

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	1
		ILLINOIS	74550	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

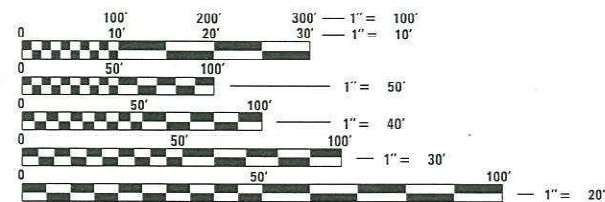
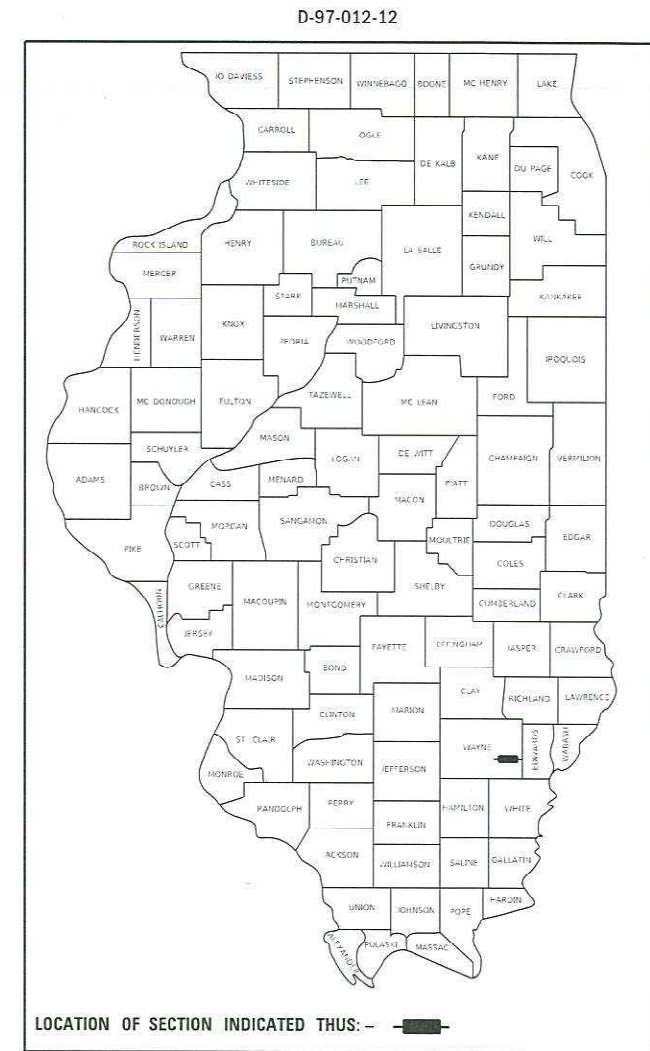
Station Equations

- 359+60.00 BK = 360+30.00 AH
- 469+79.70 BK = 480+33.10 AH
- 496+56.48 BK = 496+63.00 AH
- 663+23.39 BK = 663+65.61 AH
- 731+25.31 BK = 732+55.33 AH
- 739+23.40 BK = 738+00.00 AH

PROPOSED HIGHWAY PLANS

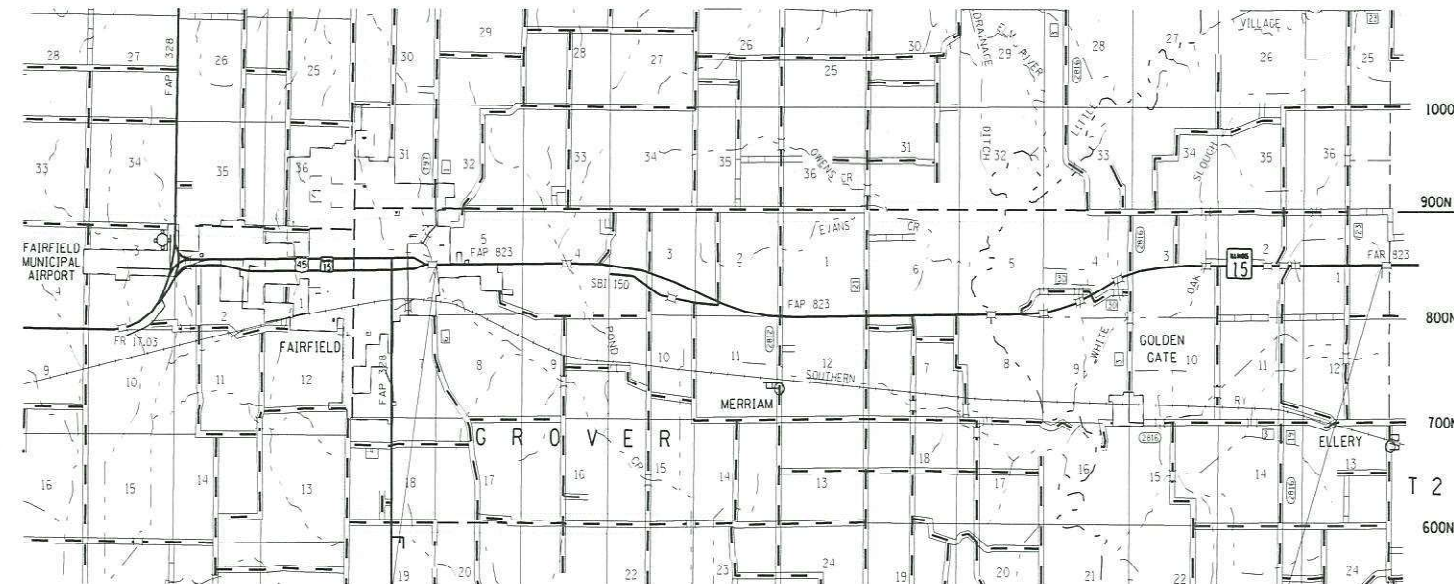
FAP ROUTE 823 (ILL 15)
SECTION (22,23,24)RS-3
PROJECT NHPP-J5LE(942)
RESURFACING (3P)
WAYNE COUNTY

C-97-015-12



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811



PROJECT START STATION: 837+34.38

PROJECT END STATION: 241+00.00

PROJECT ENGINEER – TOM RONAN 217-342-8320
PROJECT MANAGER – EDWIN TIPSWORD

GROSS LENGTH = 59,241.6 FT. = 11.22 MILE
NET LENGTH = 56,644.8 FT. = 10.73 MILE

CONTRACT NO. 74550

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED FEBRUARY 11 20 21
Jerry P. Myer
REGIONAL ENGINEER

March 19, 2021
Scott A. Elk
ENGINEER OF DESIGN AND ENVIRONMENT

March 19, 2021
James J. ...
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

THE RESIDENT ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE CURING TIME FOR THE HOT-MIX ASPHALT SURFACE COURSE.

THE MATERIAL USED FOR AGGREGATE WEDGE SHOULDERS, TYPE B SHALL BE CRUSHED STONE, CRUSHED CONCRETE, OR RAP. THE MATERIAL USED FOR AGGREGATE SURFACE COURSE, TYPE B SHALL BE CRUSHED STONE OR CRUSHED CONCRETE.

THE EXISTING PAVEMENT SHALL BE PATCHED IN ACCORDANCE WITH SECTION 442 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS. THE QUANTITY OF PATCHING SHOWN ON THE PLANS IS AN ESTIMATE ONLY, THE FINAL LOCATIONS AND QUANTITY SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH SECTION 780 OF THE STANDARD SPECIFICATIONS. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE MILLED SURFACE, BITUMINOUS MATERIALS (PRIME COAT), HOT-MIX ASPHALT BINDER COURSE, AND HOT-MIX ASPHALT SURFACE COURSE AS SPECIFIED IN SECTION 703 OF THE STANDARD SPECIFICATIONS. TEMPORARY TAPE SHALL BE USED ON THE SURFACE COURSE AND PAINT SHALL BE USED ON MILLED SURFACES.

THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

HOT MIX ASPHALT	112 LBS/SQ YD/IN
BITUMINOUS MATERIALS (TACK COAT) ON MILLED SURFACES BETWEEN HMA LIFTS	0.05 LBS/SQ FT 0.025 LBS/SQ FT
GRANULAR MATERIAL	2.05 TONS/CU YD

INDEX OF SHEETS

SHEET NO.	ITEM
1	COVER SHEET
2	INDEX OF SHEETS, GENERAL NOTES & HIGHWAY STANDARDS
3-4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6-10	SCHEDULE OF QUANTITIES
11-30	PLAN SHEETS
31-32	CONSTRUCTION DETAILS
33-40	BOX CULVERT DETAILS
41-44	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)
45	DETOUR DETAIL

THE FOLLOWING STANDARDS ARE A PART OF THESE PLANS AND ARE INCLUDED FOLLOWING THE LAST NUMBERED SHEET OF THE PLANS.

STD. NO.	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
635001-02	DELINEATORS
642006-01	SHOULDER RUMBLE STRIPS 8"
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
701006-05	OFF-RD MOVING OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY FOR SPEEDS >= 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS >= 45MPH
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-08	TRAFFIC CONTROL DEVICES
B.L.R. 21-9	TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS

APPLICATION	AC/PG	DESIGN AIR VOIDS	MIXTURE COMPOSITION	FRICTION AGGREGATE	QUALITY MANAGEMENT
HMA SURFACE COURSE, MIX "C", N70 (2")	PG 64-22	4.0% @ N=70	IL - 9.5	MIXTURE C	PPF
HMA BINDER COURSE, IL-9.5FG, N70, (1 3/4")	PG 64-22	4.0% @ N=70	IL - 9.5FG	N/A	PPF
INCIDENTAL HMA SURFACING	PG 64-22	4.0% @ N=70	IL - 9.5	MIXTURE C	QC/QA
CLASS "D" PAVEMENT PATCHING	PG 64-22	4.0% @ N=70	IL - 19.0	N/A	OC/OA

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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS &
GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	2
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

80% FED
20% STATE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 RURAL	0005 URBAN	
20700220	POROUS GRANULAR EMBANKMENT	CU YD	1163	1163		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	20	20		
28100107	STONE RIPRAP, CLASS A4	SQ YD	137	137		
28100109	STONE RIPRAP, CLASS A5	SQ YD	108	108		
28200200	FILTER FABRIC	SQ YD	246	246		
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	756	572	184	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	124342	113403	10939	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	2975	2850	125	
40600990	TEMPORARY RAMP	SQ YD	3608	3420	188	
40602970	HOT-MIX ASPHALT BINDER COURSE, IL-9.5FG, N70	TON	17938	16396	1542	
40604052	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N70	TON	20500	18738	1762	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	338	246	92	
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	181236	165155	16081	
44201798	CLASS D PATCHES, TYPE I, 13 INCH	SQ YD	1264	1264		

80% FED
20% STATE

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 RURAL	0005 URBAN	
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	854	812	42	
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	16	16		
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SQ YD	391	391		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	4702	4584	118	
50105220	PIPE CULVERT REMOVAL	FOOT	134	134		
50200450	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	CU YD	68	68		
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	2	2		
54001002	BOX CULVERT END SECTIONS, CULVERT NO. 2	EACH	2	2		
54001003	BOX CULVERT END SECTIONS, CULVERT NO. 3	EACH	2	2		
54010804	PRECAST CONCRETE BOX CULVERTS 8' X 4'	FOOT	44	44		
54010805	PRECAST CONCRETE BOX CULVERTS 8' X 5'	FOOT	134	134		
54010904	PRECAST CONCRETE BOX CULVERTS 9' X 4'	FOOT	62	62		
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	56	56		
63500310	REMOVE AND REINSTALL DELINEATORS	EACH	6	6		

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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	3
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

80% FED
20% STATE

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SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 RURAL	0005 URBAN	
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	98901	96591	2310	
66700205	PERMANENT SURVEY MARKERS, TYPE I	EACH	46	46		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	7	7		
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	7	7		
67100100	MOBILIZATION	L SUM	1	1		
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD	LSUM	1	1		
	701201					
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD	L SUM	1	1		
	701306					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD	L SUM	1		1	
	701501					
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	28	14	14	
70300100	SHORT TERM PAVEMENT MARKING	FOOT	16722	15120	1602	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1768	1680	88	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	144287	132454	11833	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	144287	132454	11833	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 RURAL	0005 URBAN	
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	705	638	67	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	39	39		
X0326440	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)	SO YD	1932	1407	525	
X0900064	MEMBRANE WATERPROOFING SYSTEM FOR BURIED STRUCTURES	SO YD	56	56		
X1200050	BOX CULVERT REMOVAL	FOOT	106	106		
X2501000	SEEDING, CLASS 2 (SPECIAL)	ACRE	0.2	0.2		
X7011800	TRAFFIC CONTROL AND PROTECTION, STANDARD BLR	L SUM	1	1		
	21					
Z0016702	DETOUR SIGNING	L SUM	1	1		
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	53054	50401	2653	
Z0049799	PROTECTING OR RESETTING SURVEY MARKERS	EACH	3	3		
* Z0054505	ROCK FILL - REPLACEMENT	TON	63	63		
Z0070202	SURVEY MARKER VAULT	EACH	3	3		

* SPECIALTY ITEM

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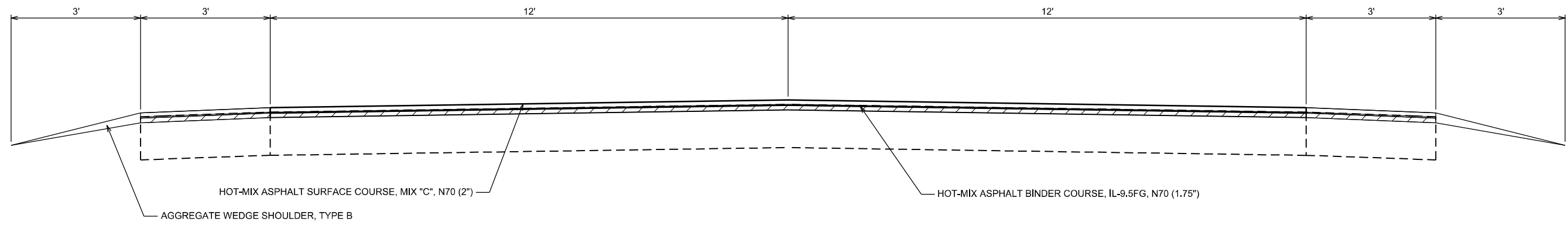
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

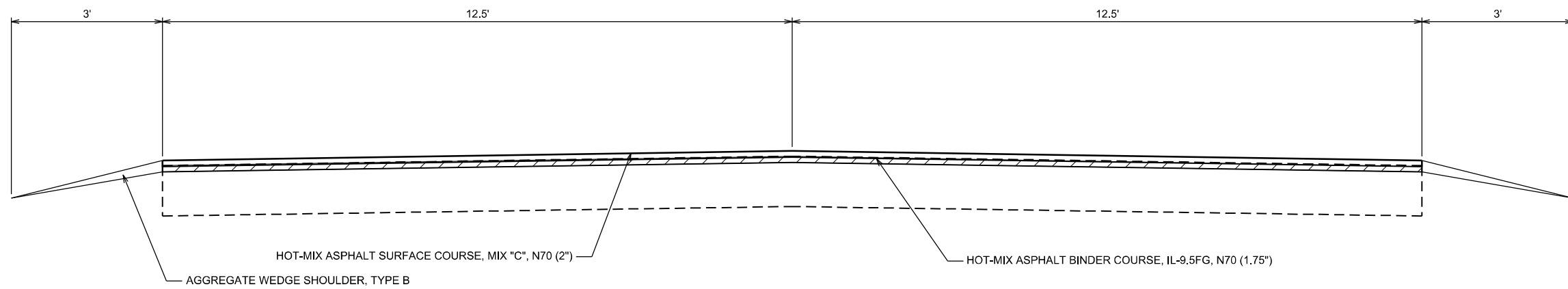
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	

PROPOSED TYPICAL SECTIONS



STA. 241+00.00 TO STA. 294+38.97
 STA. 296+61.03 TO STA. 310+37.00
 STA. 301+90.00 TO STA. 309+60.00
 STA. 311+84.00 TO STA. 347+15.01
 STA. 351+26.99 TO STA. 359+60.00
 STA. 360+30.00 TO STA. 401+73.47
 STA. 403+86.53 TO STA. 428+11.00
 STA. 429+47.00 TO STA. 450+36.00
 STA. 451+20.00 TO STA. 469+79.70
 STA. 480+33.10 TO STA. 484+60.88
 STA. 497+03.00 TO STA. 663+23.39
 STA. 663+65.61 TO STA. 731+25.31
 STA. 732+55.33 TO STA. 739+23.40
 STA. 738+00.00 TO STA. 755+60.50
 STA. 757+13.50 TO STA. 784+12.80



STA. 784+12.80 TO STA. 837+34.38

HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	5
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

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RESURFACING SCHEDULE			LENGTH	WIDTH	AREA	HOT-MIX ASPHALT SURFACE REMOVAL, 1.5"	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	HOT-MIX ASPHALT BINDER COURSE, IL-9.5FG, N70	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "C", N70	BITUMINOUS MATERIALS (TACK COAT)	AGGREGATE WEDGE SHOULDERS, TYPE B	TEMPORARY RAMP	LONGITUDINAL JOINT SEALANT	SHOULDER RUMBLE STRIPS, 3 INCH	SHORT TERM PAVEMENT MARKING	SHORT TERM PAVEMENT MARKING REMOVAL
STATION	TO	STATION	FOOT	FOOT	SQ YD	SQ YD	SQ YD	TON	TON	POJND	TON	SQ YD	FOOT	FOOT	FOOT	SQ FT
241+00.00	TO	241+45.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
241+45.00	TO	293+93.97	5249.0	30.0	17496.6	17496.6	0.0	1713.7	1958.5	11810.2	495.2	0.0	5249.0	10435.9	1574.7	174.9
293+93.97	TO	294+38.97	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
294+38.97	TO	296+61.03	222.1													
BRIDGE OMISSION																
296+61.03	TO	297+06.03	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
297+06.03	TO	300+92.00	386.0	30.0	1286.6	1286.6	0.0	122.6	140.1	868.4	26.6	0.0	386.0	559.9	115.8	12.9
300+92.00	TO	301+37.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
301+37.00	TO	301+90.00	53.0													
BRIDGE OMISSION																
301+90.00	TO	302+35.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
302+35.00	TO	309+15.00	680.0	30.0	2266.7	2266.7	0.0	218.8	250.1	1530.0	55.0	0.0	680.0	1158.0	204.0	22.7
309+15.00	TO	309+60.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
309+60.00	TO	311+84.00	224.0													
BRIDGE OMISSION																
311+84.00	TO	312+29.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
312+29.00	TO	346+70.01	3441.0	30.0	11470.0	11470.0	0.0	1124.1	1284.6	7742.3	326.6	0.0	3441.0	6882.0	1032.3	114.7
346+70.01	TO	347+15.01	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
347+15.01	TO	351+26.99	412.0													
BRIDGE OMISSION																
351+26.99	TO	351+71.99	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
351+71.99	TO	359+60.00	788.0	30.0	2626.7	2626.7	0.0	257.4	294.2	1773.0	74.8	0.0	788.0	1576.0	236.4	26.3
359+60 BK = 360+30 AH																
STATION EQUATION																
360+30.00	TO	401+28.47	4098.5	30.0	13661.6	13661.6	0.0	1326.6	1516.1	9221.6	353.5	0.0	4098.5	7448.9	1229.5	136.6
401+28.47	TO	401+73.47	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
401+73.47	TO	403+86.53	213.1													
BRIDGE OMISSION																
403+86.53	TO	404+31.53	45.0	30.0	150.0	0.0	150.0	14.7	17.0	101.3	4.3	180.0	45.0	90.0	13.5	1.5
404+31.53	TO	427+66.00	2334.5	30.0	7781.6	7781.6	0.0	760.4	868.9	5252.6	215.2	0.0	2334.5	4534.9	700.3	77.8
427+66.00	TO	428+11.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
428+11.00	TO	429+47.00	136.0													
BRIDGE OMISSION																
429+47.00	TO	429+92.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
429+92.00	TO	449+91.00	1999.0	30.0	6663.3	6663.3	0.0	653.0	746.3	4497.8	189.7	0.0	1999.0	3998.0	599.7	66.6
449+91.00	TO	450+36.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
450+36.00	TO	451+20.00	84.0													
BRIDGE OMISSION																
451+20.00	TO	451+65.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
451+65.00	TO	469+79.70	1814.7	30.0	6049.0	6049.0	0.0	592.8	677.5	4083.1	172.2	0.0	1814.7	3629.4	544.4	60.5
469+79.70 BK = 480+33.10 AH																
STATION EQUATION																
480+33.10	TO	484+15.88	382.8	30.0	1275.9	1275.9	0.0	125.0	142.9	861.3	36.3	0.0	382.8	765.6	114.8	12.8
484+15.88	TO	484+60.88	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
484+60.88	TO	496+56.48	1195.6													
BRIDGE OMISSION																
496+56.48 BK = 496+63.00 AH																
STATION EQUATION																
496+63.00	TO	497+03.00	40.0													
BRIDGE OMISSION																
497+03.00	TO	497+48.00	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
497+48.00	TO	663+23.39	16575.4	30.0	55251.3	55251.3	0.0	5392.8	6163.2	37294.6	1509.8	0.0	16575.4	31816.8	4972.6	552.5
663+23.39 BK = 663+65.61 AH																
663+65.61	TO	731+25.31	6759.7	30.0	22532.3	22532.3	0.0	2187.5	2500.0	15209.3	581.6	0.0	6759.7	12255.4	2027.9	225.3
731+25.31 BK = 732+55.33 AH																
STATION EQUATION																
732+55.33	TO	739+23.40	668.1	30.0	2226.9	2226.9	0.0	214.1	244.6	1503.2	51.3	0.0	668.1	1080.1	200.4	22.3
739+23.40 BK = 738+00.00 AH																
STATION EQUATION																
738+00.00	TO	755+15.50	1715.5	30.0	5718.3	5718.3	0.0	560.4	640.5	3859.9	162.8	0.0	1715.5	3431.0	514.7	57.2
755+15.50	TO	755+60.50	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
755+60.50	TO	757+13.50	153.0													
BRIDGE OMISSION																
757+13.50	TO	757+58.50	45.0	30.0	150.0	0.0	150.0	14.7	16.8	101.3	4.3	180.0	45.0	90.0	13.5	1.5
757+58.50	TO	784+12.80	2654.3	30.0	8847.7	8847.7	0.0	867.1	990.9	5972.2	251.9	0.0	2654.3	5308.6	796.3	88.5
RURAL TOTALS			53134.0		168004.5	165154.5	2850.0	16395.6	18737.8	113403.0	4583.6	3420.0	50401.3	96590.7	15120.4	1679.9
URBAN/RURAL SPLIT																
784+12.80	TO	809+76.00	2563.2	25.0	7120.0	8544.0	0.0	791.3	904.4	5767.2	109.6	0.0	2563.2	2310.4	1588.9	85.4
809+76.00	TO	836+89.38	2713.4	25.0	7537.2	7537.2	0.0	738.6	844.2	5037.6	4.3	0.0	45.0	0.0	13.5	1.5
836+89.38	TO	837+34.38	45.0	25.0	125.0	0.0	125.0	12.3	14.0	84.4	4.3	187.5	45.0	0.0	13.5	1.5
URBAN TOTALS			5321.6		14782.2	16081.2	125.0	1542.2	1762.6	10939.2	118.2	187.5	2653.2	2310.4	1615.9	88.4

USER NAME = stefenmk	DESIGNED -	REVISD -
	DRAWN -	REVISD -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISD -
PLOT DATE = 2/16/2021	DATE -	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RESURFACING SHCEDULE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	6
CONTRACT NO. 74550				
ILLINOIS		FED. AID PROJECT		

SURVEY MONUMENT SCHEDULE								
STATION	POINT NO.	OFFSET	TYPE OF POINT	TIE POINTS	DESCRIPTION	PERMANENT SURVEY MARKERS, TYPE 1	PROTECTING OR RESETTING SURVEY MARKERS	SURVEY MARKER VAULTS
		FEET				EACH	EACH	EACH
262+79.80	30008	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
289+04.59 (BK) = 289+07.00 (AH)	30009		P.O.T.,STA. EQ.	YES	PK NAIL		1	1
296+77.10	30010	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
306+00.00 (BK) = 305+96.06 (AH)	30011	0.00	P.O.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
315+09.10	30012		P.I.(KINK)	YES	PK NAIL		1	1
341+07.70	30014	0.00	P.I.(KINK)	YES	PK NAIL		1	1
359+60.0 (BK) = 360+22.18 (AH)	30015	0.00	P.O.T.,STA. EQ.	YES	PK NAIL	1		
361+99.53	30016	0.00	P.C.	YES	PK NAIL	1		
388+81.99	30017	0.00	P.O.C.	YES	MAG NAIL WITH WASHER	1		
415+64.45 (BK) = 415+70.63 (AH)	30018	0.00	P.T.	YES	PK NAIL	1		
417+48.32	30019	0.00	P.C.	YES	PK NAIL	1		
436+24.04	30020	0.00	P.O.C.	YES	MAG NAIL WITH WASHER	1		
454+99.75	30021	0.00	P.T./P.C.	YES	PK NAIL	1		
462+26.55	30022		P.I.	YES	PK NAIL	1		
469+53.54	30023	0.00	P.T.	YES	PK NAIL	1		
469+65.24 (BK) = 480+33.10 (AH)	30024	0.00	P.O.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
496+60.94 (BK) = 496+70.58 (AH)	30025	0.00	P.O.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
498+20.40	30026	0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
500+70.40	30027		P.I.	YES	MAG NAIL WITH WASHER	1		
503+20.40	30028	0.00	P.T.	YES	MAG NAIL WITH WASHER	1		
513+19.23	30029	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
530+99.99	30030	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
544+99.99	30031	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
SUBTOTALS						20	3	3

