

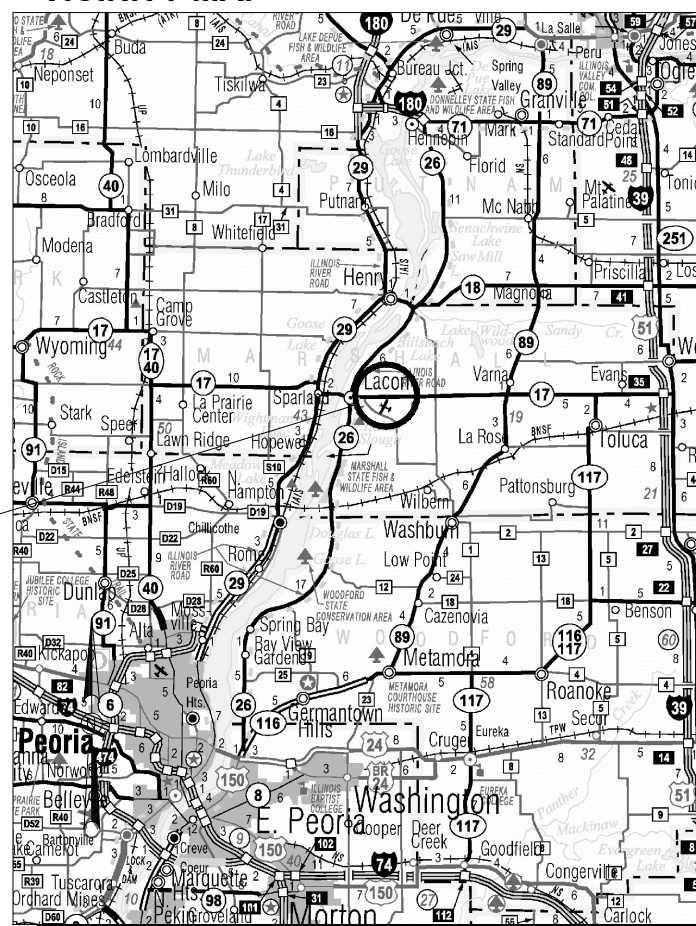
CONSTRUCTION PLANS

CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT

MARSHALL COUNTY AIRPORT BOARD  
MARSHALL COUNTY AIRPORT (C75)  
LACON, MARSHALL COUNTY, ILLINOIS

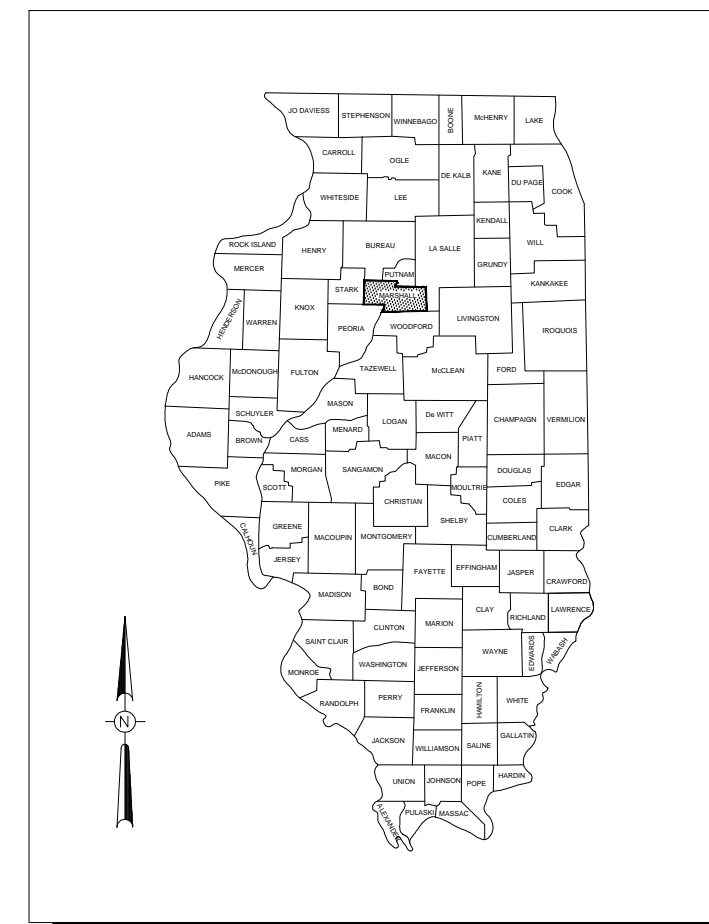
IDA PROJECT NO. C75-4346  
SBG PROJECT NO. 3-17-SBGP-TBD

VICINITY MAP



MARSHALL COUNTY AIRPORT

LOCATION MAP



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

I:\15\0BS\15A0001\15A0001\DCAD\AIRPORT\SHEET\C-001-C.VR.DWG

No.	Issue/Description	Sheets Changed	Date	By

*Kyle B. Schweizer*  
Kyle B. Schweizer, P.E.  
Project Engineer  
16 NOV 2015  
Date

HANSON PROFESSIONAL SERVICES INC.  
1525 South Sixth Street  
Springfield, Illinois 62703-2883  
Telephone: 217.788.2450  
Fax: 217.788.2503

*Robert A. Waller*  
Robert A. Waller, P.E.  
Project Manager  
16 NOV 2015  
Date

MARSHALL COUNTY AIRPORT BOARD  
P.O. Box 248  
Lacon, Illinois 61540-2048

*Charles L. Allen*  
Charles L. Allen  
President  
11/16/2015  
Date



### INDEX TO SHEETS

SHEET NO.	SHEET TITLE
1	COVER SHEET
2	SUMMARY OF QUANTITIES, INDEX TO SHEETS, & GENERAL NOTES
3	CONSTRUCTION SAFETY PLAN
4	CONSTRUCTION SAFETY NOTES & DETAILS
5	PROPOSED SITE PLAN
6	FLOWLINE S4 DETAIL
7	EROSION CONTROL DETAILS & NOTES
8	EROSION CONTROL SCHEDULE & REMOVE CONCRETE DETAIL

### SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150520	MOBILIZATION	L.S.	1	
AR152441	ON-SITE BORROW	C.Y.	1,400	
AR152443	OFFSITE TOPSOIL	C.Y.	929	
AR156510	SILT FENCE	L.F.	400	
AR156531	EROSION CONTROL BLANKET	S.Y.	5,069	
AR156544	RIPRAP-GRADATION NO. 4	S.Y.	519	
AR701208	8" CMP	L.F.	120	
AR705508	8" PERFORATED UNDERDRAIN	L.F.	300	
AR752208	METAL END SECTION 8"	EA.	6	
AR800486	REGRADE DITCH	L.F.	8,300	
AR800487	TURF REINFORCEMENT MAT, TYPE 1	S.Y.	11,361	
AR800488	TURF REINFORCEMENT MAT, TYPE 2	S.Y.	3,957	
AR800489	REMOVE CONCRETE	L.S.	1	
AR901510	SEEDING	ACRE	6	

