04-22-2016 LETTING ITEM 158

F. A. U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	1
STA.	TO STA.			
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

CONTRACT #61C52

INDEX OF SHEETS

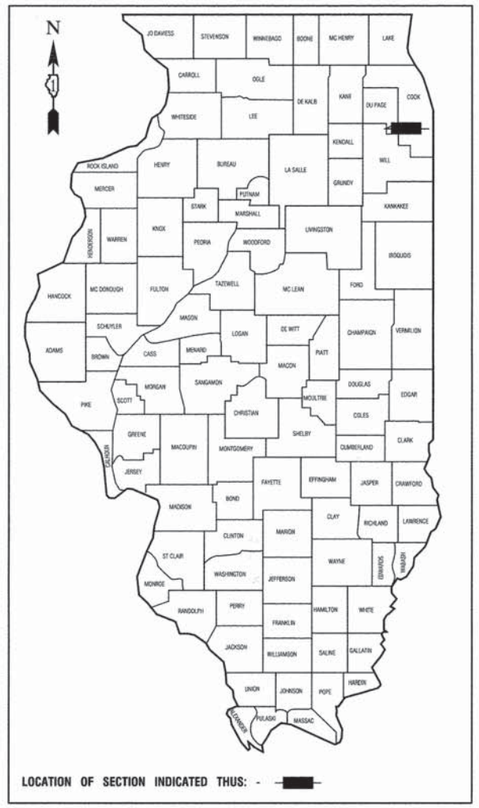
HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS
442201-03	CLASS C AND D PATCHES
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

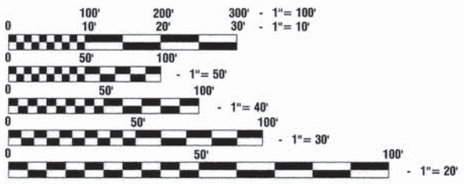
FAU 2796(122ND STREET)
CENTRAL AVENUE TO CICERO AVENUE
ROADWAY RESURFACING
SECTION NO.: 14-00098-00-RS
PROJECT NO.: M-4003(428)
VILLAGE *of* ALSIP
COOK COUNTY
JOB NO.: C-91-146-15



LOCATION OF SECTION INDICATED THUS: -

122ND STREET	
2016 ADT –	6,150
2040 ADT –	10,500
POSTED SPEED LIMIT –	25 mph
DESIGN PERIOD –	20 YEARS
DESIGN SPEED LIMIT –	30 mph
STREET CLASSIFICATION –	MAJOR COLLECTOR

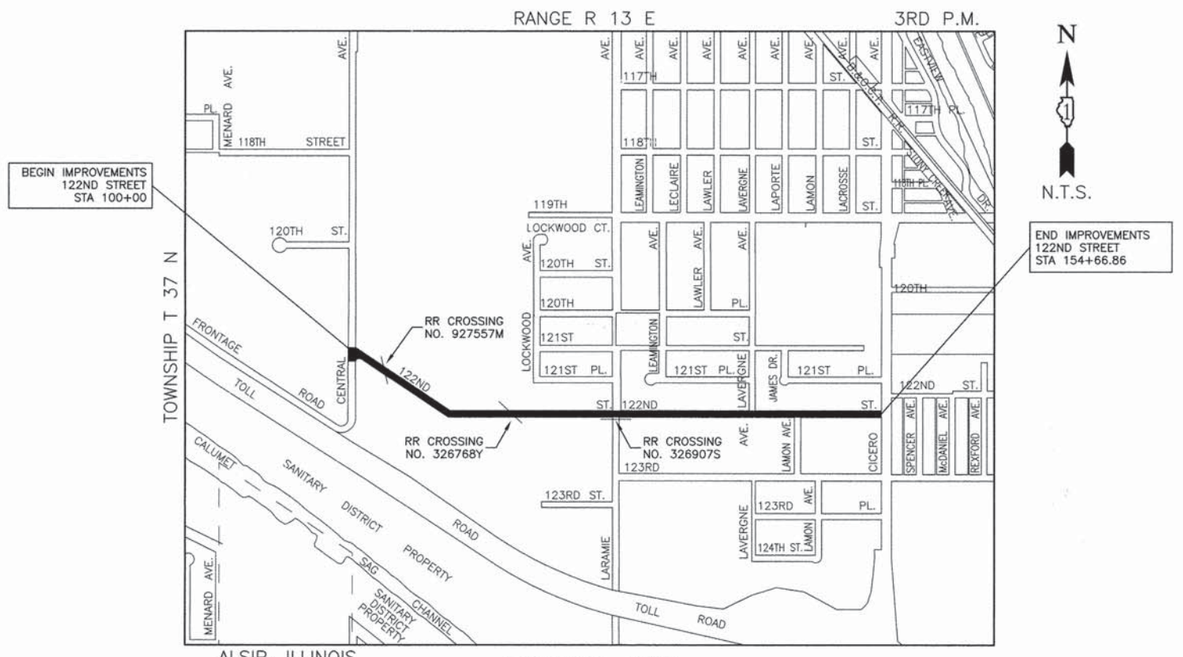
SCALES
- PLAN - 1"=50'
- PROFILE HORIZ. - 1"=50'
- PROFILE VERT. - 1"=5'
- CROSS SECTIONS - 1"=10'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1 - 800 - 892 - 0123 or 811

CONTRACT NO. 61C52



LOCATION MAP

GROSS LENGTH= 5,466.86 FEET= 1.03 MILES
NET LENGTH= 5,466.86 FEET= 1.03 MILES

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved JAN 18, 2016
Village of Alsip

Passed FEBRUARY 9, 2016
CHRISTOPHER HOLT
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review FEBRUARY 16, 2016
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

1/18/2016

WILLIAM P. DOLAN
62-57490
REGISTERED PROFESSIONAL ENGINEER
STATE OF ILLINOIS

LICENSE EXPIRES: 11/30/17

I.D.O.T. FEDERAL AID DESIGN ENGINEER: FAWAD AQUEEL, P.E., PTOE 847-705-4021, SCHAUMBURG, IL.
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

14647_02-COVR-01 - IDOT CO1

SUMMARY OF QUANTITIES

S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE ROADWAY 0005	CONSTRUCTION TYPE CODE SAFETY 0021
	20200100	EARTH EXCAVATION	CU YD	17	17	
	21101625	TOPSOIL FURNISH AND PLACE, 6"	SQ YD	726	726	
	25000210	SEEDING, CLASS 2A	ACRE	0.15	0.15	
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	50	50	
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	50	50	
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	50	50	
	25003210	INTERSEEDING, CLASS 2A	ACRE	0.4	0.4	
	35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	247		247
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	10039	10039	
	40600825	POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50	TON	937	937	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	149	149	
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	1874	1874	
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	20	20	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2046		2046
	42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	419		419
	42400800	DETECTABLE WARNINGS	SQ FT	81		81
	44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	22309	22309	
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	168	168	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2068	2068	
	44000600	SIDEWALK REMOVAL	SQ FT	285		285
	44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	282	282	
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	34	34	
	60255500	MANHOLES TO BE ADJUSTED	EACH	14	14	
	67100100	MOBILIZATION	L SUM	1	1	

* - INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES

S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	CONSTRUCTION TYPE CODE ROADWAY 0005	CONSTRUCTION TYPE CODE SAFETY 0021
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1	
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1150		1150
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	384		384
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	310		310
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10452		10452
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	133		133
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	264		264
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	80		80
	Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	102	102	
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52		52
	Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	
	X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	1	1	
	X6061815	COMBINATION CONCRETE CURB AND GUTTER, TYPE M (SPECIAL)	FOOT	2068	2068	

