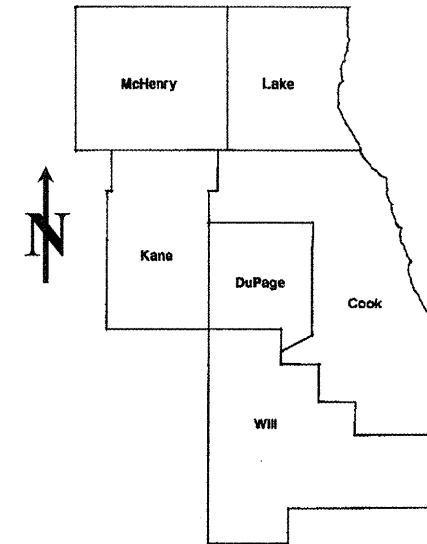


ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NUMBER
VARIOUS	2009-024 PP	COOK	33	1

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
**DISTRICT ONE**  
**PROPOSED HIGHWAY PLANS**

**CONTRACT NO. 60G23**

D-91-354-09



LOCATION OF IMPROVEMENT INDICATED THUS:

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES  
 SECTION: 2009-024 PP  
 VARIOUS LOCATIONS IN NORTH COOK COUNTY  
 INTERMITTENT PAVEMENT RESURFACING  
 PROJECT: ESP-000S (651)  
 COOK COUNTY  
 C-91-354-09

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**  
 SUBMITTED: FEBRUARY 5, 2009  
Diana M. O'Keefe *af*  
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
March 27, 2009  
Charles J. Ingersoll  
 ENGINEER OF DESIGN AND ENVIRONMENT  
March 27, 2009  
Christine M. Reed  
 DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY**  
**OF THE STATE OF ILLINOIS**

**J.U.L.I.E.: JOINT UTILITY LOCATION**  
**INFORMATION FOR EXCAVATION**  
**(312) 744-7000**

**CONTRACT NO. 60G23**

DISTRICT ONE - DESIGN - PLAN PREPARATION ENGINEER:  
 KEN ENG / (847) 705-4247

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	GENERAL LOCATION MAP
5	SUMMARY OF PATCHING SCHEDULE
6-25	PATCHING SCHEDULE
26	BUTT JOINT AND HMA TAPER DETAILS
27	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
28	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
29	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
30	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
31	ARTERIAL ROAD INFORMATION SIGN
32	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
33	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING

STATE STANDARDS

<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
000001-05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY
701336-05	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-03	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-06	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MR. WALLY CZARNY, AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET.