### GENERAL NOTES

- PROJECT DESCRIPTION:** THIS PROJECT SHALL CONSIST OF INSTALLING PERMANENT EROSION CONTROL MEASURES AT VARIOUS LOCATIONS ON THE AIRFIELD. THE PROPOSED IMPROVEMENTS INCLUDE ONSITE BORROW, DITCH GRADING, EROSION CONTROL BLANKET, TURF REINFORCEMENT MAT, RIPRAP, CONCRETE REMOVAL, OFFSITE TOPSOIL, AND SEEDING.
- THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/RESIDENT TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN, IF APPROVED BY THE ENGINEER.
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING UTILITY, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER EQUIPMENT OR PERSONNEL.
- THE CONTRACTOR SHALL MAINTAIN ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS AND PROVIDE TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS.
- THE CONTRACTOR SHALL NOTE THAT SOME AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/RESIDENT TECHNICIAN PRIOR TO BEGINNING ANY WORK. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITY WHICH HAVE BEEN PREVIOUSLY FARMED OR ARE DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION SHALL BE CHISEL PLOWED OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER) AS AN INCIDENTAL COST TO THE PROJECT.
- APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- NPDES PERMIT** – THIS PROJECT WILL DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED.
- STAGING AREA** – AT THE START OF THE PROJECT THE CONTRACTOR SHALL DELIVER AND PLACE A MINIMUM OF TWO TANDEM LOADS OF IDOT CA-6 AGGREGATE AT THE DESIGNATED CONSTRUCTION ENTRANCE TO BE LEFT IN PLACE PERMANENTLY. COST IS INCIDENTAL TO HAUL ROUTE AND MOBILIZATION PAY ITEMS.
- MATERIAL CERTIFICATIONS** – MATERIAL TO BE INCORPORATED INTO THE PROJECT **CANNOT** BE USED WITHOUT PRIOR APPROVAL. ALL MATERIAL TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN FOR APPROVAL. USE OF MATERIAL WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE IDOT – DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.
- CERTIFIED PAYROLLS** – THE RESIDENT ENGINEER/RESIDENT TECHNICIAN **CANNOT** FORWARD A CONSTRUCTION REPORT FOR PAYMENT TO THE IDOT-DIVISION OF AERONAUTICS FOR PROCESSING UNTIL **ALL** CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

### UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. **CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123.** CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

### J.U.L.I.E. INFORMATION

COUNTY.....MARSHALL  
CITY.....LACON  
TOWNSHIP.....HOPEWELL  
SECTION NO.....31  
ADDRESS.....MARSHALL COUNTY AIRPORT  
LACON, ILLINOIS 61540

### CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028

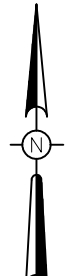
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2015

PROJECT NO: 15A0001  
CAD FILE: C-002-SOQ.DWG  
DESIGN BY: KBS  
DRAWN BY: KBS  
REVIEWED BY: CAH 11/9/15

SHEET TITLE

### SUMMARY OF QUANTITIES, INDEX TO SHEETS, & GENERAL NOTES



0' 150' 300' 600'  
HALF SIZE SCALE: 1" = 600'  
FULL SIZE SCALE: 1" = 300'

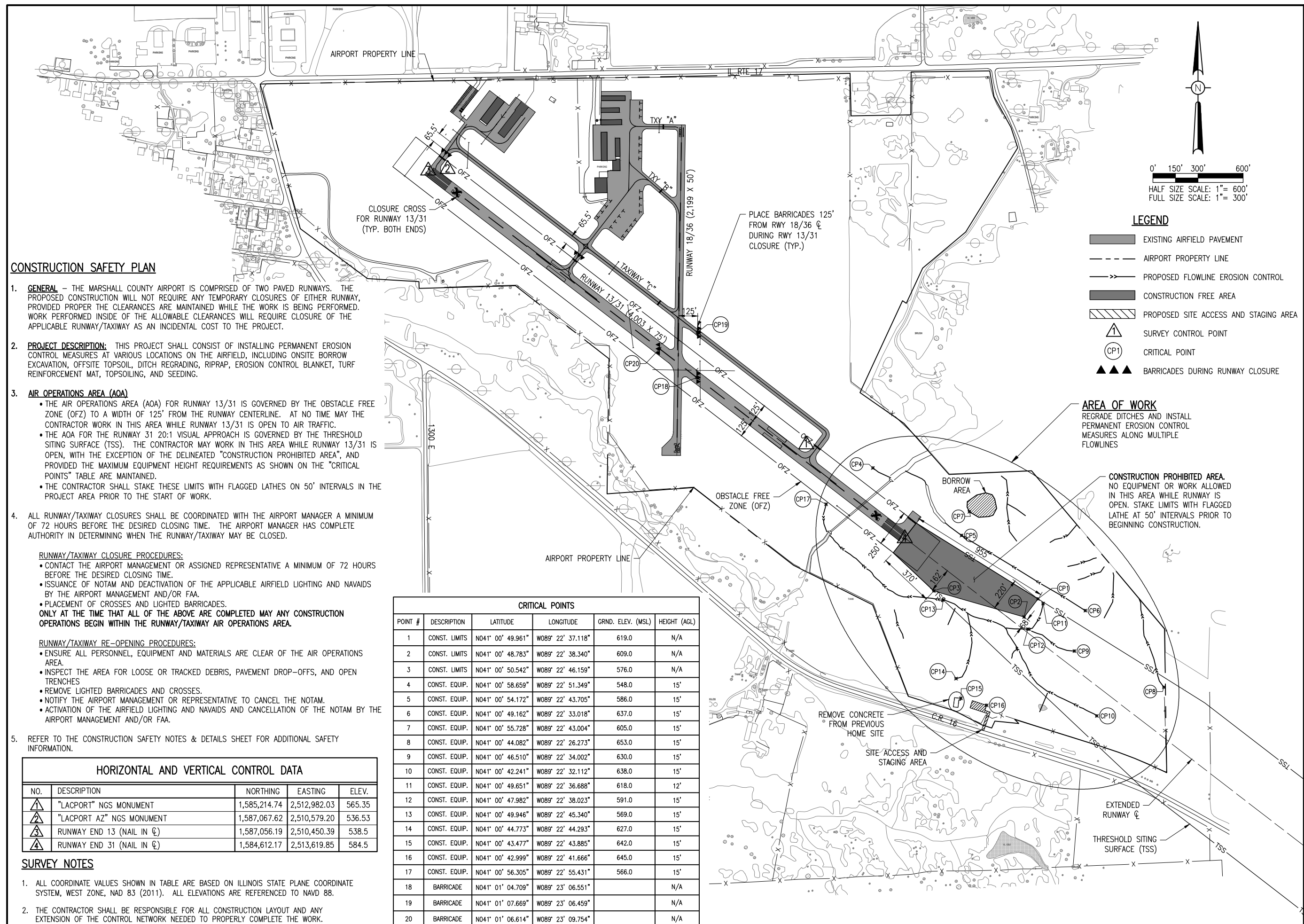
**LEGEND**

- EXISTING AIRFIELD PAVEMENT
- AIRPORT PROPERTY LINE
- PROPOSED FLOWLINE EROSION CONTROL
- CONSTRUCTION FREE AREA
- PROPOSED SITE ACCESS AND STAGING AREA
- SURVEY CONTROL POINT
- CRITICAL POINT
- BARRICADES DURING RUNWAY CLOSURE

**AREA OF WORK**

REGRADE DITCHES AND INSTALL PERMANENT EROSION CONTROL MEASURES ALONG MULTIPLE FLOWLINES

**CONSTRUCTION PROHIBITED AREA.**  
NO EQUIPMENT OR WORK ALLOWED IN THIS AREA WHILE RUNWAY IS OPEN. STAKE LIMITS WITH FLAGGED LATHE AT 50' INTERVALS PRIOR TO BEGINNING CONSTRUCTION.



