

| F. A. U. NO. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|----------|------------------------------|-----------|
| 1070 | 15-00031-00-RS | COOK | 13 | 1 |
| STA. | | TO STA. | | |
| FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-4003(596) | |

CONTRACT #61C33

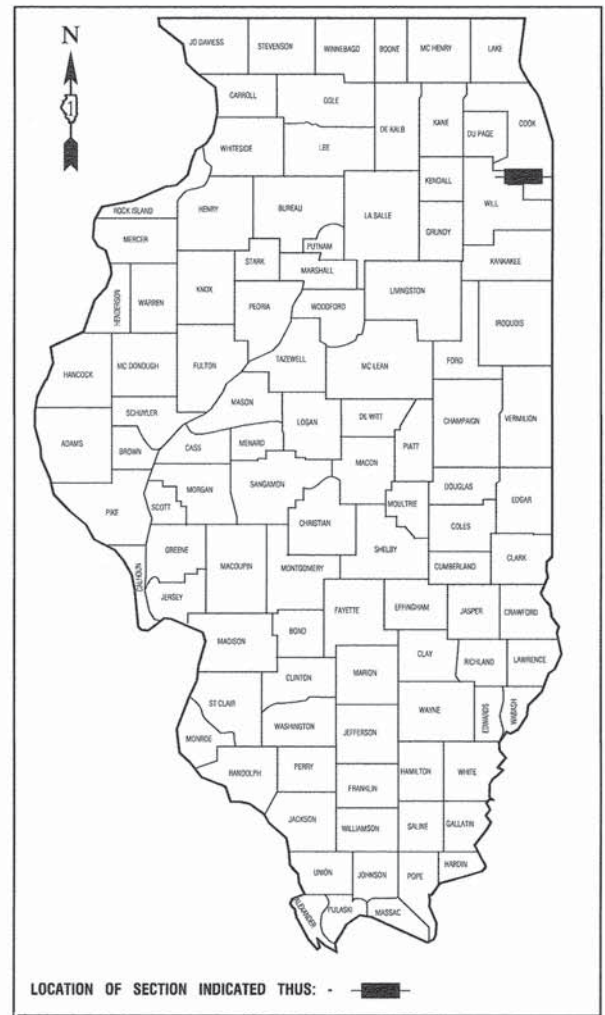
INDEX OF SHEETS 04-22-2016 LETTING ITEM 111 STATE OF ILLINOIS

1. COVER SHEET, INDEX OF SHEETS & STATE STANDARDS
2. SUMMARY OF QUANTITIES & GENERAL NOTES
3. TYPICAL CROSS SECTIONS
- 4.-5. PAVEMENT PLAN
- 6.-7. PAVEMENT MARKING PLAN
- 8.-13. IDOT DISTRICT 1 STANDARD DETAILS

DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

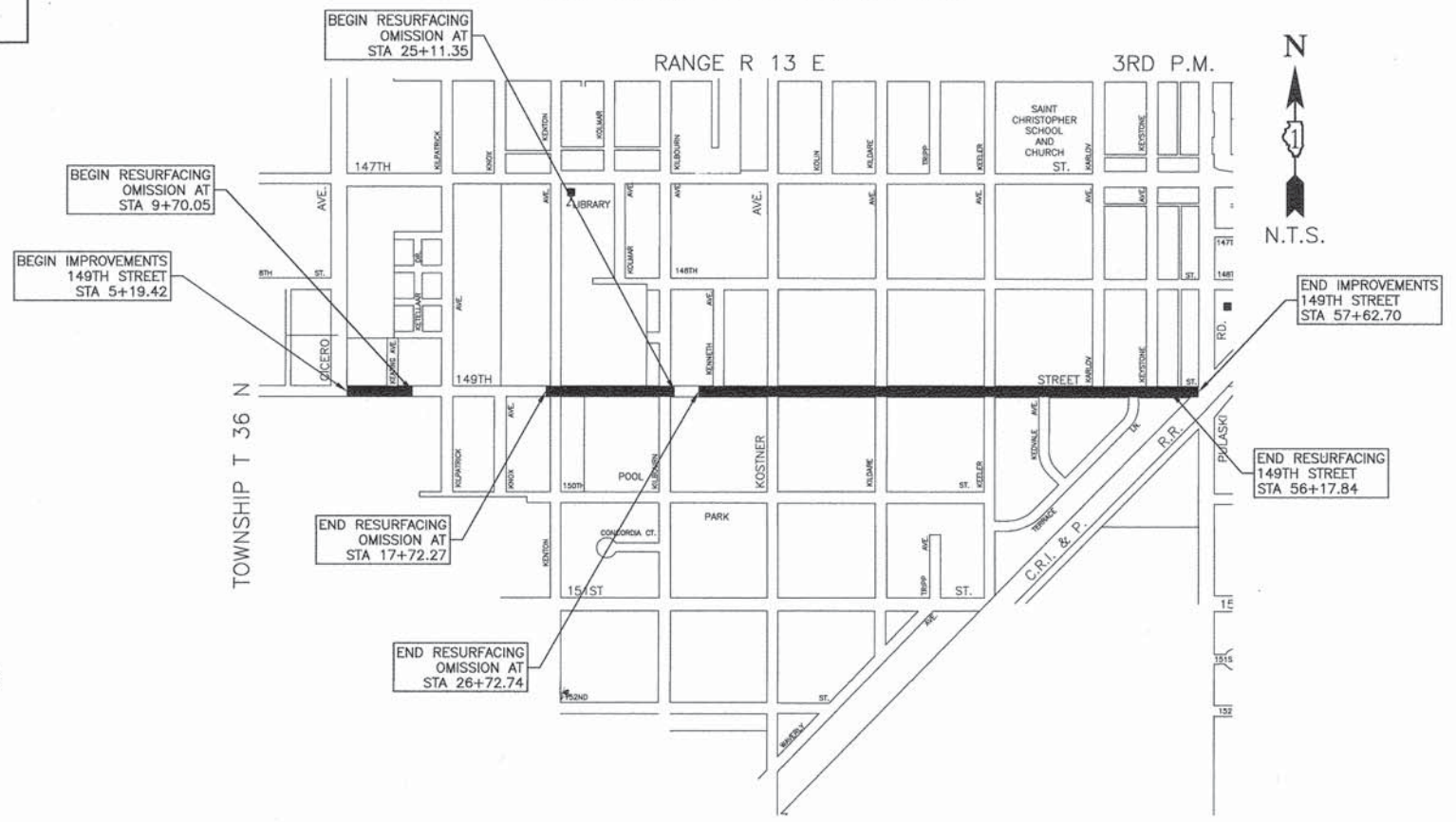
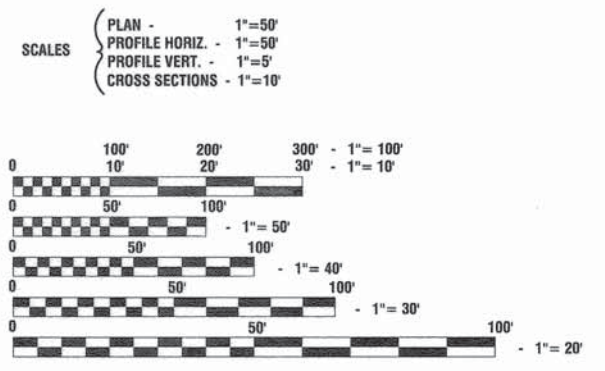
FAU 1070 (149TH STREET)
CICERO AVENUE TO PULASKI ROAD
ROADWAY RESURFACING
SECTION NO.: 15-00031-00-RS
PROJECT NO.: M-4003(596)
VILLAGE of MIDLOTHIAN,
COOK COUNTY
JOB NO.: C-91-089-16



HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 424001-08 PERPENDICULAR CURB RAMPS
- 442201-03 CLASS C AND D PATCHES
- 604001-04 FRAME AND LIDS TYPE 1
- 606001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701006-05 OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
- 701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701801-06 SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-05 TRAFFIC CONTROL DEVICES
- 780001-05 TYPICAL PAVEMENT MARKINGS

| | |
|-------------------------|-------------------------|
| <u>149TH STREET</u> | |
| 2014 ADT - | 3,500 |
| 2034 ADT - | 3,500 |
| POSTED SPEED LIMIT - | 25 mph |
| DESIGN PERIOD - | 20 YEARS |
| DESIGN SPEED LIMIT - | 30 mph |
| STREET CLASSIFICATION - | MINOR COLLECTOR - URBAN |



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 or 811

CONTRACT NO. 61C33

LOCATION MAP

GROSS LENGTH=5,243.28 FEET=0.99 MILES
NET LENGTH=4,279.67 FEET=0.81 MILES

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved: Sharon A. Rybak 12/4/15
Village of Midlothian

Passed: December 21, 2015
Christopher Hoyt
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: December 22, 2015
John Fortmann
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:
John M. [Signature]
12/13/2015

I.D.O.T. FEDERAL AID DESIGN ENGINEER: FAWAD AQUEEL, PE, PTOE 708-705-4021 SCHAUMBURG, IL
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

| SUMMARY OF QUANTITIES | | | | | CONSTRUCTION TYPE CODE |
|-----------------------|----------|---|--------|-------|---------------------------|
| S.I. | CODE NO. | PAY ITEM | UNIT | QUAN | ROADWAY 0005 |
| | 20200100 | EARTH EXCAVATION | CU YD | 37 | 37 |
| | 35101600 | AGGREGATE BASE COURSE, TYPE B 4" | SQ YD | 145 | 145 |
| | 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUNDS | 9815 | 9815 |
| | 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 611 | 611 |
| | 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 191 | 191 |
| | 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 1222 | 1222 |
| | 42300300 | PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH | SQ YD | 118 | 118 |
| | 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 4455 | 4455 |
| | 42400800 | DETECTABLE WARNINGS | SQ FT | 400 | 400 |
| | 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2" | SQ YD | 14117 | 14117 |
| | 44000200 | DRIVEWAY PAVEMENT REMOVAL | SQ YD | 302 | 302 |
| | 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 1844 | 1844 |
| | 44000600 | SIDEWALK REMOVAL | SQ FT | 4455 | 4455 |
| | 44201359 | CLASS C PATCHES, TYPE IV, 10 INCH | SQ YD | 135 | 135 |
| | 44201717 | CLASS D PATCHES, TYPE II, 6 INCH | SQ YD | 15 | 15 |
| | 44201721 | CLASS D PATCHES, TYPE III, 6 INCH | SQ YD | 24 | 24 |
| | 44201723 | CLASS D PATCHES, TYPE IV, 6 INCH | SQ YD | 36 | 36 |
| | 56109210 | WATER VALVES TO BE ADJUSTED | EACH | 13 | 13 |
| | 60250400 | CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID | EACH | 1 | 1 |
| | 60608562 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12 | FOOT | 1844 | 1844 |
| | 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| | 70102620 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501 | L SUM | 1 | 1 |
| | 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 | 1 |

* - INDICATES SPECIALTY ITEMS

| SUMMARY OF QUANTITIES | | | | | CONSTRUCTION TYPE CODE |
|-----------------------|----------|---|-------|------|---------------------------|
| S.I. | CODE NO. | PAY ITEM | UNIT | QUAN | ROADWAY 0005 |
| * | 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 300 | 300 |
| | X2520700 | SODDING, SPECIAL | SQ YD | 806 | 806 |
| | X4811300 | AGGREGATE SHOULDERS, TYPE B (SPECIAL) | TON | 56 | 56 |
| | X6030310 | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) | EACH | 33 | 33 |
| | Z0004514 | HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 4" | SQ YD | 184 | 184 |

* - INDICATES SPECIALTY ITEMS

GENERAL NOTES

1. THE ROBINSON ENGINEERING, LTD. FIELD OFFICE (708-331-6700), AND THE VILLAGE OF MIDLOTHIAN PUBLIC WORKS (708-389-9658) SHALL BE NOTIFIED TWO (2) WORKING DAYS BEFORE CONSTRUCTION BEGINS.
2. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
3. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
5. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
6. ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS WELL AS TEMPORARY AGGREGATE. ANY TEMPORARY AGGREGATE REQUIRED SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RELATED PAY ITEM IT IS NEEDED FOR WHEN DIRECTED BY THE ENGINEER.
7. THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, PAVEMENT, CURB, SIDEWALK, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. SAW CUTTING WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL ITEMS.
8. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
9. THE CONTRACTOR SHALL LEAVE ANY CLEAN EXCESS ORGANIC FILL EXCAVATED DURING THE CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT OPERATIONS ON SITE. ANY EXCESS MATERIAL SHALL BE SPREAD OR PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL AND REPLACEMENT ITEMS. RESTORATION OF AREAS WHERE EXCESS MATERIALS IS PLACED SHALL BE PAID FOR AS SODDING, SPECIAL.
10. PATCHING QUANTITIES FOR THIS CONTRACT SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER AFTER PAVEMENT MILLING.

FILE NAME = 15442-QUAN-01 - IDOT P01

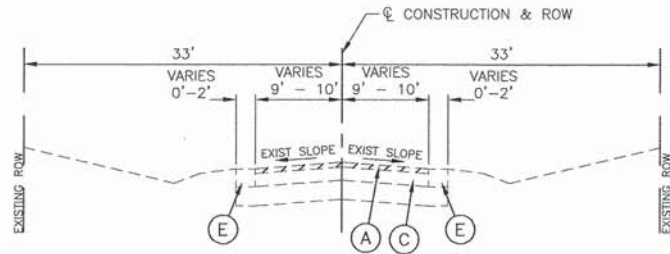
| | | |
|----------------------|----------------|-------------|
| USER NAME = | DESIGNED -- | REVISOR -- |
| | CHECKED -- JAK | REVISION -- |
| PLOT SCALE = | DRAWN -- RG | REVISION -- |
| PLOT DATE = 11-27-15 | CHECKED -- AG | REVISION -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

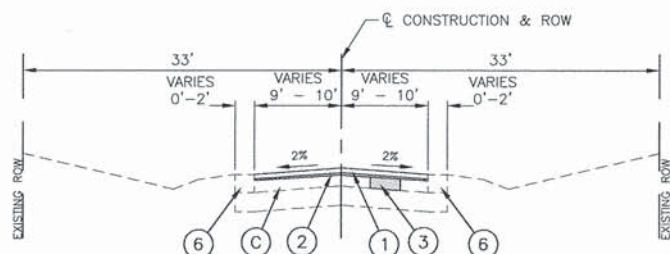
149TH STREET
ROADWAY RESURFACING
SUMMARY OF QUANTITIES & GENERAL NOTES

SCALE: NONE SHEET NO. 2 OF 13 SHEETS STA. TO STA.