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

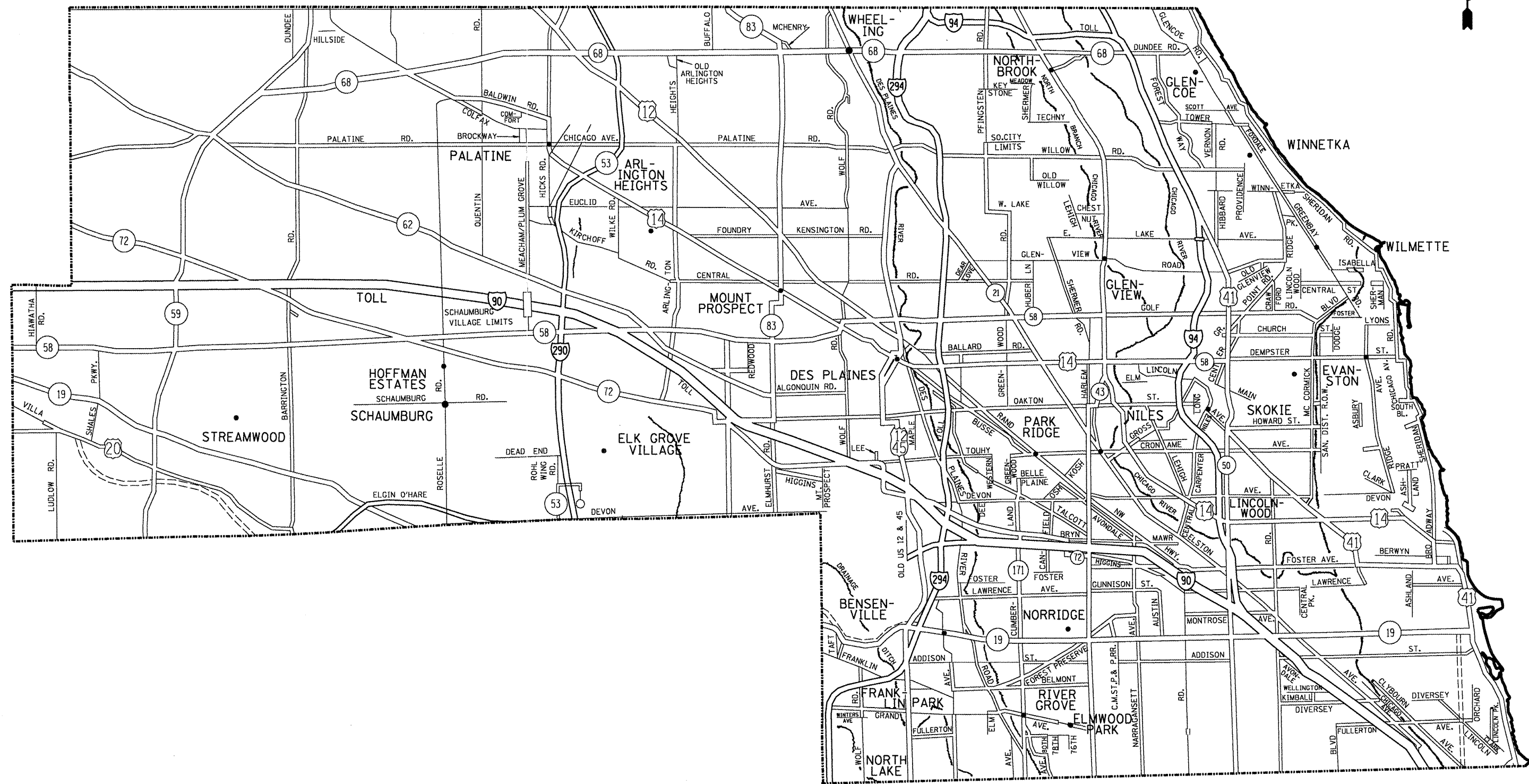
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5MM), 2"	PG 64-22	4% @ 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cd:\pw_work\PWIDOT\SMITHKL\d0125091\Desig.dgn		DRAWN -	REVISED -			VAR.	2009-024 PP	COOK	33	2	
PLOT SCALE = 100.0000' = 1" IN.		CHECKED -	REVISED -			<b>CONTRACT NO. 60G23</b>					
PLOT DATE = 2/9/2009		DATE -	REVISED -			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.





FILE NAME = c:\pwork\p1001\SMITH\CL\0125091\Desig	USER NAME = smthkl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL LOCATION MAP - NORTH COOK COUNTY</b>			F.A. - RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 100.0000' / IN.	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT	33	4
	PLOT DATE = 2/9/2009	CHECKED -	REVISED -									COOK	
		DATE -	REVISED -									<b>CONTRACT NO. 60C23</b>	



ROUTE: Caldwell between Devon and Lincoln

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
DEVON	LEHIGH	EB	2	0	0	0	0
LEHIGH	CENTRAL	EB	2	12	25	300	33
CENTRAL	LENOX	EB	2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	25	300	33
			2	12	50	600	67
			2	12	100	1200	133
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
LENOX	I-94	EB	2	12	200	2400	267
			2	12	200	2400	267
			2	12	300	3600	400
I-94	CICERO	EB	2	0	0	0	0
CICERO	GLENN FOREST	EB	2	12	50	600	67
			2	12	200	2400	267
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	200	2400	267
GLENN FOREST	KOSTNER	EB	2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
KOSTNER	ROGERS	EB	2	12	250	3000	333
			2	12	50	600	67
			2	12	100	1200	133
ROGERS	PULASKI	EB	2	12	25	300	33
			2	12	25	300	33
PULASKI	CENTRAL PARK	EB	2	12	25	300	33
			2	12	25	300	33
			2	12	50	600	67
			2	12	25	300	33
			2	12	100	1200	133
			2	12	25	300	33
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
CENTRAL PARK	KIMBAL	EB	2	0	0	0	0
KIMBAL	KEDZIE	EB	2	0	0	0	0
KEDZIE	LINCOLN	EB	2	12	50	600	67
					0	0	0
LINCOLN	KEDZIE	WB	2	12	25	300	33
KEDZIE	KIMBAL	WB	2	12	25	300	33
			2	2	100	200	22
KIMBAL	CENTRAL PARK	WB	2	12	25	300	33
CENTRAL PARK	PULASKI	WB	2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	50	600	67
			2	12	25	300	33
PULASKI	ROGERS	WB	2	12	25	300	33
			2	12	250	3000	333
			2	12	25	300	33
ROGERS	KOSTNER	WB	2	12	200	2400	267
			2	2	200	400	44
KOSTNER	CICERO	WB	2	12	200	2400	267
			2	12	50	600	67
			2	12	50	600	67
			2	2	500	1000	111
			2	12	200	2400	267

