

INTERCONNECT SCHEDULE OF QUANTITIES

UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	317
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	227
HANDHOLE	EACH	1
HEAVY-DUTY HANDHOLE	EACH	3
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
TRANSCEIVER - FIBER OPTIC	EACH	4
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	5233
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3007
ROD AND CLEAN EXISTING CONDUIT	FOOT	2832
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	5233
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1

FILE NAME = D:\6073-Sht-INTC03-Schematic.dgn
 PLOT SCALE = 100.0000 / in.
 USER NAME = Millennia Professional Services



2600 Warrenville Road, Suite 203, Downers Grove, IL 60515
 630.705.0110 voice, 630.839.2566 fax
 www.mps-ill.com

MILLENNIA PROFESSIONAL SERVICES

DESIGNED - TVN	REVISED -
DRAWN - JP	REVISED -
CHECKED - TVN	REVISED -
DATE - 12/22/2015	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT SCHEMATIC (SYSTEM NO. 84)
IL RTE 72 (HIGGINS ROAD)
PLUM GROVE RD TO I-290L RTE 53 NB RAMP/FRONTAGE RD












SCALE: N/A SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	101
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60Y73	

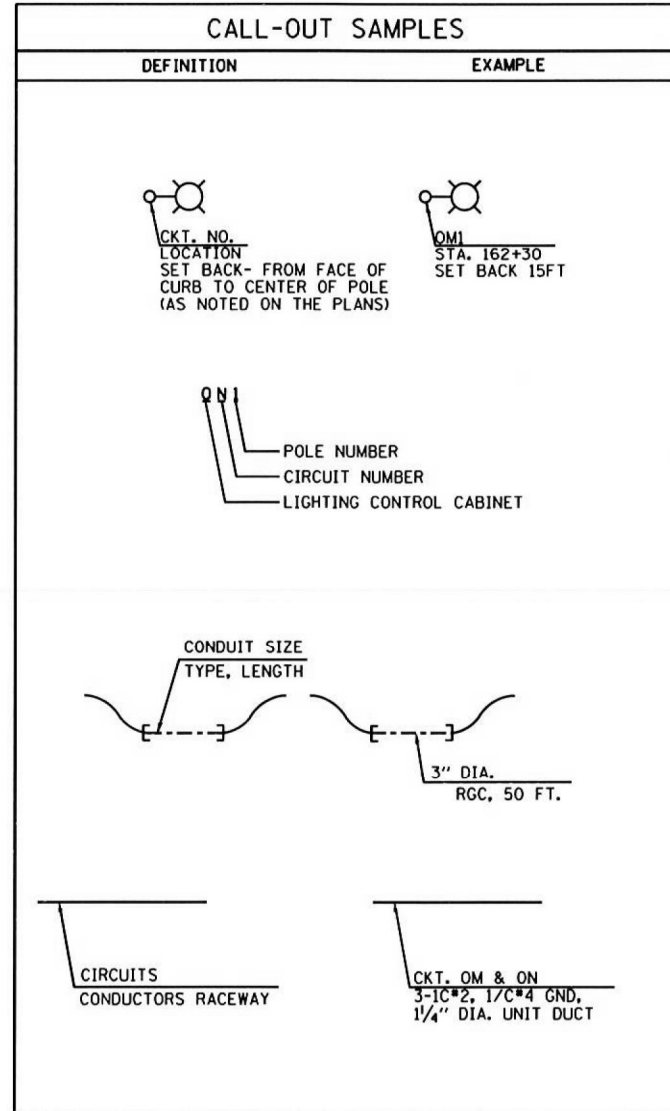
ITC-03

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LEGEND (IDOT)

-  EXISTING LIGHTING UNIT TO REMAIN IN PLACE
-  EXISTING LIGHTING UNIT TO BE RELOCATED (ORIGINAL LOCATION)
-  RELOCATED EXISTING LIGHTING UNIT (PROPOSED LOCATION)
-  UNDERPASS LIGHTING UNIT TO REMAIN IN PLACE
-  EXISTING LIGHTING HANDHOLE
-  PROPOSED UNDERGROUND CONDUITS SIZE AND TYPE AS NOTED
-  PROPOSED UNIT DUCT, AS SPECIFIED IN PLANS
-  EXISTING UNDERGROUND CABLES TO REMAIN IN PLACE
-  EXISTING LIGHTING CONTROLLER CABINET "O", TO REMAIN IN PLACE
-  EXISTING PAD MOUNTED TRANSFORMER 240V/480V VAC SINGLE PHASE, 3 WIRE, 200A, TO REMAIN IN PLACE
-  ELECTRIC GROUND ROD, 5/8" X 10FT

CALL-OUT SAMPLES



ABBREVIATIONS

SYMBOL	DESCRIPTION
AC	ALTERNATING CURRENT
A/C	AERIAL CABLE
AFG	ABOVE FINISHED GRADE
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CM	CENTIMETER
CNC	COILABLE NONMETALLIC CONDUIT
CT	CURRENT TRANSFORMER
CP	CONTROL PANEL
DA	DAVIT ARM
DC	DIRECT CURRENT
DIA	DIAMETER
DP	DISTRIBUTION PANEL
E	EXISTING UNIT TO REMAIN
ECA	ELECTRIC CABLE ASSEMBLY
EM	EXISTING UNIT TO BE MODIFIED (e.g. NEW LUMINAIRE, BALLAST OR MAST ARM)
ER	EXISTING RELOCATED UNIT
ET	EXISTING TEMPORARY UNIT TO REMAIN
ETR	EXISTING TEMPORARY RELOCATED UNIT
FT	FEET OR FOOT
FND BW	FOUNDATION BARRIER WALL
FND BW OS	FOUNDATION BARRIER WALL OFFSET
FND CON	FOUNDATION CONCRETE
FND CON OS	FOUNDATION CONCRETE OFFSET
FND MET	FOUNDATION METAL
FND PW	FOUNDATION PARAPET WALL
FU	FUSE
GND	GROUND
HID	HIGH INTENSITY DISCHARGE
JB	JUNCTION BOX
KVA	KILOVOLT-AMPERE
KW	KILOWATTS
M	METER
MA	MAST ARM
MM	MILLIMETER
MH	MOUNTING HEIGHT
NO.	NUMBER
P	PROPOSED
PB	PUSH BUTTON
PNL	PANEL
PVCC RGC	PVC COATED RIGID GALVANIZED CONDUIT
PT	POTENTIAL TRANSFORMER
R	EXISTING UNIT TO BE REMOVED (OWNER SALVAGED U.N.O.)
RR	EXISTING UNIT TO BE REMOVED AND REINSTALLED
RECP	RECEPTACLE
RGC	RIGID GALVANIZED CONDUIT
RGS	RIGID GALVANIZED STEEL
SEL SW	SELECTOR SWITCH
SPARE	SPARE
SPACE	SPACE
SS	STAINLESS STEEL
STA	STATION
T	TEMPORARY LIGHTING UNIT
TB	TRANSFORMER BASE
TMP	TEMPORARY
TR	TEMPORARY UNIT TO BE REMOVED, SALVAGE EQUIPMENT AS SPECIFIED
TRR	TEMPORARY UNIT TO BE REMOVED AND RELOCATED
TUR	TEMPORARY UNIT ON UTILITY POLE TO BE REMOVED
UD	UNIT DUCT
U.N.O.	UNLESS NOTED OTHERWISE
WP	WOOD POLE
XFMR	TRANSFORMER

GENERAL NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL OF THE INFORMATION SHOWN ON THE CONTRACT DRAWINGS, WHICH WOULD AFFECT THE WORK UNDER THIS CONTRACT.
2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ASCERTAIN EXISTING FIELD CONDITIONS BEFORE BIDDING ON THIS PROJECT, SPECIFICALLY AS THEY RELATE TO LUMP SUM ITEMS AND UNIT PRICE ITEMS.
3. ALL NEW CONDUITS, UNIT DUCTS, DIRECT BURIAL CABLES, AND APPURTENANCES ARE INDICATED DIAGRAMMATICALLY ON THE DRAWINGS. THE ACTUAL LOCATIONS IN THE FIELD SHALL MEET WITH APPROVAL OF THE ENGINEER.
4. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND ASSOCIATED SUPPLEMENTAL CONDITIONS (LATEST EDITION).
5. THE SCALE SHOWN ON PLAN DRAWINGS APPLIES ONLY TO THE FULL SIZE PLANS AND NOT TO REDUCED SIZE PLANS.
6. THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE LAMPS IN ACCORDANCE WITH THE SUPPLIER'S RECOMMENDATIONS AND IN ACCORDANCE WITH THE SPECIFICATIONS. THE COST OF THIS WORK AND MATERIAL SHALL BE INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEM. SEPARATE PAYMENT WILL NOT BE MADE.
7. ALL LUMINAIRES SHALL BE ORIENTED WITH THE OPTICS PERPENDICULAR TO THE ROADWAY UNLESS OTHERWISE INDICATED OR DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED IN THE APPLICABLE LUMINAIRE PAY ITEMS. SEPARATE PAYMENT WILL NOT BE MADE.
8. CONDUITS AND UNIT DUCTS SHALL BE INSTALLED AT A MINIMUM 30" DEPTH BELOW GRADE AND POSITIONED IN THE FIELD TO AVOID CONFLICT WITH ROADWAY UNDERDRAINS AND OTHER EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL INCREASE DEPTH OF UNIT DUCT AND CONDUIT AS REQUIRED AT NO ADDITIONAL COST TO THE STATE. THE CONTRACTOR SHALL COORDINATE RACEWAY DEPTH WITH THE ELECTRICAL DETAILS OR AS DIRECTED BY THE ENGINEER.
9. WHERE MULTIPLE CONDUITS ADJACENT TO EACH OTHER ARE INSTALLED IN A COMMON TRENCH, TRENCH AND BACKFILL WILL NOT BE PAID FOR EACH CONDUIT, BUT WILL BE PAID FOR THE LENGTH OF THE COMMON TRENCH ONLY.
10. WHERE THE CONTRACTOR'S EXCAVATION MEETS AN OBSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER FOR DIRECTION IN WRITING PRIOR TO EXCAVATION. THE CONTRACTOR SHALL RESTORE ANY DAMAGE TO EXISTING SYSTEMS OR UTILITIES AND REMOVE EXISTING OBSTRUCTIONS AND FOUNDATIONS TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE APPROPRIATE PAY ITEM.
11. BREAKAWAY DEVICE, TRANSFORMER BASE, 9", SHALL BE INSTALLED ON ALL GROUND MOUNTED POLES WITH 15" BOLT CIRCLE ON 24" DIA. FOUNDATION AS SHOWN IN THE PLANS.
12. SET BACK DISTANCES FOR LIGHT POLES ALONG ILLINOIS ROUTE 72 ARE MEASURED FROM THE FACE OF CURB TO THE CENTER OF THE PROPOSED LIGHT POLE FOUNDATION, OR EDGE OF TRAVEL PAVEMENT WHEN CURB IS NOT EXIST.
13. THE COST OF ALL GROUND RODS SHALL BE INCLUDED IN THE COST OF THE ITEM FOR WHICH IT IS INSTALLED.



Exp. 11/30/17

GANDHI AND ASSOCIATES INC.
 (184-001236)
 PETER A. VANDEMOTTER, P.E.
 # 062-043463

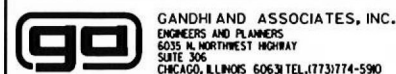
Peter A. Vandemotter

DATE: 12/20/2015
 SIGNATURE AND SEAL APPLIES
 TO DRWG NO 102 TO 106

SCHEDULE OF QUANTITIES

DESIGNATION	UNIT	TOTAL QUANTITY
• UNDERGROUND CONDUITS, GALVANIZED STEEL, 3" DIA.	FOOT	130
• UNIT DUCT, 600V, 3-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/2" DIA. POLYETHYLENE	FOOT	1600
• UNIT DUCT, 600V, 6-1C NO.6, 1/C NO.4 GROUND, (XLP-TYPE USE), 2" DIA. POLYETHYLENE	FOOT	370
LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	70
BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	8
REMOVAL OF POLE FOUNDATION	EACH	8
RELOCATE EXISTING LIGHTING UNIT	EACH	8
• REMOVE EXISTING CABLE	FOOT	400
• MAINTENANCE OF LIGHTING SYSTEM	CAL MO	6

• DESIGNATES SPECIAL PROVISIONS



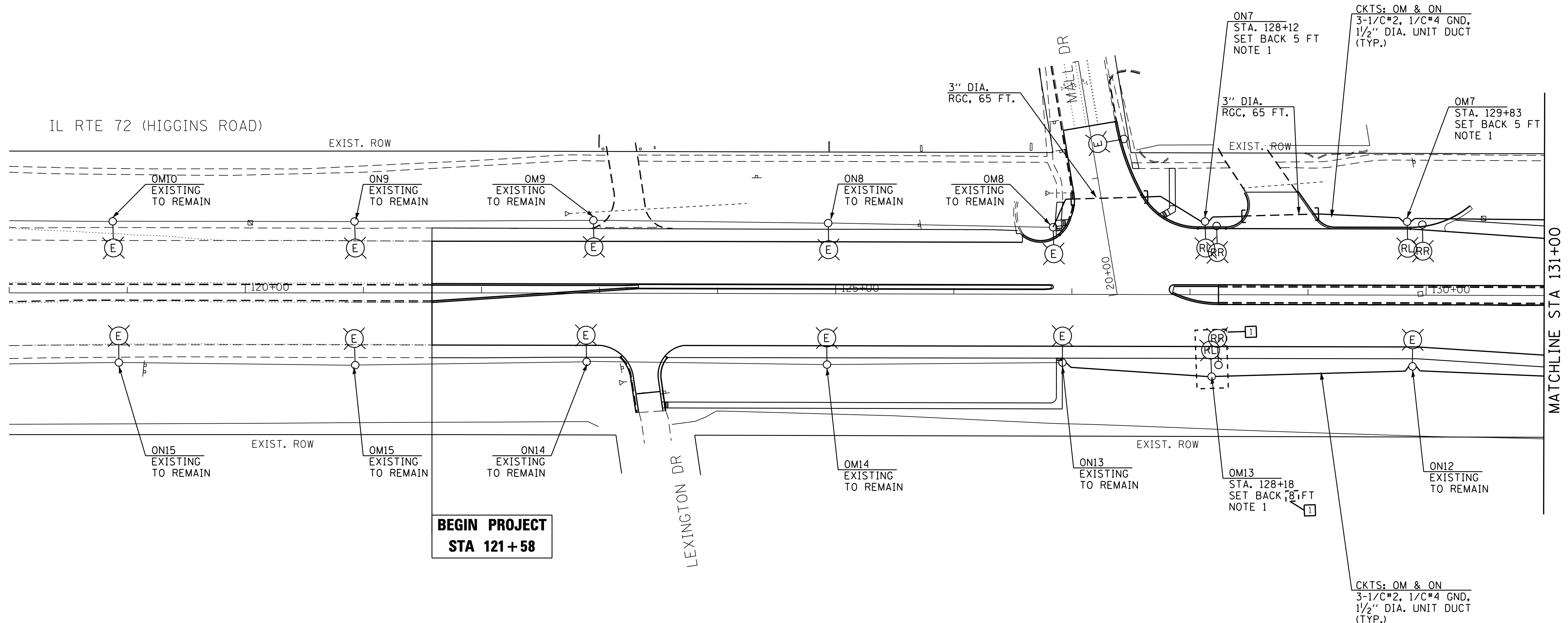
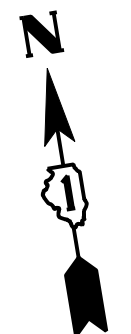
GANDHI AND ASSOCIATES, INC.
 ENGINEERS AND PLANNERS
 5035 N. NORTHWEST HIGHWAY
 SUITE 306
 CHICAGO, ILLINOIS 60631 TEL: (773) 774-5900

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		CHECKED - PKG	REVISED -
		DATE - 12/16/11	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

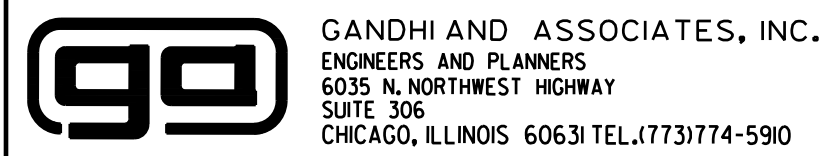
GENERAL NOTES, LEGEND, AND SCHEDULE OF QUANTITIES

SCALE: NONE	SHEET NO. 1 OF 5 SHEETS	STA.	TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				341	(132-3-R&0305-302K)TS&N-3	COOK	153	102
							CONTRACT NO. 60Y73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT								



NOTES:

1. THE EXISTING CONVENTIONAL LIGHT POLE, MAST ARM, AND LUMINAIRE SHALL BE REMOVED FROM THE EXISTING FOUNDATION AND REINSTALLED ON THE NEW FOUNDATION AT THE PROPOSED LOCATION SHOWN ON THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTING FOUNDATION BEFORE CONSTRUCTING A NEW FOUNDATION AT THE PROPOSED LOCATION, AND MATCH THE EXISTING FOUNDATION FOR THE RELOCATED LIGHT POLE.
2. THE EXISTING ORNAMENTAL LIGHT POLE, MAST ARM, AND LUMINAIRE SHALL BE REMOVED FROM THE EXISTING FOUNDATION AND REINSTALLED ON THE NEW FOUNDATION AT THE PROPOSED LOCATION SHOWN ON THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTING FOUNDATION BEFORE CONSTRUCTING A NEW FOUNDATION AT THE PROPOSED LOCATION, AND MATCH THE EXISTING FOUNDATION FOR THE RELOCATED LIGHT POLE.
3. THE CONTRACTOR SHALL VERIFY THE TYPE AND SIZE OF THE EXISTING CABLES TO MATCH THEM, AND INSTALL NEW CABLES BETWEEN THE RELOCATED LIGHT POLE AND THE EXISTING LIGHT POLE ADJACENT TO IT.
4. THE RELOCATION OF EXISTING LIGHTING UNITS SHALL BE PERFORMED DURING THE FIRST STAGE OF THE CONSTRUCTION. THE LIGHT POLE SHALL BE RELOCATED AND MADE OPERATIONAL IN ONE DAY.
5. RESTORATION OF WORK AREA - RESTORATION OF LIGHTING WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUITS, ETC. AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELD, SHALL BE SODDED OR SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

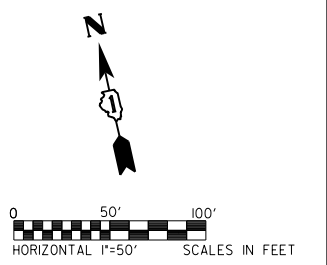
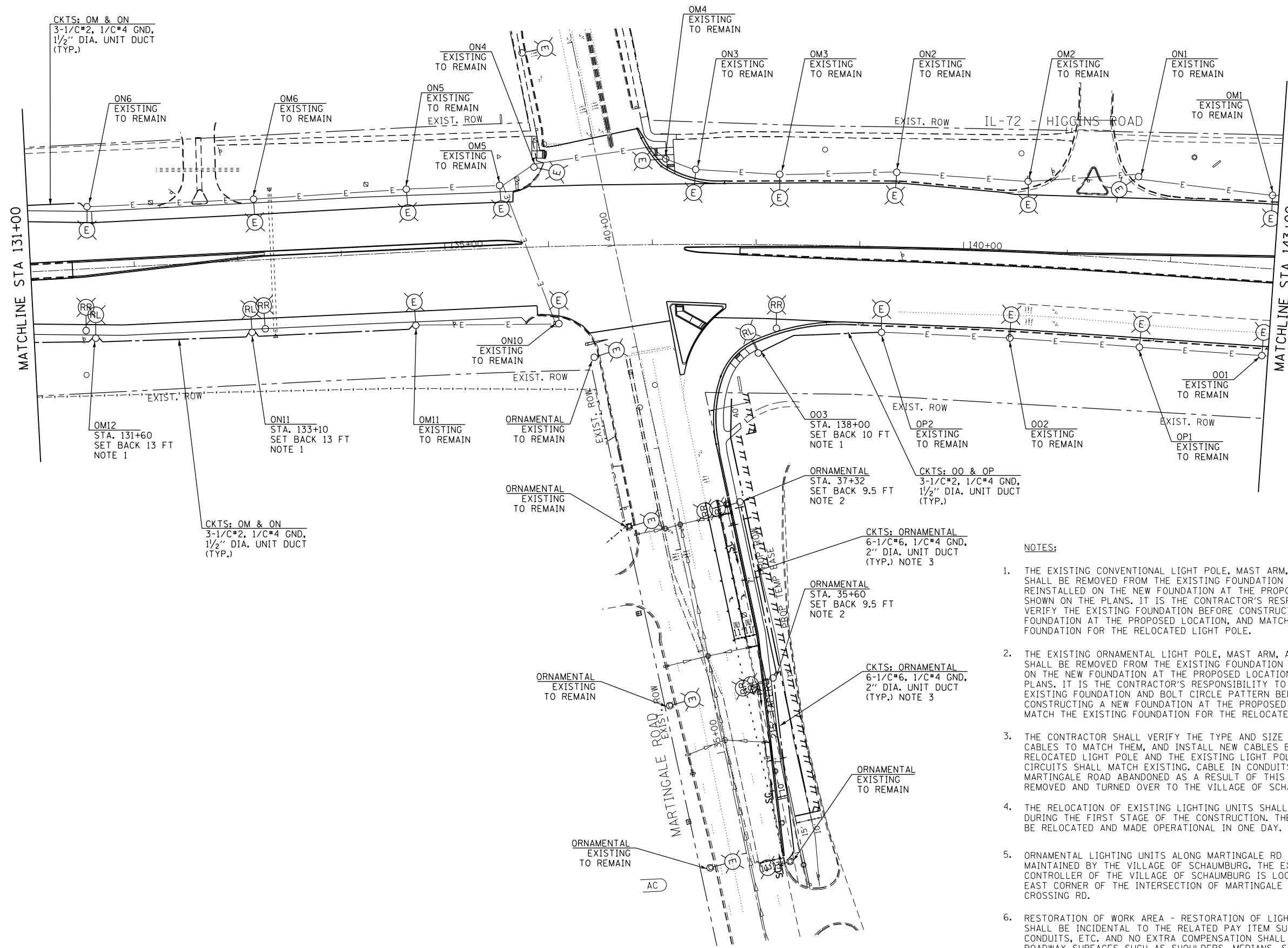


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	PLOT DATE = \$DATE\$	DATE - 03/17/16	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LIGHTING PLAN	
IL RTE 72 (HIGGINS ROAD)	
SCALE: 1"= 50'	SHEET NO. 2 OF 5 SHEETS STA118+00 TO STA.131+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	103
CONTRACT NO. 60Y73				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:**
1. THE EXISTING CONVENTIONAL LIGHT POLE, MAST ARM, AND LUMINAIRE SHALL BE REMOVED FROM THE EXISTING FOUNDATION AND REINSTALLED ON THE NEW FOUNDATION AT THE PROPOSED LOCATION SHOWN ON THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE EXISTING FOUNDATION BEFORE CONSTRUCTING A NEW FOUNDATION AT THE PROPOSED LOCATION, AND MATCH THE EXISTING FOUNDATION FOR THE RELOCATED LIGHT POLE.
 2. THE EXISTING ORNAMENTAL LIGHT POLE, MAST ARM, AND LUMINAIRE SHALL BE REMOVED FROM THE EXISTING FOUNDATION AND REINSTALLED ON THE NEW FOUNDATION AT THE PROPOSED LOCATION SHOWN ON THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTING FOUNDATION AND BOLT CIRCLE PATTERN BEFORE CONSTRUCTING A NEW FOUNDATION AT THE PROPOSED LOCATION, AND MATCH THE EXISTING FOUNDATION FOR THE RELOCATED LIGHT POLE.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE AND SIZE OF THE EXISTING CABLES TO MATCH THEM, AND INSTALL NEW CABLES BETWEEN THE RELOCATED LIGHT POLE AND THE EXISTING LIGHT POLE ADJACENT TO IT. CIRCUITS SHALL MATCH EXISTING, CABLE IN CONDUITS ALONG MARTINGALE ROAD ABANDONED AS A RESULT OF THIS WORK SHALL BE REMOVED AND TURNED OVER TO THE VILLAGE OF SCHAUMBURG.
 4. THE RELOCATION OF EXISTING LIGHTING UNITS SHALL BE PERFORMED DURING THE FIRST STAGE OF THE CONSTRUCTION. THE LIGHT POLE SHALL BE RELOCATED AND MADE OPERATIONAL IN ONE DAY.
 5. ORNAMENTAL LIGHTING UNITS ALONG MARTINGALE RD ARE OWNED AND MAINTAINED BY THE VILLAGE OF SCHAUMBURG. THE EXISTING LIGHTING CONTROLLER OF THE VILLAGE OF SCHAUMBURG IS LOCATED AT THE NORTH EAST CORNER OF THE INTERSECTION OF MARTINGALE RD AND CORPORATE CROSSING RD.
 6. RESTORATION OF WORK AREA - RESTORATION OF LIGHTING WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUITS, ETC. AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELD, SHALL BE SODDED OR SEEDDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

GA GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL: (773) 774-5910

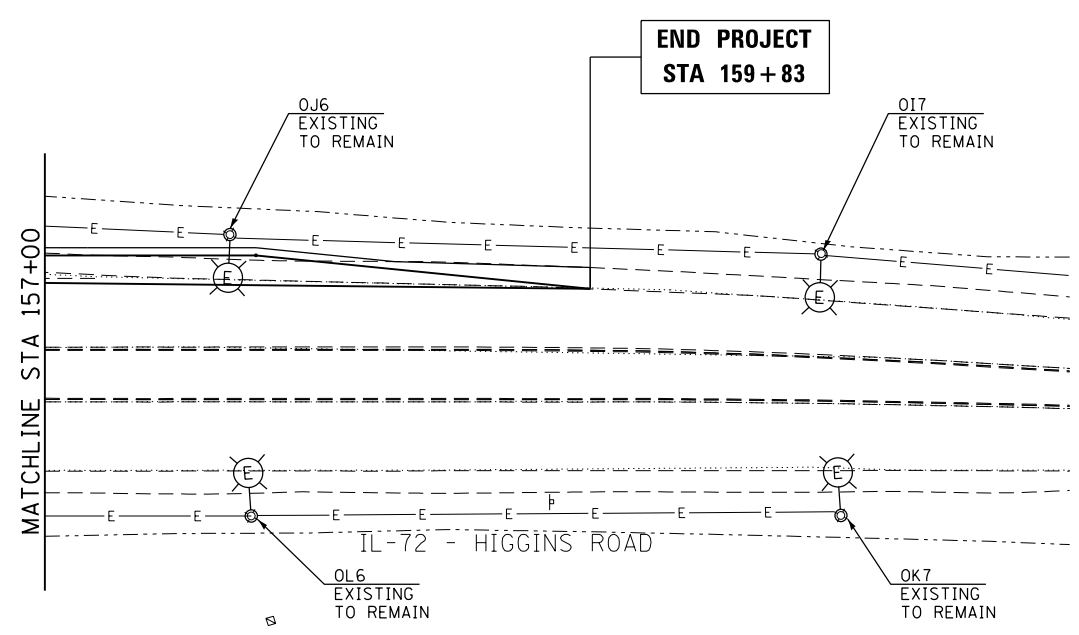
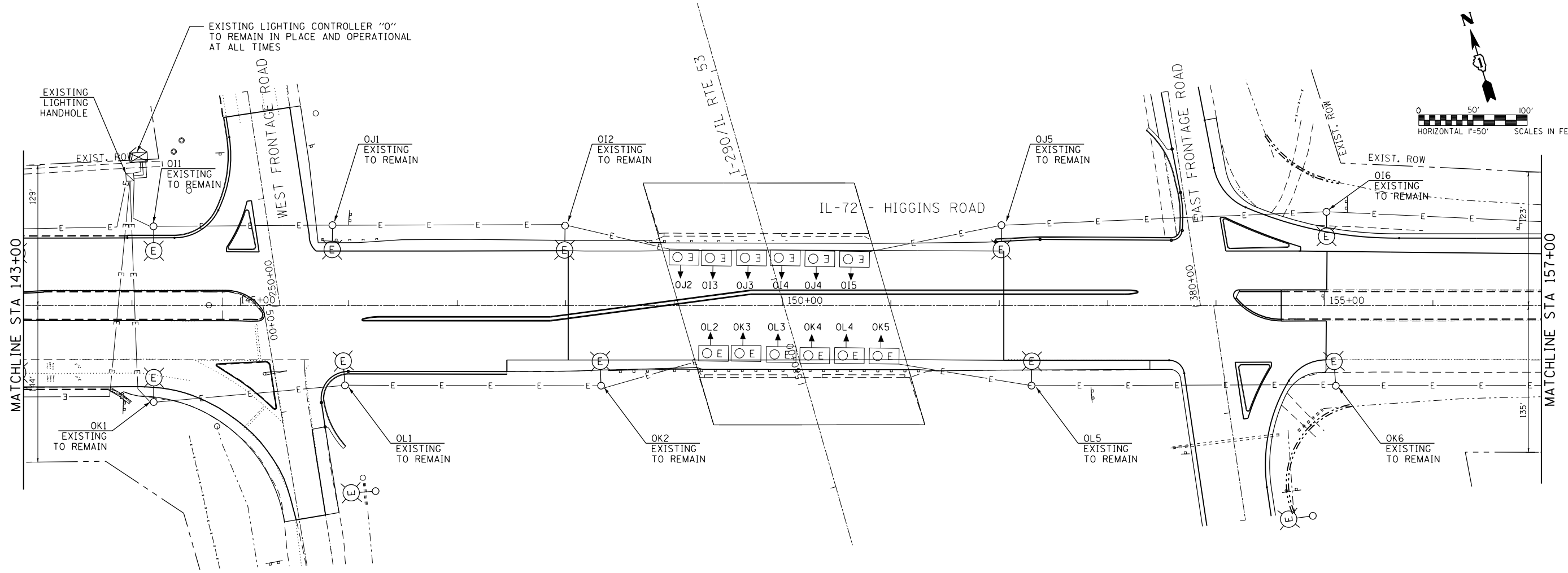
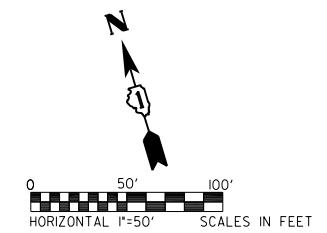
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	PLOT DATE = \$DATE\$	DATE - 12/16/11	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LIGHTING PLAN
IL RTE 72 (HIGGINS ROAD)**

SCALE: 1"= 50' SHEET NO. 3 OF 5 SHEETS STA131+00 TO STA.143+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302)ITS&N-3	COOK	153	104
CONTRACT NO. 60Y73				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GA GANDHI AND ASSOCIATES, INC.
ENGINEERS AND PLANNERS
6035 N. NORTHWEST HIGHWAY
SUITE 306
CHICAGO, ILLINOIS 60631 TEL. (773) 774-5910

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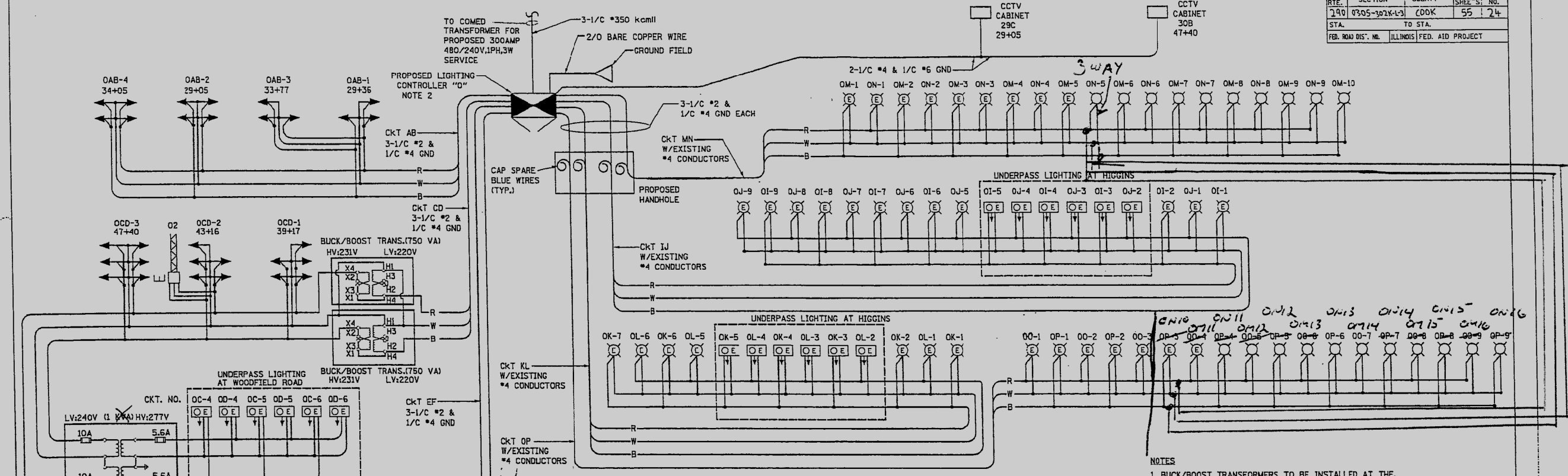
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

LIGHTING PLAN IL RTE 72 (HIGGINS ROAD)			
SCALE: 1"= 50'	SHEET NO. 4 OF 5 SHEETS	STA. 143+00 TO STA. 157+00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	105
CONTRACT NO. 60Y73				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

62877

F.A. RTE.	SECTION	COUNTY	TOTAL SHEET
740	0305-302K-L3	COOK	55 / 24
STA.	TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



LOAD TABULATION FOR LIGHTING CONTROLLER "O"

CIRCUIT	DESCRIPTION	PHASE A (RED) AMPS	PHASE B (BLACK) AMPS	BREAKER SIZE AMPS
A	7 FIXTURES	27.3		70
B	7 FIXTURES		27.3	70
C	8 FIXTURES + 1 SIGN + 3 UNDERPASS	34.5		70
D	8 FIXTURES + 2 SIGN + 3 UNDERPASS		35.4	70
E	7 FIXTURES	27.3		70
F	7 FIXTURES		27.3	70
G	8 FIXTURES + 3 SIGN + 5 UNDERPASS	35.4		70
H	8 FIXTURES + 4 SIGN + 5 UNDERPASS		36.3	70
I	6 FIXTURES + 3 UNDERPASS	15		70
J	6 FIXTURES + 3 UNDERPASS		15	70
K	4 FIXTURES + 3 UNDERPASS	10.8		70
L	3 FIXTURES + 3 UNDERPASS		8.7	70
M	10 FIXTURES	21		70
N	9 FIXTURES		18.9	70
O	9 FIXTURES	18.9		70
P	9 FIXTURES		18.9	70
Q				70
R				70
S				70
T				70
U				70
V				70
W				70
X				70
CONNECTED LOAD		187.8		90.7kVA

- NOTES**
- BUCK/BOOST TRANSFORMERS TO BE INSTALLED AT THE DISCRETION OF THE ENGINEER.
 - PROPOSED LIGHTING CONTROLLER "O" IS LOCATED AT THE NORTHWEST CORNER OF HIGGINS ROAD AND FRONTAGE ROAD.

- LEGEND**
- ← PROPOSED TOWER MOUNTED 750W HPS LUMINAIRE-240V DESIGNATION AS INDICATED
 - ⊗ EXISTING HPS LUMINAIRE TO BE REPLACED WITH HPS LUMINAIRE WITH MULTI-TAP BALLAST DESIGNATION AS INDICATED
 - ⊙ REINSTALLED LIGHTING UNIT
 - ⊕ EXISTING 150W MULTI-TAP UNDERPASS LUMINAIRE
 - ⊗ EXISTING SIGN LUMINAIRE
 - ⊠ PROPOSED LIGHTING CONTROLLER, DUPLEX TYPE WITH RADIO CONTROL AND SCADA
 - B BLACK PHASE CONDUCTOR
 - W WHITE NEUTRAL CONDUCTOR
 - R RED PHASE CONDUCTOR
 - G GREEN GROUND CONDUCTOR

J.F. EDWARDS CONSTRUCTION CO.
 APPROVED
 APPROVED AS NOTED
 DATE 9-19-06
 BY Rick Moburg

J.F. EDWARDS CONSTRUCTION CO.
 APPROVED
 APPROVED AS NOTED
 DATE 8-11-06
 BY Rick Moburg

RECORD
DRAWING

Buck Boost TRANS. .25kVA

25kVA BUCK BOOST TRANSFORMERS
 (SEE ATTACHED WIRING DIAGRAM FROM EXCEL LTD)

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		ILLINOIS ROUTE 53 SINGLE LINE DIAGRAM PROPOSED CONTROLLER "O"

SCALE: VERT. NONE
 HORIZ. DATE 02-28-2005
 DRAWN BY WDF
 CHECKED BY RER

J.F. Edwards Construction Co.