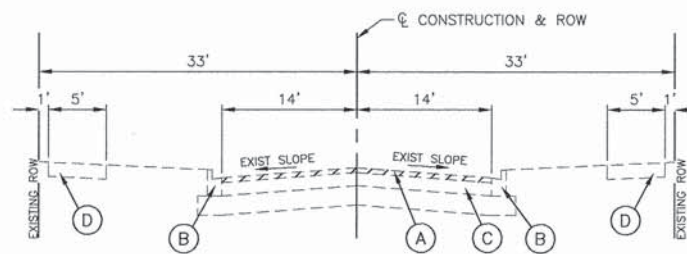
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|---|----------------|--------|-----------------|--------------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1070 | 15-00031-00-RS | COOK | 13 | 2 |
| CONTRACT NO. 61C33 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(596) | | | | |



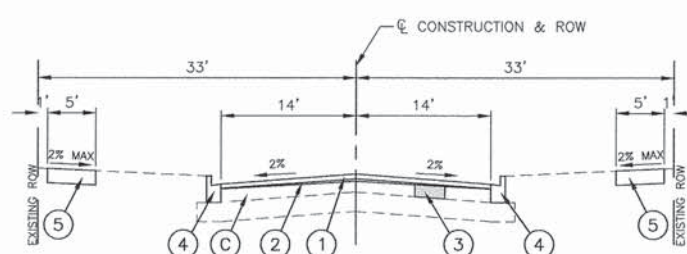
EXISTING TYPICAL SECTION
149TH STREET
CICERO AVENUE TO KILPATRICK AVENUE



PROPOSED TYPICAL SECTION
149TH STREET
CICERO AVENUE TO KILPATRICK AVENUE
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING



EXISTING TYPICAL SECTION
149TH STREET
KILPATRICK AVENUE TO PULASKI ROAD



PROPOSED TYPICAL SECTION
149TH STREET
KILPATRICK AVENUE TO PULASKI ROAD
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

EXISTING LEGEND

- (A) HOT MIX ASPHALT SURFACE REMOVAL, 2"
- (B) EXISTING CURB & GUTTER TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR AS DIRECTED BY ENGINEER
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PCC SIDEWALK TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (E) EXISTING AGGREGATE SHOULDER

PROPOSED LEGEND

- (1) HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1-1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50, 3/4" & VARIES
- (3) CLASS D PATCH, AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (4) PROPOSED COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12
- (5) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5" (REPLACEMENT AT LOCATIONS DIRECTED BY THE ENGINEER)
- (6) PROPOSED AGGREGATE SHOULDER REPAIR (REPLACEMENT AT LOCATIONS DIRECTED BY THE ENGINEER)

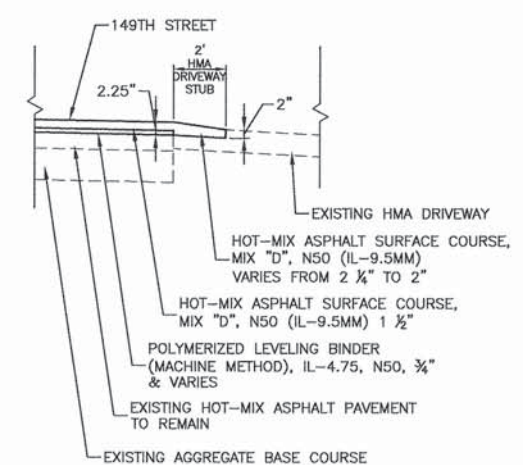
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

(CONTRACTOR SHALL MILL BEFORE PATCHING)

| MIXTURE TYPE | AIR VOIDS @ Ndes |
|--|------------------|
| RESURFACING | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50, 1-1/2" | 4% @ 50 Gyr. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75 N50, 3/4" & VARIES | 3.5% @ 50 Gyr. |
| PATCHING | |
| CLASS D PATCHES, ALL TYPES, (HMA BINDER IL-19.0mm): 6" (IN 2 LIFTS) | 4% @ 70 Gyr. |
| DRIVEWAYS | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5 MM), 1-3/4" | 4% @ 50 Gyr. |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 2-1/4" | 4% @ 50 Gyr. |

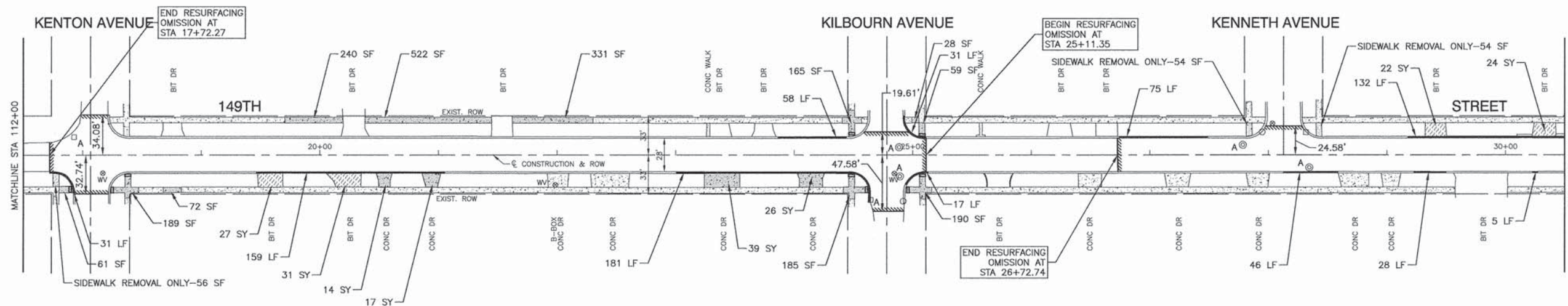
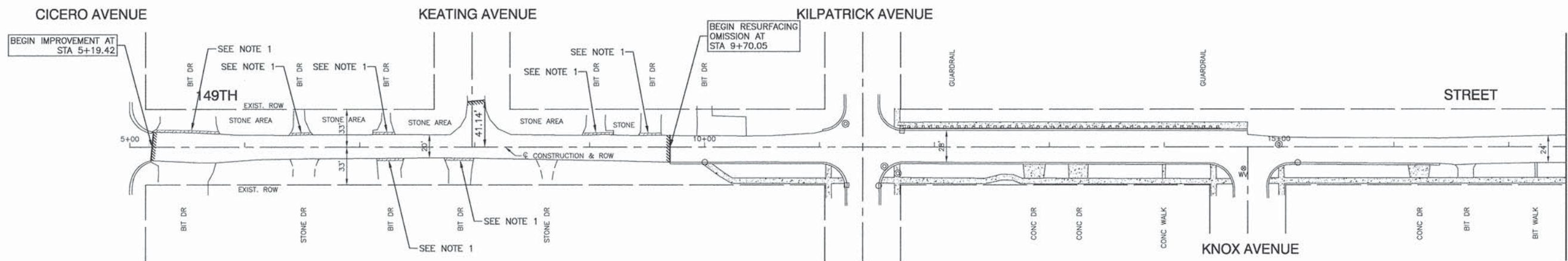
NOTES:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.



NOTE:
REMOVAL OF TOP 2" OF HMA DRIVEWAYS TO BE MEASURED IN PLACE AND PAID FOR AS HOT MIX ASPHALT SURFACE REMOVAL, 2"

UNCURBED DRIVEWAY DETAIL



NOTE:

- 1. SEE "UNCURBED DRIVEWAY DETAIL" ON SHEET 3.

LEGEND

- A STRUCTURE TO BE ADJUSTED
- WV WATER VALVE BOX TO BE ADJUSTED
- //// BUTT JOINT
- CURB AND GUTTER REMOVAL & REPLACEMENT
- CONCRETE REMOVAL & REPLACEMENT
- HMA REMOVAL & REPLACEMENT
- CLASS D PATCHES, 6"
- DETECTABLE WARNINGS (10 SF)