**CONSTRUCTION SAFETY PLAN**

1. **GENERAL** - THE MARSHALL COUNTY AIRPORT IS COMPRISED OF TWO PAVED RUNWAYS. THE PROPOSED CONSTRUCTION WILL NOT REQUIRE ANY TEMPORARY CLOSURES OF EITHER RUNWAY, PROVIDED PROPER CLEARANCES ARE MAINTAINED WHILE THE WORK IS BEING PERFORMED. WORK PERFORMED INSIDE OF THE ALLOWABLE CLEARANCES WILL REQUIRE CLOSURE OF THE APPLICABLE RUNWAY/TAXIWAY AS AN INCIDENTAL COST TO THE PROJECT.
2. **PROJECT DESCRIPTION:** THIS PROJECT SHALL CONSIST OF INSTALLING PERMANENT EROSION CONTROL MEASURES AT VARIOUS LOCATIONS ON THE AIRFIELD, INCLUDING ONSITE BORROW EXCAVATION, OFFSITE TOPSOIL, DITCH REGRADE, RIPRAP, EROSION CONTROL BLANKET, TURF REINFORCEMENT MAT, TOPSOILING, AND SEEDING.
3. **AIR OPERATIONS AREA (AOA)**
  - THE AIR OPERATIONS AREA (AOA) FOR RUNWAY 13/31 IS GOVERNED BY THE OBSTACLE FREE ZONE (OFZ) TO A WIDTH OF 125' FROM THE RUNWAY CENTERLINE. AT NO TIME MAY THE CONTRACTOR WORK IN THIS AREA WHILE RUNWAY 13/31 IS OPEN TO AIR TRAFFIC.
  - THE AOA FOR THE RUNWAY 31 20:1 VISUAL APPROACH IS GOVERNED BY THE THRESHOLD SITING SURFACE (TSS). THE CONTRACTOR MAY WORK IN THIS AREA WHILE RUNWAY 13/31 IS OPEN, WITH THE EXCEPTION OF THE DELINEATED "CONSTRUCTION PROHIBITED AREA", AND PROVIDED THE MAXIMUM EQUIPMENT HEIGHT REQUIREMENTS AS SHOWN ON THE "CRITICAL POINTS" TABLE ARE MAINTAINED.
  - THE CONTRACTOR SHALL STAKE THESE LIMITS WITH FLAGGED LATHES ON 50' INTERVALS IN THE PROJECT AREA PRIOR TO THE START OF WORK.
4. ALL RUNWAY/TAXIWAY CLOSURES SHALL BE COORDINATED WITH THE AIRPORT MANAGER A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME. THE AIRPORT MANAGER HAS COMPLETE AUTHORITY IN DETERMINING WHEN THE RUNWAY/TAXIWAY MAY BE CLOSED.

**RUNWAY/TAXIWAY CLOSURE PROCEDURES:**

- CONTACT THE AIRPORT MANAGEMENT OR ASSIGNED REPRESENTATIVE A MINIMUM OF 72 HOURS BEFORE THE DESIRED CLOSING TIME.
  - ISSUANCE OF NOTAM AND DEACTIVATION OF THE APPLICABLE AIRFIELD LIGHTING AND NAVAIDS BY THE AIRPORT MANAGEMENT AND/OR FAA.
  - PLACEMENT OF CROSSES AND LIGHTED BARRICADES.
- ONLY AT THE TIME THAT ALL OF THE ABOVE ARE COMPLETED MAY ANY CONSTRUCTION OPERATIONS BEGIN WITHIN THE RUNWAY/TAXIWAY AIR OPERATIONS AREA.**

**RUNWAY/TAXIWAY RE-OPENING PROCEDURES:**

- ENSURE ALL PERSONNEL, EQUIPMENT AND MATERIALS ARE CLEAR OF THE AIR OPERATIONS AREA.
- INSPECT THE AREA FOR LOOSE OR TRACKED DEBRIS, PAVEMENT DROP-OFFS, AND OPEN TRENCHES
- REMOVE LIGHTED BARRICADES AND CROSSES.
- NOTIFY THE AIRPORT MANAGEMENT OR REPRESENTATIVE TO CANCEL THE NOTAM.
- ACTIVATION OF THE AIRFIELD LIGHTING AND NAVAIDS AND CANCELLATION OF THE NOTAM BY THE AIRPORT MANAGEMENT AND/OR FAA.

5. REFER TO THE CONSTRUCTION SAFETY NOTES & DETAILS SHEET FOR ADDITIONAL SAFETY INFORMATION.

**HORIZONTAL AND VERTICAL CONTROL DATA**

NO.	DESCRIPTION	NORTHING	EASTING	ELEV.
1	"LACPORT" NGS MONUMENT	1,585,214.74	2,512,982.03	565.35
2	"LACPORT AZ" NGS MONUMENT	1,587,067.62	2,510,579.20	536.53
3	RUNWAY END 13 (NAIL IN $\Phi$ )	1,587,056.19	2,510,450.39	538.5
4	RUNWAY END 31 (NAIL IN $\Phi$ )	1,584,612.17	2,513,619.85	584.5

**CRITICAL POINTS**

POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)
1	CONST. LIMITS	N041° 00' 49.961"	W089° 22' 37.118"	619.0	N/A
2	CONST. LIMITS	N041° 00' 48.783"	W089° 22' 38.340"	609.0	N/A
3	CONST. LIMITS	N041° 00' 50.542"	W089° 22' 46.159"	576.0	N/A
4	CONST. EQUIP.	N041° 00' 58.659"	W089° 22' 51.349"	548.0	15'
5	CONST. EQUIP.	N041° 00' 54.172"	W089° 22' 43.705"	586.0	15'
6	CONST. EQUIP.	N041° 00' 49.162"	W089° 22' 33.018"	637.0	15'
7	CONST. EQUIP.	N041° 00' 55.728"	W089° 22' 43.004"	605.0	15'
8	CONST. EQUIP.	N041° 00' 44.082"	W089° 22' 26.273"	653.0	15'
9	CONST. EQUIP.	N041° 00' 46.510"	W089° 22' 34.002"	630.0	15'
10	CONST. EQUIP.	N041° 00' 42.241"	W089° 22' 32.112"	638.0	15'
11	CONST. EQUIP.	N041° 00' 49.651"	W089° 22' 36.688"	618.0	12'
12	CONST. EQUIP.	N041° 00' 47.982"	W089° 22' 38.023"	591.0	15'
13	CONST. EQUIP.	N041° 00' 49.946"	W089° 22' 45.340"	569.0	15'
14	CONST. EQUIP.	N041° 00' 44.773"	W089° 22' 44.293"	627.0	15'
15	CONST. EQUIP.	N041° 00' 43.477"	W089° 22' 43.885"	642.0	15'
16	CONST. EQUIP.	N041° 00' 42.999"	W089° 22' 41.666"	645.0	15'
17	CONST. EQUIP.	N041° 00' 56.305"	W089° 22' 55.431"	566.0	15'
18	BARRICADE	N041° 01' 04.709"	W089° 23' 06.551"		N/A
19	BARRICADE	N041° 01' 07.669"	W089° 23' 06.459"		N/A
20	BARRICADE	N041° 01' 06.614"	W089° 23' 09.754"		N/A

JAN 07, 2016 9:32 AM SCHWEIDT26  
I:\5\08515A0001\15A0001\CAD\AIRPORT\TSHEETC-003-SFY.DWG

**CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028

NO.	DATE	DESCRIPTION
1	1/7/16	ADD C.P. 18-20 KBS   KBS
		DES   DWN   REV

ISSUE: NOVEMBER 16, 2015  
PROJECT NO: 15A0001  
CAD FILE: C-003-SFY.DWG  
DESIGN BY: KBS  
DRAWN BY: KBS  
REVIEWED BY: CAH 11/9/15

SHEET TITLE

**CONSTRUCTION SAFETY PLAN**

**CONSTRUCT  
PERMANENT EROSION  
CONTROL AT VARIOUS  
LOCATIONS ON THE  
AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2015

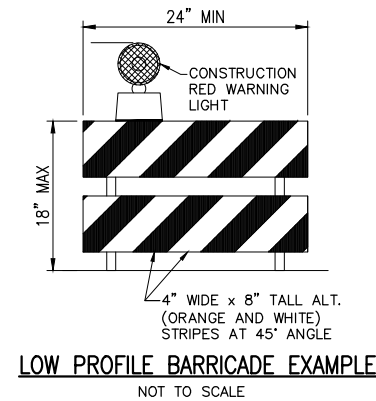
PROJECT NO: 15A0001  
CAD FILE: C-003-SFY.DWG  
DESIGN BY: KBS  
DRAWN BY: KBS  
REVIEWED BY: CAH 11/9/15

SHEET TITLE

**CONSTRUCTION  
SAFETY NOTES &  
DETAILS**

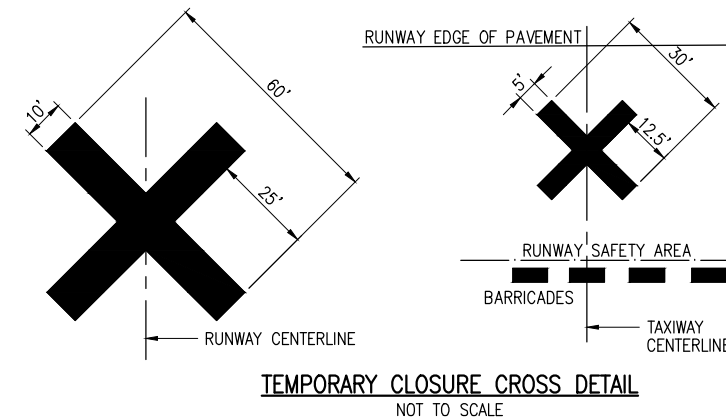
**CONSTRUCTION SAFETY NOTES**

- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER/TECHNICIAN.
- ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, **SHALL BE INCIDENTAL TO THE CONTRACT.**
- AIRFIELD SAFETY ASSURANCE** - AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2F WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILEGES ON THE AIRPORT OR A COMPLETE RESTRICTION OF ACCESS TO THE AIRFIELD ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- THE CONTRACTOR SHALL PROVIDE THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN WITH CONTACT INFORMATION FOR A PERSON ON CALL 24 HOURS A DAY FOR EMERGENCY MAINTENANCE OF BARRICADES AND HAZARD LIGHTING.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
- CLOSED AIRFIELD WORK AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- NO PAVEMENT EDGE DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
- AIRPORT SECURITY** - AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. NO CONTRACTOR PERSONNEL OR EQUIPMENT WILL BE PERMITTED OUTSIDE OF THE CONSTRUCTION LIMITS DEPICTED ON THESE PLANS. PERSONAL VEHICLES OF CONTRACTOR EMPLOYEES SHALL BE PARKED IN THE DESIGNATED STAGING AREA AND ARE NOT ALLOWED ACCESS ON THE SITE. THE CONTRACTOR WILL CLOSE AND LOCK THE GATE AT THE DESIGNATED CONSTRUCTION ENTRANCE AT THE END OF EACH DAY.
- NO CONSTRUCTION EQUIPMENT GREATER THAN 15' TALL WILL BE PERMITTED ON THE AIRPORT WITHOUT THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEP, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- RADIO CONTROL** - THE CONTRACTOR WILL BE REQUIRED TO REMAIN CONSTANT TWO-WAY RADIO CONTACT WITH THE MARSHALL COUNTY AIRPORT UNICOM (122.8 MHz) AT ALL TIMES WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.



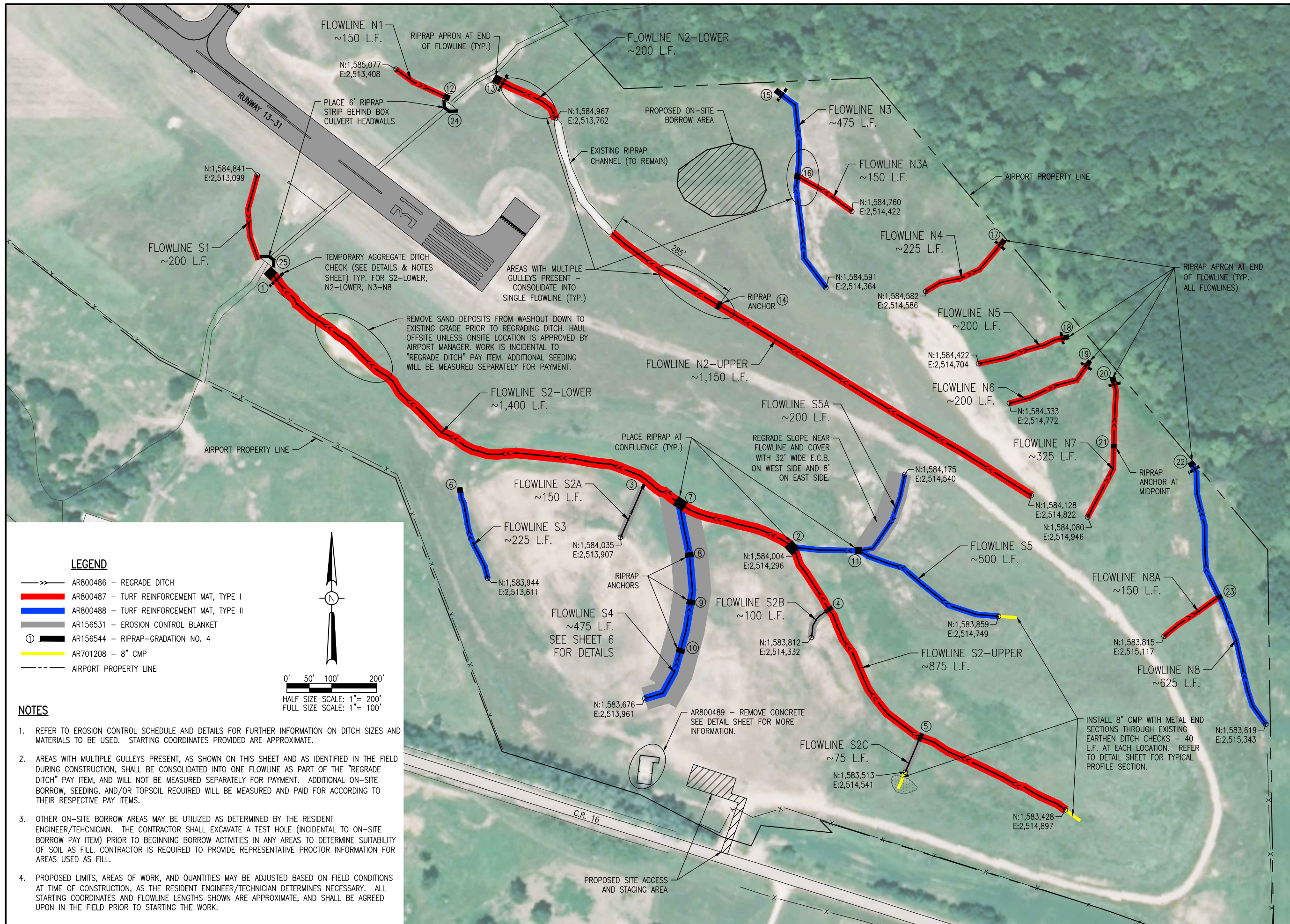
**BARRICADE NOTES**

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- LOW PROFILE BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 4' INCREMENTS AS DIRECTED BY THE ENGINEER. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON LOW PROFILE BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.



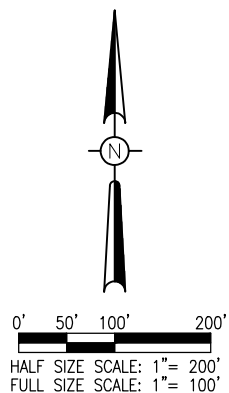
**TEMPORARY CLOSURE CROSS NOTES**

- TEMPORARY CLOSED RUNWAY AND CLOSED TAXIWAY MARKINGS SHALL BE AVIATION YELLOW.
- TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE CONSTRUCTED OF PLYWOOD, DOUBLE-LAYERED SNOW FENCE OR APPROVED FABRIC AND SHALL BE SECURED TO PAVEMENT BY SANDBAGS OR OTHER APPROVED METHOD.
- TEMPORARY CLOSED RUNWAY MARKINGS SHALL BE PLACED OVER THE RUNWAY DESIGNATION NUMBERS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- TEMPORARY CLOSED TAXIWAY MARKINGS SHALL BE PLACED WHEN AN ADJACENT RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE CLOSED TAXIWAY MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
- THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY OR TAXIWAY IS CLOSED AND REMOVED WHEN THE RUNWAY OR TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES.



**LEGEND**

- >> AR800486 - REGRADE DITCH
- AR800487 - TURF REINFORCEMENT MAT, TYPE I
- AR800488 - TURF REINFORCEMENT MAT, TYPE II
- AR156531 - EROSION CONTROL BLANKET
- ① AR156544 - RIPRAP—GRADATION NO. 4
- AR701208 - 8" CMP
- - - AIRPORT PROPERTY LINE



**NOTES**

1. REFER TO EROSION CONTROL SCHEDULE AND DETAILS FOR FURTHER INFORMATION ON DITCH SIZES AND MATERIALS TO BE USED. STARTING COORDINATES PROVIDED ARE APPROXIMATE.
2. AREAS WITH MULTIPLE GULLEYS PRESENT, AS SHOWN ON THIS SHEET AND AS IDENTIFIED IN THE FIELD DURING CONSTRUCTION, SHALL BE CONSOLIDATED INTO ONE FLOWLINE AS PART OF THE "REGRADE DITCH" PAY ITEM, AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT. ADDITIONAL ON-SITE BORROW, SEEDING, AND/OR TOPSOIL REQUIRED WILL BE MEASURED AND PAID FOR ACCORDING TO THEIR RESPECTIVE PAY ITEMS.
3. OTHER ON-SITE BORROW AREAS MAY BE UTILIZED AS DETERMINED BY THE RESIDENT ENGINEER/TECHNICIAN. THE CONTRACTOR SHALL EXCAVATE A TEST HOLE (INCIDENTAL TO ON-SITE BORROW PAY ITEM) PRIOR TO BEGINNING BORROW ACTIVITIES IN ANY AREAS TO DETERMINE SUITABILITY OF SOIL AS FILL. CONTRACTOR IS REQUIRED TO PROVIDE REPRESENTATIVE PROCTOR INFORMATION FOR AREAS USED AS FILL.
4. PROPOSED LIMITS, AREAS OF WORK, AND QUANTITIES MAY BE ADJUSTED BASED ON FIELD CONDITIONS AT TIME OF CONSTRUCTION, AS THE RESIDENT ENGINEER/TECHNICIAN DETERMINES NECESSARY. ALL STARTING COORDINATES AND FLOWLINE LENGTHS SHOWN ARE APPROXIMATE, AND SHALL BE AGREED UPON IN THE FIELD PRIOR TO STARTING THE WORK.

**CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2015

PROJECT NO: 15A0001  
CAD FILE: C-101-SIT.DWG

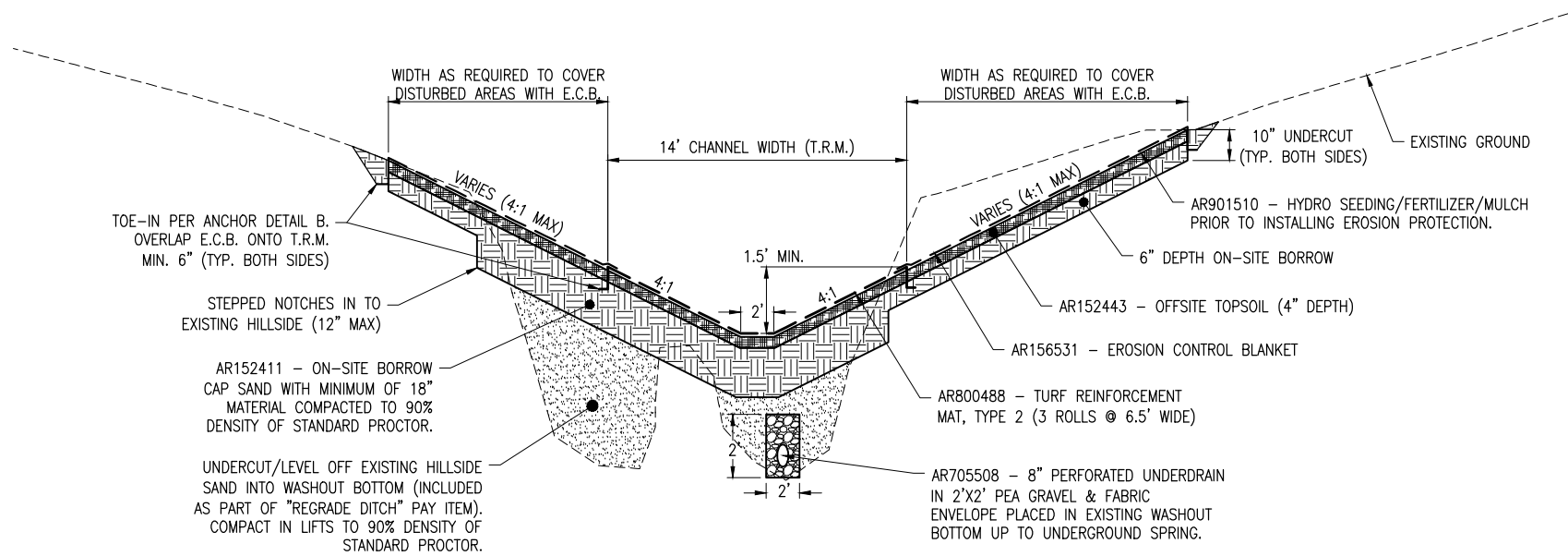
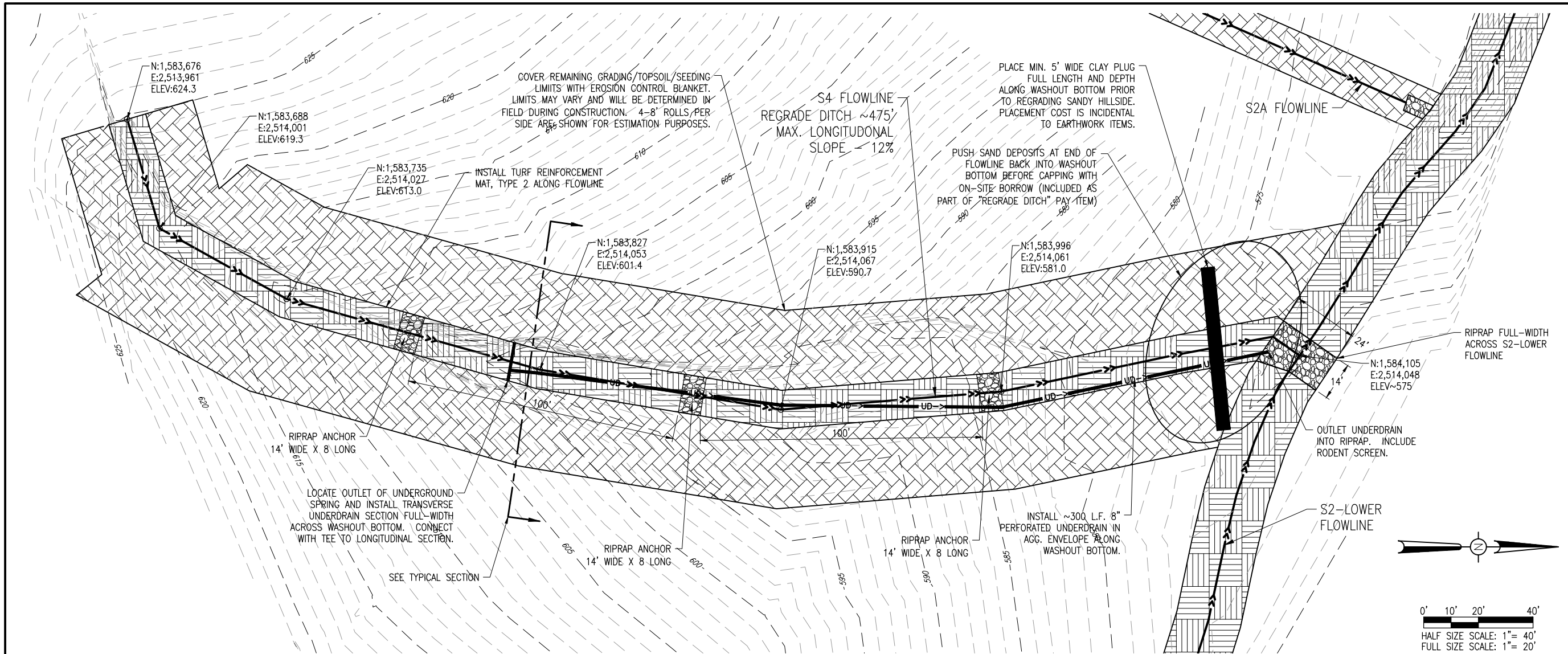
DESIGN BY: KBS  
DRAWN BY: KBS

REVIEWED BY: CAH 11/9/15

SHEET TITLE

**PROPOSED SITE PLAN**

NOV 28, 2015 3:11 PM SCHWEO1286 I:\15\05\15A0001\15A0001\CAD\AIRPORT\SHEET\C-101-SIT.DWG



**FLOWLINE S4 TYPICAL SECTION**  
NOT TO SCALE

**NOTE**

1. DUE TO THE IRREGULAR SHAPE AND VARYING NATURE OF SITE CONDITIONS, NOMINAL QUANTITIES HAVE BEEN PROVIDED AND MAY BE ADJUSTED IN THE FIELD, PROVIDED MIN. CHANNEL DEPTH AND MAX SLOPE REQUIREMENTS ARE MET.
2. CONTOUR DATA IS APPROXIMATE.

**CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

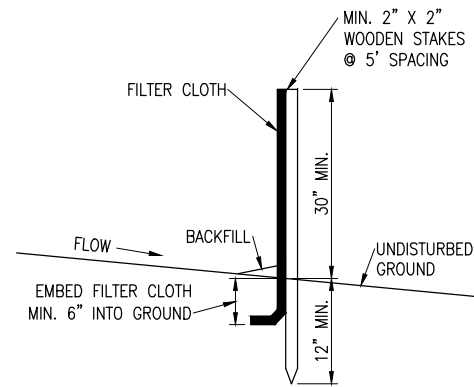
Contract No. MA028


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

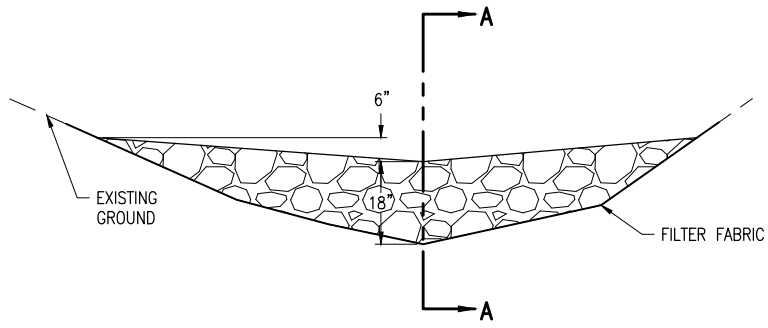
ISSUE: NOVEMBER 16, 2015  
PROJECT NO: 15A0001  
CAD FILE: C-102-SITS4.DWG  
DESIGN BY: KBS  
DRAWN BY: KBS  
REVIEWED BY: CAH 11/9/15

SHEET TITLE

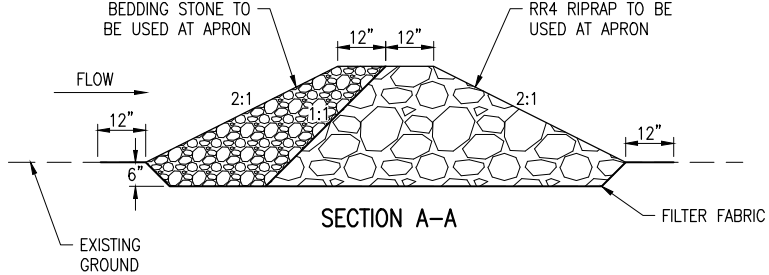
**FLOWLINE S4 DETAIL**



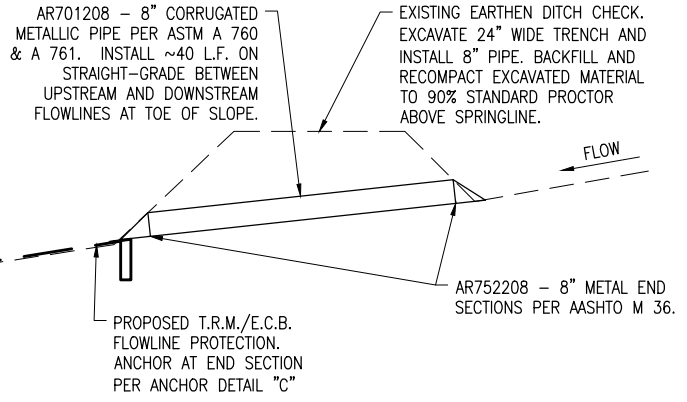
**SILT FENCE DETAIL**  
NOT TO SCALE



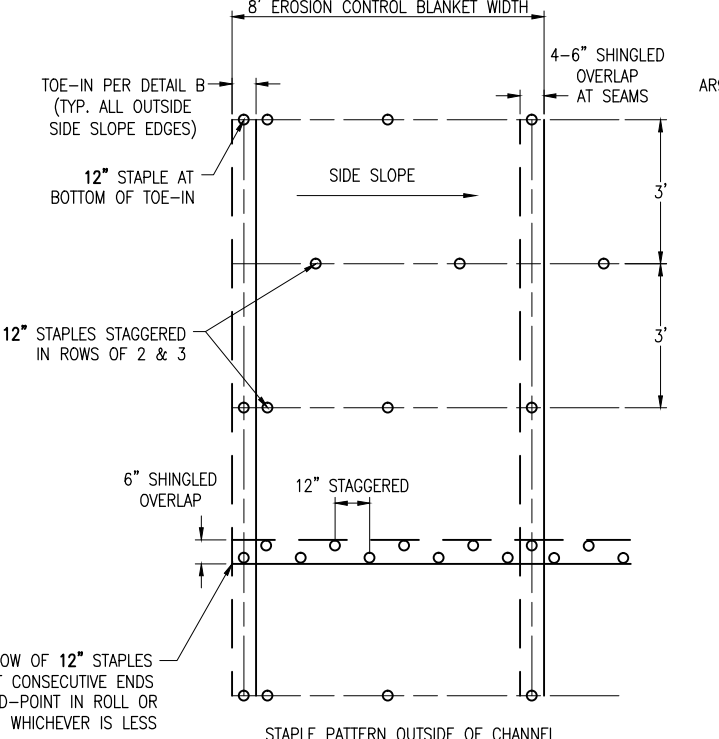
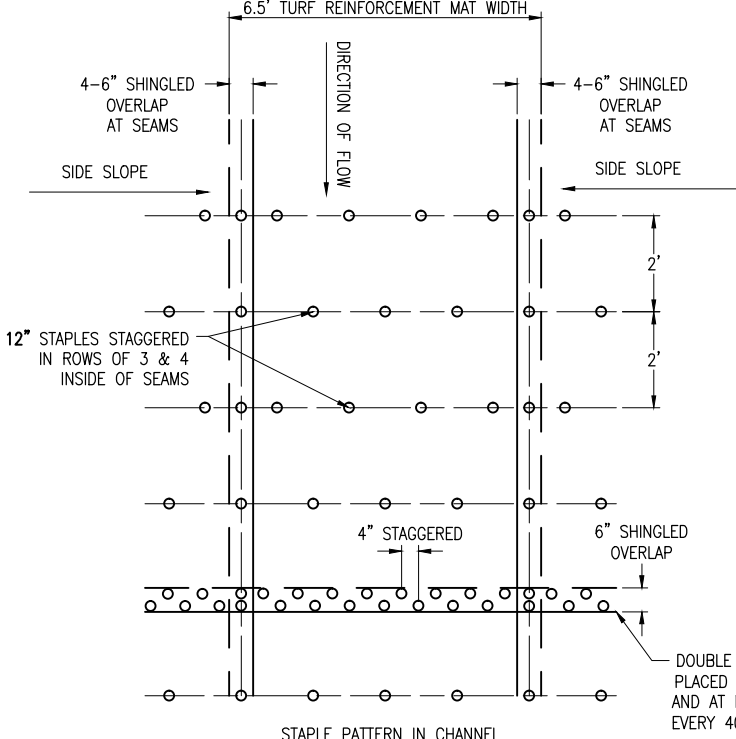
**ELEVATION**



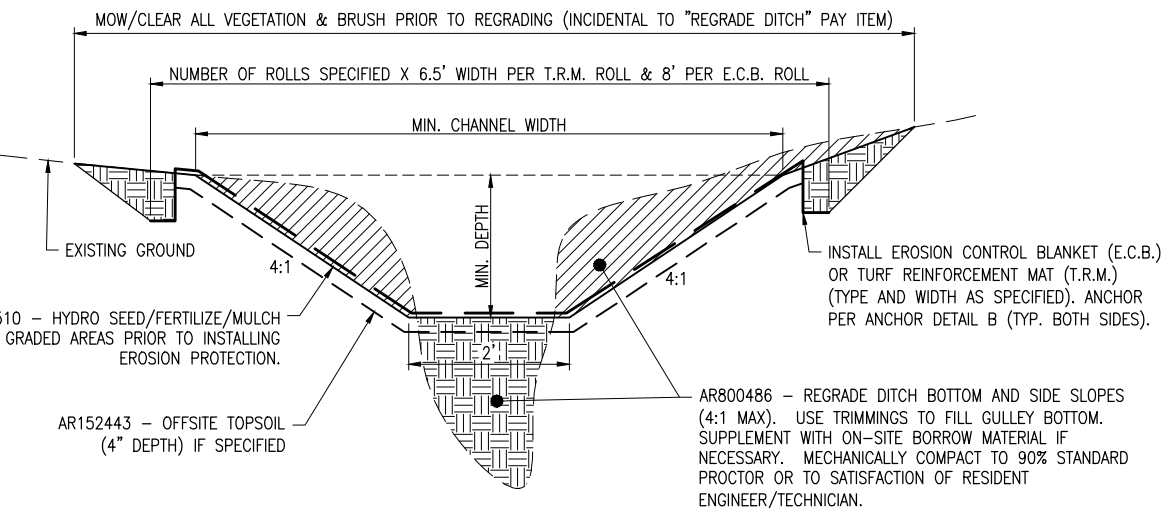
**TEMPORARY AGGREGATE DITCH CHECK**  
NOT TO SCALE



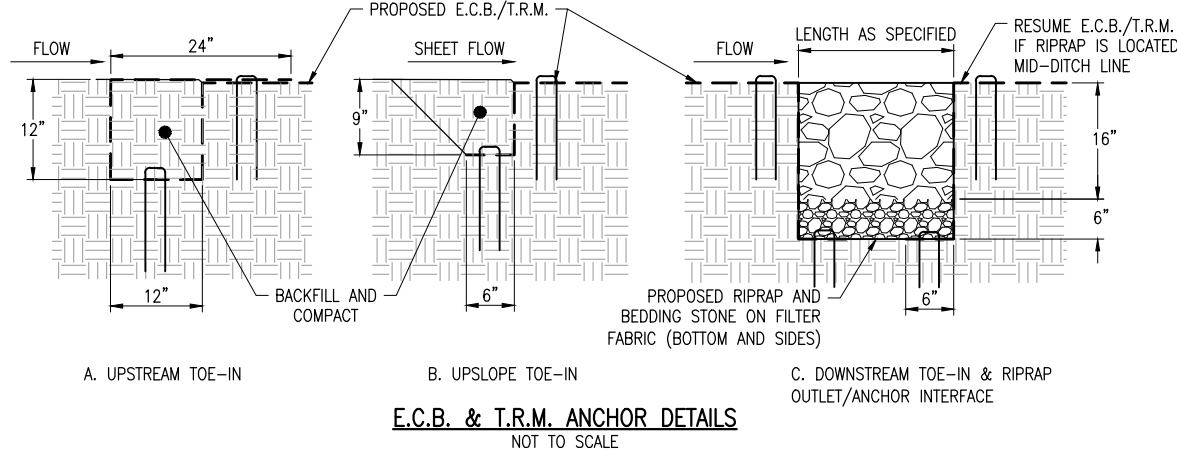
**EARTHEN DITCH CHECK DETAIL**  
NOT TO SCALE



**STAPLE PATTERN DETAILS**  
NOT TO SCALE



**REGRADE DITCH DETAIL**  
NOT TO SCALE



**EROSION CONTROL NOTES**

- REFER TO THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR ADDITIONAL MATERIAL AND INSTALLATION REQUIREMENTS.
- EROSION CONTROL BLANKET/TURF REINFORCEMENT MAT SHALL BE INSTALLED IN CONJUNCTION WITH THE DETAILS ON THIS SHEET AND THE MANUFACTURER'S RECOMMENDATIONS. THE MORE CONSERVATIVE OF THE TWO SHALL GOVERN UNLESS OTHERWISE APPROVED BY THE DESIGN ENGINEER. ALL STAPLES SHALL BE 12" IN LENGTH.
- EROSION CONTROL BLANKET SHALL BE A MACHINE PRODUCED MAT OF 100% COCONUT FIBER MATRIX WRAPPED IN A HEAVYWEIGHT BIODEGRADABLE DOUBLE NET BLANKET.
- TURF REINFORCEMENT MAT SHALL CONSIST OF A PERMANENT MULTI-LAYERED UV STABILIZED TURF REINFORCEMENT MATTING. TYPE 1 T.R.M. SHALL INCLUDE A LAYER OF 100% COCONUT FIBER MATRIX, AND TYPE 2 T.R.M. SHALL INCLUDE A LAYER OF PERMANENT UV STABILIZED POLYPROPYLENE FIBER MATRIX.
- SEEDING, FERTILIZER, AND MULCH SHALL BE APPLIED HYDRAULICALLY AS AN AQUEOUS MIXTURE WITH A HEAVY DUTY TACKFIER. LIME SHALL BE APPLIED SEPARATELY.
- THE REGRADED AREAS OF FLOWLINES S4, S5A, N2 UPPER, & N3 SHALL BE LINED WITH 4" OF TOPSOIL PRIOR TO E.C.B./T.R.M. PLACEMENT.
- EXISTING ONSITE SAND APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN MAY BE USED FOR THE CMP BEDDING AND CRADLE.
- RIPRAP SHALL CONFORM TO IDOT - RR 4 GRADATION REQUIREMENTS. BROKEN CONCRETE FROM THE "REMOVE CONCRETE" PAY ITEM MAY BE USED AND PAID FOR AS RIPRAP IN AREAS APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN AND IF IN ACCORDANCE WITH IDOT SSRBC SECTIONS 281 AND 1005.
- RIPRAP AREAS SHALL EXTEND THE ENTIRE WIDTH OF THE CHANNEL BEING PROTECTED, AND ACROSS THE ENTIRE WIDTH OF THE DOWNSTREAM CHANNEL WHEN CONVERGING.
- A TEMPORARY AGGREGATE DITCH CHECK SHALL BE FORMED AT THE ENDS OF FLOWLINES S2-LOWER, N2-LOWER, N3, N4, N5, N6, N7, AND N8 TO PROTECT THE SITE FROM SEDIMENT LOSS UNTIL THE FLOWLINES HAVE BEEN COVERED WITH THE T.R.M. THE AGGREGATE SHALL THEN BE RELOCATED TO THE RIPRAP APRON. COST IS INCIDENTAL TO THE RIPRAP PAY ITEM.
- THE "REMOVE CONCRETE" ITEM SHALL CONSIST OF ONE LUMP SUM PAY ITEM, AND INCLUDE ALL COSTS ASSOCIATED WITH CONCRETE REMOVAL AND DISPOSAL, BACKFILL, GRADING, SEEDING AND RESTORATION OF THE FORMER HOME SITE. CONTACT THE AIRPORT MANAGER (309-246-2870) TO VISIT SITE PRIOR TO BIDDING.
- A QUANTITY OF 400 L.F. OF SILT FENCE HAS BEEN INCLUDED FOR USE IN LINING DOWNSTREAM SLOPES AROUND BORROW AREAS, STOCKPILES OR STAGING AREAS THAT THE RESIDENT ENGINEER DETERMINES NECESSARY TO KEEP SEDIMENT ONSITE.



Offices Nationwide  
www.hanson-inc.com

Hanson Professional Services Inc.  
1525 S. 6th Street  
Springfield, IL 62703  
phone: 217-788-2450  
fax: 217-788-2503

Illinois Licensed  
Professional Service Corporation  
#184-001084



Marshall County Airport  
1315 Illinois 17  
Lacon, Illinois 61540  
phone: 309-246-2870

**CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2015

PROJECT NO: 15A0001

CAD FILE: C-501-TYP.DWG

DESIGN BY: KBS

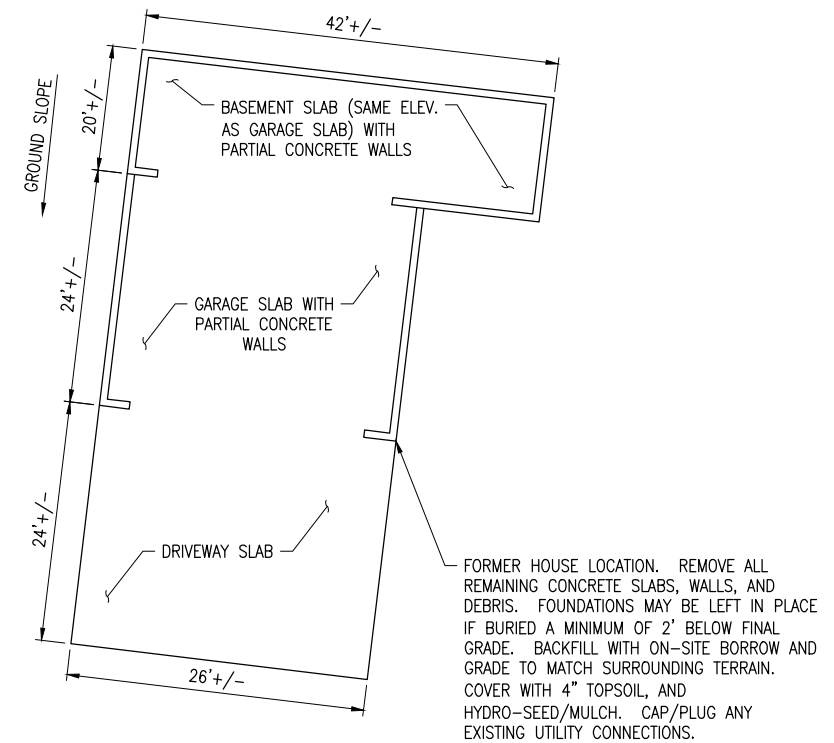
DRAWN BY: KBS

REVIEWED BY: CAH 11/9/15

SHEET TITLE

**EROSION CONTROL DETAILS & NOTES**

EROSION CONTROL SCHEDULE												
LOCATION/ FLOWLINE	REGRADE DITCH AR800486 (FT)	MINIMUM FLOWLINE DEPTH (FT)	MINIMUM CHANNEL WIDTH (FT)	NUMBER OF E.C.B./T.R.M. ROLLS X NOM. WIDTH (FT)	EROSION CONTROL BLANKET AR156531 (SY)	TURF REIN. MAT - TYPE 1 AR800487 (SY)	TURF REIN. MAT - TYPE 2 AR800488 (SY)	RIPRAP (LOCATION) WIDTH X LENGTH (FT)	RIPRAP - GRAD. NO. 4 AR156544 (SY)	ON-SITE BORROW AR152441 (CY)	OFFSITE TOPSOIL (4" DEPTH) AR152443 (CY)	SEEDING AR901510 (ACRE)
S1	200	1.0	10	2 X 6.5		289						0.06
S2 LOWER	1400	2.5	22	4 X 6.5		4045		(1) 24 X 24	64.0			0.84
S2 UPPER	875	1.5	14	3 X 6.5		1896		(2) 24 X 24	64.0			0.39
S2A	150	1.0	10	2 X 8	267			(3) 10 X 10	11.2			0.06
S2B	100	1.0	10	2 X 8	178			(4) 10 X 14	15.6			0.04
S2C	75	1.0	10	2 X 8	134			(5) 10 X 14	15.6			0.03
S3	225	1.0	10	2 X 6.5				(6) 10 X 12	13.4			0.07
S4	475	1.5	14	8 X 8 & 3 X 6.5	3378			(7) 14 X 24	37.4	1150	470	1.37
								(8-10) 14 X 8	37.5			
S5	500	1.0	10	2 X 6.5								0.15
S5A	200	1.0	10	5 X 8 & 2 X 6.5	889			(11) 15 X 15	25.0		123	0.24
N1	150	1.0	10	2 X 6.5		217		(12) 10 X 12	13.4			0.04
N2 LOWER	200	1.5	14	3 X 6.5		434		(13) 14 X 18	28.0			0.09
N2 UPPER	1150	1.5	14	3 X 6.5		2492		(14) 14 X 8	12.5		227	0.51
N3	475	1.0	10	2 X 6.5			687	(15) 10 X 12	13.4		59	0.14
N3A	150	1.0	10	2 X 6.5		217		(16) 10 X 12	13.4			0.04
N4	225	1.0	10	2 X 6.5		325		(17) 10 X 10	11.2			0.07
N5	200	1.0	10	2 X 6.5		289		(18) 10 X 10	11.2			0.06
N6	200	1.0	10	2 X 6.5		289		(19) 10 X 10	11.2			0.06
N7	325	1.0	10	2 X 6.5		470		(20) 10 X 12	13.4			0.10
								(21) 10 X 8	8.9			
N8	625	1.0	10	2 X 6.5			903	(22) 10 X 12	13.4			0.19
N8A	150	1.0	10	2 X 6.5		217		(23) 10 X 8	8.9			0.04
N. CULV. HDWL.								(24) 6 X 42	28.0			
S. CULV. HDWL.								(25) 6 X 42	28.0			
BORROW AREA												0.50
SUPPLEMENTAL	250			1 X 8 & 1 X 6.5	223	181			20.0	250	50	0.91
TOTAL	8300				5069	11361	3957		519	1400	929	6.00



**REMOVE CONCRETE DETAIL**  
NOT TO SCALE

**CONSTRUCT PERMANENT EROSION CONTROL AT VARIOUS LOCATIONS ON THE AIRPORT**

SBGP No: 3-17-SBGP-TBD  
IDA No: C75-4346

Contract No. MA028

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: NOVEMBER 16, 2015  
PROJECT NO: 15A0001  
CAD FILE: C-501-TYP.DWG  
DESIGN BY: KBS  
DRAWN BY: KBS  
REVIEWED BY: CAH 11/9/15

SHEET TITLE

**EROSION CONTROL SCHEDULE & REMOVE CONCRETE DETAIL**