ROUTE: Caldwell between Devon and Lincoln

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
CICERO	I-94	WB	2	12	50	600	67
I-94	LENOX	WB	2	12	200	2400	267
			2	12	50	600	67
LENOX	CENTRAL	WB	2	12	50	600	67
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	50	600	67
			2	12	50	600	67
			2	12	50	600	67
			2	12	150	1800	200
			2	12	200	2400	267
			2	12	200	2400	267
CENTRAL	LEHIGH	WB	2	0	0	0	0
LEHIGH	DEVON	WB	2	12	50	600	67
						0	0
DEVON	LEHIGH	EB	1	0	0	0	0
LEHIGH	CENTRAL	EB	1	0	0	0	0
CENTRAL	LENOX	EB	1	0	0	0	0
LENOX	I-94	EB	1	12	50	600	67
I-94	CICERO	EB	1	0	0	0	0
CICERO	KOSTNER	EB	1	2	100	200	22
			1	12	25	300	33
			1	12	100	1200	133
			1	2	100	200	22
KOSTNER	ROGERS	EB	1	2	100	200	22
ROGERS	PULASKI	EB	1	2	200	400	44
PULASKI	CENTRAL PARK	EB	1	12	25	300	33
			1	12	25	300	33
			1	2	100	200	22
			1	2	100	200	22
			1	2	100	200	22
CENTRAL PARK	KIMBAL	EB	1	2	100	200	22
			1	12	50	600	67
KIMBAL	KEDZIE	EB	1	2	100	200	22
			1	12	50	600	67
KEDZIE	LINCOLN	EB	1	0	0	0	0
LINCOLN	KEDZIE	WB	1	2	50	100	11
			1	12	50	600	67
KEDZIE	KIMBAL	WB	1	2	50	100	11
			1	12	25	300	33
KIMBAL	CENTRAL PARK	WB	1	2	50	100	11
			1	12	25	300	33
CENTRAL PARK	PULASKI	WB	1	2	50	100	11
			1	12	200	2400	267
PULASKI	ROGERS	WB	1	12	25	300	33
			1	12	50	600	67
ROGERS	KOSTNER	WB	1	12	200	2400	267
			1	2	100	200	22
KOSTNER	FOREST GLENN	WB	1	2	200	400	44
			1	12	300	3600	400
CICERO	LENOX	WB	1	2	100	200	22
			1	12	25	300	33
LENOX	CENTRAL	WB	1	2	100	200	22
			1	12	50	600	67
CENTRAL	LEHIGH	WB	1	0	0	0	0
LEHIGH	DEVON	WB	1	2	100	200	22
			1	12	50	600	67

TOTALS: 11600 FT 12578 SY

ROUTE: **DEMPSTER ST BETWEEN I-294 AND GREENWOOD**

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
GREENWOOD	WESTERN	WB	2	12	50	600	67
WESTERN	LUTHER	WB	2	12	400	4800	533
LUTHER	DEE	WB	2	12	50	600	67
			2	2	50	100	11
DEE	POTTER	WB	2	12	50	600	67
			2	12	25	300	33
			2	2	50	100	11
POTTER	I-294	WB	2	12	50	600	67
			2	2	250	500	56
						0	0
I-294	POTTER	EB	2	12	100	1200	133
			2	2	100	200	22
			2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
POTTER	LUTHER	EB	2	12	50	600	67
			2	2	50	100	11
LUTHER	WESTERN	EB	2	12	200	2400	267
			2	2	50	100	11
WESTERN	GREENWOOD	EB	2	12	50	600	67
			2	12	50	600	67
			2	12	250	3000	333
			2	2	200	400	44
						0	0
GREENWOOD	LUTHER	WB	1	12	25	300	33
			1	12	25	300	33
LUTHER	POTTER	WB	1	12	50	600	67
			1	2	200	400	44
POTTER	I-294	WB	1	12	100	1200	133
						0	0
I-294	POTTER	EB	1	12	200	2400	267
			1	2	200	400	44
POTTER	LUTHER	EB	1	12	25	300	33
			1	2	200	400	44
LUTHER	WESTERN	EB	1	12	25	300	33
			1	12	25	300	33
			1	2	300	600	67
WESTERN	GREENWOOD	EB	1	12	100	1200	133
			1	12	200	2400	267

