

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| 778 | * | SALINE | 74 | 2 |

FED. ROAD DIST. NO. 9 ILLINOIS FED. AID PROJECT

(2-B)-1 & (2B-1)-1

INDEX OF SHEETS

- COVER SHEET
 - INDEX OF SHEETS, GENERAL NOTES, SURVEY TIES & BENCH MARKS
 - COMBINED SUMMARY OF QUANTITIES
- SECTION (2-B)-1 ROADWAY & STRUCTURE PLANS**
- TYPICAL SECTIONS
 - SCHEDULE OF QUANTITIES
 - TRAFFIC CONTROL PLANS
 - TRAFFIC CONTROL & STAGING DETAILS
 - PLAN AND PROFILE
 - EROSION CONTROL PLAN
 - STRUCTURE PLANS SN 083-2015
 - CROSS SECTIONS
 - CHANNEL CROSS SECTIONS

SECTION (2B-1)-1 ROADWAY & STRUCTURE PLANS

- TYPICAL SECTIONS
- SCHEDULE OF QUANTITIES
- TRAFFIC CONTROL PLANS
- TRAFFIC CONTROL & STAGING DETAILS
- PLAN AND PROFILE IL 34
- PLAN AND PROFILE DENEAL RD
- EROSION CONTROL PLAN
- MISCELLANEOUS DETAILS
- STRUCTURE PLANS SN 083-2016
- CROSS SECTIONS - IL 34
- CROSS SECTIONS - DENEAL RD
- CHANNEL CROSS SECTIONS

IDOT DISTRICT 9 STANDARD DETAILS

- STD 9-12 SEEDING & MULCHING
- STD 9-16 TYPICAL CROSS SECTION SHOWING STEP CONSTRUCTION ON EXISTING FILL
- STD 9-26 TEMPORARY BITUMINOUS CONCRETE TRANSITIONS
- STD 9-34 BUTT JOINT
- STD 9-83 RURAL SIDE APPROACH DETAILS
- BITUMINOUS SHOULDER AT TRAFFIC BARRIER TERMINAL SECTION TYPE 1 (SPECIAL)

IDOT STANDARDS

| | | | |
|-----------|-----------|-----------|-----------|
| 280001-02 | 606101-01 | 701001 | 702001-03 |
| 420001-05 | 630001-04 | 701006-02 | 704001-01 |
| 420601-03 | 630101-04 | 701011 | 780001-01 |
| 420701-01 | 630201-02 | 701301-01 | 000001-04 |
| 482011-01 | 630301-02 | 701306 | 001001 |
| 515001-02 | 635006-02 | 701311-02 | BLR 23-1 |
| 542301 | 635011-01 | 701321-07 | BLR 24-1 |
| 542401 | 666001 | 701326-01 | |

GENERAL NOTES

- THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- ALL OBSTRUCTIONS WHICH ARE WITHIN THE CLEAR ZONE SHOWN ON THE TYPICAL SECTION, AND ARE NOT SHIELDED BY THE PROPOSED GUARDRAIL, SHALL BE REMOVED BETWEEN STATION 432+96 AND STATION 444+59. TYPICAL OBSTRUCTIONS ARE HEADWALLS, FOUNDATIONS, ETC. WHICH PROJECT 100 mm (4 IN.) OR MORE ABOVE THE GROUNDLINE; AND TREES WHICH WILL MATURE TO A DIAMETER OF 100 mm (4 IN.) OR GREATER.
- IF SO DIRECTED BY THE ENGINEER, DITCHES ADJACENT TO EMBANKMENTS SHALL BE CONSTRUCTED PRIOR TO STARTING THE CONSTRUCTION OF THE EMBANKMENT FILL.
- THE ALGEBRAIC DIFFERENCE BETWEEN THE PAVEMENT AND SHOULDER SLOPES SHALL NOT EXCEED 8% ON IL RT 34, AND 10% ON DENEAL RD. THE SHOULDER ON THE OUTSIDE OF SUPERELEVATED CURVES SHALL BE FLATTENED ACCORDINGLY.
- FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