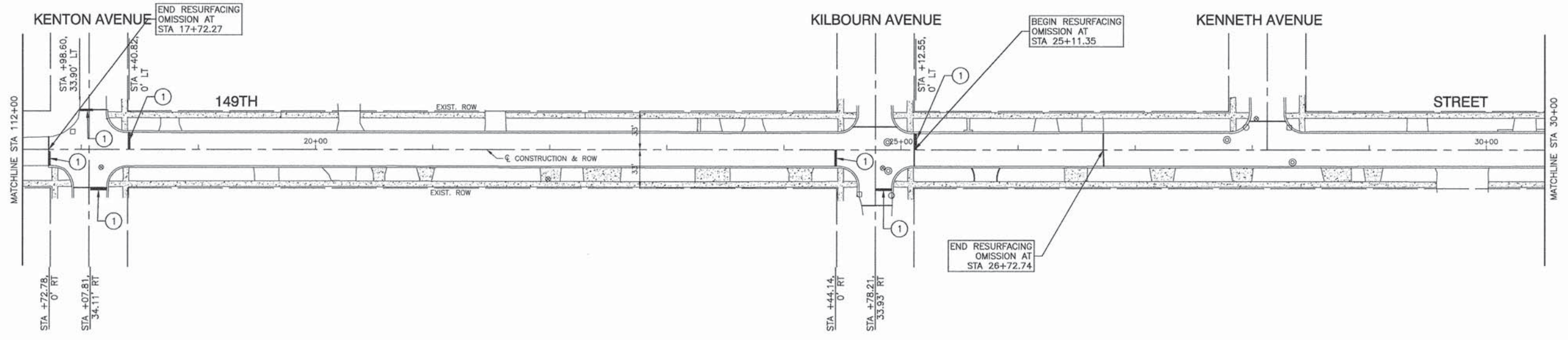
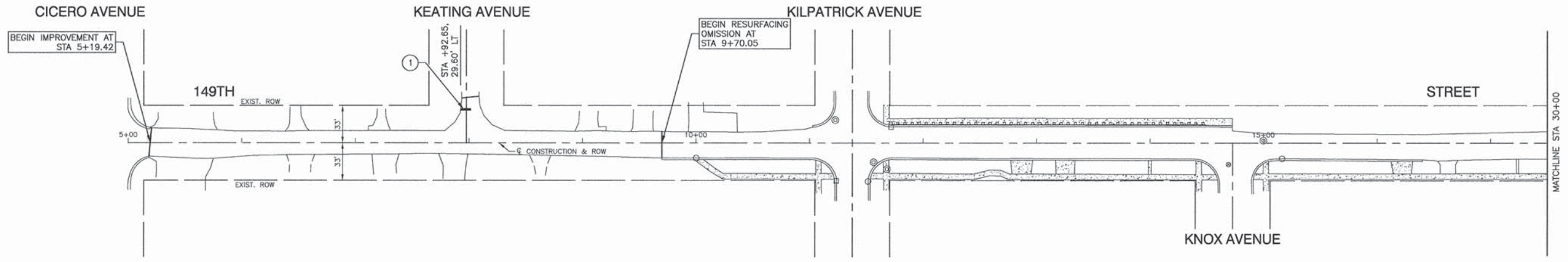
FILE NAME = 15442-PLAN-01 - IDOT P01

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| USER NAME = | DESIGNED -- | REVISED -- |
| | CHECKED -- JAK | REVISED -- |
| PLOT SCALE = | DRAWN -- ACAD | REVISED -- |
| PLOT DATE = 11-27-15 | CHECKED -- ACAD | REVISED -- |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|--|-------------|--------------|--------------|
| 149TH STREET CICERO AVENUE TO PULASKI ROAD PAVEMENT PLAN | | | |
| SCALE: | SHEET NO. 4 | OF 13 SHEETS | STA. TO STA. |

| | | | | |
|---|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1070 | 15-00031-00-RS | COOK | 13 | 4 |
| CONTRACT NO. 61C33 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(596) | | | | |



LEGEND
 (1) 24" WHITE STOP BAR

FILE NAME = 15442-PLAN-01 - IDOT L01

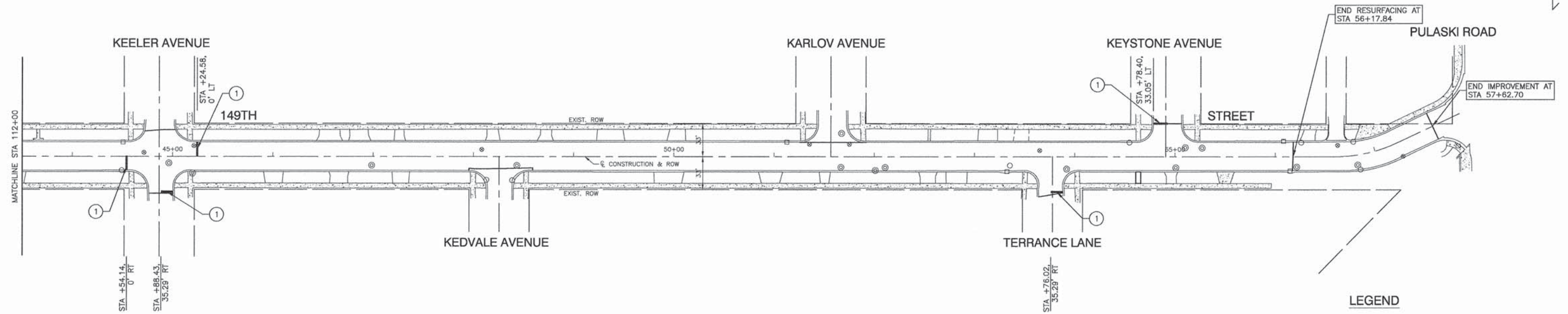
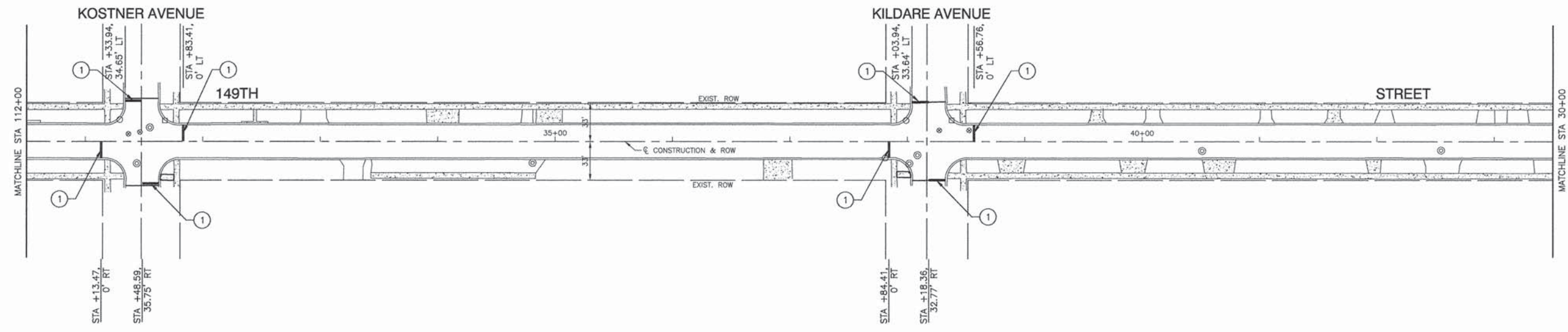
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| USER NAME = | DESIGNED -- | REVISD -- |
| | CHECKED -- JAK | REVISD -- |
| PLOT SCALE = | DRAWN -- ACAD | REVISD -- |
| PLOT DATE = 11-27-15 | CHECKED -- ACAD | REVISD -- |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

149TH STREET
 CICERO AVENUE TO PULASKI ROAD
 PAVEMENT MARKING PLAN

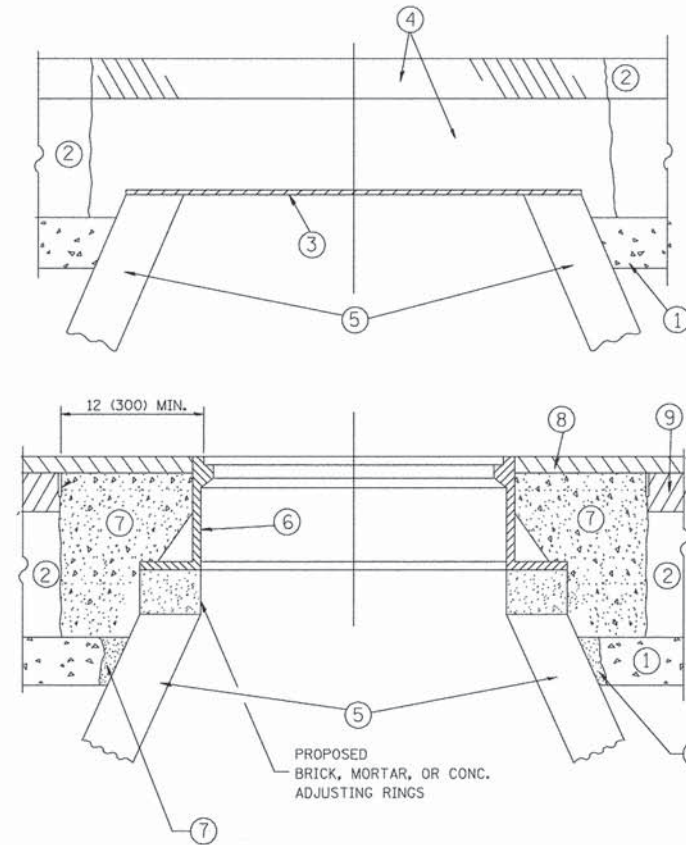
SCALE: SHEET NO. 6 OF 13 SHEETS STA. TO STA.

| | | | | |
|---|----------------|--------|--------------|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1070 | 15-00031-00-RS | COOK | 13 | 6 |
| CONTRACT NO. 61C33 | | | | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(596) | | | | |



LEGEND
 (1) 24" WHITE STOP BAR

| | | | | | | | | | | | | |
|--------------------------------------|-----------------|----------------|------------|---|--|----------------|------------------|-------------|--------------------|--------|--------------|-----------|
| FILE NAME = 15442-PLAN-01 - IDOT L02 | USER NAME = | DESIGNED -- | REVISED -- | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | 149TH STREET CICERO AVENUE TO PULASKI ROAD PAVEMENT MARKING PLAN | | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = | CHECKED -- JAK | REVISED -- | | 1070 | 15-00031-00-RS | COOK | 13 | 7 | | | |
| PLOT DATE = 11-27-15 | DRAWN -- ACAD | REVISED -- | SCALE: | | SHEET NO. 7 | OF 13 SHEETS | STA. | TO STA. | CONTRACT NO. 61C33 | | | |
| | CHECKED -- ACAD | REVISED -- | | | FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | M-4003(596) | | | | |



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

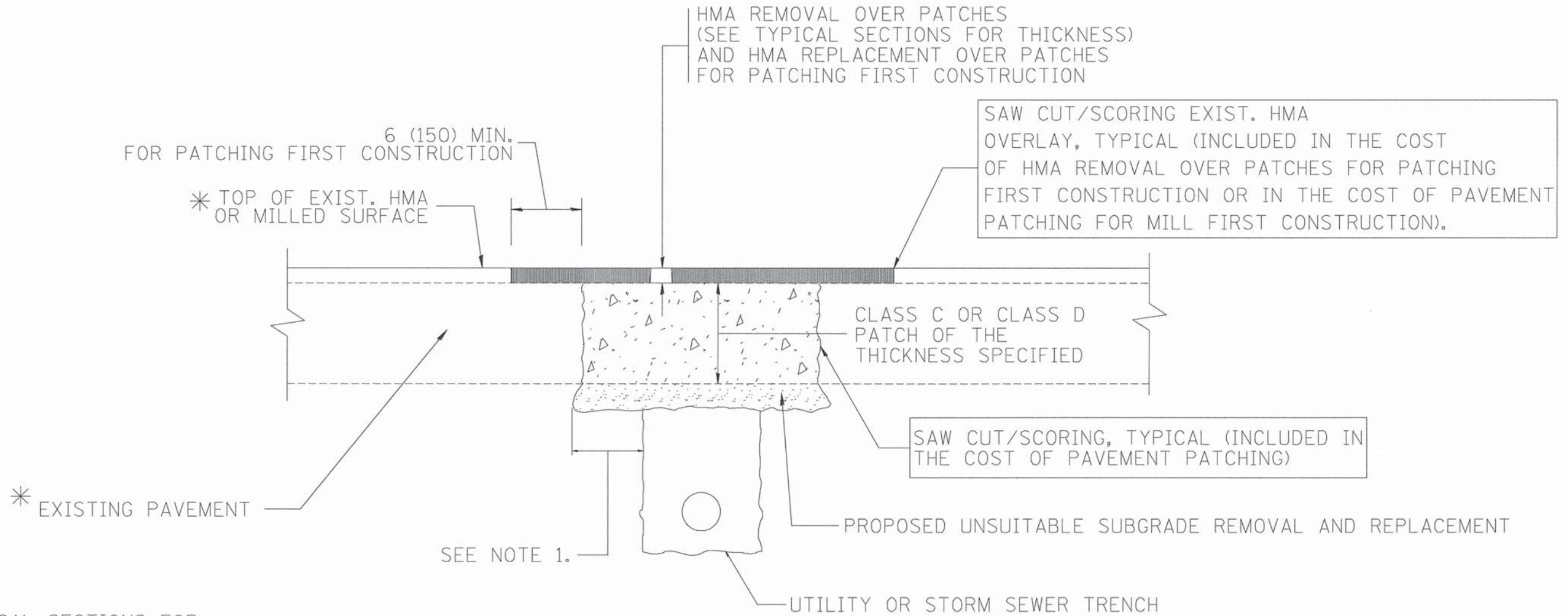
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

| | | | |
|---|---------------------------|--------------------|--------------------------------|
| FILE NAME = | USER NAME = bauerdl | DESIGNED - R. SHAH | REVISED - R. WIEDEMAN 05-14-04 |
| os\pw_work\pwsdot\bauerdl\d0100315\bd08.dgn | | DRAWN - | REVISED - R. BORO 01-01-07 |
| | PLOT SCALE = 1/8" = 1'-0" | CHECKED - | REVISED - R. BORO 03-09-11 |
| | PLOT DATE = 12/6/2011 | DATE - 10-25-94 | REVISED - R. BORO 12-06-11 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | | |
|--|--|--------------------------|----------------|--------------|--------------|------------------------------|
| DISTRICT ONE DRIVEWAY DETAILS | | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| DISTANCE BETWEEN ROW AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5m) | | 1070 | 15-00031-00-RS | COOK | 13 | 8 |
| SCALE: NONE | | SHEET NO. 8 OF 13 SHEETS | | STA. TO STA. | | |
| | | FED. ROAD DIST. NO. 1 | | ILLINOIS | | FED. AID PROJECT M-4003(596) |



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | |
|--|--------------------|--------------------|-----------------------------|
| FILE NAME = c:\projects\distatd22x34\bd22.dgn | USER NAME = beaurd | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 |
| | | DRAWN - | REVISED - R. BORO 01-01-07 |
| | | CHECKED - | REVISED - R. BORO 09-04-07 |
| | | DATE - 10-25-94 | REVISED - K. ENG 10-27-08 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

