

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR LIST OF HIGHWAY STANDARDS SEE SHEET NO. 2

**TRAFFIC DATA**

ADT:	2012 TRAFFIC	2040 TRAFFIC
BULL VALLEY ROAD (WEST LEG)	6,810 VPD	11,000 VPD
BULL VALLEY ROAD (EAST LEG)	11,955 VPD	20,000 VPD
CURRAN ROAD	6,097 VPD	10,000 VPD

ROADWAY	SPEED POSTED	DESIGN SPEED
BULL VALLEY ROAD	45 MPH	45 MPH
CURRAN ROAD	40 MPH	45 MPH

**DESIGN DESIGNATION**

FAU 3860 (BULL VALLEY ROAD) - MAJOR COLLECTOR  
FAU 0084 (CURRAN ROAD) - MAJOR COLLECTOR

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

# PLANS FOR PROPOSED FEDERAL-AID HIGHWAY

**FAU 3860 (BULL VALLEY ROAD)**  
**EAST OF DRAPER ROAD TO CURRAN ROAD**  
**CHANNELIZATION, WIDENING AND RESURFACING, TRAFFIC SIGNALS**

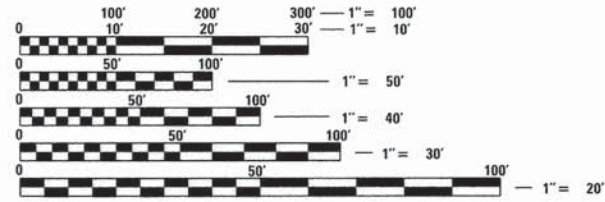
**SECTION: 14-00078-00-CH**  
**PROJECT: M-4003(630)**  
**CITY OF MCHENRY**  
**MCHENRY COUNTY**  
**JOB: C-91-149-16**



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FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

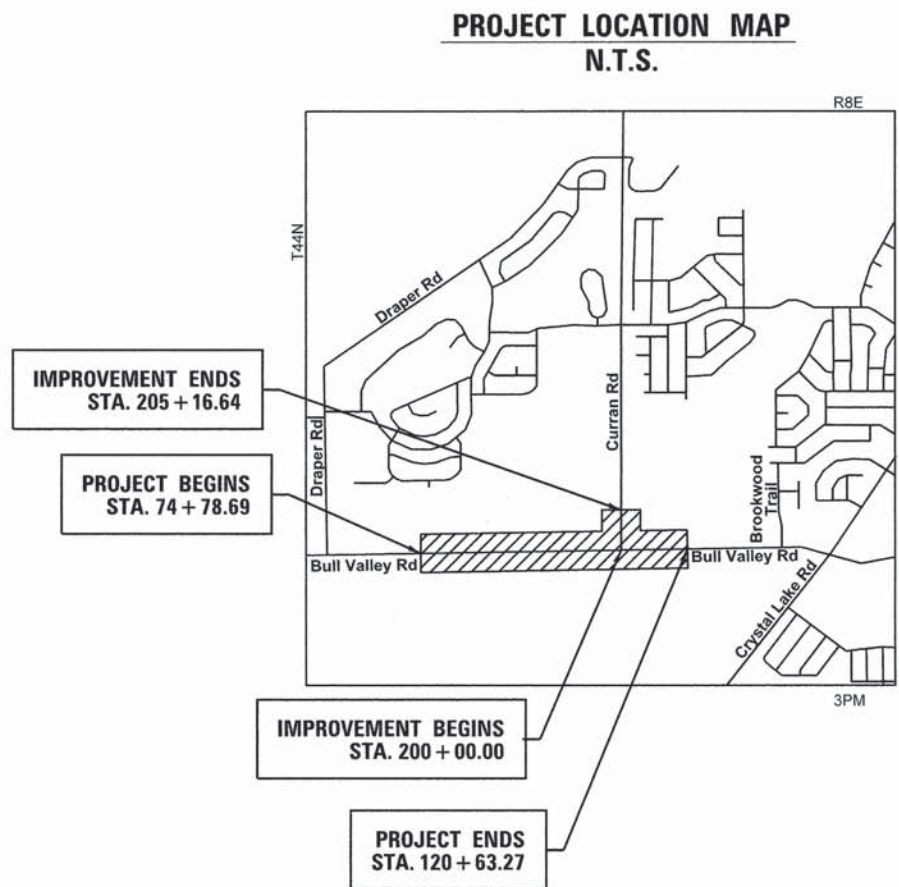
**PROJECT LOCATED IN THE  
CITY OF MCHENRY**



**HRGreen**

420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050  
Phone: 815.385.1178 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com  
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

**PROJECT ENGINEER: J. STRZALKA**  
**PROJECT MANAGER: A. CHAUDHRY**  
**CONTRACT NO. 61C45**



*Jeffrey J. Strzalka*  
JEFFREY J. STRZALKA, P.E.  
EXPIRES: 11-30-17

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED 1-25 2016  
*Jon Schmitt*  
CITY OF MCHENRY

PASSED FEBRUARY 9 2016  
*Christopher Holt*  
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID  
BASED ON LIMITED REVIEW February 4 2016  
*John Furman*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

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OF THE STATE OF ILLINOIS**

**PROJECT LENGTH**  
NET AND GROSS LENGTH OF IMPROVEMENT = 5,102 FT. = 0.97 MILES

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E. 847-705-4021 SCHAUMBURG, IL



**INDEX OF SHEETS**

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**STATE STANDARDS**

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006-00	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
542306-03	PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-04	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-03	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701326-04	LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING, FOR SPEEDS > 45 MPH
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-05	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877011-04	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS

**DISTRICT ONE DETAILS**

STANDARD NO.	LIST OF DESCRIPTION
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-11	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TS-02	DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS
TS-05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS (7 SHEETS)

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS AND STATE STANDARDS**

SCALE: NTS SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	2
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**GENERAL NOTES**

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, APRIL 1, 2016. ALL WORK TO BE COMPLETED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT THEM TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- ALL ELEVATIONS SHOWN ON THE PLANS ARE ON THE NAVD88 DATUM.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- SAW CUTTING WILL BE REQUIRED FOR ALL REMOVAL ITEMS AND SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE IN THE PORTION REMAINING. THE COST OF SAW CUTTING WILL BE INCLUDED IN CONTRACT UNIT BID PRICES FOR THE ITEMS BEING REMOVED.
- DRIVEWAYS ARE TO BE CONSTRUCTED TO THE R.O.W. UNLESS OTHERWISE NOTED.
- ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT OF WAY ACCORDING TO ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
- THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE NOTED.
- ALL SIGNS TO BE REMOVED ALONG THE PROJECT SHALL REMAIN THE PROPERTY OF THE CITY OF MCHENRY AND SHALL BE STORED ON THE JOBSITE FOR PICKUP BY THE CITY.
- CONSTRUCTION WORK MAY BE PERFORMED MONDAY THRU FRIDAY DURING THE HOURS OF 7:00 A.M. TO 9:00 P.M. AND ON WEEKENDS FROM 8:00 A.M. TO 6:00 P.M. NO WORK MAY BE PERFORMED PRIOR OR BEYOND THIS PERIOD WITHOUT PRIOR WRITTEN APPROVAL FROM THE ENGINEER AS COORDINATED WITH THE CITY. NO COMPENSATION WILL BE PAID FOR ANY INCONVENIENCE, DELAY, OR LOSS EXPERIENCE BY THE CONTRACTOR BECAUSE OF ADJUSTMENTS TO THEIR NORMAL SCHEDULE.
- TEMPORARY RAMPS WILL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS AND THE CONSTRUCTION DETAILS. THE INSTALLATION AND REMOVAL OF THE TEMPORARY RAMPS WILL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE CONTRACT UNIT PRICE FOR HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT.
- EXISTING AGGREGATE DRIVEWAYS WILL RECEIVE A TWENTY-FOUR INCH (24") WIDE ASPHALT APRON MATCHING THE PROPOSED RESURFACING LIFT THICKNESS. THE DRIVEWAY APRON WILL BE CONSTRUCTED IN CONJUNCTION WITH THE MAINLINE PAVING AND WILL NOT BE PAID FOR SEPARATELY.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE PAID FOR SEPARATELY AT THE CONTRACT UNIT PRICE FOR EXPLORATION TRENCH, SPECIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- UNLESS OTHERWISE NOTED, OFFSETS FOR DRAINAGE STRUCTURES ARE TO THE CENTER OF THE STRUCTURE.
- FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT OR RECONSTRUCTION COST.
- WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF PIPE CULVERTS WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL PIPE CULVERTS SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- PIPE CULVERT REMOVAL WILL NOT BE PAID FOR SEPARATELY WHEN THE EXISTING PIPE IS IN THE SAME TRENCH AS THE PROPOSED PIPE. THE REMOVAL WILL BE INCLUDED IN THE COST OF THE PIPE BEING INSTALLED. TRENCH BACKFILL REQUIRED TO FILL AREAS OF PIPE CULVERT REMOVAL WHERE THERE IS NO REPLACEMENT WILL BE INCLUDED IN THE COST OF THE REMOVAL.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN, IN AN OPERATING CONDITION, TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND

- CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY THE EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- GRATING FOR CONCRETE FLARED END SECTIONS WILL BE REQUIRED ON ALL FLARED END SECTIONS IN ACCORDANCE WITH SECTION 542 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL BE INCLUDED IN THE COST OF THE FLARED END SECTION BEING INSTALLED.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF THE WATER.
- PIPE CULVERTS CLASS C SHALL BE CORRUGATED ALUMINUM ALLOY PIPE IN ACCORDANCE WITH ARTICLE 542 OF THE STANDARD SPECIFICATIONS.

**BACKFILL**

- PROVIDE TRENCH BACKFILL FOR ALL UTILITY LINES WITHIN 2' OF PAVED AREAS. ALL TRENCH BACKFILL QUANTITIES FOR PIPE CULVERTS HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PIPE SIZE AND INVERT DEPTH FROM SUBGRADE.
- TRENCH BACKFILL MATERIAL SHALL CONSIST OF CA-6 CRUSHED STONE OR CRUSHED AGGREGATE.
- COST OF ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED IN THE DRAINAGE SCHEDULES, INCLUDING BEDDING HAUNCHING AND INITIAL BACKFILL MATERIAL, SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE FOR TRENCH BACKFILL.
- THE CONTRACTOR SHALL FILL THE HOLES CREATED BY THE REMOVAL OF PIPE CULVERTS, AT LOCATIONS IN WHICH THERE IS NO NEW PIPE CULVERT IN THE SAME TRENCH, WITH AGGREGATE BASE COURSE (CA-6) CRUSHED. THE COST OF THE AGGREGATE BASE COURSE WILL BE INCLUDED IN THE COST OF PIPE CULVERT REMOVAL.

**SIGNING AND STRIPING**

- SEE IDOT DISTRICT ONE DETAILS AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
- LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS. THIS WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF 728 AND 729 OF THE STANDARD SPECIFICATIONS.
- ALL SIGNS SHALL BE INSTALLED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. THIS WORK SHALL BE PAID FOR USING THE APPROPRIATE PAY ITEMS. EXISTING SIGNS AND POSTS TO BE REMOVED SHALL BE DISPOSED BY THE CONTRACTOR OFF-SITE AND CONSIDERED INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR POSTING "FRESH OIL" SIGNS (48"x48" MINIMUM) AS NEEDED FOR THE PROJECT. THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY SIGNS IN THE PARKWAY TWENTY-FOUR (24) HOURS PRIOR TO STARTING WORK ON EACH STREET. THE SIGNS SHALL READ "FRESH OIL, TRAVEL AT YOUR OWN RISK." THE CONTRACTOR SHALL REMOVE THE SIGNS AND REINSTALL THEM AS NEEDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN THE COST OF THE APPLICABLE TRAFFIC CONTROL AND PROTECTION PAY ITEMS.
- RAISED REFLECTIVE PAVEMENT MARKER REMOVAL SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 783 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.
- TELESCOPING STEEL SIGN SUPPORTS WITH FASTENING HARDWARE SHALL BE BLACK POWDER COAT.

**EARTHWORK**

- SUBGRADE SHALL BE PREPARED IN ACCORDANCE WITH ARTICLE 301.03 AND 301.04 OF THE STANDARD SPECIFICATIONS BEFORE REMOVAL OF ANY UNSUITABLE MATERIAL.
- PRIOR TO ANY EMBANKMENT PLACEMENT, ALL VEGETATION AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL AS APPROVED BY THE ENGINEER.
- AGGREGATE SUBGRADE IMPROVEMENT IN CU YD (AS) HAS BEEN PROVIDED FOR GENERAL USE. THE ACTUAL NEED FOR REMOVAL OF UNSUITABLE AND UNSTABLE SOILS AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED BY THE CONTRACTOR AT THE DIRECTION OF THE ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED FROM THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE DUE TOWARDS THE CONTRACTOR.

**SEDIMENTATION AND EROSION CONTROL**

- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY AND PERMANENT MEASURES.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- TEMPORARY EROSION CONTROL SEEDING AND EROSION CONTROL BLANKET SHALL BE APPLIED ON ALL DISTURBED AREAS IN ACCORDANCE WITH SECTIONS 250 AND 251 OF THE STANDARD SPECIFICATIONS. SEEDING SHALL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDING SHALL BE DETERMINED BY THE ENGINEER.
- DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN 14 CALENDAR DAYS OF THE END OF THE ACTIVE HYDROLOGIC DISTURBANCE, OR REDISTURBANCE IN ACCORDANCE WITH SECTIONS 250 AND 280 OF THE STANDARD SPECIFICATIONS.
- EROSION CONTROL SYSTEMS SHALL BE INSPECTED WEEKLY AND IMMEDIATELY FOLLOWING ANY STORM HAVING A RAINFALL EQUAL TO ONE-HALF INCH OR GREATER. ANY REQUIRED REPAIRS TO THE EROSION CONTROL SYSTEMS SHALL BE MADE IMMEDIATELY. ANY SILTATION OF CULVERTS, STRUCTURES, OR DITCHES SHALL BE CLEANED AND MAINTAINED BY THE CONTRACTOR UNTIL SEEDING HAS TAKEN HOLD. ALL WASHOUTS, GULLIES, ETC. WILL BE REGRADED AND RESEED BY THE CONTRACTOR. THIS WORK SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED AS DIRECTED BY THE ENGINEER.
- ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY, OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT OR AS DIRECTED BY THE ENGINEER AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES. IF DE-WATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
- THE EROSION CONTROL MEASURES INDICATED IN THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS, AND ALL LOOSE MATERIAL.

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES**

SCALE: NTS      SHEET NO. 2 OF 2 SHEETS      STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	3
*FAU3860, FAU0084			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY STU 80% FEDERAL 0004	SIGNALS STU 80% FEDERAL 0021
20200100	EARTH EXCAVATION	CU YD	459	459	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	7826	7826	
20400800	FURNISHED EXCAVATION	CU YD	3081	3081	
20800150	TRENCH BACKFILL	CU YD	88	88	
21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	7,850	7,850	
25000210	SEEDING, CLASS 2A	ACRE	1.75	1.75	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	146	146	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	146	146	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	146	146	
25100630	EROSION CONTROL BLANKET	SQ YD	7,850	7,850	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	163	163	
28000305	TEMPORARY DITCH CHECKS	FOOT	380.0	380.0	
28000400	PERIMETER EROSION BARRIER	FOOT	2,740.0	2,740.0	
28000500	INLET AND PIPE PROTECTION	EACH	26	26	
28100107	STONE RIPRAP, CLASS A4	SQ YD	62	62	
28200200	FILTER FABRIC	SQ YD	62	62	
* 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	1,540	1,540	
31101400	SUBBASE GRANULAR MATERIAL, TYPE B 6"	SQ YD	2,834	2,834	
35400500	PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 10"	SQ YD	805	805	
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	2,670	2,670	
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	204	204	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	11,098	11,098	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	37	37	
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	1,085	1,085	
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1,985	1,985	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	20	20	
44000100	PAVEMENT REMOVAL	SQ YD	448	448	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	7,875	7,875	
44000163	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/2"	SQ YD	5,014	5,014	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	111	111	
44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	253	253	

\* SPECIAL PROVISION  
+ SPECIALTY ITEM

COMPANY NAME: #COMPANY NUMBER  
PROJECT CONTACT: #PROJECT CONTACT #  
CLIENT: #CLIENT #  
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	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
BULL VALLEY ROAD

SCALE: NTS SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	4
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY STU 80% FEDERAL 0004	SIGNALS STU 80% FEDERAL 0021
44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	253	253	
44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	253	253	
44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	253	253	
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	3,970.0	3,970.0	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	290	290	
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	1,990	1,990	
542A0220	PIPE CULVERTS, CLASS A, TYPE 1 15"	FOOT	79.0	79.0	
542A8209	PIPE CULVERTS, CLASS A, TYPE 2 EQUIVALENT ROUND-SIZE 24"	FOOT	332.0	332.0	
542C0220	PIPE CULVERTS, CLASS C, TYPE 1 15"	FOOT	250.0	250.0	
50105220	PIPE CULVERT REMOVAL	FOOT	396.0	396.0	
54213660	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 15"	EACH	2	2	
54214509	PRECAST REINFORCED CONCRETE FLARED END SECTIONS, EQUIVALENT ROUND-SIZE 24"	EACH	4	4	
67100100	MOBILIZATION	LSUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	LSUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	LSUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	LSUM	1	1	
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	15	15	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3,636.0	3,636.0	
<del>70300150</del>	<del>SHORT TERM PAVEMENT MARKING REMOVAL</del>	<del>SQ FT</del>	<del>1,212.0</del>	<del>1,212.0</del>	
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	146.0	146.0	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	9,032.0	9,032.0	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	100.0	100.0	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	45.0	45.0	
<del>70301000</del>	<del>WORK ZONE PAVEMENT MARKING REMOVAL</del>	<del>SQ FT</del>	<del>4,509.0</del>	<del>4,509.0</del>	
72000100	SIGN PANEL - TYPE 1	SQ FT	54.0	40.5	13.5
72000200	SIGN PANEL - TYPE 2	SQ FT	9.8		9.8
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	18.0	18.0	
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	106.0	106.0	
+ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	219.0	219.0	
+ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17,506.0	17,506.0	
+ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	796.0	796.0	

\* SPECIAL PROVISION  
+ SPECIALTY ITEM

COMPANY NAME: #0000000000  
PROJECT CONTACT: #0000000000  
CLIENT: #0000000000  
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PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
BULL VALLEY ROAD

SCALE: NTS SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	5
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY STU 80% FEDERAL 0004	SIGNALS STU 80% FEDERAL 0021
+ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	171.0	171.0	
+ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	100.0	100.0	
+ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	175	175	
* + 80500010	SERVICE INSTALLATION - GROUND MOUNTED	EACH	1		1
* + 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	675.0		675.0
* + 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	40.0		40.0
* + 81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	220.0		220.0
* + 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	296.0		296.0
* + 81400100	HANDHOLE	EACH	2		2
+ 81400200	HEAVY-DUTY HANDHOLE	EACH	4		4
+ 81400300	DOUBLE HANDHOLE	EACH	1		1
+ 81702120	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	505.0		505.0
* + 85100500	PAINT NEW TRAFFIC SIGNAL POST	EACH	2		2
* + 85100701	PAINT NEW MAST ARM AND POLE, 40 FOOT AND OVER	EACH	1		1
* + 85100800	PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FOOT	EACH	1		1
* + 85100901	PAINT NEW COMBINATION MAST ARM AND POLE, 40 FOOT AND OVER	EACH	1		1
+ 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	444.0		444.0
+ 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,810.0		1,810.0
+ 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	912.0		912.0
+ 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,295.0		1,295.0
+ 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	49.0		49.0
* + 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	795.0		795.0
* + 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2		2
* + 87700240	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1		1
+ 87702860	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 26 FT.	EACH	1		1
+ 87702940	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 42 FT.	EACH	1		1
* + 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12.0		12.0
* + 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4.0		4.0
* + 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	39.0		39.0
* + 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4		4
* + 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4		4

\* SPECIAL PROVISION  
+ SPECIALTY ITEM

COMPANY NAME: HRGreen  
PROJECT CONTACT: #PROJECT CONTACT#  
CLIENT: #CLIENT#  
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
BULL VALLEY ROAD

SCALE: NTS SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	14-00078-00-CH	MCHENRY	59	6
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY STU 80% FEDERAL 0004	SIGNALS STU 80% FEDERAL 0021
* + 88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2		2
* + 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2		2
* + 88200410	TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	6		6
+ 88500100	INDUCTIVE LOOP DETECTOR	EACH	5		5
* + 88600100	DETECTOR LOOP, TYPE I	FOOT	616.0		616.0
* + 88700200	LIGHT DETECTOR	EACH	3		3
* + 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1		1
* + X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	442.0		442.0
* X0327487	TRIAxIAL GEOGRID REINFORCEMENT, TYPE I	SQ YD	3,543	3,543	
* + X1400107	FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET	EACH	1		1
* X2130010	EXPLORATION TRENCH, SPECIAL	FOOT	100.0	100.0	
* + X8211175	LUMINAIRE, LED, HORIZONTAL MOUNT, 175 WATT	EACH	2		2
* + X8620200	UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1		1
* Z0013798	CONSTRUCTION LAYOUT	LSUM	1	1	

\* SPECIAL PROVISION  
+ SPECIALTY ITEM

COMPANY NAME: #COMPANY NAME#  
PROJECT CONTACT: #PROJECT CONTACT#  
CLIENT: #CLIENT#  
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PLOT DATE = 1/25/2016	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
BULL VALLEY ROAD

SCALE: NTS SHEET NO. 4 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	7
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

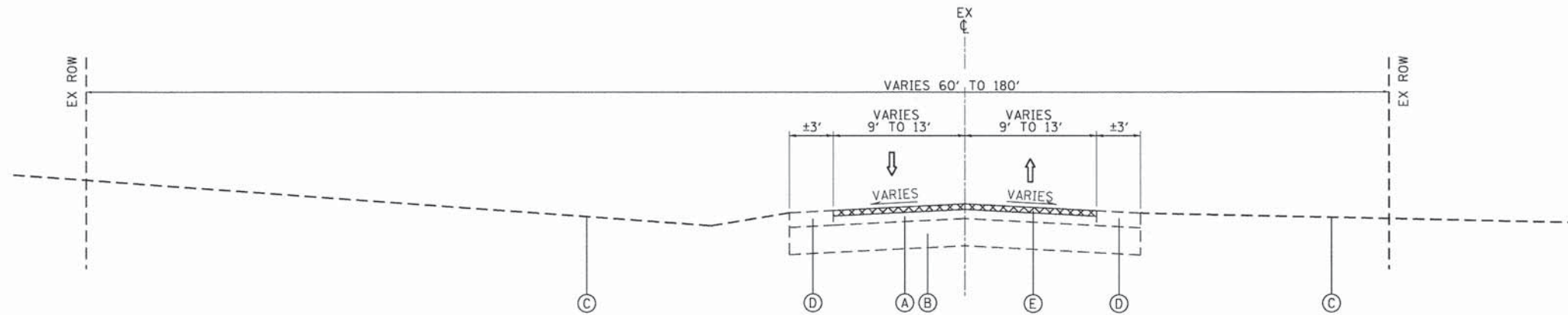


**EXISTING LEGEND**

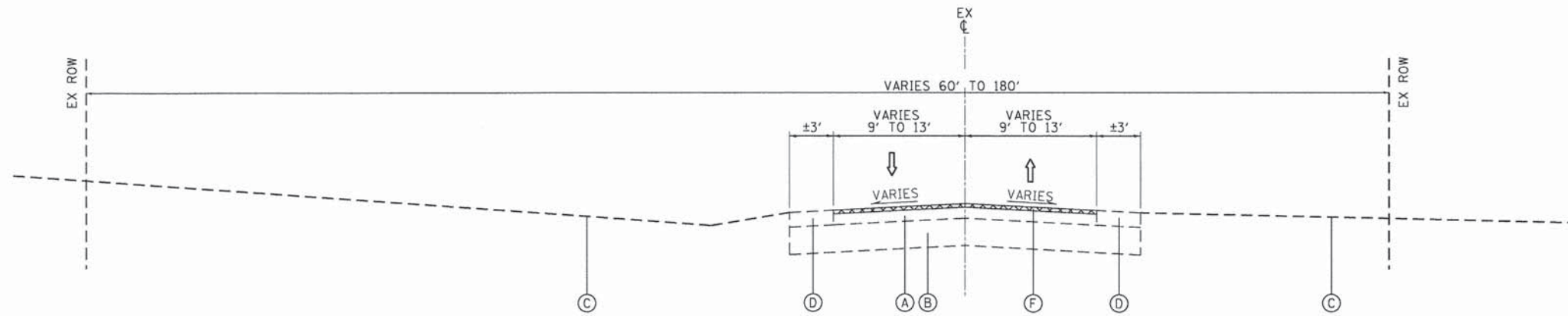
- (A) EXISTING HOT-MIX ASPHALT PAVEMENT; 7"-13"
- (B) EXISTING AGGREGATE BASE COURSE; 9"-16"
- (C) EXISTING GROUND
- (D) EXISTING AGGREGATE SHOULDER
- (E) HOT-MIX ASPHALT SURFACE REMOVAL; 3 1/2"
- (F) HOT-MIX ASPHALT SURFACE REMOVAL; 2"

**PROPOSED LEGEND**

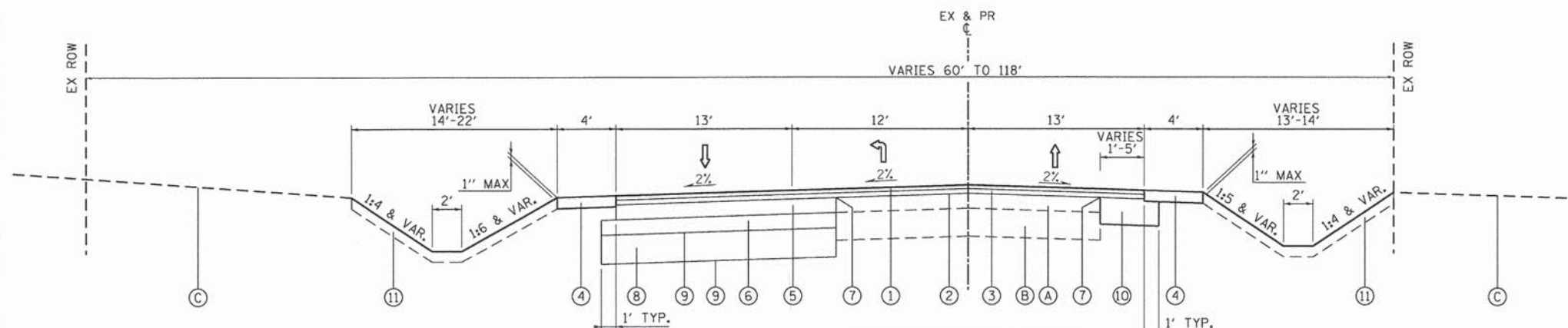
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 2"
- (2) BITUMINOUS MATERIALS (TACK COAT)
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/4"
- (4) AGGREGATE SHOULDERS, TYPE B 6" (9% MAX SLOPE) (CA-6 CRUSHED GRAVEL OR CRUSHED STONE)
- (5) HOT-MIX ASPHALT BASE COURSE; 6"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 6" (SO YD)
- (7) STRIP REFLECTIVE CRACK CONTROL
- (8) AGGREGATE SUBGRADE IMPROVEMENT (CU YD) (AS DIRECTED BY THE ENGINEER)
- (9) TRIAXIAL GEOGRID REINFORCEMENT, TYPE I
- (10) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 10" (SO YD)
- (11) SEEDING, CLASS 2A TOPSOIL FURNISH AND PLACE, 6"
- (12) AGGREGATE SHOULDERS, TYPE B (9% MAX SLOPE) (CA-6 CRUSHED GRAVEL OR CRUSHED STONE)



**EXISTING TYPICAL SECTION**  
 BULL VALLEY ROAD  
 STA. 103+23.58 TO STA. 118+35.38  
 CURRAN ROAD  
 STA. 200+00.00 TO STA. 205+16.64



**EXISTING TYPICAL SECTION**  
 BULL VALLEY ROAD  
 STA. 74+78.69 TO STA. 103+23.58  
 STA. 118+35.38 TO STA. 120+63.27



**PROPOSED TYPICAL SECTION**  
 BULL VALLEY ROAD  
 STA. 103+23.58 TO STA. 110+81.77

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>PAVEMENT WIDENING</b>	
HMA SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"	4.0% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/4"	4.0% @ 70 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm); 6"	4.0% @ 70 GYR.
<b>PATCHING</b>	
CLASS D PATCHES, (HMA BINDER, IL-19mm)	4.0% @ 70 GYR.
<b>INCIDENTAL HOT-MIX ASPHALT SURFACING</b>	
HMA SURFACE COURSE, MIX "D", N50 (IL-9.5mm); 3"	4.0% @ 50 GYR.

THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

PATCHING LOCATIONS TO BE DETERMINED BY THE ENGINEER AFTER MILLING OPERATIONS.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: [Redacted]  
 CLIENT: [Redacted]  
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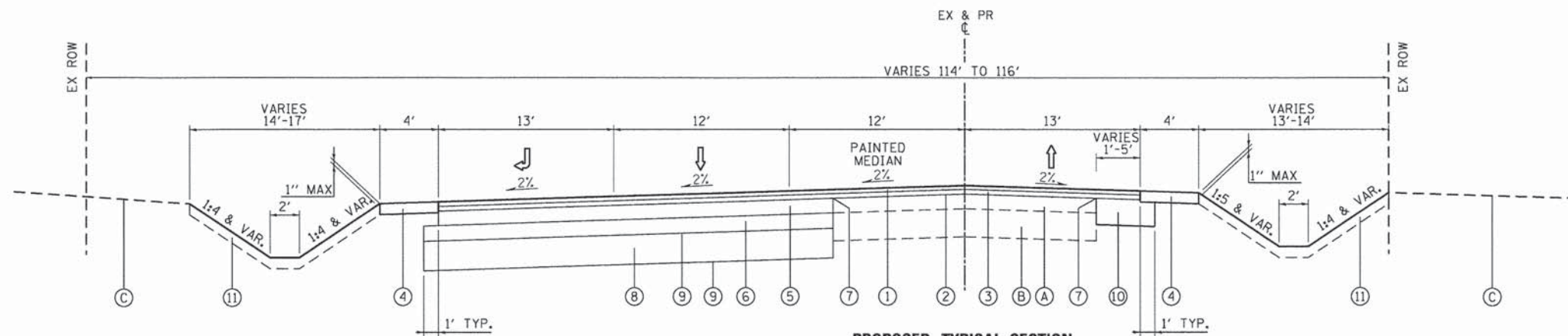
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**TYPICAL SECTIONS  
 BULL VALLEY ROAD**

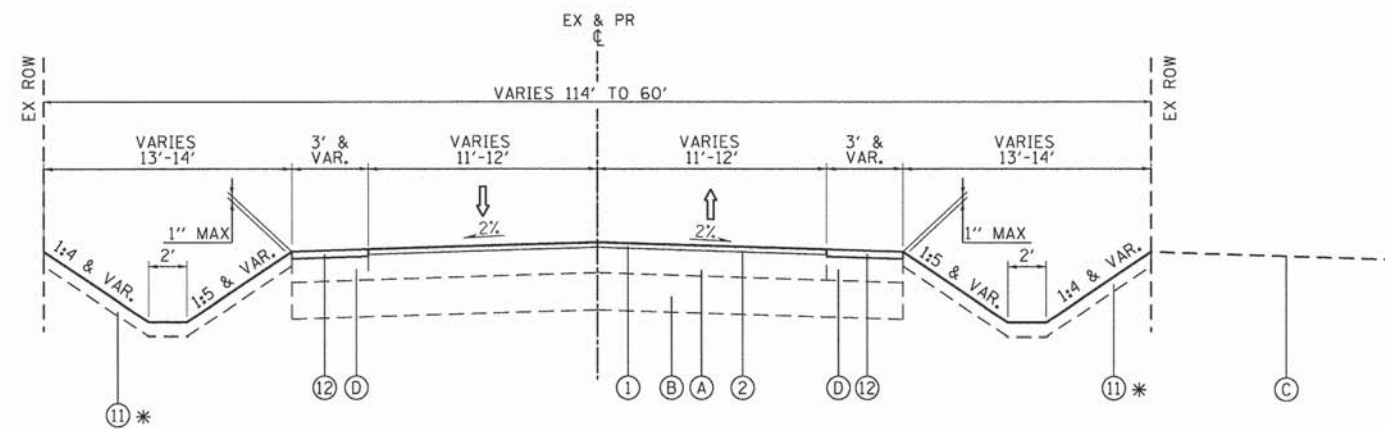
SCALE: NTS SHEET NO. 1 OF 2 SHEETS STA. BEG TO STA. END

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00078-00-CH		MCHENRY	59	8
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



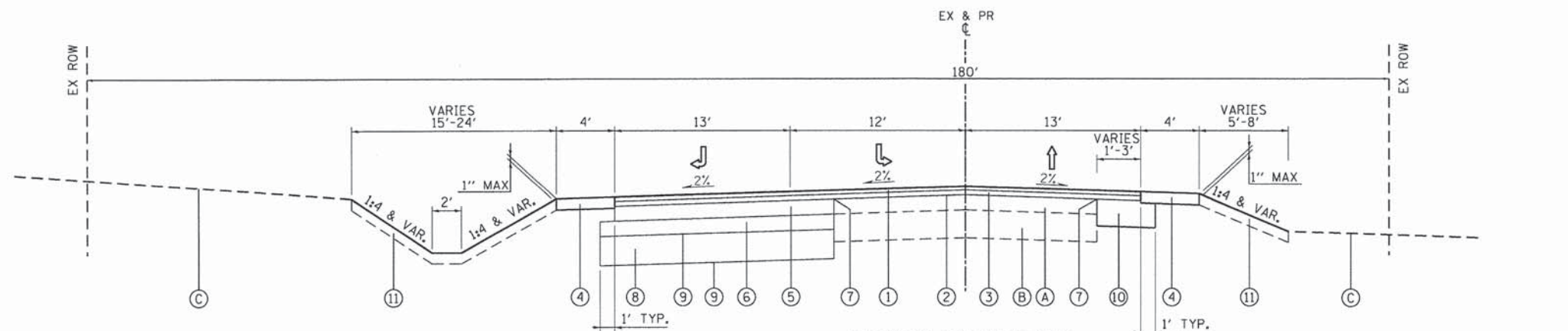


**PROPOSED TYPICAL SECTION**  
BULL VALLEY ROAD  
STA. 110+81.77 TO STA. 118+35.38



**PROPOSED TYPICAL SECTION**  
BULL VALLEY ROAD  
STA. 74+78.69 TO STA. 103+23.58  
STA. 118+35.38 TO STA. 120+63.27

\* STA. 118+35.38 TO STA. 120+63.27



**PROPOSED TYPICAL SECTION**  
CURRAN ROAD  
STA. 200+00.00 TO STA. 205+16.64

**EXISTING LEGEND**

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT; 7"-13"
- (B) EXISTING AGGREGATE BASE COURSE; 9"-16"
- (C) EXISTING GROUND
- (D) EXISTING AGGREGATE SHOULDER
- (E) HOT-MIX ASPHALT SURFACE REMOVAL; 3 1/2"
- (F) HOT-MIX ASPHALT SURFACE REMOVAL; 2"

**PROPOSED LEGEND**

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70; 2"
- (2) BITUMINOUS MATERIALS (TACK COAT)
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/4"
- (4) AGGREGATE SHOULDERS, TYPE B, 6" (9% MAX SLOPE) (CA-6 CRUSHED GRAVEL OR CRUSHED STONE)
- (5) HOT-MIX ASPHALT BASE COURSE; 6"
- (6) SUB-BASE GRANULAR MATERIAL, TYPE B 6" (SQ YD)
- (7) STRIP REFLECTIVE CRACK CONTROL
- (8) AGGREGATE SUBGRADE IMPROVEMENT (CU YD) (AS DIRECTED BY THE ENGINEER)
- (9) TRIAXIAL GEOGRID REINFORCEMENT, TYPE I
- (10) PORTLAND CEMENT CONCRETE BASE COURSE WIDENING, 10" (SQ YD)
- (11) SEEDING, CLASS 2A TOPSOIL FURNISH AND PLACE, 6"
- (12) AGGREGATE SHOULDERS, TYPE B (9% MAX SLOPE) (CA-6 CRUSHED GRAVEL OR CRUSHED STONE)

COMPANY NAME: HRGreen  
PROJECT CONTACT: JSTENZ  
DATE PLOTTED: 1/25/2016 8:31:08 AM  
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	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
BULL VALLEY ROAD

SCALE: NTS SHEET NO. 2 OF 2 SHEETS STA. BEG TO STA. END

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	9
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**SCHEDULE OF QUANTITIES – EARTHWORK – BULL VALLEY ROAD**

BULL VALLEY ROAD	VOLUME (CU YD)				
	STATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT
103+50.00					
104+00.00	81.3	1.4	66.1	0.0	
104+50.00	83.9	0.9	75.8	0.0	
105+00.00	85.7	0.6	88.3	0.0	
105+50.00	87.8	0.6	88.3	0.0	
106+00.00	93.8	1.3	76.1	0.0	
106+50.00	102.4	4.9	69.6	0.0	
107+00.00	110.7	12.0	68.5	0.0	
107+50.00	117.0	19.4	66.3	0.0	
108+00.00	118.6	20.5	61.9	0.0	
108+50.00	128.6	14.9	53.5	12.2	
109+00.00	135.3	9.2	52.0	24.2	
109+50.00	172.9	21.4	54.4	23.9	
110+00.00	223.7	29.9	46.4	34.4	
110+50.00	310.5	15.3	52.4	90.6	
111+00.00	214.7	3.3	70.9	68.1	
111+50.00	125.1	1.1	100.2	35.6	
112+00.00	232.4	2.5	104.9	69.6	
112+50.00	239.8	2.6	95.1	68.6	
113+00.00	209.7	2.4	86.2	69.6	
113+50.00	197.4	2.3	62.4	70.5	
114+00.00	189.4	2.0	52.2	65.9	
114+50.00	169.0	1.7	49.4	54.6	
115+00.00	139.2	1.1	60.6	35.8	
115+50.00	111.5	1.2	67.7	20.0	
116+00.00	99.9	2.1	63.9	15.5	
116+50.00	95.7	3.1	64.4	13.4	
117+00.00	87.6	3.7	69.4	6.2	
117+50.00	80.6	3.7	68.3	0.0	
118+00.00	79.0	3.3	56.9	0.0	
118+50.00	76.5	1.5	43.2	0.0	
119+00.00	37.5	0.0	18.0	0.0	
119+50.00	7.3	0.0	6.9	0.0	
120+00.00	17.2	0.0	14.4	0.0	
TOTALS:	4261.9	190.0	2074.9	778.7	

TOTAL (CU YD)

ITEM NO.	DESCRIPTION	TOTAL (CU YD)
20200100	EARTH EXCAVATION	190.0
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	4261.9
20400800	FURNISHED EXCAVATION	1913.4
30300001	AGGREGATE SUBGRADE IMPROVEMENT	778.7

**SCHEDULE OF QUANTITIES – EARTHWORK – CURRAN ROAD**

CURRAN ROAD	VOLUME (CU YD)				
	STATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT
200+50.00					
201+00.00	801.3	66.1	233.1	261.2	
201+50.00	408.3	53.5	121.9	130.5	
202+00.00	241.8	29.8	77.9	75.9	
202+50.00	208.7	6.5	74.4	40.5	
203+00.00	211.7	8.9	68.9	36.1	
203+50.00	190.5	13.8	80.6	16.0	
204+00.00	169.7	15.4	90.1	0.0	
204+50.00	160.7	9.6	93.4	0.0	
205+00.00	149.7	4.4	102.0	0.0	
TOTALS:	2542.4	208.1	942.1	560.2	

TOTAL (CU YD)

ITEM NO.	DESCRIPTION	TOTAL (CU YD)
20200100	EARTH EXCAVATION	208.1
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	2542.4
20400800	FURNISHED EXCAVATION	765.3
30300001	AGGREGATE SUBGRADE IMPROVEMENT	560.2

**EARTHWORK SUMMARY**

EARTHWORK SUMMARY

ITEM NO.	PAY ITEM	DESCRIPTION	CROSS SECTION BASED		TOTAL (CU YD)
			BULL VALLEY ROAD (CU YD)	CURRAN ROAD (CU YD)	
1	20200100	EARTH EXCAVATION	190.0	208.1	399
2	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	4,261.9	2,542.4	6,805
3	20400800	FURNISHED EXCAVATION	1,913.4	765.3	2,679
4	30300001	AGGREGATE SUBGRADE IMPROVEMENT	778.7	560.2	1,339

COMPANY NAME: HRGreen  
 PROJECT CONTACT: #PROJECT\_CONTACT#  
 USER: #USER#  
 PLOT DATE: 1/25/2016 8:10:12 AM  
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 PEN TABLE: #PEN\_TABLE#



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PLOT SCALE = NTS	DRAWN - HMH	REVISED -
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	DATE - 1/25/16	REVISED -

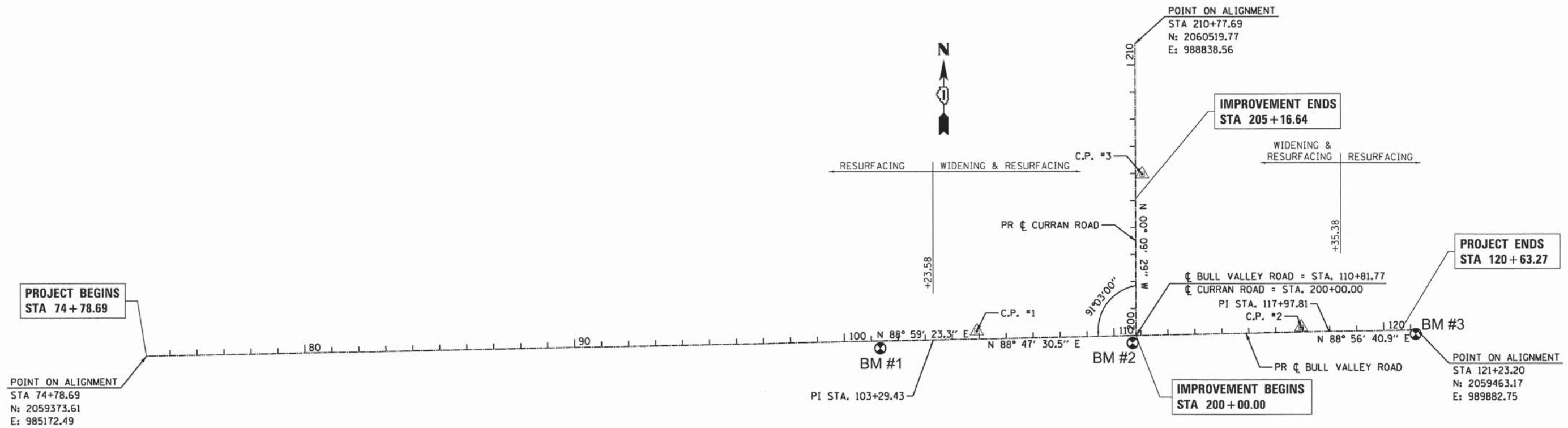
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES – EARTHWORK  
 BULL VALLEY ROAD**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	10
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



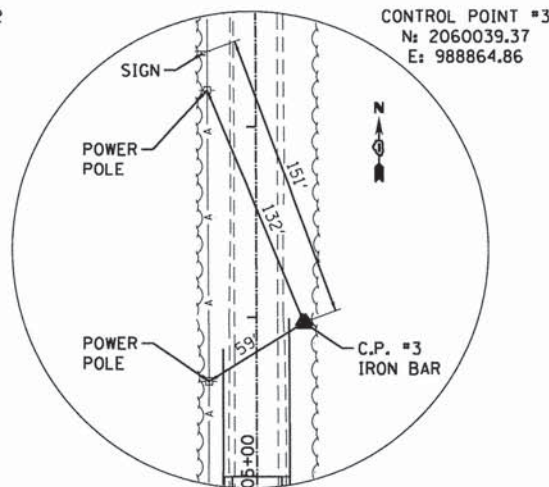
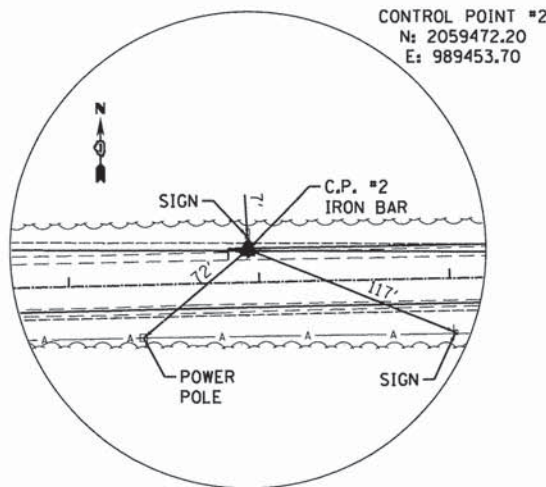
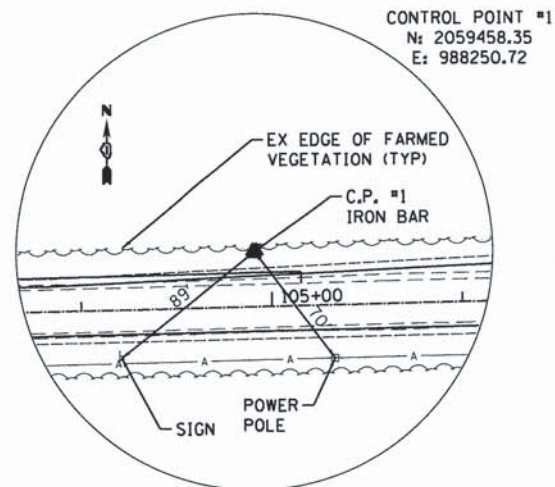


**BENCHMARKS**

BENCHMARK 1: RAILROAD SPIKE IN NORTH FACE OF 6TH POWERPOLE WEST OF CURRAN ROAD AND ON THE SOUTH SIDE OF BULL VALLEY ROAD. ELEVATION = 792.67 (NAVD 88)

BENCHMARK 2: RAILROAD SPIKE IN EAST FACE OF WOOD POWERPOLE LOCATED ON THE SOUTH SIDE OF BULL VALLEY ROAD ACROSS FROM CURRAN ROAD. ELEVATION = 784.94 (NAVD 88)

BENCHMARK 3: CHISELED SQUARE IN THE EAST END OF THE SOUTH CONCRETE WALL OF THE BULL VALLEY ROAD BRIDGE OVER BOONE CREEK. ELEVATION = 779.67 (NAVD 88)



COMPANY NAME: HRGreen  
 PROJECT CONTACT: #PROJECT CONTACT#  
 CLIENT: #CLIENT#  
 DATE PLOTTED: 1/25/2016 8:38:18 AM  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

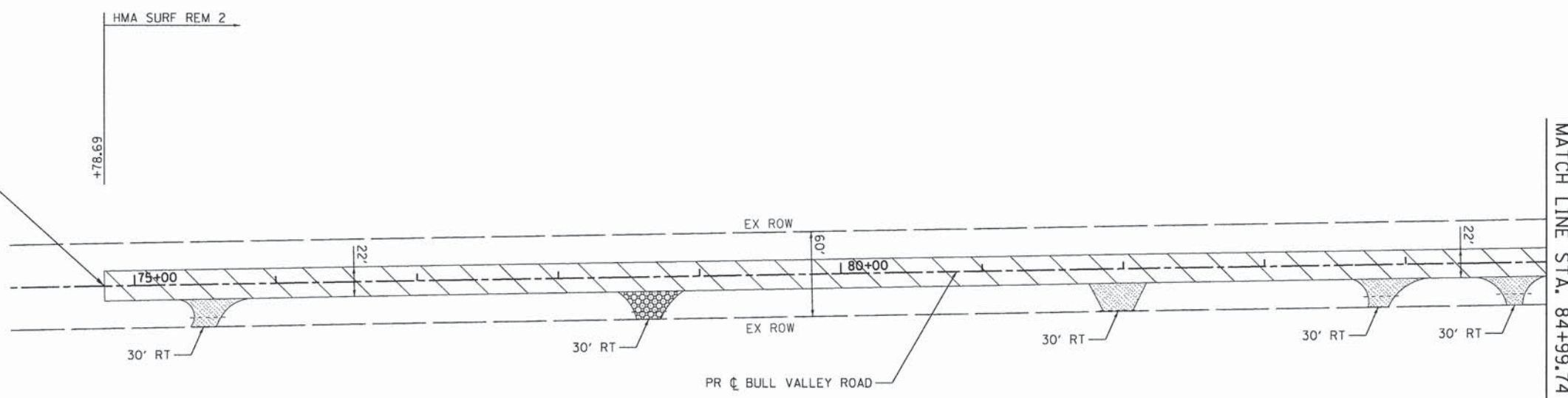
ALIGNMENT, TIES AND BENCHMARKS  
 BULL VALLEY ROAD

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	11
•FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PROJECT BEGINS  
STA. 74 + 78.69



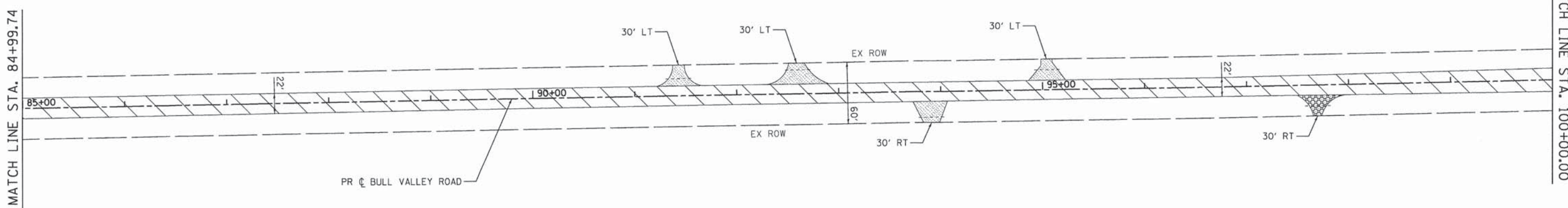
MATCH LINE STA. 84+99.74



**LEGEND:**

	HMA SURFACE REMOVAL
	PAVEMENT REMOVAL
	AGG SHOULDER REMOVAL (PAID AS EARTH EXCAVATION)
	DRIVEWAY PAVEMENT REMOVAL

NOTE:  
THERE WILL BE NO ADDITIONAL COMPENSATION TO THE CONTRACTOR FOR EXCAVATION AND DISPOSAL OF VEGETATION WITHIN THE ADJACENT FARMED AREAS. THIS WORK WILL BE INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS.



MATCH LINE STA. 100+00.00

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CLIENT: #CLIENT#  
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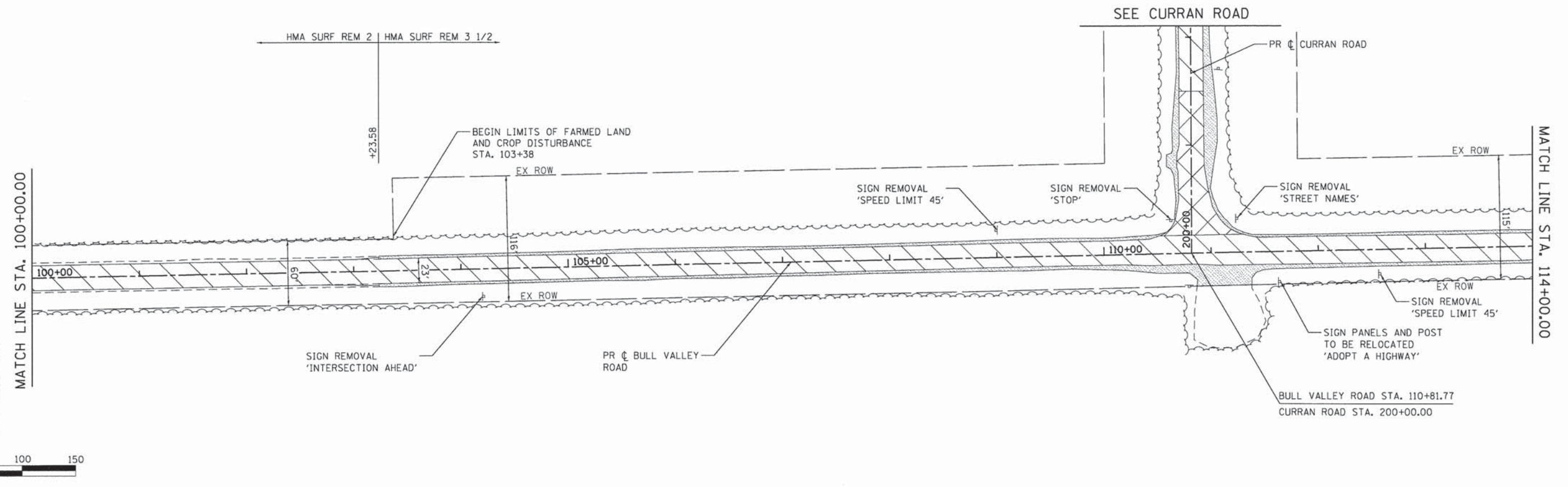
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PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY REMOVAL PLAN BULL VALLEY ROAD</b>			
SCALE: 1" = 50'	SHEET NO. 1 OF 3 SHEETS	STA. 74+78.69 TO STA. 100+00.00	

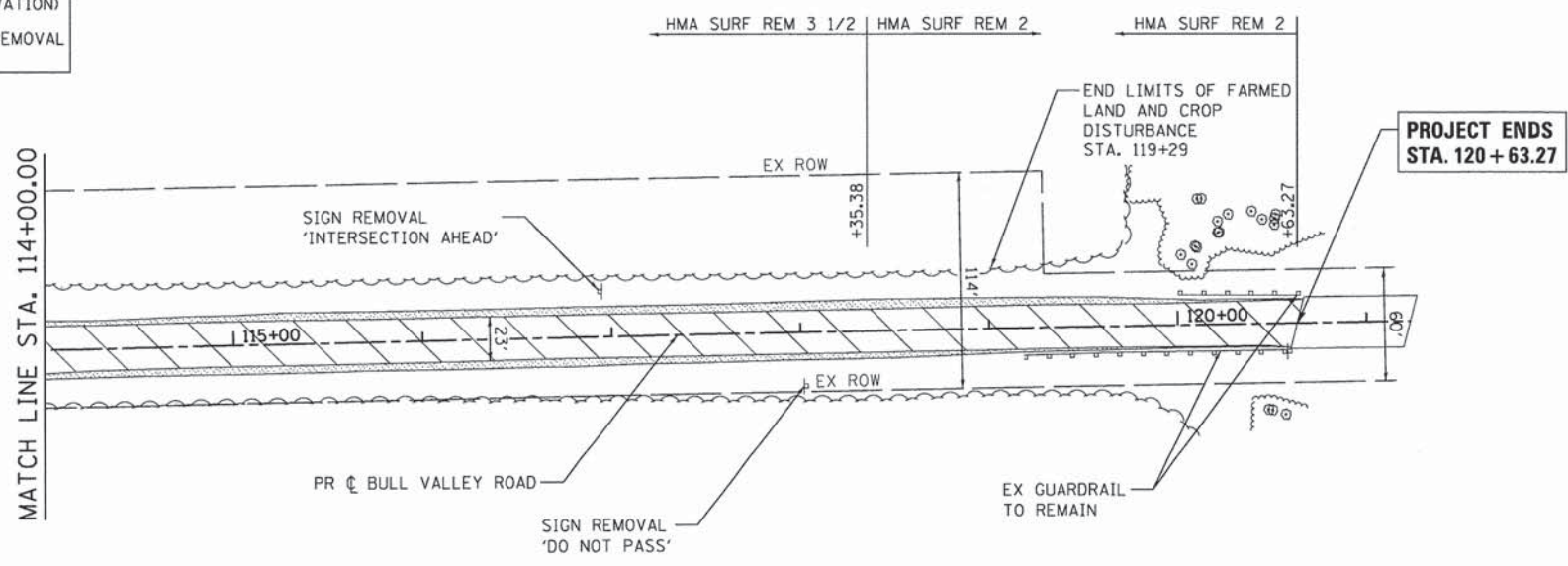
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	12
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		





**LEGEND:**

	HMA SURFACE REMOVAL
	PAVEMENT REMOVAL
	AGG SHOULDER REMOVAL (PAID AS EARTH EXCAVATION)
	DRIVEWAY PAVEMENT REMOVAL



**NOTE:**  
THERE WILL BE NO ADDITIONAL COMPENSATION TO THE CONTRACTOR FOR EXCAVATION AND DISPOSAL OF VEGETATION WITHIN THE ADJACENT FARMED AREAS. THIS WORK WILL BE INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: dstenz  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FILE NAME: 14-00078-00-CH-02.dgn  
 PLOT DATE: 1/25/2016  
 PEN TABLE: 11011008.tbl



USER NAME = dstenz	DESIGNED - JS	REVISED -
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	DATE - 1/25/16	REVISED -

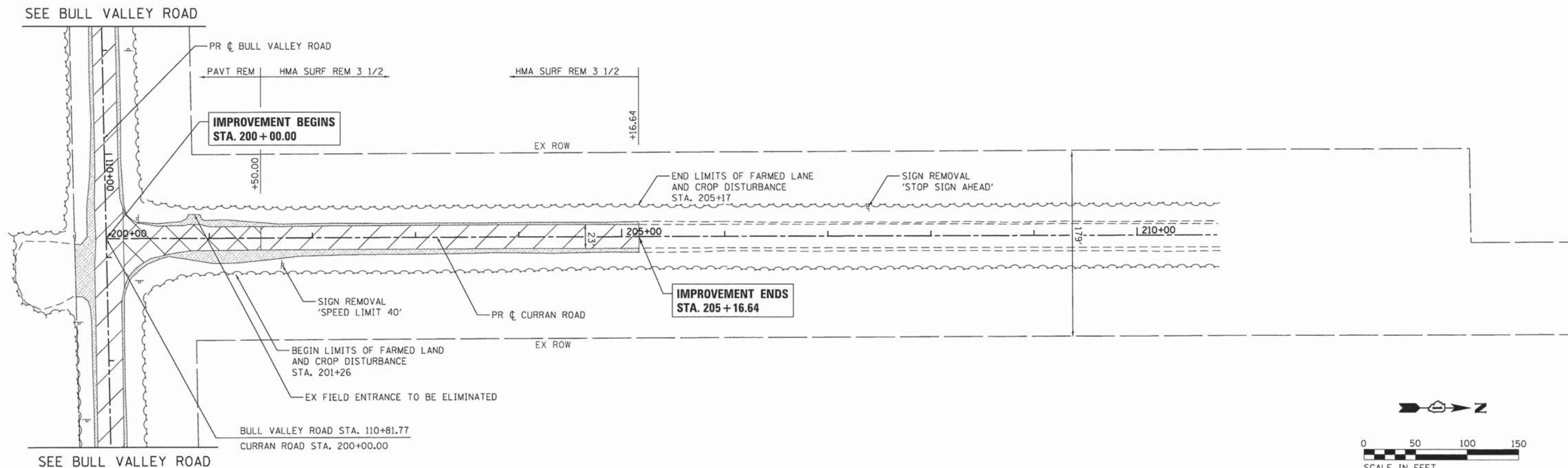
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY REMOVAL PLAN  
BULL VALLEY ROAD**

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 100+00.00 TO STA. 120+63.27

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	13
•FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		





**SEQUENCE OF CONSTRUCTION NOTE:**  
 THE FOLLOWING IS A SUGGESTED SEQUENCE OF CONSTRUCTION. THE CONTRACTOR WILL BE ALLOWED TO MODIFY THE PROPOSED SEQUENCE PROVIDED THE ROADWAY IS DRIVEABLE AND 2-WAY ACCESS IS AVAILABLE AT THE END OF EACH DAY.

THE CONTRACTOR SHALL COMPLETE PIPE CULVERT REMOVAL AND INSTALLATION AND PAVEMENT REMOVAL CONCURRENTLY AND LIMIT THEIR OPERATIONS TO ONE SIDE OF THE STREET.

THE CONTRACTOR SHALL USE FLAGGERS AND TYPE III BARRICADES TO SHIFT TRAFFIC TO ONE SIDE OF THE ROADWAY SO THAT CONSTRUCTION ACCESS TO COMPLETE THE WORK IS PROVIDED ON THE OPPOSITE SIDE OF THE STREET. THE CONTRACTOR SHALL REMOVE THE EXISTING PIPE CULVERT AND PAVEMENT AND CONSTRUCT THE PROPOSED CULVERTS, GEOGRID AND GRANULAR SUBBASE. THE GRANULAR SUBBASE WILL BE CONSTRUCTED UP TO THE EXISTING PAVEMENT ELEVATION AND USED AS A TEMPORARY PAVEMENT SURFACE UNTIL THE HMA BASE COURSE CAN BE CONSTRUCTED. THE ENDS OF THE PROPOSED CULVERTS SHALL BE CAPPED WITH 3/4" PLYWOOD.

THE CONTRACTOR SHALL THEN APPROPRIATELY ADJUST MAINTENANCE OF TRAFFIC AND CONSTRUCT THE OTHER SIDE OF THE ROADWAY AND CULVERTS TO BRING THE GRANULAR SUBBASE TO THE EXISTING PAVEMENT ELEVATION.

AGGREGATE USED FOR TEMPORARY ACCESS SHALL BE CONSTRUCTED AND PAID FOR SEPERATELY IN ACCORDANCE WITH SECTION 402 OF THE STANDARD SPECIFICATIONS.

HOT-MIX ASPHALT BASE COURSE, 6 INCH MUST BE CONSTRUCTED WITHIN 2 CALENDAR DAYS OF THE COMPLETION OF WORK ON ALL SEGMENTS AS SPECIFIED ABOVE. LIQUIDATED DAMAGES IN THE AMOUNT OF \$500 PER HOUR WILL BE ASSESSED FOR EACH HOUR BEYOND 9:00PM OF THE SECOND CALENDAR DAY.

**LEGEND:**

	HMA SURFACE REMOVAL
	PAVEMENT REMOVAL
	AGG SHOULDER REMOVAL (PAID AS EARTH EXCAVATION)
	DRIVEWAY PAVEMENT REMOVAL

**NOTE:**  
 THERE WILL BE NO ADDITIONAL COMPENSATION TO THE CONTRACTOR FOR EXCAVATION AND DISPOSAL OF VEGETATION WITHIN THE ADJACENT FARMED AREAS. THIS WORK WILL BE INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS.

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 PROJECT CONTACT: #PROJECT\_CONTACTS  
 CLIENT: #CLIENTS  
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	DRAWN - DS	REVISED -
PLOT SCALE = 1" = 50'	CHECKED -	REVISED -
PLOT DATE = 1/25/2016	DATE - 1/25/16	REVISED -

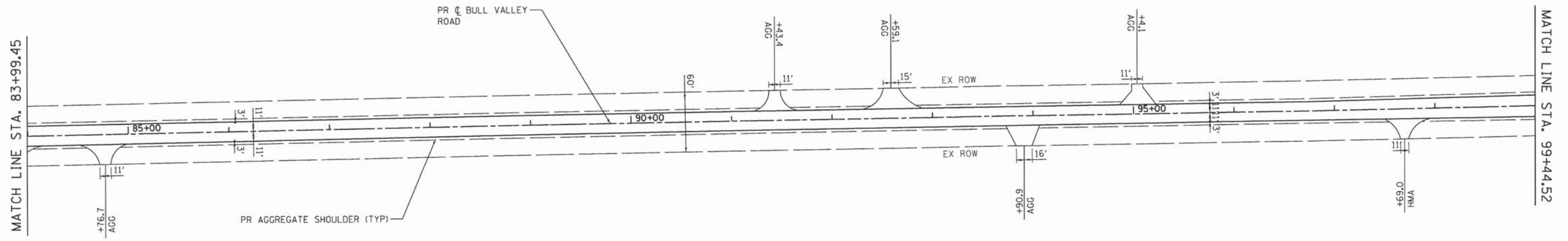
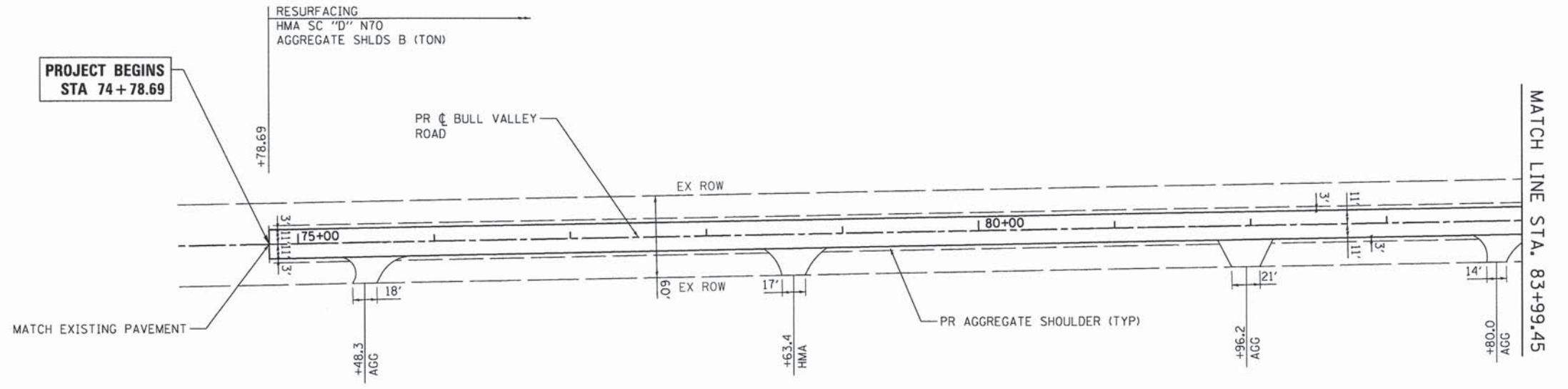
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>ROADWAY REMOVAL PLAN CURRAN ROAD</b>			
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS	STA. 200+00.00 TO STA. 205+16.64	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	14
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



NOTE:  
 WORK TO OVERLAY/RESURFACE EXISTING AGGREGATE FIELD  
 ENTRANCES SHALL BE PERFORMED IN ACCORDANCE WITH SECTIONS  
 402 AND 481 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL  
 BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR  
 AGGREGATE SHOULDERS, TYPE B.



