIONS AT EXIT RAMPS
 **POLYUREA PAVEMENT MARKING SHALL REQUIRE TWO APPLICATIONS WITH 7 DAYS BETWEEN APPLICATIONS (SEE SPECIAL PROVISION)

BUCKLEY INFIELD WORK SCHEDULE									
LOCATION	TREE REMOVAL (6 - 15" DIA)	TREE REMOVAL (>15" DIA)	TOPSOIL EX. & PLACEMENT (SPL)	PERIMETER EROSION BARRIER	MULCH METHOD 2	SEEDING CLASS 2	FERTILIZER NUTRIENTS		
	*	*			**		NITRO	PHOSP	POT
	UNIT	UNIT	CU YD	FOOT	ACRE	ACRE	POUND	POUND	POUND
BUCKLEY INTERCHANGE									
NW INFIELD	30	80	329	930	0.625	0.625	56	56	56
SE INFIELD	30	80	305	800	0.625	0.625	56	56	56
TOTAL	60	160	634	1,730	1.25	1.25	113	113	113

* TO BE DETERMINED IN THE FIELD
 ** MULCH METHOD 2, PROCEDURE 1 SHALL BE USED.

DRAINAGE SCHEDULE

STA	CULVERT	SIDE / RAMP	LENGTH		MATERIAL	FLOW DIRECTION	CLEAN EX END SEC EACH	FLAR END SEC		PIPE CULV CLEAN 36" FOOT	FURN EX ** CU YD	STONE RIPRAP CL A4 SQ YD	FILTER FABRIC SQ YD	SS REM 24" FOOT	SS REM 21" FOOT	SS TY A CL 2 21" FOOT	SS TY A CL 2 24" FOOT	REMARKS
			FOOT	INCH				REM EX EACH	INSTALL PRC 24" EACH									
I-57 MAINLINE																		
915+00	3905	NB	87	24	RCP	SE	1				2							CLEAN END SECTION
925+50	3904	NB	112	24	RCP	E	1				2							FILL EROSION AT HEADWALL AND MEDIAN INLET
931+50	3903	NB	100	24	RCP	E	1				2							FILL EROSION AT MEDIAN INLET
938+68	3901	NB	112	36	CMP	N			112									UNDER OVERHEAD STR 038-0082, APPROX 3-6" SEDIMENT
938+68	3902	SB	130	36	CMP	N	2		130									UNDER OVERHEAD STR 038-0082, APPROX 3-6" SEDIMENT
941+00	3900	NB	90	24	RCP	E	1				2							FILL EROSION AT MEDIAN INLET
961+25	3899	NB	104	24	RCP	E	1				3							FILL EROSION AT MEDIAN INLET
965+75	--	NB	94	24	RCP	E	1											CLEAN END SECTION
975+00	3898	NB	88	24	RCP	E		1	1					9			9	REMOVE LAST SECTION OF PIPE (BAD JOINTS) AND REPLACE WITH NEW PIPE & END SECTION ON EAST OUTLET
992+00	3897	NB	86	24	RCP	E	1				2	9.0	9.0					INSTALL RIPRAP + FABRIC AT END SECTION, FILL EROSION AT MEDIAN INLET
1005+00	3896	SB	87	24	RCP	W					2	8.9	8.9					INSTALL RIPRAP + FABRIC AT END SECTION
1012+25	3895	X-RD	174	36	RCP	W	2											CLEAN END SECTION @ NB/SB SIDE
1017+15	--	X-RD	380	21	RCP	W								32	32			REMOVE AND REPLACE 32' OF FIELD TILE AT DITCH AREA
1023+00	3894	SB	98	24	RCP	W					3							FILL EROSION AT MEDIAN INLET
1041+00	3893	NB	88	24	RCP	E		1	1		2							FILL EROSION AT MEDIAN INLET, REPLACE END SEC
1047+43	3891	SB	108	36	CMP	N			108									UNDER OVERHEAD STR 038-0082, APPROX 3-6" SEDIMENT
1047+43	3892	NB	108	36	CMP	N	2											UNDER OVERHEAD STR 038-0082, APPROX 3-6" SEDIMENT
1051+00	3890	NB	74	24	RCP	E					2							FILL EROSION AT MEDIAN END GRATED SECTION
1060+00	3889	X-RD	172	24	RCP	E					2	5.3	5.3					ADD RIPRAP ON NB SIDE, FILL EROSION AT MEDIAN INLET
1068+00	3888	X-RD	160	36	RCP	E	1					5.3	5.3					ADD RIPRAP ON NB SIDE, CLEAN DEBRIS AT SLOPED GRATED END SECTION
1074+00	3887	NB	74	24	RCP	E	1											CLEAN NB END SEC.
1083+00	3886	NB	74	24	RCP	E	1					5.3	5.3					CLEAN END SEC, ADD RIPRAP ON NB SIDE,
1093+00	3885	X-RD	174	48	RCP	E					4	36.8	36.8					ADD RIPRAP ON SB (14'X14') & NB (9'X15') SIDES, FILL EROSION AT MEDIAN INLET
1102+27	3884	X-RD	182	2x36	RCP	E					2	20.0	20.0					ADD RIPRAP ON NB SIDE, FILL EROSION BY INLET
1108+00	3883	X-RD	174	24	RCP	E	1					5.6	5.6					ADD RIPRAP ON SB SIDE, CLEAN NB END SECTION
1115+00	3882	NB	74	24	RCP	E	1											CLEAN NB END SEC.
1128+47	3881	NB	75	24	RCP	E	1	1	1									
1135+00	3880	NB	74	24	RCP	E												NO WORK
1164+00	3879	SB	88	24	RCP	NW	1				2							FILL EROSION AT MEDIAN INLET, CLEAN END SEC
1174+00	3878	SB	88	24	RCP	NW	1				2							FILL EROSION AT MEDIAN INLET, CLEAN END SEC
1175+00	3877	SB	144	36	RCP	SW	2				3							UNDER STR 038-0082, FILL EROSION BY SW PAVED DITCH
1181+29	3876	SB	94	24	RCP	NW	1				2							FILL EROSION AT MEDIAN INLET AREA
1190+00	3875	NB	86	24	RCP	SE	1				3							FILL EROSION AT MEDIAN INLET AREA
1205+00	3874	NB	90	24	RCP	SE	1				2							FILL EROSION AT MEDIAN INLET AREA
1217+00	3873	NB	88	24	RCP	SE	1				2							FILL EROSION AT MEDIAN INLET AREA
1226+96	3868	X-RD	276	36	RCP	E	1				2							FILL EROSION AT MEDIAN INLET AREA, CLEAN DEBRIS AT NB SLOPED GRATED END SECTION
1238+00	3867	NB	104	24	RCP	E					2							FILL EROSION AT MEDIAN INLET
1262+00	3866	SB	86	24	RCP	W	1				2							FILL EROSION AT MEDIAN INLET AREA
BUCKLEY INTERCHANGE RAMPS																		
2+00	3871	A	204	24	RCP	E	1											CLEAN EAST END SECTIONS
18+87	3870	B	206	36	RCP	E	2				8							CLEAN END SECTIONS, FILL EROSION AREAS AT END SECTIONS
9+40	3872	C	78	24	RCP	NW	2											CLEAN END SECTIONS
1+20	3869	D	168	36	RCP	E	2			168								CLEAN END SECTIONS, APPROX 4-6" SEDIMENT
TOTAL							36	3	3	518	60	96	96	9	32	32	9	

** FURNISHED EX TO BE USED TO FILL LOW SPOTS, AND DIPS CAUSED BY EROSION

GRADING & SHAPING DITCHES, EMBANKMENT REGRADING & LANDSCAPING SCHEDULE

STATION	LENGTH	GRADE & SHAPE DITCH	EROSION CONTROL BLANKET	FURN EX	SEED CLASS 2A	FERTILIZER NUTRIENTS			TEMP EROS CNTRL SEED	TEMP DITCH CHECK	FLOW DIRECTION	REMARKS
						NITRO	PHOSP	POT				
FROM - TO	FOOT	FOOT	SQ YD	CU YD	ACRE	POUND	POUND	POUND	POUND	FOOT		
SOUTHBOUND												
919+50 - 922+00	250	250	277.8		0.06	5.2	5.2	5.2	5.8	20	SOUTH	
935+00 - 936+25	125	125	138.9		0.03	2.6	2.6	2.6	2.9	20	NORTH	CULV AT 938+68
958+00 - 960+00	200	200	222.2		0.05	4.1	4.1	4.1	4.6	20	SOUTH	
971+00 - 974+00	300	300	333.3		0.07	6.2	6.2	6.2	6.9	30	NORTH	
1004+80 - 1005+20	40	40	44.4		0.01	0.8	0.8	0.8	0.9		SOUTH	CULV AT 1005+00
1018+00 - 1024+00	600	600	666.7		0.14	12.4	12.4	12.4	13.8	40	NORTH	
1024+00 - 1025+50	150	150	166.7		0.03	3.1	3.1	3.1	3.5	20	NORTH	CULV AT 1023+00
1025+50 - 1027+00	150	150	166.7		0.03	3.1	3.1	3.1	3.5	20	NORTH	
1044+00 - 1046+83	283	283	314.4		0.06	5.9	5.9	5.9	6.5	30	NORTH	CULV AT 1047+43
1059+75 - 1060+25	50	50	55.6		0.01	1	1	1	1.2	20	TO CULV	CULV AT 1060+00
1160+00 - 1162+00	200	200	222.2		0.05	4.1	4.1	4.1	4.6	20	SOUTH	
1162+00 - 1169+00	700	700	777.8		0.16	14.5	14.5	14.5	16.1	40	SOUTH	CULV AT 1164+00
1169+00 - 1174+25	525	525	583.3		0.12	10.9	10.9	10.9	12.1	50	SOUTH	CULV AT 1174+00,1175+00
1176+00 - 1179+50	350	350	388.9		0.08	7.2	7.2	7.2	8.1	30	SOUTH	
1179+50 - 1181+30	180	180	200		0.04	3.7	3.7	3.7	4.1	10	SOUTH	CULV AT 1181+29
1200+00 - 1204+00	400	400	444.4		0.09	8.3	8.3	8.3	9.2	40	NORTH	
1261+50 - 1263+00	150	150	166.7		0.03	3.1	3.1	3.1	3.5	20	SOUTH	CULV AT 1262+00
NORTHBOUND												
911+00 - 913+00	200	200	222.2		0.05	4.1	4.1	4.1	4.6	20	SOUTH	
913+00 - 920+50	750	750	833.3		0.17	15.5	15.5	15.5	17.3	50	SOUTH	CULV AT 915+00
920+50 - 924+00	350	350	388.9		0.08	7.2	7.2	7.2	8.1	20	SOUTH	
924+00 - 927+50	350	350	388.9		0.08	7.2	7.2	7.2	8.1	30	SOUTH	CULV AT 925+50
929+00 - 932+50	350	350	388.9		0.08	7.2	7.2	7.2	8.1	30	NORTH	CULV AT 931+50
932+50 - 934+00	150	150	166.7		0.03	3.1	3.1	3.1	3.5	10	NORTH	
935+50 - 937+70	220	220	244.4		0.05	4.6	4.6	4.6	5.1	20	NORTH	CULV AT 938+68
939+10 - 942+00	290	290	322.2		0.07	6	6	6	6.7	30	NORTH	CULV AT 941+00
961+00 - 962+50	150	150	166.7		0.03	3.1	3.1	3.1	3.5	20	SOUTH	CULV AT 961+25
965+25 - 966+00	75	75	83.3		0.02	1.6	1.6	1.6	1.7	10	NORTH	CULV AT 965+75
966+00 - 969+00	300	300	333.3		0.07	6.2	6.2	6.2	6.9	20	NORTH	
969+00 - 975+25	625	625	694.4		0.14	12.9	12.9	12.9	14.4	40	NORTH	CULV AT 975+00
975+25 - 983+00	775	775	861.1		0.18	16	16	16	17.8	60	NORTH	
991+80 - 992+20	40	40	44.4		0.01	0.8	0.8	0.8	0.9		NORTH	CULV AT 992+00
1040+50 - 1041+50	100	100	111.1		0.02	2.1	2.1	2.1	2.3	20	NORTH	CULV AT 1041+00
1045+25 - 1046+80	155	155	172.2		0.04	3.2	3.2	3.2	3.6	20	NORTH	CULV AT 1047+43
1050+80 - 1061+00	1020	1020	1133.3		0.23	21.1	21.1	21.1	23.5	80	NORTH	CULV AT 1051+00, 1060+00
1175+00 - 1176+00	100	100	111.1		0.02	2.1	2.1	2.1	2.3	20	SOUTH	
1187+50 - 1191+00	350	350	388.9		0.08	7.2	7.2	7.2	8.1	30	NORTH	CULV AT 1190+00
1195+50 - 1199+00	350	350	388.9		0.08	7.2	7.2	7.2	8.1	30	NORTH	
1199+00 - 1210+00	1100	1100	1222.2		0.25	22.8	22.8	22.8	25.3	80	NORTH	CULV AT 1205+00
1233+00 - 1235+00	200	200	222.2		0.05	4.1	4.1	4.1	4.6	20	NORTH	
BUCKLEY INTERCHANGE												
1+30 - 2+00	70	70	77.8		0.02	1.4	1.4	1.4	1.6	20	NORTH	CULV AT 2+00 (RAMP A)
2+00 - 2+25	25	25	27.8		0.01	0.5	0.5	0.5	0.6		NORTH	CULV AT 2+00 (RAMP A)
19+90 - 20+06	30	30	33.3		0.01	0.6	0.6	0.6	0.7		EAST	CULV AT 18+87 (RAMP B)
8+60 - 10+75	215	215	238.9		0.05	4.5	4.5	4.5	4.9	20	NORTH	CULV AT 9+40 (RAMP C)
1+28 - 2+78	150	150	166.7		0.03	3.1	3.1	3.1	3.5	20	NORTH	CULV AT 1+20 (RAMP D)
1+10 - 2+50	234	234	260		0.05	4.8	4.8	4.8	5.4	30	NORTH / EAST	CULV AT 1+20 (RAMP D)
038-0132 (AT EMBANKMENT CONES - FACING APPROACHING TRAFFIC)												
* 1046+75 - 1047+20	45	-	22.2	15	0.01	0.4	0.4	0.4	0.5		TO CULV	SB DIRECTION
* 1047+50 - 1048+00	50	-	22.2	15	0.01	0.4	0.4	0.4	0.5		TO CULV	NB DIRECTION
TOTALS	13,472	13,377	14,908	30	3	277	277	277	310	1,180		

STATIONS ARE APPROXIMATE
* SEE PLAN DETAILS FOR REGRADING REQUIREMENTS

ENVIRONMENTAL QUANTITIES		
PAY ITEM	UNIT	QUANTITY
NON-SPECIAL WASTE DISPOSAL	CU YD	100
REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1
REGULATED SUBSTANCES MONITORING	CAL DA	30
REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1
SOIL DISPOSAL ANALYSIS	EACH	1
EARTH EXCAVATION	CU YD	100

* ITEM TO BE MANAGED AS PER ARTCILE 669.05(a)(5).
SEE SPECIAL PROVISIONS FOR ADDITIONAL REQUIREMENTS

UNDERDRAIN REMOVAL					
STATION	LENGTH	SIDE	PIPE	REM CONC	SHLDR
FROM - TO	FOOT		UD	HDWL FOR	REM
			REM	PIPE DRAINS	& RPL
			FOOT	EACH	FOOT
I-57 SOUTHBOUND LANES					
909+00 - 925+50	1,650	PL/DL	3,460	8	3,364
925+50 - 927+86	236	PL/DL	472		536
STR 038-0155 (SB) @ 928+50					
929+35 - 945+00	1,565	PL/DL	3,410	14	3,194
945+00 - 963+05	1,805	PL/DL	3,610		3,674
STR 038-0153 (SB) @ 963+62.50					
964+40 - 985+00	2,060	PL/DL	4,280	8	4,184
985+00 - 1006+00	2,100	PL/DL	4,400	10	4,264
1006+00 - 1031+00	2,500	PL/DL	5,160	8	5,064
1031+00 - 1045+50	1,450	PL/DL	3,020	6	2,964
1045+50 - 1149+00	10,350	PL/DL	21,340	33	20,764
1149+00 - 1186+52	3,752	PL/DL	7,784	14	7,568
STR 038-0151 (SB) @ 1187+00					
1187+72 - 1212+00	2,428	PL/DL	5,096	12	4,920
1212+00 - 1220+00	800	PL	818	1	864
1220+00 - 1237+00	1,700	PL/DL	3,560	8	3,464
1237+00 - 1242+00	500	PL	540	2	564
1242+00 - 1251+00	900	PL/DL	1,840	2	1,864
1251+00 - 1270+88	1,988	PL/DL	4,176	9	4,040
1270+88 - 1271+81	93	DL	115	1	157
-					
SB LANES SUBTOTAL	35,877	--	73,081	136	71,449
I-57 NORTHBOUND LANES					
909+00 - 925+50	1,650	PL/DL	3,460	8	3,364
925+50 - 927+85	235	PL/DL	470		534
STR 038-0156 (NB) @ 928+50					
929+33 - 930+05	72	PL	90	1	136
930+05 - 945+00	1,495	PL/DL	3,110	5	3,054
945+00 - 962+48	1,748	PL/DL	3,656	8	3,560
962+48 - 962+78	30	PL	48	1	94
STR 038-0154 (NB) @ 963+62.50					
964+64 - 964+83	19	PL	37	1	83
964+83 - 985+00	2,017	PL/DL	4,194	7	4,098
985+00 - 1006+00	2,100	PL/DL	4,400	10	4,264
1006+00 - 1031+00	2,500	PL/DL	5,200	9	5,064
1031+00 - 1045+50	1,450	PL/DL	3,020	6	2,964
1045+50 - 1149+00	10,350	PL/DL	21,300	30	20,764
1149+00 - 1186+46	3,746	PL/DL	7,732	12	7,556
STR 038-0152 (NB) @ 1187+00					
1187+83 - 1188+25	42	DL	64	1	106
1188+25 - 1212+00	2,375	PL/DL	4,950	10	4,814
1212+00 - 1215+60	360	PL	378	1	424
1215+60 - 1232+20	1,660	PL/DL	3,480	7	3,384
1232+20 - 1233+00	80	PL	98	1	144
1233+00 - 1237+22	422	PL	440	1	486
1237+32 - 1238+00	68	PL	86	1	132
1238+00 - 1251+00	1,300	PL/DL	2,680	4	2,664
1251+00 - 1271+19	2,019	PL/DL	4,198	7	4,102
1271+19 - 1271+88	69	PL	87	1	133
-					
-					
NB LANES SUBTOTAL	35,807	--	73,178	132	71,924

UNDERDRAIN REMOVAL (CONTINUED)					
STATION	LENGTH	SIDE	PIPE	REM CONC	SHLDR
FROM - TO	FOOT		UD	HDWL FOR	REM
			REM	PIPE DRAINS	& RPL
			FOOT	EACH	FOOT
BUCKLEY INTERCHANGE (CH 9)					
RAMP A - NB I-57 ENTRANCE RAMP					
0+12 - 4+50	438	LT/RT	912	2	940
4+50 - GORE	355	LT	355	0	419
4+50 - 10+44	594	RT	612	1	658
			0		64
RAMP B - NB I-57 EXIT RAMP					
0+00 - 8+40	840	RT	858	1	904
GORE - 8+40	180	LT	198	1	244
8+40 - 20+71	1,231	LT/RT	2,498	2	2,526
			0		64
RAMP C - SB I-57 EXIT RAMP					
0+55 - 11+00	1,045	RT	1,081	2	1,109
GORE - 11+00	208	LT	226	1	272
11+00 - 20+42	942	LT/RT	1,956	4	1,948
			0		64
RAMP D - SB I-57 ENTRANCE RAMP					
0+12 - 5+00	488	LT/RT	1,012	2	1,040
5+00 - 8+70	370	RT	388	1	434
5+00 - GORE	366	LT	366	0	430
8+70 - 13+90	520	RT	520	0	584
SUBTOTAL	7,577	--	10,982	17	11,700
GRAND TOTALS	79,261	--	157,241	285	155,073

* NOTE - FOR MAINLINE UNDERDRAINS, SEE EXISTING DETAILS (AS-BUILTS).

** AVOID DAMAGES TO EXISTING BRIDGE APPROACHES AND CONNECTORS.

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	UNDERDRAIN REMOVAL SCHEDULES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ec:\pw_work\pwwd\calderont\0521537\0386H63-eh-schedule.dgn		DRAWN -	REVISED -			57	(38-7,38-6)RS	IROQUOIS	130	33	
SMODELNAMES	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

UNDERDRAIN SCHEDULE							
STATION & DIRECTION OF FLOW (←/→)	LENGTH	SIDE	PROP OUTLET	PIPE UD TY 3	PIPE UD 4" SP	CONC HDWL FOR P DRAINS	REMARKS
FROM - TO	FOOT		STA	FOOT	FOOT	EACH	
I-57 SOUTHBOUND LANES							
909+00 → 915+00	600	DL/PL	915+00	1,200	80	4	REPLACE OUTLETS AT 909+00
915+00 → 920+50	550	DL/PL	920+50	1,100	40	2	
920+50 → 924+50	400	DL/PL	925+50	800	40	2	COMMON OUTLET @ 925+50 FROM 920+50 TO 927+86
924+50 → 925+50	100	DL/PL		200			
925+50 ← 927+00	150	DL/PL		300			
927+00 ← 927+60	60	DL/PL		120			
** STR 038-0155 (SB) @ 928+50							
929+40 ← 935+00	560	DL/PL	929+40	1,120	40	2	
935+00 ← 940+00	500	DL/PL	935+00	1,000	40	2	
940+00 ← 943+00	300	DL/PL	940+00	600	40	2	
943+00 ← 945+00	200	DL/PL		400			
945+00 → 947+00	200	DL/PL	950+00	400	40	2	
947+00 → 950+00	300	DL/PL		600			
950+00 → 955+00	500	DL/PL	955+00	1,000	40	2	
955+00 → 960+00	500	DL/PL	960+00	1,000	40	2	
960+00 → 962+50	250	DL/PL	962+50	500	22	1	
960+00 → 963+05	305	PL	963+05	305	18	1	
** STR 038-0153 (SB) @ 963+62.50							
964+45 ← 969+00	455	DL/PL	REMARKS	910	40	2	964+40 & 964+62
969+00 ← 975+00	600	DL/PL	969+00	1,200	40	2	
975+00 ← 980+00	500	DL/PL	980+00	1,000	40	2	
980+00 ← 983+50	350	DL/PL		700			
983+50 ← 985+00	150	DL/PL		300			
985+00 → 990+00	500	DL/PL	990+00	1,000	40	2	
990+00 → 995+00	500	DL/PL	995+00	1,000	40	2	
995+00 → 1000+00	500	DL/PL	1000+00	1,000	40	2	
1000+00 → 1004+00	400	DL/PL	1004+00	800	40	2	
1004+00 → 1006+00	200	DL/PL	1006+00	400	40	2	COMMON OUTLET @ 1006+00 FROM 1004+00 TO 1011+00
1006+00 ← 1009+00	300	DL/PL		600			
1009+00 ← 1011+00	200	DL/PL		400			
1011+00 ← 1016+00	500	DL/PL	1011+00	1,000	40	2	
1016+00 ← 1021+00	500	DL/PL	1016+00	1,000	40	2	
1021+00 ← 1026+00	500	DL/PL	1026+00	1,000	40	2	
1026+00 ← 1029+00	300	DL/PL		600			
1029+00 ← 1031+00	200	DL/PL		400			
1031+00 → 1033+00	200	DL/PL	1036+00	400	40	2	
1033+00 → 1036+00	300	DL/PL		600			
1036+00 → 1040+50	450	DL/PL	1040+50	900	40	2	
1040+50 → 1042+00	150	DL/PL	1045+50	300	40	2	COMMON OUTLET @ 1045+50 FROM 1040+50 TO 1050+50
1042+00 → 1043+00	100	DL/PL		200			
1043+00 → 1045+50	250	DL/PL		500			
1045+50 ← 1050+50	500	DL/PL		1,000			
1050+50 ← 1055+50	500	DL/PL	1050+50	1,000	40	2	
1055+50 ← 1059+95	445	DL/PL	1055+50	890	40	2	
1060+05 ← 1064+00	395	DL/PL	1060+05	790	40	2	PIPE CULVERT AT 1060+00
1064+00 ← 1067+95	395	DL/PL	1064+00	790	40	2	
1068+05 ← 1072+00	395	DL/PL	1068+05	790	40	2	PIPE CULVERT AT 1068+00
1072+00 ← 1073+00	100	DL/PL		200			
1073+00 ← 1078+00	500	DL/PL	1073+00	1,000	40	2	
1078+00 ← 1083+00	500	DL/PL	1078+00	1,000	40	2	
1083+00 ← 1087+00	400	DL/PL	1083+00	800	40	2	
1087+00 ← 1088+00	100	DL/PL		200			

UNDERDRAIN SCHEDULE (CONTINUED)							
STATION & DIRECTION OF FLOW (←/→)	LENGTH	SIDE	PROP OUTLET	PIPE UD TY 3	PIPE UD 4" SP	CONC HDWL FOR P DRAINS	REMARKS
FROM - TO	FOOT		STA	FOOT	FOOT	EACH	
I-57 SOUTHBOUND LANES (CONTINUED)							
1088+00 ← 1092+93	493	DL/PL	1088+00	986	40	2	
1093+07 ← 1098+00	493	DL/PL	1093+07	986	40	2	
1098+00 ← 1103+00	500	DL/PL	1098+00	1,000	40	2	
1103+00 ← 1107+95	495	DL/PL	1103+00	990	40	2	
1108+05 ← 1109+50	145	DL/PL	1108+05	290	40	2	PIPE CULVERT AT 1108+00
1109+50 ← 1113+00	350	DL/PL		700			
1113+00 ← 1118+00	500	DL/PL	1113+00	1,000	40	2	
1118+00 ← 1123+00	500	DL/PL	1118+00	1,000	40	2	
1123+00 ← 1128+47	547	DL/PL	1123+00	1,094	40	2	
1128+50 ← 1134+00	550	DL/PL	1128+50	1,100	40	2	EX OUTLETS @ 1128+50, 1128+89
1134+00 ← 1139+00	500	DL/PL	1134+00	1,000	40	2	
1139+00 ← 1144+00	500	DL/PL	1139+00	1,000	40	2	
1144+00 ← 1147+00	300	DL/PL	1144+00	600	40	2	
1147+00 ← 1149+00	200	DL/PL		400			
1149+00 → 1151+00	200	DL/PL	1154+00	400	40	2	
1151+00 → 1154+00	300	DL/PL		600			
1154+00 → 1159+00	500	DL/PL	1159+00	1,000	40	2	
1159+00 → 1163+00	400	DL/PL	1163+00	800	40	2	
1163+00 → 1168+00	500	DL/PL	1168+00	1,000	40	2	
1168+00 → 1173+00	500	DL/PL	1173+00	1,000	40	2	
1173+00 → 1177+00	400	DL/PL	1177+00	800	40	2	
1177+00 → 1180+00	300	DL/PL	1181+50, 1181+25	600	40	2	COMMON OUTLETS @ 1181+50(LT) FOR 1177+00 TO 1186+25
1180+00 → 1181+50	150	DL/PL		300			
1181+50 ← 1182+50	100	DL/PL		200			
1182+50 ← 1186+25	375	DL/PL		750			
** STR 038-0151 (SB) @ 1187+00							
1187+85 ← 1192+00	415	DL/PL	REMARKS	830	40	2	1187+72, 1187+74
1192+00 ← 1197+00	500	DL/PL	1192+00	1,000	40	2	
1197+00 ← 1202+00	500	DL/PL	1197+00	1,000	40	2	
1202+00 ← 1207+00	500	DL/PL	1202+00	1,000	40	2	
1207+00 ← 1212+00	500	DL/PL	1207+00	1,000	40	2	
1212+00 ← 1217+00	500	PL	1212+00	500	18	1	
1217+00 ← 1222+00	500	PL	1217+00	500	18	1	
1220+00 ← 1222+00	200	DL	1220+00	200	22	1	
1222+00 ← 1227+15	515	DL/PL	1222+00	1,030	40	2	SKEWED CULV AT 1226+96
1227+15 ← 1232+00	485	DL/PL	REMARKS	970	40	2	1227+00, 1227+65
1232+00 ← 1237+00	500	DL/PL	1232+00	1,000	40	2	
1237+00 ← 1242+00	500	PL	1237+00	500	18	1	
1242+00 ← 1247+00	500	DL/PL	1242+00	1,000	40	2	
1247+00 ← 1249+00	200	DL/PL	1247+00	400	40	2	
1249+00 ← 1251+00	200	DL/PL		400			
1251+00 → 1252+00	100	DL/PL	1256+00	200	40	2	
1252+00 → 1256+00	400	DL/PL		800			
1256+00 → 1260+00	400	DL/PL	1260+00	800	40	2	
1260+00 → 1264+00	400	DL/PL	1264+00	800	40	2	
1264+00 → 1268+00	400	DL/PL	1268+00	800	40	2	
1268+00 → 1270+88	288	DL	1270+88	288	22	1	
1268+00 → 1271+81	381	PL	1271+81	381	18	1	
SB LANES SUBTOTAL	36,492	--	--	70,310	2,876	144	

NOTES:

- * MAY REQUIRE ADJUSTMENT IN THE FIELD FOR OUTLETS AND UNDERDRAIN
- ** TO AVOID DAMAGE TO BRIDGE APPROACHES AND CONNECTORS PROPOSED UNDERDRAIN MAY BE TIED TO EXISTING (IF REQUIRED)
- NOTE: PROVIDE AS BUILTS OF INSTALLED UNDERDRAINS AND OUTLET LOCATIONS

UNDERDRAIN SCHEDULE (CONTINUED)

Table with 8 columns: STATION & DIRECTION OF FLOW, LENGTH, SIDE, PROP OUTLET, PIPE UD TY, PIPE UD 4" SP, CONC HDWL FOR P DRAINS, REMARKS. Rows include station ranges like 909+00 to 914+00 and 925+50 to 927+00, with various pipe sizes and remarks such as 'REPLACE OUTLETS AT 909+00' and 'COMMON OUTLETS @ 925+50 FOR 920+00 TO 927+86'.

UNDERDRAIN SCHEDULE (CONTINUED)

Table with 8 columns: STATION & DIRECTION OF FLOW, LENGTH, SIDE, PROP OUTLET, PIPE UD TY, PIPE UD 4" SP, CONC HDWL FOR P DRAINS, REMARKS. Rows include station ranges like 1098+00 to 1103+00 and 1181+00 to 1183+00, with various pipe sizes and remarks such as 'PIPE CULV 1115+00' and 'COMMON OUTLET @ 1181+00 FROM 1175+00 TO 1186+46'.

NOTES:
* MAY REQUIRE ADJUSTMENT IN THE FIELD FOR OUTLETS AND UNDERDRAIN
** TO AVOID DAMAGE TO BRIDGE APPROACHES AND CONNECTORS PROPOSED UNDERDRAIN MAY BE TIED TO EXISTING (IF REQUIRED)
NOTE: PROVIDE AS BUILTS OF INSTALLED UNDERDRAINS AND OUTLET LOCATIONS

Metadata table with columns: FILE NAME, USER NAME, DESIGNED, REVISED, DRAWN, CHECKED, DATE, PLOT SCALE, PLOT DATE.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

UNDERDRAIN INSTALLATION SCHEDULES

Table with columns: SCALE, SHEET OF SHEETS, STA., TO STA.

Table with columns: F.A.I. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO., ILLINOIS FED. AID PROJECT.

UNDERDRAIN SCHEDULE (CONTINUED)

STATION & DIRECTION OF FLOW (←/→)	LENGTH FOOT	SIDE	PROP OUTLET STA	PIPE	PIPE	CONC	REMARKS
				UD TY 3	UD 4" SP	HDWL FOR P DRAINS	
FROM - TO				FOOT	FOOT	EACH	
BUCKLEY INTERCHANGE (CH 9)							
RAMP A - NB I-57 ENTRANCE RAMP							
0+12 → 4+50	438	LT/RT	4+50	876	24	2	
4+50 → GORE	355	LT	--	355	--	--	
4+50 → 10+44	594	RT	10+44	594	14	2	
RAMP B - NB I-57 EXIT RAMP							
0+09 → 4+00	391	RT	--	391	--	--	
4+00 → 8+00	400	RT	--	400	--	--	
8+00 → 8+40	40	RT	8+40	40	14	1	
* GORE → 8+00	140	LT	--	140	--	--	
8+00 → 8+40	40	LT	8+40	40	10	1	
8+40 ← 8+90	50	LT/RT	--	100	--	--	COMMON OUTLET AT 8+40
8+90 ← 15+00	610	LT/RT	15+00	1,220	24	2	
15+00 ← 20+71	571	LT/RT	--	1,142	--	--	
RAMP C - SB I-57 EXIT RAMP							
0+55 ← 5+00	445	RT	--	445	--	--	
5+00 ← 8+70	370	RT	5+00	370	14	1	
8+70 ← 11+00	230	RT	--	230	--	--	
* GORE ← 8+70	208	LT	8+70	208	24	2	
8+70 ← 11+00	230	LT	--	230	--	--	
11+00 ← 16+00	500	LT/RT	11+00	1,000	24	2	
16+00 ← 20+42	442	LT/RT	16+00	884	24	2	
RAMP D - SB I-57 ENTRANCE RAMP							
0+12 → 5+00	488	LT/RT	5+00	976	24	2	
5+00 → 8+50	350	RT	--	350	--	--	
8+50 → 8+70	20	RT	8+70	20	14	1	
5+00 → 8+50	350	LT	--	350	--	--	
* 8+50 → GORE	16	LT	--	16	--	--	
8+70 ← 8+90	20	RT	8+70	20	--	--	COMMON OUTLET AT 8+70
8+90 ← 13+90	500	RT	--	500	--	--	
BUCKLEY SUBTOTAL	7,798	--	--	10,897	210	18	
GRAND TOTALS	81,529		--	151,980	5,932	305	

NOTES:

- * MAY REQUIRE ADJUSTMENT IN THE FIELD FOR OUTLETS AND UNDERDRAIN
- ** TO AVOID DAMAGE TO BRIDGE APPROACHES AND CONNECTORS PROPOSED UNDERDRAIN MAY BE TIED TO EXISTING (IF REQUIRED)
- NOTE: PROVIDE AS BUILTS OF INSTALLED UNDERDRAINS AND OUTLET LOCATIONS

DELINEATOR SCHEDULE (MAINLINE)						
LOCATION			LENGTH	SIDE	DELINEATORS	
STA	TO	STA	FOOT	RT/LT	INSTALL EACH	REMOVE EACH
MAINLINE - SOUTHBOUND						
910+80	TO	927+60	1,680	RT	4	4
STRUCTURE OMISSION SN 038-0155						
929+40	TO	950+06.50	2,067	RT	5	5
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH						
950+05.83	TO	963+25	1,319	RT	3	3
STRUCTURE OMISSION SN 038-0153						
964+26	TO	1084+87.34	12,061	RT	30	30
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH						
1084+87.07	TO	1135+04.55	5,017	RT	13	13
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH						
1135+04.13	TO	1186+34	5,130	RT	13	13
STRUCTURE OMISSION SN 038-0151						
1187+82	TO	1211+45	2,363	RT	6	6
1211+45	TO	1218+31	686	RAMP	0	0
1218+31	TO	1222+61.96	431	RT	1	1
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK						
1222+60.95	TO	1236+88	1,427	RT	4	4
1236+88	TO	1247+54	1,066	RAMP	0	0
1247+54	TO	1267+32.14	1,978	RT	5	5
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH						
1267+31.76	TO	1269+02	170	RT	1	1
SOUTHBOUND TOTAL			35,396		85	85
MAINLINE - NORTHBOUND						
910+80	TO	927+59	1,679	LT	4	4
STRUCTURE OMISSION SN 038-0156						
929+41	TO	950+06.50	2,066	LT	5	5
STATION EQUATION STA 950+06.50 BK = STA 950+05.83 AH						
950+05.83	TO	962+99	1,293	LT	3	3
STRUCTURE OMISSION SN 038-0154						
964+00	TO	1084+87.34	12,087	LT	30	30
STATION EQUATION STA 1084+87.38 BK = STA 1084+87.07 AH						
1084+87.07	TO	1135+04.55	5,017	LT	13	13
STATION EQUATION STA 1135+04.55 BK = STA 1135+04.13 AH						
1135+04.13	TO	1186+19	5,115	LT	13	13
STRUCTURE OMISSION SN 038-0152						
1187+66	TO	1203+37	1,571	LT	4	4
1203+37	TO	1213+94	1,057	RAMP	0	0
1213+94	TO	1222+61.96	868	LT	2	2
STATION EQUATION STA 1222+61.96 BK = STA 1222+60.95 BK						
1222+60.95	TO	1234+09	1,148	LT	3	3
1234+09	TO	1241+09	700	RAMP	0	0
1241+09	TO	1267+32.14	2,623	LT	7	7
STATION EQUATION STA 1267+32.14 BK = STA 1267+31.76 AH						
1267+31.76	TO	1269+02	170	LT	0	0
NORTHBOUND TOTAL			35,395		84	84

DELINEATOR SCHEDULE (BUCKLEY INTERCHANGE)						
LOCATION			LENGTH	SIDE	DELINEATORS	
STA	TO	STA	FOOT	LT/RT	INSTALL EACH	REMOVE EACH
RAMP A - SE RAMP (NB I-57 ENTRANCE RAMP)						
0+14	TO	5+93	579	LT	6	6
5+93	TO	9+38	345	RT/LT	9	9
9+38	TO	18+88	950	RT	10	10
RAMP B - SE RAMP (NB I-57 EXIT RAMP)						
0+00	TO	6+84	684	RT	7	7
6+84	TO	12+29	545	RT/LT	11	11
12+29	TO	14+58	229	LT	2	2
14+58	TO	18+54	396	LT	7	7
18+54	TO	20+72	218	LT	2	2
RAMP C - NW RAMP (SB I-57 EXIT RAMP)						
0+00	TO	6+62	662	RT	7	7
6+62	TO	11+30	468	RT/LT	10	10
11+30	TO	13+88	258	LT	3	3
13+88	TO	18+25	437	LT	8	8
18+25	TO	20+42	217	LT	2	2
RAMP D - SW RAMP (SB I-57 ENTRANCE RAMP)						
0+12	TO	7+20	708	LT	7	7
7+20	TO	9+92	272	RT/LT	8	8
9+92	TO	19+43	951	RT	10	10
RAMPS TOTAL			7,919		109	109
GRAND TOTAL (MAINLINE & RAMPS)					278	278

SEE SPACING REQUIREMENTS IN HWY STND 635001
DELINEATORS AT ACCELERATION / DEACCELERATION RAMPS SHALL HAVE DOUBLE REFLECTORS

6" LINEAR DELINEATOR PANEL SCHEDULE	
STRUCTURE	INSTALLATION
	EACH
BUCKLEY INTERCHANGE	
038-0155	6
038-0156	6
038-0082	2
038-0153	6
038-0154	6
038-0132	NO WORK
038-0126	2
038-0151	4
038-0152	4
038-0076	2
TOTAL	38

SEE SPECIAL PROVISIONS FOR REMOVAL / INSTALLATION
* INSTALLATION AT PIER ADJACENT TO HMA SHOULDER

CURB REFLECTOR SCHEDULE	
LOCATION	INSTALLATION
	EACH
BUCKLEY INTERCHANGE	
RAMP C	8
RAMP B	8
TOTAL	16

SEE HWY STND 782001 FOR SPACING

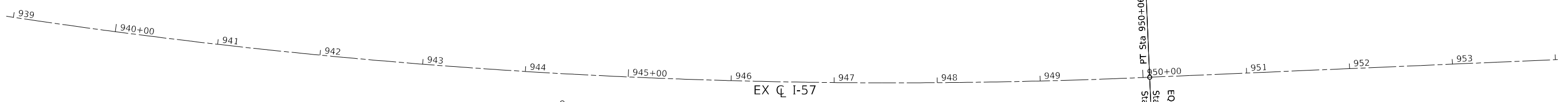
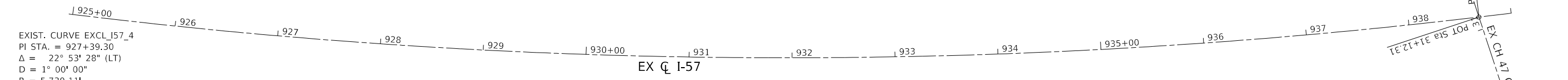
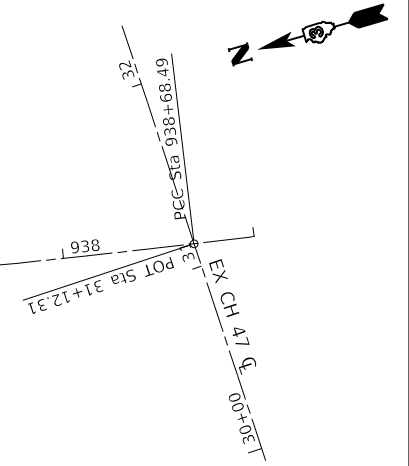
SAND MODULE IMPACT ATTENUATOR SCHEDULE	
LOCATION (MEDIAN)	REMOVE & REPLACE SAND MODULE
	EACH
038-0082	2
038-0132	6
038-0126	6
038-0076	2
TOTAL	16

WOVEN WIRE FENCE SCHEDULE				
FROM	TO	SIDE	FENCE REMOVAL	INSTALLATION
			FOOT	FOOT
MP 275	MP 276	SB	2500	2500
MP 274.5	MP 275.5	NB	2500	2500
TOTAL			5,000	5,000

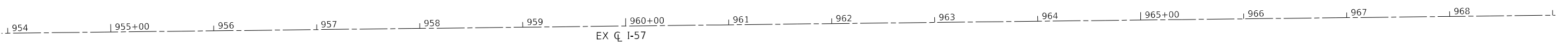
SPECIFIC AREAS FOR REMOVAL AND REPLACEMENT WILL BE DETERMINED BY THE RESIDENT AND OPERATIONS AREA SUPERVISOR

MILE MARKER SCHEDULE				
MILE	SIDE	MILE POST MARKER ASSY SPECIAL	TELESCOPING STEEL SIGN SUPPORT	REM SIGN PANEL ASSY TY A
		EACH	FOOT	EACH
MM 272	NB/SB	2	30	2
MM 273	NB/SB	2	30	2
MM 274	NB/SB	2	30	2
MM 275	NB/SB	2	30	2
MM 276	NB/SB	2	30	2
MM 277	NB/SB	2	30	2
MM 278	NB/SB	2	30	2
TOTAL		14	210	14

EXIST. CURVE EXCL_I57_4
 PI STA. = 927+39.30
 $\Delta = 22^\circ 53' 28''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,730.11'$
 $T = 1,160.14'$
 $L = 2,289.33'$
 $E = 116.26'$
 $e = \underline{\hspace{1cm}}$
 T.R. =
 S.E. RUN =
 P.C. STA. = 915+79.16
 P.T. STA. = 938+68.49



EXIST. CURVE EXCL_I57_5
 PI STA. = 944+39.37
 $\Delta = 11^\circ 22' 45''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,730.11'$
 $T = 570.89'$
 $L = 1,138.02'$
 $E = 28.37'$
 $e = \underline{\hspace{1cm}}$
 T.R. =
 S.E. RUN =
 P.C. STA. = 938+68.49
 P.T. STA. = 950+06.50



FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -
c:\pw_work\pwidot\calderon\0521537\0366H63-eh-A1B.dgn		DRAWN -	REVISED -
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	38
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



| 969 | 970+00 | 971 | 972 | 973 | 974 | 975+00 | 976 | 977 | 978 | 979 | 980+00 | 981 | 982 | 983 |

EX C I-57



| 984 | 985+00 | 986 | 987 | 988 | 989 | 990+00 | 991 | 992 | 993 | 994 | 995+00 | 996 | 997 | 998 |

EX C I-57



| 999 | 1000+00 | 1001 | 1002 | 1003 | 1004 | 1005+00 | 1006 | 1007 | 1008 | 1009 | 1010+00 | 1011 | 1012 | 1013 |

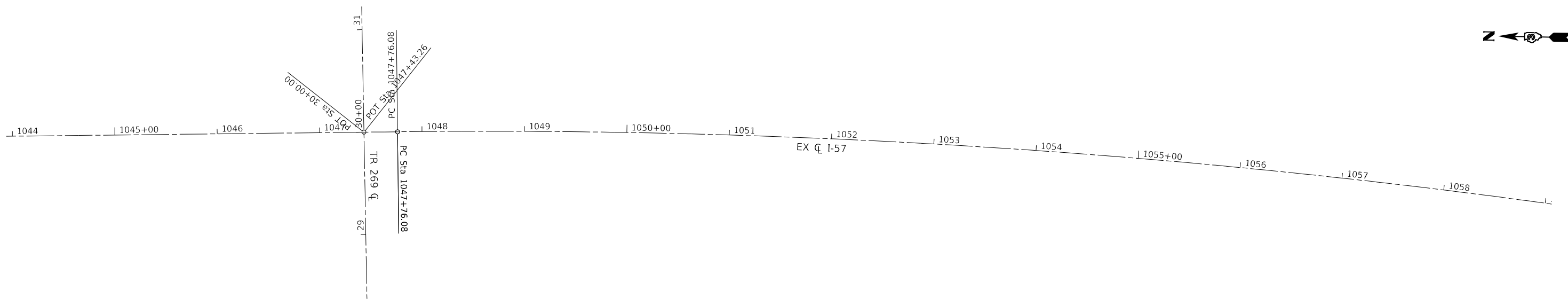
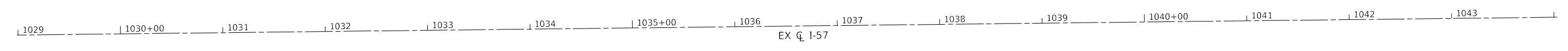
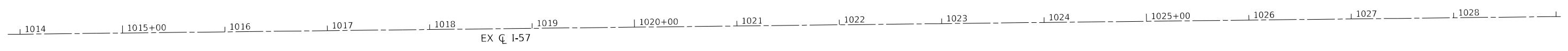
EX C I-57

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SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	39
			CONTRACT NO. 66H63	
		ILLINOIS	FED. AID PROJECT	

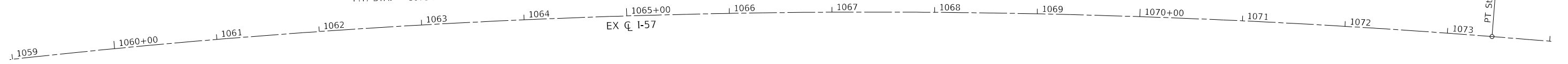


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw_work\pwidof\calderont\05215371\0368H63-eh-A1B.dgn	DRAWN -	REVISED -	57					(38-7,38-6)RS	IROQUOIS	130	40	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.					



PI Sta 1060+72.07

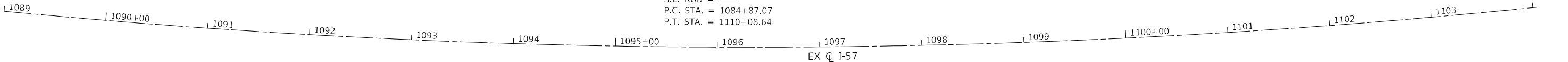
EXIST. CURVE EXCL_I57_10
 PI STA. = 1060+72.07
 $\Delta = 19^\circ 15' 38''$ (RT)
 $D = 0^\circ 45' 01''$
 $R = 7,637.85'$
 $T = 1,295.99'$
 $L = 2,567.53'$
 $E = 109.17'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 1047+76.08
 P.T. STA. = 1073+43.61



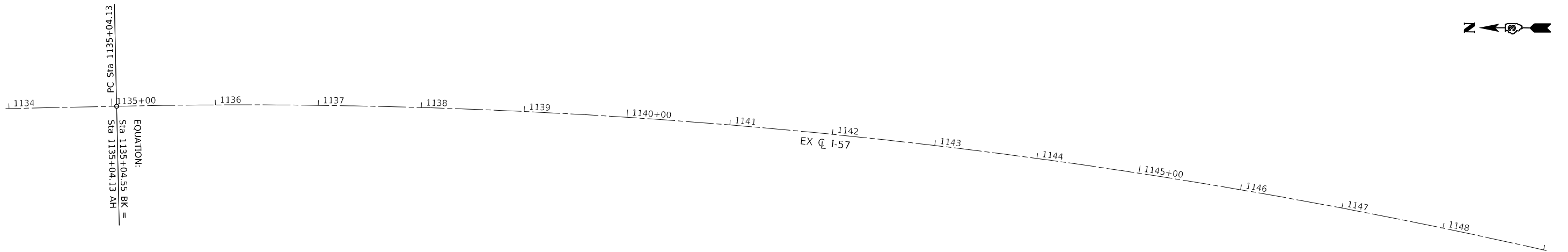
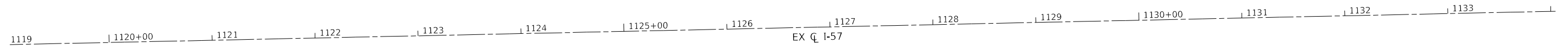
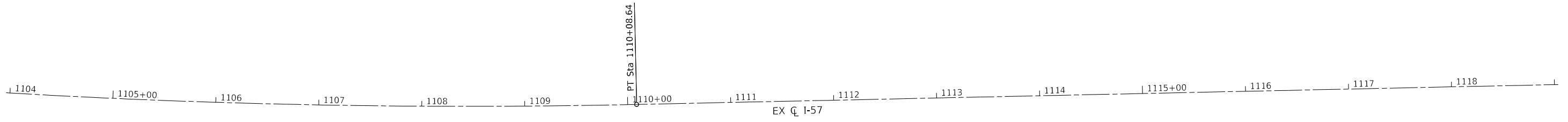
PC Sta 1084+87.07
 PT Sta 1084+87.07
 EQUATION:
 $\text{Sta } 1084+87.38 \text{ BK} =$
 $\text{Sta } 1084+87.07 \text{ AH}$



EXIST. CURVE EXCL_I57_13
 PI STA. = 1097+59.44
 $\Delta = 18^\circ 55' 16''$ (LT)
 $D = 0^\circ 45' 01''$
 $R = 7,635.65'$
 $T = 1,272.37'$
 $L = 2,521.57'$
 $E = 105.29'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 1084+87.07
 P.T. STA. = 1110+08.64



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cc:\pw_work\pwwork\calderont\0521537\10366H63-eh-A1B.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	41			
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63							
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS	FED. AID PROJECT			

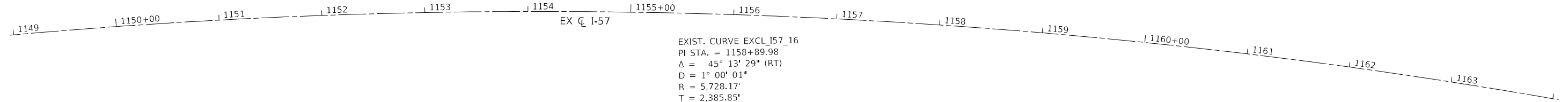


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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	PLOT DATE = 12/17/2021	DATE -	REVISED -

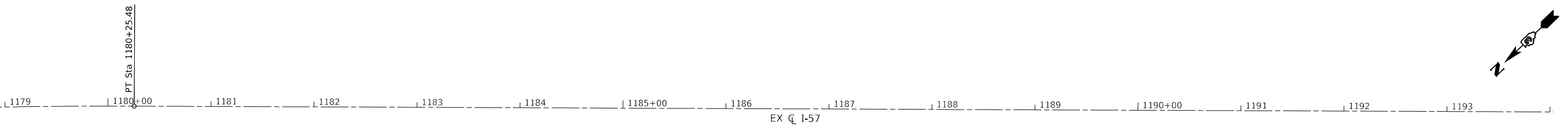
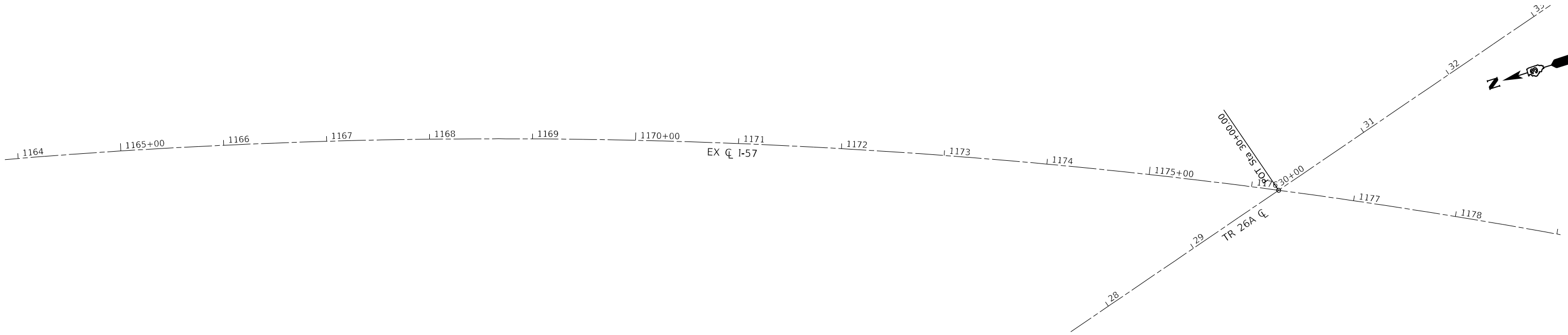
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT			
SCALE:	SHEET	OF	SHEETS
	STA.	TO STA.	

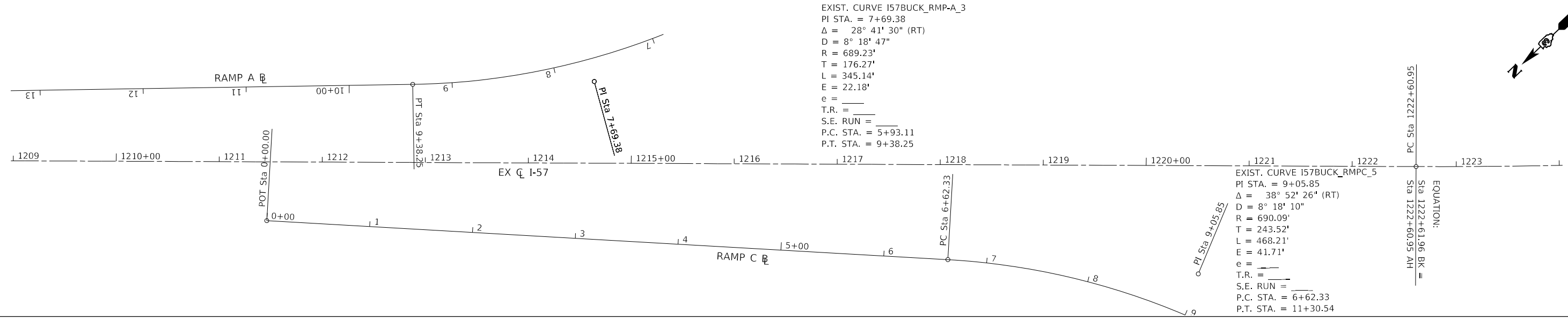
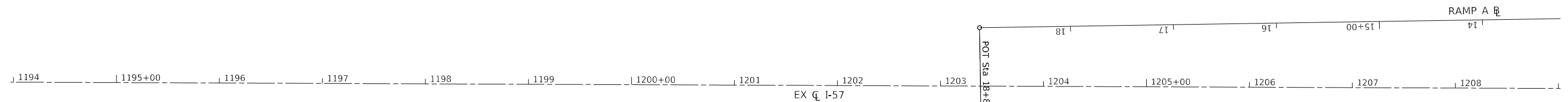
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	42
CONTRACT NO. 66H63				
		ILLINOIS	FED. AID PROJECT	



EXIST. CURVE EXCL_I57_16
 PI STA. = 1158+89.98
 $\Delta = 45^\circ 13' 29''$ (RT)
 D = $1^\circ 00' 01''$
 R = 5,728.17'
 T = 2,385.85'
 L = 4,521.35'
 E = 477.01'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 1135+04.13
 P.T. STA. = 1180+25.48



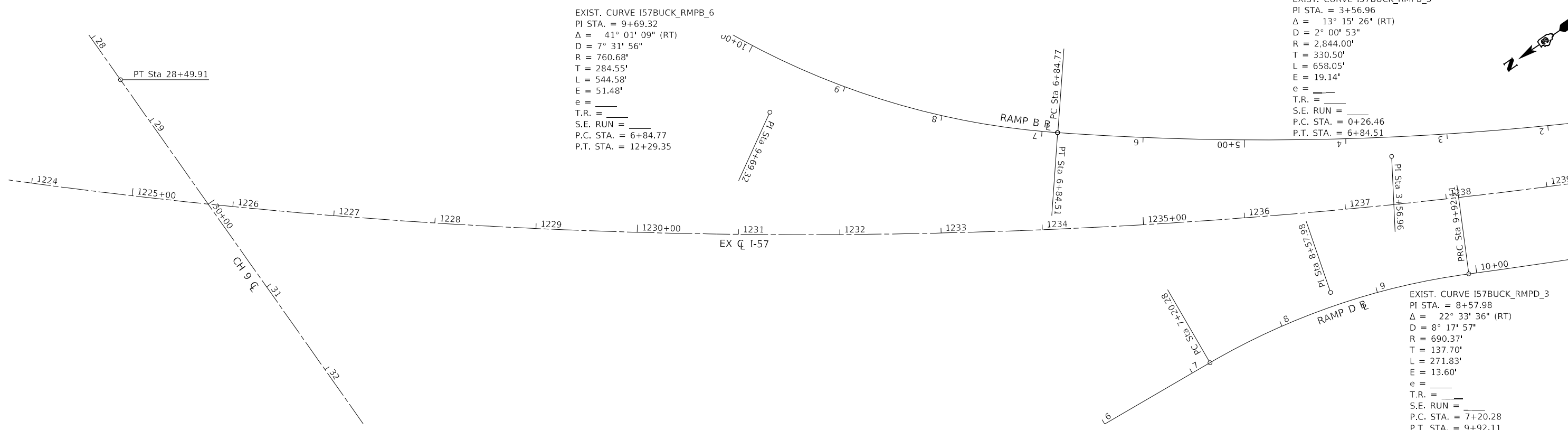
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c:\pw_work\pwidot\calderont\0521537\ID366H63-eh-A1B.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	43			
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63							
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS	FED. AID PROJECT			



EXIST. CURVE I57BUCK_RMP-A_3
 PI STA. = 7+69.38
 Δ = 28° 41' 30" (RT)
 D = 8° 18' 47"
 R = 689.23'
 T = 176.27'
 L = 345.14'
 E = 22.18'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 5+93.11
 P.T. STA. = 9+38.25

EXIST. CURVE I57BUCK_RMPC_5
 PI STA. = 9+05.85
 Δ = 38° 52' 26" (RT)
 D = 8° 18' 10"
 R = 690.09'
 T = 243.52'
 L = 468.21'
 E = 41.71'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 6+62.33
 P.T. STA. = 11+30.54

EQUATION:
 Sta 1222+60.96 BK =
 Sta 1222+60.95 AH



EXIST. CURVE I57BUCK_RMPB_6
 PI STA. = 9+69.32
 Δ = 41° 01' 09" (RT)
 D = 7° 31' 56"
 R = 760.68'
 T = 284.55'
 L = 544.58'
 E = 51.48'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 6+84.77
 P.T. STA. = 12+29.35

EXIST. CURVE I57BUCK_RMPB_3
 PI STA. = 3+56.96
 Δ = 13° 15' 26" (RT)
 D = 2° 00' 53"
 R = 2,844.00'
 T = 330.50'
 L = 658.05'
 E = 19.14'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 0+26.46
 P.T. STA. = 6+84.51

EXIST. CURVE I57BUCK_RMPD_3
 PI STA. = 8+57.98
 Δ = 22° 33' 36" (RT)
 D = 8° 17' 57"
 R = 690.37'
 T = 137.70'
 L = 271.83'
 E = 13.60'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 7+20.28
 P.T. STA. = 9+92.11

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
ec:\pw_work\pwidof\calderont\0521537\I57\366H63-eh-A1B.dgn		DRAWN -	REVISED -
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2021	DATE -	REVISED -

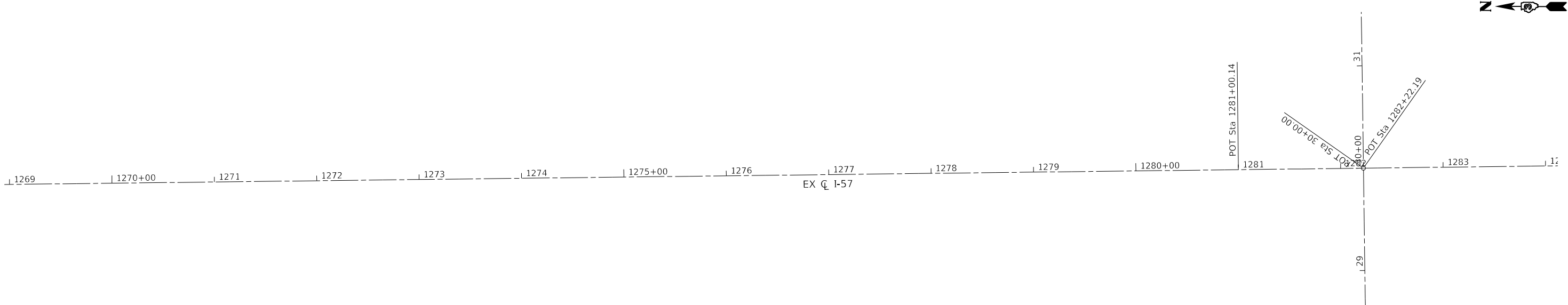
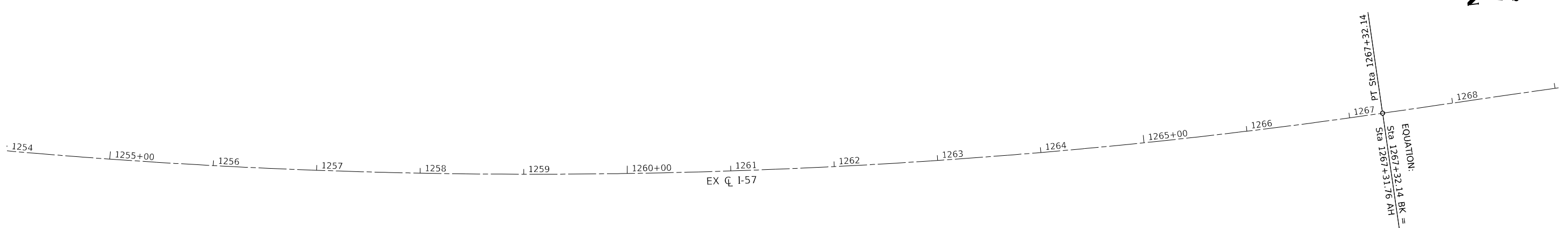
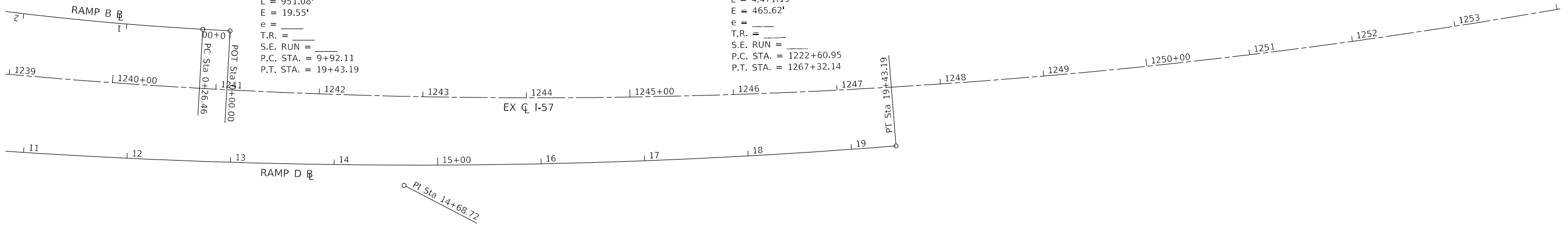
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	44
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

EXIST. CURVE I57BUCK_RMPD_4
 PI STA. = 14+68.72
 $\Delta = 9^\circ 23' 50''$ (LT)
 D = 0° 59' 17"
 R = 5,798.77'
 T = 476.61'
 L = 951.08'
 E = 19.55'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 9+92.11
 P.T. STA. = 19+43.19

EXIST. CURVE EXCL_I57_19
 PI STA. = 1246+17.35
 $\Delta = 44^\circ 42' 37''$ (LT)
 D = 1° 00' 00"
 R = 5,729.80'
 T = 2,356.40'
 L = 4,471.19'
 E = 465.62'
 e = _____
 T.R. = _____
 S.E. RUN = _____
 P.C. STA. = 1222+60.95
 P.T. STA. = 1267+32.14

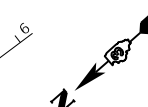
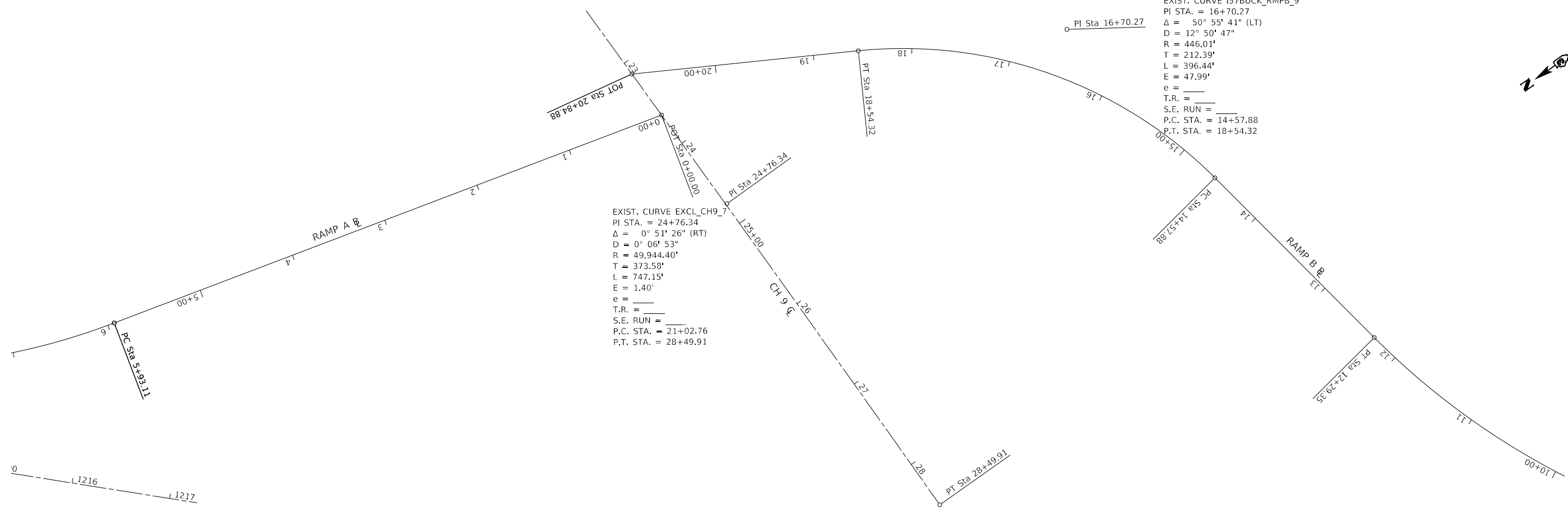


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw_work\pwidot\calderont\0521537\ID366H63-eh-A1B.dgn		DRAWN -	REVISED -					57	(38-7,38-6)RS	IROQUOIS	130	45
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63							
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				

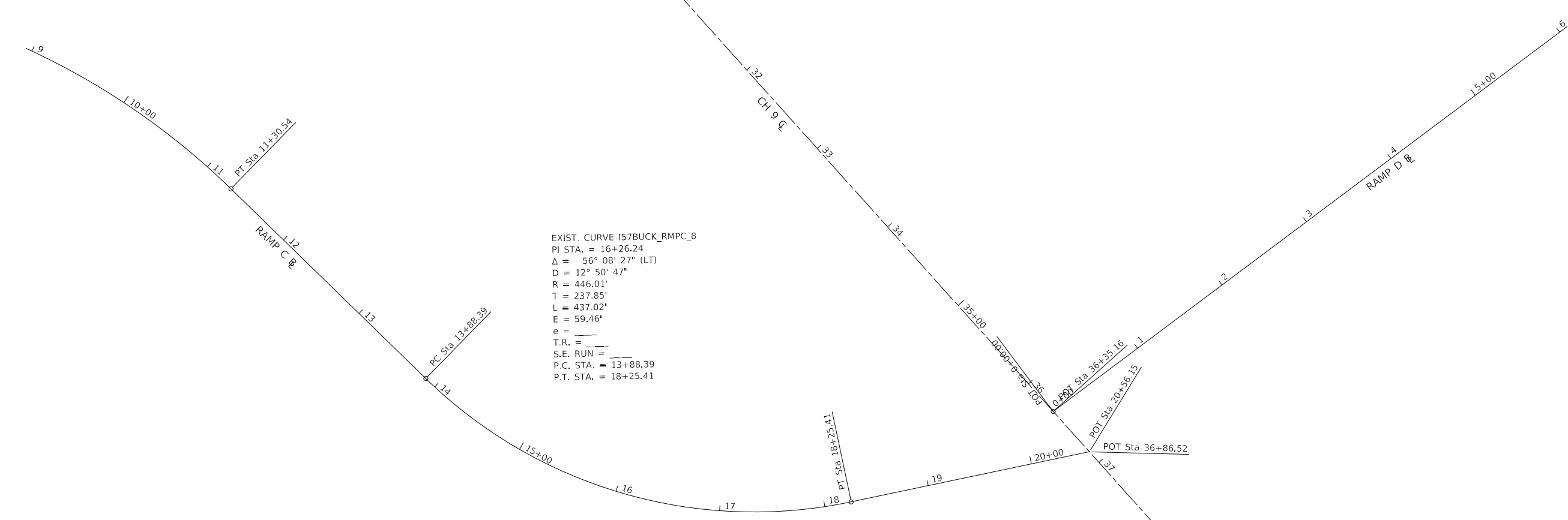


EXIST. CURVE I57BUCK_RMPB_9
 PI STA. = 16+70.27
 $\Delta = 50^\circ 55' 41''$ (LT)
 $D = 12^\circ 50' 47''$
 $R = 446.01'$
 $T = 212.39'$
 $L = 396.44'$
 $E = 47.99'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 14+57.88
 P.T. STA. = 18+54.32

EXIST. CURVE EXCL_CH9_7
 PI STA. = 24+76.34
 $\Delta = 0^\circ 51' 26''$ (RT)
 $D = 0^\circ 06' 53''$
 $R = 49,944.40'$
 $T = 373.58'$
 $L = 747.15'$
 $E = 1.40'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 21+02.76
 P.T. STA. = 28+49.91



EXIST. CURVE I57BUCK_RMPC_8
 PI STA. = 16+26.24
 $\Delta = 56^\circ 08' 27''$ (LT)
 $D = 12^\circ 50' 47''$
 $R = 446.01'$
 $T = 237.85'$
 $L = 437.02'$
 $E = 59.46'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 13+88.39
 P.T. STA. = 18+25.41



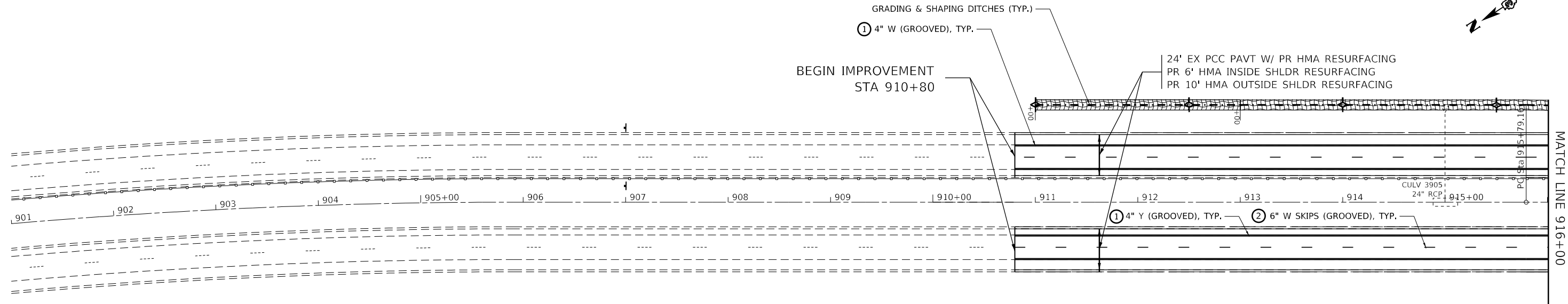
FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -
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SMODELNAME\$	PLOT SCALE = 109.2038 / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

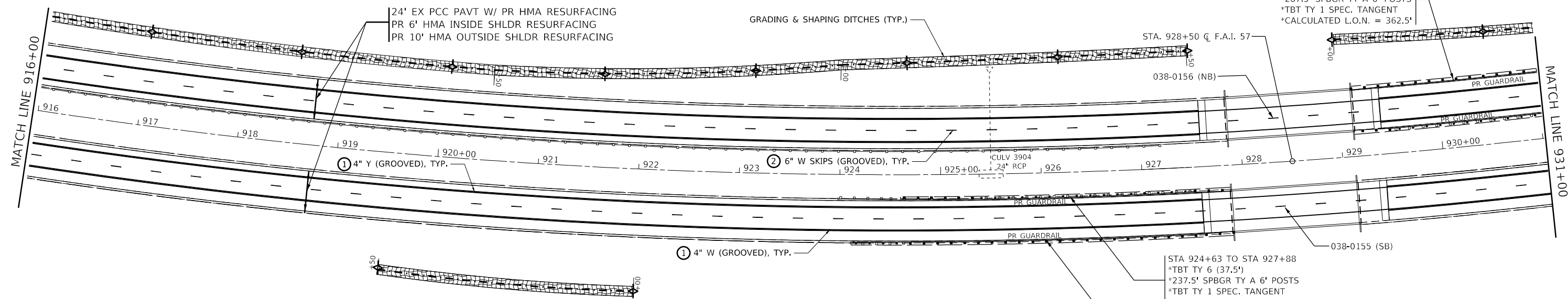
BUCKLEY INTERCHANGE RAMPS ALIGNMENT

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	46
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND		EROSION CONTROL ITEMS	
PAVEMENT MARKING		TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)	
①	POLYUREA PAVT MARKING, TY 2		
②	PREFORMED PLASTIC PAVT MARKING, TY D		EROSION CONTROL BLANKET
		TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.	

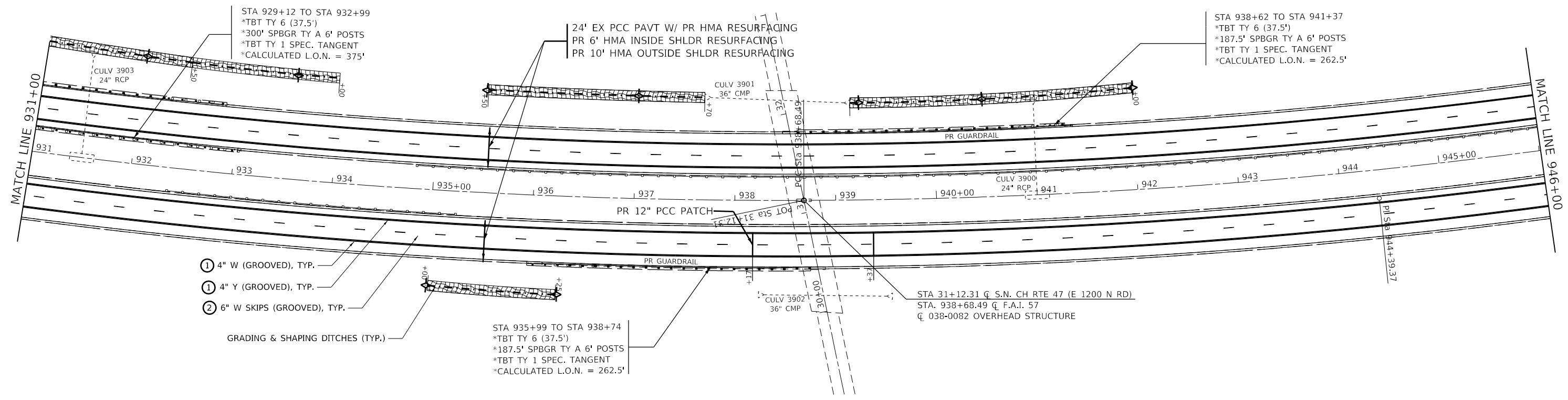


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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 PLANS				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	47
CONTRACT NO. 66H63				
		ILLINOIS	FED. AID PROJECT	



LEGEND

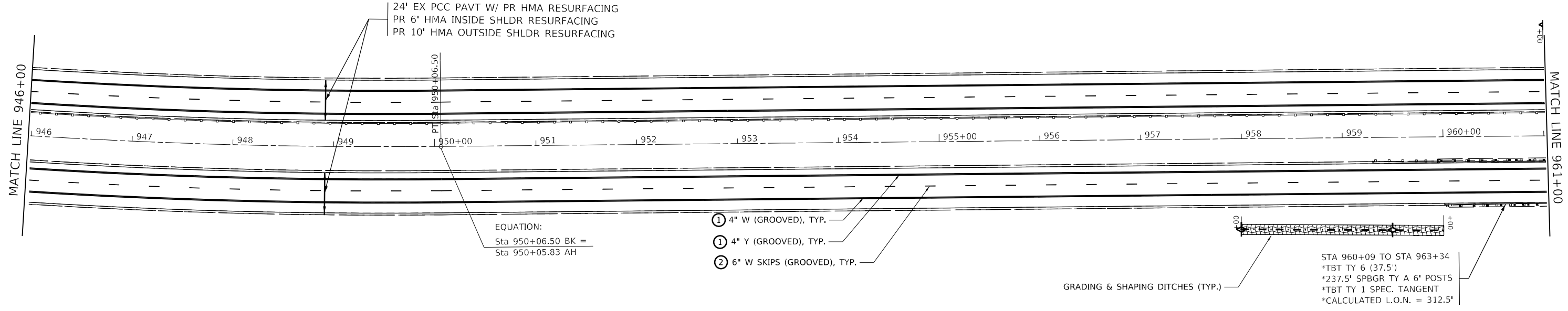
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

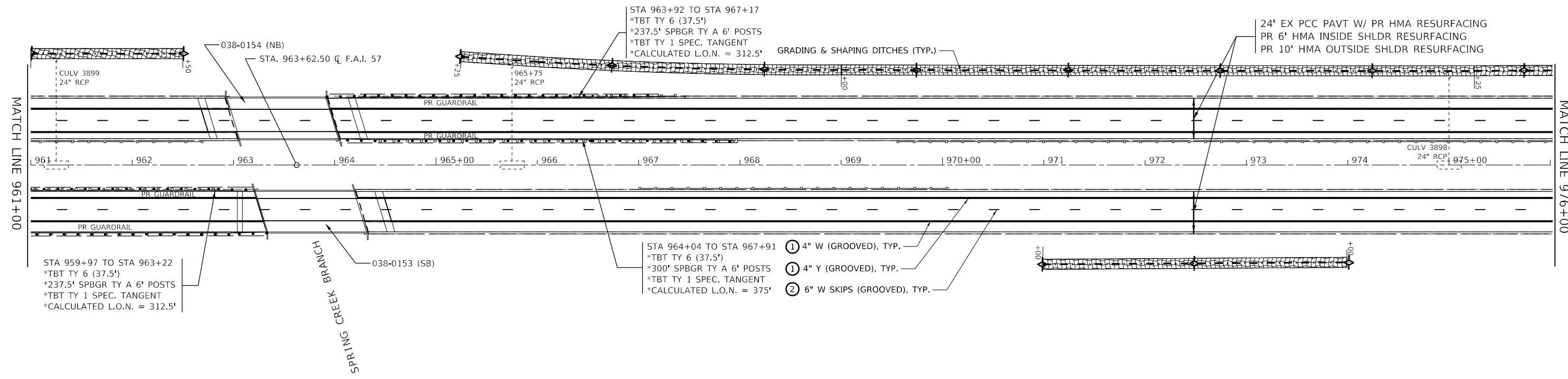
EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\p\wd\calderont\0521537\I0368H63-eh-plan.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	48				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								



LEGEND

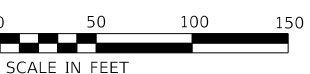
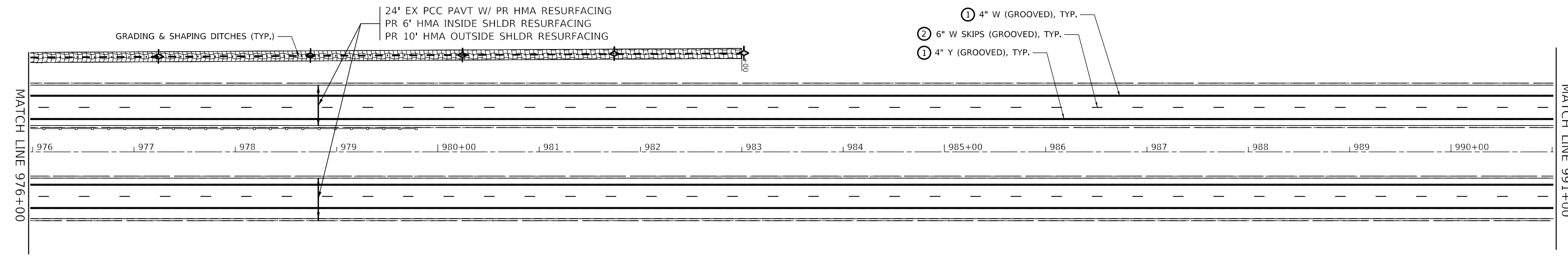
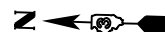
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

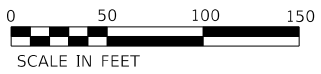
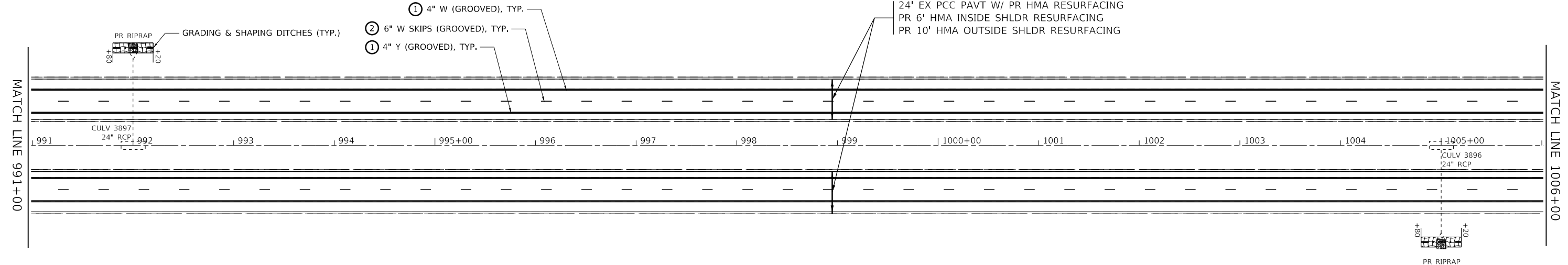
EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\pwwid\calderont\0521537\I368H63-eh-plant.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	49				
SMODELNAMES	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



LEGEND

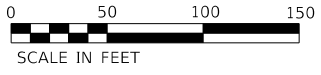
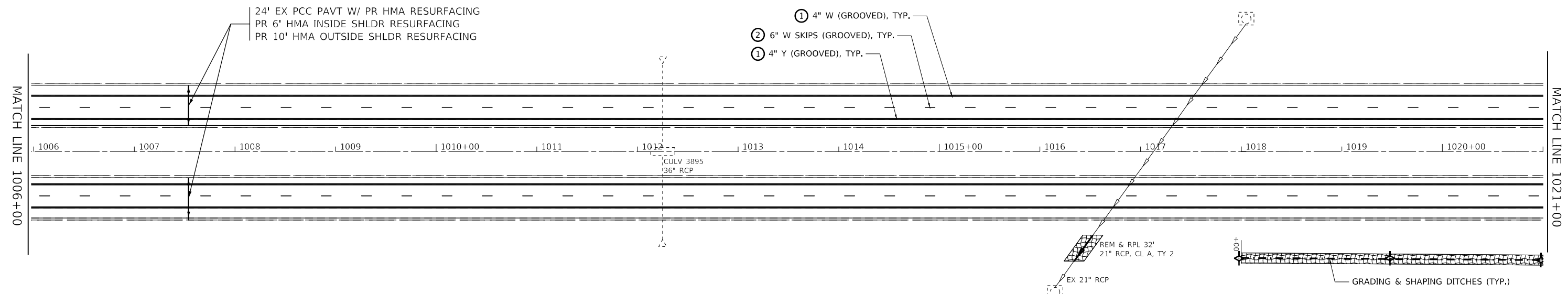
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



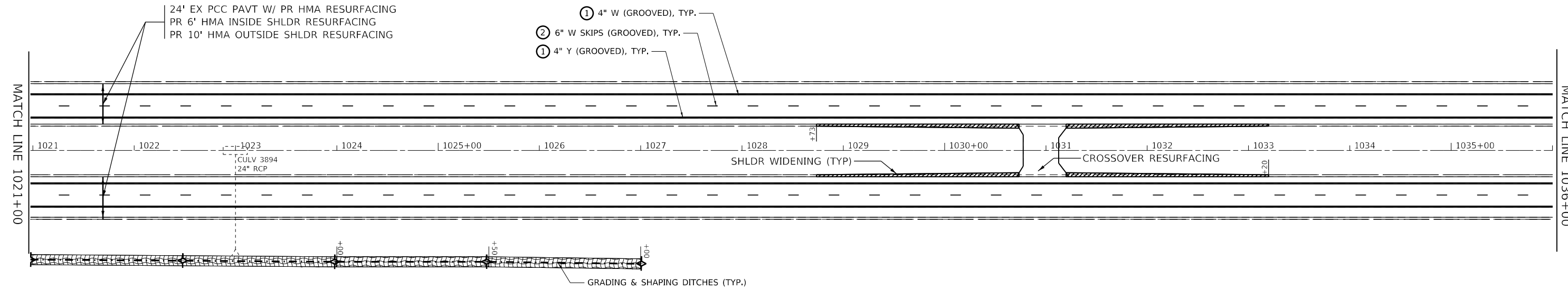
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	50
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND

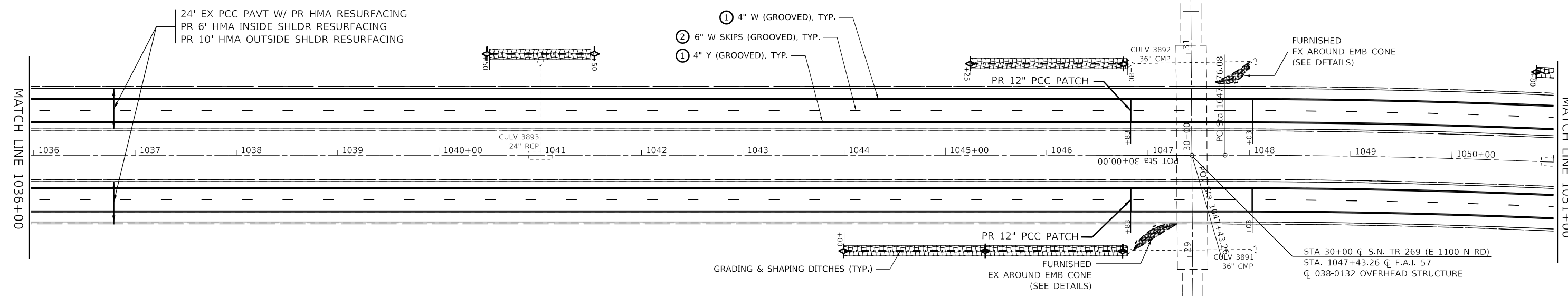
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



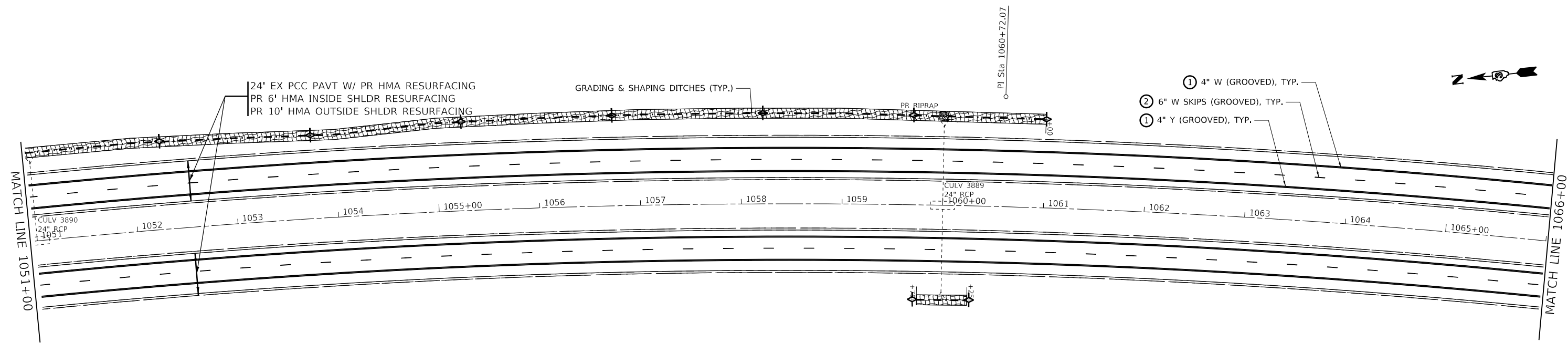
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

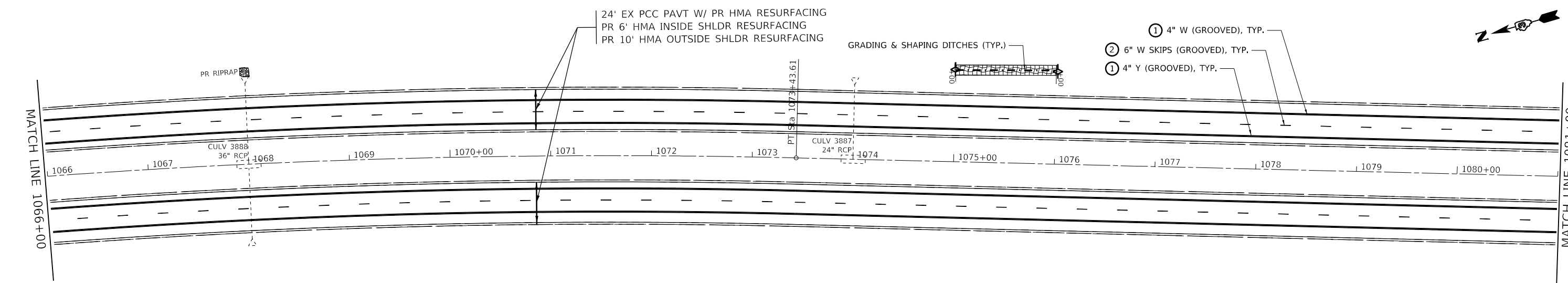
I-57 PLANS

SCALE: SHEET OF SHEETS STA. TO STA.

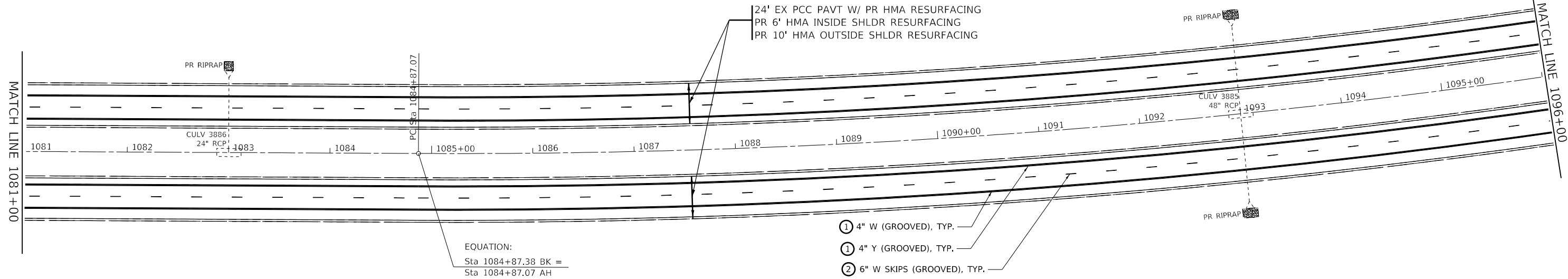
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	51
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND		EROSION CONTROL ITEMS	
PAVEMENT MARKING			TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
①	POLYUREA PAVT MARKING, TY 2		EROSION CONTROL BLANKET
②	PREFORMED PLASTIC PAVT MARKING, TY D		TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63										
	PLOT DATE = 12/17/2021	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



EQUATION:
Sta 1084+87.38 BK =
Sta 1084+87.07 AH

- ① 4" W (GROOVED), TYP.
- ① 4" Y (GROOVED), TYP.
- ② 6" W SKIPS (GROOVED), TYP.



LEGEND

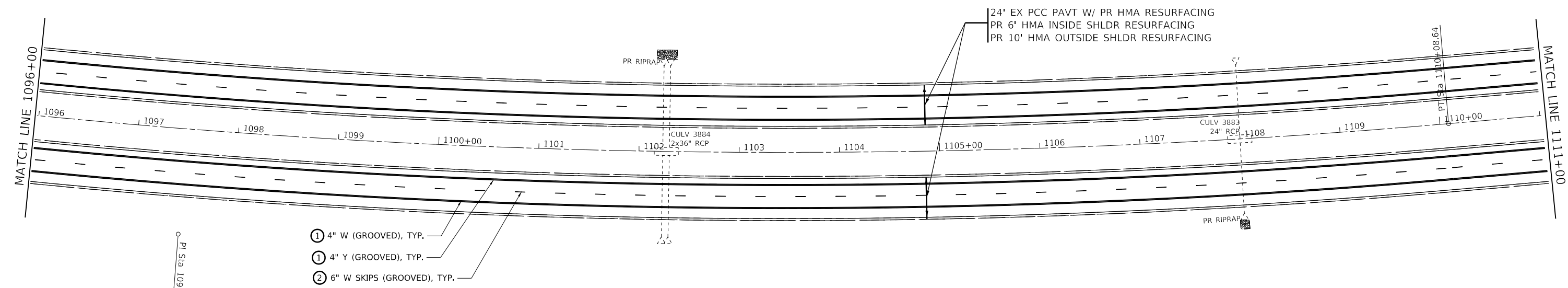
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

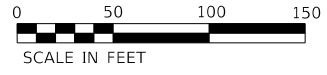
EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

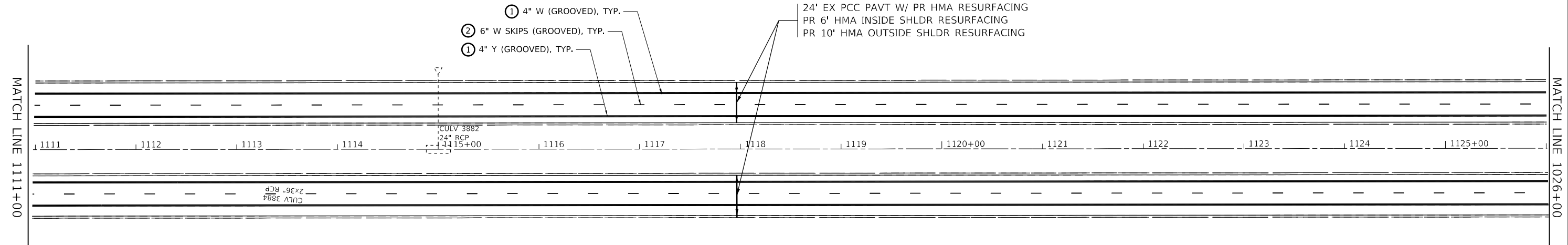
TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



- ① 4" W (GROOVED), TYP.
- ① 4" Y (GROOVED), TYP.
- ② 6" W SKIPS (GROOVED), TYP.



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\p\wd\calderont\0521537\I368H63-eh-plan.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	53				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



LEGEND

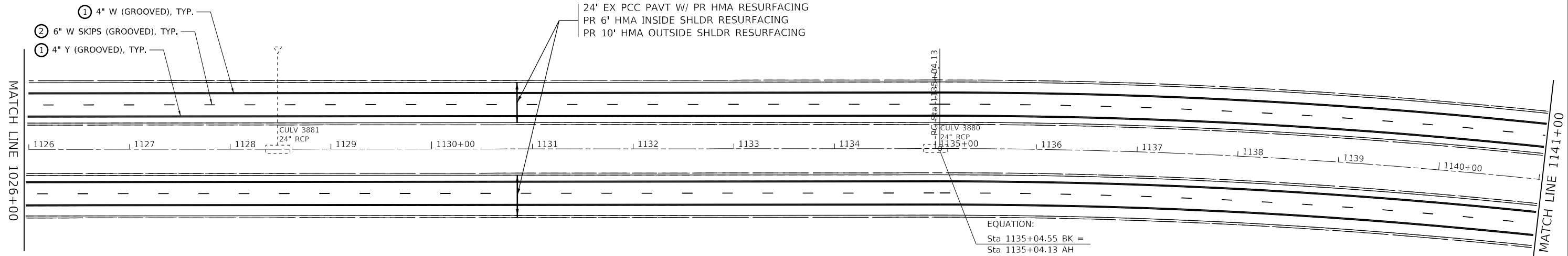
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

EROSION CONTROL ITEMS

- TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- EROSION CONTROL BLANKET

TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



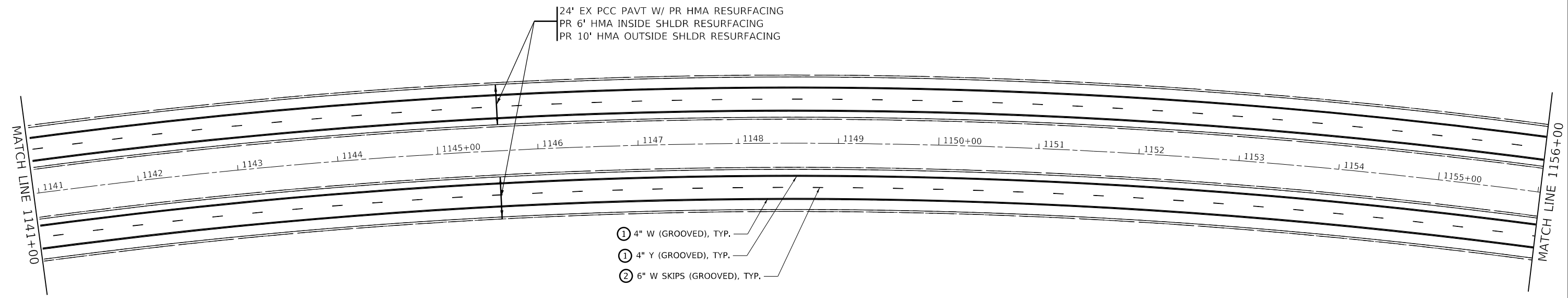
EQUATION:
Sta 1135+04.55 BK =
Sta 1135+04.13 AH

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 PLANS			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	54
CONTRACT NO. 66H63				
		ILLINOIS	FED. AID PROJECT	



- ① 4" W (GROOVED), TYP.
- ① 4" Y (GROOVED), TYP.
- ② 6" W SKIPS (GROOVED), TYP.



LEGEND

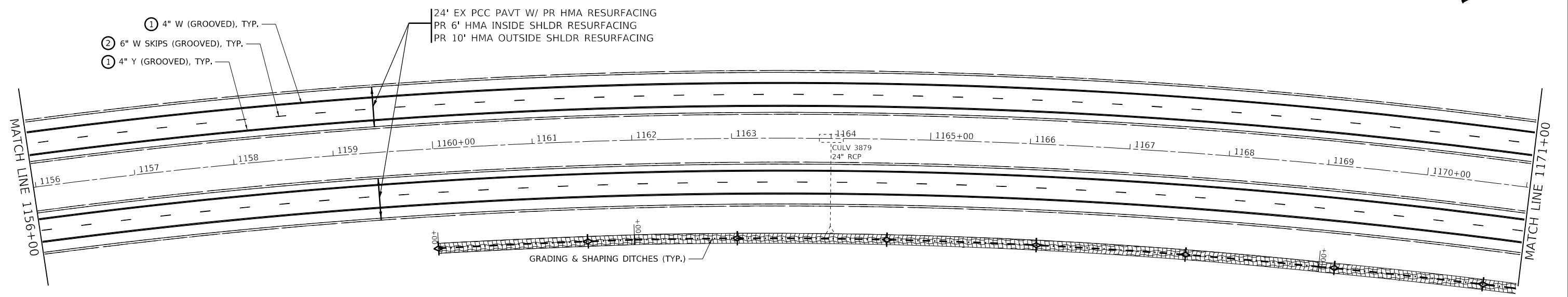
PAVEMENT MARKING

- ① POLYUREA PAVT MARKING, TY 2
- ② PREFORMED PLASTIC PAVT MARKING, TY D

EROSION CONTROL ITEMS

- ◆ TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
- ▨ EROSION CONTROL BLANKET

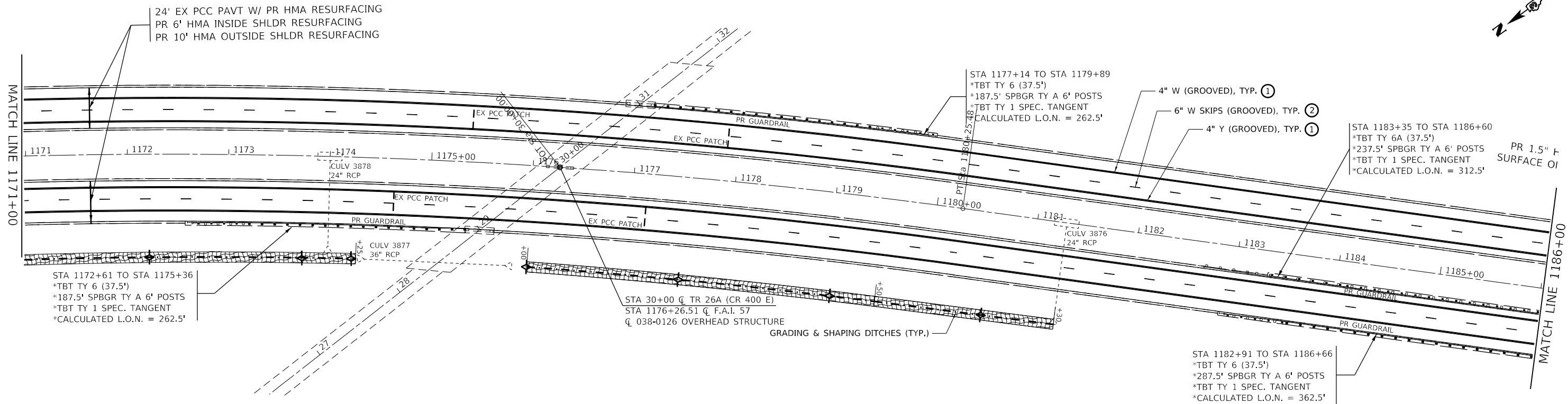
TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.



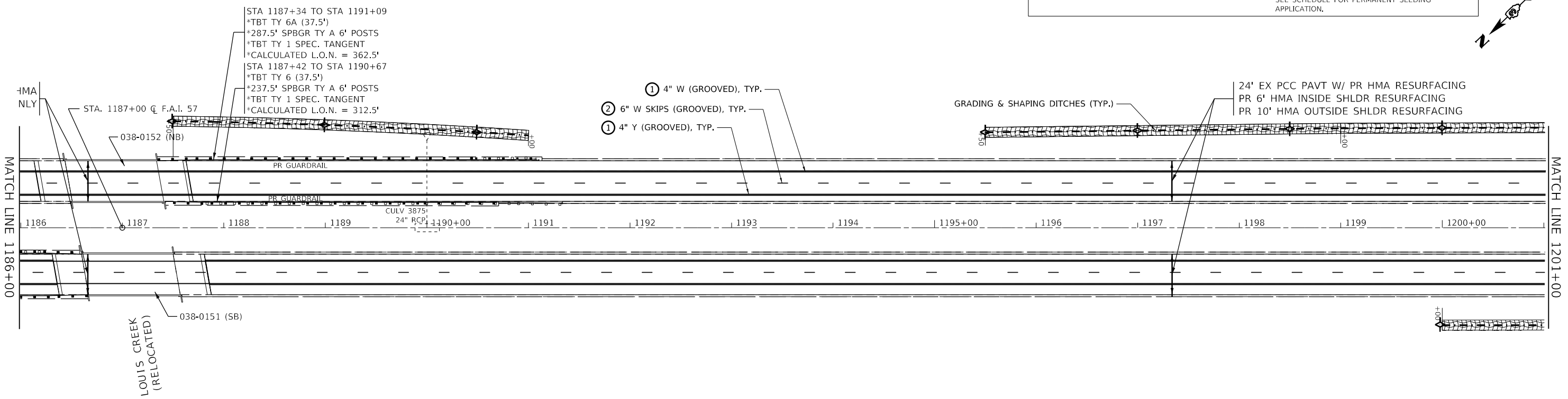
- ① 4" W (GROOVED), TYP.
- ② 6" W SKIPS (GROOVED), TYP.
- ① 4" Y (GROOVED), TYP.



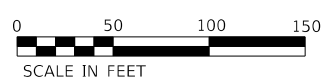
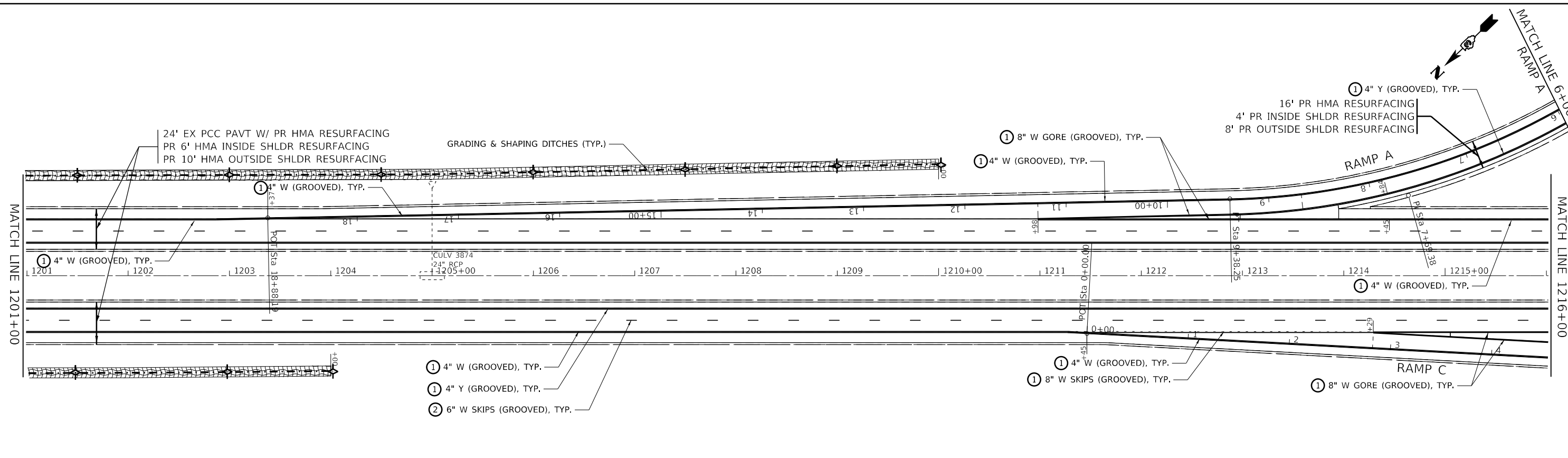
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63								
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



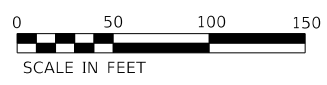
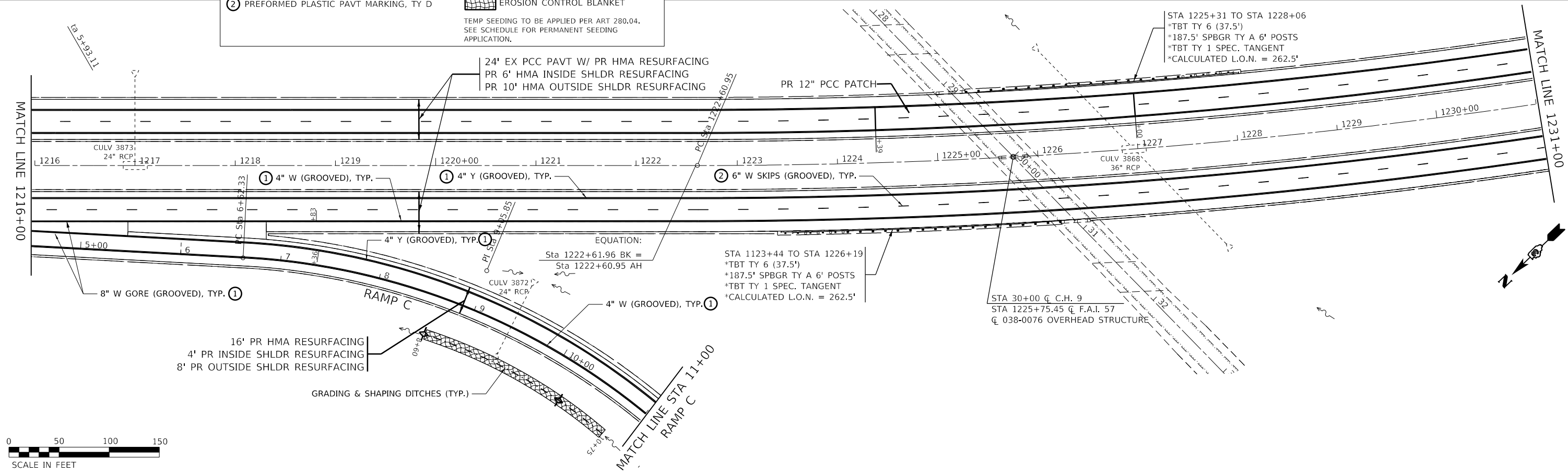
LEGEND		EROSION CONTROL ITEMS	
PAVEMENT MARKING		TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)	
①	POLYUREA PAVT MARKING, TY 2		
②	PREFORMED PLASTIC PAVT MARKING, TY D		
		TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.	



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ec:\pw_work\pwwd\calderont\0521537\0366H63-eh-plan.dgn		DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	57	(38-7,38-6)RS	IROQUOIS	130	56
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63										
	PLOT DATE = 12/17/2021	DATE -	REVISED -		ILLINOIS FED. AID PROJECT										



LEGEND	
PAVEMENT MARKING	
①	POLYUREA PAVT MARKING, TY 2
②	PREFORMED PLASTIC PAVT MARKING, TY D
EROSION CONTROL ITEMS	
	TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
	EROSION CONTROL BLANKET
TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.	

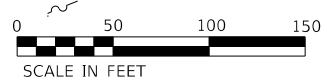
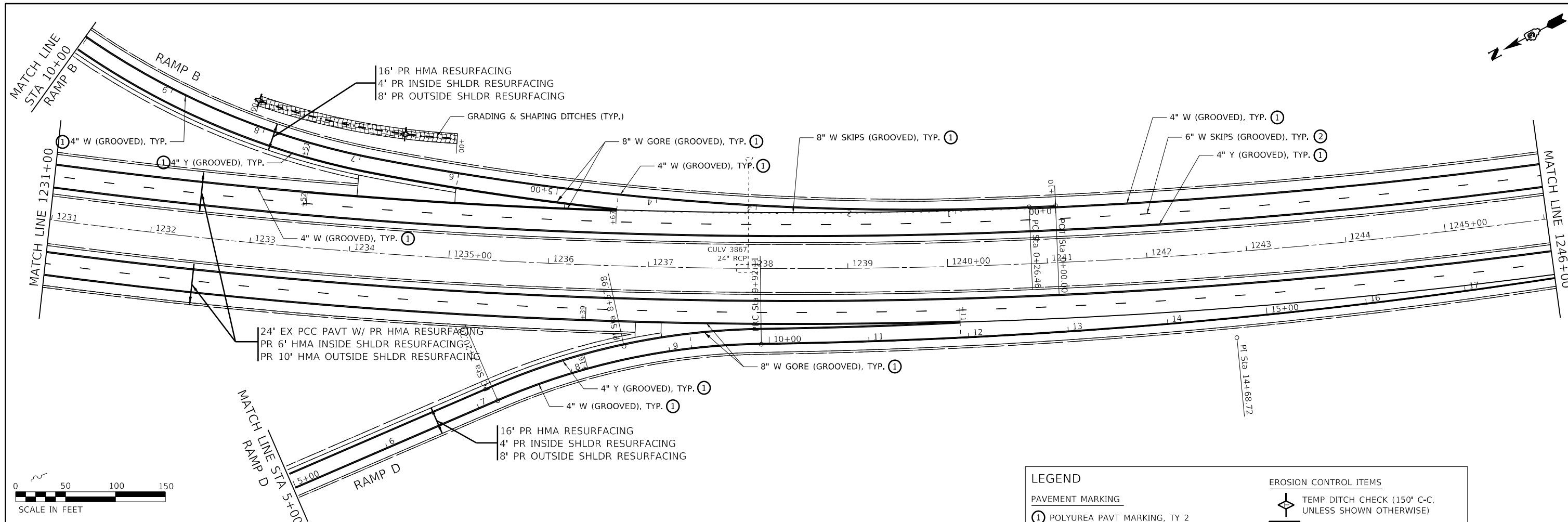


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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	PLOT DATE = 12/17/2021	DATE -	REVISED -

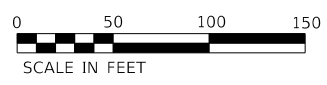
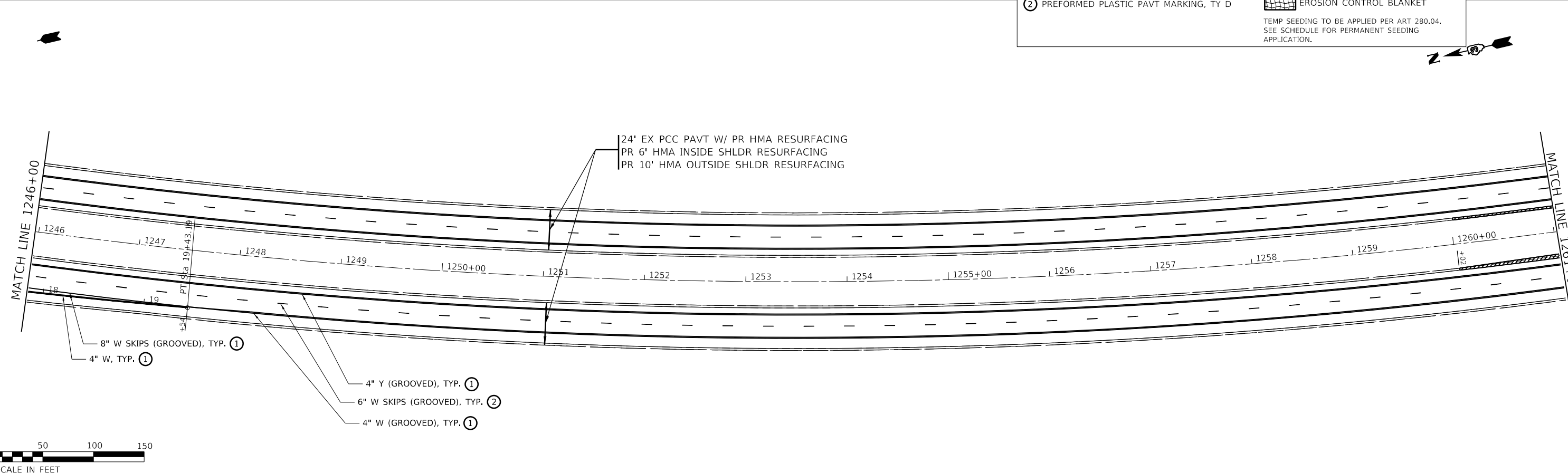
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 PLANS				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

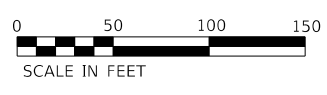
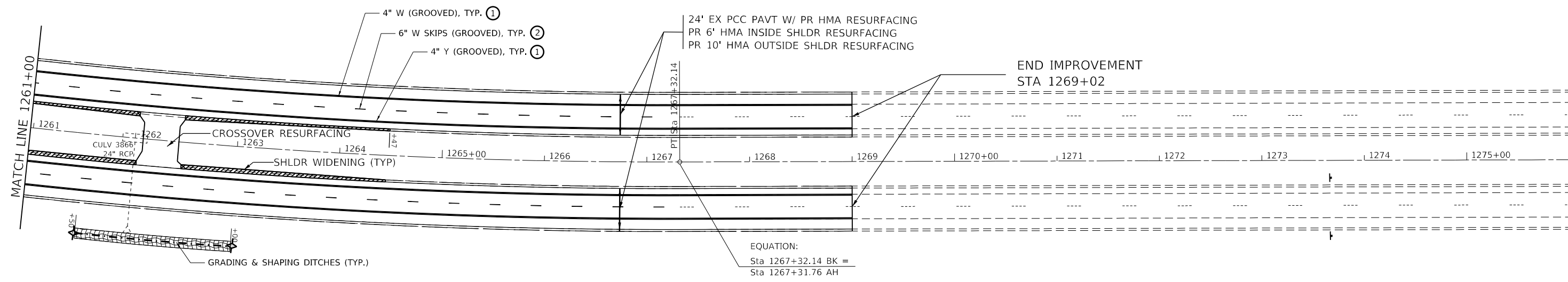
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	57
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND		EROSION CONTROL ITEMS	
PAVEMENT MARKING		TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)	
①	POLYUREA PAVT MARKING, TY 2		
②	PREFORMED PLASTIC PAVT MARKING, TY D		EROSION CONTROL BLANKET
		TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.	

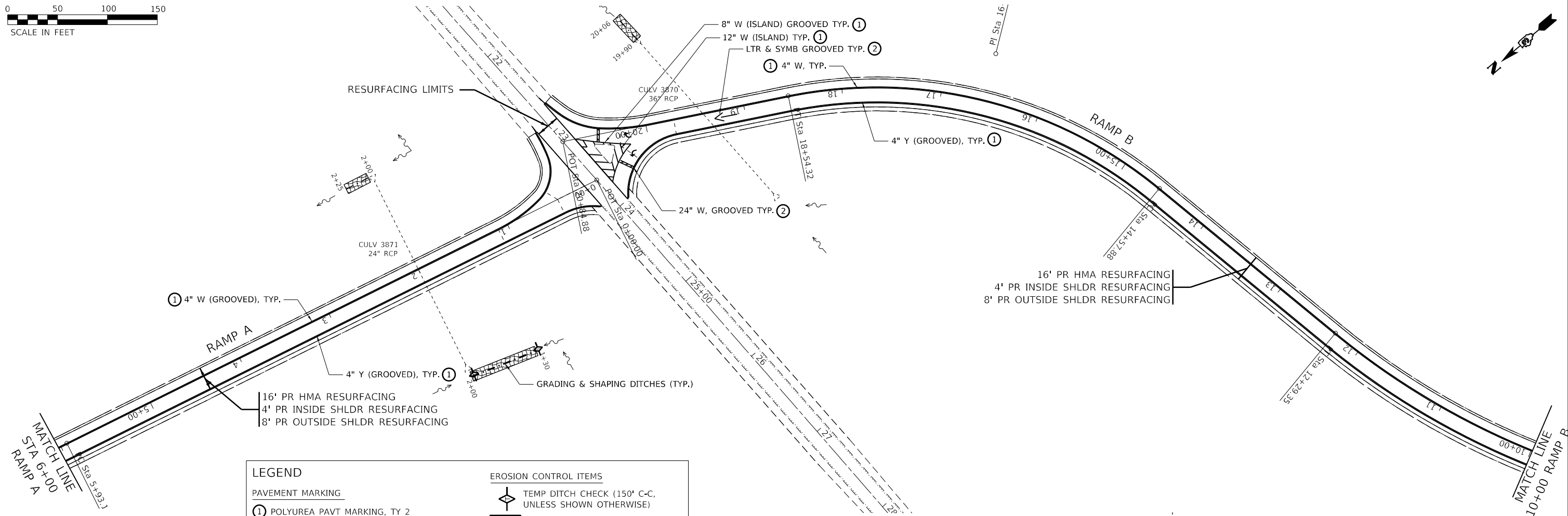
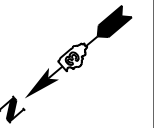


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-57 PLANS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\pwwd\calderont\0521537\0368H63-eh-1-plan.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	58				
SMODELNAMES	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



LEGEND		EROSION CONTROL ITEMS	
PAVEMENT MARKING		TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)	
①	POLYUREA PAVT MARKING, TY 2		
②	PREFORMED PLASTIC PAVT MARKING, TY D		EROSION CONTROL BLANKET
		TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.	

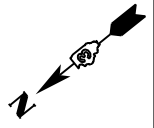
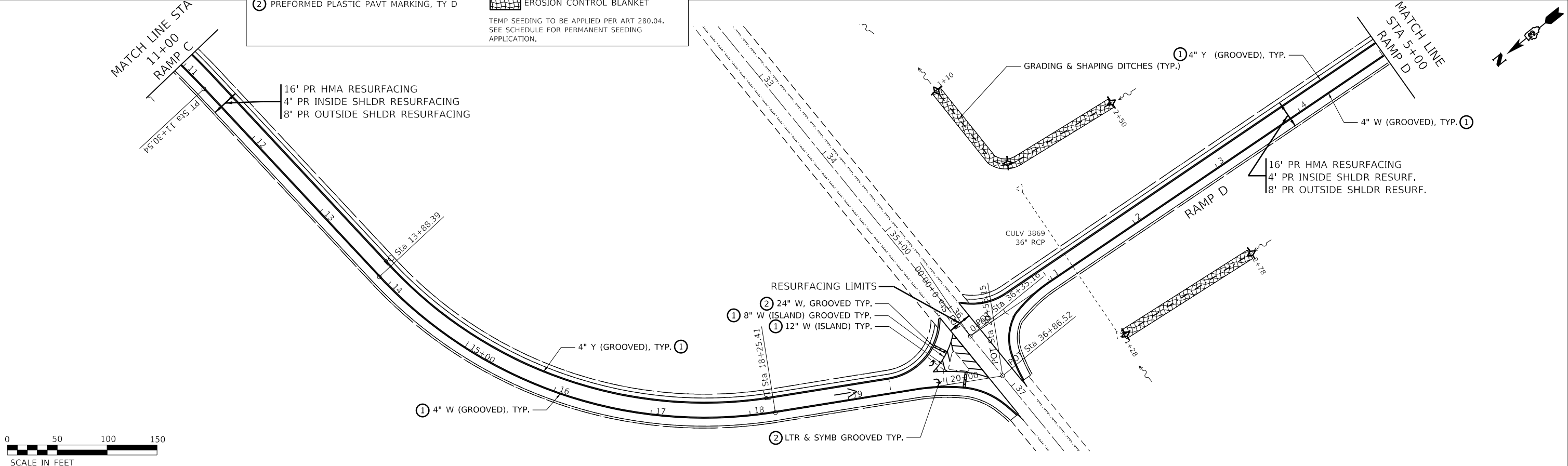
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ec:\pw_work\p\widoc\calderont\05215371\0366H63-eh-plan.dgn		DRAWN -	REVISED -			57	(38-7,38-6)RS	IROQUOIS	130	59	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		



LEGEND

PAVEMENT MARKING
 ① POLYUREA PAVT MARKING, TY 2
 ② PREFORMED PLASTIC PAVT MARKING, TY D

EROSION CONTROL ITEMS
 TEMP DITCH CHECK (150' C-C, UNLESS SHOWN OTHERWISE)
 EROSION CONTROL BLANKET
 TEMP SEEDING TO BE APPLIED PER ART 280.04. SEE SCHEDULE FOR PERMANENT SEEDING APPLICATION.

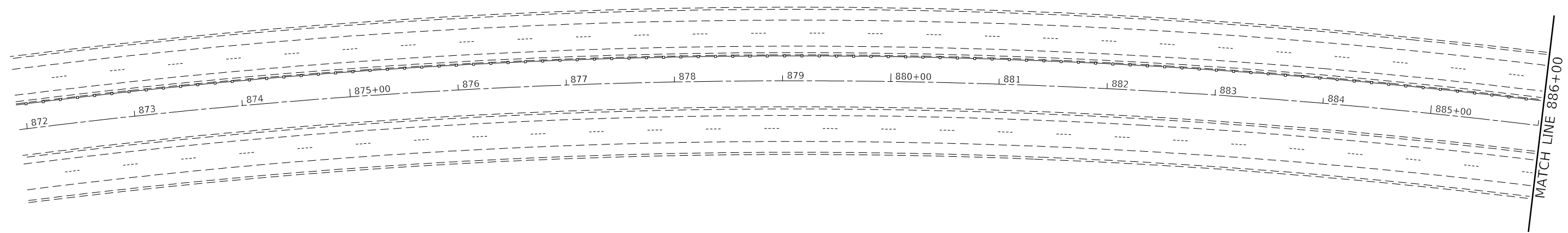


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-57 PLANS			
BUCKLEY INTERCHANGE RAMPS			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

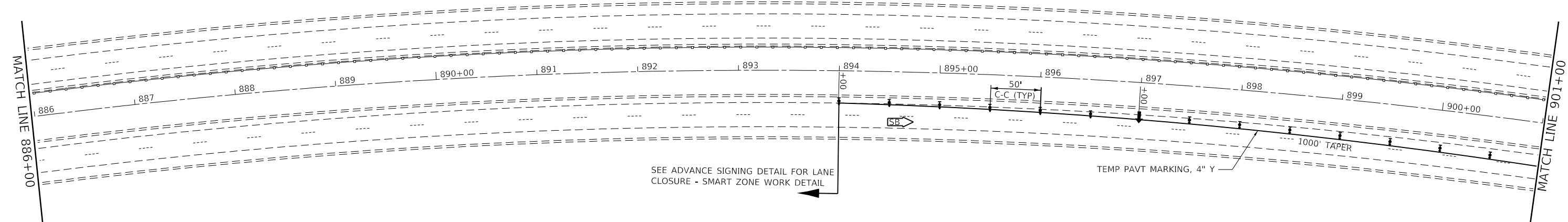
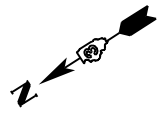
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	60
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.

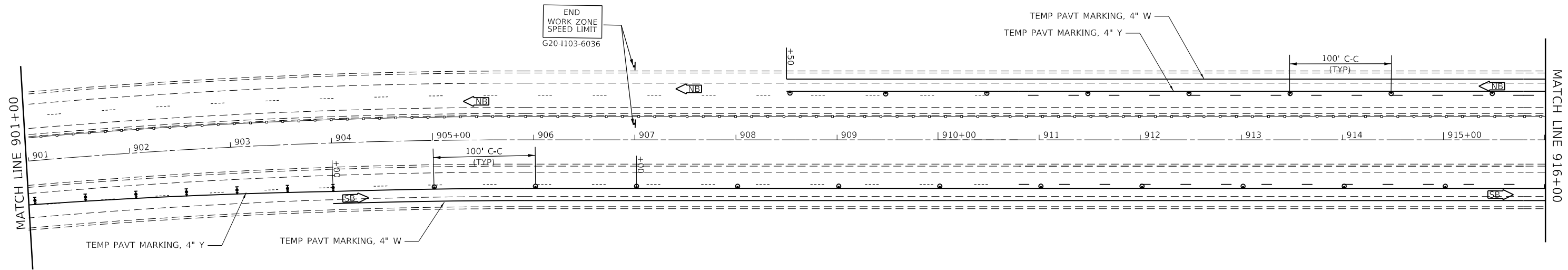


FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 1 TRAFFIC CONTROL			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

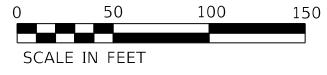
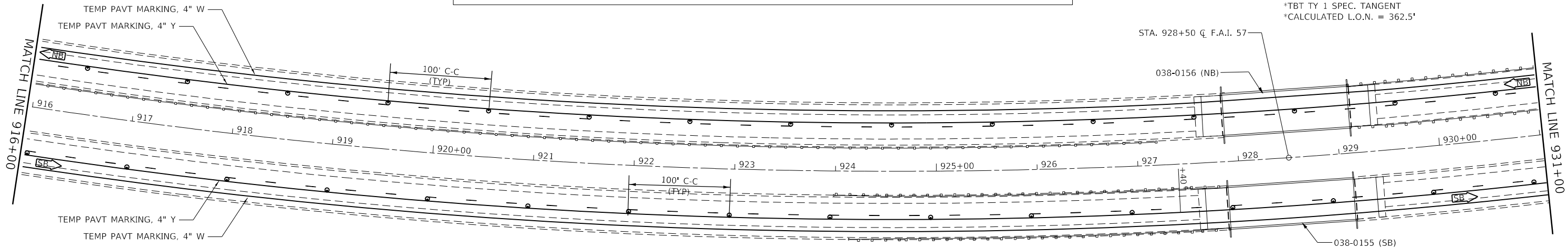
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	61
CONTRACT NO. 66H63				
ILLINOIS		FED. AID PROJECT		



LEGEND

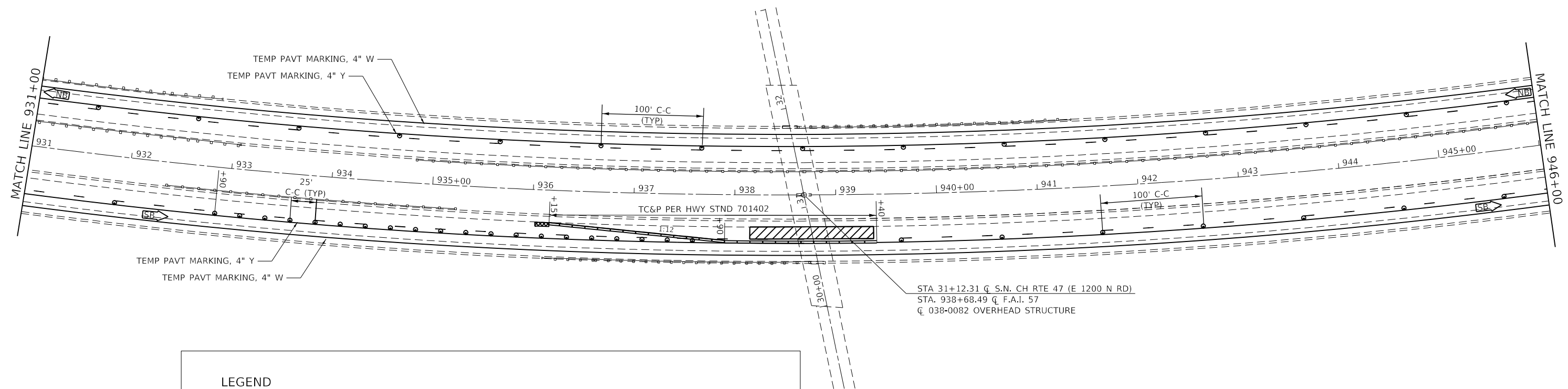
- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



STA 929+11 TO STA 932+86
 *TBT TY 6 (37.5')
 *287.5' SPBGR TY A 6' POSTS
 *TBT TY 1 SPEC. TANGENT
 *CALCULATED L.O.N. = 362.5'

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\p\wdofcalderont\05215371\0366H63-eh-1-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	62				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

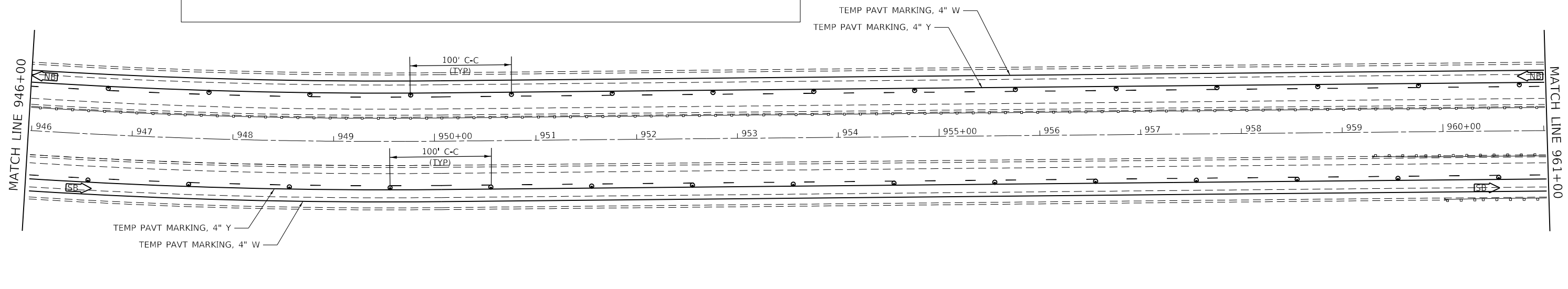


STA 31+12.31 Q S.N. CH RTE 47 (E 1200 N RD)
 STA. 938+68.49 Q F.A.I. 57
 Q 038-0082 OVERHEAD STRUCTURE

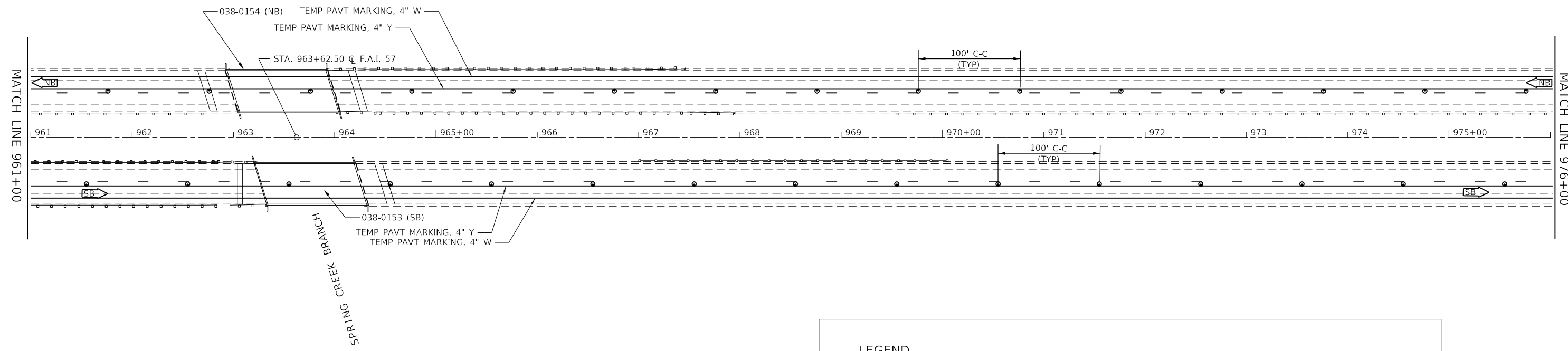
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



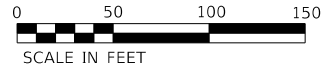
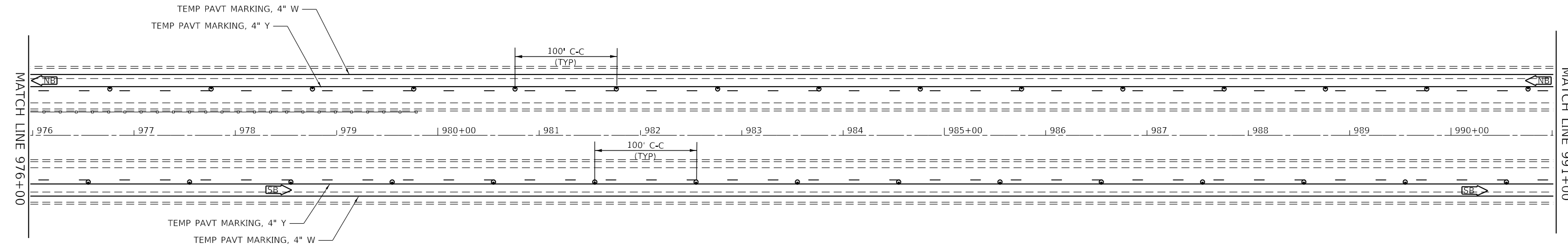
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\pwwidof\calderont\0521537\1D366H63-eh-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	63				
PLOT SCALE = 100,0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 66H63								
PLOT DATE = 12/17/2021		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



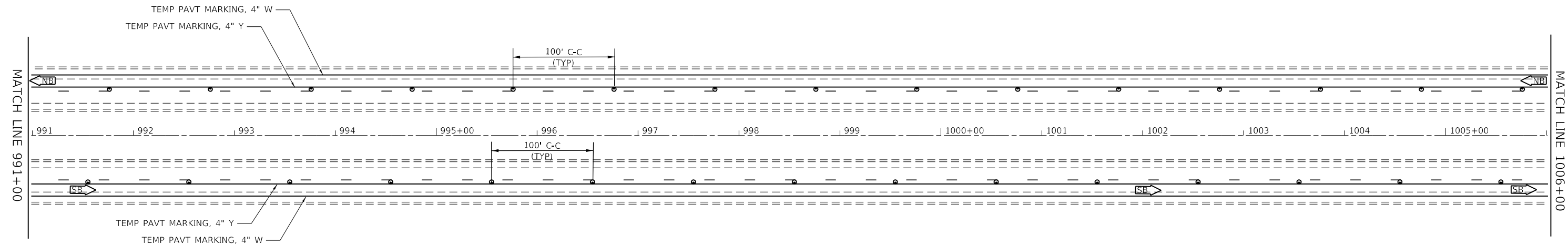
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



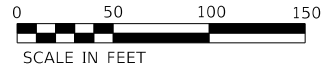
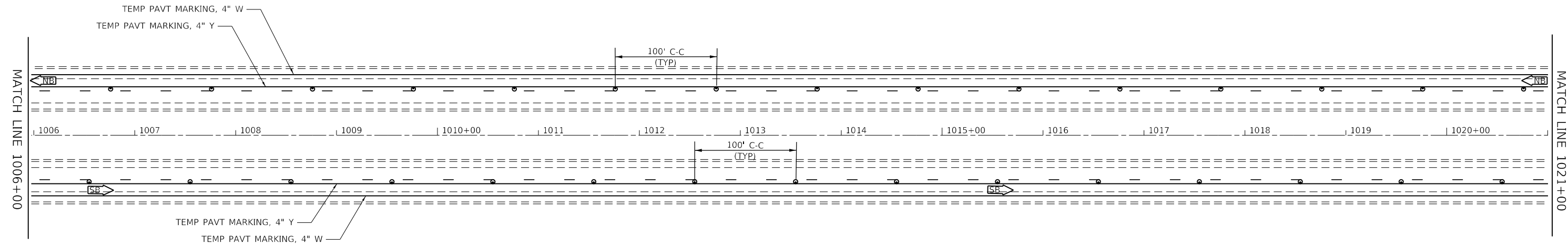
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PLOT SCALE = 100,0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 66H63								
PLOT DATE = 12/17/2021		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



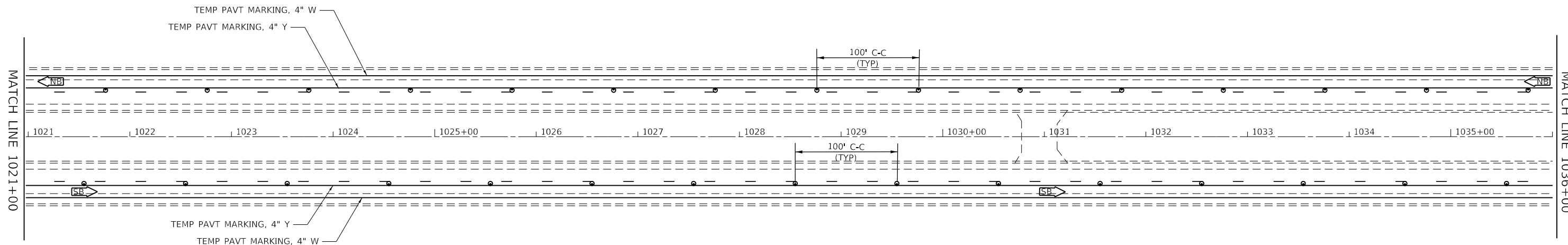
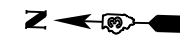
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



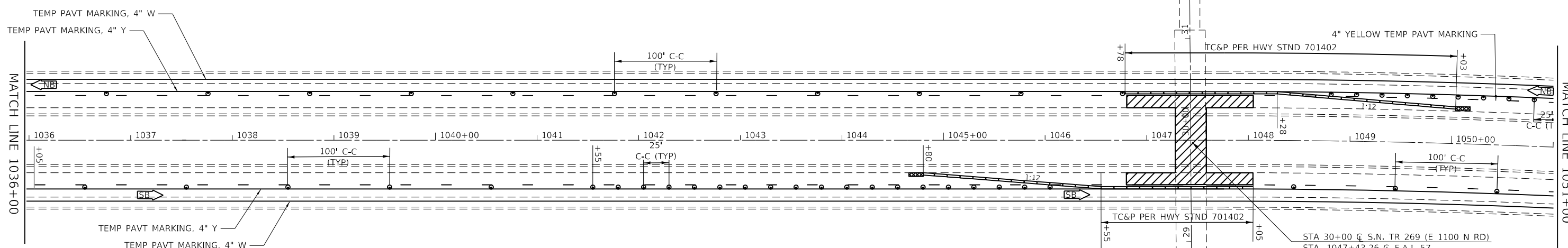
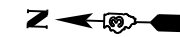
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es:\pw_work\pwwidofcalderont\0521537\ID366H63-eh-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	65				
SMODELNAME\$	PLOT SCALE = 100,0000 "/in.	CHECKED -	REVISED -		CONTRACT NO. 66H63								
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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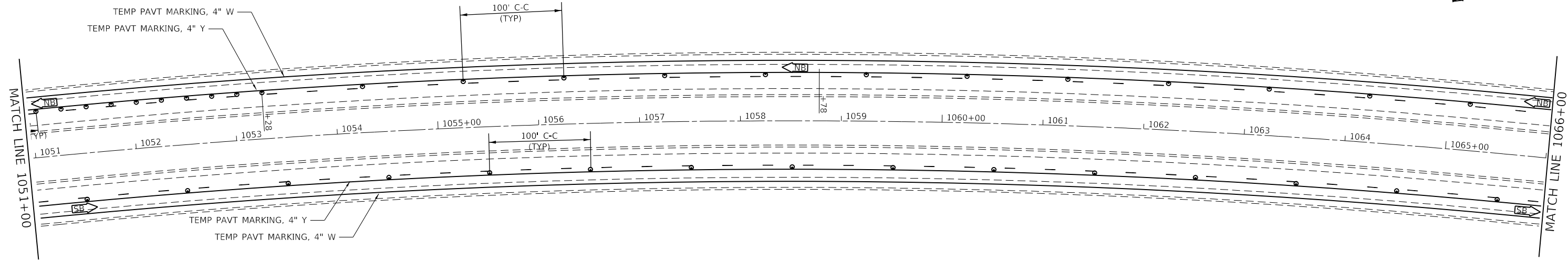
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 1 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	66
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

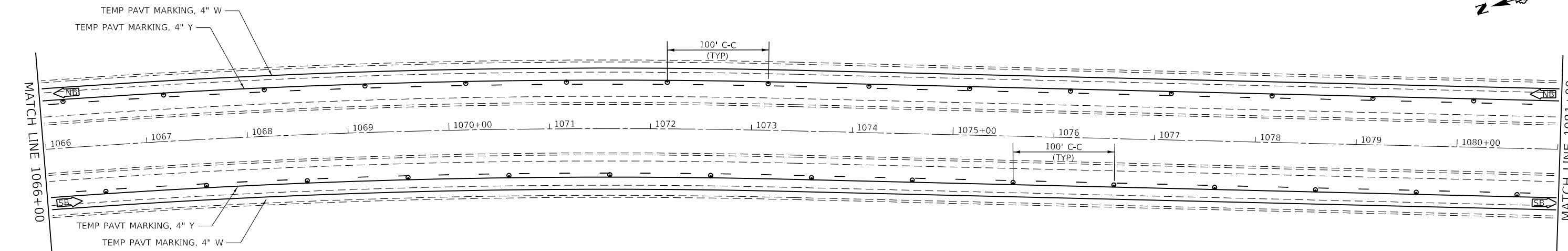
STA 30+00 \bar{C} S.N. TR 269 (E 1100 N RD)
 STA. 1047+43.26 \bar{C} F.A.I. 57
 \bar{C} 038-0132 OVERHEAD STRUCTURE



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



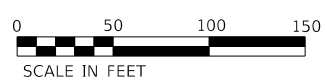
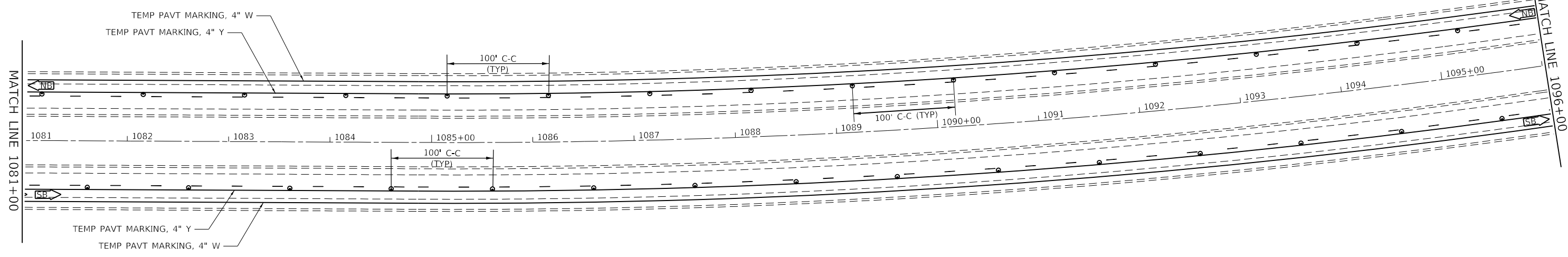
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 1 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

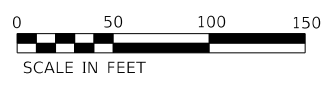
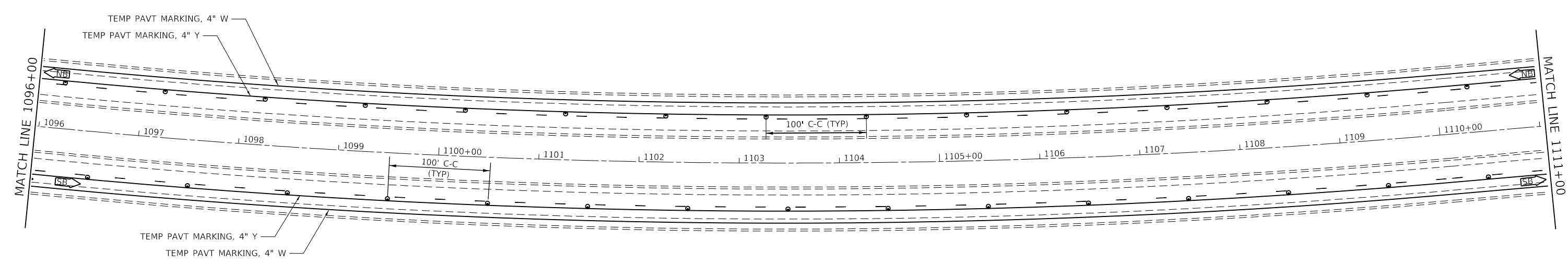
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	67
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



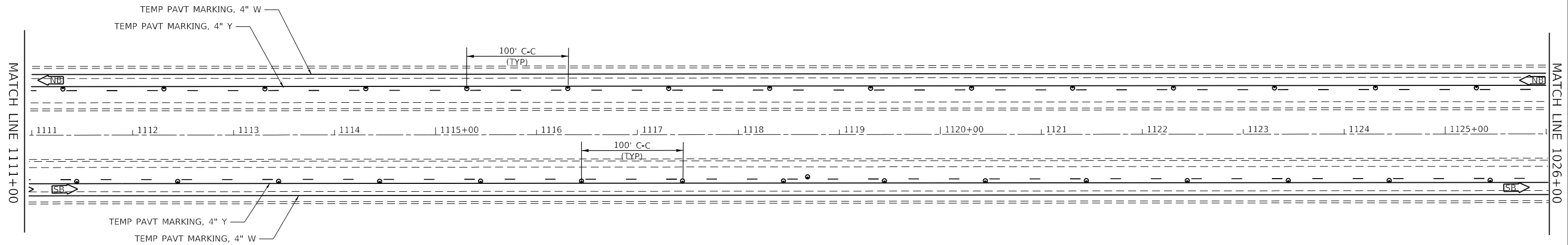
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



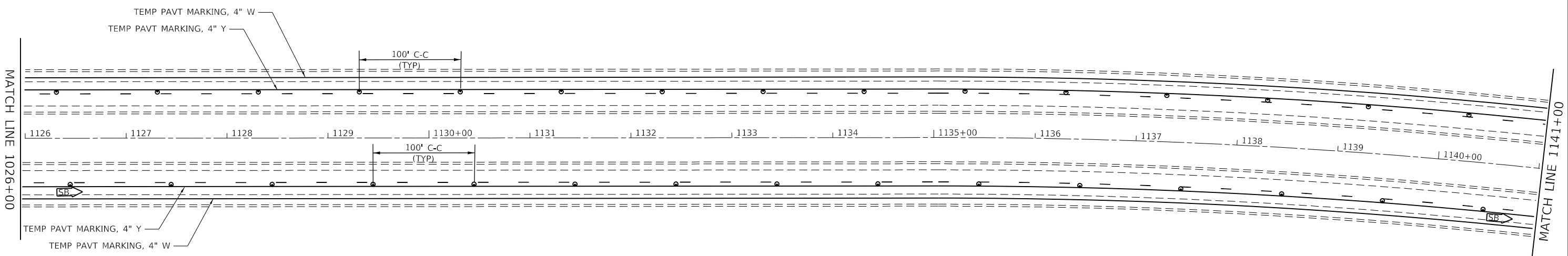
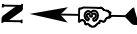
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ec:\pw_work\p\wdof\calderont\0521537\ID366H63-eh-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	68				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



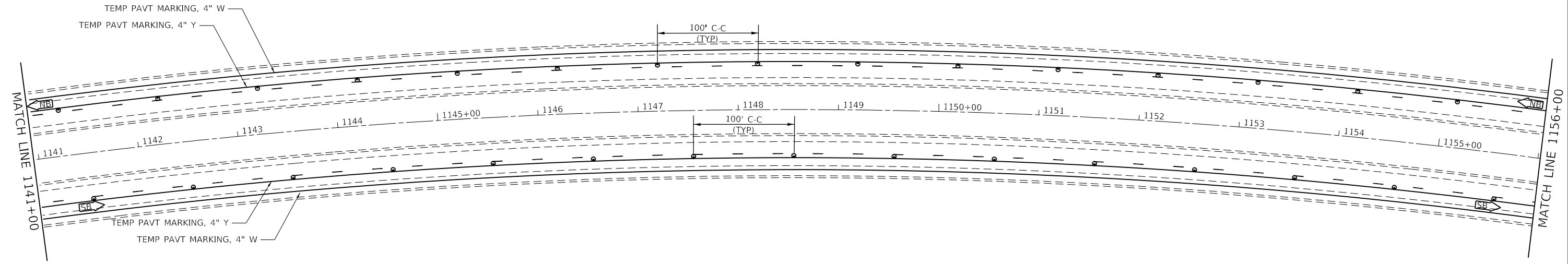
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



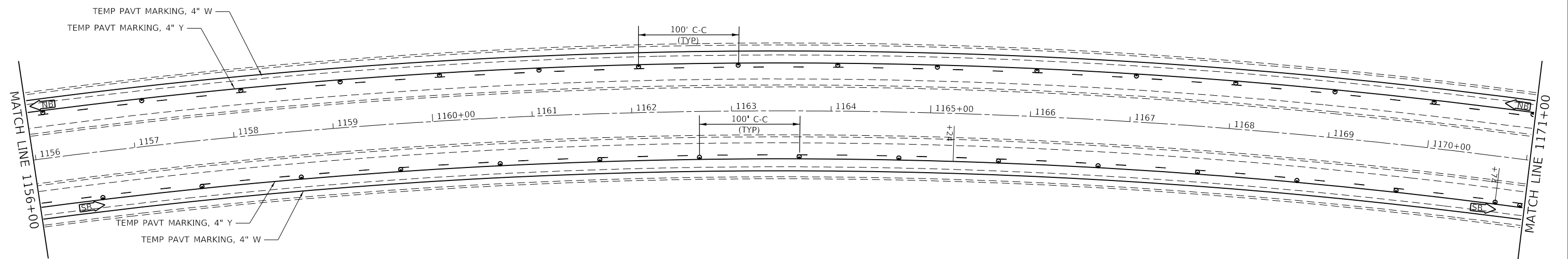
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ec:\pw_work\pwwid\calderont\0521537\1D366H63-eh-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	69				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63								
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



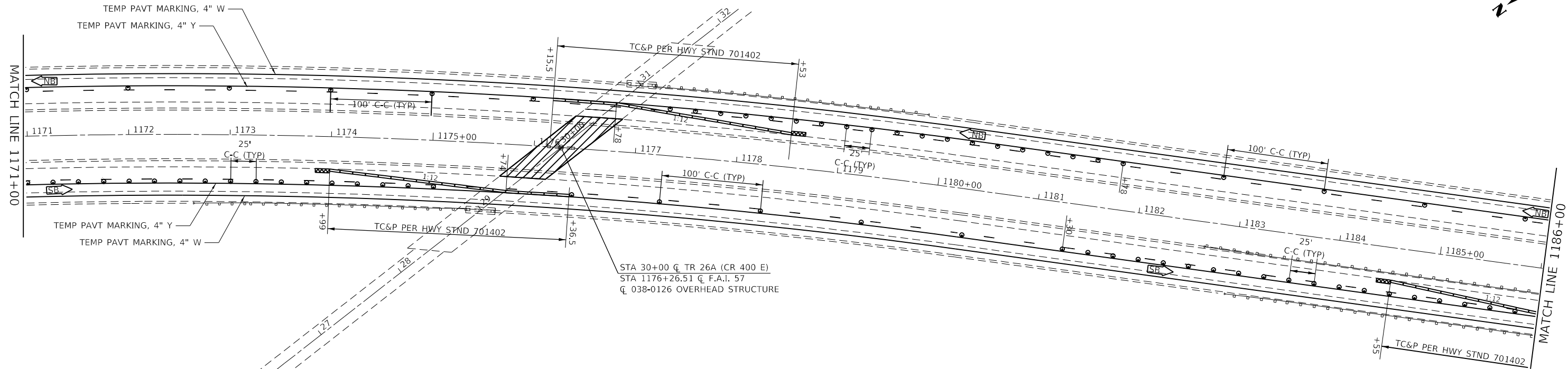
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 1 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

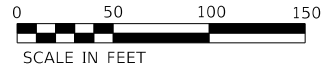
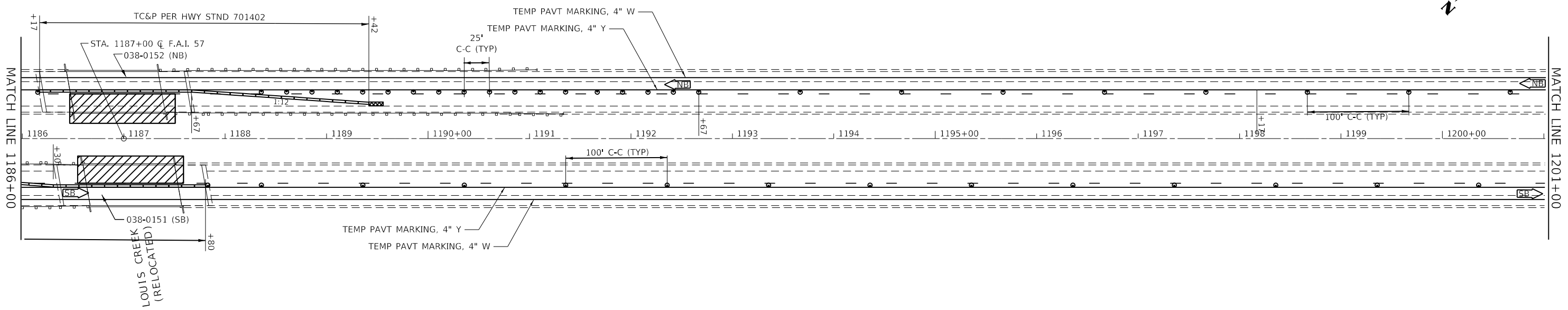
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	70
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



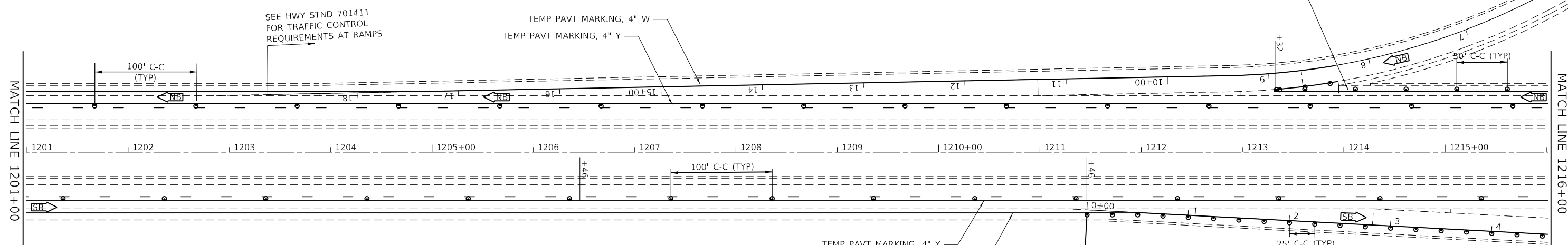
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 1 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

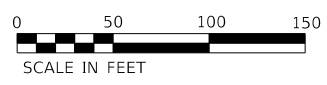
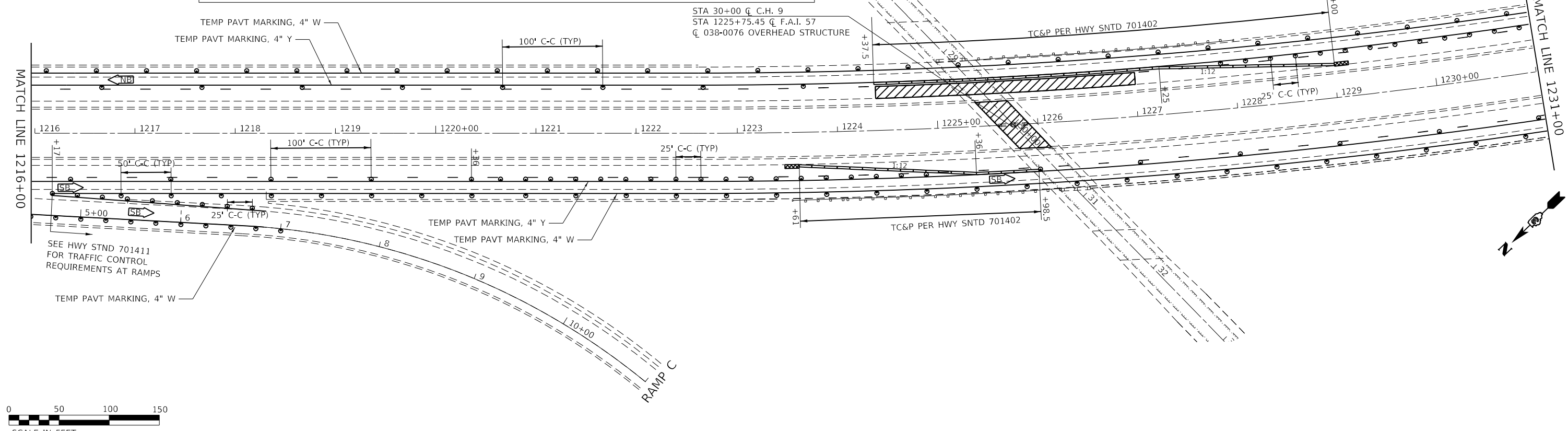
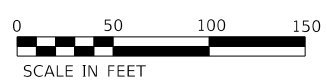
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	71
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



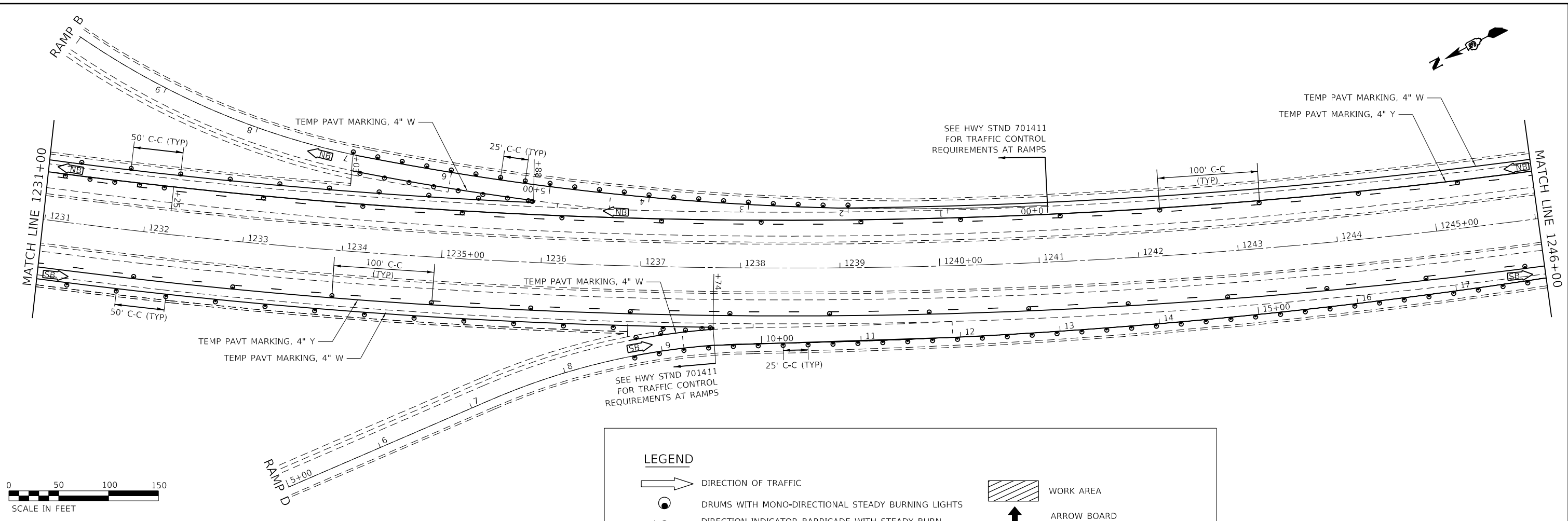
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTES:
 *ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.
 *USE TRAFFIC CONTROL 701411 FOR ALL RAMPS (NOT ALL TRAFFIC CONTROL DEVICES & SIGNING SHOWN)
 *ADJUST RAMP OPENINGS IF REQUIRED.



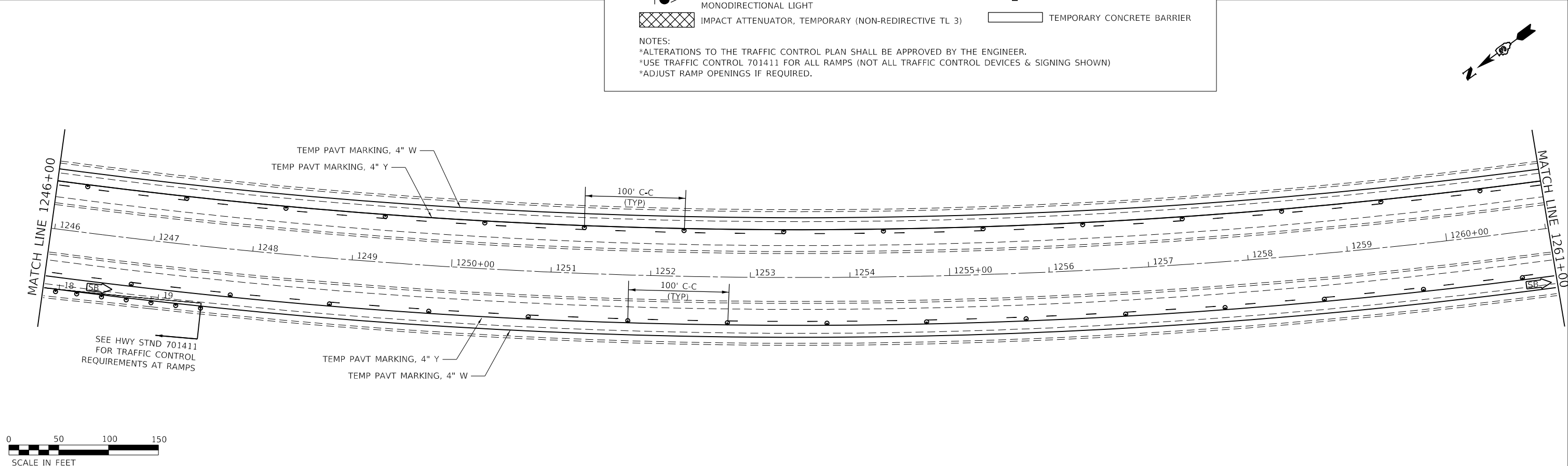
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -						57	(38-7,38-6)RS	IROQUOIS	130	72
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63					
								ILLINOIS FED. AID PROJECT					



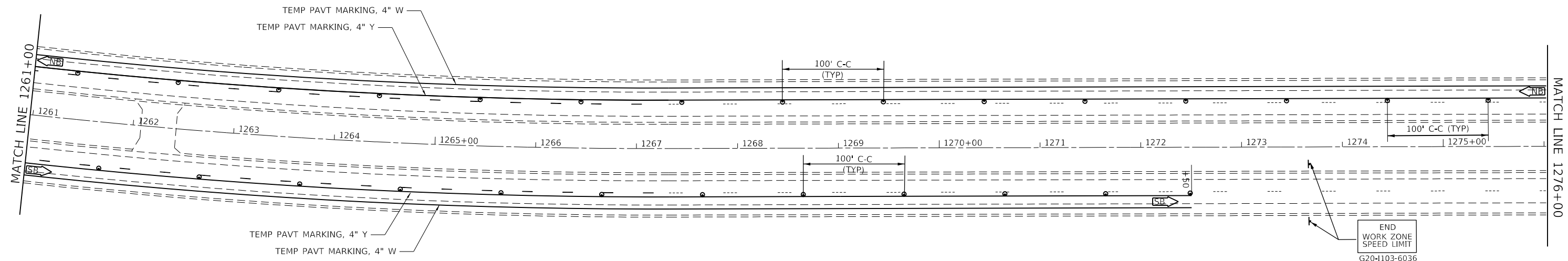
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTES:
 *ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.
 *USE TRAFFIC CONTROL 701411 FOR ALL RAMPS (NOT ALL TRAFFIC CONTROL DEVICES & SIGNING SHOWN)
 *ADJUST RAMP OPENINGS IF REQUIRED.



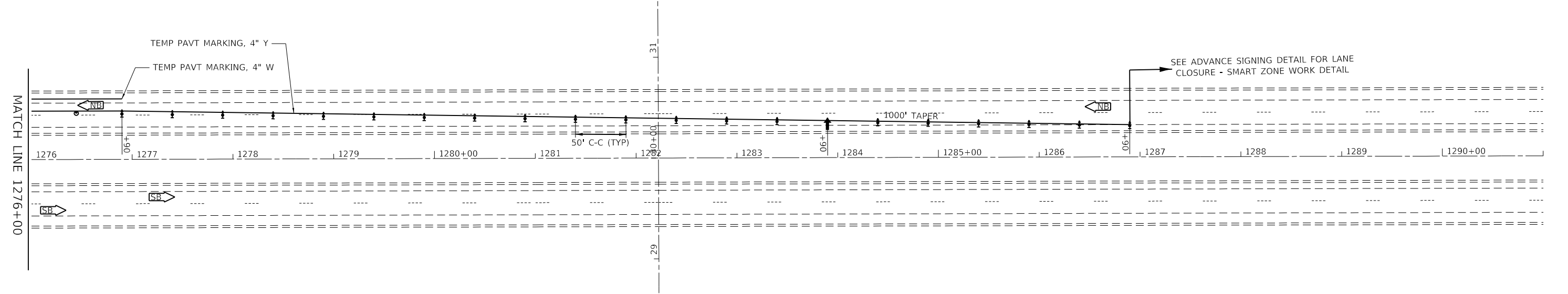
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SMODELNAME\$	es:\pw_work\pwwd\calderont\0521537\10366H63-eh-plan_Stage_1.dgn	DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	73				
	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63								
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



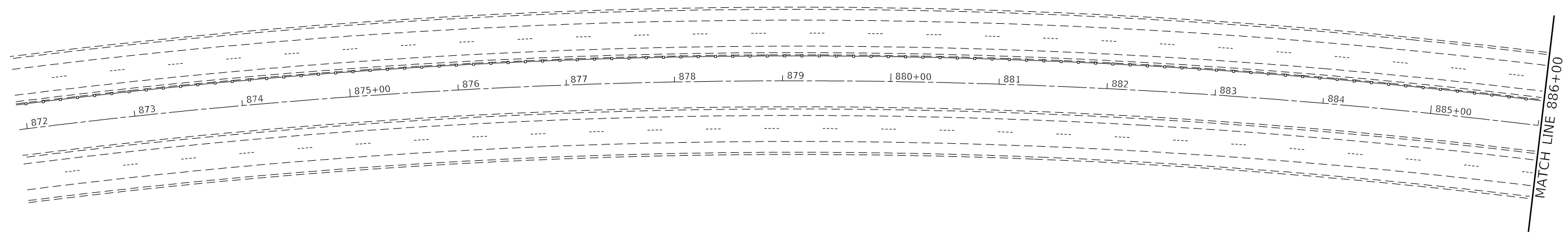
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
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- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

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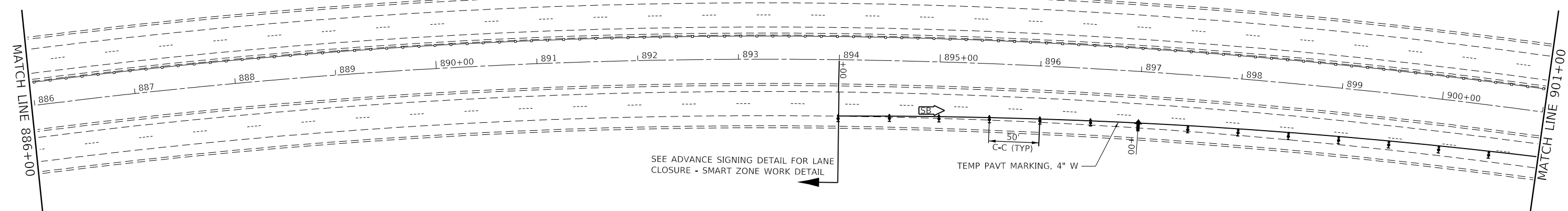
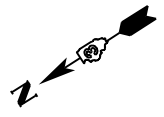
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 1 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\p\id\calderont\0521537\1D36H63-eh-plan_Stage_1.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	74				
SMODELNAMES	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



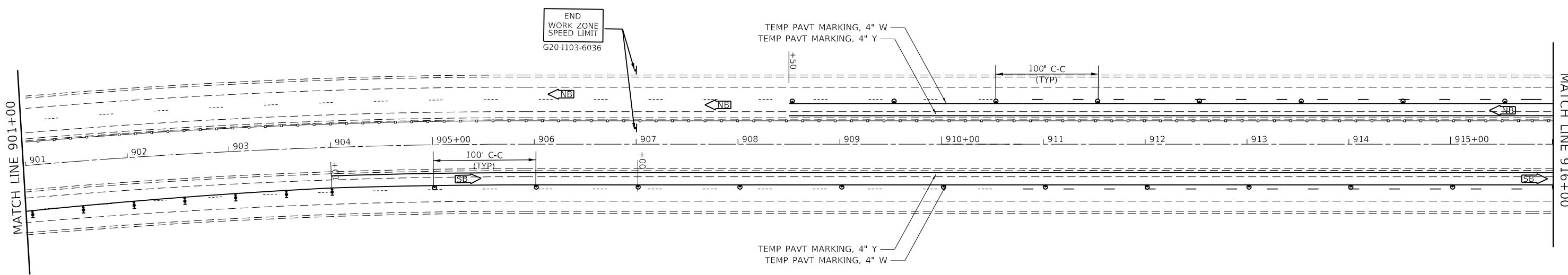
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	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

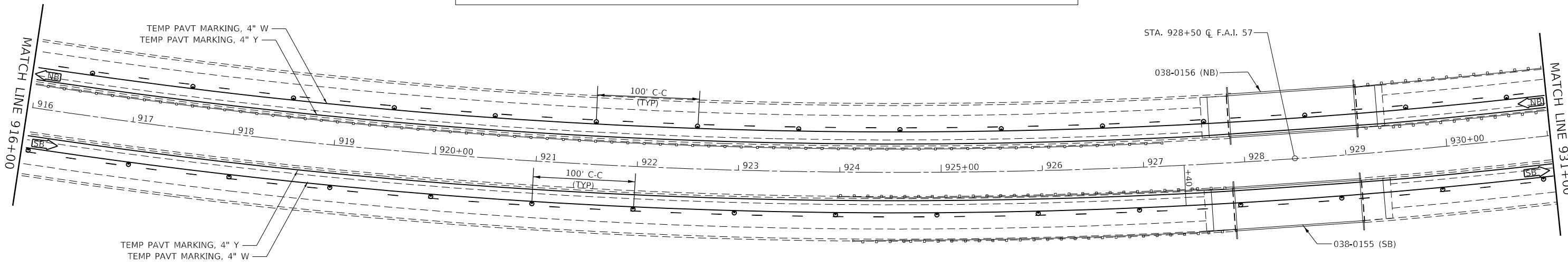
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	75
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



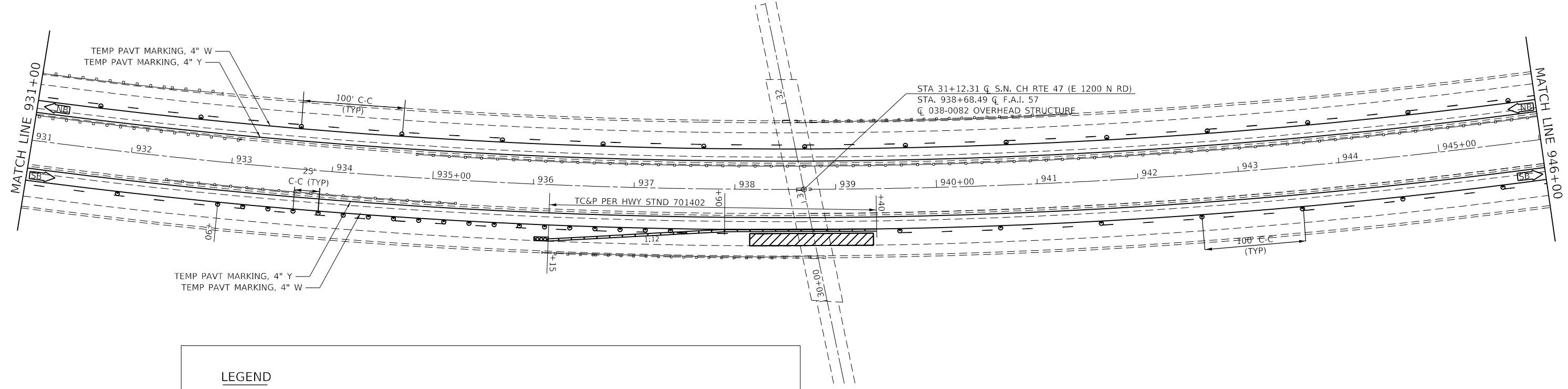
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

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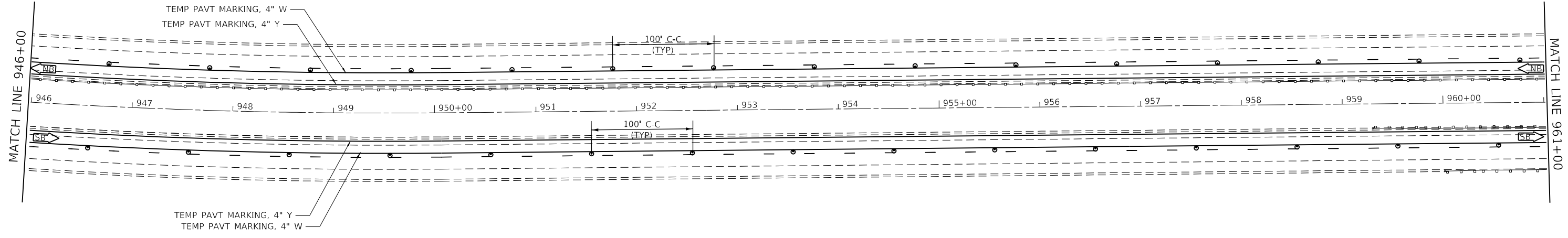
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ec:\pw_work\pwidot\calderont\0521537\ID366H63-eh-plant\Stage_2.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	76				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



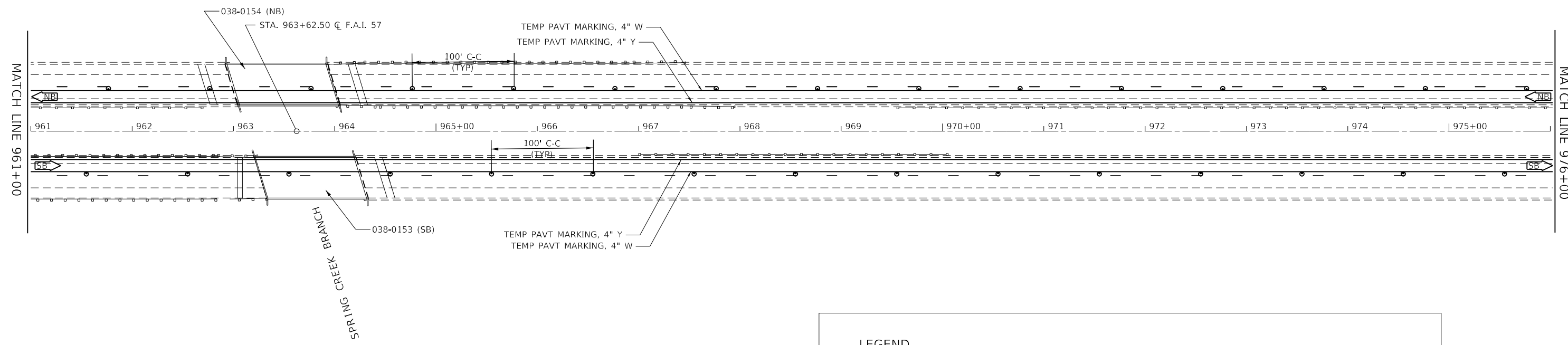
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
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- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



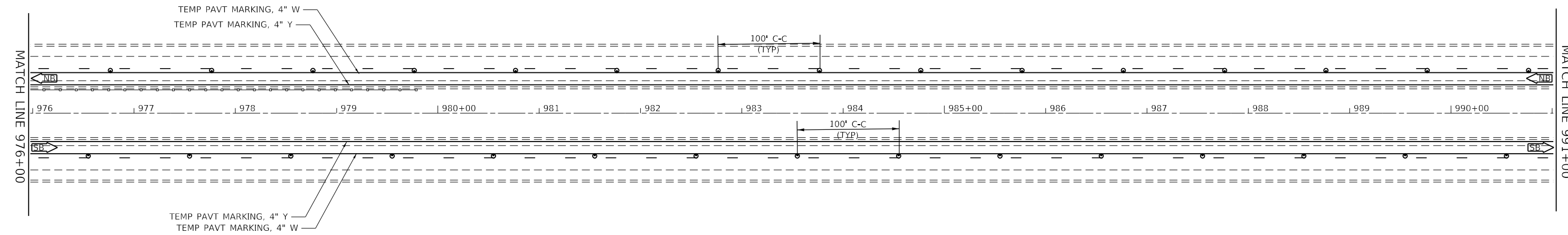
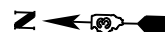
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PLOT SCALE = 100,0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 66H63								
PLOT DATE = 12/17/2021		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
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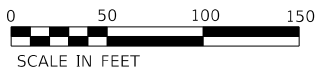
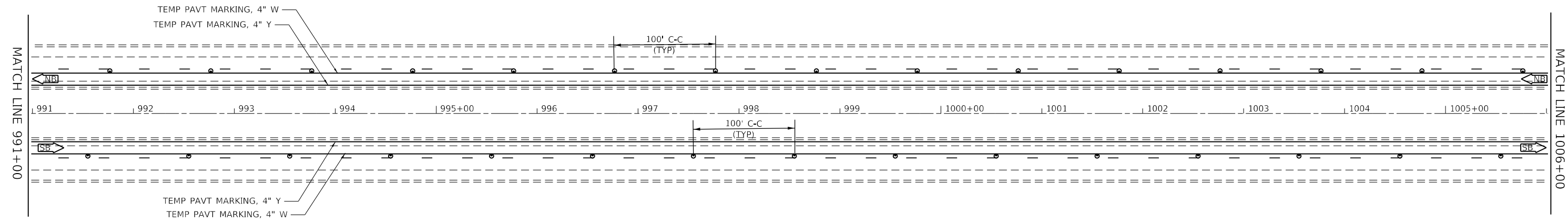
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

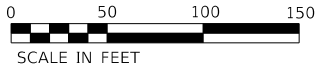
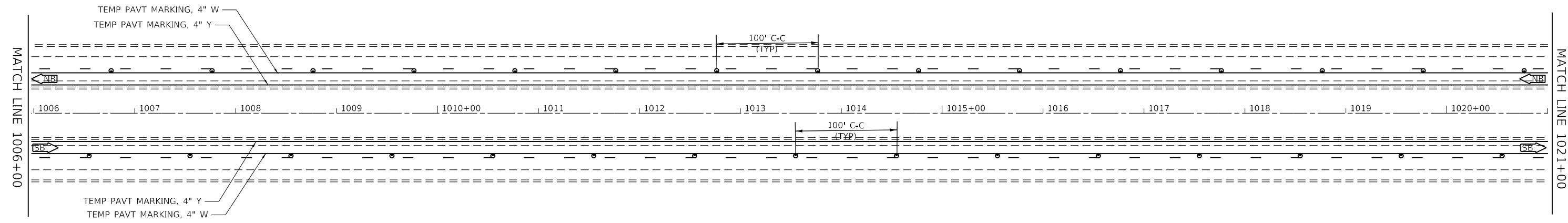
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	78
CONTRACT NO. 66H63				
		ILLINOIS	FED. AID PROJECT	



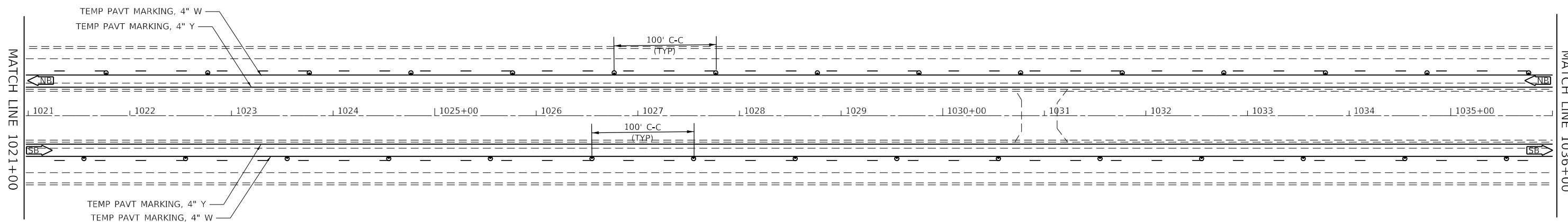
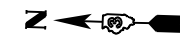
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
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- WORK AREA
- ARROW BOARD
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NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



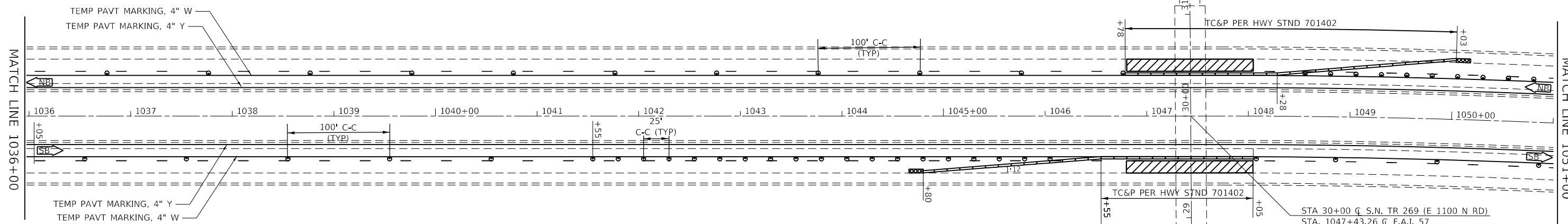
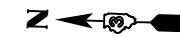
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es:\pw_work\pwwdofcalderont\0521537\10366H63-eh-plan\Stage_2.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	79				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
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STA 30+00 \bar{C} S.N. TR 269 (E 1100 N RD)
 STA. 1047+43.26 \bar{C} F.A.I. 57
 \bar{C} 038-0132 OVERHEAD STRUCTURE

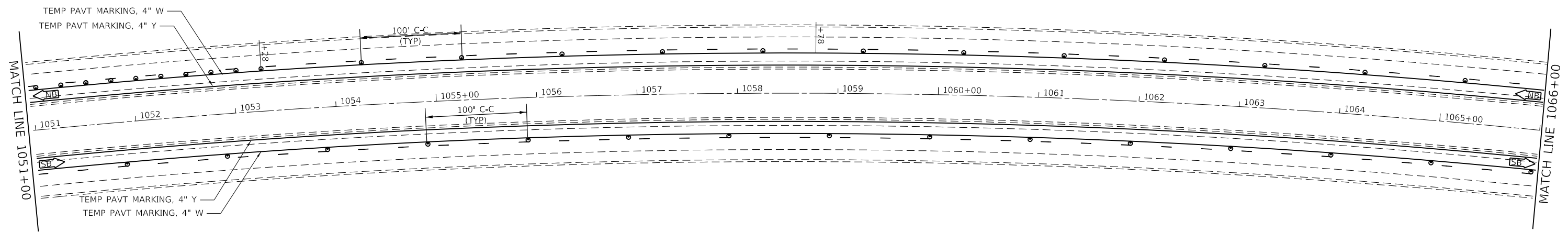
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

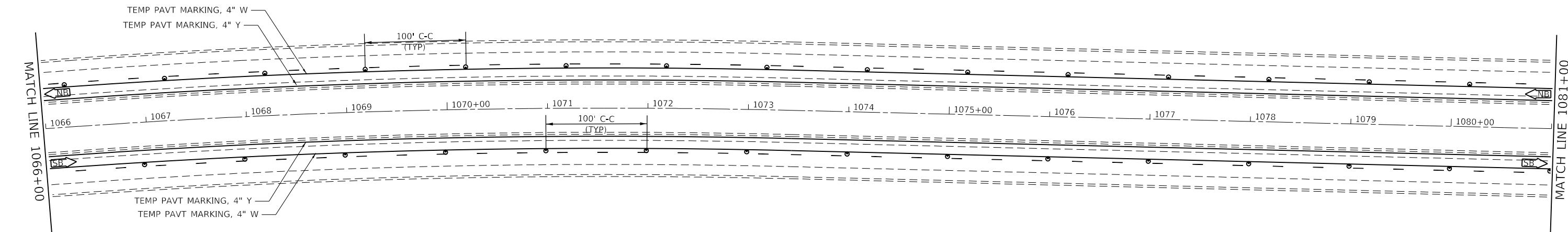
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	80
CONTRACT NO. 66H63				
ILLINOIS		FED. AID PROJECT		



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
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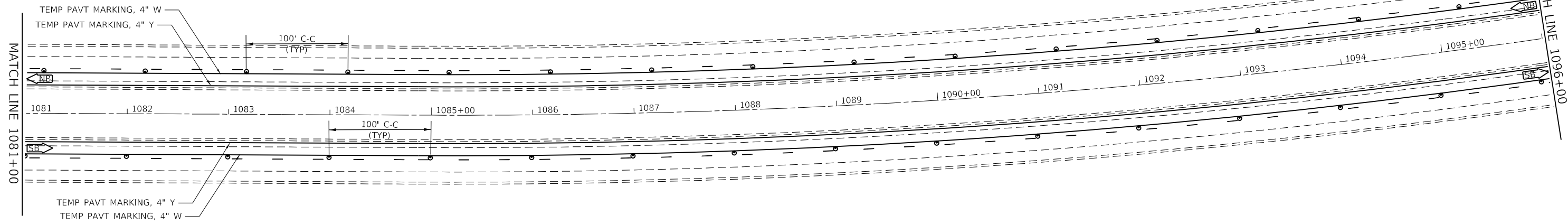
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

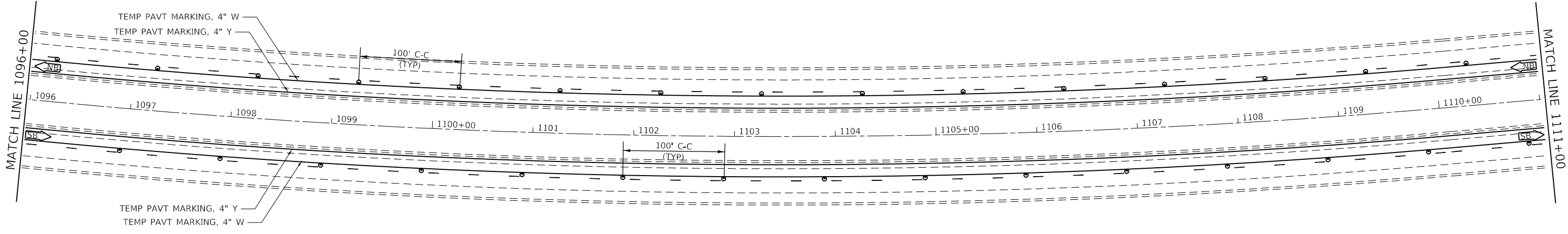
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	81
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
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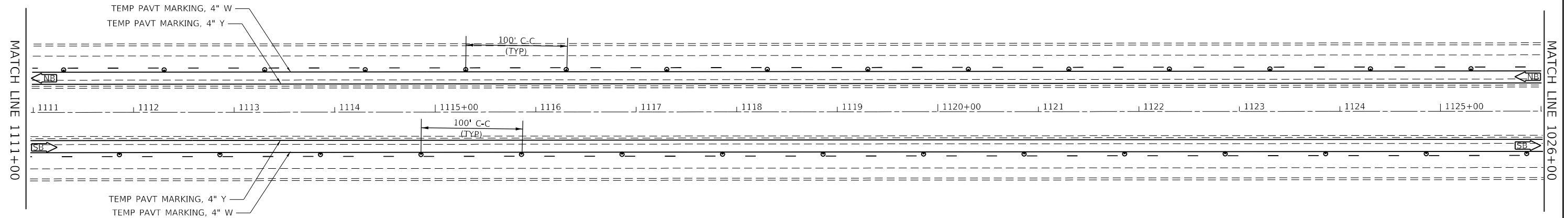
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	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAMES	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE: SHEET OF SHEETS STA. TO STA.

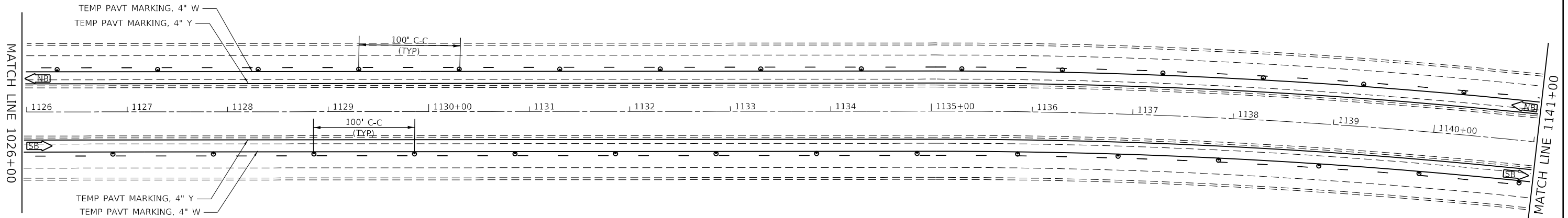
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	82
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



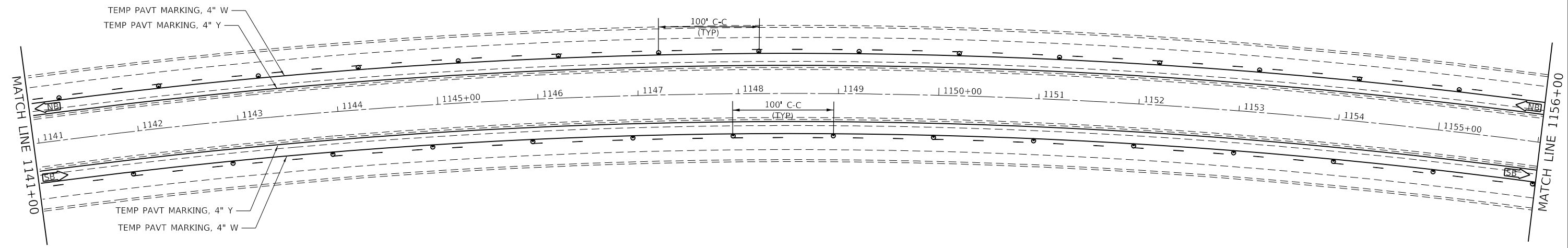
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



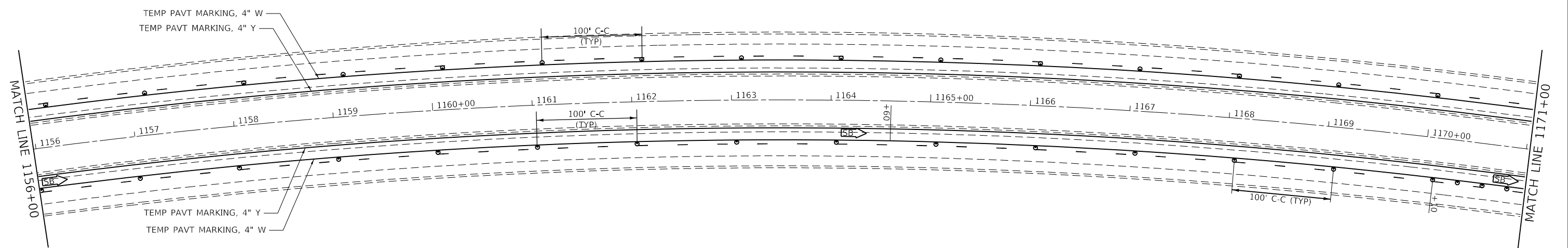
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 2 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ec:\pw_work\pwwd\calderont\0521537\10366H63-eh-plan_Stage_2.dgn	Stage_2.dgn	DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	83				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.