* - INDICATES SPECIALTY ITEMS

GENERAL NOTES

1. THE ROBINSON ENGINEERING, LTD. FIELD OFFICE (708-331-6700), AND THE STREET SUPERINTENDENT, MIKE FRAIDER, AT THE VILLAGE OF ALSIP (708-385-6902 EXT. 415) SHALL BE NOTIFIED TWO (2) WORKING DAYS BEFORE CONSTRUCTION BEGINS.
2. BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
3. UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTOR'S USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
4. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
5. THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
6. ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS WELL AS TEMPORARY AGGREGATE. ANY TEMPORARY AGGREGATE REQUIRED SHALL BE CONSIDERED INCLUDED IN THE COST OF THE RELATED PAY ITEM IT IS NEEDED FOR WHEN DIRECTED BY THE ENGINEER.
7. THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, PAVEMENT, CURB, SIDEWALK, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. SAW CUTTING WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL ITEMS.
8. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
9. THE CONTRACTOR SHALL LEAVE ANY CLEAN EXCESS ORGANIC FILL EXCAVATED DURING THE CURB AND GUTTER AND SIDEWALK REMOVAL AND REPLACEMENT OPERATIONS ON SITE. ANY EXCESS MATERIAL SHALL BE SPREAD OR PLACED AT LOCATIONS DETERMINED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS REMOVAL AND REPLACEMENT ITEMS. RESTORATION OF AREAS WHERE EXCESS MATERIALS IS PLACED SHALL BE PAID FOR AS SEEDING, CLASS 2A.
10. CLASS D PATCHING QUANTITIES FOR THIS CONTRACT SHALL BE PERFORMED AT THE DIRECTION OF THE ENGINEER AFTER PAVEMENT MILLING.
11. CONTRACTOR SHALL RESTORE DAMAGED TURF AREA DUE TO REMOVAL AND REPLACEMENT OF VARIOUS ITEMS ALONG THE PARKWAY WITH TOPSOIL FURNISH AND PLACE, 6" AND SEEDING, CLASS 2A AS DIRECTED BY THE ENGINEER (SEE SPECIAL PROVISIONS). INTERSEEDING, CLASS 2A SHALL BE USED TO REPAIR EXISTING BARREN TURF AREA IN THE PARKWAY AT LOCATIONS INDICATED ON THE PLANS.

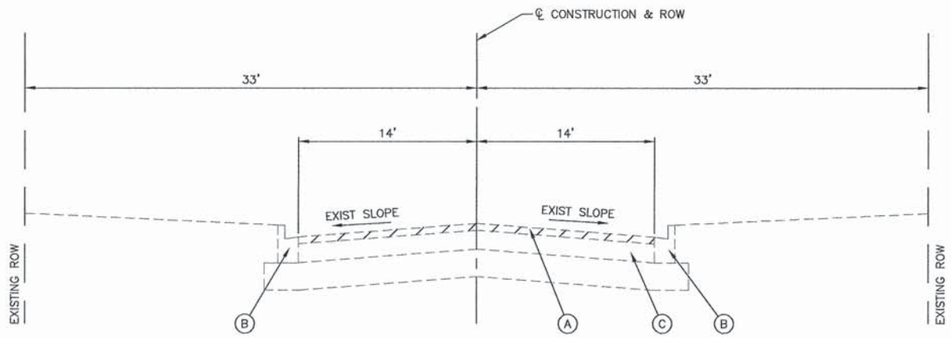
FILE NAME = 14647_02-QUAN-01 - IDOT P01	USER NAME =	DESIGNED - --	REVISED -
		CHECKED - WD	REVISED -
	PLOT SCALE =	DRAWN - RG	REVISED -
	PLOT DATE = 1-18-16	CHECKED - AG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAU 2796 (122ND STREET)
ROADWAY RESURFACING
SUMMARY OF QUANTITIES & GENERAL NOTES

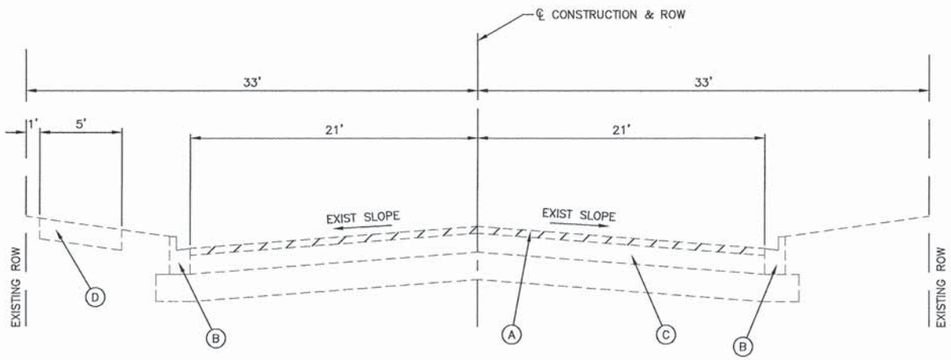
SCALE: NONE | SHEET NO. 2 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	2
		CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



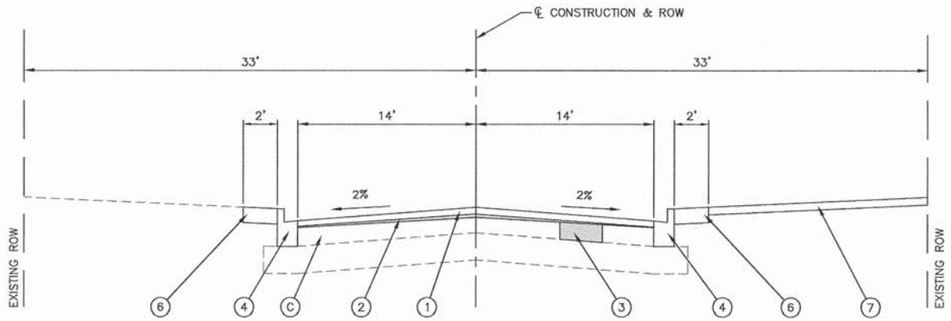
EXISTING TYPICAL SECTION

122ND STREET
CENTRAL AVENUE TO LARAMIE AVENUE



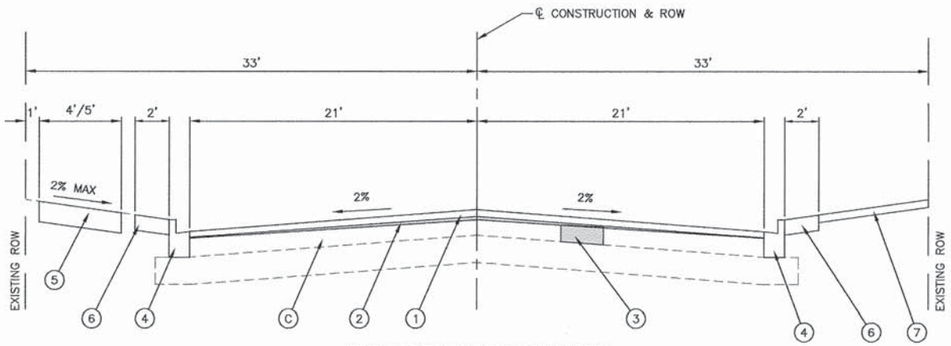
EXISTING TYPICAL SECTION

122ND STREET
LARAMIE AVENUE TO CICERO AVENUE
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING



PROPOSED TYPICAL SECTION

122ND STREET
CENTRAL AVENUE TO LARAMIE AVENUE



PROPOSED TYPICAL SECTION

122ND STREET
LARAMIE AVENUE TO CICERO AVENUE
NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

EXISTING LEGEND

- (A) HOT MIX ASPHALT SURFACE REMOVAL, 2"
- (B) EXISTING CURB & GUTTER, TYPE M, TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (C) EXISTING HOT-MIX ASPHALT PAVEMENT
- (D) EXISTING PCC SIDEWALK TO BE REMOVED AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER

PROPOSED LEGEND

- (1) HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50, 3/4"
- (3) CLASS D PATCH, 10", AT LOCATIONS SHOWN ON PLANS OR DIRECTED BY ENGINEER
- (4) PROPOSED CURB AND GUTTER, TYPE M (SPECIAL), TO BE INSTALLED AT LOCATIONS SHOWN ON PLAN OR DIRECTED BY ENGINEER (IN KIND)
- (5) PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK 5" (REPLACEMENT AT LOCATIONS DIRECTED BY THE ENGINEER)
- (6) TOPSOIL FURNISH AND PLACE, 6" AND SEEDING, CLASS 2A AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER
- (7) INTERSEEDING, CLASS 2A AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER

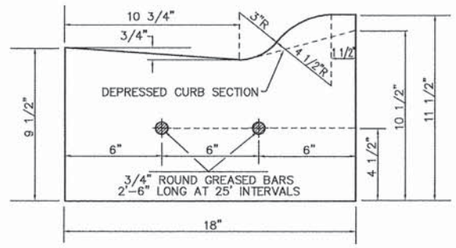
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

(CONTRACTOR SHALL MILL BEFORE PATCHING)

MIXTURE TYPE	AIR VOIDS @ Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, N70, 1-1/2"	4% @ 70 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), N50, 3/4"	3.5% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, (HMA BINDER IL-19.0mm): 10" (IN 3 LIFTS)	4% @ 70 Gyr.
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5 MM), 2"	4% @ 50 Gyr.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm); PE-6"	4% @ 50 Gyr.

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN. FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
3. CLASS D PATCHES, AT APPROXIMATE STATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.



NOTE :
FORM SIZES TO BE USED 2"x10" IN FRONT AND 2"x12" IN BACK. ANY UNDERCUT BENEATH THE CURB SHALL BE BROUGHT UP TO GRADE WITH SAND OR STONE SCREENINGS, THE COST OF WHICH SHALL BE BORNE BY THE CONTRACTOR.

TYPE M CURB AND GUTTER DETAIL

FILE NAME = 14647_02-TYPX-01 - IDOT P01	USER NAME =	DESIGNED - -	REVISED -
		CHECKED - WD	REVISED -
	PLOT SCALE =	DRAWN - RG	REVISED -
	PLOT DATE = 1-18-16	CHECKED - LTL	REVISED -

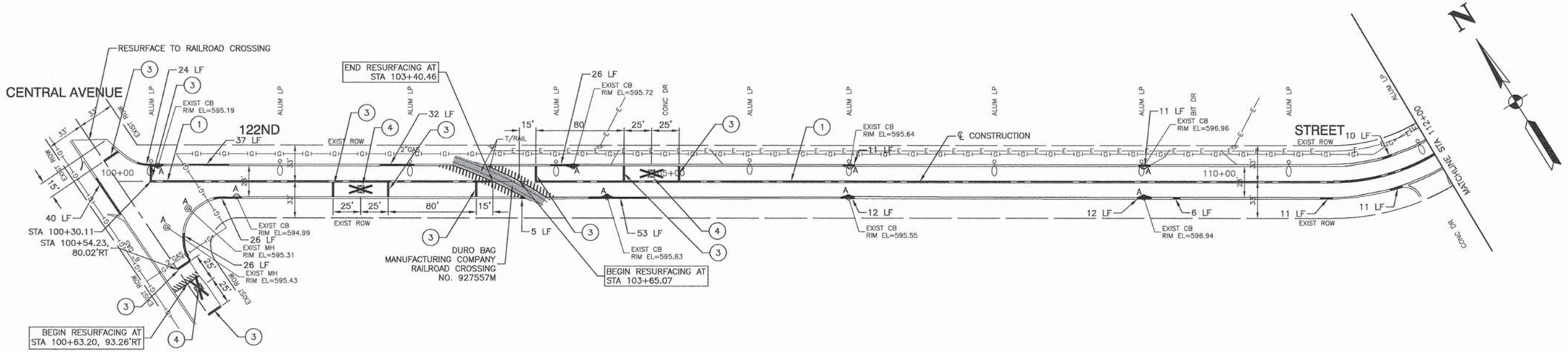
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

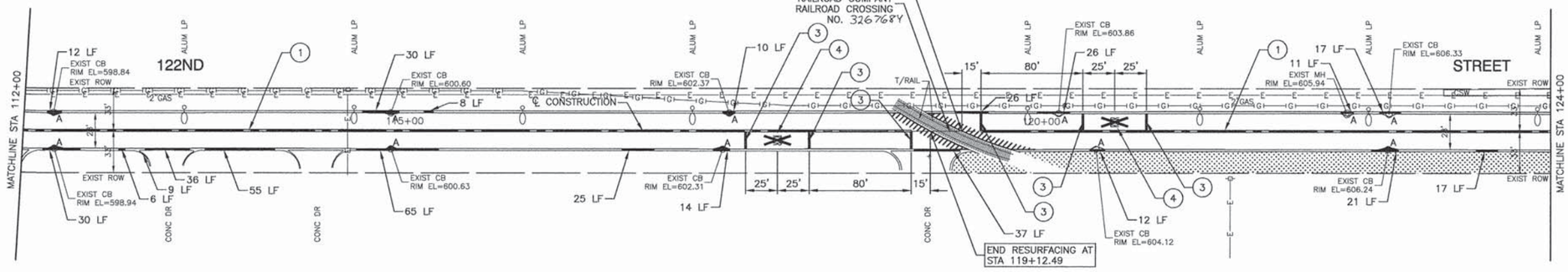
FAU 2796 (122ND STREET)
ROADWAY RESURFACING
TYPICAL CROSS SECTIONS

SCALE: NONE | SHEET NO. 3 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	3
		CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

SECTION 28, TOWNSHIP 37, RANGE 13





LEGEND

(1)	4" DOUBLE YELLOW LINE (11" C–C)
(2)	6" WHITE CROSSWALK LINE
(3)	24" WHITE STOP BAR
(4)	WHITE LETTERS & SYMBOLS
A	STRUCTURE TO BE ADJUSTED
	BUTT JOINT
	CURB AND GUTTER REMOVAL & REPLACEMENT
	INTERSEEDING, CLASS 2A
	PROPOSED SIDEWALK
	HMA DRIVEWAY REMOVAL & REPLACEMENT
	CLASS D PATCHES, TYPE IV, 10"
	SIDEWALK REMOVAL & REPLACEMENT

NOTES:

1. PROPOSED SIDEWALK THROUGH DRIVEWAYS SHALL BE PORTLAND CEMENT CONCRETE SIDEWALK, 7 INCH, AND PAID AS SUCH. EXISTING DRIVEWAY WILL BE REMOVED AND BE INCLUDED IN THE DRIVEWAY REMOVAL PAY ITEM.
2. ANY SIDEWALK OR DRIVEWAY REMOVAL AND REPLACEMENT UP TO 2' BEHIND CURB AND GUTTER REMOVAL AND REPLACEMENT, SHALL BE INCLUDED IN THE VARIOUS REMOVAL AND REPLACEMENT ITEMS.
3. TOPSOIL FURNISH AND PLACE, 6" AND SEEDING, CLASS 2A SHALL BE USED FOR RESTORATION UP TO 2' WIDTH BEHIND CURB AND GUTTER, SIDEWALK, AND DRIVEWAY REMOVAL AND REPLACEMENT AS DIRECTED BY THE ENGINEER OR AS INDICATED ON THE PLANS (SEE SPECIAL PROVISIONS).