SURVEY MONUMENT SCHEDULE (CONTINUED)								
STATION	POINT NO.	OFFSET	TYPE OF POINT	TIE POINTS	DESCRIPTION	PERMANENT SURVEY MARKERS, TYPE 1	PROTECTING OR RESETTING SURVEY MARKERS	SURVEY MARKER VAULTS
		FEET				EACH	EACH	EACH
568+94.31	30035	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
588+68.07	30037	0.00	P.I.(KINK)	YES	MAG NAIL WITH WASHER	1		
602+49.99		0.00	P.C.	YES	NO DESCRIPTION PROVIDED	1		
603+99.99		0.4 LT.	P.I.	YES	NO DESCRIPTION PROVIDED	1		
605+49.99		0.00	P.T.	YES	NO DESCRIPTION PROVIDED	1		
610+00.00		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
615+00.00		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
620+00.00		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
625+79.72		0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
627+71.39		0.00	P.I.	YES	MAG NAIL WITH WASHER	1		
629+63.05		0.00	P.T.	YES	MAG NAIL WITH WASHER	1		
637+86.90		0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
650+78.46		0.00	P.I.	YES	IRON PIN WITH CAP	1		
663+27.90 (BK) = 663+75.30 (AH)		0.00	P.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
670+07.29		0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
700+00.00		0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
721+36.00		0.00	P.O.C.	YES	MAG NAIL WITH WASHER	1		
731+22.29 (BK) = 732+48.42 (AH)		0.00	P.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
736+72.40		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
739+19.17 (BK) = 738+00.00 (AH)		0.00	P.O.T.,STA. EQ.	YES	MAG NAIL WITH WASHER	1		
744+73.00		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
751+47.25		0.00	P.C.	YES	MAG NAIL WITH WASHER	1		
756+47.26		0.00	P.I.	YES	MAG NAIL WITH WASHER	1		
756+63.35		0.00	P.O.C.	YES	MAG NAIL WITH WASHER	1		
761+47.25		0.00	P.T.	YES	MAG NAIL WITH WASHER	1		
761+96.50		0.00	P.O.T.	YES	MAG NAIL WITH WASHER	1		
SUBTOTALS						26	0	0
SUBTOTALS OTHER SCHEDULE						20	3	3
PROJECT TOTALS						46	3	3

BOX CULVERT SCHEDULE																					
LOCATION	STRUCTURE NUMBER	FILL HEIGHT	PRECAST CONCRETE BOX CULVERTS 8' X 5'	PRECAST CONCRETE BOX CULVERTS 9' X 4'	PRECAST CONCRETE BOX CULVERTS 8' X 4'	PIPE CULVERT REMOVAL	BOX CULVERT REMOVAL	BOX CULVERT END SECTIONS, CULVERT NO. 1	BOX CULVERT END SECTIONS, CULVERT NO. 2	BOX CULVERT END SECTIONS, CULVERT NO. 3	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	POROUS GRANULAR EMBANKMENT	STONE RIPRAP, CLASS A4	STONE RIPRAP, CLASS A5	FILTER FABRIC	ROCK FILL-REPLACEMENT	GEOCOMPOSITE WALL DRAIN	MEMBRANE WATERPROOFING SYSTEM FOR BURIED STRUCTURES	TEMPORARY EROSION CONTROL SEEDING	SEEDING, CLASS 2 (SPECIAL)	
		FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	CU YD	CU YD	SQ YD	SQ YD	SQ YD	TON	SQ YD	SQ YD	POUND	ACRE	
721+36	096-7080	11.0	134.0	-	-	134.0	-	2.0	-	-	29.0	881.3	-	108.2	108.2	28.7	-	-	10	0.1	
736+72	096-7079	5.0	-	62.0	-	-	62.0	-	2.0	-	20.0	218.9	70.2	-	70.2	19.8	-	-	5	0.05	
744+73	096-7078	1.3	-	-	44.0	-	44.0	-	-	2.0	18.7	62.7	67.2	-	67.2	18.6	55.4	35.4	5	0.05	
TOTALS			134.0	62.0	44.0	134.0	106.0	2.0	2.0	2.0	67.7	1162.8	137.4	108.2	245.6	67.2	55.4	35.4	20.0	0.2	

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USER NAME = steffemk	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 3/31/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SURVEY MONUMENT AND
BOX CULVERT SCHEDULE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	7
CONTRACT NO. 74550				
ILLINOIS		FED. AID PROJECT		

MODEL: Default
 FILE NAME: \\paulamr.com\data\illinois.gov\PIWDOT\Documents\DOT_Office\Drawings\74550\CAD\Drawings\74550-EntranceSchedules.dwg

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
RT	241+84	FE	0.9		
LT	241+84	FE	1.7		
RT	247+47	FE	1.5		
LT	256+00	FE	1.6		
RT	256+30	PE	4.3		
RT	256+30	MBT1	2.5		
LT	262+52	FE	1.2		
RT	262+79	PRA	3.0		
RT	262+79	PRA		3.0	17
RT	263+55	FE	2.1		
RT	264+93	PE	5.7		
LT	264+93	FE	2.0		
RT	269+31	FE	1.5		
LT	269+43	FE	2.0		
RT	276+15	FE	1.7		
LT	276+88	PE	3.1		
RT	276+88	MBT2	5.0		
RT	280+01	MBT2	5.0		
LT	280+21	CE	3.1		
RT	282+62	FE	2.1		
RT	288+98	MBT2	5.0		
LT	289+23	PE	2.3		
RT	289+56	FE	2.5		
LT	300+35	PRA		11.0	63
RT	303+08	PRA		10.6	61
LT	308+72	FE	1.9		
RT	309+71	FE	2.1		
RT	315+22	FE	2.9		
LT	323+25	FE	2.0		
RT	328+00	FE	2.5		
LT	341+08	PRA	4.2		
RT	341+08	FE	2.5		
RT	354+33	FE	2.2		
LT	354+33	FE	2.7		
RT	361+78	FE	1.8		
LT	361+78	FE	1.8		
LT	368+21	FE	3.1		
RT	368+21	PRA	3.1		
RT	394+98	PRA		25.1	144
LT	394+98	PRA	0.0	0.0	0
RT	408+90	FE	3.1		
LT	409+17	FE	2.5		
LT	418+98	PRA		7.6	43
RT	418+98	PRA	3.7		

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
LT	423+64	FE	2.3		
LT	438+54	FE	3.4		
RT	438+54	FE	2.4		
LT	463+74	FE	3.3		
RT	465+48	PRA	4.1		
RT	483+41	FE	3.7		
LT	484+11	FE	2.4		
RT	496+80	FE	1.4		
LT	500+95	PE	1.5		
RT	502+82	CE		6.9	39
RT	505+88	PE	6.4		
RT	505+88	MBT1	2.5		
LT	505+88	PE	2.2		
RT	513+83	PE	4.0		
LT	514+40	PRA		7.5	43
RT	519+14	FE	3.5		
LT	527+72	FE	1.4		
LT	532+85	PE	3.8		
RT	533+31	PE	6.3		
RT	534+52	FE	1.4		
RT	540+47	PE	5.3		
RT	540+48	MBT1	2.5		
LT	540+63	PRA		8.2	47
LT	541+63	PE	3.7		
LT	543+37	PE	4.0		
RT	543+37	MBT2	5.0		
LT	546+12	PE	2.6		
RT	546+12	MBT2	5.0		
LT	548+24	PE	2.5		
RT	548+24	MBT2	5.0		
RT	550+40	MBT2	5.0		
LT	553+44	PE	2.5		
RT	553+44	MBT2	5.0		
RT	554+05	FE	1.9		
LT	556+71	PE	1.4		
RT	556+71	MBT2	5.0		
LT	567+67	PE	4.8		
RT	567+67	MBT2	5.0		
LT	568+43	PE	4.0		
RT	569+06	PRA		6.1	35
LT	569+06	PRA		5.9	34
RT	569+78	PE	3.1		
RT	570+49	PE	3.5		
RT	571+20	PE	1.6		

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
RT	574+27	PE	2.1		
RT	582+27	PE	2.9		
RT	582+27	MBT1	2.5		
LT	592+50	FE	2.5		
RT	592+80	FE	2.5		
LT	601+39	FE	2.0		
LT	606+11	PE		5.0	28
LT	607+22	PE		2.6	15
LT	608+94	PE		3.4	20
LT	609+89	PE		3.4	19
LT	611+16	PE	1.4		
LT	612+35	PE	1.4		
LT	613+50	MBT1	2.5		
LT	613+50	PE	3.4		
LT	614+57	MBT1	2.5		
LT	614+57	PE	3.4		
LT	618+97	PE	3.0		
LT	618+98	MBT1	2.5		
RT	621+06	PRA		4.9	28
RT	621+76	PE	3.1		
RT	622+38	PE	3.3		
RT	622+84	MBT1	2.5		
RT	622+84	PE	3.3		
RT	623+78	PE	4.2		
RT	623+78	MBT1	2.5		
RT	624+65	PE	2.8		
RT	624+65	MBT1	2.5		
RT	625+53	PE	2.9		
RT	625+53	MBT1	2.5		
LT	625+69	MBT1	2.5		
LT	625+69	PE	3.3		
RT	628+44	FE	1.7		
LT	631+05	MBT1	2.5		
LT	631+05	PE	3.0		
LT	632+07	PE	1.5		
RT	633+56	PE	4.7		
RT	633+56	MBT1	2.5		
RT	638+71	FE	2.5		
LT	642+31	PE	2.0		
LT	646+09	PE	2.3		
LT	647+35	PE	3.3		
LT	647+35	MBT1	2.5		
LT	654+88	FE	3.2		
RT	655+14	PE	2.1		

USER NAME = stefenmk	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/16/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENTRANCE SCHEDULE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	8
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	

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ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
LT	661+03	PRA		6.8	39
RT	661+20	PRA		8.2	47
RT	665+45	PE	2.7		
RT	665+45	MBT1	2.5		
LT	666+03	PE		4.2	24
RT	669+06	PE	2.7		
RT	669+06	MBT1	2.5		
LT	669+16	MBT1	2.5		
LT	669+16	PE		6.3	36
LT	669+79	MBT1	2.5		
LT	669+79	PE	3.1		
RT	672+19	CE		5.8	33
RT	672+19	MBT1	2.5		
LT	672+64	MBT1	2.5		
LT	672+64	PE	2.2		
LT	675+17	PE		2.8	16
LT	680+65	PE	3.1		
RT	681+53	FE	3.2		
LT	691+07	PE	4.8		
RT	697+41	FE	2.5		
LT	697+54	CE		4.1	23
LT	697+54	MBT1	2.5		
LT	705+06	PRA		6.0	34
RT	705+06	PRA		5.6	32
RT	709+31	PE		3.1	18
RT	709+31	MBT1	2.5		
LT	709+93	PE	1.7		
LT	709+93	MBT1	2.5		
RT	712+01	PE		3.1	18
RT	712+01	MBT1	2.5		
RT	712+69	PE	2.0		
RT	712+69	MBT1	2.5		
LT	712+73	PE	2.1		
LT	712+73	MBT1	2.5		
LT	713+38	PE	1.4		
RT	715+27	PE	1.8		
RT	715+27	MBT1	2.5		
RT	716+46	PE	1.8		
RT	716+46	MBT1	2.5		
RT	717+62	PE/MBT		2.2	12
RT	717+62	MBT1	2.5		
RT	718+85	PE	2.4		
LT	719+07	PE		4.3	25
RT	723+01	MBT1	2.5		
RT	723+01	PE		3.1	18
RT	724+72	PE	1.8		

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
RT	724+72	MBT1	2.5		
LT	725+45	MBT1	2.5		
LT	725+45	PE	1.5		
RT	726+67	CE		4.7	27
LT	727+18	PRA		7.9	45
RT	727+18	MBT1	2.5		
RT	727+18	PE	2.4		
RT	733+11	PE		5.0	29
LT	734+24	MBT1	2.5		
LT	734+24	PE	1.9		
LT	734+69	MBT1	2.5		
LT	734+69	PE		2.3	13
RT	737+54	PRA		5.3	30
RT	738+40	PE	2.1		
LT	738+46	MBT1	2.5		
LT	738+46	FE	2.3		
RT	741+14	PE	1.9		
LT	741+46	FE	2.1		
LT	744+95	FE	1.8		
RT	744+95	FE	45.0		
RT	747+20	CE	0.0	6.9	39
RT	754+78	FE	1.6		
LT	758+23	PE		6.2	36
RT	763+66	PE	2.8		
LT	763+66	MBT2	5.0		
RT	770+86	PRA		6.0	34
RT	771+00	PE		4.2	24
LT	771+69	PRA		7.9	45
RT	771+82	PE	1.3		
LT	771+82	MBT2	5.0		
RT	772+44	PE	1.7		
LT	772+67	PE	1.4		
LT	772+67	MBT1	2.5		
RT	773+39	PE	2.0		
RT	774+33	PE	0.0	6.2	35
RT	774+33	MBT1	2.5		
RT	775+70	PE	4.1		
LT	775+70	MBT2	5.0		
RT	776+69	PE	2.0		
LT	776+69	MBT2	5.0		
RT	777+53	PC	1.7		
LT	777+77	FE	2.5		
RT	778+48	CE		7.0	40
LT	779+31	MBT1	2.5		
LT	779+31	PE	1.7		
RT	780+05	PE	1.9		

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE B	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
			40200800	40800050	X0326440
SIDE	STATION	TYPE	TON	TON	SQ YD
LT	780+73	MBT1	2.5		
LT	780+73	PE	2.0		
RT	780+84	PE	2.1		
LT	781+34	MBT2	5.0		
RT	782+65	CE	4.1		
LT	783+09	PE	1.6		
LT	783+09	MBT1	2.5		
LT	783+47	PE	0.9		
RT	783+83	CE	2.5		
LT	784+10	PE	1.4		
URBAN / RURAL SPLIT					
RT	784+48	PE	2.0		
LT	785+00	PE	1.4		
RT	785+78	PE	2.2		
LT	786+15	PE	1.7		
RT	786+33	PE	1.2		
LT	787+48	PE		3.2	18
RT	787+63	PE		2.9	17
LT	787+68	PE	1.1		
LT	787+68	MBT1	2.5		
RT	787+97	PE	2.7		
RT	788+94	PE	2.6		
LT	788+99	PE	2.0		
LT	788+99	MBT1	2.5		
RT	789+52	PE	2.1		
LT	789+57	PE	2.1		
RT	790+61	PE	2.3		
LT	790+78	MBT2	5.0		
LT	791+88	MBT1	2.5		
LT	791+88	PE	2.4		
LT	791+88	MBT1	2.5		
RT	791+94	PE	2.6		
RT	793+34	PE		2.9	17
LT	794+20	MBT1	2.5		
LT	794+20	PE		3.4	19
RT	794+63	PE		2.9	17
RT	795+48	PE	2.6		
RT	797+12	PE		1.9	11
LT	797+12	MBT2	5.0		
LT	798+39	MBT1	2.5		
LT	790+39	PC	2.0		
RT	798+39	PE	2.1		
RT	801+91	PE	0.0	0.0	0
LT	801+91	MBT2	5.0		
RT	803+51	PE	2.6		
LT	803+62	MBT2	5.0		

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	DRAWN -	REVISED -
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ENTRANCE SCHEDULE

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	9
CONTRACT NO. 74550				
		ILLINOIS	FED. AID PROJECT	

ENTRANCE SCHEDULE			AGGREGATE SURFACE COURSE, TYPE R	INCIDENTAL HOT-MIX ASPHALT SURFACING	SURFACE REMOVAL, VARIABLE DEPTH (SPECIAL)
SIDE	STATION	TYPE	40200800 TON	40800050 TON	X0326440 SQ YD
LT	804+51	CE	0.0	0.0	0
RT	805+55	CE		7.6	43
RT	806+58	CE		5.8	33
LT	806+58	MBT2	5.0		
RT	808+36	PE	3.0		
LT	810+00	PRA		11.5	66
RT	810+43	PE	4.6		
RT	811+12	PE	3.6		
LT	811+12	MBT2	5.0		
RT	812+10	PE	1.5		
RT	812+35	PE	2.4		
LT	812+68	MBT2	5.0		
RT	813+88	FE	2.1		
RT	816+09	PE	2.9		
LT	816+95	PRA		6.3	36
RT	819+53	PE		6.4	37
LT	820+10	MBT2	5.0		
LT	820+48	PE	1.9		
LT	821+01	MBT1	2.5		
LT	821+01	PE	2.0		
LT	821+01	MBT1	2.5		
LT	821+78	PRA		6.0	34
RT	822+92	PE		4.8	27
LT	822+92	MBT2	5.0		
LT	824+27	MBT1	2.5		
LT	824+27	PE	3.7		
RT	824+63	FE	2.3		
LT	825+43	CE	1.9		
RT	826+35	PE	1.8		
LT	827+01	MBT1	2.5		
LT	827+01	PE		6.5	37
LT	827+62	PE		4.0	23
RT	827+71	PE	1.8		
LT	829+15	MBT1	2.5		
LT	829+15	CE	3.4		
LT	829+63	CE	2.9		
LT	830+14	MBT2	5.0		
LT	830+70	CE	3.6		
LT	831+35	CE		5.4	31
RT	832+37	CE		4.1	23
LT	832+61	CE		6.4	37
LT	833+20	CE	2.6		
LT	833+60	CE	2.6		
LT	834+09	CE	3.1		
LT	835+28	MBT1	2.5		
LT	835+28	PE	2.3		
LT	836+01	MBT1	2.5		
LT	836+01	PE	1.8		
RT	836+06	CE	3.7		
LT	836+98	PE	2.3		

RURAL TOTAL	572	246	1407
URBAN TOTAL	184	92	525
TOTAL	756	338	1933

PAVEMENT MARKING SCHEDULE			PAINT PAVEMENT MARKING - LINE 4"	RAISED REFLECTIVE PAVEMENT MARKERS
STATION	TO	STATION	FOOT	EACH
241+00.00	TO	359+60.00	27452.0	142
359+60.00 BK = 360+30 AH				
360+30.00	TO	469+79.70	24235.8	134
469+79.70 BK = 480+33.1 AH				
480+33.10	TO	496+56.48	3652.6	6
496+56.48 BK = 496+63.00 AH				
496+63.00	TO	663+23.39	44673.8	208
663+23.39 BK = 663+65.61 AH				
663+65.61	TO	731+25.31	18347.5	84
731+25.31 BK = 738+00 AH				
732+55.33	TO	739+23.40	1453.2	8
739+23.4 BK = 738+00 AH				
738+00.00	TO	784+12.00	12639.0	56
URBAN / RURAL SPLIT				
RURAL TOTAL			132453.8	638.0
784+12.00	TO	837+34.00	11832.8	67
URBAN TOTAL			11832.8	67.0
GRAND TOTAL			144286.6	705.0

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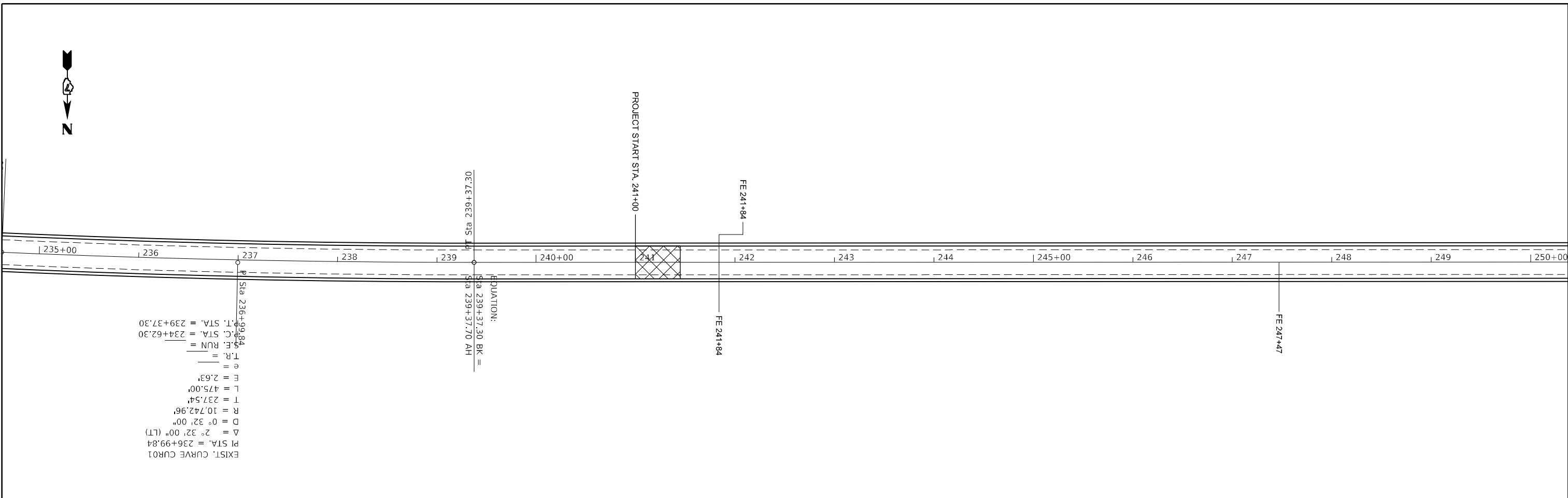
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PLOT DATE = 2/16/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND PAVEMENT
MARKING SCHEDULE**

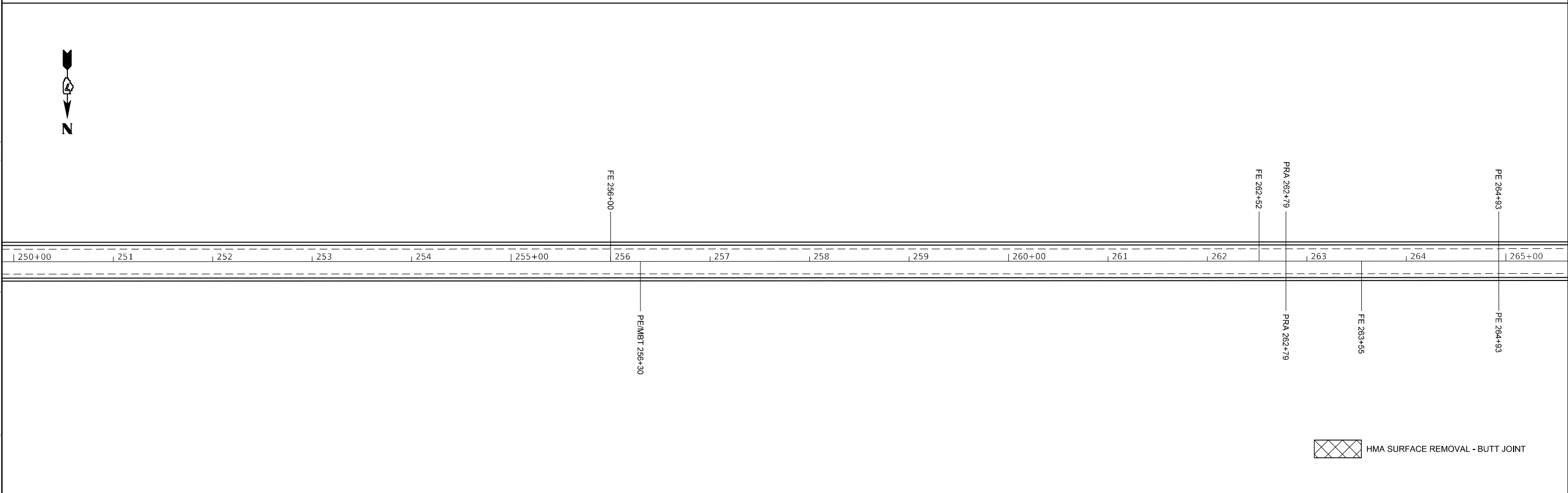
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	10
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



EXIST. CURVE CUR01
 PI STA. = 236+99.84
 $\Delta = 2^\circ 32' 00''$ (LT)
 D = 0° 32' 00"
 R = 10,742.96'
 T = 237.54'
 L = 475.00'
 E = 2.63'
 e =
 T.R. =
 S.E. RUN =
 C. STA. = 234+62.30
 I. STA. = 239+37.30