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: #PROJECT\_CONTACT#  
 CLIENT: #CLIENT#  
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 PEN TABLE: plot.tbl



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PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN  
 BULL VALLEY ROAD

SCALE: 1" = 50' SHEET NO. 1 OF 4 SHEETS STA. 74+78.69 TO STA. 99+44.52

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	15
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



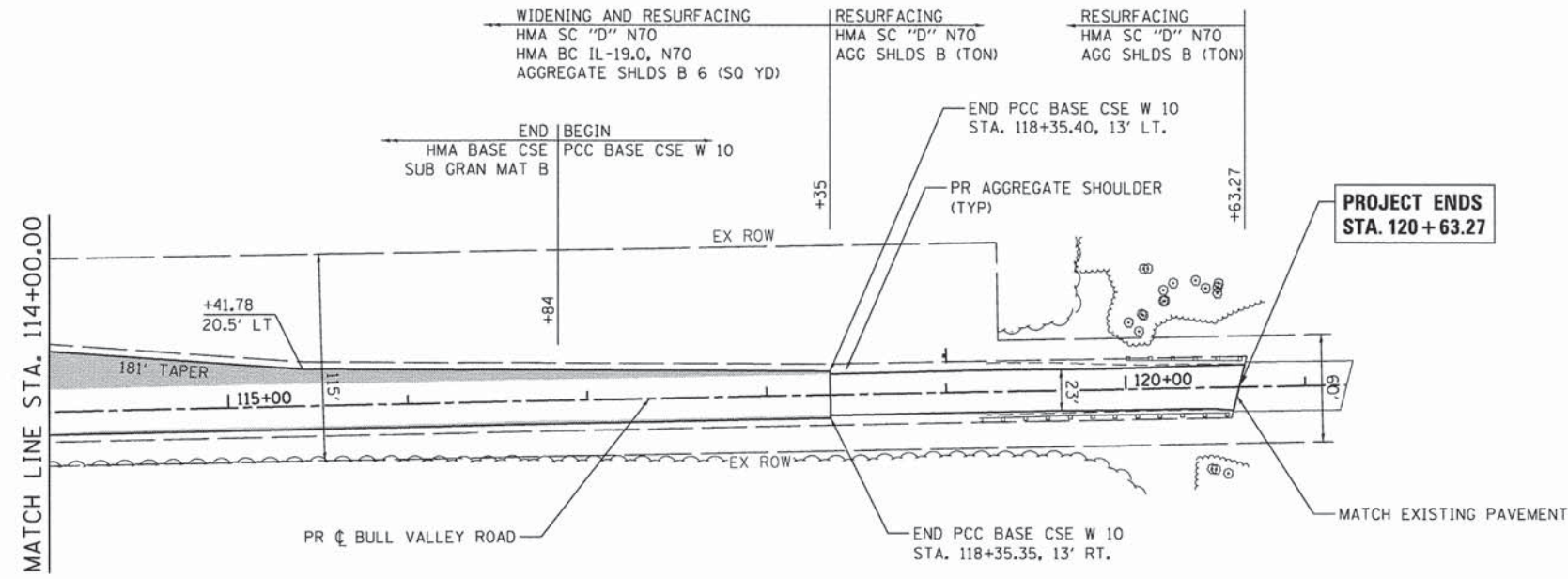




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	PLOTTED		
	CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		
	NO.		

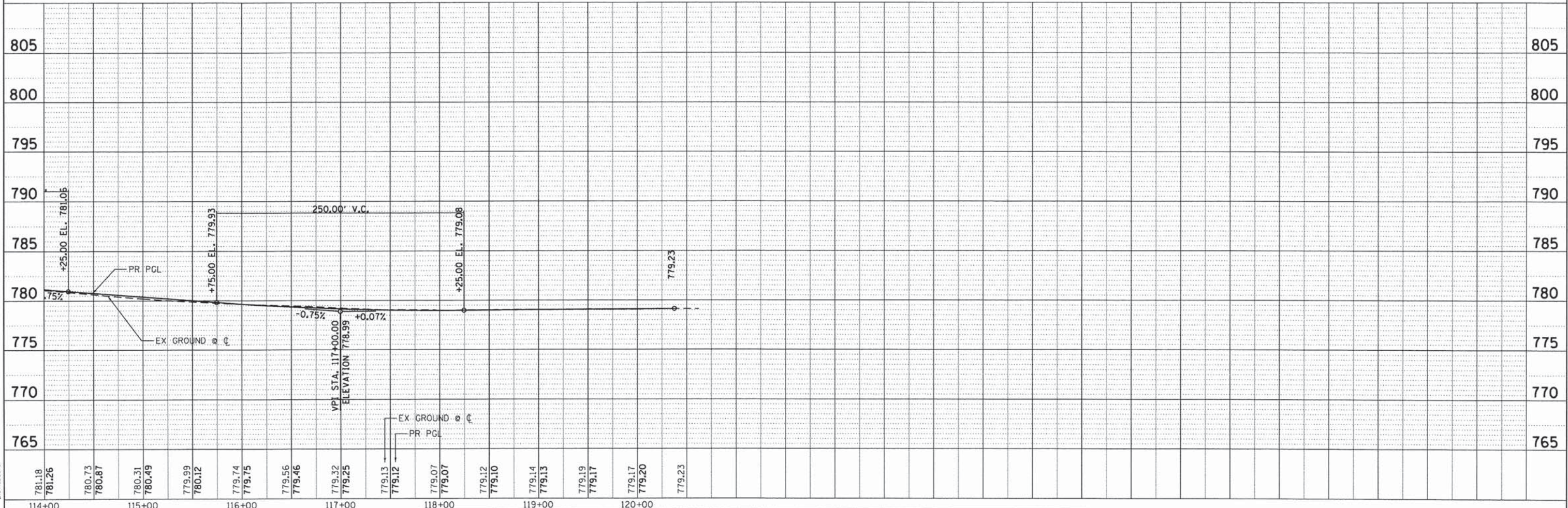
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	STRUCTURE NOTATIONS CHKD		
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LEGEND:

WIDENING AREA



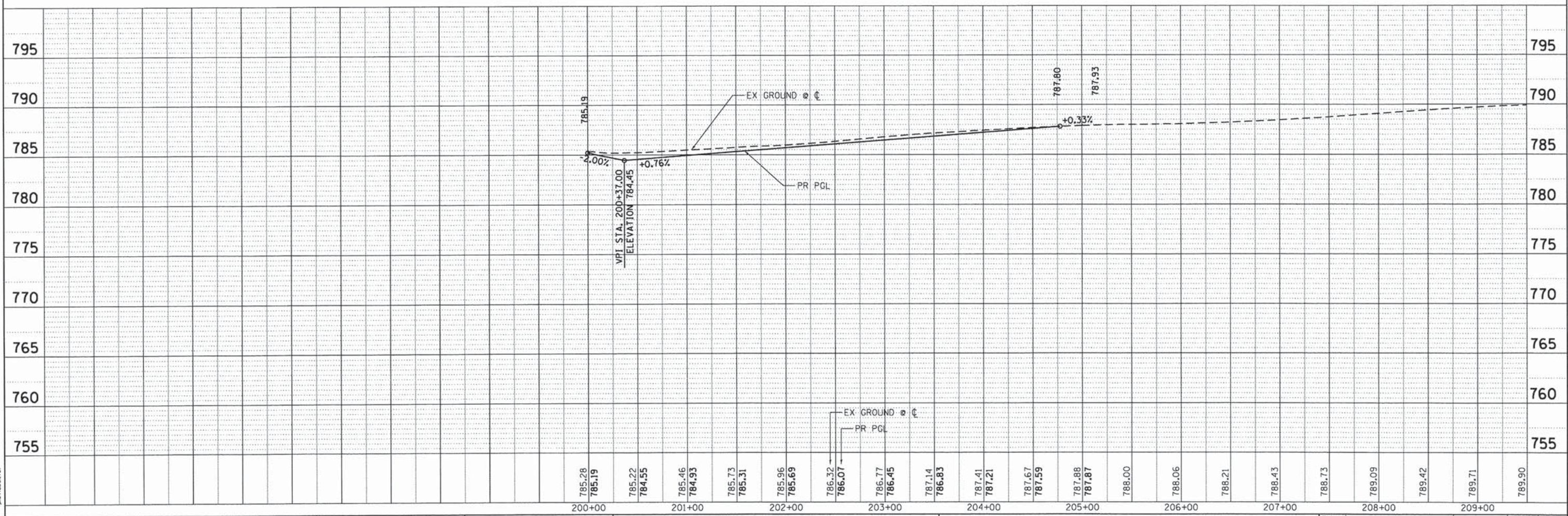
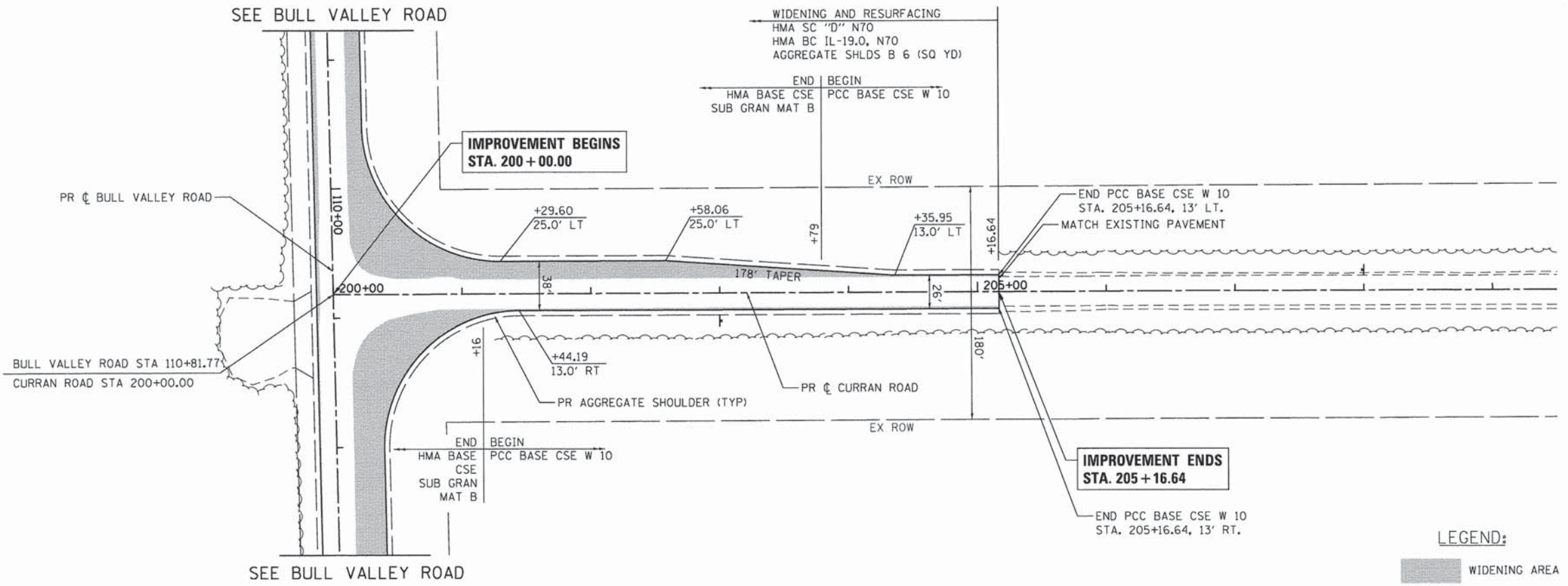
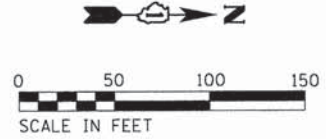
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DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	

DATE	
BY	
REVIEWED	
PLANNED	
NOTED	
NO.	

COMPANY NAME: HRGreen  
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

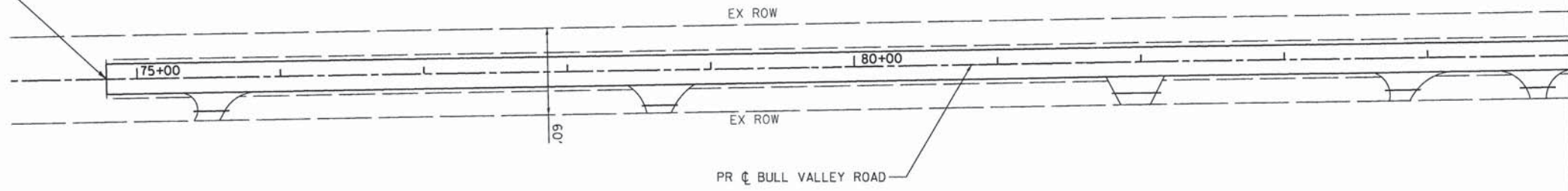
ROADWAY PLAN AND PROFILE  
 CURRAN ROAD  
 SCALE: 1" = 50'  
 SHEET NO. 4 OF 4 SHEETS  
 STA. 200+00.00 TO STA. 205+16.64

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	18
*FAU3860, FAU0084		CONTRACT NO. 61C45		
ILLINOIS FED. AID PROJECT				

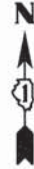


- LEGEND:**
-  INLET FILTERS (EA)
  -  PERIMETER EROSION BARRIER (FT)
  -  TEMPORARY DITCH CHECK (FT)
  -  TEMPORARY EROSION CONTROL SEEDING (LB)  
(APPLICATION RATE = 100 LB/ACRE)
  -  SEEDING, CLASS 2A (ACRE)
  -  TOPSOIL, FURNISH AND PLACE, 6"
  -  EROSION CONTROL BLANKET
  -  NITROGEN, PHOSPHOROUS, AND POTASSIUM FERTILIZER NUTRIENTS
  -  RIPRAP
  -  INLET AND PIPE PROTECTION (EA)

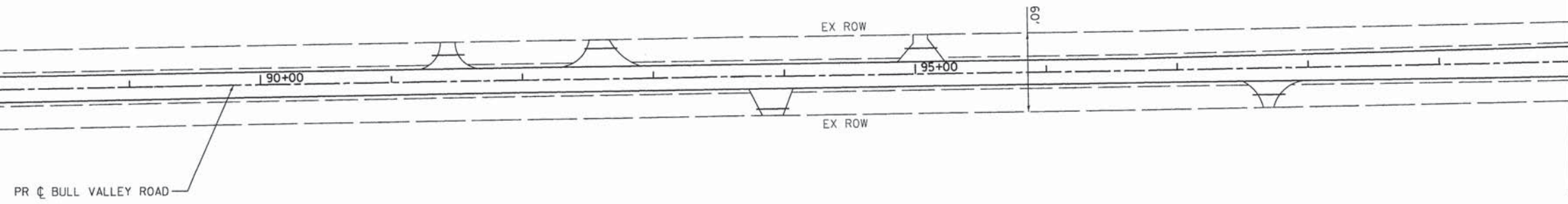
PROJECT BEGINS  
STA. 74 + 78.69



MATCH LINE STA. 84+99.74



MATCH LINE STA. 84+99.74



MATCH LINE STA. 100+00.00

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

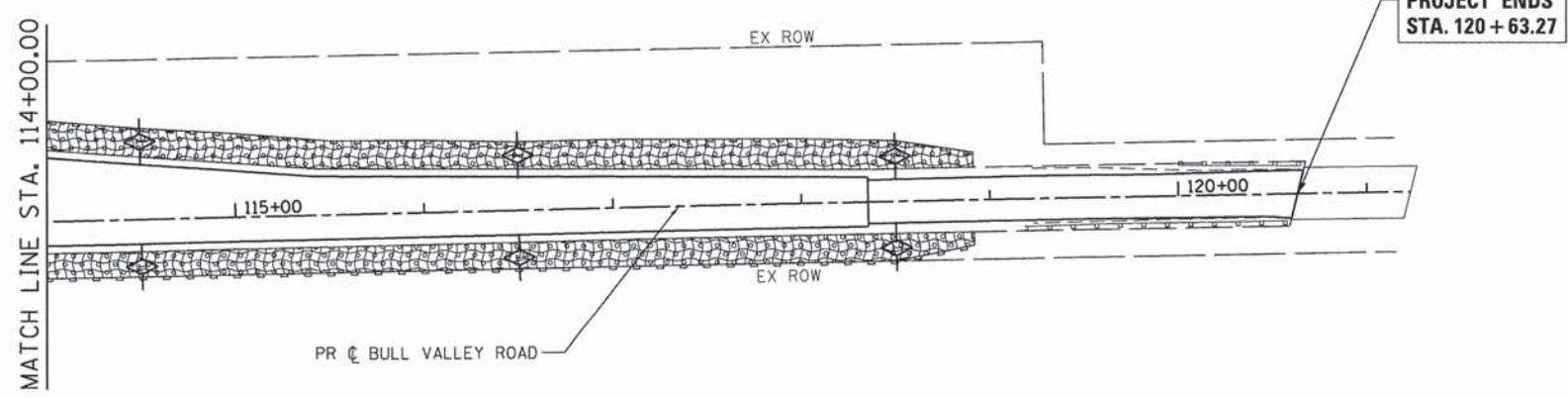
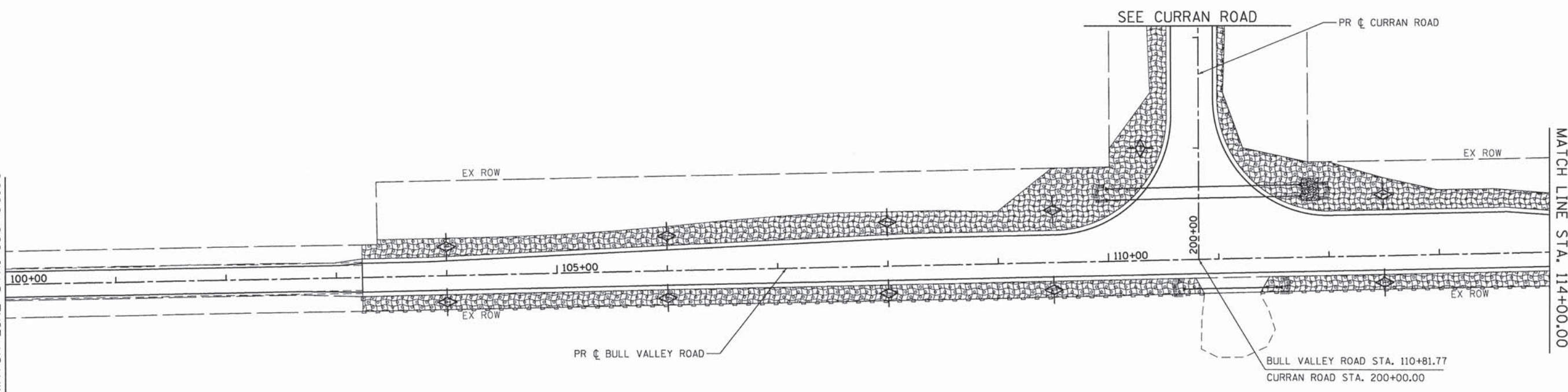
<b>EROSION CONTROL PLAN BULL VALLEY ROAD</b>			
SCALE: 1" = 50'	SHEET NO. 1 OF 3 SHEETS	STA. 74+78.69	TO STA. 100+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	19
*FAU3860, FAU0084			CONTRACT NO. 61C45	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



MATCH LINE STA. 100+00.00

MATCH LINE STA. 114+00.00



**LEGEND:**

- INLET FILTERS (EA)
- PERIMETER EROSION BARRIER (FT)
- TEMPORARY DITCH CHECK (FT)
- TEMPORARY EROSION CONTROL SEEDING (LB) (APPLICATION RATE = 100 LB/ACRE)
- SEEDING, CLASS 2A (ACRE)
- TOPSOIL, FURNISH AND PLACE, 6"
- EROSION CONTROL BLANKET
- NITROGEN, PHOSPHOROUS, AND POTASSIUM FERTILIZER NUTRIENTS
- RIPRAP
- INLET AND PIPE PROTECTION (EA)

COMPANY NAME: HRGreen  
 PROJECT CONTACT: #PROJECT\_CONTACT#  
 CLIENT: #CLIENT#  
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 PEN TABLE:



USER NAME = dstencz	DESIGNED - JS	REVISED -
	DRAWN - DS	REVISED -
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PLLOT DATE = 1/25/2016	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

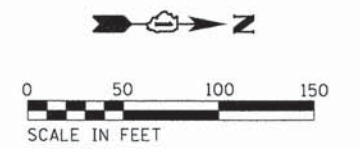
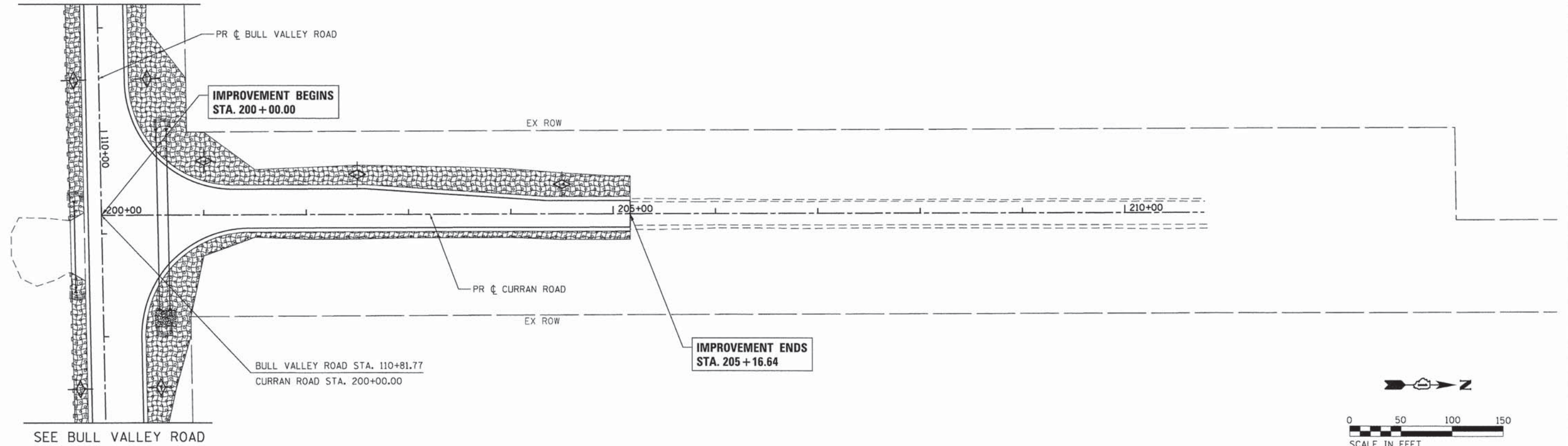
**EROSION CONTROL PLAN  
 BULL VALLEY ROAD**

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 100+00.00 TO STA. 120+63.27

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	20
*FAU3860, FAU0084		<b>CONTRACT NO. 61C45</b>		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



SEE BULL VALLEY ROAD



**LEGEND:**

- INLET FILTERS (EA)
- PERIMETER EROSION BARRIER (FT)
- TEMPORARY DITCH CHECK (FT)
- TEMPORARY EROSION CONTROL SEEDING (LB)  
(APPLICATION RATE = 100 LB/ACRE)
- SEEDING, CLASS 2A (ACRE)  
TOPSOIL, FURNISH AND PLACE, 6"  
EROSION CONTROL BLANKET  
NITROGEN, PHOSPHOROUS, AND  
POTASSIUM FERTILIZER NUTRIENTS
- RIPRAP
- INLET AND PIPE PROTECTION (EA)

COMPANY NAME: #COMPANY\_NAME  
 PROJECT CONTACT: #PROJECT\_CONTACT  
 DATE PLOTTED: 1/25/2016 8:32:20 AM  
 FILE NAME: 375-shr-eros-01.dgn  
 PLOT DRIVER: ILpdf\_bwhfcfg  
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HRGreen.com  
 Illinois Professional Design Firm  
 # 184-001322

USER NAME = dstencz	DESIGNED - JS	REVISED -
	DRAWN - DS	REVISED -
PLOT SCALE = 1" = 50'	CHECKED -	REVISED -
PLOT DATE = 1/25/2016	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

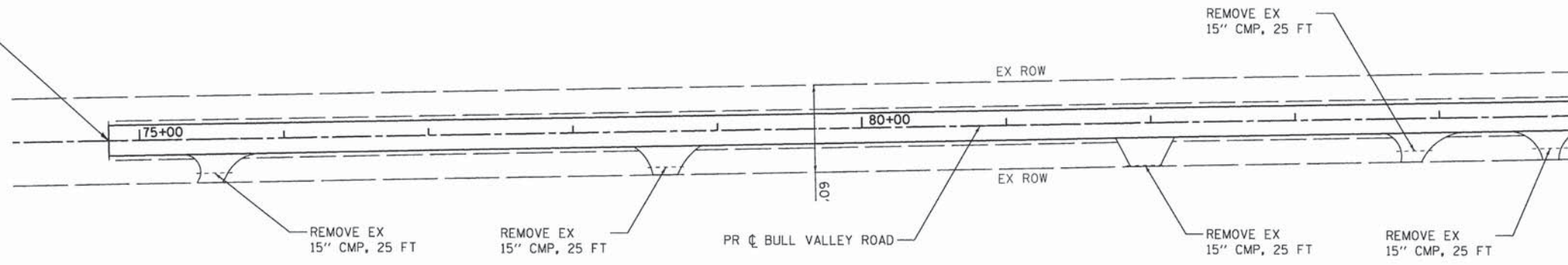
<b>EROSION CONTROL PLAN</b>	
<b>CURRAN ROAD</b>	
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS STA. 200+00.00 TO STA. 205+16.64

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	21
*FAU3860, FAU0084			<b>CONTRACT NO. 61C45</b>	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PROJECT BEGINS  
STA. 74 + 78.69

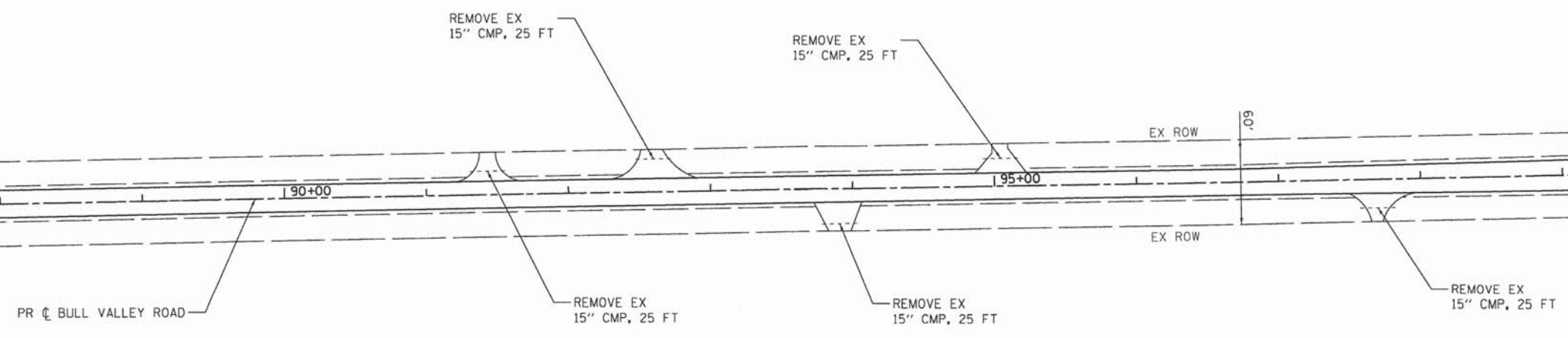
MATCH LINE STA. 84+99.74



NOTE:  
IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE MATERIALS TYPES AND SIZES OF THE EXISTING PIPE CULVERTS TO BE REMOVED. THE CONTRACTOR WILL BE PAID FOR PER FOOT FOR PIPE CULVERT REMOVAL REGARDLESS OF THE SIZE, TYPE OR DEPTH OF THE PIPE CULVERTS TO BE REMOVED.

MATCH LINE STA. 84+99.74

MATCH LINE STA. 100+00.00



COMPANY NAME: HRGreen.com  
PROJECT CONTACT: #PROJECTCONTACT#  
CLIENT: #CLIENT#  
DATE PLOTTED: 1/25/2016 8:29:25 AM  
FILE NAME: 175-001-00-00-00.dgn  
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	DRAWN - HMH	REVISED -
PLOT SCALE = 1" = 50'	CHECKED -	REVISED -
PLOT DATE = 1/25/2016	DATE - 1/25/16	REVISED -

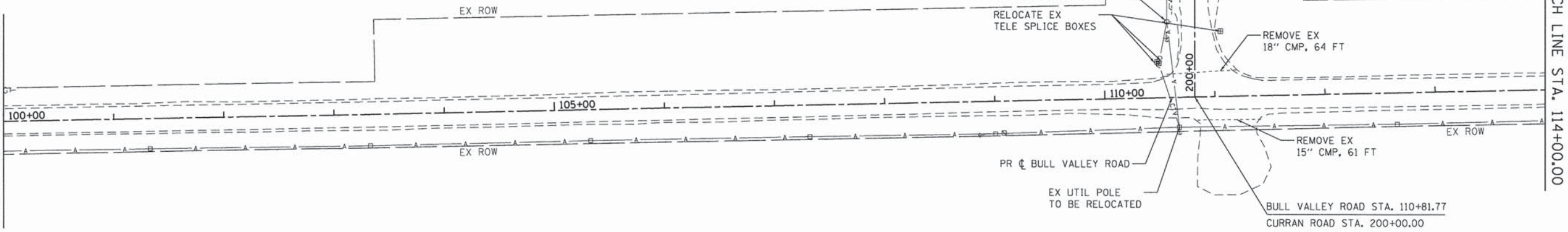
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY REMOVAL PLAN			
BULL VALLEY ROAD			
SCALE: 1" = 50'	SHEET NO. 1 OF 3 SHEETS	STA. 74+78.69 TO STA. 100+00.00	

F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	22
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

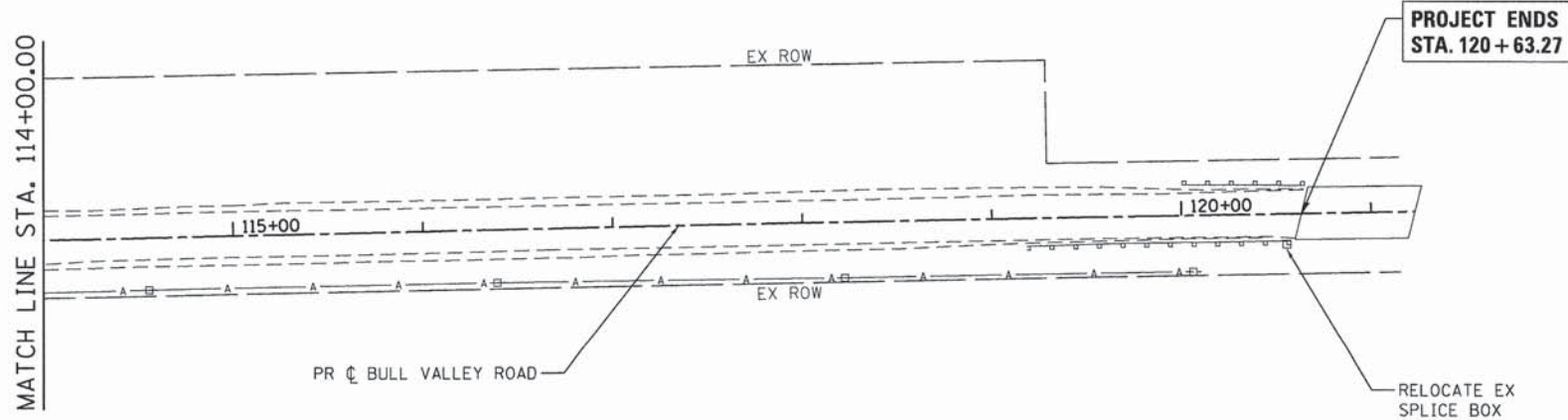


MATCH LINE STA. 100+00.00



MATCH LINE STA. 114+00.00

NOTE:  
IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE MATERIALS TYPES AND SIZES OF THE EXISTING PIPE CULVERTS TO BE REMOVED. THE CONTRACTOR WILL BE PAID FOR PER FOOT FOR PIPE CULVERT REMOVAL REGARDLESS OF THE SIZE, TYPE OR DEPTH OF THE PIPE CULVERTS TO BE REMOVED.



COMPANY NAME: HRGreen  
PROJECT CONTACT: #CLERK1  
DATE PLOTTED: 1/25/2016 8:42:29 AM  
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PLOT SCALE = 1" = 50'	DRAWN - DS	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY REMOVAL PLAN  
BULL VALLEY ROAD

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 100+00.00 TO STA. 120+63.27

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	23
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



SEE BULL VALLEY ROAD

PR  $\phi$  BULL VALLEY ROAD

IMPROVEMENT BEGINS  
STA. 200 + 00.00

EX ROW

RELOCATE EX  
UTIL POLE

RELOCATE EX  
UTIL POLE

200+00

205+00

210+00

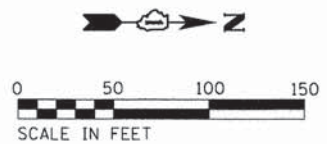
IMPROVEMENT ENDS  
STA. 205 + 16.64

PR  $\phi$  CURRAN ROAD

EX ROW

BULL VALLEY ROAD STA. 110+81.77  
CURRAN ROAD STA. 200+00.00

SEE BULL VALLEY ROAD



NOTE:  
IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE MATERIALS, TYPES AND SIZES OF THE EXISTING PIPE CULVERTS TO BE REMOVED. THE CONTRACTOR WILL BE PAID FOR PER FOOT FOR PIPE CULVERT REMOVAL REGARDLESS OF THE SIZE, TYPE OR DEPTH OF THE PIPE CULVERTS TO BE REMOVED.

COMPANY NAME: #COMPANY NAME  
PROJECT CONTACT: #PROJECT CONTACT#  
CLIENT: #CLIENT#  
DATE PLOTTED: 1/25/2016 8:22:33 AM  
FILE NAME: 375-sh1-dr-oh-rem-03.dgn  
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PEN TABLE: plottbl.tbl



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PLOT SCALE = 1" = 50'	DRAWN - DS	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

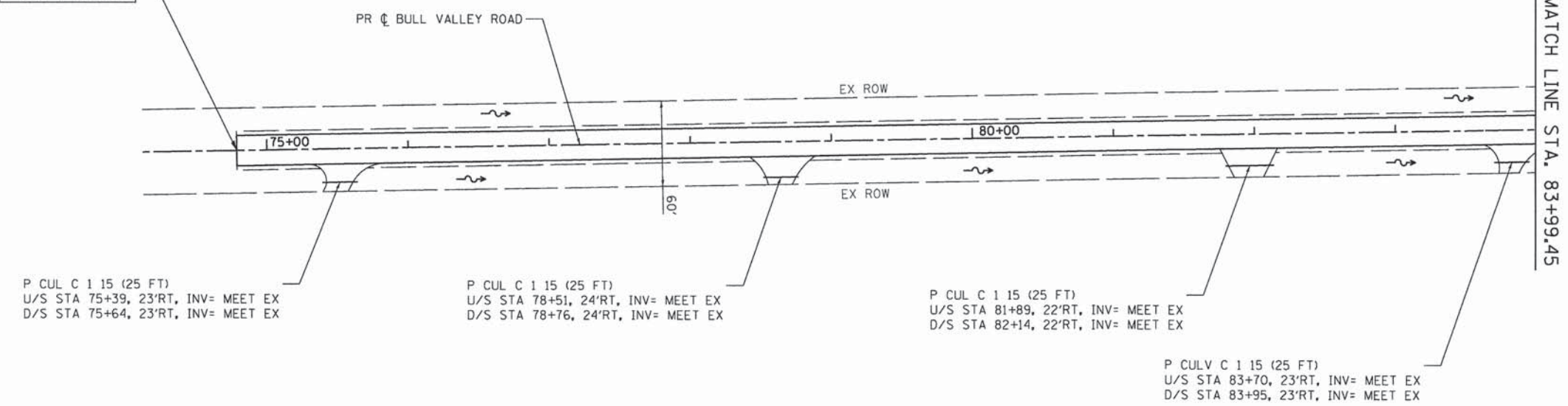
DRAINAGE & UTILITY REMOVAL PLAN  
CURRAN ROAD

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. 200+00.00 TO STA. 205+16.64

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	24
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



PROJECT BEGINS  
STA 74+78.69

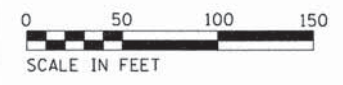


P CUL C 1 15 (25 FT)  
U/S STA 75+39, 23'RT, INV= MEET EX  
D/S STA 75+64, 23'RT, INV= MEET EX

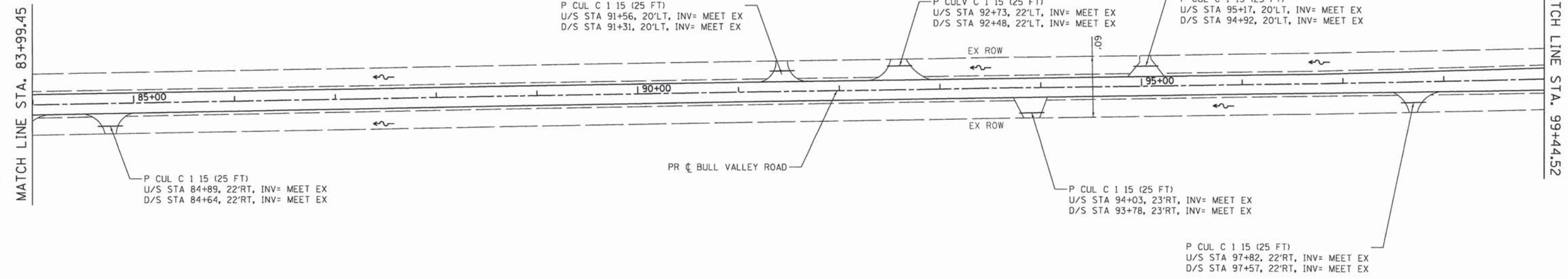
P CUL C 1 15 (25 FT)  
U/S STA 78+51, 24'RT, INV= MEET EX  
D/S STA 78+76, 24'RT, INV= MEET EX

P CUL C 1 15 (25 FT)  
U/S STA 81+89, 22'RT, INV= MEET EX  
D/S STA 82+14, 22'RT, INV= MEET EX

P CULV C 1 15 (25 FT)  
U/S STA 83+70, 23'RT, INV= MEET EX  
D/S STA 83+95, 23'RT, INV= MEET EX



MATCH LINE STA. 83+99.45



P CUL C 1 15 (25 FT)  
U/S STA 91+56, 20'LT, INV= MEET EX  
D/S STA 91+31, 20'LT, INV= MEET EX

P CULV C 1 15 (25 FT)  
U/S STA 92+73, 22'LT, INV= MEET EX  
D/S STA 92+48, 22'LT, INV= MEET EX

P CUL C 1 15 (25 FT)  
U/S STA 95+17, 20'LT, INV= MEET EX  
D/S STA 94+92, 20'LT, INV= MEET EX

P CUL C 1 15 (25 FT)  
U/S STA 84+89, 22'RT, INV= MEET EX  
D/S STA 84+64, 22'RT, INV= MEET EX

P CUL C 1 15 (25 FT)  
U/S STA 94+03, 23'RT, INV= MEET EX  
D/S STA 93+78, 23'RT, INV= MEET EX

P CUL C 1 15 (25 FT)  
U/S STA 97+82, 22'RT, INV= MEET EX  
D/S STA 97+57, 22'RT, INV= MEET EX

MATCH LINE STA. 83+99.45

MATCH LINE STA. 99+44.52

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: #184-001322  
DATE PLOTTED: 1/25/2016 8:32:37 AM  
FILE NAME: I:\proj\bw\hcrfg  
PLOT DRIVER: plot.tbl



USER NAME = dstencz	DESIGNED - JS	REVISED -
PLOT SCALE = 1" = 50'	DRAWN - HMH	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRAINAGE & UTILITY PLAN AND PROFILE	
BULL VALLEY ROAD	
SCALE: 1" = 50'	SHEET NO. 1 OF 4 SHEETS
STA. 74+78.69	TO STA. 99+44.52

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	25
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		







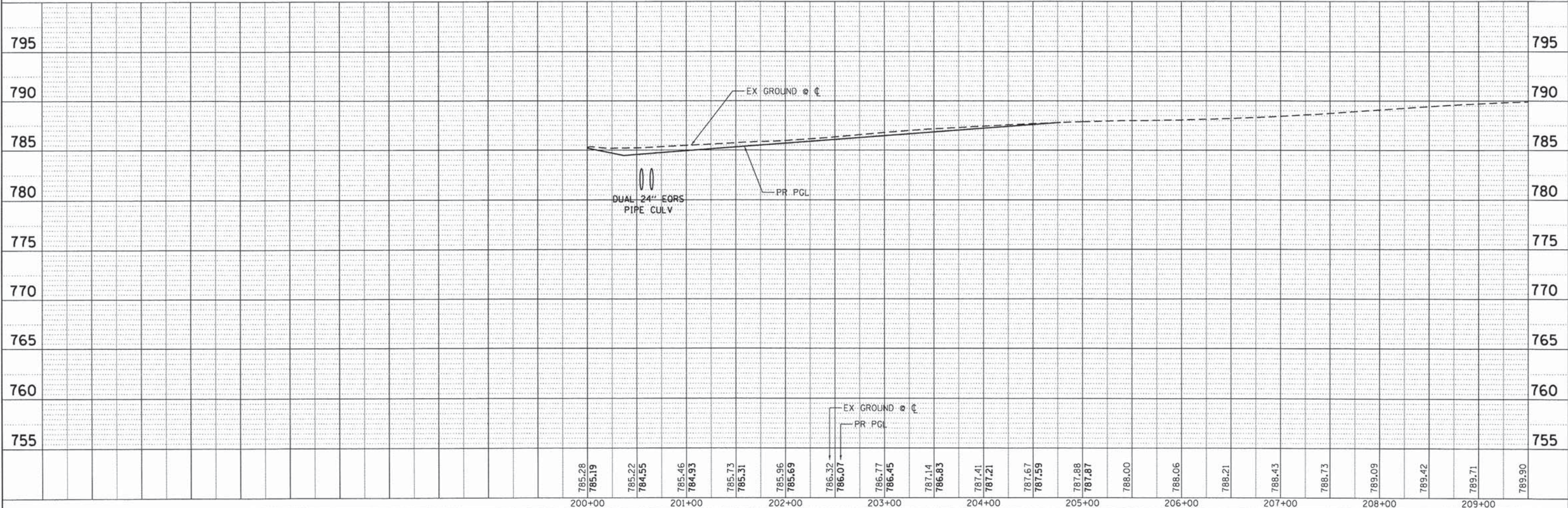
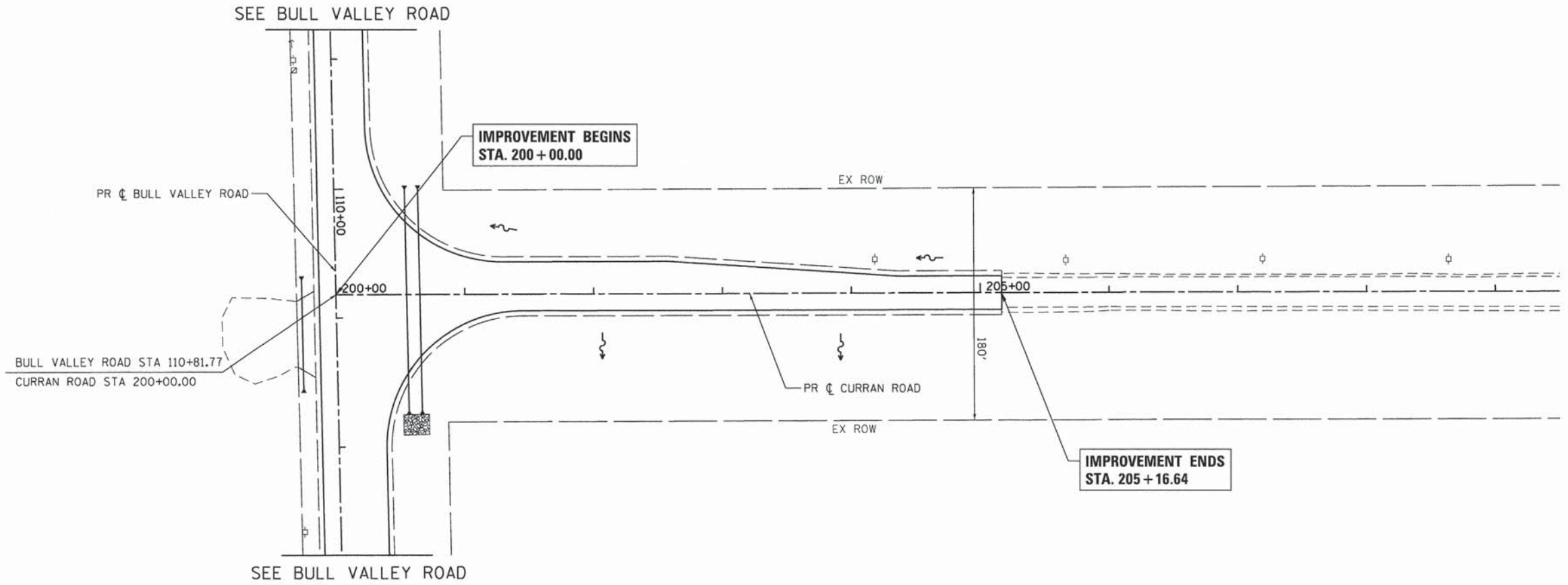
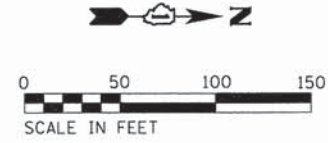




PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	REVISIONS		
	NO. OF WAYS CHECKED		
	CADD FILE NAME		
	NO.		

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NO.		

COMPANY NAME: HRGreen  
 PROJECT CONTACT: #PROJECT\_CONTACT#  
 CLIENT: #CLIENT#  
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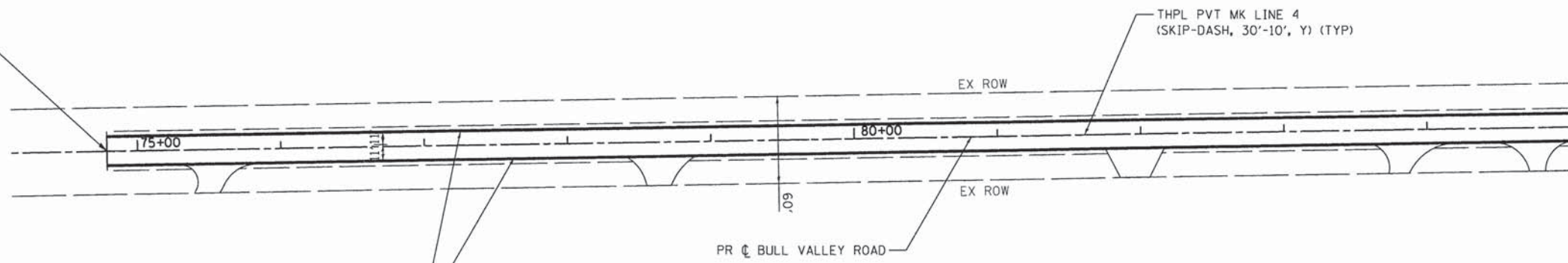


HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = dstancz FILE NAME = 375-shr-drain-B4.dgn PLOT SCALE = 1" = 50' PLOT DATE = 1/25/2016	DESIGNED - JS DRAWN - DS CHECKED - DATE - 1/25/16	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>DRAINAGE &amp; UTILITY PLAN AND PROFILE</b> <b>CURRAN ROAD</b>	SCALE: 1" = 50' SHEET NO. 4 OF 4 SHEETS STA. 200+00.00 TO STA. 205+16.64	<table border="1"> <tr> <th>F.A. RTE.</th> <th>SECTION</th> <th>COUNTY</th> <th>TOTAL SHEETS</th> <th>SHEET NO.</th> </tr> <tr> <td>14-00078-00-CH</td> <td></td> <td>MCHENRY</td> <td>59</td> <td>28</td> </tr> </table>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	14-00078-00-CH		MCHENRY	59	28	<table border="1"> <tr> <td>#FAU3860, FAU0084</td> <td>CONTRACT NO. 61C45</td> </tr> </table>	#FAU3860, FAU0084	CONTRACT NO. 61C45
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.																
14-00078-00-CH		MCHENRY	59	28																
#FAU3860, FAU0084	CONTRACT NO. 61C45																			



PROJECT BEGINS  
STA. 74+78.69

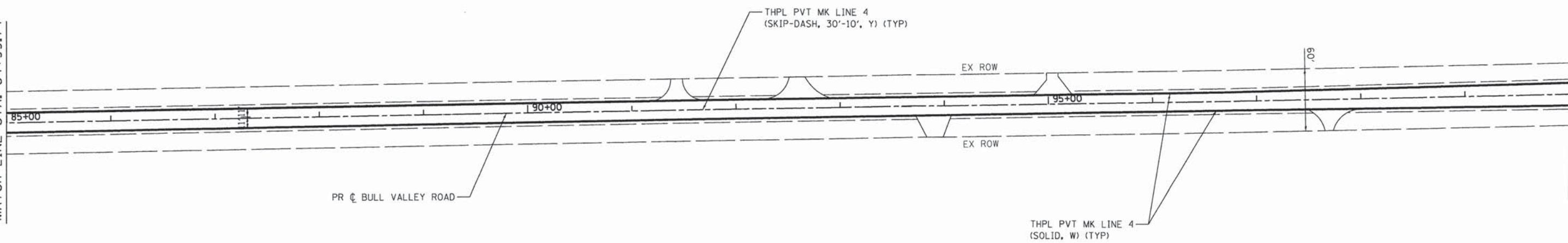
MATCH LINE STA. 84+99.74



NOTE:  
REFER TO DETAIL TC-11 FOR  
RAISED REFLECTIVE PAVEMENT  
MARKER LAYOUT AND SPACING

MATCH LINE STA. 84+99.74

MATCH LINE STA. 100+00.00



COMPANY NAME: HRGreen.com  
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PEN TABLE: plotabel.tbl



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PLOT SCALE = 1" = 50'	DRAWN - HMH	REVISED -
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	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

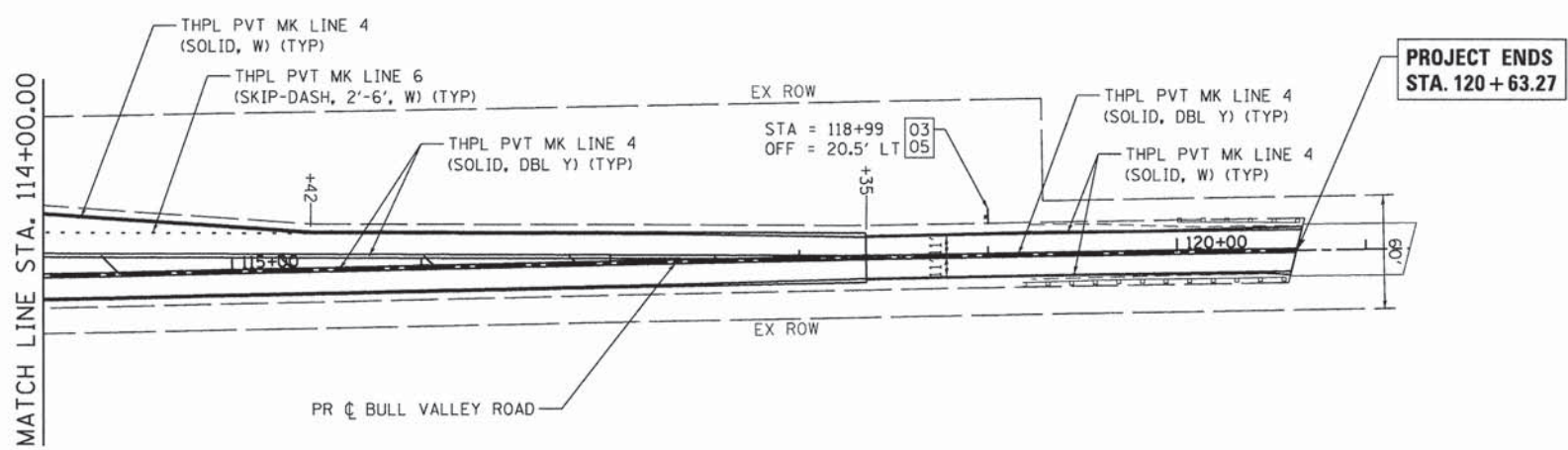
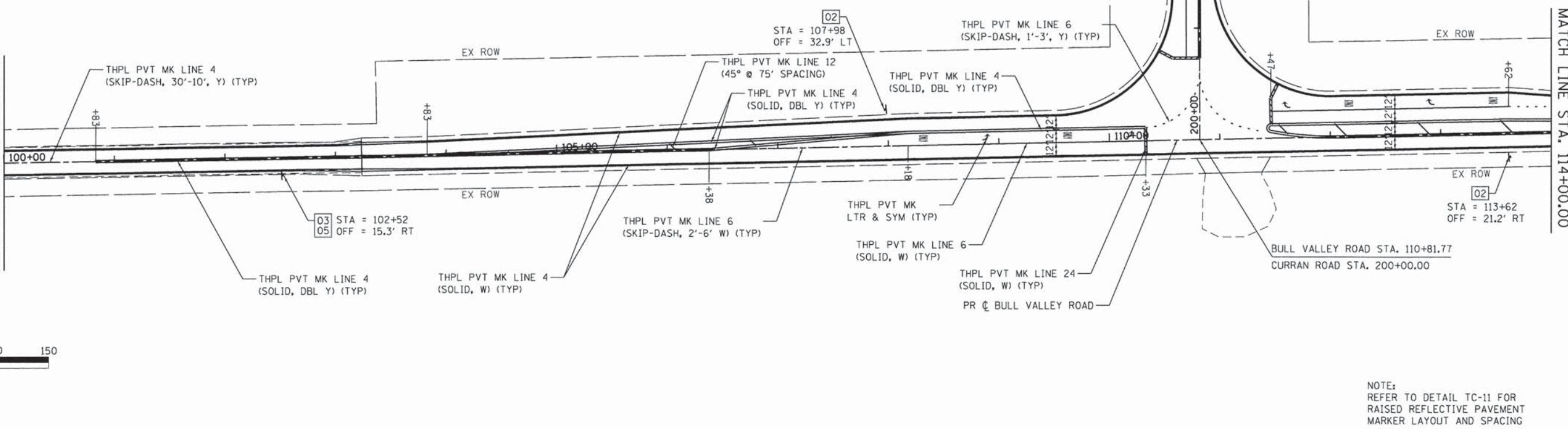
PAVEMENT MARKING & SIGNING PLAN  
BULL VALLEY ROAD

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. 74+78.69 TO STA. 100+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	29
•FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



MATCH LINE STA. 100+00.00



COMPANY NAME: HRGreen  
 PROJECT CONTACT: detancz  
 CLIENT: ILLINOIS  
 DATE PLOTTED: 1/25/2016 8:33:00 AM  
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PLOT SCALE = 1" = 50'	DRAWN - DS	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

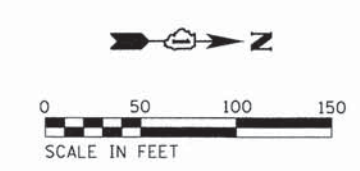
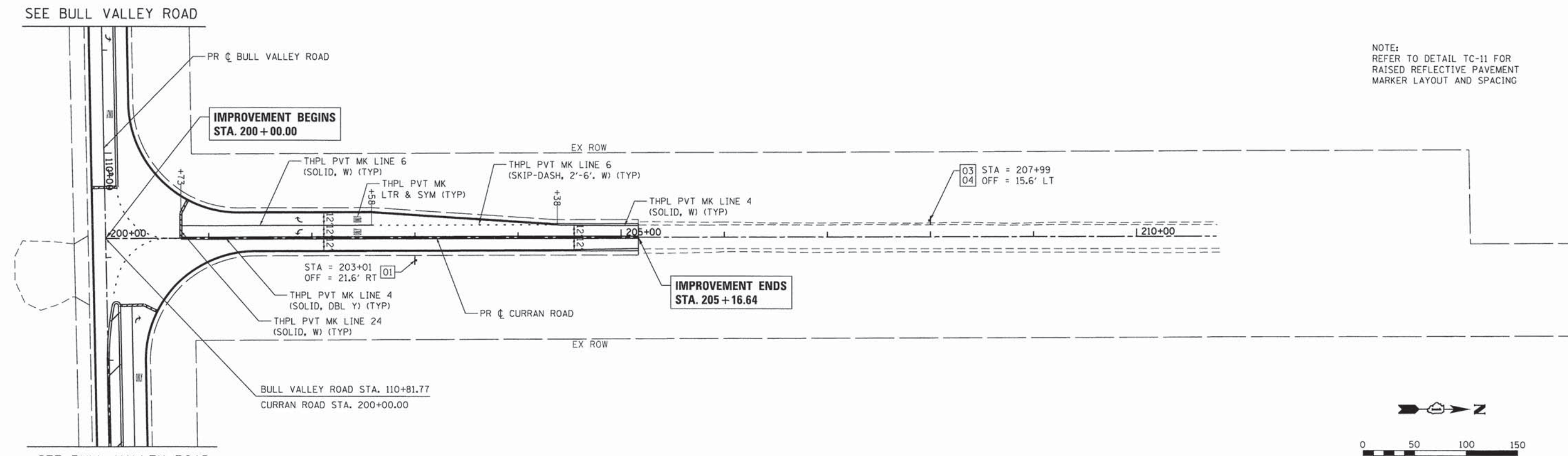
PAVEMENT MARKING & SIGNING PLAN  
BULL VALLEY ROAD

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 100+00.00 TO STA. 120+63.27

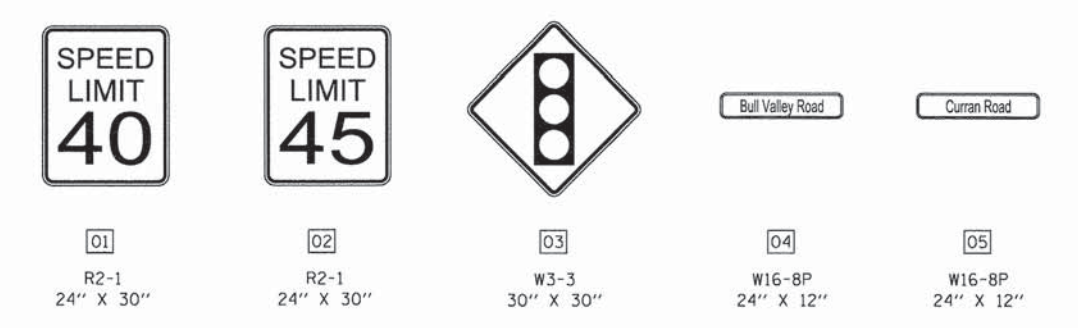
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00078-00-CH		MCHENRY	59	30
*FAU3860, FAU0084			CONTRACT NO. 61C45	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTE:  
REFER TO DETAIL TC-11 FOR  
RAISED REFLECTIVE PAVEMENT  
MARKER LAYOUT AND SPACING



**SIGN LEGEND**



COMPANY NAME: HRGreen  
 PROJECT CONTACT: [unreadable]  
 DATE PLOTTED: 1/25/2016 8:13:05 AM  
 FILE NAME: J:\5-spr-sme-03.dgn  
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**HRGreen.com**  
 Illinois Professional Design Firm  
 #184-001322

USER NAME = dstencz	DESIGNED - JS	REVISED -
PLOT SCALE = 1" = 50'	DRAWN - DS	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

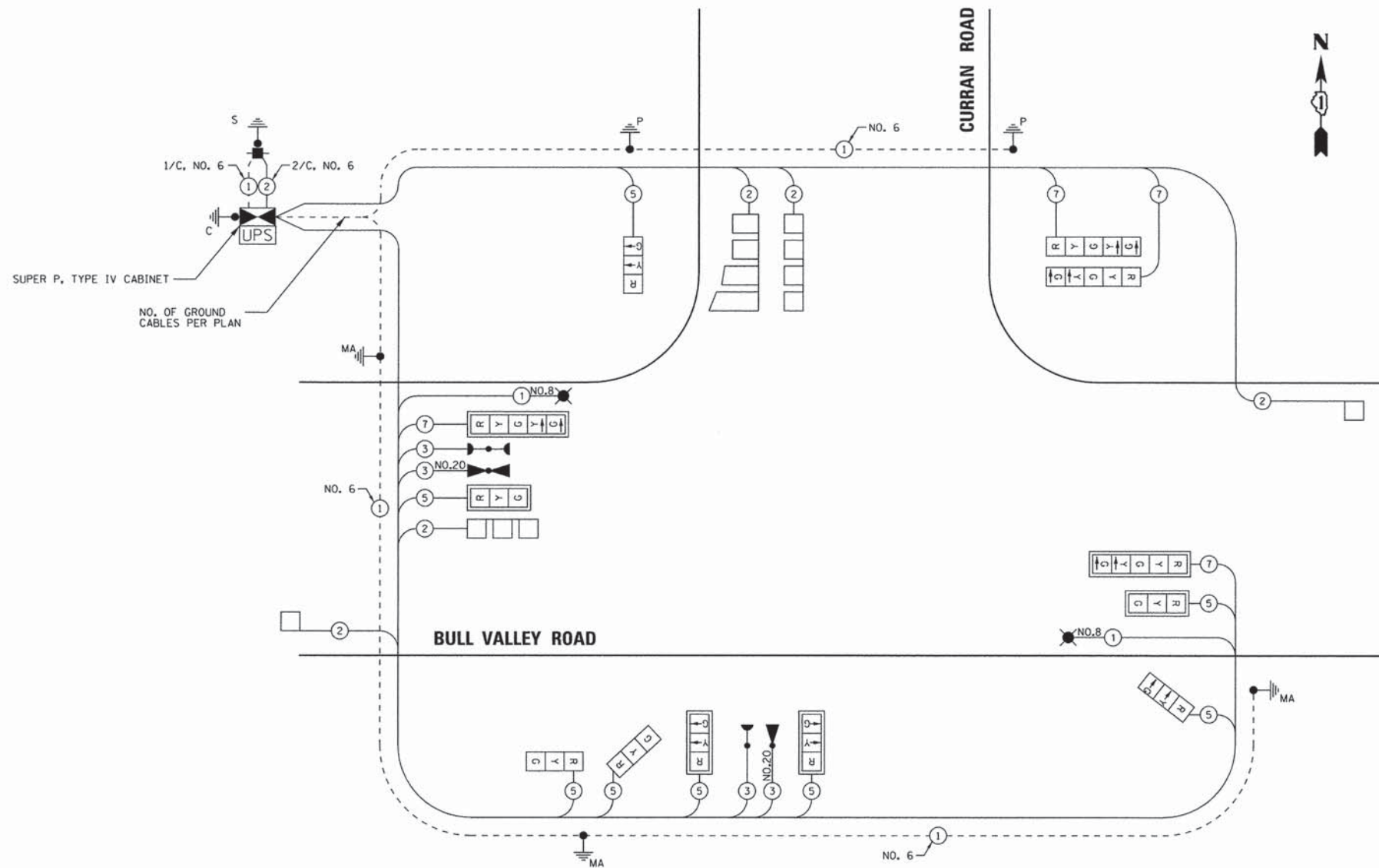
<b>PAVEMENT MARKING &amp; SIGNING PLAN CURRAN ROAD</b>			
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS	STA. 200+00.00 TO STA. 205+16.64	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	31
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			







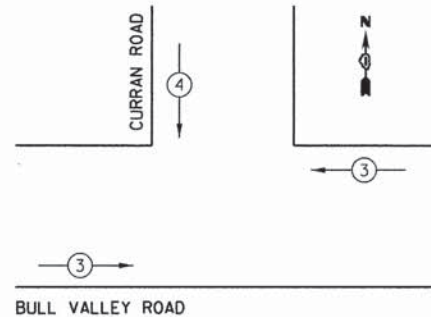


**CABLE PLAN**

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102.0
(YELLOW)	12		25	0.25	75.0
(GREEN)	12		15	0.25	45.0
ARROW	8		12	0.10	9.6
PEDESTRIAN SIGNAL CONTROLLER	0		25	1.00	0.0
LUMINAIRE	2		175	0.50	175.0
UPS	1		25	1.00	25.0
TOTAL =					531.6

ENERGY COSTS TO: CITY OF MCHENRY  
333 SOUTH GREEN STREET  
MCHENRY, ILLINOIS 60050

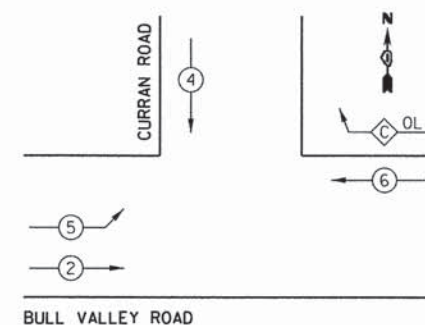
ENERGY SUPPLY CONTACT: NORA FERNANDEZ  
PHONE: (815) 490-2335  
COMPANY: COMMONWEALTH EDISON



**EMERGENCY VEHICLE PREEMPTION SEQUENCE**

PROPOSED EMERGENCY VEHICLE PREEMPTORS	
PROPOSED EMERGENCY VEHICLE PREEMPTORS	3 4
MOVEMENT	—

**CONTROLLER SEQUENCE**



**CONTROLLER SEQUENCE LEGEND**

- ← ⊕ → DUAL ENTRY PHASE
- ← ⊞ → SINGLE ENTRY PHASE
- ← ⊠ OL → OVERLAP
- \* NUMBER REFERRING TO ASSOCIATED PHASE
- ← ⊕ → PEDESTRIAN PHASE

**PHASE DESIGNATION DIAGRAM**

OVERLAP PHASE    PERMISSIVE PHASE    PROTECTED PHASE  
C = 6 + 4

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: PROJECT\_CONTACTS  
 CLIENT: CLIENTS  
 DATE PLOTTED: 2/18/2016 11:30:12 PM  
 FILE NAME: 375-shr-sigcab-01.dgn  
 PLOT DRIVER: JL.pdf\_bw.ctb  
 PEN TABLE: plot.tbl



USER NAME = jstz01	DESIGNED - JS	REVISED -
PLOT SCALE = 1" = 50'	DRAWN - DS	REVISED -
PLOT DATE = 2/18/2016	CHECKED -	REVISED -
	DATE - 2/18/16	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

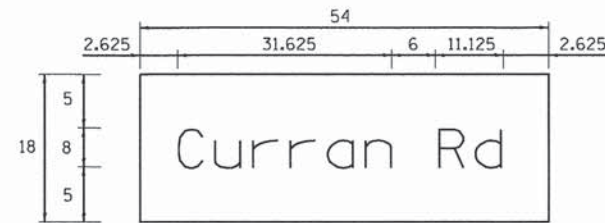
**CABLE PLAN AND PHASE DESIGNATION DIAGRAM  
BULL VALLEY ROAD AT CURRAN ROAD**

SCALE: 1" = 50'    SHEET NO. 2 OF 3 SHEETS    STA. BEG.    TO STA. END

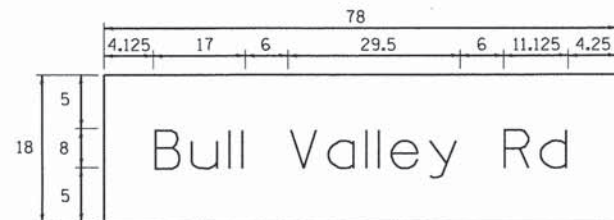
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	33
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.    [ILLINOIS] FED. AID PROJECT				



**SIGN PANEL – TYPE 1 OR TYPE 2**



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	6.75	1	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	9.75	2	ZZ	1

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL.

**SCHEDULE OF QUANTITIES**

PAY ITEM DESCRIPTION	UNIT	TOTAL
SIGN PANEL - TYPE 1	SQ FT	13.50
SIGN PANEL - TYPE 2	SQ FT	9.75
SERVICE INSTALLATION - GROUND MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	675.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	40.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	220.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	296.0
HANDHOLE	EACH	2
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 8	FOOT	505.0
PAINT NEW TRAFFIC SIGNAL POST	EACH	2
PAINT NEW MAST ARM AND POLE, 40 FOOT AND OVER	EACH	1
PAINT NEW COMBINATION MAST ARM AND POLE, UNDER 40 FOOT	EACH	1
PAINT NEW COMBINATION MAST ARM AND POLE, 40 FOOT AND OVER	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	444.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,810.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	912.0
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,295.0
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	49.0
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	795.0
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 26 FT.	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE 42 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	12.0
CONCRETE FOUNDATION, TYPE C	FOOT	4.0
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	39.0
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	2
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	6
INDUCTIVE LOOP DETECTOR	EACH	5
DETECTOR LOOP, TYPE II	FOOT	616.0
LIGHT DETECTOR	EACH	3
LIGHT DETECTOR AMPLIFIER	EACH	1
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	442.0
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET	EACH	1
LUMINAIRE, LED, HORIZONTAL MOUNT, 175 WATT	EACH	2
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1

COMPANY NAME: HRGreen  
 PROJECT CONTACT: #PROJECT\_CONTACTS  
 CLIENT: #CLIENTS  
 FILE NAME: 375-001-01000-02.dgn  
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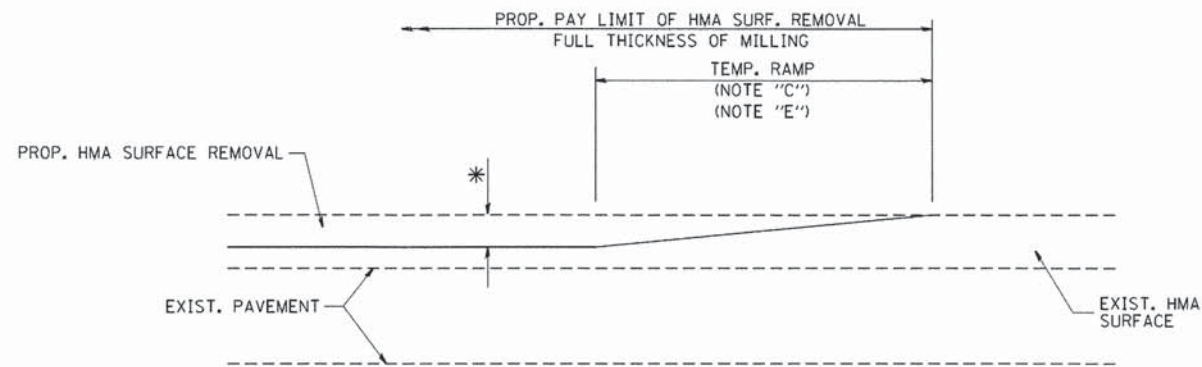
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	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

SIGN DETAILS AND SCHEDULE OF QUANTITIES			
BULL VALLEY ROAD AT CURRAN ROAD			
SCALE: 1" = 50'	SHEET NO. 3 OF 3 SHEETS	STA. BEG	TO STA. END

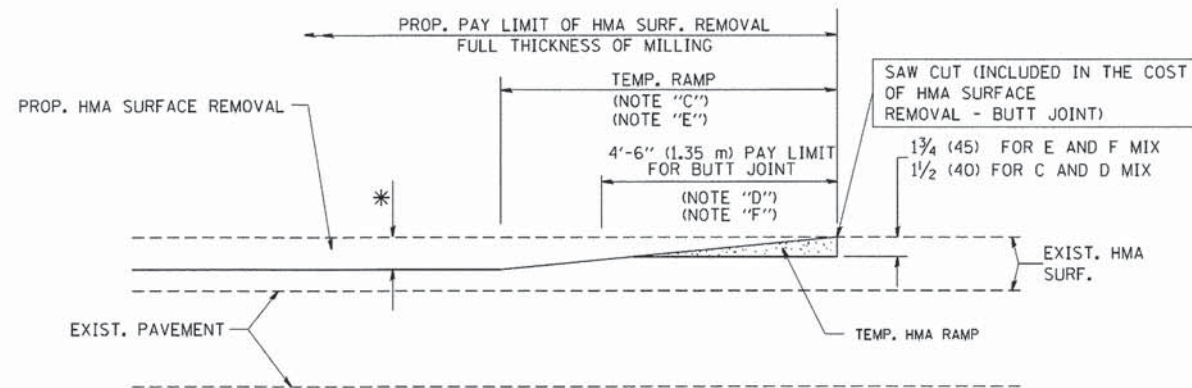
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14-00078-00-CH		MCHENRY	59	34
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		





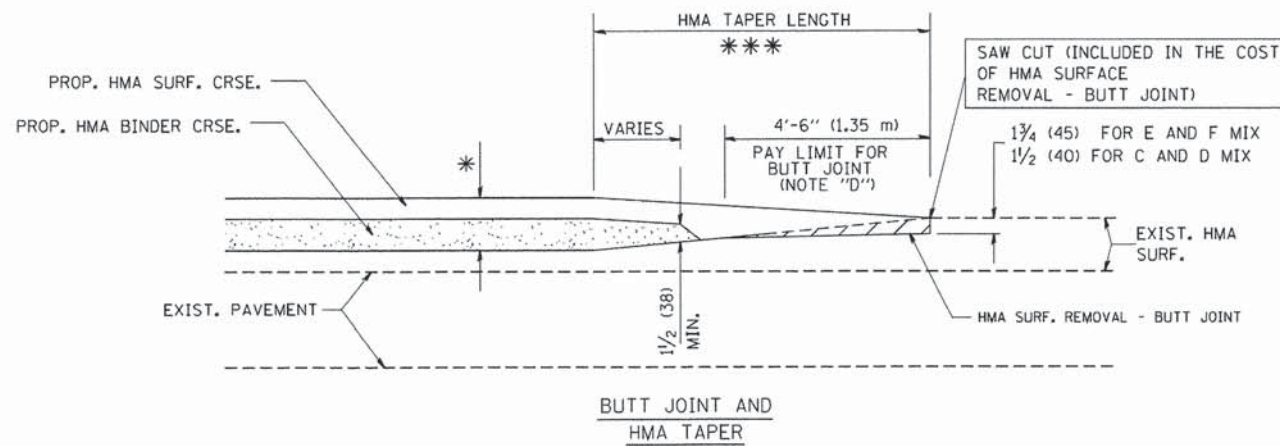
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

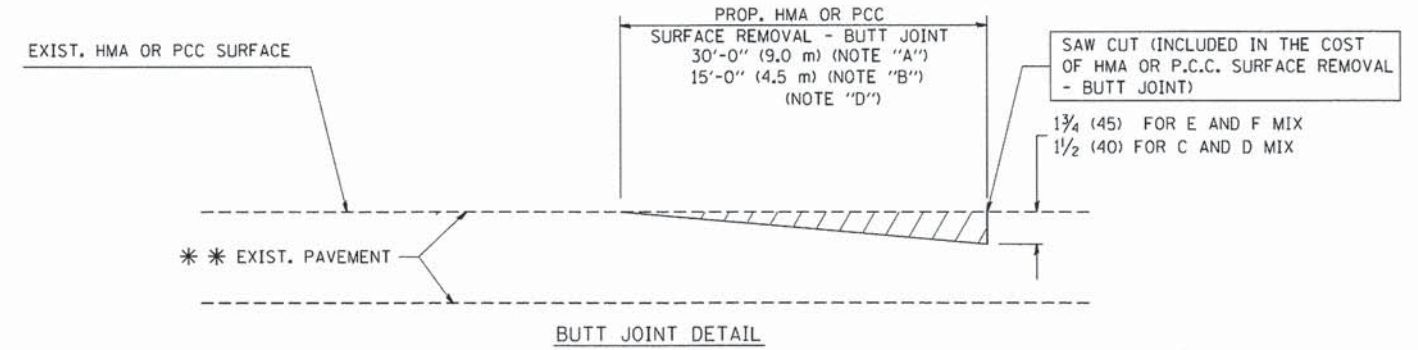


HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

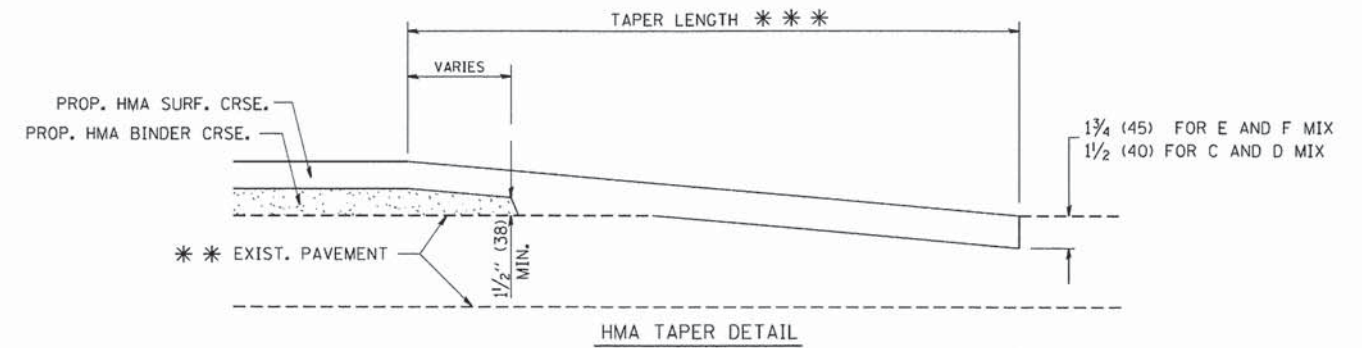
**OPTION 2**  
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER**  
**FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**TYPICAL BUTT JOINT AND HMA TAPER**  
**FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

\*FAU3860, FAU0084

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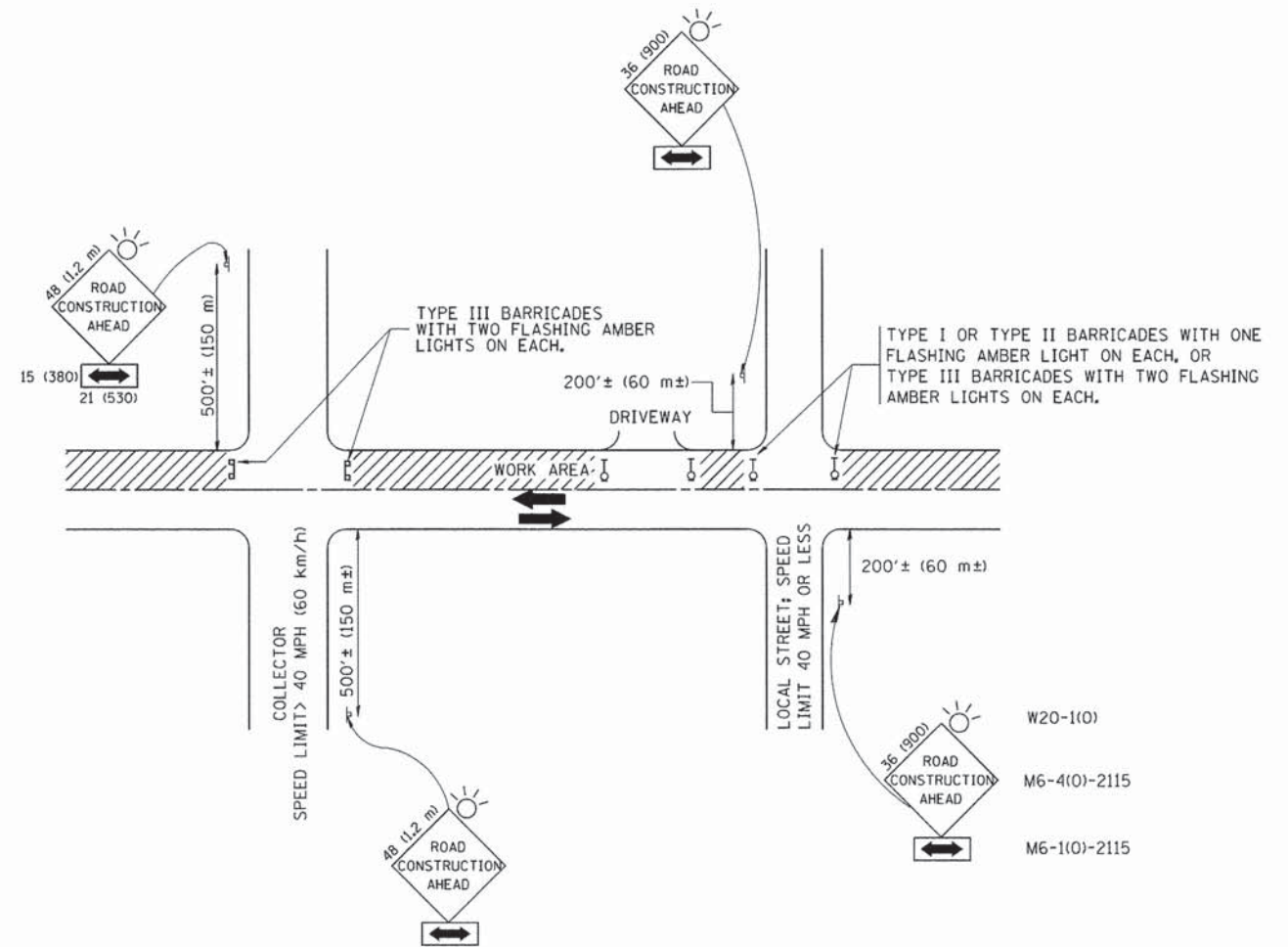
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	14-00078-00-CH	MCHENRY	59	35
<b>BD400-05 BD32</b>			<b>CONTRACT NO. 61C45</b>	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				





TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown. #FAU3860, FAU0084

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 CLIENT: #CLIENT#  
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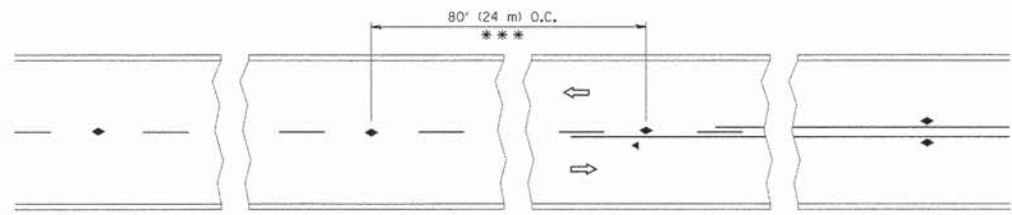
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR  
 SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

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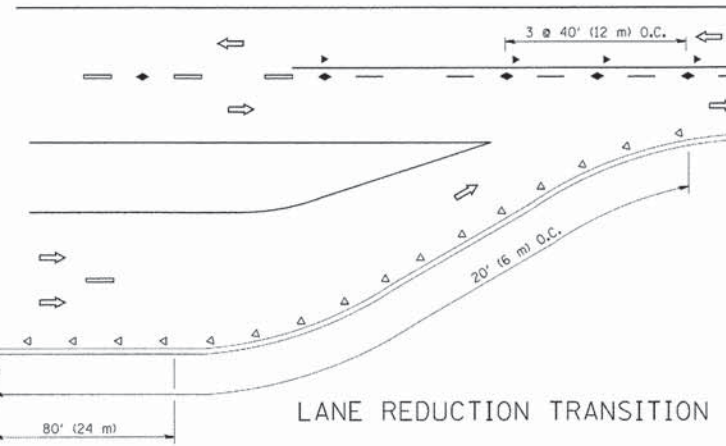
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TC-10			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



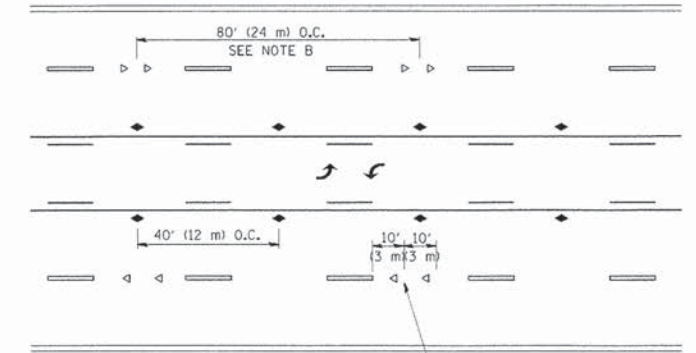


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

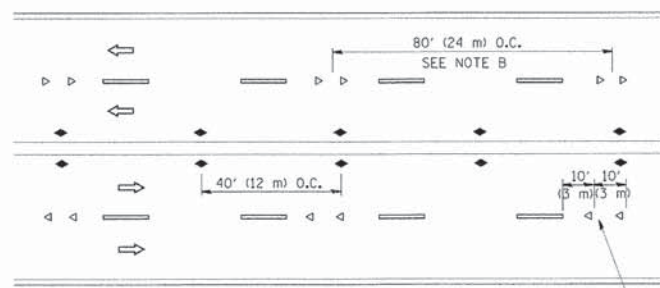
TWO-LANE/TWO-WAY



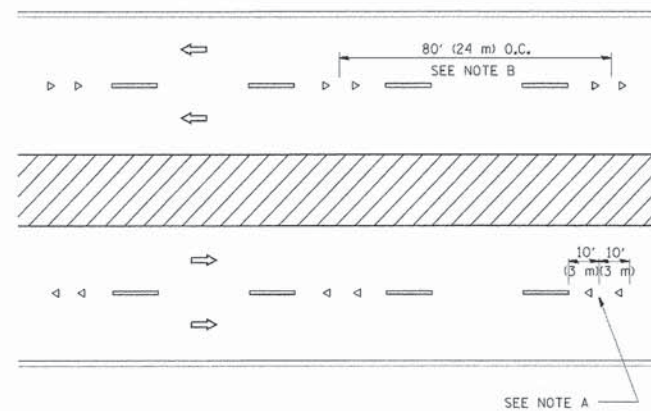
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

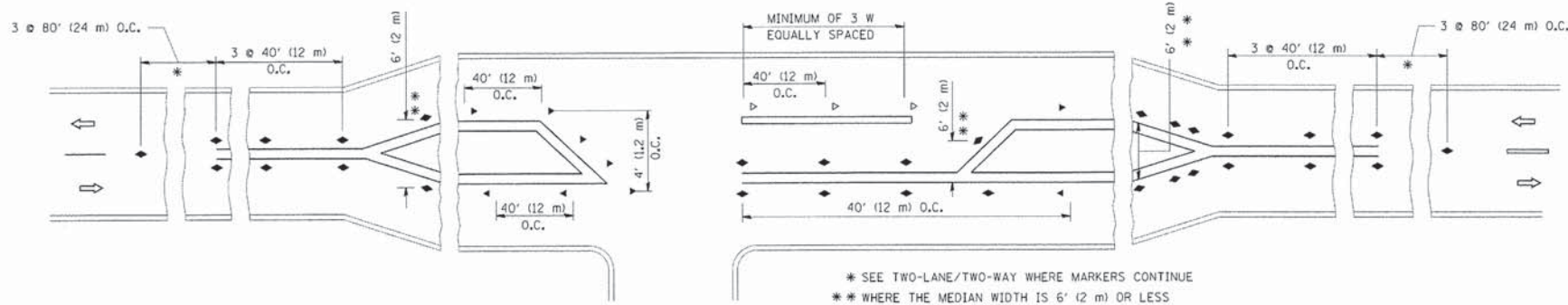
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

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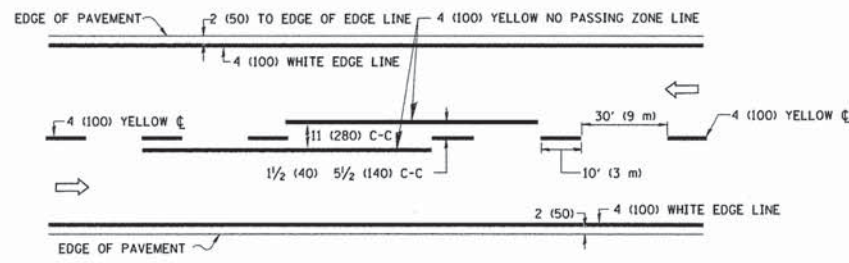
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS	
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

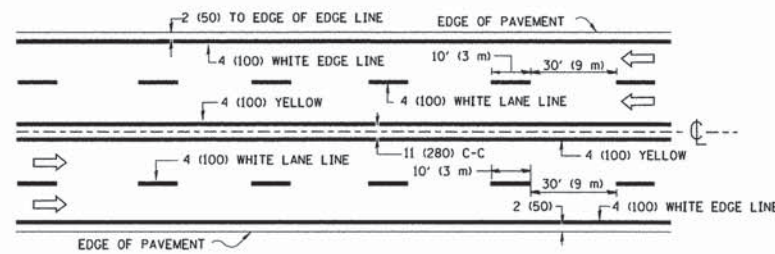
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•	14-00078-00-CH	MCHENRY	59	37
TC-11			CONTRACT NO.	61C45
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

\*FAU3860, FAU0084

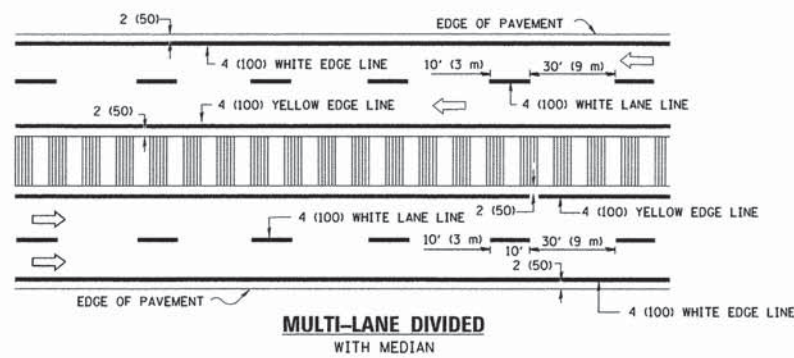




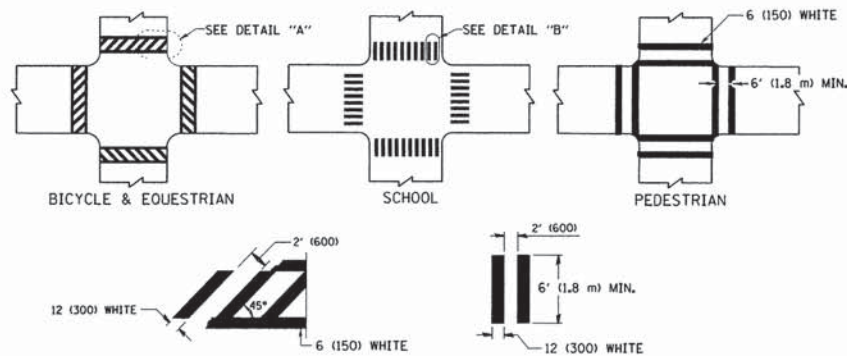
**2-LANE ROADWAY**



**MULTI-LANE UNDIVIDED**

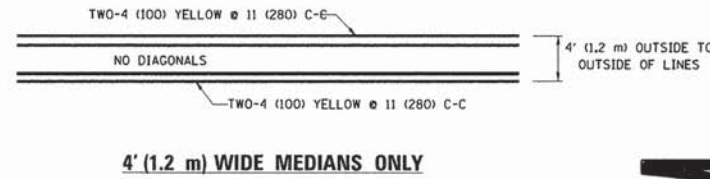


**TYPICAL LANE AND EDGE LINE MARKING**

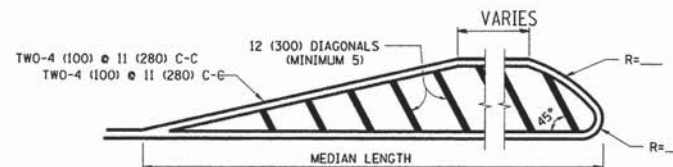


**DETAIL "A" TYPICAL CROSSWALK MARKING**  
**DETAIL "B" TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

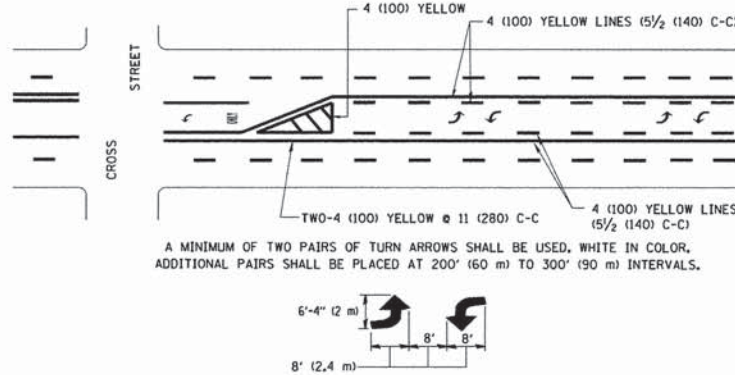


**4' (1.2 m) WIDE MEDIANS ONLY**

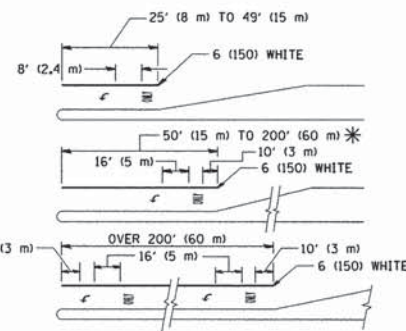


**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (OVER 45MPH (70 km/h))



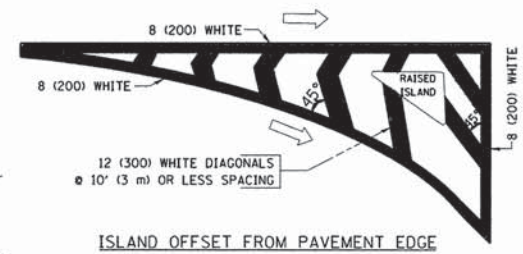
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



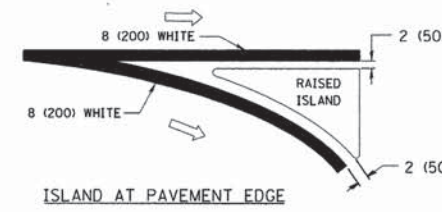
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

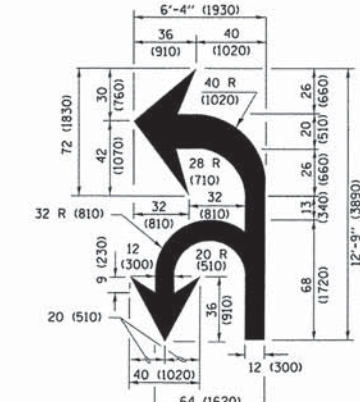


**ISLAND OFFSET FROM PAVEMENT EDGE**

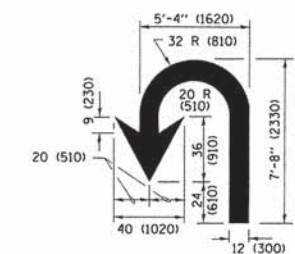


**ISLAND AT PAVEMENT EDGE**

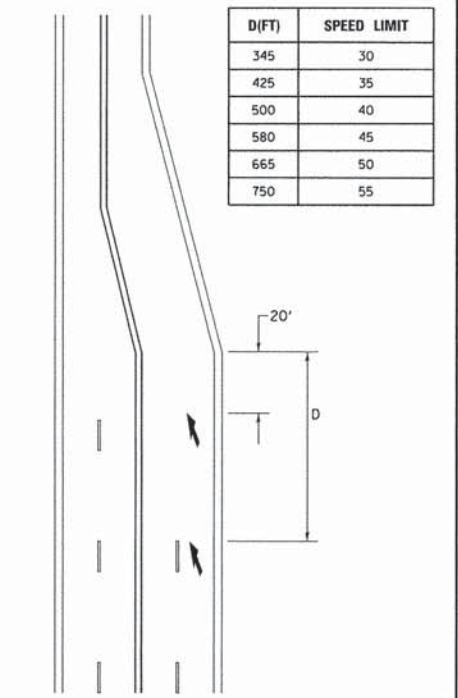
**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**



**LANE REDUCTION TRANSITION**  
 \* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8' )	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

\*FAU3860, FAU0084

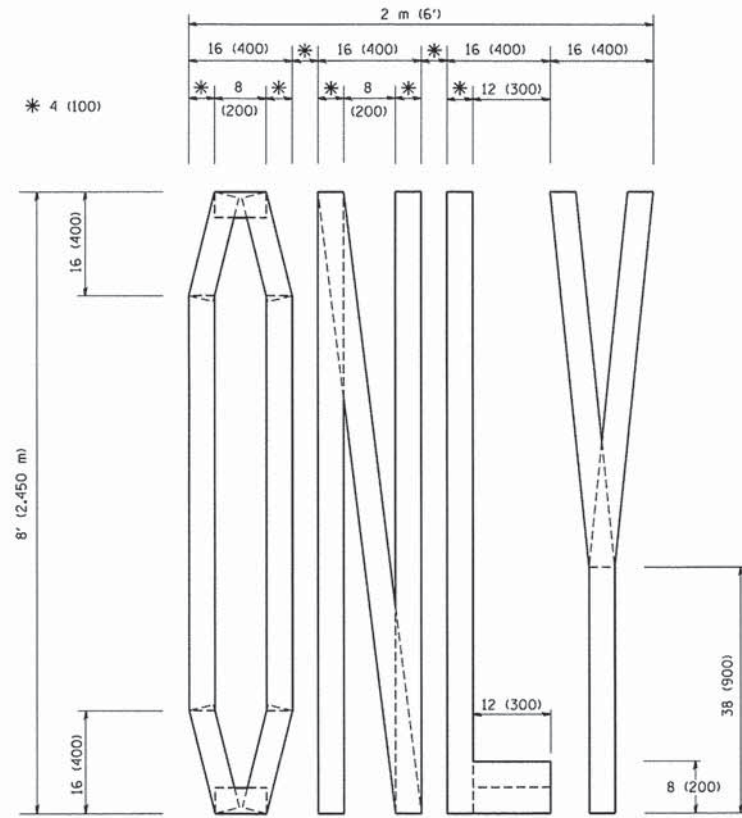
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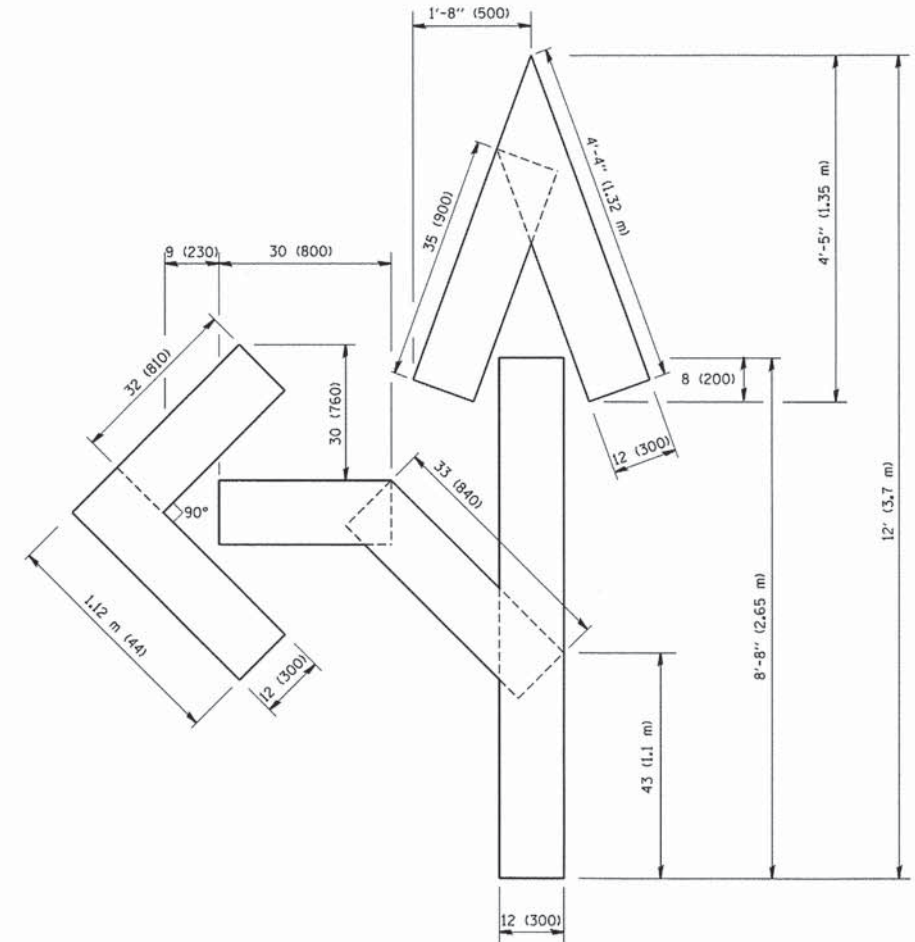
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TYPICAL PAVEMENT MARKINGS			14-00078-00-CH	MCHENRY	59	38
SCALE: NONE		TC-13		CONTRACT NO. 61C45		
SHEET 1 OF 1 SHEETS		ILLINOIS FED. AID PROJECT				

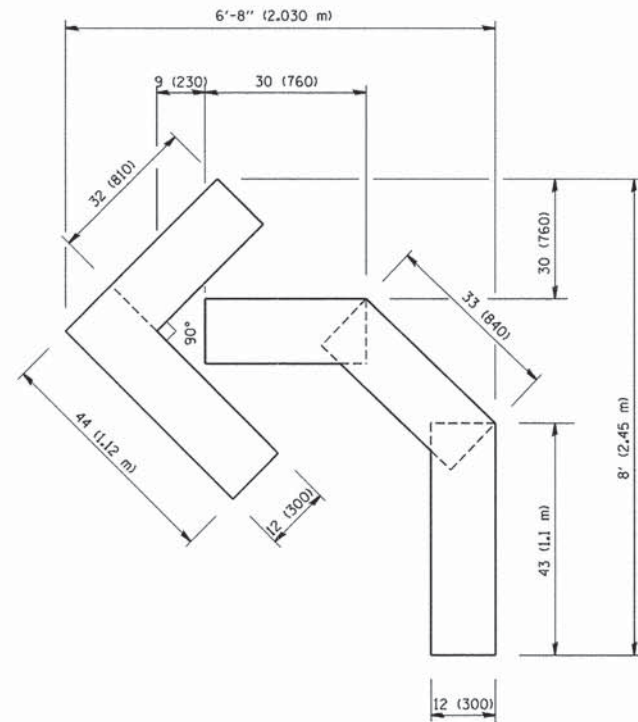




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS  
 FOR TRAFFIC STAGING

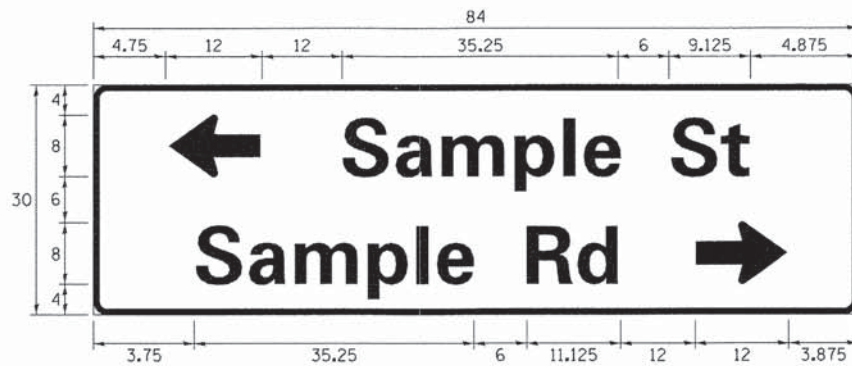
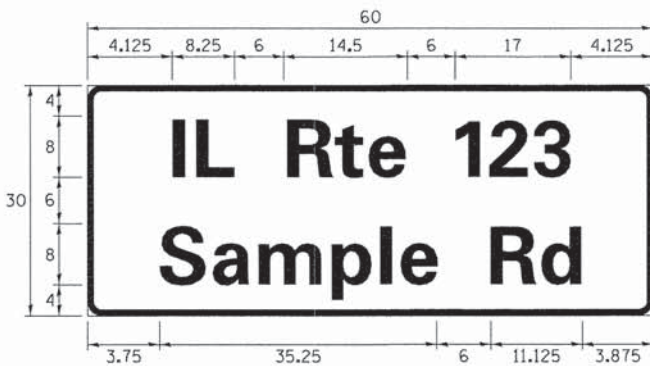
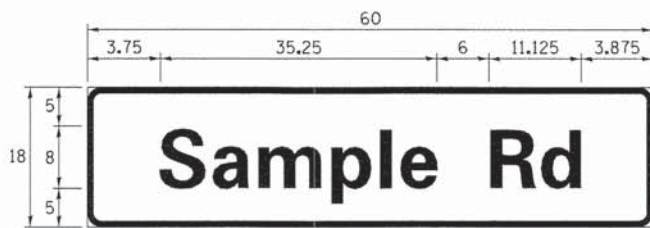
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

\*FAU3860, FAU0084



**SIGN PANEL – TYPE 1 OR TYPE 2**



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

**COMMON STREET NAME ABBREVIATIONS AND WIDTHS**

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

**GENERAL NOTES**

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012. AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THERE IS SPACE AVAILABLE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

**LOCAL SUPPLIERS:**

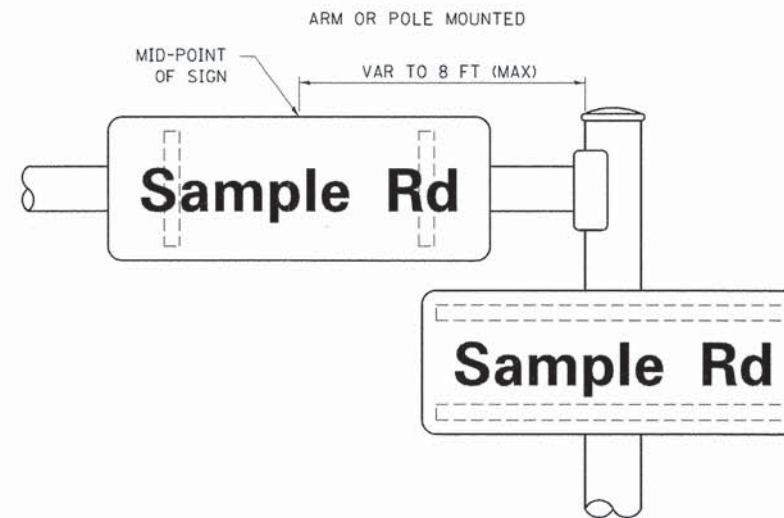
- J.O. HERBERT COMPANY, INC  
MIDLOTHIAN, VA
- WESTERN REMAC, INC.  
WOODRIDGE, IL

**PARTS LISTING:**

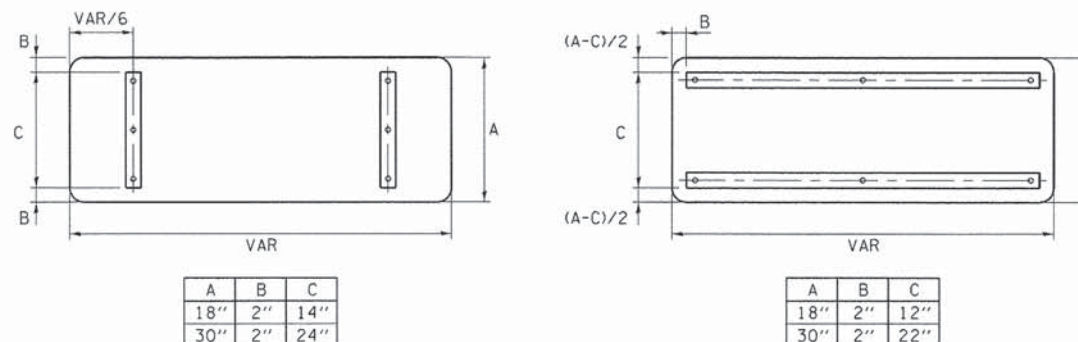
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
- SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
- BRACKETS SELF TAPPING WITH NEOPRENE WASHER
- CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING PART #HPN034 (UNIVERSAL)

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BRACKET OF THE ABOVE PRODUCT.

**MOUNTING LOCATION**



**SUPPORTING CHANNELS**



**STANDARD ALPHABETS SPACING CHART**

(8") UPPER CASE AND (6") LOWER CASE

CHARACTER	FHWA SERIES "C"			FHWA SERIES "D"			
	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
MAST ARM MOUNTED STREET NAME SIGNS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	40
TS-02		CONTRACT NO.	61C45	
[ILLINOIS] FED. AID PROJECT				

\*FAU3860, FAU0084



# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER				HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER				DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY				JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				UNDERGROUND CONDUIT, GALVANIZED STEEL (UC)				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F SM24F			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH			CT	GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)			CNC	CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S	S	STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I	IP	ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	RL			SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				QUEUE DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				PREFORMED QUEUE DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				"RB" INDICATES REFLECTIVE BACKPLATE				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL							
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED							
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID							
ILLUMINATED SIGN "NO LEFT TURN"				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER							
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO INTERCONNECT							
DETECTOR LOOP, TYPE I				RADIO REPEATER							
PREFORMED DETECTOR LOOP				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED							
MICROWAVE VEHICLE SENSOR				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)							
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											

## RAILROAD SYMBOLS

	EXISTING	PROPOSED
RAILROAD CONTROL CABINET		
RAILROAD CANTILEVER MAST ARM		
FLASHING SIGNAL		
CROSSING GATE		
CROSSBUCK		

\*FAU3860, FAU0084

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

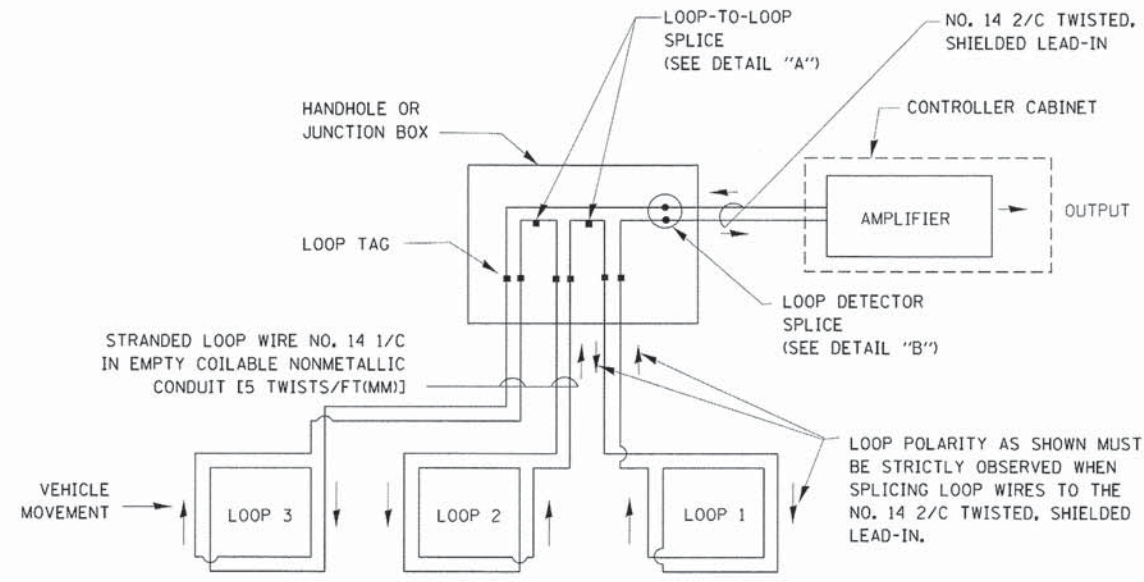
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<b>STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			
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F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
.	14-00078-00-CH	MCHENRY	59	41
<b>TS-05</b>		CONTRACT NO.	61C45	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



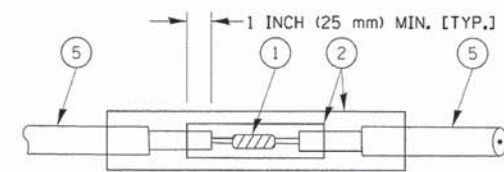
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

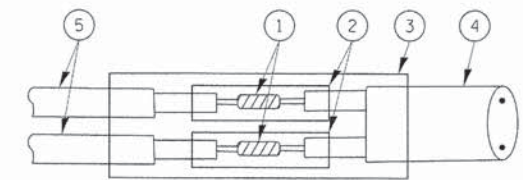


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

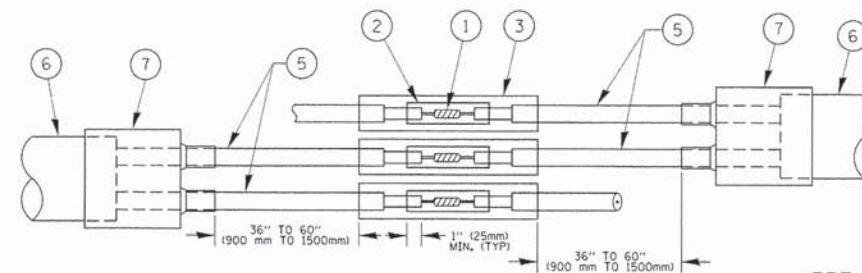


DETAIL "A"  
LOOP-TO-LOOP SPLICE

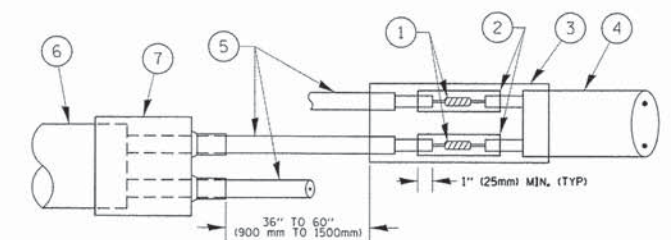


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



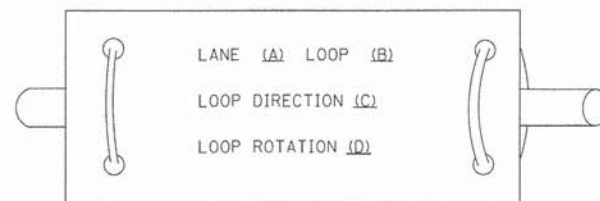
DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PRE-FORMED LOOP**

**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

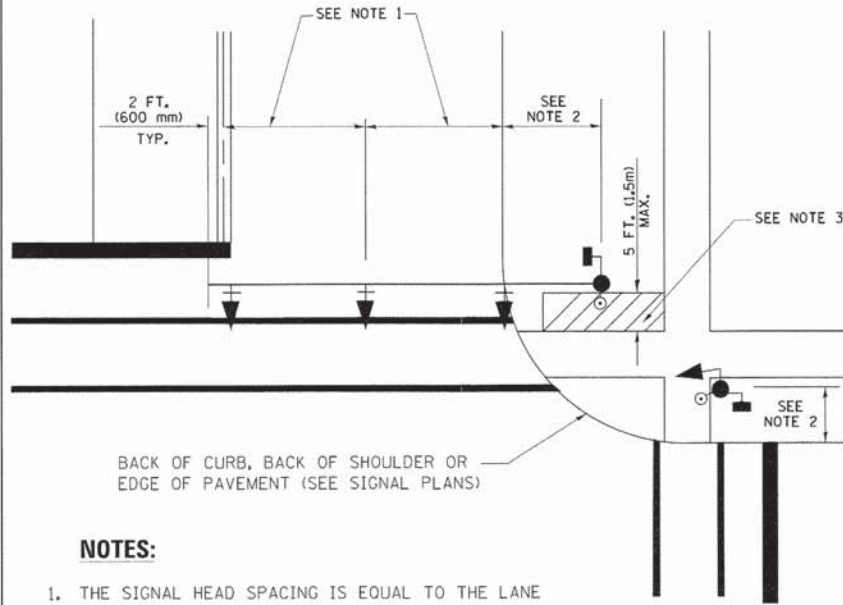
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	42
<b>TS-05</b>		CONTRACT NO. 61C45		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\*FAU3860, FAU0084



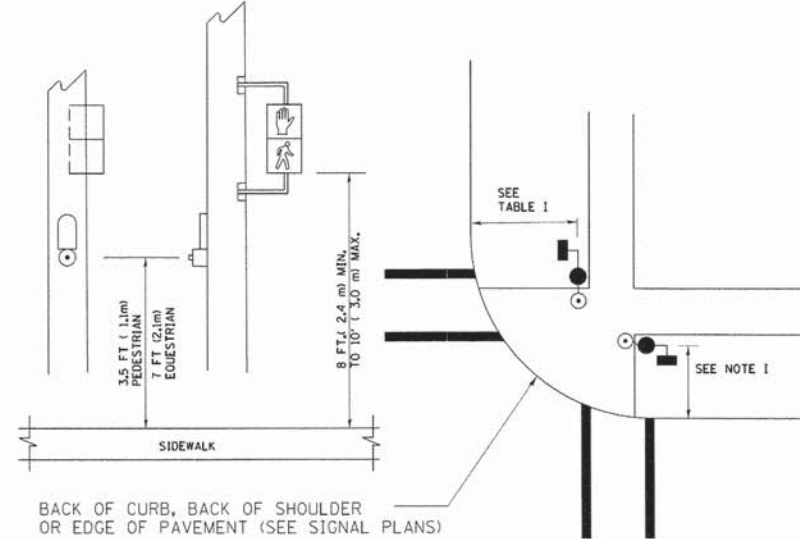
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

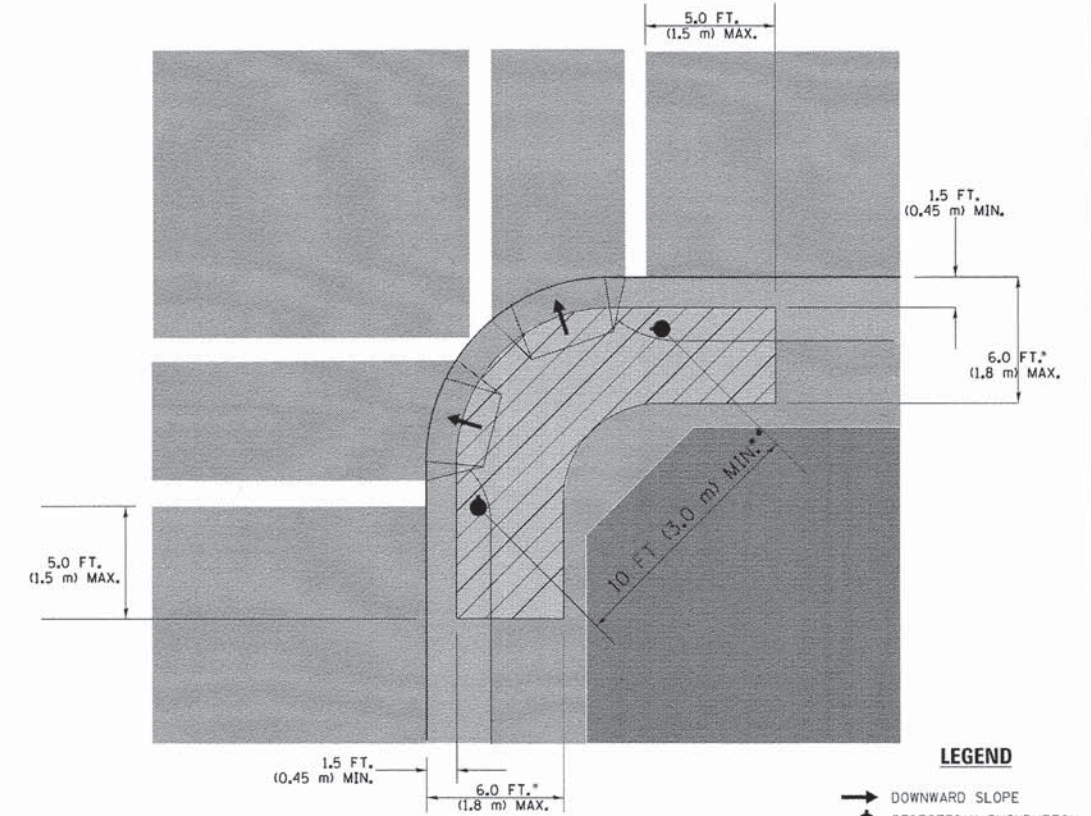
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPARATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

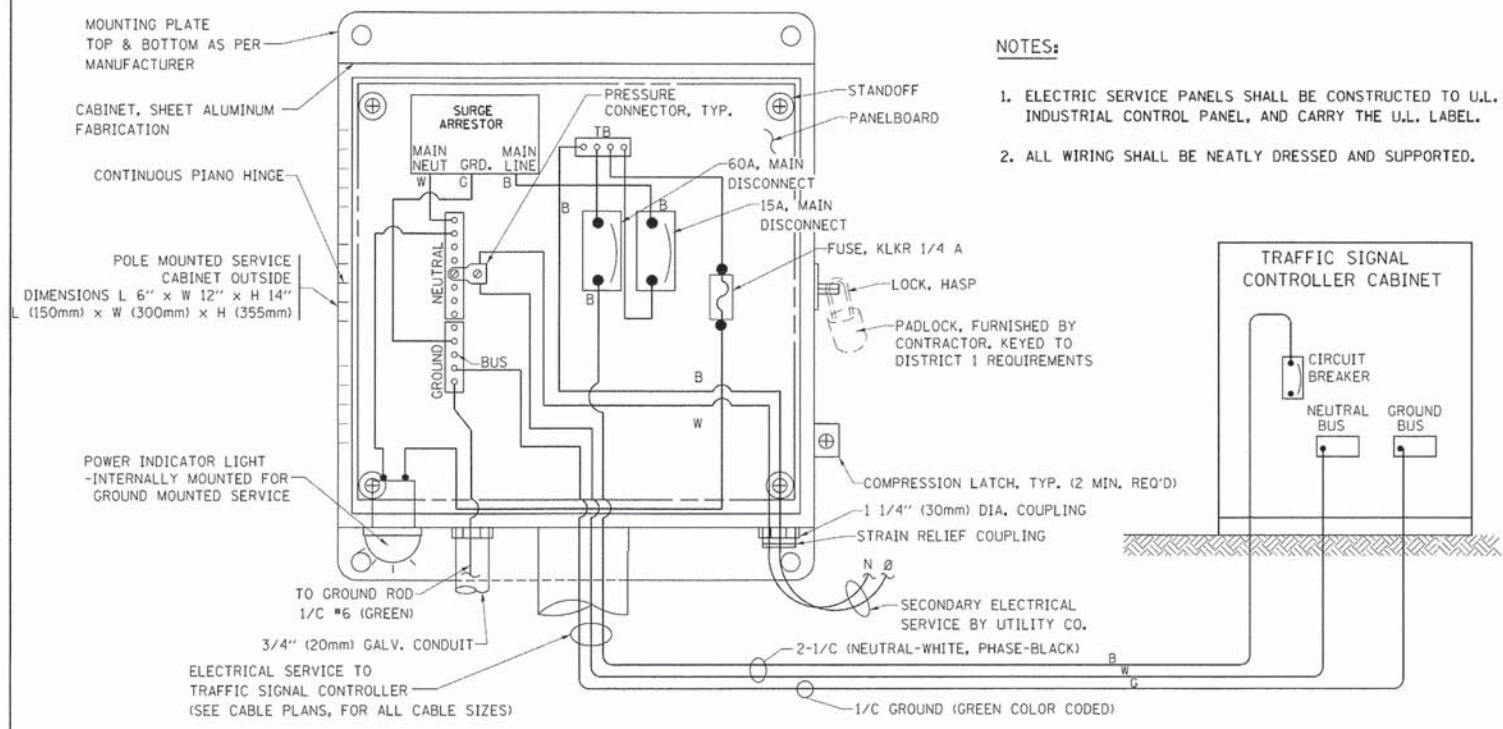
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STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

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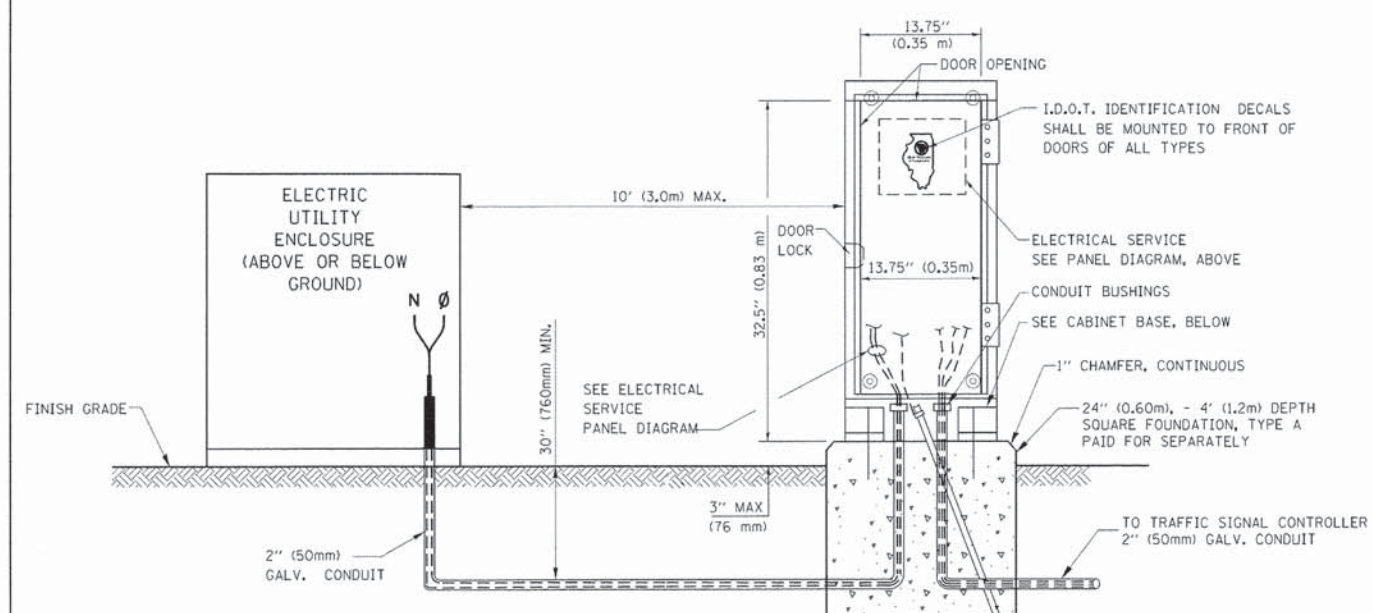
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	<b>TS-05</b>	CONTRACT NO.	61C45	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

#FAU3860, FAU0084

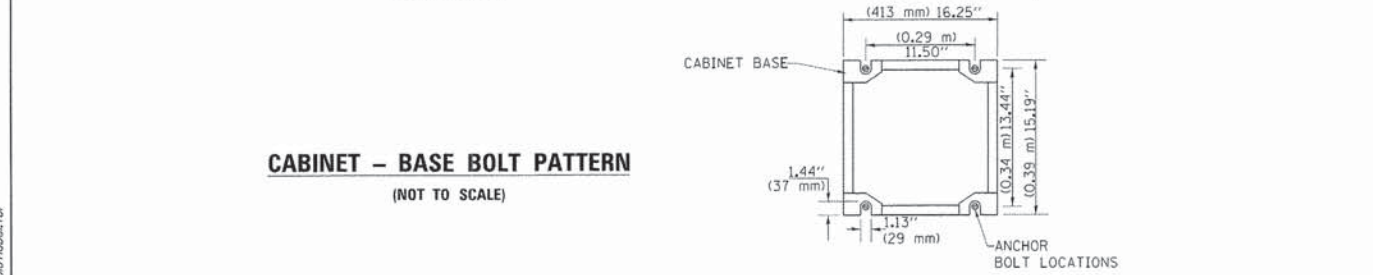




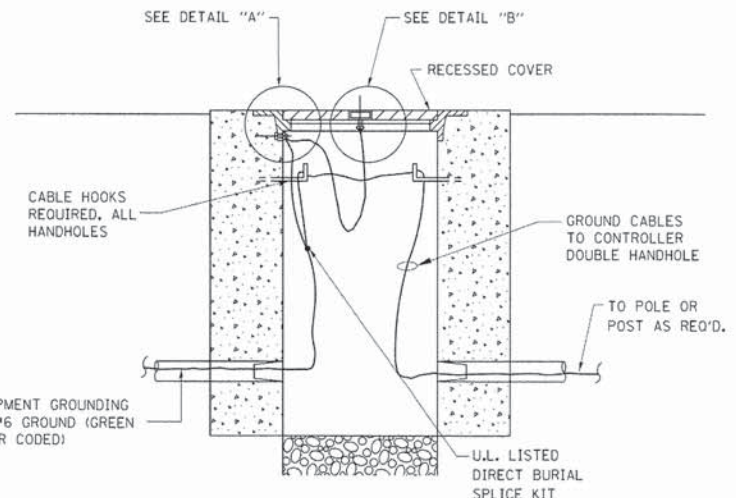
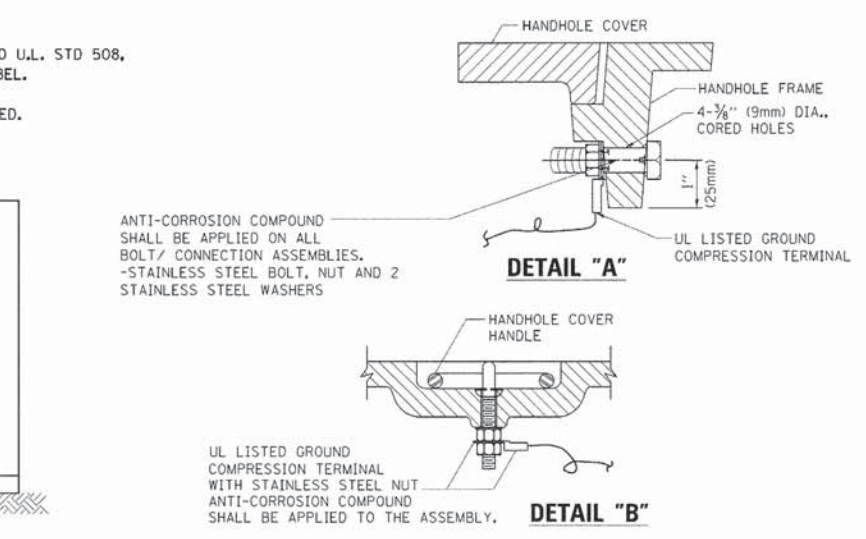
**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)**