TOTAL 3875 FT 3333 SY

ROUTE: GOLF RD. (I-294 to WAUKEGAN RD.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
I-294 (EB)	REDING ST.	EB	1	12	12	144.0	16.0
"	"	EB	2	12	6	72.0	8.0
"	"	EB	1,2	24	6	144.0	16.0
"	"	EB	2	12	4	48.0	5.3
"	"	EB	1,2	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1	12	4	48.0	5.3
"	"	EB	CL	30	2	60.0	6.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1,2	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1	12	12	144.0	16.0
"	"	EB	1	12	4	48.0	5.3
"	"	EB	CL	25	2	50.0	5.6
"	"	EB	1	50	12	600.0	66.7
I-294 (EB)	REDING ST.	EB	2	12	4	48.0	5.3
LINCOLN ST. (EB)	HARLEM AVE	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	3	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	LT,1	24	4	96.0	10.7
"	"	EB	2,3	24	10	240.0	26.7
LINCOLN ST. (EB)	HARLEM AVE	EB	1,2,3	36	4	144.0	16.0
HARLEM AVE (EB)	ALLSTATE ENTRANCE	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1	8	4	32.0	3.6
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2,3	65	4	260.0	28.9
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	LT,1	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1,2,3	36	4	144.0	16.0
HARLEM AVE (EB)	ALLSTATE ENTRANCE	EB	2,3	24	4	96.0	10.7
ALLSTATE ENTRANCE	WAUKEGAN RD.	EB	1	12	4	48.0	5.3
"	"	EB	1	12	8	96.0	10.7
"	"	EB	2	12	8	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	6	72.0	8.0
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1,2,3	36	4	144.0	16.0
"	"	EB	3	12	4	48.0	5.3
"	"	EB	2	12	8	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	1,2,3	36	4	144.0	16.0

ROUTE: GOLF RD. (I-294 to WAUKEGAN RD.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
HARLEM AVE (EB)	ALLSTATE ENTRANCE	EB	1	12	4	48.0	5.3
ALLSTATE ENTRANCE	WAUKEGAN RD.	EB	2	12	4	48.0	5.3
"	"	EB	3	12	4	48.0	5.3
"	"	EB	2	12	8	96.0	10.7
"	"	EB	1	12	4	48.0	5.3
"	"	EB	2,3	24	4	96.0	10.7
"	"	EB	LT,1	24	4	96.0	10.7
"	"	EB	1,2,3	36	4	144.0	16.0
ALLSTATE ENTRANCE	WAUKEGAN RD.	EB	LT	12	4	48.0	5.3
WAUKEGAN RD.	ALLSTATE ENTRANCE	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1	12	4	48.0	5.3
"	"	WB	3	12	4	48.0	5.3
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2	12	4	48.0	5.3
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	3	12	6	72.0	8.0
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2	12	4	48.0	5.3
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1	24	4	96.0	10.7
"	"	WB	2,3	36	4	144.0	16.0
"	"	WB	1	24	6	144.0	16.0
"	"	WB	2,3	12	5	60.0	6.7
"	"	WB	1,2,3	24	4	96.0	10.7
"	"	WB	2,3	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	LT,1	36	6	216.0	24.0
"	"	WB	1	8	4	32.0	3.6
"	"	WB	2,3	12	4	48.0	5.3
"	"	WB	2	36	4	144.0	16.0
"	"	WB	1,2,3	24	4	96.0	10.7
"	"	WB	1	12	4	48.0	5.3
ALLSTATE ENTRANCE	HARLEM AVE	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2	2	24	48.0	5.3
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
"	"	WB	1,2,3	36	4	144.0	16.0
WASHINGTON ST.	LINCOLN ST.	WB	2,3	24	4	96.0	10.7
"	"	WB	1	12	4	48.0	5.3
"	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	2	12	4	48.0	5.3
WASHINGTON ST.	LINCOLN ST.	WB	1,2,3	36	5	180.0	20.0
REDING ST.	I-294	WB	1,2	24	8	192.0	21.3
"	"	WB	1,2	24	200	4800.0	533.3
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	1,2	24	4	96.0	10.7
"	"	WB	1,2	2	200	400.0	44.4
REDING ST.	I-294	WB	1,2	2	100	200.0	22.2

TOTALS 1152 FT 2112 SY

FILE NAME =	USER NAME = smithk1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE GOLF RD.</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw_work\PWIDDT\SMITHKL\d0125091\Desig	n.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	COOK	33	8
		CHECKED -	REVISED -								CONTRACT NO. 60623		
		DATE -	REVISED -								FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT





ROUTE: Hibbard Road - