

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60D70		

D-91-089-08

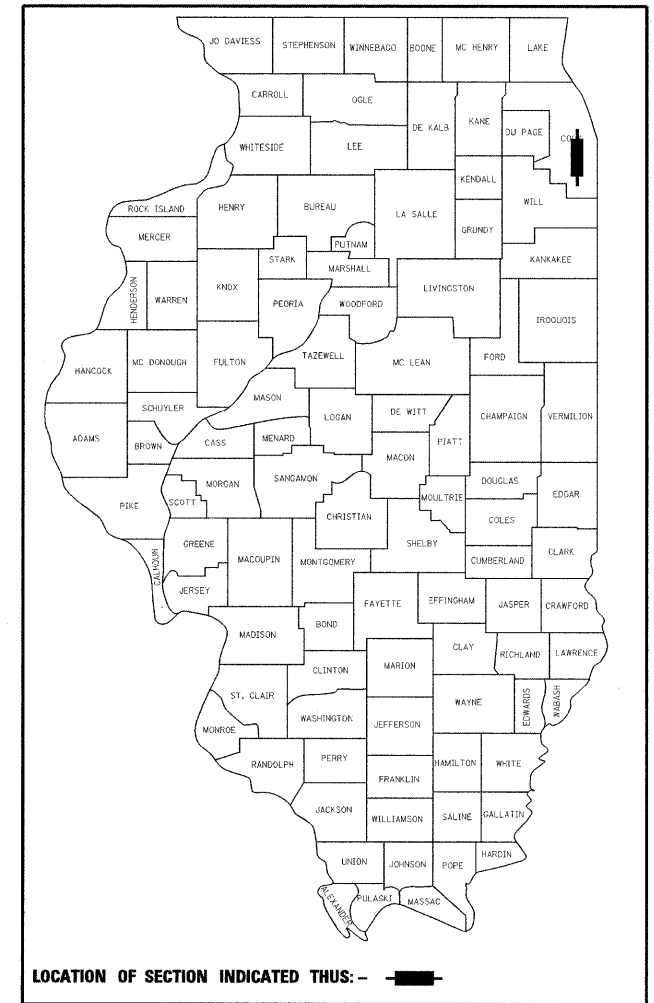
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 348: IL 43
OVER STONY CREEK
SECTION 3128-Z-B-1
BRIDGE REPAIR & RESURFACE APPROACHES
STRUCTURE NO. 016-0322
COOK COUNTY
C-91-089-08

PROJECT: ESP-0348(042)

FOR INDEX OF SHEETS, SEE SHEET NO. 2



LOCATION OF SECTION INDICATED THIS: - [Symbol] -

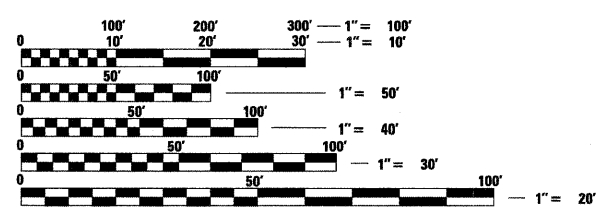
IMPROVEMENT LOCATED IN
THE CITY OF PALOS HILLS
AND THE VILLAGE OF WORTH

IMPROVEMENT BEGINS
STA. 177 + 96

IMPROVEMENT ENDS
STA. 184 + 10

TRAFFIC DATA

2006 ADT = 45,100 VEHICLES PER DAY
SPEED LIMIT = 35 MPH

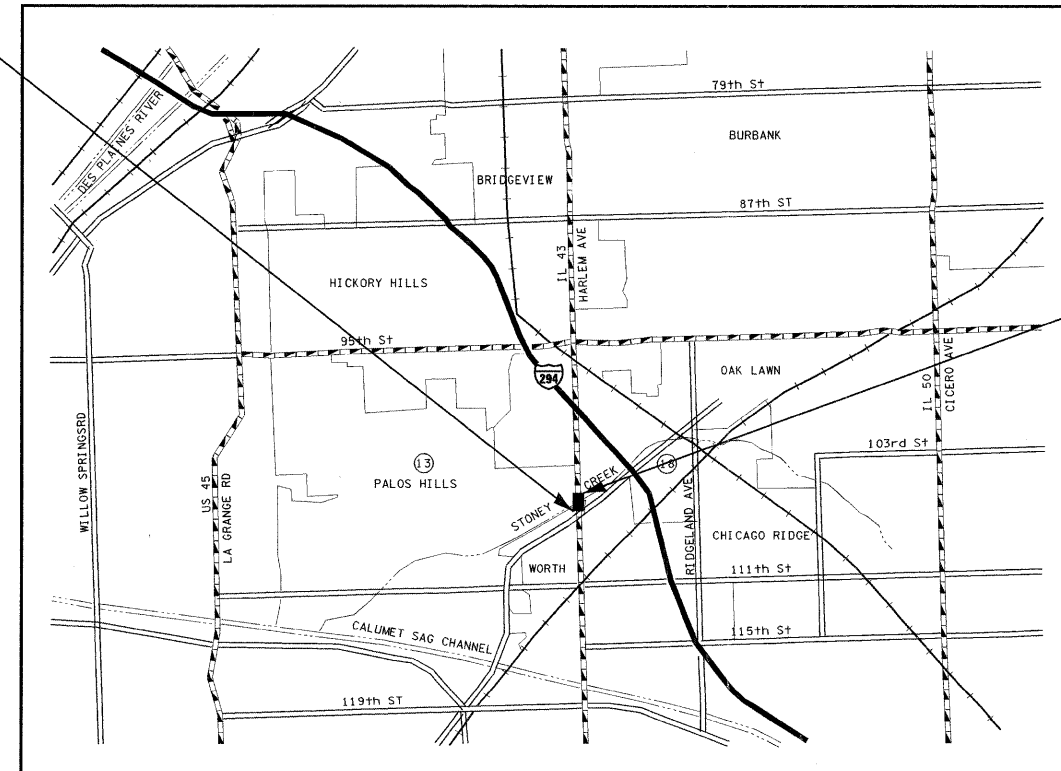


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

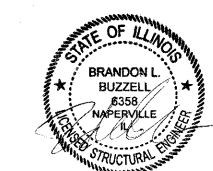
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: MICHELLE AQUINO (847) 705-4606
PROJECT MANAGER: LONG TRAN (847) 705-4232

CONTRACT NO. 60D70



GROSS & NET LENGTH OF PROJECT = 214 FT = 0.04 MILES



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 6, 20 09

Diana M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 27, 20 09
Charles G. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT

March 27, 20 09
Christine M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

rjngroup License # 184-000813

200 West Front Street
Wheaton, IL 60187

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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5	PAVEMENT MARKING PLAN
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8	INTERCONNECT SCHEMATICS
9-14	TRAFFIC SIGNAL SCHEMATICS
15	ALTERNATE ROUTE PLAN
16	GENERAL PLAN AND ELEVATION
17	TYPICAL CROSS SECTIONS WITH STAGING
18	EXPANSION JOINT REMOVAL AT DECK
19	EXPANSION JOINT REHABILITATION AT DECK
20	EXPANSION JOINT REHABILITATION AT PARAPET AND WINGWALL
21	LONGITUDINAL JOINT DETAILS
22	PARAPET REHABILITATION
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28	PIER REPAIRS I
29	PIER REPAIRS II
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31	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
32	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
33	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
34	ARTERIAL ROAD INFORMATION SIGN
35	DRIVEWAY ENTRANCE SIGNING

GENERAL NOTES - ROADWAY

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800)892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED.)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT). IN ACCORDANCE WITH "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREA.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE ENGINEER SHALL CONTACT PATRICE HARRIS TRAFFIC FIELD ENGINEER (OR TECHNICIAN), AT (708)597-9800 A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM BRIDGE INSPECTORS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.
- SAW CUTTING OF PAVEMENTS, SHOULDERS, ETC., SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN, STRAIGHT EDGE ON THE PORTION REMAINING. THE COST OF SAW CUTTING REMOVAL ITEMS SHALL BE INCLUDED IN THE UNIT PRICES OF THESE ITEMS.

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
515001-03	NAME PLATE FOR BRIDGES
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701606-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIUM
701801-04	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN PANEL ERECTION DETAILS
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES - TRAFFIC CONTROL & PROTECTION

- STAGING PROCEDURES PRESENTED ARE THE SUGGESTED SEQUENCE OF OPERATIONS. AT HIS OPTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE STAGING PROPOSAL TO THE ENGINEER FOR HIS APPROVAL.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OF CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. THE CONTRACTOR SHALL RESPOND TO ANY REQUEST MADE BY THE ENGINEER FOR CORRECTION WITHIN TWO HOURS FROM THE TIME OF NOTIFICATION.
- ALL TEMPORARY PAVEMENT MARKINGS PROPOSED WITHIN THE WORK AREA SHALL BE COMPLETED PRIOR TO THE CONSTRUCTION PHASE CHANGE.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.

PLOT DATE = 2/12/2009
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = JTracey
 MODEL = Default



Excellence through Ownership

200 West Front Street
Wheaton, IL 60187

DESIGNED - B.B.	REVISED -
DRAWN - J.T.T.	REVISED -
CHECKED - M.K.	REVISED -
DATE - 1/30/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322**

**INDEX OF SHEETS, STANDARDS,
AND GENERAL NOTES**

SCALE: 1 SHEET NO. 2 OF 35 SHEETS STA. TO STA.

F.A.P. RTE. 348	SECTION 3128-Z-B-1	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 2
CONTRACT NO. 60D70				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE BRIDGE SN 016-0322 100% FED. SFTY-2A	
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	QUANTITY	QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	85	85	
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	23	23	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	50	50	
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	52	52	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SO FT	90	90	
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	527	527	
44000600	SIDEWALK REMOVAL	SO FT	90	90	
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SO YD	676	676	
50102400	CONCRETE REMOVAL	CU YD	102.7	102.7	
50104000	BRIDGE RAIL REMOVAL	FOOT	211	211	
50300255	CONCRETE SUPERSTRUCTURE	CU YD	117.1	117.1	
50300260	BRIDGE DECK GROOVING	SO YD	612	612	
50300300	PROTECTIVE COAT	SO YD	336	336	
50500715	JACK AND REMOVE EXISTING BEARINGS	L SUM	1	1	
50606701	CLEANING AND PAINTING STRUCTURAL STEEL, LOCATION 1	L SUM	1	1	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	16,070	16,070	
50800515	BAR SPLICERS	EACH	48	48	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	200	200	
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	48	48	
67100100	MOBILIZATION	L SUM	1	1	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6	
70101800	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10	
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	171	171	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	57	57	
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	694	694	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	24	24	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SO FT	54	54	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	18	18	
* 81100700	CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVANIZED STEEL	FOOT	60	60	
* 81300835	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"x18"x10"	EACH	2	2	
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	2	
* 89502200	MODIFY EXISTING CONTROLLER	EACH	2	2	
* 89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	1,200	1,200	

* SPECIALTY ITEM

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE BRIDGE SN 016-0322 100% FED. SFTY-2A	
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	QUANTITY	QUANTITY
X0322185	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SO YD	634	634	
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	77	77	
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SO FT	3	3	
X0325305	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SO FT	22	22	
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	2,061	2,061	
Z0006202	BRIDGE DECK HYDRO-SCARIFICATION 3/4"	SO YD	634	634	
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SO YD	20	20	
Z0016200	DECK SLAB REPAIR (PARTIAL)	SO YD	4	4	
Z0073300	TEMPORARY SHORING AND CRIBBING	L SUM	1	1	
* X0325938	TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1	1	
* X0326357	ROADWAY LIGHTING MODIFICATIONS	L SUM	1	1	
* XX003079	REMOVE JUNCTION BOX	EACH	6	6	
* XX007743	ROADWAY LIGHTING CONDUIT REMOVAL	FOOT	40	40	
* B1300550	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12"x12"x6"	EACH	4	4	
* X0323574	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	3	3	

* SPECIALTY ITEM
 • Y080

PLOT DATE = 2/12/2009
 FILE NAME = 601123002.ctb
 USER = JTP
 MODEL = Default



200 West Front Street
 Wheaton, IL 60187

DESIGNED -	B.B.	REVISED -	
DRAWN -	J.T.T.	REVISED -	
CHECKED -	M.K.	REVISED -	
DATE -	1/30/09	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

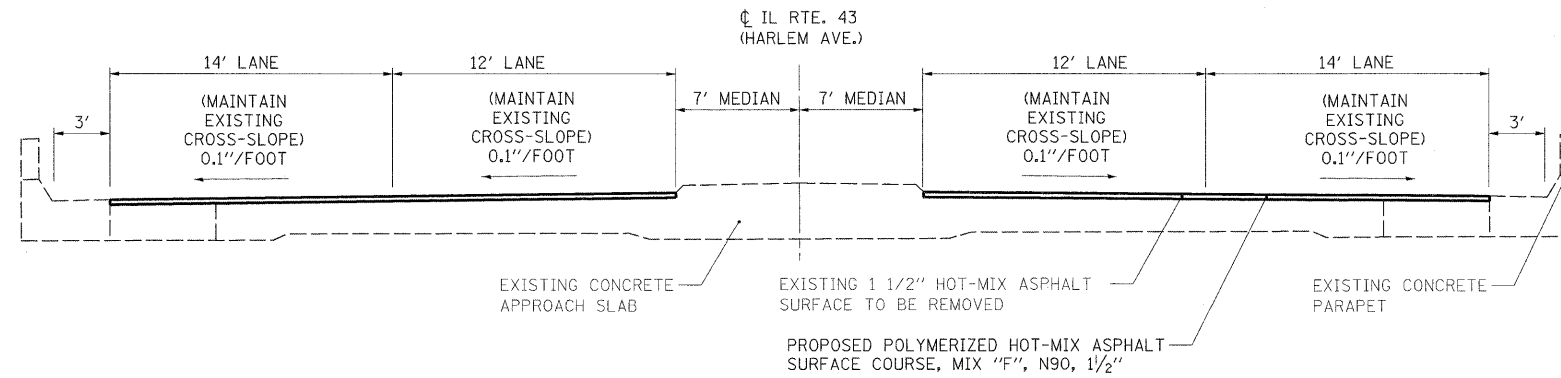
IL-43 OVER STONY CREEK
 STRUCTURE NO. 016-0322

SUMMARY OF
 QUANTITIES

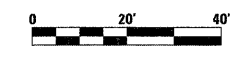
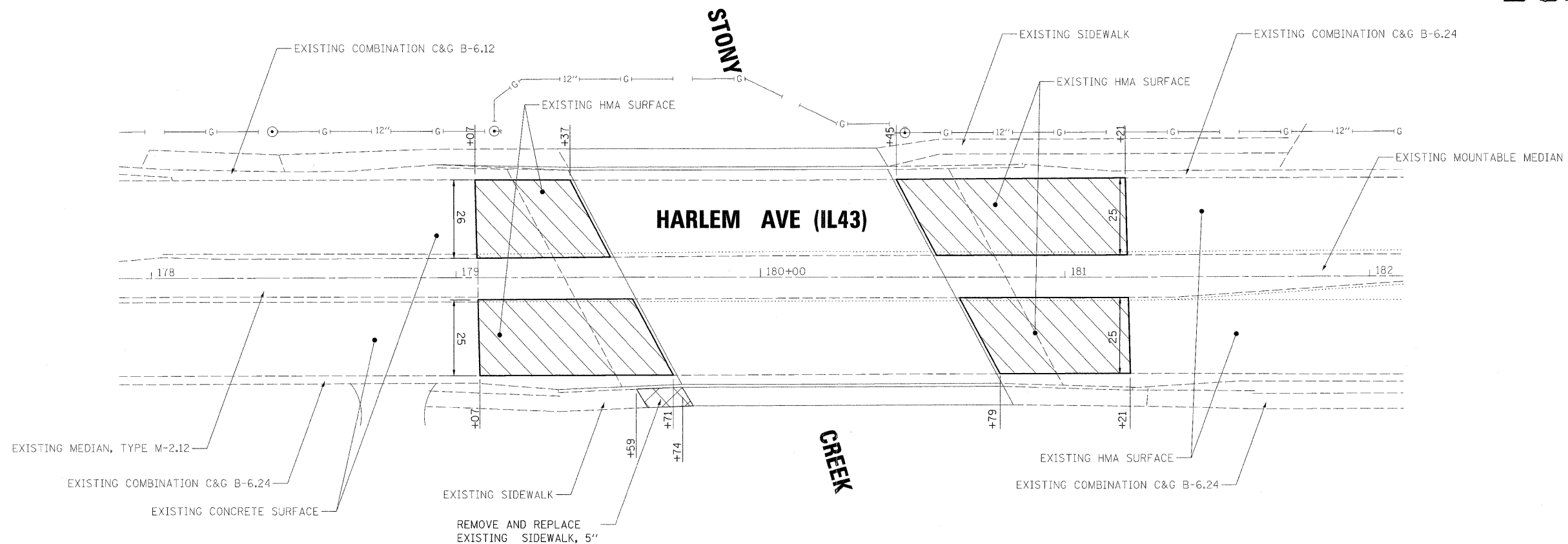
F.A.P. RTE. 348	SECTION 3128-Z-B-1	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 3
CONTRACT NO. 60D70				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				


Rev.

SCALE: SHEET NO. 3 OF 35 SHEETS STA. TO STA.



TYPICAL SECTION
 STA. 179+35.53 TO 179+55.32
 STA. 180+60.58 TO 180+80.52



 PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 1/2"

MIXTURE REQUIREMENTS		
MIXTURE USES	AC TYPE	VOIDS
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, IL-9.5mm	SBS / SBR PG 70-22	4% @ 90 GYR.
LEVELING BINDER (MACHINE METHOD), N70, IL-9.5mm	PG 64-22 *	4% @ 70 GYR.

NOTES:
 THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.
 *WHEN RAP EXCEEDS 20% , THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

PLOT DATE = 2/12/2009
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = JTFreese
 MODEL = Default



200 West Front Street
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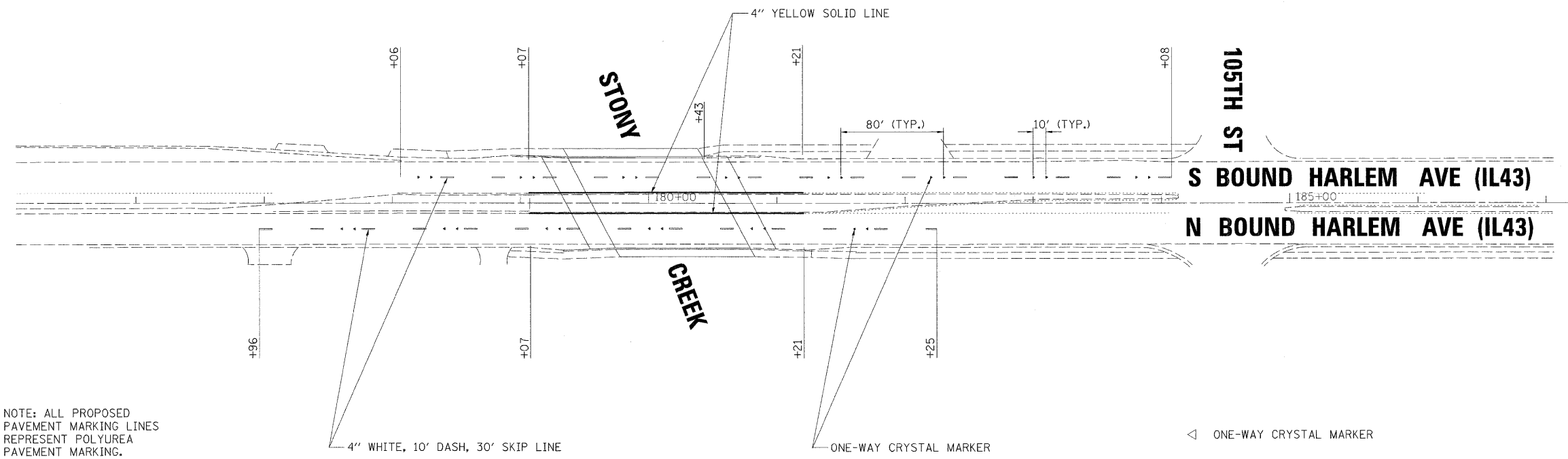
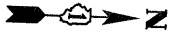
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

IL-43 OVER STONY CREEK
 STRUCTURE NO. 016-0322
 SCALE: 1:20 SHEET NO. 4 OF 35 SHEETS

ROADWAY PLAN
 STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	3128-Z-B-1	COOK	35	4
CONTRACT NO. 60D70				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

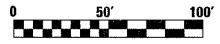


NOTE: ALL PROPOSED PAVEMENT MARKING LINES REPRESENT POLYUREA PAVEMENT MARKING.

4" WHITE, 10' DASH, 30' SKIP LINE

ONE-WAY CRYSTAL MARKER

◁ ONE-WAY CRYSTAL MARKER



PLOT DATE = 2/12/2009
 PLOT SCALE = 1"=50'
 USER NAME = JTFault
 MODEL = Default



200 West Front Street
Wheaton, IL 60187

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CHECKED - M.K.	REVISED -
DATE - 1/30/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

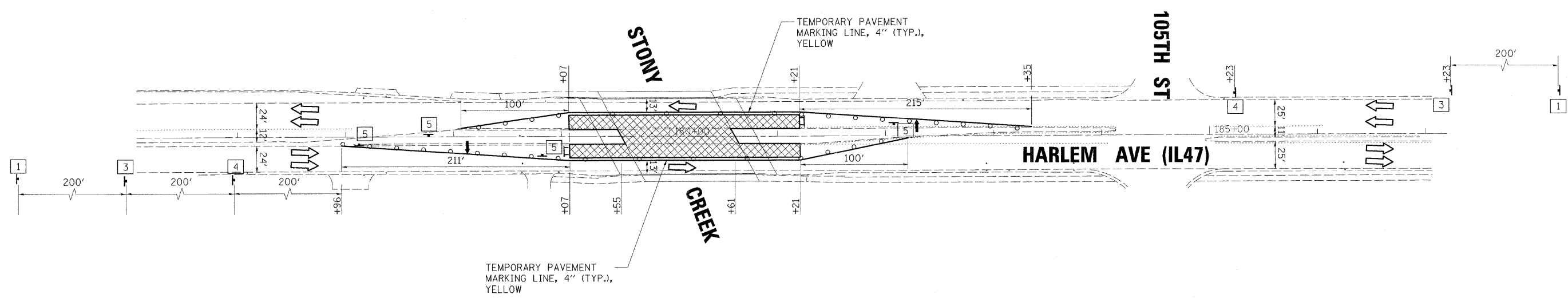
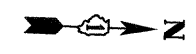
**IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322**

**PAVEMENT MARKING
PLAN**

SCALE: 1:50 SHEET NO. 5 OF 35 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	5
CONTRACT NO. 60D70				

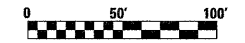
FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



- LEGEND**
- WORK AREA
 - SIGN
 - TYPE II BARRICADES
 - o 50' C-C ON TANGENT
 - o 25' C-C ON TAPERS (TYP.)
 - TYPE III BARRICADE
 - ARROW BOARD

NOTE: PAVEMENT MARKING AND RPM REMOVAL STA. 176+96 TO 179+07 AND STA. 181+22 TO 184+08

- 1 W20-1(O)-48
- 2 W-20-5R(O)-48
- 3 W20-5L(O)-48
- 4 W-21-1110-48 OR W-21-1a(O)-48
- 5 R3-5R



PLOT DATE = 2/12/2009
 FILE NAME = 91122502_01v1\143_sheets\W07_IL47.dgn
 PLOT SCALE = 1/8"=1'-0"
 USER NAME = JTreece
 MODEL = Stage 1



200 West Front Street
Wheaton, IL 60187

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DRAWN -	J.T.T.	REVISED -	
CHECKED -	M.K.	REVISED -	
DATE -	1/30/09	REVISED -	

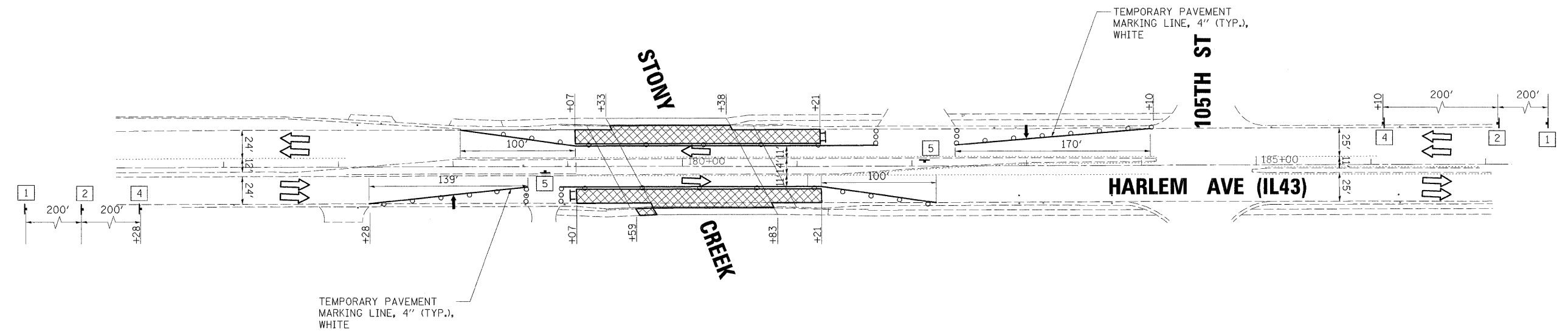
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322**

**MAINTENANCE OF TRAFFIC
STAGE 1**

SCALE: 1:50 SHEET NO. 6 OF 35 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	6
CONTRACT NO. 60D70				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LEGEND

- WORK AREA
- SIGN
- TYPE II BARRICADES
 @ 50' C-C ON TANGENT
 @ 25' C-C ON TAPERS (TYP.)
- TYPE III BARRICADE
- ARROW BOARD

NOTE: PAVEMENT MARKING AND RPM REMOVAL STA. 176+96 TO 179+07 AND STA. 181+22 TO 184+08

1		4	
	W20-1(O)-48		W-21-1110-48 W-21-1d(O)-48
2		5	
	W-20-5R(O)-48		R3-5R
3			
	W20-5L(O)-48		



PLOT DATE = 2/12/2009
 PLOT SCALE = 1/8"=1'-0"
 PLOT USER = JTFreese
 PLOT MODEL = Stage 2



200 West Front Street
Wheaton, IL 60187

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CHECKED - M.K.	REVISED -
DATE - 1/30/09	REVISED -

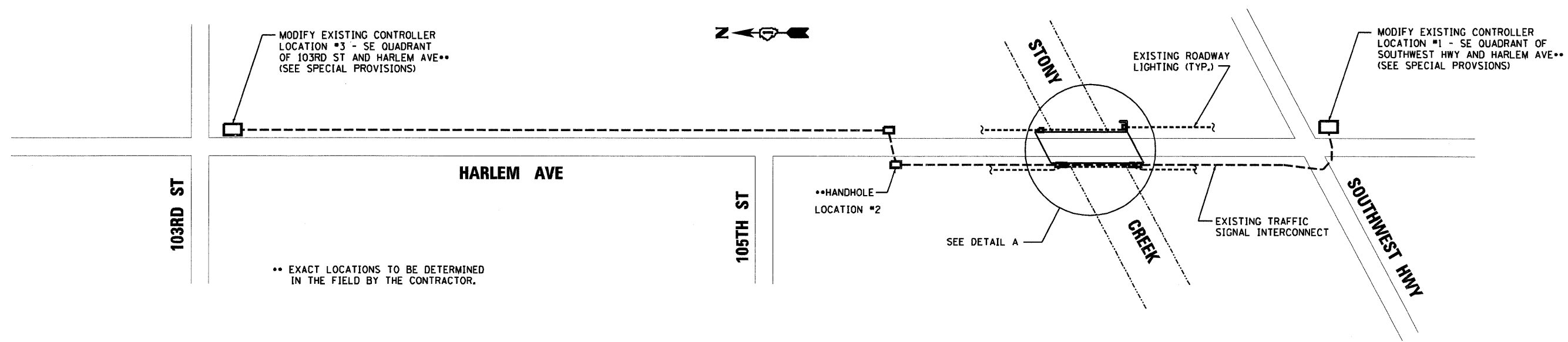
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322**

**MAINTENANCE OF TRAFFIC
STAGE 2**

SCALE: 1/8"=1'-0" SHEET NO. 7 OF 35 SHEETS STA. TO STA.

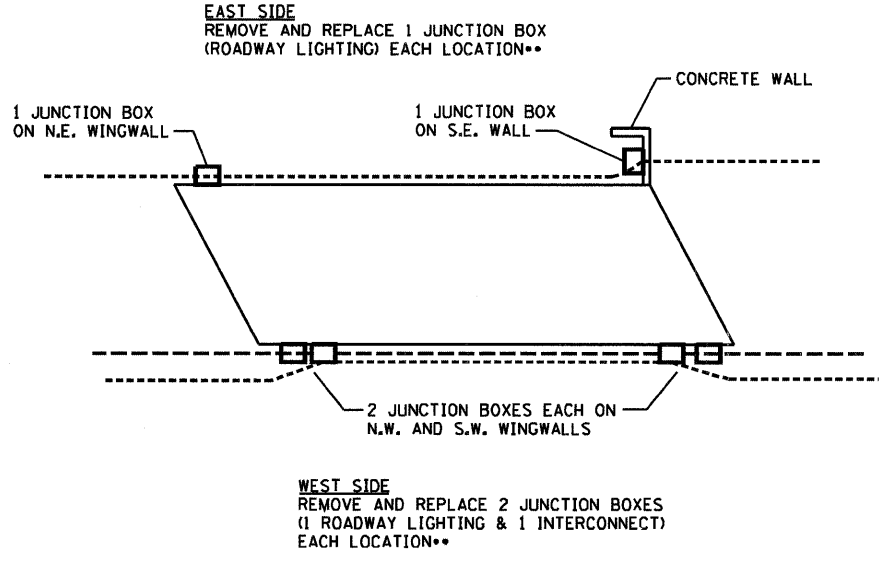
F.A.P. RTE. 348	SECTION 3128-Z-B-1	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 7
CONTRACT NO. 60D70				
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



INTERCONNECT AND UTILITY SCHEMATIC
NOT TO SCALE

NOTES

- EXISTING FIBER OPTIC CABLE SHALL BE PULLED OUT OF THE EXISTING CONDUIT, STARTING AT THE EXISTING CONTROLLER BOX AT IL-43 AND SOUTHWEST HIGHWAY (LOCATION #1), ALL THE WAY TO THE FIRST HANDHOLE NORTH OF THE BRIDGE (LOCATION #2). THIS CABLE SHALL BE SPOOLED UP NEAR LOCATION #2 AND PROTECTED ON-SITE FOR THE DURATION OF THE BRIDGE CONSTRUCTION. AFTER BRIDGE CONSTRUCTION IS COMPLETED AND NEW JUNCTION BOXES AND CONDUIT ARE IN PLACE, THE EXISTING CABLE SHALL BE REINSTALLED IN THE CONDUIT, ALL THE WAY BACK TO THE CONTROLLER BOX AT LOCATION #1. COST INCLUDED WITH "REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT".
- REMOVAL OF FIBER OPTIC CABLE SHALL NOT BEGIN UNTIL ALL COMPONENTS OF THE TEMPORARY WIRELESS INTERCONNECT SYSTEM ARE IN PLACE AND OPERATIONAL.
- ESTIMATED LENGTH OF INTERCONNECT CABLE TO BE REMOVED AND REINSTALLED = 900 FEET.
- THE TWO EXISTING ROADWAY LIGHTING CABLES SHALL BE CUT AT THE JUNCTION BOXES AT THE NORTH END OF THE BRIDGE, PULLED THROUGH THE CONDUIT AND JUNCTION BOX AT THE SOUTH END OF THE BRIDGE TO THE NEAREST HANDHOLE SOUTH OF THE BRIDGE, THEN SPOOLED UP AND PROTECTED NEAR THAT LOCATION FOR THE DURATION OF THE BRIDGE CONSTRUCTION. AFTER BRIDGE CONSTRUCTION IS COMPLETE AND NEW CONDUIT AND JUNCTION BOXES ARE IN PLACE, THE EXISTING CABLE SHALL BE REINSTALLED, AND FIELD-SPLICED WITHIN THE NEW JUNCTION BOXES AT THE NORTH END OF THE BRIDGE. COST INCLUDED WITH "REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT".
- REMOVAL OF ROADWAY LIGHTING CABLE SHALL NOT BEGIN UNTIL ALL PARTS ARE IN PLACE AND FUNCTIONAL TO ALLOW THE CONTINUED OPERATION OF THE EXISTING ROADWAY LIGHTING SYSTEM.
- ESTIMATED LENGTH OF ROADWAY LIGHTING CABLE TO BE REMOVED AND REINSTALLED = 150 FEET AT EACH LOCATION (ONE ATTACHED TO EAST SIDE, ONE ATTACHED TO WEST SIDE), 300 FEET TOTAL.
- A NOMINAL QUANTITY OF 10 FEET OF NEW CONDUIT HAS BEEN PROVIDED AT EACH OF THE SIX JUNCTION BOX LOCATIONS, IN THE EVENT THAT CONDUIT IN THESE LOCATIONS NEEDS TO BE REMOVED AND REPLACED. REMOVAL OF THE EXISTING ROADWAY LIGHTING CONDUIT, IF NEEDED, SHALL BE PAID FOR AS "ROADWAY LIGHTING CONDUIT REMOVAL". REMOVAL OF THE EXISTING TRAFFIC SIGNAL INTERCONNECT CONDUIT, IF NEEDED, IS INCLUDED WITH THE CONTRACT UNIT PRICE BID FOR "JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"x10"x10" ".
- ANY HARDWARE OR ACCESSORIES REQUIRED TO ATTACH THE NEW CONDUIT TO THE NEW JUNCTION BOXES OR TO THE EXISTING CONDUIT WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED WITH THE CONTRACT UNIT PRICE BID FOR "CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVANIZED STEEL".
- SEE TRAFFIC SIGNAL SPECIFICATIONS AND SPECIAL PROVISIONS FOR MORE INFORMATION.



DETAIL A

BILL OF MATERIAL

81100700	CONDUIT ATTACHED TO STRUCTURE, 2 1/2" DIA., GALVANIZED STEEL	FOOT	60
81300550	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 12"x12"x6"	EACH	4*
81300835	JUNCTION BOX, STAINLESS STEEL, ATTACHED TO STRUCTURE, 18"x18"x10"	EACH	2*
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2
89502200	MODIFY EXISTING CONTROLLER	EACH	2
89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	1200
X0323574	MAINTENANCE OF LIGHTING SYSTEM	CAL MO	3
X0325938	TEMPORARY WIRELESS INTERCONNECT, COMPLETE	L SUM	1
XX003079	REMOVE JUNCTION BOX	EACH	6
XX007743	ROADWAY LIGHTING CONDUIT REMOVAL	FOOT	40

* LARGE BOXES ARE FOR FIBER OPTIC INTERCONNECT CABLE;
SMALL BOXES ARE FOR ROADWAY LIGHTING CABLE

PLOT DATE = 3/25/2009
 FILE NAME = K:\11225602\Civil\1143\Sheets\Utilities\IL43.dgn
 PLOT SCALE = 1"=100'
 SHEET NAME =
 MODEL =
 Default



200 West Front Street
Wheaton, IL 60187

DESIGNED -	B.B.	REVISED -	
DRAWN -	J.T.T.	REVISED -	
CHECKED -	M.K.	REVISED -	
DATE -	1/30/09	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322

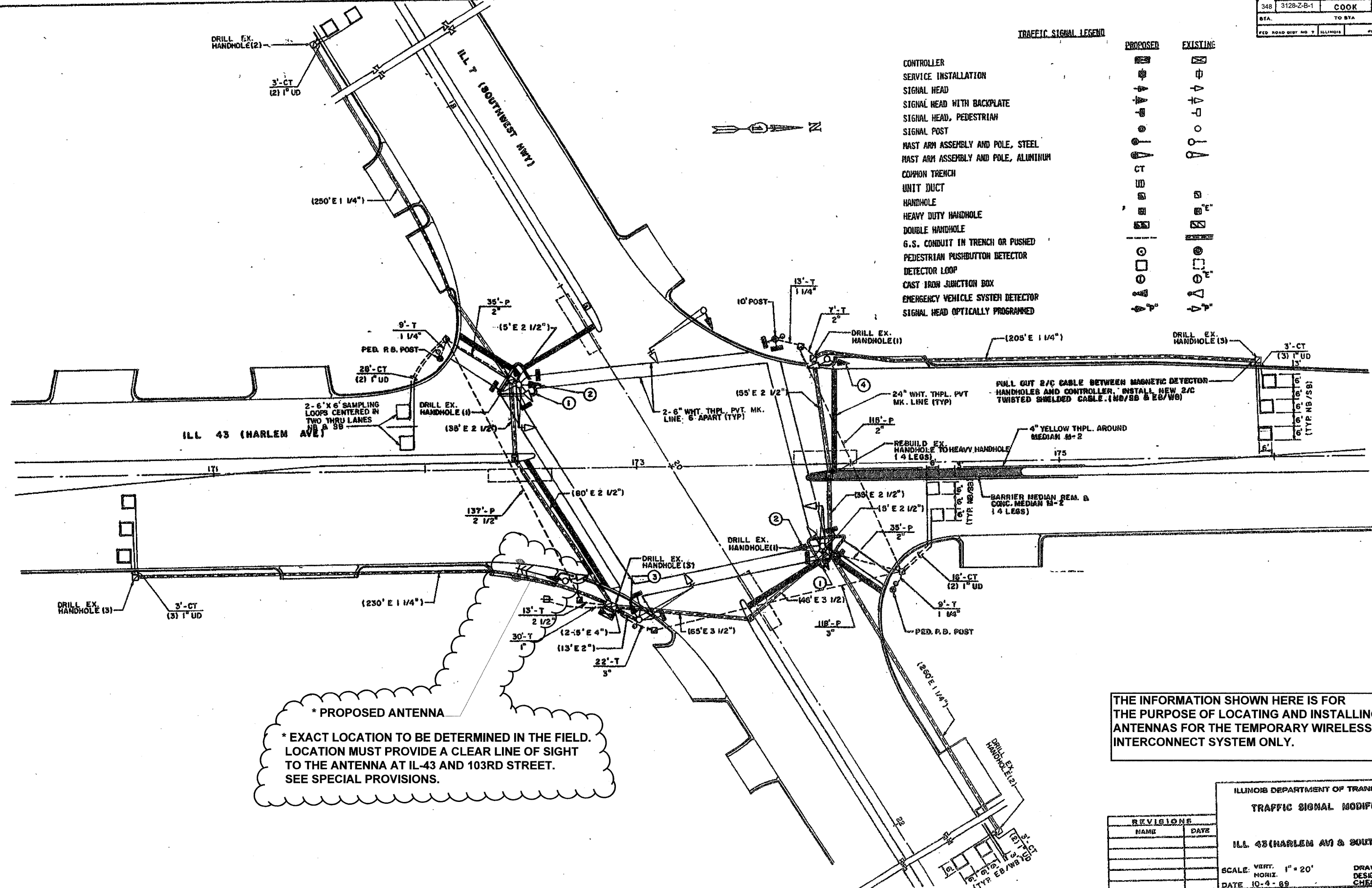
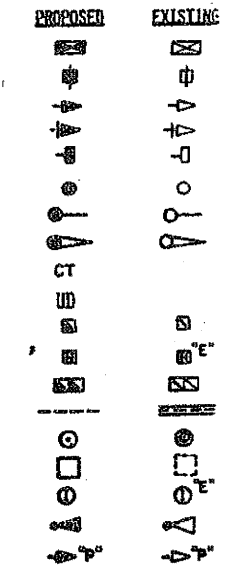
INTERCONNECT SCHEMATICS

SCALE: 1/40 SHEET NO. 8 OF 35 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	8
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D70	

TRAFFIC SIGNAL LEGEND

- CONTROLLER
- SERVICE INSTALLATION
- SIGNAL HEAD
- SIGNAL HEAD WITH BACKPLATE
- SIGNAL HEAD, PEDESTRIAN
- SIGNAL POST
- MAST ARM ASSEMBLY AND POLE, STEEL
- MAST ARM ASSEMBLY AND POLE, ALUMINUM
- COMMON TRENCH
- UNIT DUCT
- HANDHOLE
- HEAVY DUTY HANDHOLE
- DOUBLE HANDHOLE
- G.S. CONDUIT IN TRENCH OR PUSHED
- PEDESTRIAN PUSHBUTTON DETECTOR
- DETECTOR LOOP
- CAST IRON JUNCTION BOX
- EMERGENCY VEHICLE SYSTEM DETECTOR
- SIGNAL HEAD OPTICALLY PROGRAMMED



* PROPOSED ANTENNA
 * EXACT LOCATION TO BE DETERMINED IN THE FIELD.
 LOCATION MUST PROVIDE A CLEAR LINE OF SIGHT
 TO THE ANTENNA AT IL-43 AND 103RD STREET.
 SEE SPECIAL PROVISIONS.

THE INFORMATION SHOWN HERE IS FOR
 THE PURPOSE OF LOCATING AND INSTALLING
 ANTENNAS FOR THE TEMPORARY WIRELESS
 INTERCONNECT SYSTEM ONLY.

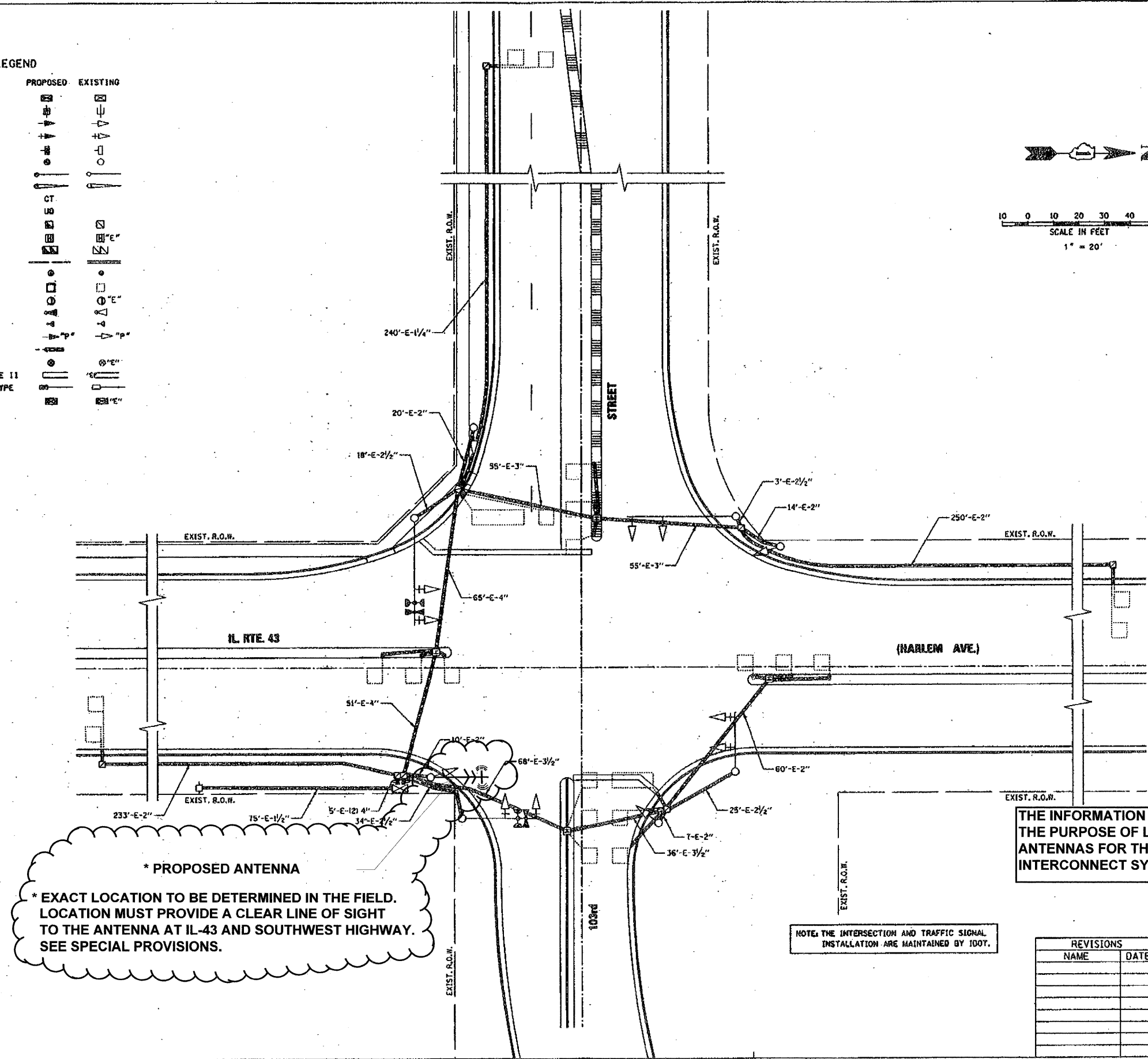
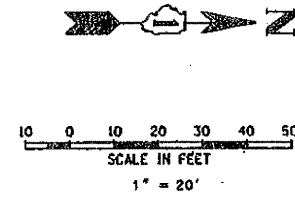
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC SIGNAL MODIFICATIONS
 ILL 43 (HARLEM AV) & SOUTHWEST HWY
 SCALE: VERT. 1" = 20'
 HORIZ. 1" = 20'
 DATE 10-4-89
 DRAWN BY PG
 DESIGNED BY RKF
 CHECKED BY DAZ

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMMON TRENCH		
UNIT DUCT		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
SIGNAL HEAD OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		



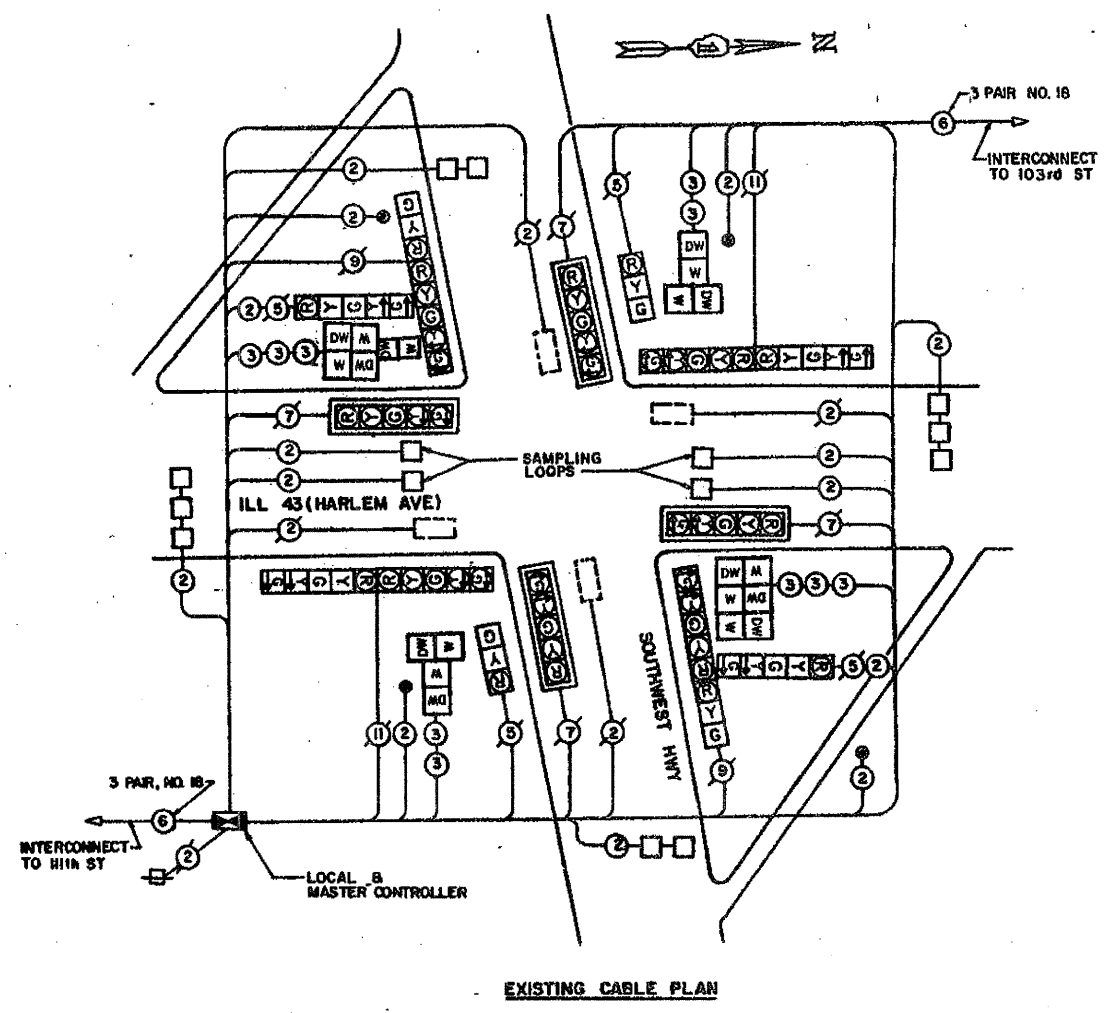
* PROPOSED ANTENNA
 * EXACT LOCATION TO BE DETERMINED IN THE FIELD. LOCATION MUST PROVIDE A CLEAR LINE OF SIGHT TO THE ANTENNA AT IL-43 AND SOUTHWEST HIGHWAY. SEE SPECIAL PROVISIONS.