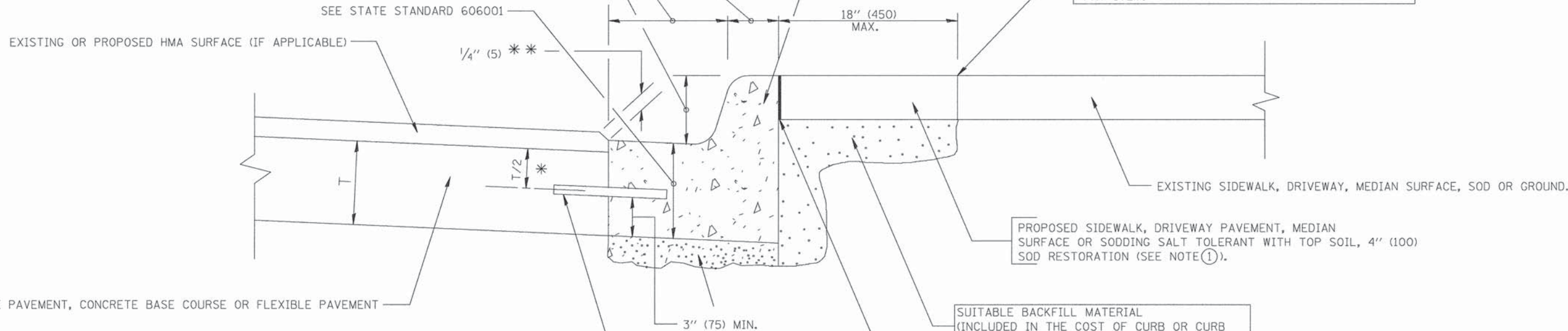
SCALE: NONE SHEET NO. 9 OF 13 SHEETS STA. TO STA.

| FAU RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------------------|----------------|------------------------------|--------------|-----------|
| 1070 | 15-00031-00-RS | COOK | 13 | 9 |
| BD400-04 (BD-22) | | CONTRACT NO. 61C33 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-4003(596) | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SPECIAL AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

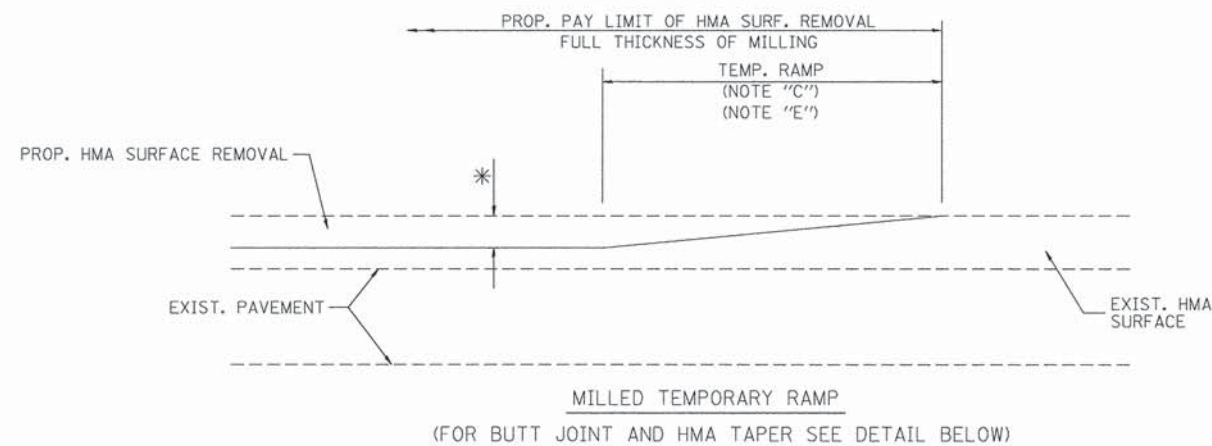
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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| cr\pw_work\pwsdot\drivakoagn\d0108315\bd24.dgn | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - M. GOMEZ 01-22-01 |
| | PLOT DATE = 12/15/2009 | DATE - 03-11-94 | REVISED - R. BORO 12-15-09 |

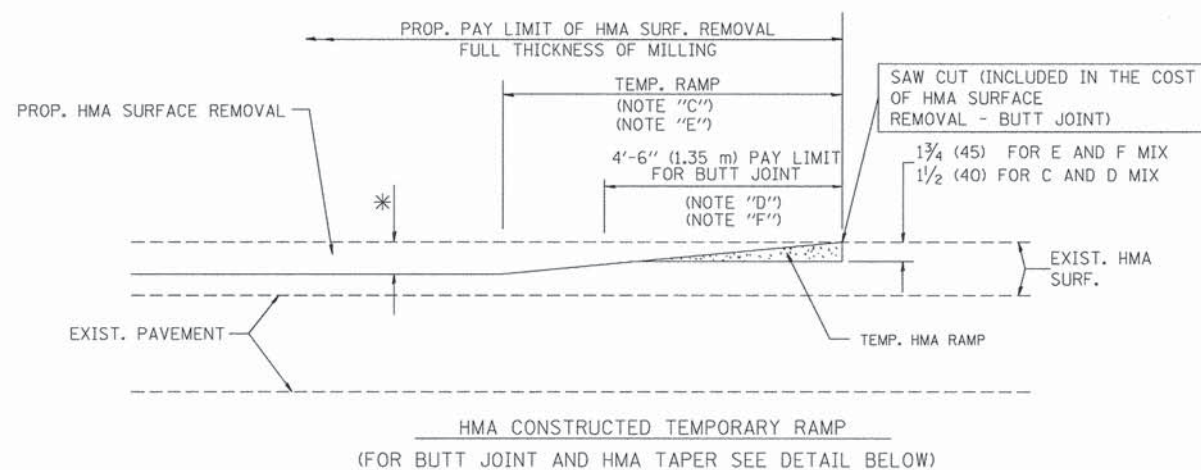
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | |
|--|---------------------------|
| CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | |
| SCALE: NONE | SHEET NO. 10 OF 13 SHEETS |
| STA. | TO STA. |

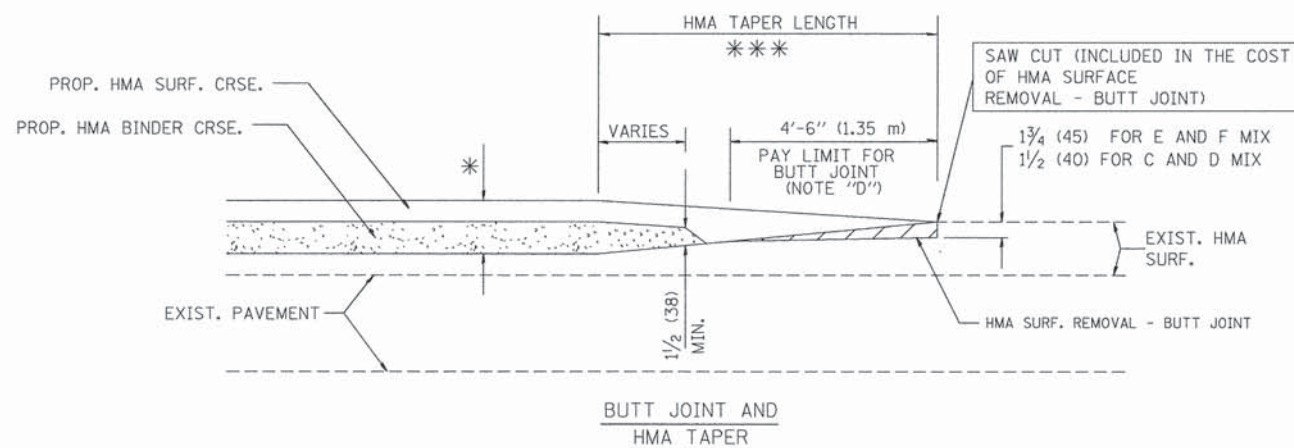
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|-----------------------|----------------|------------------------------|--------------|-----------|
| FAU RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| RD600-06 (RD-24) | | CONTRACT NO. 61C33 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-4003(596) | | |



