

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60E51		

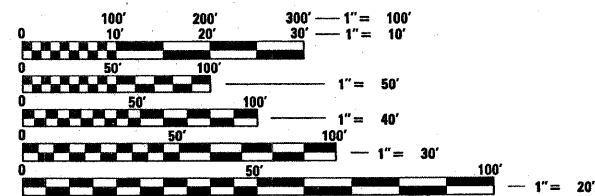
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

F.A.U. ROUTE 3523 /U.S. ROUTE 12 (RAND RD.)
3RD AVE. TO LUNDERGAN AVE.
SECTION: 2008-021 RS
RESURFACING (MAINTENANCE)
PROJECT: *ESP-3523(006)*
COOK COUNTY
C-91-416-08

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE
CITY OF DES PLAINES



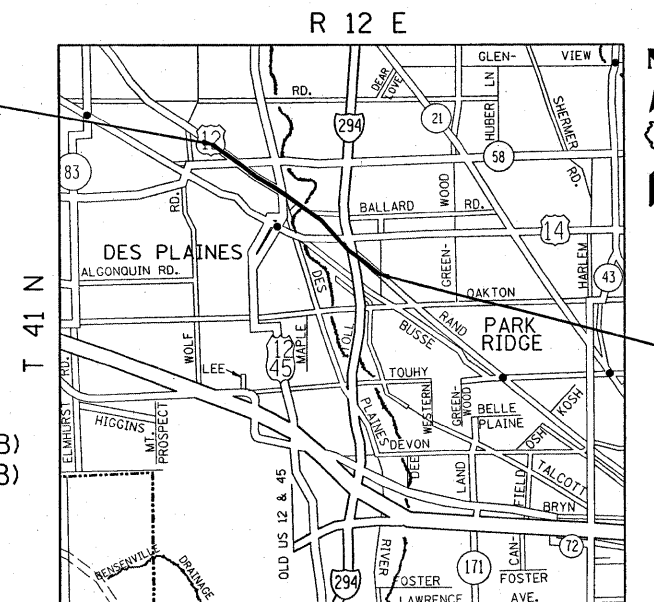
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT BEGINS STA. 5+37

OMISSION

- STA. 16+17 TO STA. 16+35
- STA. 21+84 TO STA. 27+77
- STA. 51+74 TO STA. 52+04
- STA. 87+57 TO STA. 89+70
- STA. 126+88 TO STA. 131+88 (NB)
- STA. 125+88 TO STA. 131+88 (SB)



PROJECT ENDS STA. 141+80

TRAFFIC DATA
SPEED LIMIT: 40 MPH
2006 ADT: 28,400

PROJECT MANAGER: KEN ENG (847) 705-4247
PROJECT ENGINEER: ANA ABREU (847) 705-4586

CONTRACT NO. 60E51

GROSS LENGTH OF PROJECT = 13,643 FEET (2.6 MILES)
NET LENGTH OF PROJECT = 12,189 FEET (2.3 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 27, 20 09

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 27, 20 09

Charles J. Ingersoll
ENGINEER OF DESIGN AND ENVIRONMENT

March 27, 20 09

Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.

DESCRIPTION

STATE STANDARDS

GENERAL NOTES:

1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
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5-9	TYPICAL SECTIONS
10-14	ROADWAY & PAVEMENT MARKING PLANS
15-20	DETECTOR LOOP REPLACEMENT PLANS
21	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
22	PAVEMENT PATCHING FOR HOT-MIX ASPHALT SURFACED PAVEMENT (BD-22)
23	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
24	BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS (BD-32)
25	TRAFFIC CONTROL AND PROTECTION FOR SIDES ROADS, INTERSECTIONS AND DRIVEWAYS (TC-10)
26	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT) (TC-11)
27	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
28	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
29	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)
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000001-05	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
701602-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF DES PLAINES.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ALL HOT-MIX ASPHALT PAVEMENT PATCHING SHALL BE CLASS D.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 45 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

10 FEET (3 METER) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURB AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OR RESURFACING.

ALL PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO DISTRICT 1 TYPICAL PAVEMENT MARKING.

AT LEAST TWO WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS, CONTACT WALLY CZARNY, AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386.

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED THROUGHOUT THE IMPROVEMENT ACCORDING TO THE DISTRICT STANDARDS AS NOTED IN THE DETAIL.

THE UNIT WEIGHT (CONVERSION FACTOR) QUOTED IS FOR THE ESTIMATING PLAN QUANTITIES ONLY. ACTUAL QUANTITIES TO FULFILL CONTRACT REQUIREMENTS WILL BE DETERMINED BASED ON UNIT WEIGHT OF APPROVED MIX DESIGN, PLAN DIMENSIONS, AND DENSITY LIMITATIONS. MAXIMUM PAYMENT WILL BE COMPUTED BASED ON WEIGHT AVERAGE DENSITIES OF THE IN-PLACE MIXTURE.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK AT (847) 705-4470.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 12 (RAND ROAD)		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\pwidot\abreuah\dms89779\0111904-Design.dgn		DRAWN -	REVISED -		INDEX OF SHEETS STATE STANDARDS AND GENERAL NOTES		3523	2008 07 RS	COOK	30	2
		CHECKED -	REVISED -		SCALE: 1" = 50'				CONTRACT NO. 60E51		
		DATE -	REVISED -		SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

CONTRACT NO. 60E51

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

D-91-119-04

SUMMARY OF QUANTITIES			URBAN 100% FED. TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN 100% FED. TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		1000						CODE NO	ITEM		UNIT	1000				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	64	64					70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
40600300	AGGREGATE (PRIME COAT)	TON	321	321					70300100	SHORT-TERM PAVEMENT MARKING	FOOT	10335	10335					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	24	24					70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1618	1618					
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	37893	37893					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1131	1131					70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2484	2484					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	672	672					70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1276	1276					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	7840	7840					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	553	553					
42001300	PROTECTIVE COAT	SO YD	628	628					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	660	660					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	67	67					70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	1148	1148					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	80005	80005					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1618	1618					
44000600	SIDEWALK REMOVAL	SO FT	67	67					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	37893	37893					
44000705	BARRIER MEDIAN REMOVAL	SO FT	120	120					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2484	2484					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1535	1535					* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1276	1276					
44002212	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3"	SO YD	4000	4000					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	553	553					
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	2048	2048					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	660	660					
44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	1000	1000					* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	911	911					
44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	952	952					78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	729	729					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	50	50					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1540	1540					
60404940	FRAMES AND GRATES, TYPE 23	EACH	1	1					X0322256	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4					
60623800	CONCRETE BARRIER MEDIAN	SO FT	120	120					X0323094	TYPE 1 FRAME, OPEN LID	EACH	23	23					
60624600	CORRUGATED MEDIAN	SO FT	1733	1733					X0323095	TYPE 1 FRAME, CLOSED LID	EACH	16	16					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					X0656100	DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SO YD	291	291					
67100100	MOBILIZATION	L SUM	1	1					X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	3150	3150					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1														
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1														

* Specialty Items

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
U.S. ROUTE 12 (RAND RD.)

PLOT DATE: 1/26/2009

1/26/2009 11:54:30 AM C:\New World\Drawings\60E51\60E51-030.dwg

CONTRACT NO. 60E51

F.A.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	4
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

D-91-119-04

SUMMARY OF QUANTITIES			URBAN 100% FED. TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		1000				
X6060500	CORRUGATED MEDIAN REMOVAL	SO FT	1733	1733				
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	22	22				
NP Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	83	83				
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	2	2				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
© Z0076600	TRAINEES	HOUR	1000	1000				

SUMMARY OF QUANTITIES				TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT							

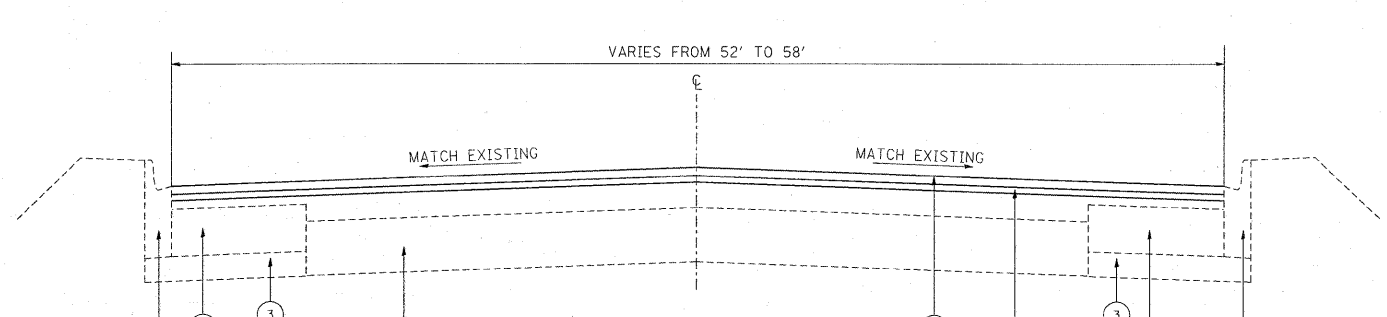
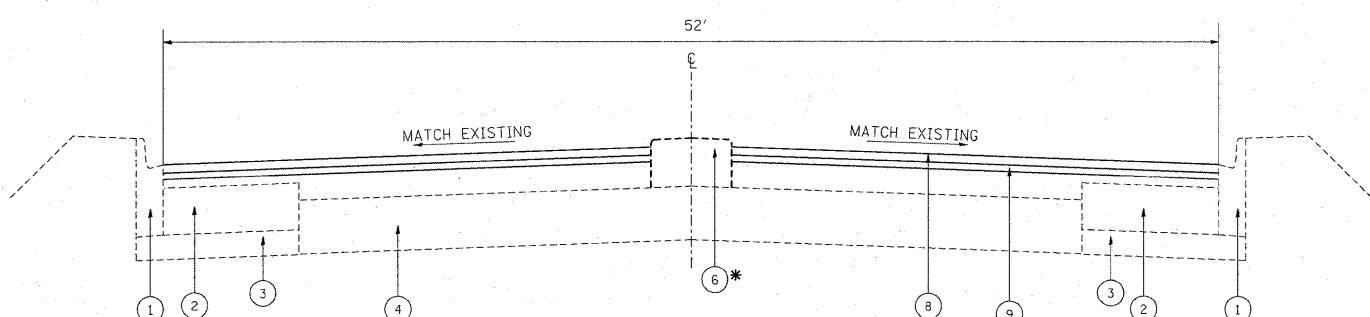
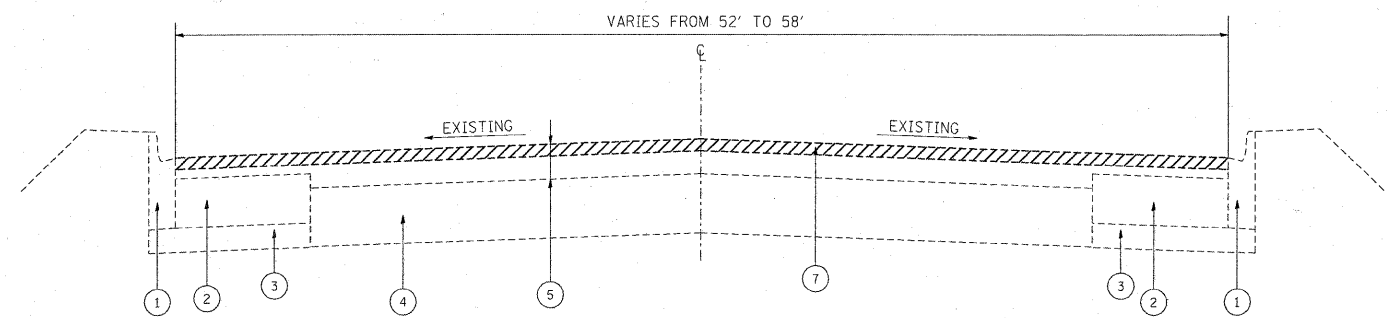
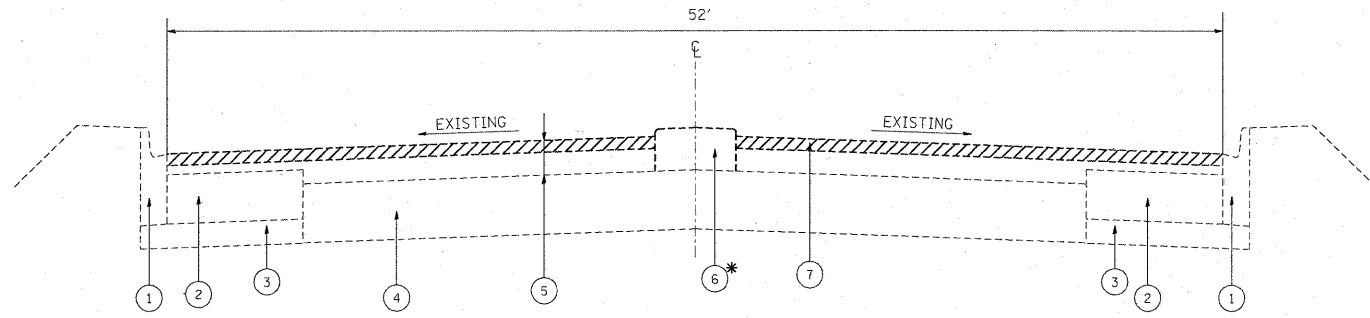
NP - Non-participating
© Y080

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
SUMMARY OF QUANTITIES
U.S. ROUTE 12 (RAND RD.)

PLOT DATE: 1/26/2009

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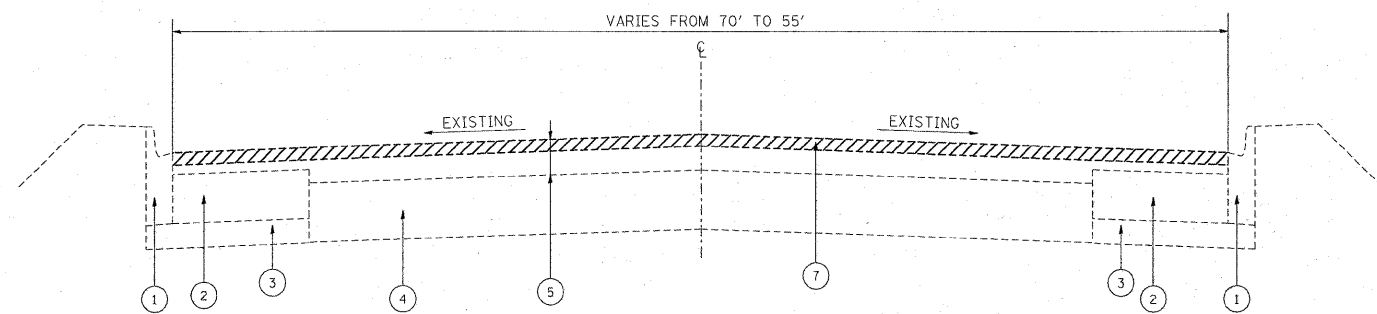
LEGEND

- ① EXISTING COMBINATION CONCRETE CURB & GUTTER
- ② EXISTING PCC BASE COURSE WIDENING ±9"
- ③ EXISTING SUB-BASE
- ④ EXISTING PCC BASE COURSE ±9"
- ⑤ EXISTING HMA OVERLAY ±5 1/2"
- ⑥ EXISTING CORRUGATED P.C. CONCRETE MEDIAN OR P.C. CONCRETE BARRIER MEDIAN
- ⑦ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑧ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

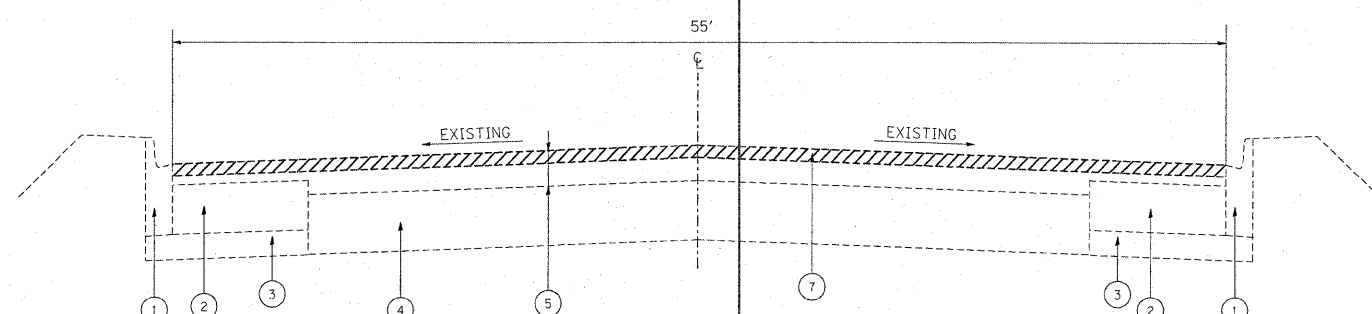
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE USES	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90 (IL-9.5 MM)	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19MM)	CLASS D PATCHES (HMA BINDER IL-19MM)
AC TYPE	SBS/SBR PG 76-28/-22	SBS/SBR PG 70-22	PG 64-22 *	PG 64-22 *
DESIGN AIR VOIDS	4.0% @ 50 GYR	4.0% @ 90 GYR	4% @ 70 GYR	4% @ 70 GYR

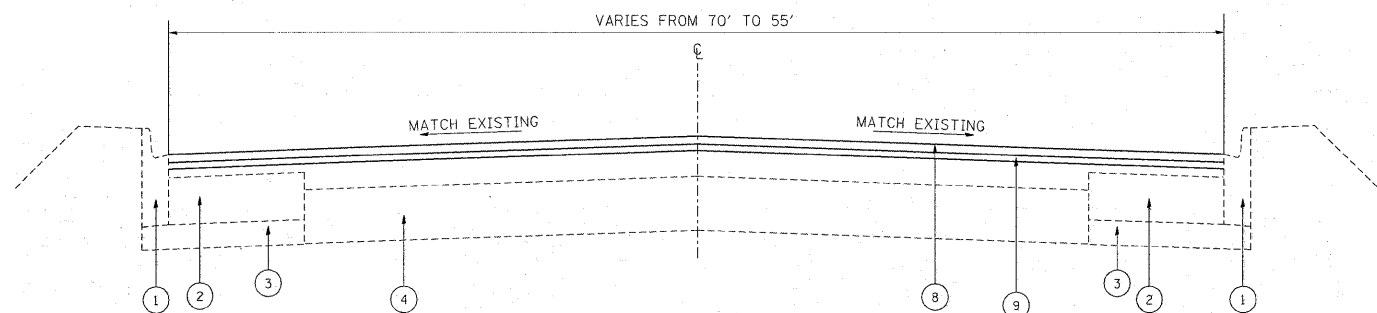
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIX QUANTITIES IS 112 LBS/SY/IN
* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58 -22



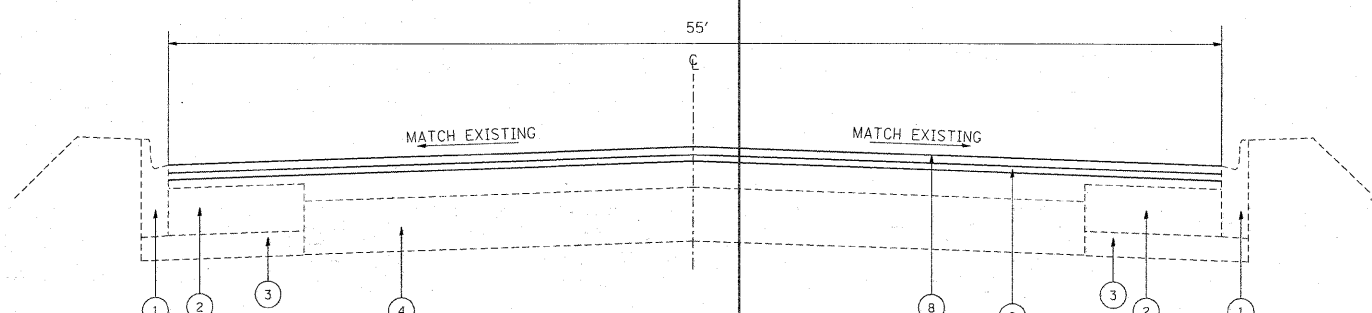
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STA. 27+77 TO STA. 31+39



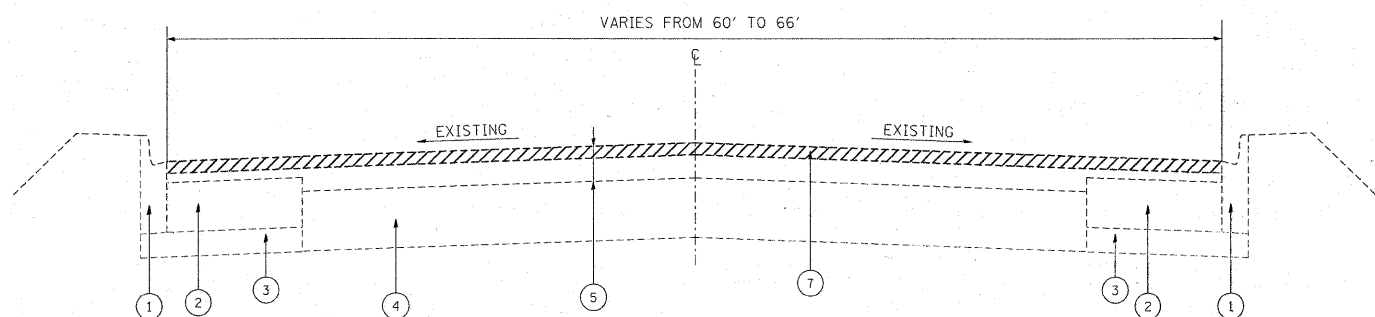
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STA. 52+04 TO STA. 54+73
STA. 91+12 TO STA. 95+18



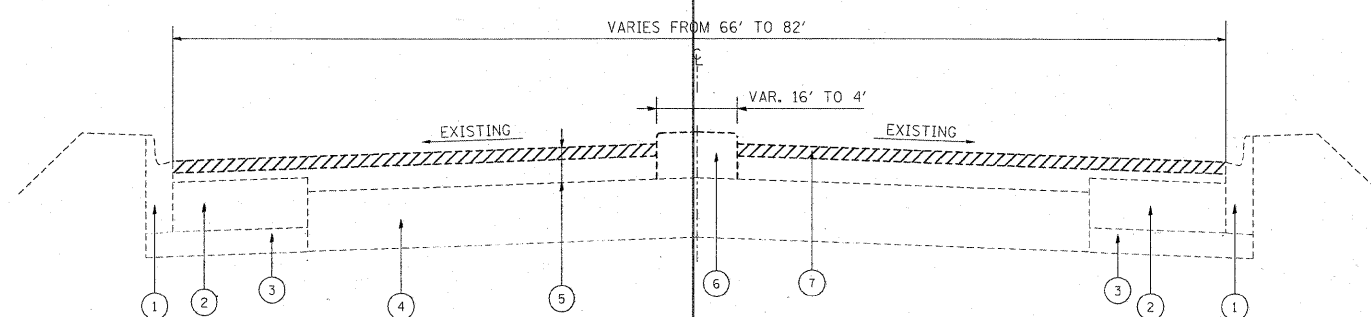
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STA. 27+77 TO STA. 31+39



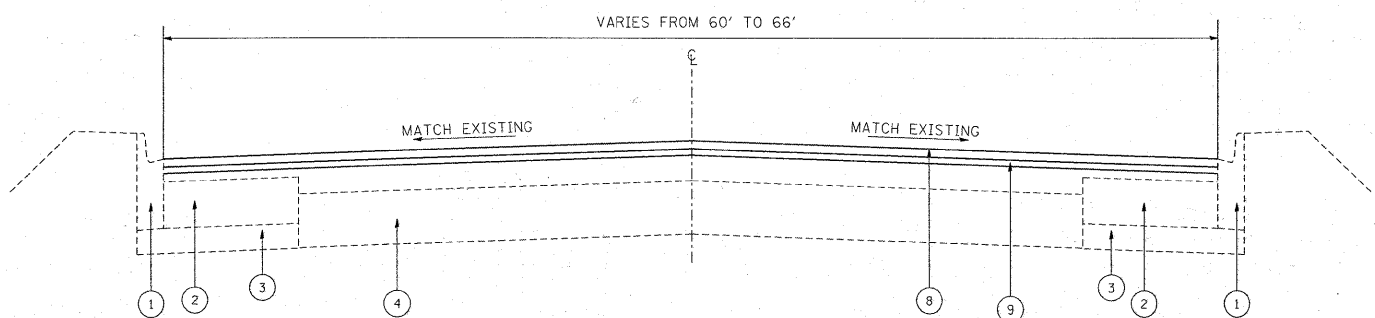
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STA. 31+39 TO STA. 51+74
STA. 52+04 TO STA. 54+73
STA. 91+12 TO STA. 95+18



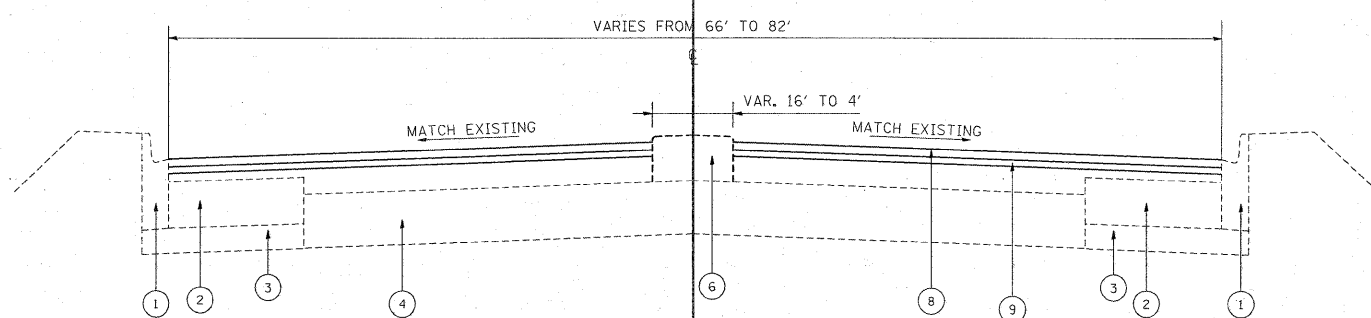
EXISTING TYPICAL SECTION
STA. 54+73 TO STA. 61+46



EXISTING TYPICAL SECTION
STA. 61+46 TO STA. 66+00



PROPOSED TYPICAL SECTION
STA. 54+73 TO STA. 61+46



EXISTING TYPICAL SECTION
STA. 61+46 TO STA. 66+00

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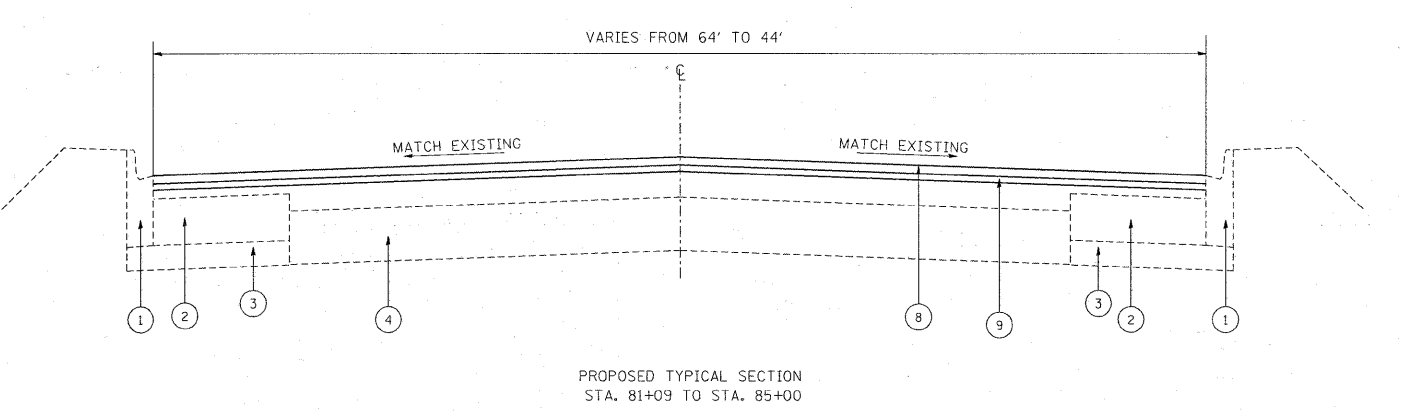
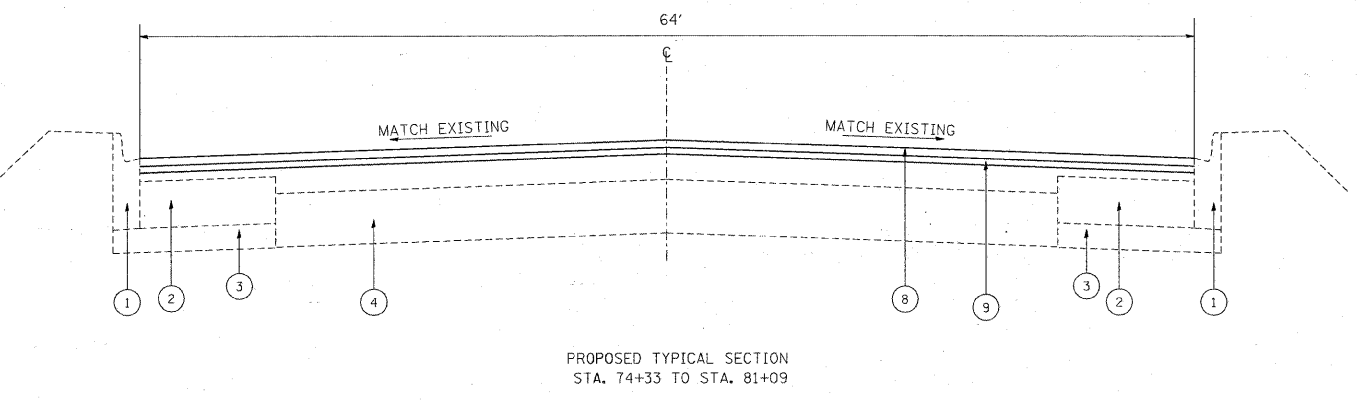
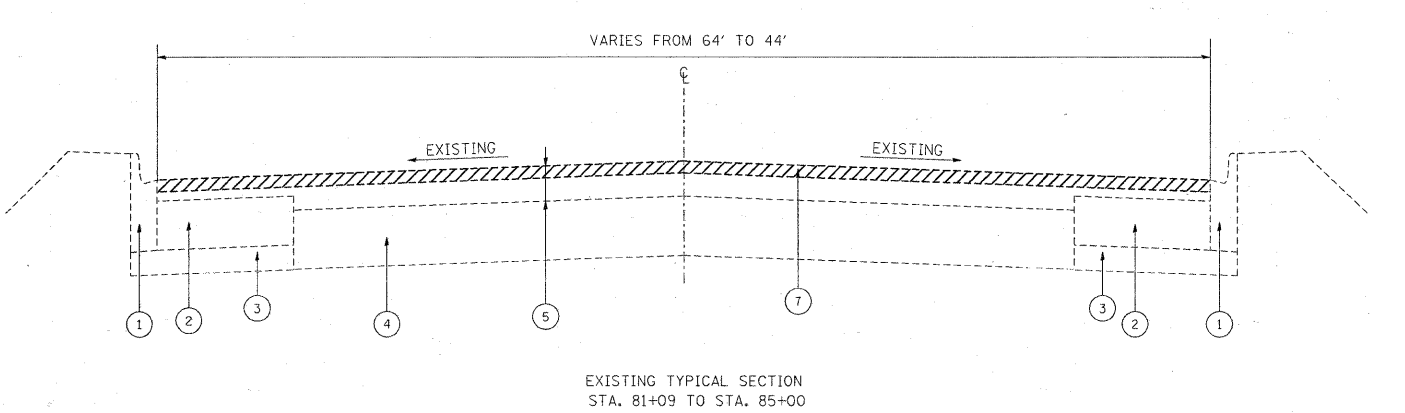
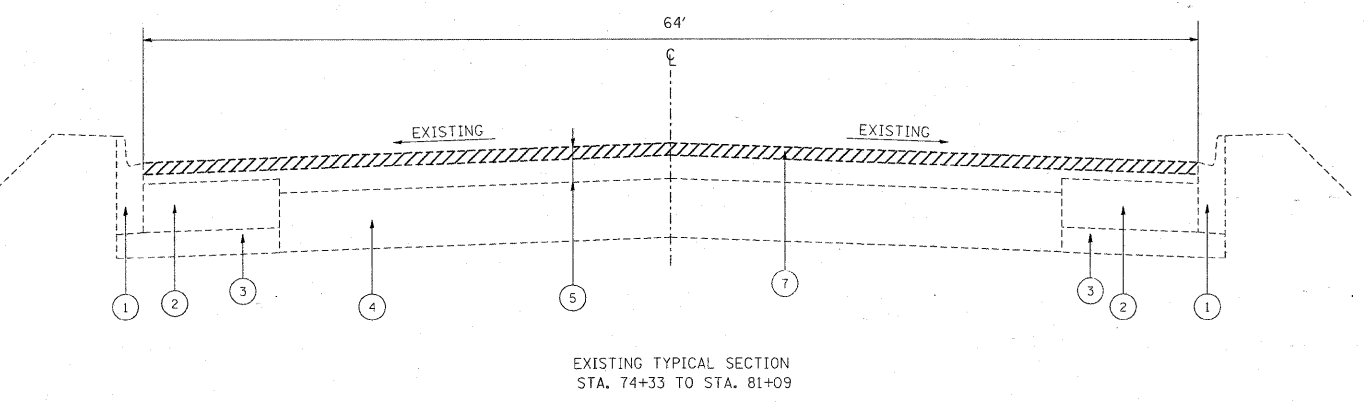
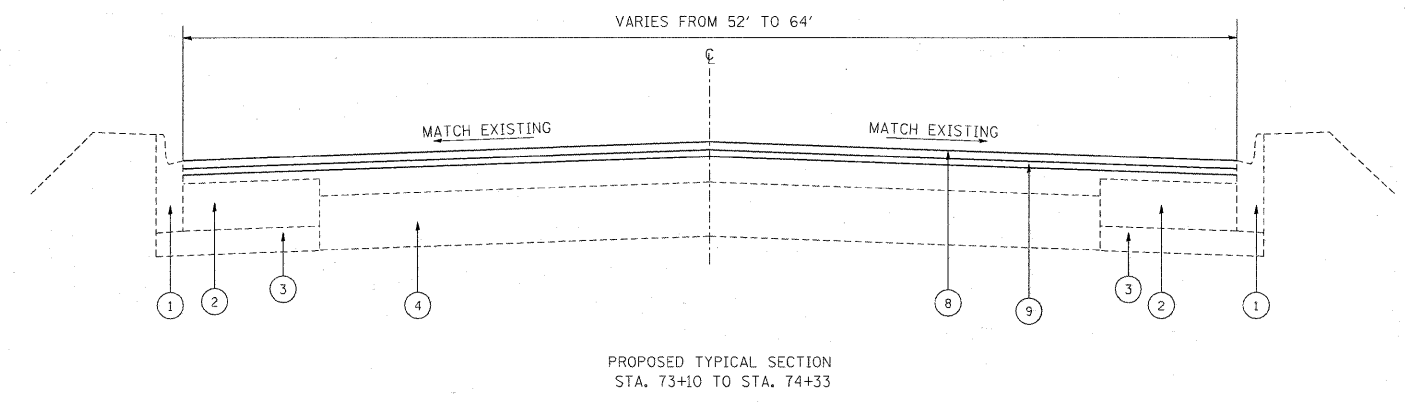
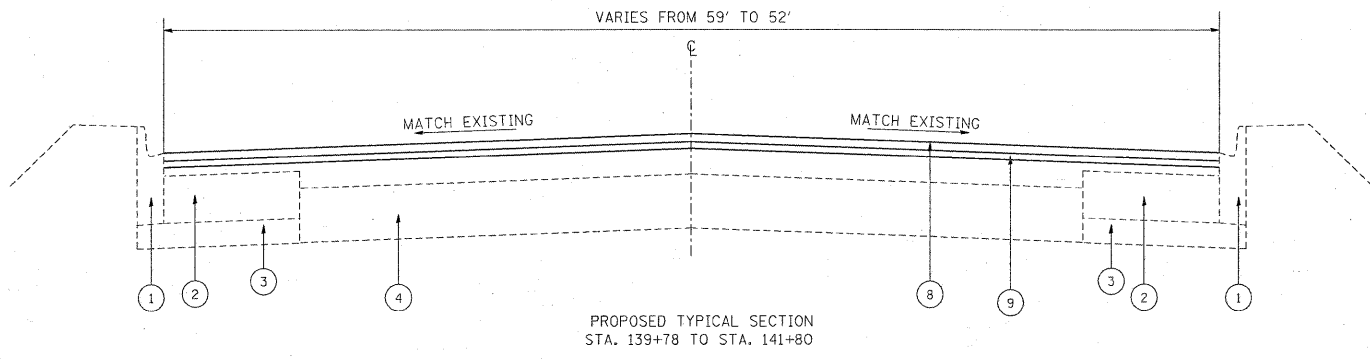
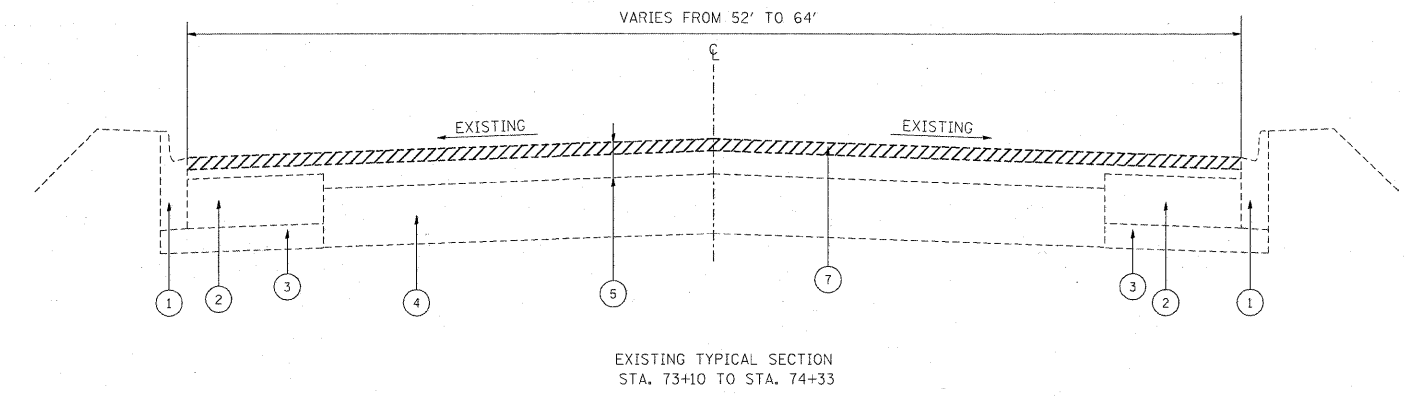
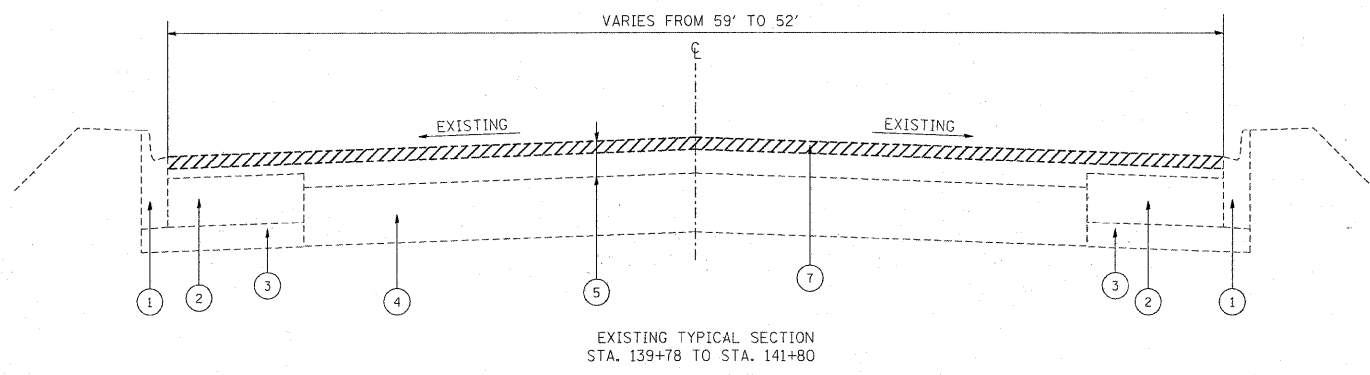
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