FILE NAME = 14647_02-PLAN-01 - IDOT P1	USER NAME =	DESIGNED — --	REVISED —
		CHECKED — WD	REVISED —
	PLOT SCALE =	DRAWN — RG	REVISED —
	PLOT DATE = 1-18-16	CHECKED — LTL	REVISED —

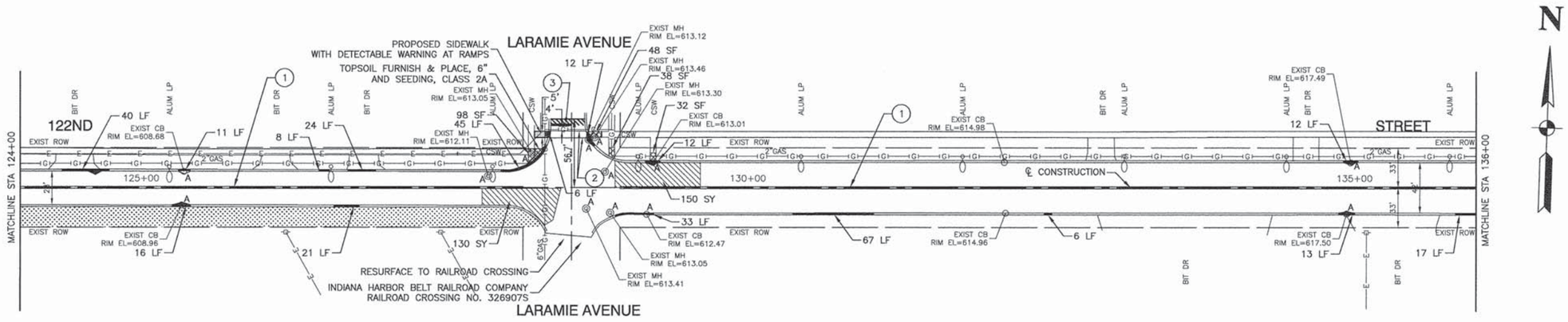
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

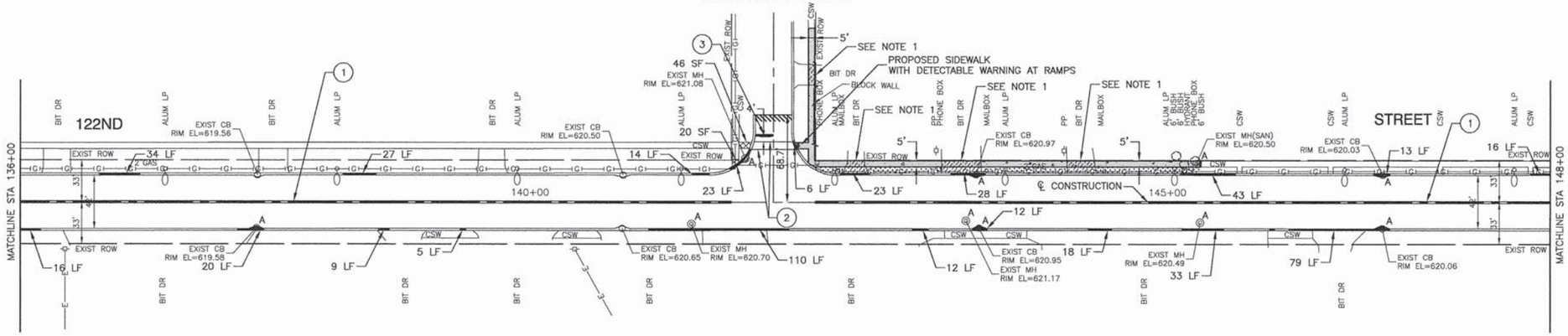
FAU 2796 (122ND STREET)
ROADWAY RESURFACING
PAVEMENT PLAN & PAVEMENT MARKING PLAN

SCALE: 1"=50' | SHEET NO. 4 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	4
		CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

SECTION 28, TOWNSHIP 37, RANGE 13





NOTES:

1. PROPOSED SIDEWALK THROUGH DRIVEWAYS SHALL BE PORTLAND CEMENT CONCRETE SIDEWALK, 7 INCH, AND PAID AS SUCH. EXISTING DRIVEWAY WILL BE REMOVED AND BE INCLUDED IN THE DRIVEWAY REMOVAL PAY ITEM.
2. ANY SIDEWALK OR DRIVEWAY REMOVAL AND REPLACEMENT UP TO 2' BEHIND CURB AND GUTTER REMOVAL AND REPLACEMENT, SHALL BE INCLUDED IN THE VARIOUS REMOVAL AND REPLACEMENT ITEMS.
3. TOPSOIL FURNISH AND PLACE, 6" AND SEEDING, CLASS 2A SHALL BE USED FOR RESTORATION UP TO 2' WIDTH BEHIND CURB AND GUTTER, SIDEWALK, AND DRIVEWAY REMOVAL AND REPLACEMENT AS DIRECTED BY THE ENGINEER OR AS INDICATED ON THE PLANS (SEE SPECIAL PROVISIONS).

LEGEND

1	4" DOUBLE YELLOW LINE (11" C-C)
2	6" WHITE CROSSWALK LINE
3	24" WHITE STOP BAR
4	WHITE LETTERS & SYMBOLS
A	STRUCTURE TO BE ADJUSTED
	BUTT JOINT
	CURB AND GUTTER REMOVAL & REPLACEMENT
	INTERSEEDING, CLASS 2A
	PROPOSED SIDEWALK
	HMA DRIVEWAY REMOVAL & REPLACEMENT
	CLASS D PATCHES, TYPE IV, 10"
	SIDEWALK REMOVAL & REPLACEMENT

FILE NAME = 14647_02-PLAN-01 - IDOT P2	USER NAME =	DESIGNED - -	REVISED -
		CHECKED - WD	REVISED -
	PLOT SCALE =	DRAWN - MED	REVISED -
	PLOT DATE = 1-18-16	CHECKED - AG	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

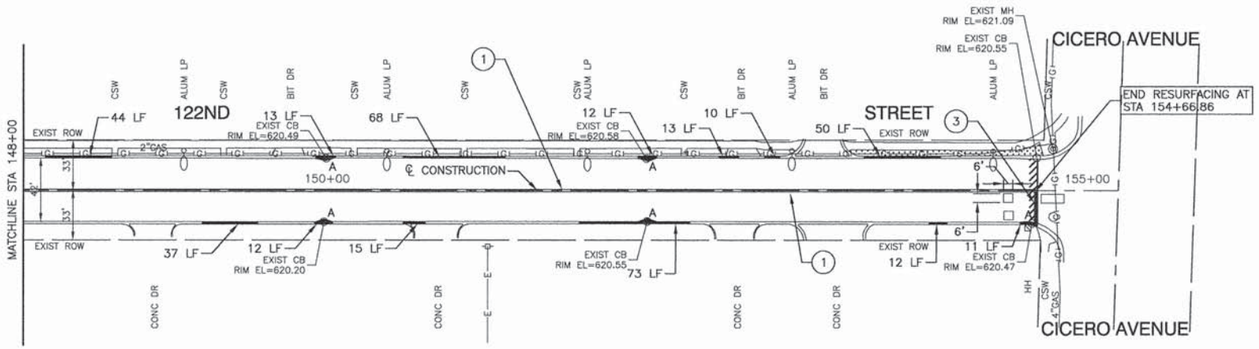
CENTRAL AVENUE TO CICERO AVENUE
122ND STREET
PAVEMENT PLAN & PAVEMENT MARKING PLAN

SCALE: 1"=50' | SHEET NO. 5 OF 14 SHEETS | STA. 124+00 TO STA. 148+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	5
		CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

SECTION 28, TOWNSHIP 37, RANGE 13

N



NOTES:

1. PROPOSED SIDEWALK THROUGH DRIVEWAYS SHALL BE PORTLAND CEMENT CONCRETE SIDEWALK, 7 INCH, AND PAID AS SUCH. EXISTING DRIVEWAY WILL BE REMOVED AND BE INCLUDED IN THE DRIVEWAY REMOVAL PAY ITEM.
2. ANY SIDEWALK OR DRIVEWAY REMOVAL AND REPLACEMENT UP TO 2' BEHIND CURB AND GUTTER REMOVAL AND REPLACEMENT, SHALL BE INCLUDED IN THE VARIOUS REMOVAL AND REPLACEMENT ITEMS.
3. TOPSOIL FURNISH AND PLACE, 6" AND SEEDING, CLASS 2A SHALL BE USED FOR RESTORATION UP TO 2' WIDTH BEHIND CURB AND GUTTER, SIDEWALK, AND DRIVEWAY REMOVAL AND REPLACEMENT AS DIRECTED BY THE ENGINEER OR AS INDICATED ON THE PLANS (SEE SPECIAL PROVISIONS).