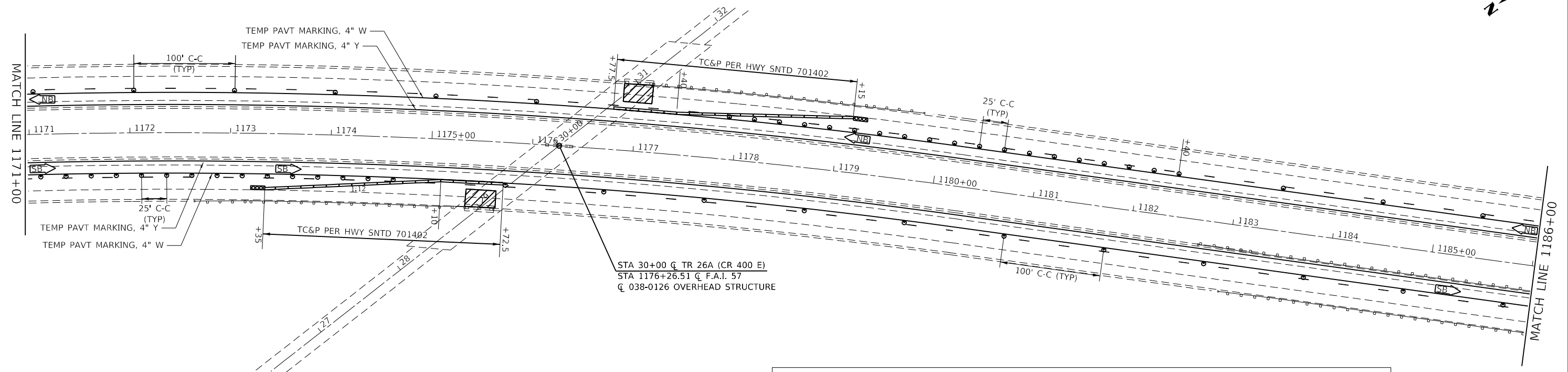
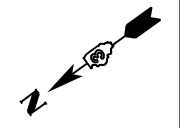


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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

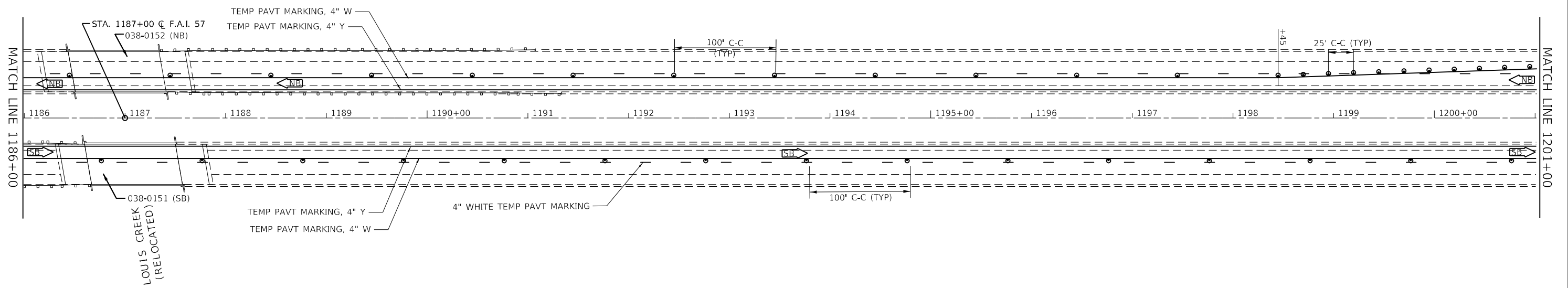
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CONTRACT NO. 66H63				
ILLINOIS		FED. AID PROJECT		



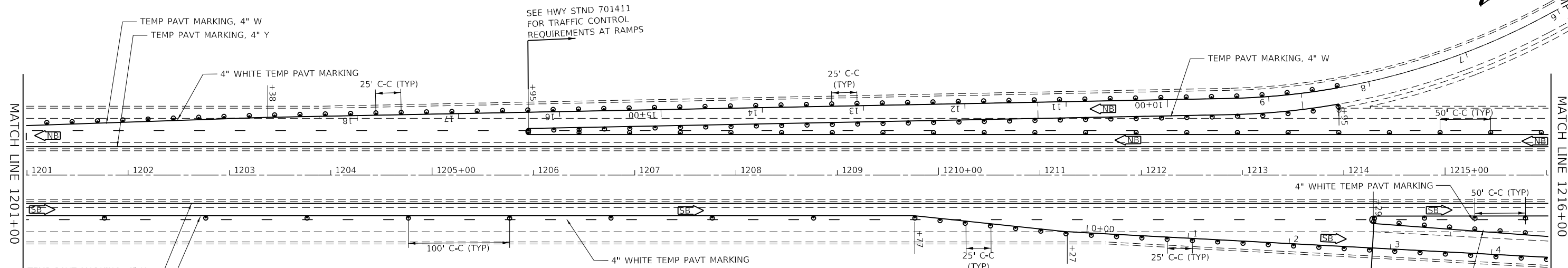
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



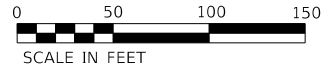
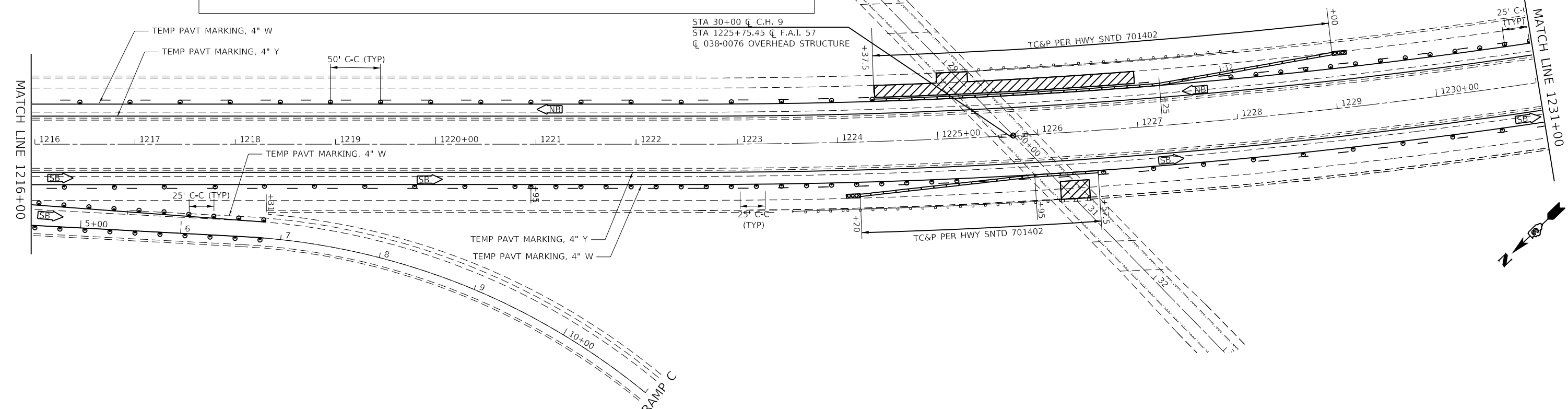
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SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63				ILLINOIS FED. AID PROJECT				
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE:
 *ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.
 *USE TRAFFIC CONTROL 701411 FOR ALL RAMPS (NOT ALL TRAFFIC CONTROL DEVICES & SIGNING SHOWN)
 *ADJUST RAMP OPENINGS IF REQUIRED.



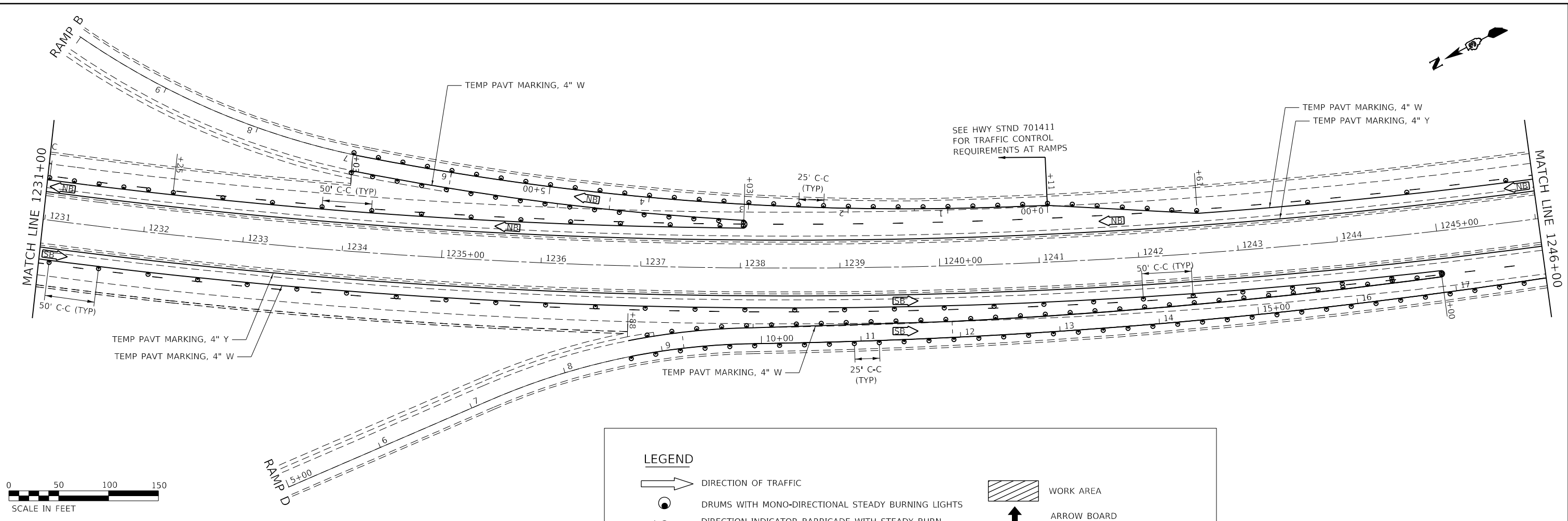
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		CHECKED -	REVISED -
		DATE -	REVISED -
SMODELNAME\$	PLOT DATE = 12/17/2021		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE 2 TRAFFIC CONTROL

SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
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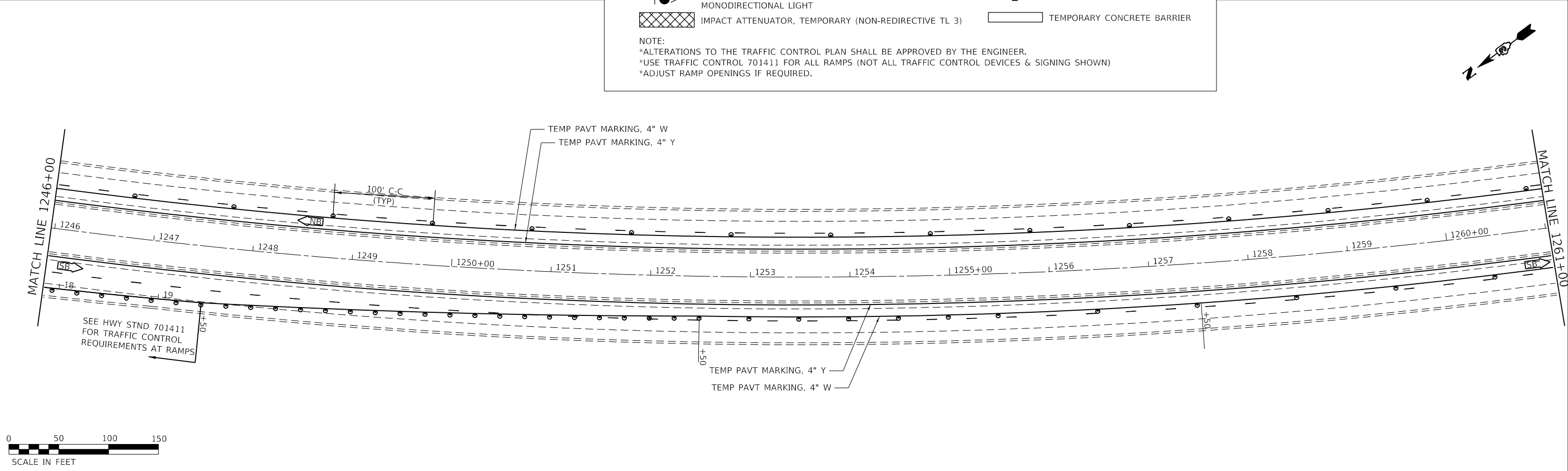
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CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



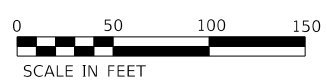
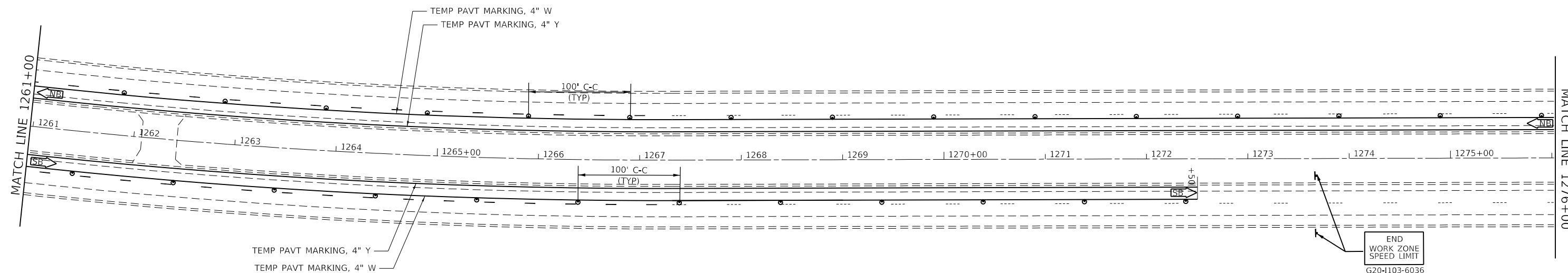
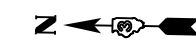
LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE:
 *ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.
 *USE TRAFFIC CONTROL 701411 FOR ALL RAMPS (NOT ALL TRAFFIC CONTROL DEVICES & SIGNING SHOWN)
 *ADJUST RAMP OPENINGS IF REQUIRED.



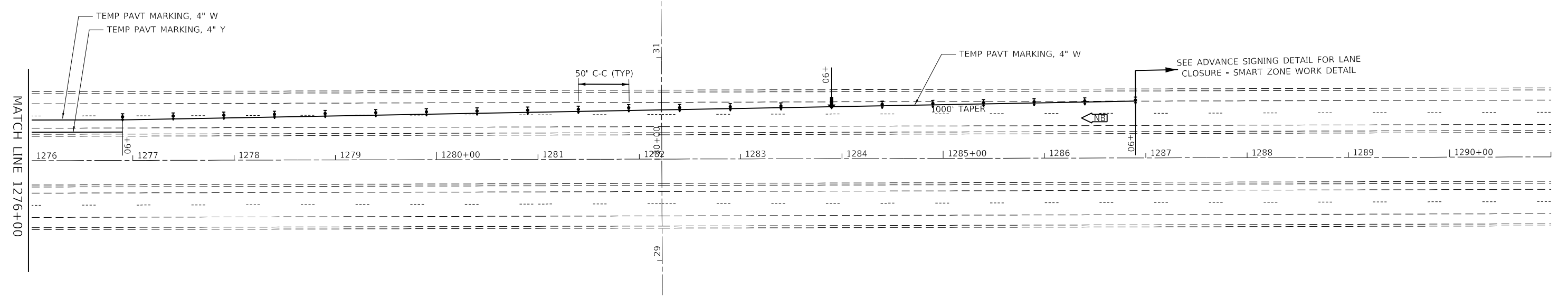
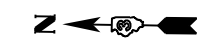
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ec:\pw_work\pwwd\calderont\0521537\1D366H63-eh-plan_Stage_2.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	87				
PLOT SCALE = 100,0000' / in.		CHECKED -	REVISED -		CONTRACT NO. 66H63								
PLOT DATE = 12/17/2021		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT



LEGEND

- DIRECTION OF TRAFFIC
- DRUMS WITH MONO-DIRECTIONAL STEADY BURNING LIGHTS
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- IMPACT ATTENUATOR, TEMPORARY (NON-REDIRECTIVE TL 3)
- WORK AREA
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER

NOTE: ALTERATIONS TO THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE ENGINEER.



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STAGE 2 TRAFFIC CONTROL				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\p\wdot\calderont\0521537\1D366H63-eh-plant\Stage_2.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	88				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				CONTRACT NO. 66H63				
	PLOT DATE = 12/17/2021	DATE -	REVISED -						ILLINOIS FED. AID PROJECT				

EXISTING STRUCTURE: SN 038-0132 IS A 2-SPAN BRIDGE CONSISTING ON CONTINUOUS 2-SPAN COMPOSITE WF GIRDERS UNITS ON VAULT ABUTMENTS AND A OPEN COLUMN PIER. THE STRUCTURE WAS CONSTRUCTED IN 1968 AS SECTION 38-7HB. IN 2000, THE STRUCTURE SLOPEWALL WAS REPAIRED AND CRASH WALL AT CENTERLINE PIER WAS INCREASED BY 1 FOOT. IN 2012 AND 2016, CONCRETE SEALER WAS APPLIED TO THE DECK. TRAFFIC WILL BE MAINTAINED WITH TEMPORARY BARRIER WALLS.

SCOPE OF WORK

1. SET UP TRAFFIC CONTROL USING TEMPORARY BARRIER WALL
2. INSTALL CRASH WALL EXTENSION SO TOTAL HEIGHT IS 5 FEET ABOVE FINISHED GRADE.
3. AFTER CONCRETE CURING, APPLY PROTECTIVE COAT TO EXPOSED AREAS.
4. RESTORE SURROUNDING WORK AREA.

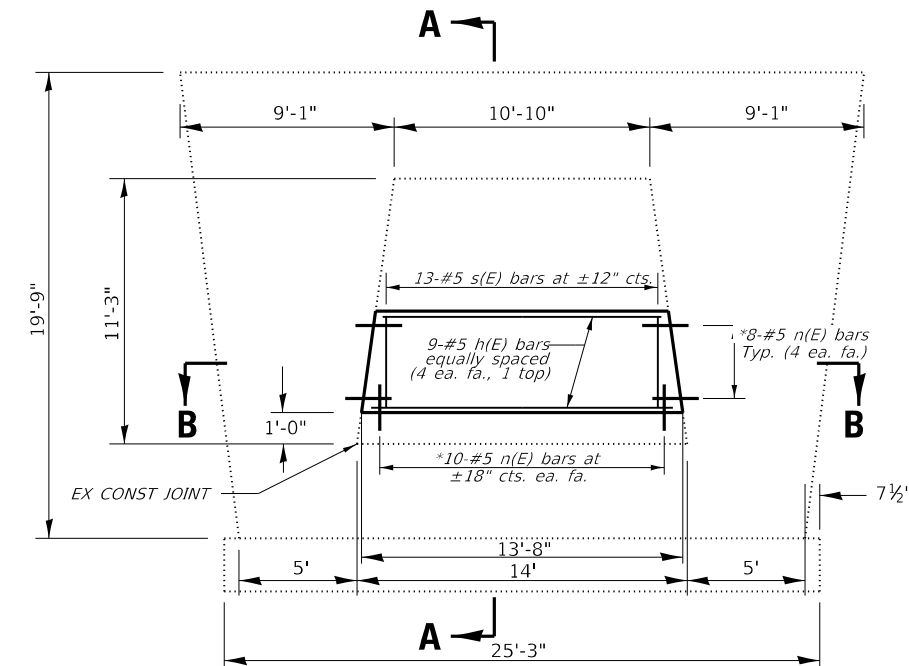
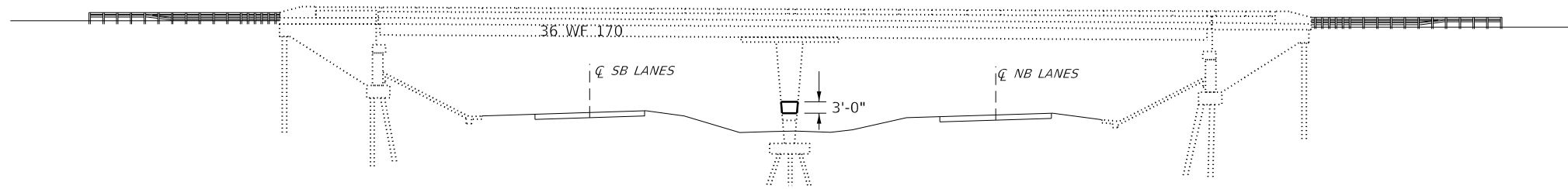
NOTES

THE COST OF EPOXY GROUTING THREADED RODS SHALL BE INCLUDED WITH REINFORCEMENT BARS, EPOXY COATED.

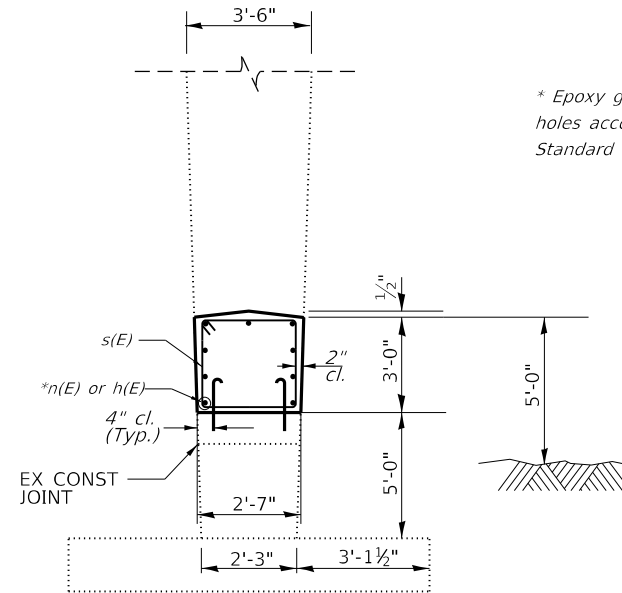
PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

USE EXISTING PLANS INCLUDED TO AVOID DAMAGING LONGITUDINAL REINFORCEMENT IN THE EXISTING CRASHWALL WHILE DRILLING & EPOXY GROUTING n(E) BARS.

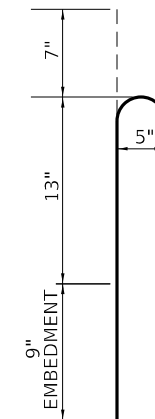


CRASHWALL EXTENSION AT CENTERLINE PIER
ELEVATION

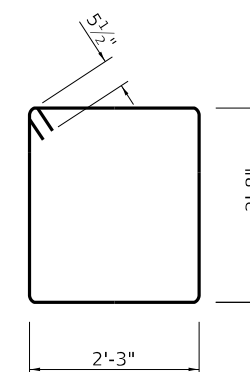


SEC. A-A

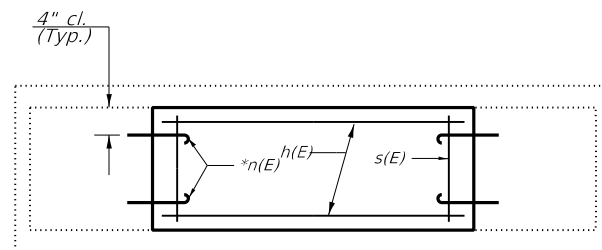
* Epoxy grout n(E) bars in 9" min. holes according to Article 584 of the Standard Specifications



BAR n(E)



BAR s(E)



SEC. B-B



Expires 11-30-2022

David Carl Pusey

02/07/2022

BILL OF MATERIAL (038-0132)

Bar	No	Size	Length	Shape
h(E)	10	#5	12'-6"	—
n(E)	34	#5	2'-5"	⌋
s(E)	13	#5	10'-9"	⌈
Concrete Structures			Cu. Yd.	3.7
Reinforcement Bars, Epoxy Coated			Pound.	362
Protective Coat			Sq. Yd.	12.5

MODEL: 4\MODEL\MAR15... FILE: Mar15..._work\spw\calderon\05215370\366463-shr-bridge_Repair.dgn

USER NAME = calderon	DESIGNED - BDD	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - SMR	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER CRASHWALL EXTENSION
SN 038-0132

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

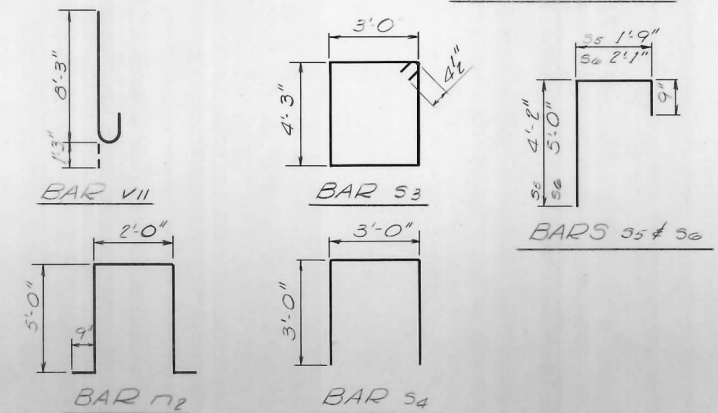
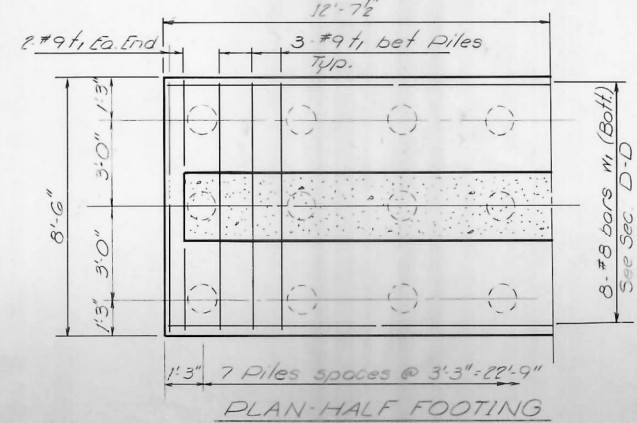
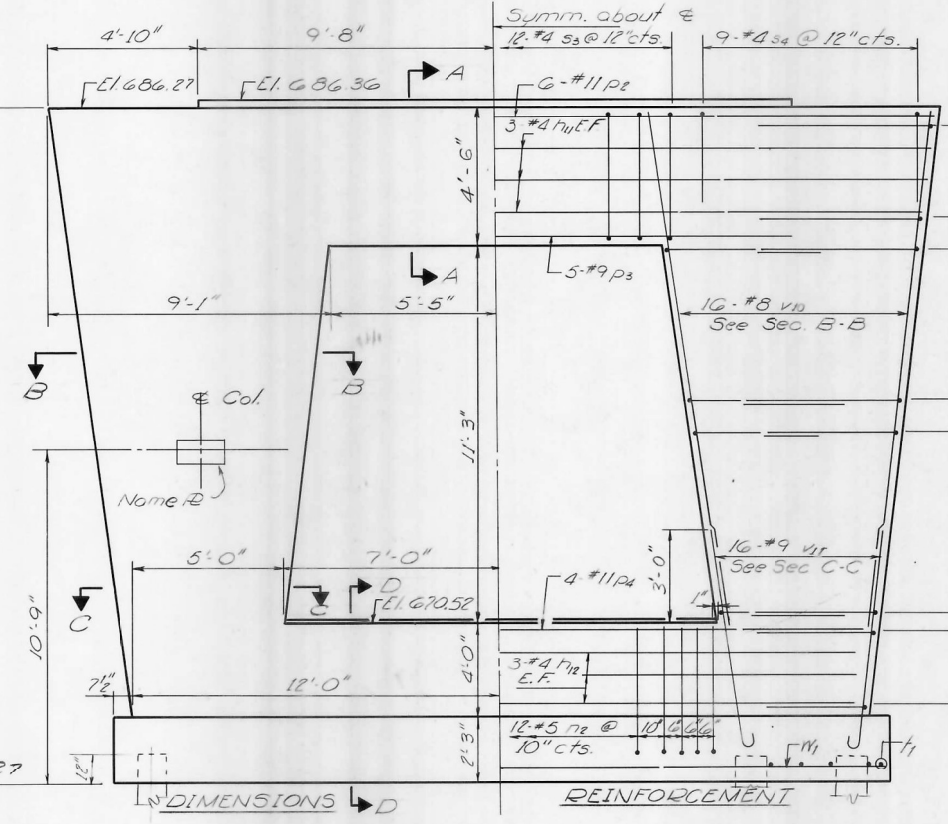
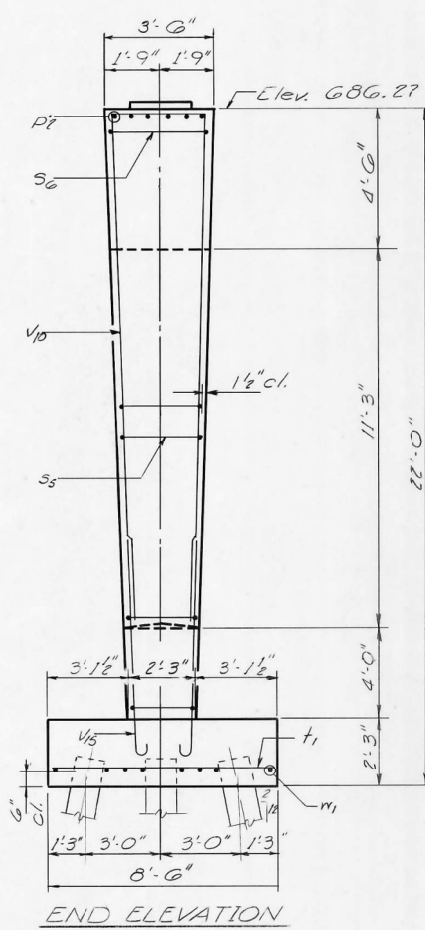
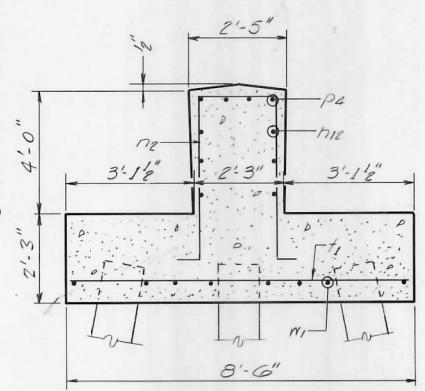
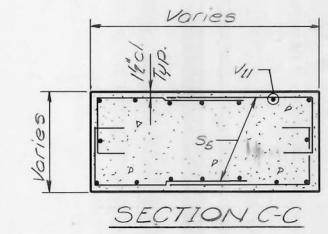
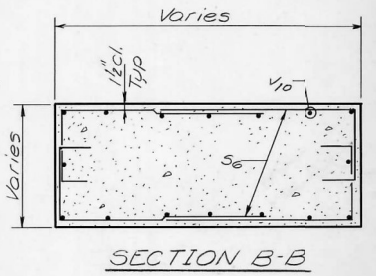
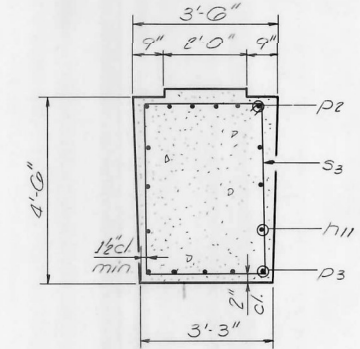
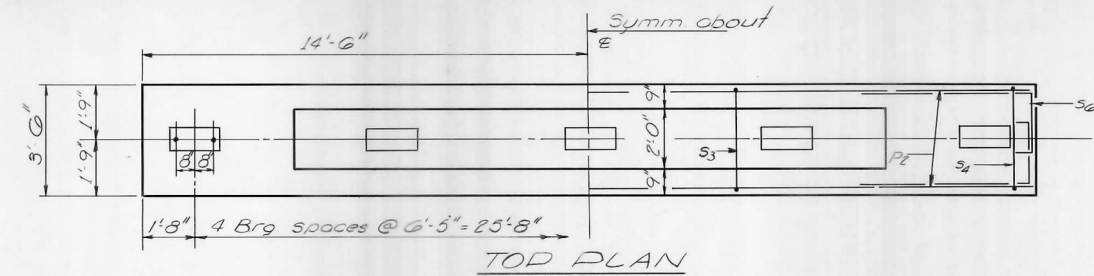
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	89
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F. A. I.				11 SHEETS
F. A.				
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

Note
Space reinforcement in cap to miss anchor bolts.
Min bar laps = 20 dia. unless otherwise noted.
All edges shall have standard 3/4" chamfers except as noted.
Pour steps monolithically with cap.

FOR INFORMATION ONLY



BILL OF MATERIAL

Bar	Nb	Size	Length	Shape
n11	6	#4	28'-0"	—
n12	6	#4	23'-9"	—
n2	20	#5	13'-6"	L
p2	6	#11	28'-9"	—
p3	5	#9	22'-6"	—
p4	4	#11	24'-6"	—
s3	12	#4	15'-3"	□
s4	18	#4	9'-0"	□
s5	64	#5	6'-8"	□
s6	64	#5	7'-10"	□
t1	25	#9	8'-9"	—
n10	32	#8	15'-6"	—
n11	32	#9	9'-6"	—
n1	8	#8	25'-0"	—

Class X Concrete	Cu. Yds	62.0
Reinforcement Bars	Lbs	7100
Creosoted Piles	Lin. Ft.	322
Test Piles	Timber	Eq.

PILE DATA

Type	Creosoted
Capacity	23.5 Tons
Est. Length	14 ft.
No. Required	23
Test Piles	1

DESIGNED	W. W. W.	EXAMINED	19
CHECKED		PASSED	ENGINEER OF BRIDGE AND TRAFFIC STRUCTURES
DRAWN	J. SCHNELLER	APPROVED	ENGINEER OF DESIGN
CHECKED			CHIEF HIGHWAY ENGINEER

REVISED - PIER
F.A.I. RT 57 SEC. 38-7HB
IROQUOIS COUNTY
STA 1047+43 CG

MODEL: 440DFEIMMITS
FILE: 440df_eimmts_wor\caldern\calderon\05215370\366h63-dht-bridge_repair.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

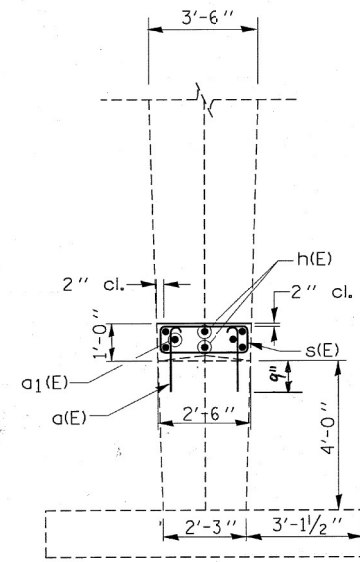
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FOR INFORMATION ONLY
SN 038-0132 - ORIGINAL CONSTRUCTION (1968)

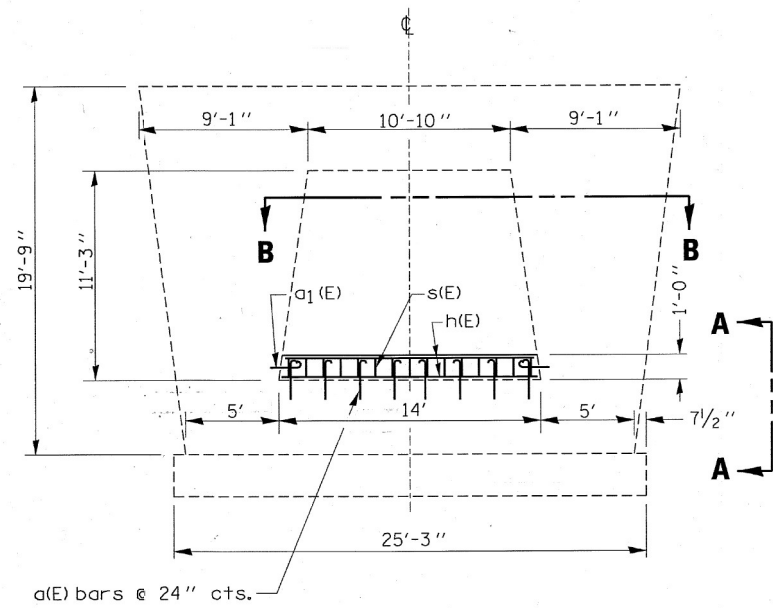
SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	90
			CONTRACT NO. 66H63	
		ILLINOIS	FED. AID PROJECT	

FOR INFORMATION ONLY



SEC. A-A

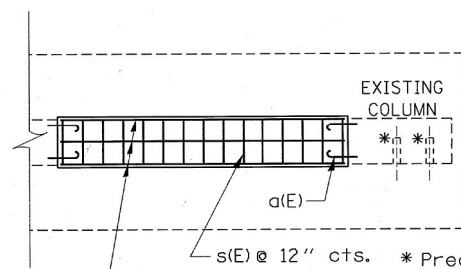


a (E) and a₁ (E) bars should be epoxy grouted in accordance with Section 584 of the Standard Specifications. Cost included in the cost of Reinforcement Bars, Epoxy Coated.

(QUANTITIES ARE FOR ONE PIER ONLY)

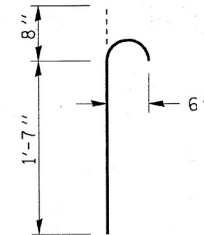
BILL OF MATERIALS				
BAR	NO.	SIZE	LENGTH	SHAPE
a(E)	18	#6	2'-3"	
h(E)	6	#4	13'-8"	
s(E)	14	#4	6'-6"	
CONCRETE STRUCTURE				3.3 CU YDS
REINFORCEMENT BARS, EPOXY COATED				175 LBS

FORMED CONCRETE INSERTS TO BE USED TO ATTACH GUARDRAIL TO NEW CONCRETE ARE INCLUDED IN THE COST OF CONCRETE STRUCTURE.

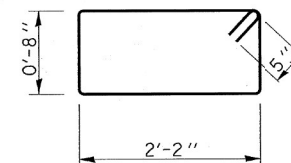


SEC. B-B

* Predrill 1" Ø Hole Into Pier In Order To Attach Type 6 Guardrail (See Standard). No Holes Shall Be Drilled Into New Concrete.



Bar a(E)



Bar s(E)

DESIGNED	
CHECKED	
DRAWN	
CHECKED	

EXAMINED	19
PASSED	ENGINEER OF BRIDGE DESIGN
	ENGINEER OF BRIDGES AND STRUCTURES

EXISTING STRUCTURE: SN 038-0126 IS A 4-SPAN BRIDGE CONSISTING ON CONTINUOUS 4-SPAN COMPOSITE WF GIRDERS UNITS ON STUB ABUTMENTS AND CAP AND OPEN COLUMN PIERS. ABUTMENTS ARE SUPPORTED BY CONCRETE PILES. THE STRUCTURE WAS CONSTRUCTED IN 1968 AS SECTION 38-7HB-1. IN 1994, CRASH WALLS AT PIER 2 WERE INCREASED WITH ADDITIONAL 2-FEET OF CONCRETE. IN 2000, CRASH WALLS AT ALL PIERS WERE INCREASED WITH AN ADDITIONAL OF 1-FOOT OF CONCRETE AND BRIDGE APPROACHES WERE REPAIRED. IN 2012 AND 2016, CONCRETE SEALER WAS APPLIED TO THE DECK. IN 2013, THE BRIDGE STRUCTURAL STEEL BEAMS WERE PAINTED. TRAFFIC WILL BE MAINTAINED WITH TEMPORARY BARRIER WALLS.

SCOPE OF WORK

1. SET UP TRAFFIC CONTROL USING TEMPORARY BARRIER WALL AT EACH WORK AREA.
2. INSTALL CRASH WALL EXTENSION SO TOTAL HEIGHT IS 5 FEET ABOVE FINISHED GRADE.
3. REPAIR CONCRETE AREA AT NB PIER.
4. AFTER CONCRETE CURING, APPLY PROTECTIVE COAT TO EXPOSED AREAS.
5. RESTORE SURROUNDING WORK AREA.

NOTES

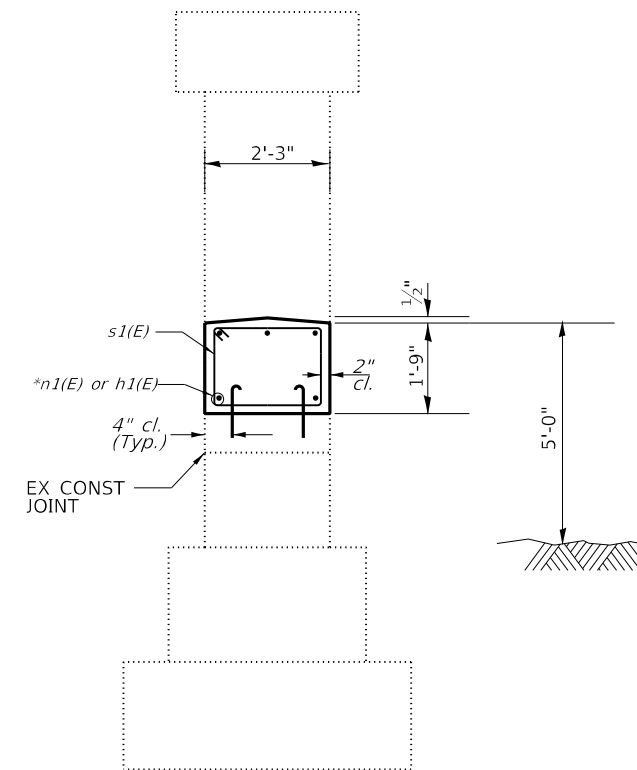
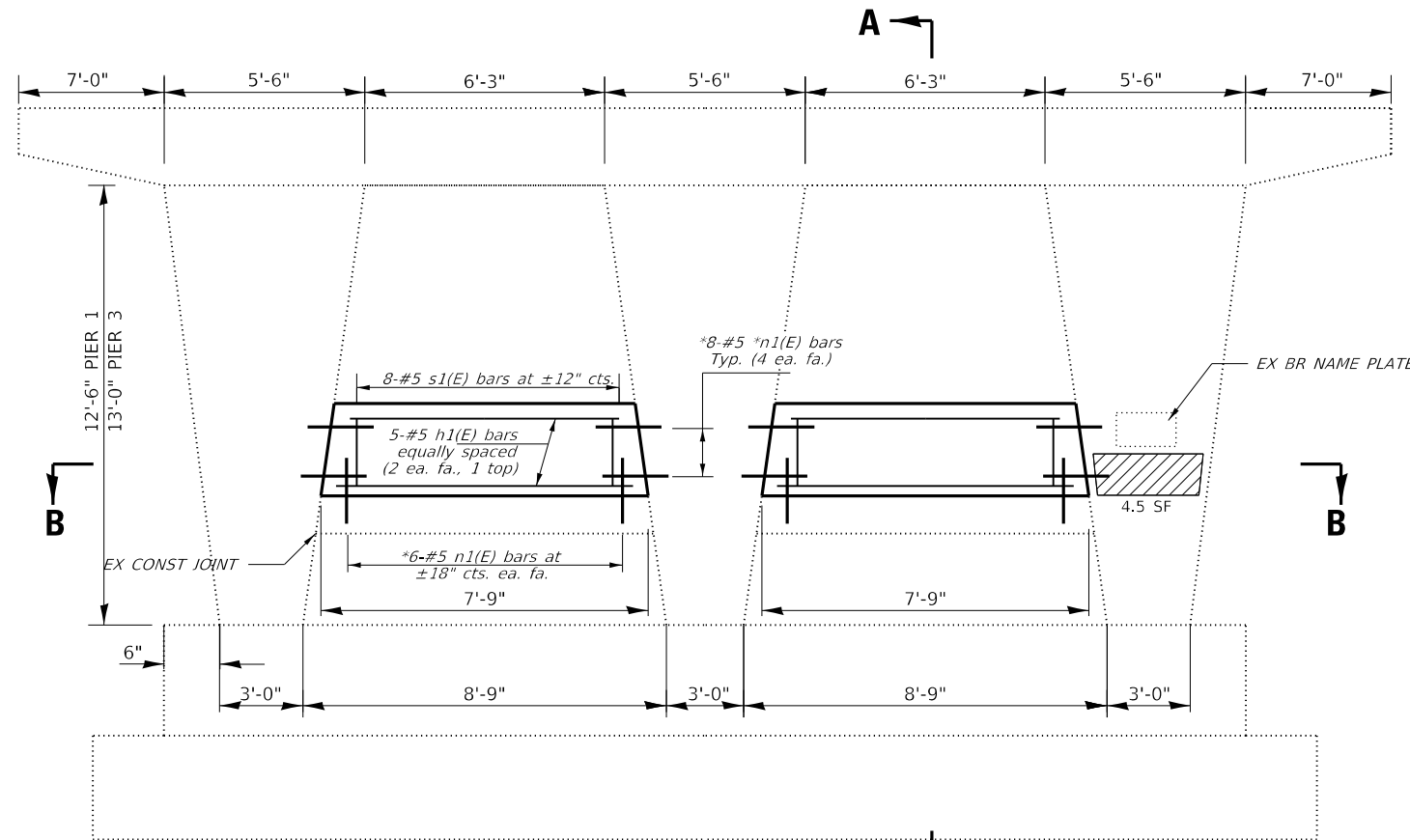
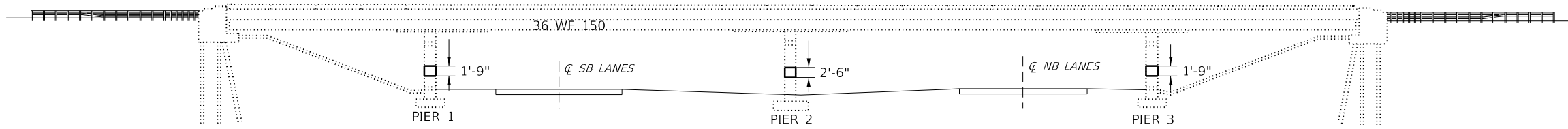
THE COST OF EPOXY GROUTING THREADED RODS SHALL BE INCLUDED WITH REINFORCEMENT BARS, EPOXY COATED.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

USE EXISTING PLANS INCLUDED TO AVOID DAMAGING LONGITUDINAL REINFORCEMENT IN THE EXISTING CRASHWALL WHILE DRILLING & EPOXY GROUTING n1(E) OR n2(E) BARS.

* Epoxy grout n1(E) or n2(E) bars in 9" min. holes according to Article 584 of the Standard Specifications



TOTAL BILL OF MATERIAL (STR 038-0126)

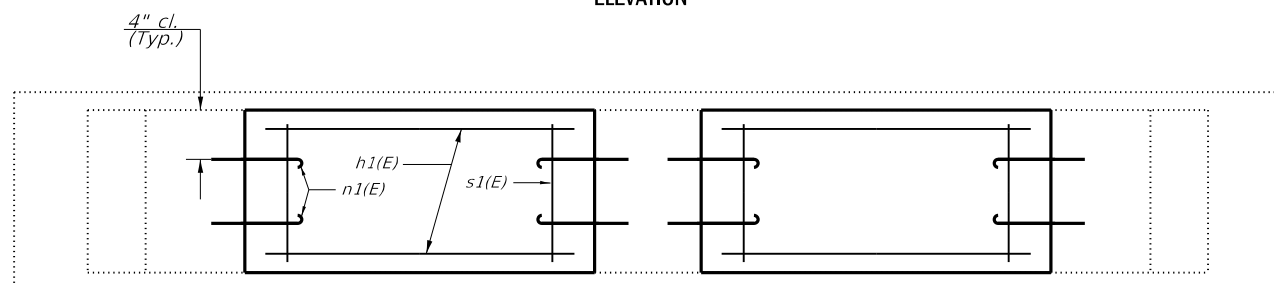
ITEM	UNIT	TOTAL
Concrete Structures	Cu. Yd.	7.8
Reinforcement Bars, Epoxy Coated	Pound.	924
Protective Coat	Sq. Yd.	31.9
Structural Repair of Concrete (Depth Equal or Less than 5 inches)	Sq. Ft.	4.5

Name Plate removal and reinstallation (if required) shall be included in the cost of Structural Repair of concrete.

CRASHWALL EXTENSION AT PIER 1 & 3

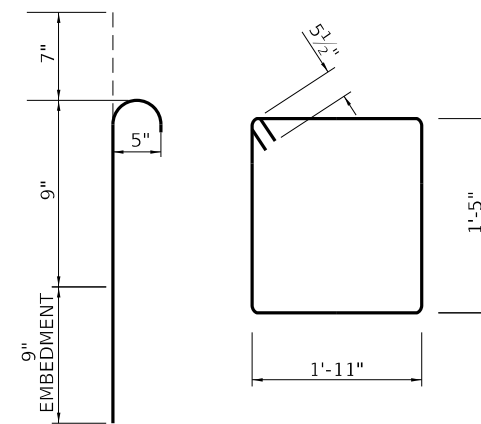
ELEVATION

STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL OR LESS THAN 5")



SEC. B-B

SEC. A-A



BAR n1(E)

BAR s1(E)

BILL OF MATERIAL (PIER 1 & 3)

Bar	No	Size	Length	Shape
h1(E)	20	#5	7'-2"	—
n1(E)	80	#5	2'-1"	U
s1(E)	32	#5	7'-7"	□
Concrete Structures			Cu. Yd.	4.4
Reinforcement Bars, Epoxy Coated			Pound.	577
Protective Coat			Sq. Yd.	19.2



Expires 11-30-2022

David Carl Pizzei

02/07/2022

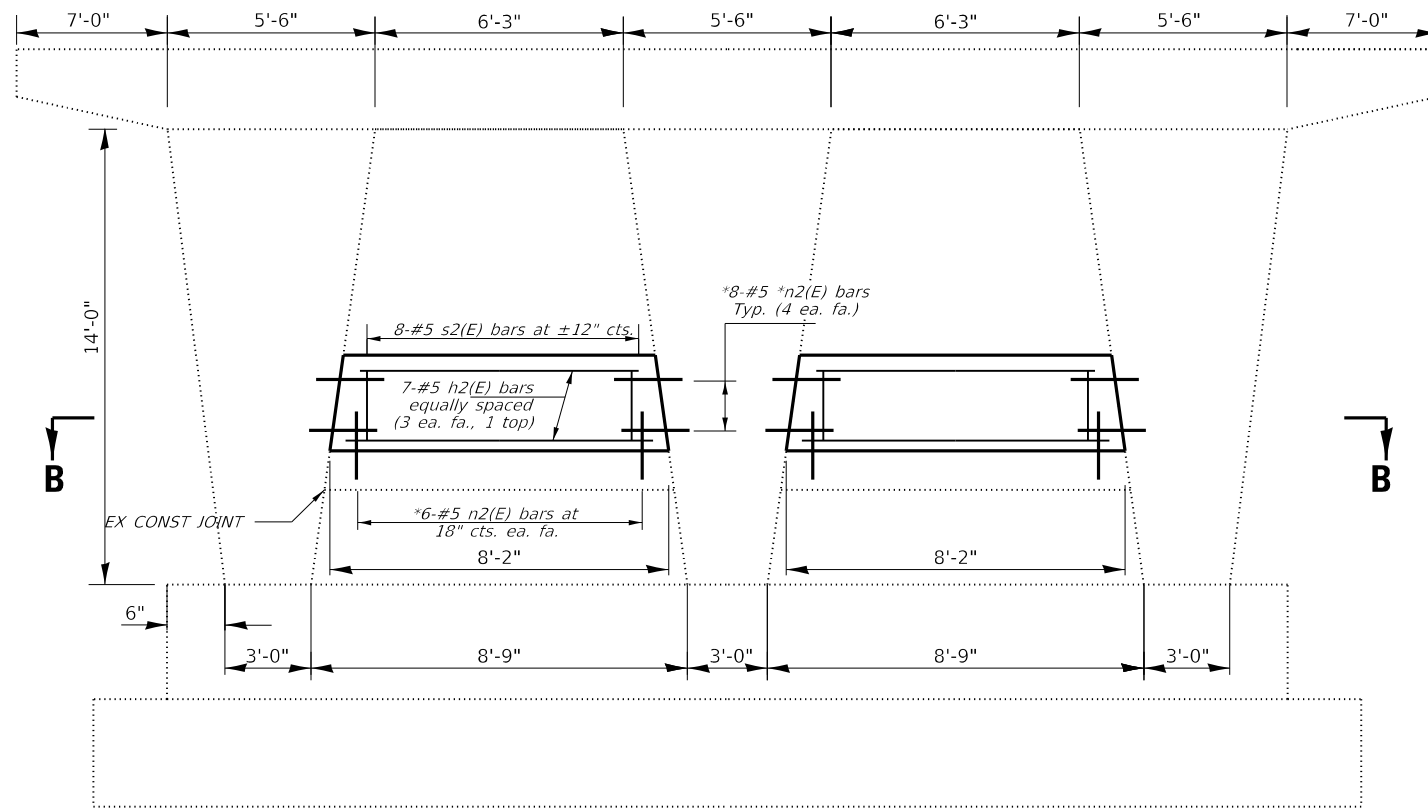
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER CRASHWALL EXTENSION
SN 038-0126

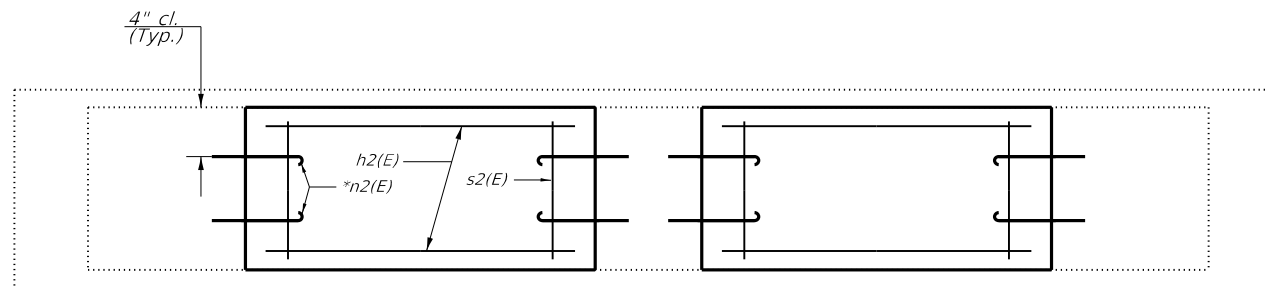
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	92
			CONTRACT NO. 66H63	
		ILLINOIS FED. AID PROJECT		

SCALE: SHEET 1 OF 6 SHEETS STA. TO STA.

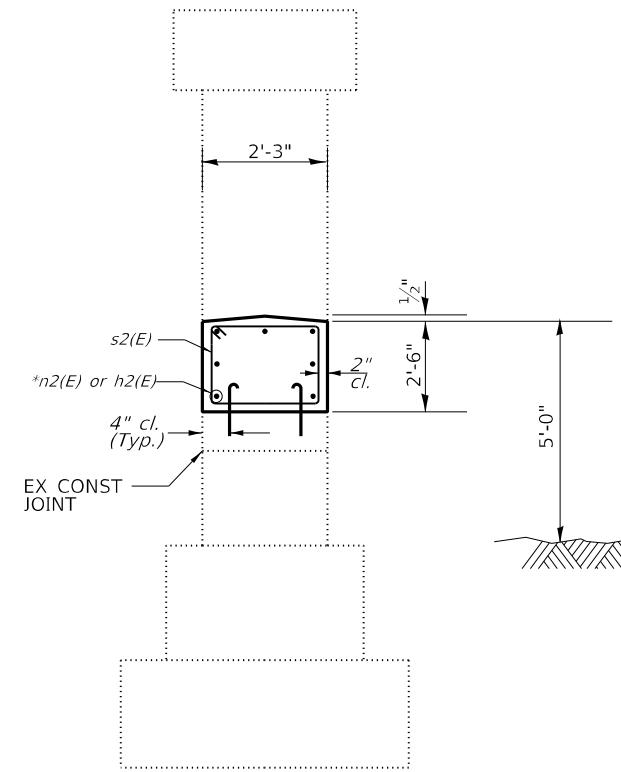
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FILE: MAR15_CrashWall\crashwall\0521537D\3666163-shr-brIDGE_Repair.dgn



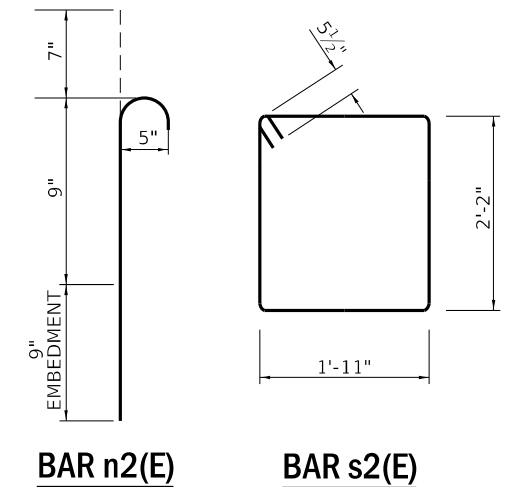
CRASHWALL EXTENSION AT PIER 2
ELEVATION



SEC. B-B



SEC. A-A



BAR n2(E)

BAR s2(E)

BILL OF MATERIAL (PIER 2)

Bar	No	Size	Length	Shape
h2(E)	14	#5	7'-5"	—
n2(E)	40	#5	2'-1"	U
s2(E)	16	#5	9'-1"	□
Concrete Structures			Cu. Yd.	3.4
Reinforcement Bars, Epoxy Coated			Pound.	347
Protective Coat			Sq. Yd.	12.7

MODEL: \\MODELM\A\MS
 FILE: \\MODELM\A\MS\work\bridge\calderon\05215370\66H63-shr-bridge_Repair.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER CRASHWALL EXTENSION
SN 038-0126

SCALE: SHEET 2 OF 6 SHEETS STA. TO STA.

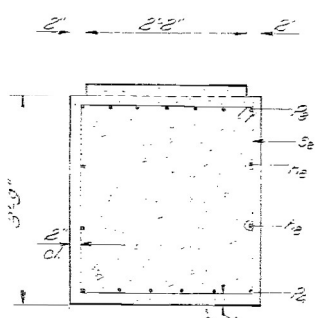
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	93
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY

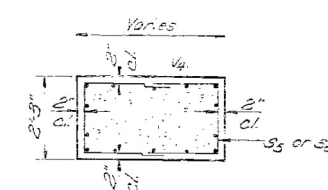
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	BLK.	TOTAL SHEETS	SHEET NO.	SHEET NO.
157	38-7	28-1	24	12	11
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT	

Note:
Space reinforcement in cap to miss anchor bolts.
Min. bar laps 20 dia. unless otherwise noted.
All edges shall have standard 3/4 chamfers except as noted.
Four steps monolithically with cap.



SECTION A-A



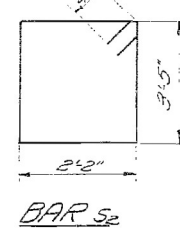
SECTION B-B

PIERS 1 & 3
BILL OF MATERIALS

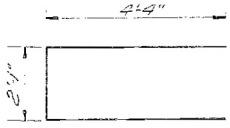
Bar	No.	Size	Length	Volume
S2	16	#5	22'-0"	
S3	24	#5	25'-0"	
S4	34	#8	7'-0"	
S5	14	#1	22'-0"	
S6	12	#5	23'-6"	
S7	8	#5	7'-6"	
S8	58	#4	11'-11"	
S9	108	#5	7'-6"	
S10	28	#5	6'-2"	
S11	36	#5	3'-11"	
S12	60	#5	7'-5"	
S13	23	#5	13'-6"	
S14	20	#5	10'-5"	
S15	24	#5	2'-9"	
S16	24	#5	2'-9"	
S17	24	#5	2'-9"	
S18	24	#5	2'-9"	
S19	24	#5	2'-9"	
S20	24	#5	2'-9"	
S21	24	#5	2'-9"	
S22	24	#5	2'-9"	
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S26	24	#5	2'-9"	
S27	24	#5	2'-9"	
S28	24	#5	2'-9"	
S29	24	#5	2'-9"	
S30	24	#5	2'-9"	
S31	24	#5	2'-9"	
S32	24	#5	2'-9"	
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S37	24	#5	2'-9"	
S38	24	#5	2'-9"	
S39	24	#5	2'-9"	
S40	24	#5	2'-9"	
S41	24	#5	2'-9"	
S42	24	#5	2'-9"	
S43	24	#5	2'-9"	
S44	24	#5	2'-9"	
S45	24	#5	2'-9"	
S46	24	#5	2'-9"	
S47	24	#5	2'-9"	
S48	24	#5	2'-9"	
S49	24	#5	2'-9"	
S50	24	#5	2'-9"	
S51	24	#5	2'-9"	
S52	24	#5	2'-9"	
S53	24	#5	2'-9"	
S54	24	#5	2'-9"	
S55	24	#5	2'-9"	
S56	24	#5	2'-9"	
S57	24	#5	2'-9"	
S58	24	#5	2'-9"	
S59	24	#5	2'-9"	
S60	24	#5	2'-9"	
S61	24	#5	2'-9"	
S62	24	#5	2'-9"	
S63	24	#5	2'-9"	
S64	24	#5	2'-9"	
S65	24	#5	2'-9"	
S66	24	#5	2'-9"	
S67	24	#5	2'-9"	
S68	24	#5	2'-9"	
S69	24	#5	2'-9"	
S70	24	#5	2'-9"	
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S73	24	#5	2'-9"	
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S75	24	#5	2'-9"	
S76	24	#5	2'-9"	
S77	24	#5	2'-9"	
S78	24	#5	2'-9"	
S79	24	#5	2'-9"	
S80	24	#5	2'-9"	
S81	24	#5	2'-9"	
S82	24	#5	2'-9"	
S83	24	#5	2'-9"	
S84	24	#5	2'-9"	
S85	24	#5	2'-9"	
S86	24	#5	2'-9"	
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S88	24	#5	2'-9"	
S89	24	#5	2'-9"	
S90	24	#5	2'-9"	
S91	24	#5	2'-9"	
S92	24	#5	2'-9"	
S93	24	#5	2'-9"	
S94	24	#5	2'-9"	
S95	24	#5	2'-9"	
S96	24	#5	2'-9"	
S97	24	#5	2'-9"	
S98	24	#5	2'-9"	
S99	24	#5	2'-9"	
S100	24	#5	2'-9"	

A & B DIMENSIONS

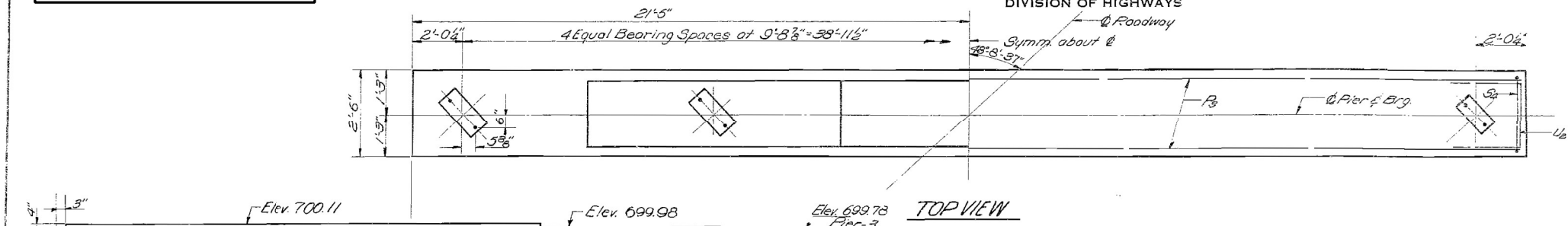
Bar	A	B
S2	2'-2"	2'-2"
S3	2'-2"	2'-0"
S4	1'-11"	3'-6"
S5	2'-2"	5'-8"
S6	2'-2"	2'-2"



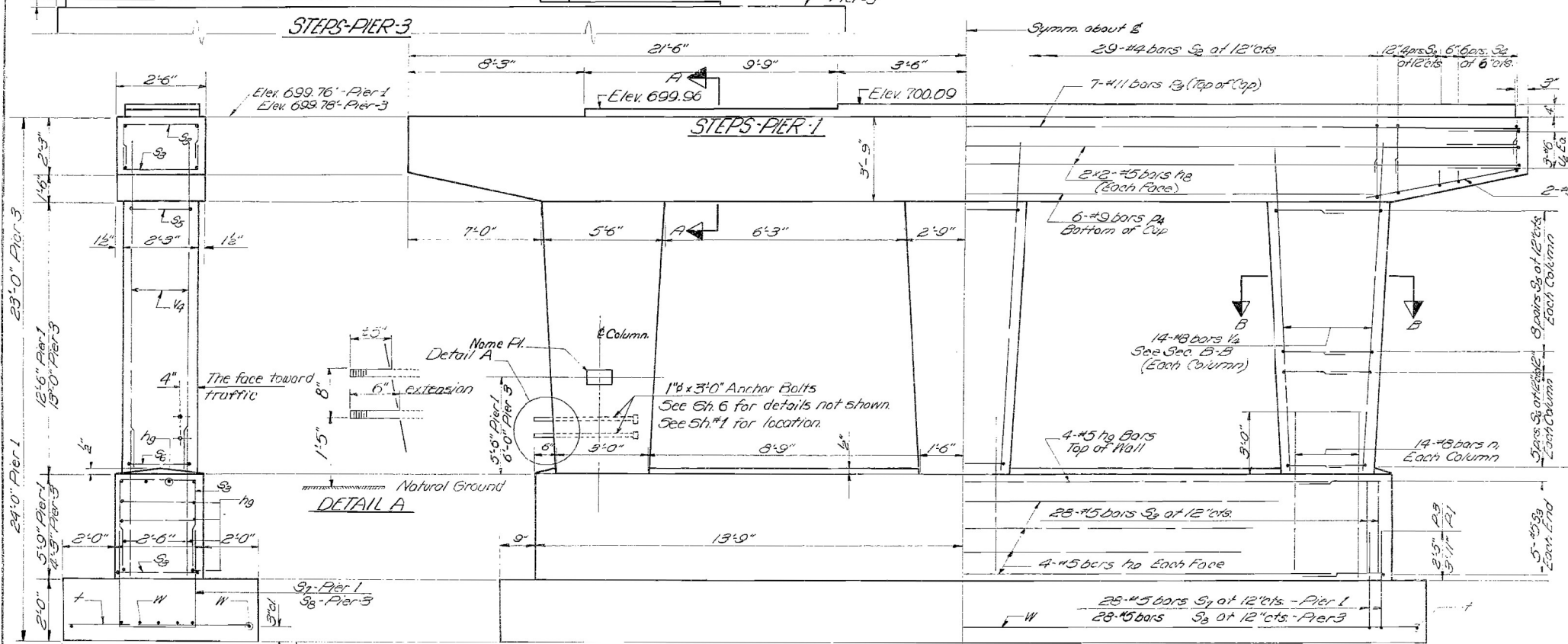
BAR S2



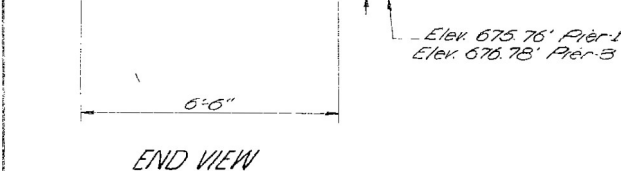
BAR S3



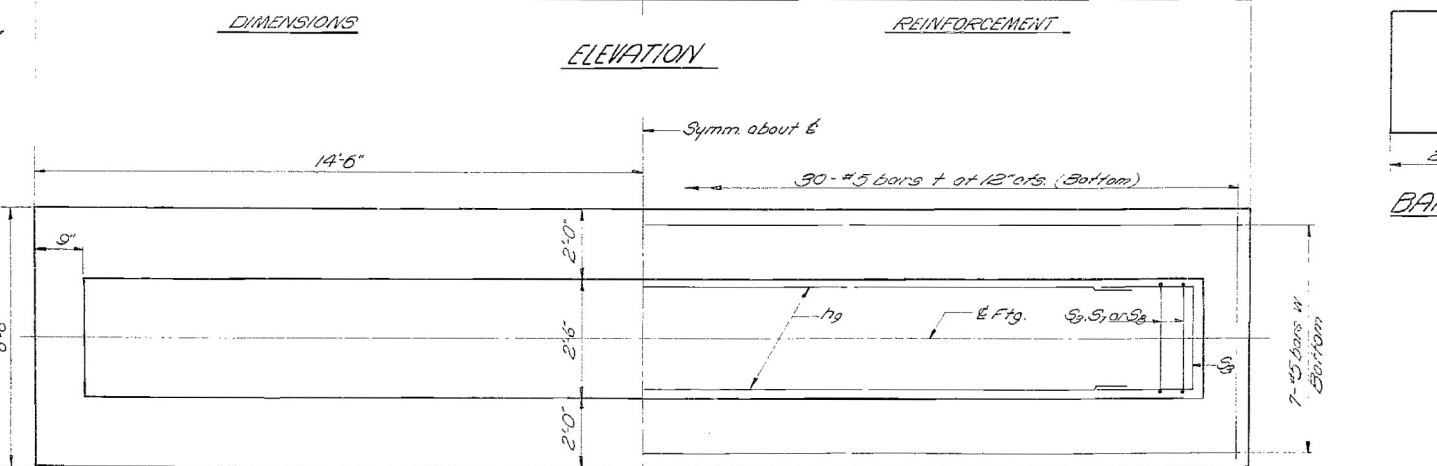
TOP VIEW



ELEVATION



END VIEW



FOOTING PLAN

DESIGNED	I. Kaspar	EXAMINED	[Signature]
CHECKED	[Signature]	PAUSED	[Signature]
DRAWN	D. Ferraro	APPROVED	[Signature]
CHECKED	[Signature]		

Max. Soil Pressure = 2.5 tons/sq ft

PIERS 1 & 3
F.A.I. RT. 57 SEC. 38-THE-1
IROQUOIS COUNTY
STA. 1176+26.51

Rev. 12-8-67: Rail anchorage. J.B.N.

