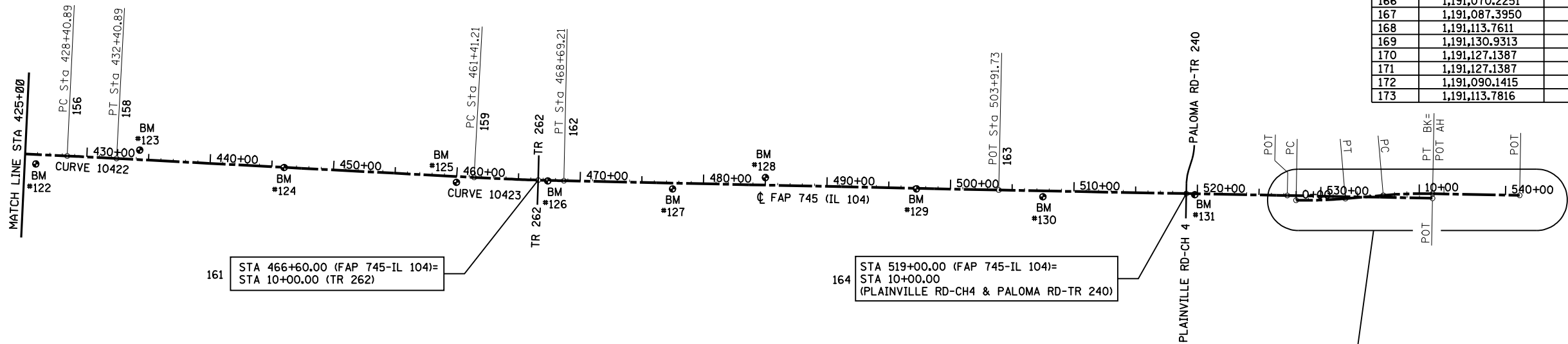


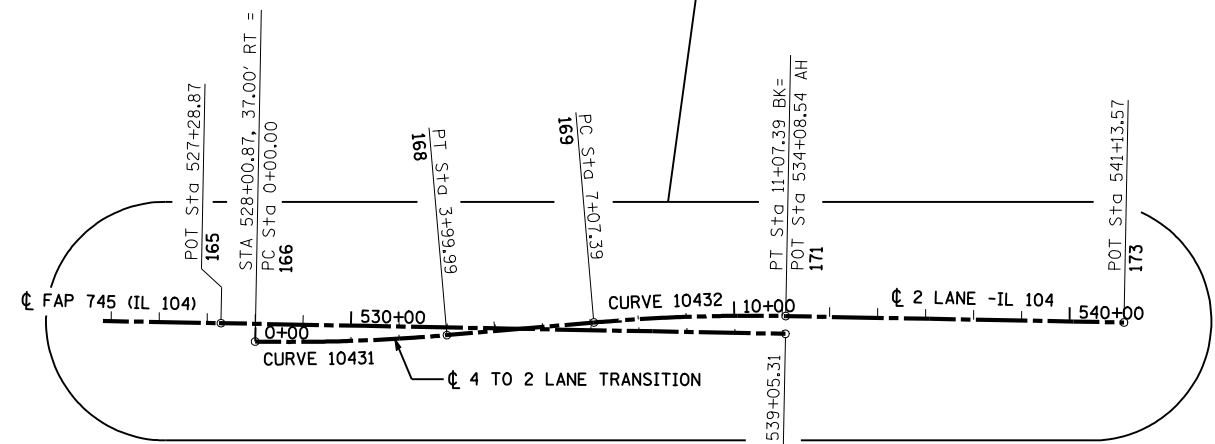
EXIST. CURVE 10422
 157 PI STA. = 430+40.89
 $\Delta = 00^\circ 21' 03''$ (RT)
 D = 00° 05' 16"
 R = 65,326.21'
 T = 200.00'
 L = 400.00'
 E = 0.31'
 156 P.C. STA. = 428+40.89
 158 P.T. STA. = 432+40.89

EXIST. CURVE 10423
 160 PI STA. = 465+05.24
 $\Delta = 01^\circ 48' 46''$ (LT)
 D = 00° 14' 56"
 R = 23,008.71'
 T = 364.03'
 L = 728.00'
 E = 2.88'
 159 P.C. STA. = 461+41.21
 162 P.T. STA. = 468+69.21