THE INFORMATION SHOWN HERE IS FOR THE PURPOSE OF LOCATING AND INSTALLING ANTENNAS FOR THE TEMPORARY WIRELESS INTERCONNECT SYSTEM ONLY.

NOTE: THE INTERSECTION AND TRAFFIC SIGNAL INSTALLATION ARE MAINTAINED BY IDOT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL MODIFICATION PLAN
 IL. RTE. 43 (HARLEM AVENUE) AND 103rd STREET
 SCALE 1" = 20'
 DATE 9-25-98
 DRAWN BY FPB / FCP
 CHECKED BY ABR / GMZ



- CABLE PLAN LEGEND**
- G 8" TRAFFIC SIGNAL SECTION
 - R 12" TRAFFIC SIGNAL SECTION
 - W 12" PEDESTRIAN SIGNAL SECTION
 - C CONTROLLER CABINET
 - S SERVICE INSTALLATION
 - VEHICLE DETECTOR, INDUCTION LOOP
 - PUSHBUTTON DETECTOR
 - ② DENOTES NUMBER OF CONDUCTORS (NEW). ALL LOOP DETECTOR CABLE TO BE SHIELDED. ALL CABLE NO. 14 EXCEPT AS INDICATED.
 - ② INDICATES EXISTING CABLE
 - R SIGNAL FACE WITH BACKPLATE
 - p INDICATES PROGRAMMED
 - R EXISTING SIGNAL SECTION

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

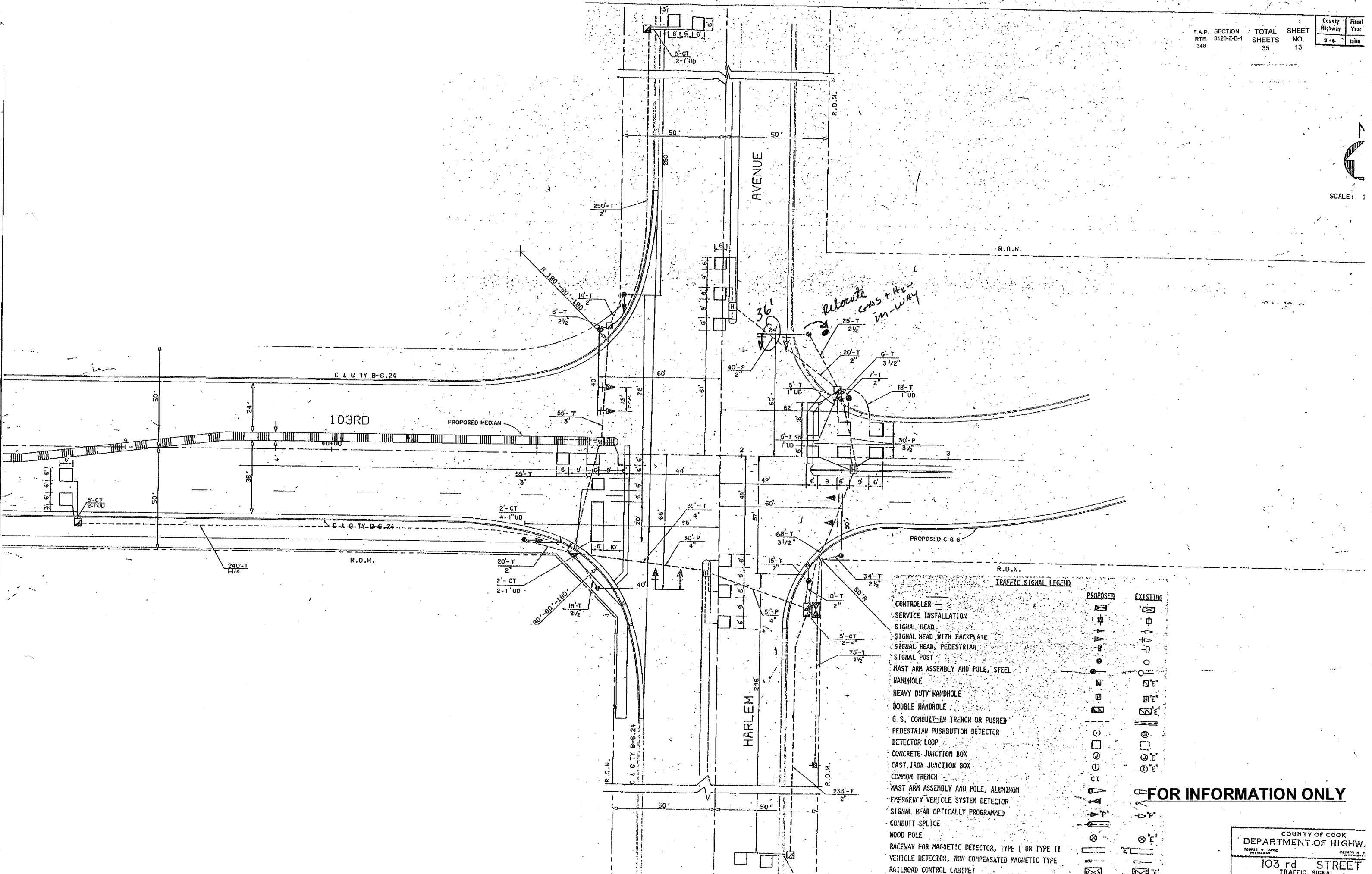
ILLINOIS DEPARTMENT OF TRANSPORTATION
EXISTING CABLE PLAN
 SCHEDULE OF QUANTITIES
 ILL. 43 (HARLEM AVE) & SOUTHWEST HWY

SCALE: VERT. NONE
 HORIZ. NONE
 DATE: 10-11-89

DRAWN BY P.D.
 DESIGNED BY R.K.F.
 CHECKED BY D.A.Z.



SCALE: 1"=40'

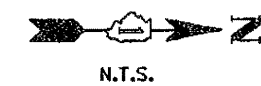


TRAFFIC SIGNAL LEGEND

—	CONTROLLER	PROPOSED	EXISTING
—	SERVICE INSTALLATION		
—	SIGNAL HEAD		
—	SIGNAL HEAD WITH BACKPLATE		
—	SIGNAL HEAD, PEDESTRIAN		
—	SIGNAL POST		
—	MAST ARM ASSEMBLY AND POLE, STEEL		
—	HANDHOLE		
—	HEAVY DUTY HANDHOLE		
—	DOUBLE HANDHOLE		
—	G.S. CONDUIT-IN TRENCH OR PUSHED		
—	PEDESTRIAN PUSHBUTTON DETECTOR		
—	DETECTOR LOOP		
—	CONCRETE JUNCTION BOX		
—	CAST IRON JUNCTION BOX		
—	COMMON TRENCH		
—	MAST ARM ASSEMBLY AND POLE, ALUMINUM		
—	EMERGENCY VEHICLE SYSTEM DETECTOR		
—	SIGNAL HEAD OPTICALLY PROGRAMMED		
—	CONDUIT SPLICE		
—	WOOD POLE		
—	RACEMAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
—	VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
—	RAILROAD CONTRL CABINET		

FOR INFORMATION ONLY

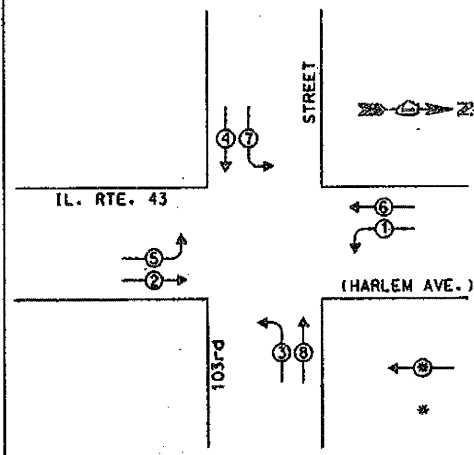
F.A.P. RTE. 348	SECTION 3128-2-B-1	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 14
STA. _____		TO STA. _____		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT _____		



CABLE PLAN LEGEND

- | | | |
|-----------------|-----------------|---|
| EXISTING | PROPOSED | |
| | | 8" TRAFFIC SIGNAL SECTION |
| | | 12" TRAFFIC SIGNAL SECTION |
| | | 12" PEDESTRIAN SIGNAL SECTION |
| | | 12" PEDESTRIAN SIGNAL SECTION |
| | | CONTROLLER CABINET |
| | | SERVICE INSTALLATION |
| | | VEHICLE DETECTOR, INDUCTION LOOP |
| | | MAGNETIC DETECTOR |
| | | EMERGENCY VEHICLE LIGHT DETECTOR |
| | | CONFIRMATION BEACON |
| | | PUSHBUTTON DETECTOR |
| | | 2 DENOTES NUMBER OF CONDUCTORS. ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| | | SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD. |
| | | NO. 62.5/125 12F FIBER OPTIC CABLE |

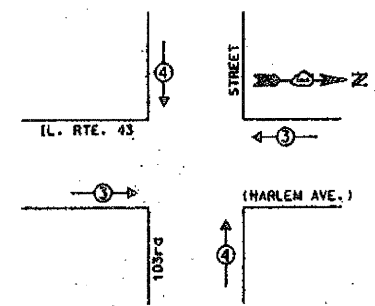
EXISTING CONTROLLER SEQUENCE



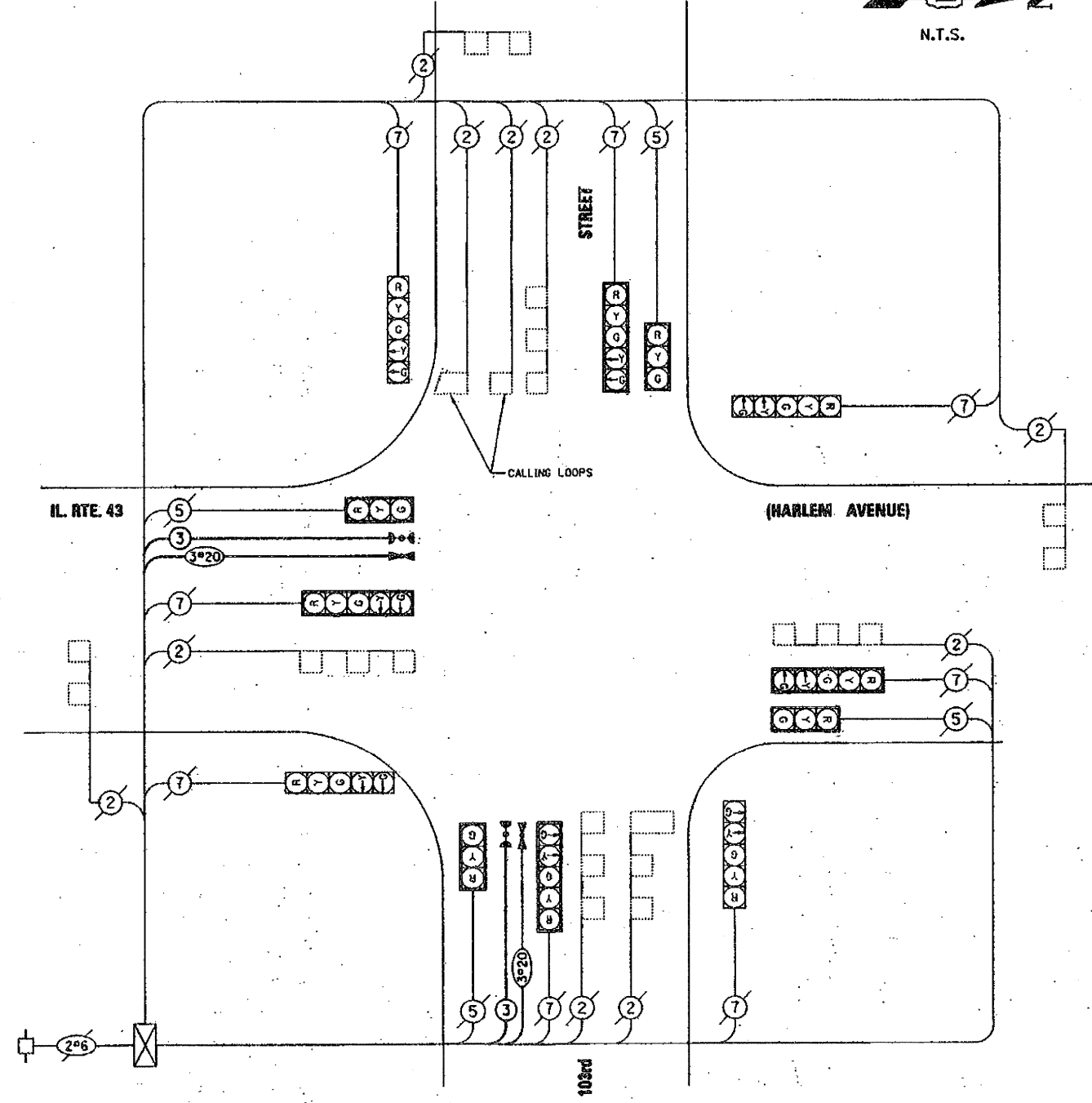
LEGEND
 DUAL ENTRY PHASE
 * NUMBER REFERS TO ASSOCIATED PHASE

EXISTING PHASE DESIGNATION DIAGRAM

EMERGENCY VEHICLE PREEMPTION SEQUENCE



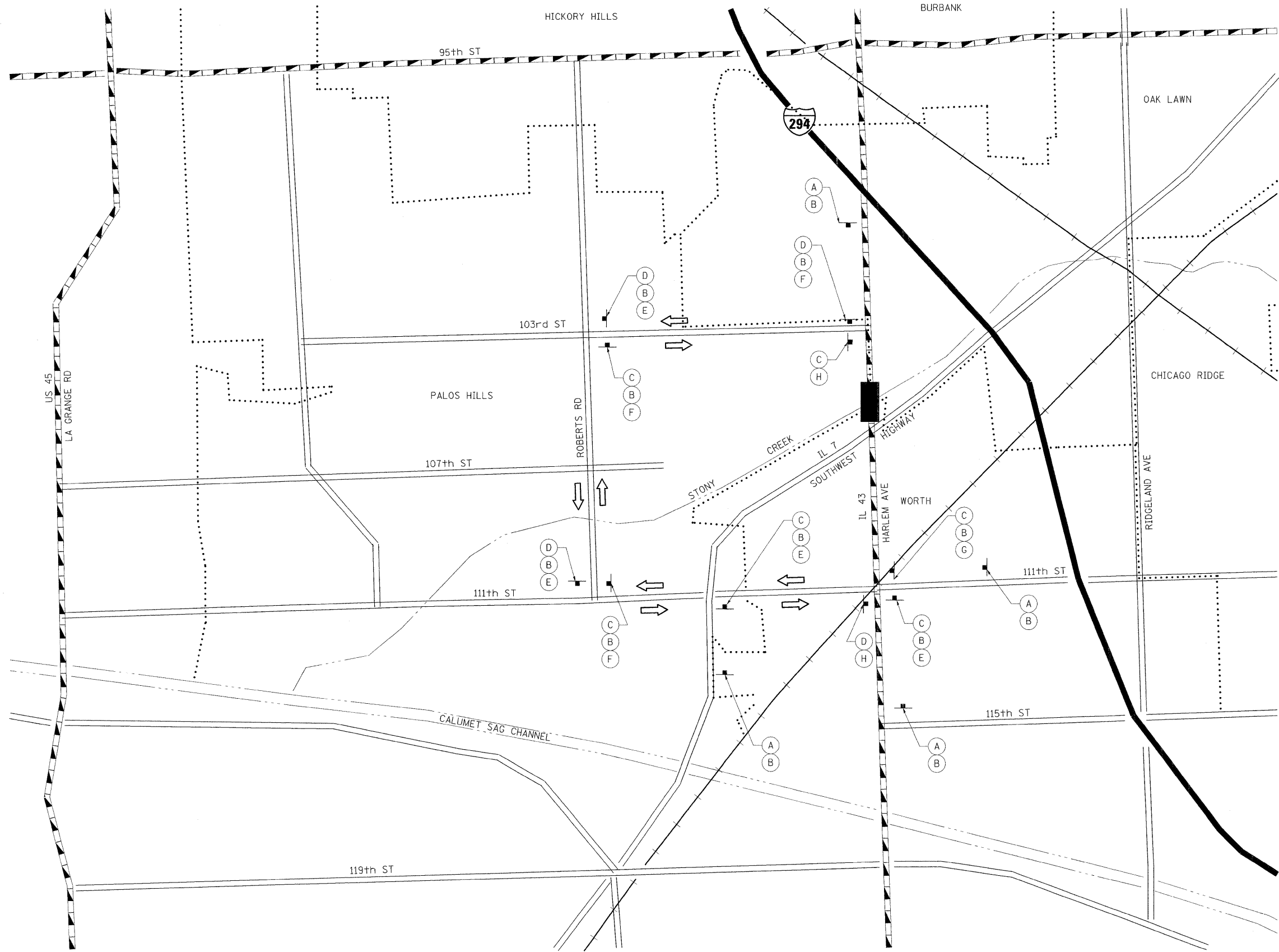
PROPOSED EMERGENCY VEHICLE PREEMPTOR		
EMERGENCY VEHICLE PREEMPTOR	3	4
MOVEMENT		



FOR INFORMATION ONLY

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION PLAN AND EMERGENCY VEHICLE PREEMPTION SEQUENCE IL. RTE. 43 (HARLEM AVENUE) AND 103rd STREET
NAME	DATE	

SCALE N.T.S. DRAWN BY FFB / FCP
 DATE 9-25-98 CHECKED BY ABR / GMZ



- (A) ROAD CONSTRUCTION AHEAD
- (B) ALTERNATE ROUTE
- (C) ILLINOIS 43 NORTH
- (D) ILLINOIS 43 SOUTH
- (E) Left turn arrow
- (F) Right turn arrow
- (G) Up arrow
- (H) ALTERNATE ROUTE ENDS

- LEGEND**
- INTERSTATE
 - CITY STREET
 - STATE HIGHWAY
 - RAILROAD
 - WORK ZONE
 - BOUNDARY

NOTE: ALL TRAFFIC CONTROL ITEMS PRESENTED ON THIS PLAN WILL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION (SPECIAL)

PLOT DATE = 2/12/2009
 PLOT SCALE = 1"=100'
 USER NAME = JTF
 MODEL = Default



200 West Front Street
Wheaton, IL 60187

DESIGNED - B.B.	REVISED -
DRAWN - J.T.T.	REVISED -
CHECKED - M.K.	REVISED -
DATE - 1/30/09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL-43 OVER STONY CREEK
STRUCTURE NO. 016-0322**

ALTERNATE ROUTE

SCALE: 1"=100' SHEET NO. 15 OF 35 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	15
CONTRACT NO. 60D70				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

The existing structure was built in 1960 and widened in 1977. It is a three span continuous reinforced concrete slab structure. The substructure consists of open stub abutments and solid shaft piers. The rehabilitation work will be done utilizing stage construction.

The proposed improvements consist of removal of the existing HMA wearing surface, hydroscarifying the deck, deck slab repairs, replacing the existing expansion joints, removing the longitudinal joint, replacing the existing expansion bearings at the abutments, reconstructing the parapets, construction of a latex concrete overlay, and repair of the substructure.

The existing aluminum railing is not to be salvaged.

GENERAL NOTES

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

Plan dimensions and details relative to existing plans are subject to routine variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished based upon the unit price bid for the work.

These plans have been prepared from notes received from the Illinois Department of Transportation field maintenance Engineers.

Protective Coat shall not be applied over Latex Concrete Overlay.

INDEX OF SHEETS

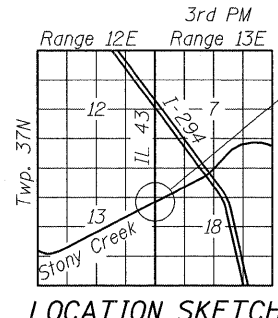
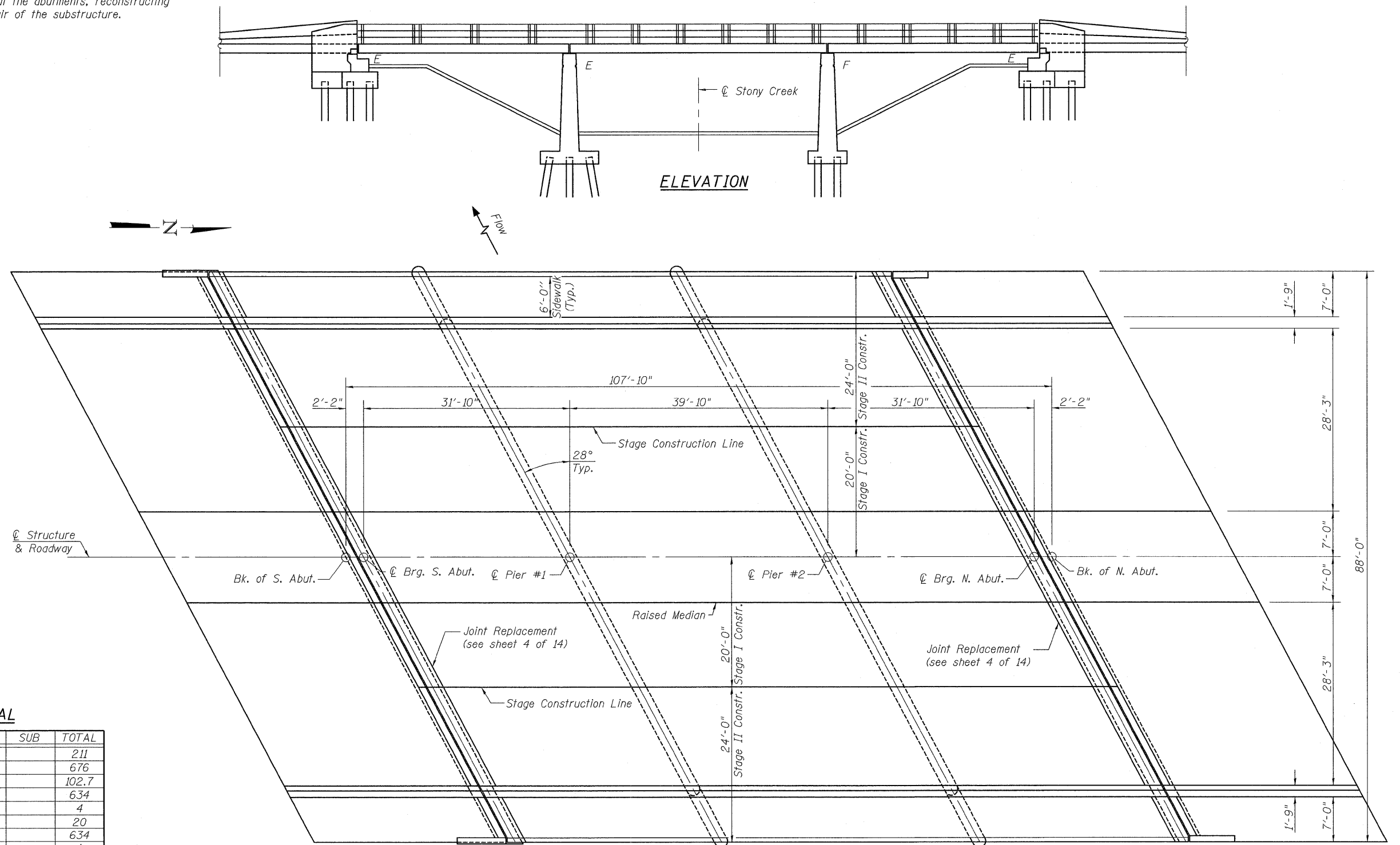
1. General Plan and Elevation
2. Typical Cross Sections with Staging
3. Expansion Joint Removal at Deck
4. Expansion Joint Rehabilitation at Deck
5. Expansion Joint Rehabilitation at Parapet and Wingwall
6. Longitudinal Joint Details
7. Parapet Rehabilitation
8. Preformed Joint Strip Seal
9. Bar Splicer Assembly Details
10. Bearing Replacement Details
11. Deck Repair Plan
12. Abutment Repairs
13. Pier Repairs - I
14. Pier Repairs - II

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Rail Removal	Foot	211		211
Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	676		676
Concrete Removal	Cu. Yd.	102.7		102.7
Bridge Deck Hydro-Scarification, 3/4"	Sq. Yd.	634		634
Deck Slab Repair (Partial)	Sq. Yd.	4		4
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	20		20
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	634		634
Temporary Shoring and Cribbing	L. Sum	1		1
Elastomeric Bearing Assembly, Type I	Each	48		48
Preformed Joint Strip Seal	Foot	200		200
Concrete Superstructure	Cu. Yd.	117.1		117.1
Protective Coat	Sq. Yd.	336		336
Bridge Deck Grooving	Sq. Yd.	612		612
Reinforcement Bars, Epoxy Coated	Pound	16,070		16,070
Bar Splicers	Each	48		48
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.		22	22
Structural Repair of Concrete (Depth greater than 5")	Sq. Ft.		3	3
Cleaning and Painting Structural Steel, Location 1	L. Sum	1		1