MODEL: SHODEL.MARITS
FILE: MARITS_CAD\work\pds\calderon\05215370366\63-dh-bridge_Ropaslr.dgn

USER NAME = calderon	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ORIGINAL PIER 1 & 3 CONSTRUCTION (1968)
SN 038-0126 (FOR INFORMATION ONLY)

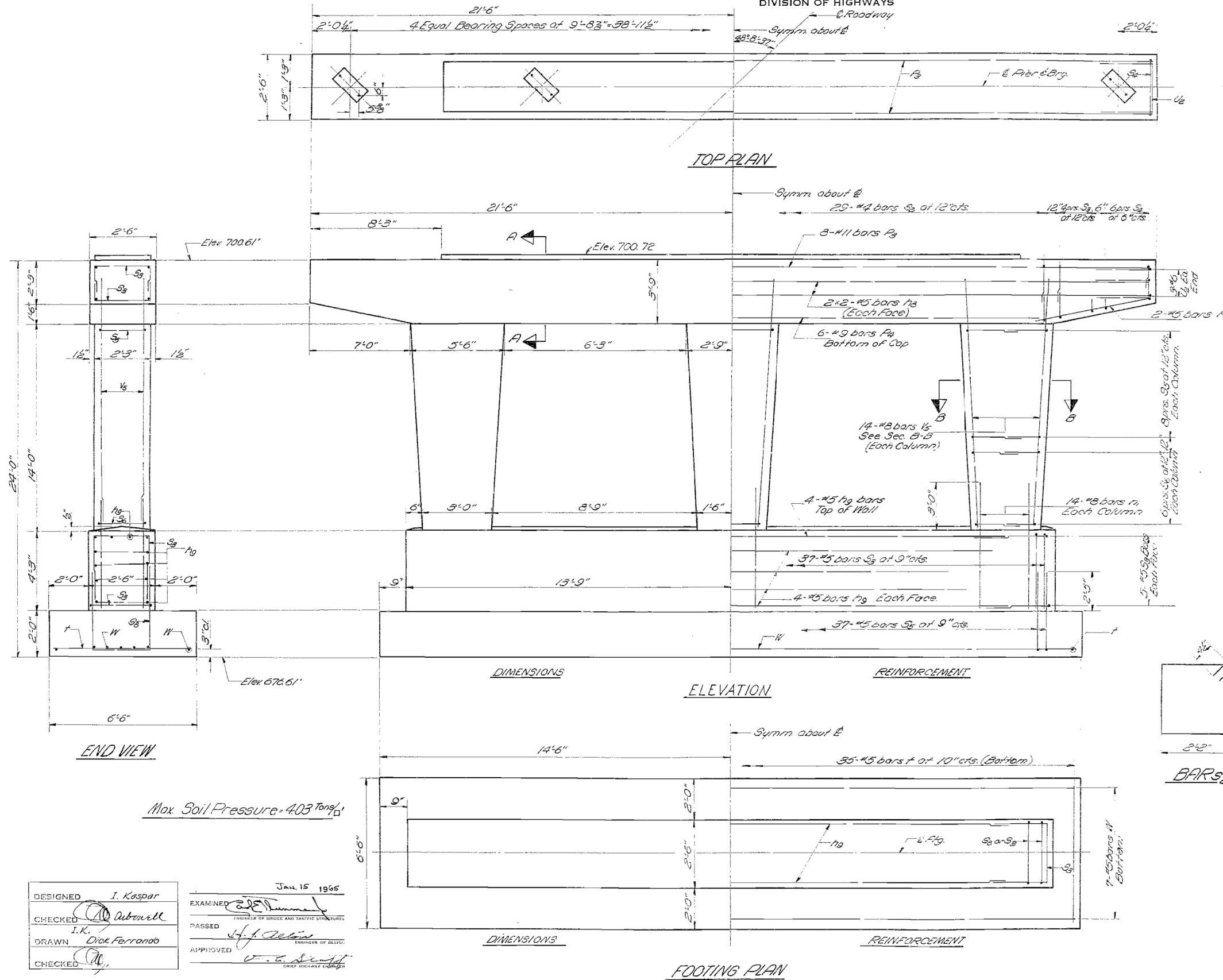
SCALE: SHEET 3 OF 6 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	94
ILLINOIS FED. AID PROJECT			CONTRACT NO. 66H63	

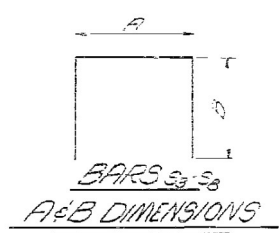
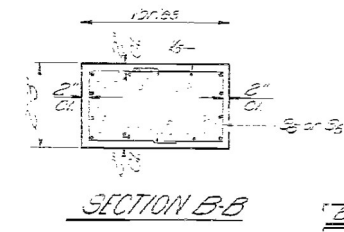
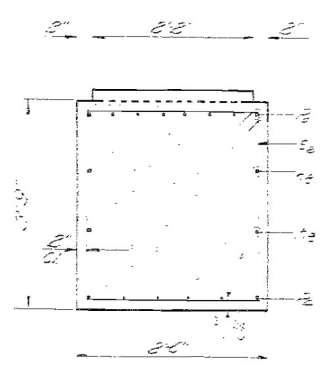
FOR INFORMATION ONLY

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	DATE	BY	SHEET NO.
37	1703000	24		11
TOTAL SHEETS 11				



Note:
Space reinforcement in cap to miss anchor bars.
Min. bar laps = 20 dia. unless otherwise noted.
All edges shall have standard chamfers except as noted.
Four steps monolithically with cap.



PIER 2
BILL OF MATERIALS

Bar	No.	Size	Length	Volume
12	8	#5	23'-0"	
13	12	#5	25'-0"	
14	42	#5	7'-0"	
15	3	#11	23'-0"	
16	6	#9	25'-0"	
17	4	#5	7'-0"	
S3	23	#5	14'-0"	
S4	23	#5	7'-0"	
S5	24	#5	6'-0"	
S6	43	#5	3'-0"	
S7	36	#5	7'-0"	
S8	37	#5	10'-0"	
7	55	#5	6'-0"	
8	6	#5	10'-0"	
18	42	#5	16'-0"	
A	7	#5	25'-0"	

DESIGNED *I. Kaspar*
CHECKED *O. O'Brien*
DRAWN *D. Ferrando*
CHECKED *[Signature]*

EXAMINED *[Signature]*
PASSED *[Signature]*
APPROVED *[Signature]*

Jan. 15 1965

Max. Soil Pressure = 4.03 tons/sq ft

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ORIGINAL PIER 2 CONSTRUCTION (1968)
SN 038-0126 (FOR INFORMATION ONLY)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	95
CONTRACT NO. 66H63			ILLINOIS FED. AID PROJECT	

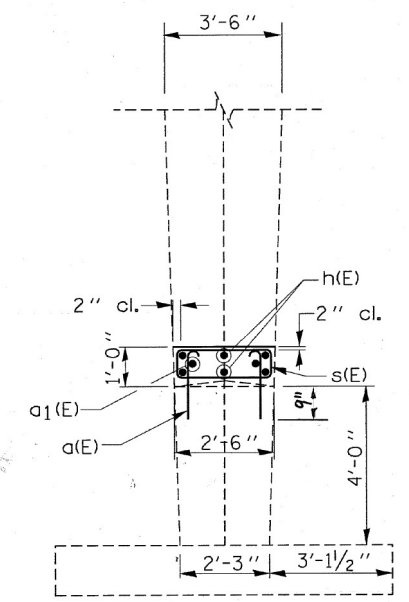
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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

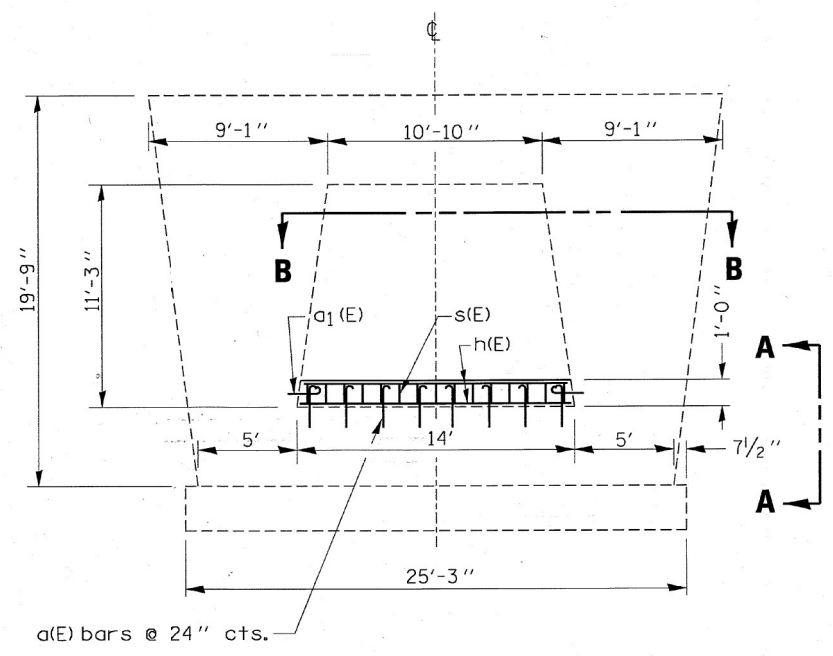
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
S.R.L.	F.A. 57	* IROQUOIS	98 61	2 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	38-(5,6,7)RS&I	

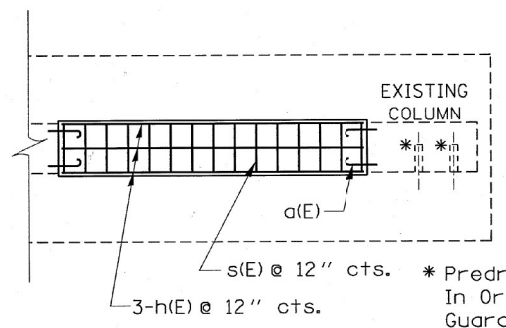
FOR INFORMATION ONLY



SEC. A-A

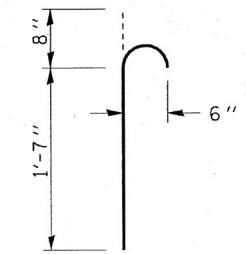


a (E) and a₁ (E) bars should be epoxy grouted in accordance with Section 584 of the Standard Specifications. Cost included in the cost of Reinforcement Bars, Epoxy Coated.

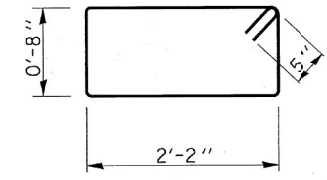


SEC. B-B

* Predrill 1" Ø Hole Into Pier In Order To Attach Type 6 Guardrail (See Standard). No Holes Shall Be Drilled Into New Concrete.



Bar a(E)



Bar s(E)

(QUANTITIES ARE FOR ONE PIER ONLY)

BILL OF MATERIALS				
BAR	NO.	SIZE	LENGTH	SHAPE
a(E)	18	#6	2'-3"	
h(E)	6	#4	13'-8"	
s(E)	14	#4	6'-6"	
CONCRETE STRUCTURE				3.3 CU YDS
REINFORCEMENT BARS, EPOXY COATED				175 LBS

FORMED CONCRETE INSERTS TO BE USED TO ATTACH GUARDRAIL TO NEW CONCRETE ARE INCLUDED IN THE COST OF CONCRETE STRUCTURE.

DESIGNED	19
CHECKED	EXAMINED
DRAWN	PASSED
CHECKED	

S.N. 038-0132
STA. 1047 + 43.26
CRASH WALL DETAIL

MODEL: I:\MODELS\MAINTS FILE: Maints - Copy - work\pilot\calderoni\05215370\366h63-cht-bridge_Repair.dgn

EXISTING STRUCTURE: SN 038-0076 IS A 4-SPAN BRIDGE CONSISTING ON CONTINUOUS 4-SPAN COMPOSITE WF GIRDERS UNITS ON STUB ABUTMENTS AND CAP AND OPEN COLUMN PIERS. ABUTMENTS ARE SUPPORTED BY CONCRETE PILES. THE STRUCTURE WAS CONSTRUCTED IN 1967 AS SECTION 38-7HB-2. IN 1994, CRASH WALLS AT PIER 2 WERE INCREASED WITH ADDITIONAL 2-FEET OF CONCRETE. IN 2000, CRASH WALLS AT ALL PIERS WERE INCREASED WITH AN ADDITIONAL OF 1.5 FOOT OF CONCRETE AND SEVERAL BRIDGE DECK REPAIRS WERE PERFORMED. IN 2002, THE BRIDGE STRUCTURAL STEEL BEAMS WERE PAINTED. TRAFFIC WILL BE MAINTAINED WITH TEMPORARY BARRIER WALLS.

SCOPE OF WORK

1. SET UP TRAFFIC CONTROL USING TEMPORARY BARRIER WALL AT EACH WORK AREA.
2. INSTALL CRASH WALL EXTENSION SO TOTAL HEIGHT IS 5 FEET ABOVE FINISHED GRADE.
3. REPAIR CONCRETE AREA AT PIER 1 AND PIER 3.
4. AFTER CONCRETE CURING, APPLY PROTECTIVE COAT TO EXPOSED AREAS.
5. RESTORE SURROUNDING WORK AREA.

NOTES

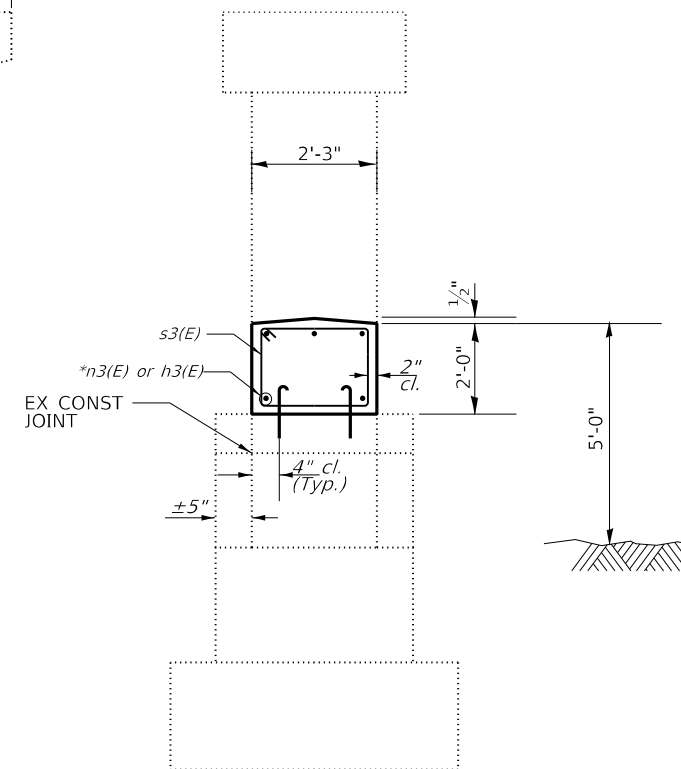
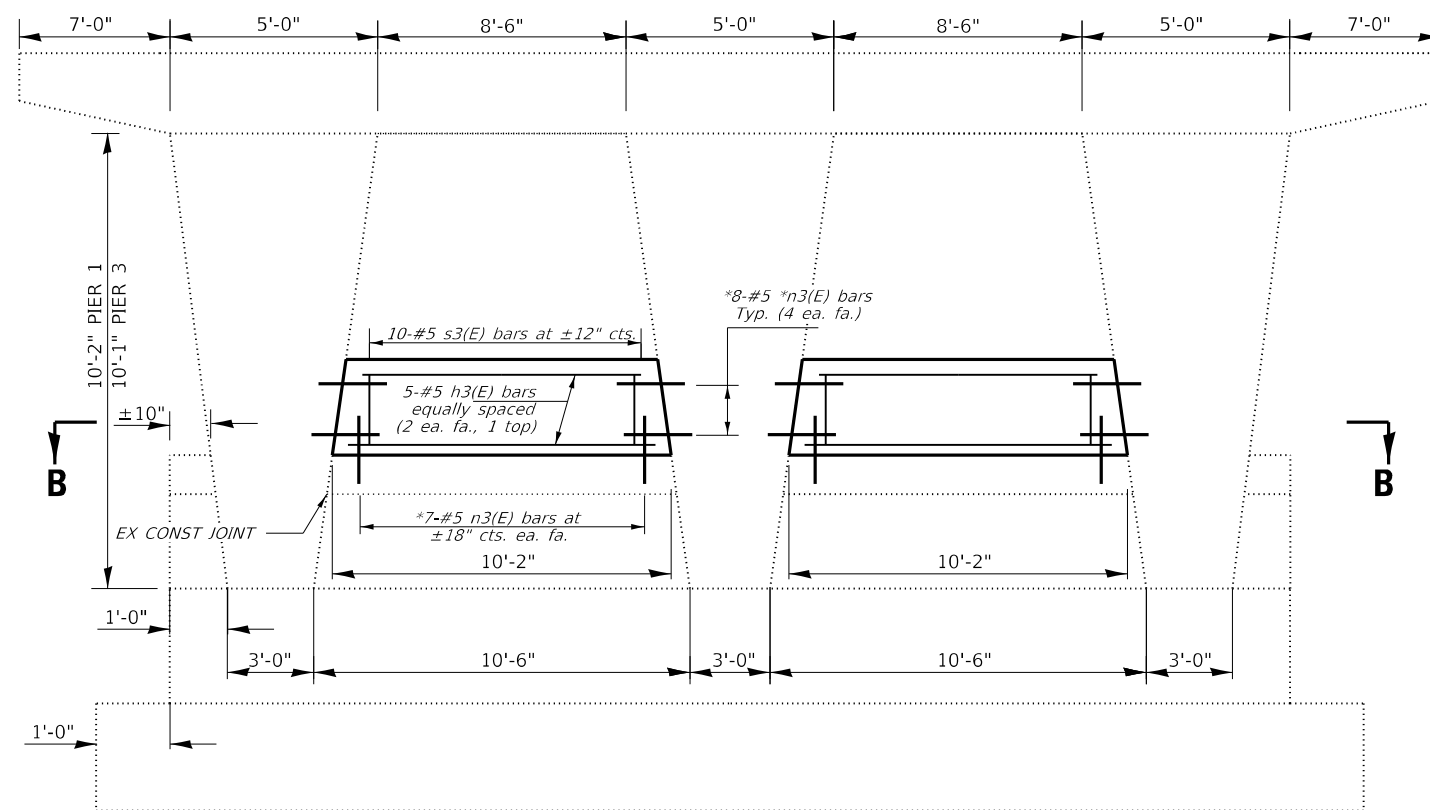
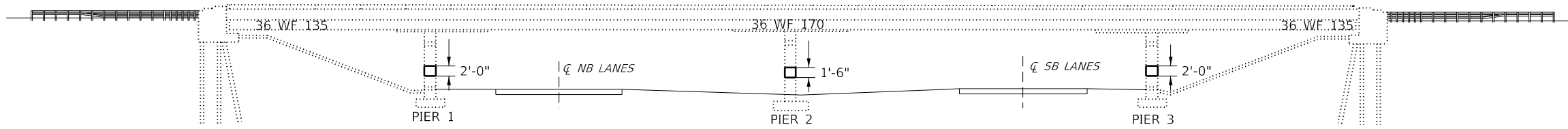
THE COST OF EPOXY GROUTING THREADED RODS SHALL BE INCLUDED WITH REINFORCEMENT BARS, EPOXY COATED.

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

USE EXISTING PLANS INCLUDED TO AVOID DAMAGING LONGITUDINAL REINFORCEMENT IN THE EXISTING CRASHWALL WHILE DRILLING & EXPOXY GROUTING n3(E) OR n4(E) BARS.

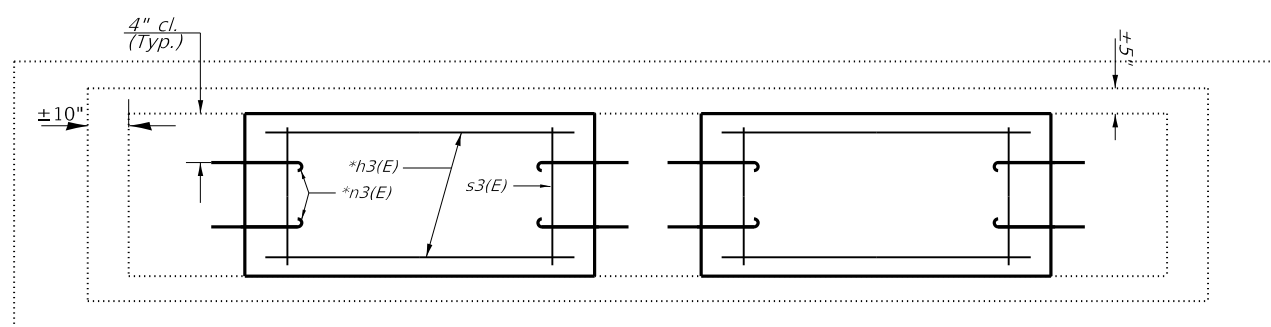
* Epoxy grout n3(E) or n4(E) bars in 9" min. holes according to Article 584 of the Standard Specifications



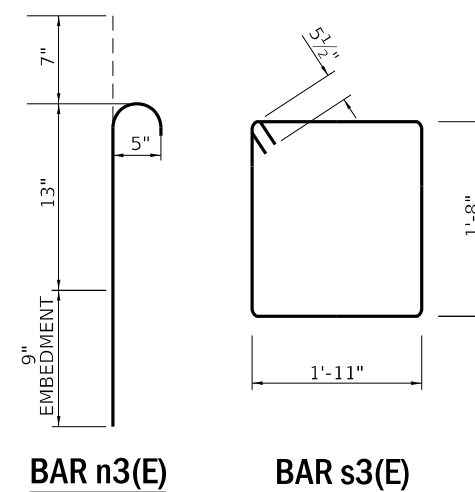
CRASHWALL EXTENSION AT PIER 1 & 3

ELEVATION

SEC. A-A



SEC. B-B



TOTAL BILL OF MATERIAL (STR 038-0076)

ITEM	UNIT	TOTAL
Concrete Structures	Cu. Yd.	9.4
Reinforcement Bars, Epoxy Coated	Pound.	1098
Protective Coat	Sq. Yd.	41.2
Structural Repair of Concrete (Depth Equal or Less than 5 inches)	Sq. Ft.	19.4

BILL OF MATERIAL (PIER 1 & 3)

Bar	No	Size	Length	Shape
h3(E)	20	#5	9'-5"	—
n3(E)	88	#5	2'-5"	⌋
s3(E)	40	#5	8'-1"	□
Concrete Structures			Cu. Yd.	6.8
Reinforcement Bars, Epoxy Coated			Pound.	756
Protective Coat			Sq. Yd.	27.6



Expires 11-30-2022
02/07/2022

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

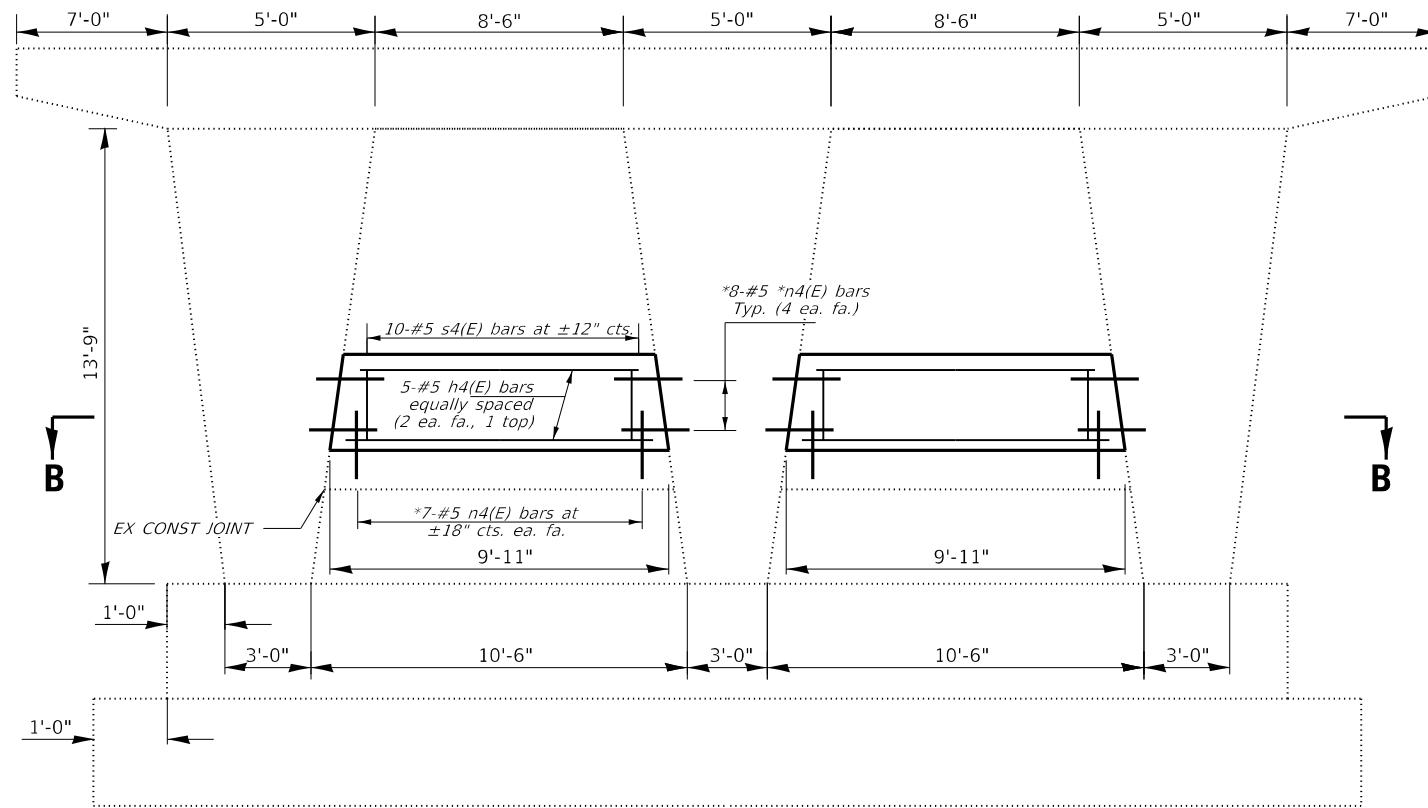
PIER CRASHWALL EXTENSION
SN 038-0076

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	98
			CONTRACT NO. 66H63	
			ILLINOIS FED. AID PROJECT	

SCALE: SHEET 1 OF 8 SHEETS STA. TO STA.

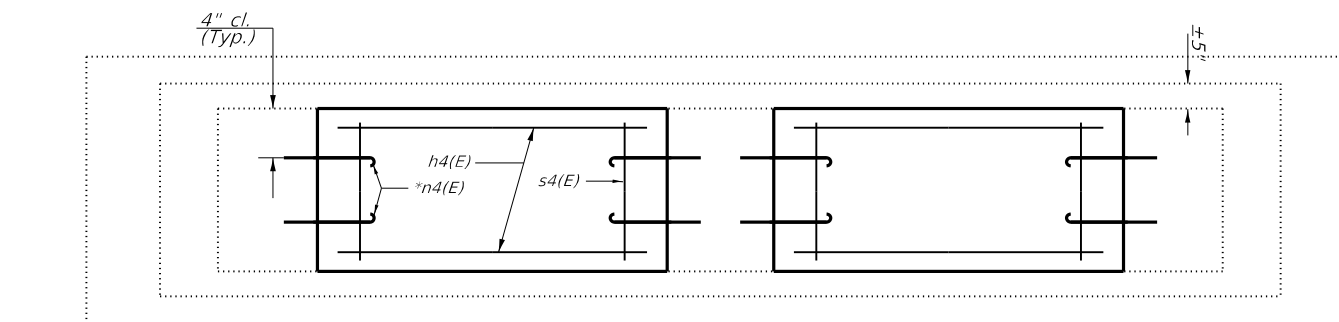
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USER NAME = calderon	DESIGNED - BDD	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED - SMR	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -



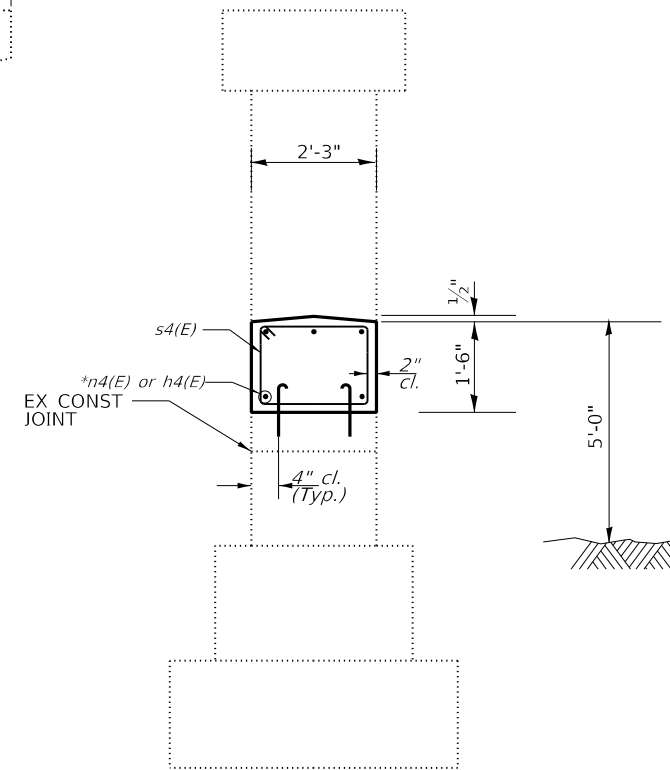
CRASHWALL EXTENSION AT PIER 2

ELEVATION

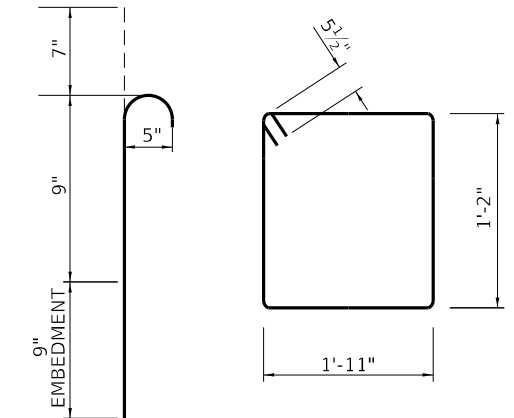


SEC. B-B

* Epoxy grout n3(E) or n4(E) bars in 9" min. holes according to Article 584 of the Standard Specifications



SEC. A-A



BAR n4(E)

BAR s4(E)

BILL OF MATERIAL (PIER 2)

Bar	No	Size	Length	Shape
h4(E)	10	#5	9'-5"	—
n4(E)	44	#5	2'-1"	U
s4(E)	20	#5	7'-1"	□
Concrete Structures			Cu. Yd.	2.6
Reinforcement Bars, Epoxy Coated			Pound.	342
Protective Coat			Sq. Yd.	13.6

MODEL: \\MODELM\A\MTS
FILE: \\MODELM\csm\work\side\calderon\05215370\66H63-shr-Bridge_Repair.dgn

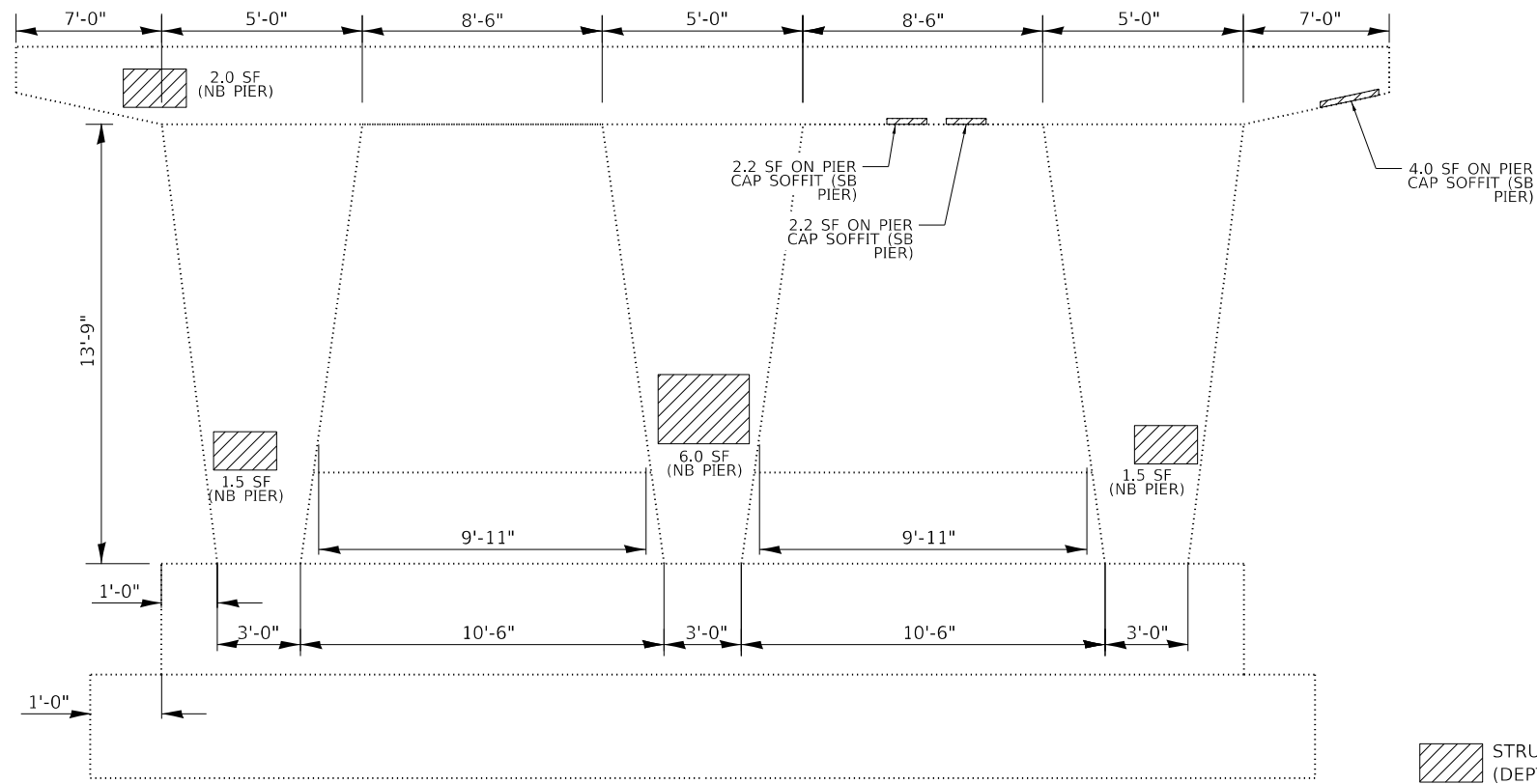
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -


**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PIER CRASHWALL EXTENSION
SN 038-0076**

SCALE: SHEET 2 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	99
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				



 STRUCTURAL REPAIR OF CONCRETE
(DEPTH EQUAL OR LESS THAN 5")

STRUCTURAL REPAIR OF CONCRETE - PIER 1 & 3

ELEVATION
FACING ROADWAY

MODEL: \\MODELS\MARIS
 FILE: \\MARIS_CAD\work\sidebat\calderon\05215370\66H63-ht-bridge_Repair.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

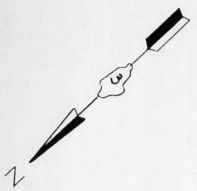
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL REPAIR OF CONCRETE
SN 038-0076**

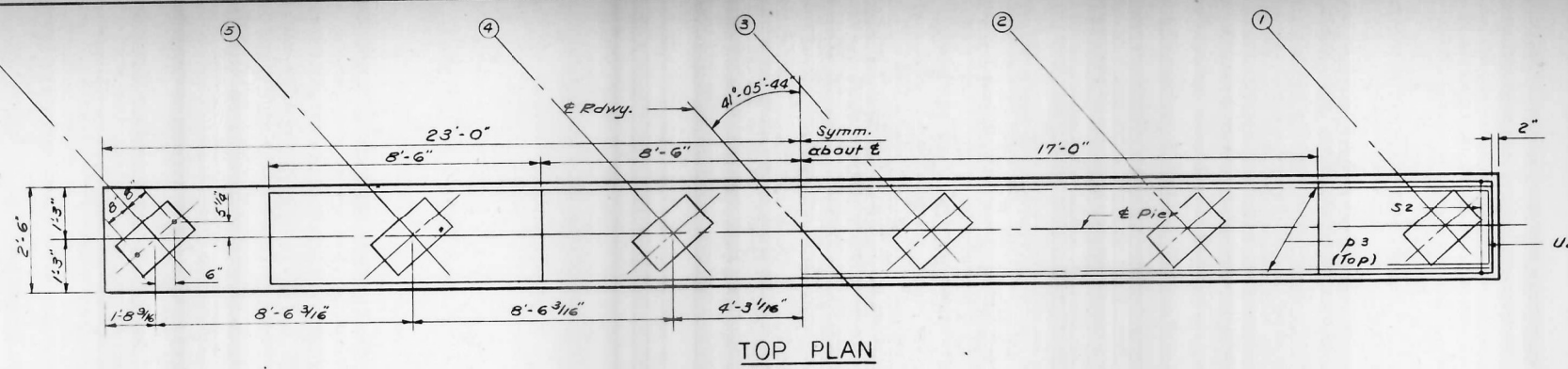
SCALE: SHEET 3 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	100
CONTRACT NO. 66H63			ILLINOIS FED. AID PROJECT	

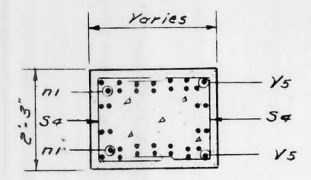
FOR INFORMATION ONLY



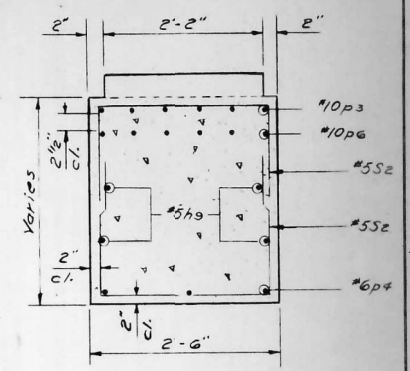
Pier #1
Sta. 29+09.92
Ct. El. 720.96



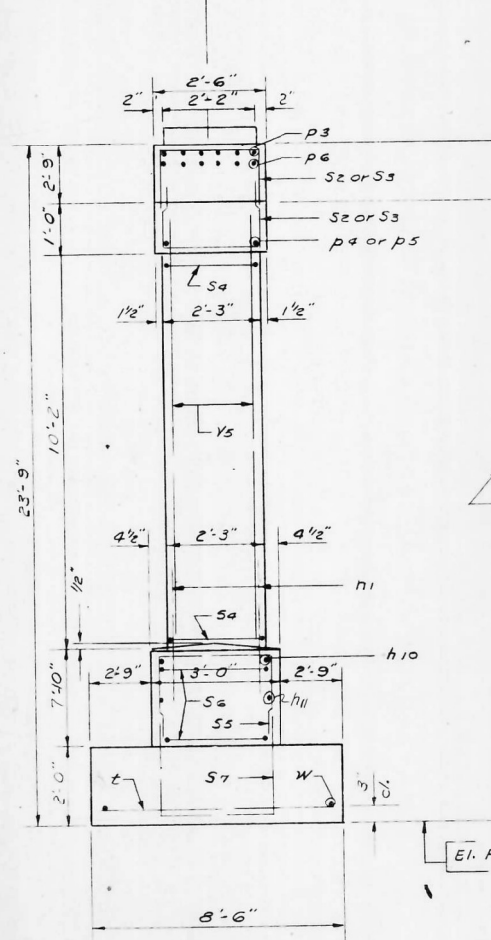
TOP PLAN



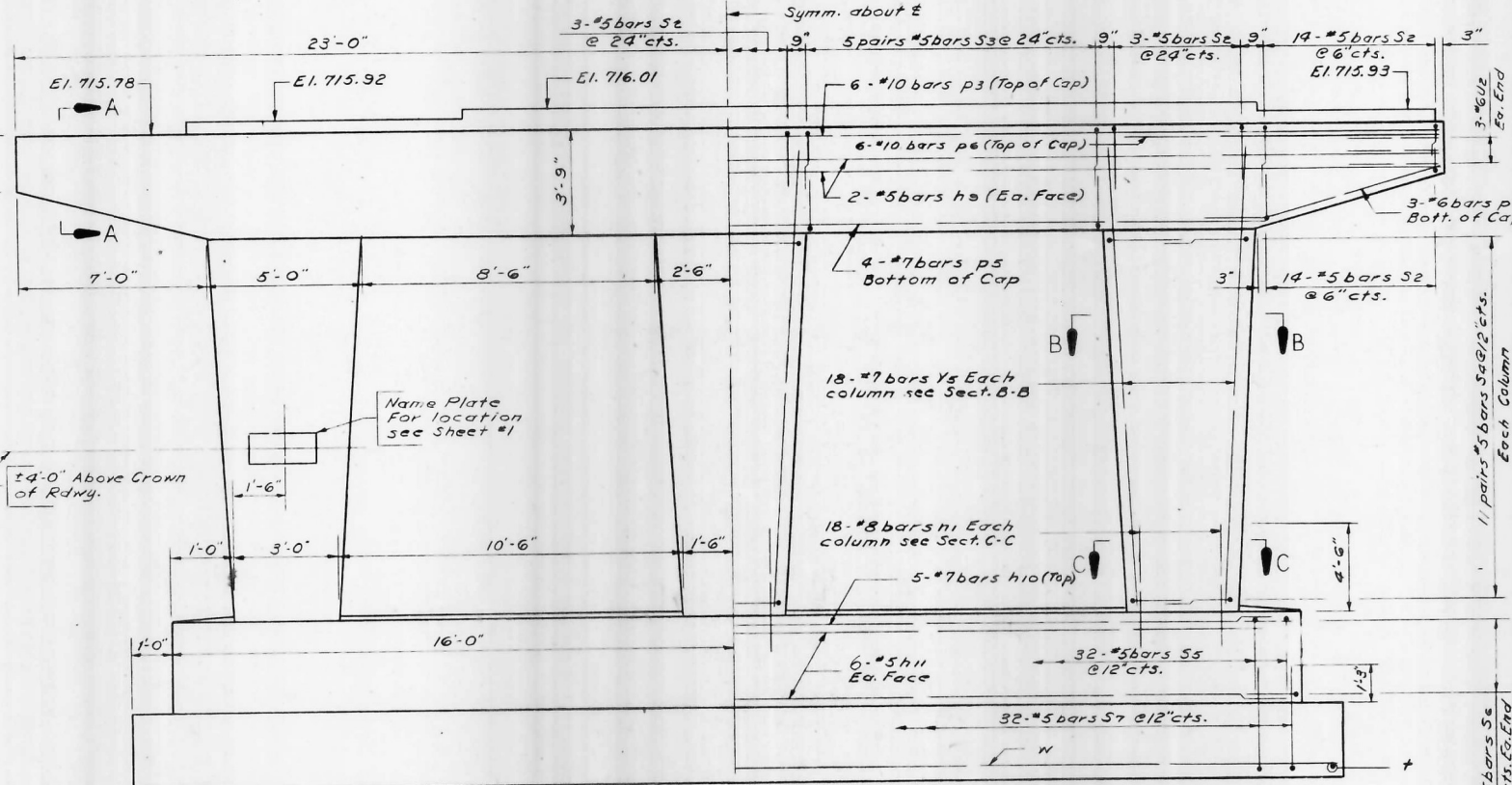
SECTION C-C



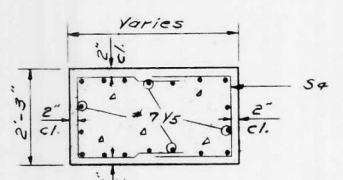
SECTION A-A



END VIEW



ELEVATION



SECTION B-B

BARS S

Bar	A	B
S2	2'-2"	2'-6"
S3	2'-2"	2'-3"
S4	1'-11"	2'-8"
S5	2'-8"	7'-0"
S6	2'-6"	3'-0"
S7	2'-8"	3'-0"

A & B DIMENSIONS

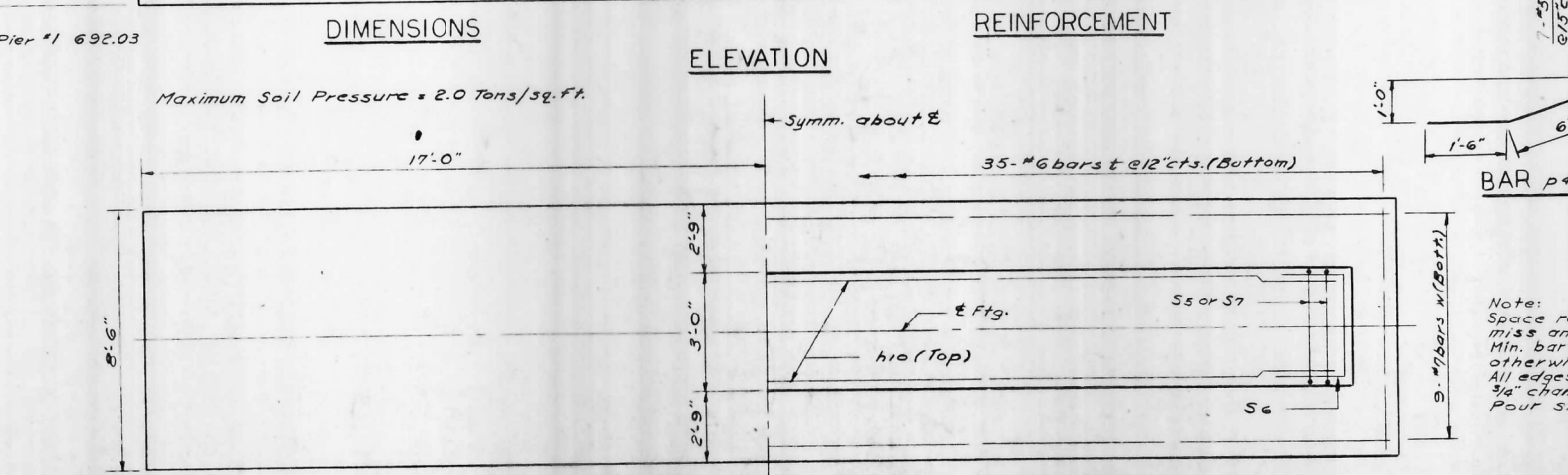
Bar	A	B
S2	2'-2"	2'-6"
S3	2'-2"	2'-3"
S4	1'-11"	2'-8"
S5	2'-8"	7'-0"
S6	2'-6"	3'-0"
S7	2'-8"	3'-0"

PIER #1

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h9	8	#5	23'-6"	
h10	5	#7	30'-6"	
h11		#5	30'-6"	
n1	54	#8	6'-6"	
p3	12	#10	24'-0"	
p4	6	#6	8'-3"	
p5	4	#7	31'-6"	
p6	12	#10	11'-0"	
S2	65	#5	7'-2"	
S3	20	#5	6'-8"	
S4	16	#5	7'-3"	
S5	32	#5	11'-8"	
S6	7	#5	8'-6"	
S7	32	#5	8'-8"	
t	35	#6	8'-0"	
U2	6	#6	7'-0"	
Y5	54	#7	12'-0"	
W	9	#7	33'-6"	

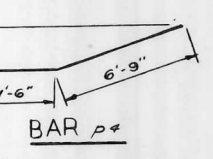
Class X Concrete Cu.Yds. 10.1
Reinforcement Bars Lbs. 8,100
Name Plates Ea. 1



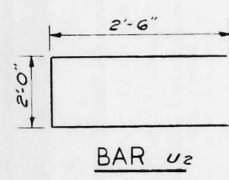
DIMENSIONS

FOOTING PLAN

REINFORCEMENT



BAR p4



BAR U2

Note:
Space reinforcement in cap to miss anchor bolts.
Min. bar laps = 20 dia. unless otherwise noted.
All edges shall have standard 3/4" chamfers except as noted.
Pour steps monolithically with cap.

PIER #1
F.A.I. RT. 57 SEC.38-7HB-2
IROQUOIS COUNTY
STA. 1225+75.45

DESIGNED E.L.
CHECKED J.B., R.R.
DRAWN J. Garrett
CHECKED R.R.

Rev. Cl. X Conc. from 63.2 to 75.4 Cu Yds. Reinf. from 8,650# to 8,520# 8-22-67 F.S.

MODEL: 4400E1M1M1S
FILE: 4400E1M1M1S.dwg
C:\Users\calderon\OneDrive\Documents\4400E1M1M1S.dwg

USER NAME	DESIGNED	REVISIONS
= calderon	E.L.	-
	J.B., R.R.	-
	J. Garrett	-
	R.R.	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

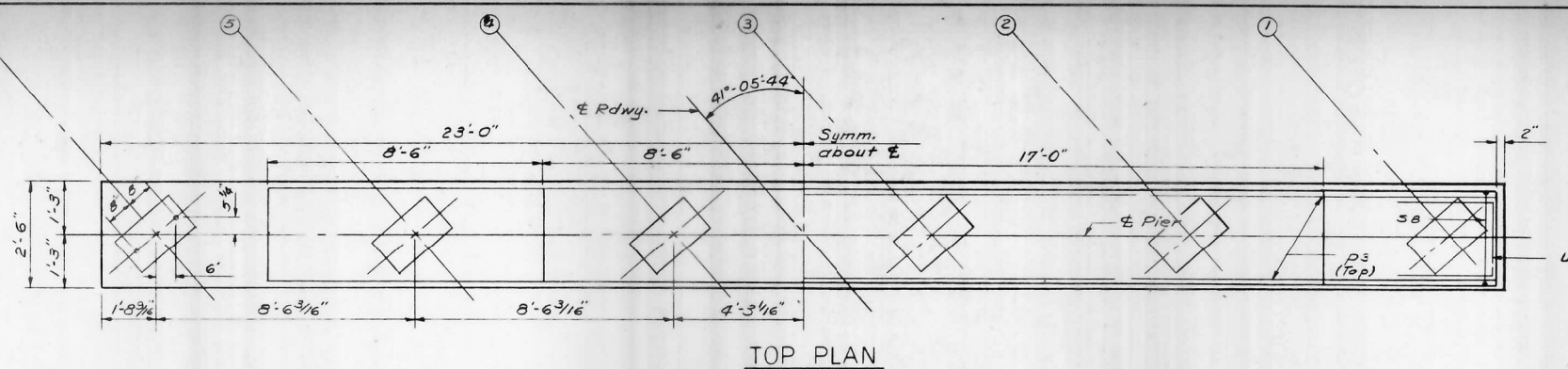
SCALE:	SHEET	OF	SHEETS	STA.	TO STA.
	4	8			

ORIGINAL PIER CONSTRUCTION (1967)
SN 038-0076 (FOR INFORMATION ONLY)

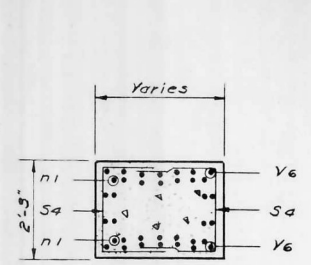
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	101

CONTRACT NO. 66H63
ILLINOIS FED. AID PROJECT

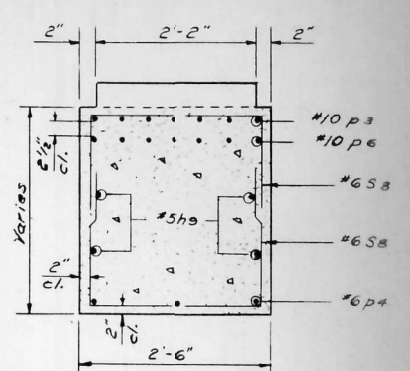
FOR INFORMATION ONLY



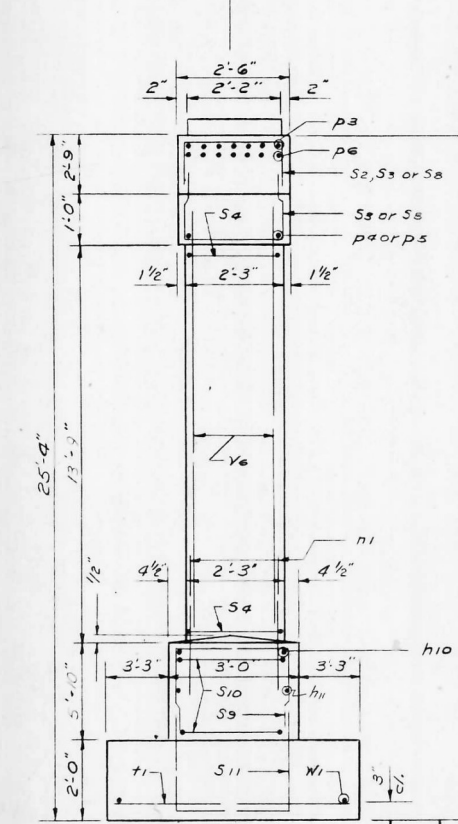
TOP PLAN



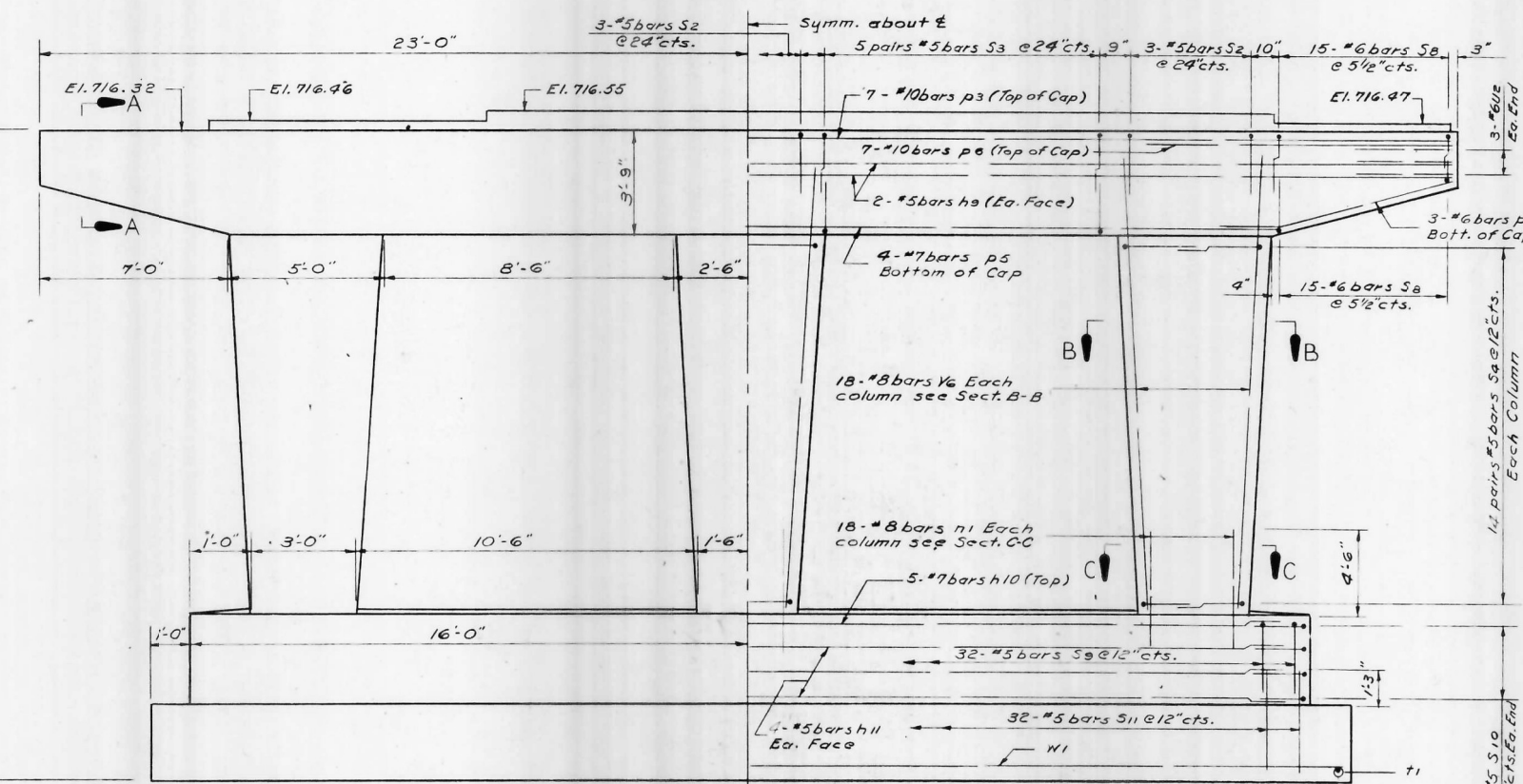
SECTION C-C



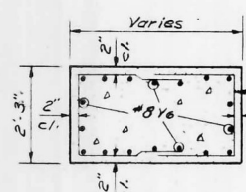
SECTION A-A



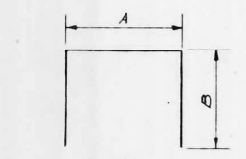
END VIEW



ELEVATION



SECTION B-B



BAR S

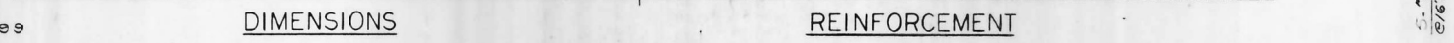
A&B DIMENSIONS

Bar	A	B
S2	2'-2"	2'-6"
S3	2'-2"	2'-3"
S4	1'-11"	2'-8"
S8	2'-2"	2'-6"
S9	2'-2"	2'-6"
S10	2'-6"	2'-0"
S11	2'-8"	3'-0"

PIER #2
BILL OF MATERIAL

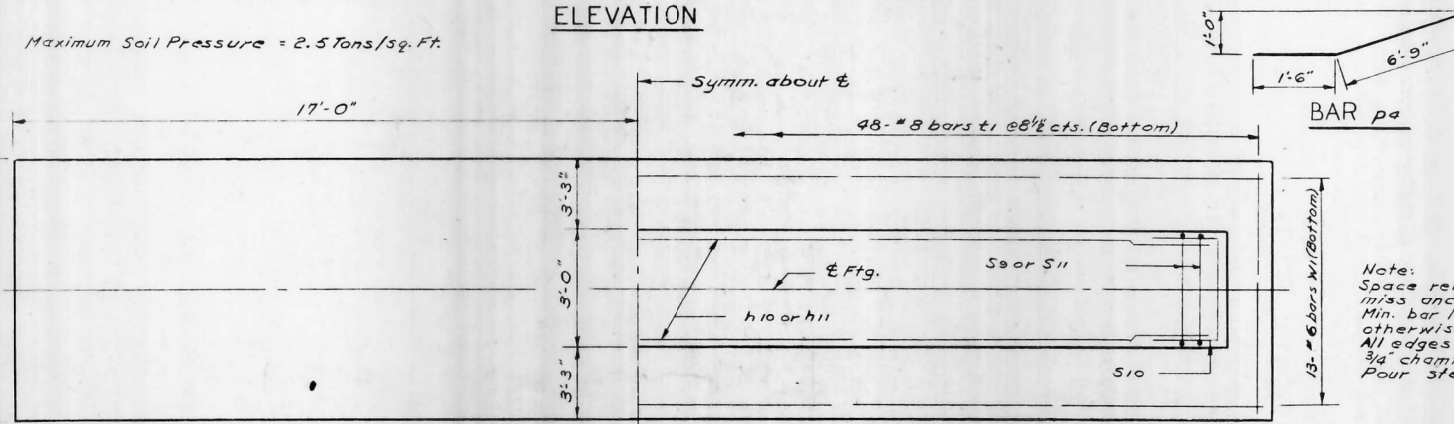
Bar	No.	Size	Length	Shape
h9	8	#5	23'-6"	
h10	5	#7	30'-6"	
h11	8	#5	30'-6"	
n1	54	#8	6'-6"	
p3	14	#10	24'-0"	
p4	6	#6	8'-3"	
p5	4	#7	31'-6"	
p6	14	#10	11'-0"	
S2	9	#5	7'-2"	
S3	20	#5	6'-8"	
S4	47	#5	7'-3"	
S8	60	#6	7'-2"	
S9	32	#5	13'-3"	
S10	10	#5	6'-6"	
S11	32	#5	8'-8"	
t1	48	#8	9'-0"	
U2	6	#6	7'-0"	
Y6	54	#8	1'-10"	
W1	13	#6	33'-6"	

Class X Concrete Cu.Yds. 71.5
Reinforcement Bars Lbs. 10,450



DIMENSIONS

REINFORCEMENT



DIMENSIONS

REINFORCEMENT

FOOTING PLAN

Maximum Soil Pressure = 2.5 Tons/sq. Ft.