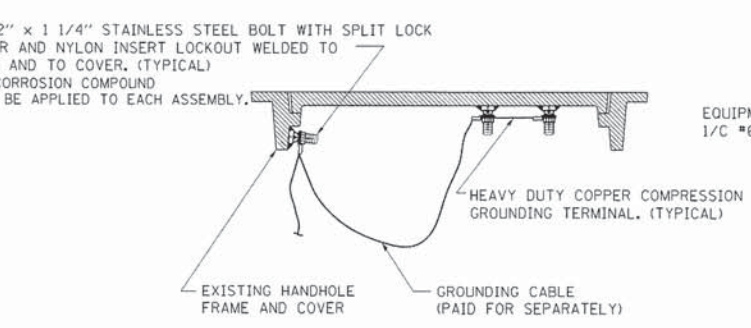
**SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)**



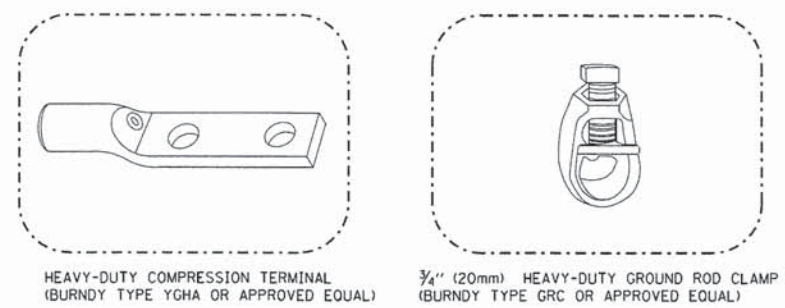
**CABINET – BASE BOLT PATTERN (NOT TO SCALE)**



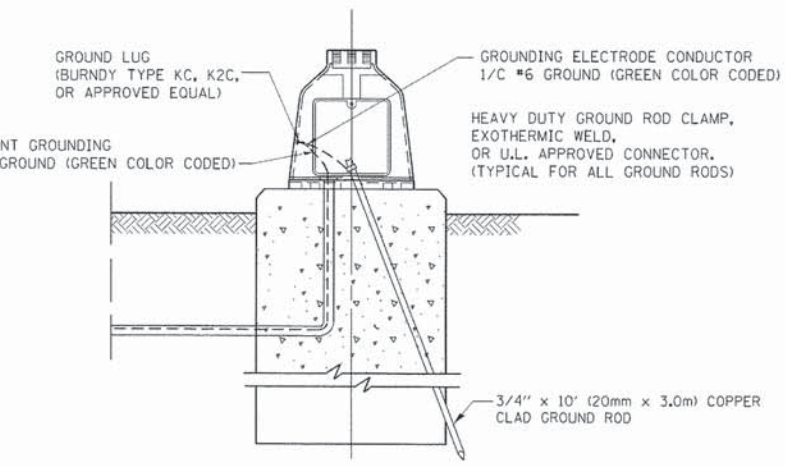
**HANDHOLE COVER & FRAME – GROUNDING DETAIL (NOT TO SCALE)**



**EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL (NOT TO SCALE)**



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE /POST-GROUNDING DETAIL (NOT TO SCALE)**

**NOTES: GROUNDING SYSTEM**

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

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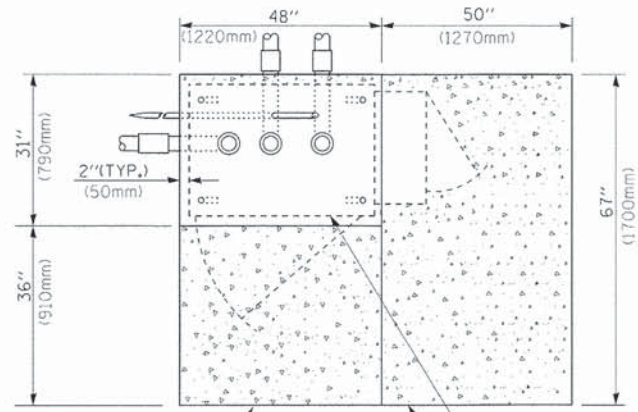
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
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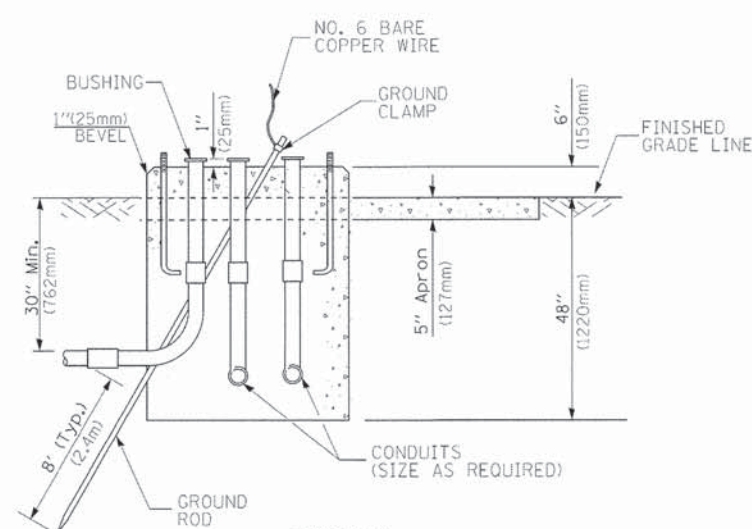
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TS-05			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\*FAU3860, FAU0084

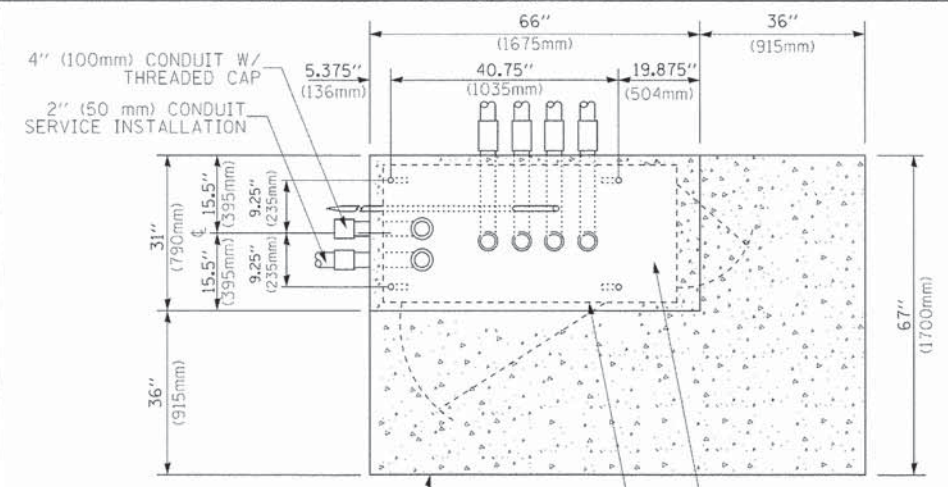




**TOP VIEW**

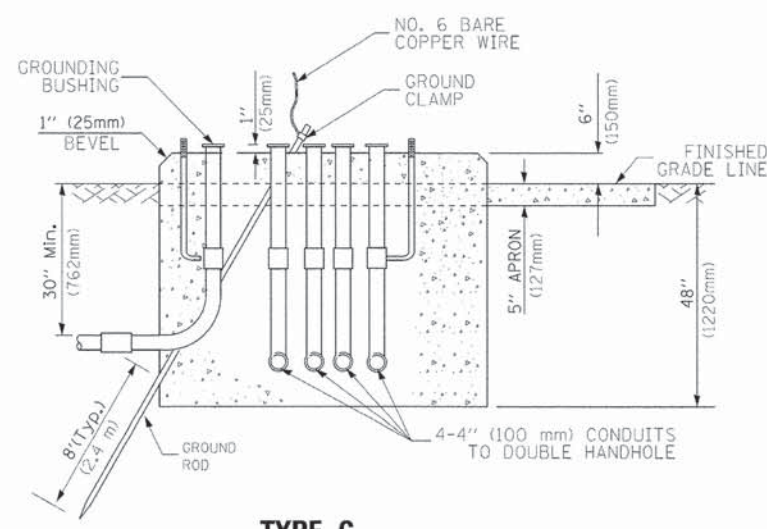


**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**

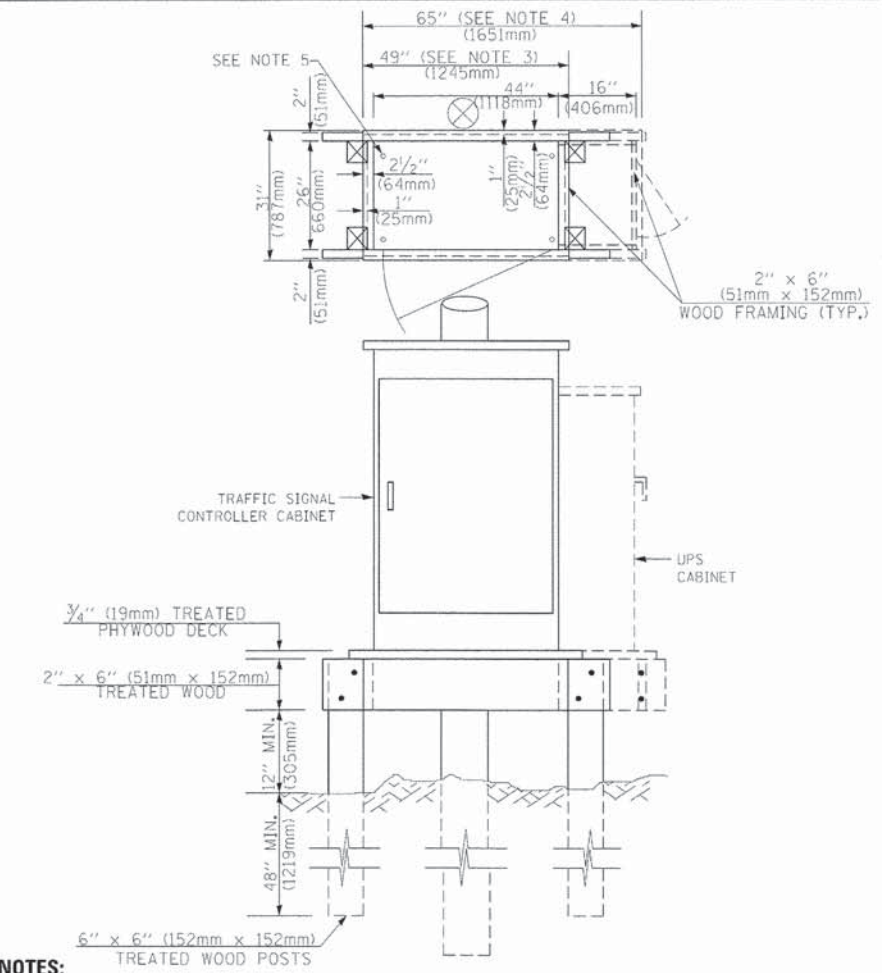


**TOP VIEW**

**NOTE:**  
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001..

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**

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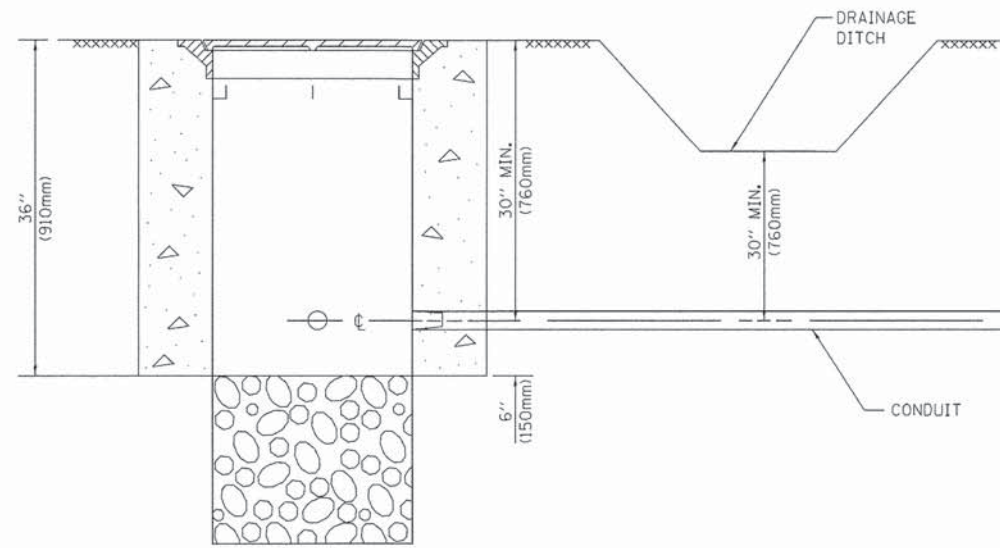
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			
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F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

#FAU3860, FAU0084

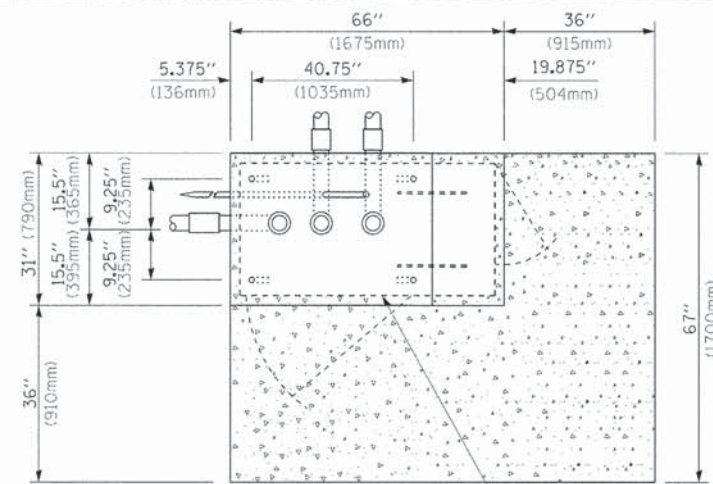




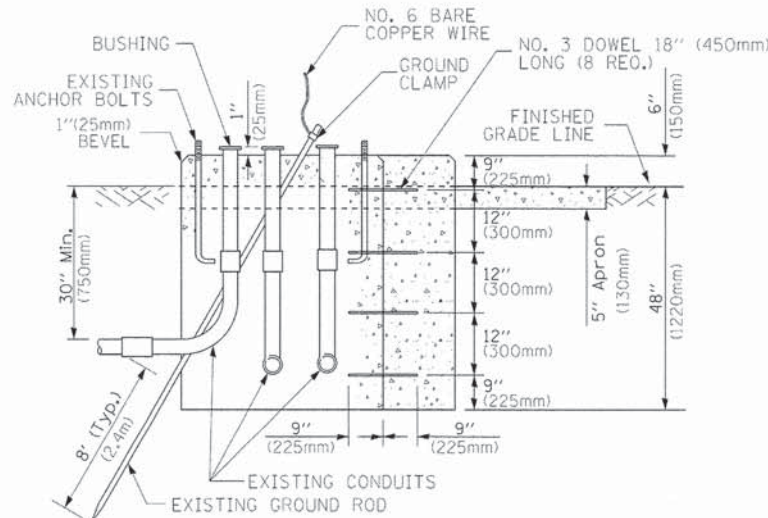
**NOTES:**

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

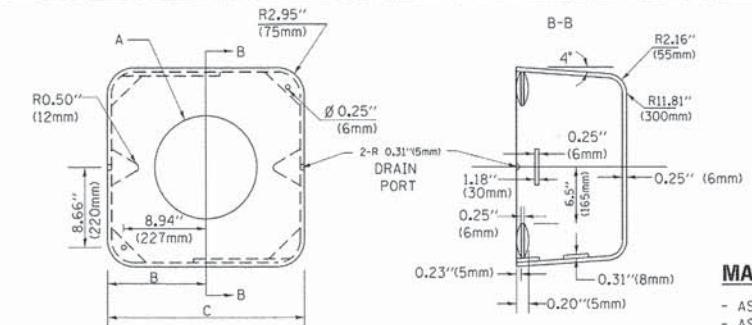
**HANDHOLE WITH MINIMUM CONDUIT DEPTH**  
(NOT TO SCALE)



**TOP VIEW**  
(NOT TO SCALE)



**MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION**  
(NOT TO SCALE)



**MATERIAL:**  
- ASTM A36 STEEL  
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

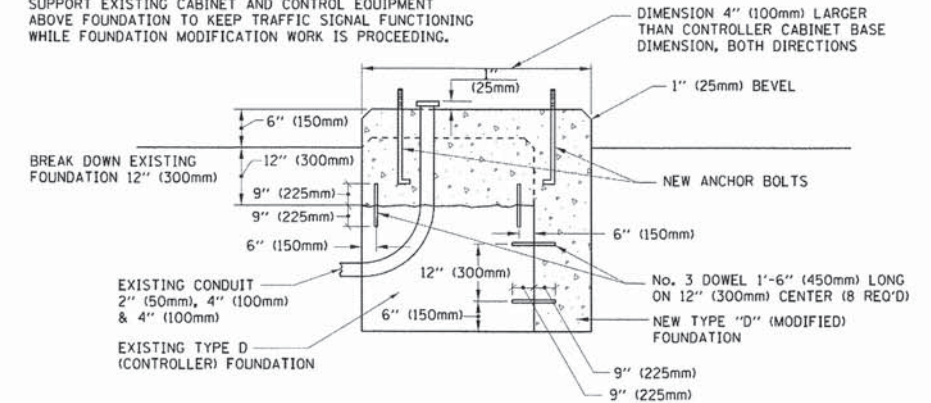
**SHROUD**

**NOTES:**

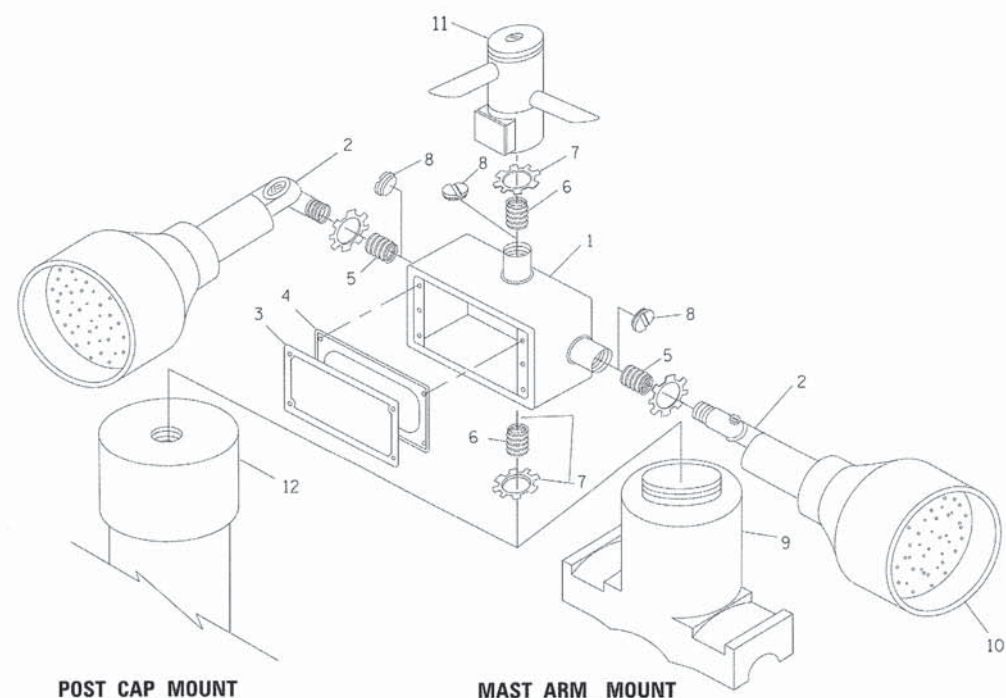
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

**NOTE:**

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



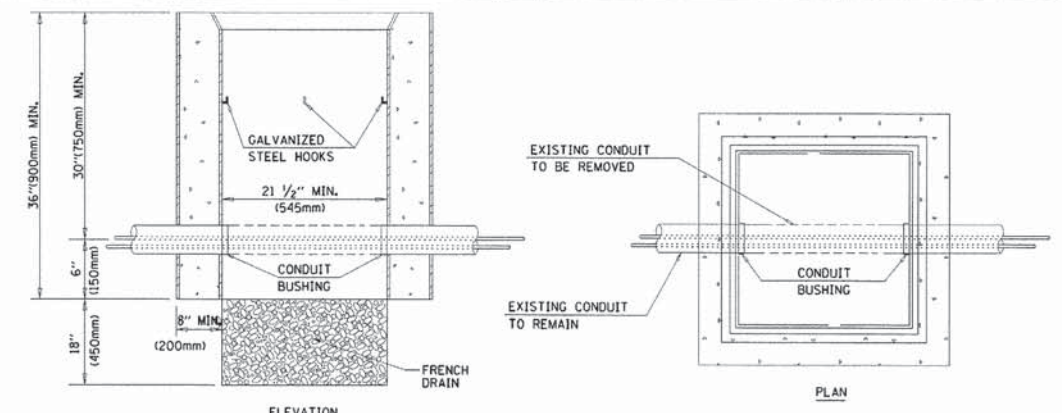
**MODIFY EXISTING TYPE "D" FOUNDATION**



ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

**NOTES:**

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEORNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



**NOTES:**

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

**EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL**

FILE NAME =	USER NAME = foatemj	DESIGNED - DAD	REVISED - DAG 1-1-14
PROJECT CONTACT =		DRAWN - BCK	REVISED -
DATE PLOTTED = 1/26/2014 9:04:09 AM		CHECKED - DAD	REVISED -
FILE NAME = 375-sh1-det-1905f.dgn		DATE - 10-28-09	REVISED -
PLOT DRIVER = plotttbl			
PEN TABLE =			

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

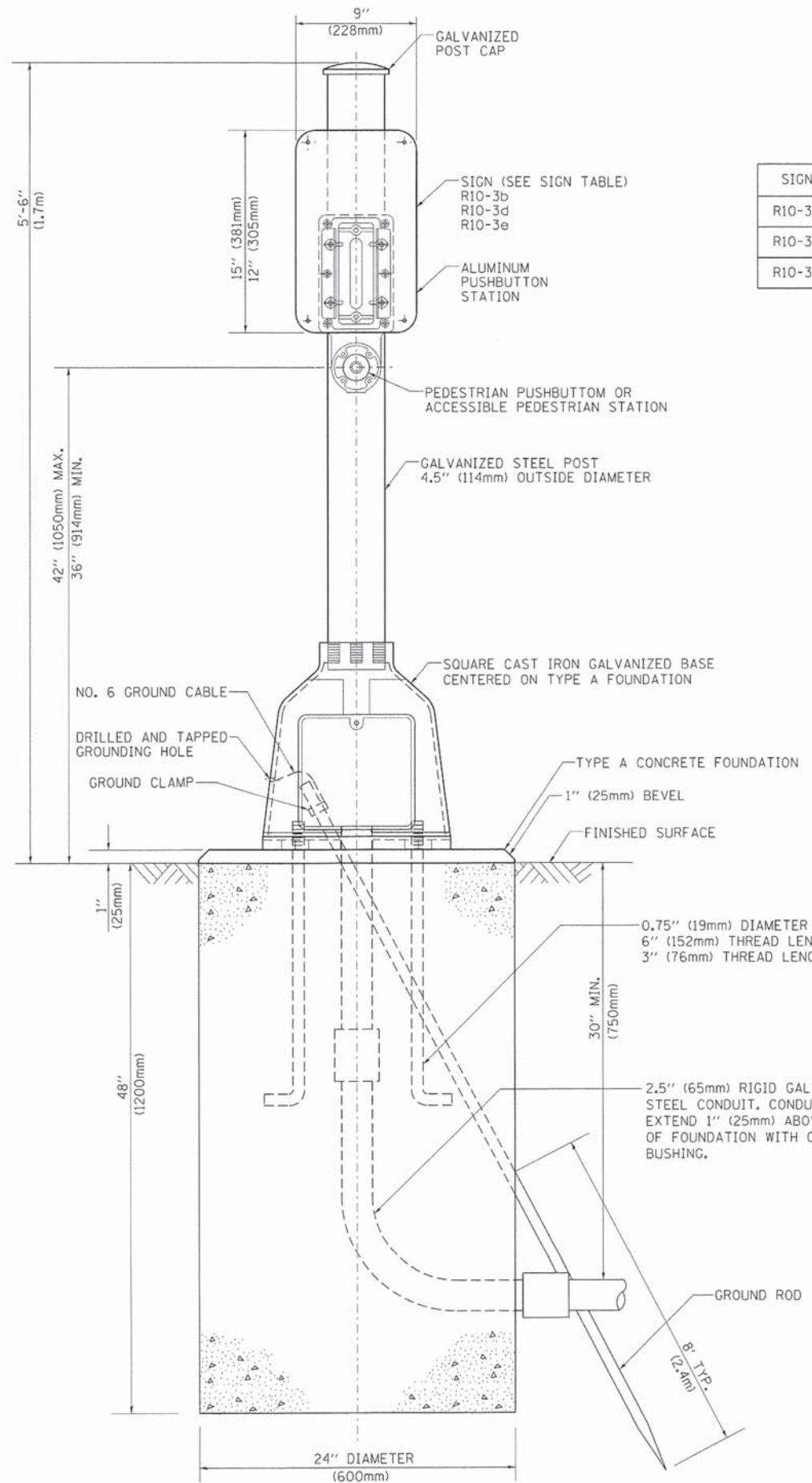
**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**  
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	14-00078-00-CH	MCHENRY	59	46
TS-05			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

COMPANY NAME: #COMPANY.NAMES  
PROJECT CONTACT: #PROJECT.CONTRACT#  
DATE PLOTTED: 1/26/2014 9:04:09 AM  
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PLOT DRIVER: plotttbl  
PEN TABLE:

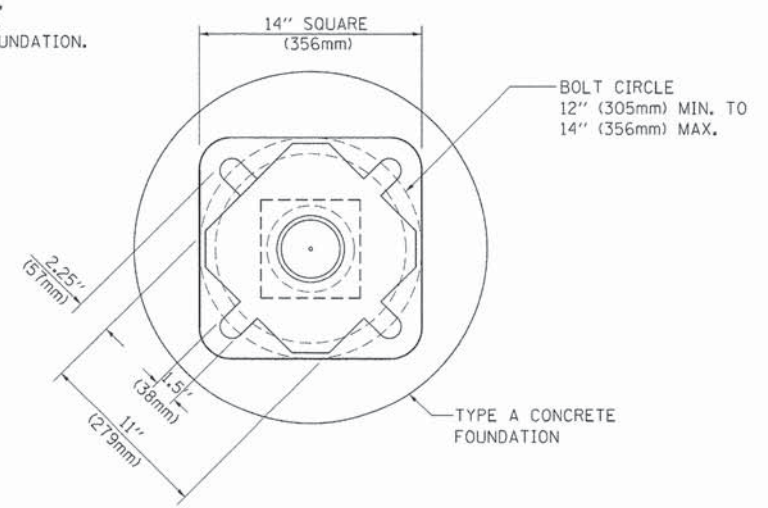
#FAU3860, FAU0084





**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**  
**PEDESTRIAN PUSH BUTTON POST, TYPE A**

COMPANY NAME: #COMPANY NAME#  
PROJECT CONTACT: #PROJECT CONTACT#  
CLIENT: #CLIENT#  
DATE: 1/13/2014 9:02:34 AM  
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PEN TABLE: plot\_hobbit.tbl

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PLOT DATE = 1/13/2014		DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>			
SCALE: NONE	SHEET NO. 7 OF 7 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	47
<b>TS-05</b>			CONTRACT NO. 61C45	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

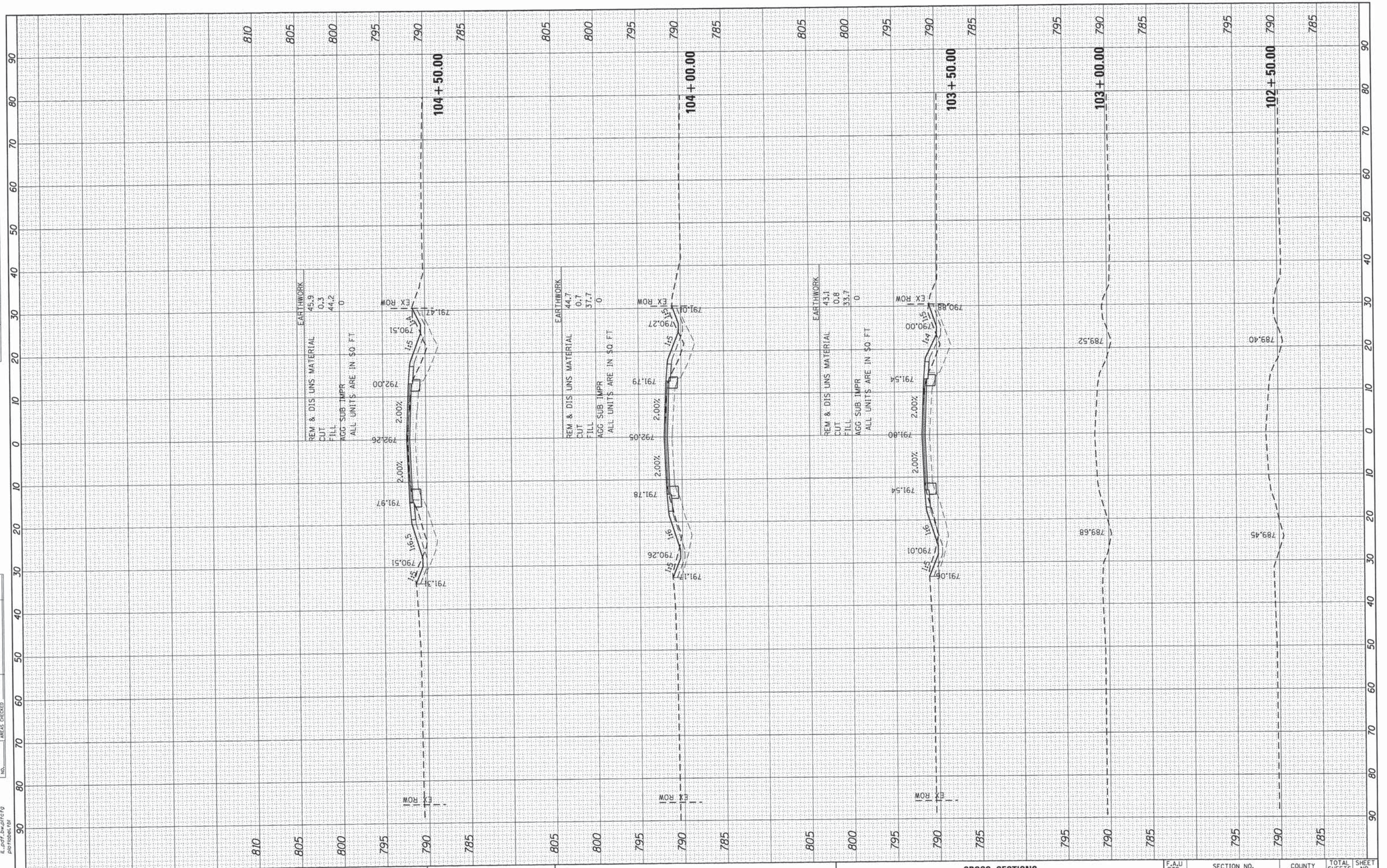
•FAU3860, FAU0084



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
NO.	AREAS CHECKED		

HRG PROJECT NO.: 6650375  
 HRG PROJ. CONTACT: JFS-epf-vest+bulv.dgn  
 FILE NUMBER: il\_coff\_bv01c1c19  
 PEN TABLE: p10tab01.tbl



**HRGreen.com**  
 Illinois Professional Design Firm  
 #184-01322

USER NAME = dstancz	DESIGNED - JS	REVISED -
PLLOT SCALE = 10Hx5V	DRAWN - HMH	REVISED -
PLLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