DRAWING IS PROVIDED FOR INFORMATION ONLY

GENERAL NOTES

DESIGN: AASHTO Standard Specifications for Structural Supports for Highway Signs, Luminaires and Traffic Signals. ("AASHTO Specifications")

CONSTRUCTION: Current (at time of letting) Illinois Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications and Special Provisions. ("Standard Specifications")

LOADING: 90 M.P.H. WIND VELOCITY

WALKWAY LOADING: Dead load plus 500 lbs. concentrated live load.

DESIGN STRESSES:

Field Units
 $f'_c = 3,500$ p.s.i.
 $f_y = 60,000$ p.s.i. (reinforcement)

WELDING: All welds to be continuous unless otherwise shown. All welding to be done in accordance with current AWS D1.1 and D1.2 Structural Welding Codes (Steel and Aluminum) and the Standard Specifications.

MATERIALS: Aluminum Alloys as shown throughout plans. All Structural Steel Pipe shall be ASTM A53 Grade B or A500 Grade B or C. If A500 pipe is substituted for A53, then the outside diameter shall be as detailed and wall thickness greater than or equal to A53.

All Structural Steel Plates and Shapes shall conform to AASHTO M270 Gr. 36, Gr. 50 or Gr. 50W*. Stainless steel for shims, sleeves and handhole covers shall be ASTM A240, Type 302 or 304, or another alloy suitable for exterior exposure and acceptable to the Engineer.

The steel pipe and stiffening ribs at the base plate for the column shall have a minimum longitudinal Charpy V-Notch (CVN) energy of 15 lb.-ft. at 40° F. (Zone 2) before galvanizing.

FASTENERS FOR ALUMINUM TRUSSES: All bolts noted as "high strength" must satisfy the requirements of AASHTO M164 (ASTM A325), or approved alternate, and must have matching lock nuts. Threaded studs for splices (if Members interfere) must satisfy the requirements of ASTM A449, ASTM A193, Grade B7, or approved alternate, and must have matching lock nuts. Bolts and lock nuts not required to be high strength must satisfy the requirements of ASTM A307. All bolts and lock nuts must be hot dip galvanized per AASHTO M232. The lock nuts must have nylon or steel inserts. A stainless steel flat washer conforming to ASTM A240 Type 302 or 304, is required under both head and nut or under both nuts where threaded studs are used. High strength bolt installation shall conform to Article 505.04 (f) (2)d of the IDOT Standard Specifications for Road and Bridge Construction. Rotational capacity ("ROCAP") testing of bolts will not be required.

U-BOLTS AND EYEBOLTS: U-Bolts and Eyebolts must be produced from ASTM A276 Type 304, 304L, 316 or 316L, Condition A, cold finished stainless steel, or an equivalent material acceptable to the Engineer. All nuts for U-Bolts and Eyebolts must be lock nuts equivalent to ASTM A307 with nylon or steel inserts and hot dip galvanized per AASHTO M232. A stainless steel flat washer conforming to ASTM A240, Type 302 or 304, is required under each U-Bolt and Eyebolt lock nut.

GALVANIZING: All Steel Grating, Plates, Shapes and Pipe shall be Hot Dip Galvanized after fabrication in accordance with AASHTO M111. Painting is not permitted.

ANCHOR RODS: Shall conform to ASTM F1554 Gr. 105.

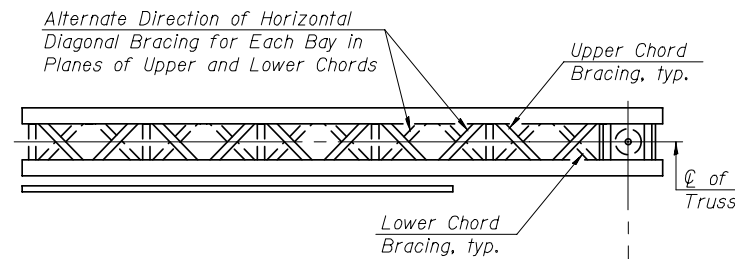
CONCRETE SURFACES: All concrete surfaces above an elevation 6" below the lowest final ground line at each foundation shall be cleaned and coated with Bridge Seat Sealer in accordance with the Standard Specifications.

REINFORCEMENT BARS: Reinforcement Bars designated (E) shall be epoxy coated in accordance with the Standard Specifications.

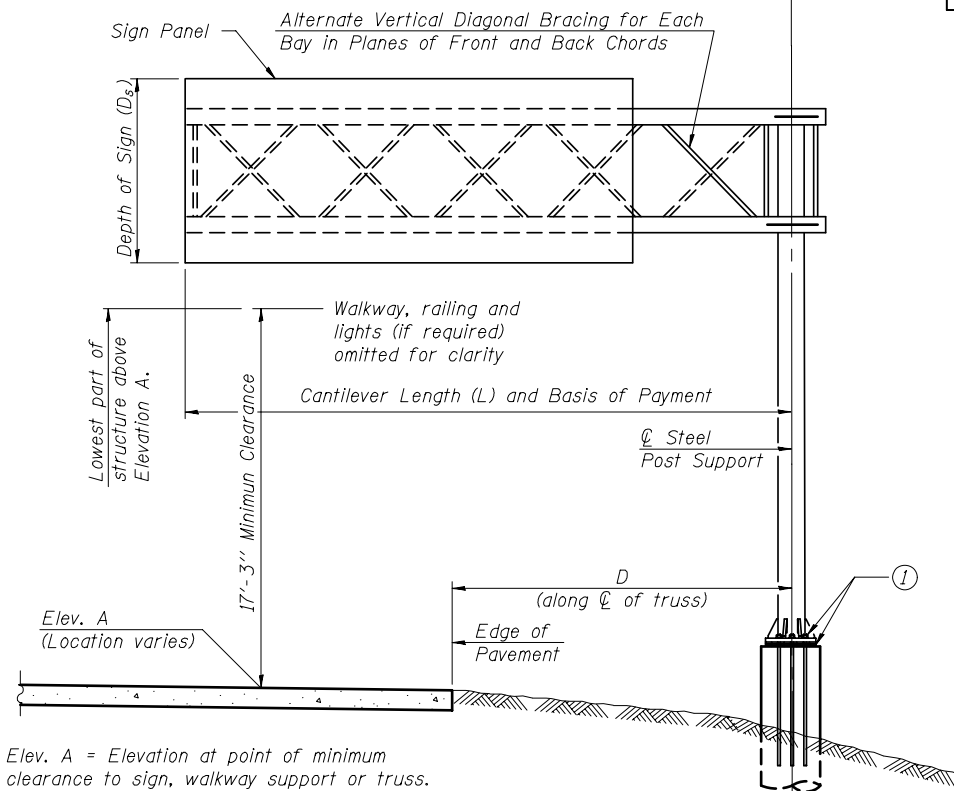
FOUNDATIONS: The contract unit price for Drilled Shaft Concrete Foundations shall include reinforcement bars complete in place.

Structure Number	Station	Design Truss Type	Cantilever Length (L)	Elev. A	Dim. D	D _s	Total Sign Area
IC016S072R(NA)	132+00.0	III-C-A	35'-6"	735.26	21'-9"	14'	217.0 SF

Truss Type	Maximum Sign Area	Maximum Length
I-C-A	170 Sq. Ft.	25 Ft.
II-C-A	340 Sq. Ft.	30 Ft.
III-C-A	400 Sq. Ft.	40 Ft.



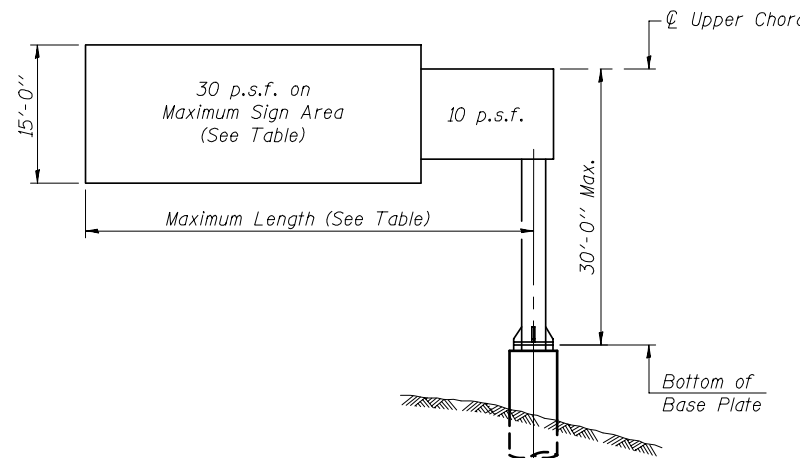
TYPICAL PLAN
(Walkway not shown)



TYPICAL ELEVATION
Looking in Direction of Traffic

Elev. A = Elevation at point of minimum clearance to sign, walkway support or truss.

Sign support structures may be subject to damaging vibrations and oscillations when sign panels are not in place during erection or maintenance of the structure. To avoid these vibrations and oscillations, consideration should be given to attaching temporary blank sign panels to the structure.



DESIGN WIND LOADING DIAGRAM

Parameters shown are basis for I.D.O.T. Standards. Installations not within dimensional limits shown require special analysis for all components.

Note:

Trusses shall be shipped individually with adequate provision to prevent detrimental motion during transport. This may require ropes between horizontals and diagonals or energy dissipating (elastic) ties to the vehicle. The contractor is responsible for maintaining the configuration and protection of the trusses.

① After adjustments to level truss and insure adequate vertical clearance, all top and leveling nuts shall be tightened against the base plate with a minimum torque of 200 lb.-ft. Stainless steel mesh shall then be placed around the perimeter of the base plate. Secure to base plate with stainless steel banding.

* If M270 Gr. 50W (M222) steel is proposed, chemistry for plate to be used shall first be approved by the Engineer as suitable for galvanizing and welding.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE I-C-A	Foot	
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE II-C-A	Foot	
OVERHEAD SIGN STRUCTURE CANTILEVER TYPE III-C-A	Foot	35'-6"
OVERHEAD SIGN STRUCTURE WALKWAY, TYPE A	Foot	
DRILLED SHAFT CONCRETE FOUNDATIONS	Cu. Yds.	12.6

OSC-A-1

6-1-12

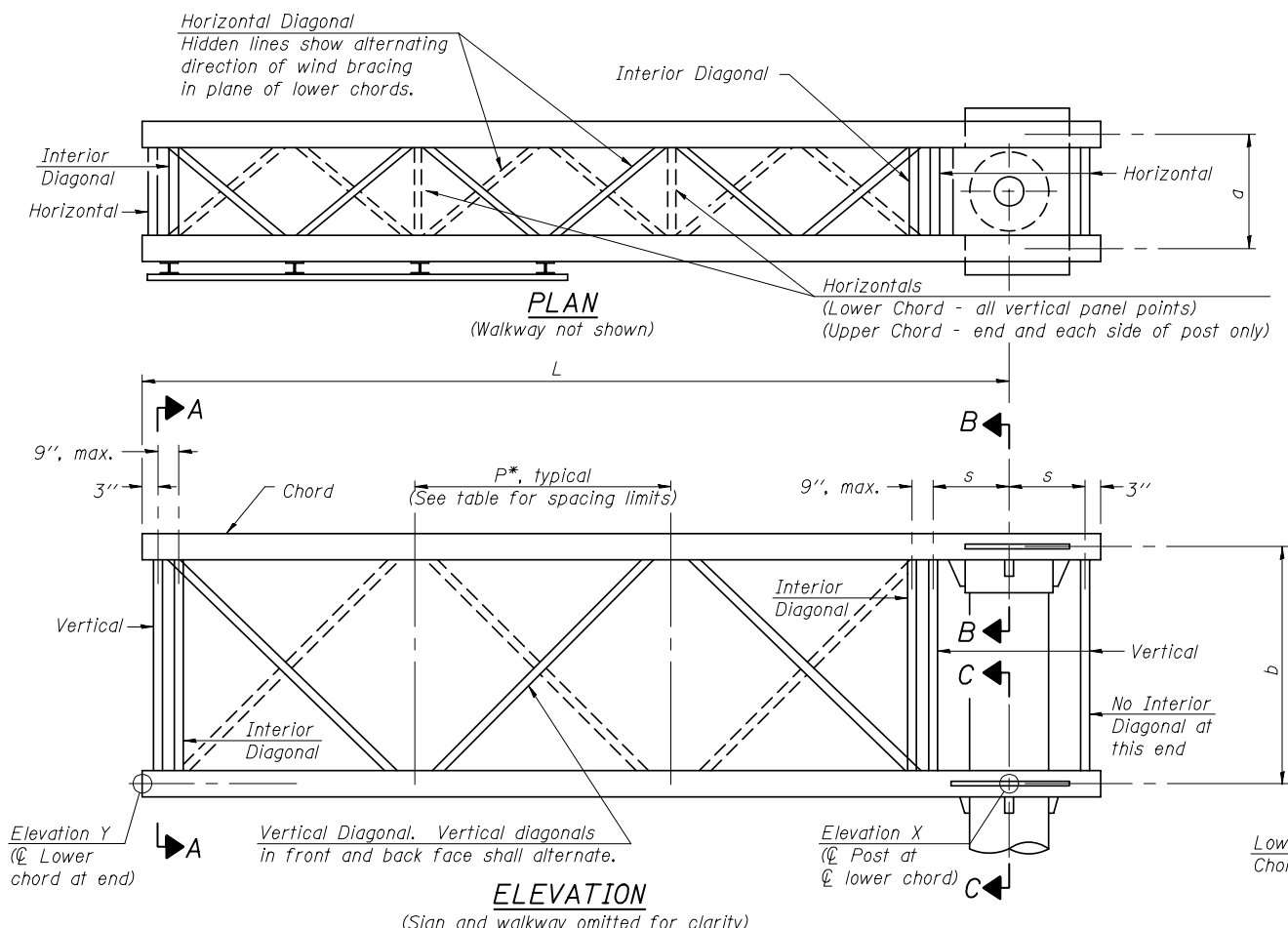
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	PLOT DATE =	CHECKED -	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - GENERAL PLAN & ELEVATION
ALUMINUM TRUSS & STEEL POST

SHEET NO. OF SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302K)TS&N-3	COOK	153	107
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	



TYPICAL TRUSS UNIT
(Sign and walkway omitted for clarity)

Note: For Section B-B and Section C-C, see Base Sheet OSC-A-3.

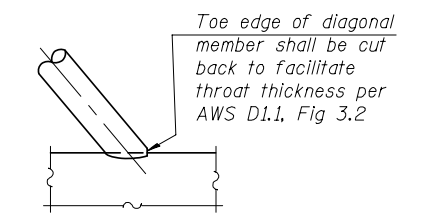
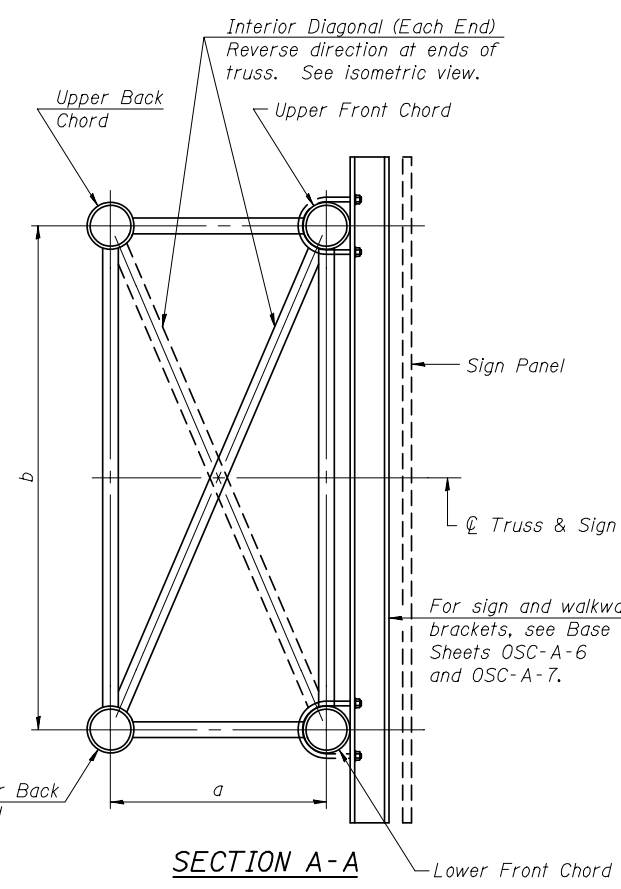
There are twice as many horizontal diagonals as there are vertical diagonals.

TRUSS UNIT TABLE

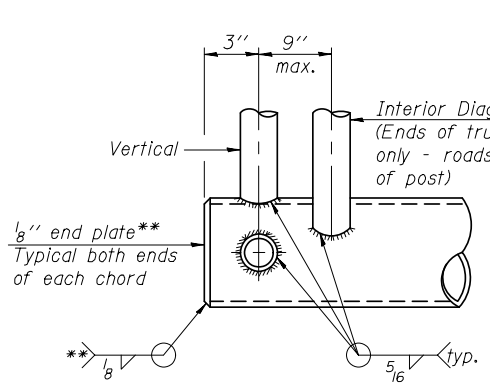
Truss Type	Dimension "a"	Dimension "b"	Dimension "s"	Limits for Panel Spacing (P)*	Up. & Low. Chord		Verticals; Horizontals; Vertical, Horizontal, and Interior Diagonals	
					O.D.	Wall	O.D.	Wall
I-C-A	24"	54"	16"	36" min. to 48" max.	5"	5/16"	2 1/2"	5/16"
II-C-A	36"	66"	21"	42" min. to 54" max.	6 1/2"	5/16"	3 1/4"	5/16"
III-C-A (35' Max.)	36"	84"	21"	48" min. to 66" max.	7"	3/8"	3 1/2"	3/8"
III-C-A (>35' to 40')	36"	84"	21"	48" min. to 66" max.	8"	3/8"	3 1/2"	3/8"

*P = $\frac{L-s-3''}{\# \text{ Panels}}$

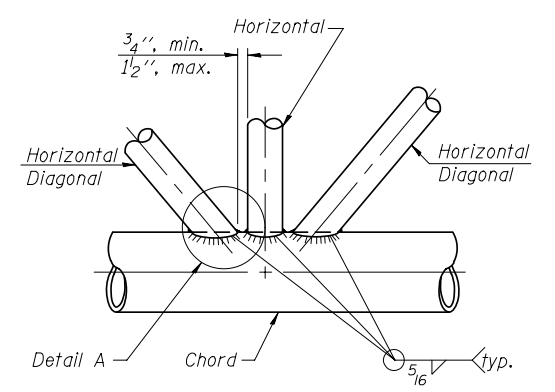
Structure Number	Station	Truss Type	Design Length (L)	Number of Panels Per Unit	Panel Length (P)*
IC016S072R(NA)	132+00.0	III-C-A	35'-6"	5	6.7'



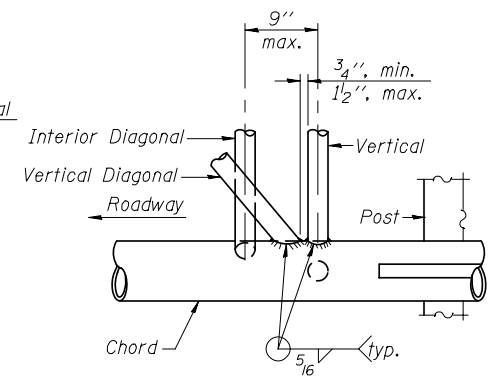
DETAIL A



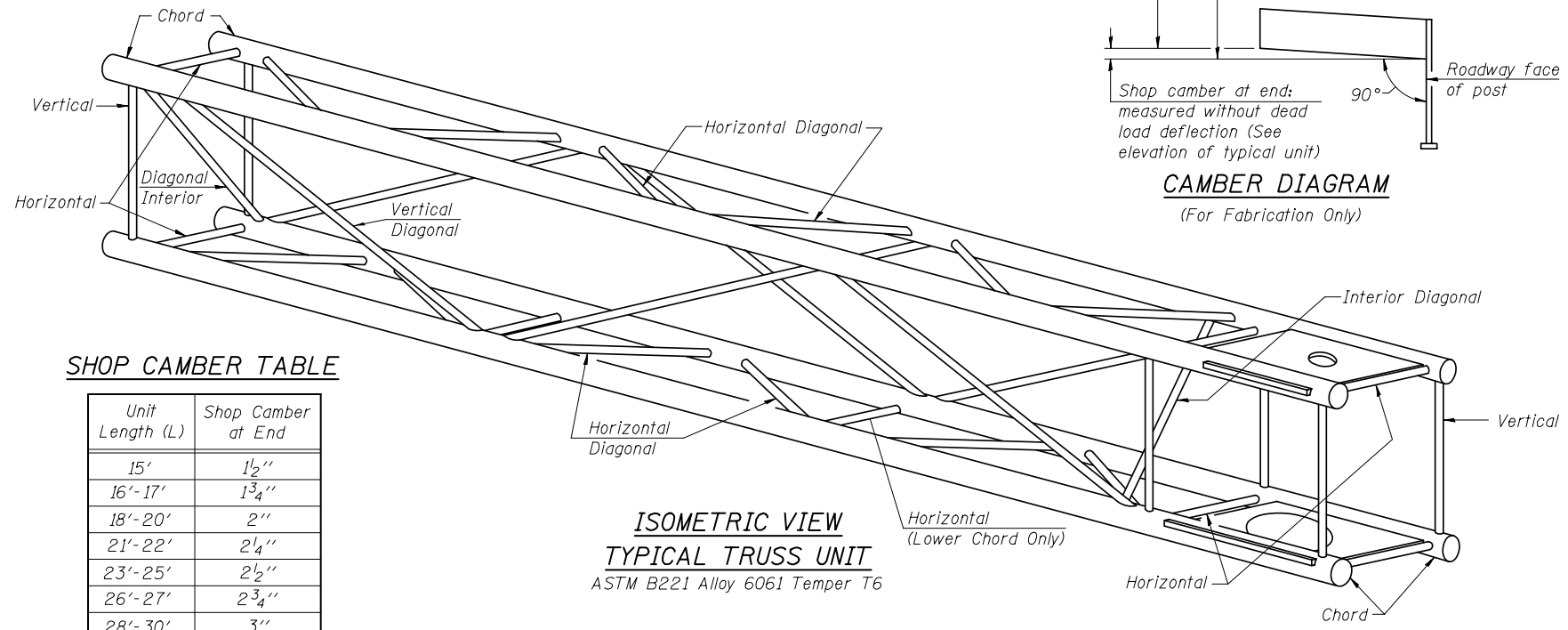
CANTILEVER END JOINT DETAIL



TRUSS INTERIOR JOINT DETAIL

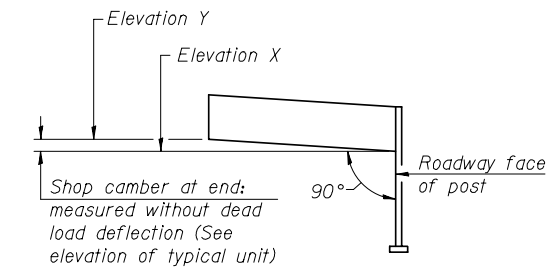


POST END JOINT DETAIL



SHOP CAMBER TABLE

Unit Length (L)	Shop Camber at End
15'	1 1/2"
16'-17'	1 3/4"
18'-20'	2"
21'-22'	2 1/4"
23'-25'	2 1/2"
26'-27'	2 3/4"
28'-30'	3"
31'-32'	3 1/4"
33'-35'	3 1/2"
36'-37'	4"
38'-40'	4 1/2"



OSC-A-2

6-1-12

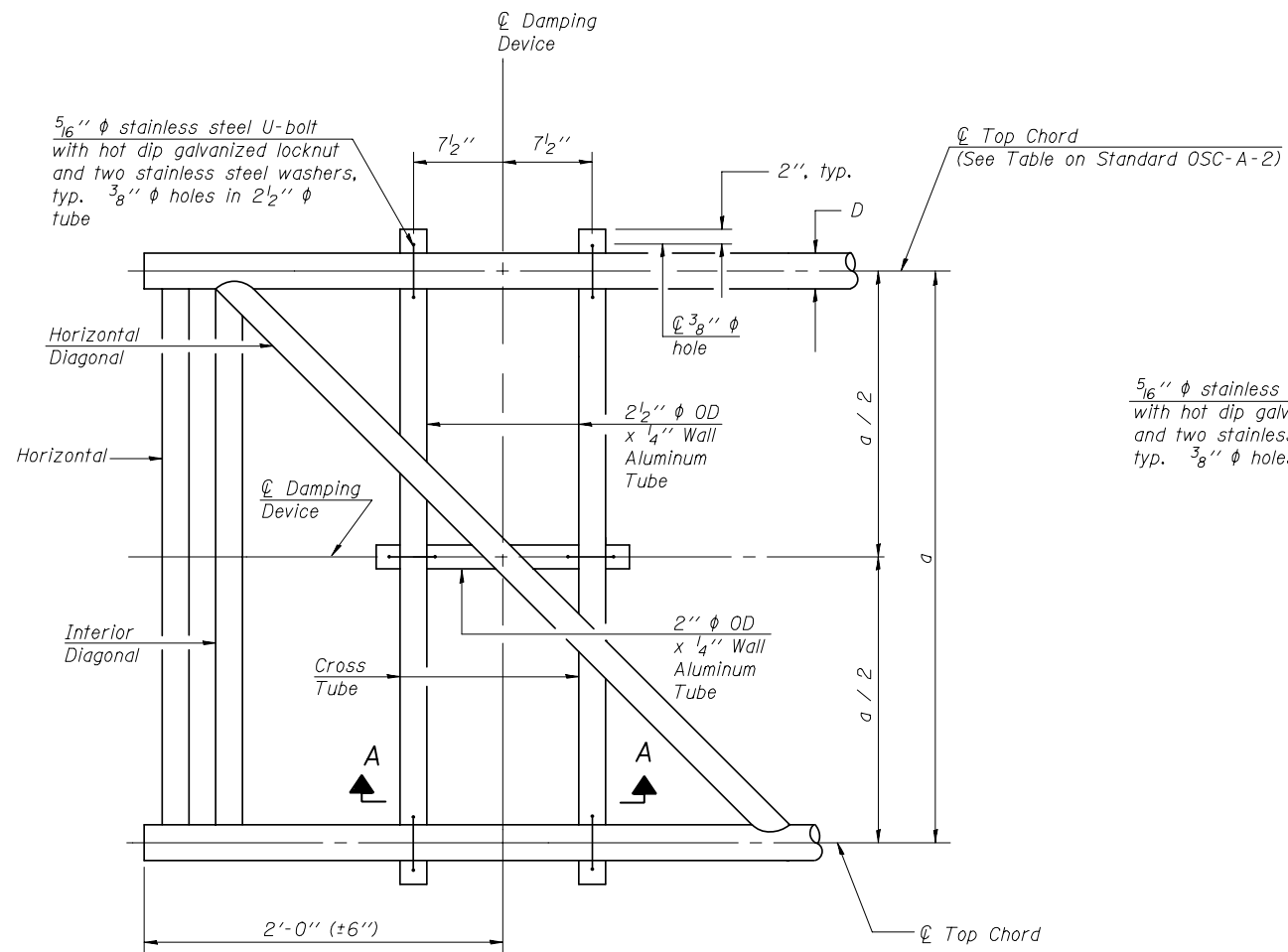
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

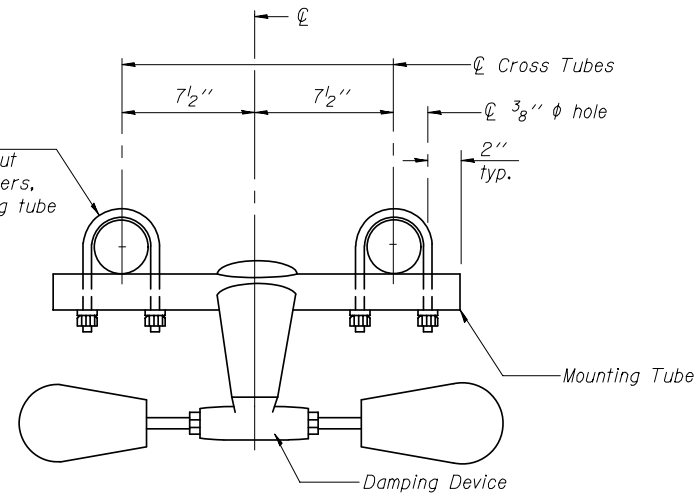
CANTILEVER SIGN STRUCTURES - TRUSS DETAILS
ALUMINUM TRUSS & STEEL POST

SHEET NO. OF SHEETS

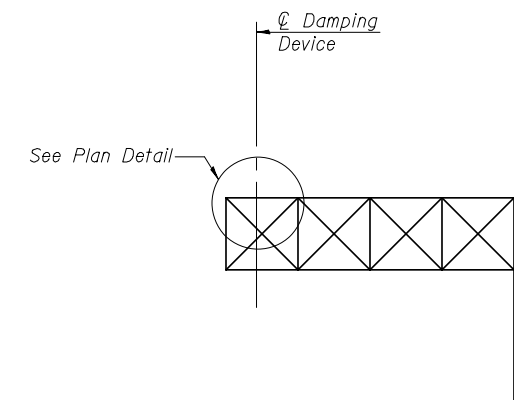
F.A.P R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302K)TS&N-3	COOK	153	108
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	



PLAN DETAIL



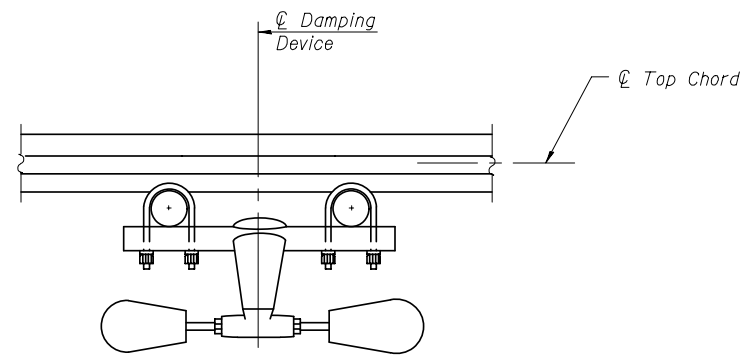
TRUSS DAMPING DEVICE CONNECTION DETAIL



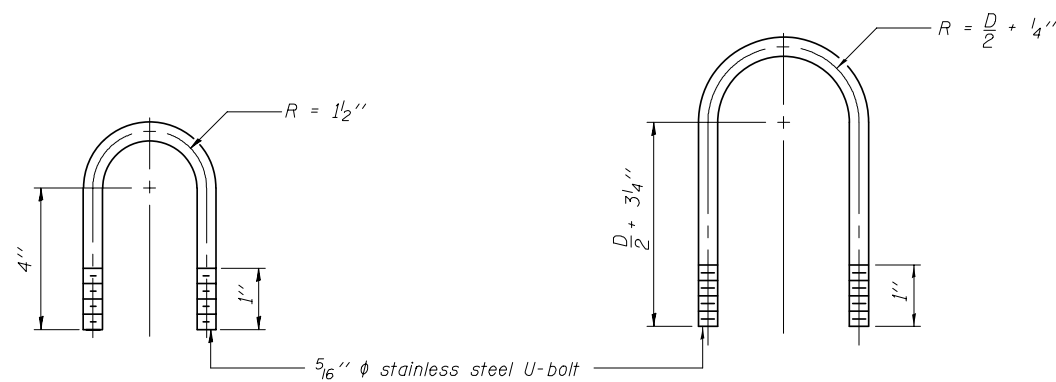
ELEVATION
Aluminum Cantilever Sign Structure

GENERAL NOTES

- Damper: One damper per truss. (31 lbs. Stockbridge-Type Aluminum-29" minimum between ends of weights)
- Materials: Aluminum tubes shall be ASTM B221 alloy 6061 temper T6



SECTION A-A



DAMPING DEVICE MOUNTING TUBE U-BOLT DETAIL
(Typical)

TOP CHORD TO CROSS TUBE U-BOLT DETAIL
(Typical)

OSC-A-D

6-1-12

FILE NAME =	USER NAME =	DESIGNED -	REVISED
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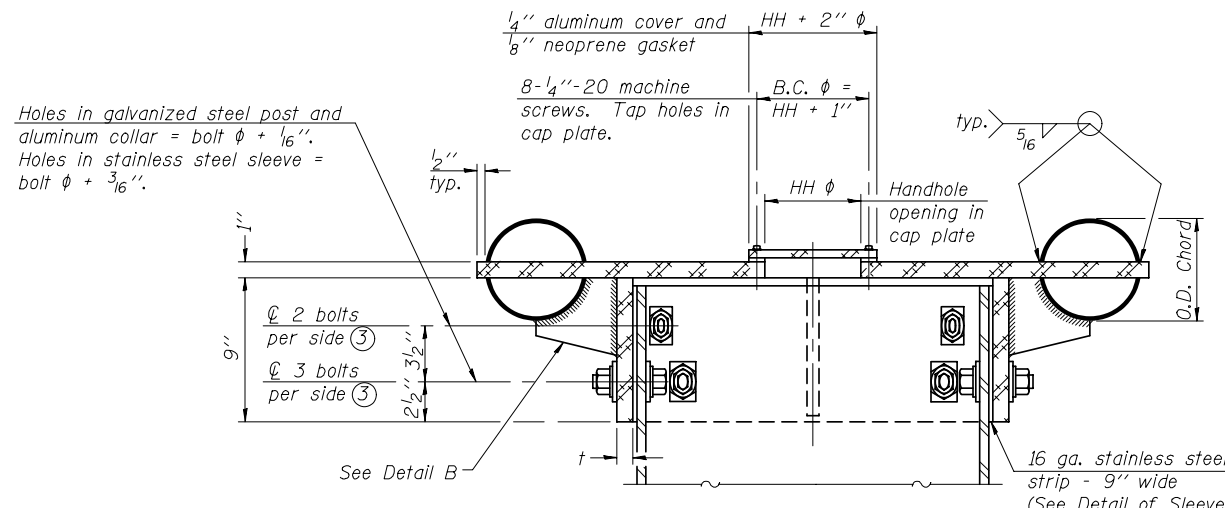
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CANTILEVER SIGN STRUCTURE
DAMPING DEVICE**

SHEET NO. OF SHEETS

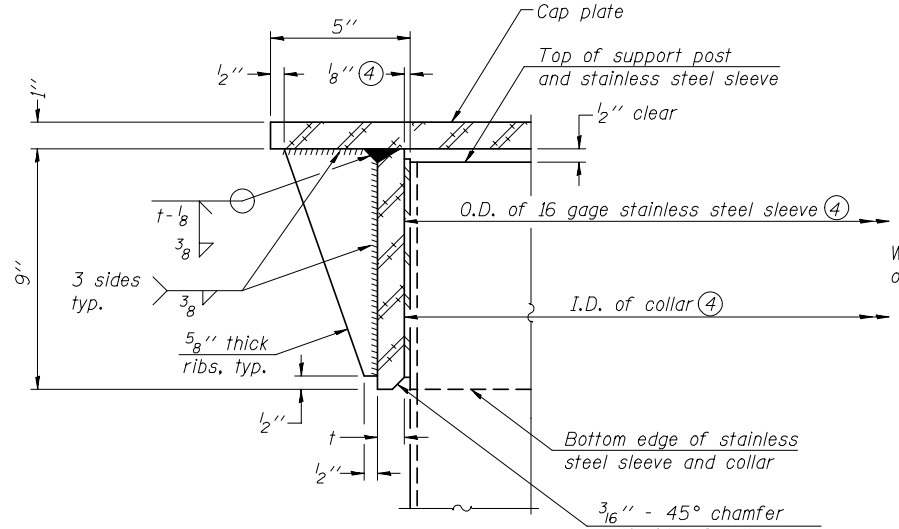
F.A.P R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302K)TS&N-3	COOK	153	109
CONTRACT NO. 60Y73				

ILLINOIS FED. AID PROJECT

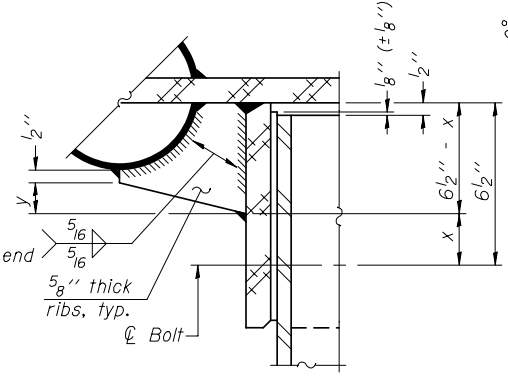


④ Collar I.D. shall be manufactured to correspond to O.D. of actual galvanized post and stainless steel sleeve plus 1/8" (+/- 1/16"). Maximum gap between post and collar at any location equals 1/8" before tightening bolts.