Note:
Space reinforcement in cap to 1/16" anchor bolts.
Min. bar laps = 20 dia. unless otherwise noted.
All edges shall have standard 3/4 chamfers except as noted.
Pour steps monolithically with cap.

DESIGNED E.L.
CHECKED J.B.
DRAWN J.Garrett
CHECKED R.R.

PIER #2
F.A.I. RT. 57 SEC. 38-7 HB-2
IROQUOIS COUNTY
STA 1225+75.45

Rev. Cl. X Conc from 71.0 to 74.5 Cu Yds. Reinf from 10,730# to 10,580# 8-22-67 FS

MODEL: MODEL\MARIS
FILE: MARIS - copy - work\pds\calderon\05215370\66h63-dht-8r\fige_Ropasfr.dgn

USER NAME	DESIGNED	REVISION
= calderon	-	-
	DRAWN	REVISION
	-	-
	CHECKED	REVISION
	-	-
	DATE	REVISION
	-	-

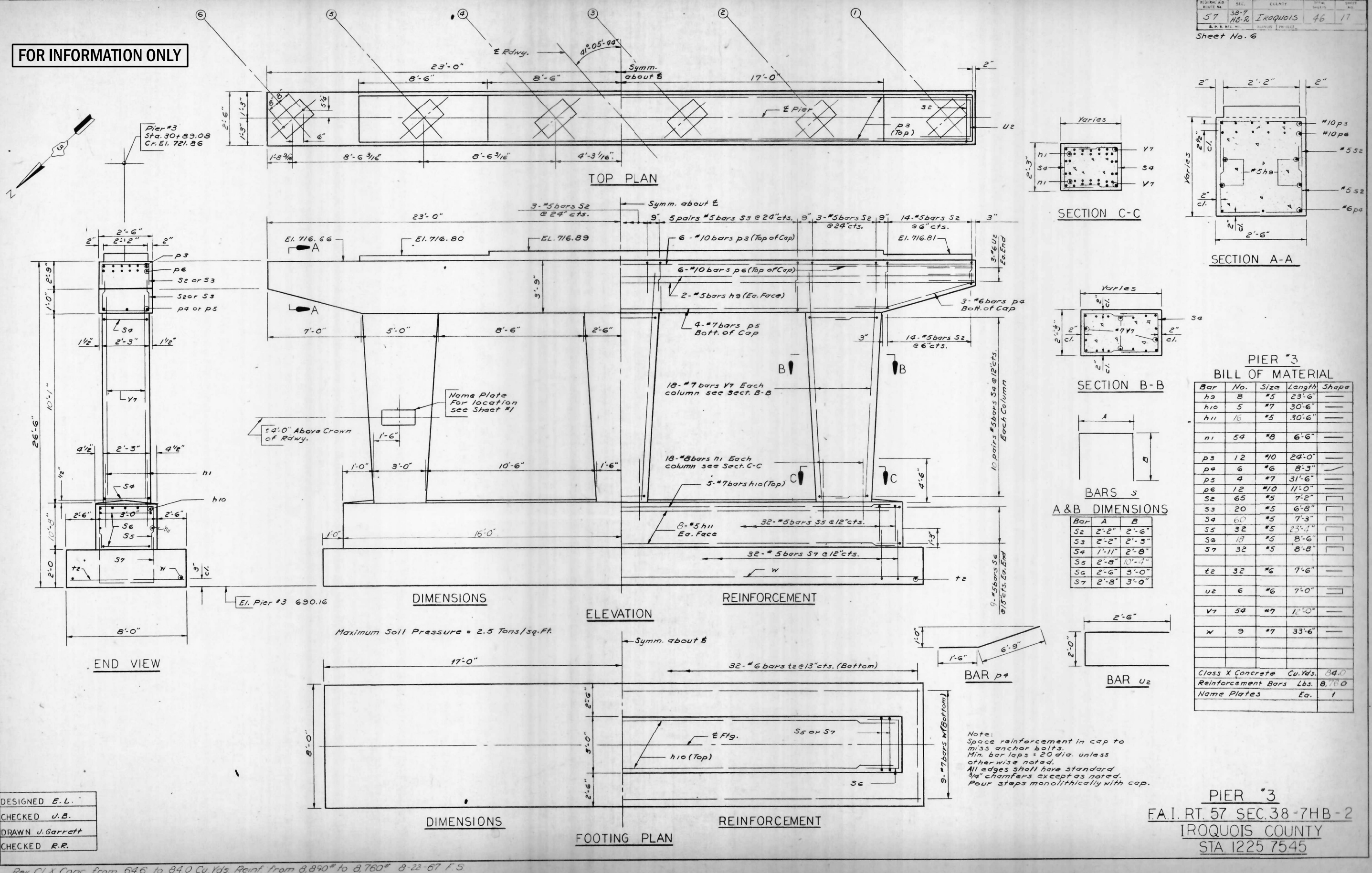
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ORIGINAL PIER CONSTRUCTION (1967)
SN 038-0076 (FOR INFORMATION ONLY)

SCALE: SHEET 5 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	102
ILLINOIS FED. AID PROJECT			CONTRACT NO. 66H63	

FOR INFORMATION ONLY



PIER #3
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h9	8	#5	23'-6"	
h10	5	#7	30'-6"	
h11	16	#5	30'-6"	
n1	54	#8	6'-6"	
p3	12	#10	24'-0"	
p4	6	#6	8'-3"	
p5	4	#7	31'-6"	
p6	12	#10	11'-0"	
S2	65	#5	7'-2"	
S3	20	#5	6'-8"	
S4	60	#5	7'-3"	
S5	32	#5	23'-4"	
S6	18	#5	8'-6"	
S7	32	#5	8'-8"	
t2	32	#6	7'-6"	
u2	6	#6	7'-0"	
V7	54	#7	1'-0"	
W	9	#7	33'-6"	
Class X Concrete				Cu. Yds. 84.0
Reinforcement Bars				Lbs. 8,760
Name Plates				Ea. 1

DESIGNED E.L.
CHECKED U.B.
DRAWN J. Garrett
CHECKED R.R.

PIER #3
F.A.I. RT. 57 SEC. 38-7HB-2
IROQUOIS COUNTY
STA 1225 7545

Rev. CLX Conc. from 646 to 84.0 Cu. Yds Reinf. from 8,890* to 8,760* 8-23-67 F.S.

USER NAME = calderoni	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

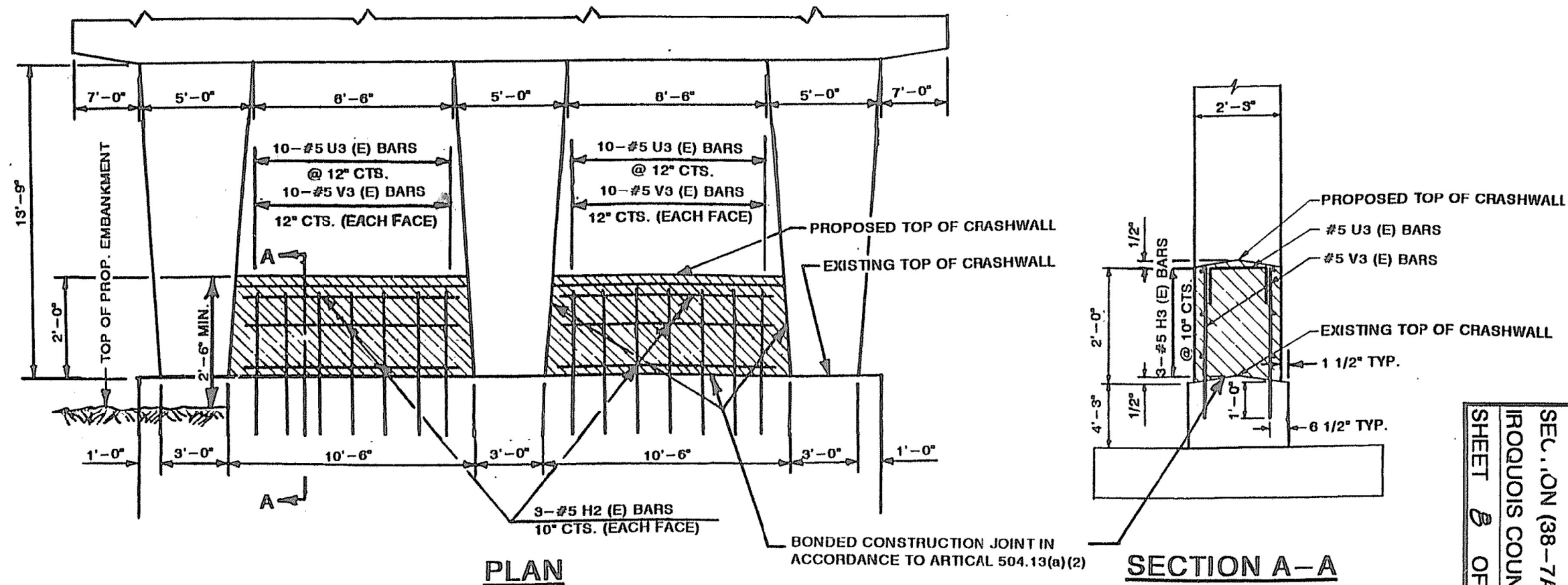
ORIGINAL PIER CONSTRUCTION (1967)
SN 038-0076 (FOR INFORMATION ONLY)
SCALE: SHEET 6 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	103
ILLINOIS FED. AID PROJECT			CONTRACT NO. 66H63	

BILL OF BARS FOR LOCATION NO. 3						CLASS X CONCRETE (MISC)	
BAR	SIZE	LENGTH	NUMBER	POUNDS	SHAPE	LOCATION	CU YDS
V3 (E)	#5	6'-7"	44	302	—	BETWEEN CENTER-EAST COLUMNS	1.6
H3 (E)	#5	9'-0"	12	113	—	BETWEEN CENTER-WEST COLUMNS	1.6
U3 (E)	#5	5'-1"	22	117	┘	TOTAL	3.2
SUBTOTAL				532			

FOR INFORMATION ONLY

EXISTING SAND MODULE ATTENUATORS AT LOCATION #3	20 EACH
STEEL PLATE BEAM GUARD RAIL REMOVAL AT LOCATION #3	137.5 LIN FT ✓
PROPOSED EMBANKMENT REQUIRED AT LOCATION #3	432.0 532 CU YD
PROPOSED ATTENUATER BASE AT LOCATION #3	51.6 SQ YD ✓
PROPOSED INERTIAL BARRIER INSTALLATION AT LOCATION #3	2 EACH ✓
PROPOSED SEEDING CLASS 2 AT LOCATION #3	0.33 0.83 ACRE



LOCATION NO. 3
 I-57 @ CH 9
 At Buckley interchange.
 STRUCTURE NO. 038-0076
 STA. 1225+75.45

NOTES: Hatched areas denote proposed CLASS X CONCRETE (MISC).
 V₃ bars to be set in predrilled holes 12" minimum depth and as specified in BSP-11.
 All reinforcement bars to be epoxy coated.

FAI ROUTE 57
 SECTION 57 (38-7,38-6)RS
 IROQUOIS COUNTY
 SHEET B OF 9

MODEL: MODEL.MAKES
 FILE: MAKE: C:\p\work\shd\calderon\05215370\366\63-dht-d\figs_R\p\shd.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

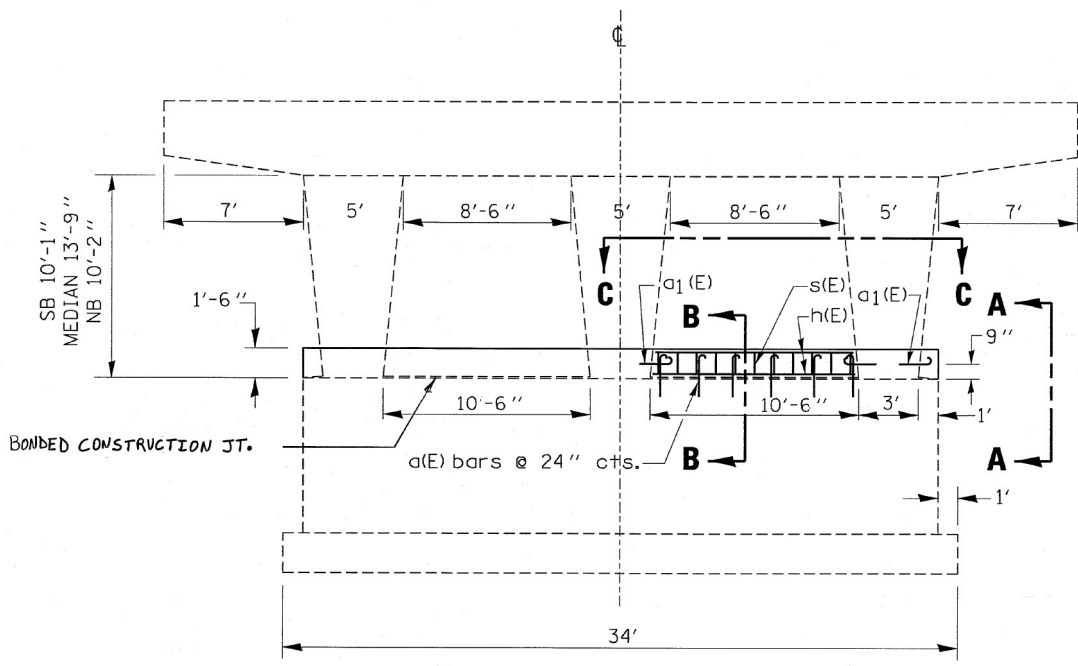
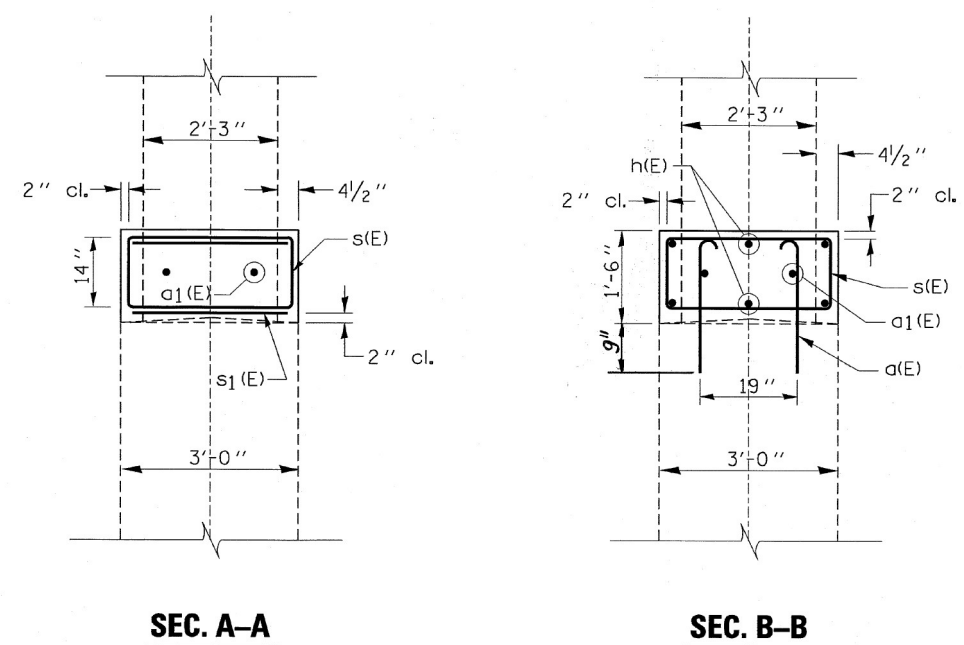
PIER CRASHWALL EXTENSION (1994)
 SN 038-0076 (FOR INFORMATION ONLY)
 SCALE: SHEET 7 OF 8 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	104
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

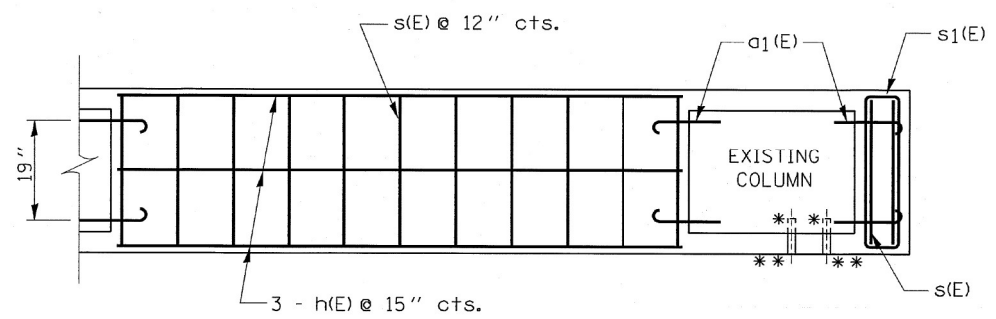
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 7 10 SHEETS
S.R.L. P.A. 57	*	IROQUOIS	98	71	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT	*38-(5,6,7)RS&I		

FOR INFORMATION ONLY



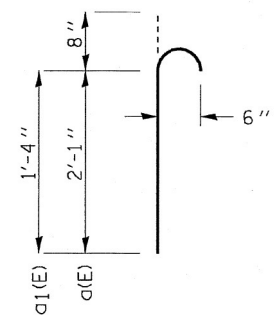
a (E) and a₁ (E) bars should be epoxy grouted in accordance with Section 584 of the Standard Specifications. Cost included in the cost of Reinforcement Bars, Epoxy Coated.

ELEVATION

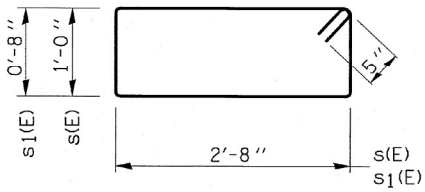


SEC. C-C

- * Predrill 1" Ø Hole Into Pier
- ** Place Formed Concrete Insert Prior To Pouring In Order To Attach Type 6 Guardrail (See Standard). No Holes Shall Be Drilled Into New Concrete.



Bars a(E) & a₁(E)



Bars s(E) & s₁(E)

* (QUANTITIES ARE FOR ONE PIER ONLY)

BILL OF MATERIALS				
BAR	NO.	SIZE	LENGTH	SHAPE
a(E)	28	#6	2'-9"	
a ₁ (E)	12	#6	2'-0"	
h(E)	12	#4	10'-0"	
s(E)	24	#4	8'-2"	
s ₁ (E)	4	#4	7'-6"	
CONCRETE STRUCTURE				4.2 CU YDS
REINFORCEMENT BARS, EPOXY COATED				395 LBS

FORMED CONCRETE INSERTS TO BE USED TO ATTACH GUARDRAIL TO NEW CONCRETE ARE INCLUDED IN THE COST OF CONCRETE STRUCTURE.

* 3 PIERS TOTAL

DESIGNED	19
CHECKED	EXAMINED
DRAWN	PASSED
CHECKED	ENGINEER OF BRIDGES AND STRUCTURES

S.N. 038-0076
STA. 1225 + 75.45

CRASH WALL DETAIL

MODEL: I:\MODELS\MAINTS... FILE: Maints..._work\pilot\calderon\0521537D\366163-dht-bridge_Repair.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER CRASHWALL EXTENSION (2000)			
SN 038-0076 (FOR INFORMATION ONLY)			
SCALE:	SHEET 8	OF 8 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	105
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

EXISTING STRUCTURE: STRUCTURES 038-0151 & 038-0152 ARE TWIN 3-SPAN SPAN BRIDGES CONSISTING OF 3-SPAN SOLID SLAB DECK ON STUB ABUTMENTS AND CAP & SOLID WALL PIERS. IT WAS CONSTRUCTED IN 1968 AS SECTION 38-7B. IN 1976, THE HMA WAS ADDED TO THE DECK AND SOME REPAIRS WERE PERFORMED. IN 1999, HMA SURFACE WAS REMOVED AND MICROSILICA CONCRETE OVERLAY WAS ADDED AS FINAL WEARING SURFACE IN ADDITION TO SOME STRUCTURAL REPAIRS. IN 2000, MINOR REPAIRS WERE PERFORMED ON THE PCC RAILING AND CHANNEL EXCAVATION WAS INCLUDED. IN 2012 AND 2016, CONCRETE DECK SEALER WAS APPLIED TO THE DECK. TRAFFIC WILL BE MAINTAINED WITH TEMPORARY BARRIER WALLS.

SCOPE OF WORK

1. SET UP TRAFFIC CONTROL USING TEMPORARY BARRIER WALL AT EACH WORK AREA.
2. REMOVE EXISTING PCC HANDRAIL AS SHOWN IN PLANS.
3. INSTALL BRIDGE HANDRAIL TY 2399 AT AREAS DESIGNATED IN THE PLANS.
4. RESTORE SURROUNDING WORK AREA
5. INSTALL GUARDRAIL PRIOR REOPENING THE ROAD.

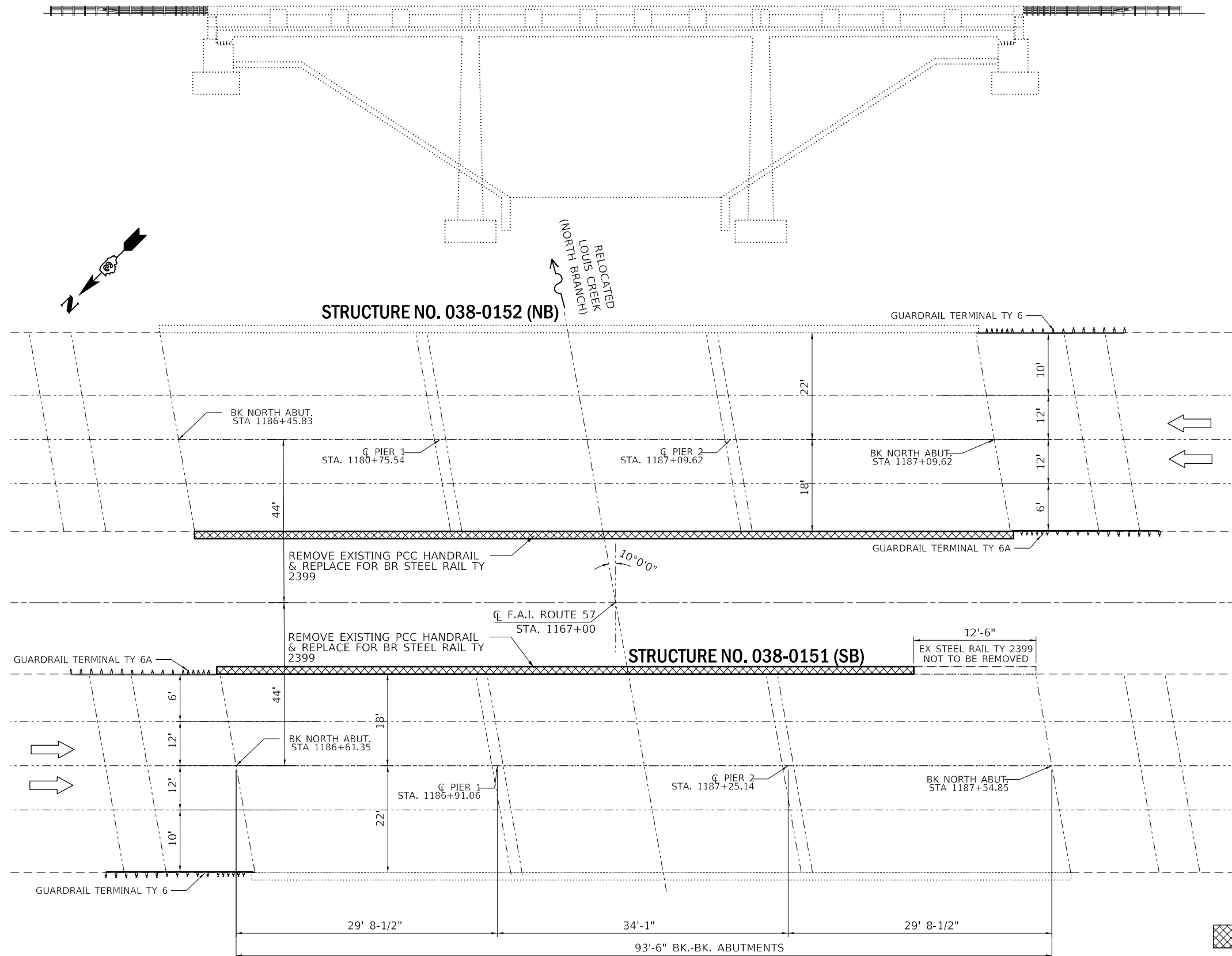
NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

TOTAL BILL OF MATERIAL (STR 038-0151 & 038-0152)

ITEM	UNIT	TOTAL
Concrete Removal	Cu. Yd.	6.7
Steel Railing Type 2399	Foot	184



Expires 11-30-2022

02/07/2022

CONCRETE REMOVAL

MODEL: \\MODELS\MARIS... FILE: \\MARIS..._work\pds\calderon\0521537\0366\63-dht-bridge_Repairs.dgn

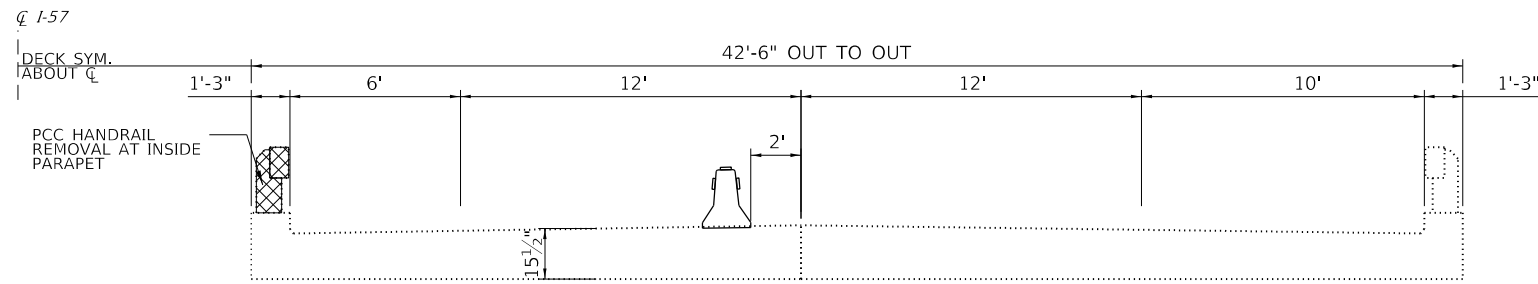
USER NAME = calderonl	DESIGNED - BDD	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - SMR	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL REPAIRS
038-0151 (SB)/038-0152 (NB)**

SCALE: SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	106
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				

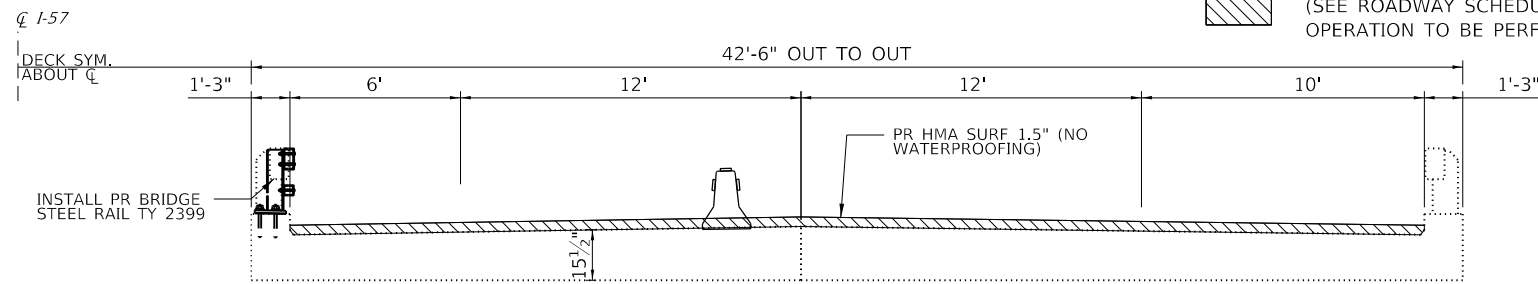


EXISTING CROSS SECTION - REMOVAL

LOOKING NORTH

CONCRETE REMOVAL

PROP HMA SURFACE
(SEE ROADWAY SCHEDULES)
OPERATION TO BE PERFORMED DURING RESURFACING



PROPOSED CROSS SECTION

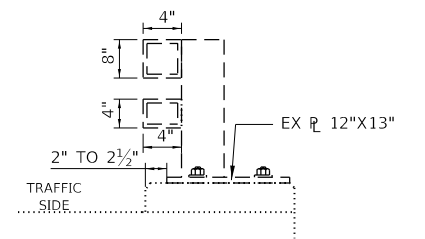
LOOKING NORTH

BILL OF MATERIAL (038-0151)

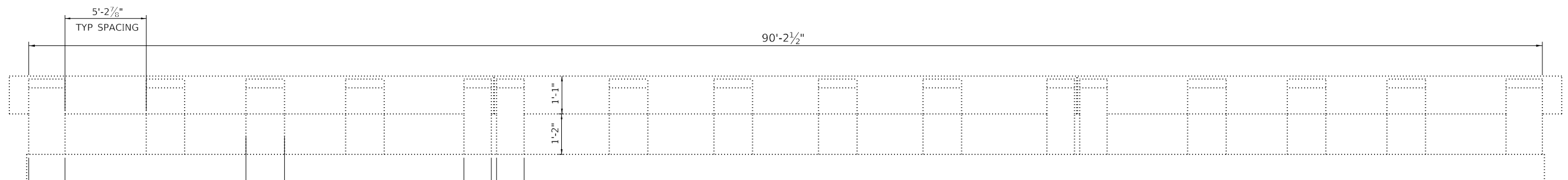
ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	3.1
Steel Railing Type 2399	Foot	81

BILL OF MATERIAL (038-0152)

ITEM	Length	QUANTITY
Concrete Removal	Cu. Yd.	3.6
Steel Railing Type 2399	Foot	93

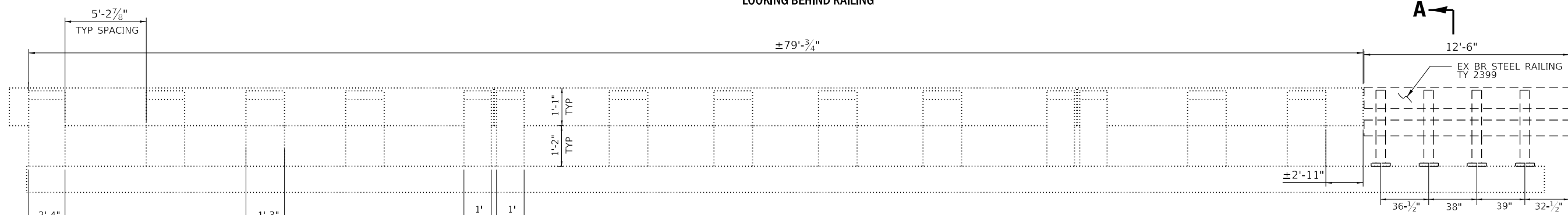


SECTION A-A



EX PCC RAILING LAYOUT

LOOKING BEHIND RAILING



EX PCC RAILING LAYOUT WITH EX STEEL RAILING (051-0151)

LOOKING BEHIND RAILING

MODEL: \\MODELM\HMF5
FILE: \\hmf5\cadd\work\bridge\calderon\05215370\366163-shr-bridge_Repair.dgn

USER NAME = calderon	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/17/2021	CHECKED -	REVISED -
	DATE -	REVISED -

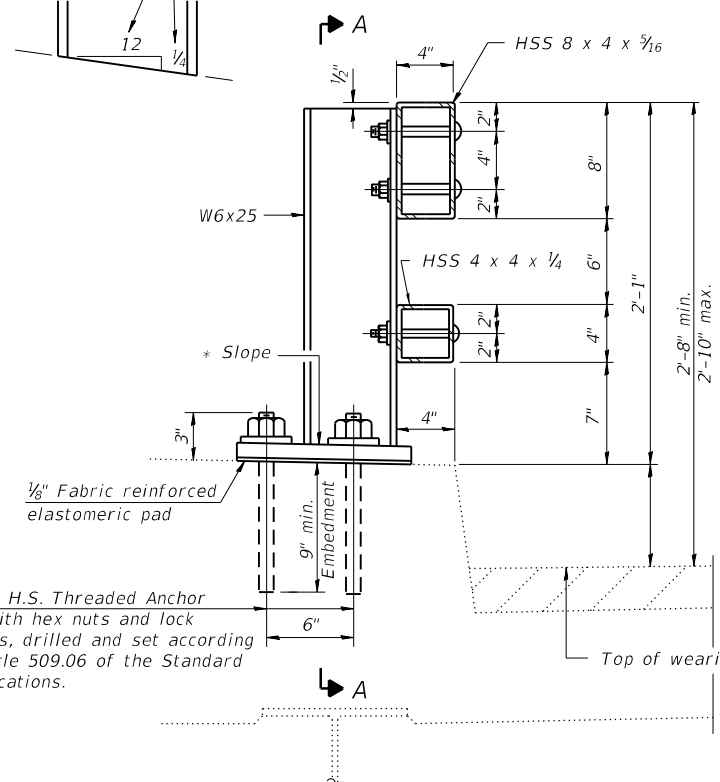
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURAL REPAIRS
038-0151 (SB)/038-0152 (NB)**

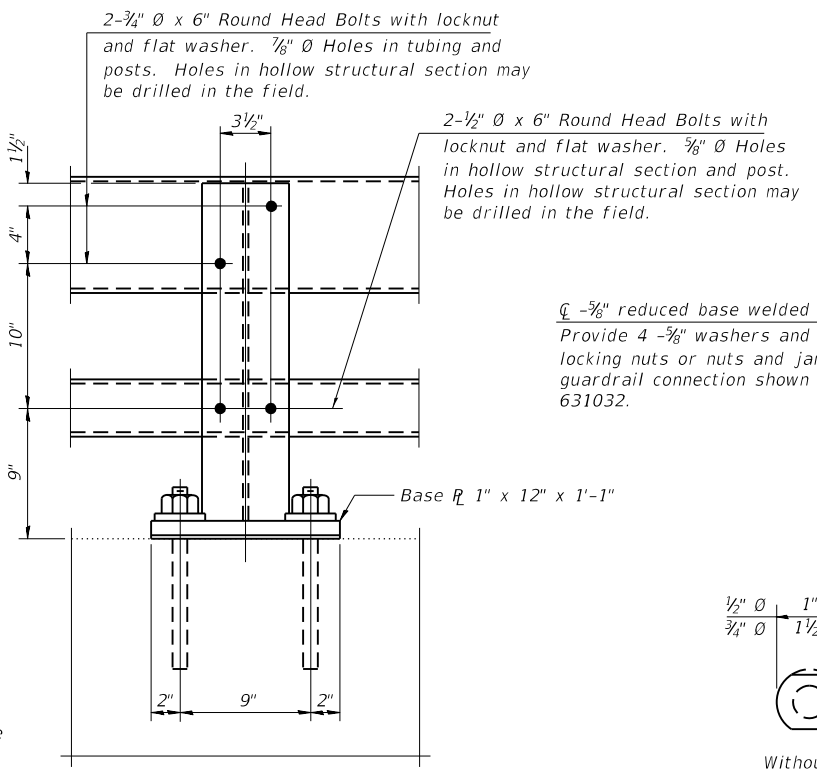
SCALE: SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	107
			CONTRACT NO. 66H63	
ILLINOIS FED. AID PROJECT				

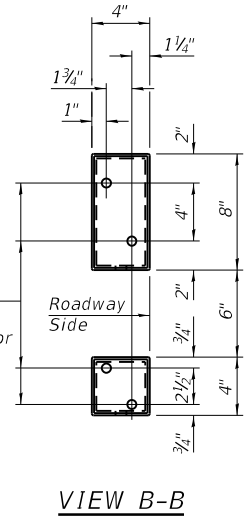
* Cut bottom end of post to curb slope.



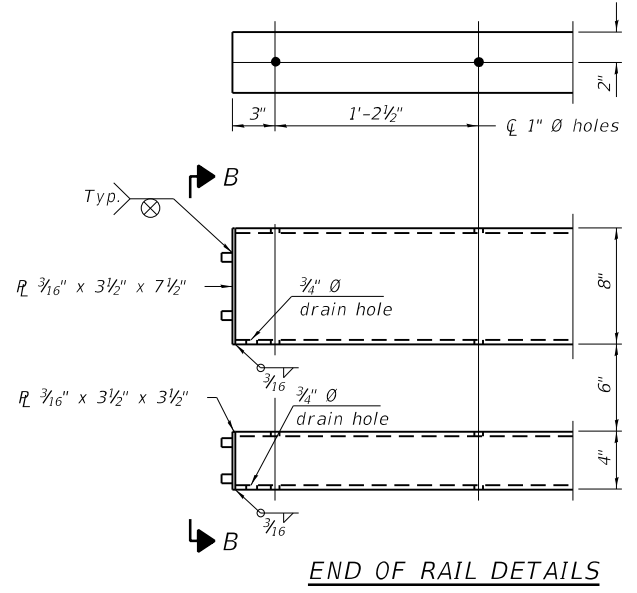
SECTION AT RAIL POST



SECTION A-A



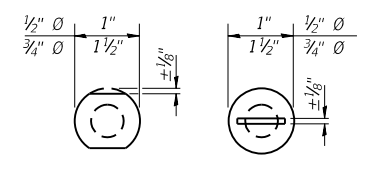
VIEW B-B



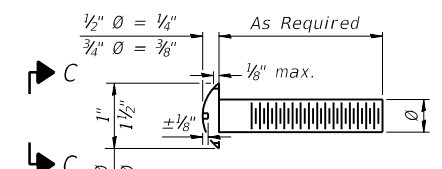
END OF RAIL DETAILS

4- 1" Ø H.S. Threaded Anchor Rods with hex nuts and lock washers, drilled and set according to Article 509.06 of the Standard Specifications.

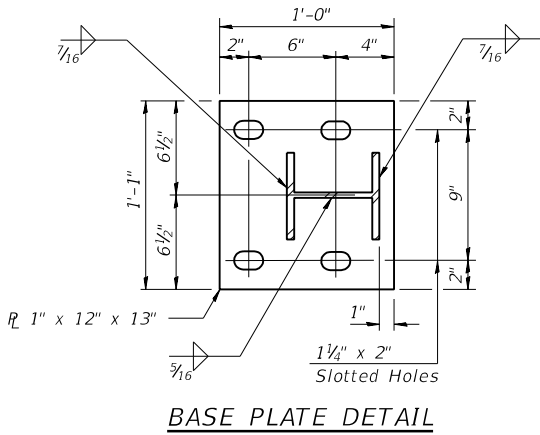
Provide 4 - 3/8" washers and self-locking nuts or nuts and jam nuts for guardrail connection shown on Std. 631032.



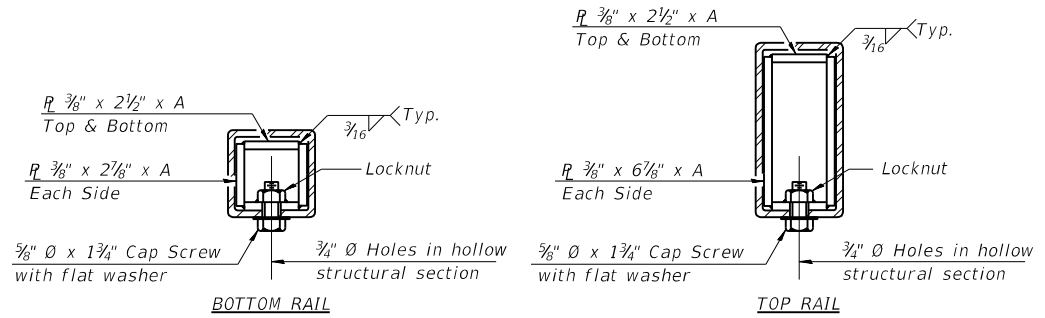
VIEW C-C



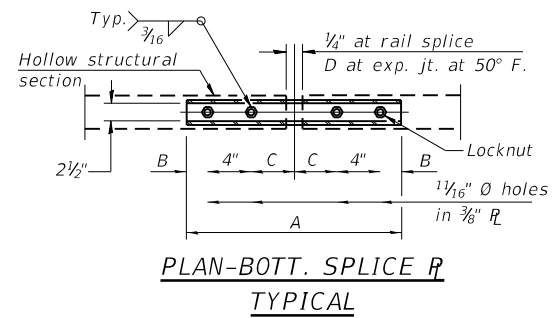
DETAIL OF 1/2" Ø & 3/4" Ø ROUND HEAD BOLTS



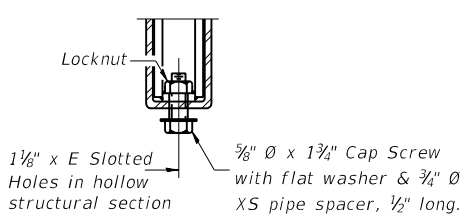
BASE PLATE DETAIL



SECTIONS AT RAIL SPLICE



PLAN-BOTT. SPLICE R TYPICAL



RAIL SPLICE CONNECTION AT EXPANSION JT.

Notes:
 Posts shall not be located closer than 1'-3" to an existing bridge expansion joint or end of bridge.
 Steel Bridge Rail expansion joint shall be provided between any two (2) posts which span a bridge expansion joint. Bolts located at expansion joint shall be provided with locknuts and shall be tightened only to a point that will allow railing movement.
 Provide one 1/8" and two 1/16" steel shims for 25% of the posts. Shims shall be similar to base plates in size and holes.
 All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

SPLICE DIMENSIONS

T	D	A	B	C	E
≤ 4"	2 1/2"	1'-8"	2"	4"	2 1/2"
> 4" ≤ 6 1/2"	3 3/4"	2'-0"	2 1/2"	5 1/2"	3 1/2"
> 6 1/2" ≤ 9"	5"	2'-4"	3 1/2"	6 1/2"	9"
> 9" ≤ 13"	7"	2'-10"	4 1/2"	8 1/2"	11"
Rail Splice	1/2"	1'-8"	2"	4"	—

T = Total movement at expansion joint as shown on the design plans.

BILL OF MATERIAL

Item	Unit	Quantity
Steel Railing, Type 2399	Foot	174

R-31

8-11-2017

(6'-3" Maximum Post Spacing)

STEEL RAILING, TYPE 2399
 STRUCTURE NO. 038-0151 / 038-0152

MODEL: 4\MODEL\MAR15... FILE: MAR15... \work\splicing\calderon\0521537D\0366\63-shr-bridge_Railing.dgn

USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STEEL RAILING, TYPE 2399		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
038-0151 (SB)/038-0152 (NB)		57	(38-7,38-6)RS	IROQUOIS	130	108
SCALE:		SHEET 3 OF 7 SHEETS		STA.	TO STA.	

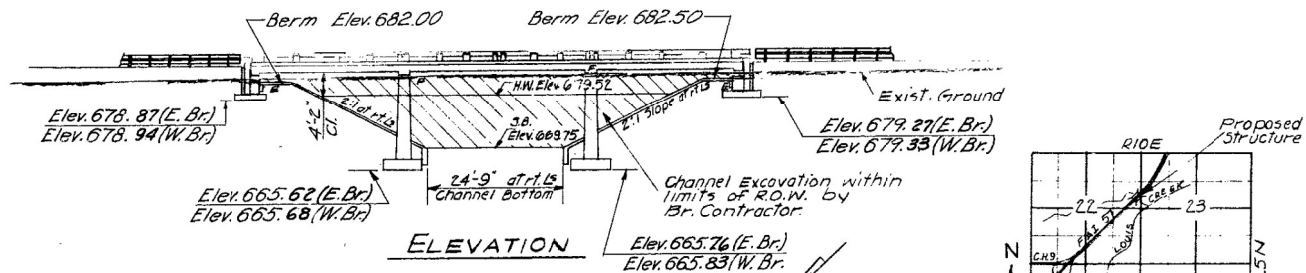
ILLINOIS		FED. AID PROJECT	
CONTRACT NO. 66H63			

BM: #33 R.R. spike in 14" tree
Sta. 1184+00 320' Lt. Elev. 678.51

FOR INFORMATION ONLY

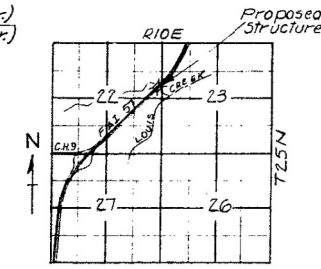
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RT. 57	38-6	IROQUOIS	101	29
SHEET NO. 1 OF 9 SHEETS				



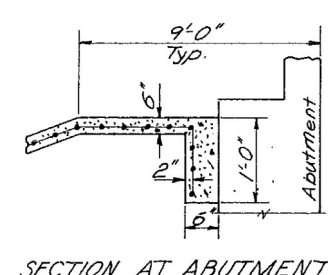
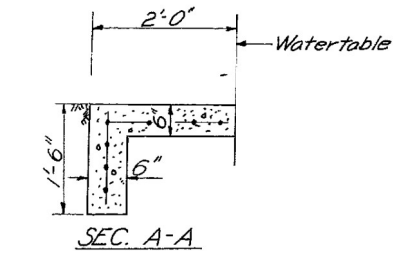
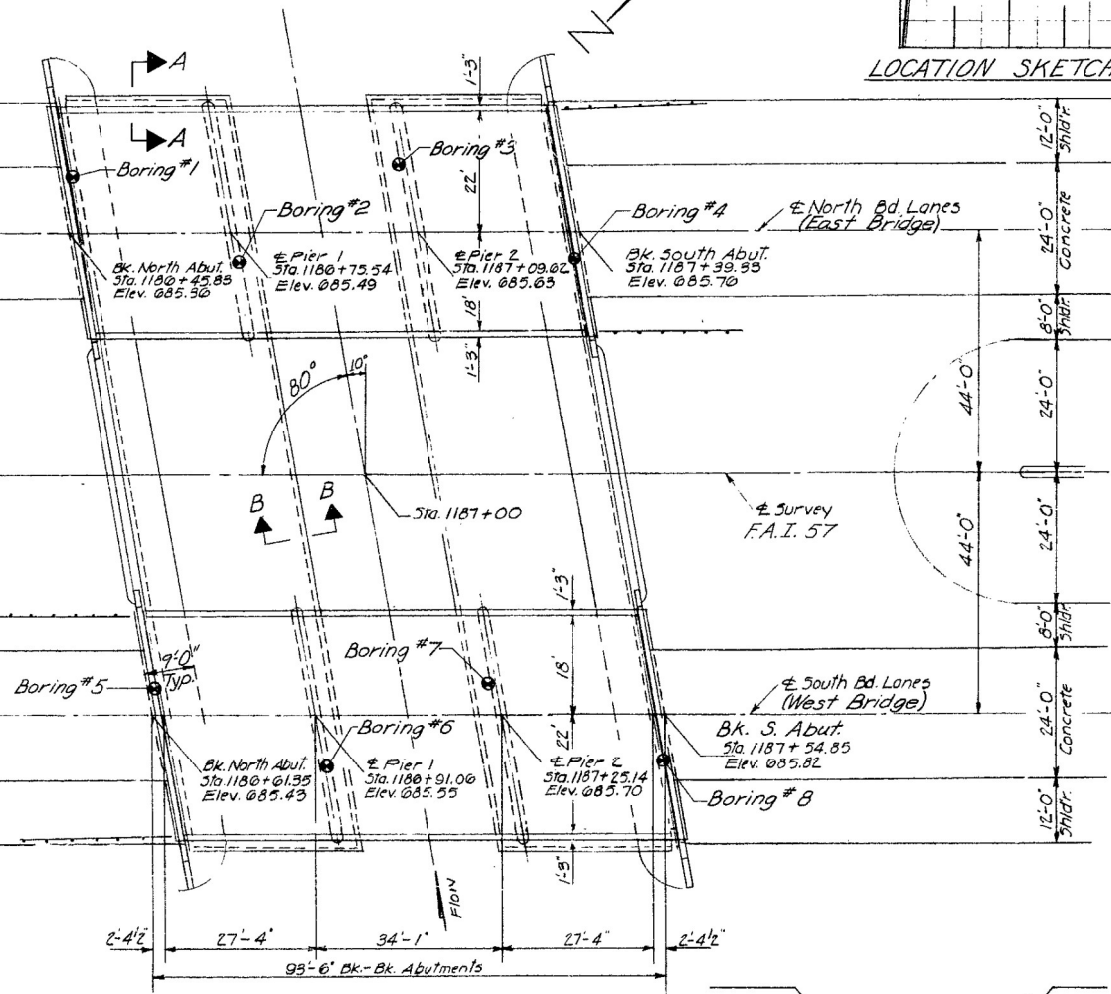
STATION 1187+00
BUILT 196 BY
STATE OF ILLINOIS
F.A.I. RT. 57 SEC. 38-7B
F.A. PROJ. 1-57-6(95)
LOADING H520 & ALT

NAME PLATE
See Std. 2113-1



GENERAL NOTES

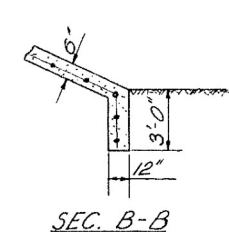
The concrete floor slab shall be finished in accordance with Art. 5.1.9 of the Standard Specifications.
Slope Wall shall be reinforced with welded wire fabric 6" x 5" mesh, weighing 58 # per 100 Sq. Ft.
Layout of slope walls may be varied to suit ground conditions in the field as directed by the Engineer.
The handrail concrete in the rail post and railing shall be poured in separate operations.
All reinforcement bars shall be lapped 20 diameters unless otherwise shown.
All structural steel shall conform to A.S.T.M. Designation A-36.
Exposed surfaces of the expansion devices, inaccessible after erection, shall receive two shop coats of red lead paint. All other surfaces shall be given one shop coat of red lead paint. Anchor studs shall not be painted.
Expansion devices are included in the quantity of structural steel. Est. weight = 4480 Lbs.
Except as otherwise provided, all structural steel shall receive one shop coat of red lead paint and two field coats of aluminum paint. See Article 56.1 to 56.5 inclusive of the Standard Specifications.



TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER.	SUB.	TOTAL
* Class A Excav. for Structures	Cu. Yds.	750		750
Handrail Concrete	Cu. Yds.	14.2		14.2
Class A Concrete	Cu. Yds.	336.8		336.8
Class X Concrete	Cu. Yds.	408.2	140.4	548.6
Protective Coat	Sq. Yds.	910		910
Structural Steel	Lbs.	8020		8020
Reinforcement Bars	Lbs.	115,400	25,540	140,940
Name Plates	Ea.	2		2
Slope Wall (6")	Sq. Yds.			1100
** Preformed Jt. Sealer	Lin. Ft.	173		173

* Includes slope wall excavation.
** At abutments only.



TYP. CHANNEL SECTION

WATERWAY INFORMATION

Drainage Area ----- 7000 Acres
Character ----- Level, Cultivated
Required Opening ----- (50 Yr. Flood) - 400 Sq. Ft.
Present Opening ----- None
Proposed Opening ----- 400 Sq. Ft.
Low Water Elev. ----- 670.7

PROPOSED CHANNEL CHANGE

DESIGN STRESSES

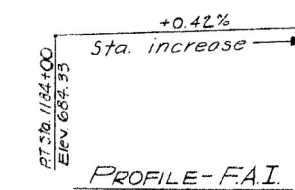
$f_c = 1400$ psi (Super. & Sub.)
 $f_s = 20,000$ psi (Reinf. & Struct.)
 $v_c = 75$ psi. (Flgs)
 $n = 10$
Max. Soil Pressure at Abut. 2500 psf.
Max. Soil Pressure at Piers 5000 psf.
LOADING H520-44 & ALT.

**GENERAL PLAN & ELEVATION
RELOCATED LOUIS CREEK
(NORTH BRANCH)
F.A. PROJ. 1-57-6(95)
F.A.I. RT. 57 SEC. 38-7B
IROQUOIS COUNTY
STA. 1187+00**

DESIGNED John A. Clark
CHECKED Wei Hsiang
DRAWN R. P. Summer
CHECKED Wei Hsiang

EXAMINED [Signature]
PASSED [Signature]
APPROVED [Signature]

JUNE 29 1965

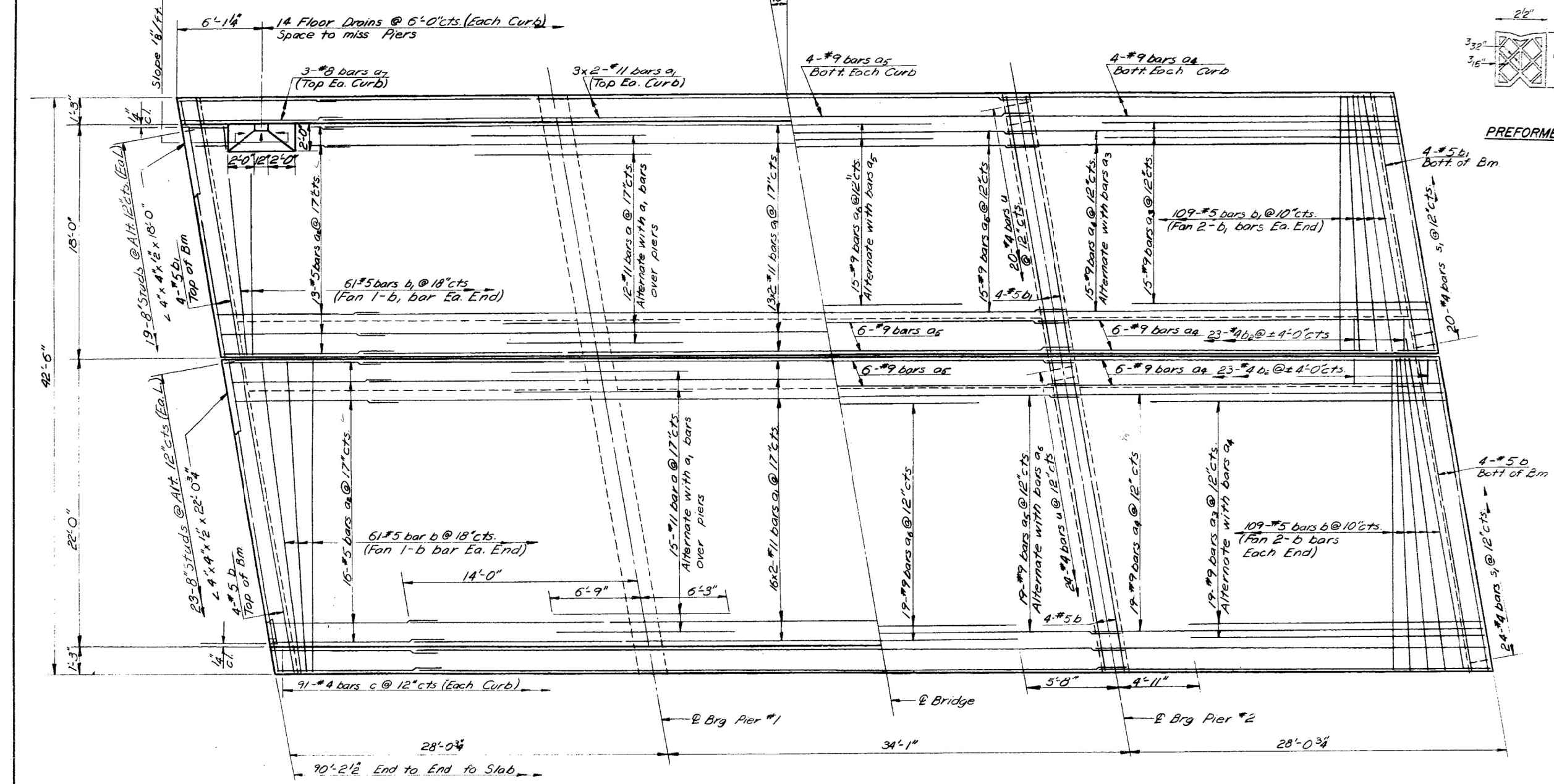
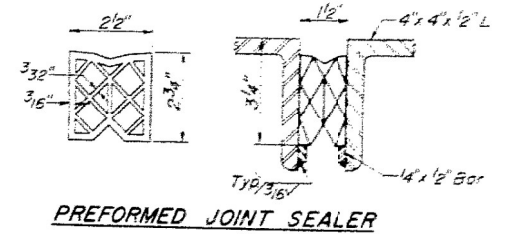


MODEL: 4MODELMARIS
FILE: MARIS.ctb
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R:\proj\157\666\63-rt-57-6\fig_0666h63.dwg

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
44-57	38-7A 38-7B 38-7C	IROQUOIS	101	30
F.A.I. 57		ILLINOIS	FED. AID PROJECT	9 SHEETS

FOR INFORMATION ONLY

STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS



REINFORCEMENT TOP OF SLAB

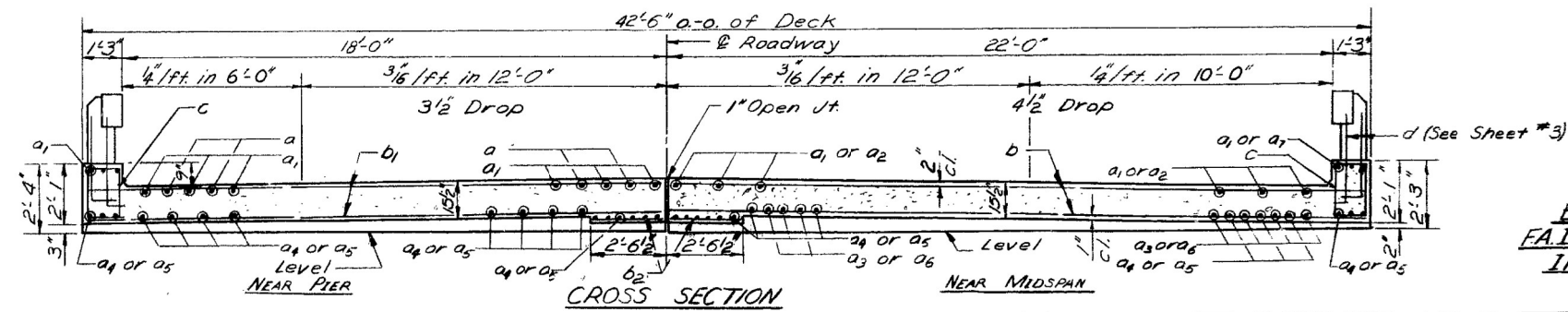
REINFORCEMENT BOTTOM OF SLAB

For details & Bill of Material see Sheet #3

Note:
Bars indicated thus 16x2-#11 etc indicates 16 lines of bars with 2 lengths per line.
Min. bar laps = 20 dia.

PLAN
(Showing West Bridge)
(Rotate 180° for East Bridge)

DESIGNED	John W. Clark	EXAMINED	June 23 1965
CHECKED	Wei Hsing	PASSED	
DRAWN	R. P. Summer	APPROVED	
CHECKED	Wei Hsing		



CROSS SECTION

SUPERSTRUCTURE
EAST & WEST BRIDGES
F.A.I. RT. 57 SECTION 38-7B
IROQUOIS COUNTY
STA. 1187+00

MODEL: 4MODELMAR165
FILE: MAR165.ctb
WORKSPACE: C:\calderon\10521537D\66H63-dtd-bridge_Repair.dgn

USER NAME	= calderon
PLOT SCALE	= 100,0000' / in.
PLOT DATE	= 12/17/2021

DESIGNED	-	REVISED	-
DRAWN	-	REVISED	-
CHECKED	-	REVISED	-
DATE	-	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ORIGINAL BRIDGE CONSTRUCTION (1967)
SN 038-0151 & 038-0152 (FOR INFORMATION ONLY)

SCALE: SHEET 5 OF 7 SHEETS STA. TO STA.

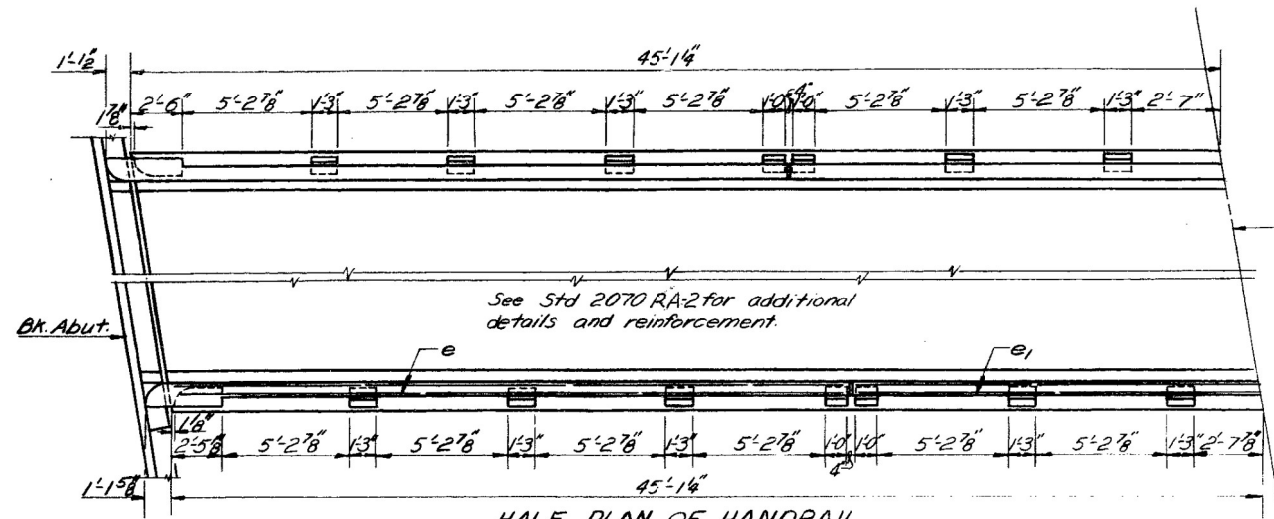
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	110
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 66H63

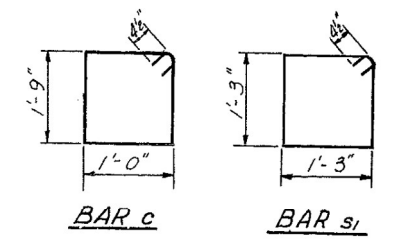
STATE OF ILLINOIS
DEPARTMENT OF PUBLIC WORKS & BUILDINGS
DIVISION OF HIGHWAYS

FOR INFORMATION ONLY

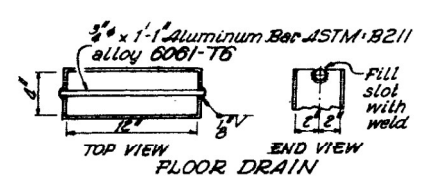
7/8" holes at 12" cts for 3/8" bolts. Set on normal gage line. All bolts shall be burned, sawed or clipped off flush with the back of the Ls after forms are removed.



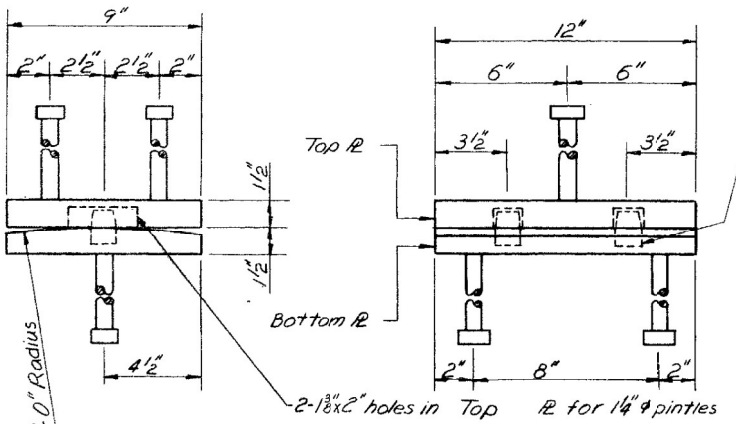
HALF PLAN OF HANDRAIL



PLAN TOP PLATE



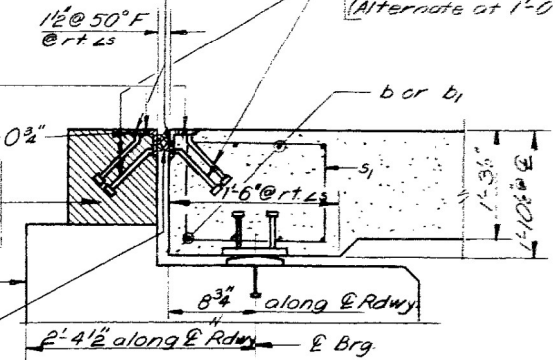
FLOOR DRAIN



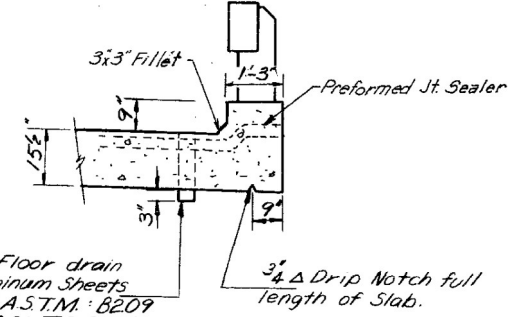
ABUTMENT BEARING ASSEMBLY DETAIL

Handrail Symm. about C of Slab by horizontal rotation thru 180 degrees

Hatched area to be poured after Superstructure forms have been removed. Quantity of Class X Concrete included with Superstructure.



