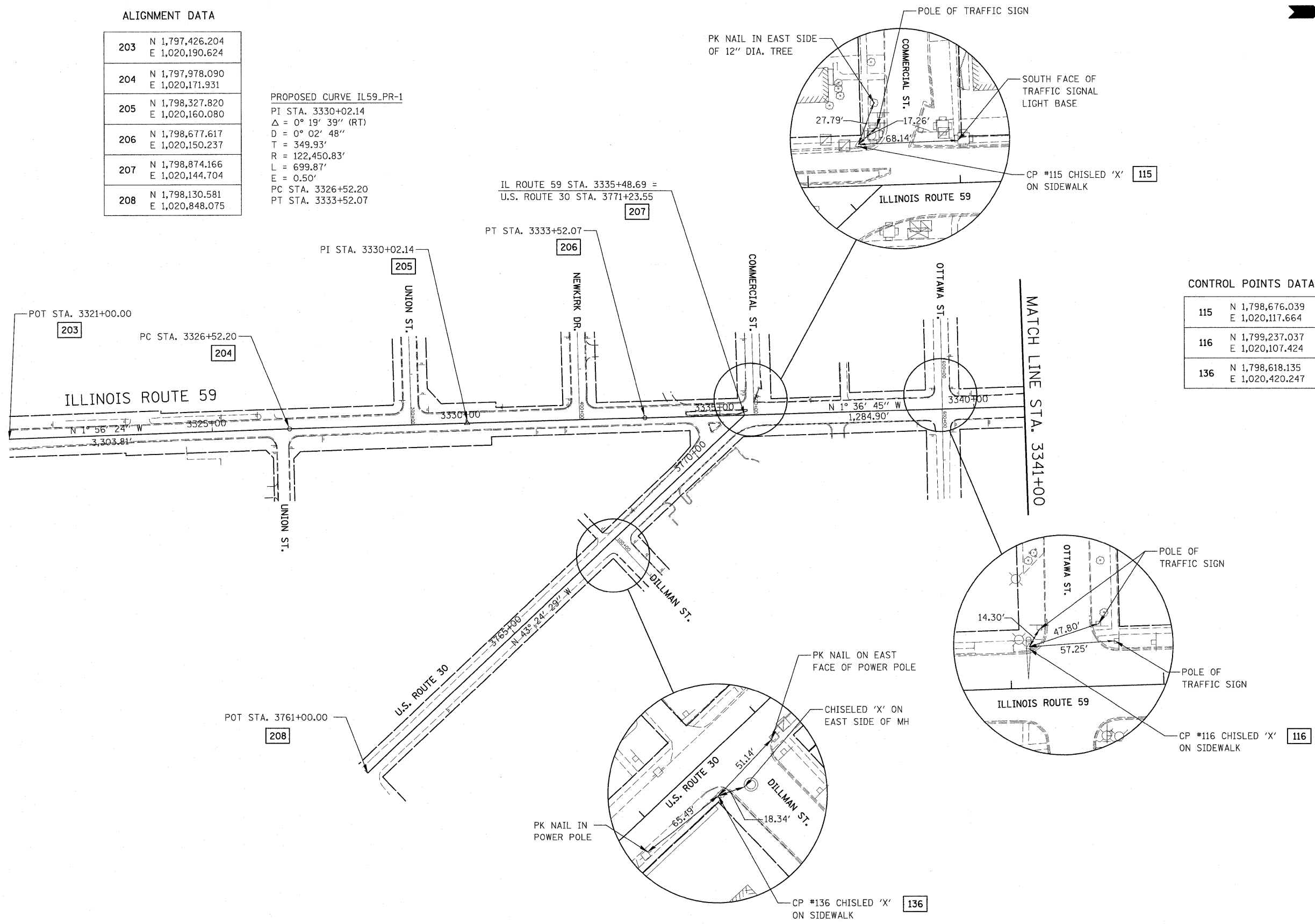


ALIGNMENT DATA

203	N 1,797,426.204 E 1,020,190.624
204	N 1,797,978.090 E 1,020,171.931
205	N 1,798,327.820 E 1,020,160.080
206	N 1,798,677.617 E 1,020,150.237
207	N 1,798,874.166 E 1,020,144.704
208	N 1,798,130.581 E 1,020,848.075

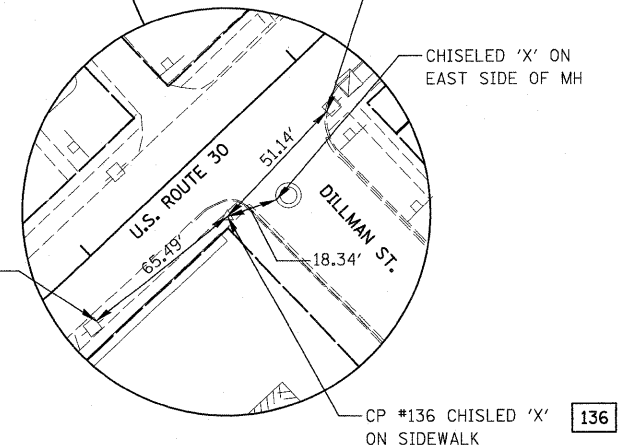
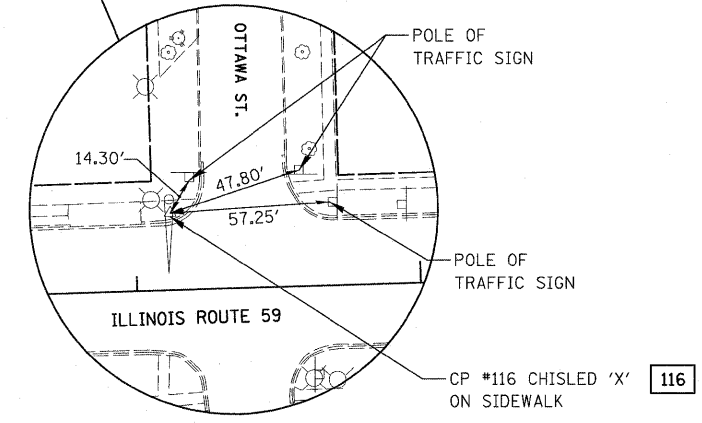
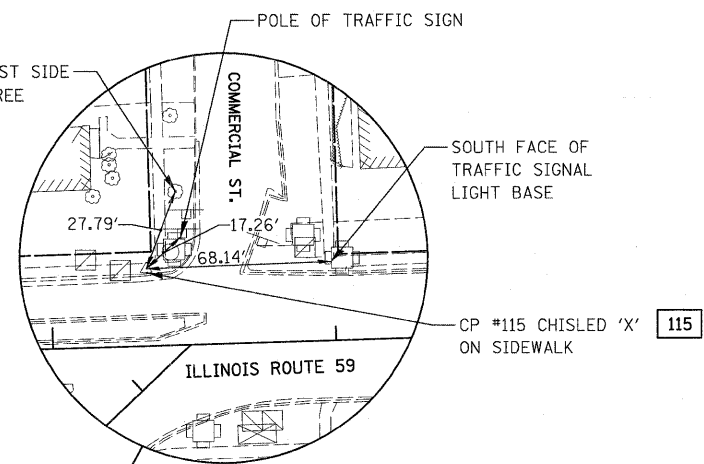
PROPOSED CURVE IL59_PR-1

PI STA. 3330+02.14
 $\Delta = 0^\circ 19' 39''$ (RT)
 $D = 0^\circ 02' 48''$
 $T = 349.93'$
 $R = 122,450.83'$
 $L = 699.87'$
 $E = 0.50'$
 PC STA. 3326+52.20
 PT STA. 3333+52.07

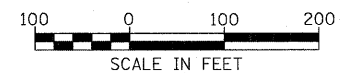


CONTROL POINTS DATA

115	N 1,798,676.039 E 1,020,117.664
116	N 1,799,237.037 E 1,020,107.424
136	N 1,798,618.135 E 1,020,420.247



NOTE:
ENLARGED DETAILS ARE NOT TO SCALE.



FILE NAME =	USER NAME = #USER#	DESIGNED - JCM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENT, BENCHMARK, AND TIES ILLINOIS ROUTE 59 AND U.S. ROUTE 30	F.A. RTE. 338	SECTION 113 N-2	COUNTY WILL	TOTAL SHEETS 260	SHEET NO. 39	
#FILE#	PLOT SCALE = #SCALE#	DRAWN - REW	REVISED -			SCALE: AS SHOWN	SHEET NO. 1 OF 2 SHEETS	STA. 3325+00 TO STA. 3341+00	FED. ROAD DIST. NO. ILLINOIS	CONTRACT NO. 60E73	FAP 338 (IL RTE. 59)
	PLOT DATE = #DATE#	CHECKED - JCM	REVISED -								
		DATE - JANUARY 16, 2009	REVISED -								