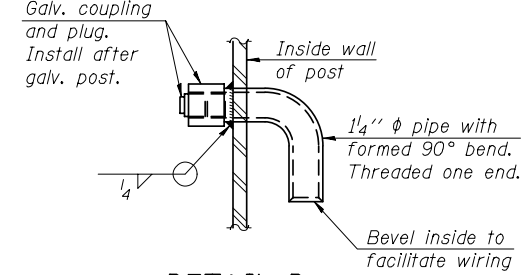
SECTION B-B
Bolts, washers (including contoured washers), and locknuts shall be stainless steel.



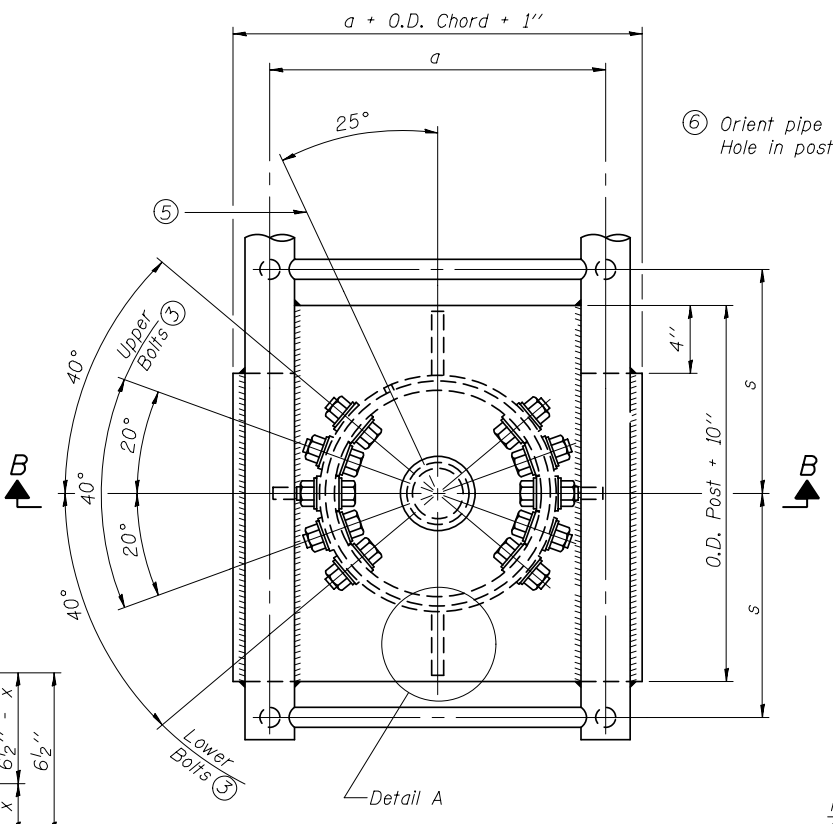
DETAIL A
(Two locations)



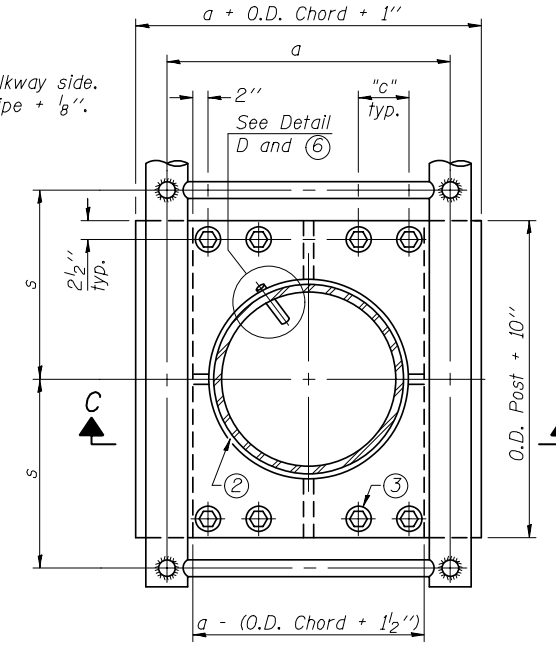
DETAIL B
Two locations
(For details not shown, see Detail C)



DETAIL D

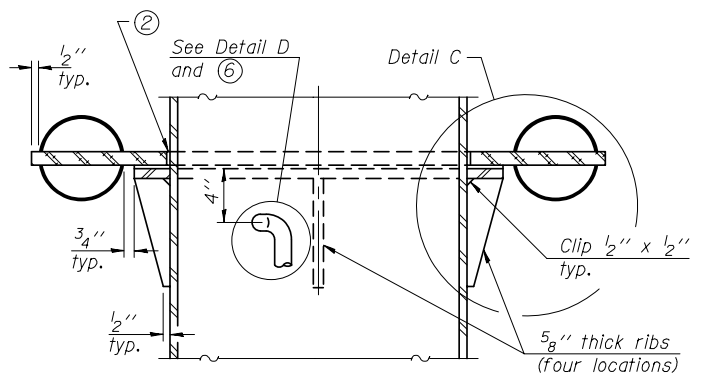


PLAN VIEW - TOP OF COLUMN
⑤ Optional full penetration weld in collar. (Two locations maximum....(180° apart)....X-ray or UT 100%)

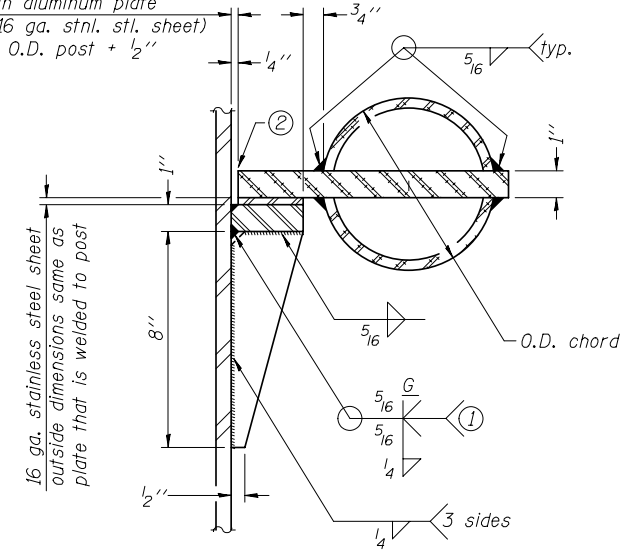


SECTION THRU POST ABOVE LOWER CHORDS

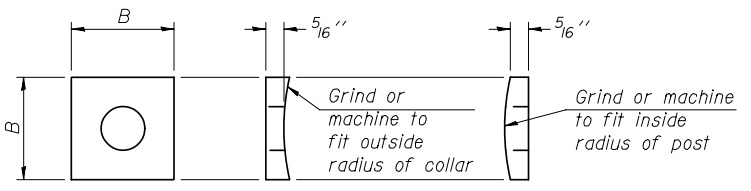
Hole in aluminum plate (and 16 ga. stnl. stl. sheet) to be O.D. post + 1/2"



SECTION C-C



DETAIL C



CONTOURED WASHERS

Bolt Size	Contoured Washers	
	Hole Dia.	B
7/8"	1"	2 1/2"
1"	1 1/8"	3"
1 1/4"	1 3/8"	3 1/4"

DETAIL OF STAINLESS STEEL SLEEVE

Weld to post after galvanizing. (Prepare post surface to insure tight, uniform fit and allow welding.) Welds to be 1/2" long at 6" cts. along top edge and at 1/4" opening.

Truss Type	Post Size	Upper & Lower Connection Bolt Diameter ③	Lower Juncture Bolt Spacing Dimension "c" ③	Opening in Cap Plate "HH"	Collar Thickness (t)	Side Ribs	
						x	y
I-C-A	16" φ (83#/')	7/8"	3 1/4"	8"	5/8"	1 3/4"	2 1/4"
II-C-A	24" φ (125#/')	1"	3 1/2"	12"	7/8"	2"	1 1/4"
III-C-A (35' max.)	24" φ (125#/')	1 1/4"	3 1/2"	12"	7/8"	2"	1"
III-C-A (>35' to 40')	24" φ (171#/')	1 1/4"	3 1/2"	12"	7/8"	2"	1"

- ① Grind top if required to fully seat aluminum plate and stainless steel sheet.
- ② After tightening lower connection bolts, fill gap with non-hardening, silicone caulk suitable for exterior exposure and acceptable to the Engineer. Cost is included in Overhead Sign Structure Cantilever.
- ③ Upper and lower connection bolts in collar and bolts at lower chord connection shall be high strength with matching locknuts. Connection bolts shall have 2 stainless steel flat washers each.

OSC-A-3

6-1-12

FILE NAME =	USER NAME =	DESIGNED -	REVISED
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		DRAWN -	REVISED
		CHECKED -	REVISED

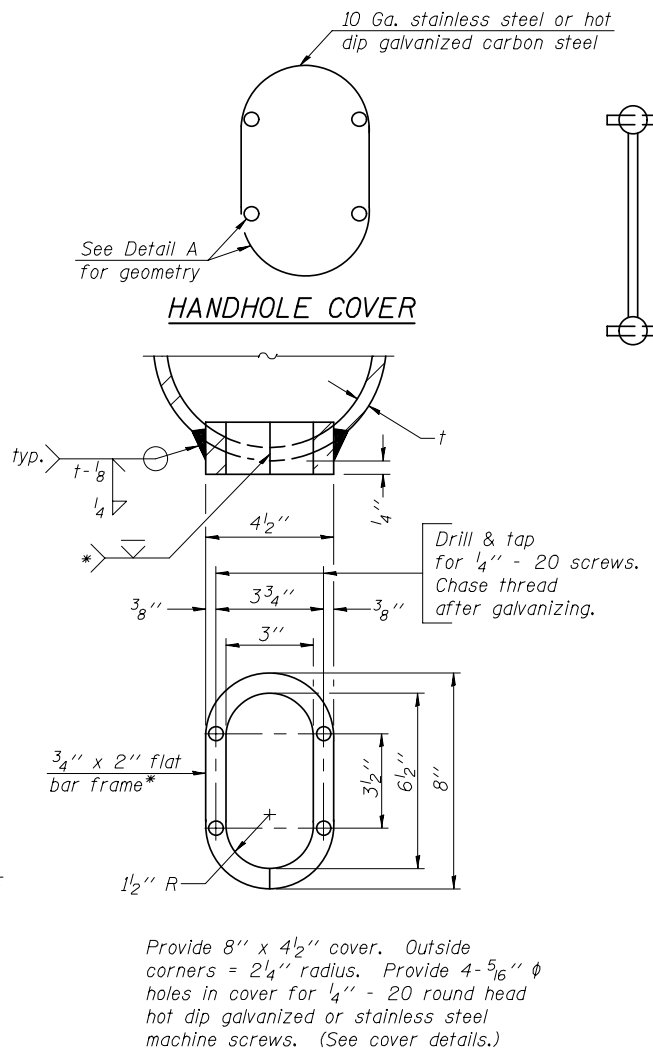
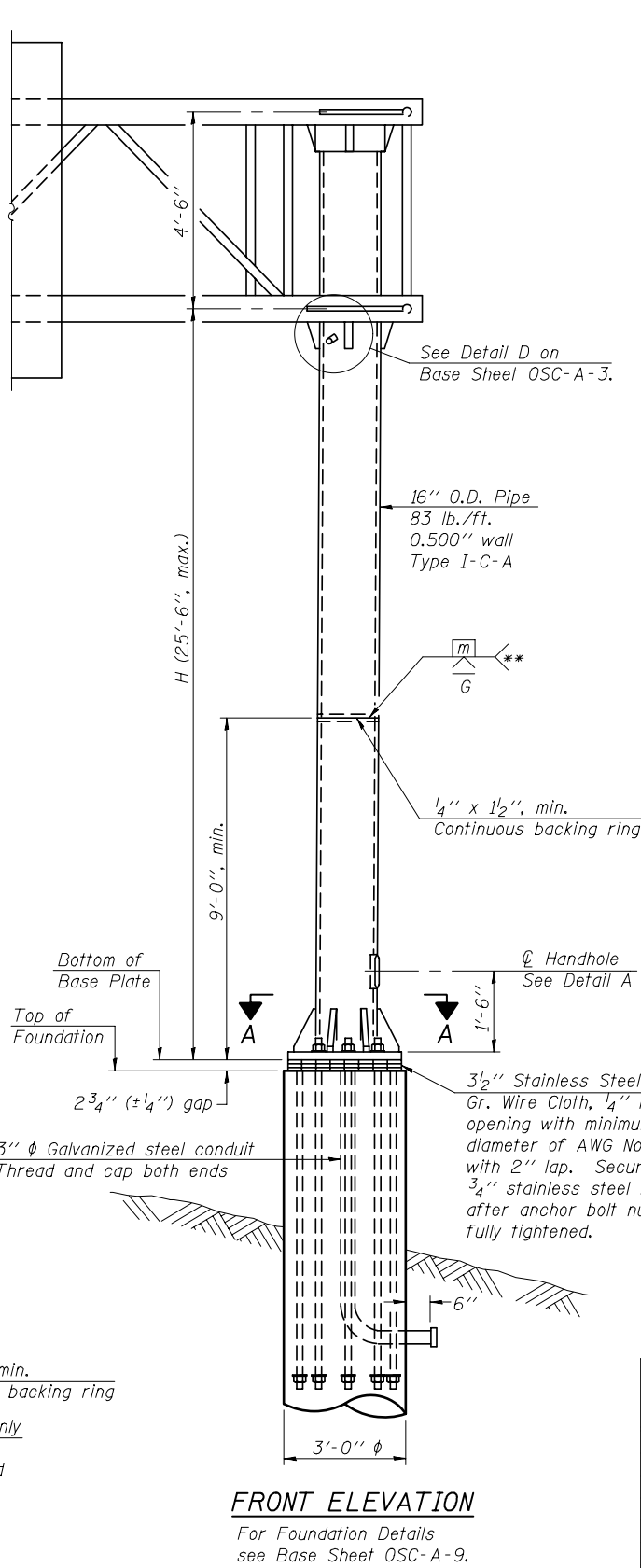
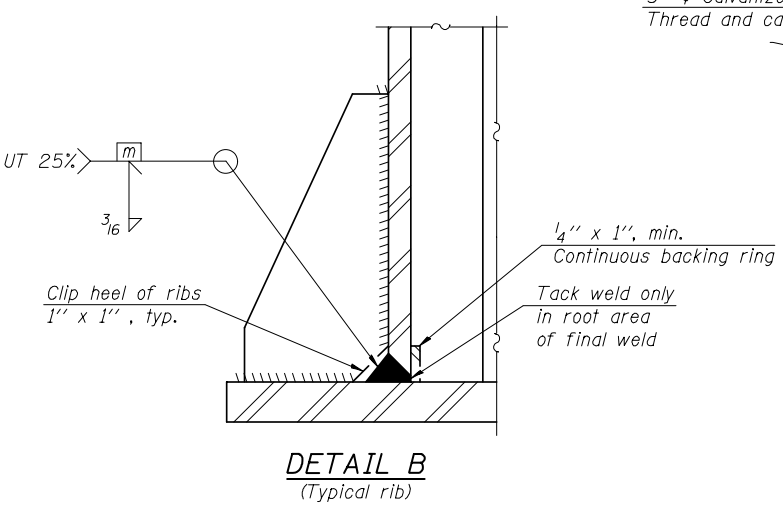
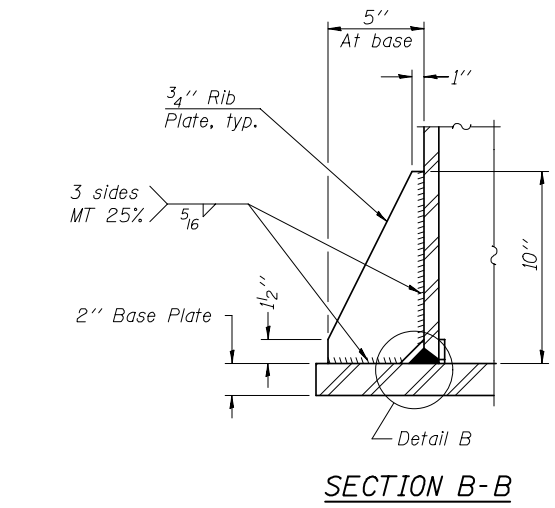
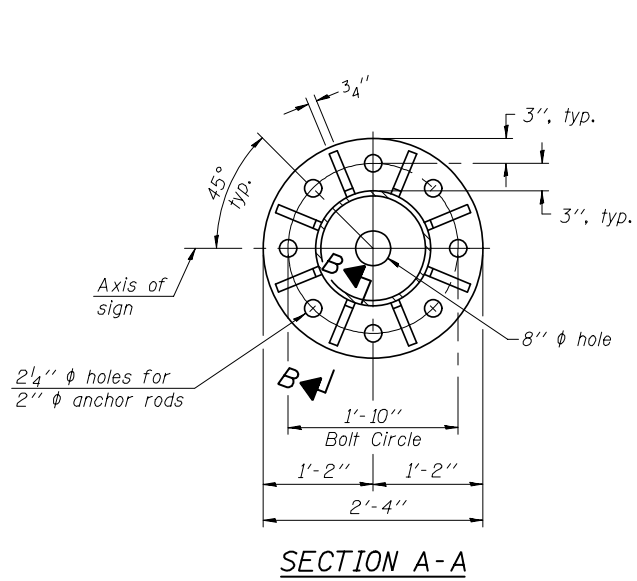
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - JUNCTURE DETAILS
ALUMINUM TRUSS & STEEL POST

SHEET NO. OF SHEETS

F.A.P R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302K)TS&N-3	COOK	153	110
CONTRACT NO. 60Y73				

ILLINOIS FED. AID PROJECT



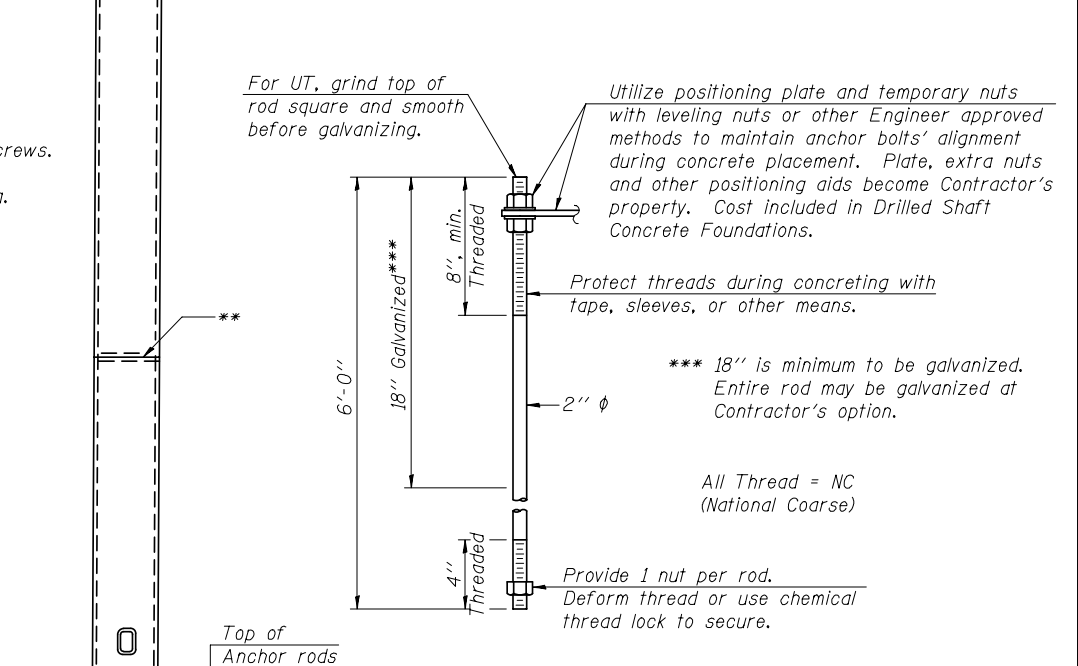
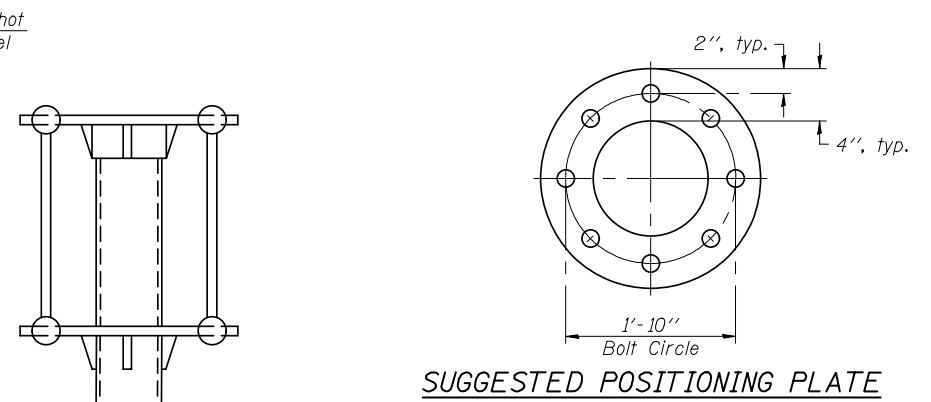
DETAIL A

* Bent bars may be butt welded top and bottom or bottom only. In lieu of fabricated handhole frame as shown, may cut from 2" plate (rolling direction vertical). All cut faces to be ground to ANSI Roughness of 500 μ in or less.

** Butt welded joint in post is only allowed for post heights (H) over 20 ft. in length. If used, weld procedure must be preapproved by Engineer and joint shall receive 100% RT or UT (tension criteria) at Contractor's expense.

Structure Number	Station	H
IC016S072R(NA)	132+00.0	22.0'

Note: "H" based on 15'-0" or actual sign height, whichever is greater.



ANCHOR ROD DETAIL

Anchor rods shall conform to ASTM F1554 Grade 105. Galvanize the upper 18" (minimum***) and associated AASHTO M291, Grade A, C or DH heavy hex nuts and hardened washers per AASHTO M232. No welding shall be permitted on rods. Provide a nut at bottom, a hexagon locknut and washer above base plate and a leveling nut and washer below base plate. Nuts shall each be tightened with 200 lb.-ft. minimum torque against base plate. Before or after threading, but before galvanizing, each anchor rod shall be ultrasonically tested (UT) by a Level II or III Inspector, qualified in accord with ANSI guidelines, to insure no rejectable flaws exist in the upper 18" (tension criteria). Cost of testing included in Drilled Shaft Concrete Foundations.

OSC-A-4

6-1-12

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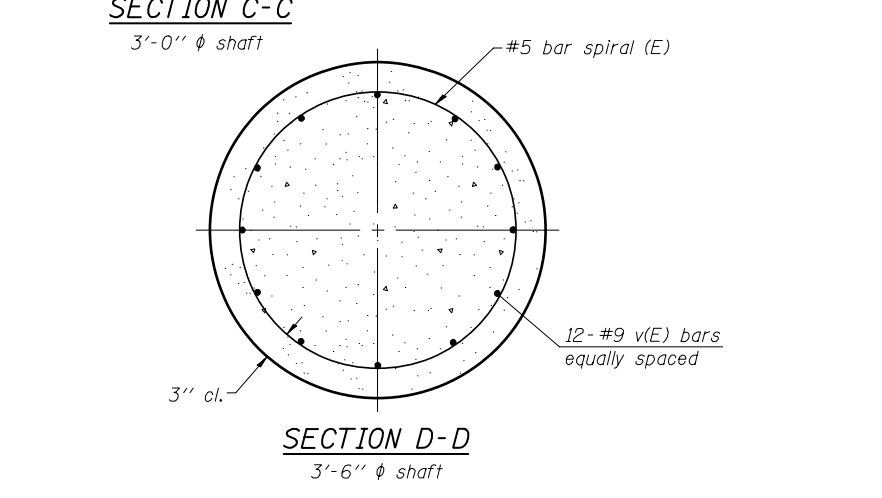
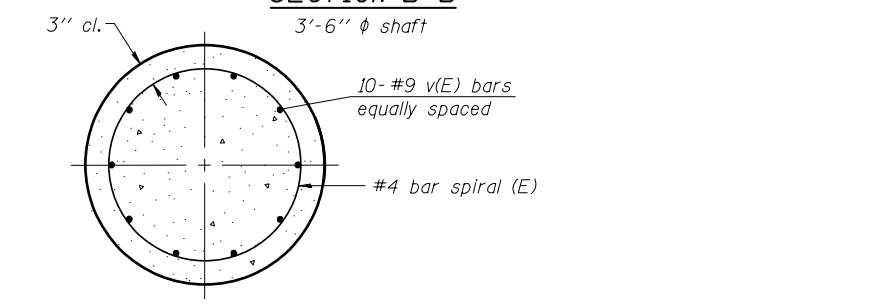
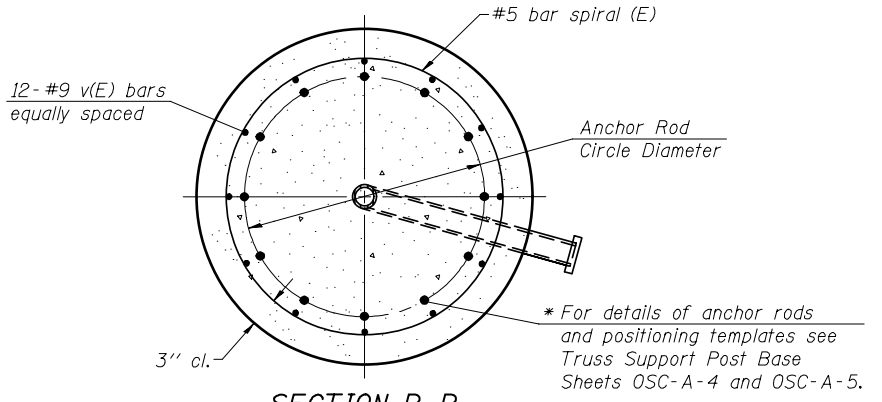
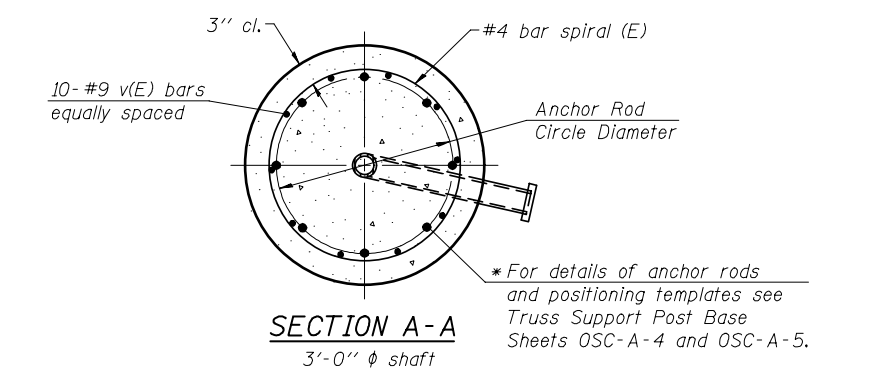
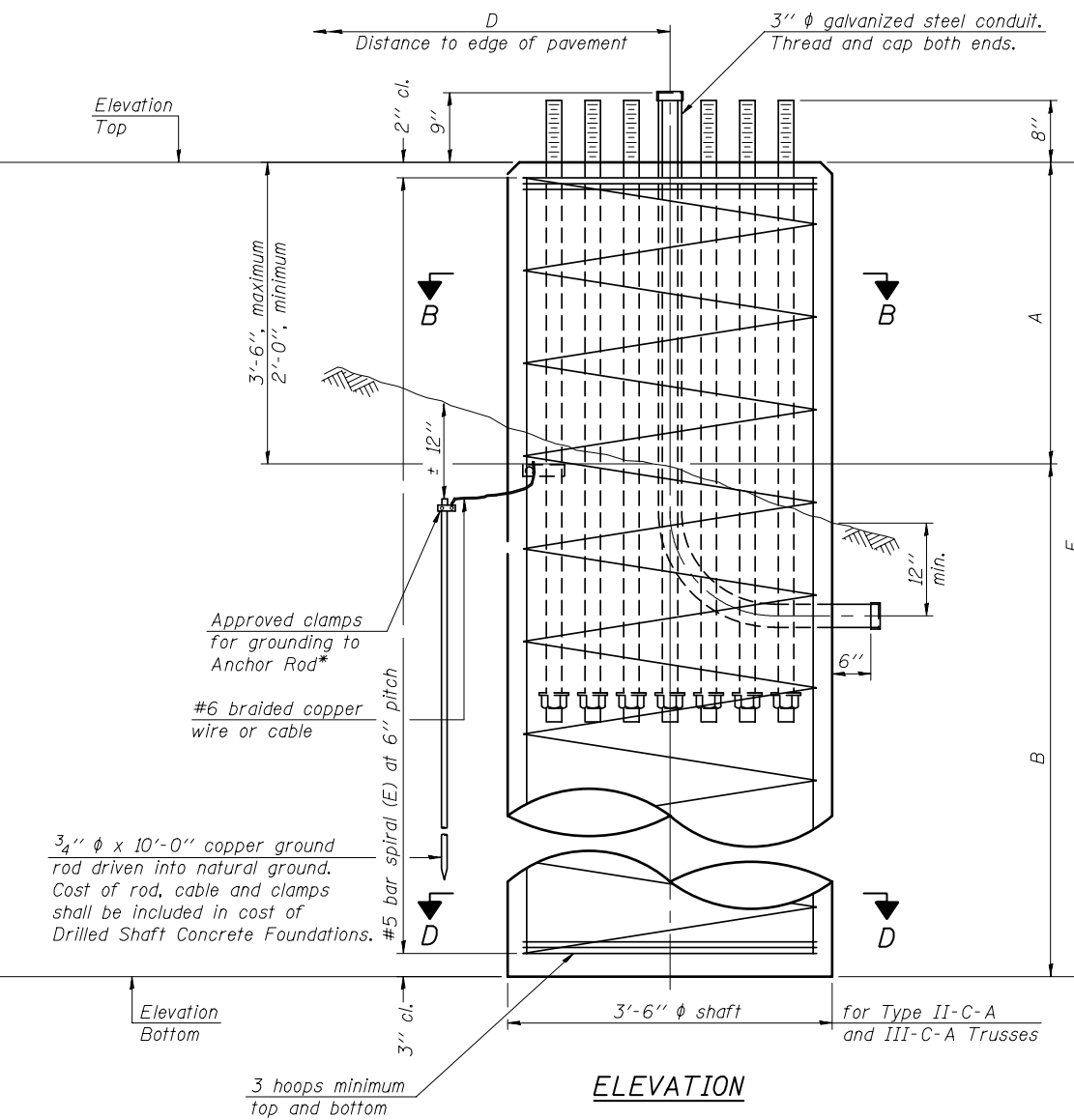
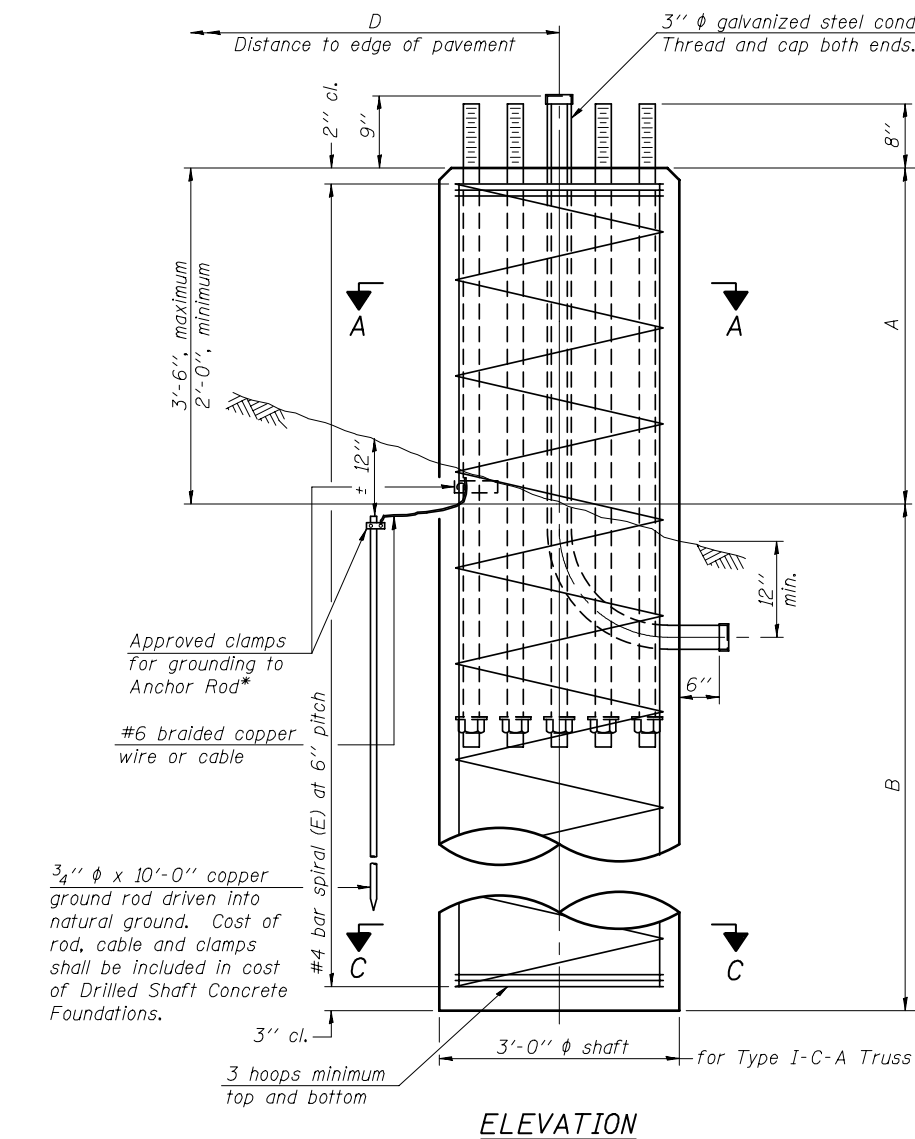
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CANTILEVER SIGN STRUCTURES - TYPE I-C-A TRUSS
SUPPORT POST - ALUMINUM TRUSS & STEEL POST

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302K)TS&N-3	COOK	153	111
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

SHEET NO. OF SHEETS

* Grind anchor rod to bright finish at ground clamp location before installing clamp.



NOTES:
 The foundation dimensions shown in the Foundation Design Table are based on the presence of mostly cohesive soils with an average Unconfined Compressive Strength (Qu) of at least 1.25 tsf, which must be determined by previous soil investigations at the jobsite. When other conditions are indicated, the boring data will be included in the plans and the foundation dimensions shown in the Foundation Data Table will be the result of site specific designs.
 If the conditions encountered are different than those indicated, the Contractor shall notify the Engineer to determine if the foundation dimensions need to be modified. If dimensions "B" or "F" are revised by more than 12" by the Contractor, "as-built" plans shall be prepared and submitted to the District Bureau of Operations for future reference.
 No sonotubes or decomposable forms shall be used below the lower conduit entrance. Permanent metal forms or other shielding may not be left in place below that elevation without the Engineer's written permission.
 Concrete shall be placed monolithically, without construction joints.
 Backfill shall be placed per Article 502 of Standard Specification and prior to erection of support column.
 A normal surface finish followed by a Bridge Seat Sealer application will be required on concrete surfaces above the lowest elevation 6" below finished ground line. Cost included in "Drilled Shaft Concrete Foundation".

Truss Type	Post Base Sheet	Maximum Cantilever Length (ft)	Maximum Total Sign Area (sq ft)	Shaft Diameter (in)	"B" Depth (ft)	Anchor Rods		Anchor Rod Circle Diameter (in)
						No.	Diameter (in)	
I-C-A	OSC-A-4	25	170	3.0	16.0	8	2	22
II-C-A	OSC-A-5	30	170	3.5	17.0	12	2	30
II-C-A	OSC-A-5	30	340	3.5	21.5	12	2	30
III-C-A	OSC-A-5	35	170	3.5	19.0	12	2	30
III-C-A	OSC-A-5	35	250	3.5	22.5	12	2	30
III-C-A	OSC-A-5	35	400	3.5	26.5	12	2	30
III-C-A	OSC-A-5	40	400	3.5	32.0	12	2	30

Structure Number	Station	Truss Type	Shaft Diameter	Elevation Top	Elevation Bottom	Qu	A	B	F	Class DS Concrete Cubic Yards
IC016S072R(NA)	132+00.0	III-C-A	3.5	735.01	709.01	1.5	3'-6"	32'-0"	35'-6"	12.6

OSC-A-9

6-1-12

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

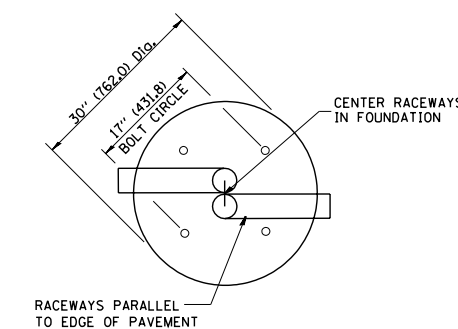
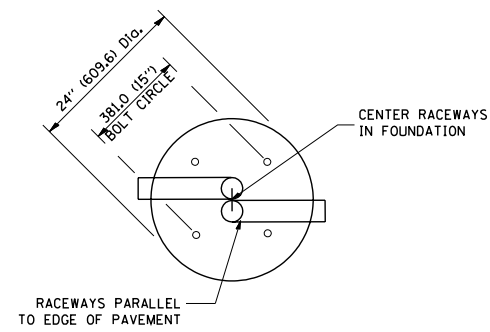
CANTILEVER SIGN STRUCTURES - DRILLED SHAFT
ALUMINUM TRUSS & STEEL POST

SHEET NO. OF SHEETS

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
314	(32-3-R&0305-302)KTS&N-3	COOK	153	112
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O _u = 0.375 TON/SO. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY O _u = 0.75 TON/SO.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY O _u = 1.50 TON/SO. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)

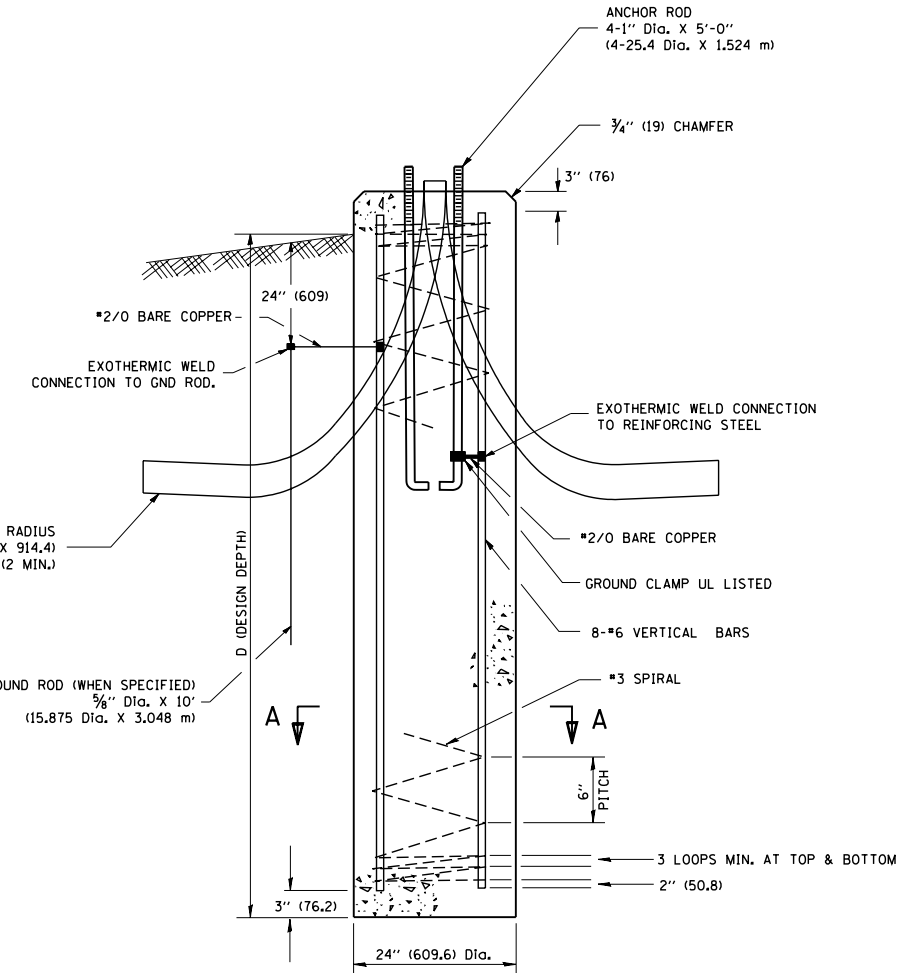


TOP VIEW

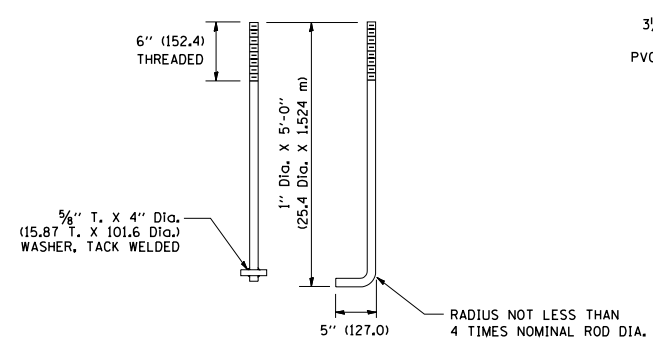
TOP VIEW

NOTES

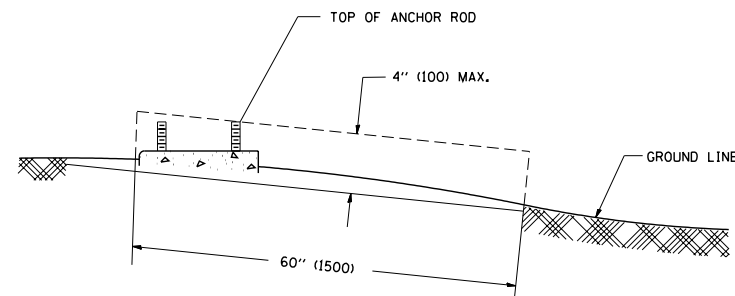
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UMG MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



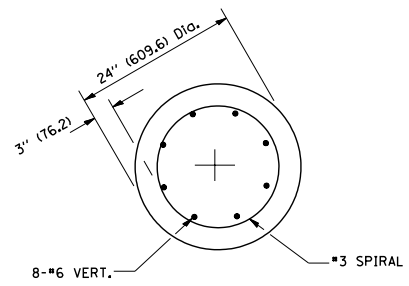
FOUNDATION DETAIL



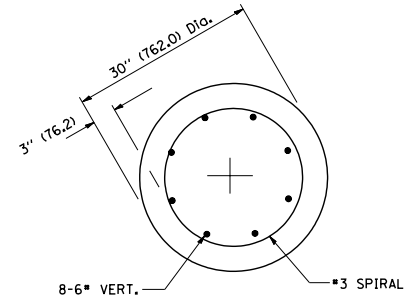
ANCHOR ROD DETAIL



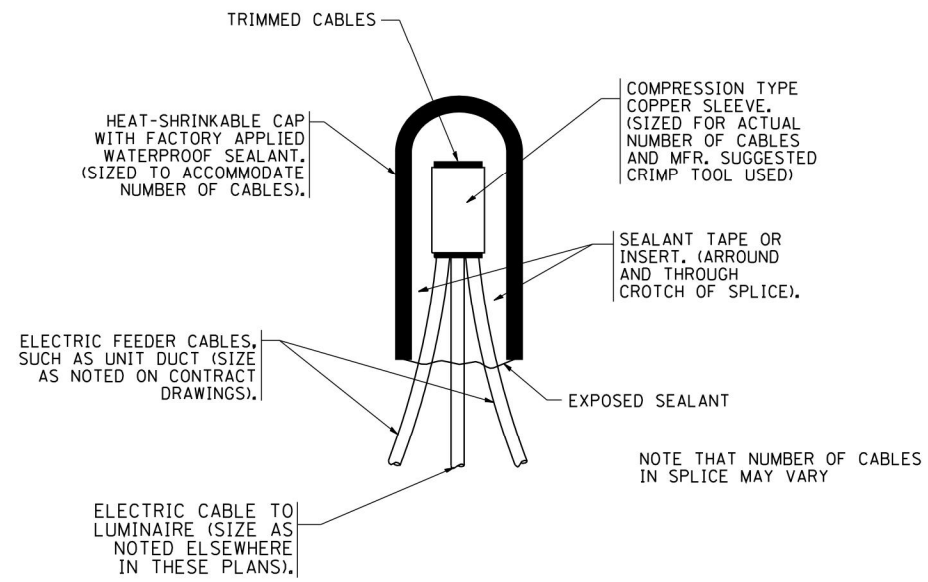
FOUNDATION EXTENSION DETAIL



SECTION A-A

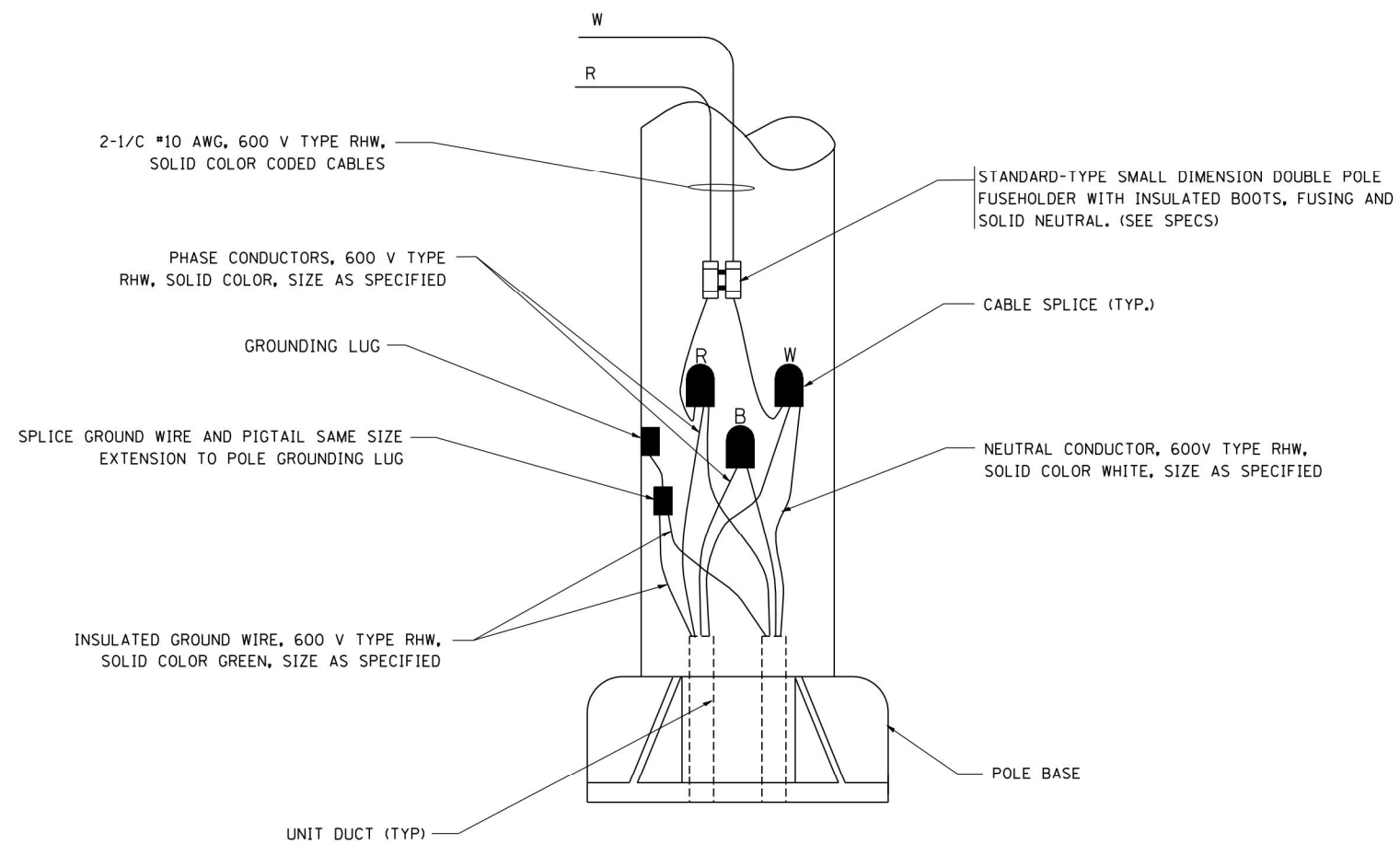


SECTION A-A



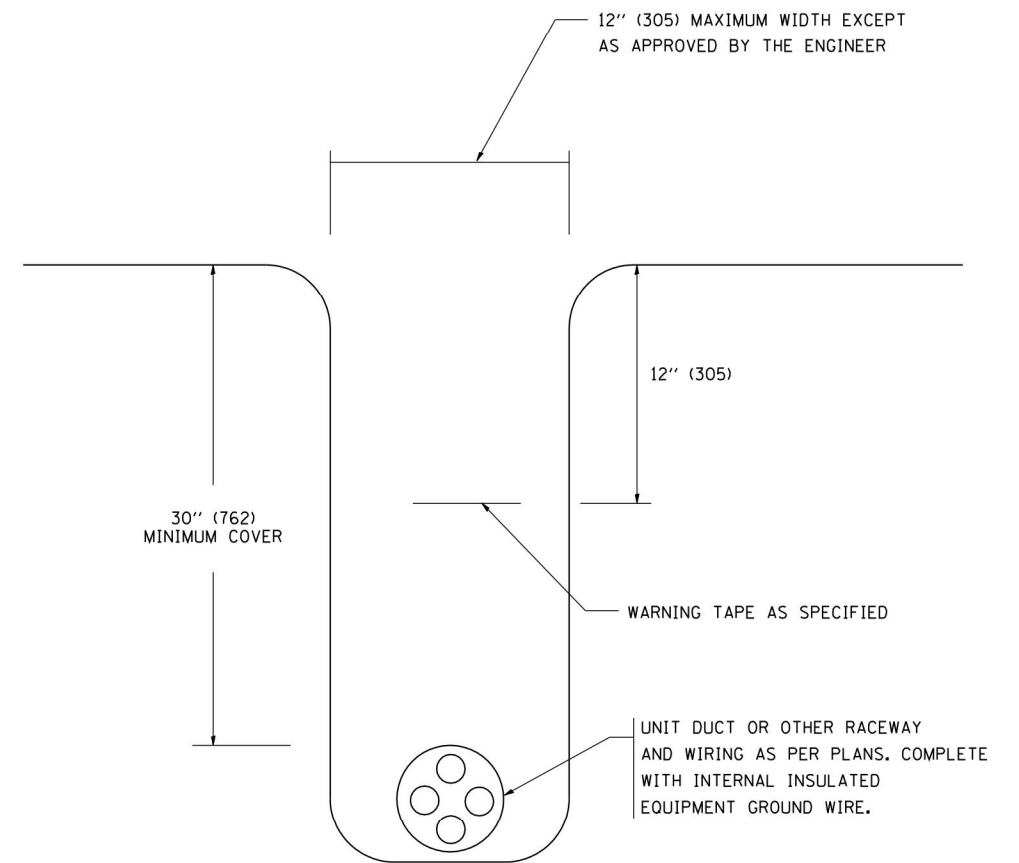
TYPICAL SPLICE DETAIL

N.T.S.



POLE WIRING DETAIL

N.T.S.



TYPICAL WIRING IN TRENCH DETAIL

N.T.S.

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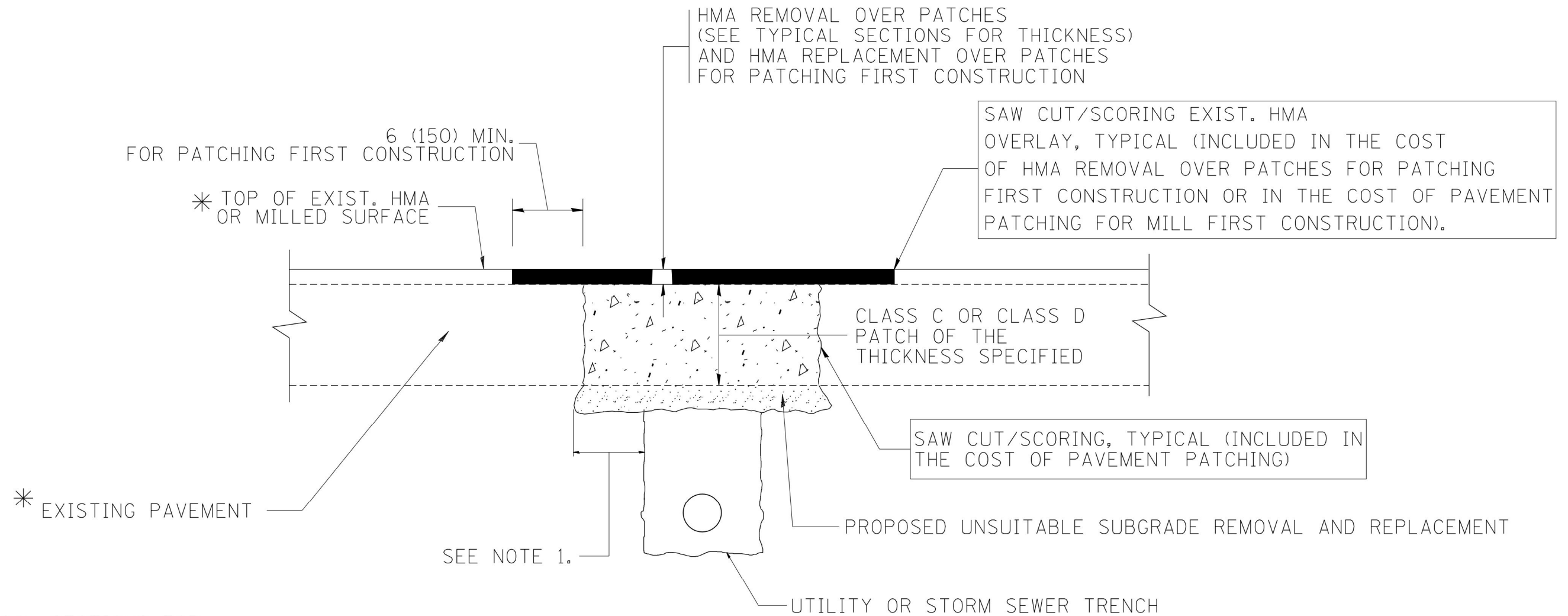
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DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**MISC. ELECTRICAL DETAILS
 SHEET A**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	114
BE-702		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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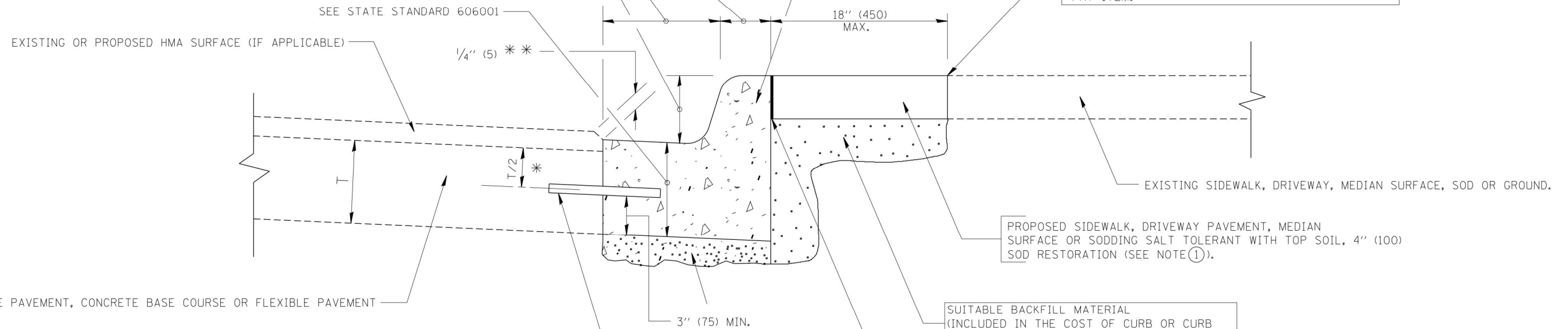
**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	115
BD400-04 (BD-22)			CONTRACT NO. 60Y73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

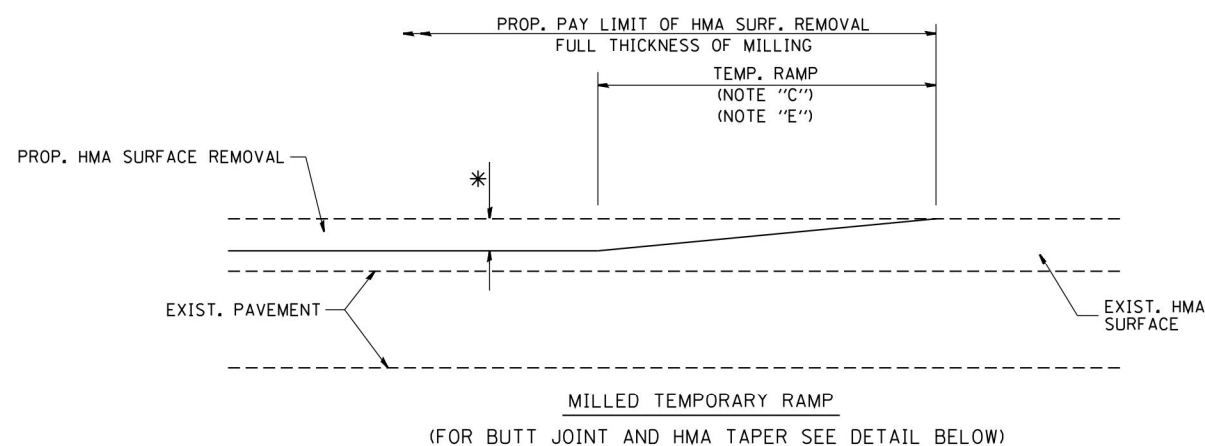
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

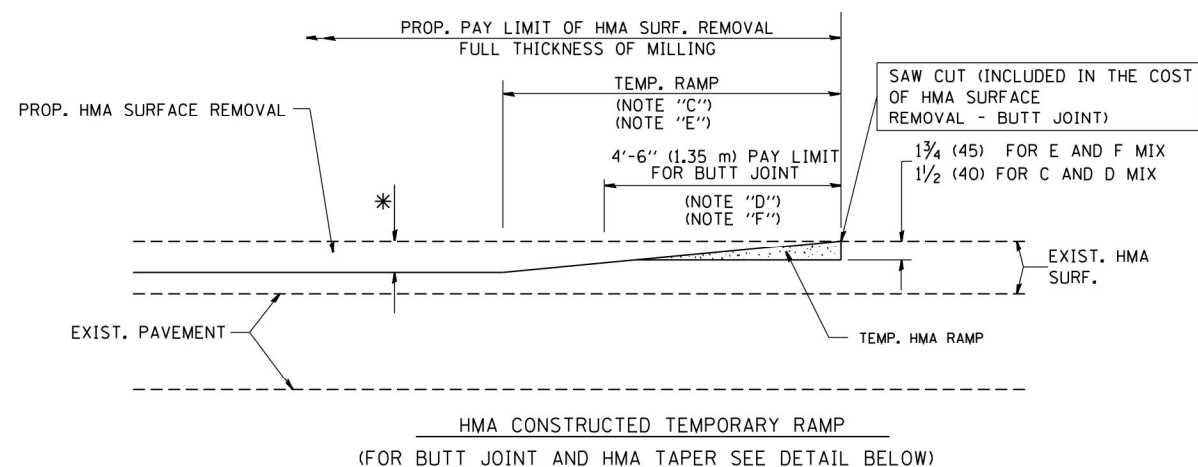
**CURB OR CURB AND GUTTER
REMOVAL AND REPLACEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	116
BD600-06 (BD-24)		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

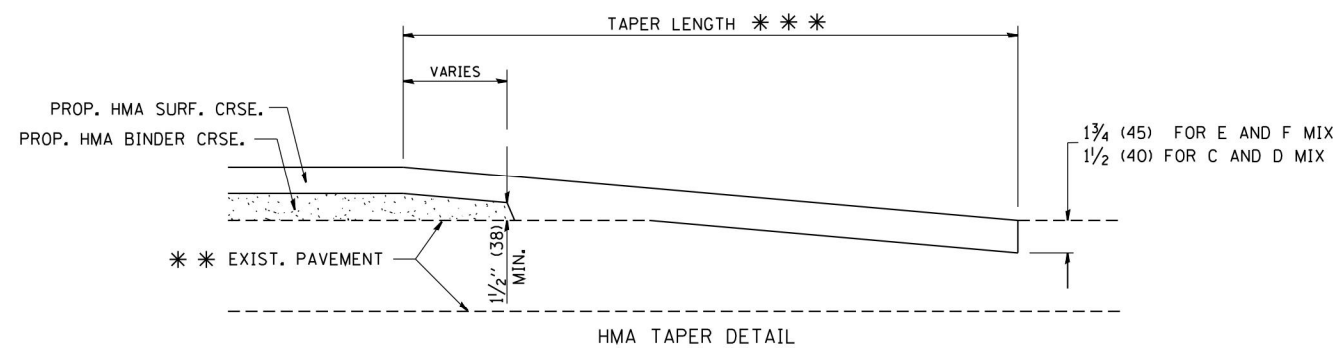
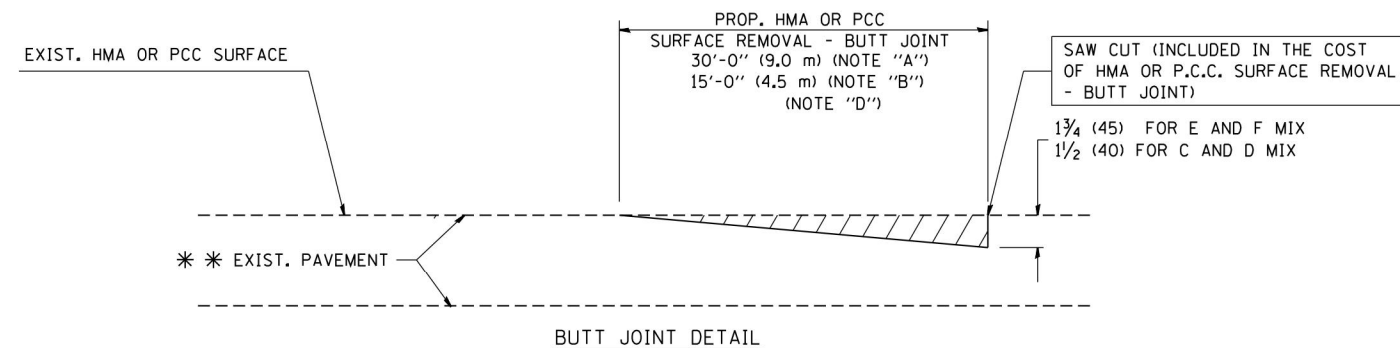


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

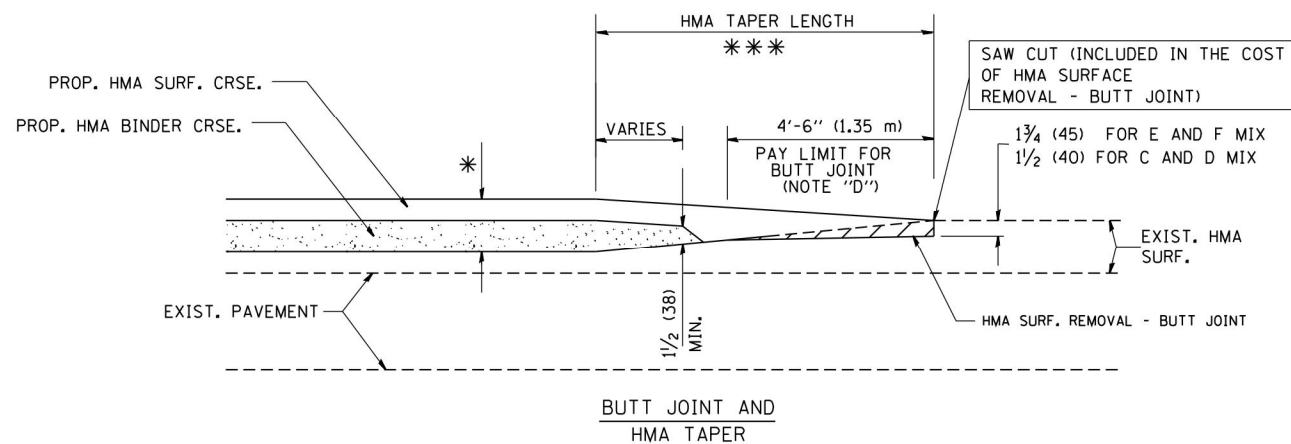
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

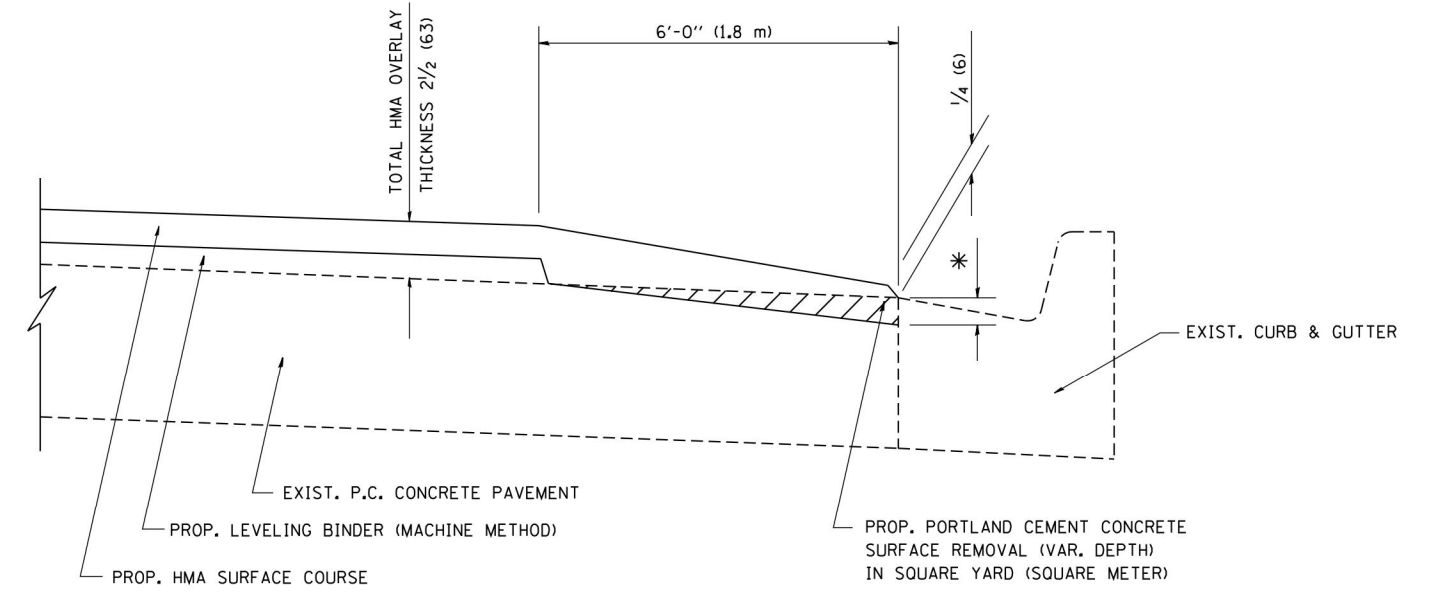
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	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	117
BD400-05 BD32		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	LEVELING BINDER	* MILLING AT GUTTER FLAG	
MIX	THICKNESS	THICKNESS	
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
F	1 3/4 (44)	3/4 (19)	1/2 (38)

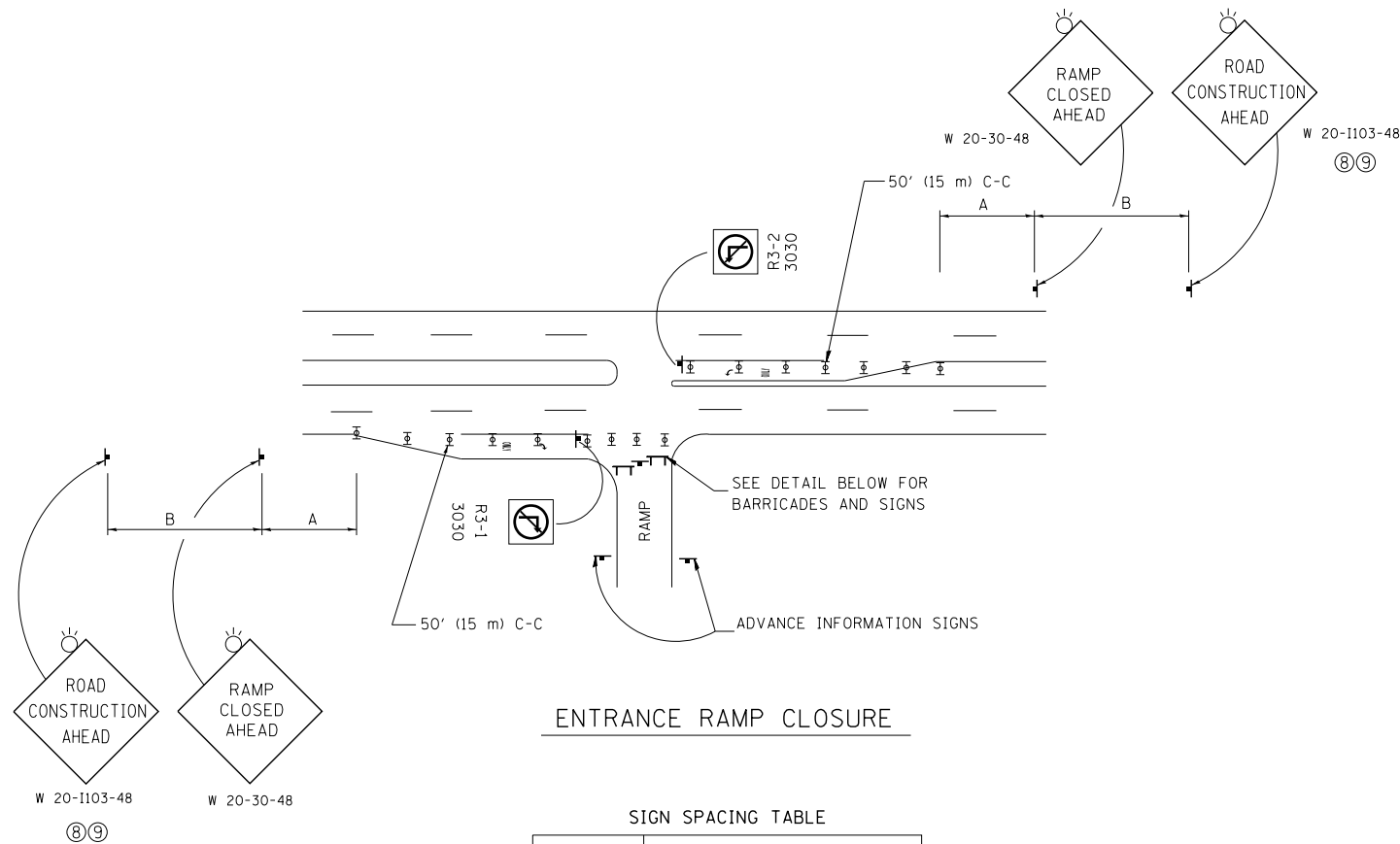
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	PLOT DATE = 1/4/2008	DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C. PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	118
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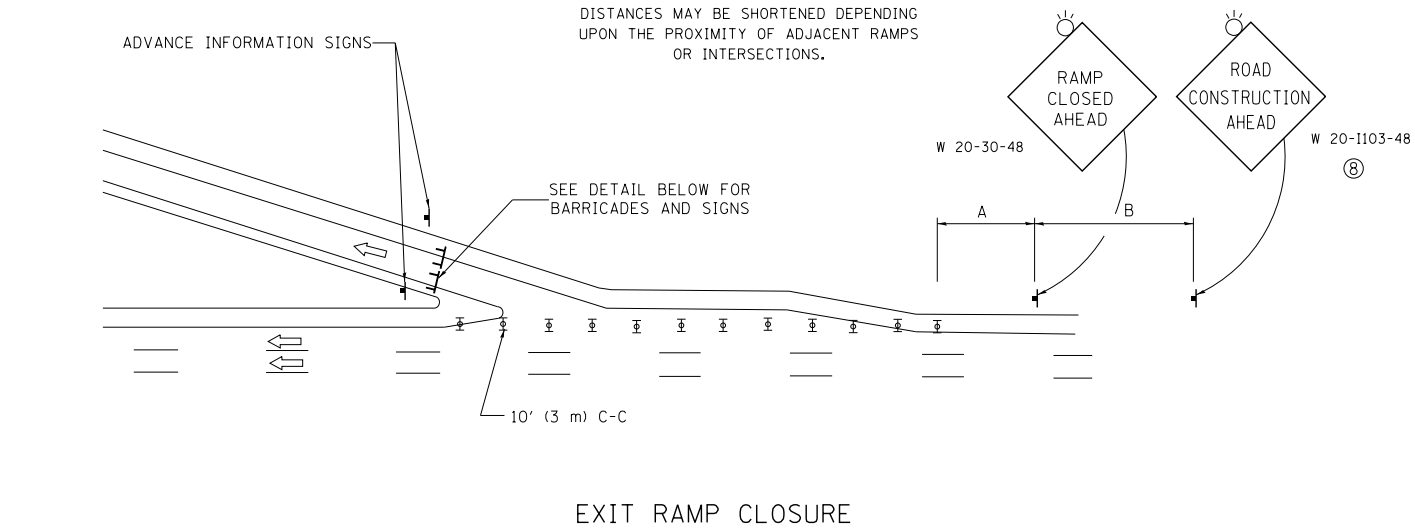


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

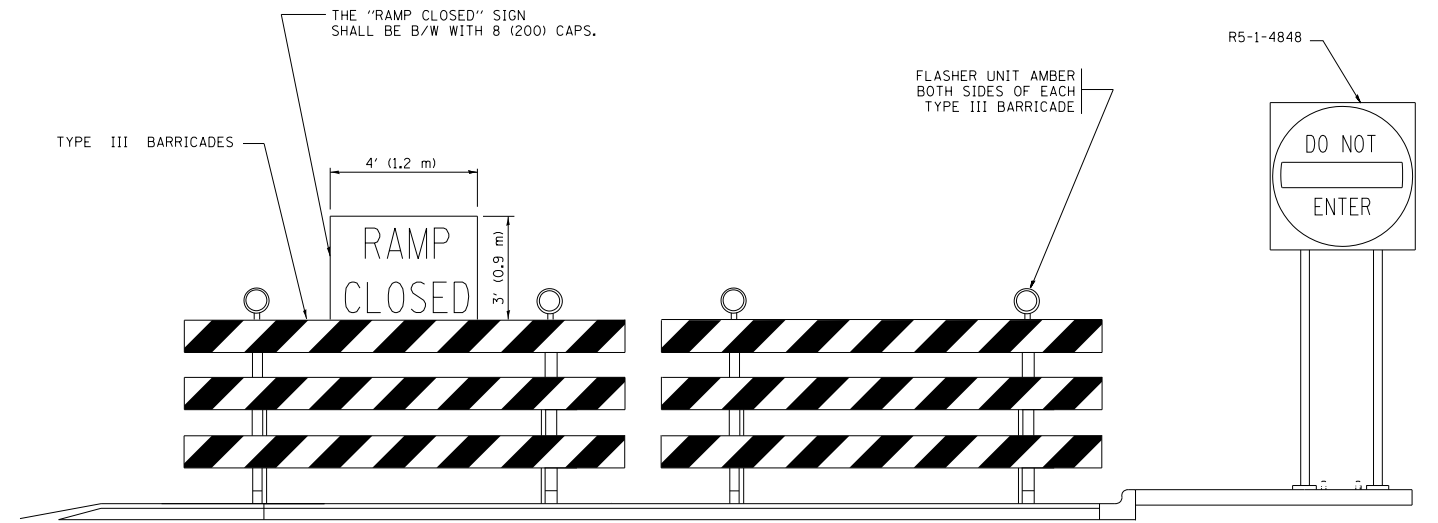
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

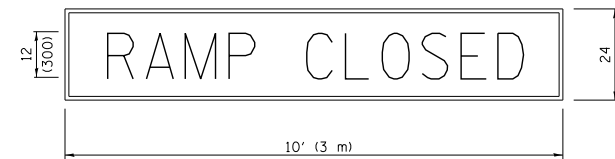
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

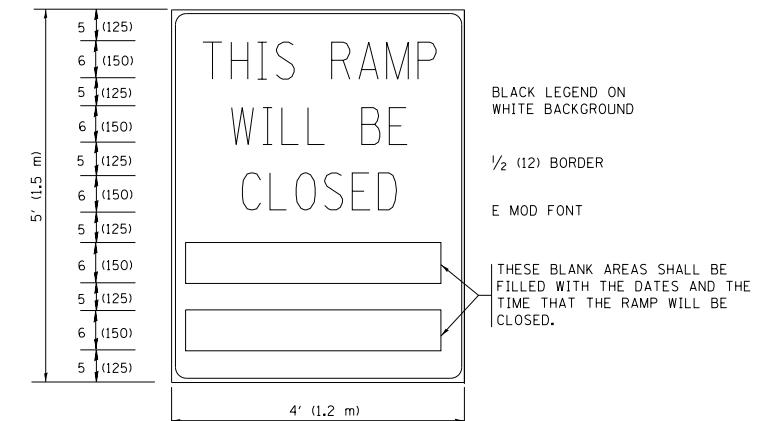
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06
ct:\pwork\pwork\dot\footemj\d0108315\tc08.dgn		DRAWN -	REVISED - SPB 01-07
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	PLOT DATE = 7/8/2013	DATE - 02-83	REVISED - MD 06-13

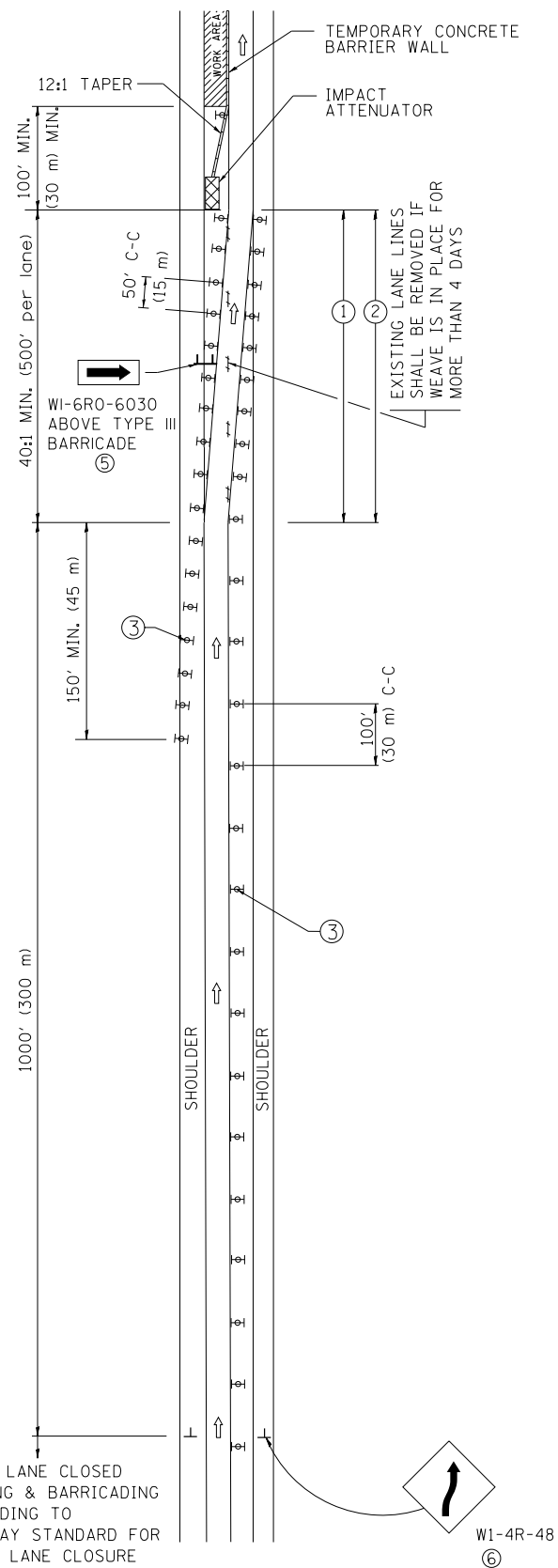
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

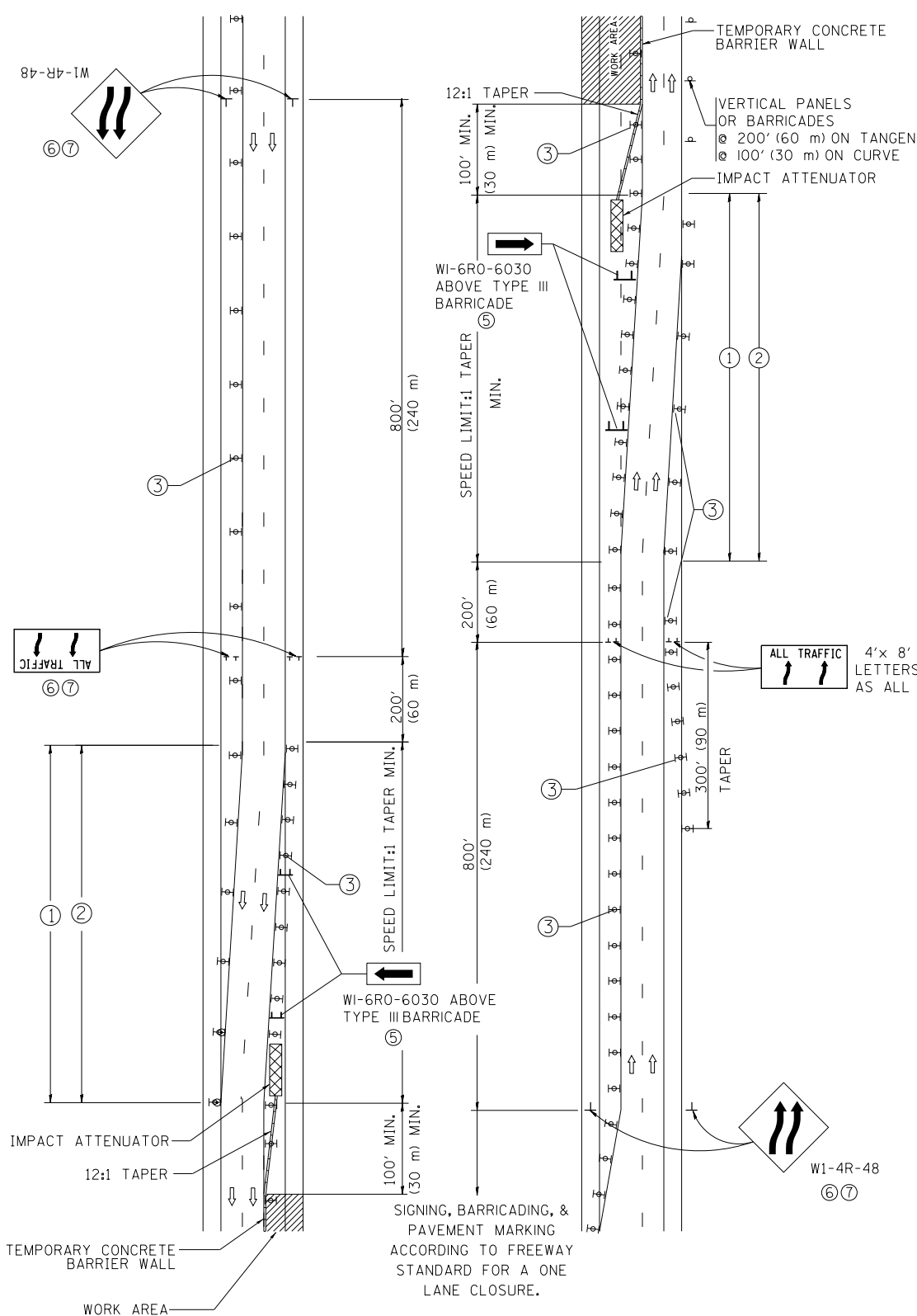
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	118A
TC-08			CONTRACT NO. 60Y73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

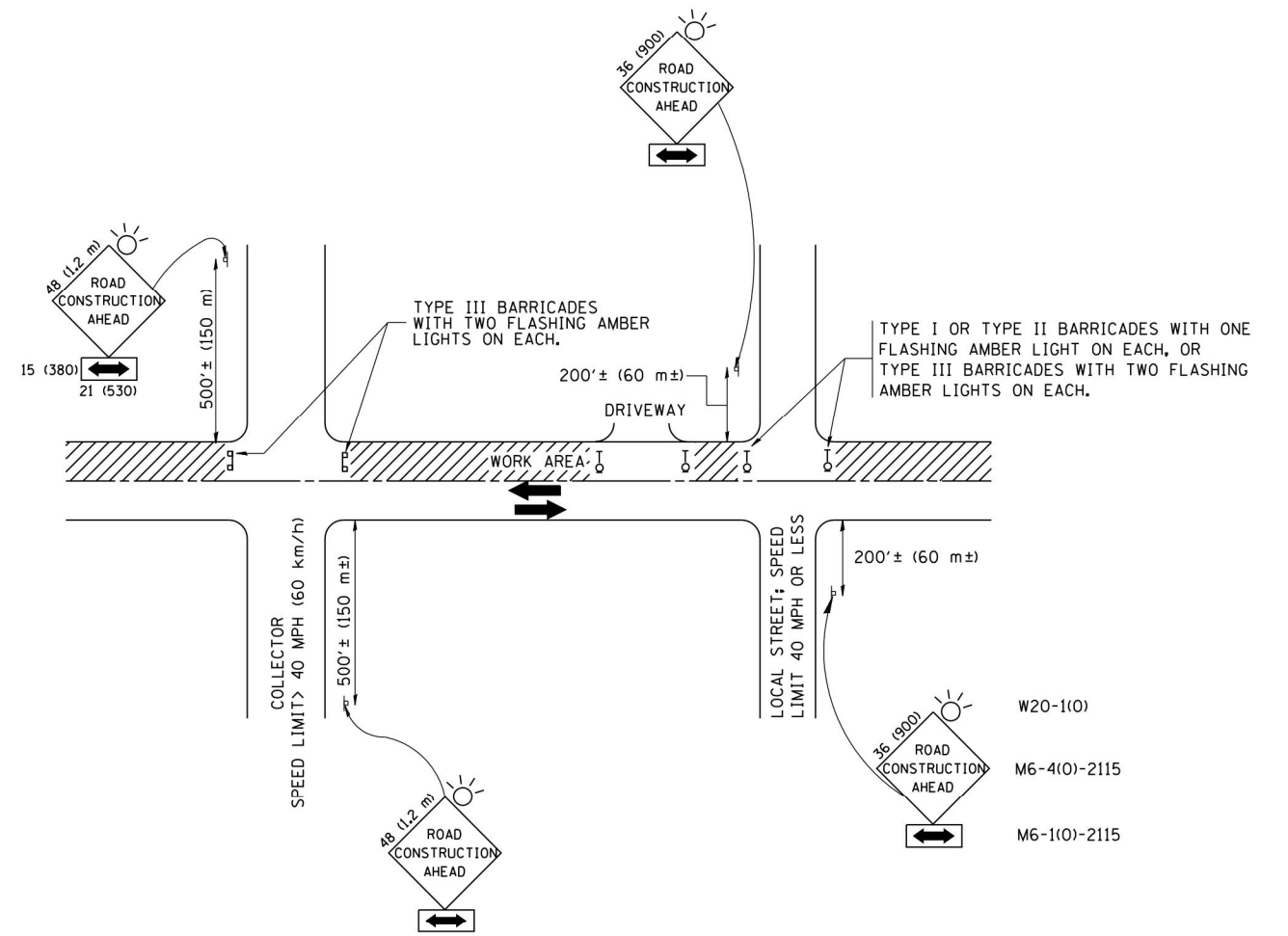
- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = footemj	DESIGNED - DWS	REVISED - JAF 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw\work\p\dot\footemj\d0108315\tc09.dgn	DRAWN -	REVISED - SPB 01-07	341			(32-3-R&0305-302K)TS&N-3	COOK	153	118B	
PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - SPB 12-09	TC-09			CONTRACT NO. 60Y73				
PLOT DATE = 7/1/2013	DATE - 02-87	REVISED - MD 06-13	FED. ROAD DIST. NO. 1			ILLINOIS	FED. AID PROJECT			
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

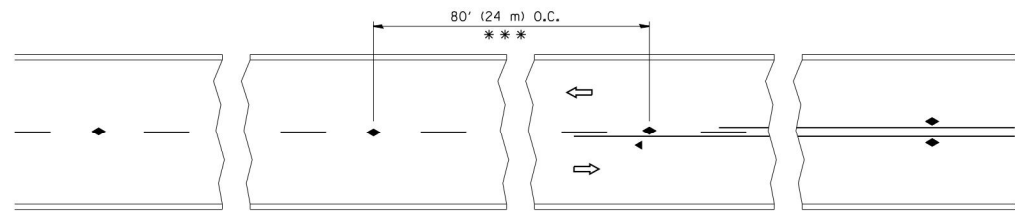
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

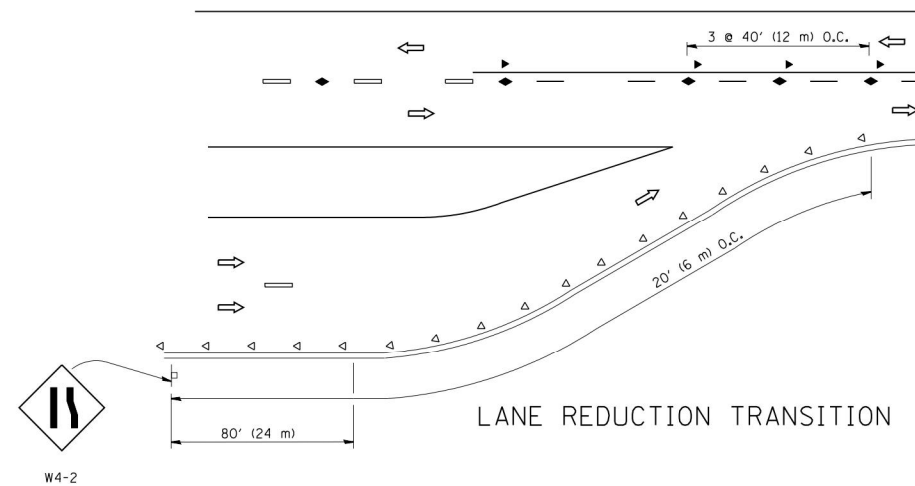
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F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	119
TC-10			CONTRACT NO. 60Y73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

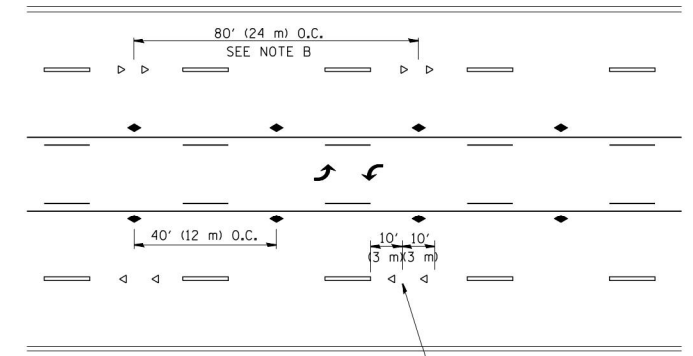


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

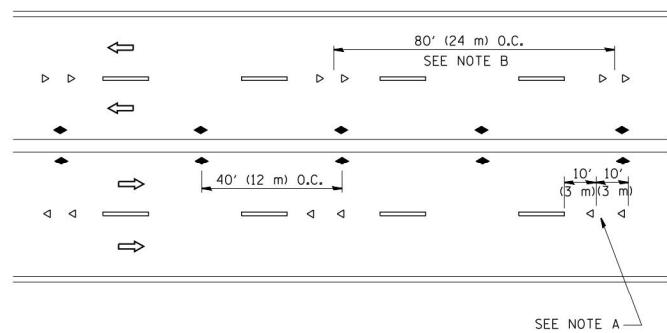
TWO-LANE/TWO-WAY



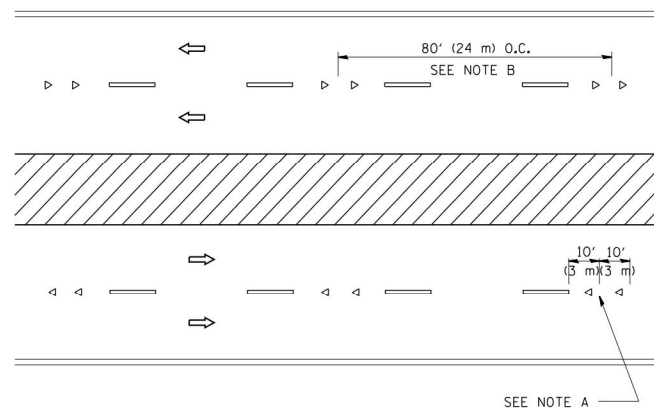
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

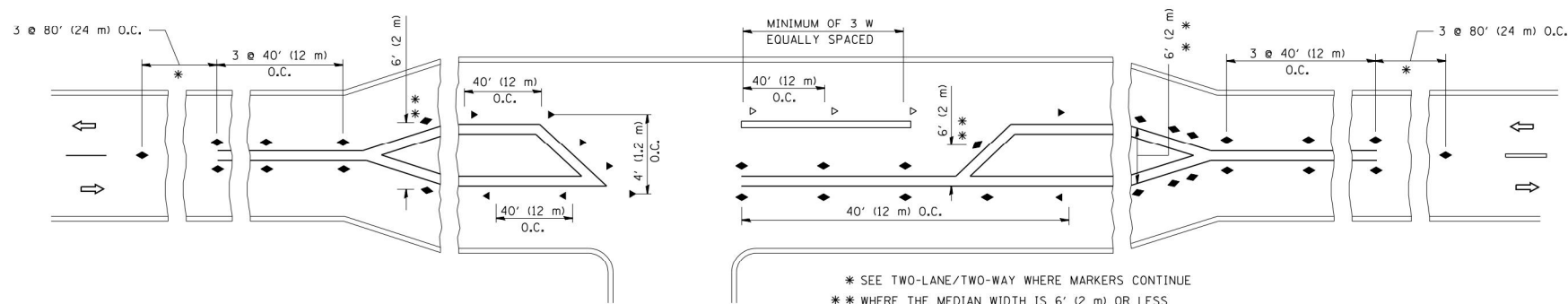
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

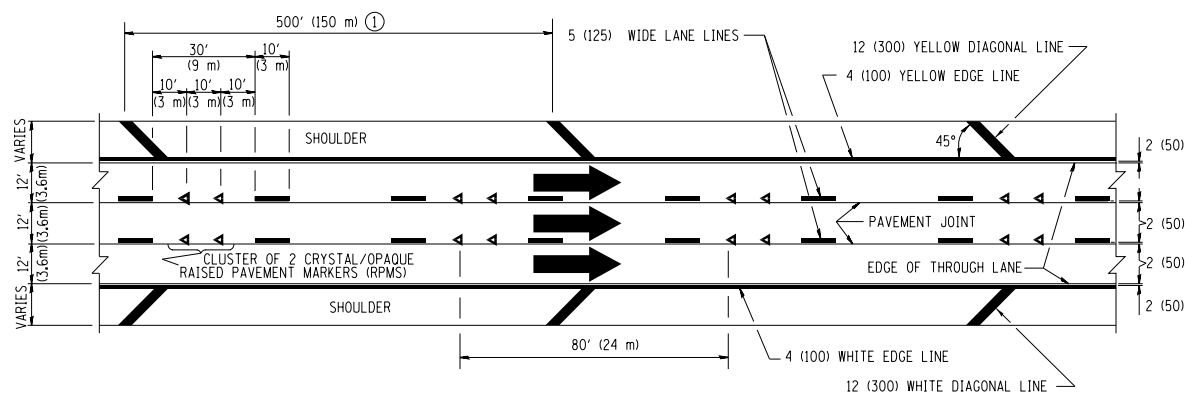
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	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

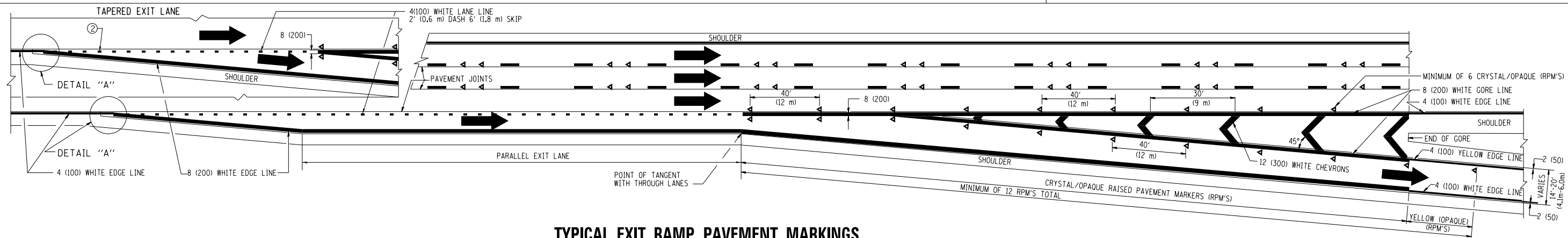
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341	(32-3-R&0305-302K)TS&N-3	COOK	153	120
TC-11		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



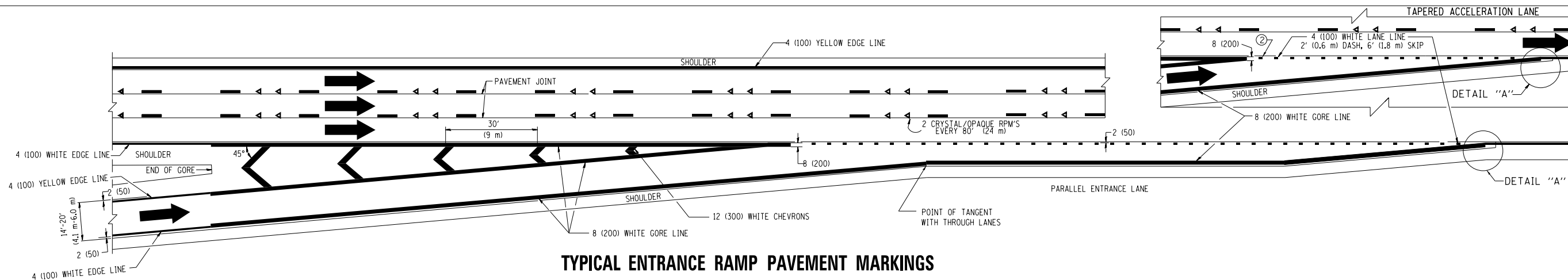
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

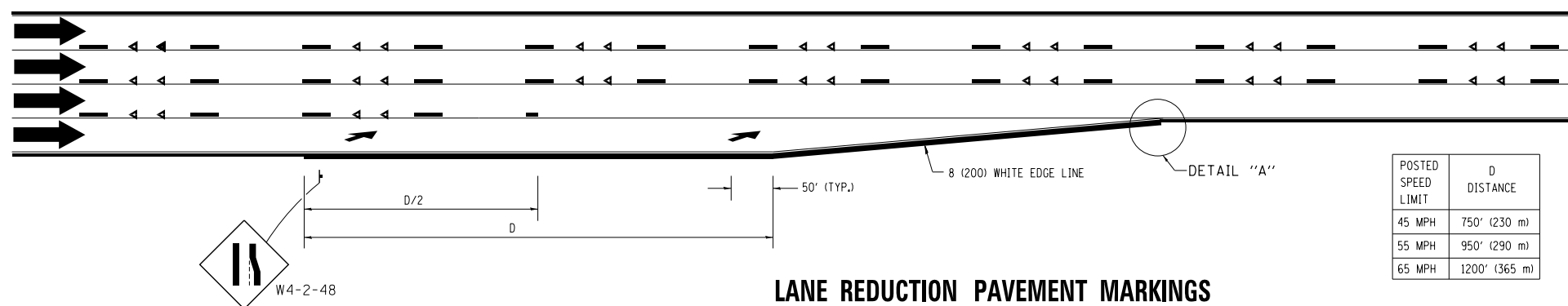
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE; INLAID OR GROOVED IN SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENT PROJECTS.
3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC PROJECTS.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

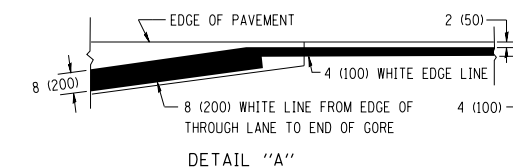


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



NOTES:

- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
- ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

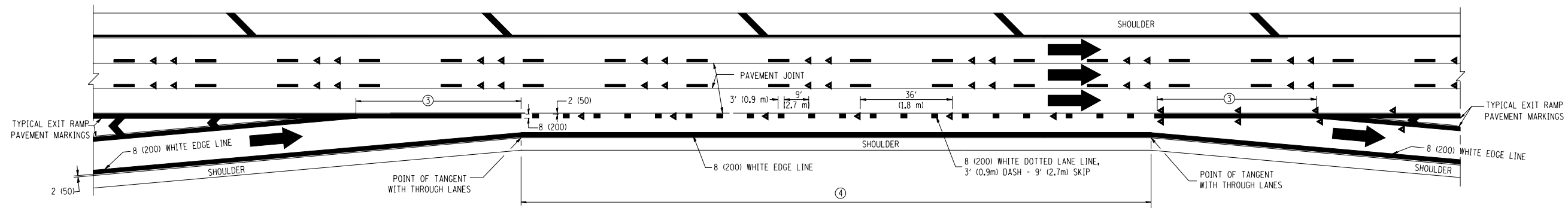
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	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - S.P.B. 01-10
	PLOT DATE = 7/8/2013	DATE - 01-90	REVISED - M.D. 05-13

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

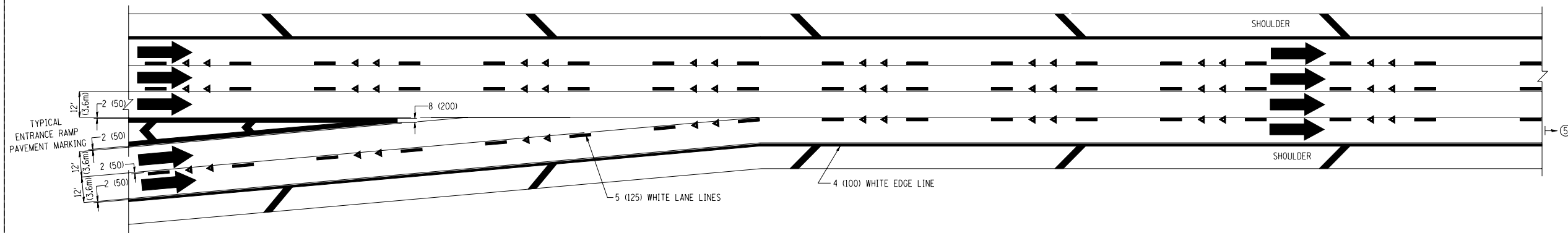
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET NO. 1 OF 2 SHEETS STA. TO STA.

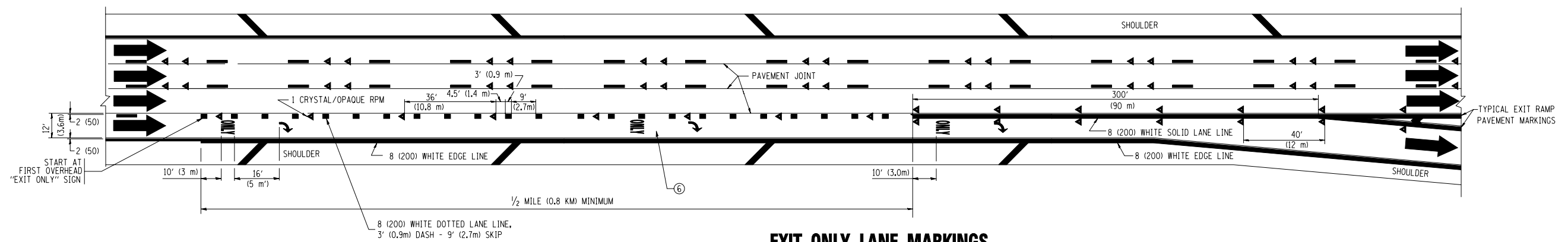
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TC-12		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



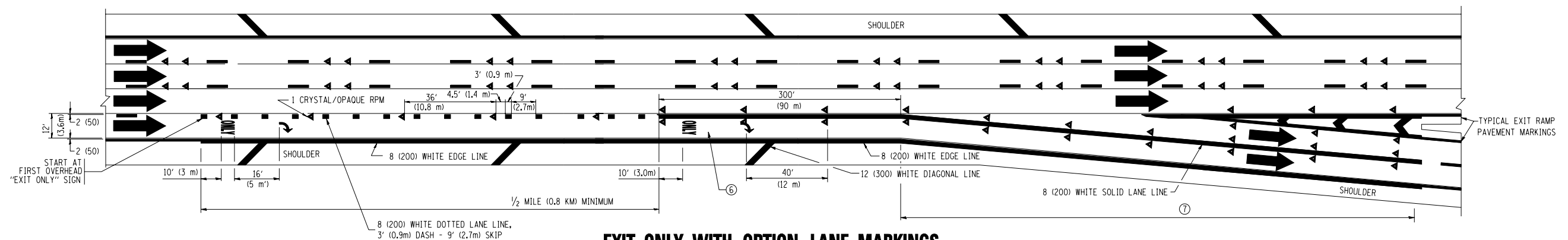
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

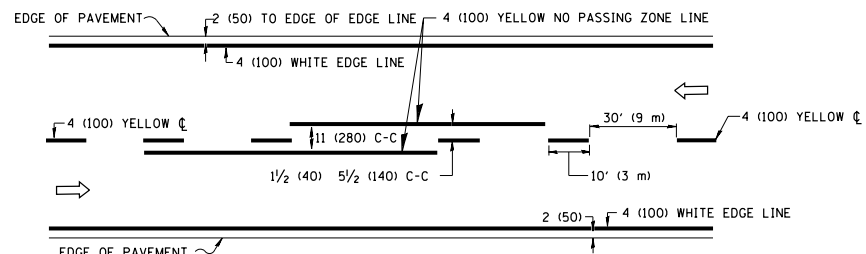
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = leuss	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96
ce:\pwork\PWID01\LEYSAN\0108315\c12.dgn		DRAWN -	REVISED - J.A.F. 02-06
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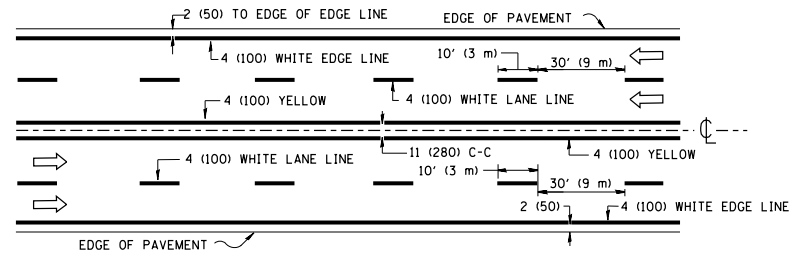
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA.	TO STA.