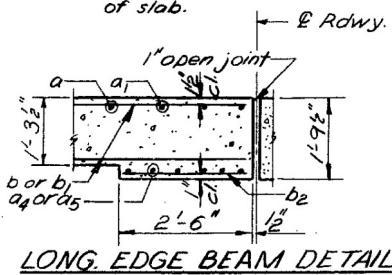
SECTION THRU ABUTMENT



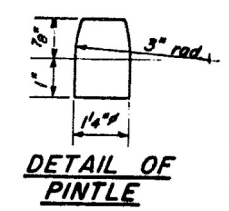
SECTION AT PIER (Typ at pier)

TWO SUPERSTRUCTURES
BILL OF MATERIAL

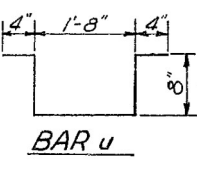
Bar	No.	Size	Length	Shape
a	108	#11	13'-0"	
a1	140	#11	32'-3"	
a2	116	#5	16'-3"	
a3	136	#9	24'-3"	
a4	216	#9	28'-9"	
a5	108	#9	36'-3"	
a6	68	#9	22'-9"	
a7	24	#8	16'-3"	
b	368	#5	23'-0"	
b1	368	#5	19'-0"	
b2	92	#4	2'-3"	
c	364	#4	6'-9"	
d	328	#3	4'-0"	L
e	48	#8	27'-3"	
e1	24	#8	33'-3"	
r	128	#4	3'-0"	
r1	24	#4	4'-0"	
s	728	#3	3'-5"	
s1	176	#4	5'-9"	
u	176	#4	3'-8"	
Handrail Concrete		Cu. Yds.	14.2	
Class X Concrete		Cu. Yds.	408.2	
Reinforcement Bar		Lbs.	115400	
Structural Steel		Lbs.	18500	



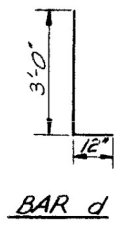
LONG EDGE BEAM DETAIL



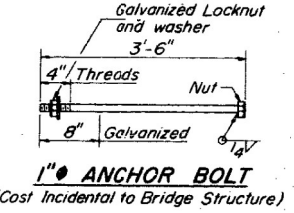
DETAIL OF PINTLE



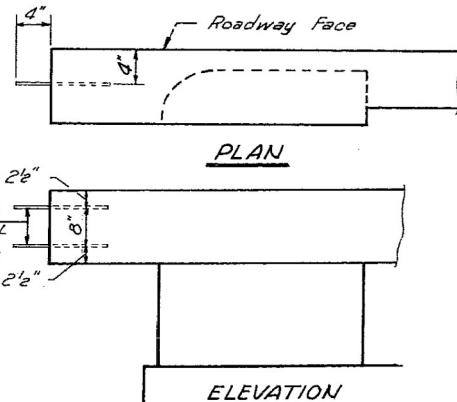
BAR u



BAR d



1" ANCHOR BOLT (Cost incidental to Bridge Structure)

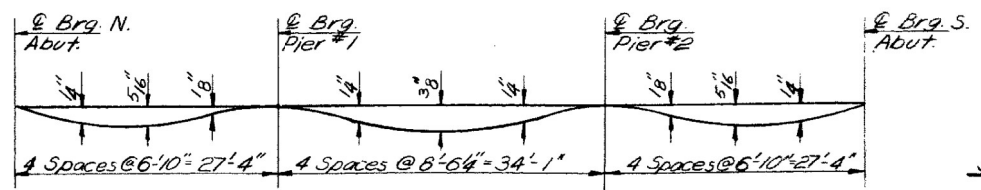


MODIFIED END POST
North end West Bridge; South end East Bridge

TABLE OF MOMENTS AND REACTIONS

Load	MOMENTS*		REACTIONS*	
	4 Sp #1 Pier #1	.5 Sp #2	Abut.	Pier
D.L.	11.04	-20.66	10.75	95.4
L.L.	20.21	-16.47	20.50	90.1
IMP.	6.06	-4.94	6.15	27.0
TOTAL	37.31	-42.07	37.40	212.5

* Moments are per ft width of slab in ft-kips.
* Reactions are for full width of Bridge in kips.



DEAD LOAD DEFLECTION DIAGRAM
Includes weight of concrete only.

SUPERSTRUCTURE DETAILS
EAST & WEST BRIDGES
F.A.I. RT. 57 SECTION 38-78
IROQUOIS COUNTY
STA. 1187+00

DESIGNED: John W. Clark Jr.
CHECKED: Wei Hsiang
DRAWN: R.P. Summer
CHECKED: Wei Hsiang

EXAMINED: Carl E. Hummer
PASSED: H. J. Altman
APPROVED: V. G. Staff

June 23 1965

USER NAME = calderoni
DESIGNED -
DRAWN -
PLOT SCALE = 100,0000' / in.
PLOT DATE = 12/17/2021

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

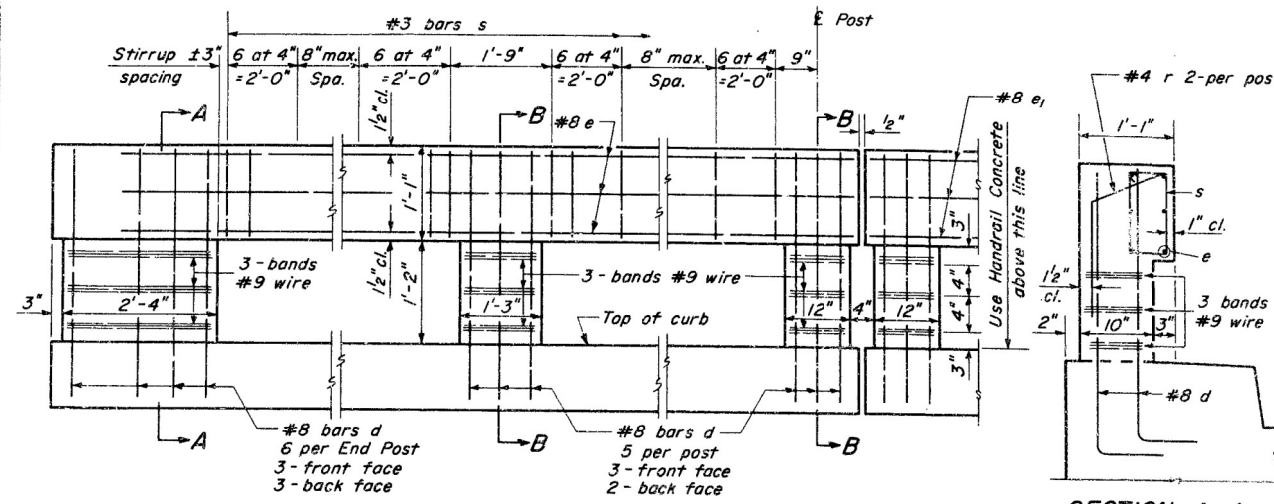
ORIGINAL BRIDGE CONSTRUCTION (1967)
SN 038-0151 & 038-0152 (FOR INFORMATION ONLY)

SCALE: SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.I. RTE. 57 SECTION (38-7,38-6)RS COUNTY IROQUOIS TOTAL SHEETS 130 SHEET NO. 111 CONTRACT NO. 66H63 ILLINOIS FED. AID PROJECT

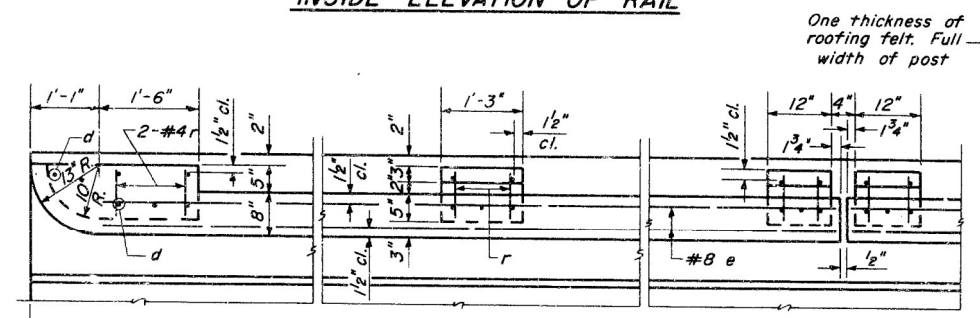
FOR INFORMATION ONLY

STANDARD DESIGN
REINFORCED CONCRETE HANDRAIL
WITHOUT OPEN EXPANSION JOINT - WITH SAFETY WALK

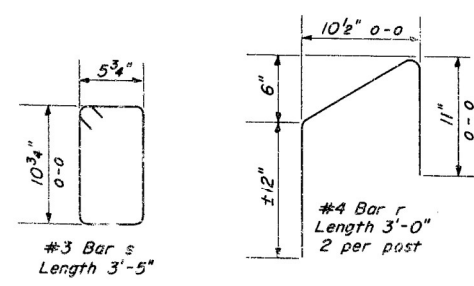


INSIDE ELEVATION OF RAIL

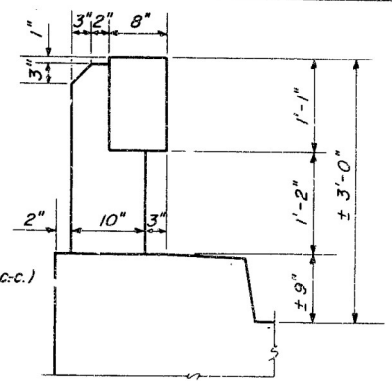
SECTION A-A



SECTION B-B



NOTE:
See Bridge Plans for spacing of posts. (Max. Post spacing 8'-0" c.c.)
Use Double Post and 1/2" open joint only where called for on Bridge Plans.
The cost of furnishing and placing the #9 gage wire, tire tape and roofing felt shall be included in the unit price bid per cubic yard for Handrail Concrete.
No Construction Joint will be permitted between Top of Slab and Top of Curb.



QUANTITIES

End Post Cu. Yds.	Interior Post Cu. Yds.	Double Post Each Cu. Yds.
0.1797	0.0628	0.0503

0.7222 Cu.Ft. per. ft. of Rail (between End Posts)

STATE OF ILLINOIS DEPARTMENT OF PUBLIC WORKS & BUILDINGS DIVISION OF HIGHWAYS		REVISIONS	
EXAMINED	DATE	BY	DATE
W. E. Blumhagen	SEPT 2 1964		
PASSED			
APPROVED			

STANDARD 2070RB-2

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

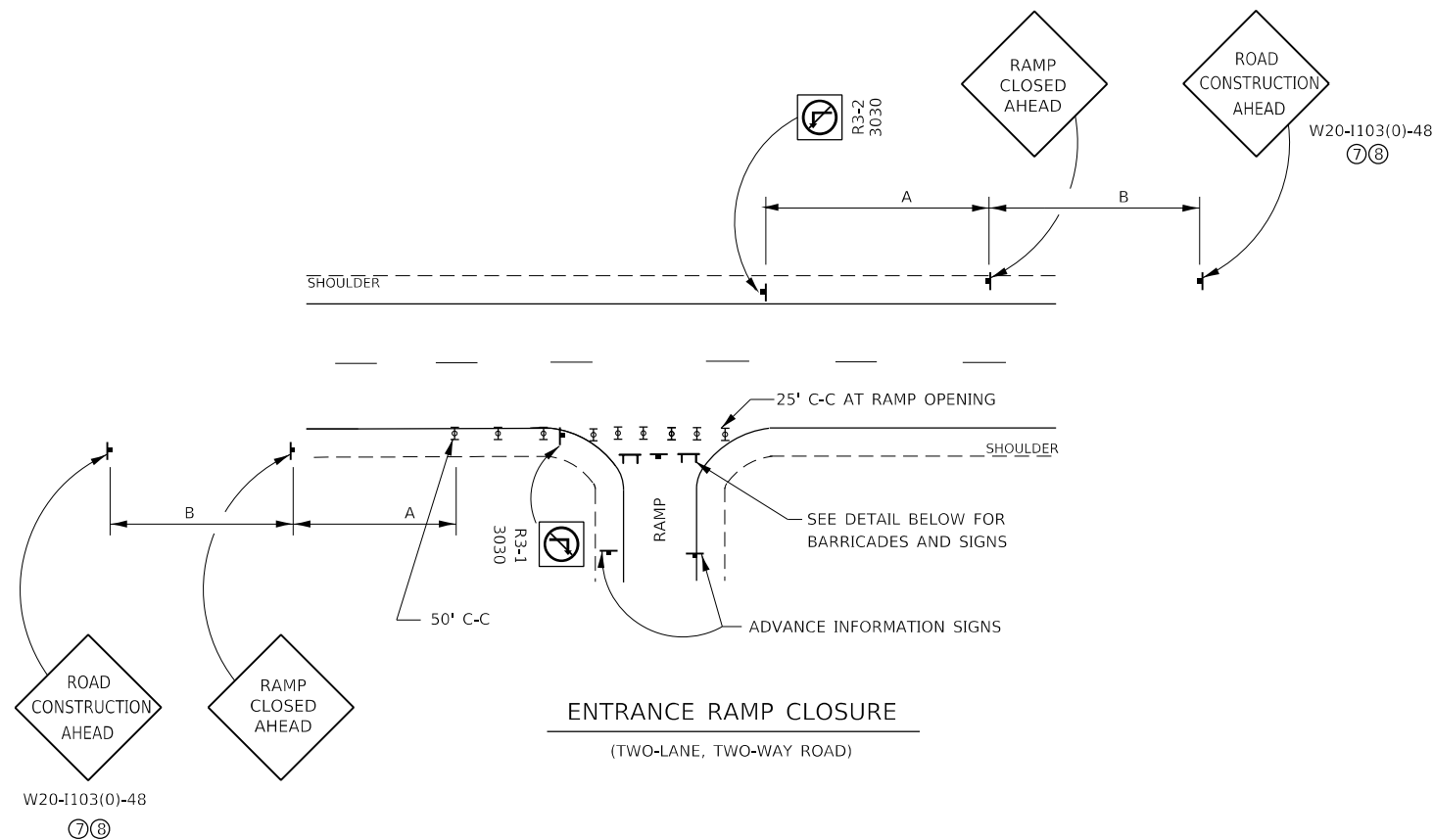
PCC HANDRAIL (OLD STANDARD)
SN 038-0151 & 038-0152 (FOR INFORMATION ONLY)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	112
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

USER NAME	DESIGNED	REVISED
= calderoni	-	-
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

SCALE: SHEET 7 OF 7 SHEETS STA. TO STA.

MODEL: 440DEFIMARFS
FILE: 440defimarf.dwg
C:\pwworkspace\calderoni\05215370\66h63-112-bridge_repair.dwg

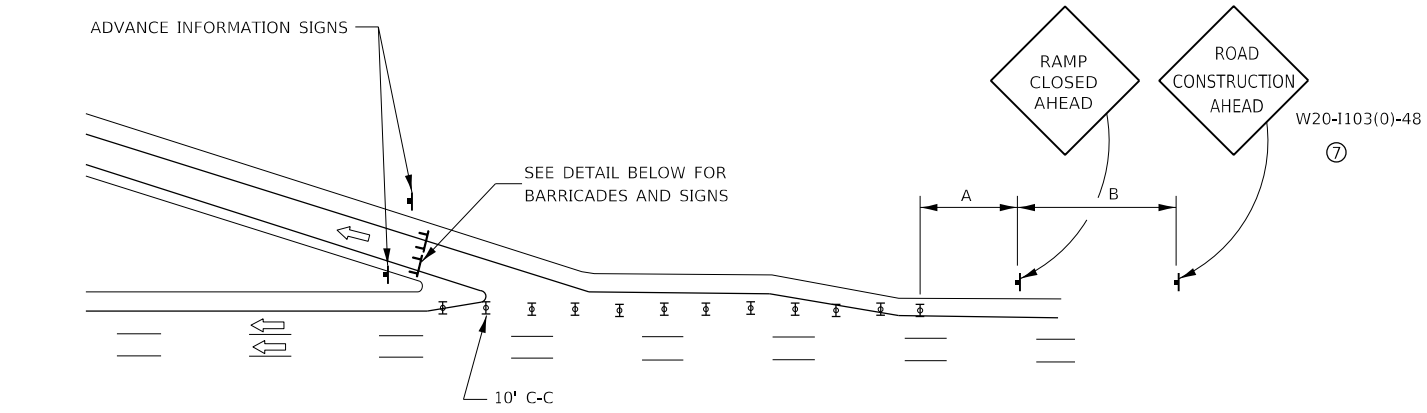


ENTRANCE RAMP CLOSURE
(TWO-LANE, TWO-WAY ROAD)

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000'	1500'
EXPRESSWAY ≤24 HOURS	500'	500'
ARTERIAL ≥45 MPH	350'	350'
ARTERIAL <45 MPH	150'	150'

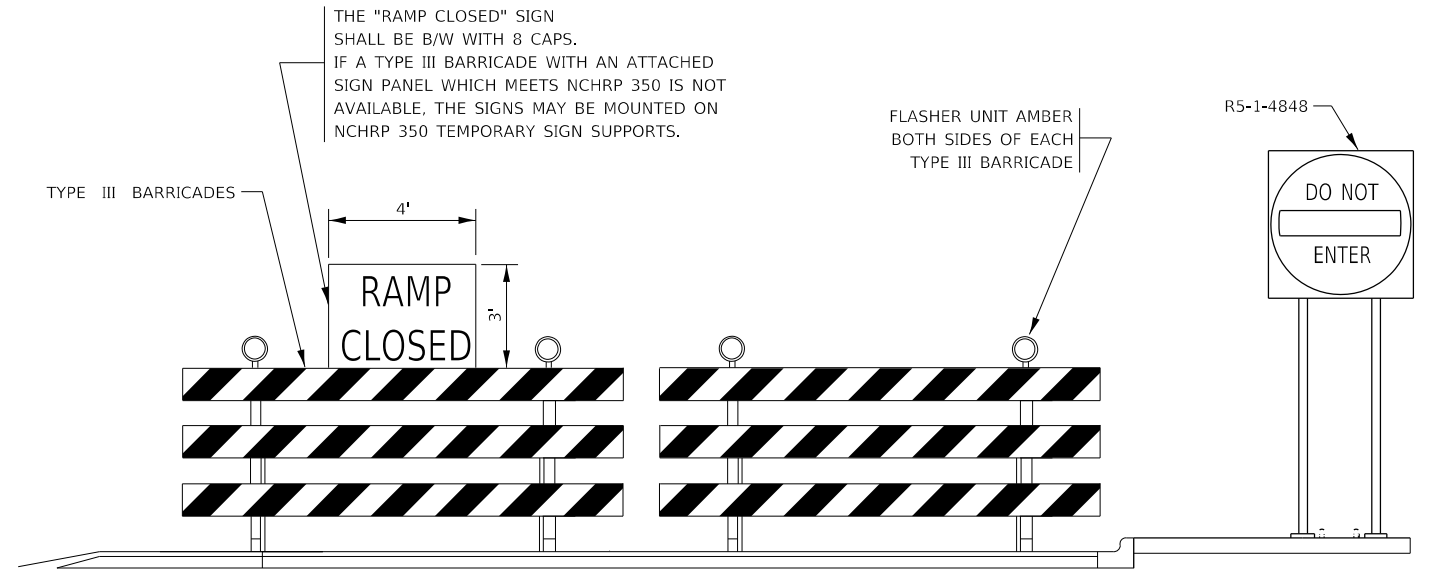
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

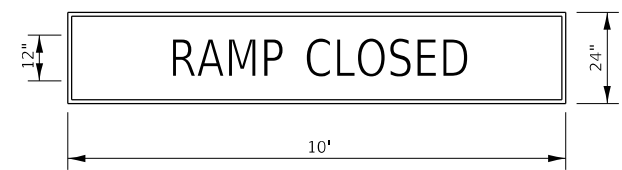
SYMBOLS

- ☐ DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ☐ TYPE III BARRICADE WITH FLASHING LIGHT



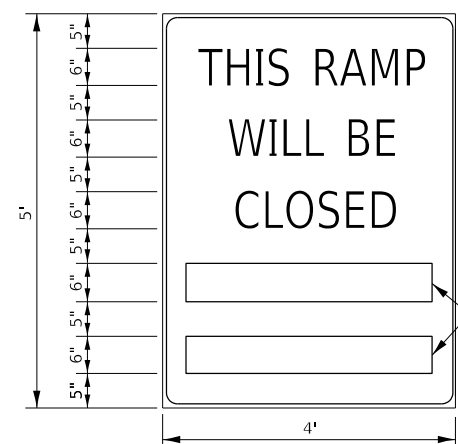
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE REFLECTORIZED BACKGROUND
1" BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIST GUIDE SIGNS FOR THE CLOSED EXIT RAMPS

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE REFLECTORIZED BACKGROUND

1/2" BORDER

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

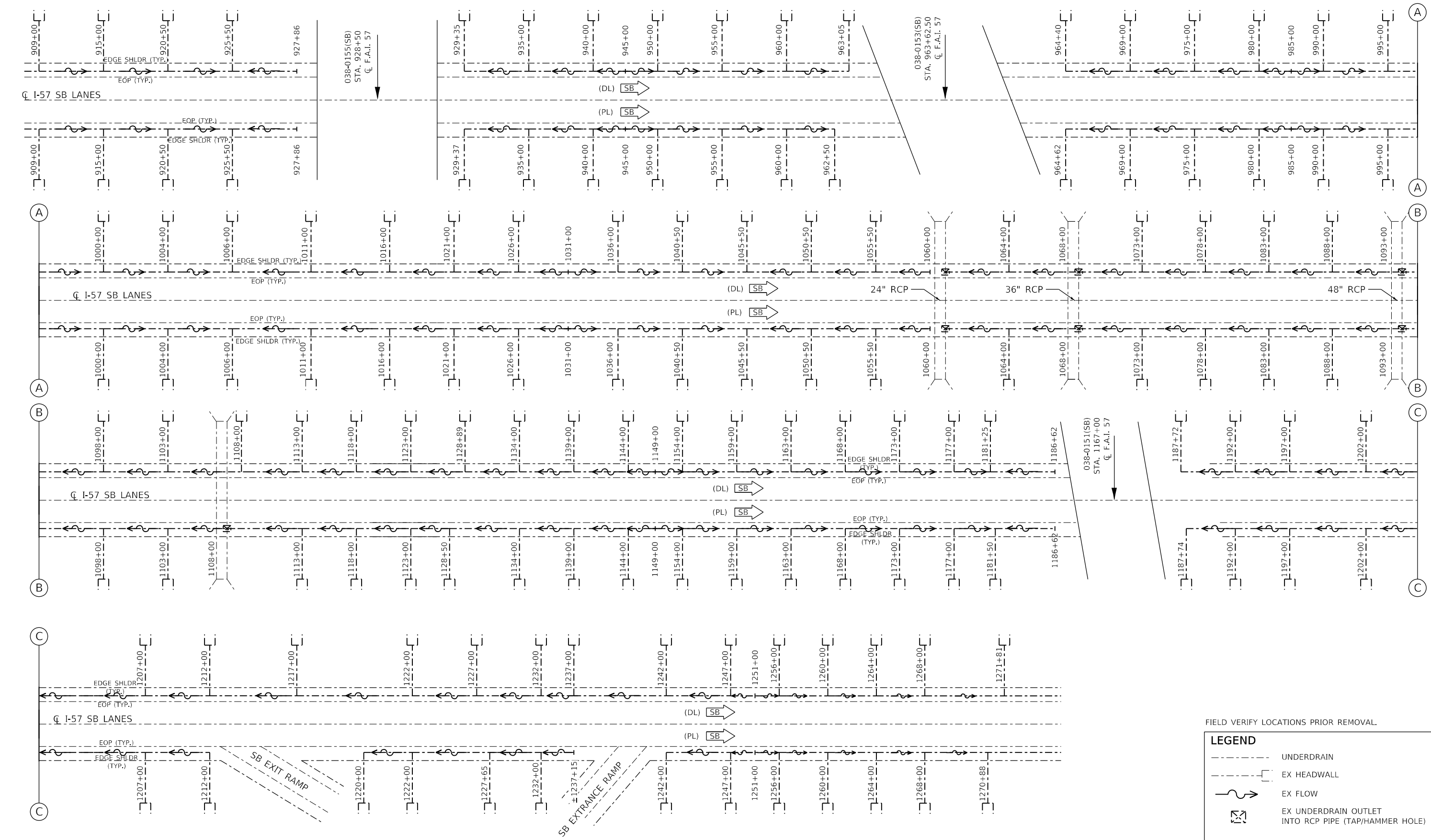
THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

GENERAL NOTES:

- ① STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ② A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ③ ALL ROUTE MARKERS AND SIGN ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ④ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701451, SPECIAL.
- ⑤ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF OPERATIONS IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑥ CONTRACTOR TO NOTIFY THE ENGINEER 21 DAYS PRIOR RAMP CLOSURES.
- ⑦ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑧ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

EXISTING UNDERDRAIN (SB LANES)

DRAWING NOT TO SCALE



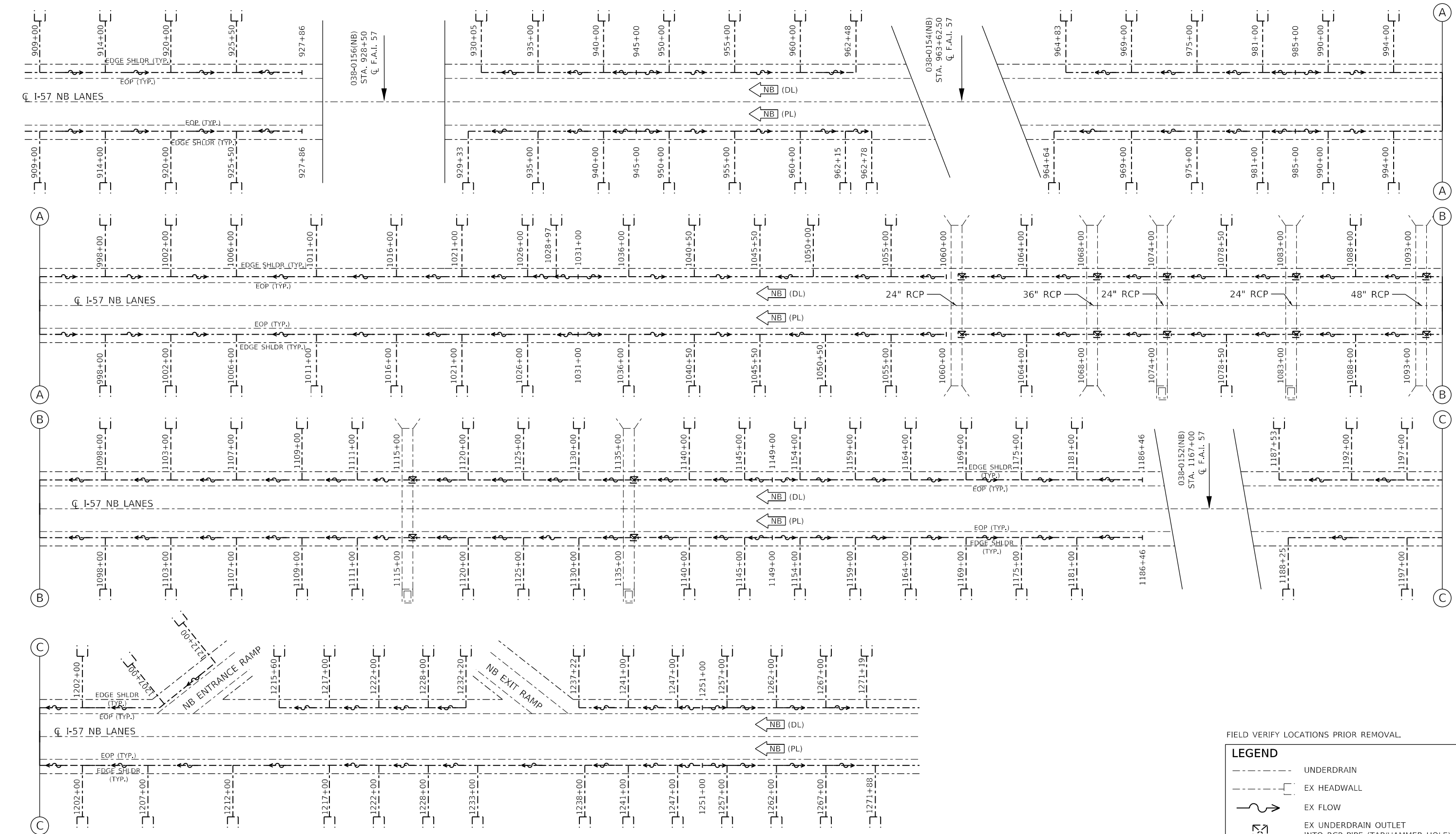
FIELD VERIFY LOCATIONS PRIOR REMOVAL.

LEGEND	
	UNDERDRAIN
	EX HEADWALL
	EX FLOW
	EX UNDERDRAIN OUTLET INTO RCP PIPE (TAP/HAMMER HOLE)

FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING UNDERDRAIN INFORMATION (1984 AS-BUILTS)				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
es:\pw_work\pwwdofcalderon\0521537\0366H63-eh-detailed.dgn		DRAWN -	REVISED -		57	(38-7,38-6)RS	IROQUOIS	130	115				
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63								
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS	FED. AID PROJECT

EXISTING UNDERDRAIN (NB LANES)

DRAWING NOT TO SCALE

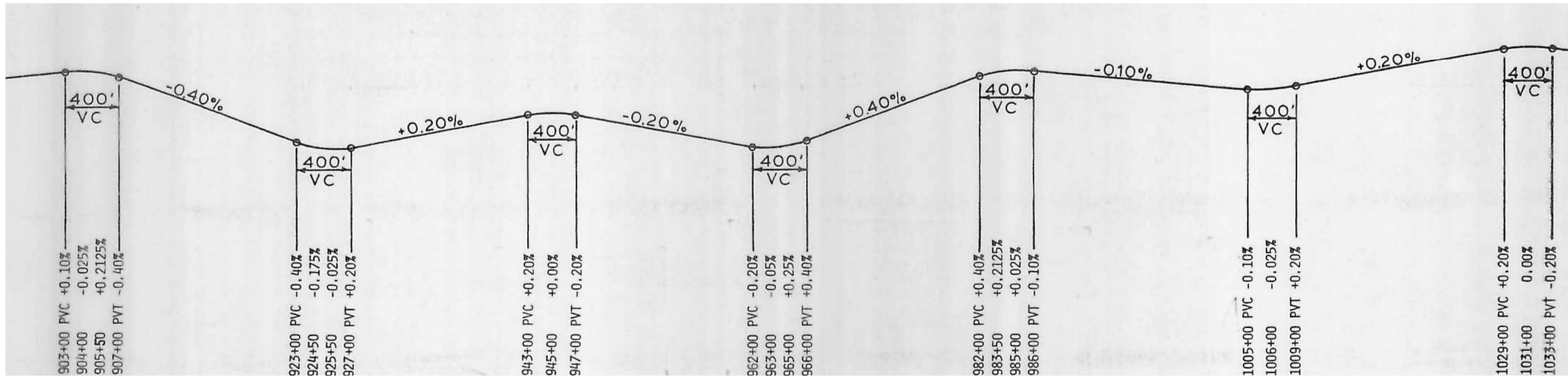


FIELD VERIFY LOCATIONS PRIOR REMOVAL.

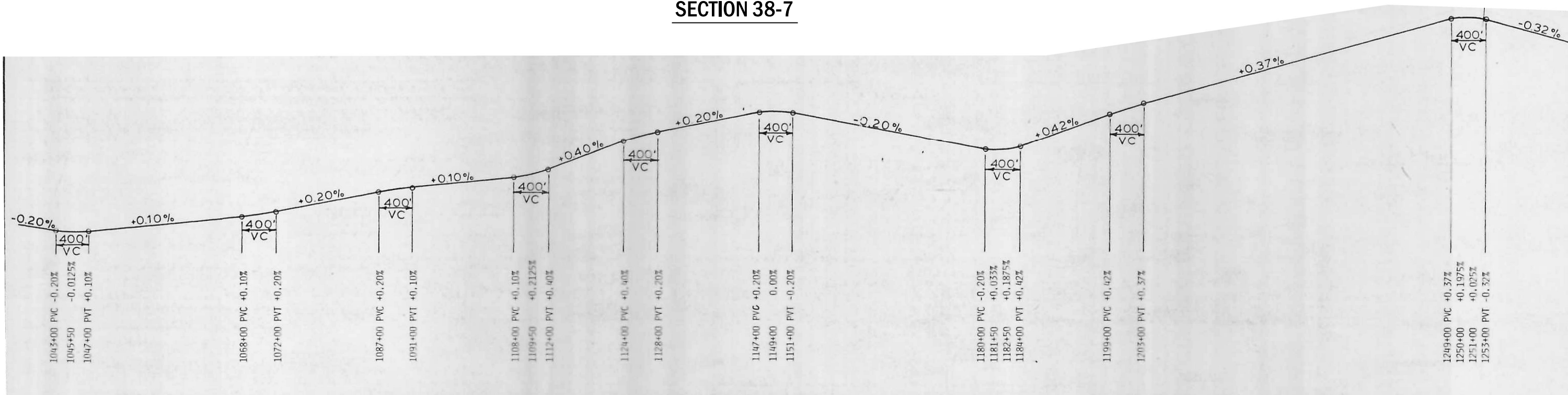
LEGEND	
	UNDERDRAIN
	EX HEADWALL
	EX FLOW
	EX UNDERDRAIN OUTLET INTO RCP PIPE (TAP/HAMMER HOLE)

FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING UNDERDRAIN INFORMATION (1984 AS-BUILTS)				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SMODELNAMES	PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -						57	(38-7,38-6)RS	IROQUOIS	130	116
	PLOT DATE = 12/17/2021	CHECKED -	REVISED -		CONTRACT NO. 66H63				ILLINOIS FED. AID PROJECT				
		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.		

**MAINLINE PAVEMENT GRADES
SECTION 38-6**



**MAINLINE PAVEMENT GRADES
SECTION 38-7**



FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING PAVEMENT GRADES
FOR INFORMATION ONLY (1984 AS-BUILTS)**

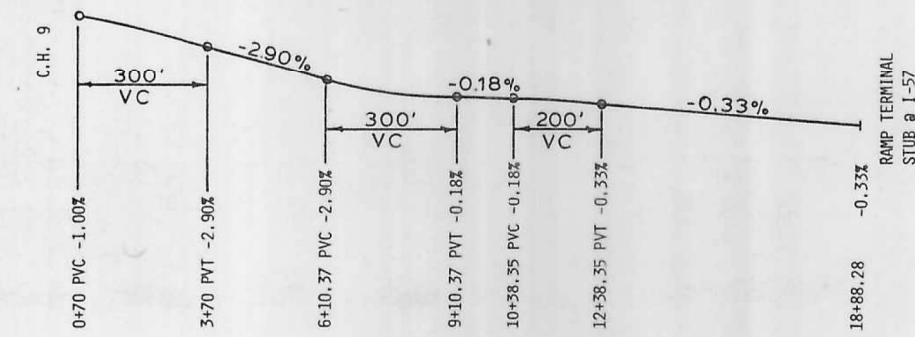
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	117
CONTRACT NO. 66H63				
ILLINOIS		FED. AID PROJECT		

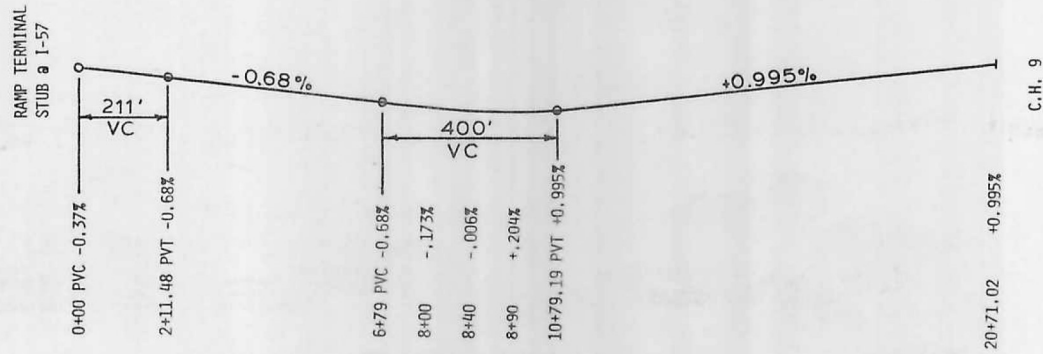
BUCKLEY INTERCHANGE RAMP PAVEMENT GRADES

EXISTING RAMP PAVEMENT GRADES (%)

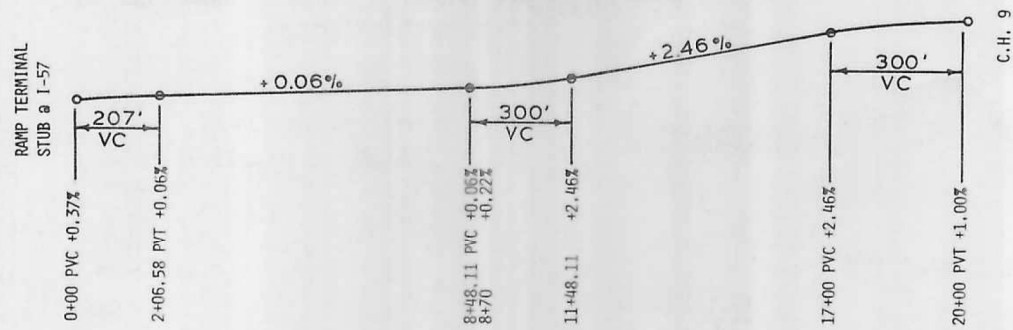
RAMP A
NBL ENTRANCE



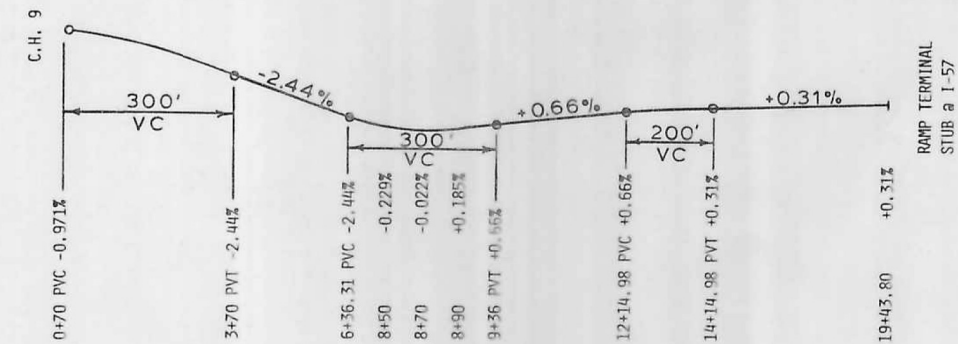
RAMP B
NBL EXIT



RAMP C
SBL EXIT



RAMP D
SBL ENTRANCE



FILE NAME =	USER NAME = calderon	DESIGNED -	REVISED -
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

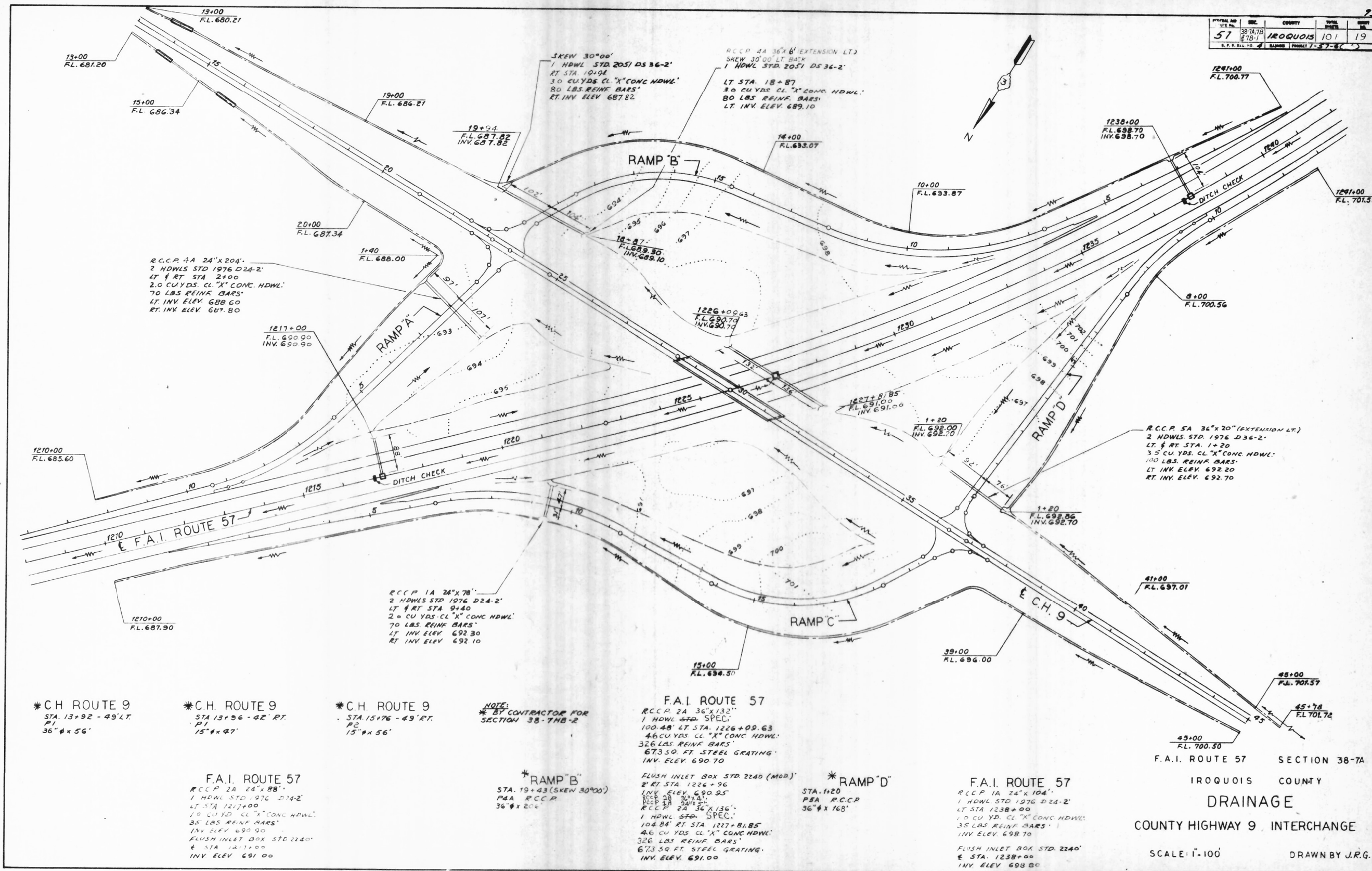
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING PAVEMENT GRADES
FOR INFORMATION ONLY (1984 AS-BUILTS)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	118
CONTRACT NO. 66H63				
		ILLINOIS	FED. AID PROJECT	

FOR INFORMATION ONLY



PROJECT NO.	57	SHEET NO.	119
SECTION	38-7A	COUNTY	IROQUOIS
TOTAL SHEETS	101	DATE	19

*CH ROUTE 9
STA. 13+92 - 49' LT.
P1
36" x 56"

*CH ROUTE 9
STA. 13+96 - 42' RT.
P1
15" x 47"

*CH ROUTE 9
STA. 15+76 - 49' RT.
P2
15" x 56"

NOTE:
* BY CONTRACTOR FOR SECTION 38-7HB-2

*RAMP "B"
STA. 19+43 (SKEW 30°00')
P4A RCCP
36" x 20"

F.A.I. ROUTE 57
RCCP 2A 24" x 88"
1 HDWL STD. 1976 D24-2'
LT STA. 1217+00
1.0 CU YD. CL "X" CONC HDWL.
35 LBS REINF BARS
INV. ELEV. 690.90
FLUSH INLET BOX STD. 2240"
STA. 1217+00
INV. ELEV. 691.00

F.A.I. ROUTE 57
RCCP 2A 36" x 132"
1 HDWL STD. SPEC.
100.48' LT. STA. 1226+09.63
4.6 CU YDS. CL "X" CONC HDWL.
326 LBS REINF BARS
673.50 FT. STEEL GRATING
INV. ELEV. 690.70

*RAMP "D"
STA. 1+20
P5A RCCP
36" x 168"

F.A.I. ROUTE 57
RCCP 1A 24" x 104"
1 HDWL STD. 1976 D24-2'
LT STA. 1238+00
1.0 CU YD. CL "X" CONC HDWL.
35 LBS REINF BARS
INV. ELEV. 698.70
FLUSH INLET BOX STD. 2240"
STA. 1238+00
INV. ELEV. 698.80

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

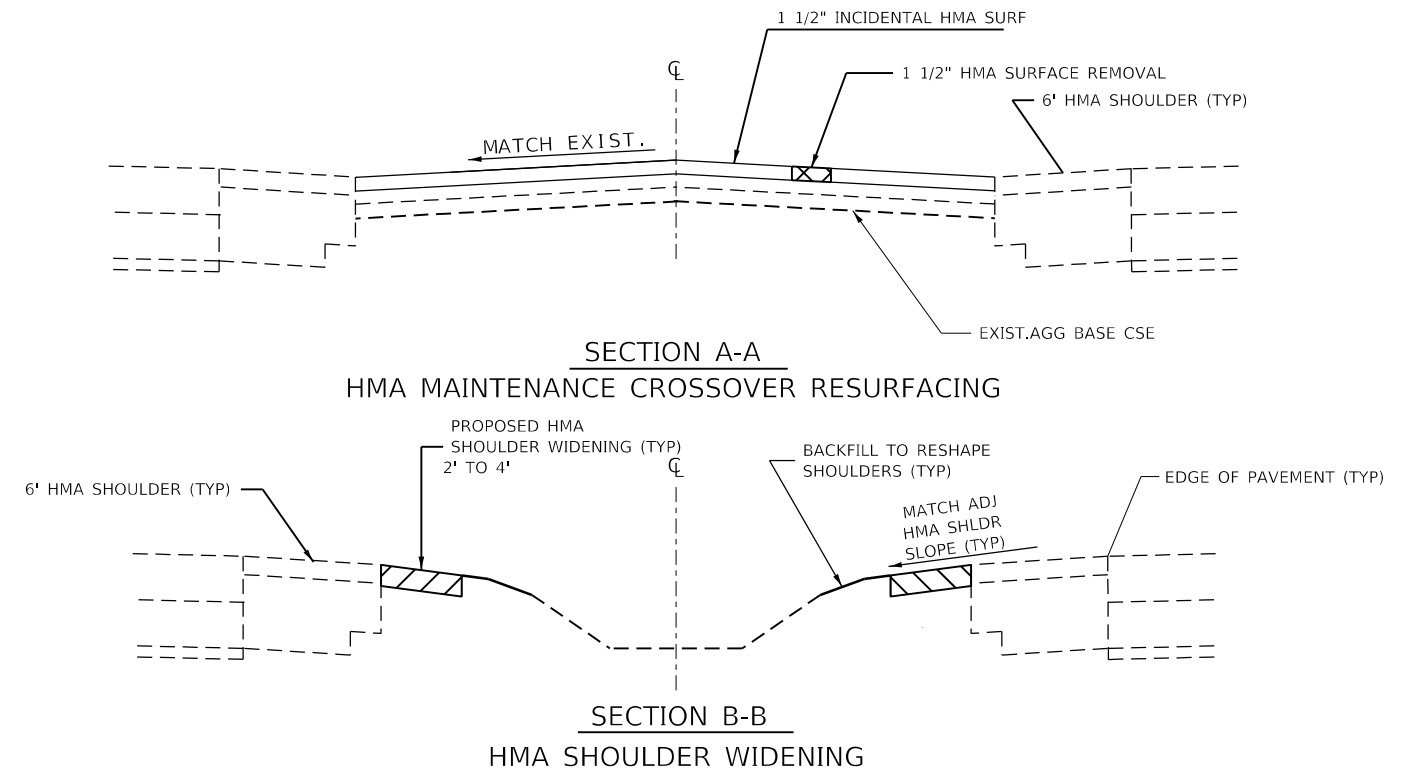
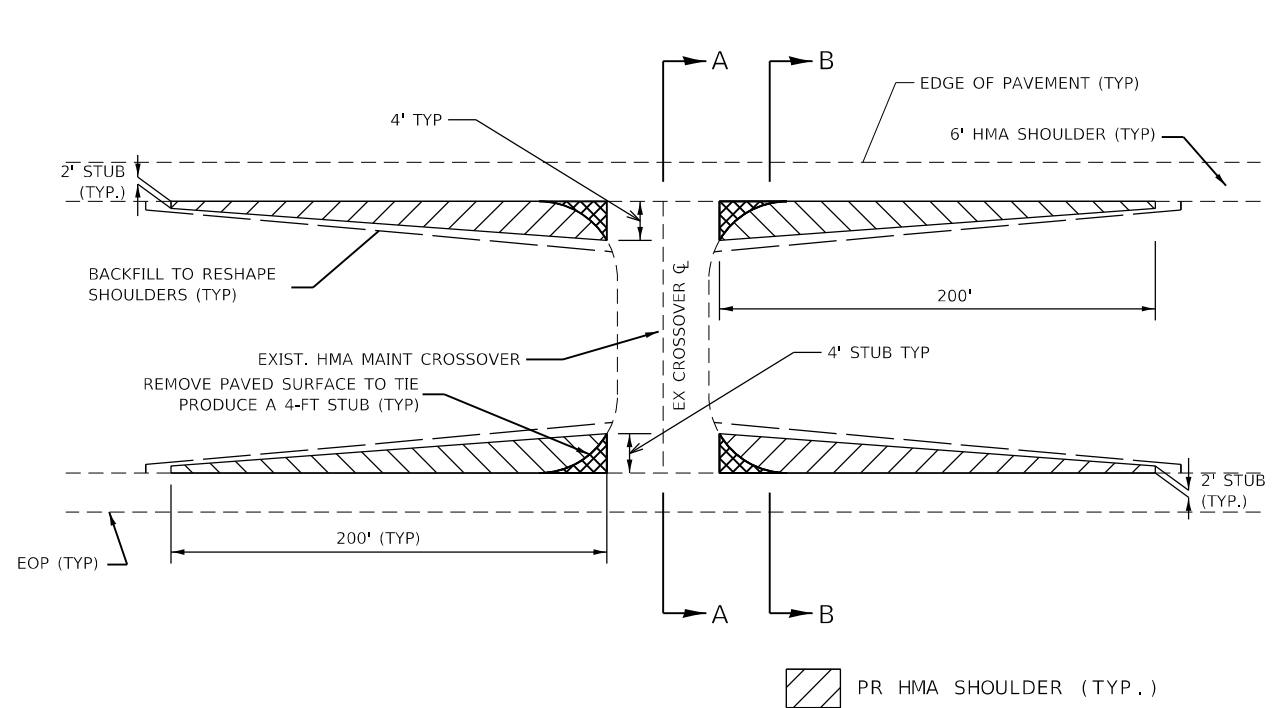
CH 9 EXISTING DRAINAGE DETAILS
FOR INFORMATION ONLY

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	119
CONTRACT NO. 66H63				

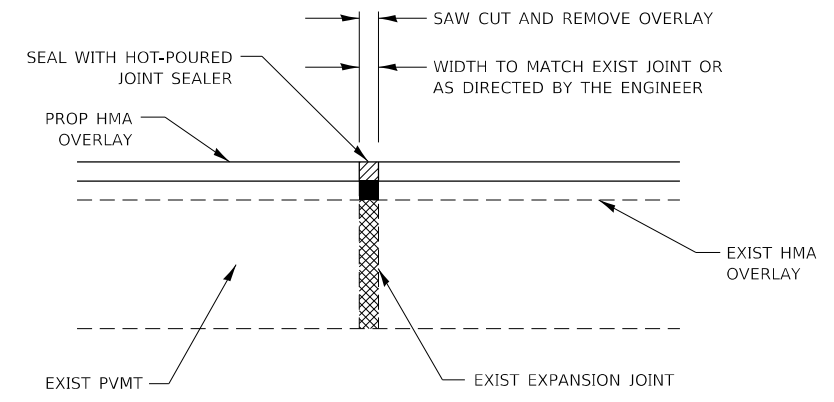
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	PLOT DATE = 12/17/2021	DATE -	REVISED -

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT



HMA MAINTENANCE CROSSOVER RESURFACING & SHOULDER WIDENING

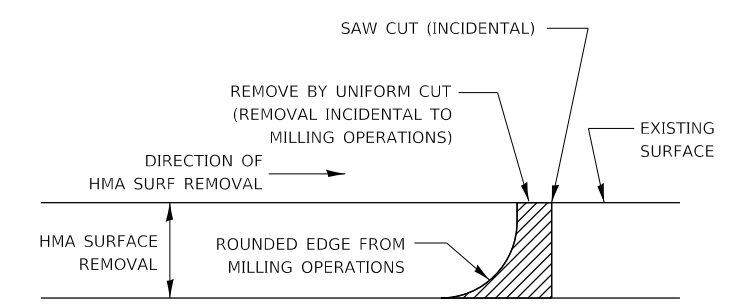


EXPANSION JOINT REHABILITATION DETAIL

GENERAL NOTES:

1. THE NEW HMA OVERLAY SHALL BE SAWED, REMOVED AND THE JOINT AREA CLEANED. PRIOR TO PLACING THE HOT-POURED JOINT SEALER, THE JOINT SHALL BE BLOWN OUT WITH COMPRESSED AIR.
2. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR EXPANSION JOINT REHABILITATION.

406-7



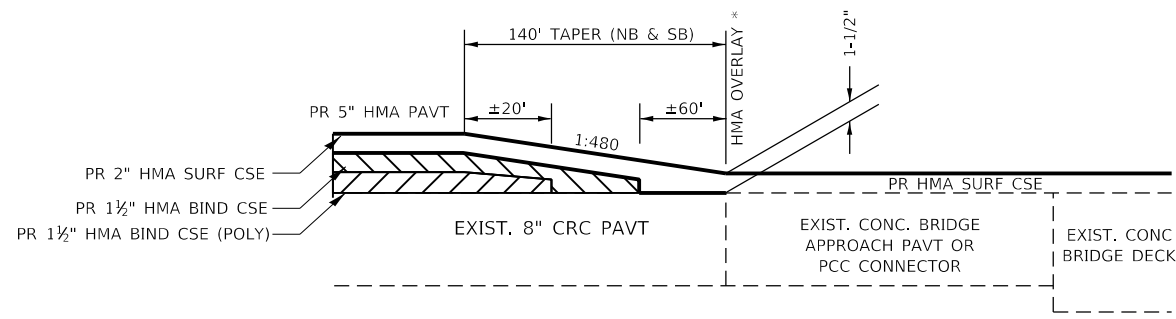
NOTE:

WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

HMA DETAIL AT BUTT JOINTS

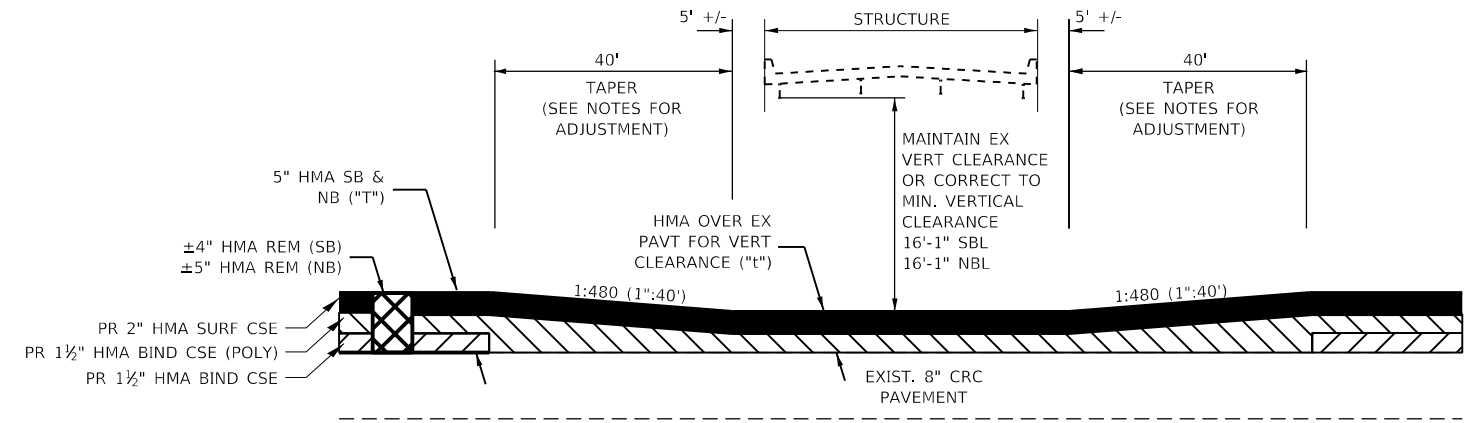
406-8

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ec:\pw_work\pwt\calderont\0521537\1D366H63-eh-deta.dgn	DRAWN -	REVISED -	57			(38-7,38-6)RS	IROQUOIS	130	120		
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.