LEGEND

(1)	4" DOUBLE YELLOW LINE (11" C–C)
(2)	6" WHITE CROSSWALK LINE
(3)	24" WHITE STOP BAR
(4)	WHITE LETTERS & SYMBOLS
A	STRUCTURE TO BE ADJUSTED
	BUTT JOINT
	CURB AND GUTTER REMOVAL & REPLACEMENT
	INTERSEEDING, CLASS 2A
	PROPOSED SIDEWALK
	HMA DRIVEWAY REMOVAL & REPLACEMENT
	CLASS D PATCHES, TYPE IV, 10"
	SIDEWALK REMOVAL & REPLACEMENT

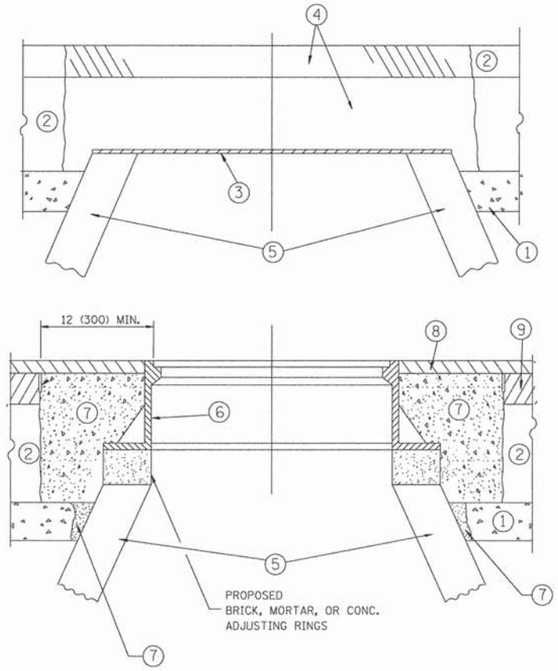
FILE NAME = 14647_02-PLAN-01 - IDOT P3	USER NAME =	DESIGNED — --	REVISED —
		CHECKED — WD	REVISED —
	PLOT SCALE =	DRAWN — MED	REVISED —
	PLOT DATE = 1-18-16	CHECKED — AG	REVISED —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CENTRAL AVENUE TO CICERO AVENUE
122ND STREET
PAVEMENT PLAN & PAVEMENT MARKING PLAN

SCALE: 1"=50' | SHEET NO. 6 OF 14 SHEETS | STA. 148+00 TO STA. 155+00

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	6
		CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

(1) SUB-BASE GRANULAR MATERIAL
(2) EXISTING PAVEMENT
(3) 36 (900) DIAMETER METAL PLATE
(4) PROPOSED CRUSHED STONE AND HMA SURFACE MIX
(5) EXISTING STRUCTURE
(6) FRAME AND LID (SEE NOTES)
(7) CLASS PP-1* CONCRETE
(8) PROPOSED HMA SURFACE COURSE
(9) PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

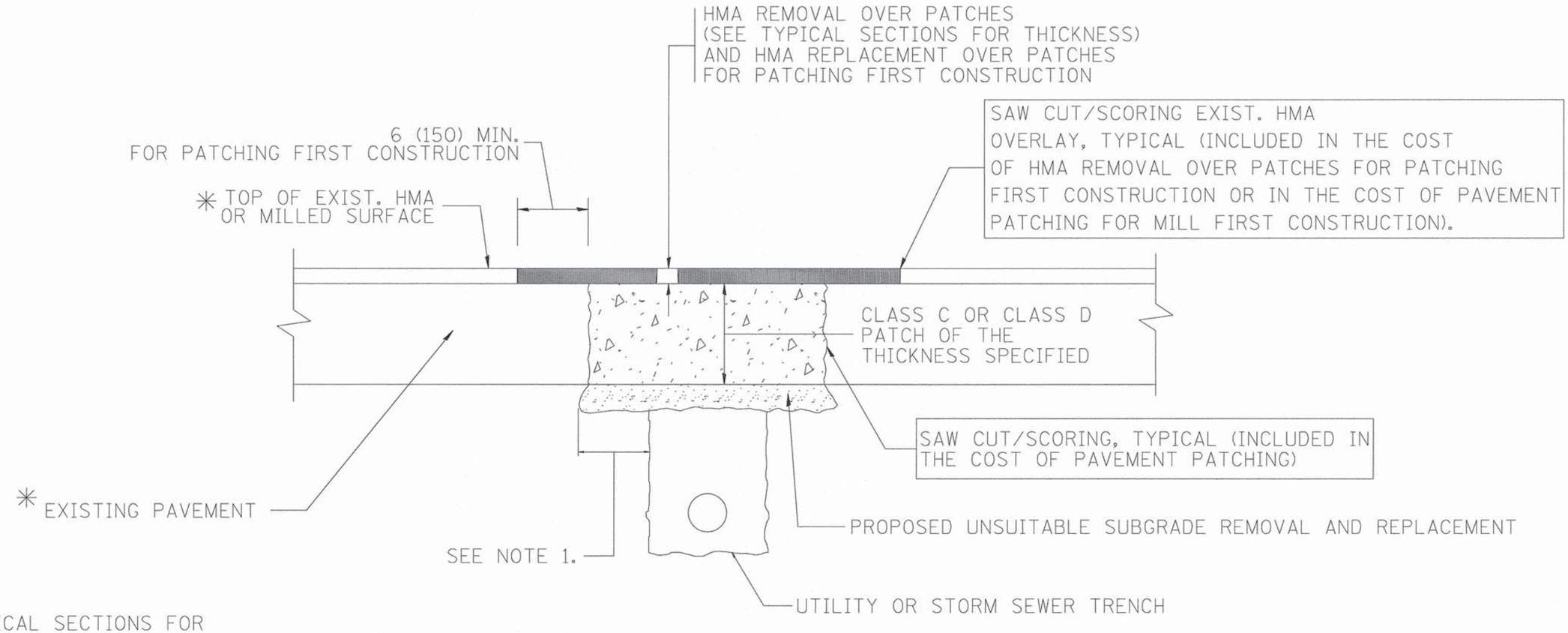
FILE NAME = c:\pw_work\pwidot\bauerdl\d0108315\bd08.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 1968.5000 ' / m	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 7 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	7
	BD600-03 (BD-8)	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

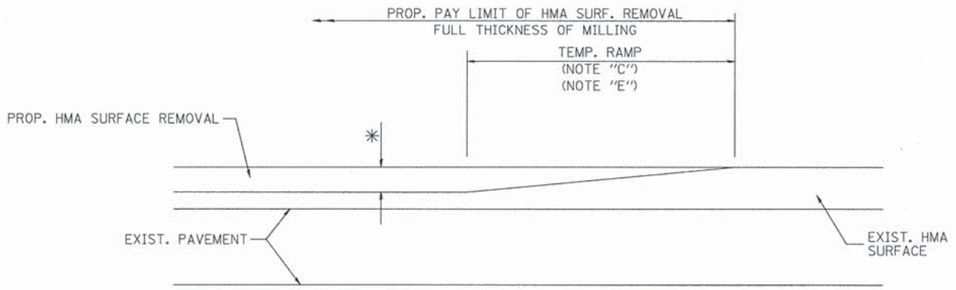
FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.000 '/ IN.	CHECKED -	REVISED - R. BORO 09-04-07
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE | SHEET NO. 8 OF 14 SHEETS | STA. | TO STA.

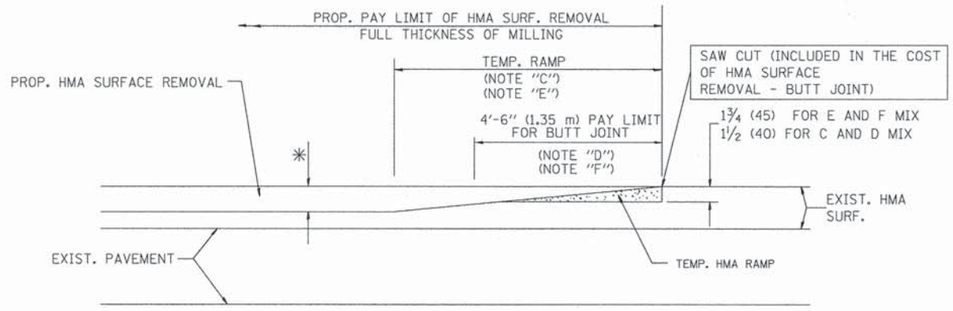
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	8
	BD400-04 (BD-22)	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

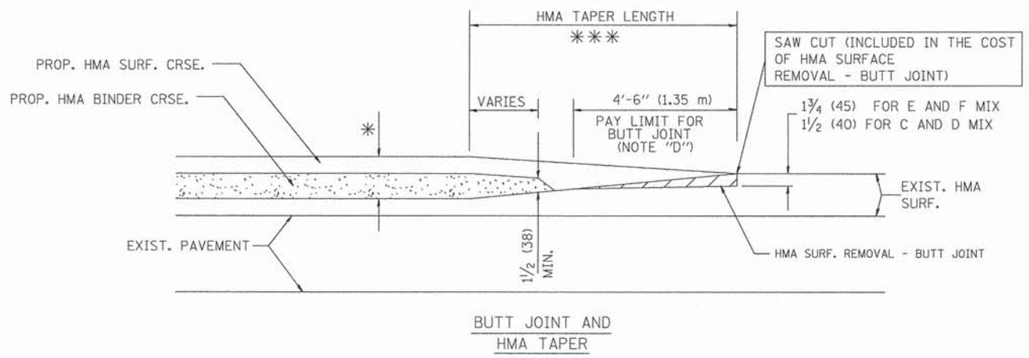


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

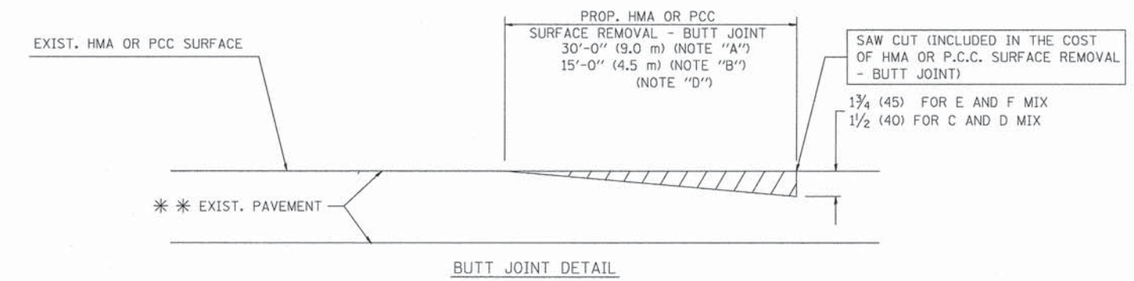
OPTION 2

TYPICAL TEMPORARY RAMP

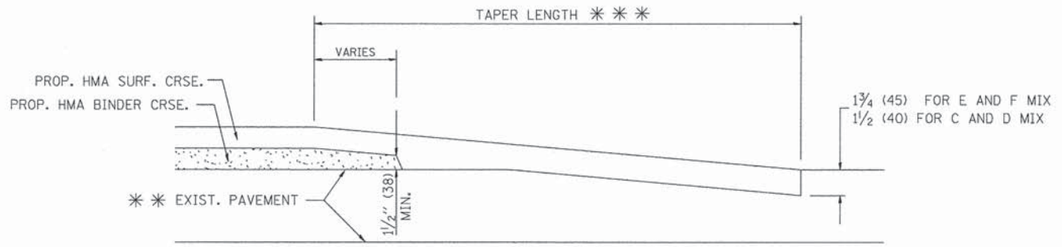


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

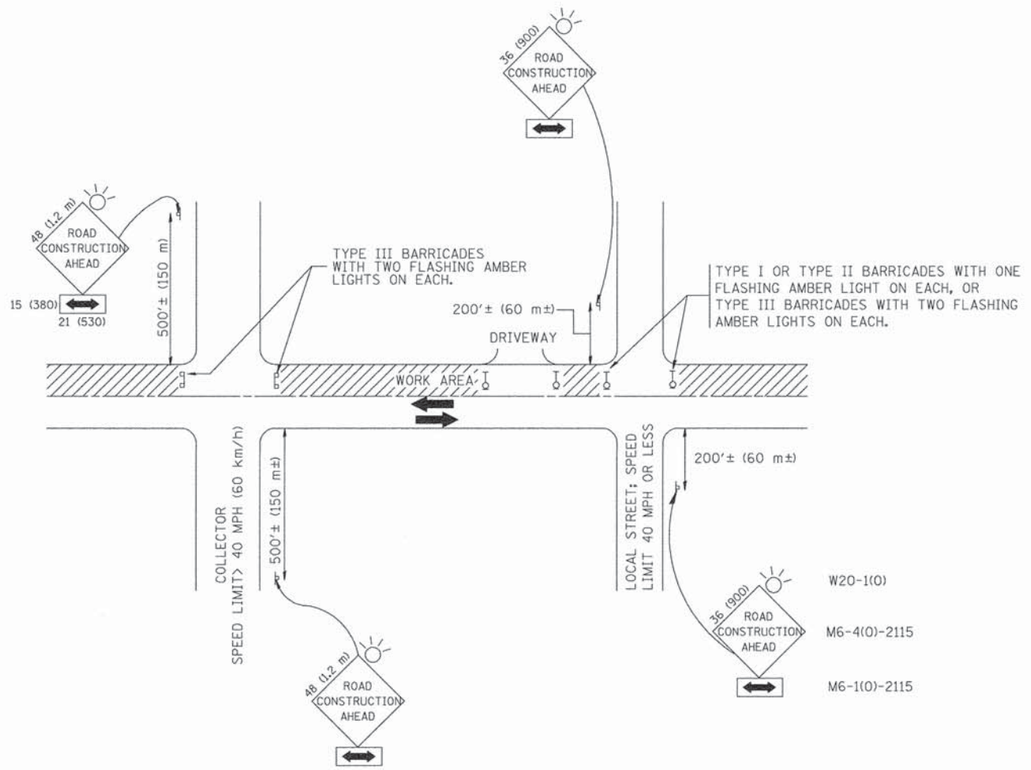
FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gaglianobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 9 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	9
	BD400-05 BD32	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

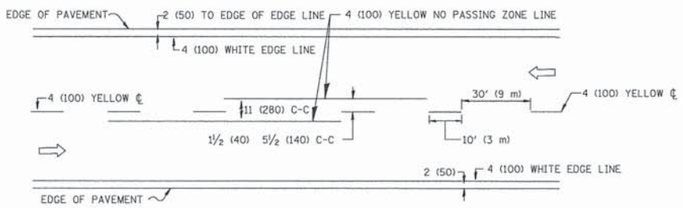
FILE NAME = W:\diststd\22x34\tc10.dgn	USER NAME = gaglenobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

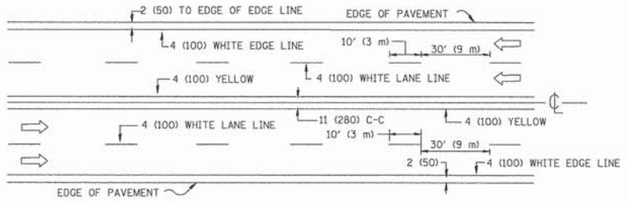
DISTRICT ONE
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET NO. 10 OF 14 SHEETS | STA. | TO STA.