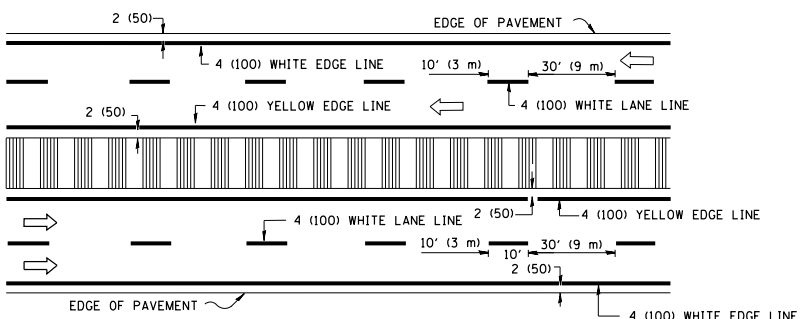
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341	(32-3-R&0305-302)TS&N-3	COOK	153	120B
TC-12		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

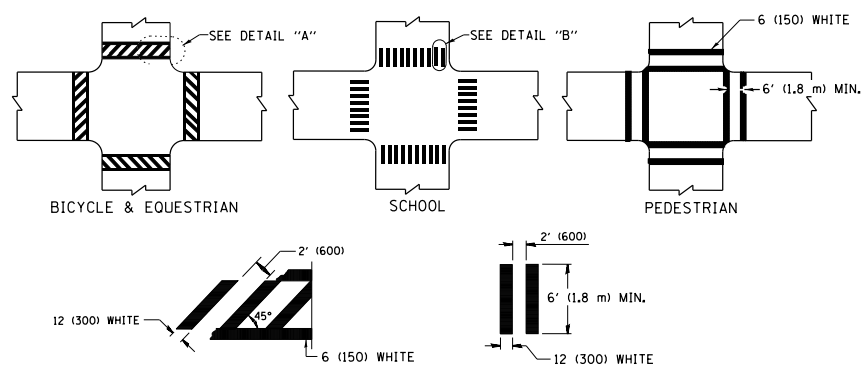


MULTI-LANE UNDIVIDED



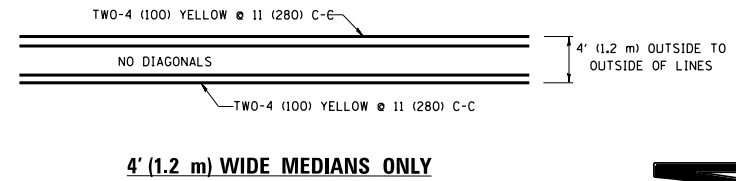
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

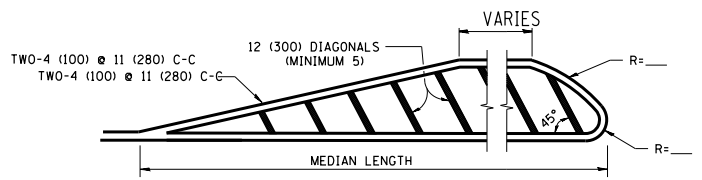


TYPICAL CROSSWALK MARKING

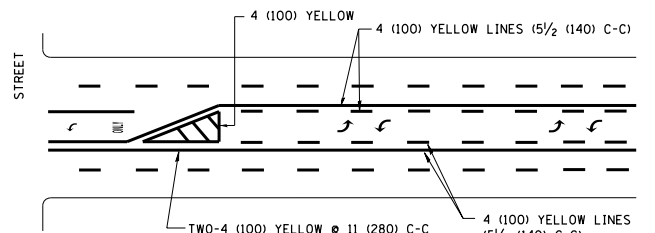
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



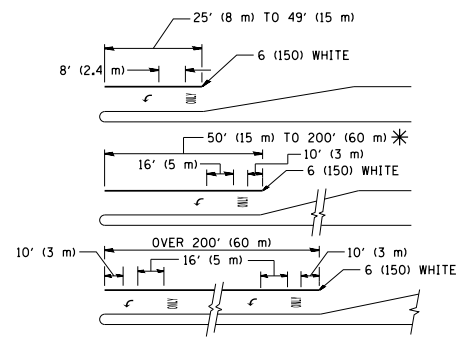
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

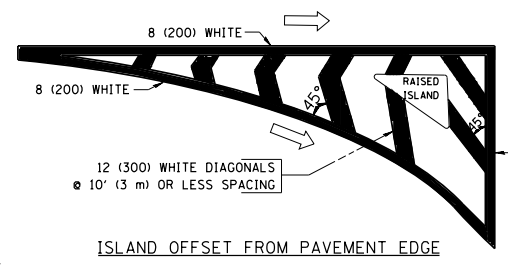


MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

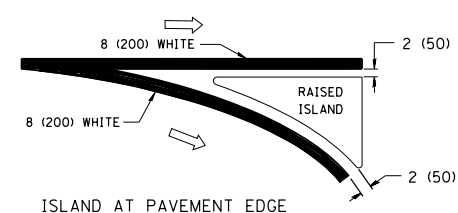


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

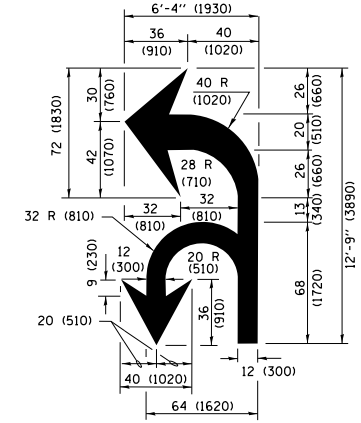
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



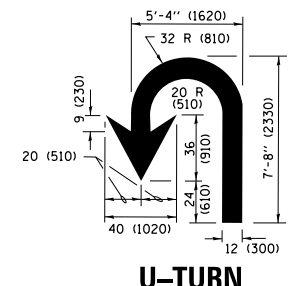
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
 * LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

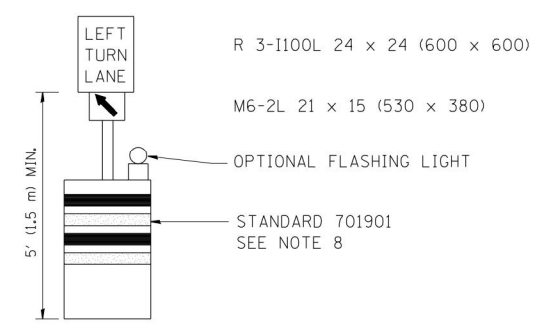
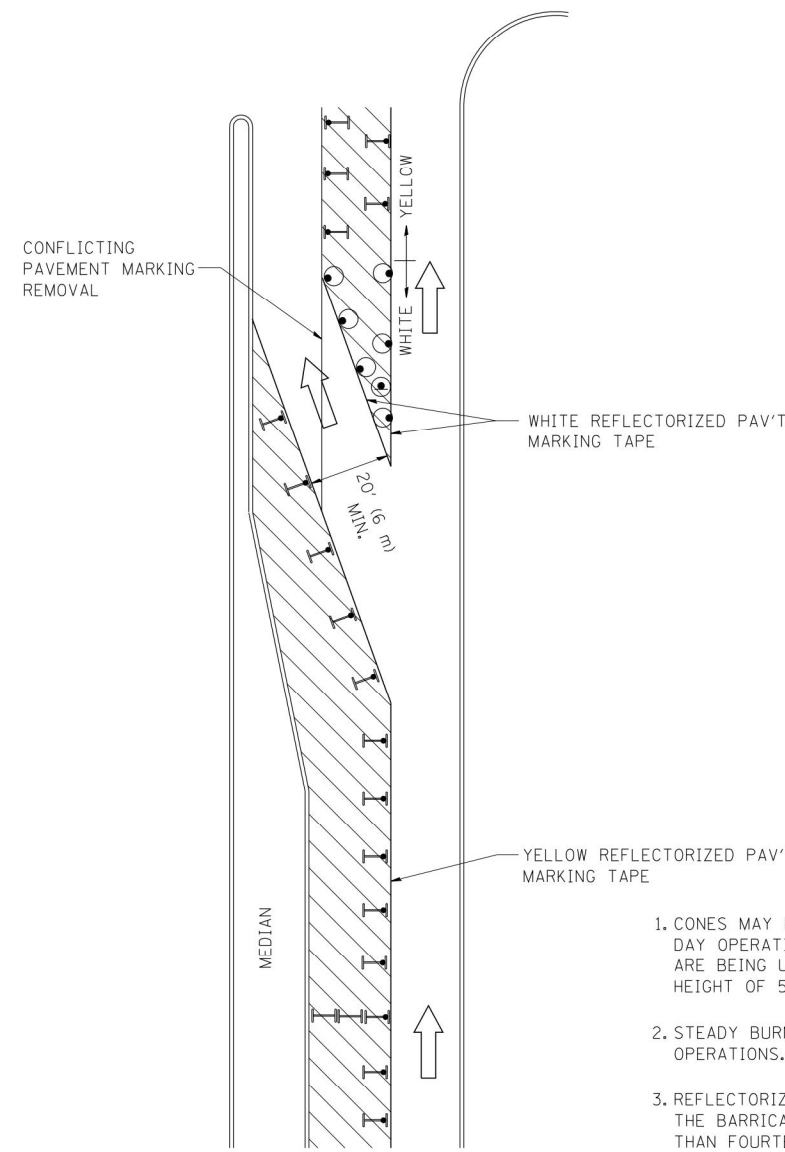
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Default	PLOT SCALE = 50.000' / in.	DATE - 03-19-90	REVISED - C. JUICIUS 07-01-13
	PLOT DATE = 12/21/2015		REVISED - C. JUICIUS 12-21-15

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	121
TC-13			CONTRACT NO. 60Y73	
ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

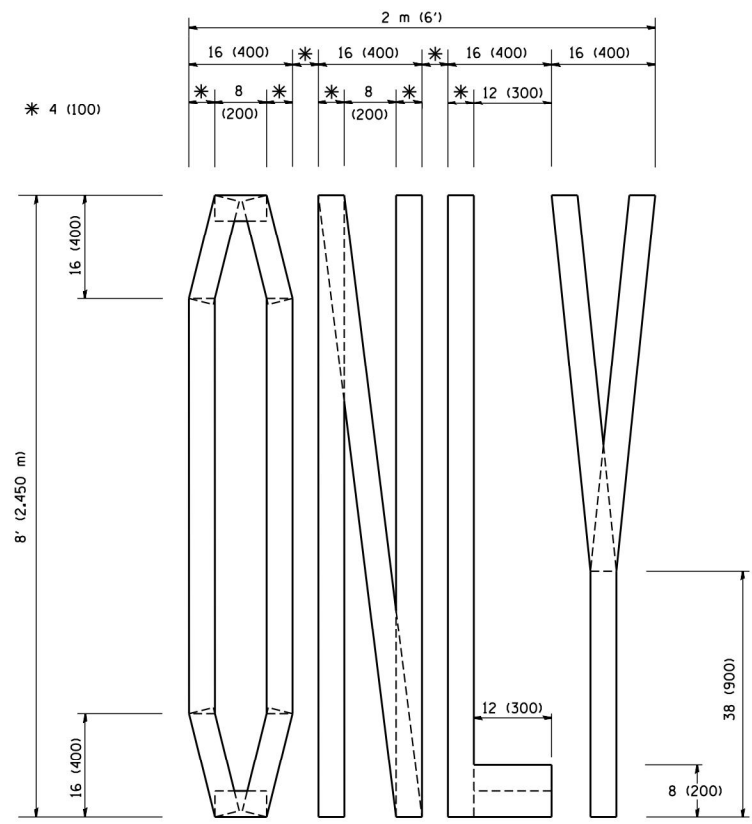
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ct:\pw\work\PWIDOT\DRIVAKOSGN\d0108315\14.dgn		REVISED - A. HOUSEH 11-07-95	REVISED -
		REVISED - A. HOUSEH 10-12-96	REVISED -
		REVISED -T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

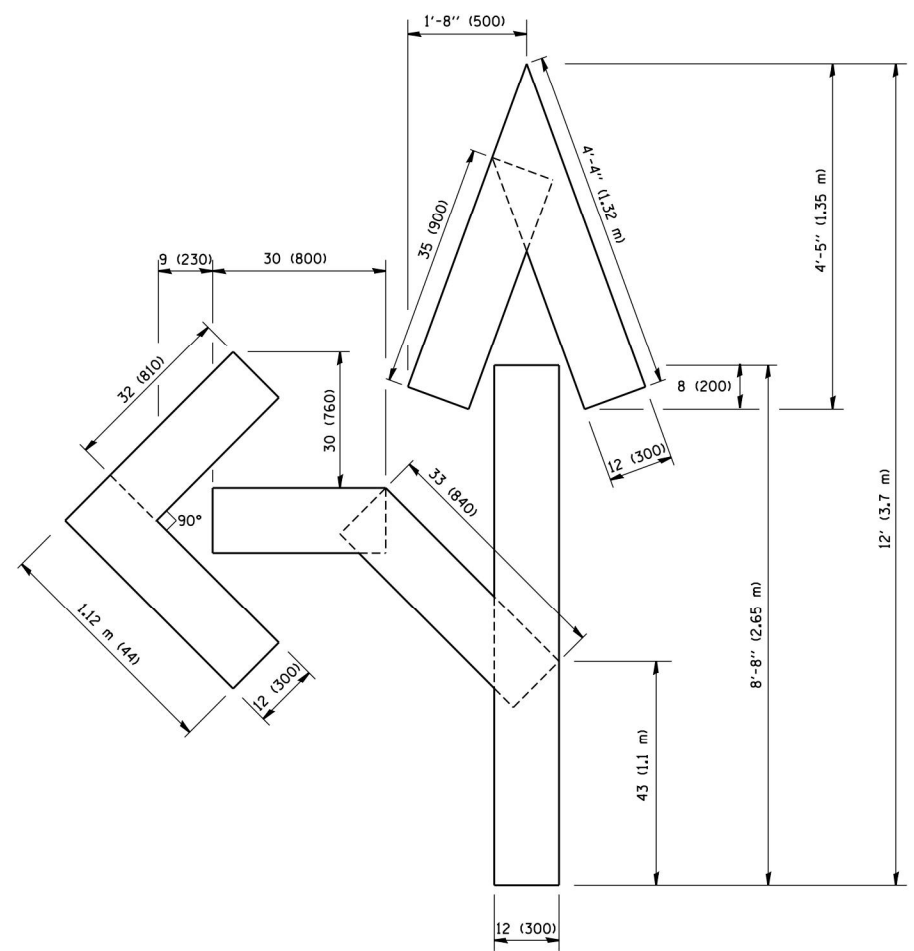
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

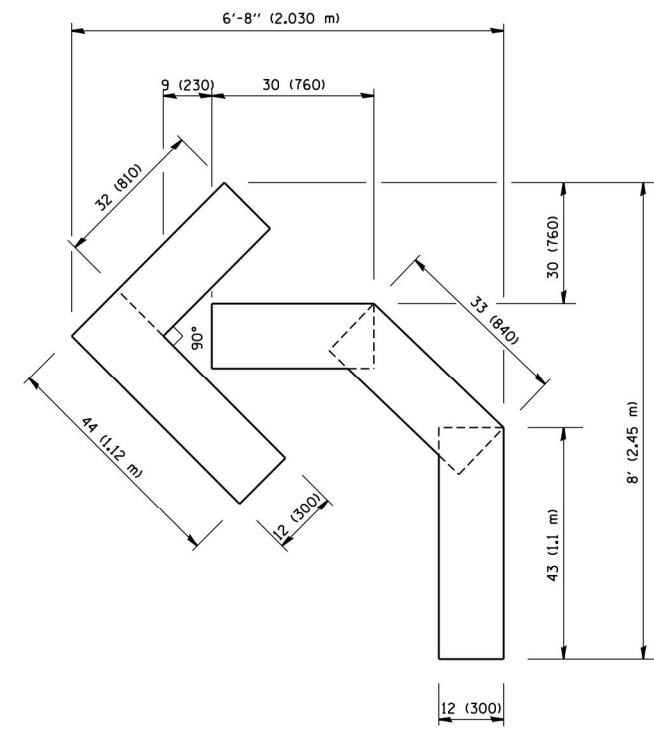
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	122
TC-14		CONTRACT NO. 60Y73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

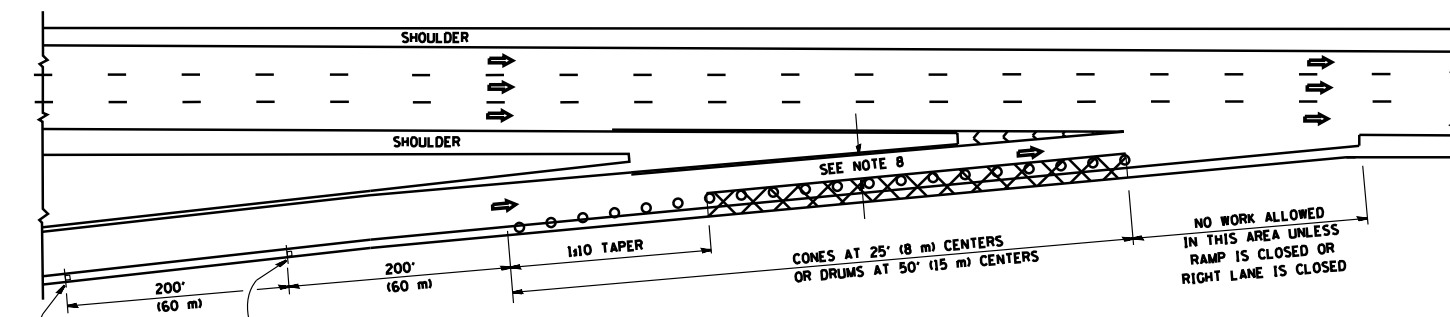
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		CHECKED -	REVISED -T. RAMMACHER 03-02-98
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

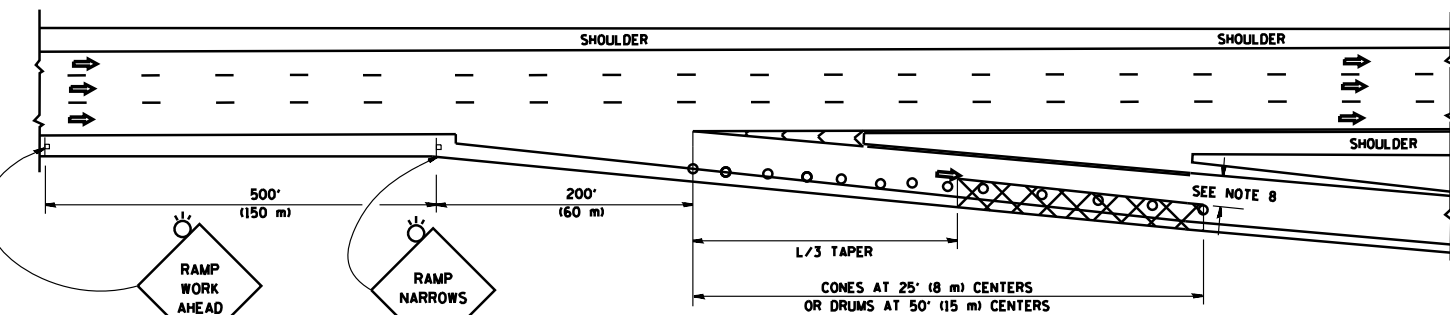
PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	123
TC-16			CONTRACT NO. 60Y73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

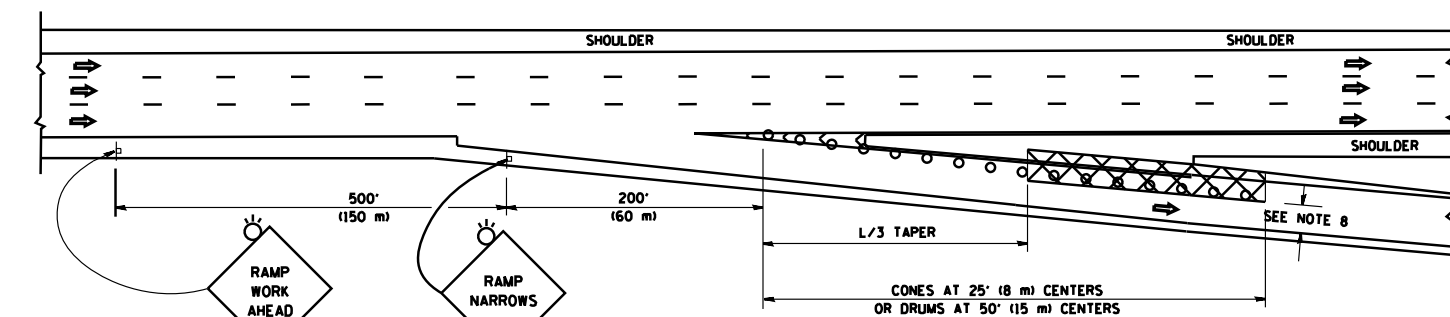
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

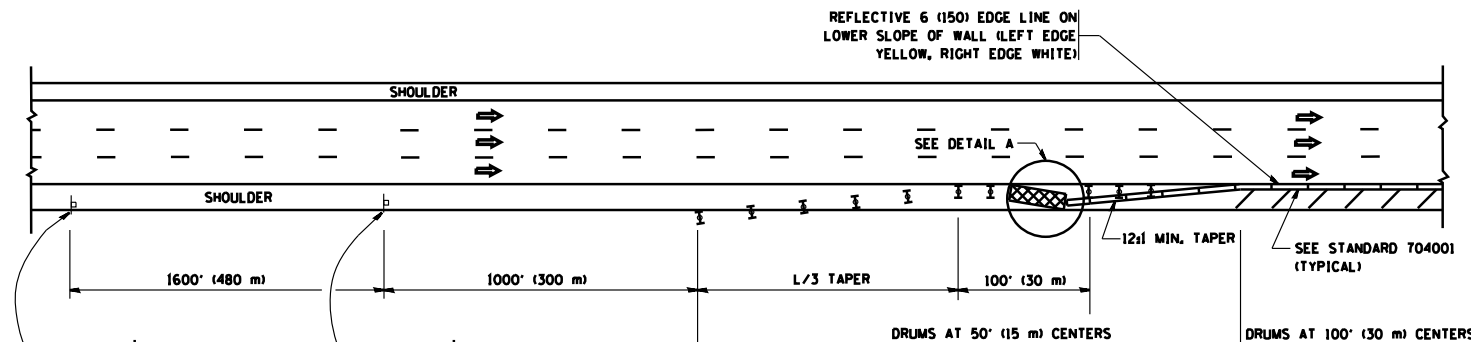
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

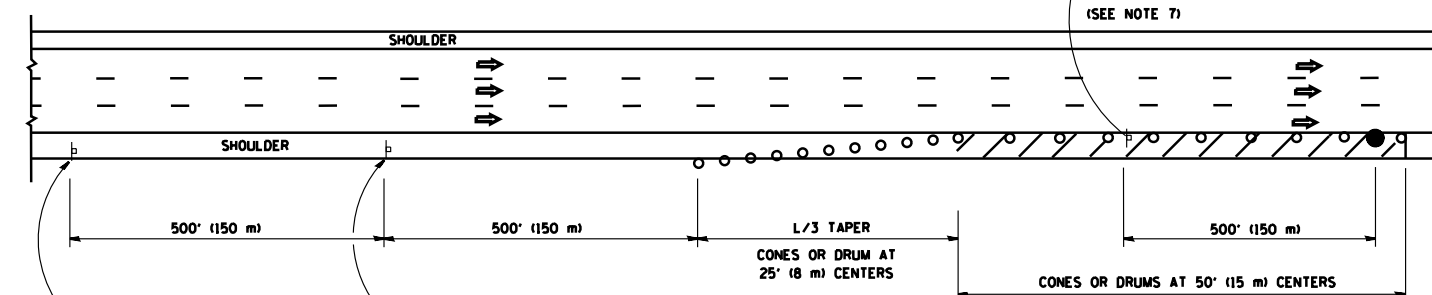
SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L = 0.65(WNS)$ $L = (WNS)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

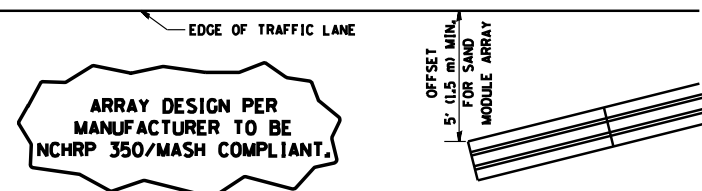


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.

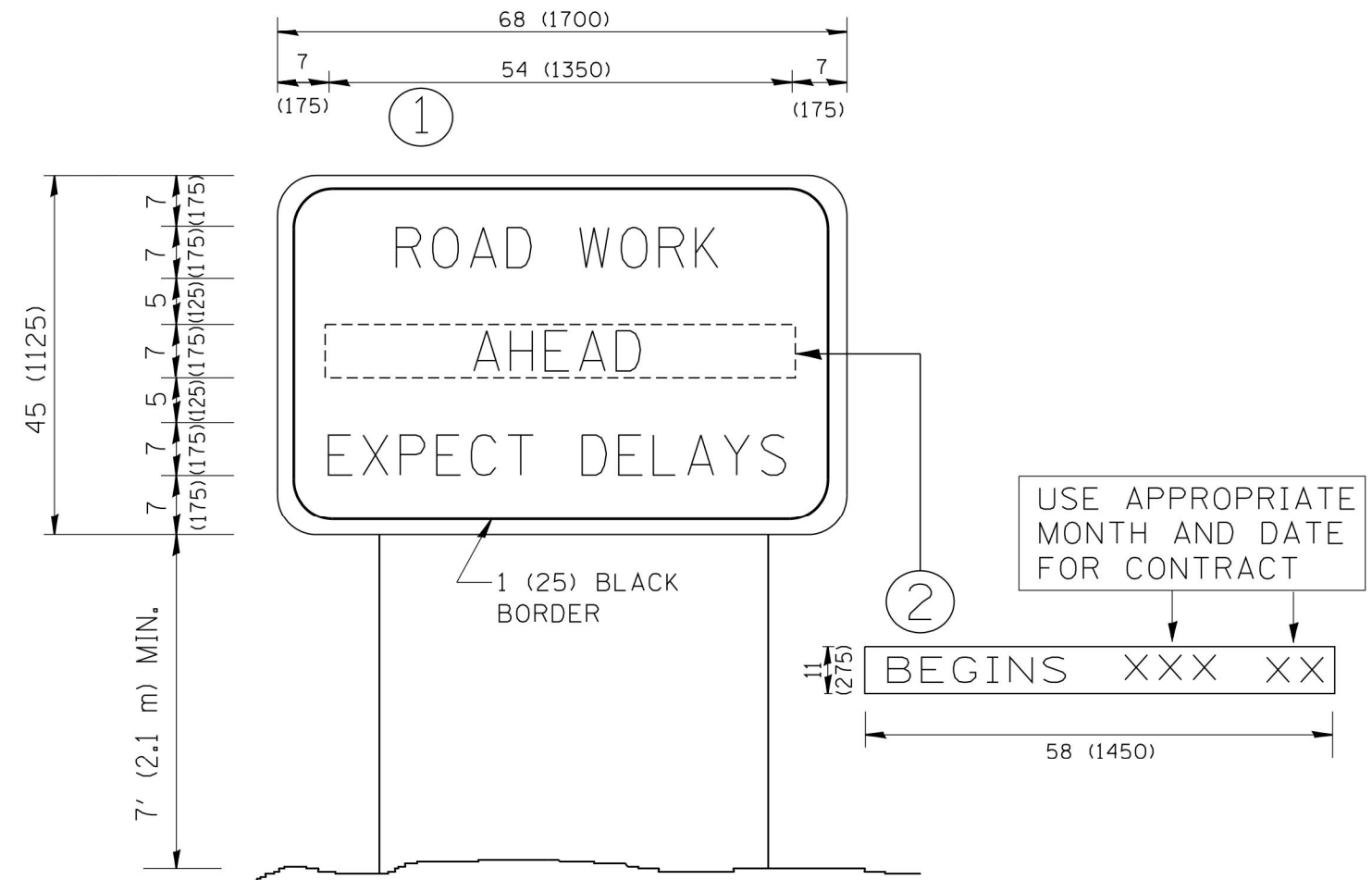


**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCRANCHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = lveysa	DESIGNED -	REVISED - J.A.F. 12-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES		F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
et:\pwork\pwork\lveysa\0108315\cl17.dgn		DRAWN - D.W.S.	REVISED - S.P.B. 01-07		314	(32-3-R&0305-302K)TS&N-3	COOK	153	124			
		CHECKED -	REVISED - S.P.B. 12-09		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		CONTRACT NO. 60Y73	
		DATE - 11-96	REVISED - M.D. 06-13		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

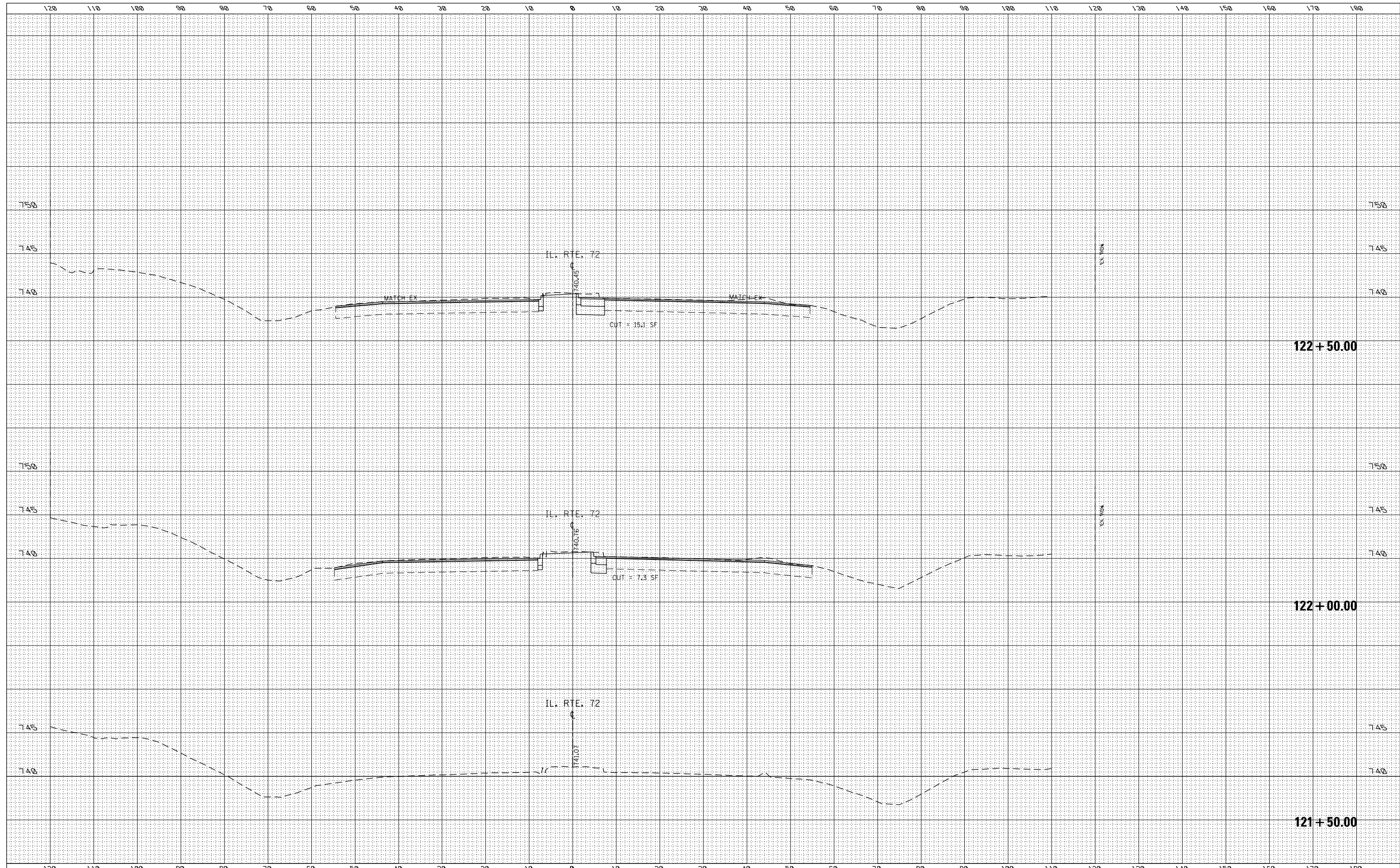
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&0305-302K)TS&N-3	COOK	153	125
TC-22			CONTRACT NO. 60Y73	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				

FINAL SURVEY NO.	SURVEYED PLOTTED	DATE
NOTE BOOK	TEMPLATE	
AREAS CHECKED	AREAS CHECKED	

ORIGINAL SURVEY NO.	SURVEYED PLOTTED	DATE
NOTE BOOK	TEMPLATE	
AREAS CHECKED	AREAS CHECKED	



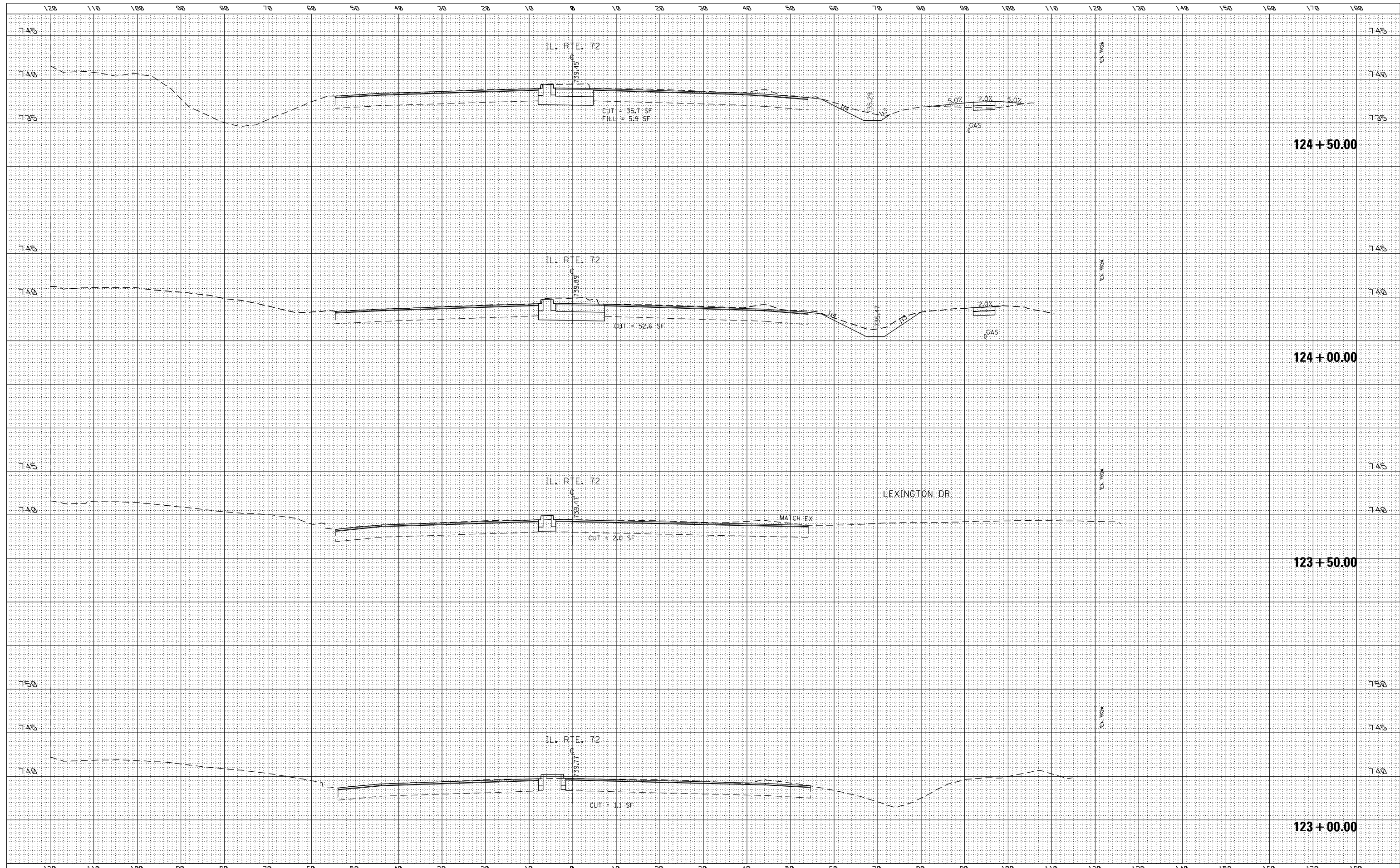
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	PLOT DATE = 12/21/2015	DATE -	REVISED -	STA. 121+50.00 TO STA. 122+50.00		ILLINOIS FED. AID PROJECT		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE



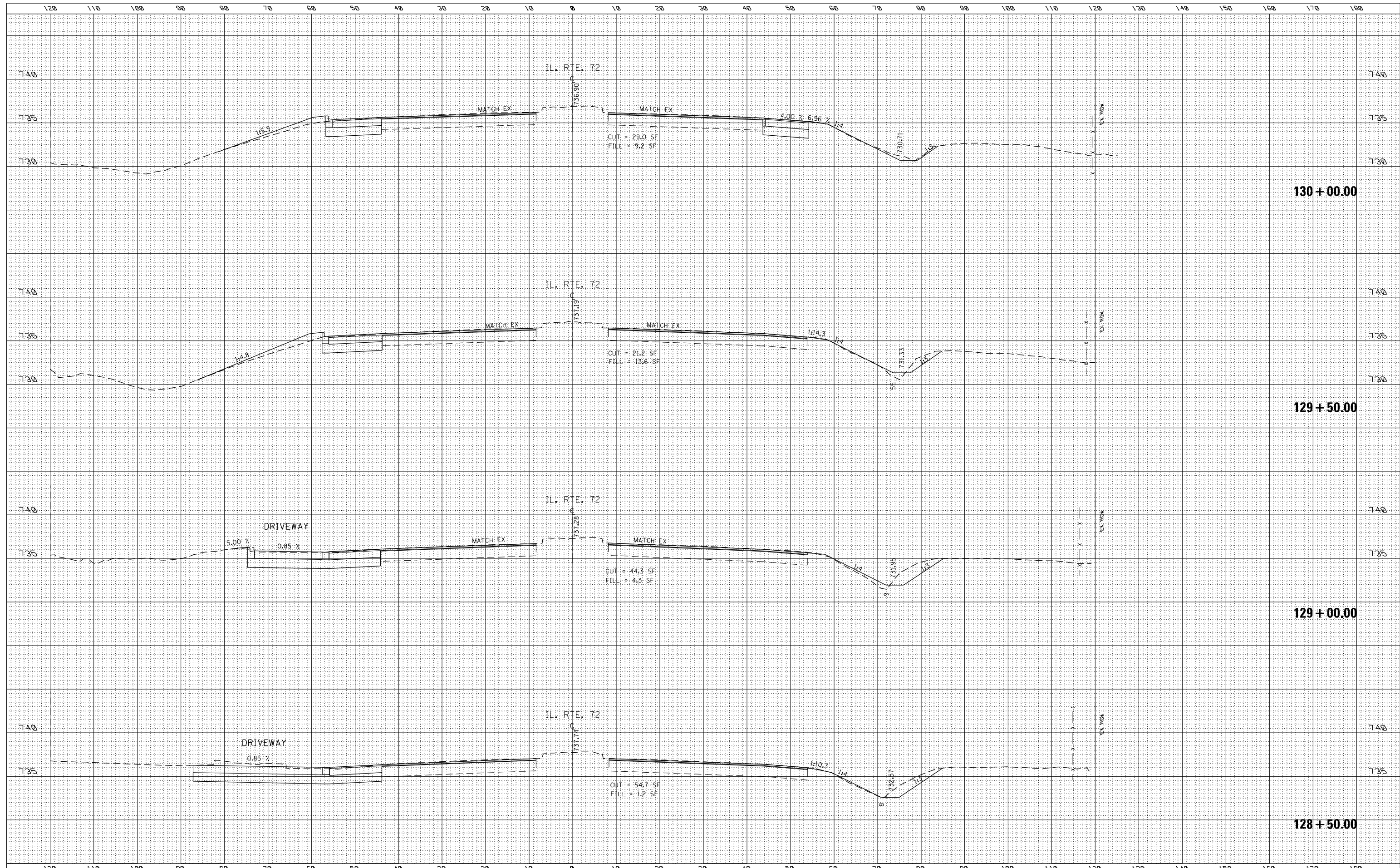
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	PLOT DATE = 12/21/2015	DATE -	REVISED -	SCALE:	SHEET OF SHEETS	STA. 123+00.00 TO STA. 124+50.00			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

DATE	BY
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED

DATE	BY
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS
	CHECKED



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		DATE -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

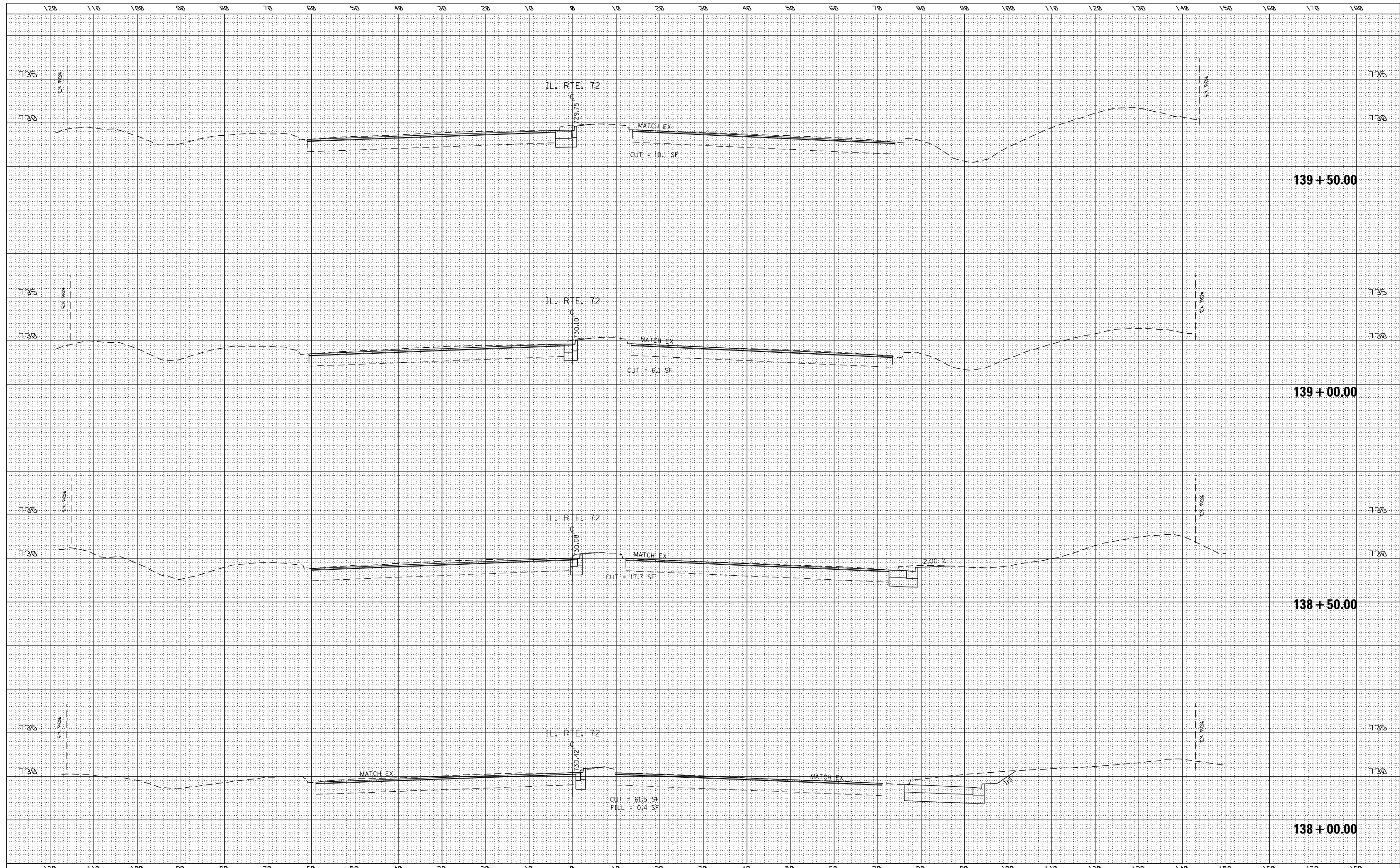
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SCALE: SHEET OF SHEETS STA. 128+50.00 TO STA. 130+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302KITS&N-3	COOK	153	130
CONTRACT NO. 60Y73				
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	
	AREAS CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS CHECKED	
	AREAS CHECKED	



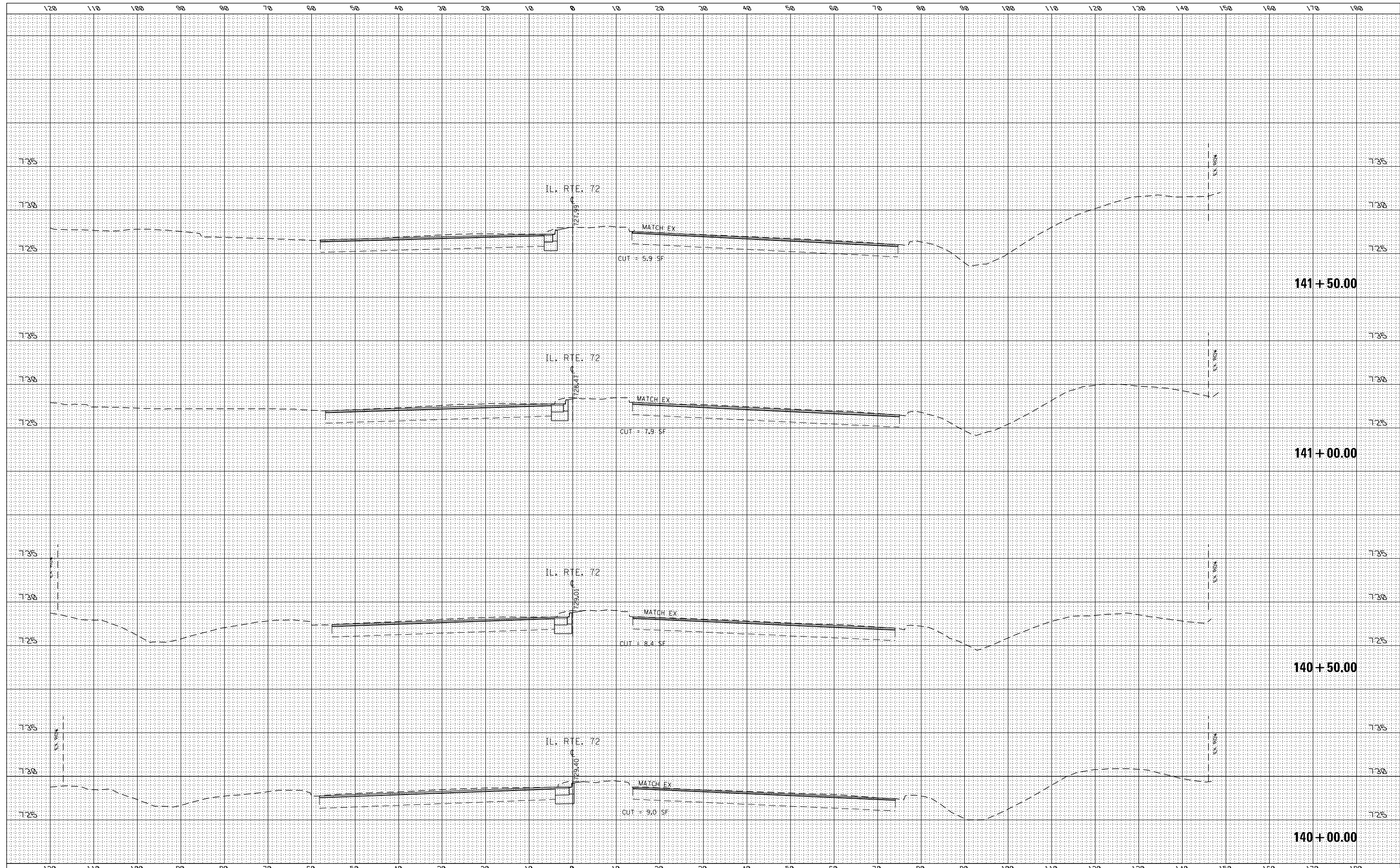
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	PLOT DATE = 12/21/2015	DATE -	REVISED -	SCALE:	SHEET OF SHEETS	STA. 138+00.00 TO STA. 139+50.00			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

DATE	
BY	
FINAL SURVEY NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

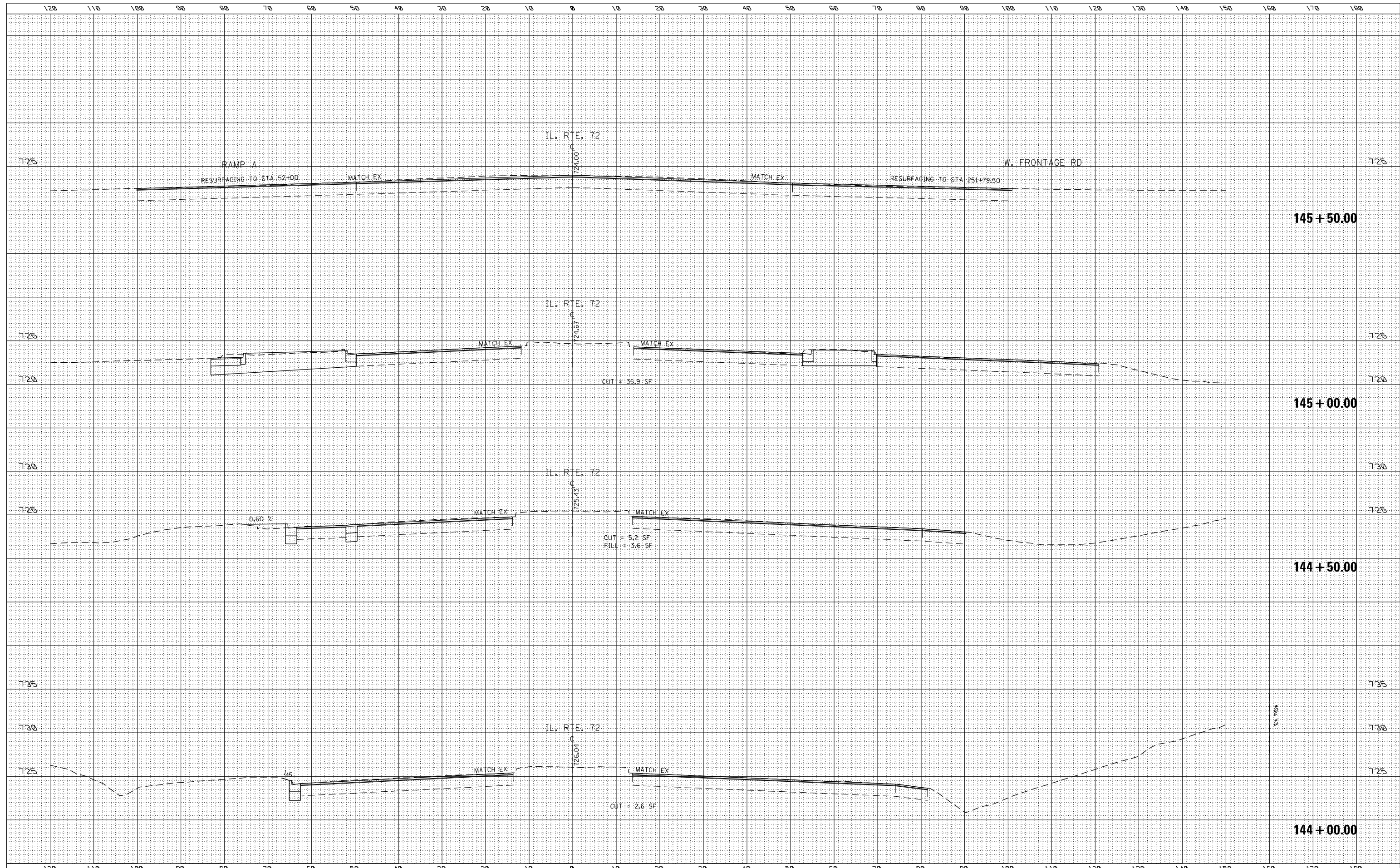
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SCALE: SHEET OF SHEETS STA. 140+00.00 TO STA. 141+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	136
CONTRACT NO. 60Y73				
ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED



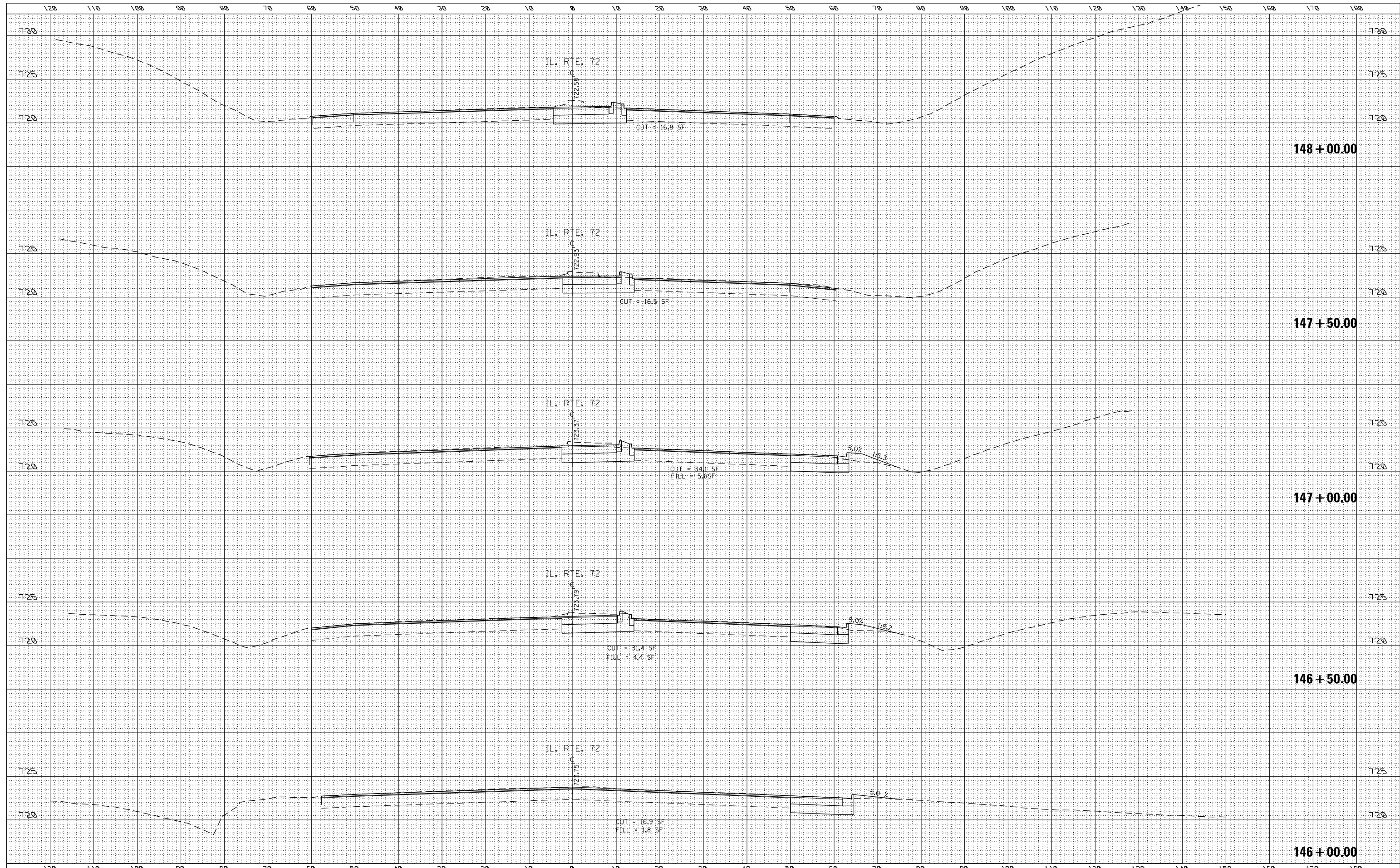
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\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED - TVN	REVISED -	CONTRACT NO. 60Y73		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 12/21/2015	DATE -	REVISED -	SCALE:	SHEET OF SHEETS	STA. 144+00.00 TO STA. 145+50.00			

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
	AREAS	
	CHECKED	



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

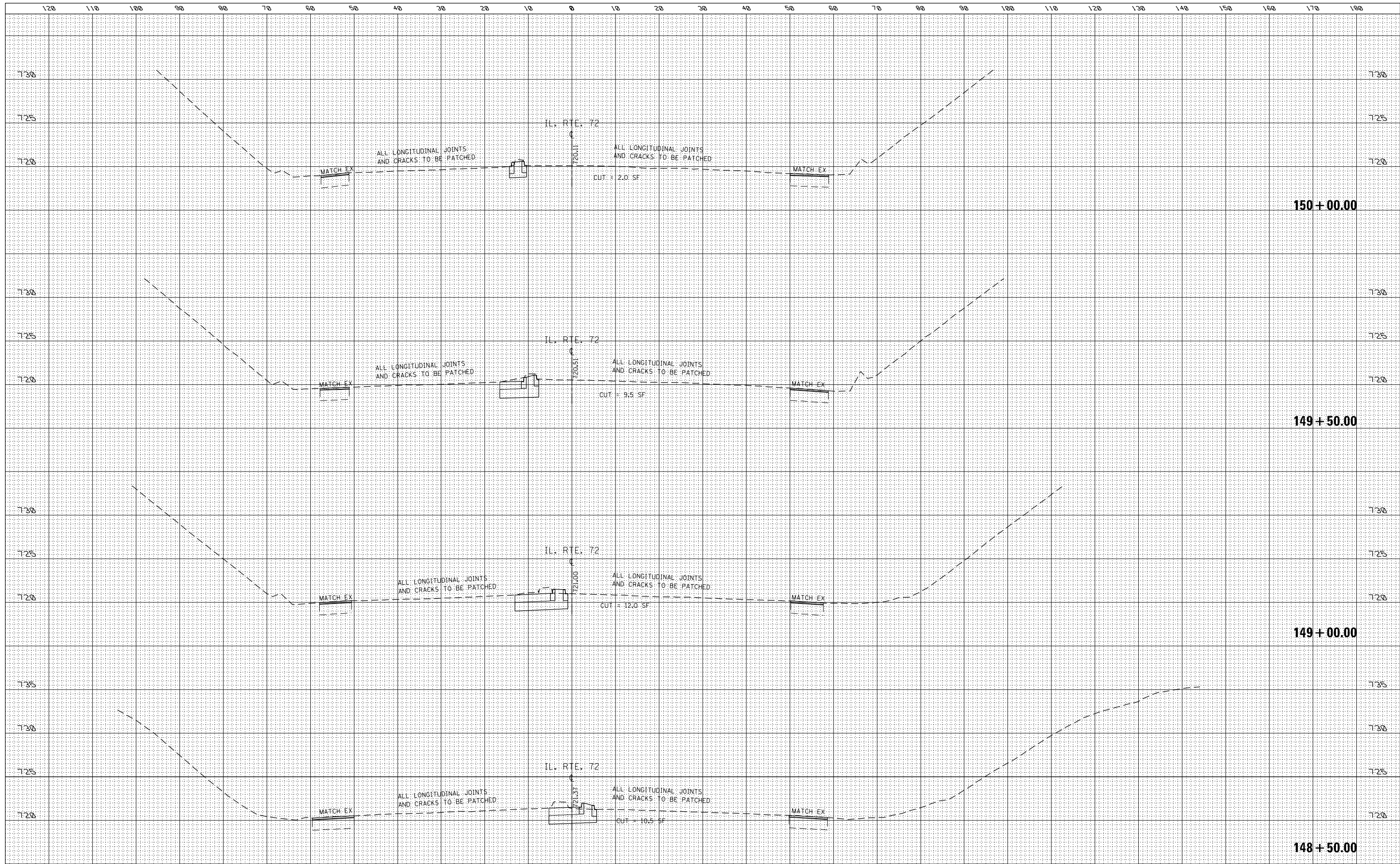
SCALE: SHEET OF SHEETS STA. 146+00.00 TO STA. 148+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302K)TS&N-3	COOK	153	139
CONTRACT NO. 60Y73				

ILLINOIS FED. AID PROJECT

FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED



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 PLOT DATE = 12/21/2015

DESIGNED - TVN
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

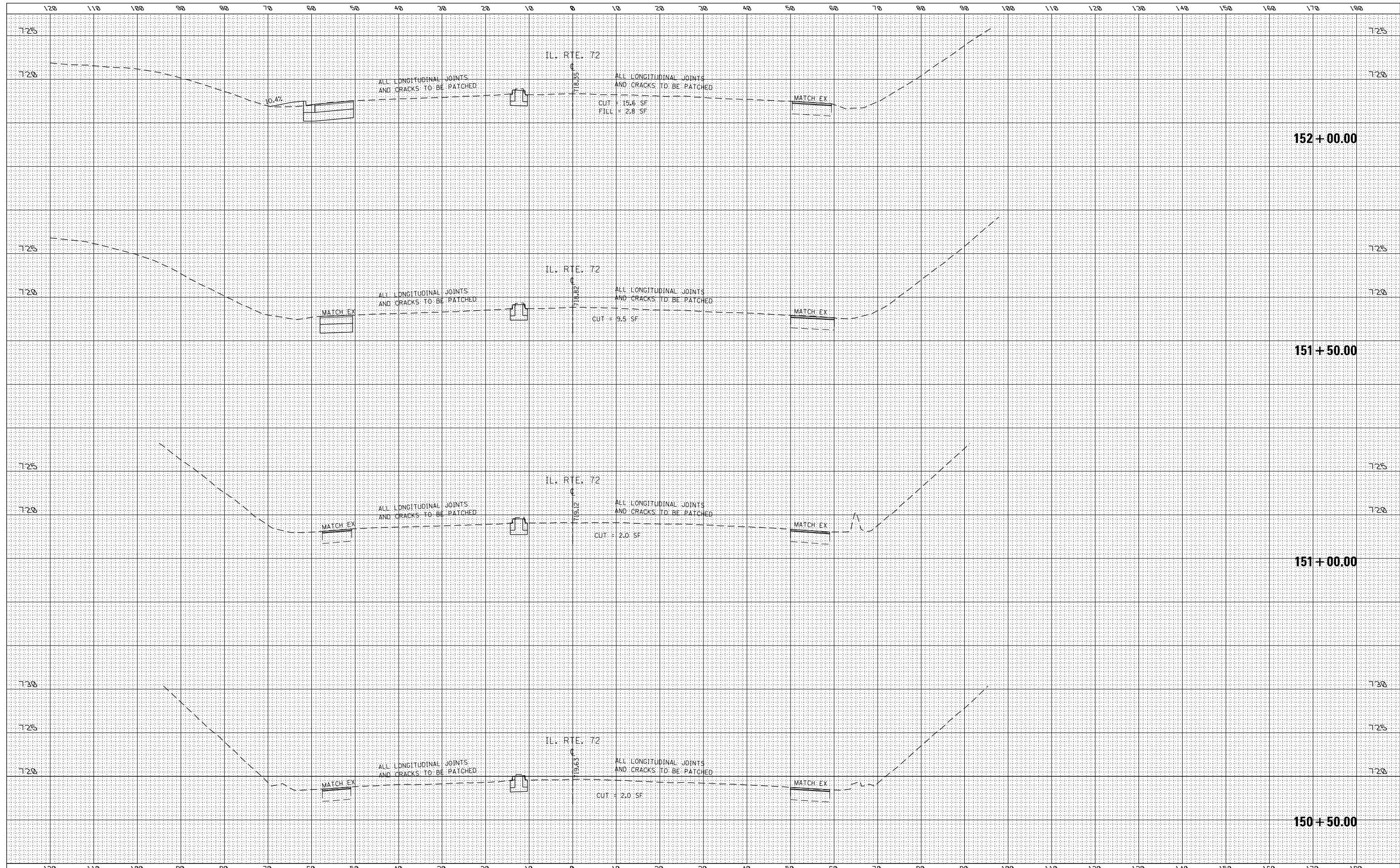
CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

SCALE: SHEET OF SHEETS STA. 148+50.00 TO STA. 150+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	140
			CONTRACT NO. 60Y73	
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

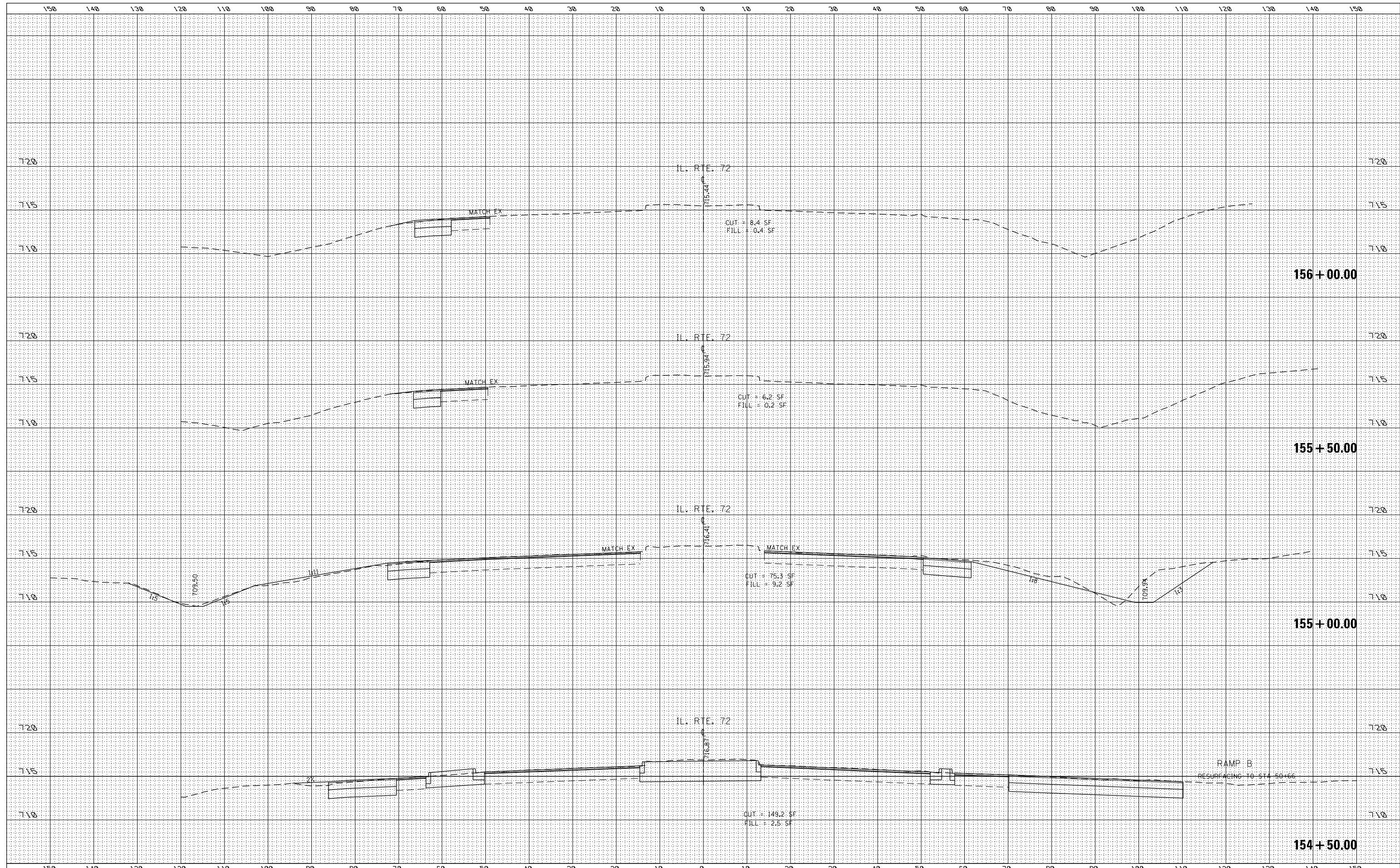
ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FILE NAME =	USER NAME = oattawia	DESIGNED - TVN	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTION - IL RTE 72 (HIGGINS ROAD)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P:\2014\ME14027_Var\Var_Phi\W06_IL72_IL290\CADD\Shets\DI60Y73-SHT-PXS-IL72-HIGGINS.dgn	DRAWN - JP	REVISED -	341			(32-3-R&O305-302K)TS&N-3	COOK	153	141	
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED - TVN	REVISED -			CONTRACT NO. 60Y73				
	PLOT DATE = 12/21/2015	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

DATE	
BY	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED

DATE	
BY	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
NO.	TEMPLATE
	AREAS CHECKED
	AREAS CHECKED



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P:\2014\ME14027_Var\Var_Phil\W06_IL72_IL290\CA00\Shets\DI60Y73-SHT-PXS-IL72-HIGGINS.dgn		DRAWN - JP	REVISED -	341	(32-3-R&O305-302)KITS&N-3	COOK	153	143
\$MODELNAME\$		CHECKED - TVN	REVISED -	CONTRACT NO. 60Y73				
		DATE -	REVISED -	ILLINOIS FED. AID PROJECT				