| | |
|-------------------------------|----------------------|
| ALL BITUMINOUS CONCRETE | (2.016 TONS/CU.YD.) |
| ALL AGGREGATE | (2.05 TONS/CU.YD.) |
| BITUMINOUS MATERIALS: | |
| ON PAVEMENT | (0.09 GAL./SQ.YD.) |
| INTERMEDIATE LIFTS (FOG COAT) | (0.04 GAL./SQ.YD.) |
| ON AGGREGATE SURFACE | (0.32 GAL./SQ.YD.) |
| AGGREGATE (PRIME COAT) | (0.0015 TONS/SQ.YD.) |
| RIPRAP | (1.50 TONS/CU.YD.) |

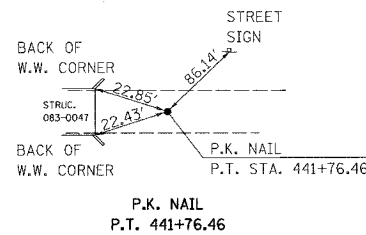
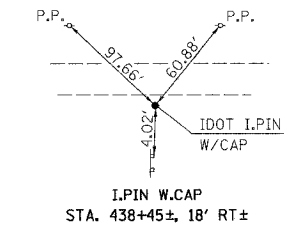
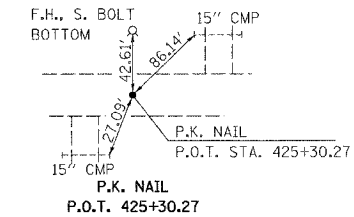
- TREES SHALL BE PRESERVED THROUGHOUT THIS SECTION AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER. GENERALLY, TREES OUTSIDE THE CLEAR ZONE, AND WHICH DO NOT INTERFERE WITH CONSTRUCTION, SHALL NOT BE DISTURBED.
- ATTAINMENT OF PROPER CROWN OR SUPERELEVATION SHALL BE FULLY ACCOMPLISHED WITH THE BITUMINOUS SURFACE REMOVAL OR BITUMINOUS CONCRETE BINDER COURSE OR LEVELING BINDER, WHEN SPECIFIED.
- AGGREGATE SURFACE COURSE TYPE B SHALL BE USED AS DIRECTED BY THE ENGINEER FOR MAINTENANCE PURPOSES. THE GRADATION SHALL BE CA-6 OR CA-10 AS DIRECTED BY THE ENGINEER. A QUANTITY OF 5 TONS PER SECTION HAS BEEN ESTIMATED FOR THIS WORK.
- TRENCH BACKFILL REQUIRED FOR STORM SEWER, SANITARY SEWER, OR WATER MAINS SHALL ONLY BE PLACED UP TO ONE FOOT BELOW THE FINAL GRADE IN AREAS HAVING A PROPOSED GRASS OR SOD SURFACE.
- WHEN WIDENING EXISTING SHOULDERS, THE CONTRACTOR SHALL TRIM EXISTING SURFACE AND FLEXIBLE BASE TO A FIRM, NEAR VERTICAL PLANE BEFORE CONSTRUCTING THE WIDENING. THE COST OF THIS REQUIREMENT IS INCLUDED IN THE UNIT PRICE BID FOR THE BITUMINOUS SHOULDERS.
- AT ALL LOCATIONS WHERE THE PROPOSED BITUMINOUS OR CONCRETE PAVEMENT JOINS AN EXISTING BITUMINOUS OR CONCRETE PAVEMENT, A FULL DEPTH SAWED JOINT SHALL BE CONSTRUCTED. THE COST OF THIS JOINT WILL BE INCLUDED IN THE COST OF THE TYPE OF PAVEMENT BEING CONSTRUCTED.
- THE MINIMUM VERTICAL CLEARANCE FOR PERMANENT SIGNS PLACED ON BACKSLOPES SHALL BE 0.914 m (3 FT.) MEASURED FROM A POINT DIRECTLY BENEATH THE FAR EDGE OF THE SIGN.
- THE LIMITS OF EARTH SLOPES SHOWN IN THE CROSS SECTIONS ARE APPROXIMATE. THE ACTUAL SLOPE USED SHALL BE DETERMINED BY THE MATERIAL CLASSIFICATION AS DEFINED IN ARTICLE 202.04, AND AS DIRECTED BY THE ENGINEER.
- THE DISTRICT BUREAU OF OPERATIONS SHALL BE NOTIFIED AT LEAST 10 DAYS PRIOR TO PLACEMENT OF THE FINAL PAVEMENT MARKINGS. THE BUREAU OF OPERATIONS WILL THEN DETERMINE THE ACTUAL LIMITS TO BE STRIPED AS "NO PASSING" ZONES.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF TRAFFIC OPERATIONS 72 HOURS PRIOR TO THE SHUT-DOWN OR CUTTING OF EXISTING DETECTOR LOOPS.
- RECLAIMED ASPHALT PAVEMENT (RAP) WILL NOT BE ALLOWED FOR USE AS AGGREGATE IN AGGREGATE SHOULDERS, TYPE B.
- THE ADVANCE DETECTOR LOOPS ARE TYPICALLY LOCATED 300 FEET IN ADVANCE OF THE STOP BAR. THE BUREAU OF OPERATIONS SHOULD APPROVE THE LOOP LOCATIONS PRIOR TO INSTALLATION.
- THE CENTERLINE PAVEMENT MARKING SHALL BE REMOVED FROM THE STOP BAR TO THE SAND ATTENUATORS OR DRUMS. EDGE LINE PAVEMENT MARKING SHALL BE REMOVED IF A 10 FOOT LANE WIDTH CANNOT BE MAINTAINED. TEMPORARY EDGE LINES SHOULD BE INSTALLED WHEN THE EDGE LINES ARE REMOVED.
- THE BARRIER WALL REFLECTORS SHALL BE INSTALLED PRIOR TO OPENING TO TRAFFIC.
- ANY TIME THE CONCRETE BARRIER IS NOT IN THE PROPER POSITION, FLAGGERS SHALL BE IN PLACE TO CONTROL TRAFFIC. THE TEMPORARY TRAFFIC SIGNALS SHALL BE SET TO FLASH ALL RED.
- "NARROW BRIDGE" SIGNS WITH ADVISORY TAGS "14 FT 0 IN" SHALL BE ERECTED BETWEEN ONE ROAD CONSTRUCTION AHEAD AND THE SIGNAL AHEAD SIGNS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING STRUCTURE. NOMINAL CONSTRUCTION VARIATIONS AND PLAN DETAILS WILL NOT BE CAUSE FOR ADDITIONAL COMPENSATION. EXISTING PLANS ARE AVAILABLE FOR INSPECTION AT THE DISTRICT OFFICE.
- RELOCATION OF EXISTING SIGNS WILL NOT BE MEASURED FOR PAYMENT AND WILL BE CONSIDERED INCIDENTAL TO THE WORK CAUSING THE RELOCATION.
- WORK ON DENEAL RD. (SEC (2B-1)-1) MAY BE PERFORMED CONCURRENTLY WITH WORK ON SEC (2-B)-1 IN ORDER TO EXPEDITE CONSTRUCTION AND MINIMIZE DOUBLE HANDLING OF EXCAVATED MATERIALS.
- COMMITMENTS: NONE

SECTION (2-B)-1 NOTES

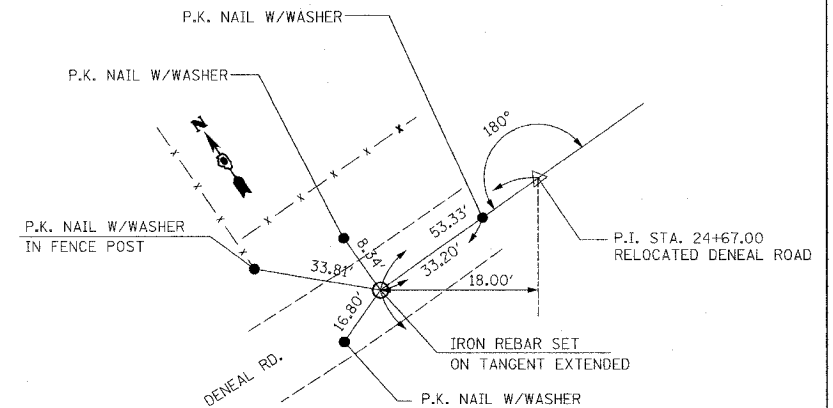
- ALL FIELD ENTRANCE CULVERT EXTENSIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH METHOD III AS SPECIFIED IN ARTICLE 542.06 OF THE STANDARD SPECIFICATIONS. THE ENTIRE LENGTH OF THE EXISTING CULVERT SHALL BE CLEANED OF ALL EARTH AND DEBRIS BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. THE COST OF THIS WORK SHALL BE PAID ACCORDING TO ARTICLE 109.04.