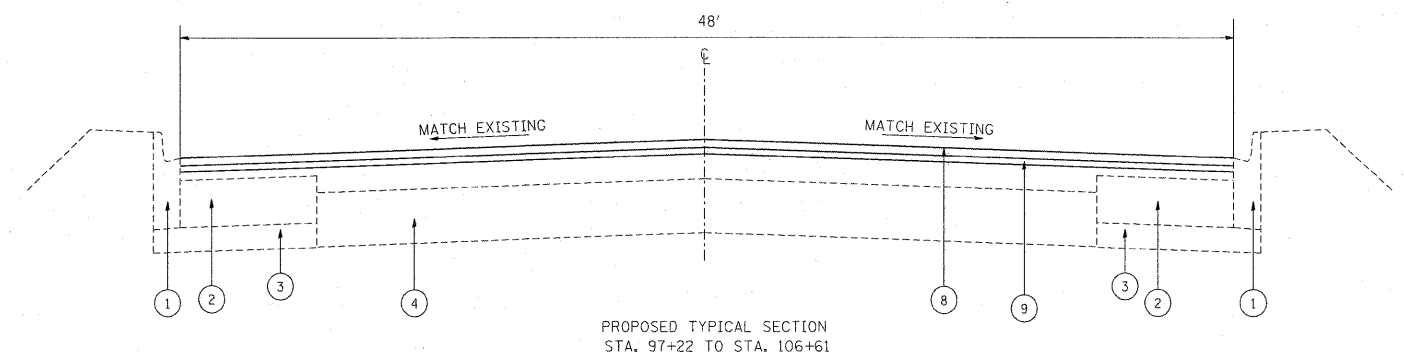
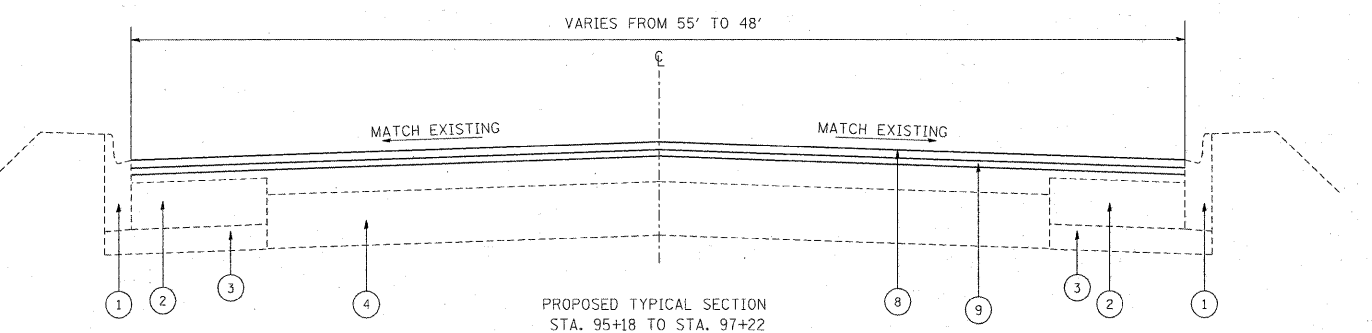
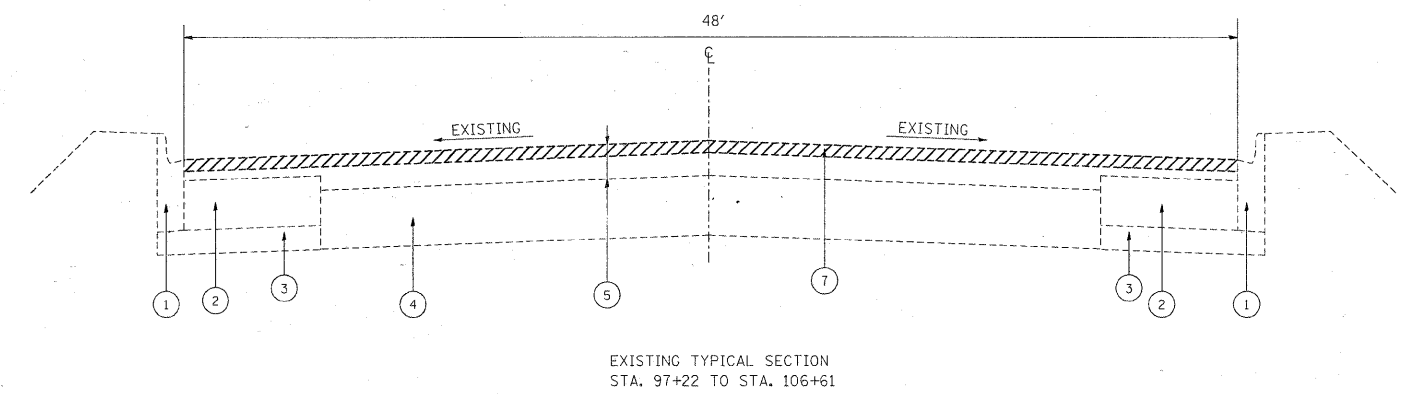
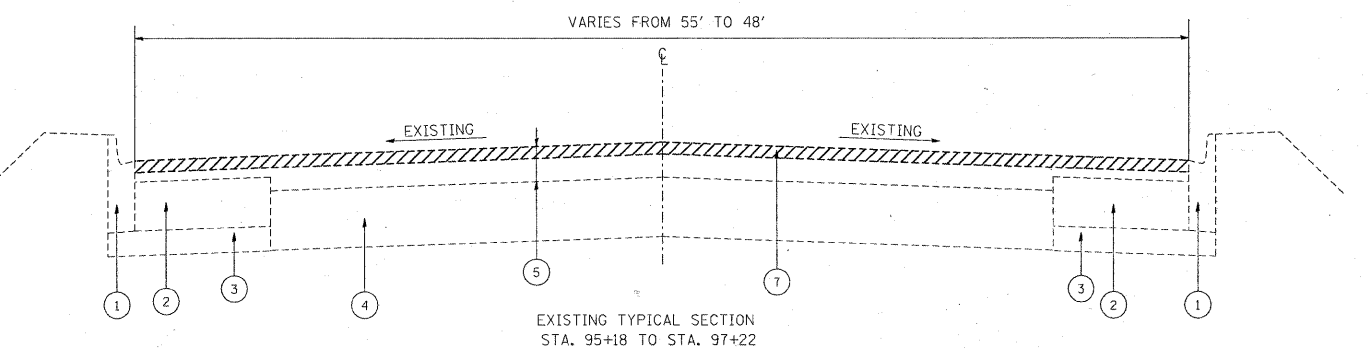
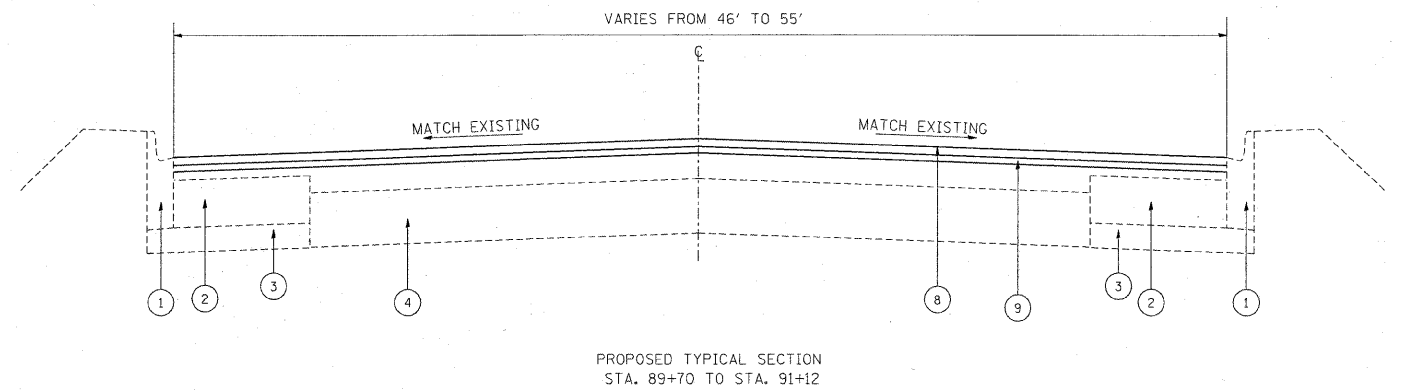
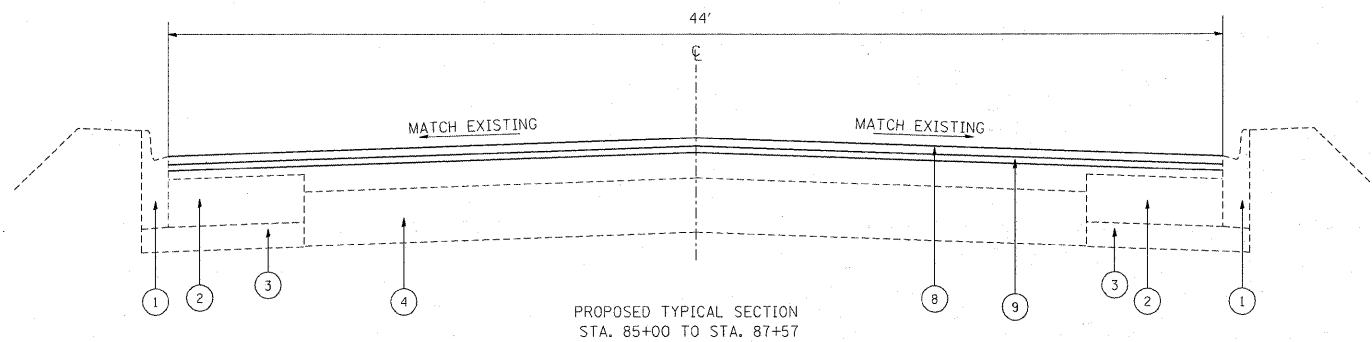
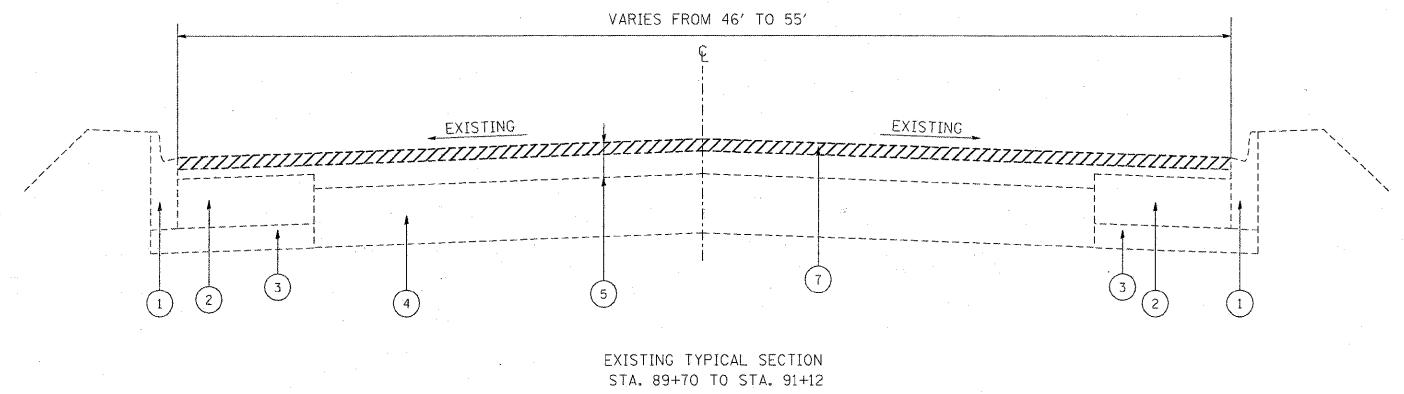
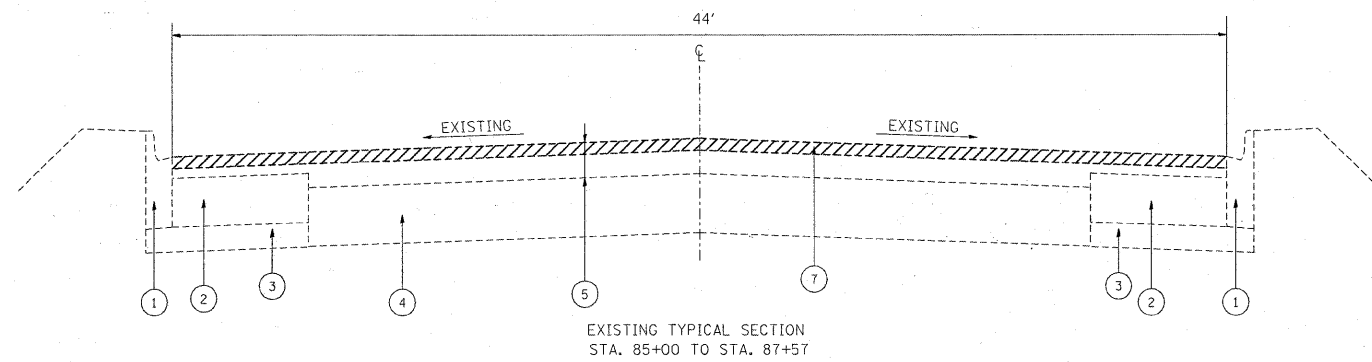
**U.S. ROUTE 12 (RAND RD.)
TYPICAL SECTIONS**

SCALE: NTS SHEET NO. OF SHEETS STA. X TO STA. X

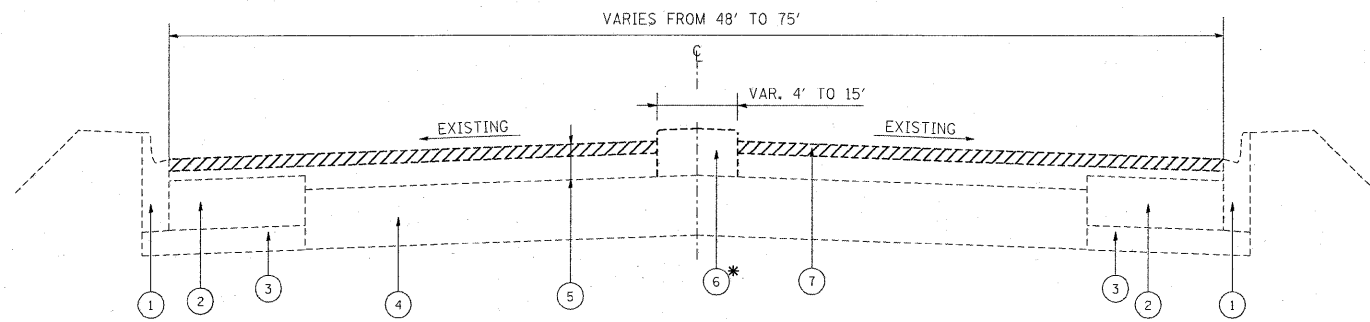
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	6
CONTRACT NO. 60E51				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = abraueh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 12 (RAND RD.) TYPICAL SECTIONS		F.A.U. RTE. 3523	SECTION 2008-021 RS	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 7	
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		CHECKED -	REVISED -							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
		DATE -	REVISED -									

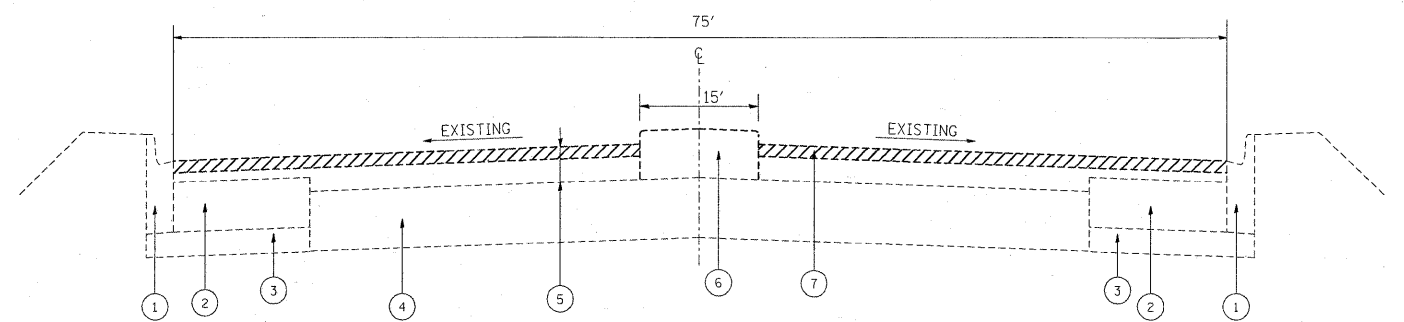


FILE NAME =	USER NAME = obreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	U.S. ROUTE 12 (RAND RD.) TYPICAL SECTIONS			F.A.U. RTE. 3523	SECTION 2008-021 RS	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 8
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		CHECKED -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED -									

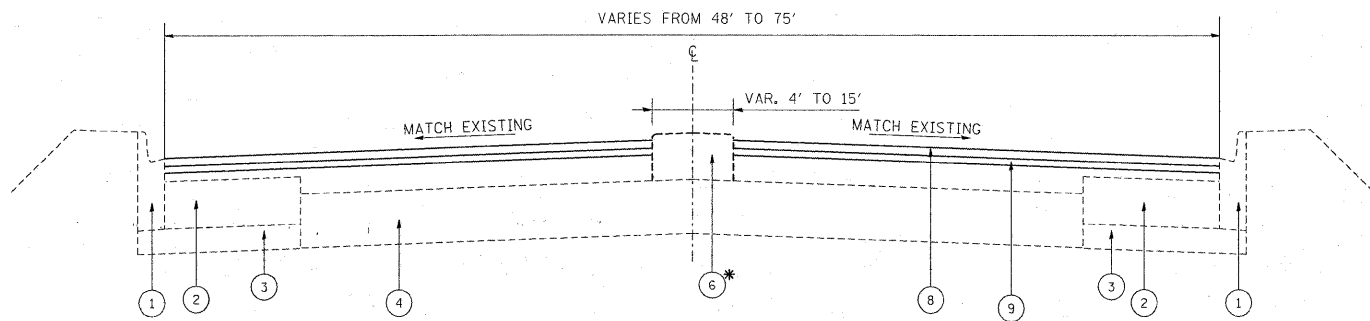


EXISTING TYPICAL SECTION
STA. 106+61 TO STA. 110+35

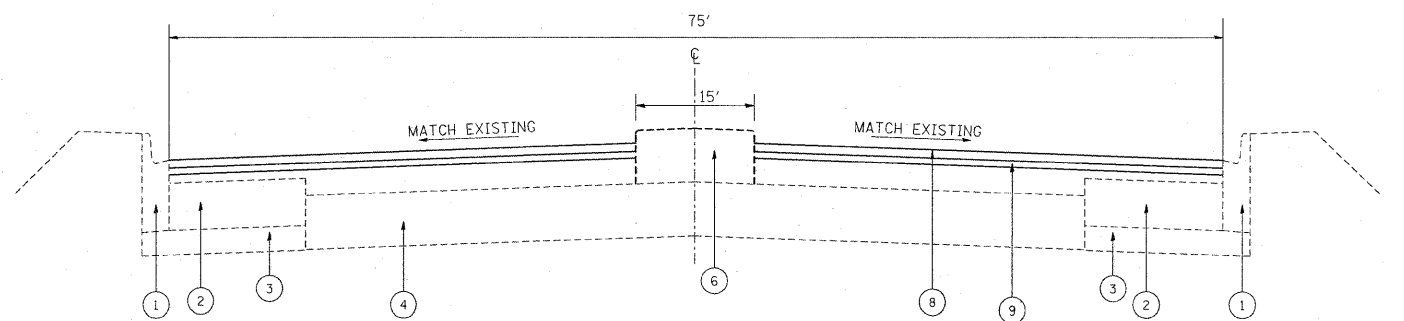
*PCC MEDIAN ONLY BETWEEN
STA. 107+40 AND STA. 110+35



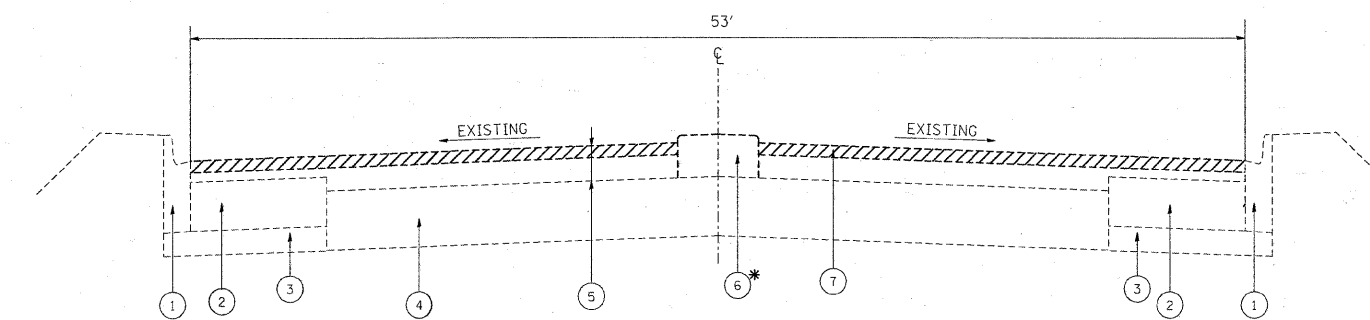
EXISTING TYPICAL SECTION
STA. 110+35 TO STA. 114+32



PROPOSED TYPICAL SECTION
STA. 106+61 TO STA. 110+35

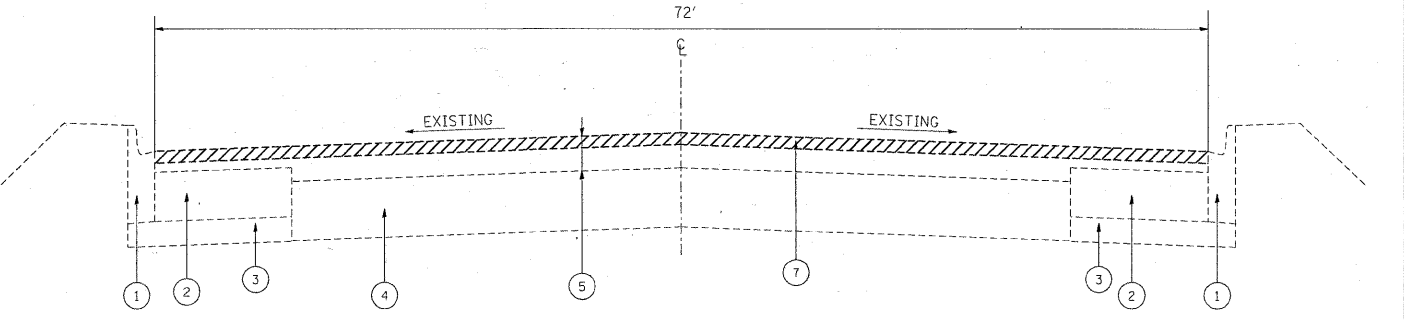


PROPOSED TYPICAL SECTION
STA. 110+35 TO STA. 114+32

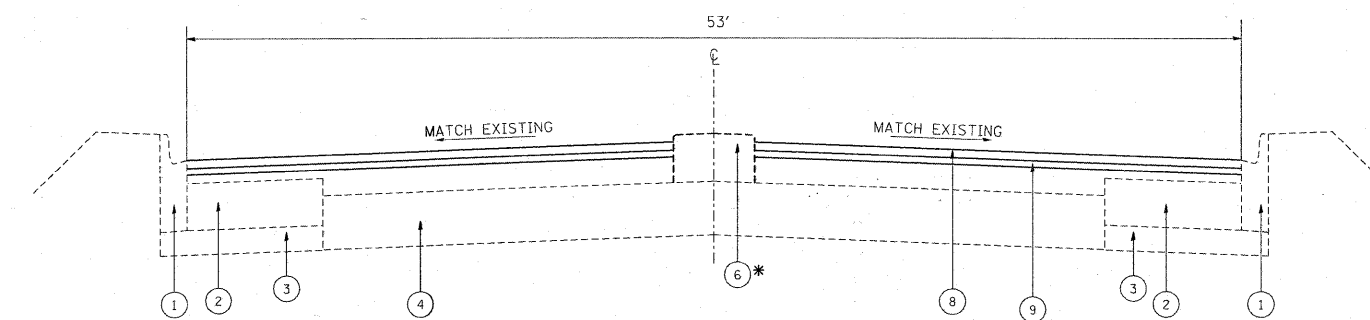


EXISTING TYPICAL SECTION
STA. 124+95 TO STA. 131+65

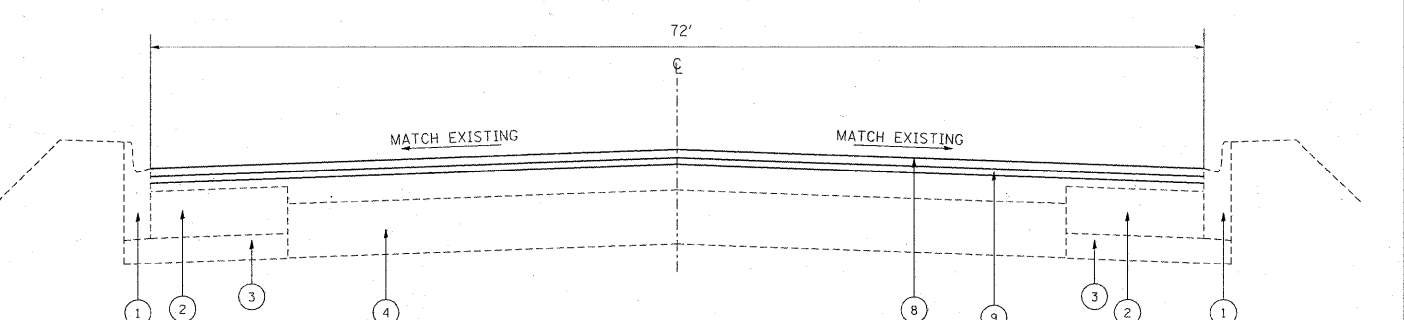
*CORRUGATED MEDIAN ENDS AT STA. 129+60
NOTE: MEDIAN REPAIR AT LOCATIONS TO BE DETERMINED
BY ENGINEER BETWEEN STA. 115+00 TO STA. 125+00



EXISTING TYPICAL SECTION
STA. 133+03 TO STA. 134+32



PROPOSED TYPICAL SECTION
STA. 124+95 TO STA. 131+65*



PROPOSED TYPICAL SECTION
STA. 133+03 TO STA. 134+32

FILE NAME =
c:\pw_work\pwtidot\abreuh\dms89779\011194-Design.dgn

USER NAME = abreuh
PLDT SCALE = 50,0000 ' / IN.
PLDT DATE = 1/26/2009

DESIGNED -
DRAWN -
CHECKED -
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. ROUTE 12 (RAND RD.)
TYPICAL SECTIONS**

SCALE: NTS SHEET NO. OF SHEETS STA. X TO STA. X

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	9
CONTRACT NO. 60E51				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

POT. STA. 0+00.00

3rd AVE.

U.S. RTE. 12
(RAND ROAD)

PROJECT BEGINS
STA. 5+37

PAVEMENT MARKING LEGEND

- ① 6" WHITE LINE
- ② WHITE LETTERS & SYMBOLS
- ③ 24" WHITE LINE
- ④ 4" SOLID DOUBLE YELLOW LINE
- ⑤ 12" YELLOW DIAGONAL LINE
- ⑥ 4" WHITE SKIP DASH LANE LINE
- ⑦ 4" YELLOW SKIP DASH CENTER LINE
- ⑧ 4" YELLOW SOLID SINGLE MEDIAN LINE
- ⑨ 16" WHITE LINE
- ⑩ 8" WHITE LINE
- ⑪ 12" WHITE LINE
- ⑫ 6" WHITE DOTTED EXTENSIONS
- ⑬ WHITE TWO WAY LEFT TURN

MEADOW DRIVE

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

U.S. RTE. 12
(RAND ROAD)

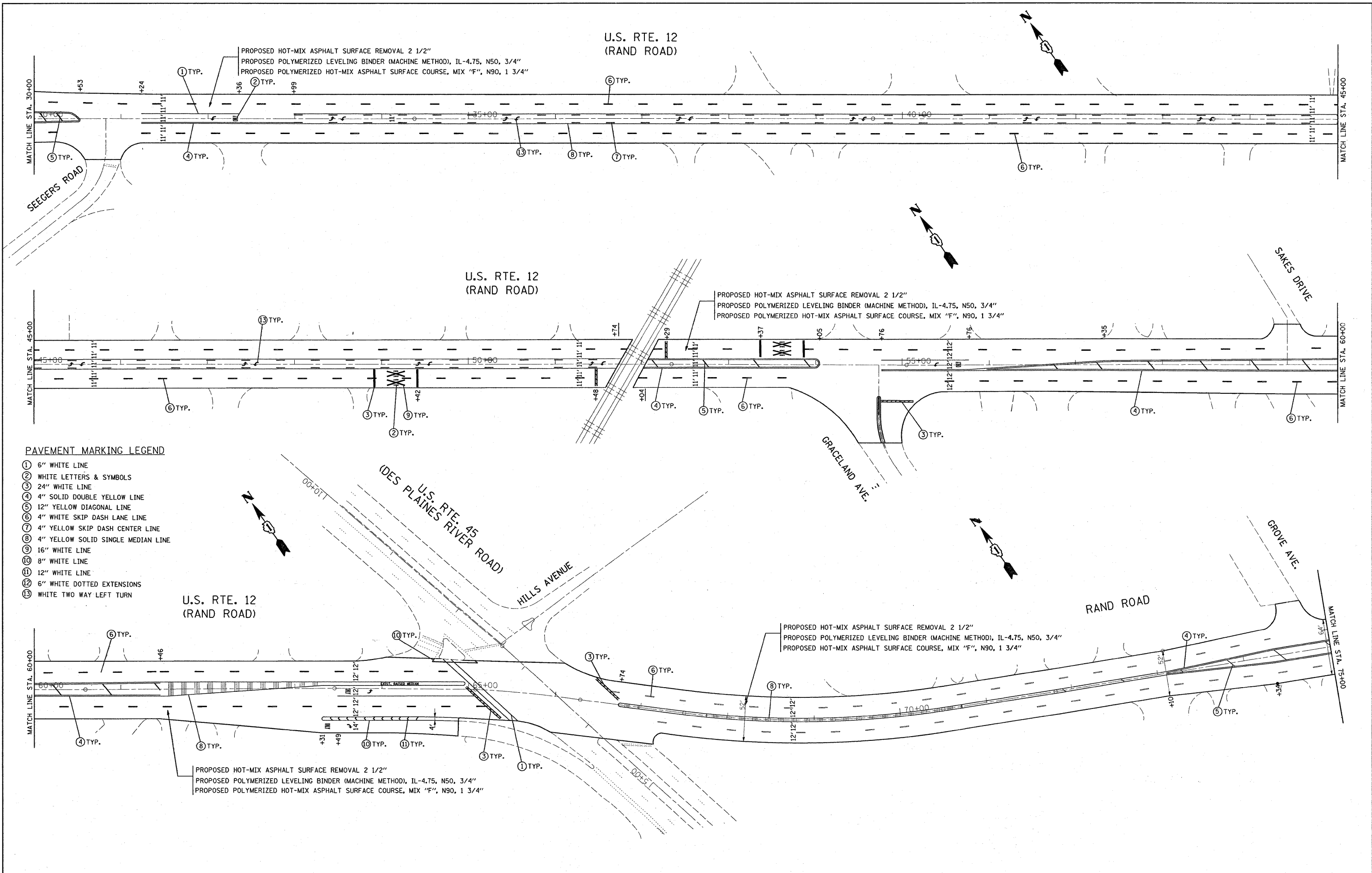
IL. RTE. 58
(GOLF ROAD)

PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

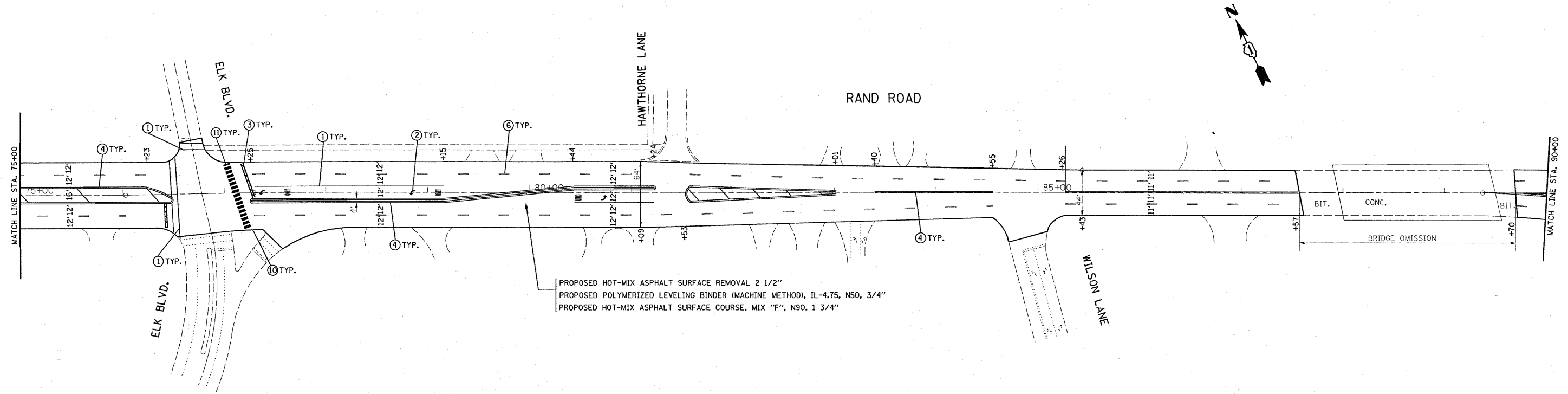
PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"

PROJECT OMISSION

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. ROUTE 12 (RAND RD.) 3rd AVE. TO LUNDERGAN AVE.		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pwsdot\abreuah\dms89779\dl1984-sht-plan.dgn		DRAWN -	REVISED -		3523	2008-021 RS	COOK	30	10		
PLOT SCALE = 50.00000 ' / IN.		CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60E51				
PLOT DATE = 1/26/2009		DATE -	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



FILE NAME =	USER NAME = abrauh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN U.S. ROUTE 12 (RAND RD.) 3rd AVE. TO LUNDERGAN AVE.		F.A.U. RTE. 3523	SECTION 2008-021 RS	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 11	
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	PLOT DATE = 1/25/2009	CHECKED -	REVISED -									
		DATE -	REVISED -									



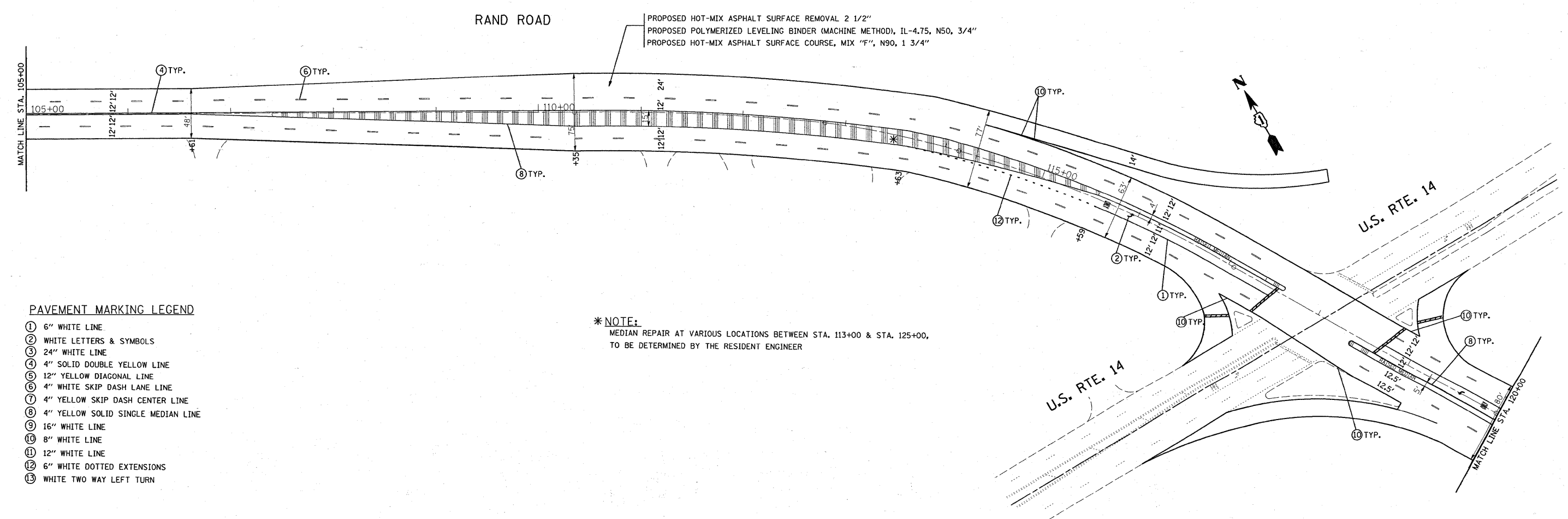
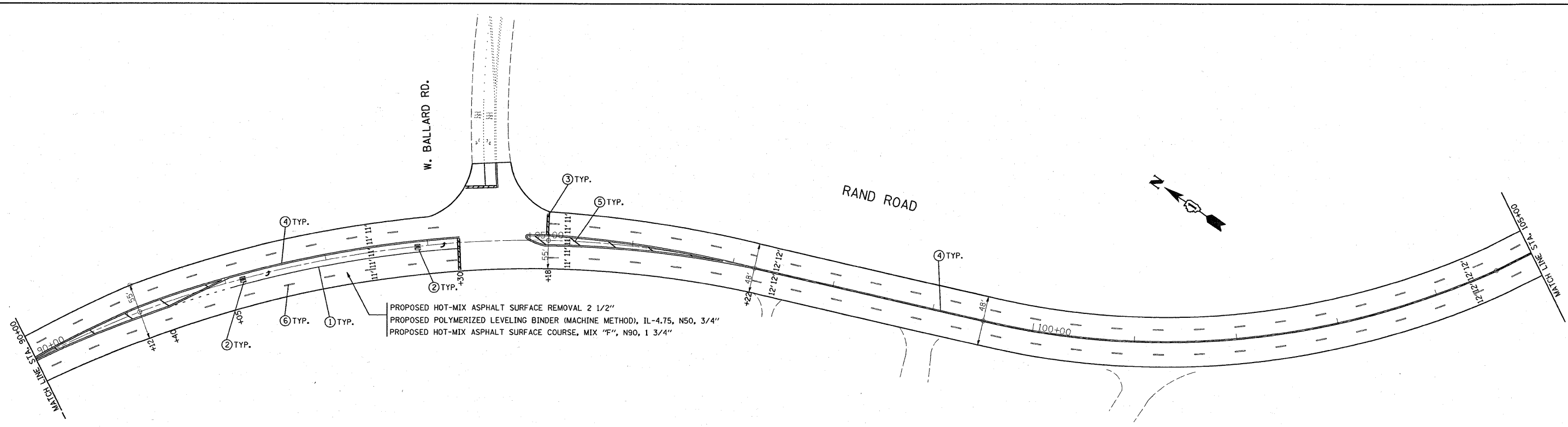
PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL 2 1/2"
 PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1 3/4"



PAVEMENT MARKING LEGEND

- ① 6" WHITE LINE
- ② WHITE LETTERS & SYMBOLS
- ③ 24" WHITE LINE
- ④ 4" SOLID DOUBLE YELLOW LINE
- ⑤ 12" YELLOW DIAGONAL LINE
- ⑥ 4" WHITE SKIP DASH LANE LINE
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- ⑨ 16" WHITE LINE
- ⑩ 8" WHITE LINE
- ⑪ 12" WHITE LINE
- ⑫ 6" WHITE DOTTED EXTENSIONS
- ⑬ WHITE TWO WAY LEFT TURN

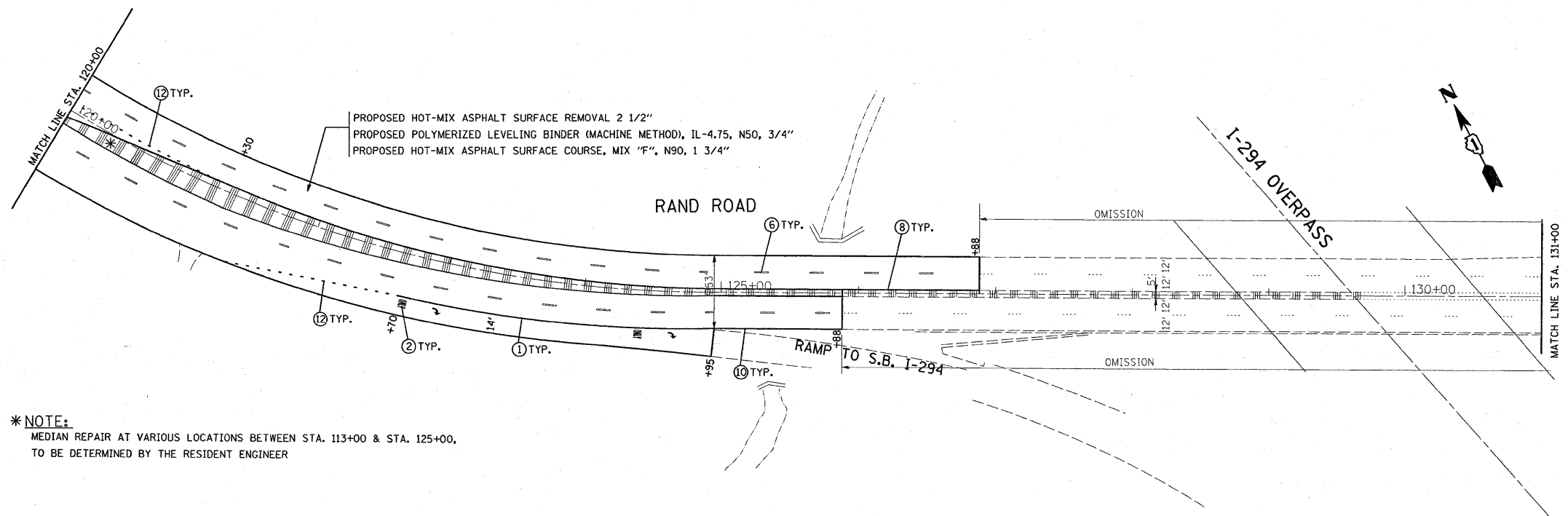
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	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -		3523	2008-021 RS	COOK	30	12			
PLOT DATE = 1/26/2009	CHECKED -	REVISED -	CONTRACT NO. 60E51		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
	DATE -	REVISED -	SCALE:		SHEET NO. OF SHEETS	STA. TO STA.						



- PAVEMENT MARKING LEGEND**
- ① 6" WHITE LINE
 - ② WHITE LETTERS & SYMBOLS
 - ③ 24" WHITE LINE
 - ④ 4" SOLID DOUBLE YELLOW LINE
 - ⑤ 12" YELLOW DIAGONAL LINE
 - ⑥ 4" WHITE SKIP DASH LANE LINE
 - ⑦ 4" YELLOW SKIP DASH CENTER LINE
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 - ⑨ 16" WHITE LINE
 - ⑩ 8" WHITE LINE
 - ⑪ 12" WHITE LINE
 - ⑫ 6" WHITE DOTTED EXTENSIONS
 - ⑬ WHITE TWO WAY LEFT TURN

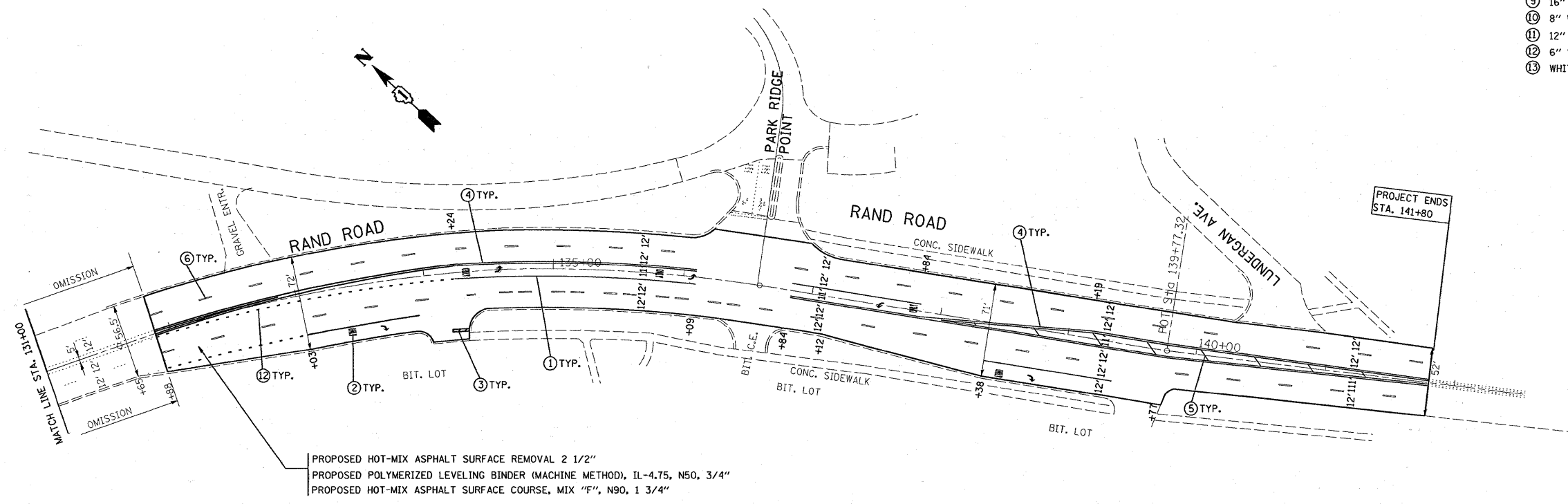
***NOTE:**
 MEDIAN REPAIR AT VARIOUS LOCATIONS BETWEEN STA. 113+00 & STA. 125+00,
 TO BE DETERMINED BY THE RESIDENT ENGINEER

FILE NAME =	USER NAME = abrouah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN U.S. ROUTE 12 (RAND RD.) 3RD AVE. TO LUNDERGAN AVE.			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -										CONTRACT NO. 60E51
		DATE -	REVISED -										FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



*NOTE:
 MEDIAN REPAIR AT VARIOUS LOCATIONS BETWEEN STA. 113+00 & STA. 125+00,
 TO BE DETERMINED BY THE RESIDENT ENGINEER

- PAVEMENT MARKING LEGEND
- ① 6" WHITE LINE
 - ② WHITE LETTERS & SYMBOLS
 - ③ 24" WHITE LINE
 - ④ 4" SOLID DOUBLE YELLOW LINE
 - ⑤ 12" YELLOW DIAGONAL LINE
 - ⑥ 4" WHITE SKIP DASH LANE LINE
 - ⑦ 4" YELLOW SKIP DASH CENTER LINE
 - ⑧ 4" YELLOW SOLID SINGLE MEDIAN LINE
 - ⑨ 16" WHITE LINE
 - ⑩ 8" WHITE LINE
 - ⑪ 12" WHITE LINE
 - ⑫ 6" WHITE DOTTED EXTENSIONS
 - ⑬ WHITE TWO WAY LEFT TURN

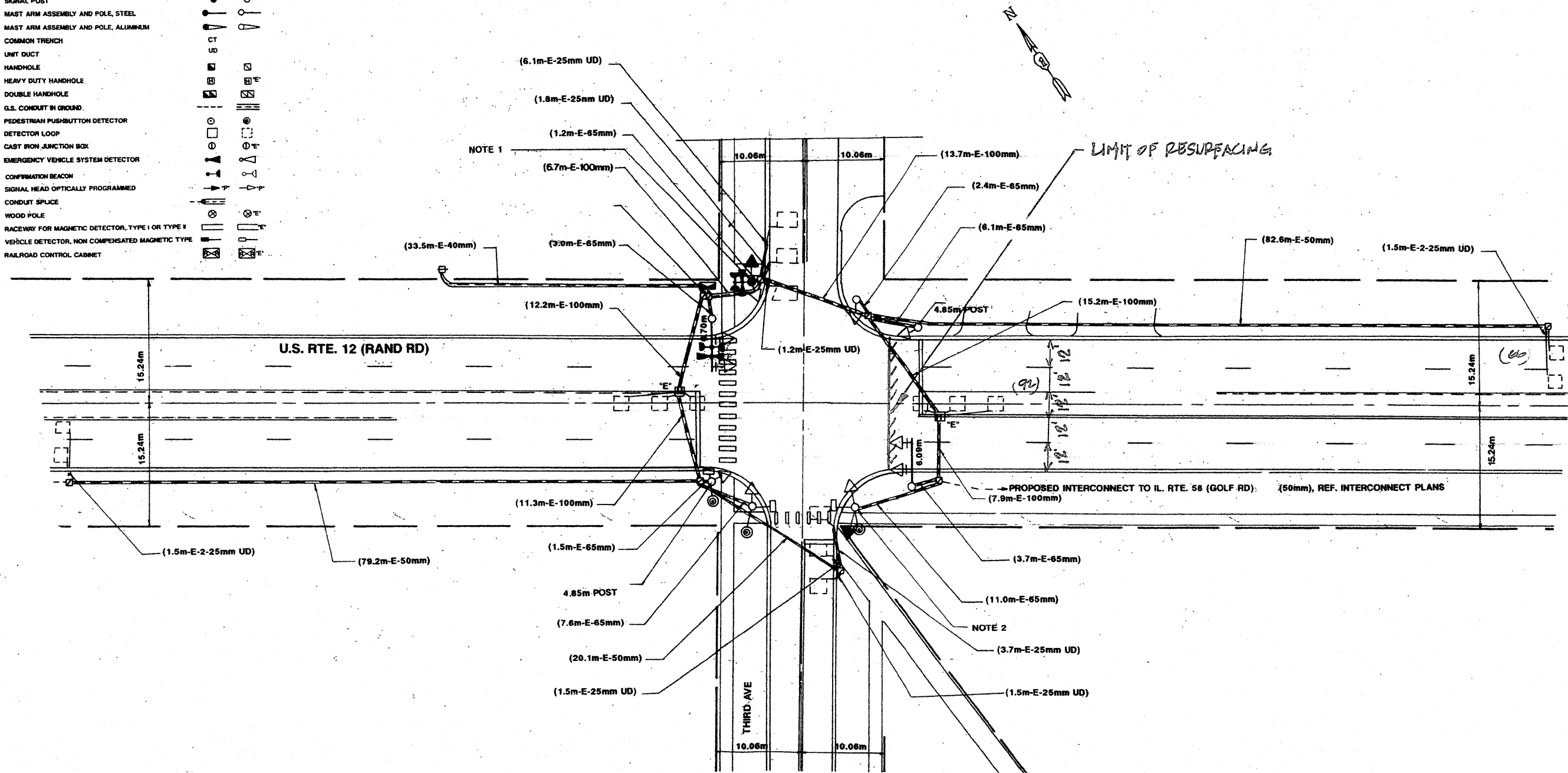


FILE NAME =	USER NAME = obreuh	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN U.S. ROUTE 12 (RAND RD.) 3RD AVE. TO LUNDERGAN AVE.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60E51					
PLOT DATE = 1/26/2009		DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF
 REPLACING THE DETECTOR LOOPS ONLY.
 ALL OTHER INFORMATION SHOWN IS NOT
 RELATED AND WILL BE DISREGARDED.

- TRAFFIC SIGNAL LEGEND**
- CONTROLLER
 - SERVICE INSTALLATION
 - SIGNAL HEAD
 - SIGNAL HEAD WITH BACKPLATE
 - SIGNAL HEAD, PEDESTRIAN
 - SIGNAL POST
 - MAST ARM ASSEMBLY AND POLE, STEEL
 - MAST ARM ASSEMBLY AND POLE, ALUMINUM
 - COMMON TRENCH
 - UNIT DUCT
 - HANDHOLE
 - HEAVY DUTY HANDHOLE
 - DOUBLE HANDHOLE
 - G.S. CONDUIT IN GROUND
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - DETECTOR LOOP
 - CAST IRON JUNCTION BOX
 - EMERGENCY VEHICLE SYSTEM DETECTOR
 - CONFIRMATION BEACON
 - SIGNAL HEAD OPTICALLY PROGRAMMED
 - CONDUIT SPLICE
 - WOOD POLE
 - RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
 - VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
 - RAILROAD CONTROL CABINET

- TRAFFIC SIGNAL LEGEND CONTINUED**
- ILLUMINATED SIGN, FIBER OPTIC
"NO LEFT TURN"
 - ILLUMINATED SIGN, FIBER OPTIC
"NO RIGHT TURN"
 - TELEPHONE CONNECTION



REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
80000600	158	Foot	Detector Loop Replacement

FILE NAME =	USER NAME = kentophixeybc	DESIGNED -	REVISED -
c:\projects\traffic\1070022\11581forestry.dgn		DRAWN - J.E.	REVISED -
PLOT SCALE = 40.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 10/5/2007		DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

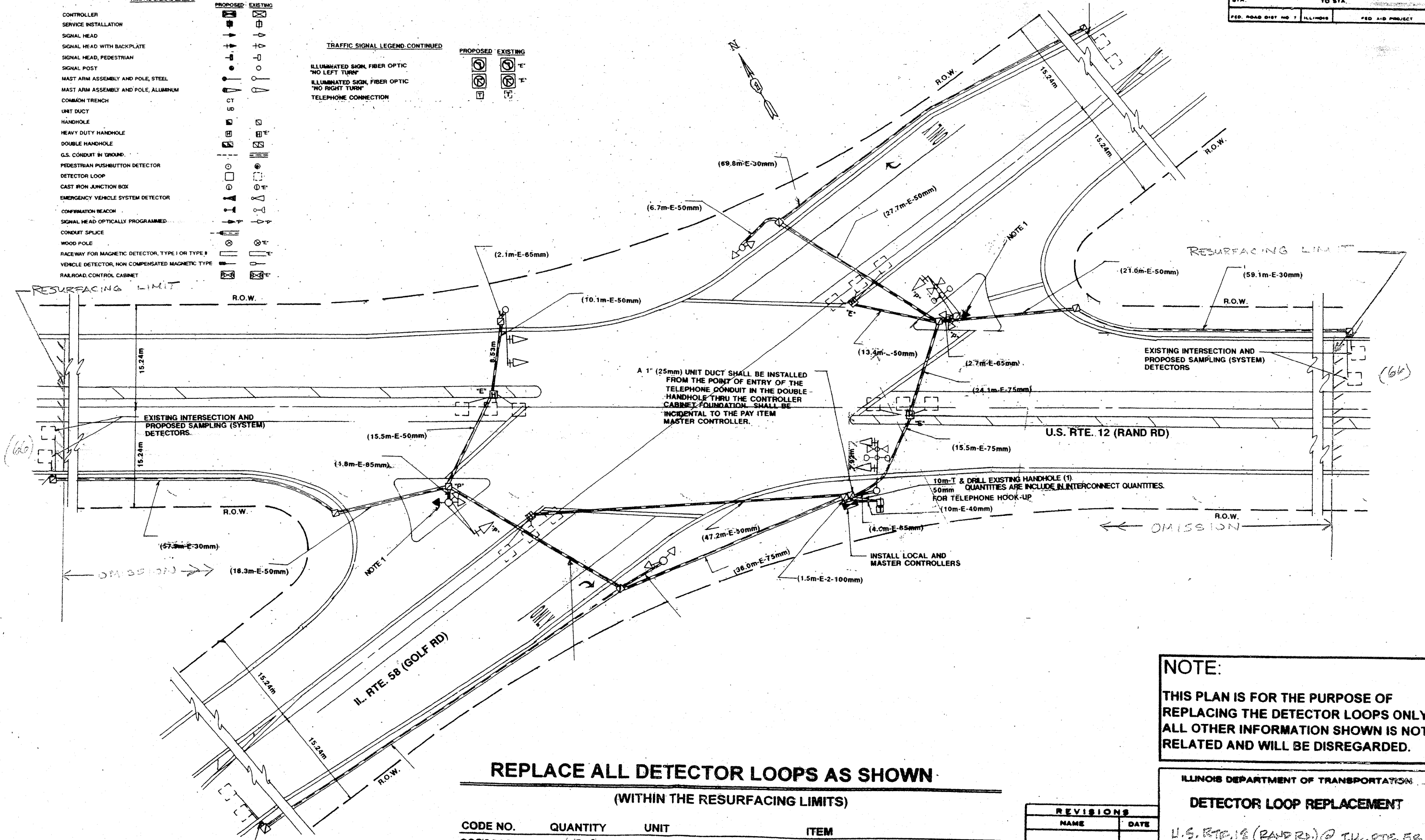
U.S. ROUTE 12 (RAND RD.) @ THIRD AVENUE

F.A.U./PROJECT RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	15
SCALE:		SHEET NO. OF SHEETS		STA. TO STA.
		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT

CONTRACT NO. 60E81

- TRAFFIC SIGNAL LEGEND**
- CONTROLLER
 - SERVICE INSTALLATION
 - SIGNAL HEAD
 - SIGNAL HEAD WITH BACKPLATE
 - SIGNAL HEAD, PEDESTRIAN
 - SIGNAL POST
 - MAST ARM ASSEMBLY AND POLE, STEEL
 - MAST ARM ASSEMBLY AND POLE, ALUMINUM
 - COMMON TRENCH
 - UNIT DUCT
 - HANDHOLE
 - HEAVY DUTY HANDHOLE
 - DOUBLE HANDHOLE
 - G.S. CONDUIT IN GROUND
 - PEDESTRIAN PUSHBUTTON DETECTOR
 - DETECTOR LOOP
 - CAST IRON JUNCTION BOX
 - EMERGENCY VEHICLE SYSTEM DETECTOR
 - CONFIRMATION BEACON
 - SIGNAL HEAD OPTICALLY PROGRAMMED
 - CONDUIT SPLICE
 - WOOD POLE
 - RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
 - VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
 - RAILROAD CONTROL CABINET

- TRAFFIC SIGNAL LEGEND-CONTINUED**
- ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
 - ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
 - TELEPHONE CONNECTION



NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

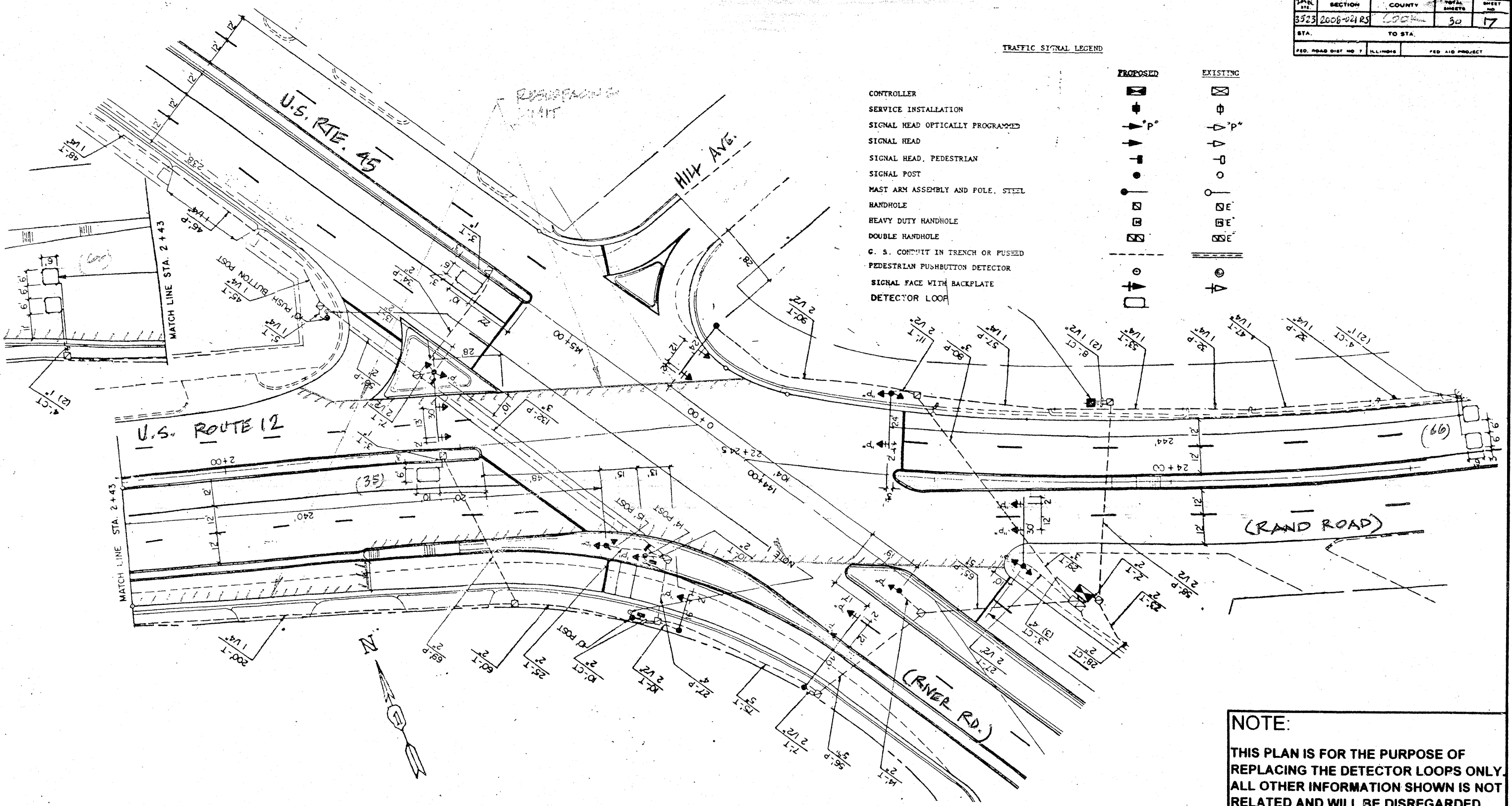
CODE NO.	QUANTITY	UNIT	ITEM
86600600	132	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
U.S. RTE. 12 (RAND RD) @ IL. RTE. 58
SCALE: NONE
DATE: JUNE 08
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD.

TRAFFIC SIGNAL LEGEND

- | | | |
|-----------------------------------|-----------------|-----------------|
| | PROPOSED | EXISTING |
| CONTROLLER | | |
| SERVICE INSTALLATION | | |
| SIGNAL HEAD OPTICALLY PROGRAMMED | | |
| SIGNAL HEAD | | |
| SIGNAL HEAD, PEDESTRIAN | | |
| SIGNAL POST | | |
| MAST ARM ASSEMBLY AND POLE, STEEL | | |
| HANDHOLE | | |
| HEAVY DUTY HANDHOLE | | |
| DOUBLE HANDHOLE | | |
| C. S. CONDUIT IN TRENCH OR PUSHD | | |
| PEDESTRIAN PUSHBUTTON DETECTOR | | |
| SIGNAL FACE WITH BACKPLATE | | |
| DETECTOR LOOP | | |



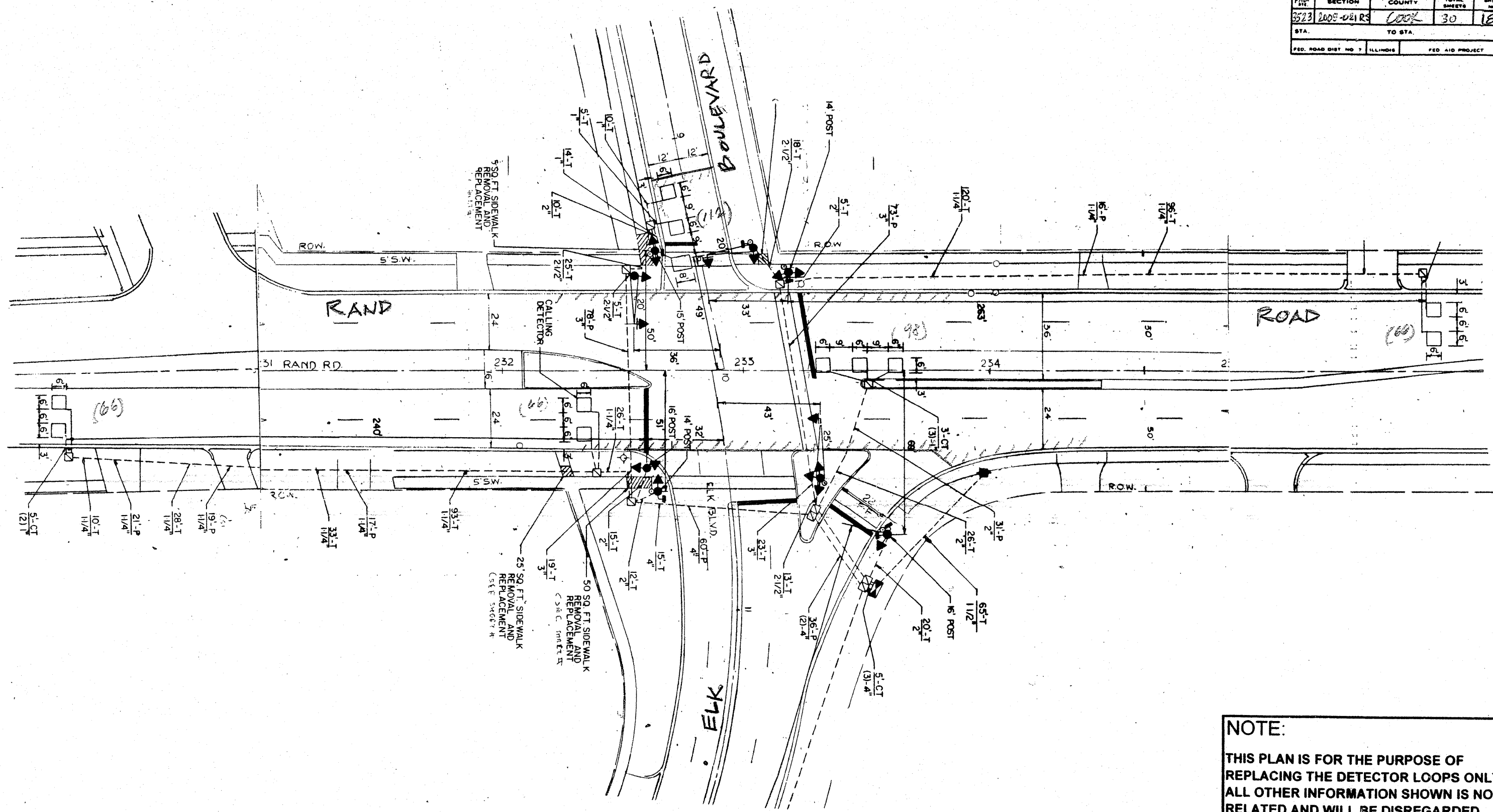
REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	167	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

NOTE:
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
U.S. RTE. 12 (RAND RD.) @ U.S. RTE. 45
SCALE: NONE
DATE: JUNE 08
DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD.



REPLACE ALL DETECTOR LOOPS AS SHOWN

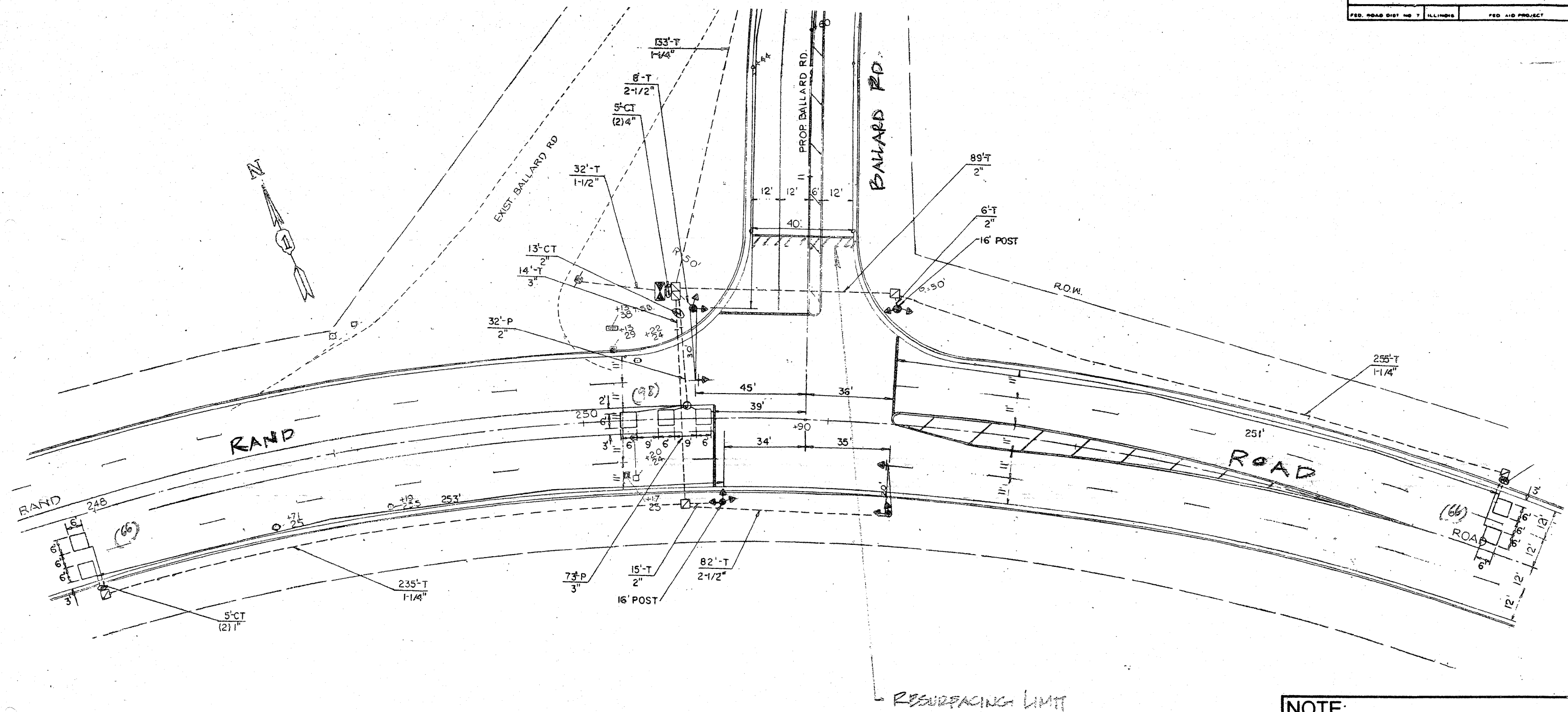
(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	408	Foot	Detector Loop Replacement

NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 RAND ROAD @ ELK BLVD.
 SCALE: NONE
 DATE: JUNE 08
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD.



NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	230	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 RAND ROAD @ BALLARD ROAD

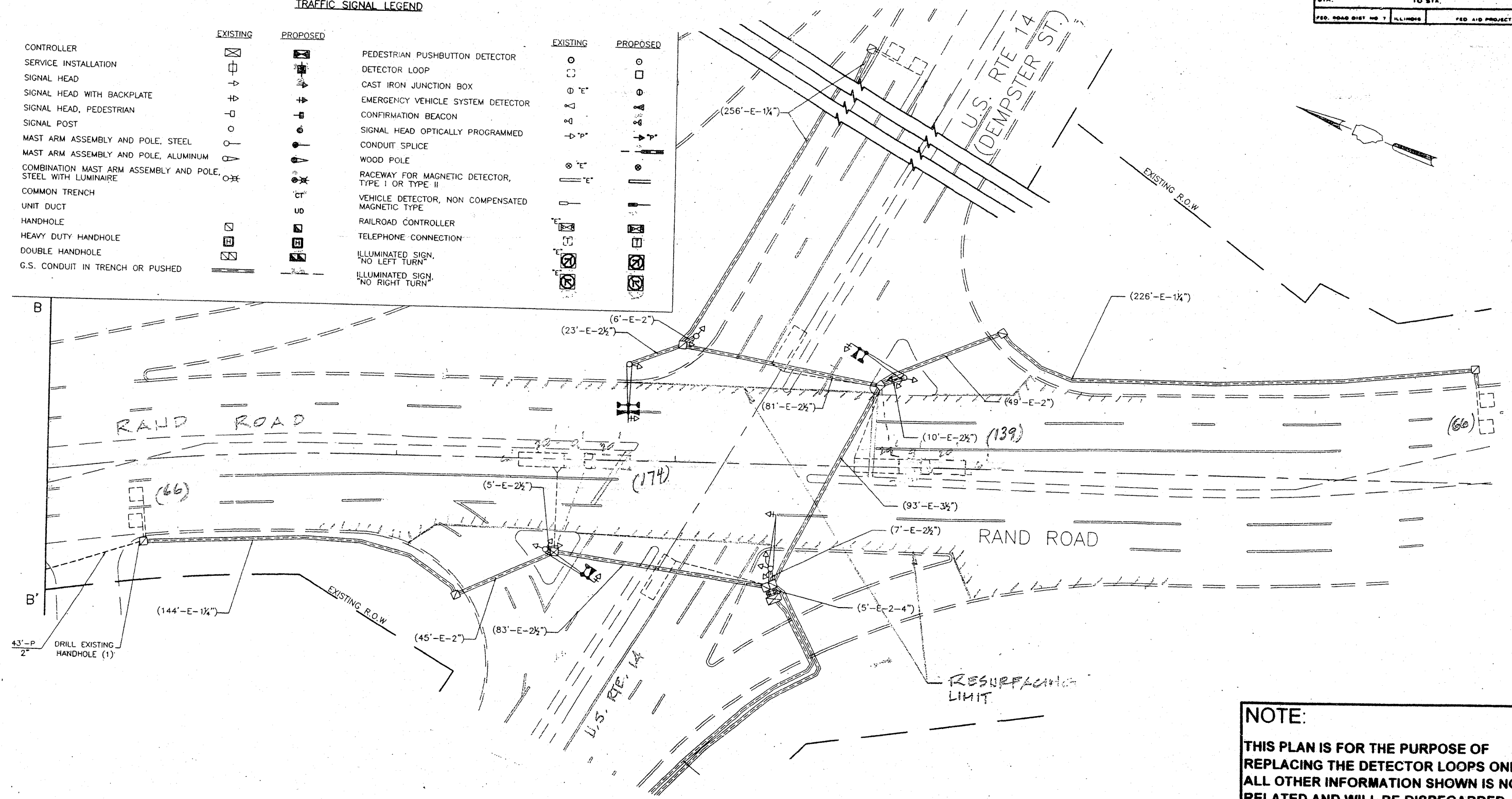
SCALE: NONE
 DATE: JUNE 08

DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: DAD.

PLAN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
8523	2008-021RS	COLE	30	20
STA.		TO STA.		
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT

TRAFFIC SIGNAL LEGEND

CONTROLLER	EXISTING	PROPOSED	PEDESTRIAN PUSHBUTTON DETECTOR	EXISTING	PROPOSED
SERVICE INSTALLATION			DETECTOR LOOP		
SIGNAL HEAD			CAST IRON JUNCTION BOX		
SIGNAL HEAD WITH BACKPLATE			EMERGENCY VEHICLE SYSTEM DETECTOR		
SIGNAL HEAD, PEDESTRIAN			CONFIRMATION BEACON		
SIGNAL POST			SIGNAL HEAD OPTICALLY PROGRAMMED		
MAST ARM ASSEMBLY AND POLE, STEEL			CONDUIT SPLICE		
MAST ARM ASSEMBLY AND POLE, ALUMINUM			WOOD POLE		
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE			RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
COMMON TRENCH			VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
UNIT DUCT			RAILROAD CONTROLLER		
HANDHOLE			TELEPHONE CONNECTION		
HEAVY DUTY HANDHOLE			ILLUMINATED SIGN, "NO LEFT TURN"		
DOUBLE HANDHOLE			ILLUMINATED SIGN, "NO RIGHT TURN"		
G.S. CONDUIT IN TRENCH OR PUSHED					



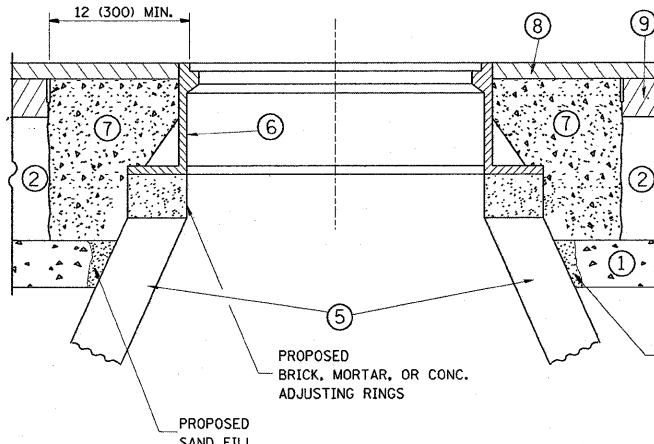
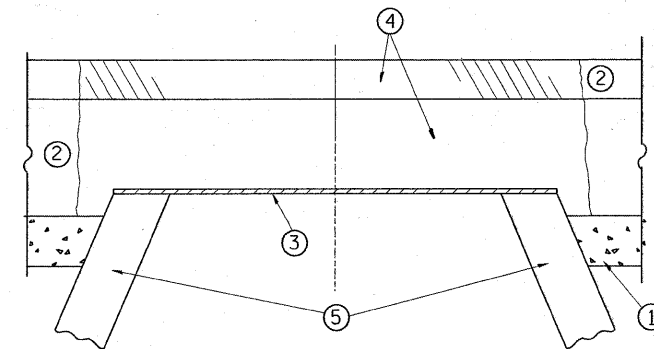
NOTE:
 THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REPLACE ALL DETECTOR LOOPS AS SHOWN
 (WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	445	Foot	Detector Loop Replacement

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETECTOR LOOP REPLACEMENT
 RAND ROAD @ U.S. RTE. 14.
 SCALE: AS SHOWN
 DATE: JUNE 2008
 DRAWN BY: JHE
 DESIGNED BY: JHE
 CHECKED BY: D.A.D.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

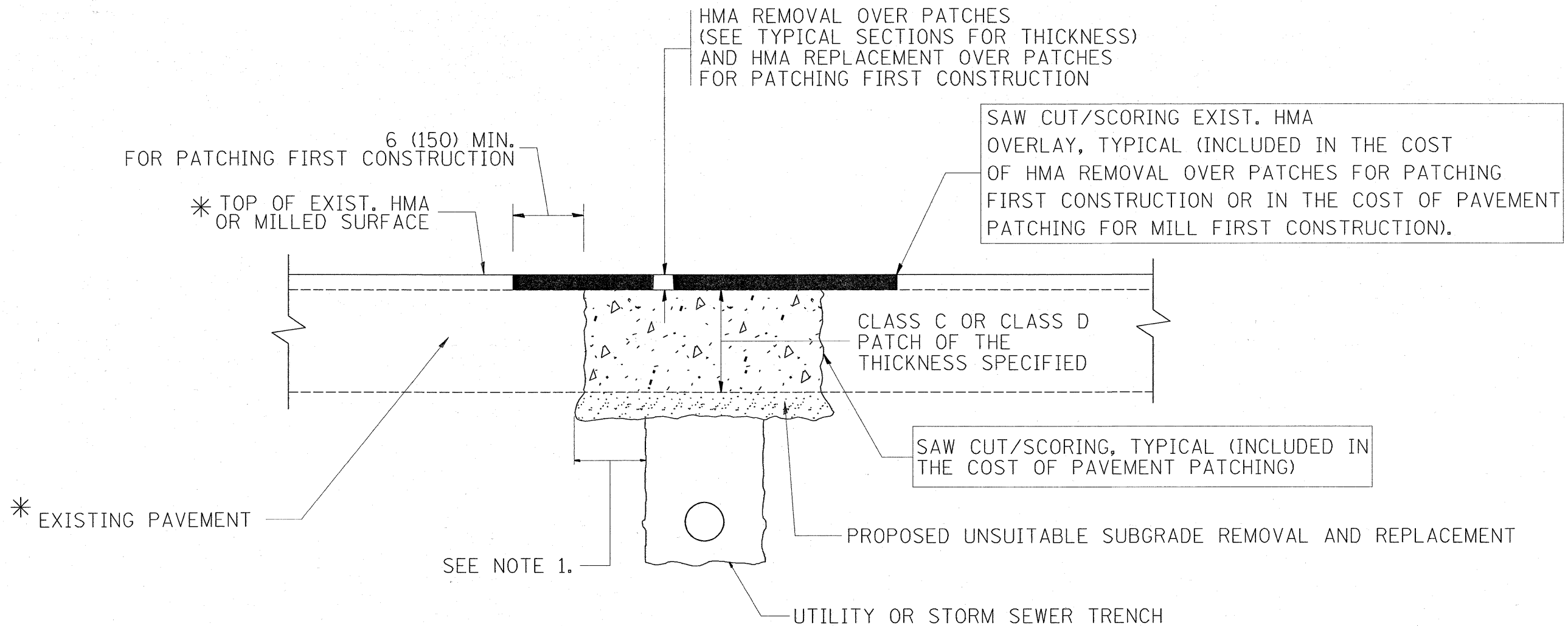
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = abreuah	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\pwwork\abreuah\dms89779\011184-01stStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	3523	2008-021 RS	COOK	30	21
		PLOT SCALE = 50.0000' / IN.	REVISED - R. WIEDEMAN 05-14-04						BD600-03 (BD-8)				
		PLOT DATE = 1/26/2009	REVISED - R. BORO 01-01-07						CONTRACT NO. 60E51				
								FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abreuh	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pawdot\obreuh\dms89779\0111194-DistStd.dgn	DRAWN -	REVISED - R. BORO 01-01-07	3523			2008-021 RS	COOK	30	22	
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07	BD400-04 (BD-22)			CONTRACT NO. 60E51				
PLOT DATE = 1/26/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

T/2 *

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

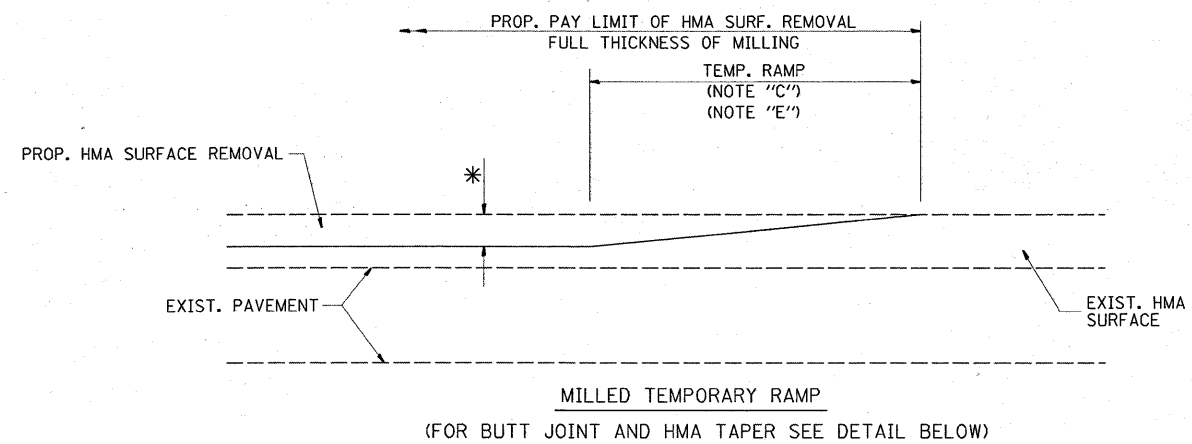
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

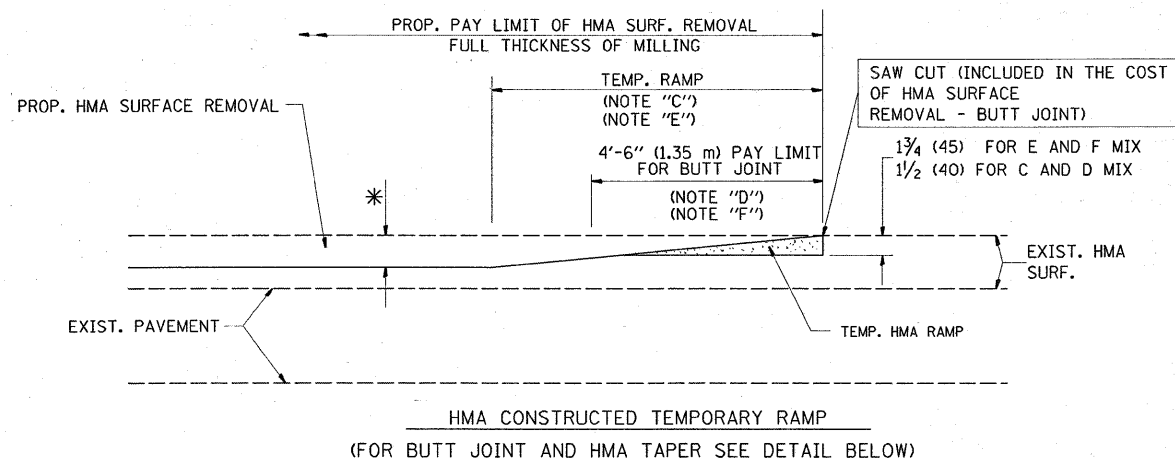
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

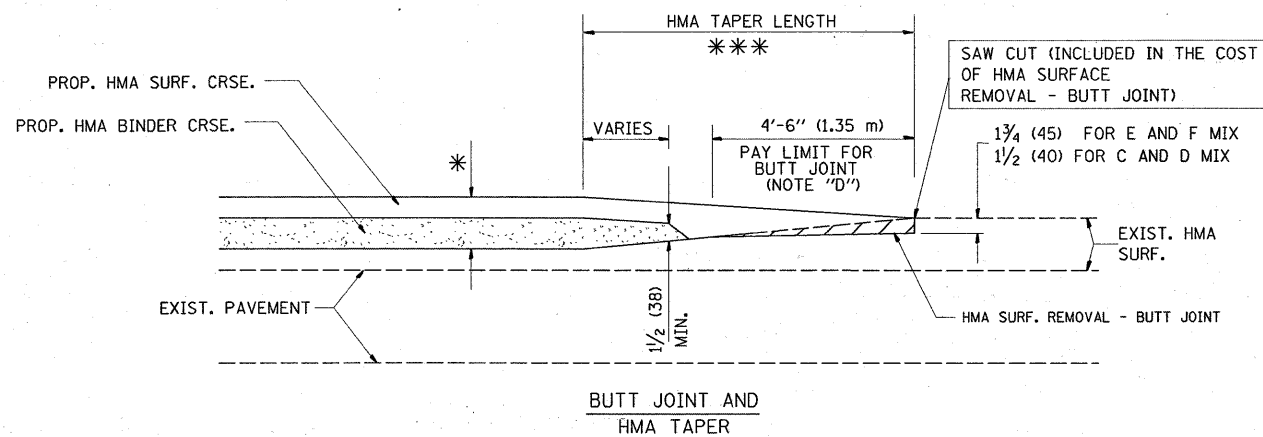
FILE NAME =	USER NAME = abreuh	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\pwidot\obreuh\dms89779\0111924-01st5Std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			3523	2008-021 RS	COOK	30	23
PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 60E51		
PLOT DATE = 1/26/2009		DATE - 03-11-94	REVISED - R. BORO 01-01-07			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



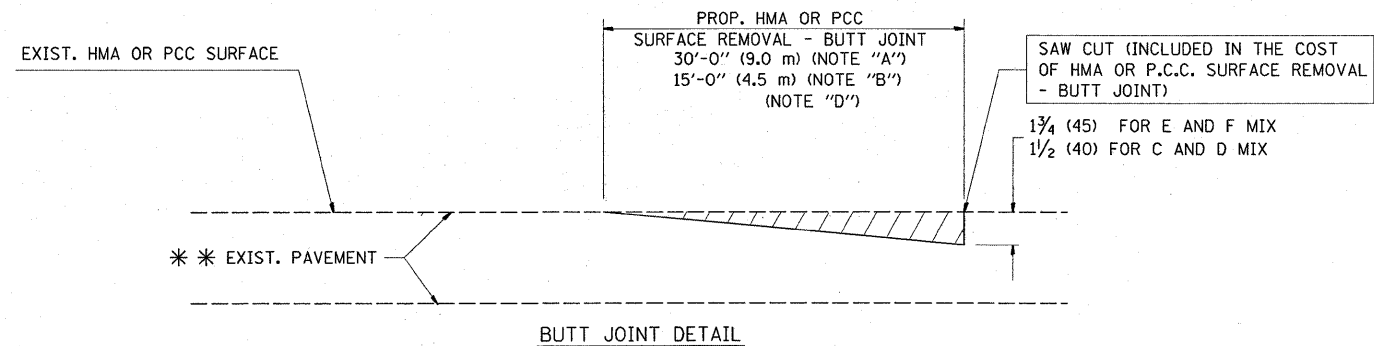
OPTION 1



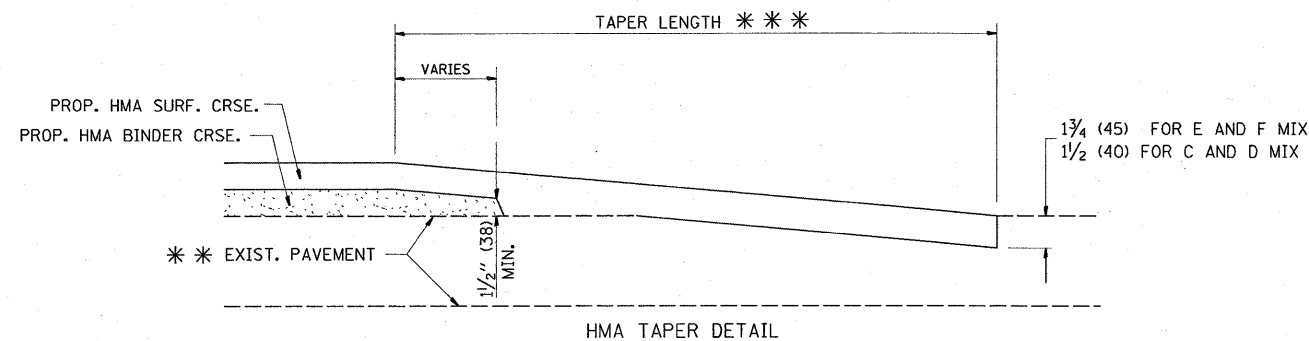
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

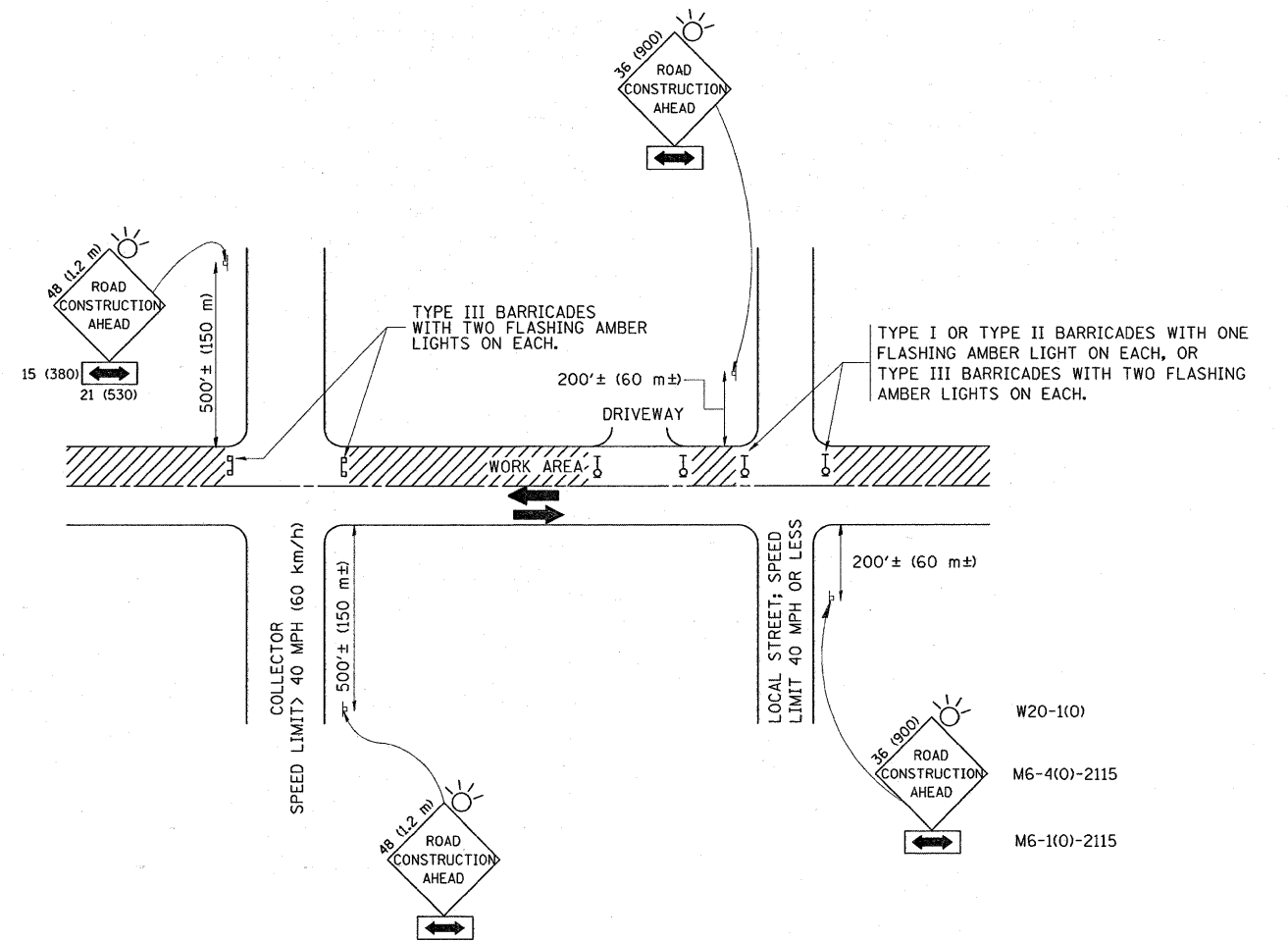
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ct:\pw_work\pwidot\abreuah\dms89779\011194-DistStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 1/26/2009		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	24
BD400-05 BD32			CONTRACT NO. 60E51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

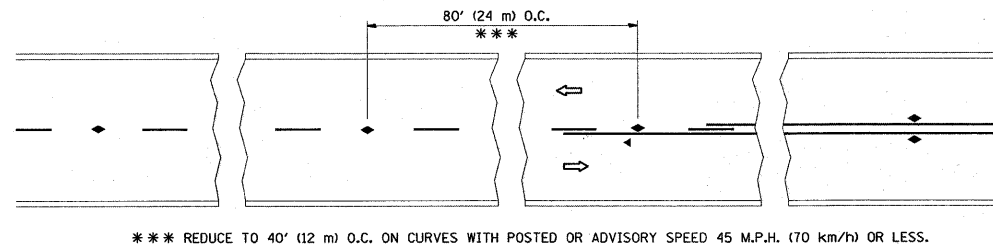
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

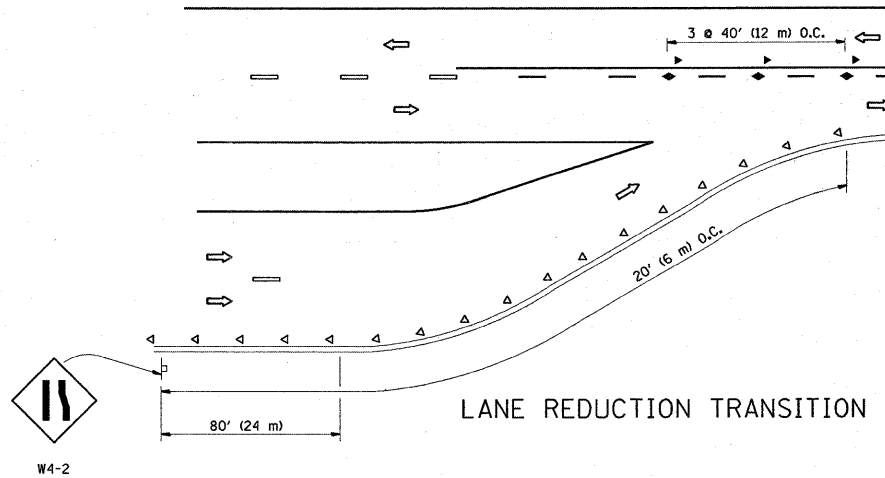
TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

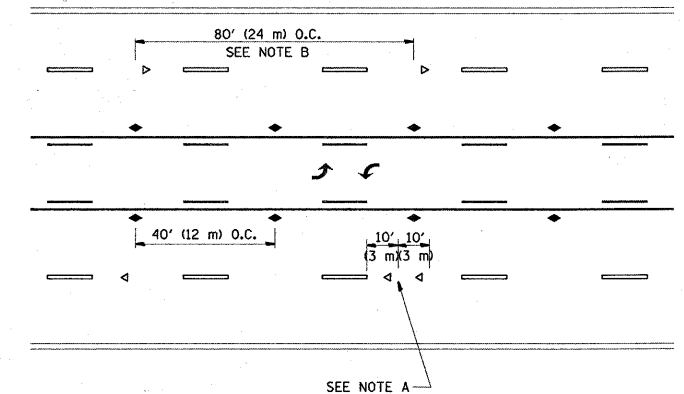
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	25
TC-10		CONTRACT NO. 60E51		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



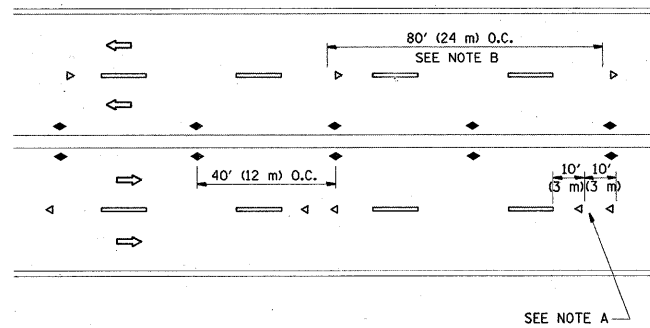
TWO-LANE/TWO-WAY



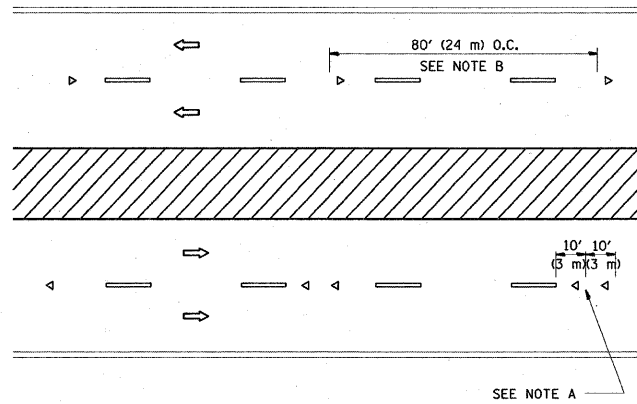
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

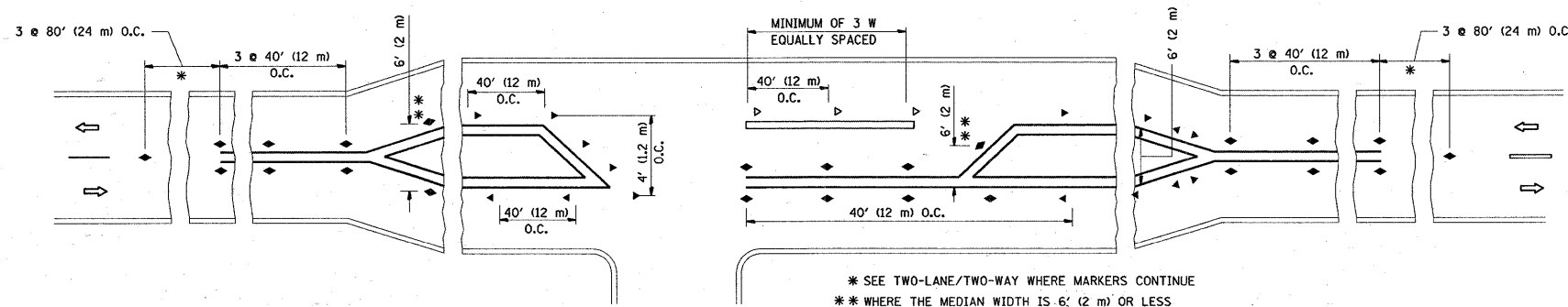
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

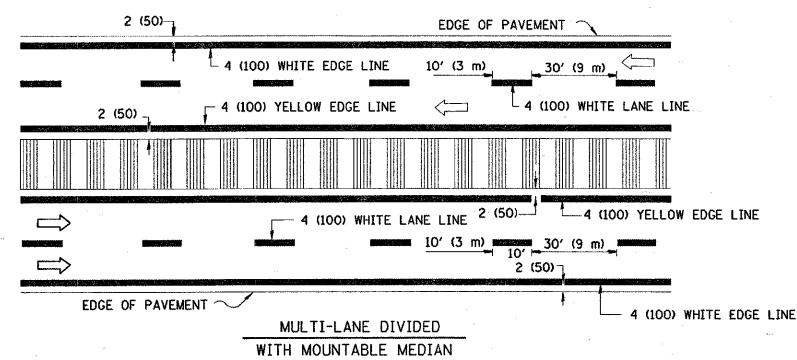
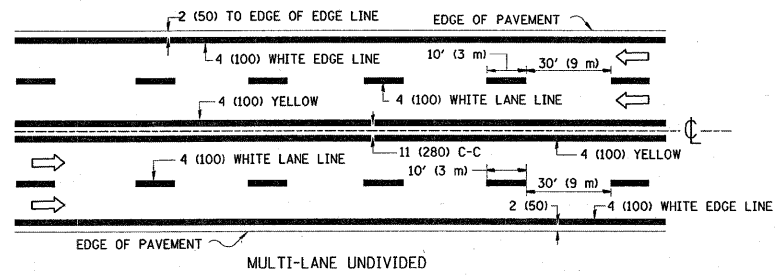
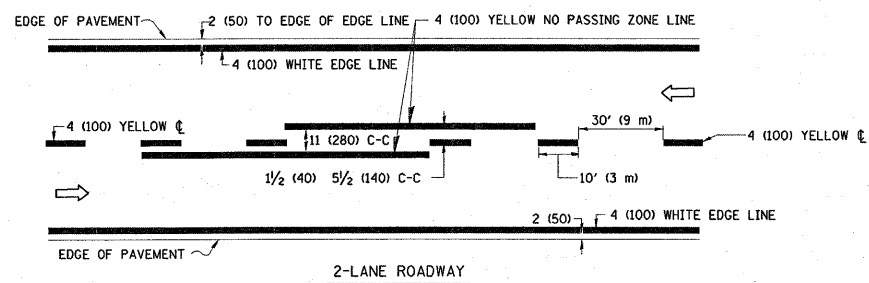
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
c:\pw_work\p\dot\abreuah\dms89779\011184-01ststd.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
		CHECKED -	REVISED - T. RAMMACHER 01-06-00
		DATE -	REVISED -

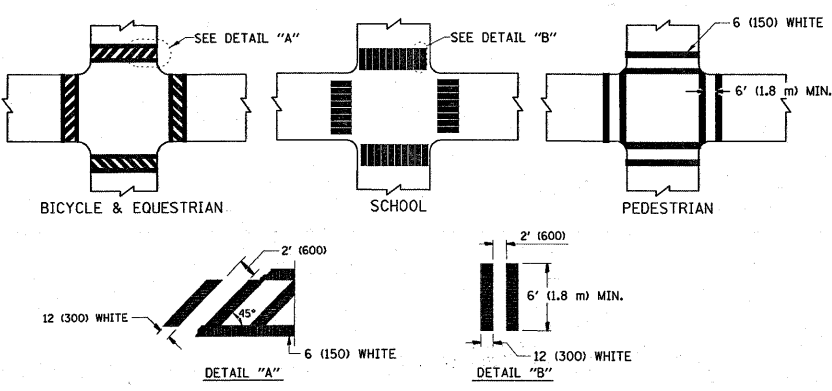
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)		3523	2008-021 RS	COOK	30	26
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	
		FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

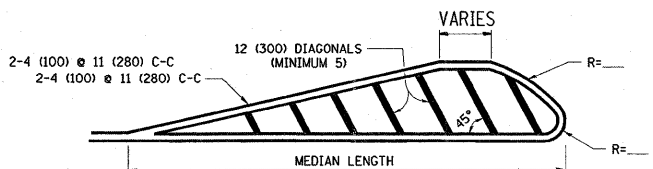
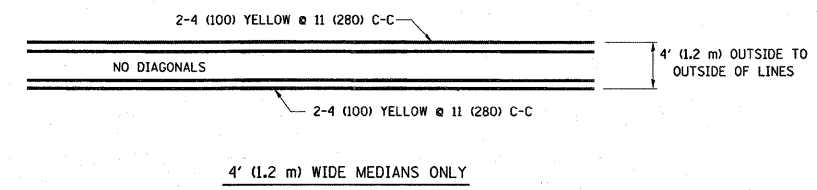


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

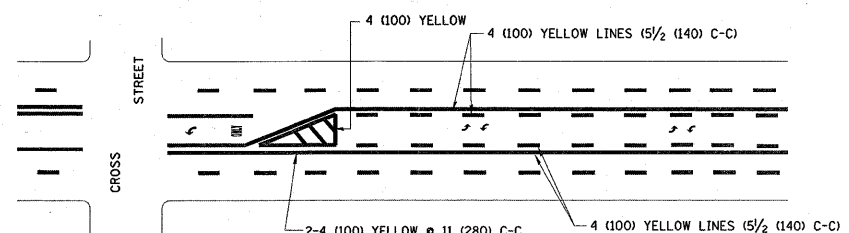


TYPICAL CROSSWALK MARKING

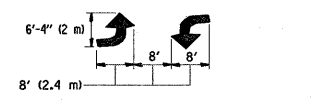


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

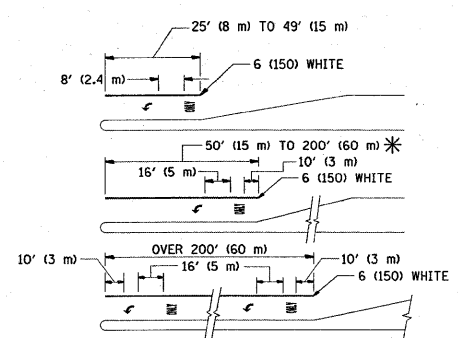


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

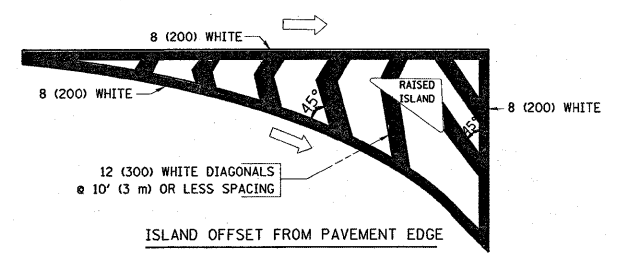
TYPICAL PAINTED MEDIAN MARKING



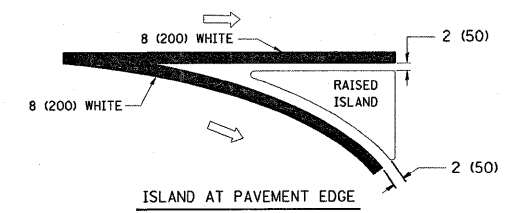
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



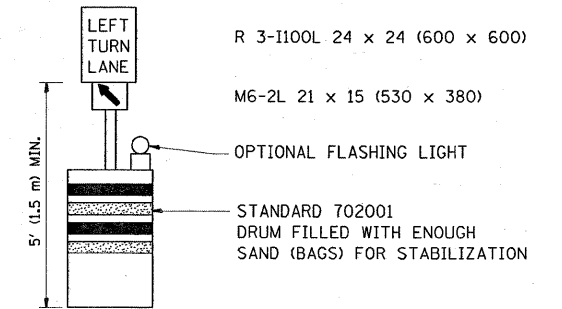
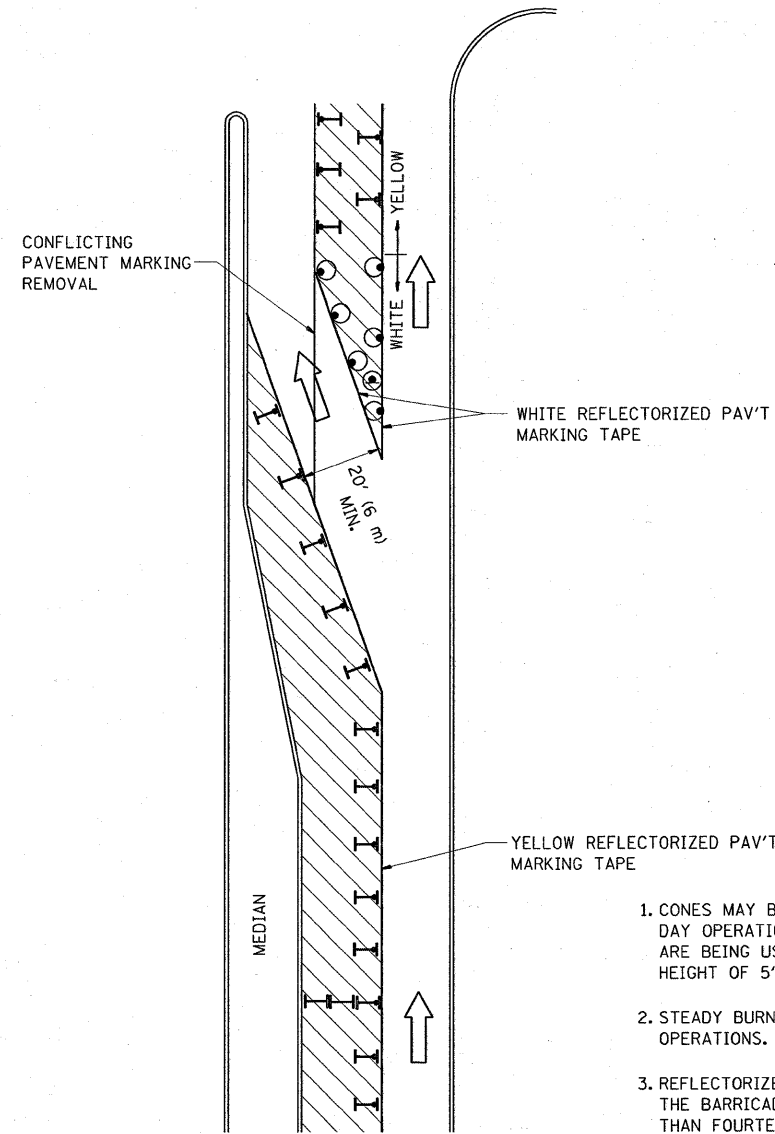
ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' (4.5 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

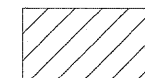







GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = obreueh	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
c:\pw_work\pw\dot\obreueh\dms89779\0111194-01stStd.dgn		DRAWN -	REVISED - A. HOUSEH 11-07-95
		CHECKED -	REVISED - A. HOUSEH 10-12-96
		DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

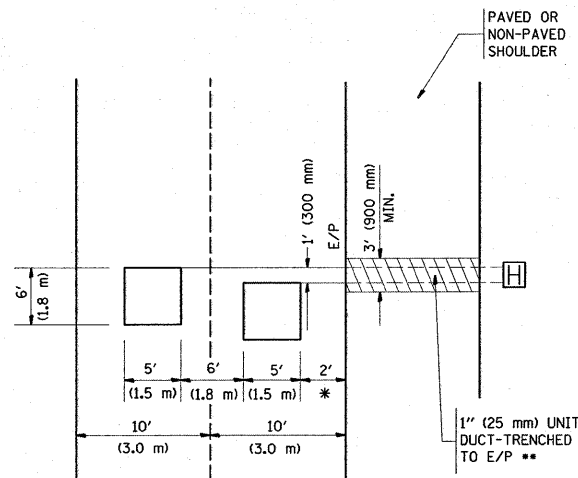
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3523	2008-021 RS	COOK	30	28
TC-14			CONTRACT NO. 60E51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



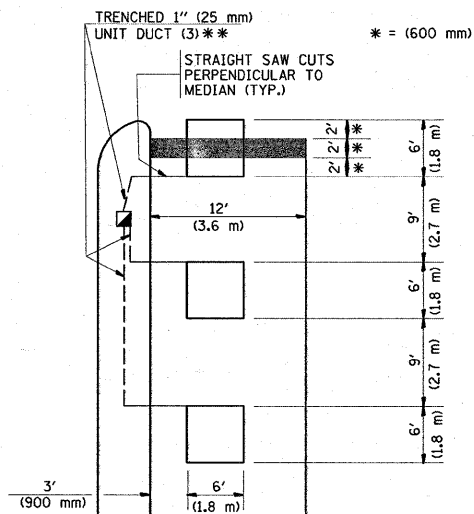
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



* = (600 mm)

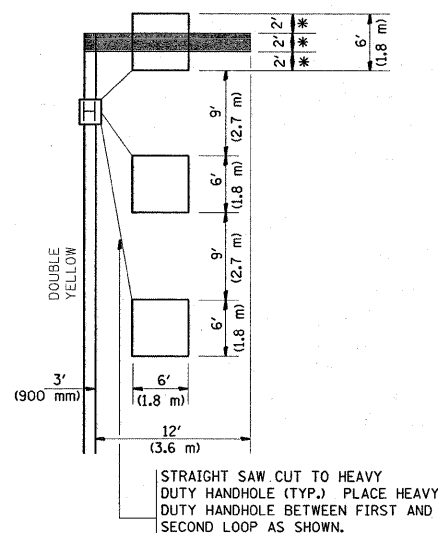
** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

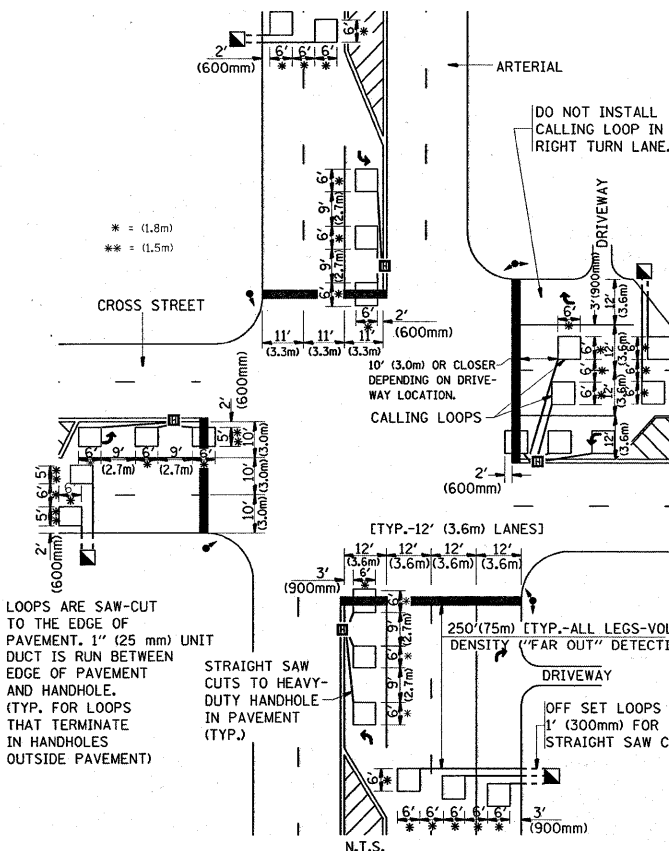
* = (600 mm)



STRAIGHT SAW CUT TO HEAVY DUTY HANDHOLE (TYP.) PLACE HEAVY DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN.

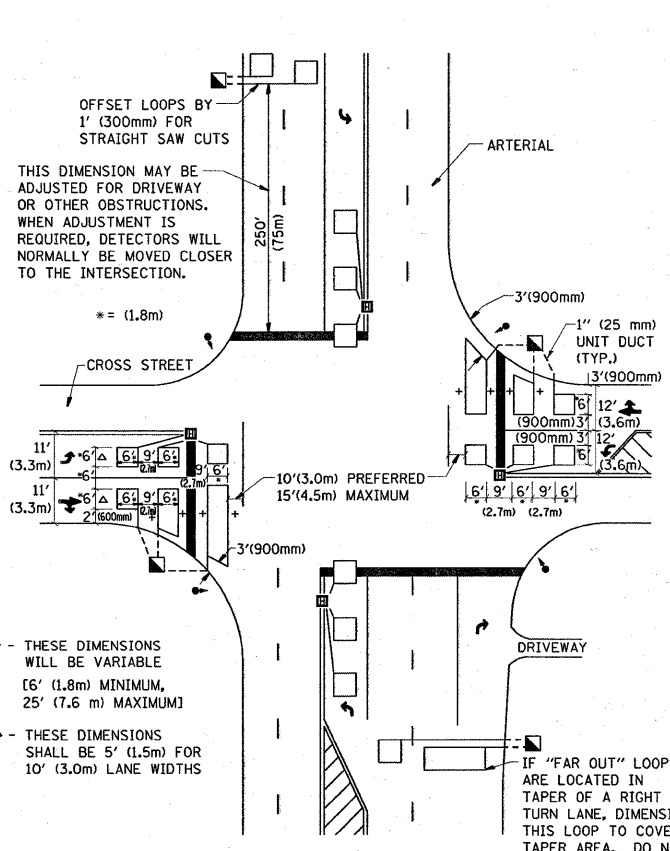
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (I.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

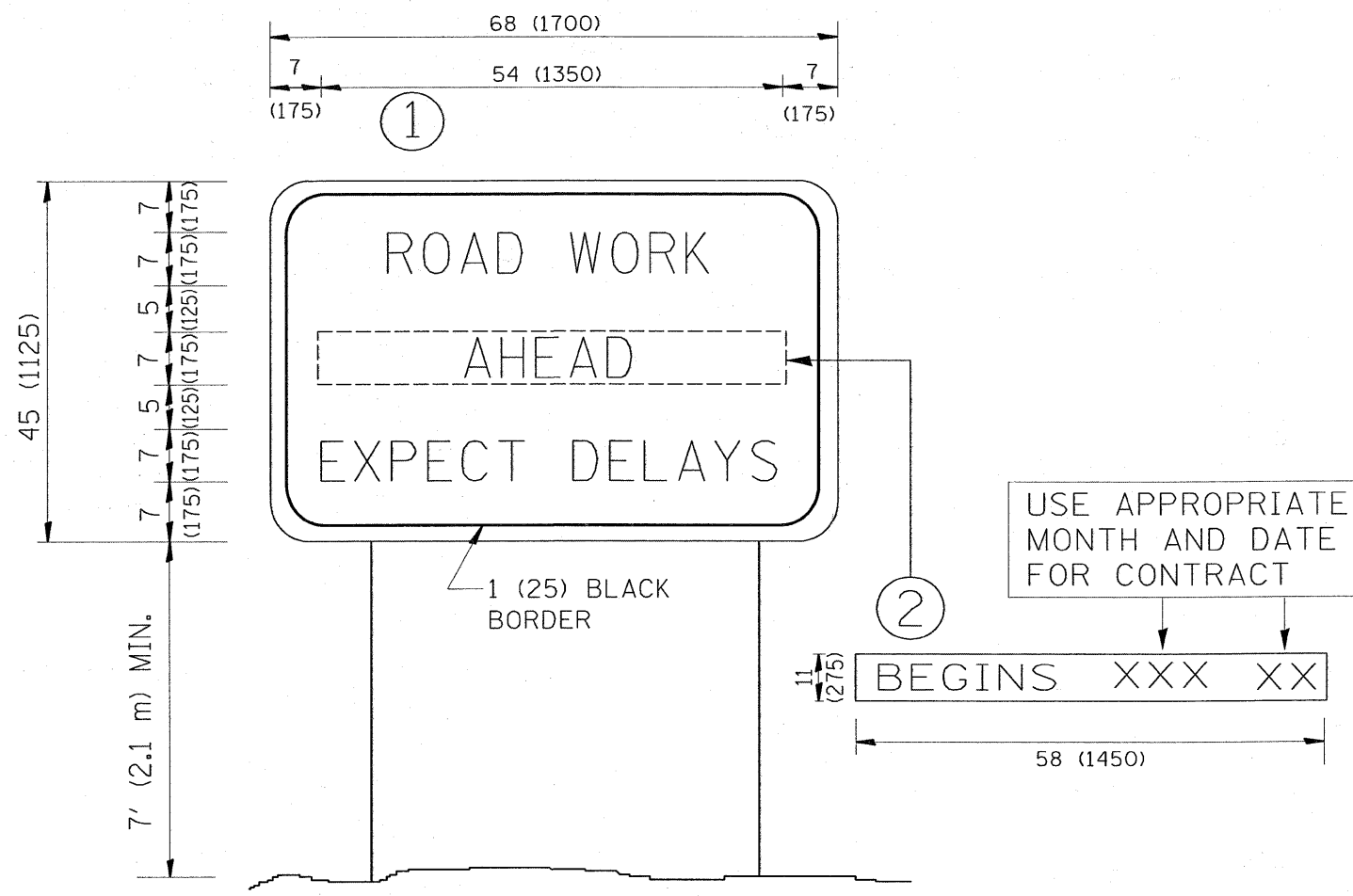
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\pwork\dot\abreuah\dms89779\011184-01stStd.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			3523	2008-021 RS	COOK	30	29	
PLOT DATE = 1/26/2009	DATE -	CHECKED - R.K.F.	REVISED -			TS-07		CONTRACT NO. 60E51			
		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abreuah	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw_work\pxidot\abreuah\dms89779\DI11194-DistStd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	3523			2008-021 RS	COOK	30	30	
PLT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60E51				
PLT DATE = 1/26/2009	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		