CONTROL POINTS		
POINT	NORTHING	EASTING
156	1,191,432.6385	2,002,079.3442
156	1,191,423.2603	2,002,279.1242
157	1,191,412.6590	2,002,478.8430
158	1,191,258.9232	2,005,375.0856
159	1,191,239.6272	2,005,738.6039
160	1,191,237.2665	2,005,893.4105
161	1,191,231.8407	2,006,102.5506
162	1,191,127.8041	2,011,143.2626
163	1,191,112.3586	2,011,960.9868
164	1,191,074.0055	2,012,032.2752
165	1,191,074.0055	2,012,032.2752
166	1,191,070.2251	2,012,232.4195
167	1,191,087.3950	2,012,431.8618
168	1,191,113.7611	2,012,738.1260
169	1,191,130.9313	2,012,937.5713
170	1,191,127.1387	2,013,137.7183
171	1,191,127.1387	2,013,137.7183
172	1,191,090.1415	2,013,137.2170
173	1,191,113.7816	2,013,842.6173

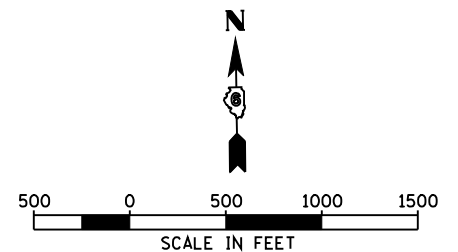


BENCHMARKS			
MARK	STATION/OFFSET	ELEVATION	DESCRIPTION
BM #122	425+90, 76.3' RT	728.706	PUNCH HOLE IN W. END OF 15" CMP AT W. ENTRANCE TO FLYING INN TAVERN (RT-RTE 104), 5.7 MILES EAST OF I-172
BM #123	434+25, 77.5' LT	728.399	PUNCH HOLE IN E. END OF 15" CMP OF FIELD ENTRANCE (LT-RTE 104) 5.9 MILES EAST OF I-172
BM #124	446+00, 1.3' RT	701.114	CHISELED "□" ON NW CORNER OF CONCRETE OPENING TO 24" RCP GOING S. W/METAL BRACE IN (C RTE 104), 6.1 MILES EAST I-172
BM #125	460+00, 47.5' RT	734.255	CHISELED "□" JUST N. OF STA STAMP 460+00 SN E. BOUND CONCRETE (RTE 104) 6.4 MILES EAST I-172
BM #126	467+38, 4.0' RT	747.191	CHISELED "□" ON E. SIDE OF CONCRETE INLET STRUCTURE E. SIDE TURNAROUND AT INT 1600TH ST & (C RTE 104) 6.55 MILES EAST I-172
BM #127	477+50, 47.0' RT	754.937	CHISELED "□" JUST N. OF STA STAMP 477+50 ON CONCRETE E. BOUND LANE (RTE 104) 6.7 MILES EAST I-172
BM #128	485+00, 61.0 LT	747.956	CHISELED "□" ON STOP SIGN ISLAND AT MAIN ENTRANCE TO QUINCY AIRPORT (LT, RTE 104), 6.9 MILES EAST I-172 (START NEW MILE LOG)
BM #129	497+25, 4.4' RT	758.604	CHISELED "□" ON W. EDGE CONC INLET STRUCTURE E. SIDE TURNAROUND (C RTE 104) AT HANGER ENTRANCE TO AIRPORT, 0.2 MILES EAST QUINCY AIRPORT ENTRANCE
BM #130	507+50, 47.5' RT	767.841	CHISELED "□" JUST N. OF STA STAMP 507+50 ON CONCRETE E. BOUND LANE (RTE 104) 0.4 MILES E. MAIN AIRPORT ENTRANCE
BM #131	519+80, 7.9' RT	766.371	CHISELED "□" ON W. EDGE OF GRATE INLET E. SIDE TURNAROUND AT PLAINVILLE RD S. (C RTE 104) 0.6 MILES E. MAIN AIRPORT ENTRANCE



EXIST. CURVE 10431
 167 PI STA. = 2+00.18
 $\Delta = 06^\circ 00' 09''$ (LT)
 D = 01° 30' 02"
 R = 3,818.05'
 T = 200.18'
 L = 399.99'
 E = 5.24'
 166 P.C. STA. = 0+00.00
 168 P.T. STA. = 3+99.99

EXIST. CURVE 10432
 170 PI STA. = 9+07.57
 $\Delta = 06^\circ 00' 22''$ (RT)
 D = 01° 30' 05"
 R = 3,815.90'
 T = 200.18'
 L = 400.00'
 E = 5.25'
 169 P.C. STA. = 7+07.39
 171 P.T. STA. = 11+07.39



FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
et:\pwork\pwork\LAUGHLINRL\dms23418\07100_A1\alignmt-3.DGN		DRAWN -	REVISED -
PLOT SCALE = 1000.0000 "/>		CHECKED -	REVISED -
PLOT DATE = Jan-23-2009 11:47:38AM		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ALIGNMENT & BENCHMARK DATA

SCALE: 1"=500' SHEET NO. 3 OF 3 SHEETS STA. 425+00 TO STA. 540+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
745	103(RS-12), 104(RS-5)	ADAMS	109	26
CONTRACT NO. 72877				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				