EQUATION:
 Sta 239+37.30 BK =
 Sta 239+37.70 AH



 HMA SURFACE REMOVAL - BUTT JOINT

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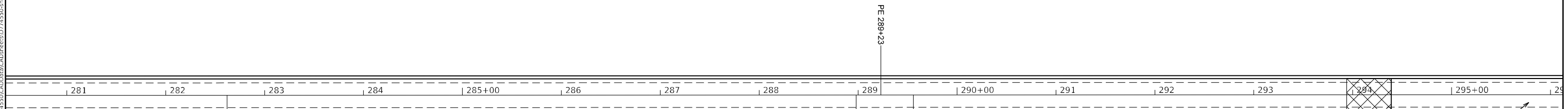
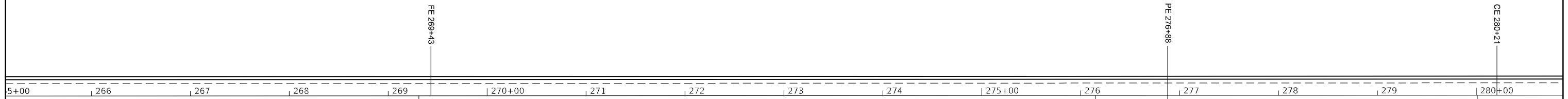
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	11
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



 HMA SURFACE REMOVAL - BUTT JOINT

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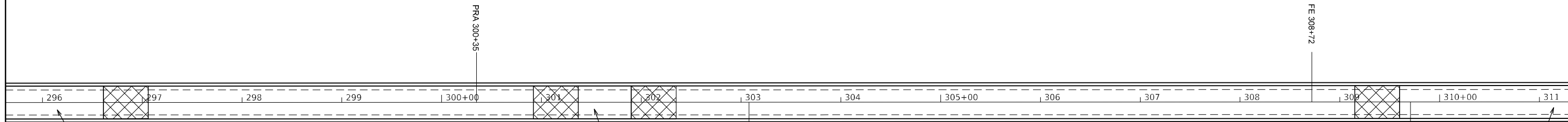
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

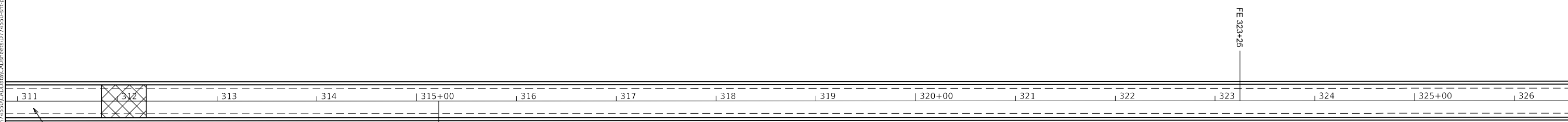
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823	(22,23,24)RS-3	WAYNE	45	12
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



STRUCTURE OMISSION

STRUCTURE OMISSION

STRUCTURE OMISSION



STRUCTURE OMISSION

 HMA SURFACE REMOVAL - BUTT JOINT

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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 823	SECTION (22,23,24)RS-3	COUNTY WAYNE	TOTAL SHEETS 45	SHEET NO. 13
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



6 | 327 | 328 | 329 | 330+00 | 331 | 332 | 333 | 334 | 335+00 | 336 | 337 | 338 | 339 | 340+00 | 341

FE 328+00

PRA 341+08

FE 341+08



| 342 | 343 | 344 | 345+00 | 346 | 347 | 348 | 349 | 350+00 | 351 | 352 | 353 | 354 | 355+00 | 356 | 357

FE 354+33

FE 354+33

STRUCTURE OMISSION

 HMA SURFACE REMOVAL - BUTT JOINT

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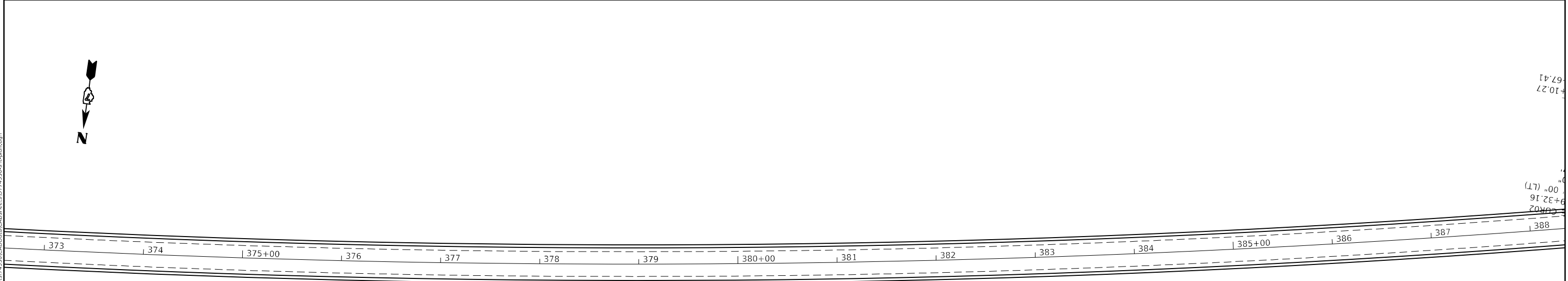
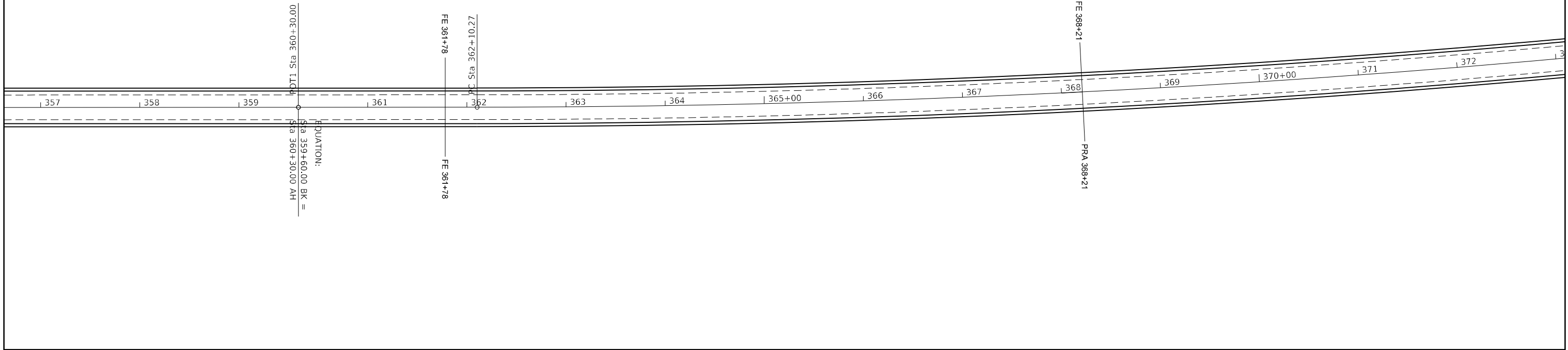
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	14
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



 HMA SURFACE REMOVAL - BUTT JOINT

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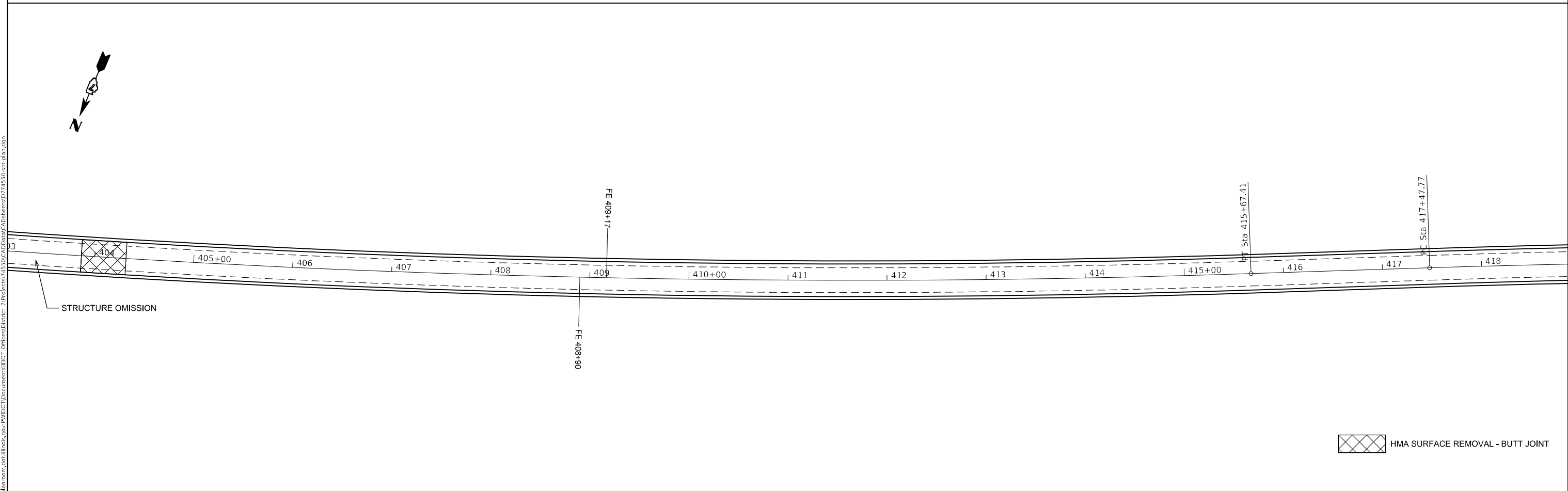
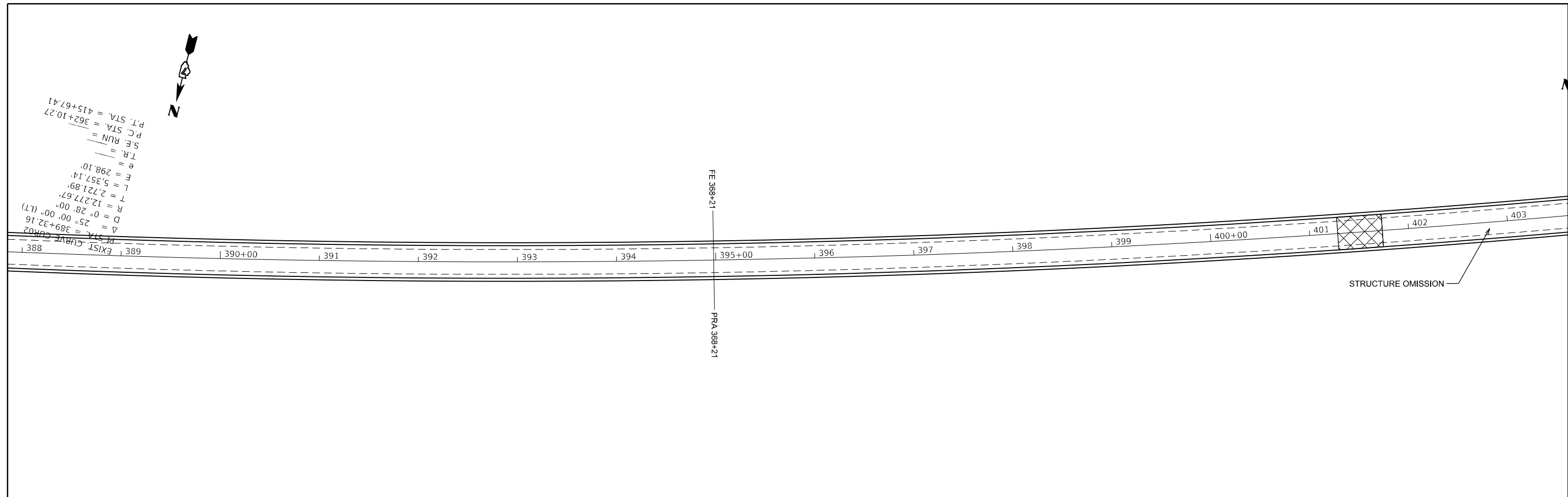
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	15
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

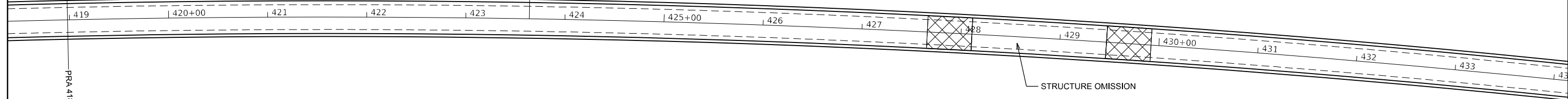
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	16
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	



PRA 418+98

PRA 418+98

FE 423+64

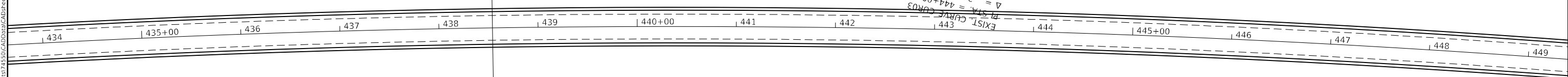


STRUCTURE OMISSION



FE 438+54

FE 438+54



EXIST. CURVE CORDS
P.T. STA. = 444+00.43
D = 24° 23' 00" (RT)
R = 12,277.67'
T = 2,652.66'
L = 5,225.00'
E = 283.29'
e =
T.R. =
S.E. RUN =
P.C. STA. = 417+47.77
P.T. STA. = 469+72.77

 HMA SURFACE REMOVAL - BUTT JOINT

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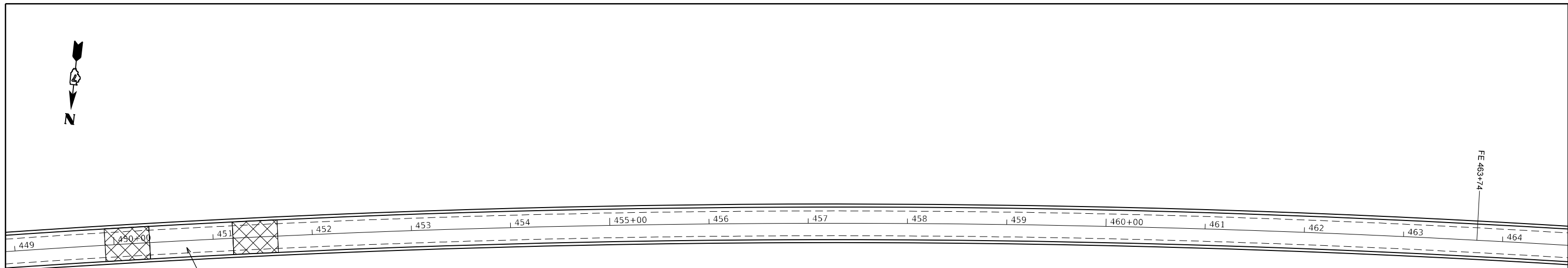
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

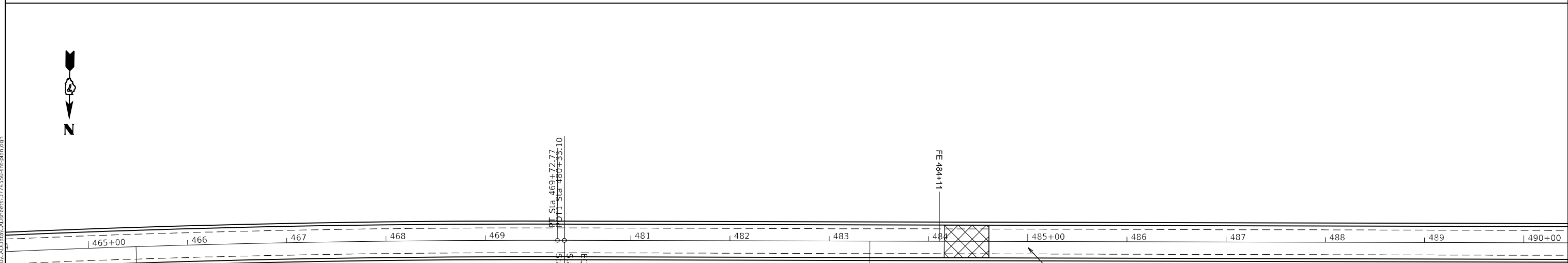
PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	17
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



STRUCTURE OMISSION



STRUCTURE OMISSION

01:55:40.81 PLS L100
 77.27+697.93
 EQUATION:
 Sta 469+79.70 BK =
 Sta 480+33.10 AH

 HMA SURFACE REMOVAL - BUTT JOINT

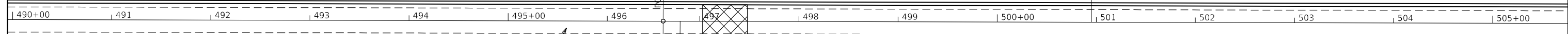
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PLOT DATE = 2/16/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEET			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE. 823	SECTION (22,23,24)RS-3	COUNTY WAYNE	TOTAL SHEETS 45	SHEET NO. 18
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

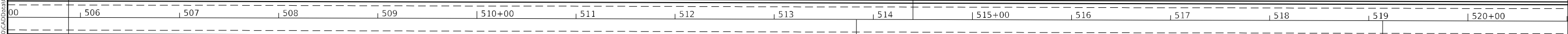


STRUCTURE OMISSION

ADJUSTION:
Sta 496+56.48 BK =
Sta 496+63.00 AH

FE 496+80

PE 500+96



PE 505+88

PE/MBT 505+88

PE 513+83

PRA 514+40

FE 519+14

 HMA SURFACE REMOVAL - BUTT JOINT

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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	19
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



521 522 523 524 525+00 526 527 528 529 530+00 531 532 533 534 535+00 536

FE 527+72

PE 532+85

PE 533+31

FE 534+52



536 537 538 539 540+00 541 542 543 544 545+00 546 547 548 549 550+00 551

PRA 540+63

PE 541+63

PE 543+37

PE 546+12

PE 548+24

PE/MBT 540+47

MBT 543+37

MBT 546+12

MBT 548+24

MBT 550+40

 HMA SURFACE REMOVAL - BUTT JOINT

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OFFICE: DISTRICT 7
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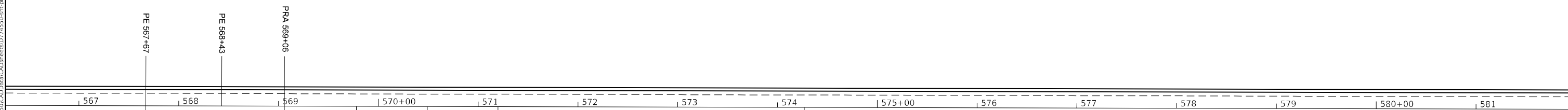
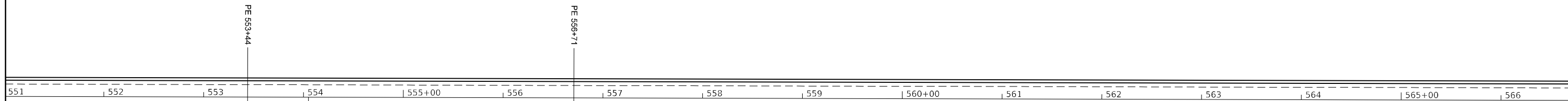
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	20
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				



 HMA SURFACE REMOVAL - BUTT JOINT

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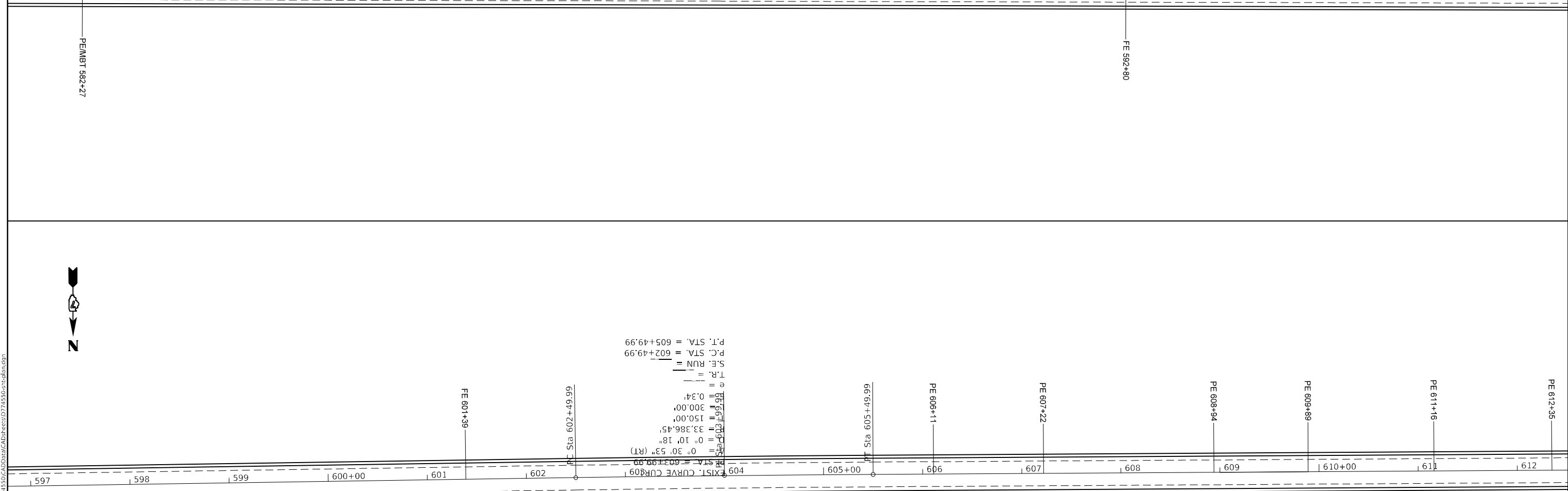
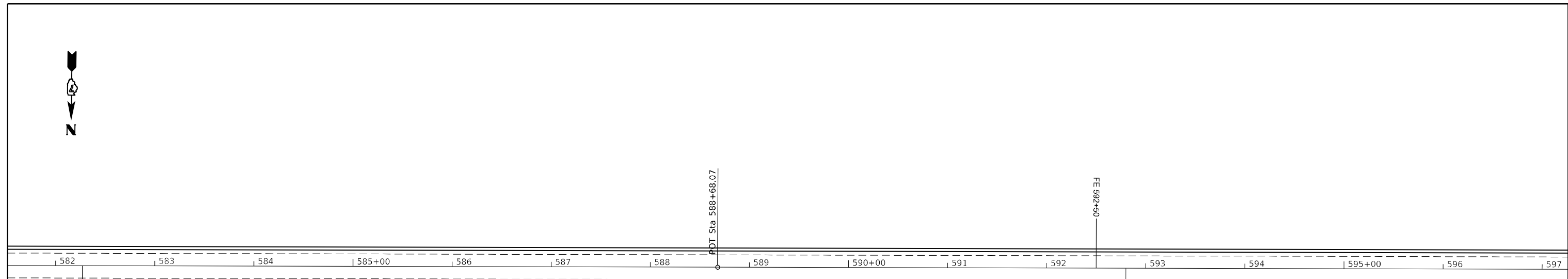
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PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	21
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



HMA SURFACE REMOVAL - BUTT JOINT

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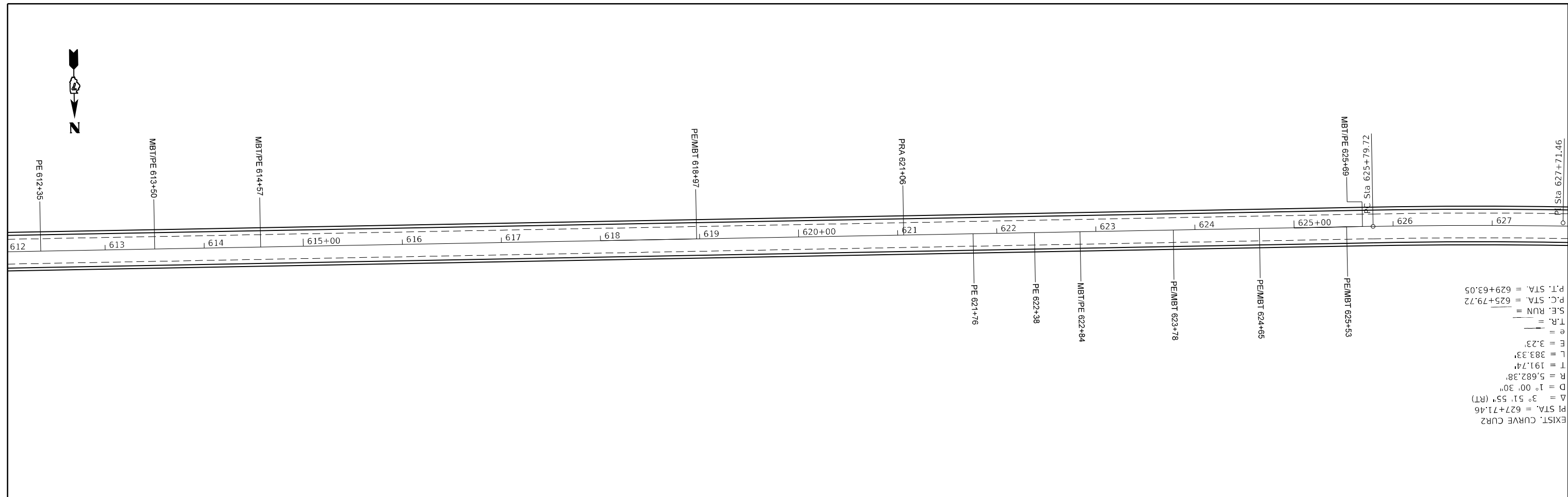
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