DESIGNED - JS	REVISED -
DRAWN - HMH	REVISED -
CHECKED -	REVISED -
DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
 BULL VALLEY ROAD**

SCALE: 10Hx5V    SHEET NO. 1 OF 12 SHEETS    STA. 102+50.00 TO STA. 104+50.00

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	48
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			











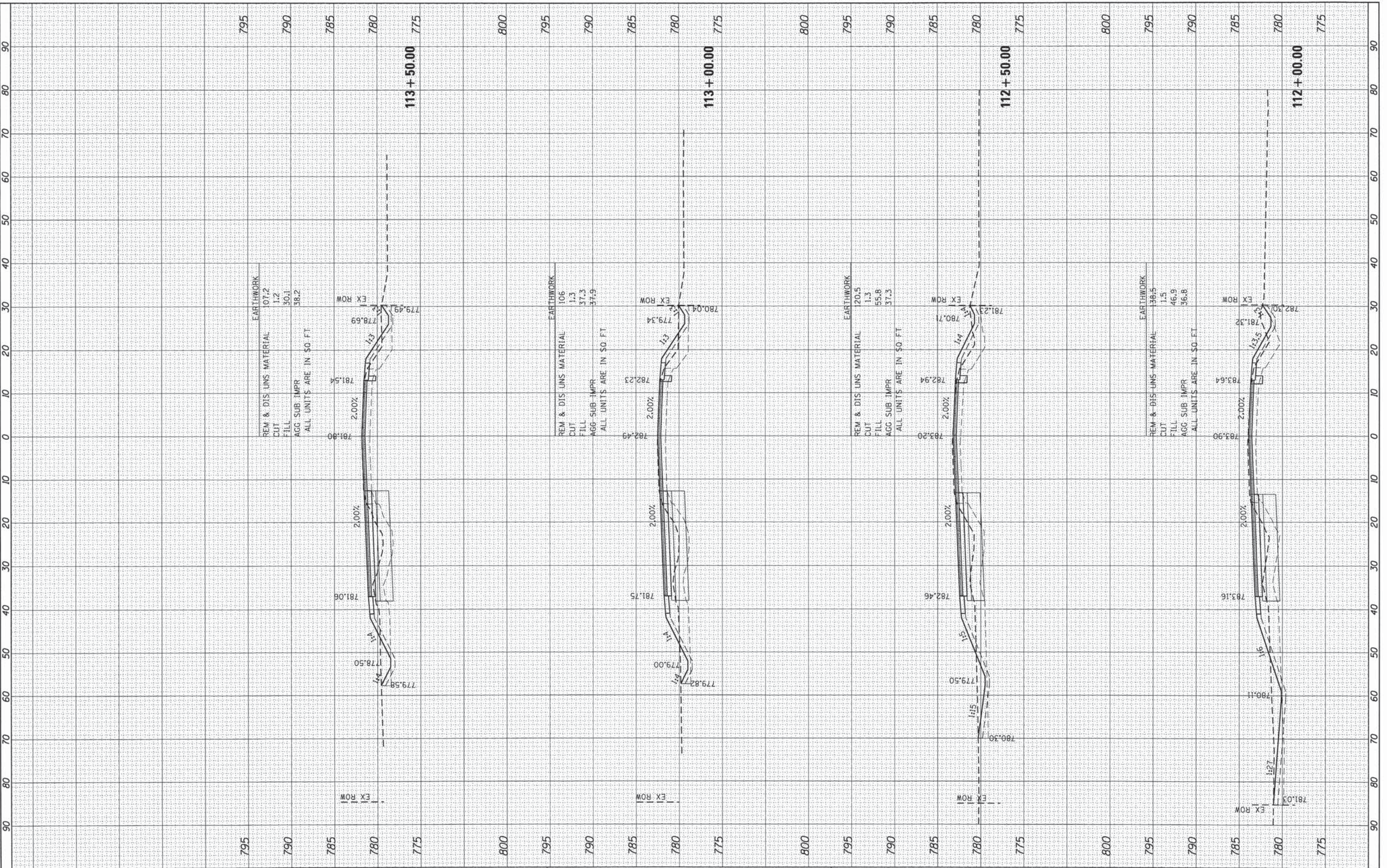




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NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

HRG PROJECT NO. 1 8620375  
 HRG PROJ. CONTACT: JTS-sht-vcash7-BullValley  
 FILE NAME: S:\proj\Drawings\CG  
 PLOT NUMBER: 610166759

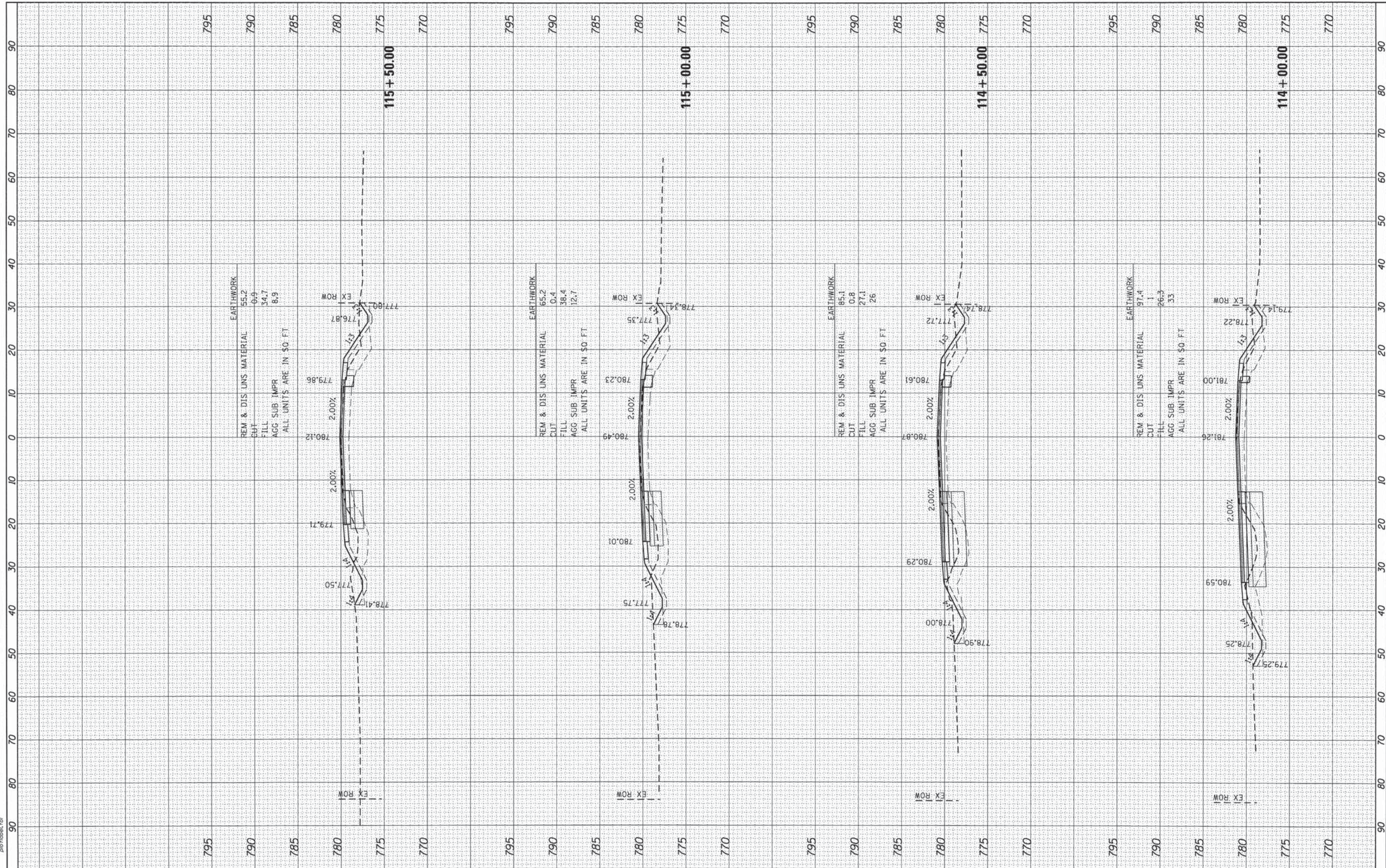




FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

HRG PROJECT NO.: 8630375  
 HRG PROJ CONTACT: 775.681.4647  
 PLOT DRIVER: d:\off\mccrory  
 PEN TABLE: p10tbltbl.tbl



REM & DIS UNS MATERIAL  
 CUT 55.2  
 FILL 0.9  
 AGG SUB IMPR 34.7  
 ALL UNITS ARE IN \$0. FT  
 8.9

REM & DIS UNS MATERIAL  
 CUT 65.2  
 FILL 0.4  
 AGG SUB IMPR 38.4  
 ALL UNITS ARE IN \$0. FT  
 12.7

REM & DIS UNS MATERIAL  
 CUT 85.1  
 FILL 0.8  
 AGG SUB IMPR 27.1  
 ALL UNITS ARE IN \$0. FT  
 26

REM & DIS UNS MATERIAL  
 CUT 97.4  
 FILL 1  
 AGG SUB IMPR 26.3  
 ALL UNITS ARE IN \$0. FT  
 33

USER NAME = dstanz	DESIGNED - JS	REVISED -
PLOT SCALE = 10H:5V	DRAWN - HMH	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCALE: 10H:5V	SHEET NO. 6 OF 12 SHEETS	STA. 114+00.00 TO STA. 115+50.00
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CROSS SECTIONS  
 BULL VALLEY ROAD

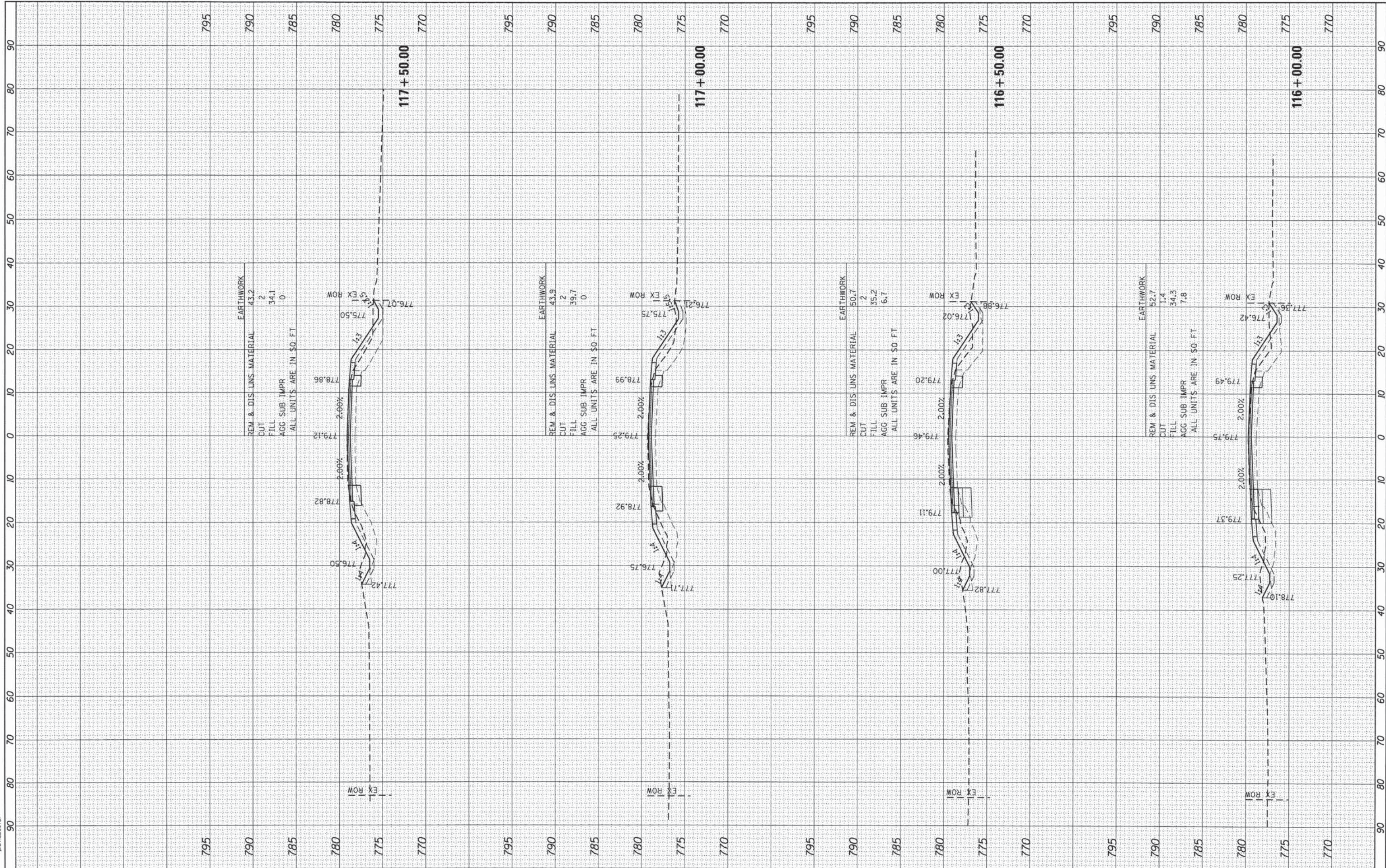
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	14-00078-00-CH	MCHENRY	59	53
	FAU3860, FAU0084		CONTRACT NO. 61C45	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



BY	DATE
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	

BY	DATE
SURVEYED	
PLOTTED	
NOTE BOOK	
AREAS CHECKED	

HRG PROJECT NO. 8620375  
 HRG PROJ CONTACT: JTS-ent-wash1-BulValley  
 FILE NAME: L:\proj\BulValley\p101066k.dwg  
 PLOT DRIVER: p101066k.tbl  
 PEN TABLE:



HRGreen.com
Illinois Professional Design Firm
#184-01332

USER NAME = dstancz
DESIGNED - JS
DRAWN - HMH
CHECKED -
DATE - 1/25/16
REVISIONS
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REVISIONS

PLLOT SCALE = 10H+5V
PLLOT DATE = 1/25/2016

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS</b>			
<b>BULL VALLEY ROAD</b>			
SCALE: 10H+5V	SHEET NO. 7 OF 12 SHEETS	STA. 116+00.00	TO STA. 117+50.00

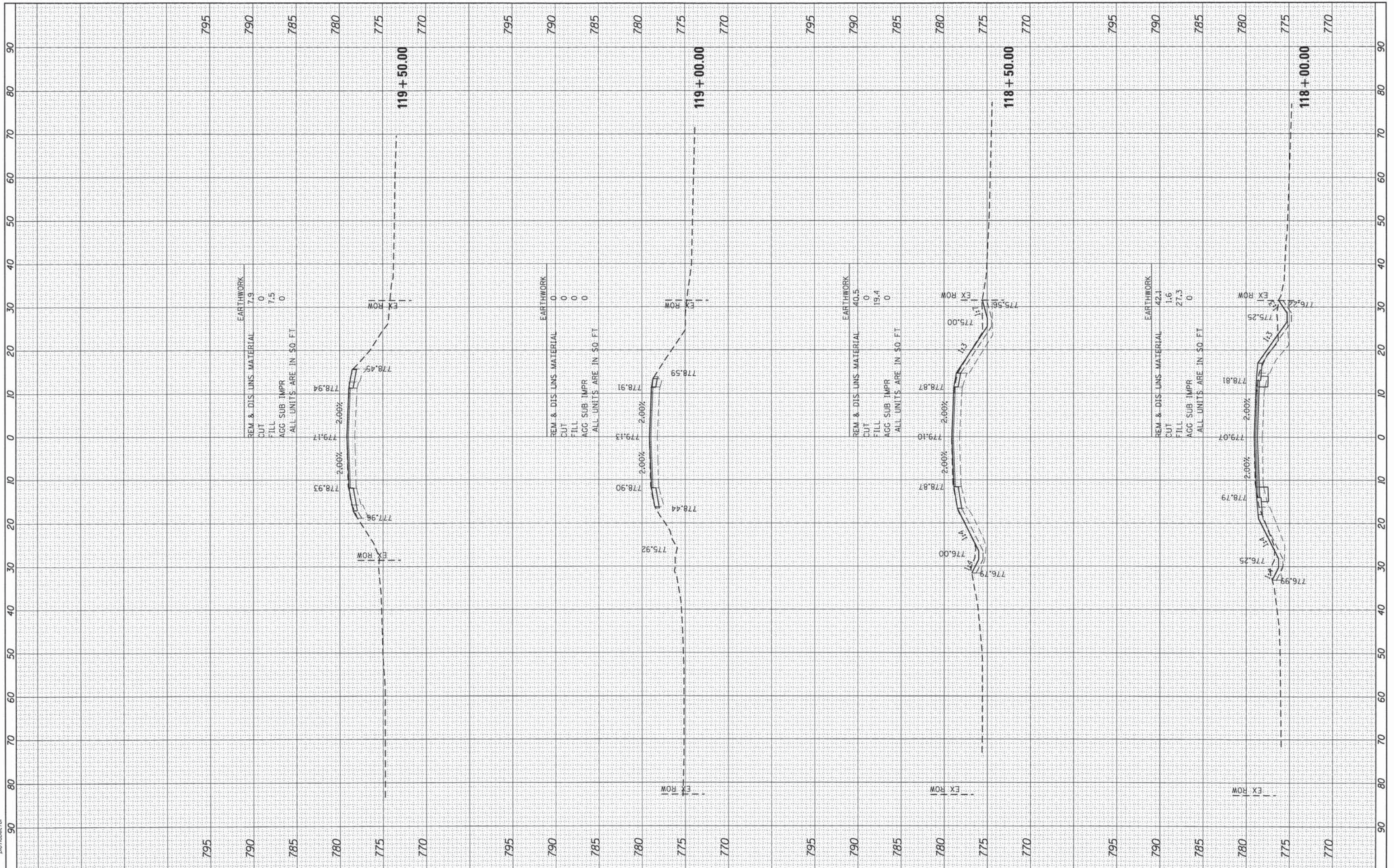
F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	54
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

HRC PROJECT NO. 6650375  
 HRC PROJECT CONTACT 375-871-5587-Bulk.dgn  
 FILE NAME: 118+00.ctb  
 PLOT DRIVER: plot.tbl  
 PEN TABLE:



USER NAME = dstancz	DESIGNED - JS	REVISED -
PLLOT SCALE = 10H:5V	DRAWN - HMH	REVISED -
PLLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS</b>			
<b>BULL VALLEY ROAD</b>			
SCALE: 10H:5V	SHEET NO. 8 OF 12 SHEETS	STA. 118+00.00 TO STA. 119+50.00	

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	55
•FAU3860, FAU0084				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
				CONTRACT NO. 61C45

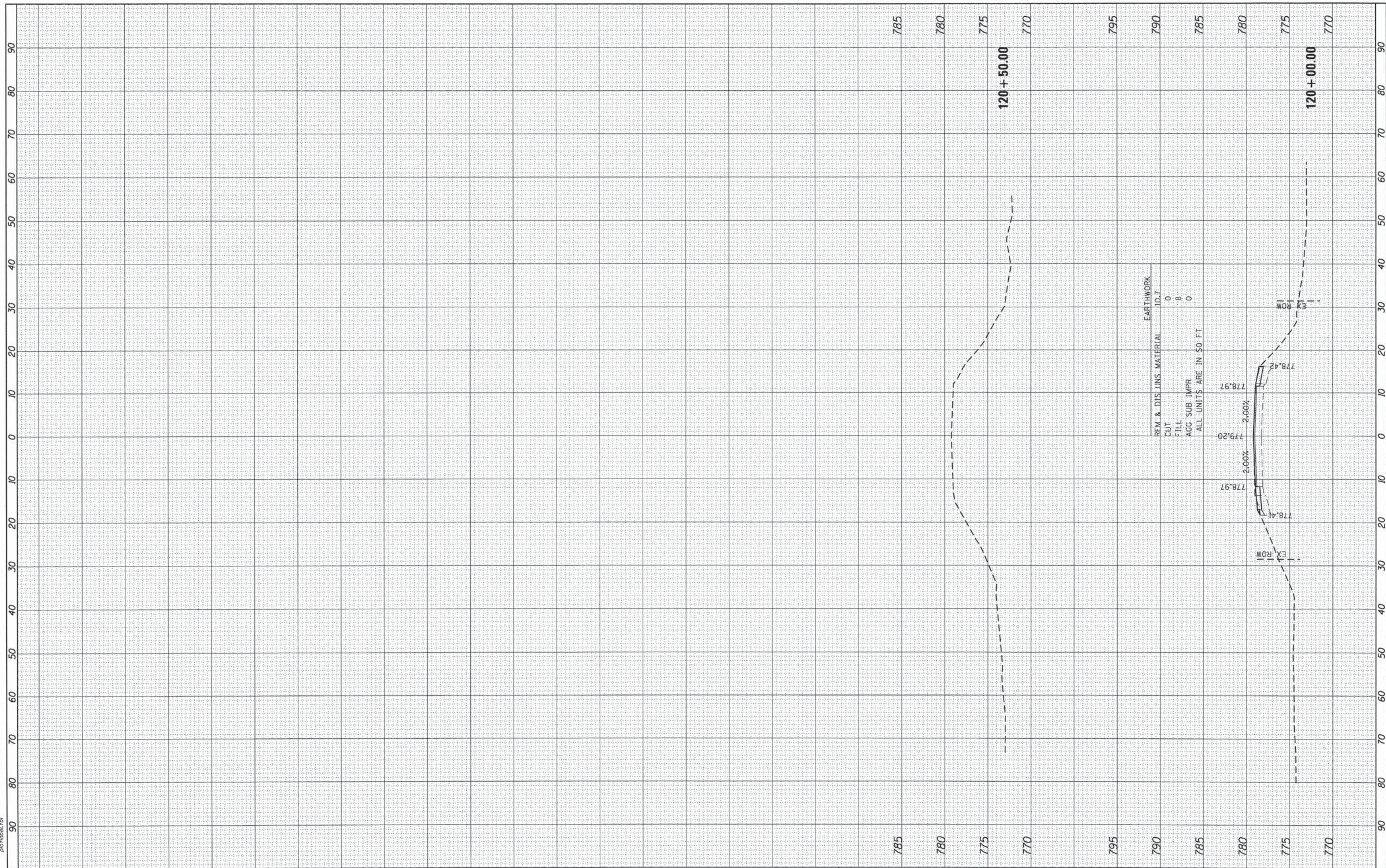




FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		

HRG PROJECT NO.: #630375  
 HRG PROJ CONTACT: JTS  
 FILE NAME: 14-00078-00-CH-9.dwg  
 PLOT NUMBER: 9  
 PER TABLE: 1



REM & DIS LINS. MATERIAL	10.7
EARTHWORK	0
CUT	8
FILL	0
AGG SUB. IMPR	0
ALL UNITS ARE IN SQ. FT.	

USER NAME = dstanz	DESIGNED - JS	REVISED -
	DRAWN - HMH	REVISED -
	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -
PLOT SCALE = 10H:5V		
PLOT DATE = 1/25/2016		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS BULL VALLEY ROAD</b>			
SCALE: 10H:5V	SHEET NO. 9 OF 12 SHEETS	STA. 120+00.00	TO STA. 120+50.00

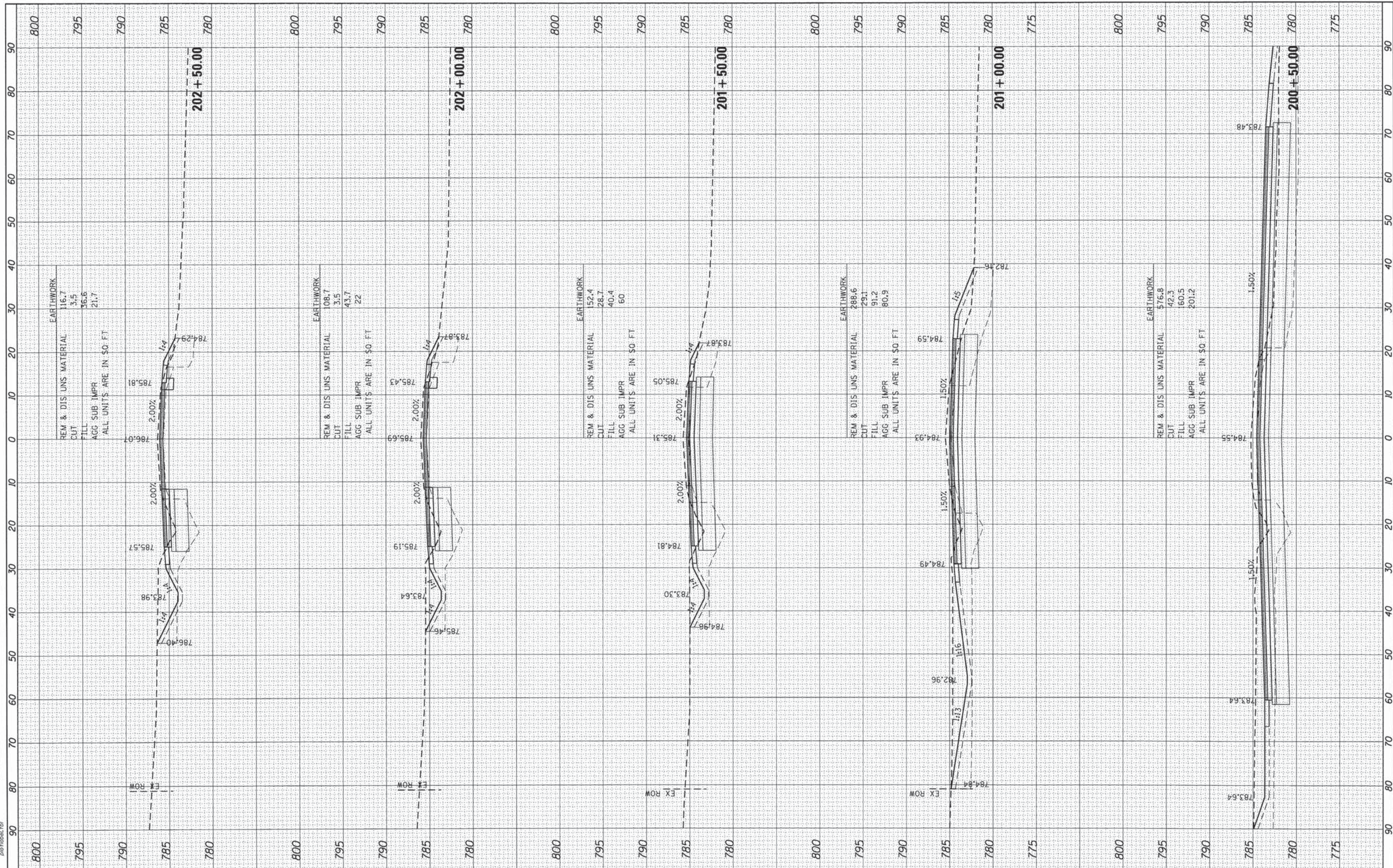
F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	56
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS		
	CHECKED		

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 HRG PROJ CONTACT: JTS-ahr-ssahh-curr.dgn  
 FILE NAME: 14-00078-00-CH-10.dwg  
 PLOT NUMBER: 10  
 PEN TABLE:



USER NAME =	distancz
DESIGNED -	JS
DRAWN -	HMH
CHECKED -	
DATE -	1/25/16
REVISED -	
REVISED -	
REVISED -	
REVISED -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

SCALE:	10H:5V
SHEET NO.	10 OF 12 SHEETS
STA.	200+50.00 TO STA. 202+50.00

**CROSS SECTIONS  
 CURRAN ROAD**

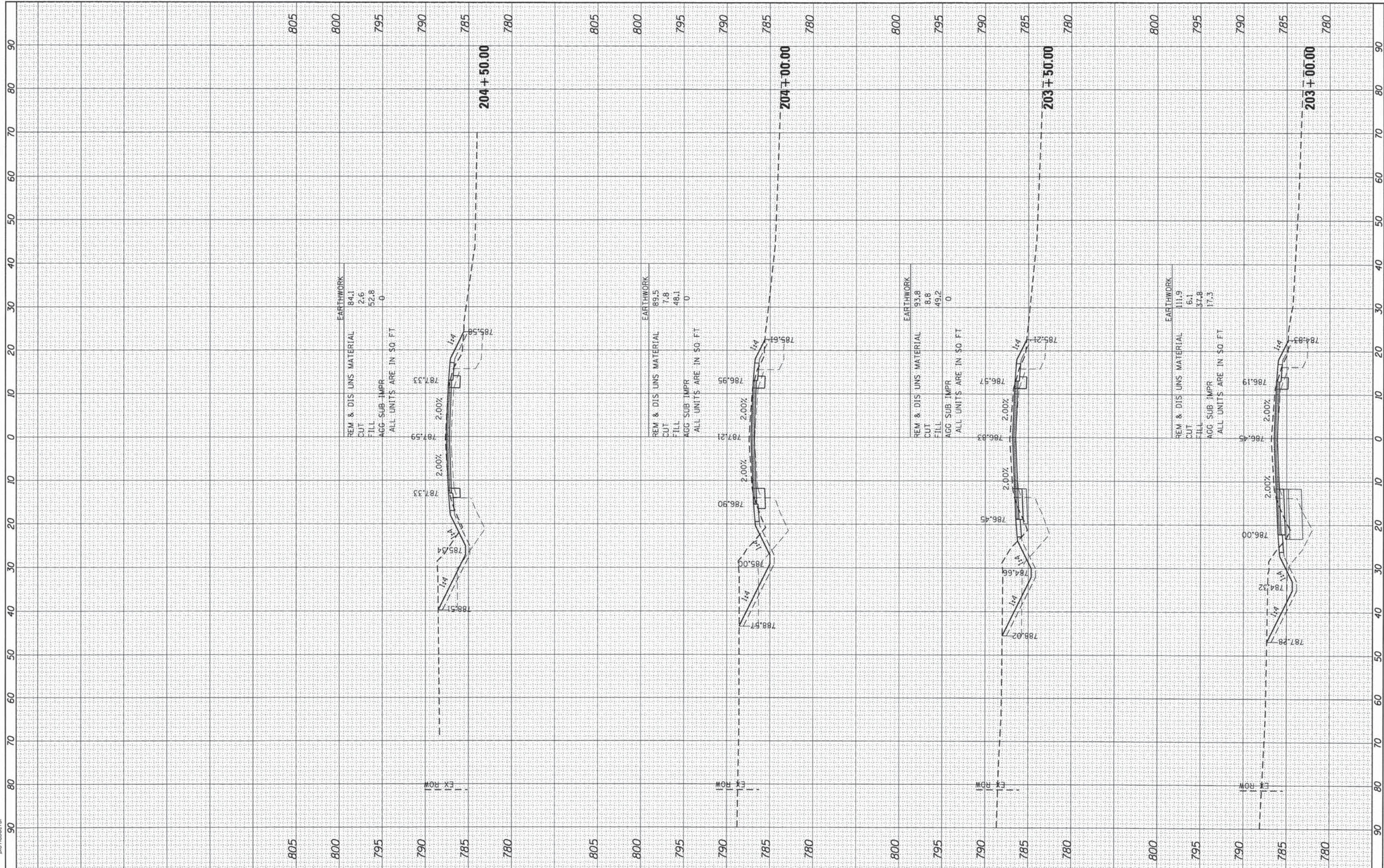
F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
*	14-00078-00-CH	MCHENRY	59	57
*FAU3860, FAU0084				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
				CONTRACT NO. 61C45



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

HRG PROJECT NO. 6650375  
 HRG PROJ. CONTACT: JTS-6650375-001-001.dgn  
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USER NAME = dstanz	DESIGNED - JS	REVISED -
PLOT SCALE = 10H:5V	DRAWN - HMH	REVISED -
PLOT DATE = 1/25/2016	CHECKED -	REVISED -
	DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
CURRAN ROAD**

SCALE: 10H:5V    SHEET NO. 11 OF 12 SHEETS    STA. 203+00.00 TO STA. 204+50.00

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	58
*FAU3860, FAU0084			CONTRACT NO. 61C45	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

HRC PROJECT NO. 86/0375  
 HRC PROJ. CONTACT 375-897-5587-Curr.dgn  
 FILE NAME: IL\_Pdf.dwg/1c1g  
 PLOT DRIVER: plottabel.tbl  
 PEN TABLE:



HRGreen.com Illinois Professional Design Firm # 184-001322	USER NAME = dstancz	DESIGNED - JS	REVISED -
	PLOT SCALE = 10H:5V	DRAWN - HMH	REVISED -
	PLOT DATE = 1/25/2016	CHECKED -	REVISED -
		DATE - 1/25/16	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS CURRAN ROAD</b>	
SCALE: 10H:5V	SHEET NO. 12 OF 12 SHEETS
STA. 205+00.00 TO STA. 205+00.00	

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
•	14-00078-00-CH	MCHENRY	59	59
*FAU3860, FAU0084		CONTRACT NO. 61C45		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		