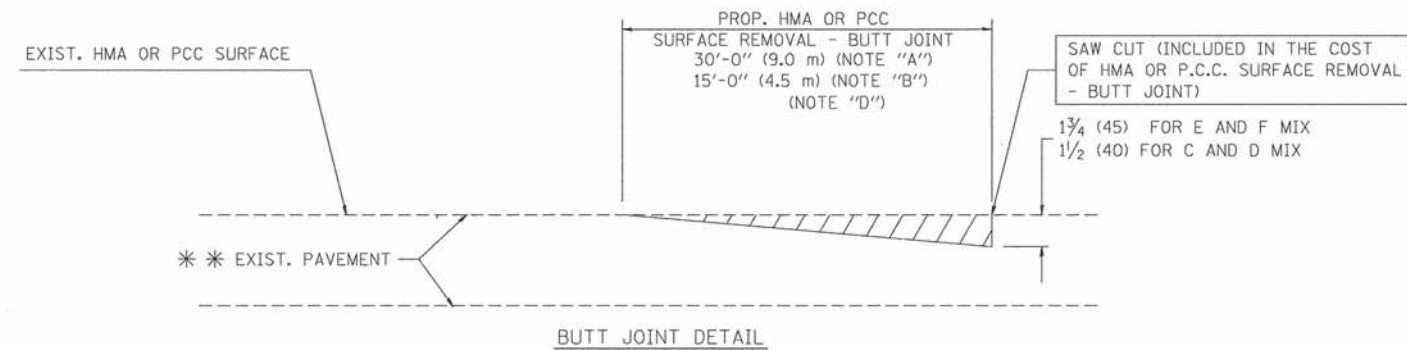
OPTION 1



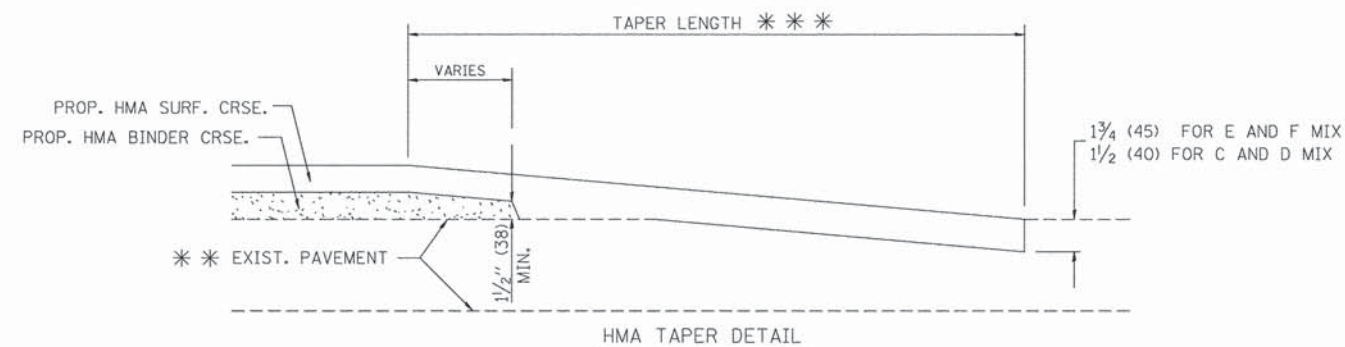
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

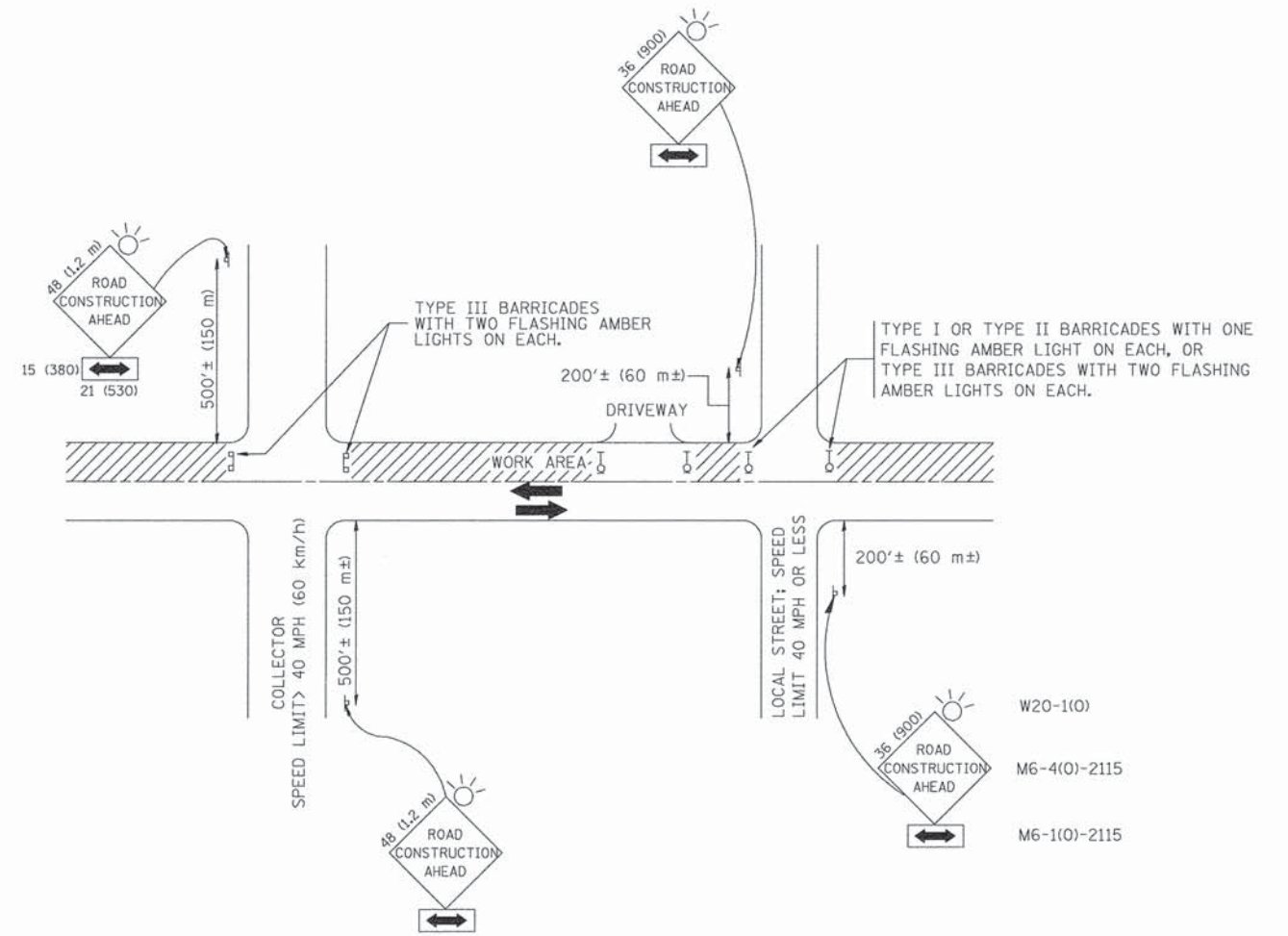
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| | | DRAWN - | REVISED - A. ABBAS 03-21-95 |
| | PLLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED - M. GOMEZ 04-06- |
| | PLLOT DATE = 1/4/2008 | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
BUTT JOINT AND HMA TAPER
DETAILS

SCALE: NONE SHEET NO. 11 OF 13 SHEETS STA. TO STA.

| | | | | |
|-----------------------|----------------|------------------------------|-----------------|--------------|
| FAU RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1070 | 15-00031-00-RS | COOK | 13 | 11 |
| BD400-05 BD32 | | CONTRACT NO. 61C33 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT M-4003(596) | | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