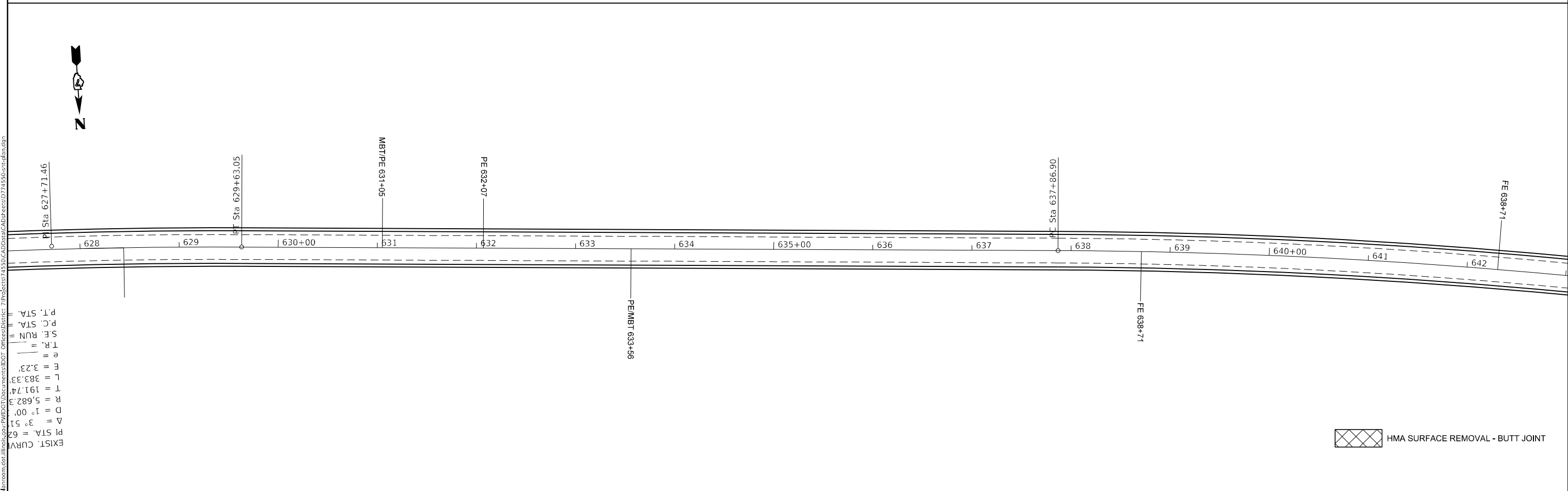
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	22
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

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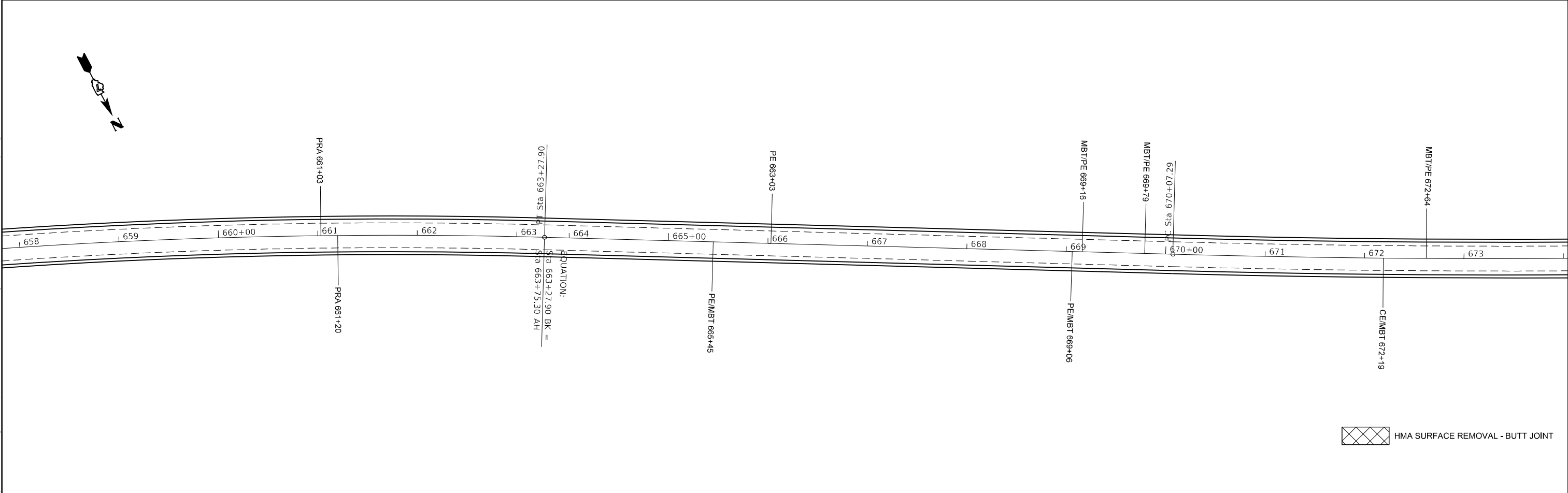
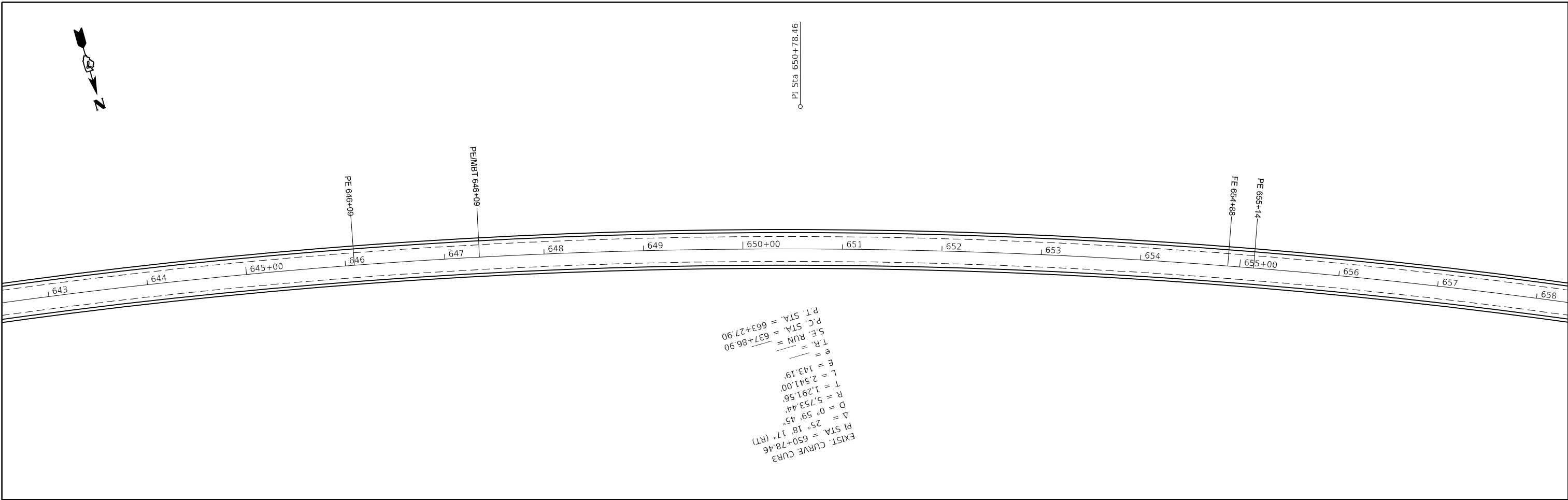
EXIST. CURVE CUR2
 PI STA. = 627+71.46
 $\Delta = 3^\circ 51' 55''$ (RT)
 D = 1° 00' 30"
 R = 5,682.38'
 T = 191.74'
 L = 383.33'
 E = 3.23'
 e = —
 T.R. = —
 S.E. RUN = —
 P.C. STA. = 625+79.72
 P.T. STA. = 629+63.05



EXIST. CURV
 PI STA. = 62
 $\Delta = 3^\circ 51'$
 D = 1° 00'
 R = 5,682.3
 T = 191.74'
 L = 383.33'
 E = 3.23'
 e = —
 T.R. = —
 S.E. RUN = —
 P.C. STA. = —
 P.T. STA. = —

HMA SURFACE REMOVAL - BUTT JOINT

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SCALE: SHEET OF SHEETS STA. TO STA.						CONTRACT NO. 74550 ILLINOIS FED. AID PROJECT					



 HMA SURFACE REMOVAL - BUTT JOINT

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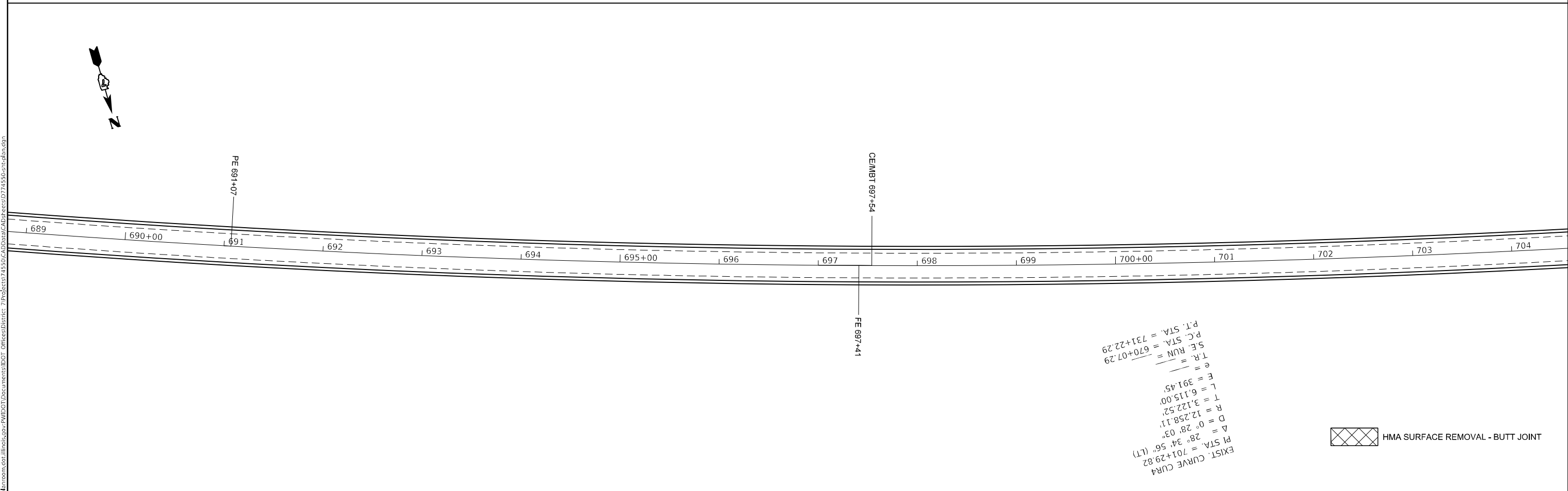
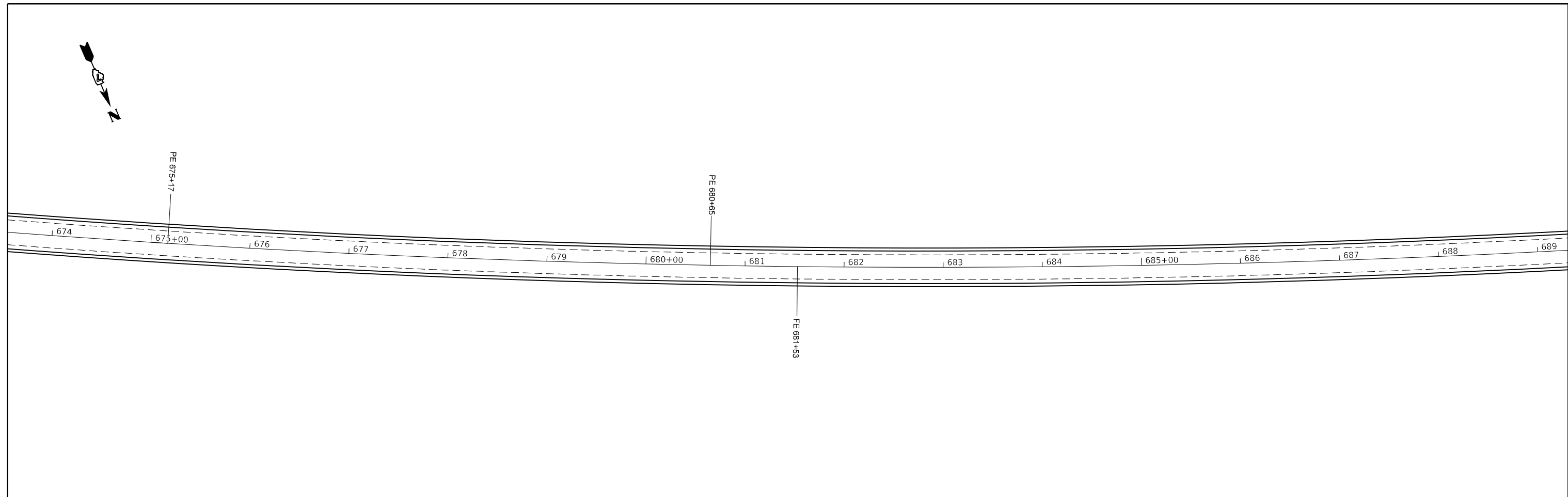
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PLAN SHEET			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	

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EXIST. CURVE CUR4
 P1 STA. = 701+29.82
 $\Delta = 28^\circ 34' 56''$ (LT)
 $D = 0^\circ 28' 03''$
 $R = 12,258.11'$
 $T = 3,122.52'$
 $L = 6,115.00'$
 $E = 391.45'$
 $e = \text{---}$
 $T.R. = \text{---}$
 $S.E. RUN = 670+07.29$
 $P.C. STA. = 731+22.29$
 $P.T. STA. = 731+22.29$

HMA SURFACE REMOVAL - BUTT JOINT

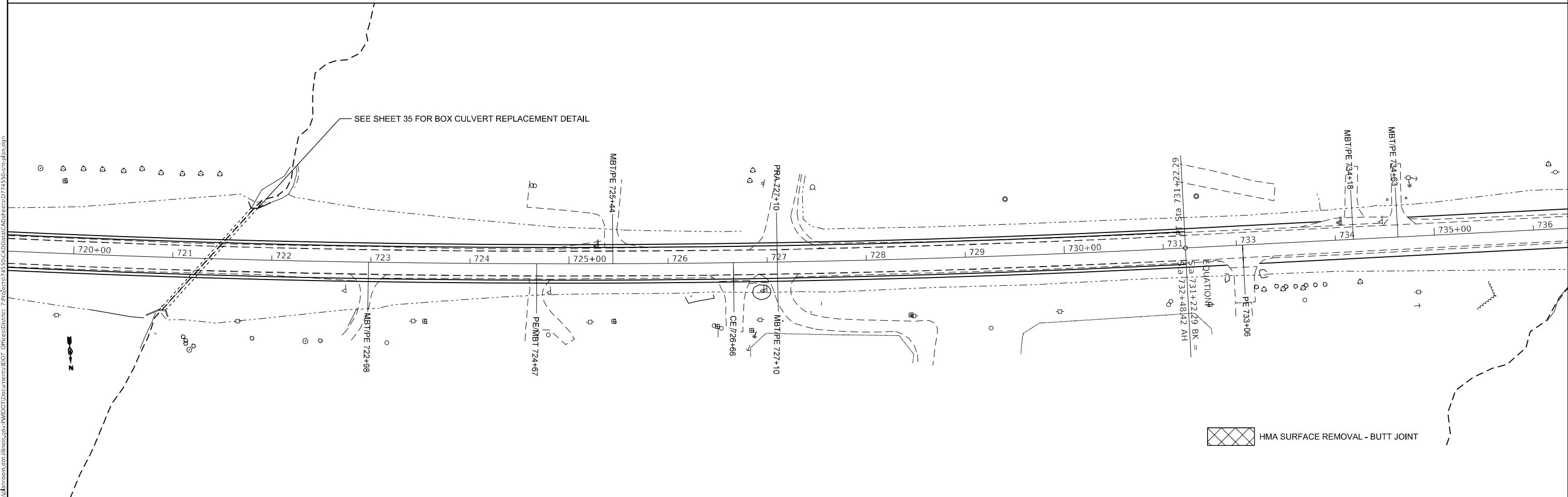
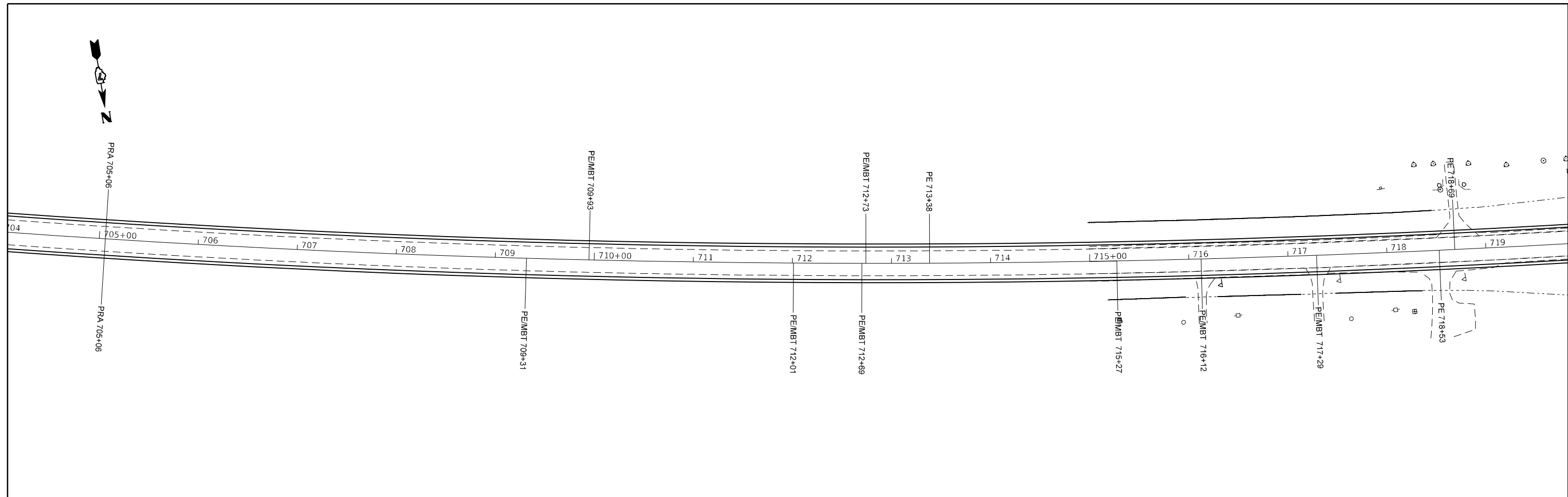
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 74550				
ILLINOIS		FED. AID PROJECT		



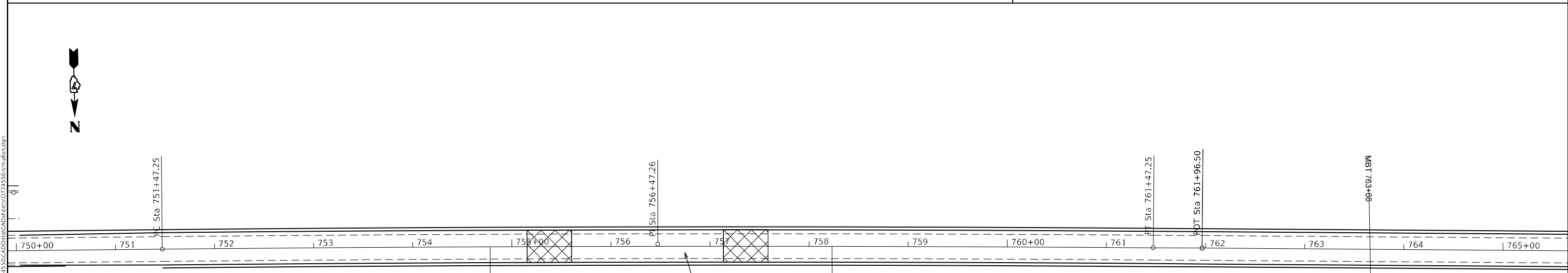
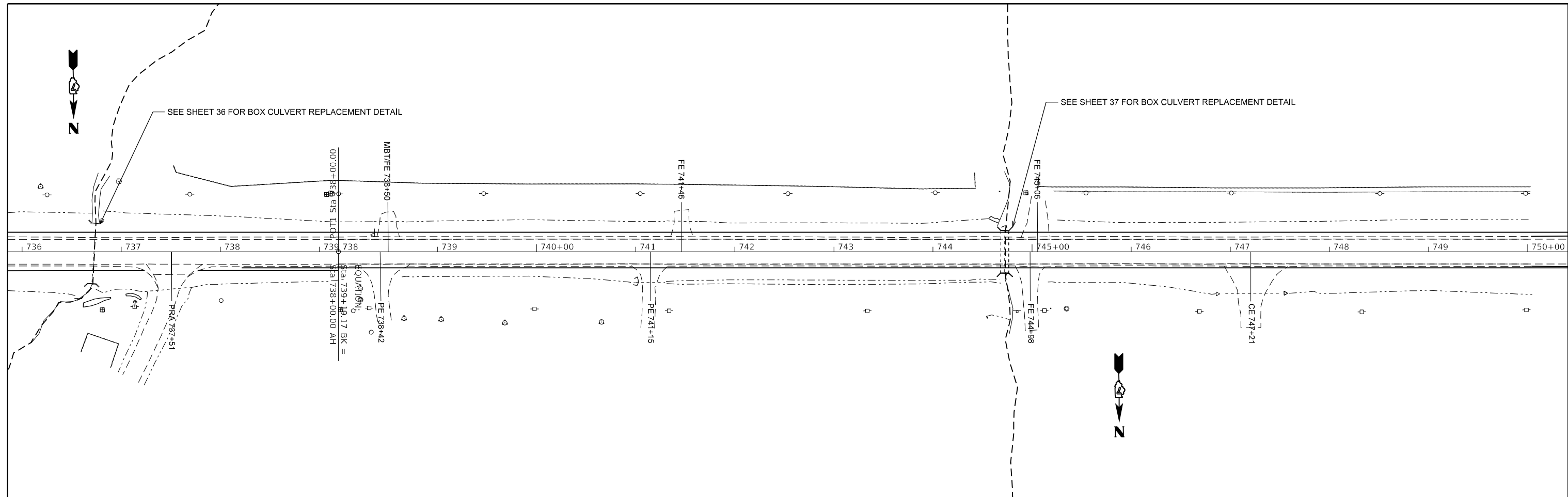
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	26
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE CUR5
 PI STA. = 756+47.26
 $\Delta = 0^\circ 59' 20''$ (RT)
 $D = 0^\circ 05' 56''$
 $R = 57.945.56'$
 $T = 500.01'$
 $L = 1,000.00'$
 $E = 2.16'$
 $T.P. =$
 $S.E. RUN =$
 $P.C. STA. = 751+47.25$
 $P.T. STA. = 761+47.25$

 HMA SURFACE REMOVAL - BUTT JOINT

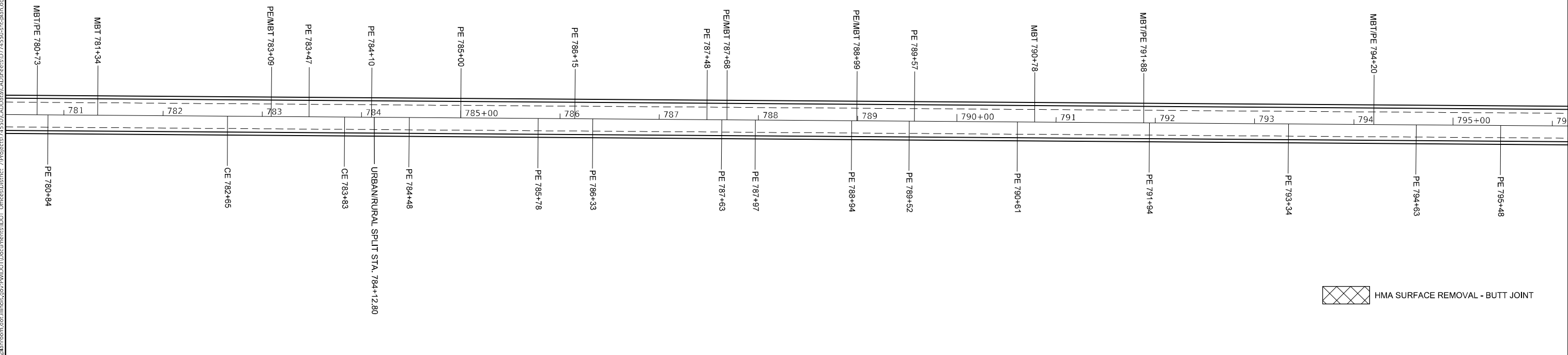
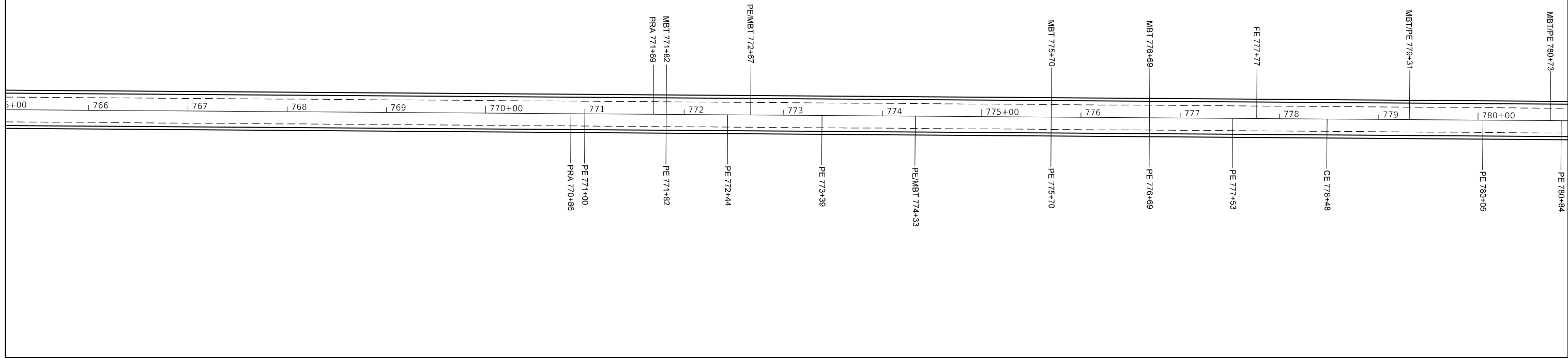
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PLOT DATE = 2/16/2021	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE. = 823	SECTION = (22,23,24)RS-3	COUNTY = WAYNE	TOTAL SHEETS = 45	SHEET NO. = 27
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				



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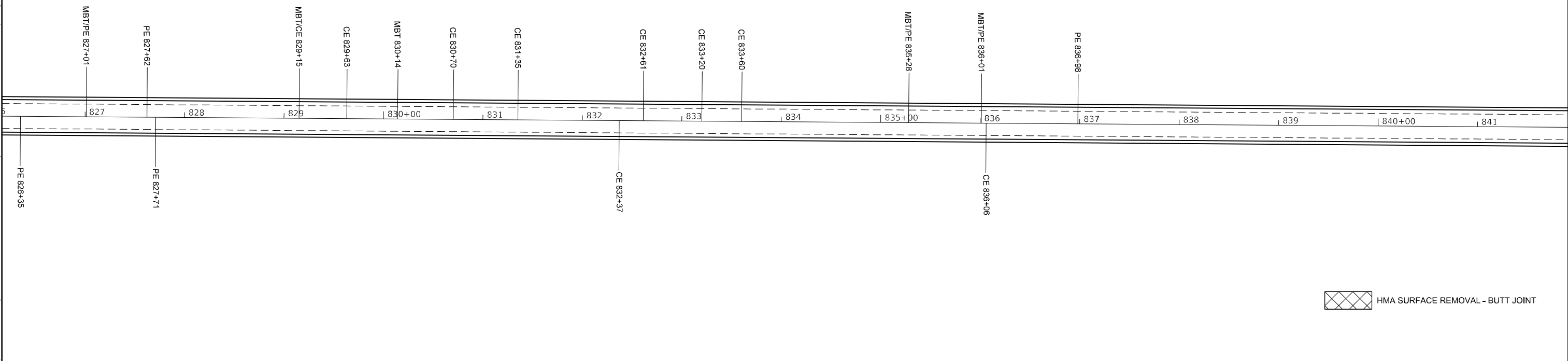
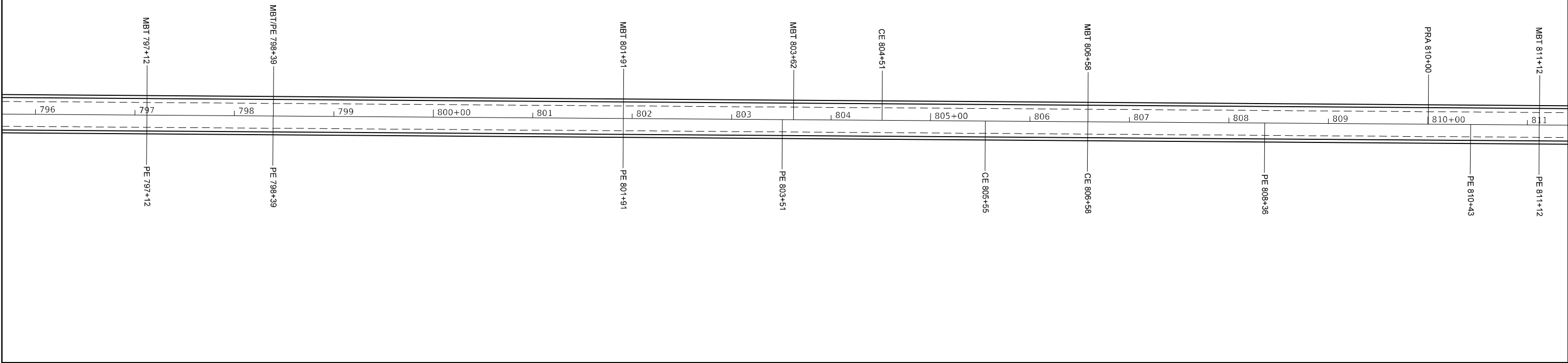
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	28
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



 HMA SURFACE REMOVAL - BUTT JOINT

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PLAN SHEET

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	45	29
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



MBT/PE 827+01

PE 827+62

MBT/CE 829+15

CE 829+63

MBT 830+14

CE 830+70

CE 831+35

CE 832+61

CE 833+20

CE 833+60

MBT/PE 835+28

MBT/PE 836+01

PE 836+98

PROJECT END STA. 837+34.38

827

828

829

830+00

831

832

833

834

835+00

836

837

838

839

840+00

841

PE 826+35

PE 827+71

CE 832+37

CE 836+06



842

843

844

845+00

846

847

848

849

850+00

DOT Sta. 850+00.00



HMA SURFACE REMOVAL - BUTT JOINT

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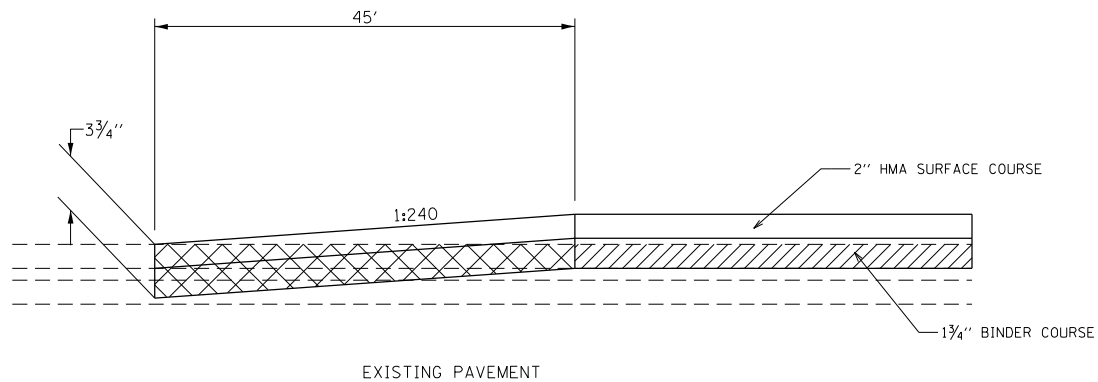
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

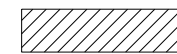

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	WAYNE	30	30
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	



BUTT JOINT DETAIL

- STA. 241+00.00 TO STA. 241+45.00
- STA. 293+93.97 TO STA. 294+38.97
- STA. 296+61.03 TO STA. 297+06.03
- STA. 300+92.00 TO STA. 301+37.00
- STA. 301+90.00 TO STA. 302+35.00
- STA. 309+15.00 TO STA. 309+60.00
- STA. 311+84.00 TO STA. 312+29.00
- STA. 346+70.01 TO STA. 347+15.01
- STA. 351+26.99 TO STA. 351+71.99
- STA. 401+28.47 TO STA. 401+73.47
- STA. 403+86.53 TO STA. 404+31.53
- STA. 427+66.00 TO STA. 428+11.00
- STA. 429+47.00 TO STA. 429+92.00
- STA. 449+91.00 TO STA. 450+36.00
- STA. 451+20.00 TO STA. 451+65.00
- STA. 484+15.88 TO STA. 484+60.88
- STA. 497+03.00 TO STA. 497+48.00
- STA. 755+15.50 TO STA. 755+60.50
- STA. 757+13.50 TO STA. 757+58.50
- STA. 836+89.38 TO STA. 837+34.38

-  HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"
-  HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

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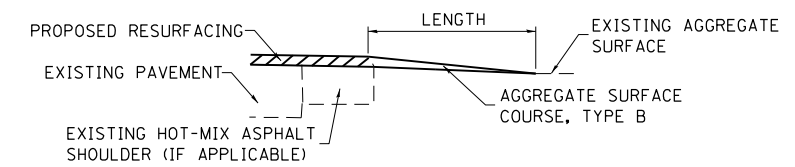
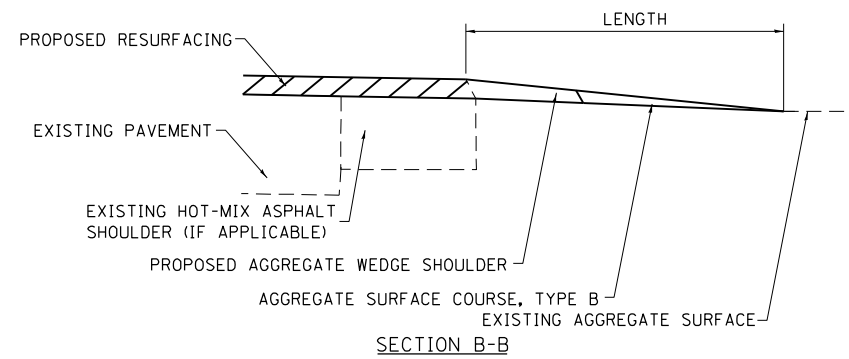
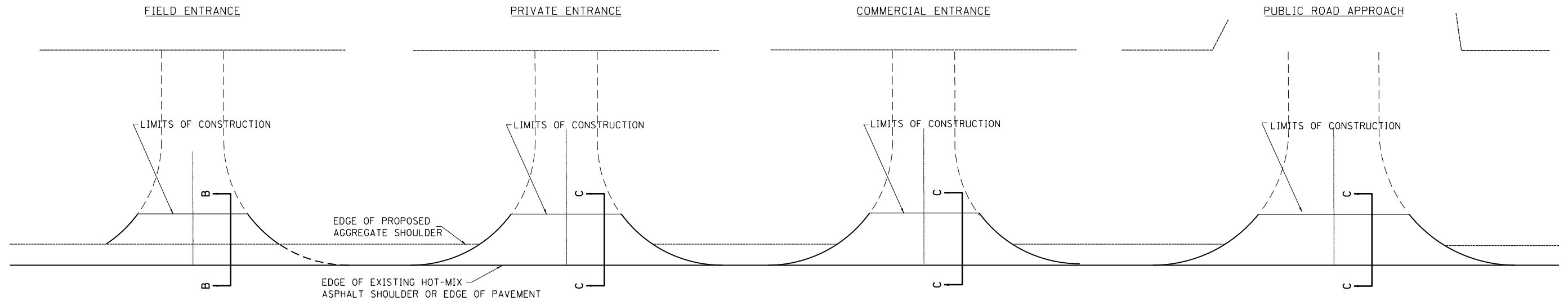
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

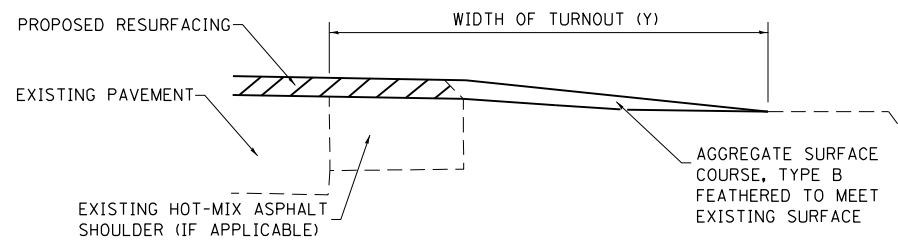
BUTT JOINT DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	31
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	

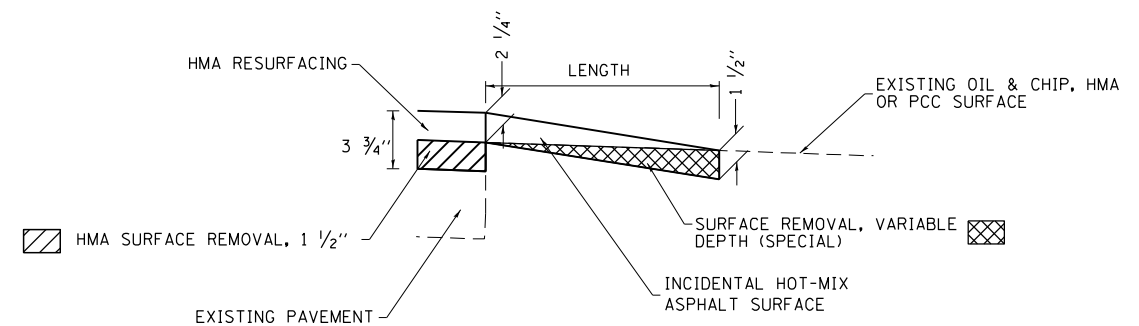


SECTION C-C



TYPICAL SECTION AT MAILBOX TURNOUT

NOTE: SEE STANDARD 406201 FOR MAILBOX TURNOUT DETAILS



SECTION C-C

NOTES

LENGTH = 10' UNLESS OTHERWISE NOTED ON PLANS

IF THERE IS NOT EXISTING HOT-MIX ASPHALT SHOULDER THEN THE ENTRANCE TAPER STARTS AT THE EDGE OF EXISTING PAVEMENT.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RURAL ENTRANCE AND MAILBOX TURNOUT DETAILS
WITH SHOULDERS

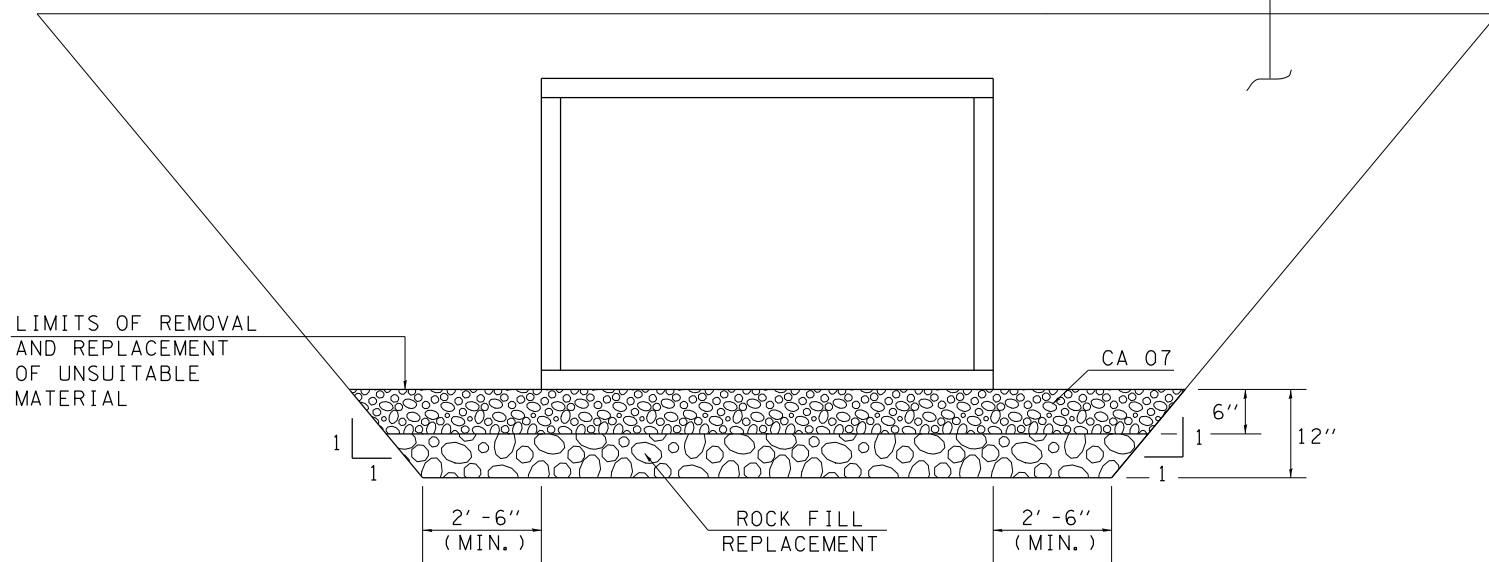
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823	(22,23,24)RS-3	Wayne	45	32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	

GENERAL NOTES

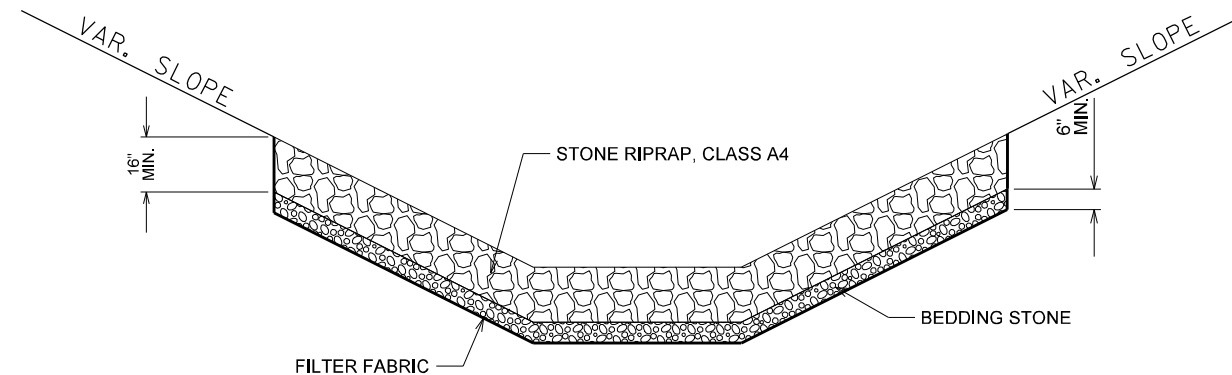
The design fill height for this box is ft. The precast box culvert sections shall conform to the requirements of ASTM C 1577.
 Drain holes shall be provided on exterior culvert walls for each precast box segment with a clear rise greater than 3 ft. The drain hole shall be located within 1/3 of the clear rise of the box culvert, shall not intercept the haunch, and shall conform to the requirements of Article 503.11 of the Standard Specification.
 The 6 in. thick layer of porous granular material required for the precast concrete box culvert per Art. 540.06 of the Standard Specifications shall also apply to the end sections. Cost of the porous granular material will not be paid for separately but shall be included in the unit price of the work for which it is required.
 Nonwoven geotextile fabric shall conform to the requirements of Art. 1080.01 of the Standard Specifications. The minimum weight of the fabric shall be 6 ounces per square yard.
 Precast concrete box culverts and box culvert end sections shall be backfilled with Porous Granular Embankment below the top of the box culvert extending to a vertical plane 2 ft from the exterior sides of the culvert, 2 ft from the back face of the end sections, and not closer than 2 ft from the face of embankment.

POROUS GRANULAR BACKFILL WITHIN LIMITS OF PROPOSED ROADWAY AND PAVED SHOULDERS. OUTSIDE LIMITS OF PAVED SHOULDERS THE CULVERT SHALL BE BACKFILLED IN ACCORDANCE WITH SECTION 502 OF THE STANDARD SPECIFICATIONS.

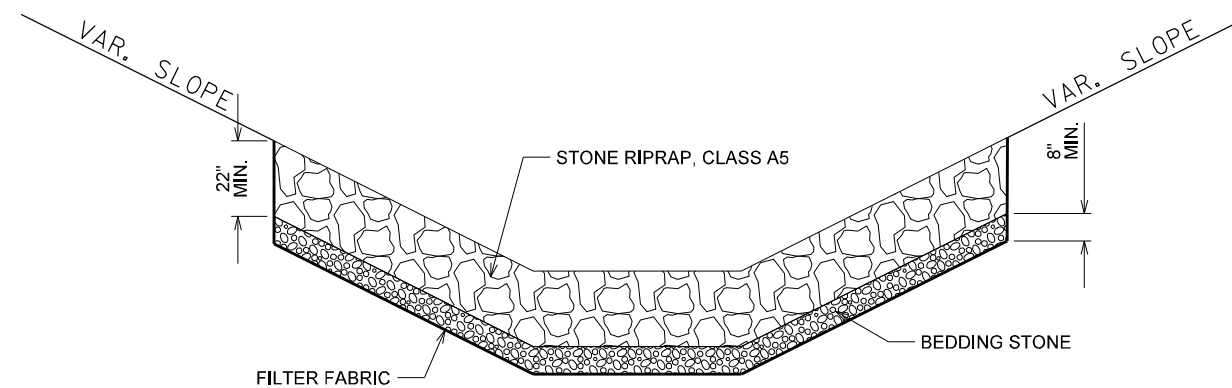


NOTE:
 THE LIMITS AND QUANTITIES SHOWN ARE ESTIMATED. THE FINAL DEPTH OF UNDERCUT WILL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION.

CULVERT UNDERCUT DETAIL



DETAIL OF RIPRAP, CLASS A4



DETAIL OF RIPRAP, CLASS A5

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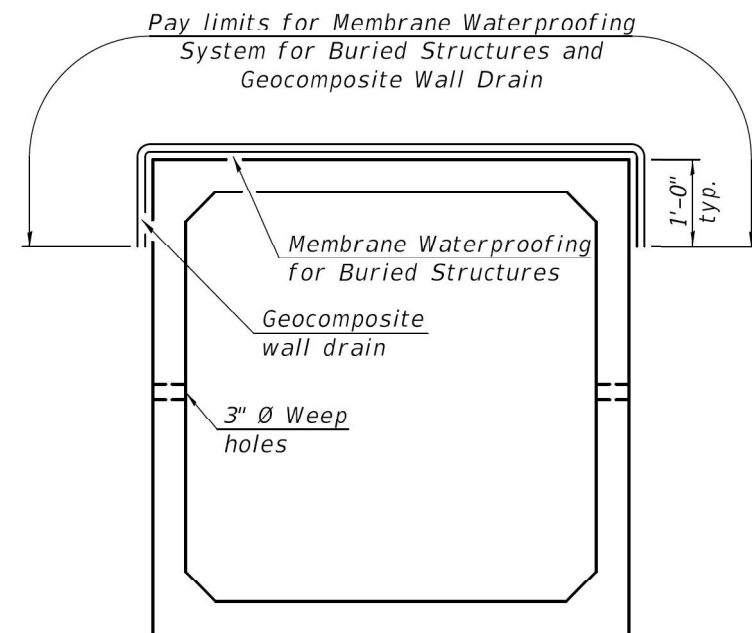
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BOX CULVERT EXCAVATION
 AND RIP-RAP DETAIL**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	33
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	



PRECAST CONCRETE

BOX CULVERT

Fill Height ≤ 3 ft.

For fill heights > 3 ft., omit Membrane Waterproofing System for Buried Structures and Geocomposite Wall Drain.

Note:
Geocomposite Wall Drain shall be according to Section 591 of the Standard Specifications, except that concrete nails shall not be used in areas where it overlaps Membrane Waterproofing System for Buried Structures.

MEMBRANE WATERPROOFING SYSTEM FOR PRECAST CULVERTS

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

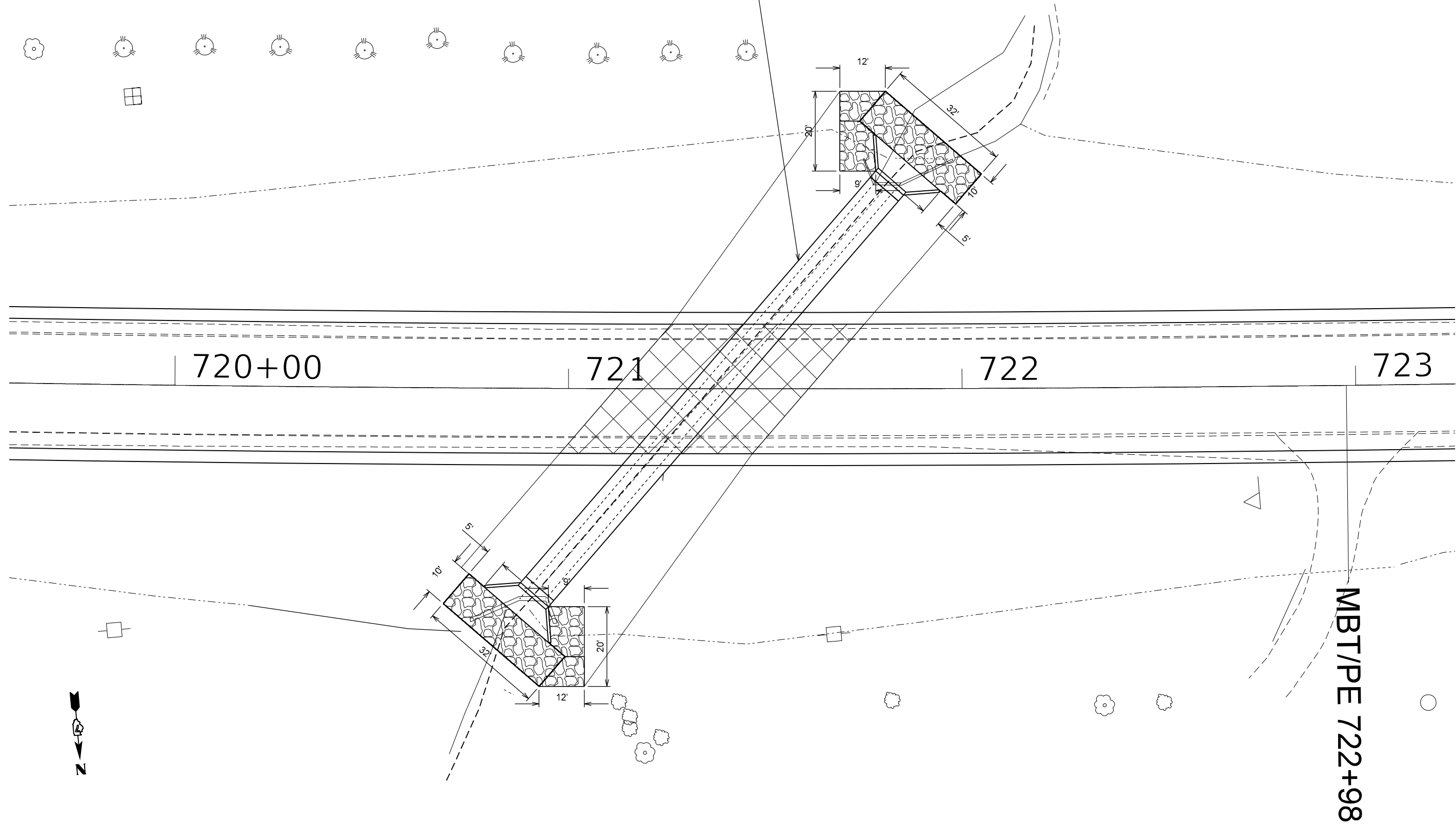
**MEMBRANE WATERPROOFINS SYSTEM
 DETAIL FOR STRUCTURE 096-7078**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	34
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

PRECAST CONCRETE BOX CULVERTS 8' X 5'
 STA. 721+36
 LENGTH = 134.0 FT
 BOX CULVERT END SECTIONS,
 CULVERT NO. 3, 2 EACH
 40 DEGREE SKEW
 U.S.F.L. ELEVATION = 425.84 O/S = 40.67' RT
 D.S.F.L. ELEVATION = 424.50 O/S = 40.55' LT

S.N. 096-7080
 REMOVAL AND DISPOSAL OF
 UNSUITABLE MATERIAL = 29 CU YD
 ROCK FILL REPLACEMENT = 29 TONS
 POROUS GRANULAR EMBANKMENT = 881 CU YD
 PIPE CULVERT REMOVAL = 134.0'



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PLOT DATE = 2/16/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BOX CULVERT DETAIL
S.N. 096-7080

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	35
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	

MBT/PE 734+63

735+00

736

737

738

PRECAST CONCRETE BOX CULVERTS 9' X 4'
 STA. 736+72
 LENGTH = 62.0 FT
 BOX CULVERT END SECTIONS,
 CULVERT NO. 2, 2 EACH
 0 DEGREE SKEW
 U.S.F.L. ELEVATION = 416.90
 D.S.F.L. ELEVATION = 416.17

O/S = 24.80' RT
 O/S = 24.80' LT

S.N. 096-7079

REMOVAL AND DISPOSAL OF
 UNSUITABLE MATERIAL = 20 CU YD
 ROCK FILL REPLACEMENT = 20 TONS
 POROUS GRANULAR EMBANKMENT = 218 CU YD
 BOX CULVERT REMOVAL = 62.0'

PRA 737+51



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PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/16/2021	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BOX CULVERT DETAIL
 S.N. 096-7079**

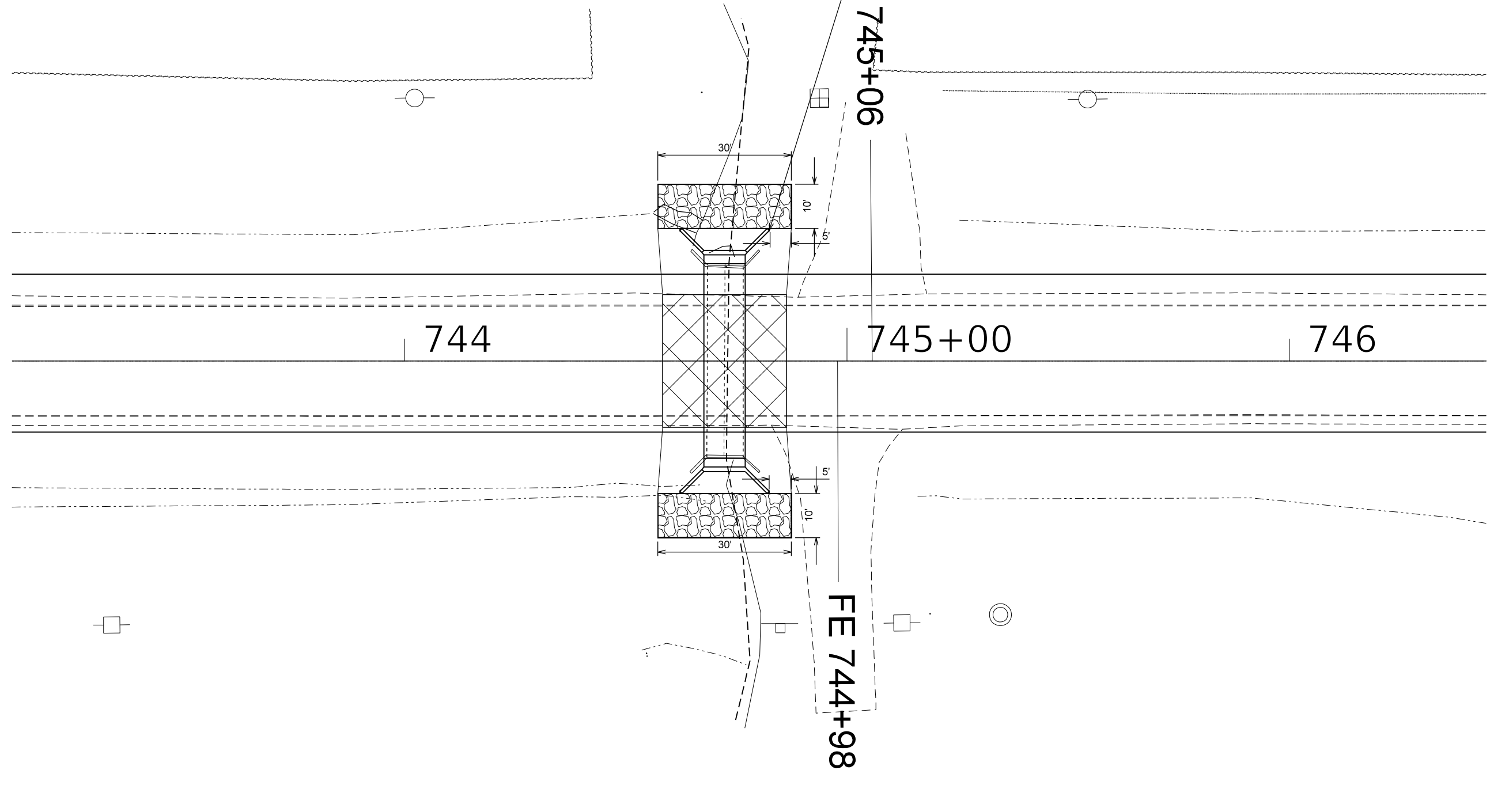
SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74550			45	36
ILLINOIS FED. AID PROJECT			CONTRACT NO.	

S.N. 096-7078

PRECAST CONCRETE BOX CULVERTS 8' X 4'
 STA. 744+23
 LENGTH = 44.0 FT
 BOX CULVERT END SECTIONS,
 CULVERT NO. 1, 2 EACH
 0 DEGREE SKEW
 U.S.F.L. ELEVATION = 412.25
 O/S = 17.60' RT
 O/S = 17.60' LT
 U.S.F.L. ELEVATION = 411.98

REMOVAL AND DISPOSAL OF
 UNSUITABLE MATERIAL = 19 CU YD
 ROCK FILL REPLACEMENT = 19 TONS
 POROUS GRANULAR EMBANKMENT = 63 CU YD
 GEOCOMPOSITE WALL DRAIN = 55.4 SQ YD
 MEMBRANE WATERPROOFING SYSTEM
 FOR BURIED STRUCTURES = 55.4 SQ YD



MODEL: Default
 FILE NAME: p:\pub\harronm.dwt\illinois.gov\PWIDOT\Documents\DOT_Offices\District 7\Projects\74550\CAAD\Drawings\74550-PC-Details.dgn

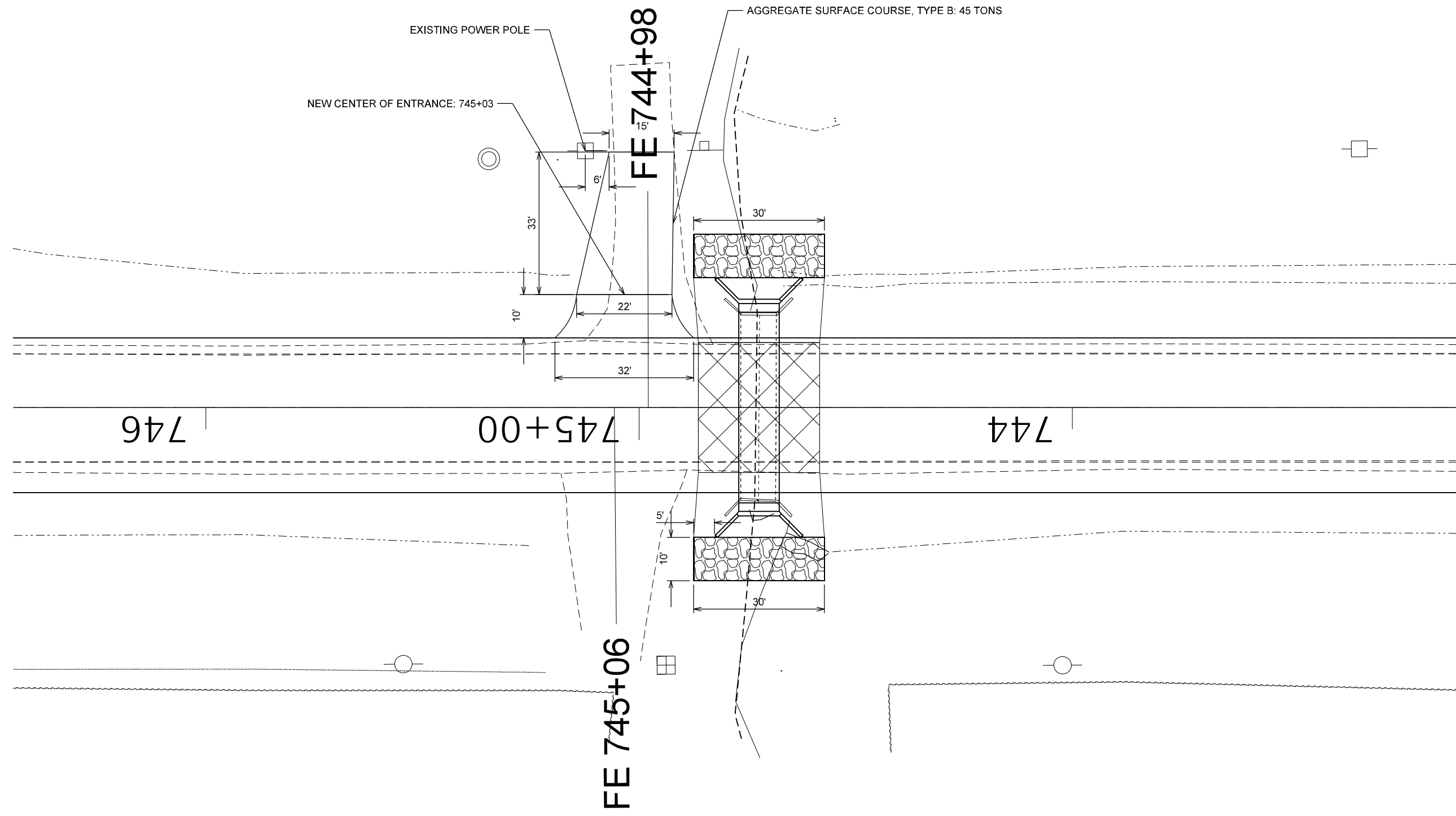
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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/16/2021	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**BOX CULVERT DETAIL
 S.N. 096-7078**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	37
CONTRACT NO. 74550			ILLINOIS FED. AID PROJECT	



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	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/16/2021	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ENTRANCE RELOCATION DETAIL
FOR S.N. 096-7078

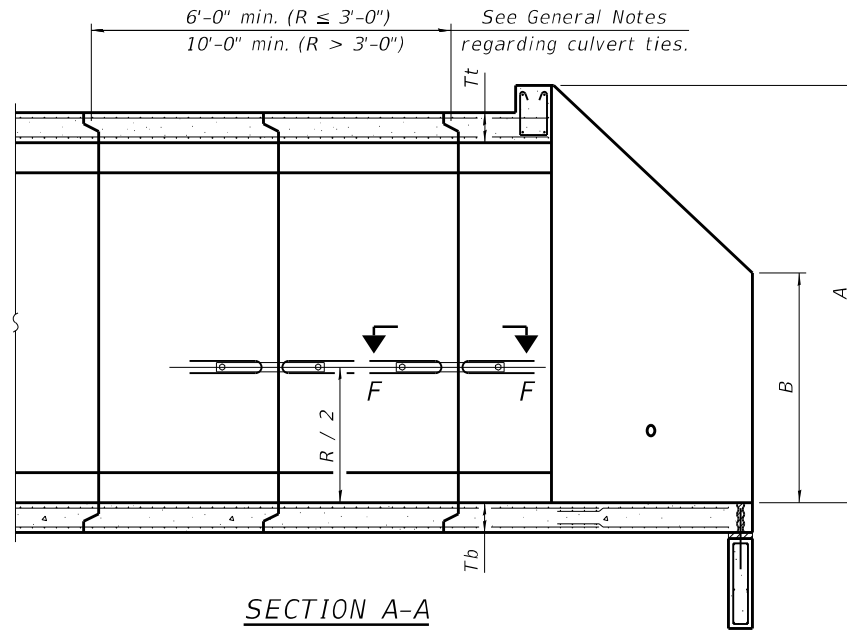
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	38
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

APRON END SECTION DIMENSIONS

Span (S)	Rise (R)	Tt	Tb	Ts	A	B	C	D	E	Concrete Cu. Yd.	Culvert Ties Required
3'-0"	2'-0"	7"	6"	4"	3'-4"	2'-2"	2'-10 ⁵ / ₈ "	4'-1"	10'-4 ⁵ / ₈ "	2.8	Yes
3'-0"	2'-0"	4"	4"	4"	3'-1"	2'-1"	2'-7 ¹ / ₈ "	3'-9"	9'-11"	2.3	Yes
3'-0"	3'-0"	7"	6"	4"	4'-4"	2'-8"	3'-10 ⁵ / ₈ "	5'-6"	12'-4 ⁵ / ₈ "	3.7	Yes
3'-0"	3'-0"	4"	4"	4"	4'-1"	2'-7"	3'-7 ¹ / ₈ "	5'-2"	11'-11"	3.1	Yes
4'-0"	2'-0"	7.5"	6"	5"	3'-4 ¹ / ₂ "	2'-2 ¹ / ₂ "	2'-11 ³ / ₈ "	4'-2"	11'-8"	3.3	Yes
4'-0"	2'-0"	5"	5"	5"	3'-2"	2'-1"	2'-8 ¹ / ₂ "	3'-10"	11'-2 ³ / ₈ "	2.8	Yes
4'-0"	3'-0"	7.5"	6"	5"	4'-4 ¹ / ₂ "	2'-8 ¹ / ₂ "	3'-11 ³ / ₈ "	5'-7"	13'-8 ³ / ₈ "	4.2	Yes
4'-0"	3'-0"	5"	5"	5"	4'-2"	2'-7"	3'-8 ¹ / ₂ "	5'-3"	13'-2 ³ / ₈ "	3.7	Yes
4'-0"	4'-0"	7.5"	6"	5"	5'-4 ¹ / ₂ "	3'-2 ¹ / ₂ "	4'-11 ³ / ₈ "	7'-0"	15'-8 ³ / ₈ "	5.3	Yes
4'-0"	4'-0"	5"	5"	5"	5'-2"	3'-1"	4'-8 ³ / ₈ "	6'-8"	15'-2 ¹ / ₂ "	4.7	Yes
5'-0"	2'-0"	8"	7"	6"	3'-5"	2'-3"	2'-11 ³ / ₈ "	4'-2"	12'-10"	3.9	Yes
5'-0"	2'-0"	6"	6"	6"	3'-3"	2'-2"	2'-10"	4'-0"	12'-7 ¹ / ₄ "	3.5	Yes
5'-0"	3'-0"	8"	7"	6"	4'-5"	2'-9"	3'-11 ³ / ₈ "	5'-7"	14'-10 ¹ / ₈ "	4.9	Yes
5'-0"	3'-0"	6"	6"	6"	4'-3"	2'-8"	3'-10"	5'-5"	14'-7 ¹ / ₄ "	4.5	Yes
5'-0"	4'-0"	8"	7"	6"	5'-5"	3'-3"	4'-11 ³ / ₈ "	7'-0"	16'-10 ¹ / ₈ "	6.1	Yes
5'-0"	4'-0"	6"	6"	6"	5'-3"	3'-2"	4'-9 ¹ / ₄ "	6'-9"	16'-5 ⁷ / ₈ "	5.5	Yes
5'-0"	5'-0"	8"	7"	6"	6'-5"	3'-9"	5'-11 ³ / ₈ "	8'-5"	18'-10 ¹ / ₈ "	7.4	Yes
5'-0"	5'-0"	6"	6"	6"	6'-3"	3'-8"	5'-9 ¹ / ₄ "	8'-2"	18'-5 ⁷ / ₈ "	6.8	Yes
6'-0"	2'-0"	8"	7"	7"	3'-5"	2'-3"	2'-11 ³ / ₈ "	4'-2"	14'-0"	4.3	Yes
6'-0"	2'-0"	7"	7"	7"	3'-4"	2'-2"	2'-10 ³ / ₈ "	4'-1"	13'-10 ³ / ₈ "	4.2	Yes
6'-0"	3'-0"	8"	7"	7"	4'-5"	2'-9"	3'-11 ³ / ₈ "	5'-7"	16'-0 ¹ / ₈ "	5.4	Yes
6'-0"	3'-0"	7"	7"	7"	4'-4"	2'-8"	3'-10 ⁵ / ₈ "	5'-6"	15'-10 ⁵ / ₈ "	5.2	Yes
6'-0"	4'-0"	8"	7"	7"	5'-5"	3'-3"	4'-11 ³ / ₈ "	7'-0"	18'-0 ³ / ₈ "	6.5	Yes
6'-0"	4'-0"	7"	7"	7"	5'-4"	3'-2"	4'-10 ³ / ₄ "	6'-11"	17'-10 ³ / ₄ "	6.5	Yes
6'-0"	5'-0"	8"	7"	7"	6'-5"	3'-9"	5'-11 ³ / ₈ "	8'-5"	20'-0 ¹ / ₈ "	8.0	Yes
6'-0"	5'-0"	7"	7"	7"	6'-4"	3'-8"	5'-10 ³ / ₄ "	8'-4"	19'-10 ³ / ₄ "	7.8	Yes
6'-0"	6'-0"	8"	7"	7"	7'-5"	4'-3"	6'-11 ¹ / ₂ "	9'-10"	22'-0 ¹ / ₄ "	9.5	Yes
6'-0"	6'-0"	7"	7"	7"	7'-4"	4'-2"	6'-10 ³ / ₄ "	9'-9"	21'-10 ³ / ₄ "	9.3	Yes
7'-0"	2'-0"	8"	8"	8"	3'-5"	2'-3"	2'-11 ³ / ₈ "	4'-2"	15'-2"	4.9	Yes
7'-0"	3'-0"	8"	8"	8"	4'-5"	2'-9"	3'-11 ³ / ₈ "	5'-7"	17'-2 ¹ / ₈ "	6.1	Yes
7'-0"	4'-0"	8"	8"	8"	5'-5"	3'-3"	4'-11 ³ / ₈ "	7'-0"	19'-2 ¹ / ₈ "	7.4	Yes
7'-0"	5'-0"	8"	8"	8"	6'-5"	3'-9"	5'-11 ³ / ₈ "	8'-5"	21'-2 ¹ / ₈ "	8.9	Yes
7'-0"	6'-0"	8"	8"	8"	7'-5"	4'-3"	6'-11 ¹ / ₂ "	9'-10"	23'-2 ¹ / ₄ "	10.6	Yes
8'-0"	2'-0"	8"	8"	8"	3'-5"	2'-3"	2'-11 ³ / ₈ "	4'-2"	16'-2"	5.3	Yes
8'-0"	3'-0"	8"	8"	8"	4'-5"	2'-9"	3'-11 ³ / ₈ "	5'-7"	18'-2 ¹ / ₈ "	6.5	Yes
8'-0"	4'-0"	8"	8"	8"	5'-5"	3'-3"	4'-11 ³ / ₈ "	7'-0"	20'-2 ¹ / ₈ "	7.8	Yes
8'-0"	5'-0"	8"	8"	8"	6'-5"	3'-9"	5'-11 ³ / ₈ "	8'-5"	22'-2 ¹ / ₈ "	9.3	Yes
8'-0"	6'-0"	8"	8"	8"	7'-5"	4'-3"	6'-11 ¹ / ₂ "	9'-10"	24'-2 ¹ / ₄ "	11.0	Yes
9'-0"	2'-0"	9"	9"	9"	3'-6"	2'-3"	3'-0 ³ / ₄ "	4'-4"	17'-6 ³ / ₈ "	6.2	Yes
9'-0"	3'-0"	9"	9"	9"	4'-6"	2'-9"	4'-0 ³ / ₄ "	5'-9"	19'-6 ¹ / ₈ "	7.5	Yes
9'-0"	4'-0"	9"	9"	9"	5'-6"	3'-3"	5'-0 ³ / ₄ "	7'-2"	21'-6 ³ / ₈ "	9.0	Yes
9'-0"	5'-0"	9"	9"	9"	6'-6"	3'-9"	6'-0 ³ / ₈ "	8'-7"	23'-7"	10.6	Yes
9'-0"	6'-0"	9"	9"	9"	7'-6"	4'-3"	7'-0 ³ / ₈ "	9'-11"	25'-5 ⁵ / ₈ "	12.4	Yes
10'-0"	2'-0"	10"	10"	10"	3'-7"	2'-4"	3'-1 ¹ / ₂ "	4'-5"	18'-10 ¹ / ₄ "	7.1	No
10'-0"	3'-0"	10"	10"	10"	4'-7"	2'-10"	4'-1 ¹ / ₂ "	5'-10"	20'-10 ¹ / ₄ "	8.6	No
10'-0"	4'-0"	10"	10"	10"	5'-7"	3'-4"	5'-1 ¹ / ₂ "	7'-3"	22'-10 ³ / ₈ "	10.2	Yes
10'-0"	5'-0"	10"	10"	10"	6'-7"	3'-10"	6'-1 ¹ / ₂ "	8'-8"	24'-10 ³ / ₈ "	12.0	Yes
10'-0"	6'-0"	10"	10"	10"	7'-7"	4'-4"	7'-1 ¹ / ₂ "	10'-1"	26'-10 ³ / ₈ "	13.9	Yes
11'-0"	2'-0"	11"	11"	11"	3'-8"	2'-4"	3'-2 ¹ / ₈ "	4'-7"	20'-3 ³ / ₈ "	8.2	No
11'-0"	3'-0"	11"	11"	11"	4'-8"	2'-10"	4'-2 ¹ / ₈ "	6'-0"	22'-3 ³ / ₈ "	9.8	No
11'-0"	4'-0"	11"	11"	11"	5'-8"	3'-4"	5'-2 ¹ / ₄ "	7'-4"	24'-1 ³ / ₄ "	11.5	Yes
11'-0"	5'-0"	11"	11"	11"	6'-8"	3'-10"	6'-2 ¹ / ₄ "	8'-9"	26'-1 ³ / ₄ "	13.3	Yes
11'-0"	6'-0"	11"	11"	11"	7'-8"	4'-4"	7'-2 ¹ / ₄ "	10'-2"	28'-1 ³ / ₈ "	15.5	Yes
12'-0"	2'-0"	12"	12"	12"	3'-9"	2'-5"	3'-3 ³ / ₈ "	4'-8"	21'-6 ¹ / ₂ "	9.3	No
12'-0"	3'-0"	12"	12"	12"	4'-9"	2'-11"	4'-3 ³ / ₈ "	6'-1"	23'-6 ¹ / ₂ "	11.1	No
12'-0"	4'-0"	12"	12"	12"	5'-9"	3'-5"	5'-3 ³ / ₈ "	7'-6"	25'-6 ³ / ₈ "	13.0	Yes
12'-0"	5'-0"	12"	12"	12"	6'-9"	3'-11"	6'-3 ³ / ₈ "	8'-11"	27'-6 ³ / ₈ "	14.1	Yes
12'-0"	6'-0"	12"	12"	12"	7'-9"	4'-5"	7'-3 ³ / ₈ "	10'-4"	29'-6 ³ / ₈ "	17.4	Yes

Note: Two sets of apron end section dimensions are shown above for some box culvert sizes due to the top and bottom slabs having different thicknesses per ASTM C 1577 for design fill heights less than 2 ft. (Sheet 1 of 2)



SECTION A-A

GENERAL NOTES

Box Culvert End Sections shall be constructed according to the requirements of Section 540 of the Standard Specifications except as modified herein. End sections will be paid for at the contract unit price per each for Box Culvert End Sections.

The Contractor may furnish the end section as a single precast concrete piece or construct the end section in the field using cast-in-place (CIP) construction. For CIP construction, the bottom slab thickness shall be increased by 2" and the clear cover to the bottom mat of reinforcement shall be increased to 3".

Box section dimensions, materials, and reinforcement details for Box Culvert End Sections shall be according to the requirements for ASTM C 1577 as required for the design of the portion of the culvert within the limits of Precast Concrete Box Culverts except as modified herein.

The number of culvert ties shall be sufficient to engage the minimum length of culvert barrel shown within the pay limits for Precast Concrete Box Culverts and will be dependent upon the length of box culvert segments furnished by the Contractor. Culvert ties are not required for box culverts having a rise (R) less than or equal to 3 ft and a span (S) greater than or equal to 10 ft.

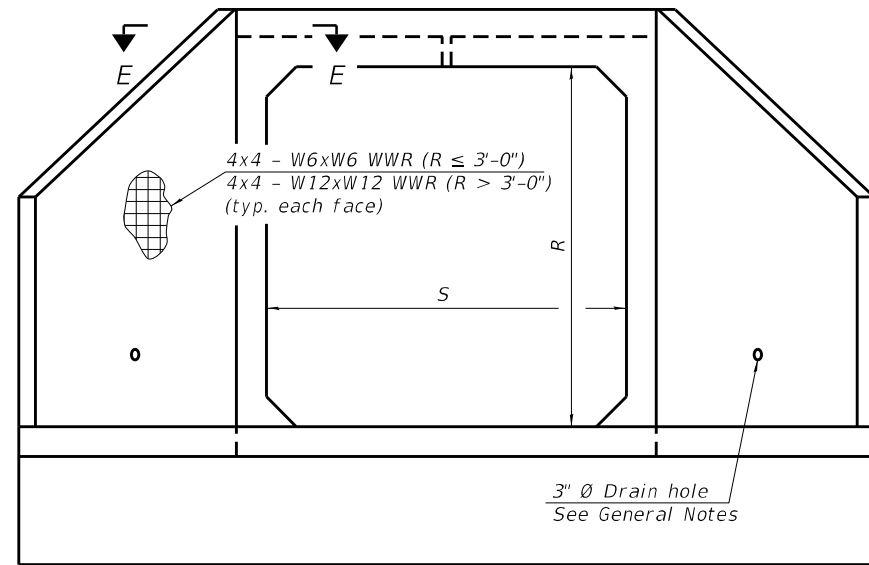
All costs associated with furnishing and installing or constructing the toewall and culvert ties will not be measured for payment but shall be included in the unit price for Box Culvert End Sections of the culvert number specified.

Shop drawings that detail slab thickness and reinforcement layout for the Box Culvert End Sections shall be provided to the Engineer for review and approval. Reinforcement bars not detailed herein shall be detailed with a clear distance at the end of the reinforcement not less than 1/2" nor more than 2". For the precast option, it shall be the Contractor's responsibility for determining a method of handling and a construction procedure shall be included in the shop drawings. The Contractor shall determine and detail in the shop drawings any necessary strengthening or stiffening provisions necessary to handle the precast segment. Any required modifications shall be at no extra charge.

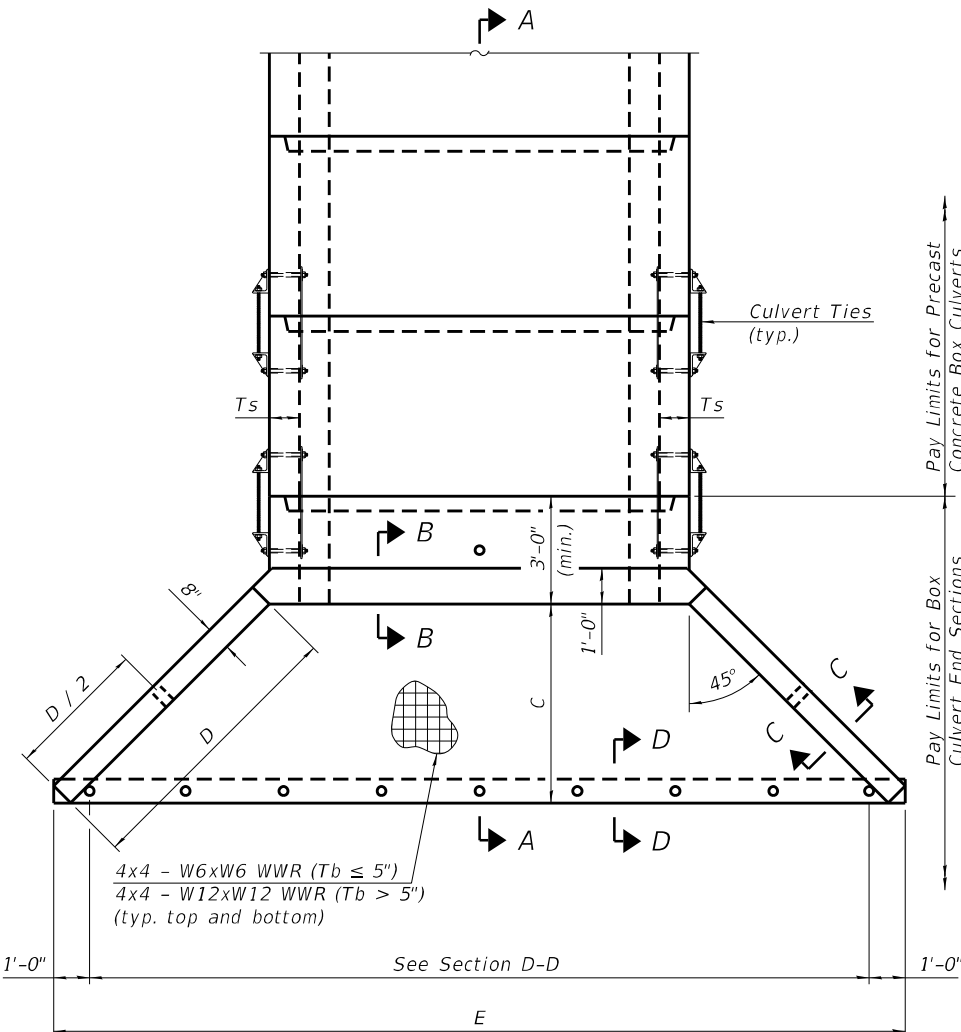
The Contractor may use reinforcement bars in lieu of welded wire reinforcement (WWR). Reinforcement bars shall be limited to the sizes of #3 through #5 bars, a maximum spacing of the lesser of 8" or the member thickness, and shall result in an area of reinforcement equal to or greater than that provided by the WWR. Minimum lap lengths detailed herein are applicable to WWR and reinforcement bars.

Reinforcement (circumferential and longitudinal) in the culvert barrel portion of the end section being lapped with reinforcement from the wingwalls or bottom slab of the end section shall not be less than that required by ASTM C 1577 for the design fill height or the reinforcement detailed for the end section, whichever is greater.

One drain hole shall be provided in each wingwall for end sections of box culverts having an opening with a clear rise greater than 3 ft. The drain hole shall be located within the lower 1/3 of the clear rise of the box culvert and shall conform to the requirements of Article 503.11 of the Standard Specifications.



END VIEW



PLAN

SCB-AES

2-17-2017

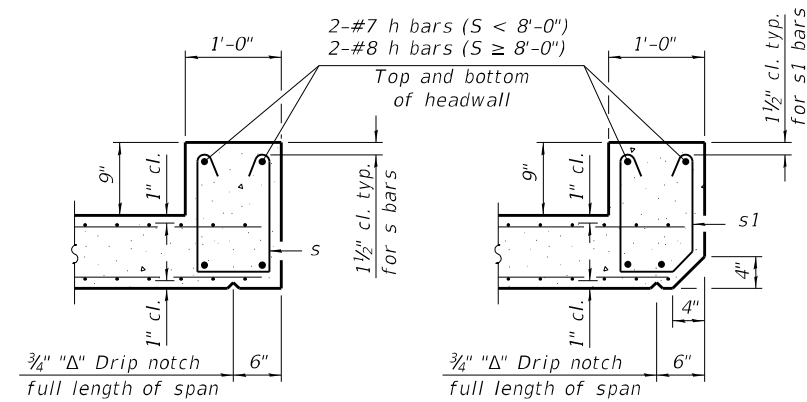
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PLOT DATE = 2/16/2021	DATE -	REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE BOX CULVERT APRON END
SECTION DETAILS - STRUCTURE NO.

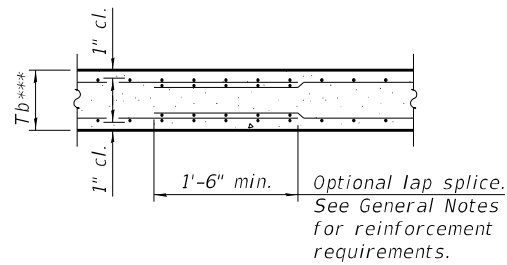
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	39
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				



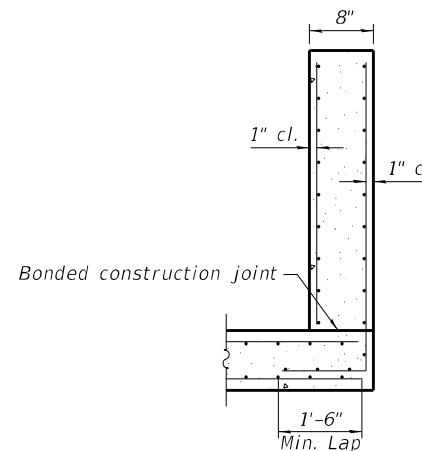
SECTION B-B
(Top slab at downstream end)

SECTION B-B
(Top slab at upstream end)

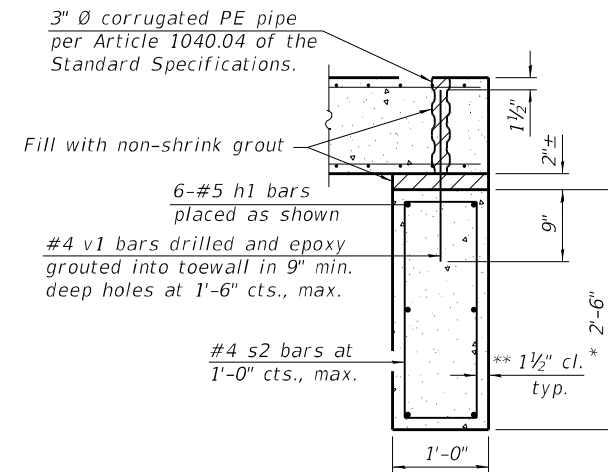


SECTION B-B
(Bottom Slab)

*** This dimension shall be increased by 2" for CIP construction.



SECTION C-C



SECTION D-D

TOEWALL CONSTRUCTION SEQUENCE

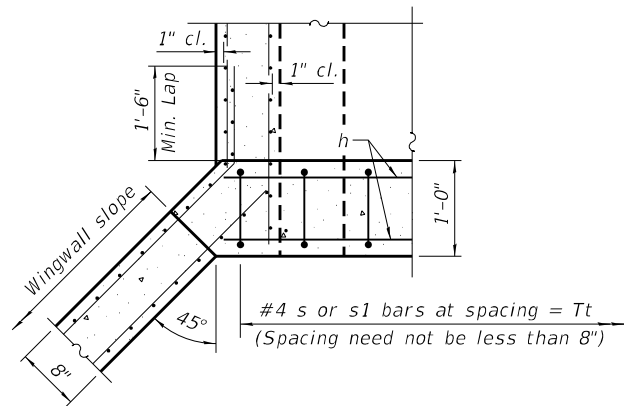
1. Perform excavation and construct toewall.
2. Backfill accordingly and place bedding for precast box culvert end sections.
3. Set precast box culvert end section.
4. Drill and epoxy grout reinforcement in toewall in accordance with Section 584 of the Standard Specifications.
5. Pressure grout voids using non-shrink grout conforming to Section 1024 of the Standard Specifications.

* The Contractor may furnish a precast or cast-in-place toewall. The Contractor shall be responsible for the strength and stability of the precast toewall during handling. Additional lifting points may be required depending upon the length of the toewall or the Contractor may need to modify the design of the toewall for the proposed handling method.

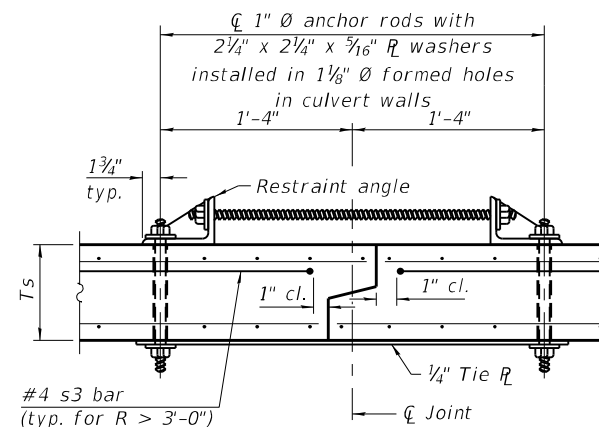
** If soil conditions permit, the sides of the toewall may be poured directly against the soil. The clear cover on the sides of the toewall shall be increased to 3" by increasing the thickness of the toewall.

Notes:

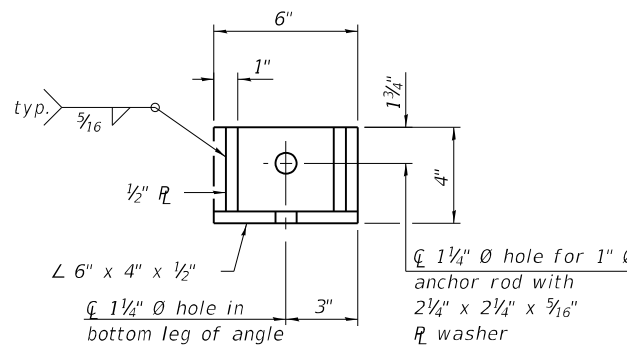
1" diameter anchor rods for the culvert ties shall conform to the requirements of ASTM F1554, Grade 105. Structural steel for the tie plate and restraint angle shall conform to the requirements of Article 1006.04 of the Standard Specifications. All components of the culvert tie detail shall be galvanized according to the requirements of AASHTO M 111 or M 232 as applicable. 2 1/4" x 2 1/4" x 3/16" plate washers shall be provided under each nut required for the anchor rods. Anchor rods connecting precast sections shall be brought to a snug tight condition followed by an additional 1/2 turn on one of the nuts for anchor rods installed in the walls. Match marks shall be provided on the bolt and nut to verify relative rotation between the bolt and the nut. Holes in the walls for the culvert tie assembly may be drilled using core bits in lieu of using formed holes.



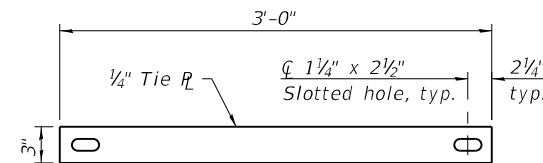
SECTION E-E



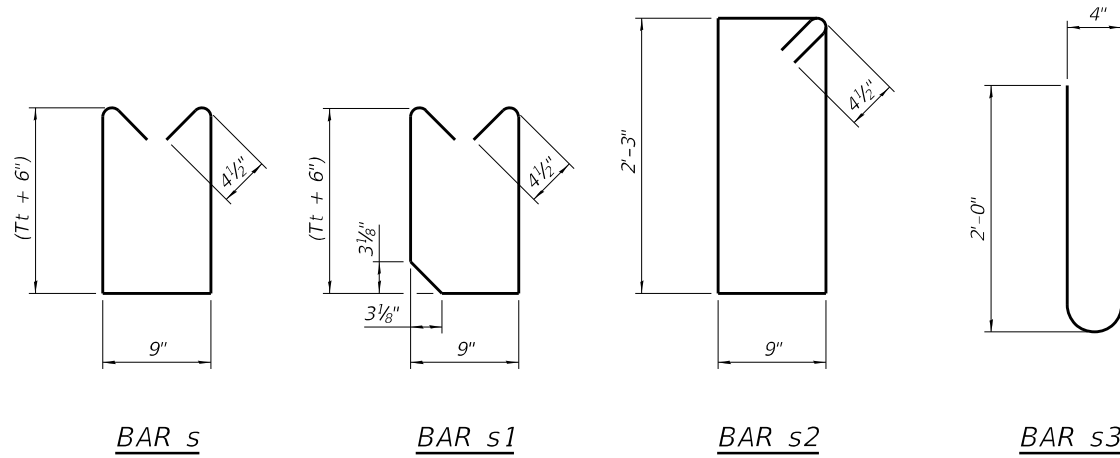
SECTION F-F
(Showing culvert tie details)



RESTRAINT ANGLE DETAIL



TIE PLATE DETAIL



BAR s

BAR s1

BAR s2

BAR s3

SCB-AES

2-17-2017

(Sheet 2 of 2)

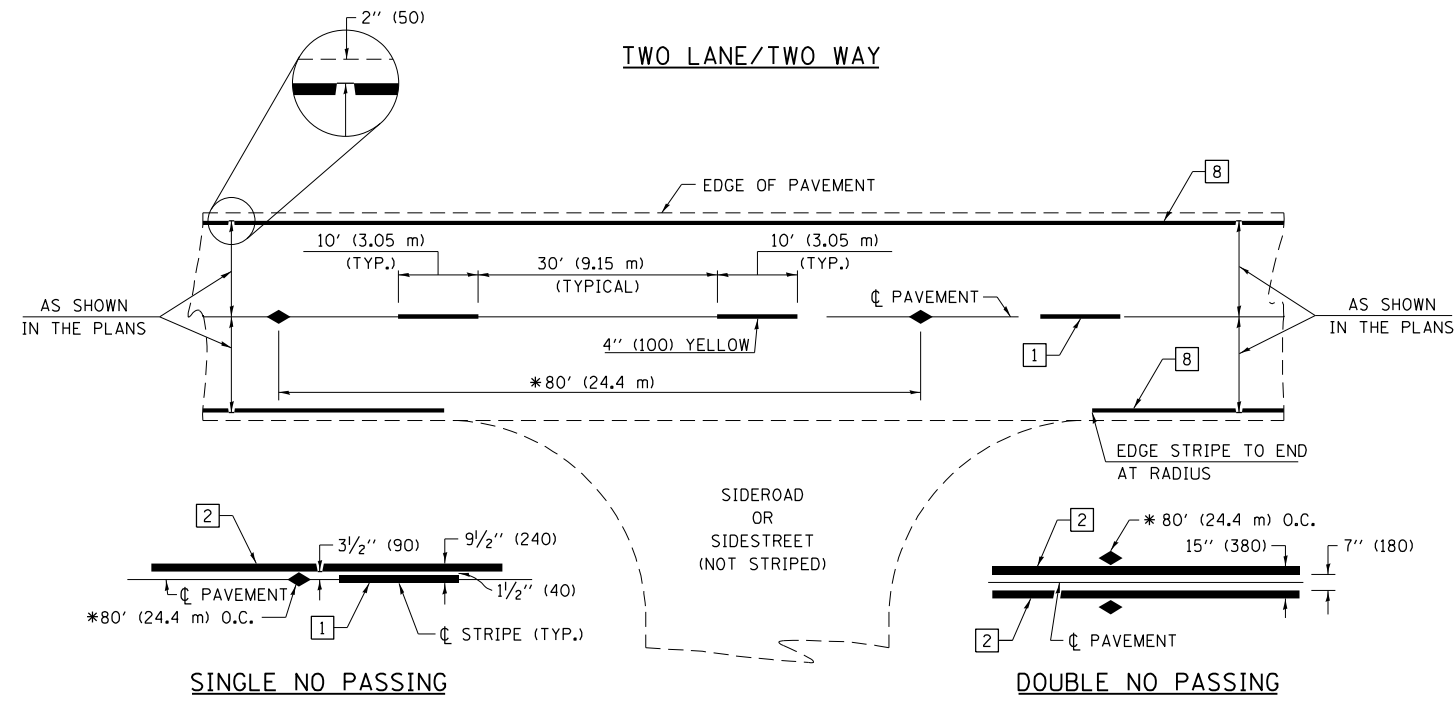
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PRECAST CONCRETE BOX CULVERT APRON END
SECTION DETAILS - STRUCTURE NO.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	40
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				

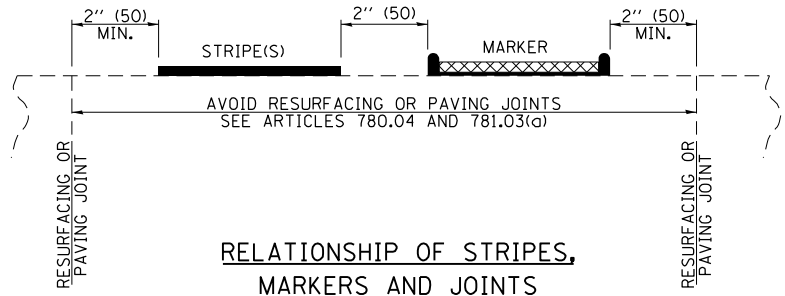
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PROJECT: 74550
OFFICE: DISTRICT 7
DRAWN: stfennmk
CHECKED: -
DATE: -
DESIGNED: -
REVISIONS: -
REVISIONS: -
REVISIONS: -
REVISIONS: -



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

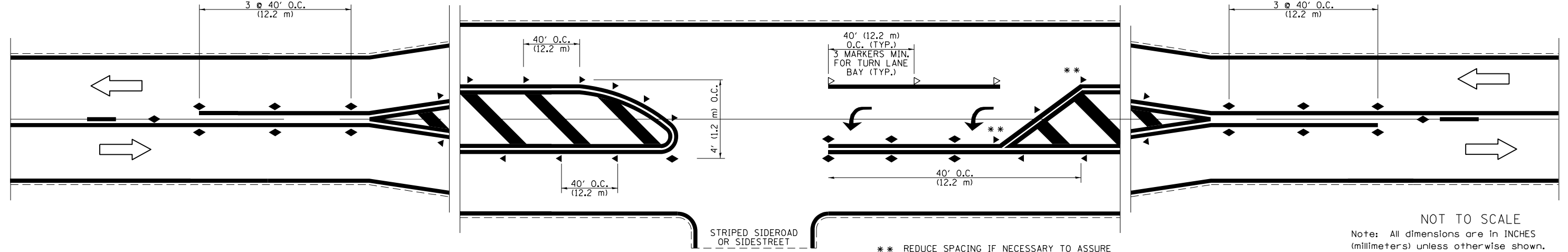


RELATIONSHIP OF STRIPES, MARKERS AND JOINTS

TYPICAL PAVEMENT MARKERS LEGEND

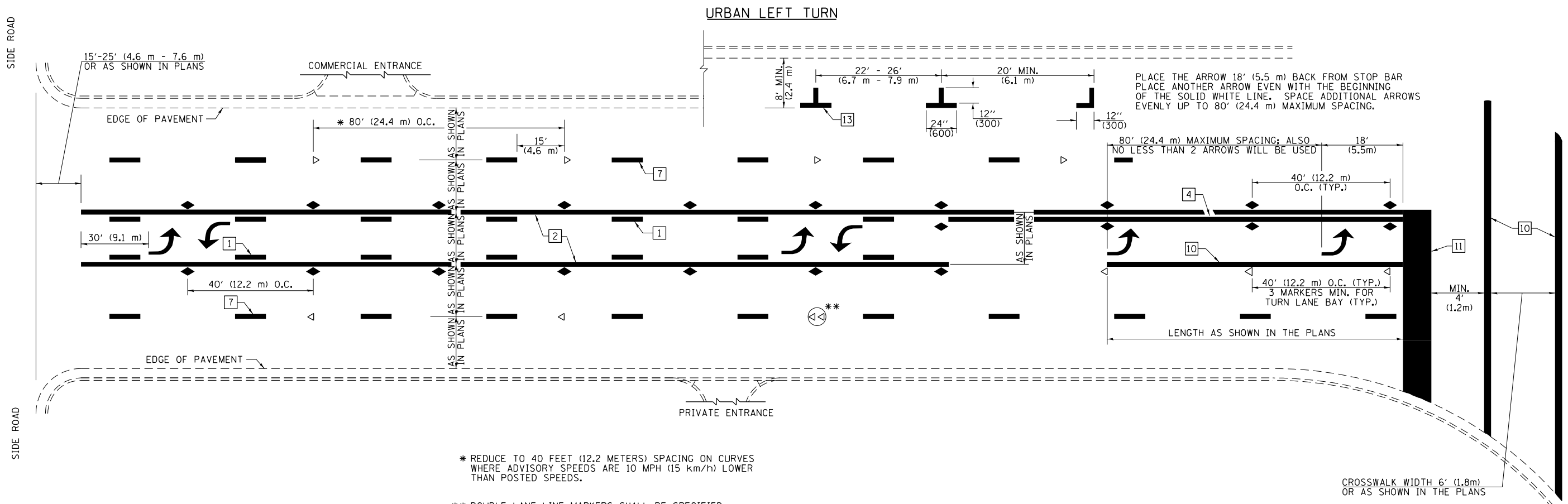
- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RAISED REFLECTIVE PAVEMENT MARKERS



** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

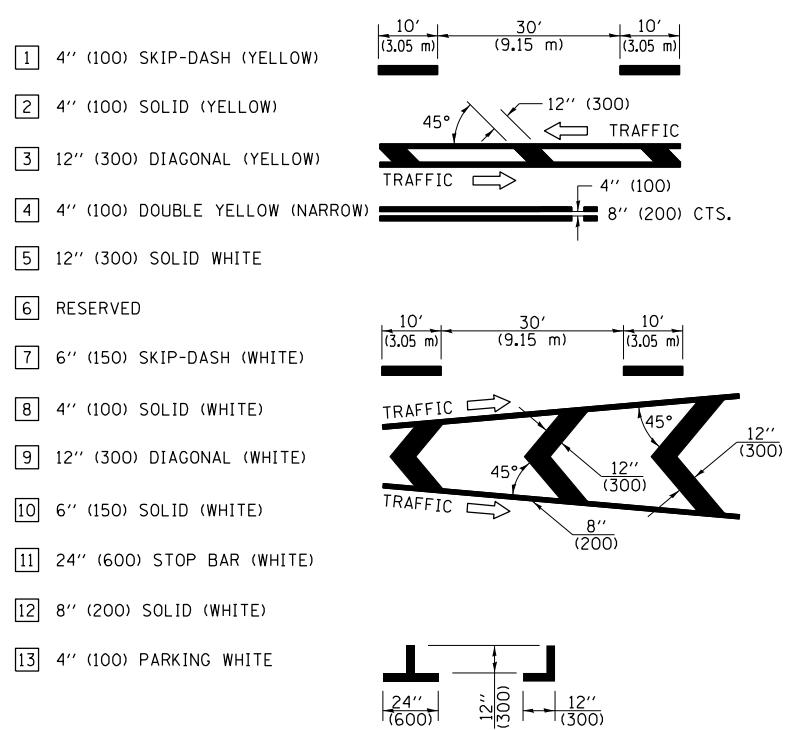
NOT TO SCALE
Note: All dimensions are in INCHES (millimeters) unless otherwise shown.



* REDUCE TO 40 FEET (12.2 METERS) SPACING ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

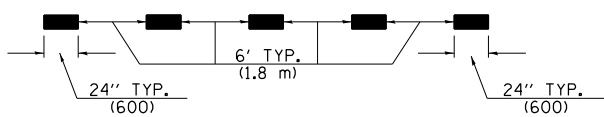
** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

PAVEMENT MARKING LEGEND

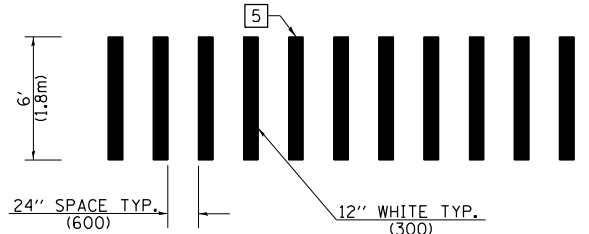


GENERAL NOTES

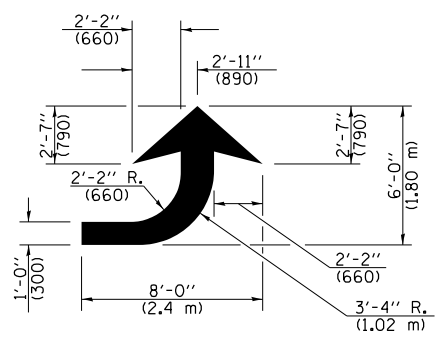
- TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE. USE A MINIMUM OF TWO PAIRS PER BLOCK.
- THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER.
- USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)
- LANE LINE EXTENSIONS SHALL BE THE SAME COLOR AND WIDTH AS THE LANE LINE BEING EXTENDED.



LANE LINE EXTENSIONS

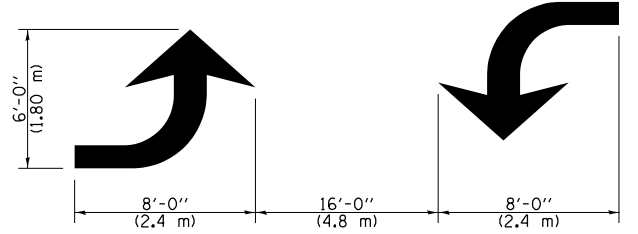


CROSSWALK DETAIL (DECATUR CITY LIMITS ONLY)



LEFT ARROW

REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)



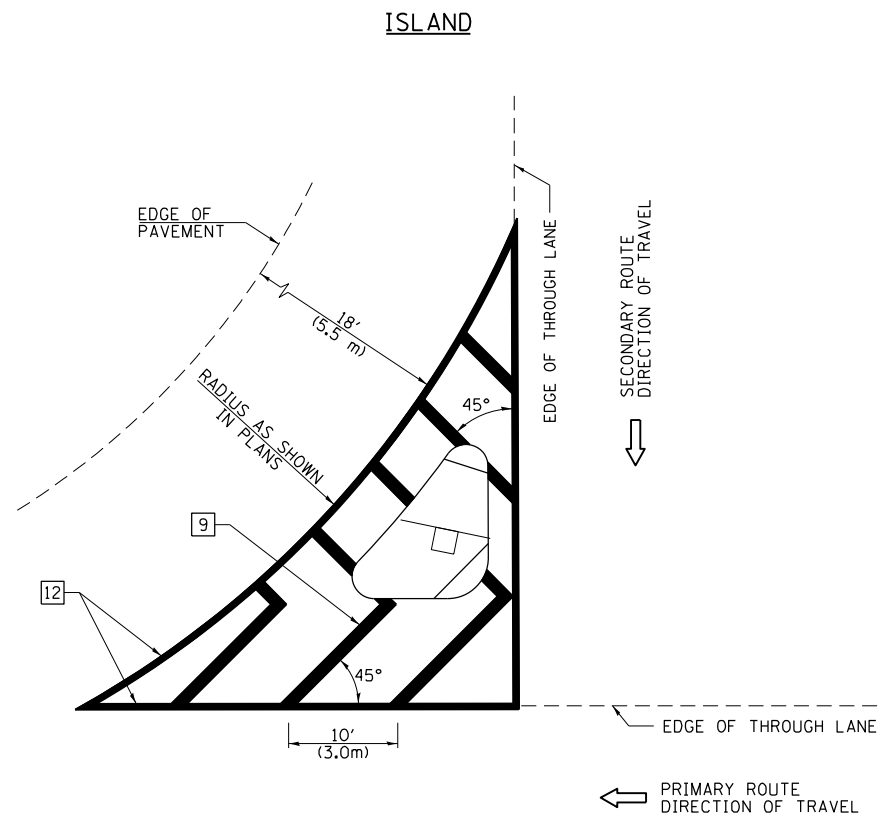
TYPICAL DOUBLE TURN ARROWS (WHITE)

NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = steffennk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 7\Projects\74550\DRAWN\CADsheets\0774550-sht-details.dgn		CHECKED -	REVISED -			823	(22,23,24)RS-3	Wayne	45	42	
		DATE -	REVISED -			CONTRACT NO. 74550		ILLINOIS FED. AID PROJECT			
						SCALE:	SHEET NO. 2 OF 4 SHEETS	STA.	TO STA.		

DISTRICT 7 DETAIL NO. 7800001

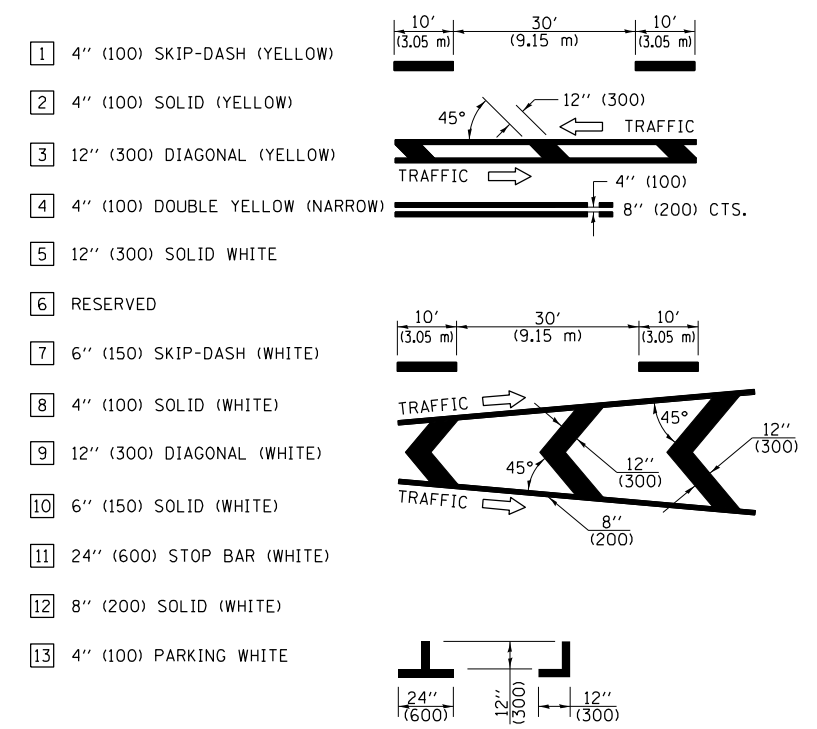


GENERAL NOTES

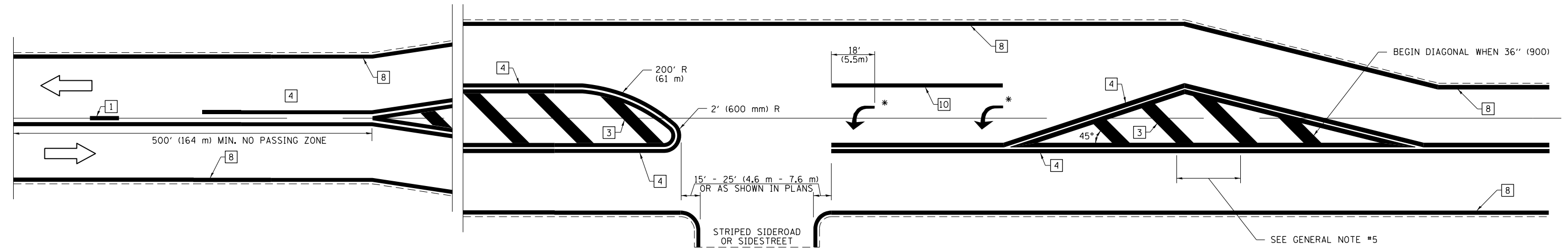
1. RAISED AND CORRUGATED MEDIANS SHALL BE OUTLINED WITH [2] IF PRESENT.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
5. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING:

< 30 MPH (< 50 km/h)	15' (4.5 m)
30-45 MPH (50-75 km/h)	20' (6.0 m)
> 45 MPH (> 75 km/h)	30' (9.0 m)

PAVEMENT MARKING LEGEND



RURAL LEFT TURN STRIPING



* PLACE AN ARROW 18' (5.5 m) BACK FROM STOP BAR. PLACE ANOTHER ARROW EVEN WITH THE BEGINNING OF THE SOLID WHITE LINE. SPACE ADDITIONAL ARROWS EVENLY UP TO 80' (24.4 m) MAXIMUM SPACING. USE MINIMUM OF 2 ARROWS.

NOT TO SCALE
 Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 7800001

FILE NAME =	USER NAME = steffenmk	DESIGNED -	REVISED -
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 7\Projects\74550\DRAWN\CADsheets\0774550-sht-details.dgn		REVISOR -	REVISED -
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	PLOT DATE = 2/16/2021	DATE -	REVISED -

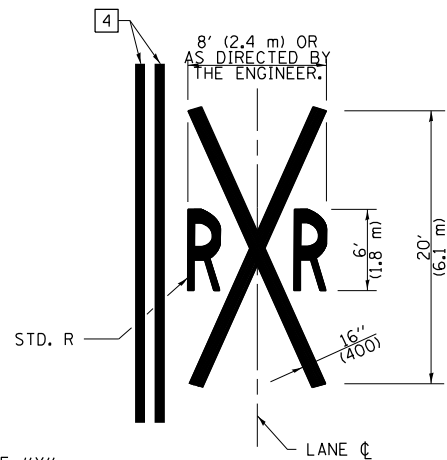
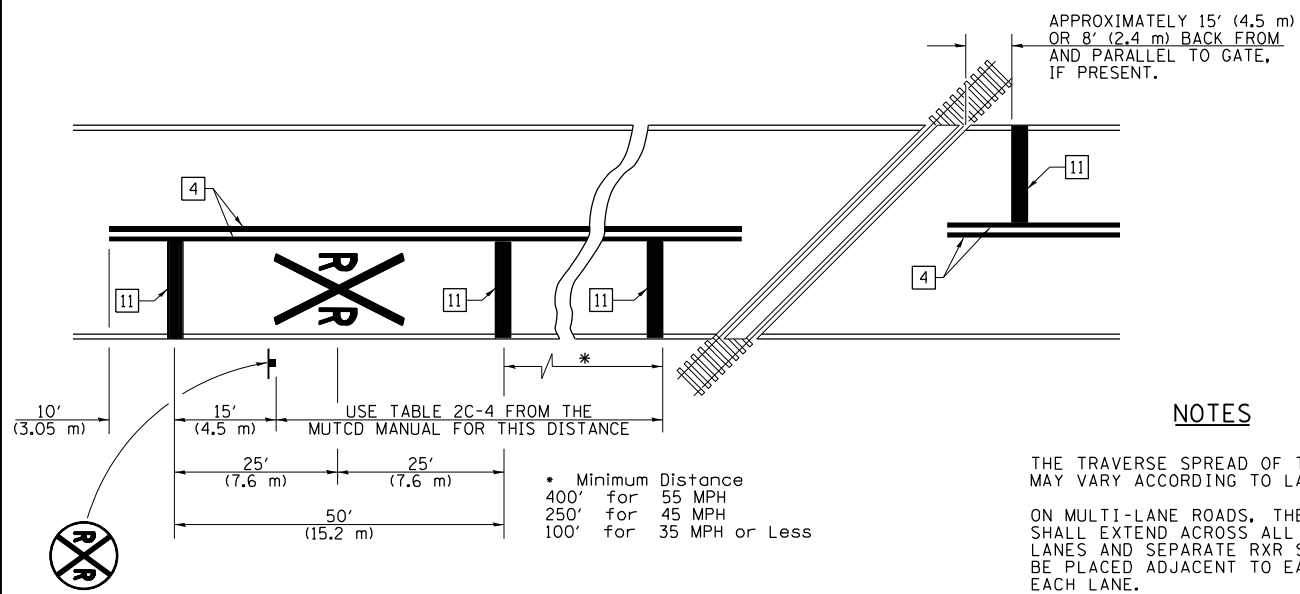
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS
 (RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	43
ILLINOIS FED. AID PROJECT			CONTRACT NO. 74550	

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING



NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

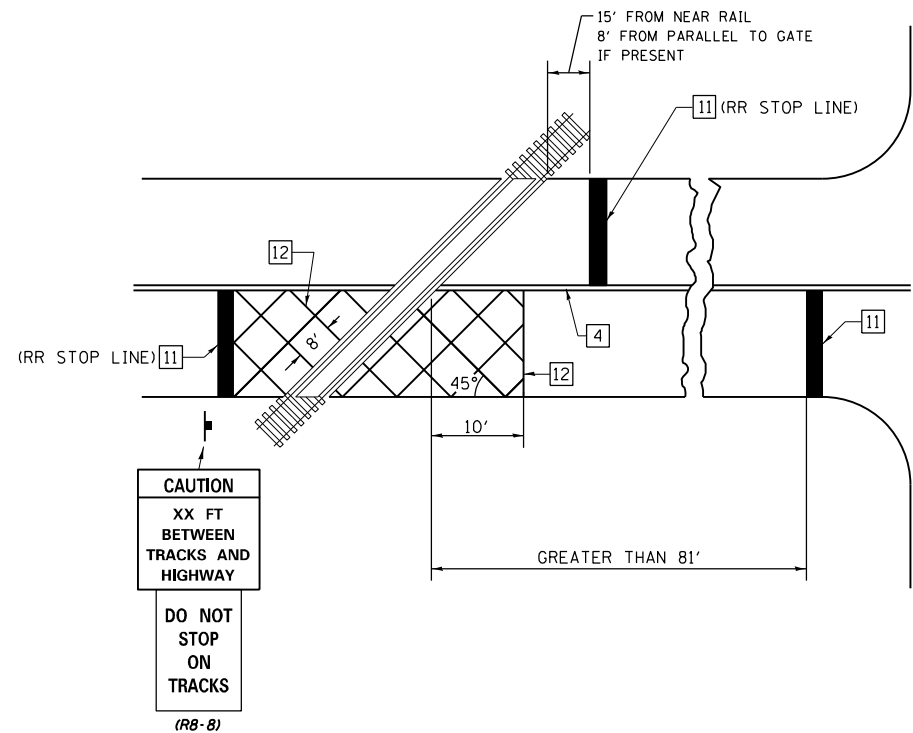
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

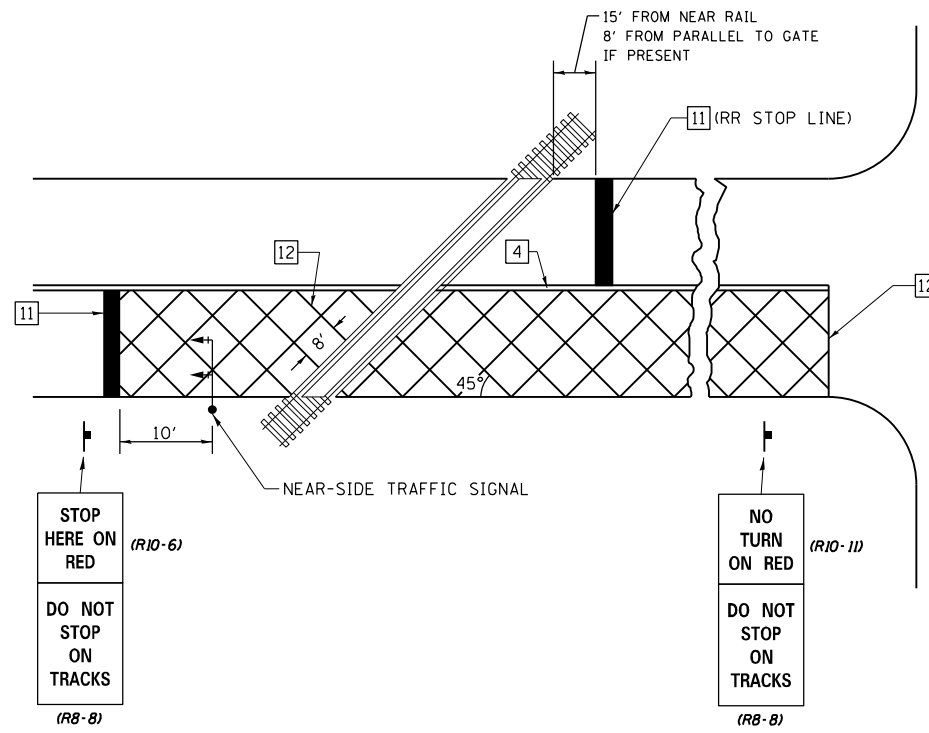
PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 12" (300) SOLID WHITE
- 6 RESERVED
- 7 6" (150) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) PARKING WHITE

RAILROAD CROSSING WITH INTERCONNECT ONLY



RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.

SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

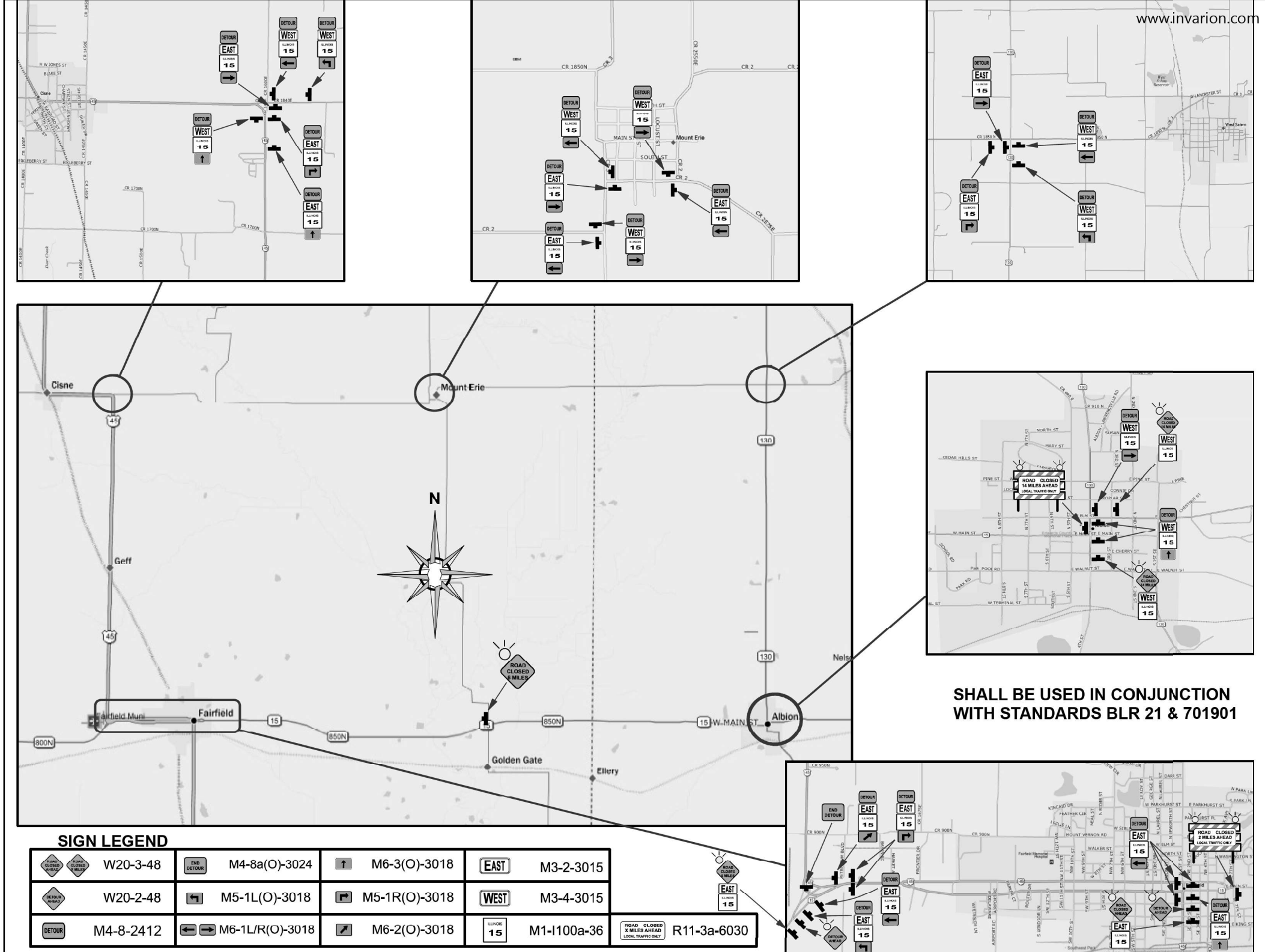
NOT TO SCALE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 7 DETAIL NO. 78000001

FILE NAME =	USER NAME = steffenk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING AND RAISED REFLECTIVE PAVEMENT MARKERS (RURAL & URBAN APPLICATIONS)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISIONS	REVISIONS			CONTRACT NO. 74550		ILLINOIS FED. AID PROJECT			
PLOT DATE = 2/16/2021	DATE -	REVISIONS	REVISIONS			SCALE:	SHEET NO. 4 OF 4 SHEETS	STA.	TO STA.		

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SIGN LEGEND			
W20-3-48	W20-2-48	M4-8-2412	M4-8a(O)-3024
M5-1L(O)-3018	M5-1R(O)-3018	M6-1L/R(O)-3018	M6-2(O)-3018
M6-3(O)-3018	M6-1R(O)-3018	M6-2(O)-3018	M6-3(O)-3018
M3-2-3015	M3-4-3015	M1-100a-36	R11-3a-6030
M3-2-3015	M3-4-3015	M1-100a-36	R11-3a-6030

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DETOUR DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
823	(22,23,24)RS-3	Wayne	45	45
CONTRACT NO. 74550				
ILLINOIS FED. AID PROJECT				