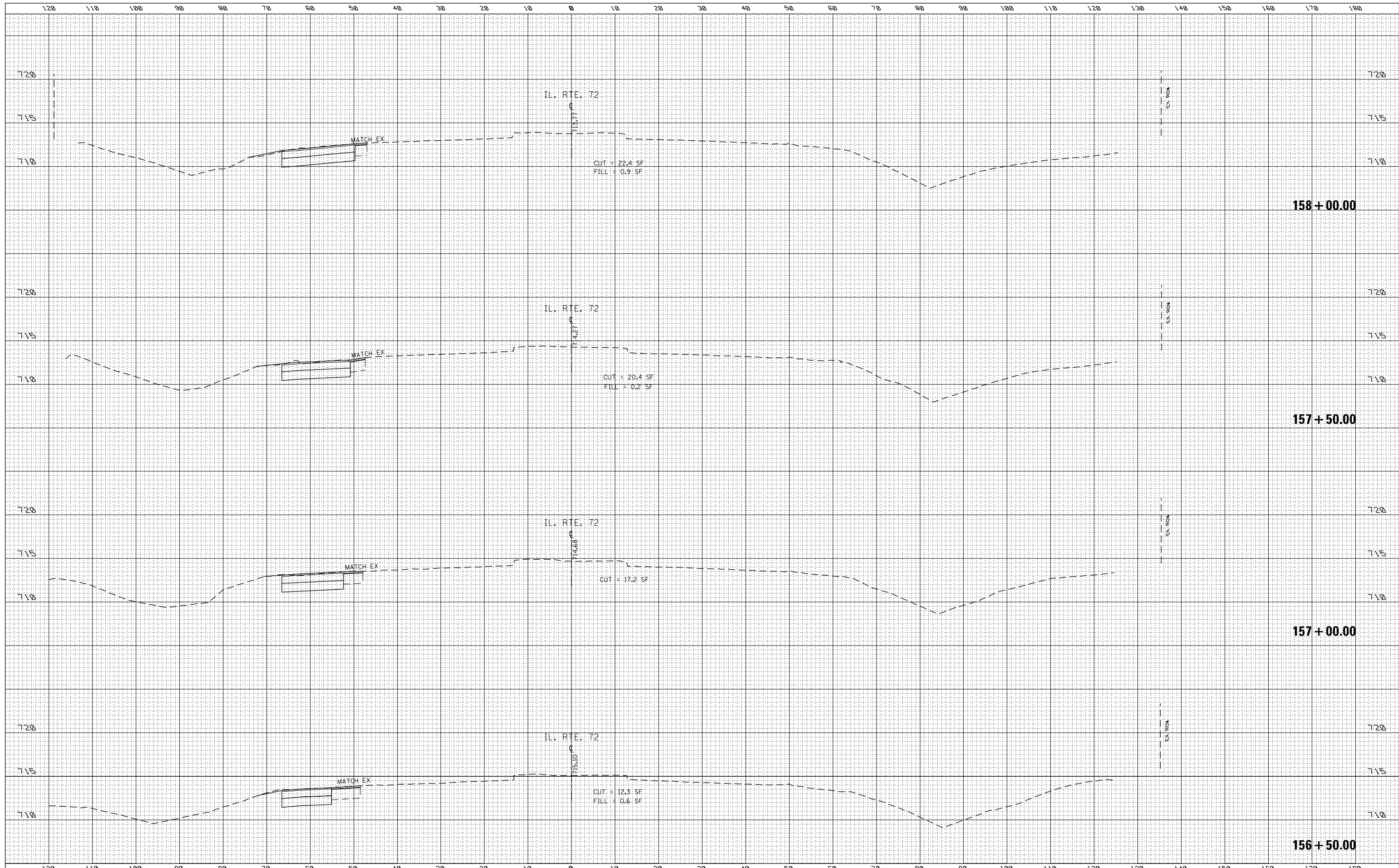
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

SCALE: SHEET OF SHEETS STA. 154+50.00 TO STA. 156+00.00

FINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY NO.	SURVEYED	DATE
NOTE BOOK NO.	PLOTTED	BY
AREAS CHECKED	TEMPLATE	
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PLOT DATE = 12/21/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

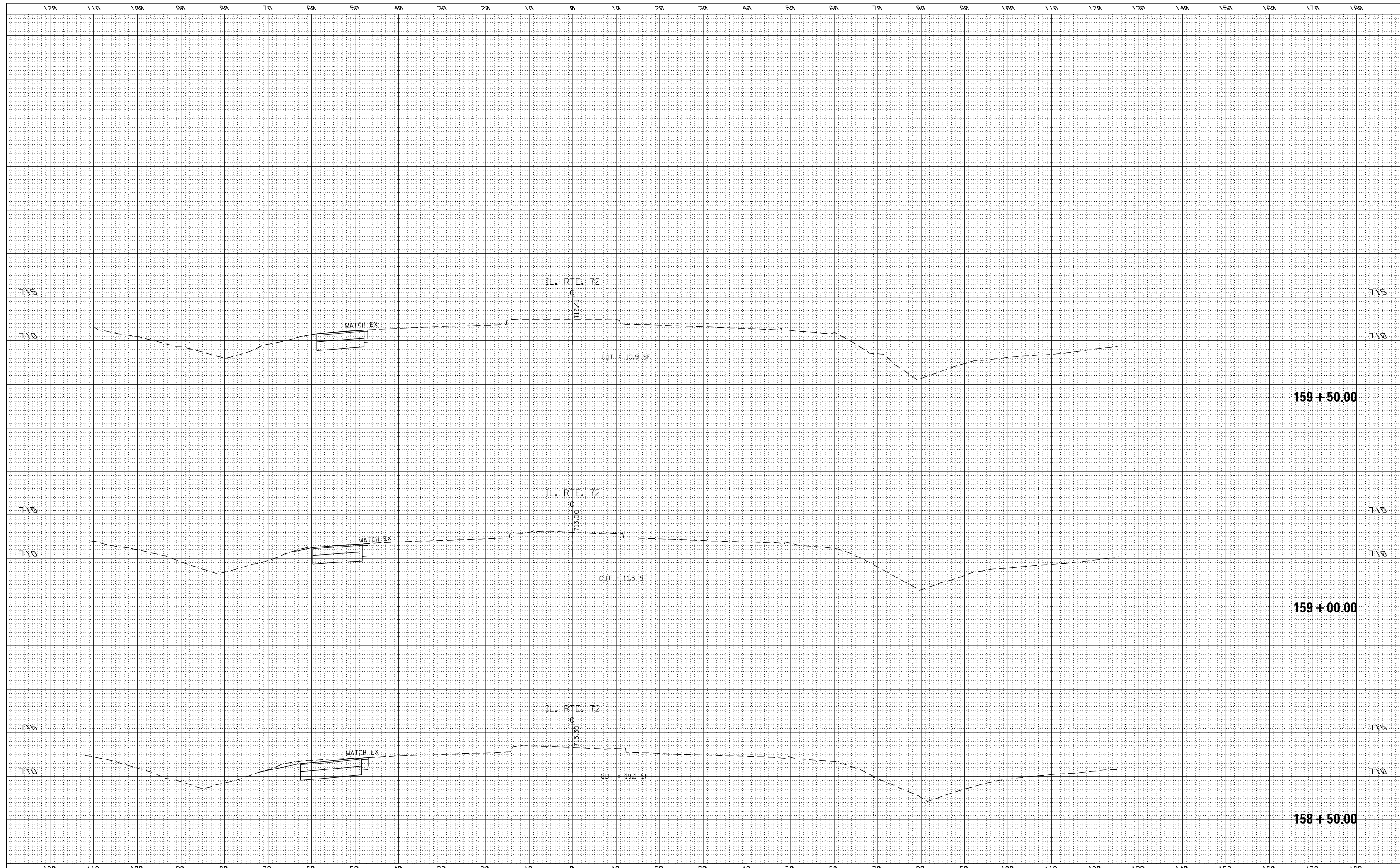
CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

SCALE: SHEET OF SHEETS STA. 156+50.00 TO STA. 158+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302K)TS&N-3	COOK	153	144
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
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	CHECKED		



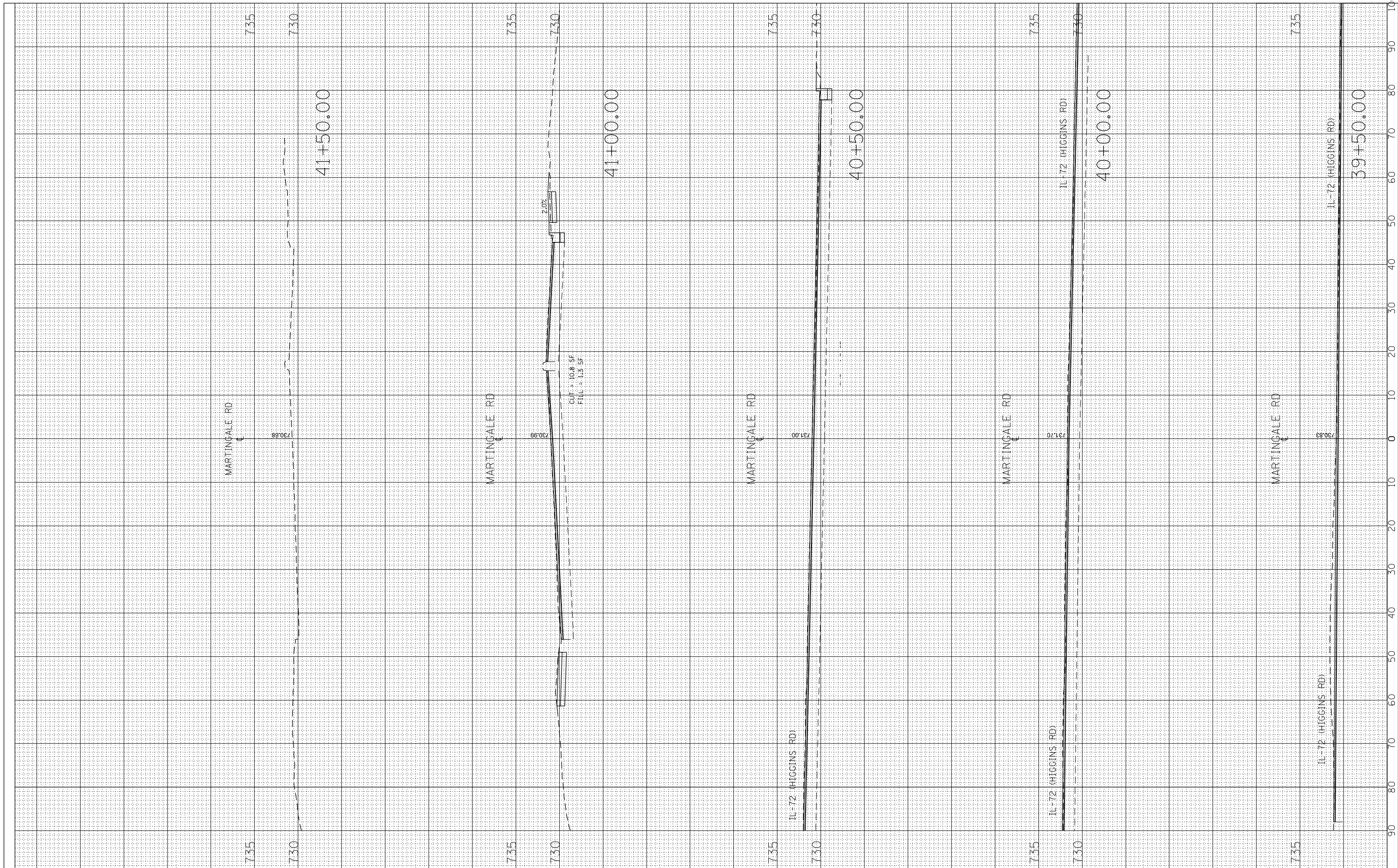
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P:\2014\ME14027_Var\Var_Phil\W06_IL72_IL290\CADD	Shits\DI60Y73-SHT-PXS-IL72-HIGGINS.dgn	DRAWN - JP	REVISED -	341	(32-3-R&O305-302K)TS&N-3	COOK	153	145
\$MODELNAME\$	PLOT SCALE = 20.0000' / in.	CHECKED - TVN	REVISED -	SCALE:		SHEET OF SHEETS		CONTRACT NO. 60Y73
	PLOT DATE = 12/21/2015	DATE -	REVISED -	STA. 158+50.00 TO STA. 159+50.00		ILLINOIS FED. AID PROJECT		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - IL RTE 72 (HIGGINS ROAD)

FINAL SURVEY NO.	SURVEYED PLOTTED	BY	DATE
	NOTE BOOK AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED	BY	DATE
	NOTE BOOK AREAS CHECKED		



FILE NAME = P:\2014\ME14027_Var-Var_Phil\W06_IL72_I290\CA00\Shets\DI60Y73-SHT-PXS-MARTINGALE.dgn

USER NAME = aattawia
 DRAWN - JP
 CHECKED - TVN
 DATE - 12/21/2015

DESIGNED - TVN
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

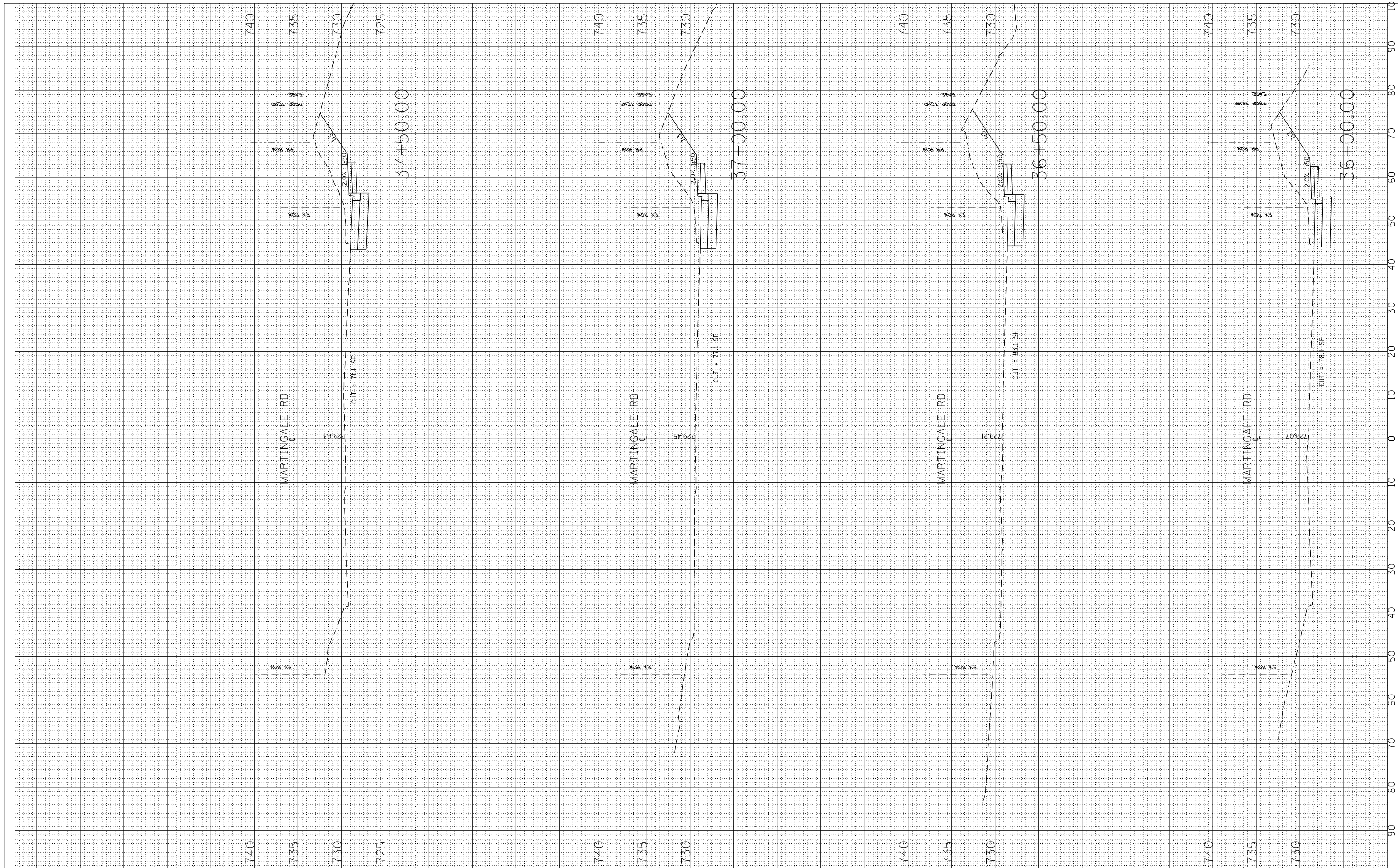
CROSS SECTION - MARTINGALE ROAD

SCALE: SHEET OF SHEETS STA. 39+50.00 TO STA. 41+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	146
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 12/21/2015

DESIGNED - TVN
 DRAWN - JP
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

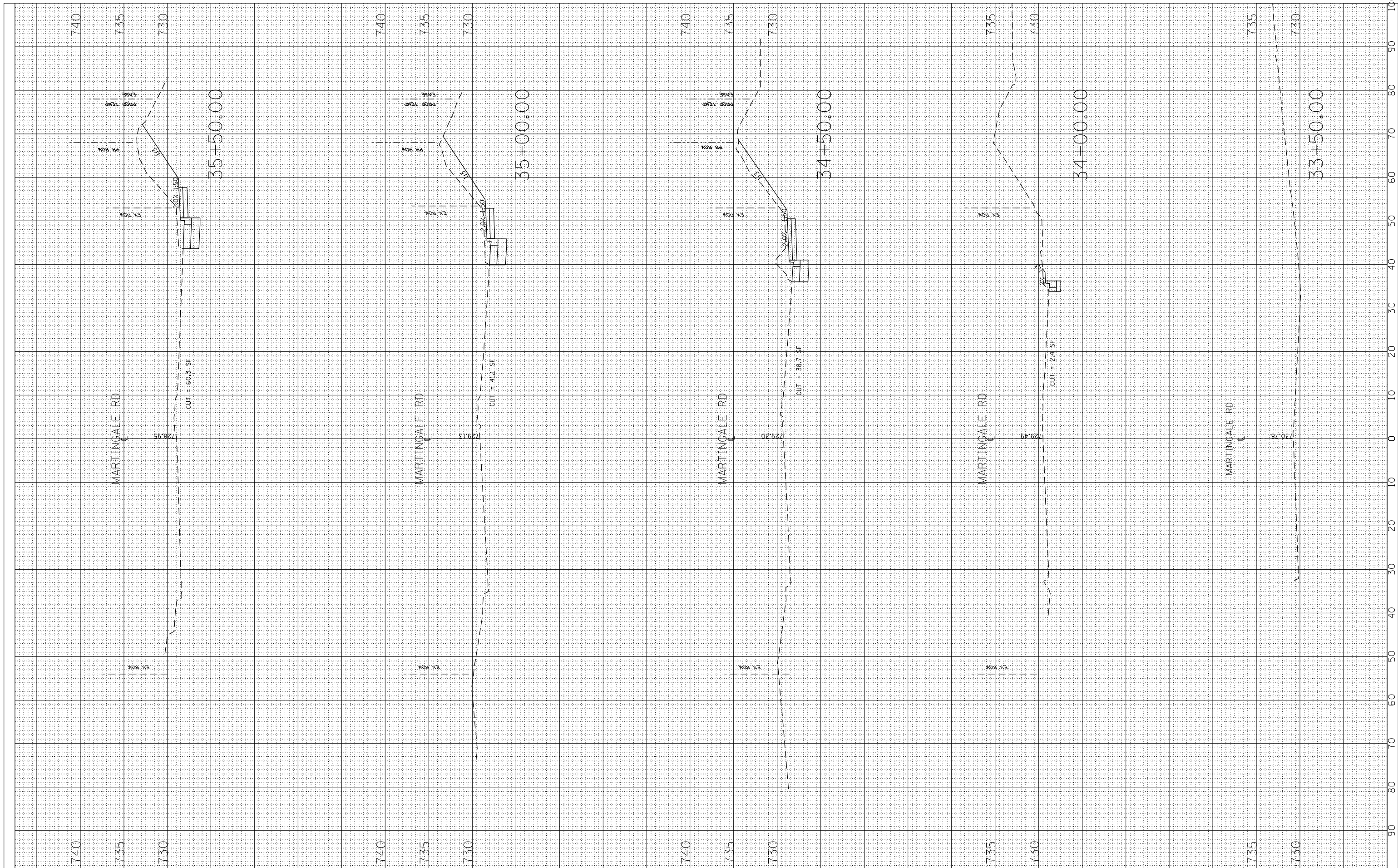
CROSS SECTION - MARTINGALE ROAD

SCALE: SHEET OF SHEETS STA. 36+00.00 TO STA. 37+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)KITS&N-3	COOK	153	148
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



FILE NAME = P:\2014\ME14027_Var\Var_Phil\W06_IL72_IL290\CA00\Shets\DI60Y73-SHT-PXS-MARTINGALE.dgn
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 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 12/21/2015

DESIGNED - TVN
 DRAWN - JP
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

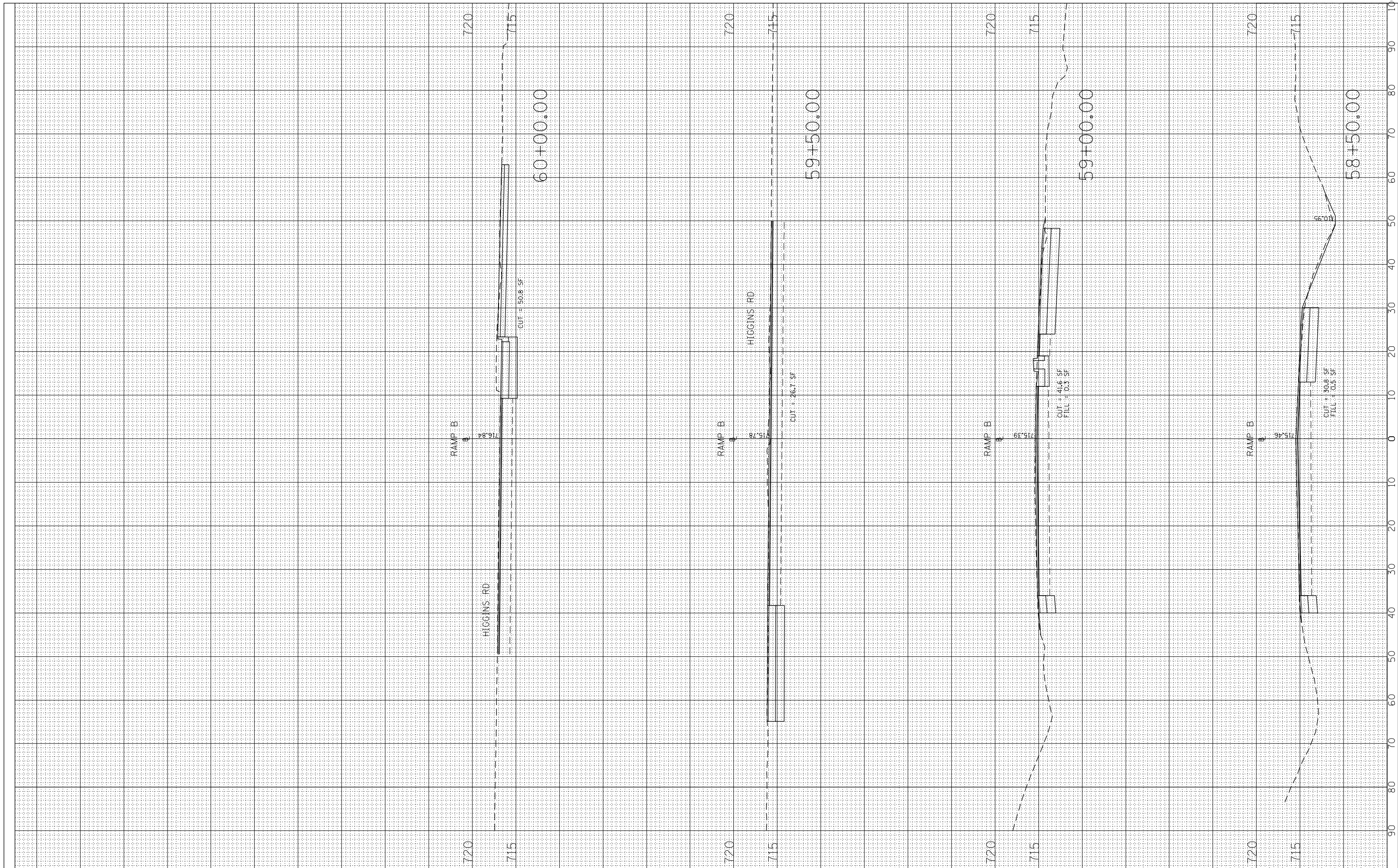
CROSS SECTION - MARTINGALE ROAD

SCALE: SHEET OF SHEETS STA. 33+50.00 TO STA. 35+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	149
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE



FILE NAME = P:\2014\ME14027_Var\Var_Phil\W06_IL72_I290\CADD\Shets\DI60Y73-SHT-PXS-RAMP B.dgn

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 PLOT DATE = 12/21/2015

DESIGNED - TVN
 DRAWN - JP
 CHECKED - TVN
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

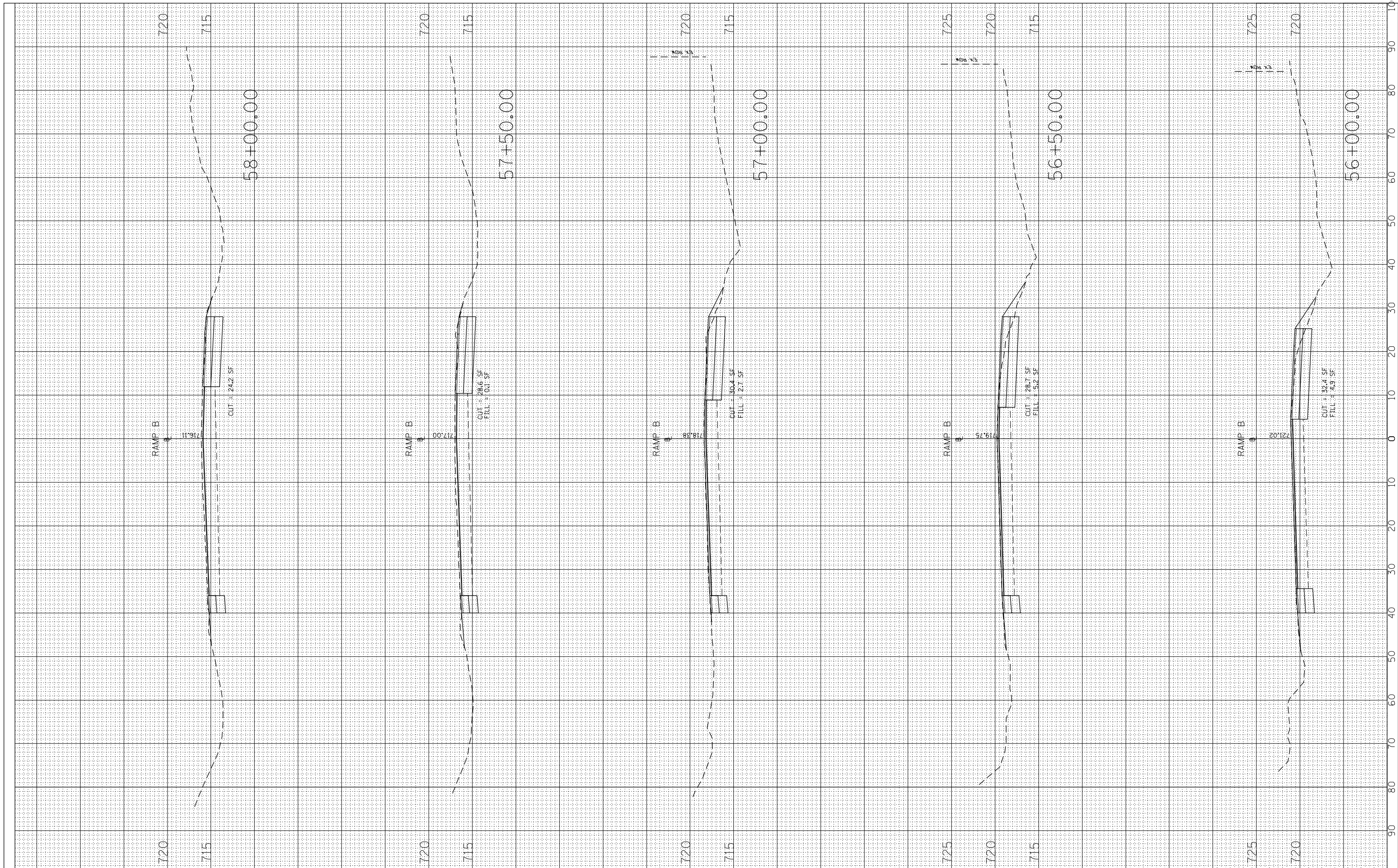
CROSS SECTION - RAMP B

SCALE: SHEET OF SHEETS STA. 58+50.00 TO STA. 60+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	150
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		



FILE NAME = P:\2014\ME14027_Var\Var_Phil\W06_IL72_IL290\CADD\Shets\DI60Y73-SHT-PXS-RAMP B.dgn

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 DRAWN - JP
 CHECKED - TVN
 DATE - 12/21/2015

DESIGNED - TVN
 DRAWN - JP
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

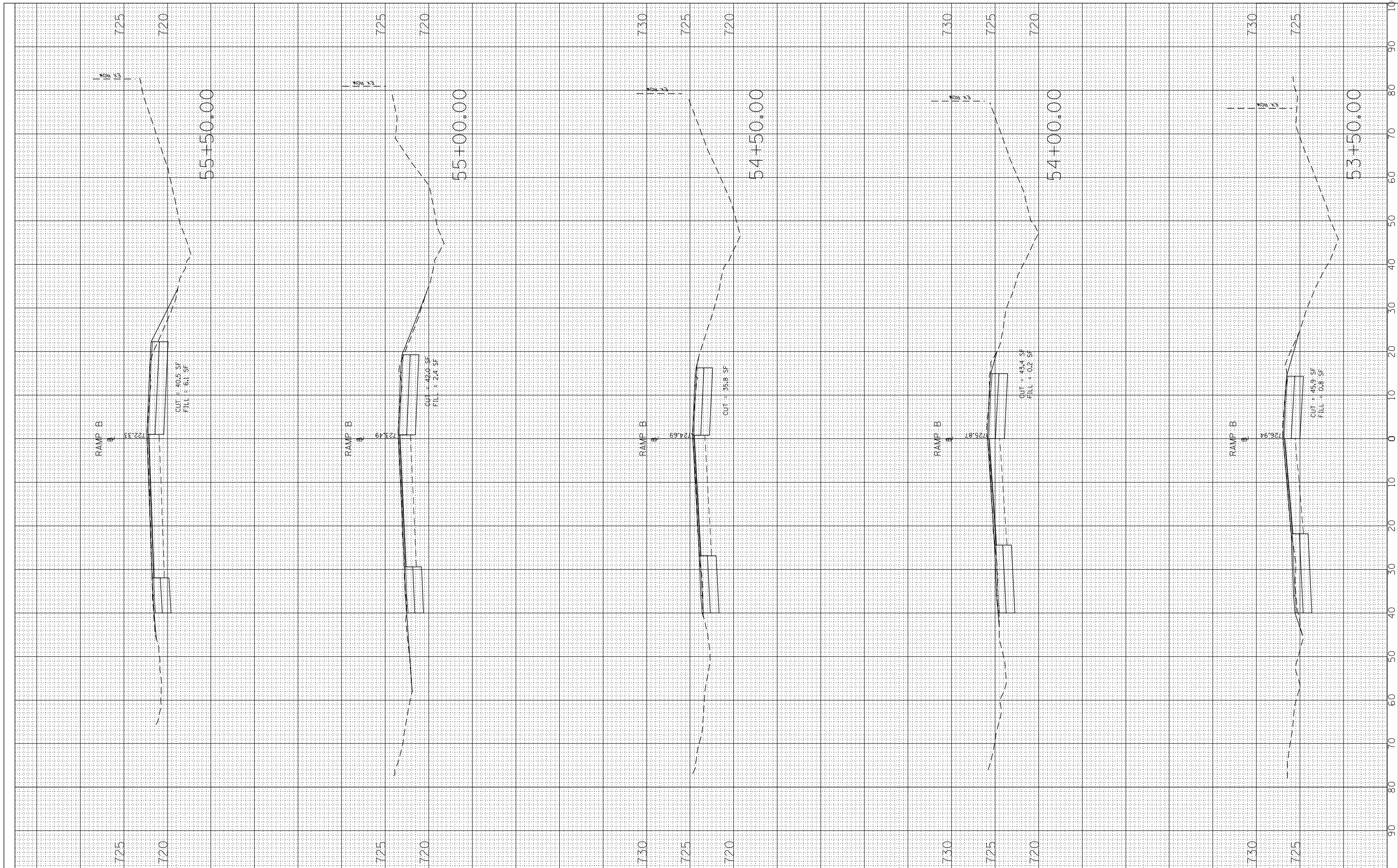
CROSS SECTION - RAMP B

SCALE: SHEET OF SHEETS STA. 56+00.00 TO STA. 58+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	151
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE

ORIGINAL SURVEY NO.	SURVEYED PLOTTED TEMPLATE AREAS CHECKED	BY	DATE



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 PLOT DATE = 12/21/2015

DESIGNED - TVN
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

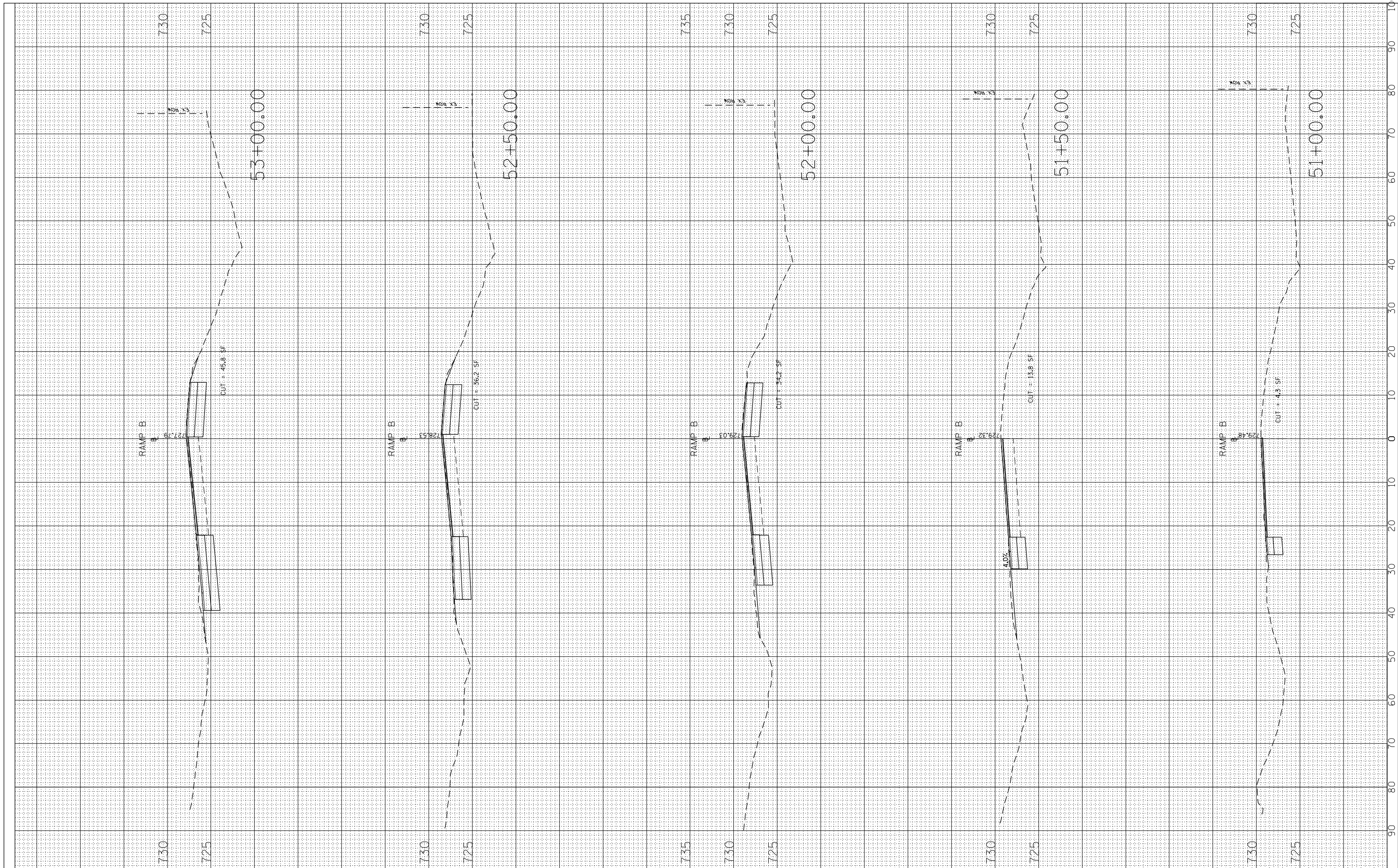
SCALE: SHEET OF SHEETS STA. 53+50.00 TO STA. 55+50.00

CROSS SECTION - RAMP B

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302K)TS&N-3	COOK	153	152
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED PLOTTED AREAS CHECKED	BY	DATE
NOTE BOOK NO.	TEMPLATE AREAS CHECKED		



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 PLOT DATE = 12/21/2015

DESIGNED - TVN
 DRAWN - JP
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CROSS SECTION - RAMP B

SCALE: SHEET OF SHEETS STA. 51+00.00 TO STA. 53+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	(32-3-R&O305-302)ITS&N-3	COOK	153	153
CONTRACT NO. 60Y73			ILLINOIS FED. AID PROJECT	