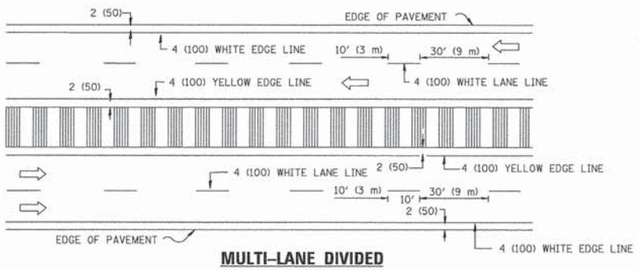
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	10
	TC-10	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



2–LANE ROADWAY

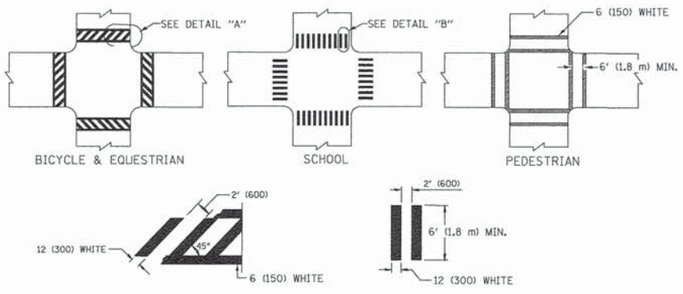


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

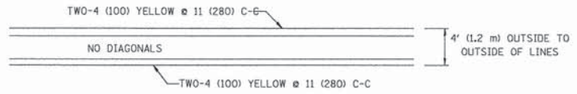
TYPICAL LANE AND EDGE LINE MARKING



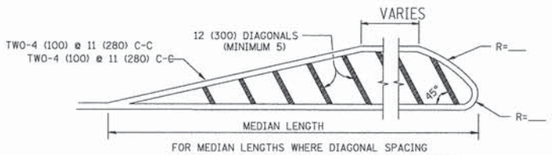
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



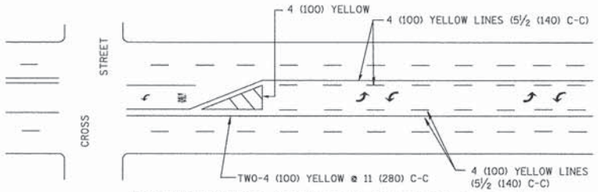
4' (1.2 m) WIDE MEDIANS ONLY



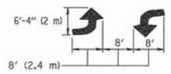
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

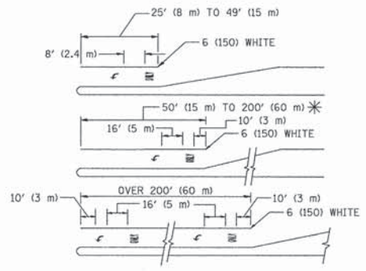


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

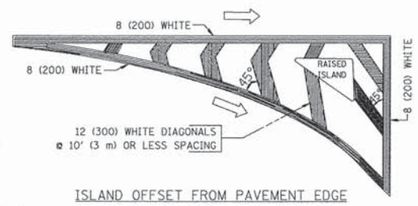


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

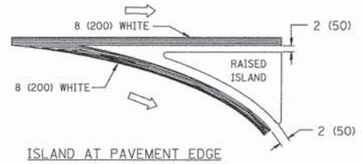
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

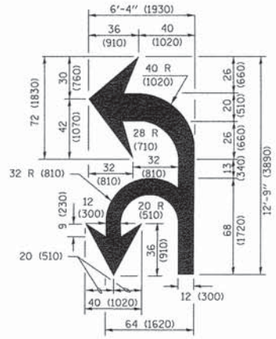


ISLAND OFFSET FROM PAVEMENT EDGE



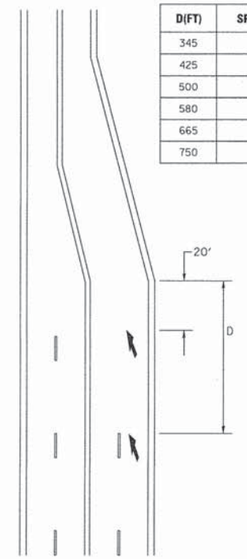
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN

U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ø 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ø 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ø 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ø 6 (150) 12 (300) ø 45° 12 (300) ø 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ø 4 (100) WITH 12 (300) DIAGONALS ø 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ø 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ø 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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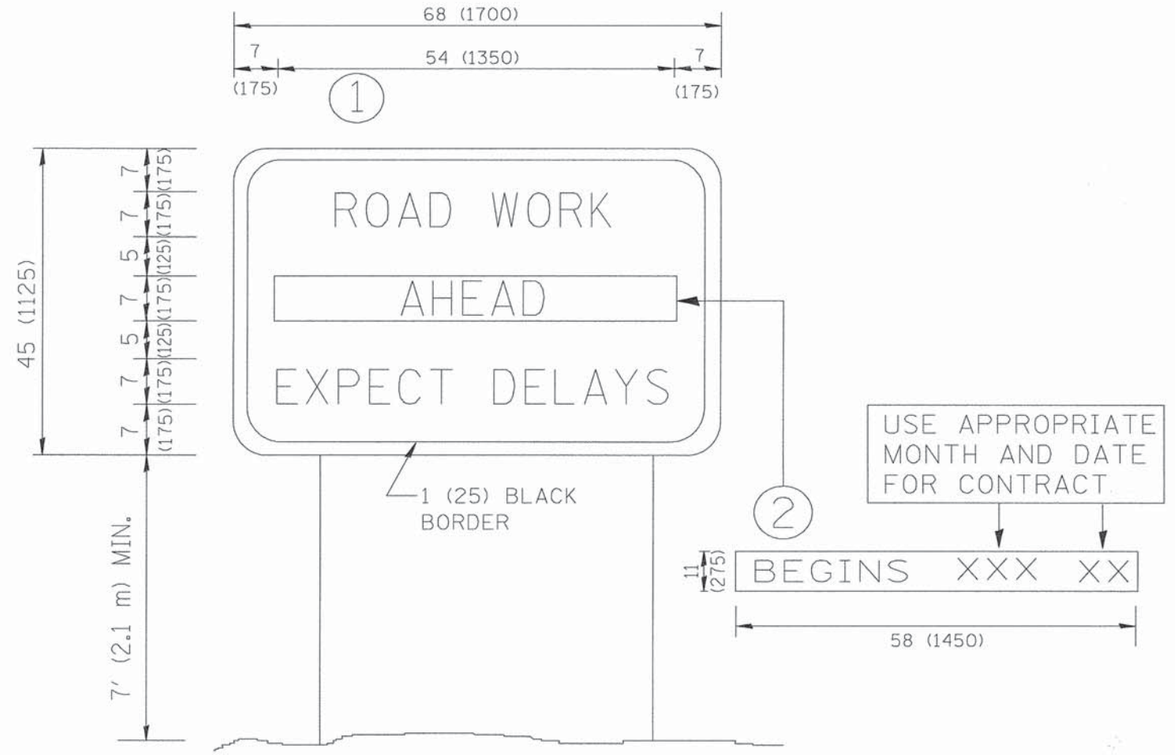
Default	PLOT DATE = 12/21/2015	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 11 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	11
	TC-13	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
ARTERIAL ROAD
INFORMATION SIGN

SCALE: NONE | SHEET NO. 12 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	12
	TC-22	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

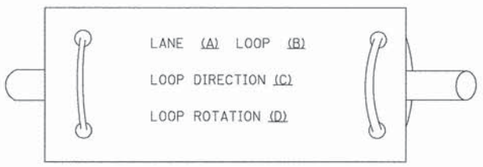
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