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 1
FAP 348	3128-Z-B-1	Cook	35	16	14 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #60D70



DESIGN STRESSES
FIELD UNITS
 f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)

LOADING HS 20-44
DESIGN SPECIFICATIONS
 2002 AASHTO Standard Specifications
 for Highway Bridges

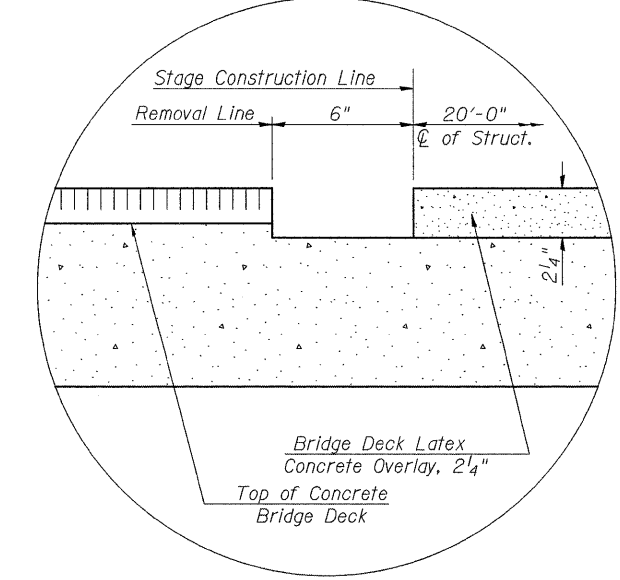
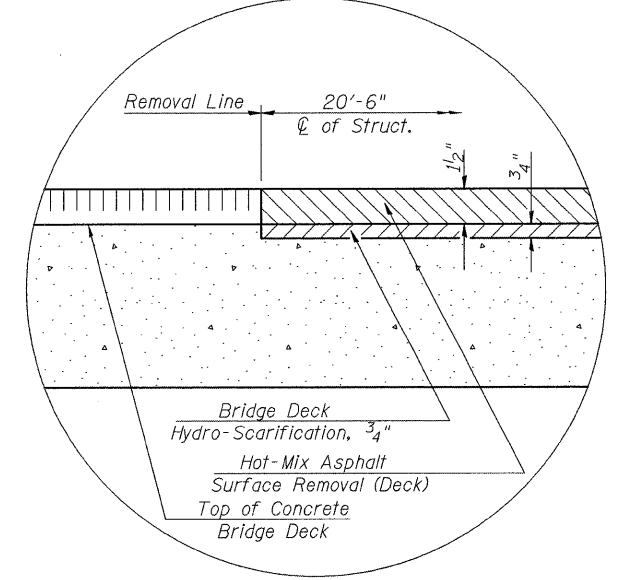
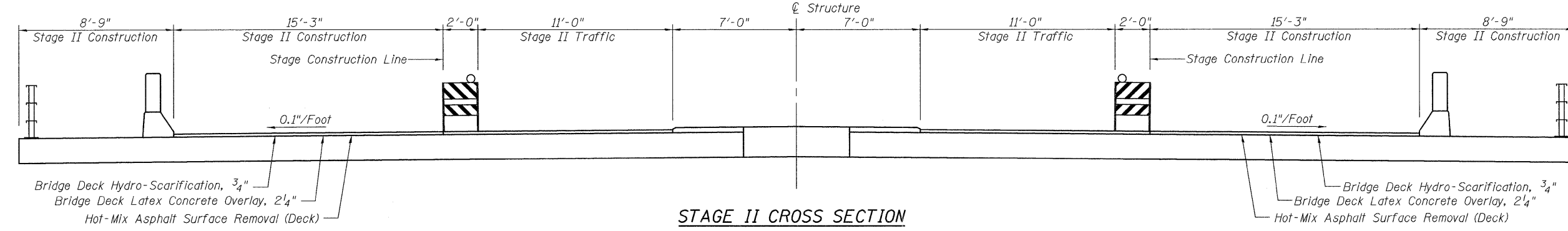
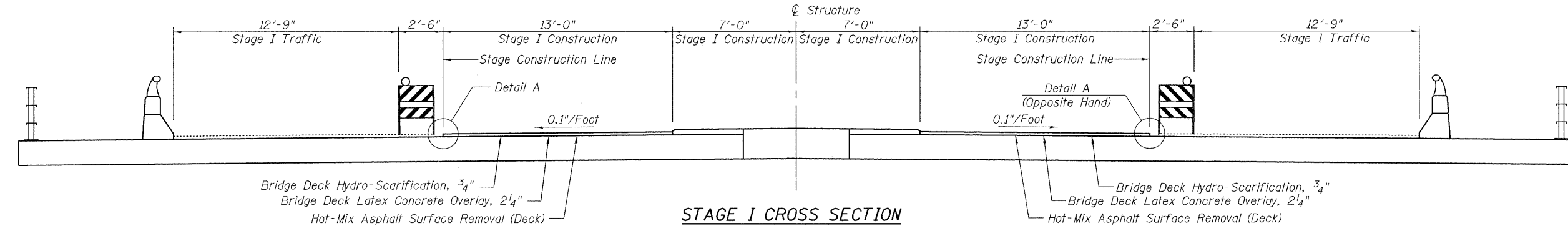
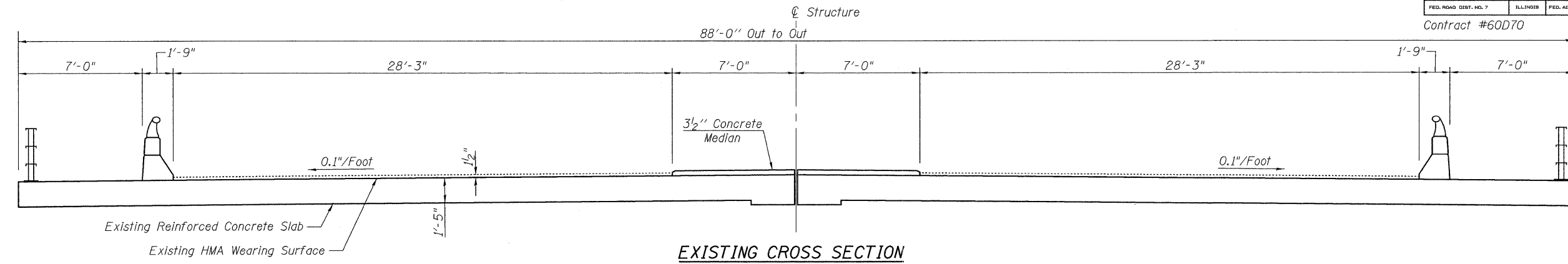
PLAN



rjngroup
 Excellence through Ownership
 200 West Front Street
 Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION
 GENERAL PLAN AND ELEVATION
 IL 43 OVER STONY CREEK
 FAP RTE 348 SECTION 3128-Z-B-1
 COOK COUNTY
 STATION 179+01.40
 STRUCTURE NO. 016-0322
 DATE: 1-30-2009
 DRAWN BY WJV
 CHECKED BY BLB

3/17/2009 K:\11225502\Structures\IL 43 over Stony Creek\Final Plans\GPE.dgn



<p>Excellence through Ownership</p> <p>200 West Front Street Wheaton, IL 60187</p>	<p>ILLINOIS DEPARTMENT OF TRANSPORTATION TYPICAL CROSS SECTIONS WITH STAGING IL 43 OVER STONY CREEK FAP RTE 348 SECTION 3128-Z-B-1 COOK COUNTY STATION 179+01.40 STRUCTURE NO. 016-0322</p>
	<p>DATE: 1-30-2009</p> <p>DRAWN BY WJV CHECKED BY BLB</p>

2/12/2009 ks11225592\structures\11_43 over stony creek\final plans\09E.dgn

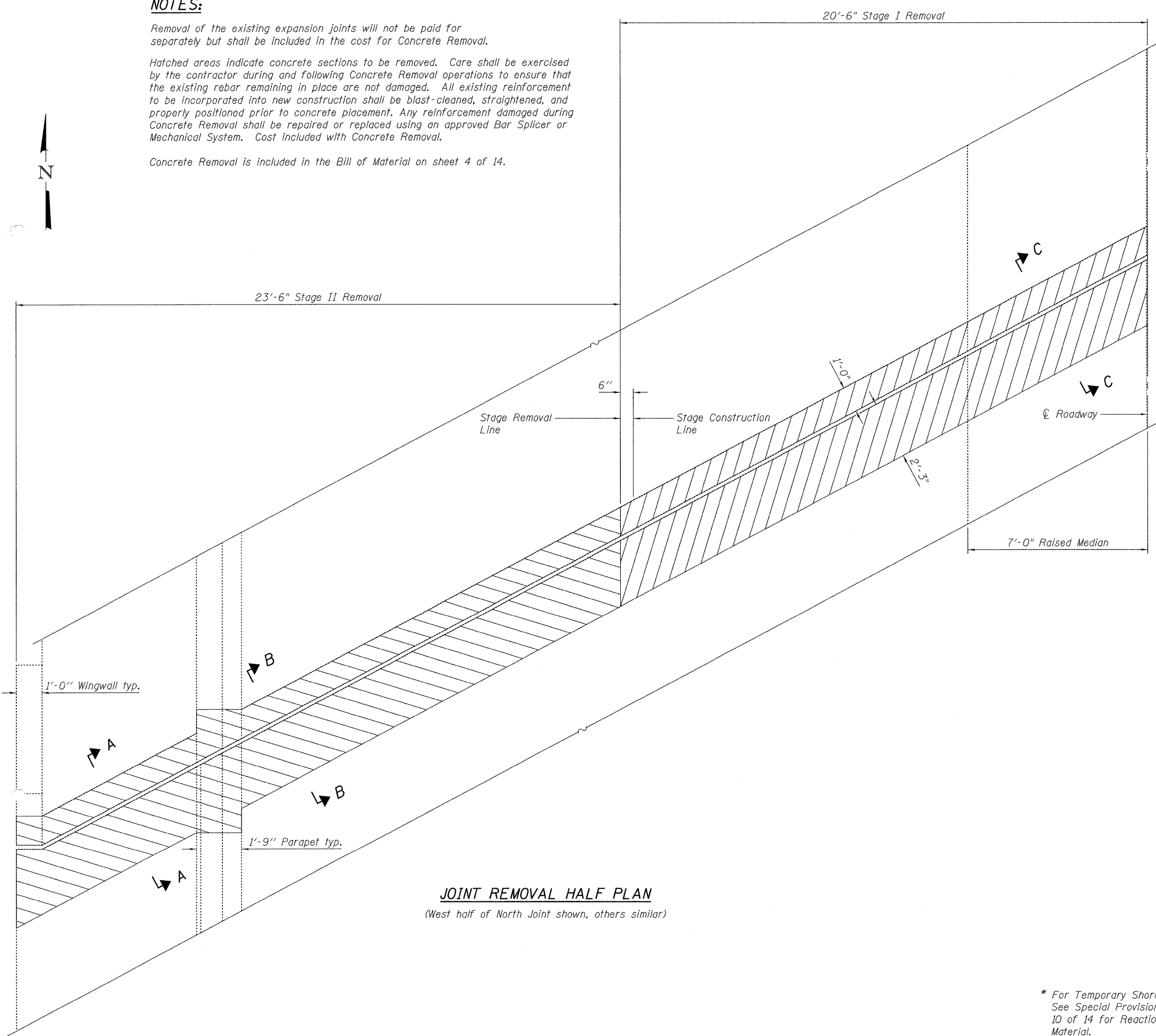
Contract #60D70

NOTES:

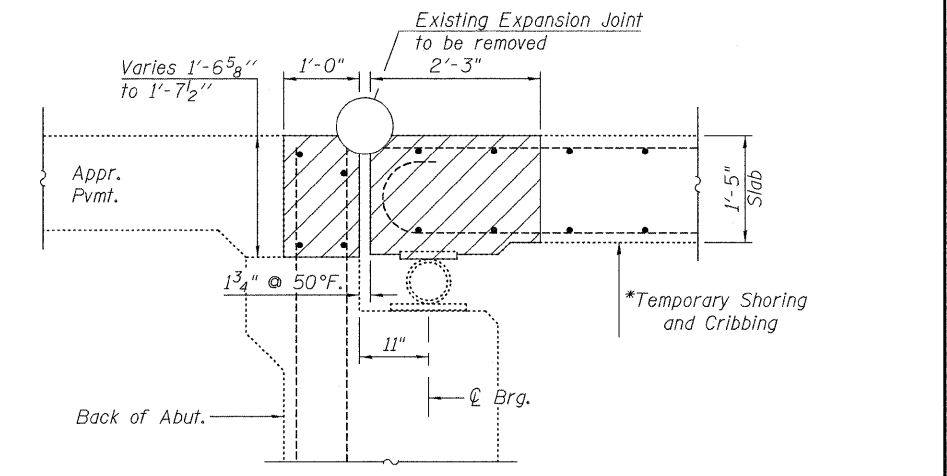
Removal of the existing expansion joints will not be paid for separately but shall be included in the cost for Concrete Removal.

Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following Concrete Removal operations to ensure that the existing rebar remaining in place are not damaged. All existing reinforcement to be incorporated into new construction shall be blast-cleaned, straightened, and properly positioned prior to concrete placement. Any reinforcement damaged during Concrete Removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal.

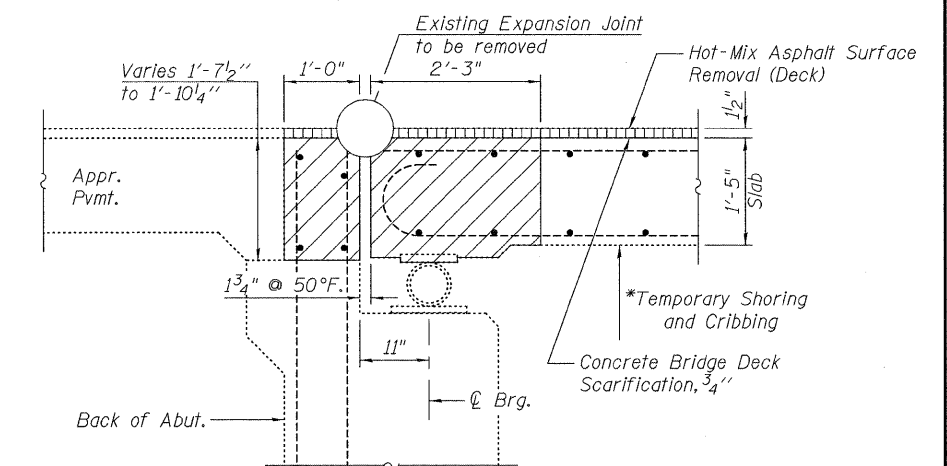
Concrete Removal is included in the Bill of Material on sheet 4 of 14.



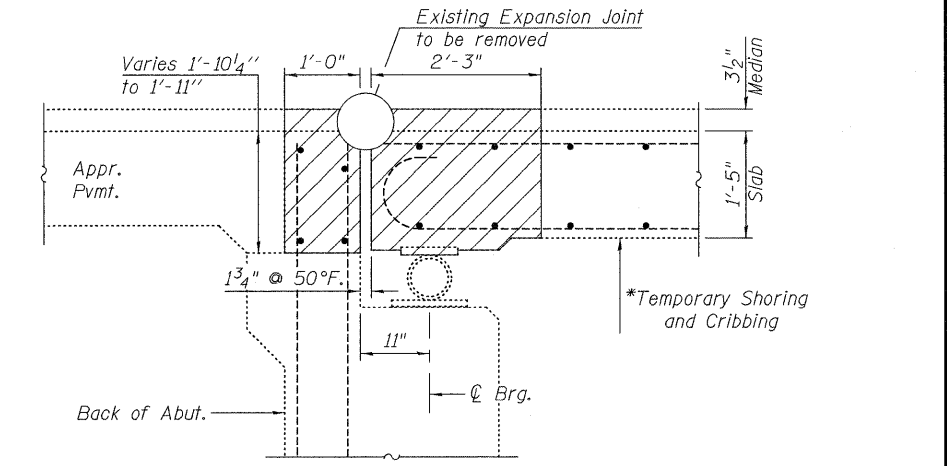
JOINT REMOVAL HALF PLAN
(West half of North Joint shown, others similar)



SECTION A-A



SECTION B-B



SECTION C-C

* For Temporary Shoring and Cribbing
See Special Provision and also see sheet
10 of 14 for Reaction Table and Bill of
Material.



200 West Front Street
Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT REMOVAL
AT DECK
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322

DATE: 1-30-2009

DRAWN BY LM
CHECKED BY BLB

2/12/2009 ka11225582\structures\11_43 over stony creek\final plans\jointremoval.dgn

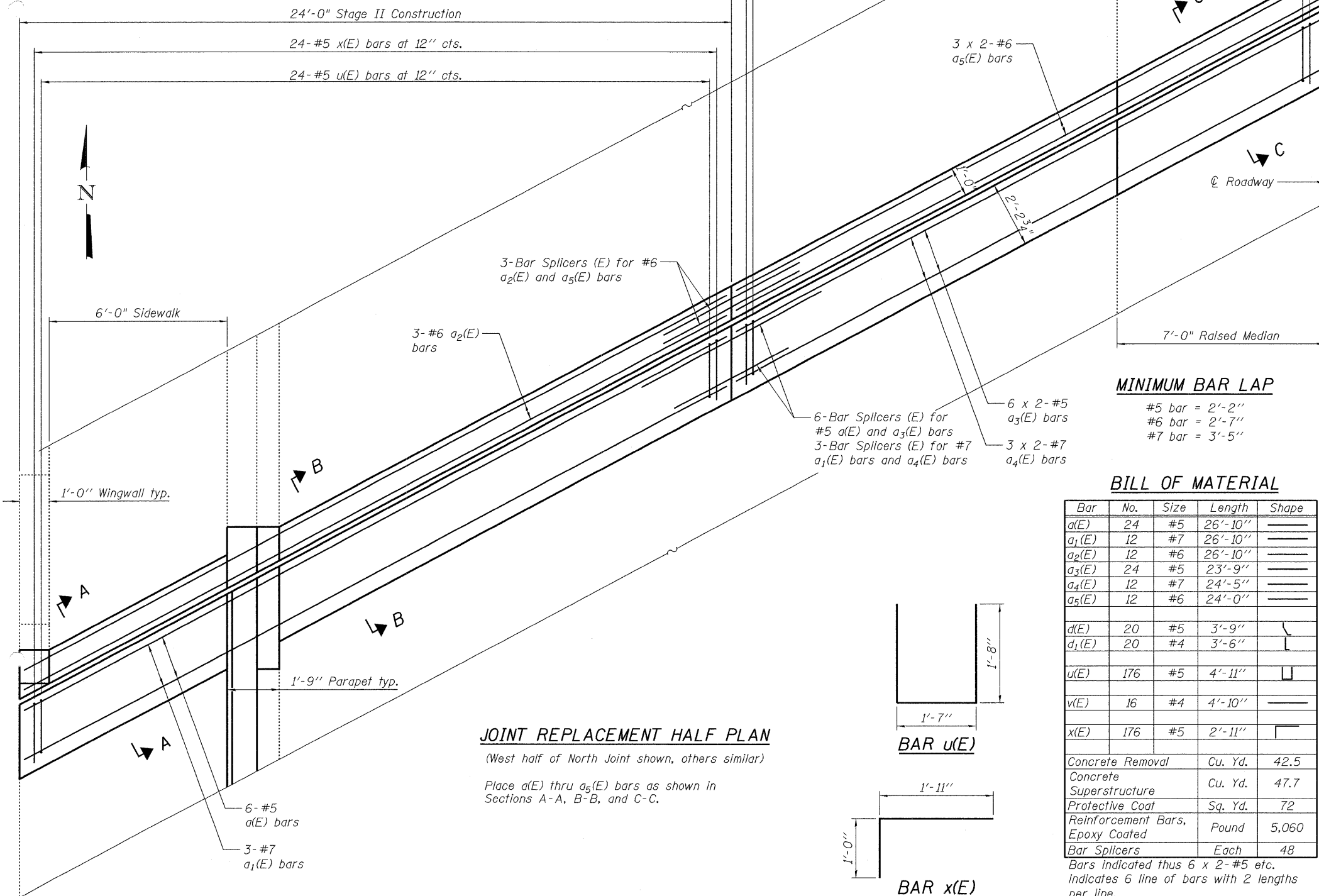
NOTES:

Interference is expected from the existing conduit and/or junction boxes at all four corners of the structure. The Traffic Signal Interconnect attached to the west side of the bridge requires special attention, and the Contractor shall refer to Traffic Signal Schematics and Special Provisions. For other conduits, refer to the Special Provision for Roadway Lighting Modifications.

Existing Name Plate at the NW corner shall be protected from damage during construction and incorporated into the new construction. Cost included with Concrete Superstructure.

Interference is expected from the existing bridge rail post. The bridge rail post shall be temporarily supported during rehabilitation. The Post Anchorage Assembly embedded in the Concrete shall be cleaned and incorporated into the new construction. Care shall be taken not to damage the Post Anchorage Assembly. The NW and SE post anchorage assembly may interfere with the joint and may need to be cut in the field. All labor and equipment necessary to clean, cut and incorporate the existing bridge rail post anchorage assembly into the new construction shall be included in the cost for Concrete Superstructure.

See Sheet 10 of 14 for Bearing Details.



JOINT REPLACEMENT HALF PLAN

(West half of North Joint shown, others similar)

Place a(E) thru a₅(E) bars as shown in Sections A-A, B-B, and C-C.

BAR u(E)

BAR x(E)

MINIMUM BAR LAP

- #5 bar = 2'-2"
- #6 bar = 2'-7"
- #7 bar = 3'-5"

BILL OF MATERIAL

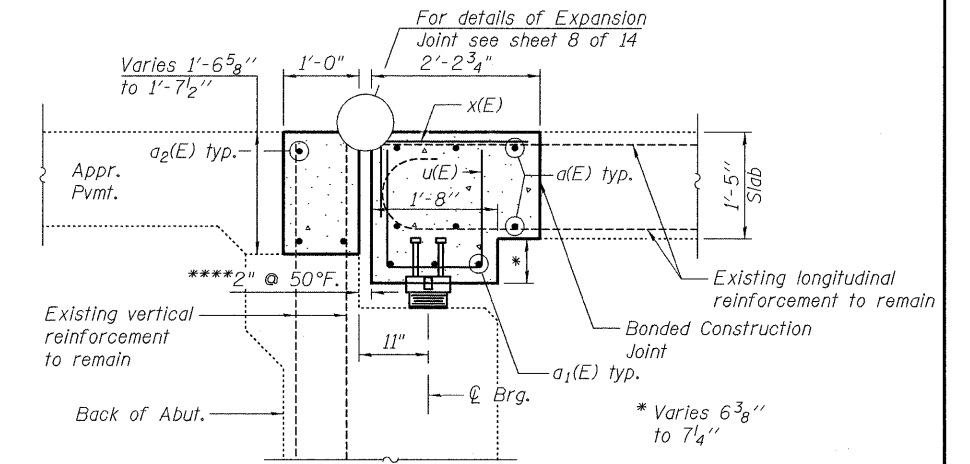
Bar	No.	Size	Length	Shape
a(E)	24	#5	26'-10"	—
a ₁ (E)	12	#7	26'-10"	—
a ₂ (E)	12	#6	26'-10"	—
a ₃ (E)	24	#5	23'-9"	—
a ₄ (E)	12	#7	24'-5"	—
a ₅ (E)	12	#6	24'-0"	—
d(E)	20	#5	3'-9"	—
d ₁ (E)	20	#4	3'-6"	—
u(E)	176	#5	4'-11"	—
v(E)	16	#4	4'-10"	—
x(E)	176	#5	2'-11"	—

Concrete Removal	Cu. Yd.	42.5
Concrete Superstructure	Cu. Yd.	47.7
Protective Coat	Sq. Yd.	72
Reinforcement Bars, Epoxy Coated	Pound	5,060
Bar Splicers	Each	48

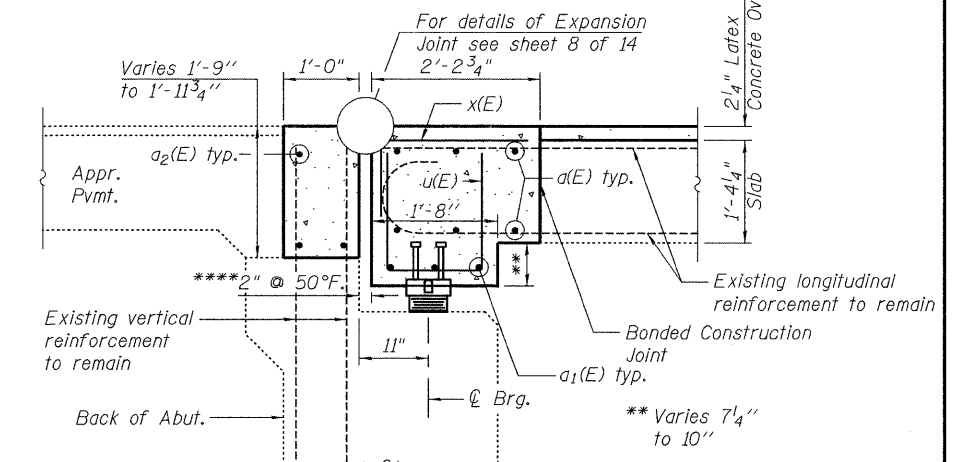
Bars indicated thus 6 x 2-#5 etc. Indicates 6 line of bars with 2 lengths per line.