TAPER DETAIL AT STRUCTURES WITH PR HMA OVERLAY

SN 038-0151 (SB) STA 1186+34 * TO STA 1187+82 *
 SN 038-0152 (NB) STA 1186+19 * TO STA 1187+66 *



PR HMA TAPER DETAIL UNDER STRUCTURES

TO BE USED WHERE FIELD CONDITIONS JUSTIFY THE USE OF HMA TAPERS TO MAINTAIN OR IMPROVE VERTICAL CLEARANCE UNDER OVERHEAD STRUCTURES

$["T" - "t"] \times 40 \text{ ft/in} = Lt$
 T = Prop HMA thickness
 t = Required thickness of HMA overlay to maintain min vertical clearance
 Lt = Length of HMA taper

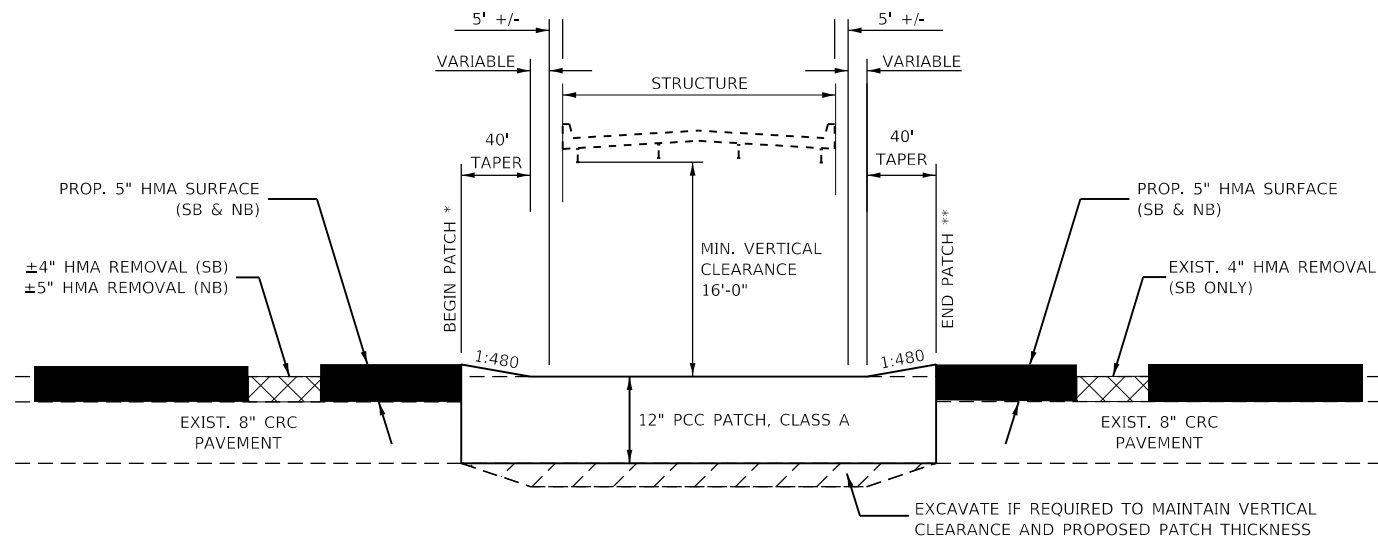
MODEL: \\MODELM\MRF5
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USER NAME = calderon	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/17/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TAPER DETAILS			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	121
			CONTRACT NO. 66H63	
		ILLINOIS FED. AID PROJECT		

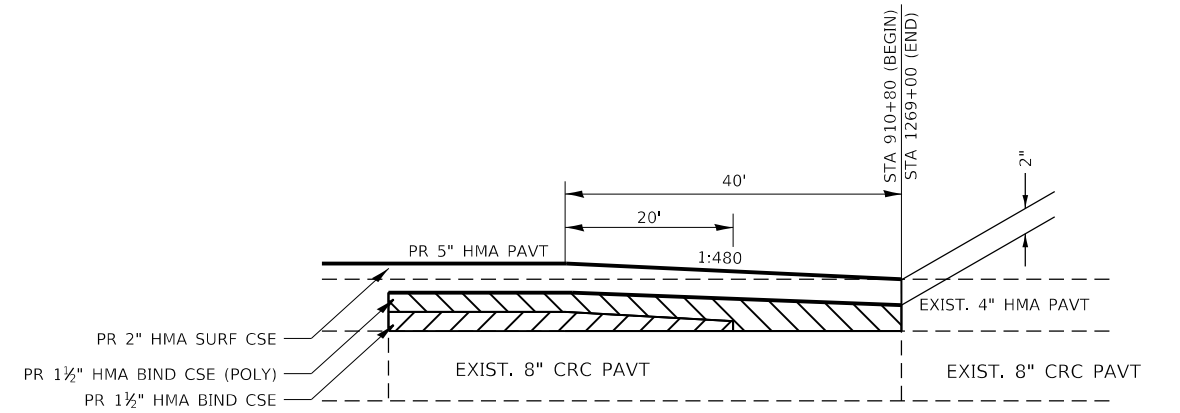


PR PCC PATCH DETAIL UNDER STRUCTURES

SOUTHBOUND
 SN 038-0082 * STA 938+17 TO ** STA 939+37
 SN 038-0132 * STA 1046+83 TO ** STA 1048+03

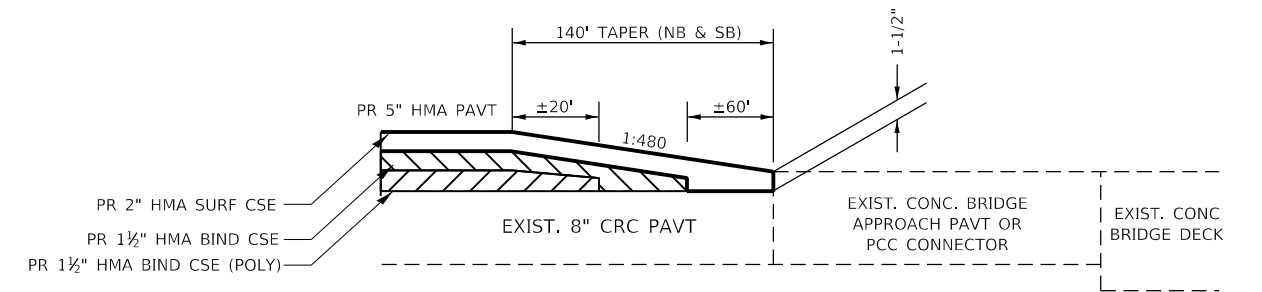
NORTHBOUND
 SN038-0132 * STA 1046+83 TO ** STA 1048+03
 SN 038-0076 * STA 1224+39 TO ** STA 1226+11

EXCAVATE IF REQUIRED TO MAINTAIN VERTICAL CLEARANCE AND PROPOSED PATCH THICKNESS

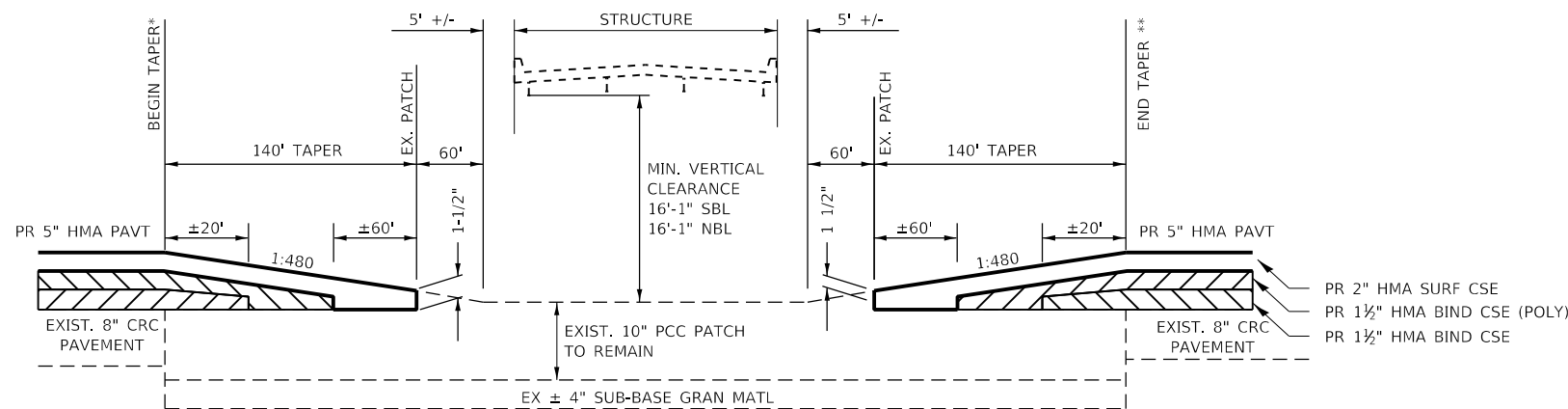


BEGINNING & ENDING TAPERS

SOUTHBOUND LANES ONLY



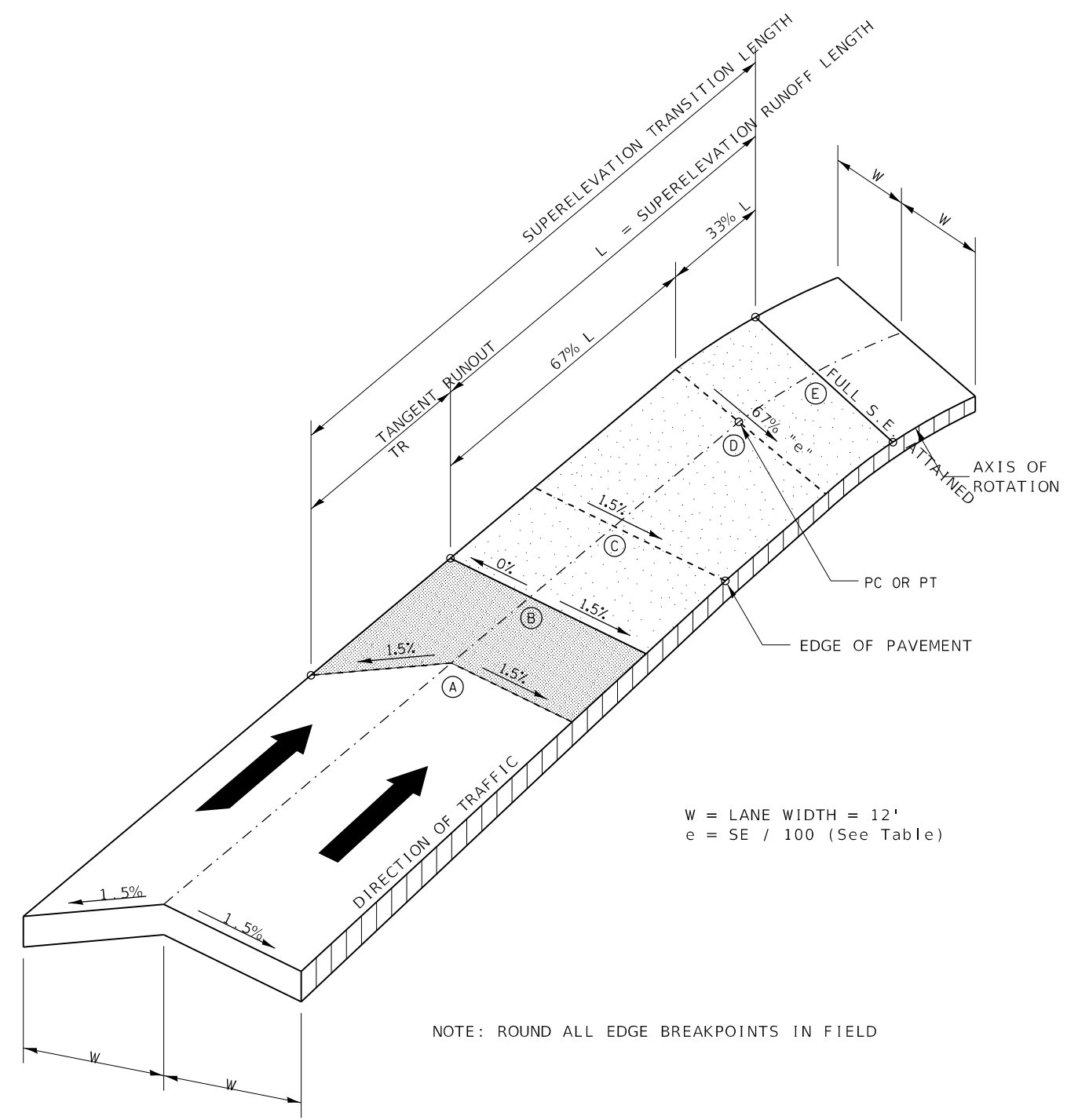
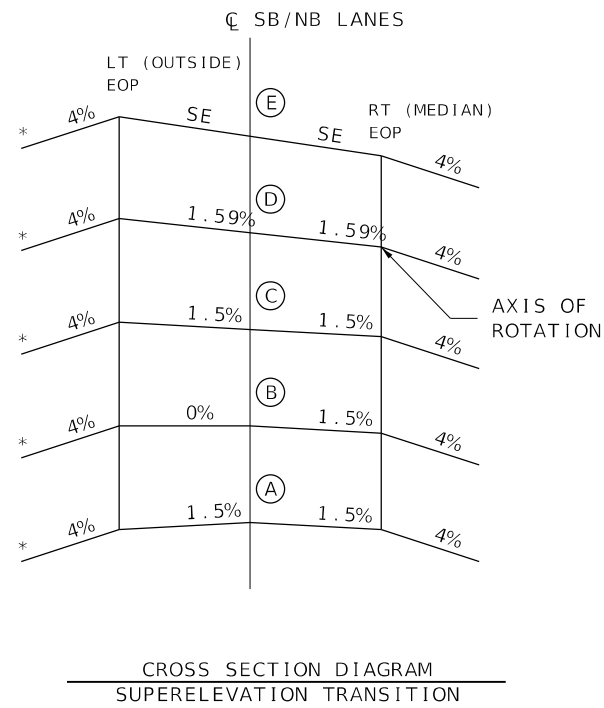
TAPER DETAIL AT STRUCTURES



EX PATCH & HMA TAPER DETAIL UNDER STRUCTURES

SN 038-0126
 SBL STA 1173+24* TO STA 1178+54 **
 NBL STA 1173+99* TO STA 1179+29 **

FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TAPER DETAILS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ec:\pw_work\pwidot\calderont\0521537\1D366H63-ehh-detai.dgn	DRAWN -	REVISED -	57					(38-7,38-6)RS	IROQUOIS	130	122	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63							
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	ILLINOIS



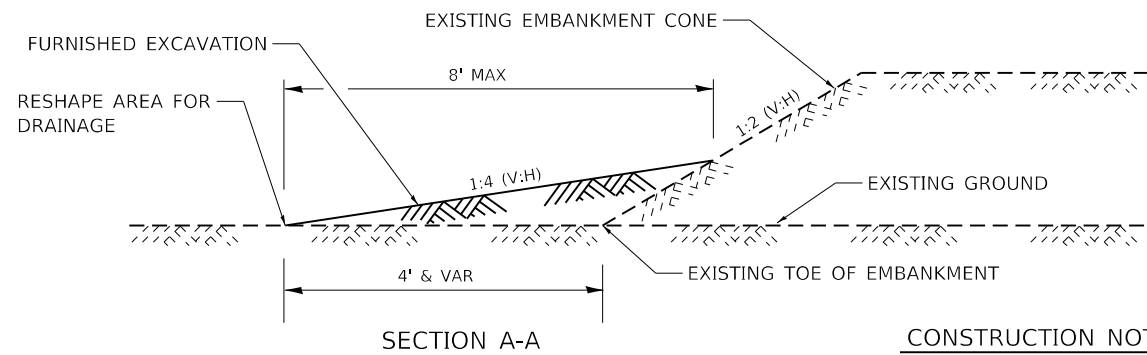
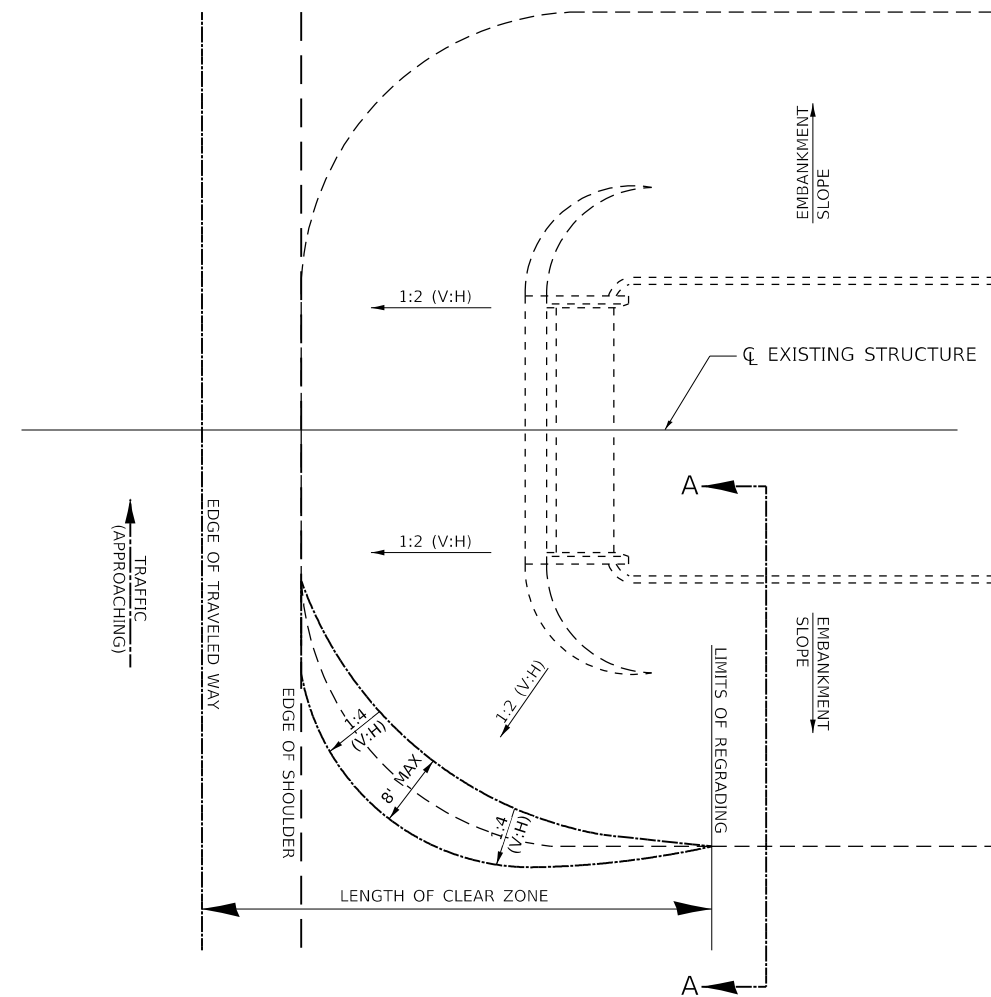
W = LANE WIDTH = 12'
e = SE / 100 (See Table)

NOTE: ROUND ALL EDGE BREAKPOINTS IN FIELD

CURVE	A	B	C	D	E	SE	TR	L
16	914+48.04 951+35.34	914+94.04 950+89.34	9158+40.04 950+43.34	915+78.03 950+05.35	916+02.03 949+63.35	2.8	46	126
17	1046+59.51 1074+59.71	1047+12.51 1074+06.71	1047+65.51 1073+53.71	1047+75.84 1073+43.38	1048+07.5 1073+11.72	2.1	53	95
18	1083+70.86 111125.12	1084+23.86 1110+72.12	1084+76.86 1110+19.12	1084+87.19 1110+08.79	1085+18.85 1109+77.13	2.1	53	95
19	1133+73.81 1181+55.25	1134+65.81 1180+63.25	1134+65.81 1180+63.25	1135+03.8 1180+25.26	1135+45.8 1179+83.26	2.8	46	126
20	1221+30.96 1268+62.12	1221+76.96 1268+16.12	1222+22.96 1267+70.12	1222+60.95 1267+32.13	1223+02.95 1266+90.13	2.8	46	126

ROUND POINTS TO NEAREST FOOT

420-4



FILLING AND REGRADING TOE OF EMBANKMENT CONE AT OVERHEAD STRUCTURES

CONSTRUCTION NOTES:

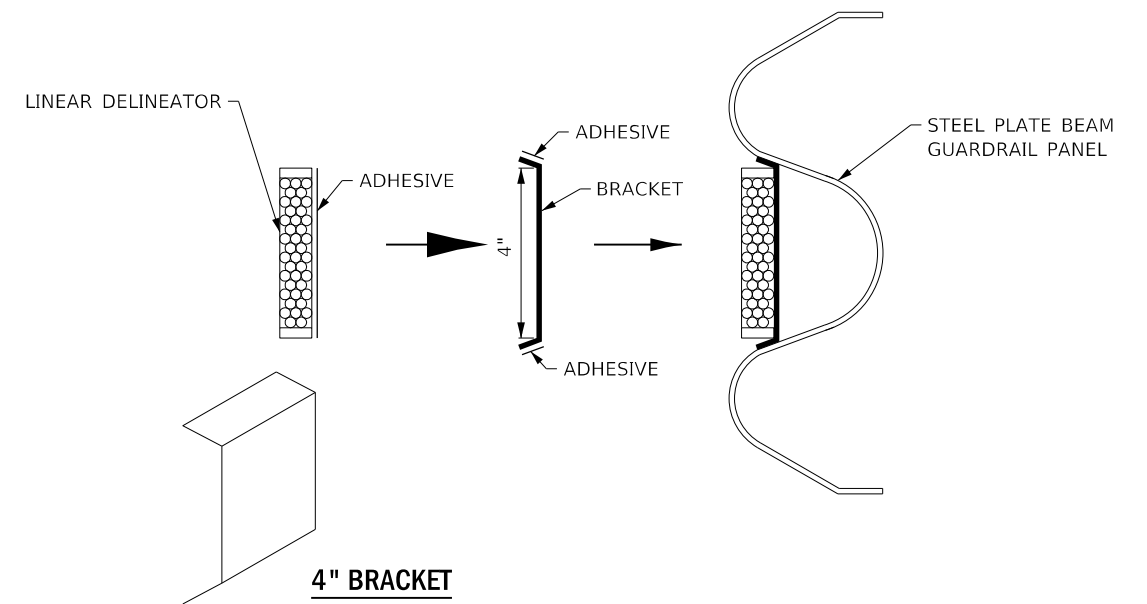
THE LIMITS OF FILLING AND REGRADING THE TOE OF EMBANKMENT CONE SHALL BE NO LESS THAN THE LENGTH OF THE PROJECT'S DESIGN CLEAR ZONE.

THE CLEAR ZONE TO BE USED IN THIS CONTRACT IS 30 FT FROM THE EDGE OF TRAVELED WAY (EOP).

DIMENSIONS MAY BE ADJUSTED BY THE ENGINEER IF FIELD CONDITIONS PERMIT.

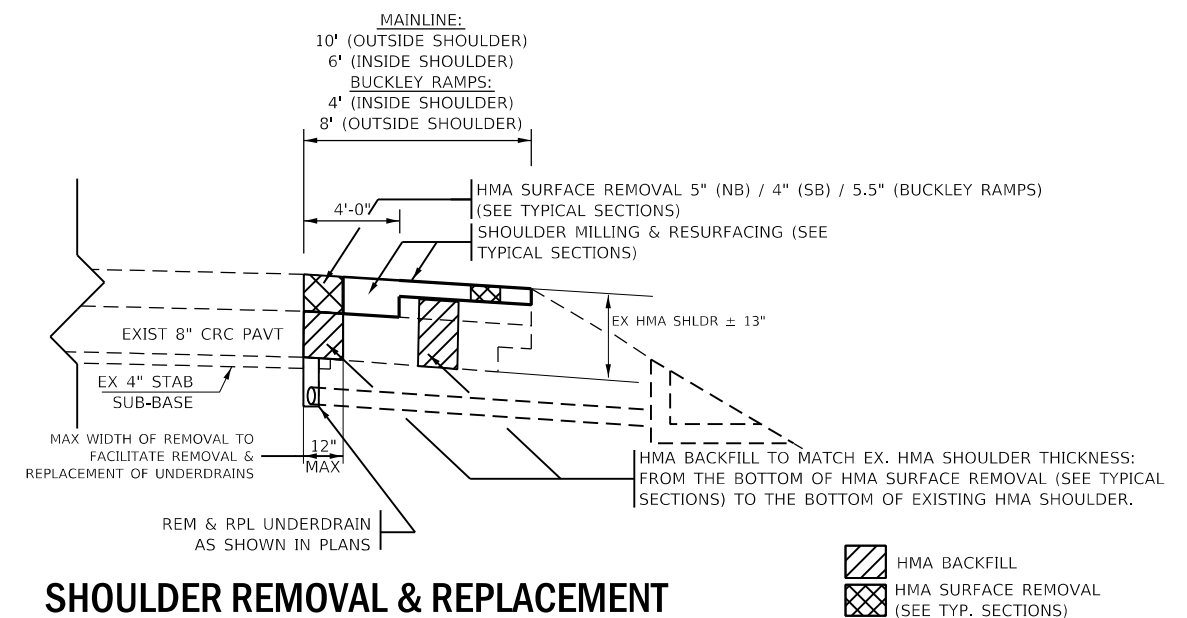
FILLING, COMPACTING AND REGRADING THE EMBANKMENT CONE SHALL BE INCLUDED IN THE COST OF FURNISHED EXCAVATION.

AREAS TO BE RESEDED WILL BE PAID SEPARATELY.



LINEAR DELINEATOR APPLICATION TO STANDARD GALVANIZED GUARDRAIL

LINEATOR DELINEATOR SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS



NOTES:

- *HMA BACKFILL SHALL BE USED TO MATCH THE EXISTING HMA SHOULDER THICKNESS.
- *MAX TRENCH WIDTH SHALL BE A MAXIMUM OF 12" UNLESS APPROVED BY THE ENGINEER TO FACILITATE REMOVAL OF UNDERDRAIN.
- *REMOVAL OF UNDERDRAIN SHALL NOT DAMAGE ORIGINAL PCC PAVEMENT.

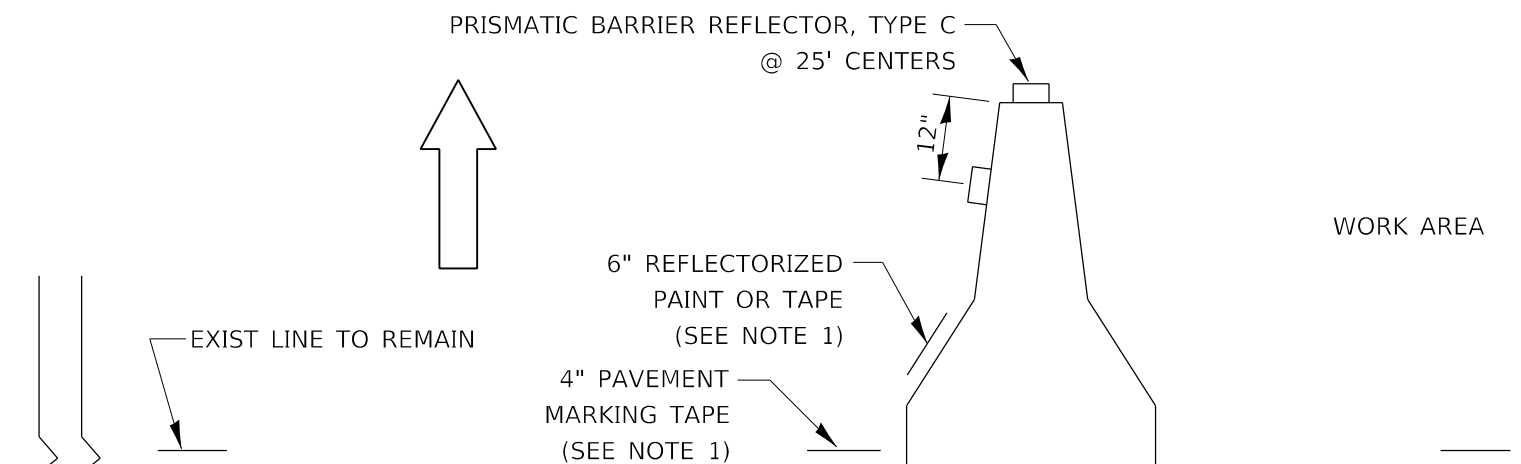
FILE NAME =	USER NAME = calderont	DESIGNED -	REVISED - LC 1/20/2021
ec:\pw_work\pwidofcalderont\0521537\0368H63-eh-deta.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
SMODELNAME\$	PLOT DATE = 1/20/2022	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:		SHEET OF SHEETS		STA. TO STA.	
		DETAILS			

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	124
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				

☉ RDWY.

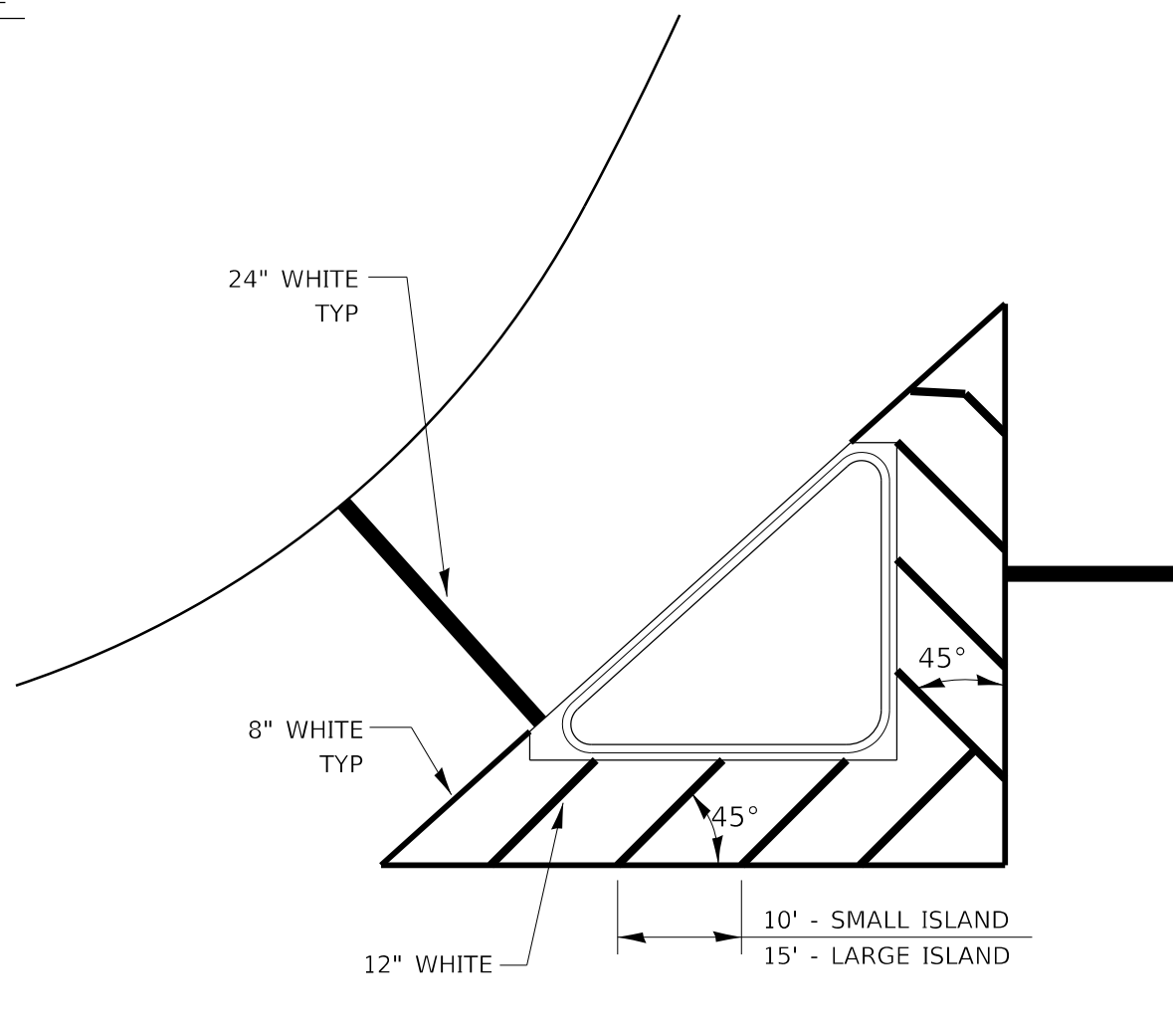


NOTES:

1. THE CONTRACTOR HAS THE OPTION OF USING EITHER THE LINE ON THE TEMPORARY CONCRETE BARRIER OR ON THE PAVEMENT.
2. THE COLOR OF THE REFLECTORS AND PAVEMENT/BARRIER MARKING LINE WILL VARY WITH STAGING AND SHALL MATCH THE EXISTING LINE IN THE WORK AREA.
3. THE COST OF THE REFLECTORS AND THE PAVEMENT/BARRIER MARKING LINE IS INCLUDED IN THE COST OF THE TEMPORARY CONCRETE BARRIER.

TRAFFIC CONTROL DETAIL
FOR TEMPORARY CONCRETE BARRIER

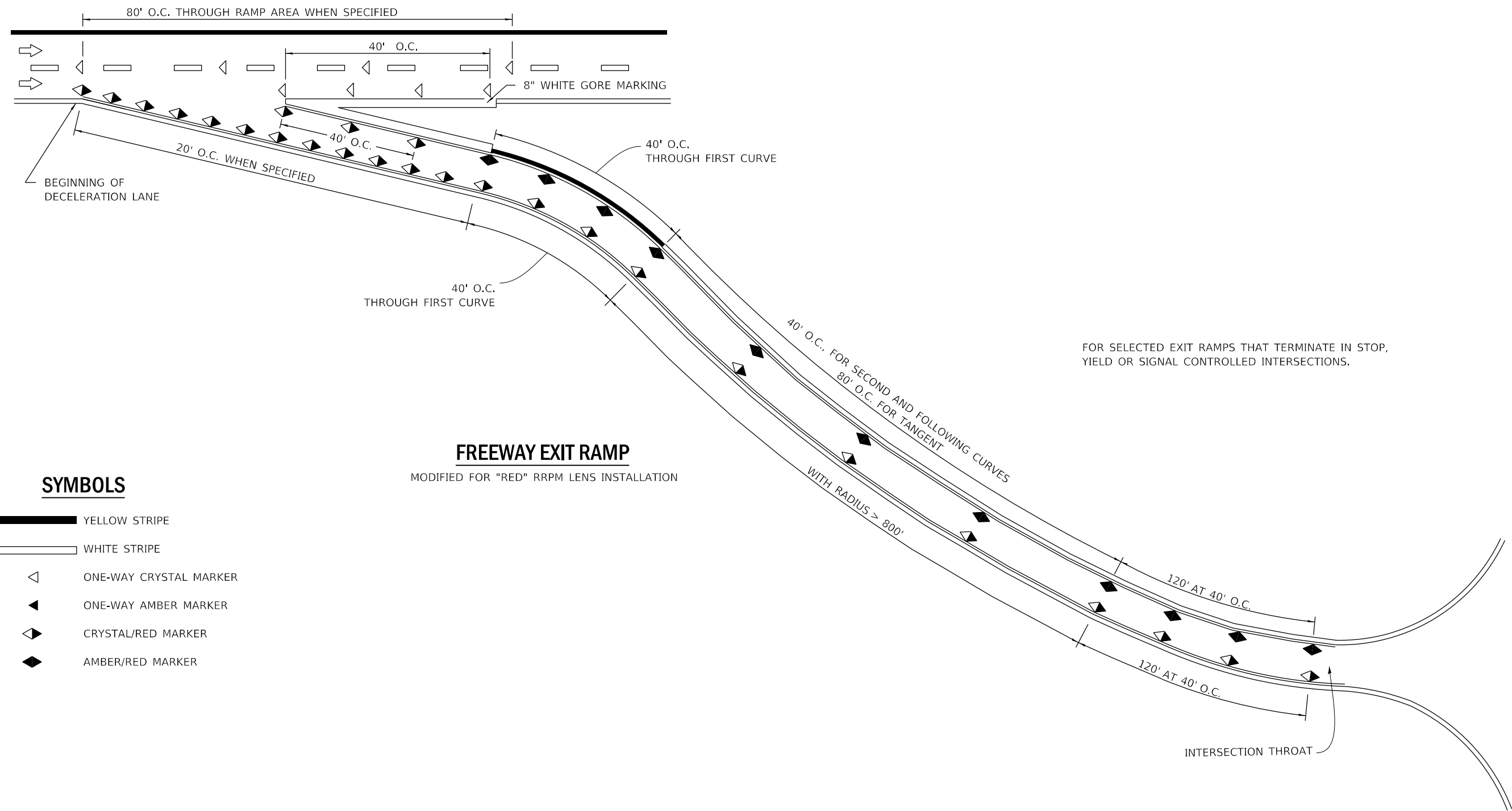
701-3









TYPICAL ISLAND

780-21

FILE NAME =	USER NAME = calderoni	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ec:\pw_work\pwidof\calderoni\0521537\1D366H63-eh-deta.dgn	DRAWN -	REVISED -	57					(38-7,38-6)RS	IROQUOIS	130	125	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63							
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



SYMBOLS

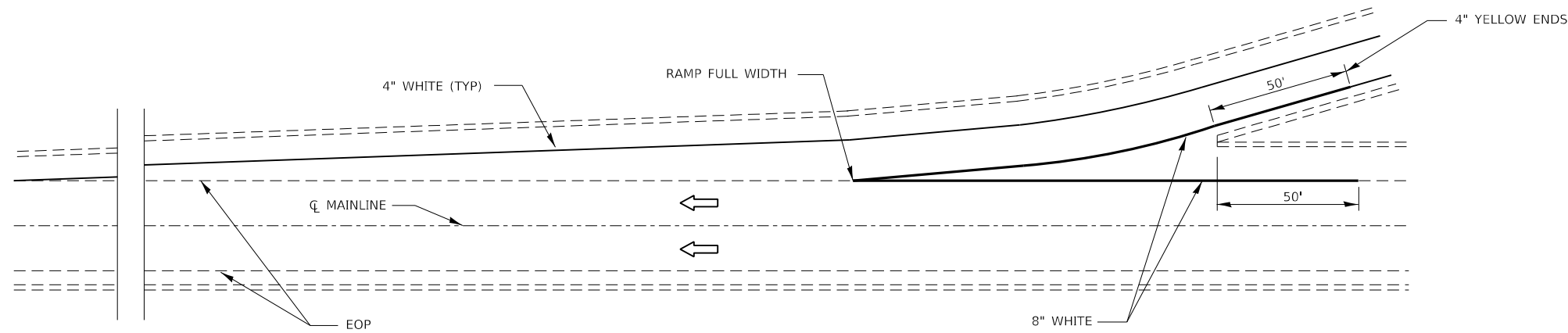
-  YELLOW STRIPE
-  WHITE STRIPE
-  ONE-WAY CRYSTAL MARKER
-  ONE-WAY AMBER MARKER
-  CRYSTAL/RED MARKER
-  AMBER/RED MARKER

FREEWAY EXIT RAMP
MODIFIED FOR "RED" RRPM LENS INSTALLATION

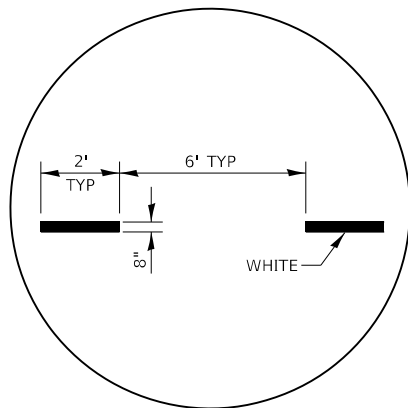
FOR SELECTED EXIT RAMP THAT TERMINATE IN STOP, YIELD OR SIGNAL CONTROLLED INTERSECTIONS.

781-2

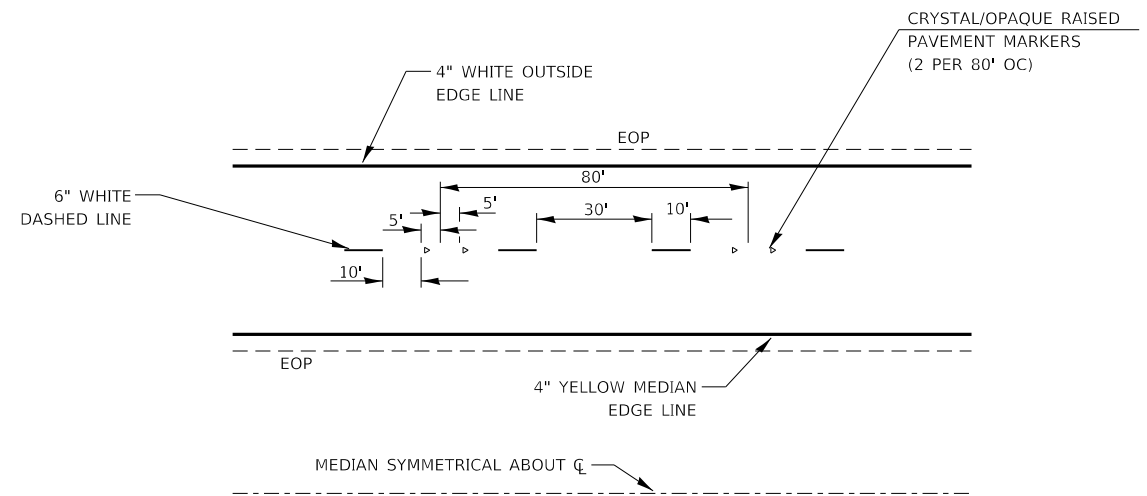
FILE NAME =	USER NAME = calderoni	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAISED REFLECTIVE PAVEMENT MARKERS AT FREEWAY EXIT RAMP				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ec:\pw_work\pwidof\calderoni\05215371\0366H63-ehd-deta.dgn	DRAWN -	REVISED -	REVISED -						57	(38-7,38-6)RS	IROQUOIS	130	126
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63				ILLINOIS	FED. AID PROJECT			
	PLOT DATE = 12/17/2021	DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.			



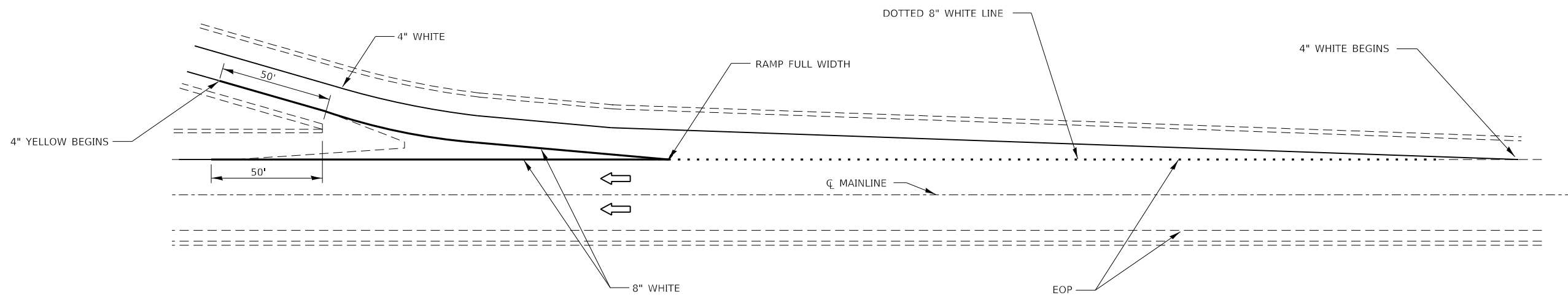
TYPICAL PAVEMENT MARKING FOR ENTRANCE RAMP TERMINALS



INTERSTATE RAMP TRANSITION LINE



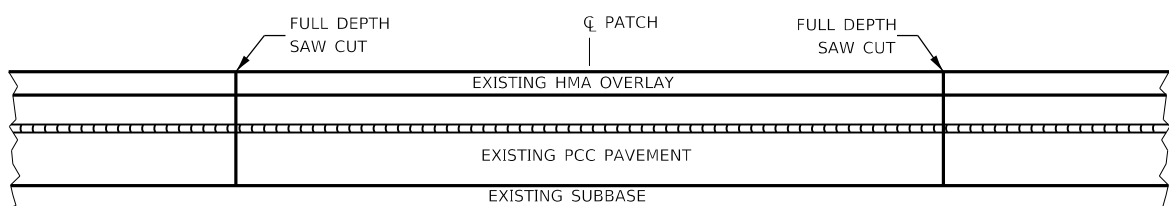
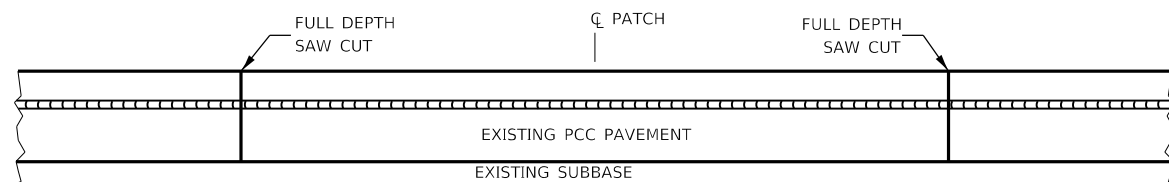
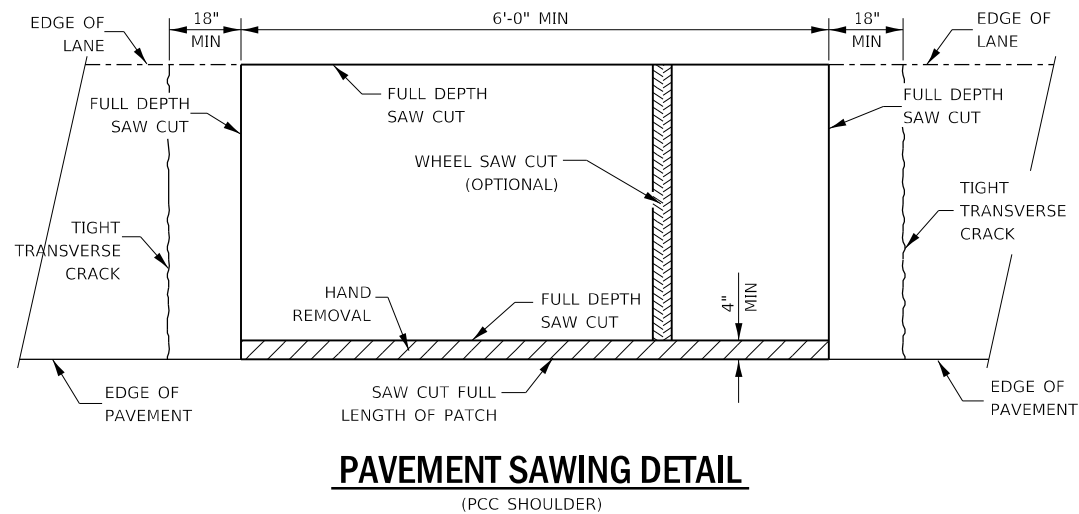
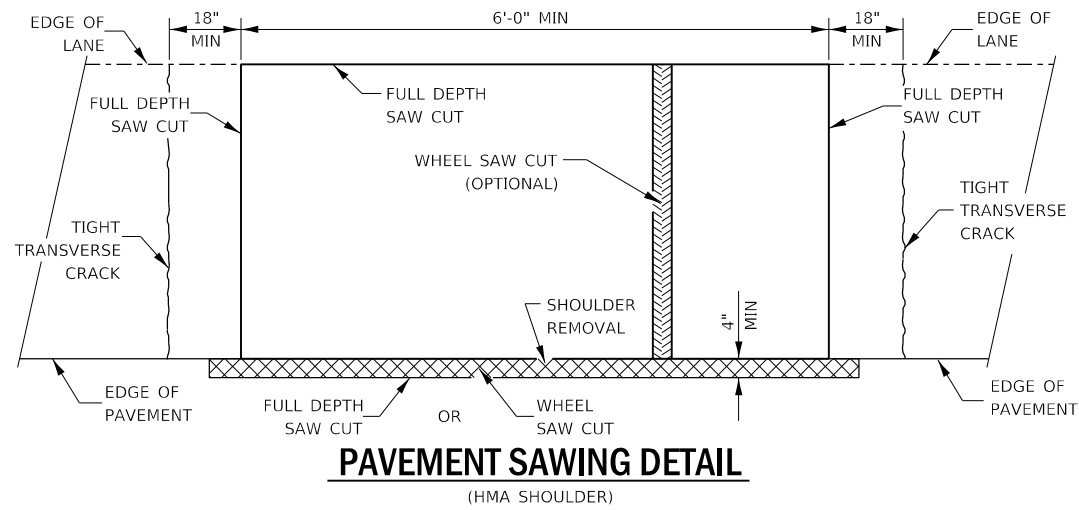
TYPICAL PAVEMENT MARKINGS



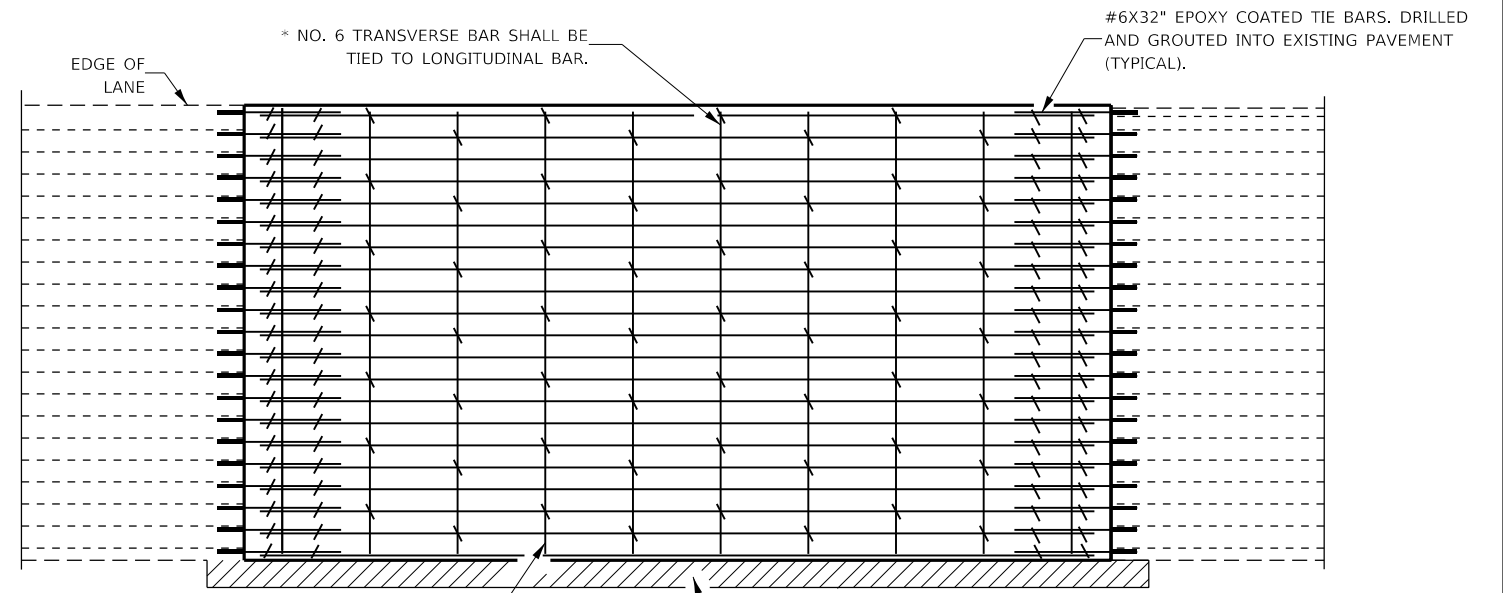
TYPICAL PAVEMENT MARKINGS FOR EXIT RAMP TERMINALS

780-12

FILE NAME =	USER NAME = calderoni	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS				F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ec:\pw_work\p\widof\calderoni\0521537\ID366H63-ehd-deta.dgn		DRAWN -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	57	(38-7,38-6)RS	IROQUOIS	130 127
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 66H63									
	PLOT DATE = 12/17/2021	DATE -	REVISED -		ILLINOIS FED. AID PROJECT									

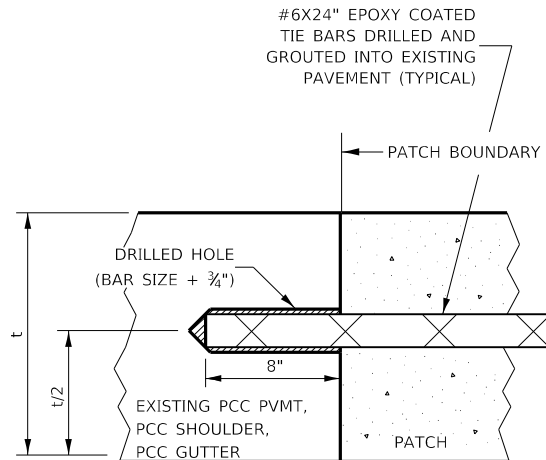


PAVEMENT SAWING DETAIL FOR CLASS A PATCHING (CRC PAVEMENT)



TRANSVERSE REBAR SHALL EXTEND TO OUTER LONGITUDINAL REBAR WHILE PROVIDING A MINIMUM 3" CLEARANCE FROM EXISTING PAVEMENT EDGE

PAVEMENT REINFORCEMENT DETAIL



LONGITUDINAL CONSTRUCTION JOINT

PATCHES MORE THAN 20' IN LENGTH SHALL BE TIED TO ADJACENT PAVEMENT, PCC SHOULDERS OR PCC CURB AND GUTTER WITH #6X24" EPOXY COATED TIE BARS AT 36" CENTERS.

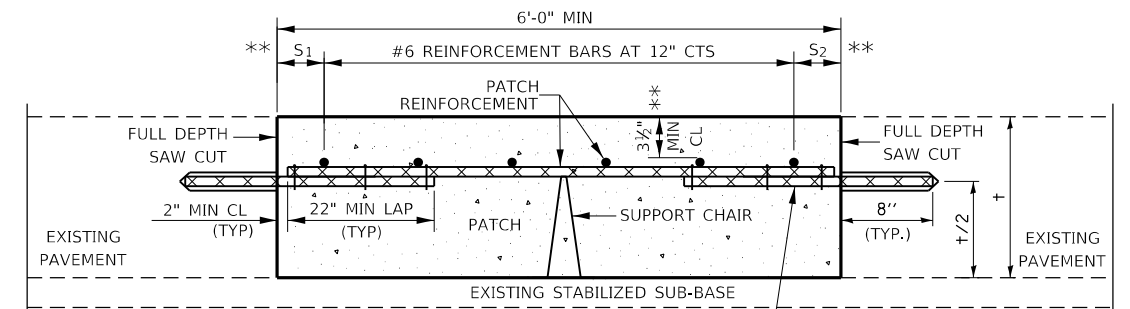
NOTES:

EXISTING LONGITUDINAL REINFORCEMENT SPACING; ±6".

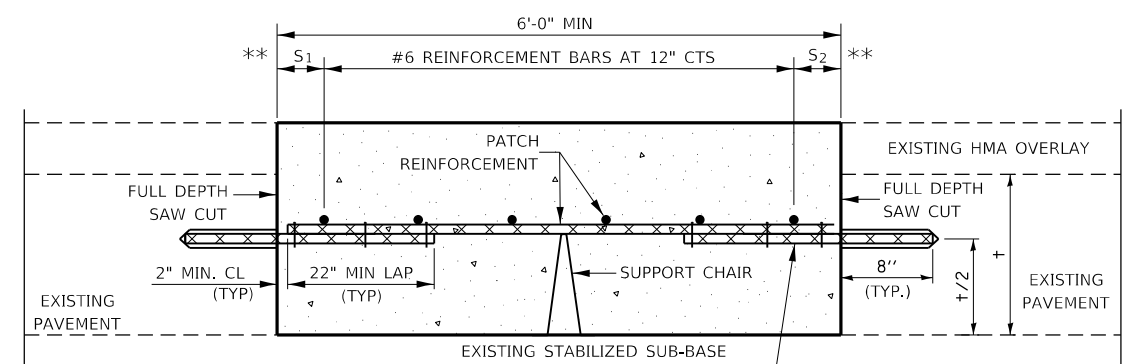
SPACING OF NEW LONGITUDINAL REINFORCEMENT AND NEW LONGITUDINAL TIE BARS SHALL MATCH SPACING OF EXISTING REINFORCEMENT AND SHALL BE TIED TOGETHER WITH A MINIMUM OF TWO TIES PER BAR.

* EVERY THIRD INTERSECTION MUST BE TIED. WHEN THE MINIMUM CLEARANCE CANNOT BE OBTAINED WITH THE TRANSVERSE REBAR ON TOP, THEN THE TRANSVERSE REBAR SHALL BE TIED TO THE BOTTOM OF THE LONGITUDINAL REBAR.

** VARIABLE: WHERE S₁ AND S₂ ARE 2 1/2" MIN. AND 12" MAX.



WITHOUT HMA OVERLAY



WITH HMA OVERLAY

PATCHING DETAIL FOR CLASS A PATCHING (CRC PAVEMENT)

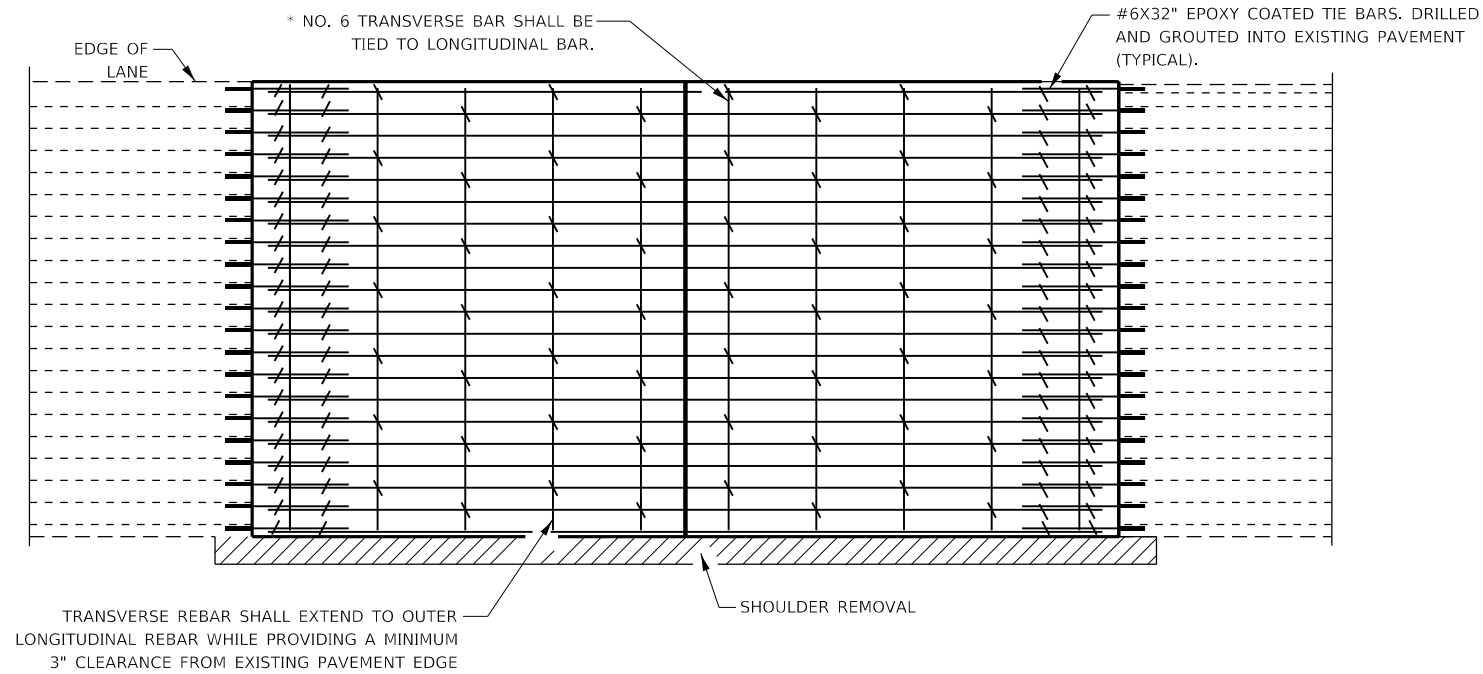
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SMODELNAME\$	PLOT DATE = 12/17/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

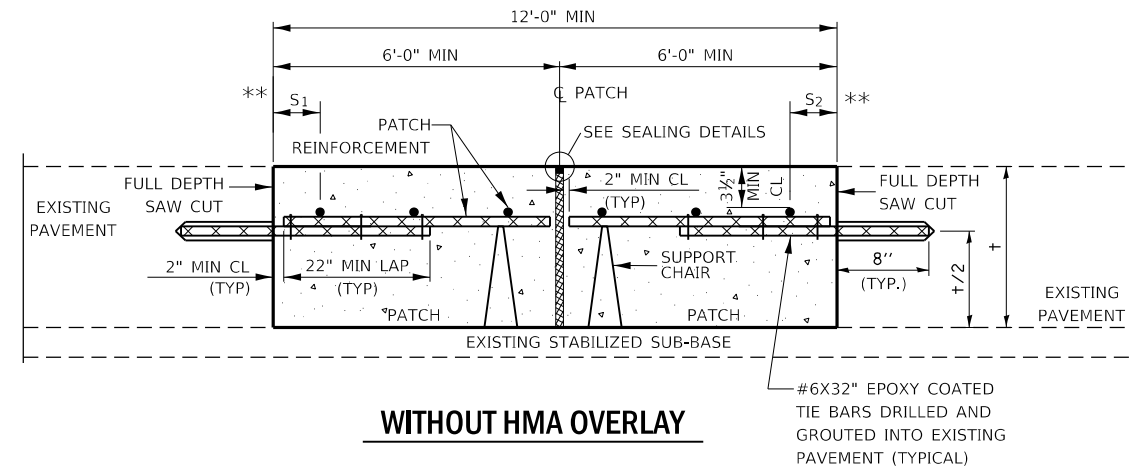
CLASS A PATCHING DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

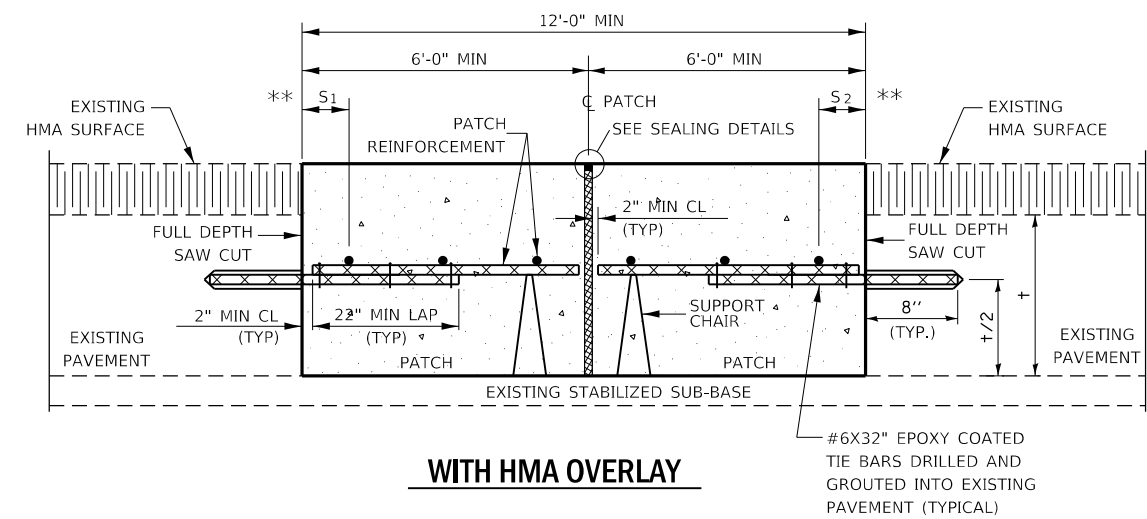
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	(38-7,38-6)RS	IROQUOIS	130	128
CONTRACT NO. 66H63				
ILLINOIS FED. AID PROJECT				



PAVEMENT REINFORCEMENT DETAIL

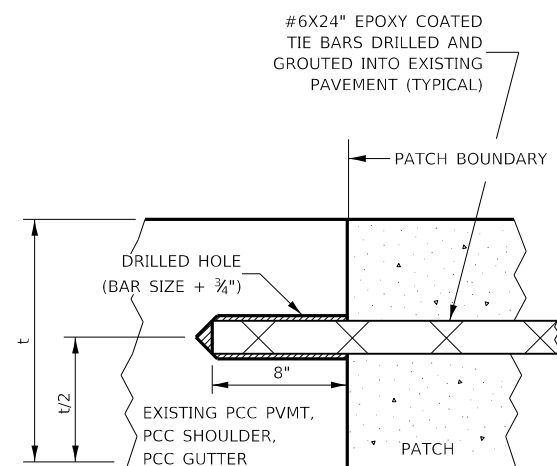


WITHOUT HMA OVERLAY



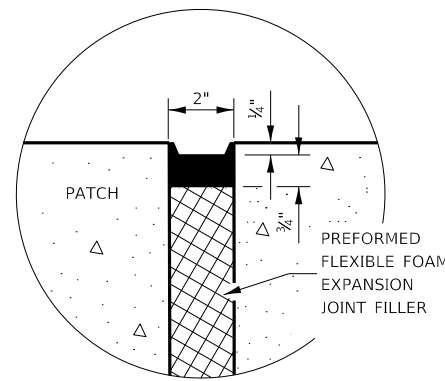
WITH HMA OVERLAY

CLASS A PATCH WITH EXPANSION JOINT

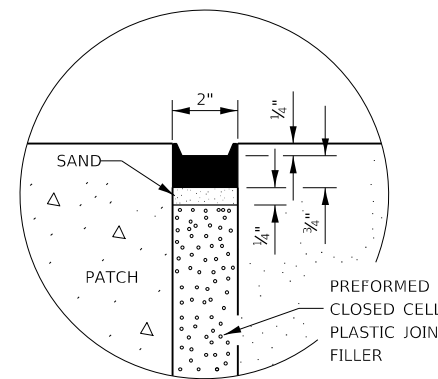


LONGITUDINAL CONSTRUCTION JOINT

PATCHES MORE THAN 20' IN LENGTH SHALL BE TIED TO ADJACENT PAVEMENT, PCC SHOULDERS OR PCC CURB AND GUTTER WITH #6X24" EPOXY COATED TIE BARS AT 36" CENTERS.



SEALING DETAIL



SEALING DETAIL

NOTES:

EXISTING LONGITUDINAL REINFORCEMENT SPACING; $\pm 6"$.

SPACING OF NEW LONGITUDINAL REINFORCEMENT AND NEW LONGITUDINAL TIE BARS SHALL MATCH SPACING OF EXISTING REINFORCEMENT AND SHALL BE TIED TOGETHER WITH A MINIMUM OF TWO TIES PER BAR.

* EVERY THIRD INTERSECTION MUST BE TIED. WHEN THE MINIMUM CLEARANCE CANNOT BE OBTAINED WITH THE TRANSVERSE REBAR ON TOP, THEN THE TRANSVERSE REBAR SHALL BE TIED TO THE BOTTOM OF THE LONGITUDINAL REBAR.

** VARIABLE: WHERE S_1 AND S_2 ARE $2\frac{1}{2}"$ MIN. AND 12" MAX.

FILE NAME =	USER NAME = calderoni	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CLASS A PATCHING DETAILS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ec:\pw_work\pwidot\calderoni\0521537\10368\63-eh-deta.dgn		DRAWN -	REVISED -			57	(38-7,38-6)RS	IROQUOIS	130	129	
SMODELNAME\$	PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

CONSTRUCTION NOTES:

TREE REMOVAL LIMITS AND LOCATION SHALL DETERMINED BY THE ENGINEER.

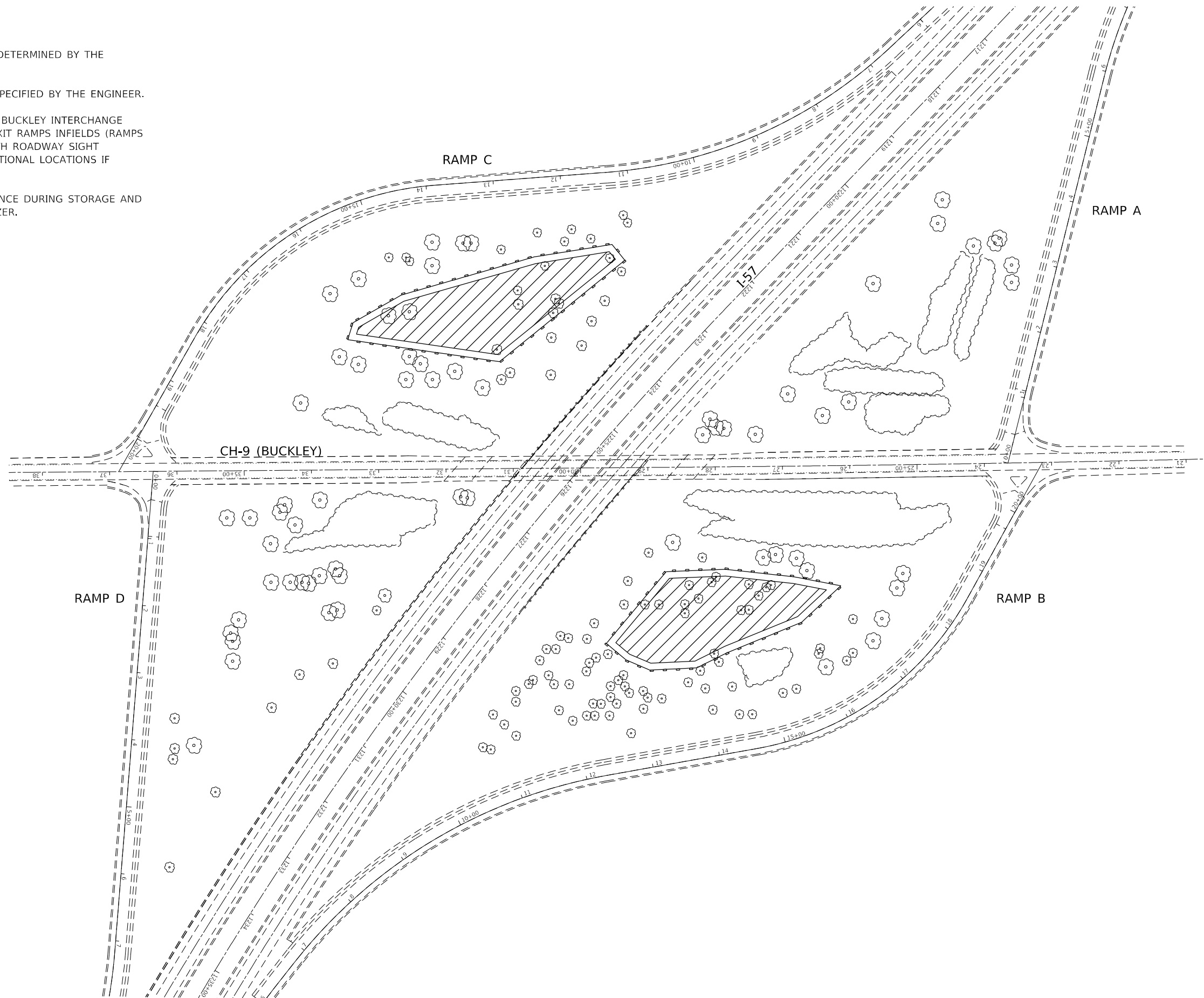
TOPSOIL SHALL BE REMOVED AT LOCATIONS SPECIFIED BY THE ENGINEER.

THE SURPLUS MATERIAL SHALL BE STORED IN BUCKLEY INTERCHANGE INFIELDS. IT IS RECOMMENDED TO USE THE EXIT RAMP INFIELDS (RAMPS C AND B) SINCE THEY DO NOT INTERFERE WITH ROADWAY SIGHT DISTANCE. THE ENGINEER MAY PROPOSE ADDITIONAL LOCATIONS IF REQUIRED.

THE AREA SHALL BE SURROUNDED BY SILT FENCE DURING STORAGE AND SHALL RESTORED WITH SEEDING AND FERTILIZER.

 EST. TOPSOIL REMOVAL AREA

 PERIMETER EROSION BARRIER



FILE NAME =	USER NAME = calderoni	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	STORAGE OF SURPLUS MATERIALS AT BUCKLEY INTERCHANGE INFIELDS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ec:\pw_work\pwid\calderoni\0521537\0366H63-eh-detailed.dgn	DRAWN -	REVISED -	57			(38-7,38-6)RS	IROQUOIS	130	130		
SMODELNAME\$	PLOT SCALE = 153,8128' / in.	CHECKED -	REVISED -			CONTRACT NO. 66H63					
	PLOT DATE = 12/17/2021	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.