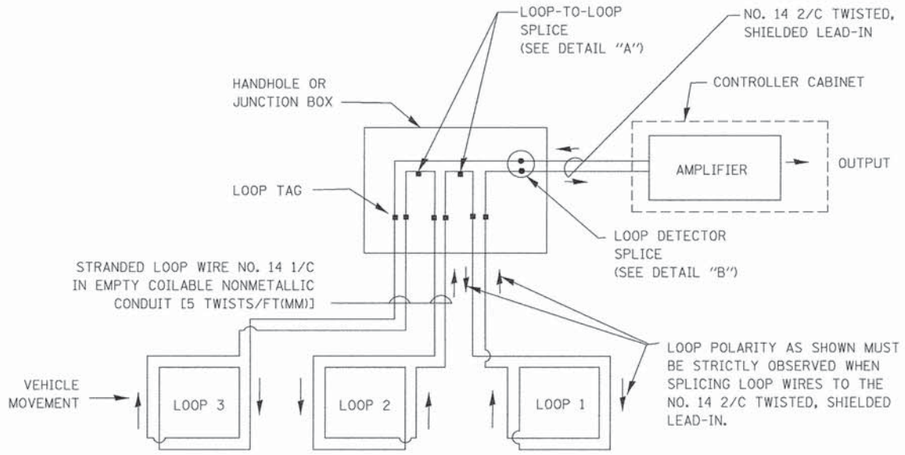


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

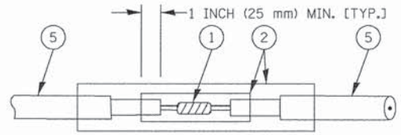
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

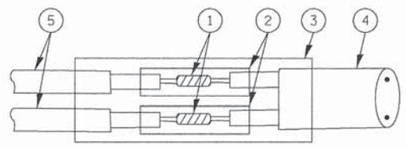


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

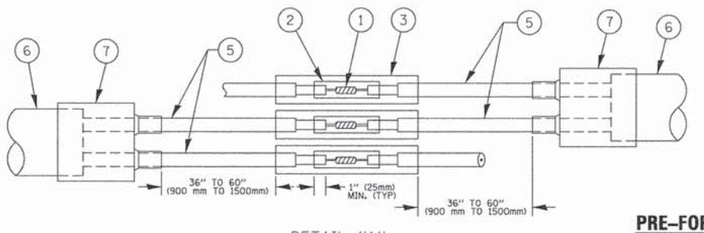


DETAIL "A"
LOOP-TO-LOOP SPLICE



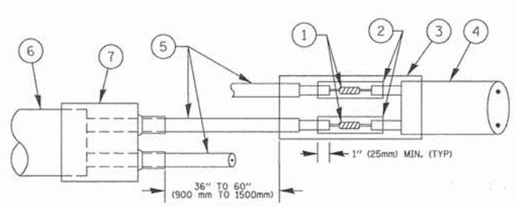
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

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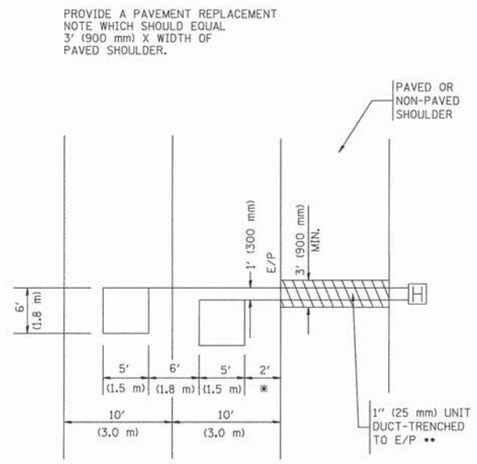
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 13 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	13
	TS-05B	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		

LOOPS NEXT TO SHOULDERS

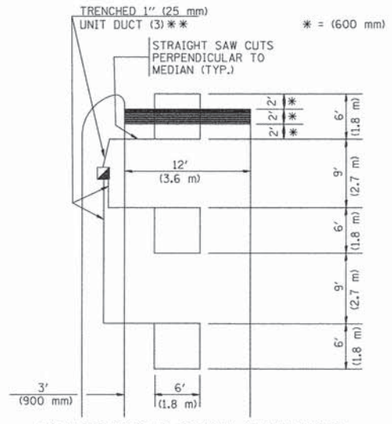


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

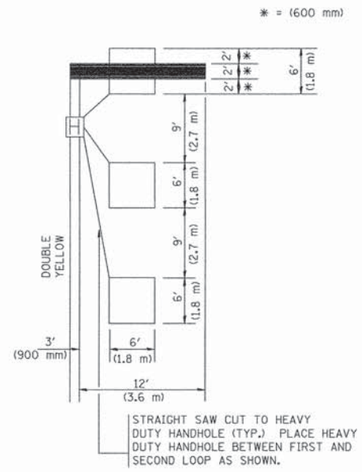
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

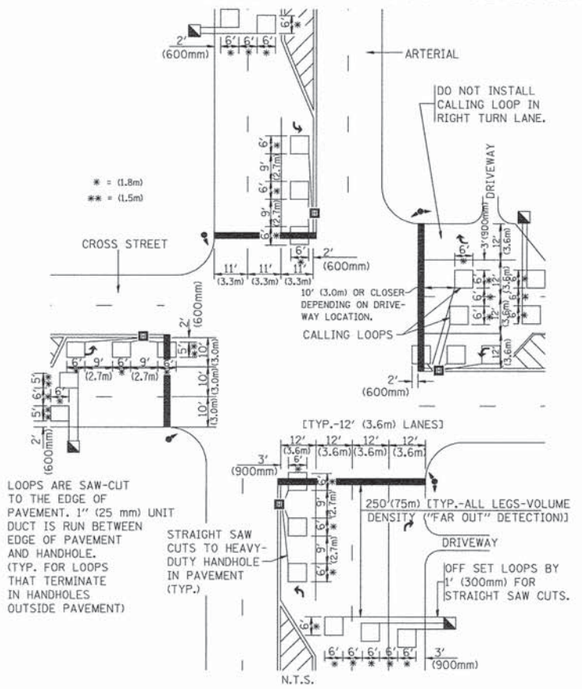
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

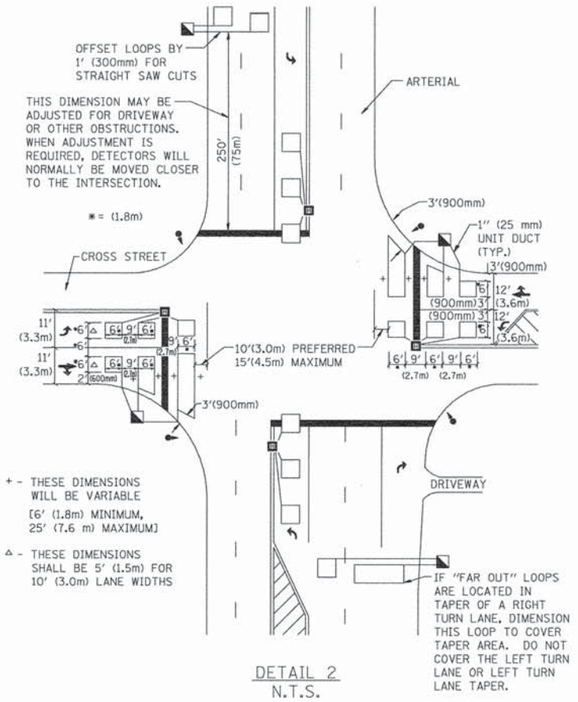
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

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	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 14 OF 14 SHEETS | STA. | TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2796	14-00098-00-RS	COOK	14	14
	TS-07	CONTRACT NO. 61C52		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT M-4003(428)		