SECTION (2B-1)-1 NOTES

- FORMS FOR CONCRETE GUTTER SHALL BE OF METAL ONLY, EXCEPT THAT WOOD FORMS MAY BE USED ON SHORT RADIUS CURVES.
- ALL PIPE CULVERTS DESIGNATED ON THE PLANS (R.C.C.P.) SHALL BE "REINFORCED CONCRETE CULVERT, STORM DRAIN AND SEWER PIPE" CONFORMING TO THE REQUIREMENTS OF ARTICLE 1040.03.
- PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS AS NEEDED ACCORDING TO THE SEASONAL REQUIREMENTS OF ARTICLE 420.21.
- THE REMOVAL OF EXISTING ENTRANCE PIPE CULVERTS ENCASED IN CONCRETE WILL BE CONSIDERED INCLUDED IN THE OTHER ITEMS OF CONSTRUCTION IF ONLY THE ENDS OF THE CULVERT [0.6 m (2 FT.) OR LESS] ARE ENCASED. IF MORE THAN 0.6 m (2 FT.) AT THE ENDS OF THE CULVERT ARE ENCASED IN CONCRETE, THE REMOVAL WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.



IL ROUTE 34 SURVEY TIES



DENEAL ROAD SURVEY TIE

Commitment

The field entrance at Station 434+35.00 RT shall be modified as follows; the entrance shall extend to the new ROW line, shall taper to a 16' surface width at this point, the back of the entrance shall be centered at Station 434+43.00, the existing 24" clay pipe beneath the entrance shall be removed and replaced with a 24" CMP with a minimum length of 24', and the entrance shall be blended to match into the existing surface at the new ROW line. This work shall be done per Article 109.04 of the Standard Specifications.

BENCHMARKS

- Bench Mark 200:
R.R. Spike In P.P. Sta. 432+31±, 25' Rt. El. 378.77
- IDOT Bench Mark 100:
R.R. Spike In P.P. Sta. 439+33.7±, 36.9' Rt. El. 378.465
- Iron Pin w/Cap * 083-400 Sta. 438+50±, 18' Rt. El. 379.792
- Iron Pin w/Cap * 083-300 Sta. 442+24±, 34' Lt. El. 384.419

Prepared By: *De Zambino*
DISTRICT STUDIES & PLANS ENGINEER

Examined By: *James Travis Emery*
DISTRICT LAND ACQUISITION ENGINEER

Examined By: *[Signature]*
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: *[Signature]*
DISTRICT OPERATIONS ENGINEER

Examined By: *[Signature]*
DISTRICT CONSTRUCTION ENGINEER

Examined By: *[Signature]*
DISTRICT MATERIALS ENGINEER

Examined By: *[Signature]*
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Approved By: *[Signature]*
DISTRICT ENGINEER

DATE: November 3, 2003

| REVISIONS | |
|-----------|------|
| NAME | DATE |
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ILLINOIS DEPARTMENT OF TRANSPORTATION
FAP ROUTE 778 (IL 34)
SECS. (2-B)-1 & (2B-1)-1, SALINE COUNTY
**INDEX OF SHEETS, GENERAL NOTES,
SURVEY TIES AND BENCHMARKS**

SCALE: VERT. _____
HORIZ. _____
DATE _____ DRAWN BY _____
CHECKED BY _____

REV