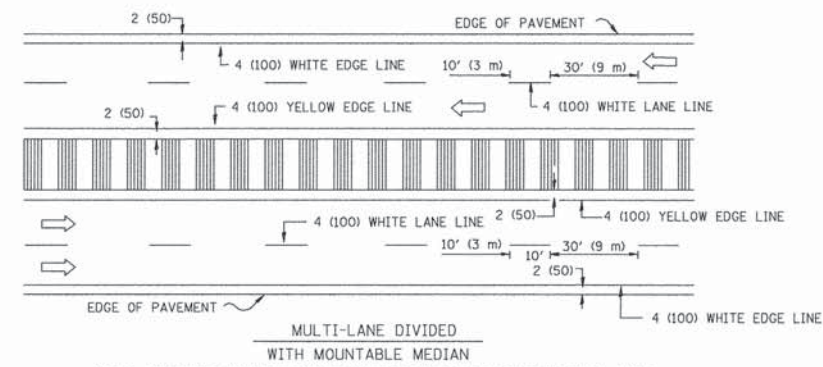
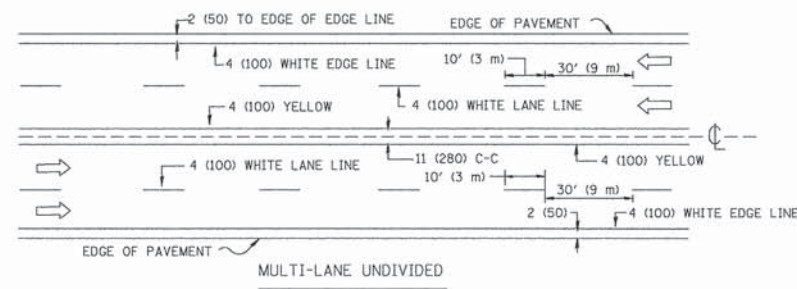
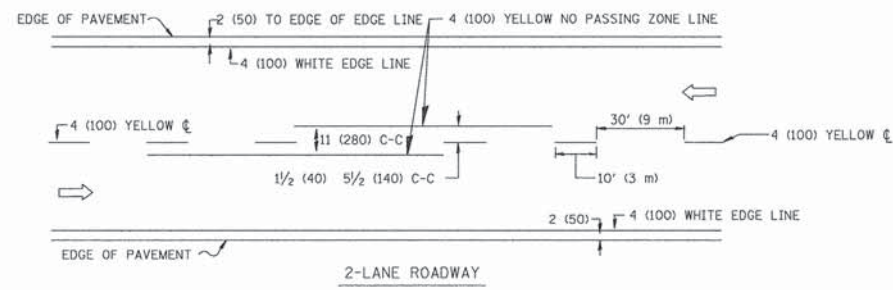
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| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLDT SCALE = 50.0000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLDT DATE = 1/4/2008 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

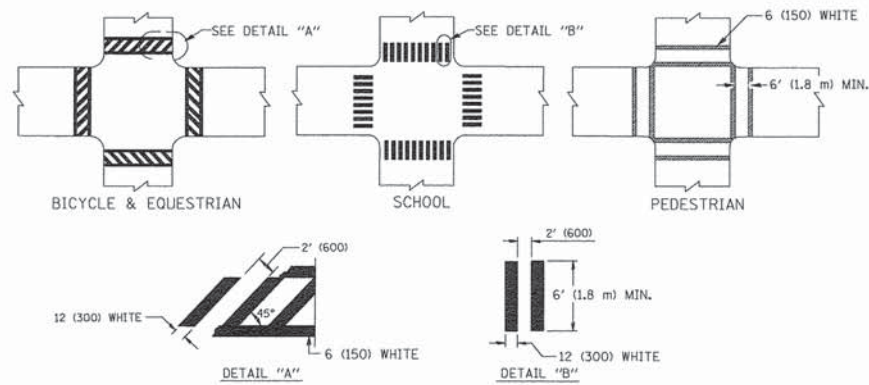
SCALE: NONE SHEET NO. 12 OF 13 SHEETS STA. TO STA.

| FAU RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-----------------------|----------------|--------------------|--------------|-----------|
| 1070 | 15-00031-00-RS | COOK | 13 | 12 |
| TC-10 | | CONTRACT NO. 61C33 | | |
| FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT | M-4003(596) | |

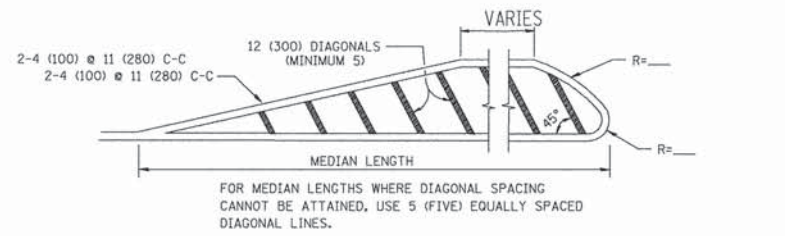
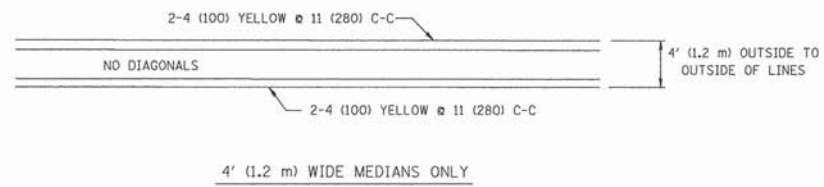


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

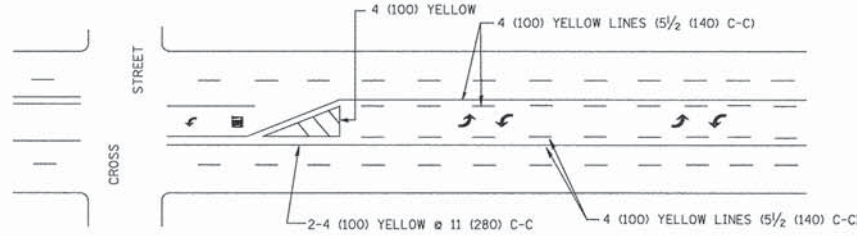


TYPICAL CROSSWALK MARKING

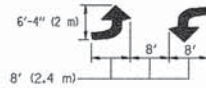


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

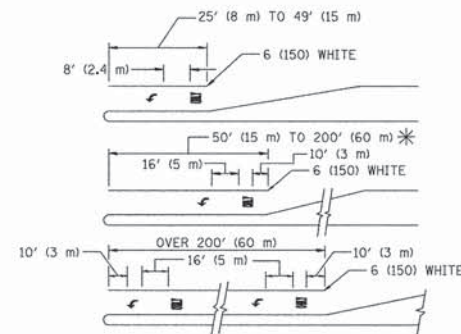


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

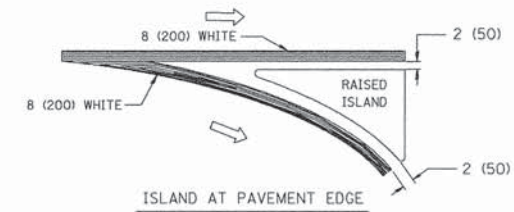
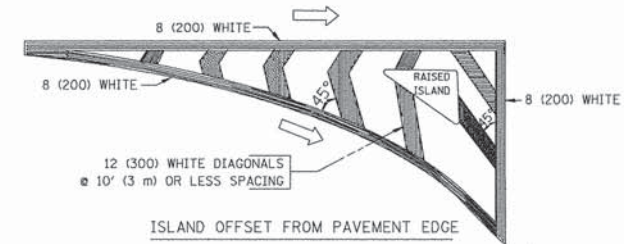


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| CORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

| | | | |
|---|----------------------------|------------------|--------------------------------|
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| | PLOT SCALE = 50.000 "/ IN. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2009 | DATE - 03-19-90 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | | |
|---------------------------|--|---------------------------|----------------|--------------|------------------------------|-----------|
| DISTRICT ONE | | FAU RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| TYPICAL PAVEMENT MARKINGS | | 1070 | 15-00031-00-RS | COOK | 13 | 13 |
| SCALE: NONE | | SHEET NO. 13 OF 13 SHEETS | | STA. TO STA. | CONTRACT NO. 61C33 | |
| | | FED. ROAD DIST. NO. 1 | | ILLINOIS | FED. AID PROJECT M-4003(596) | |