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.	SHEET NO.
FAP 348	3128-Z-B-1	Cook	35	19	14 SHEETS

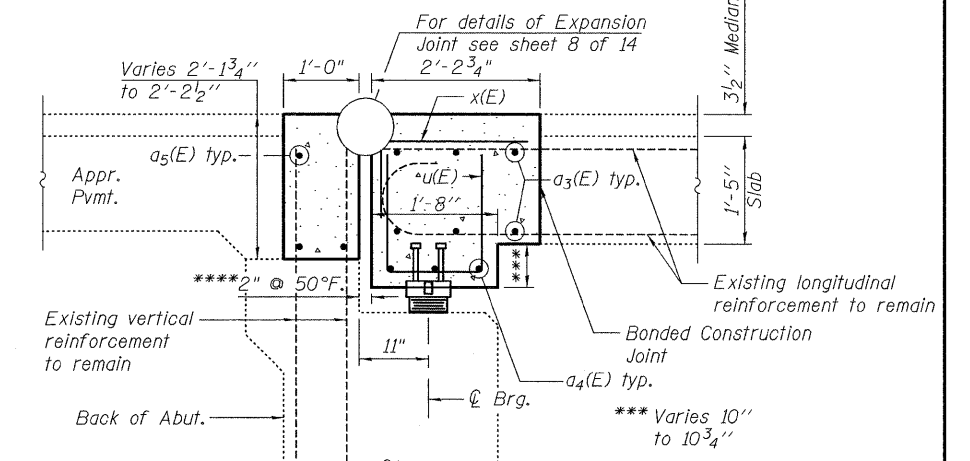
Contract #60D70



SECTION A-A



SECTION B-B



SECTION C-C

****See sheet 8 of 14 for Notes.



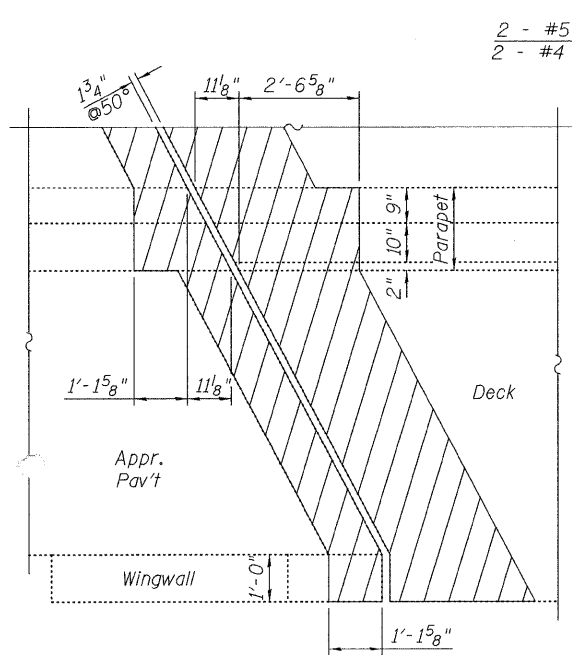
200 West Front Street
Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT REHABILITATION
AT DECK
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322

DATE: 1-30-2009

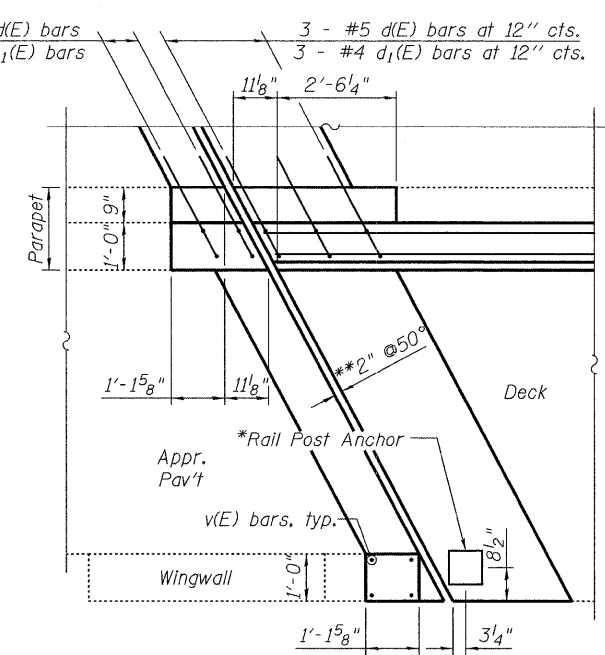
DRAWN BY LM
CHECKED BY BLB

ks:\1225582\structures\11_43 over stony creek\final\plans\joint.dgn 2/12/2009



PLAN - CONCRETE REMOVAL

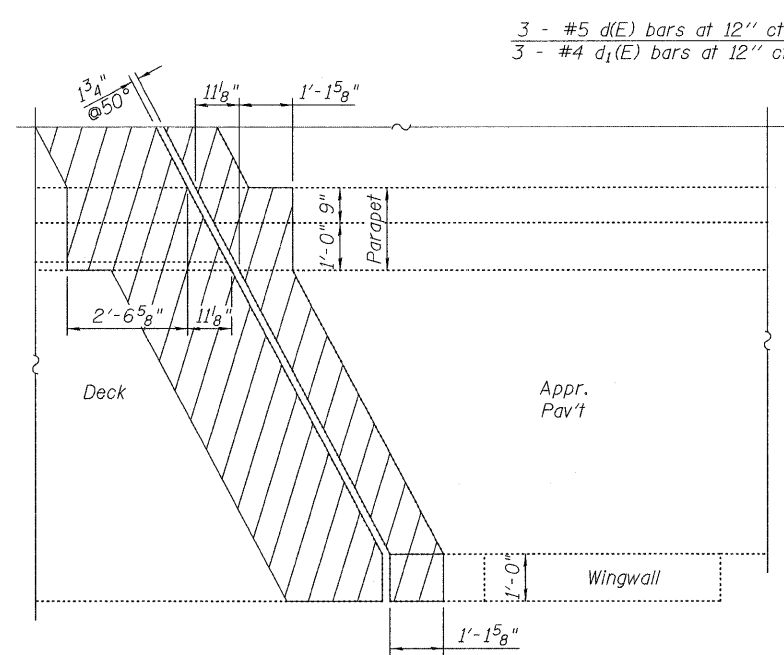
Typ. at West end of North Abut. and East end of South Abut.



PLAN - CONCRETE REPLACEMENT

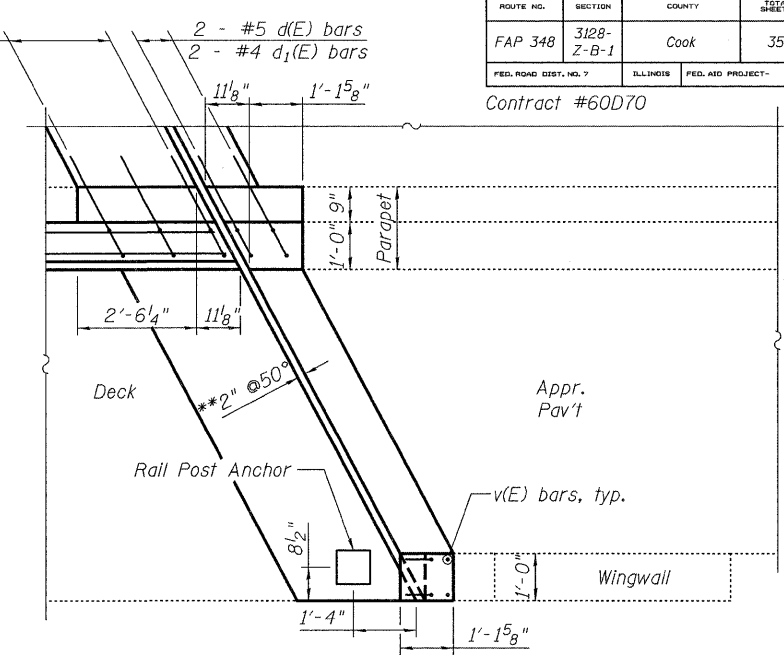
Typ. at West end of North Abut. and East end of South Abut.

*Interferes with joint, post anchorage assembly may need to be cut in the field, 2 Bolt maximum. See Sheet 4 of 14 for note.



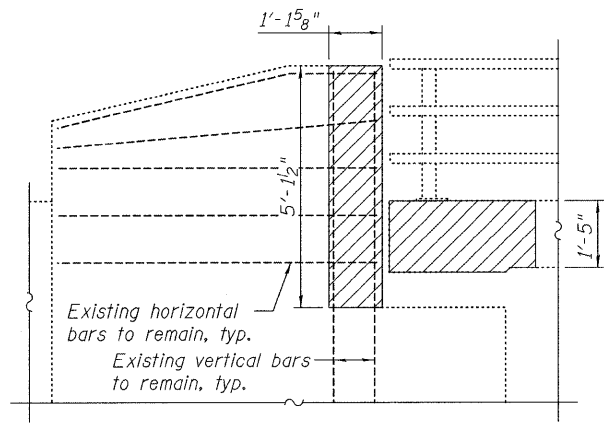
PLAN - CONCRETE REMOVAL

Typ. at East end of North Abut. and West end of South Abut.



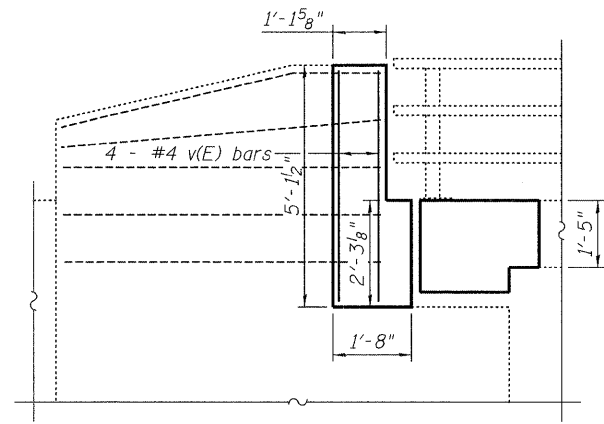
PLAN - CONCRETE REPLACEMENT

Typ. at East end of North Abut. and West end of South Abut.



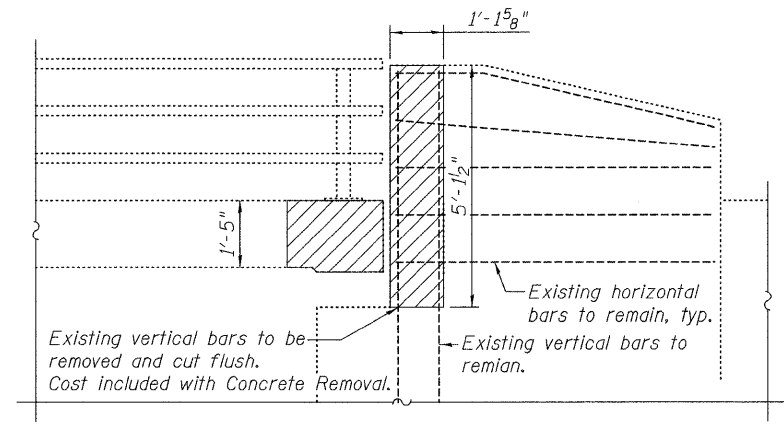
**WINGWALL ELEVATION
CONCRETE REMOVAL**

Typ. at Northwest and Southeast walls



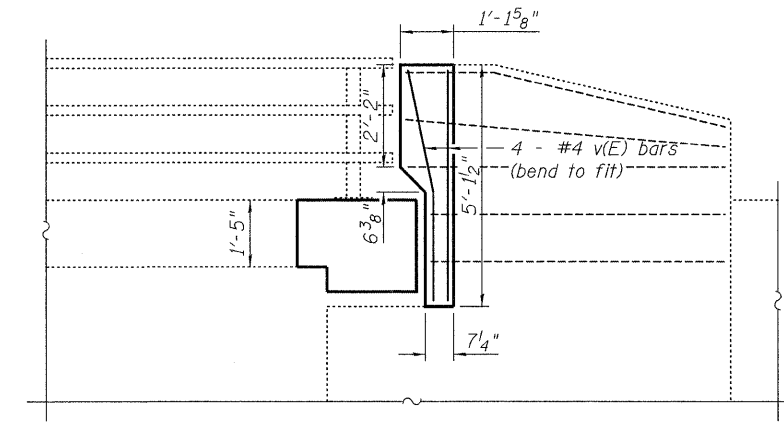
**WINGWALL ELEVATION
CONCRETE REPLACEMENT**

Typ. at Northwest and Southeast walls



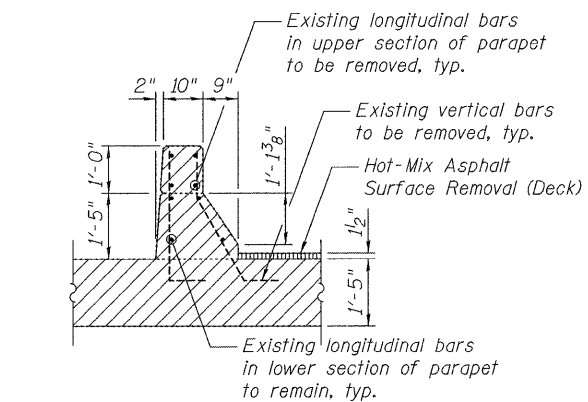
**WINGWALL ELEVATION
CONCRETE REMOVAL**

Typ. at Northeast and Southwest walls

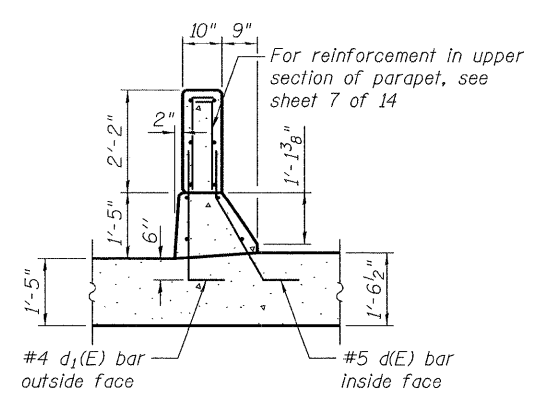


**WINGWALL ELEVATION
CONCRETE REPLACEMENT**

Typ. at Northeast and Southwest walls



**SECTION THRU PARAPET
CONCRETE REMOVAL**



**SECTION THRU PARAPET
CONCRETE REPLACEMENT**

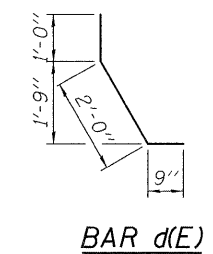
NOTES:

Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following Concrete Removal operations to ensure that the existing rebar remaining in place are not damaged. All existing reinforcement to be incorporated into new construction shall be blast-cleaned, straightened, and properly positioned prior to concrete placement. Any reinforcement damaged during Concrete Removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal.

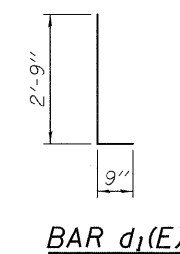
Concrete Removal and Concrete Superstructure for the parapet are included in the Bill of Material on sheet 7 of 14. Bars d(E) and d1(E) for the parapet are included in the Bill of Material on sheet 4 of 14.

For simplicity, only proposed reinforcement critical to expansion joint rehabilitation is shown.

For Bill of Material, see sheet 4 of 14.



BAR d(E)

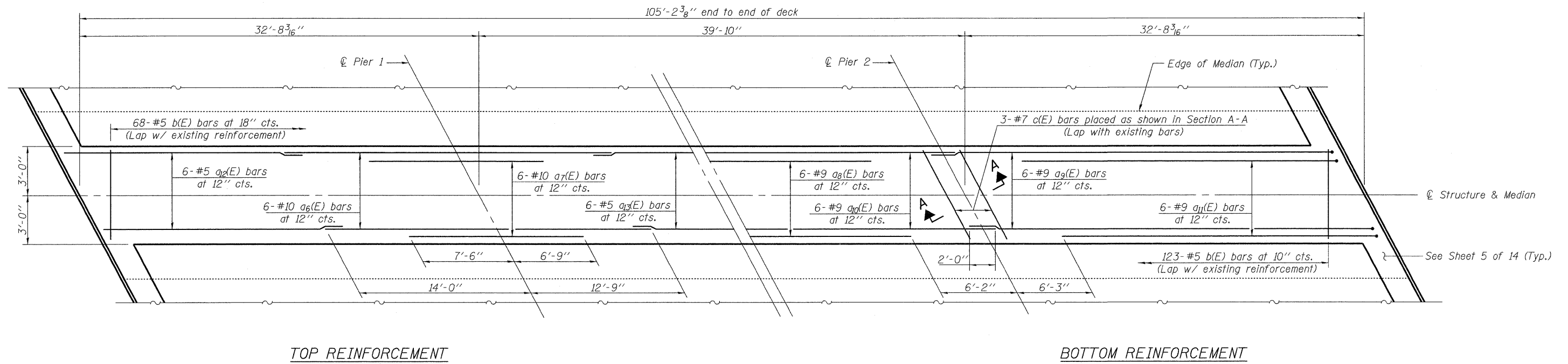


BAR d1(E)

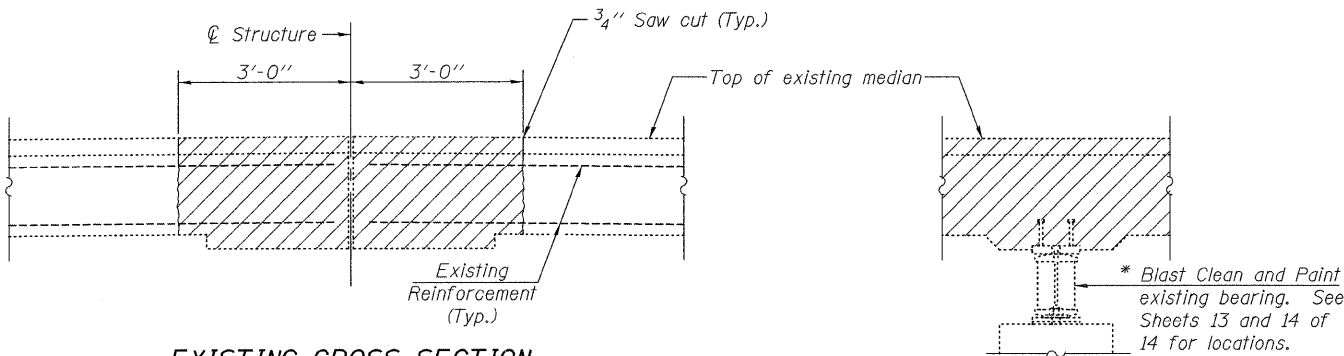
**See sheet 8 of 14 for Notes.

<p>Excellence through Ownership</p> <p>200 West Front Street Wheaton, IL 60187</p>	<p>ILLINOIS DEPARTMENT OF TRANSPORTATION EXPANSION JOINT REHABILITATION AT PARAPET AND WINGWALL IL 43 OVER STONY CREEK FAP RTE 348 SECTION 3128-Z-B-1 COOK COUNTY STATION 179+01.40 STRUCTURE NO. 016-0322</p>
	<p>DATE: 1-30-2009</p> <p>DRAWN BY: LM CHECKED BY: BLB</p>

2/12/2009 ka\122552\structures\11_43 over stony creek\final\plans\jmb\tds\gn

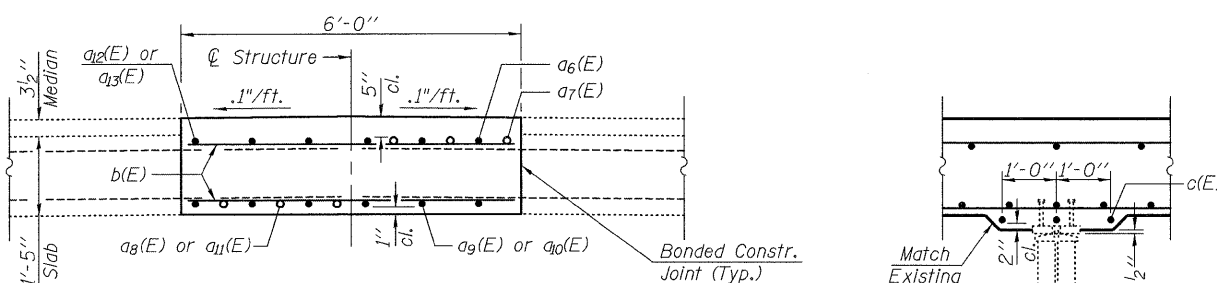


LONGITUDINAL JOINT CLOSURE PLAN



EXISTING CROSS SECTION

SECTION A-A REMOVAL



AT MIDSPAN NEAR PIERS

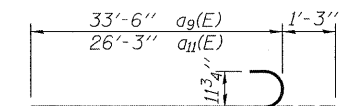
PROPOSED CROSS SECTION

SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a ₆ (E)	12	#10	26'-9"	—
a ₇ (E)	12	#10	14'-3"	—
a ₈ (E)	6	#9	27'-6"	—
a ₉ (E)	12	#9	34'-9"	—
a ₁₀ (E)	6	#9	41'-9"	—
a ₁₁ (E)	12	#9	27'-6"	—
a ₁₂ (E)	12	#5	20'-0"	—
a ₁₃ (E)	6	#5	17'-0"	—
b(E)	191	#5	5'-9"	—
c(E)	6	#7	6'-6"	—
Concrete Removal			Cu. Yd.	37.5
Concrete Superstructure			Cu. Yd.	38.3
Protective Coat			Sq. Yd.	67
Reinforcement Bars, Epoxy Coated			Pound	7660
Cleaning and Painting Structural Steel, Location 1			L. Sum	1

* Cost of Blast Cleaning and Painting of Bearings shall be included with Cleaning and Painting Structural Steel, Location 1. See Special Provision for Cleaning and Painting Existing Steel Structures. The bearing shall be cleaned per New White Blast Cleaning-SSPC-SPI0 and shall be painted according to the requirements of paint system 1-OZ/E/U. The color of the final finish coat shall be Gray, Munsell No. 5B 7/1.



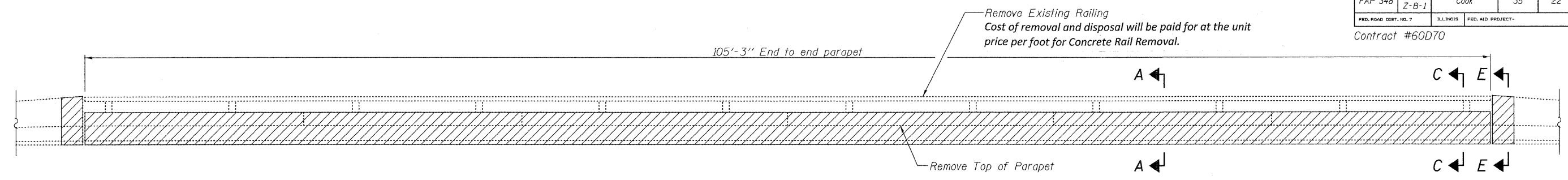
a₉(E) & a₁₁(E) BARS

NOTES

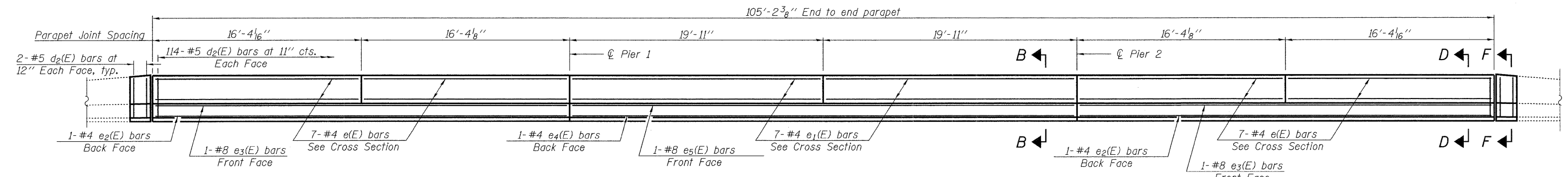
Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following Concrete Removal operations to ensure that the existing rebar remaining in place are not damaged. All existing reinforcement to be incorporated into new construction shall be blast-cleaned, straightened, and properly positioned prior to concrete placement. Any reinforcement damaged during Concrete Removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal. Concrete Removal at existing bearings to remain in place shall be performed according to Article 501.05 of the Standard Specifications. Min. Lap Length for #5 bars = 1'-4".

<p>Excellence through Ownership</p> <p>200 West Front Street Wheaton, IL 60187</p>	ILLINOIS DEPARTMENT OF TRANSPORTATION
	<p>LONGITUDINAL JOINT DETAILS IL 43 OVER STONY CREEK FAP RTE 348 SECTION 3128-Z-B-1 COOK COUNTY STATION 179+01.40 STRUCTURE NO. 016-0322</p>
<p>DATE: 1-30-2009</p>	<p>DRAWN BY: BLB CHECKED BY: WJV</p>

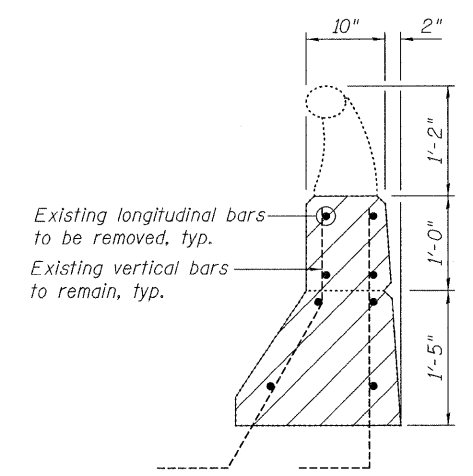
Contract #60D70



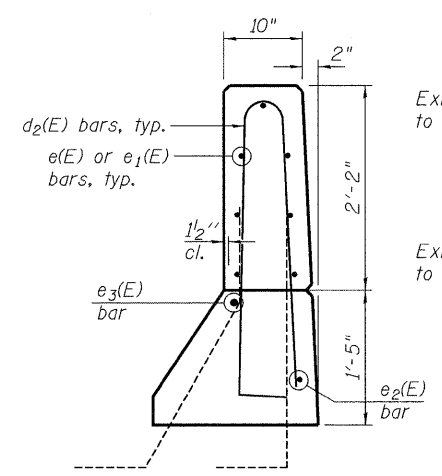
EXISTING PARAPET REMOVAL ELEVATION



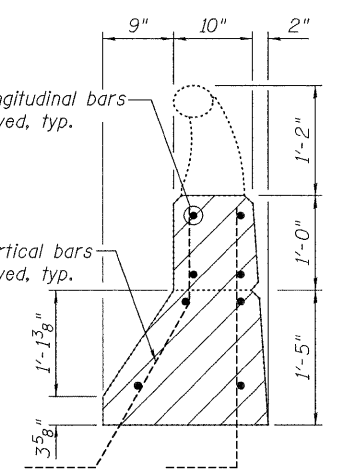
PROPOSED PARAPET ELEVATION



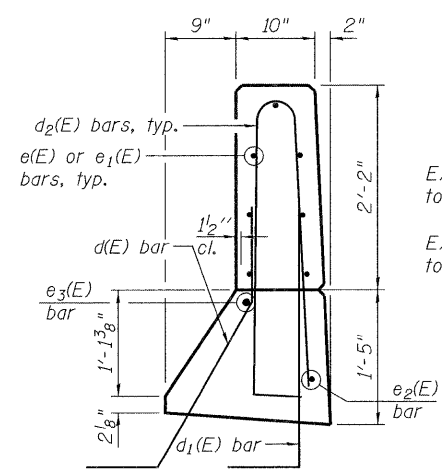
SECTION A-A



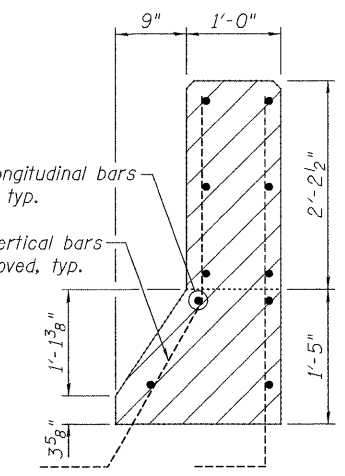
SECTION B-B



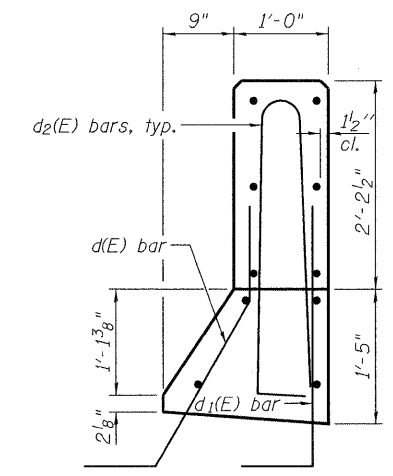
SECTION C-C



SECTION D-D



SECTION E-E



SECTION F-F

placement and quantity of d(E) and d1(E) bars shown on page 5 of 14

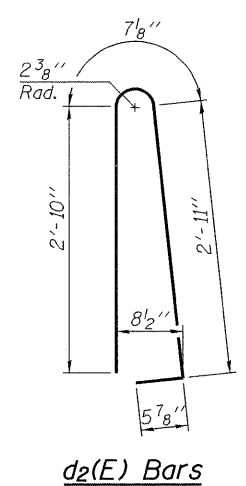
placement and quantity of d(E) and d1(E) bars shown on page 5 of 14

NOTES

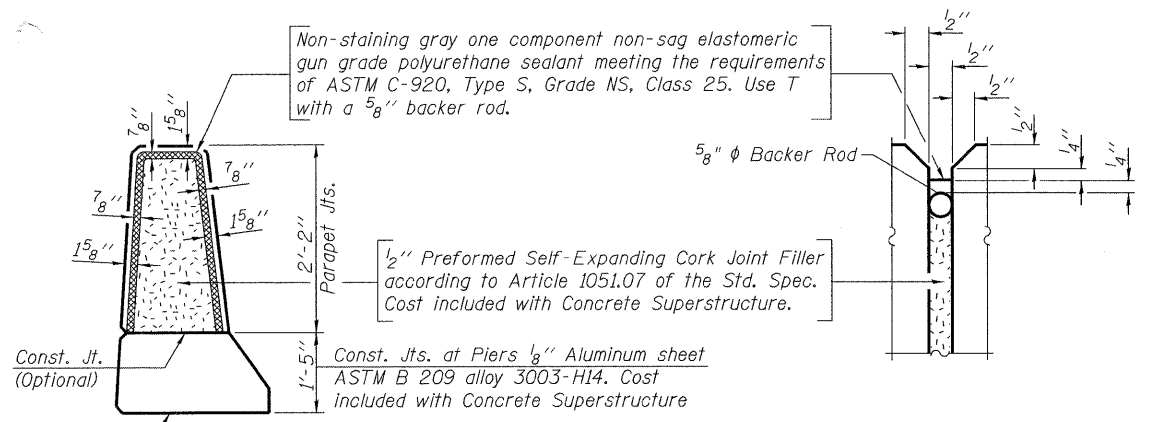
Hatched areas indicate concrete sections to be removed. Care shall be exercised by the contractor during and following Concrete Removal operations to ensure that the existing rebar remaining in place are not damaged. All existing reinforcement to be incorporated into new construction shall be blast-cleaned, straightened, and properly positioned prior to concrete placement. Any reinforcement damaged during Concrete Removal shall be repaired or replaced using an approved Bar Splicer or Mechanical System. Cost included with Concrete Removal.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d2(E)	236	#5	6'-10"	U
e(E)	56	#4	16'-1"	—
e1(E)	28	#4	19'-8"	—
e2(E)	4	#4	32'-5"	—
e3(E)	4	#8	32'-5"	—
e4(E)	2	#4	39'-7"	—
es(E)	2	#8	39'-7"	—
Bridge Rail Removal	Foot		211	
Concrete Removal	Cu. Yd.		22.7	
Concrete Superstructure	Cu. Yd.		31.1	
Protective Coat	Sq. Yd.		197	
Reinforcement Bars, Epoxy Coated	Pound		3350	



d2(E) Bars



PARAPET JOINT DETAILS



200 West Front Street
Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION

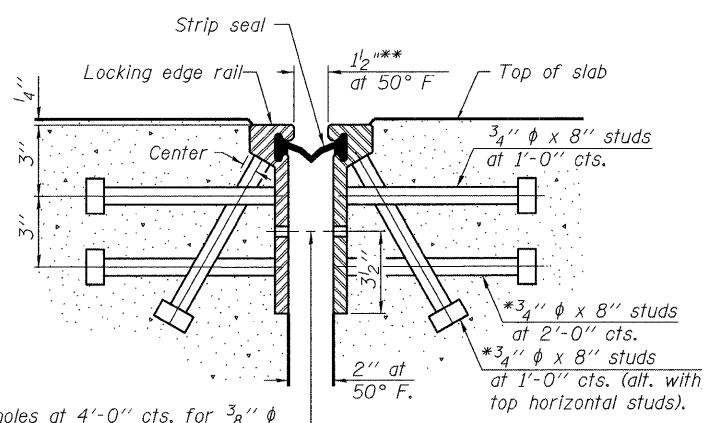
PARAPET REHABILITATION
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322

DATE: 1-30-2009

DRAWN BY WJV
CHECKED BY BLB

2/12/2009 k:\11225502\structures\1.43 over stony creek\final plans\Repairs.edg

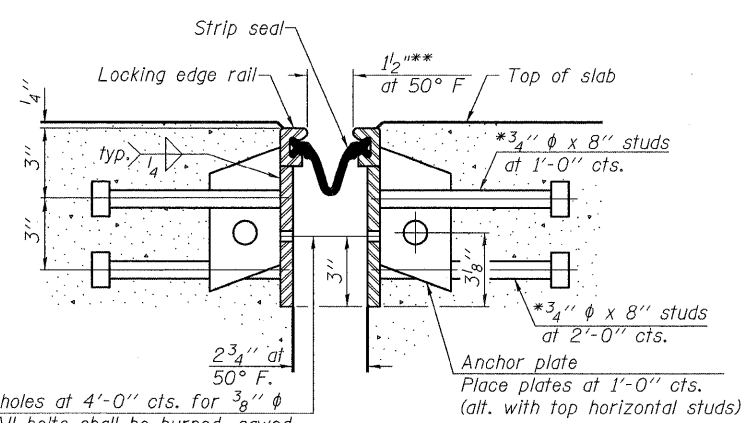
Contract #60D70



7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

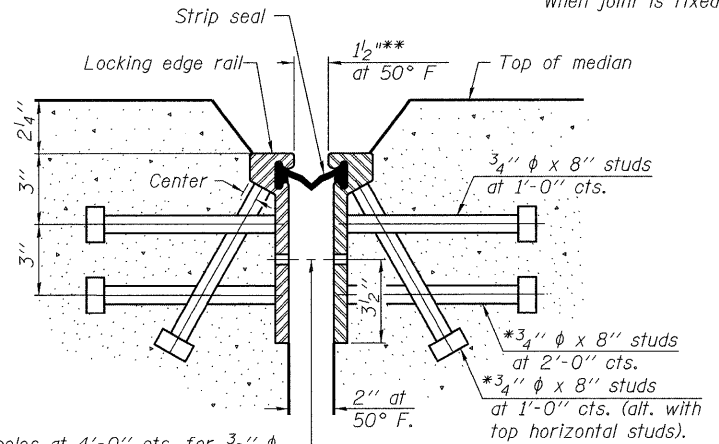
SECTION THRU ROLLED RAIL JOINT

*Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.
**When joint is fixed, dimension is set at 1 1/2".



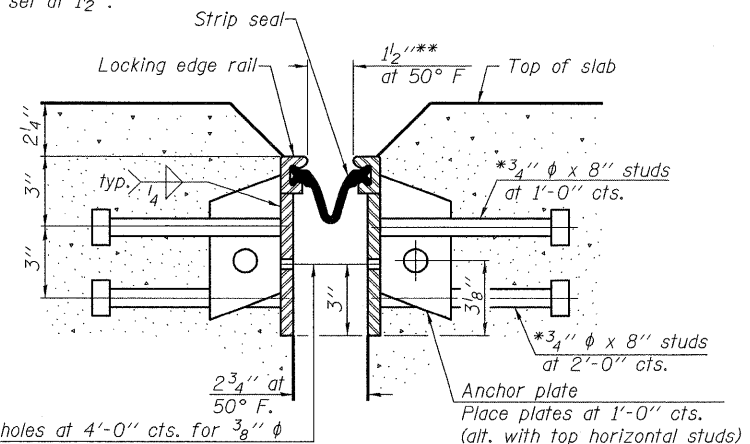
7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU WELDED RAIL JOINT



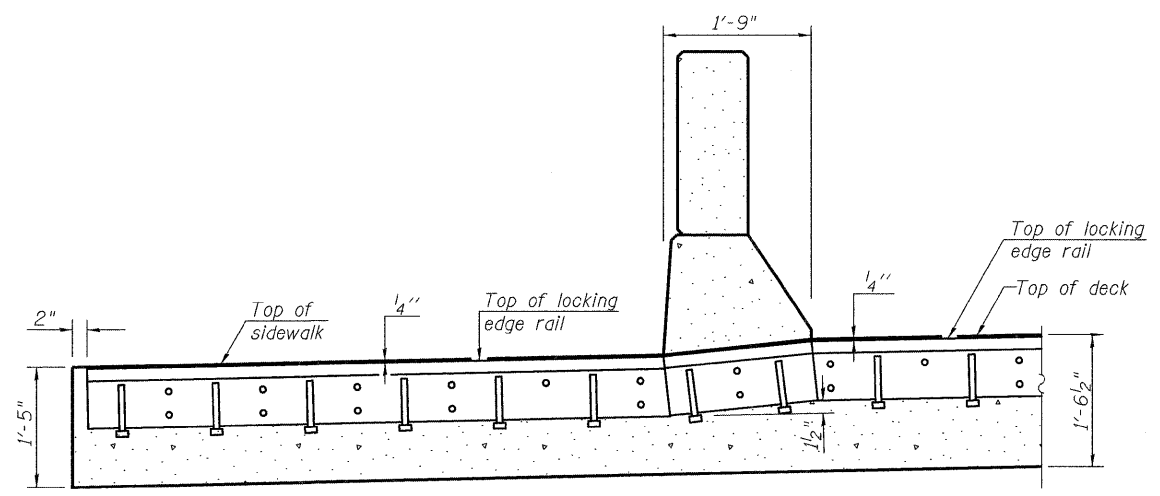
7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU ROLLED RAIL JOINT AT MEDIAN

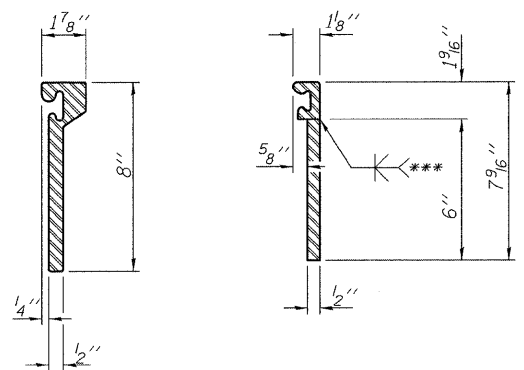


7/16" ϕ holes at 4'-0" cts. for 3/8" ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

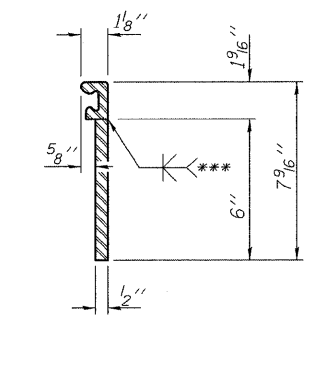
SECTION THRU WELDED RAIL JOINT AT MEDIAN



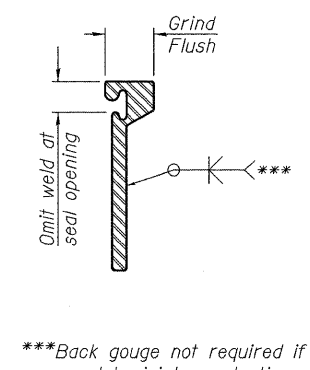
SECTION THRU SIDEWALK AND PARAPET



ROLLED (EXTRUDED) RAIL

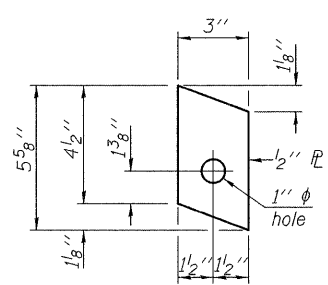


WELDED RAIL



LOCKING EDGE RAIL SPLICE

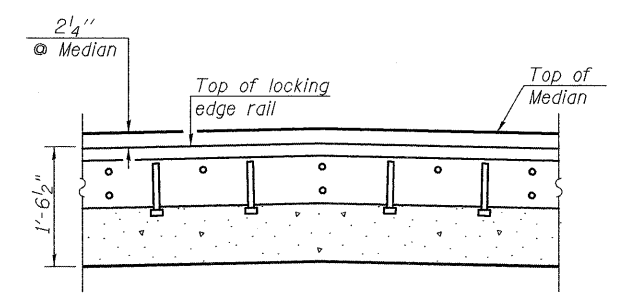
The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PLATE (for welded rail)

***Back gouge not required if complete joint penetration is verified by mock-up.

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



SECTION THRU MEDIAN

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	200

rjngroup
Excellence through Ownership
200 West Front Street
Wheaton, IL 60187

ILLINOIS DEPARTMENT OF TRANSPORTATION
PREFORMED JOINT STRIP SEAL
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322
DATE: 1-30-2009
DRAWN BY: WJV
CHECKED BY: BLB

LOCKING EDGE RAILS

2/12/2009 ka:\11225592\structure\1143 over stony creek\final plans\submit.dgn

Contract #60D70

NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) = $1.25 \times f_y \times A_t$
 - ② Minimum *Pull-out Strength (Tension in kips) = $0.66 \times f_y \times A_t$
- Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_t = Tensile stress area of lapped reinforcement bars.
* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8

The diameter of this part is equal or larger than the diameter of bar spliced.

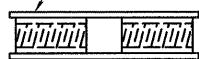
The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



**** ONE PIECE**

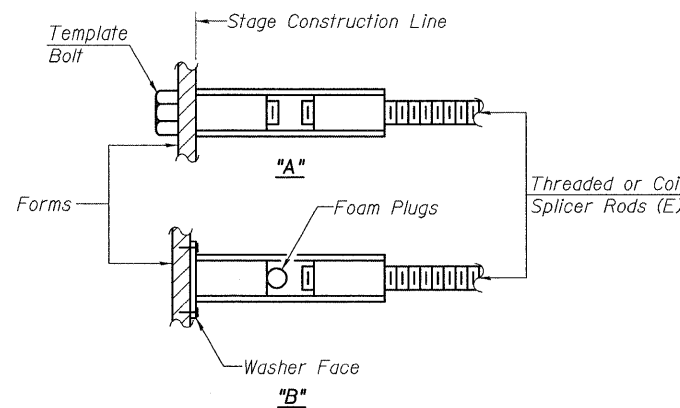
Wire Connector



WELDED SECTIONS

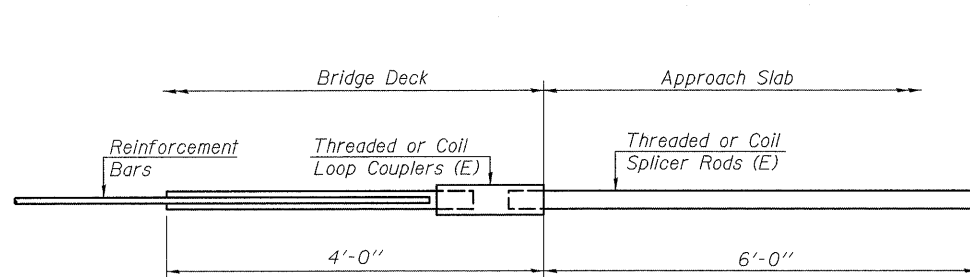
BAR SPLICER ASSEMBLY ALTERNATIVES

**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



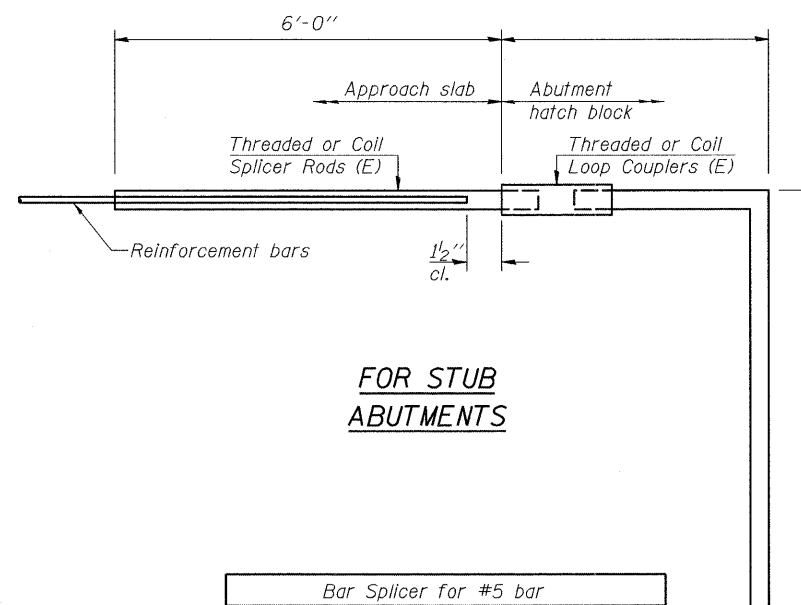
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



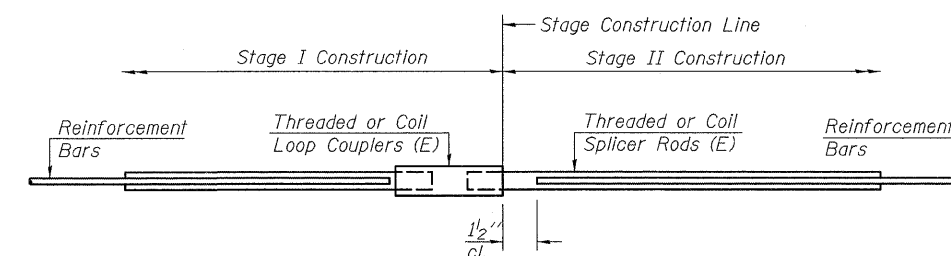
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 0



FOR STUB ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 0



STANDARD

Bar Size	No. Assemblies Required	Location
#5	24	Deck
#6	12	T/Abut.
#7	12	Deck

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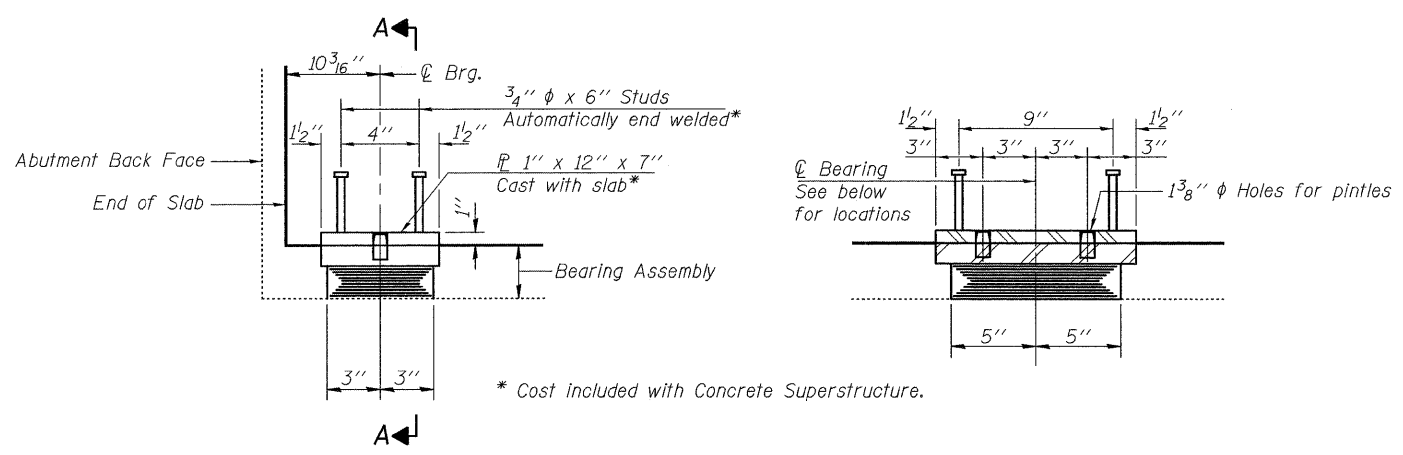
BAR SPLICER ASSEMBLY DETAILS
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322

DATE: 1-30-2009

DRAWN BY LM
CHECKED BY BLB

2/12/2009 ks11225502\structures\1.43 over stony creek\final plans\01mtd.dgn

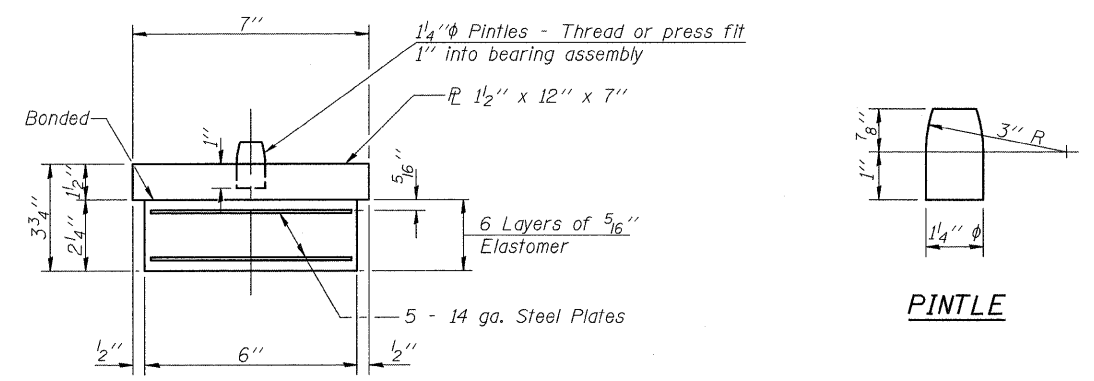
Contract #60D70



ELEVATION AT ABUT.
Dimensions given along \varnothing Brg.

SECTION A-A

TYPE I ELASTOMERIC EXP. BRG.



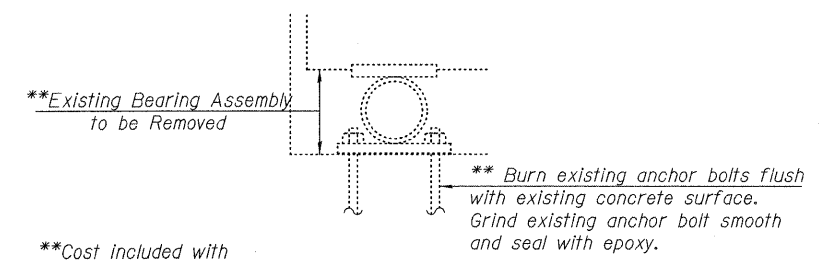
BEARING ASSEMBLY

PINTLE

Note:
Shim plates shall not be placed under Bearing Assembly.

TEMPORARY SHORING AND CRIBBING PROCEDURES

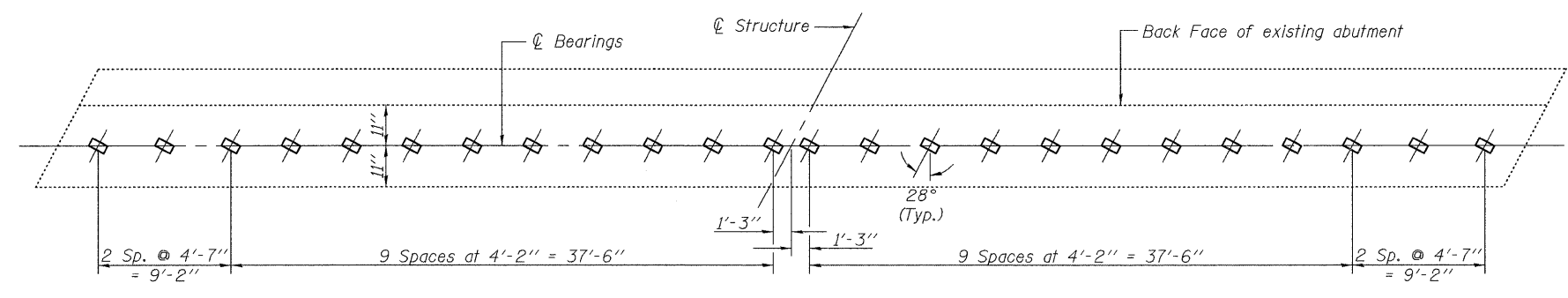
1. Prior to commencing any work at the bearings, the contractor shall submit plans for temporary shoring and cribbing of the slab for approval by the Engineer.
2. See Special Provision for Temporary Shoring and Cribbing.
3. Bearings are to be removed after concrete is removed and replaced prior to placement of new concrete.



REMOVE EXISTING BEARING AT ABUTMENTS

TYPICAL BEARING REACTION TABLE		
Abutment		
R _∅	(k)	11.9
R _L	(k)	19.5
Imp.	(k)	5.9
R _{Total}	(k)	37.3

Applies to bearings spaced at 4'-2"



BEARING LOCATIONS

BILL OF MATERIAL

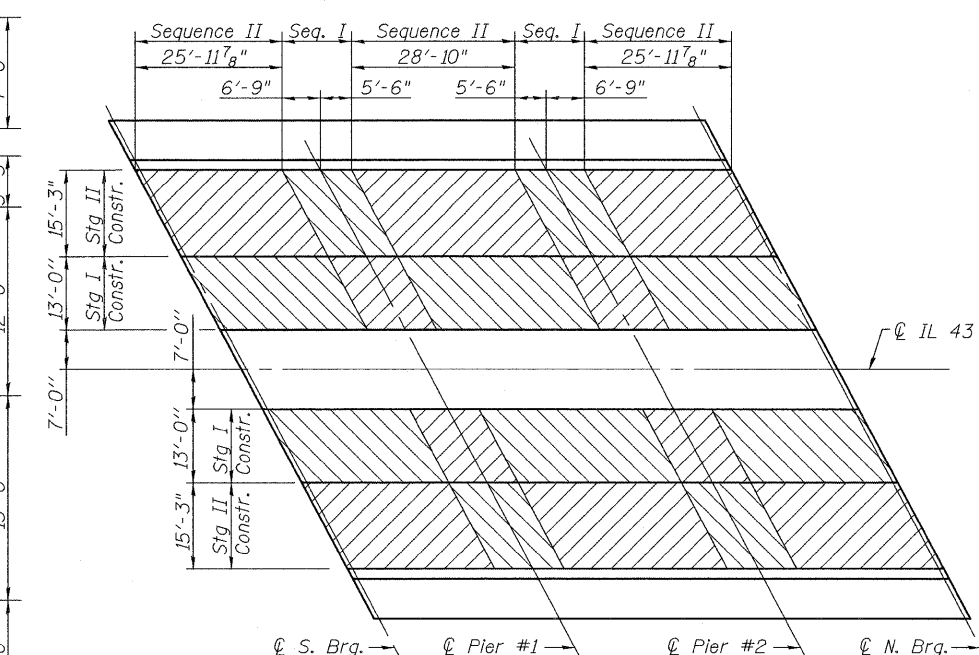
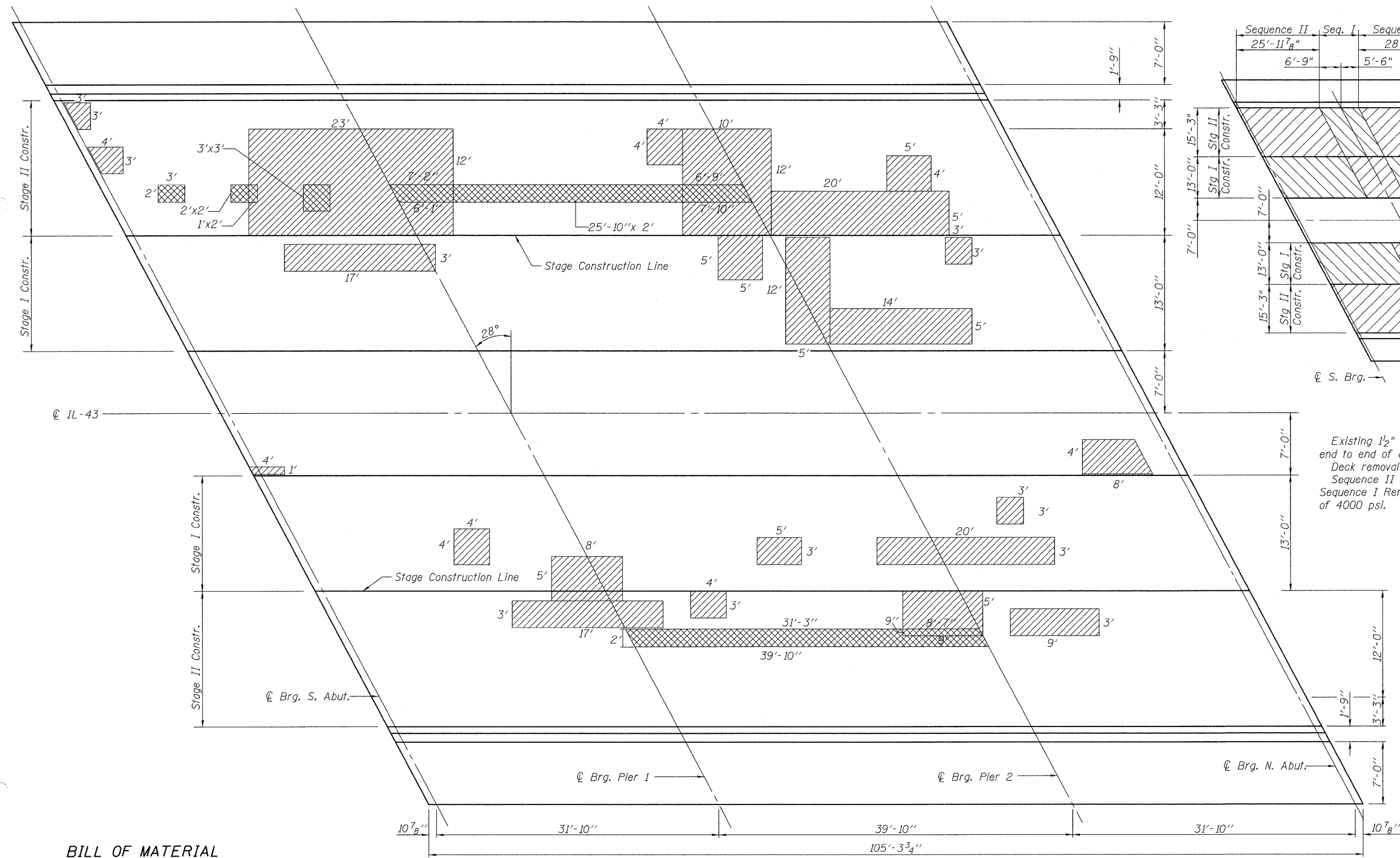
Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	48
Temporary Shoring and Cribbing	L. Sum	1

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BEARING REPLACEMENT DETAILS
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322
DATE: 1-30-2009
DRAWN BY: BLB
CHECKED BY: WJV

2/12/2009 K:\1125592\Structures\11_43 over Stony Creek\Final Plans\Bearing.rvt

Contract #60D70



PLAN
STAGING SEQUENCE DETAIL

Existing 1 1/2" Hot-Mix Asphalt Wearing Surface shall be removed end to end of deck full length before Stage Removal starts.
Deck removal sequencing applies to Stage I and Stage II Construction.
Sequence II Removal Areas shall not be started until the new overlay in Sequence I Removal Areas has reached a minimum compressive strength of 4000 psi.

BILL OF MATERIAL

Item	Unit	Quantity
Deck Slab Repair (Partial)	Sq. Yd.	4
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	20
Bridge Deck Latex Concrete Overlay, 2 1/4"	Sq. Yd.	634
** Hot-Mix Asphalt Surface Removal (Deck)	Sq. Yd.	676
Bridge Deck Hydro-Scarification, 3/4"	Sq. Yd.	634
Bridge Deck Grooving	Sq. Yd.	612
Protective Coat	Sq. Yd.	269

**Includes the 12" width over the top of the abutment backwall

NOTES

- Existing Hot-Mix Asphalt surface and Waterproof Membrane shall be removed from entire deck.
- Entire existing deck to be Hydro-Scarified 3/4" and overlaid with 2 1/4" Latex Concrete.
- Existing reinforcement bars & dowel rods extending into new concrete are to be cleaned and incorporated into new construction.
- Deck repair areas are estimated from IDOT in the Fall of 2007. Actual locations of repairs made shall be shown by the Engineer on As Built plans.

PLAN

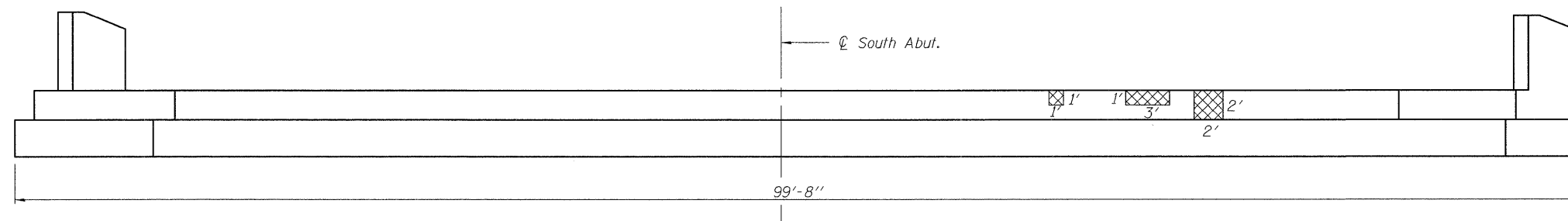
- Full-Depth Patching
- *Partial Depth Patching (110 Sq. Yd. on roadway)
- *Partial Depth shown for information only on roadway. The Partial Depth on the median will be paid for as Deck Slab Repair (Partial)

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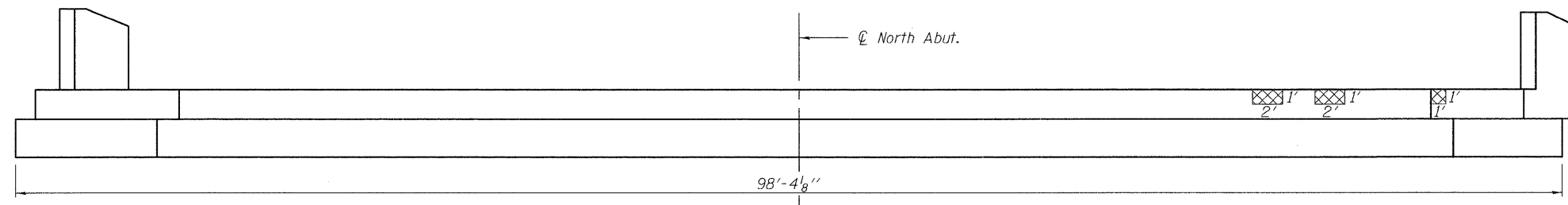
ILLINOIS DEPARTMENT OF TRANSPORTATION
DECK REPAIR PLAN
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322
DATE: 1-30-2009
DRAWN BY JMT
CHECKED BY WJV

2/12/2009 ks:\122552\structures\11_43 over stony creek\final\plans\Repairs.rvt

Contract #60D70



SOUTH ABUTMENT ELEVATION



NORTH ABUTMENT ELEVATION

NOTES:

 Structural Repair of Concrete (Depth equal to or less than 5')

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair of Concrete (Depth equal to or less than 5')	Sq. Ft.	13.0

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ILLINOIS DEPARTMENT OF TRANSPORTATION

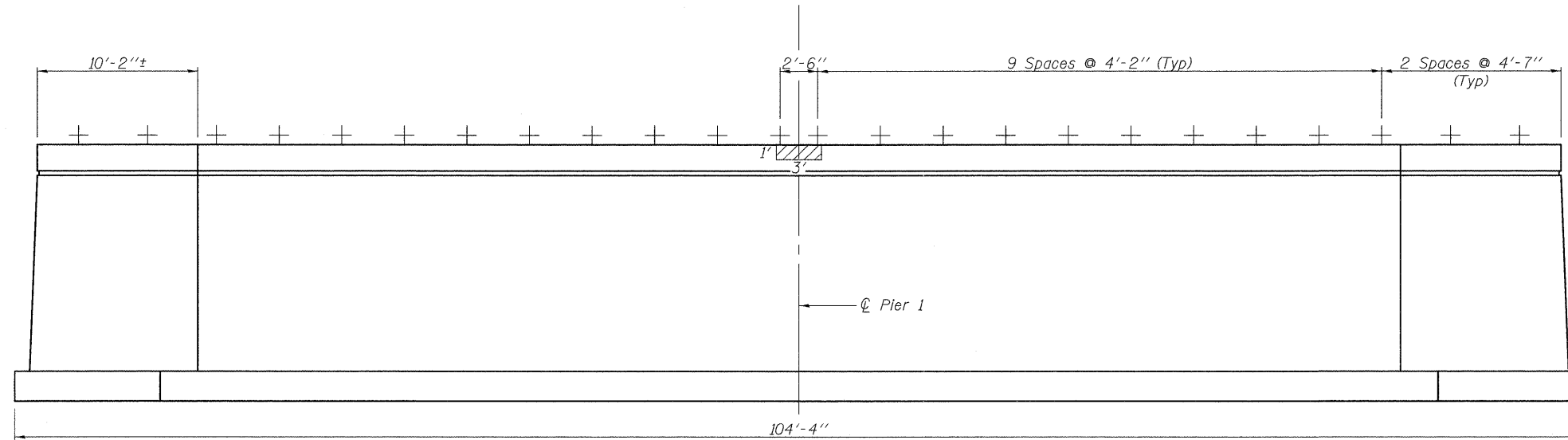
ABUTMENT REPAIRS
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322

DATE: 1-30-2009

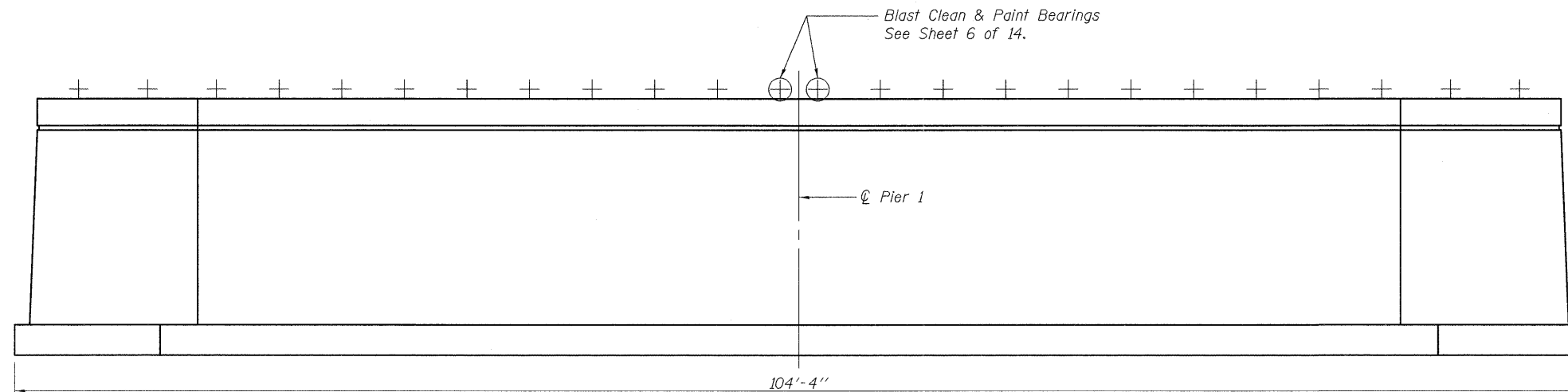
DRAWN BY JMT
CHECKED BY WJV

2/12/2009 ks11225592\structures\11_43 over stony creek\final plans\Repairs.dgn

Contract #60D70



PIER #1 SOUTH FACE ELEVATION



PIER #1 NORTH FACE ELEVATION

NOTES

- + Bearing
- ⊕ Blast Clean & Paint Bearings
- ▨ Structural Repair of Concrete (Depth greater than 5')

BILL OF MATERIAL

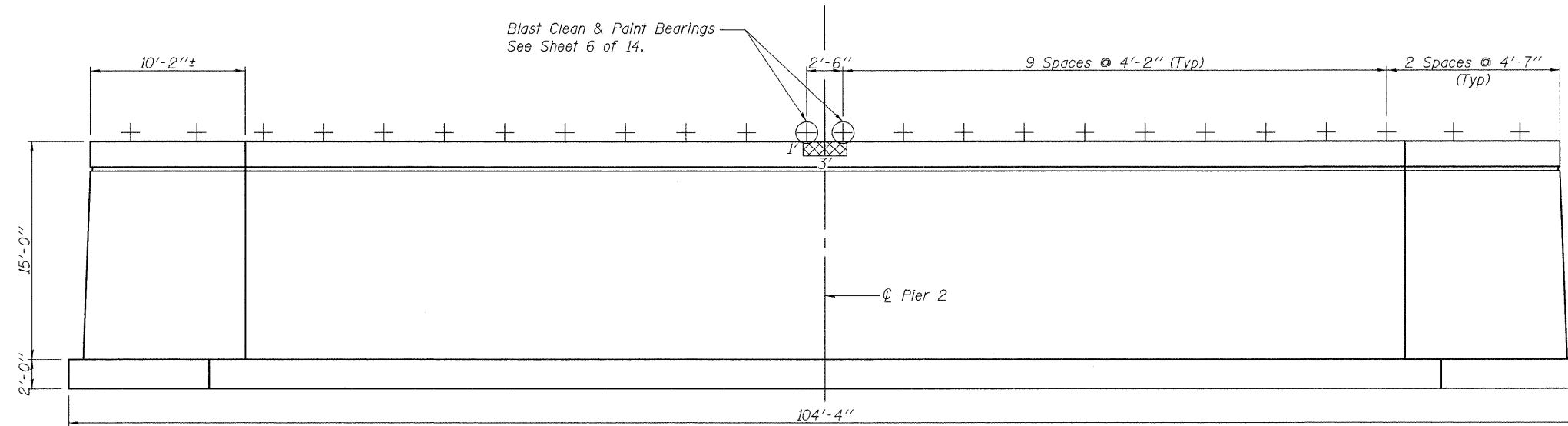
Item	Unit	Quantity
Structural Repair of Concrete (Depth greater than 5')	Sq. Ft.	3.0

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200 West Front Street
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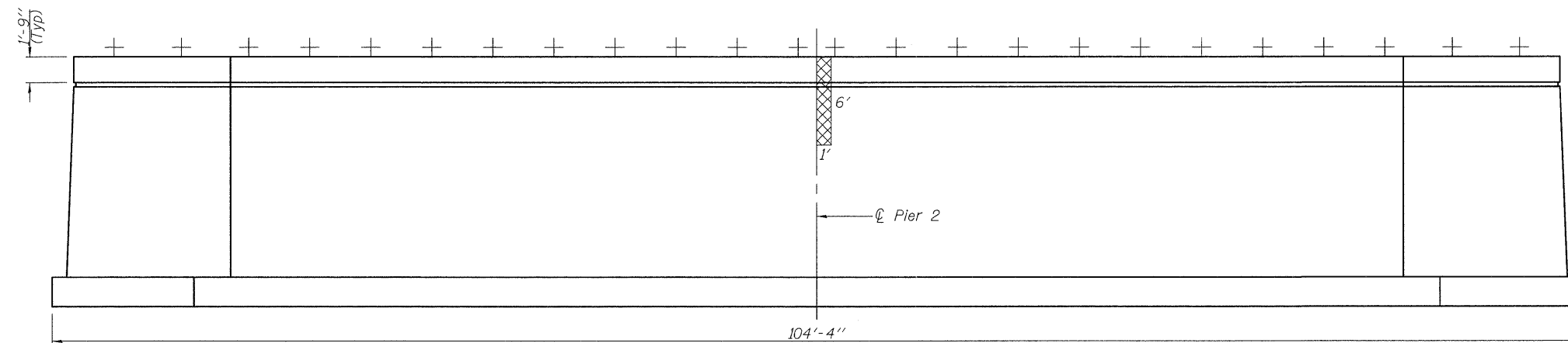
ILLINOIS DEPARTMENT OF TRANSPORTATION
PIER REPAIRS - I
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322
DATE: 11-30-2009
DRAWN BY: JMT
CHECKED BY: WJV

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Contract #60D70



PIER #2 NORTH FACE ELEVATION



PIER #2 SOUTH FACE ELEVATION

NOTES

- + Bearing
- ⊕ Blast Clean & Paint Bearings
- ⊗ Structural Repair of Concrete (Depth equal to or less than 5")

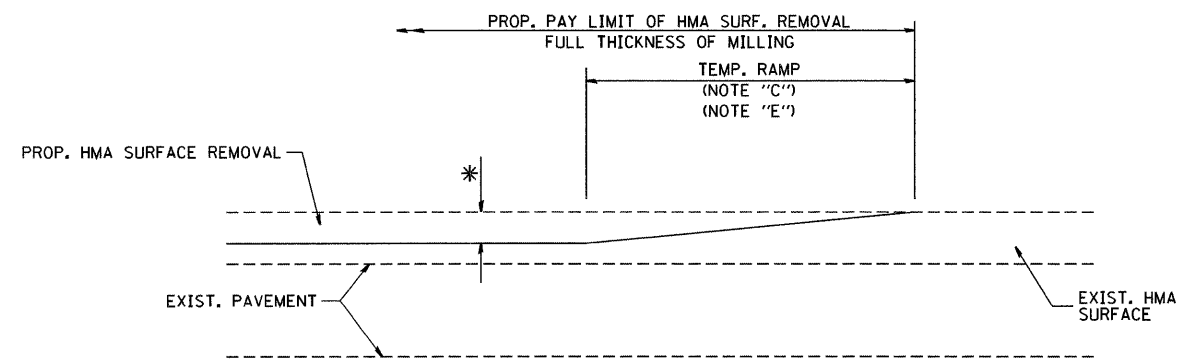
BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair of Concrete (Depth equal to or less than 5")	Sq. Ft.	9.0

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200 West Front Street
Wheaton, IL 60187

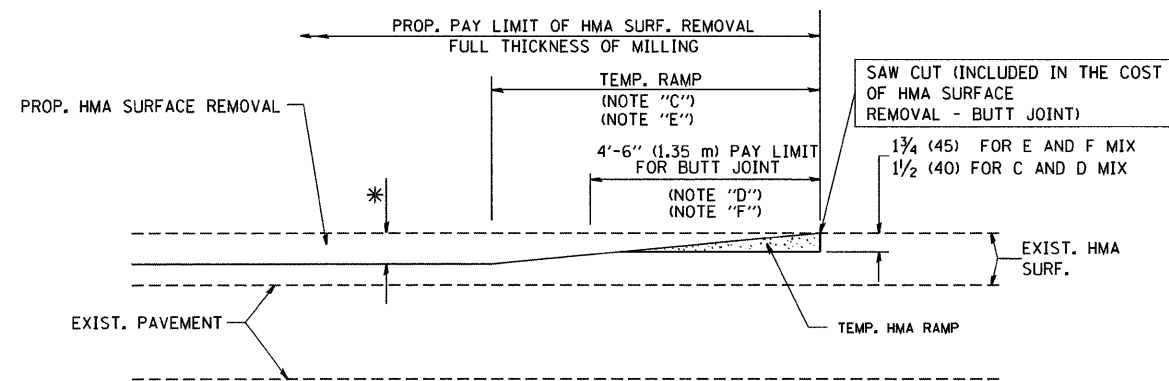
ILLINOIS DEPARTMENT OF TRANSPORTATION
PIER REPAIRS - II
IL 43 OVER STONY CREEK
FAP RTE 348 SECTION 3128-Z-B-1
COOK COUNTY
STATION 179+01.40
STRUCTURE NO. 016-0322
DATE: 1-30-2009
DRAWN BY JMT
CHECKED BY WJV

2/12/2009 k:\1122562\structures\11_43 over stony creek\final plans\Repairs.dgn



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

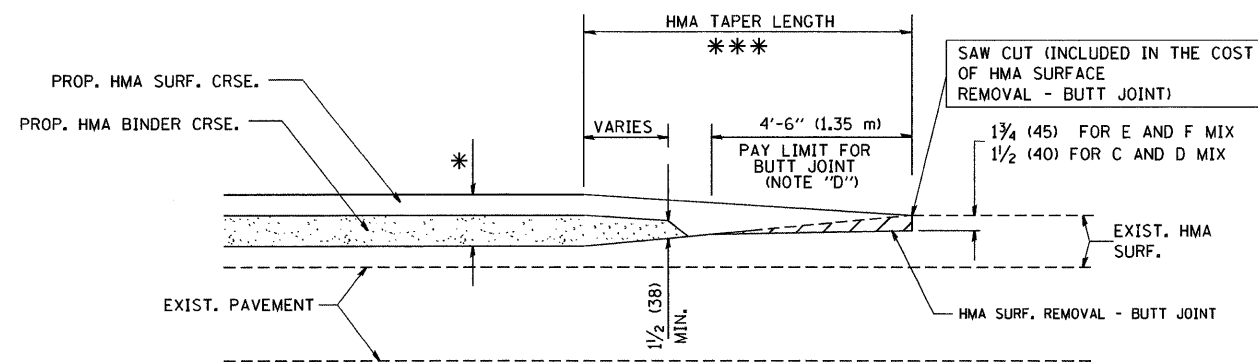
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

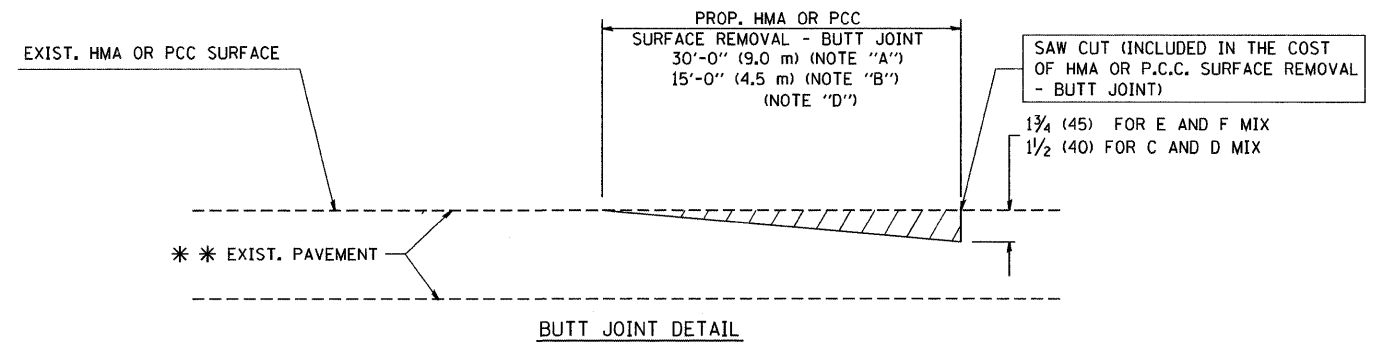
OPTION 2

TYPICAL TEMPORARY RAMP

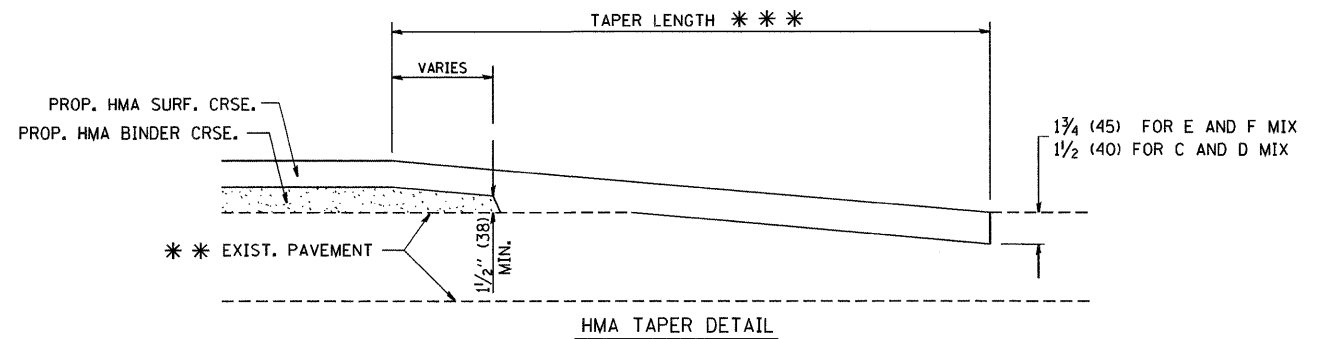


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =
W:\dststd\22x34\bd32.dgn

USER NAME = geglionobt
PLOT SCALE = 50.0000 ' / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

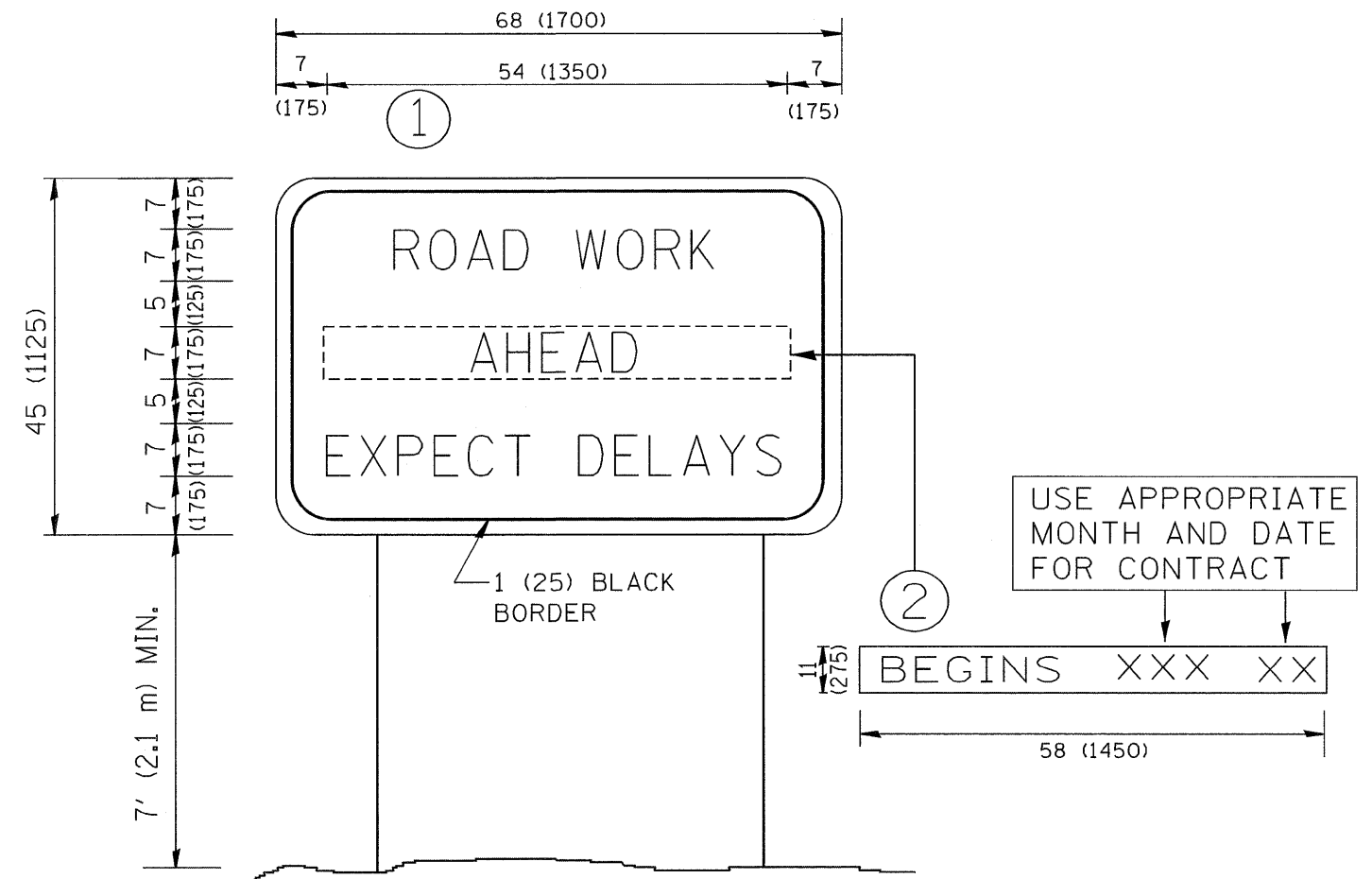
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
348	3128-Z-B-1	COOK	35	30
BD400-05 BD32			CONTRACT NO. 60D70	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

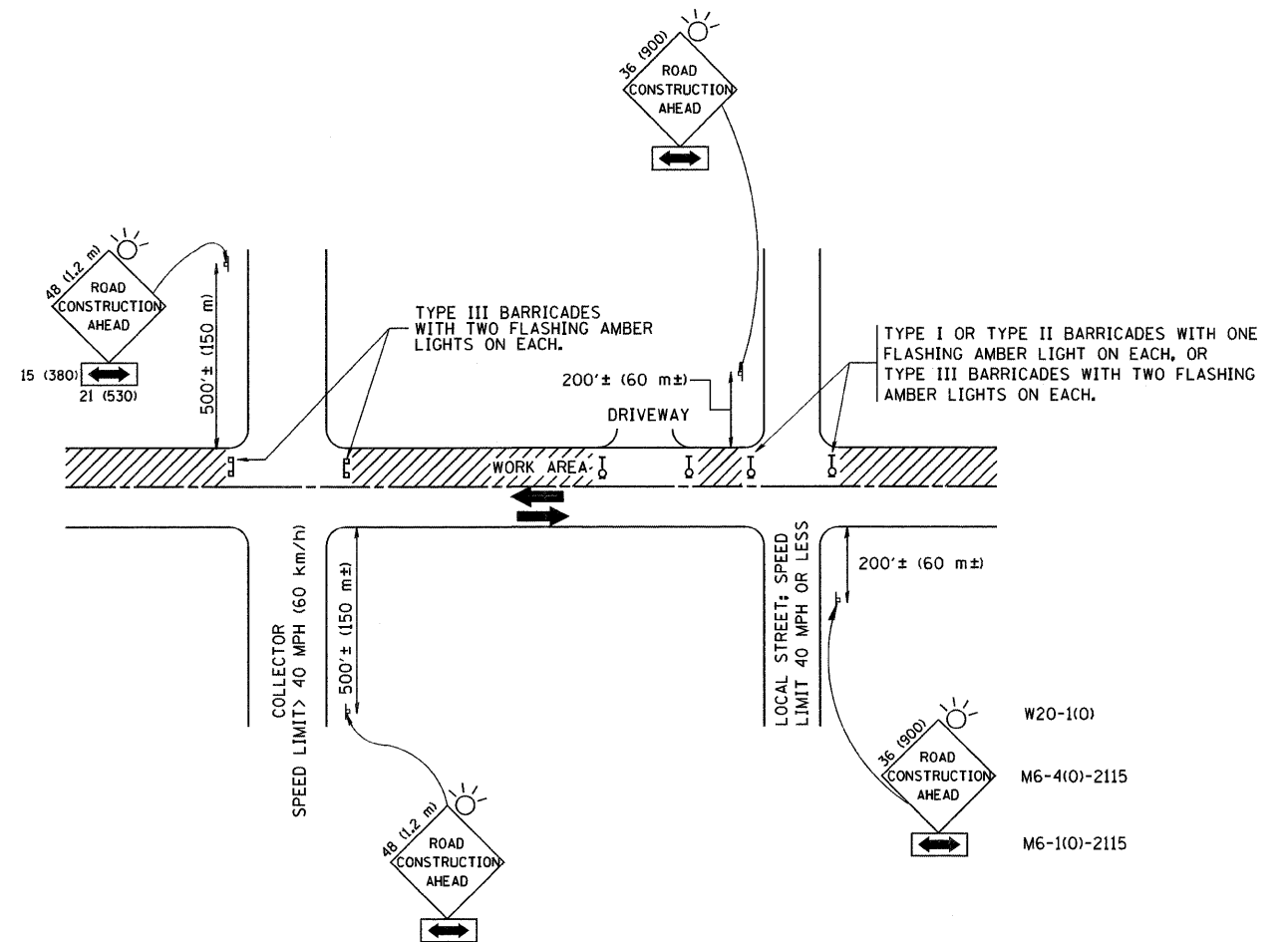


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = w:\diststd\22x34\to22.dgn	USER NAME = geglionobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			348	3128-Z-B-1	COOK	35	31
PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-22		CONTRACT NO. 60D70
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

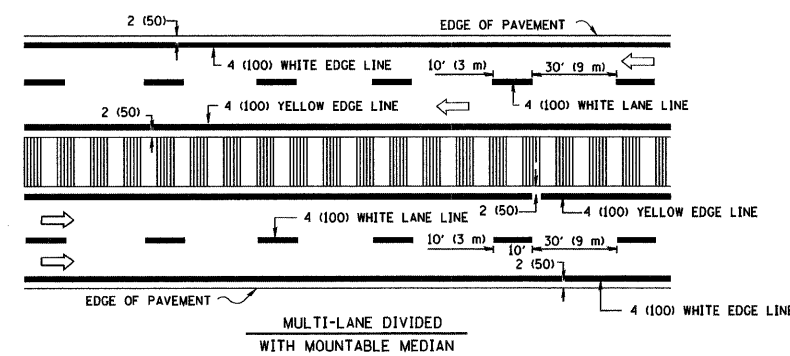
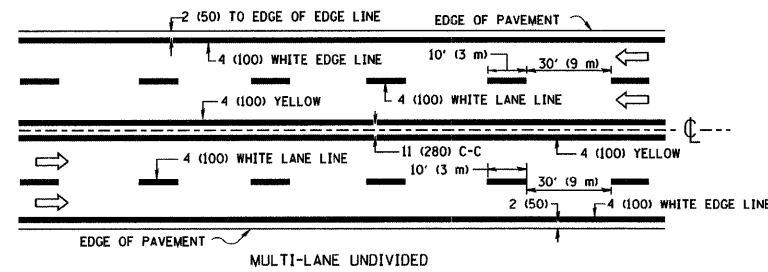
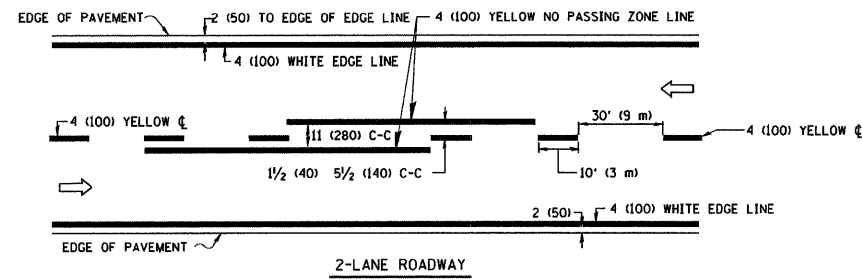
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl0.dgn	USER NAME = gaglionob	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
		PLOT SCALE = 50.000' / IN.	REVISED - A. HOUSEH 10-15-96
		PLOT DATE = 1/4/2008	REVISED - T. RAMMACHER 01-06-00
		DATE - 06-89	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

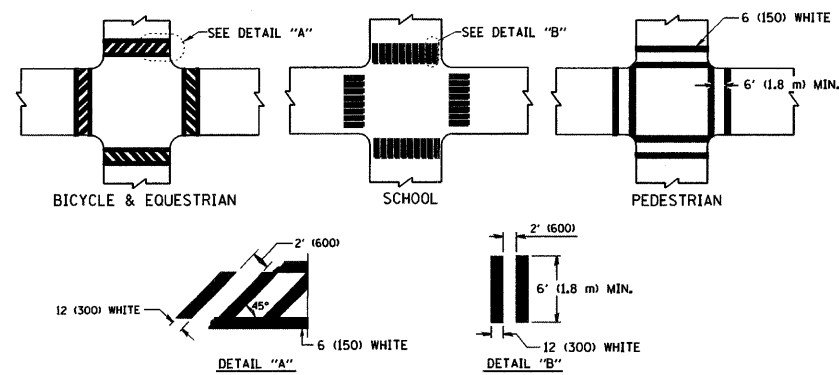
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.

F.A.P. RTE. 348	SECTION 3128-Z-B-1	COUNTY COOK	TOTAL SHEETS 35	SHEET NO. 32
TC-10			CONTRACT NO. 60D70	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

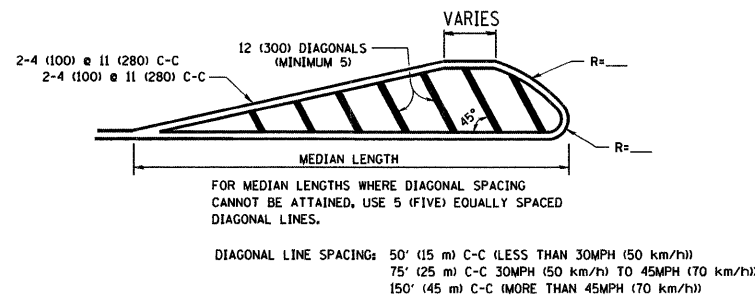
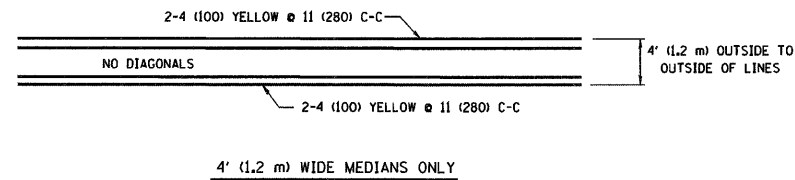


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

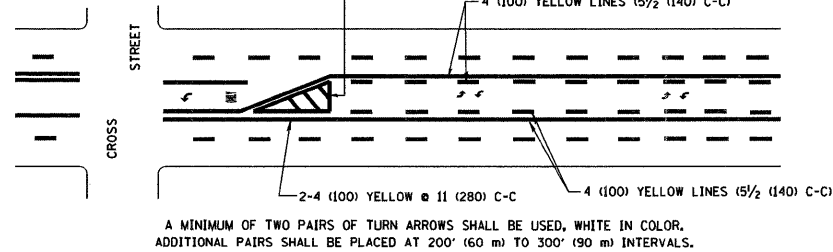
TYPICAL LANE AND EDGE LINE MARKING



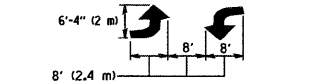
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE

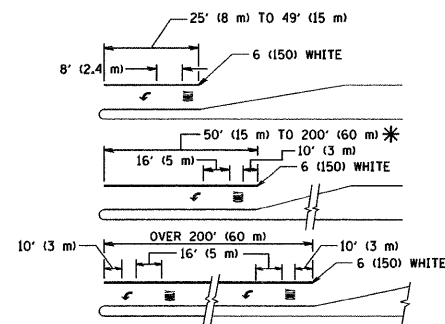


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

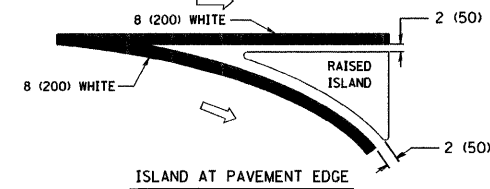
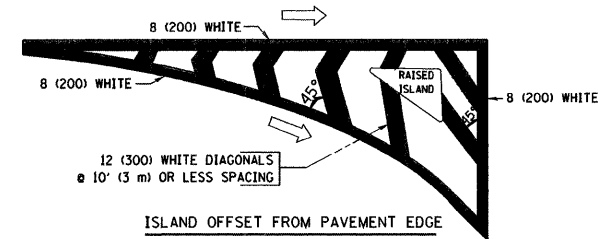
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 □ AREA = 15.6 SQ. FT. (1.5 m²) □ AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

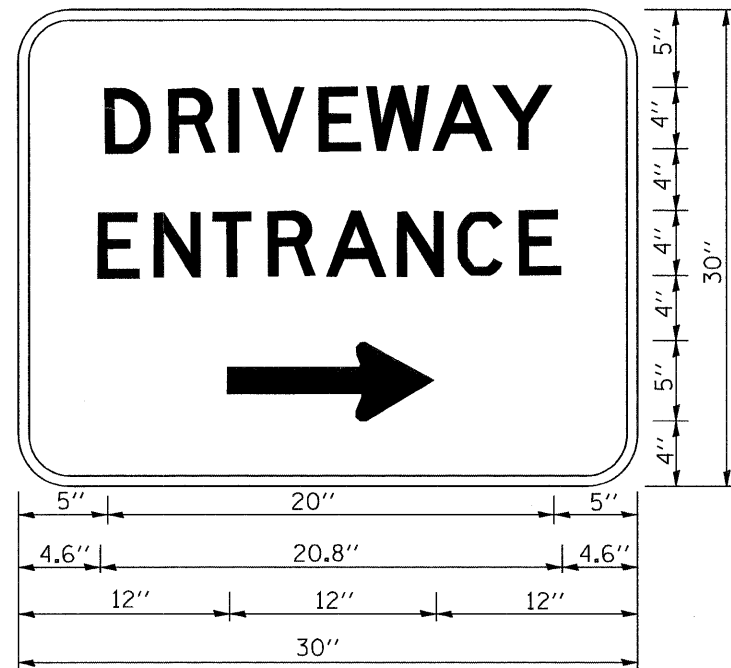


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME = W:\diststd\22x34\tc26.dgn	USER NAME = goglionobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000 ' / IN.	DRAWN -	REVISED -					348	3128-Z-B-1	COOK	35	35
PLOT DATE = 1/4/2008	DATE -	CHECKED -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-26 CONTRACT NO. 60D70 FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			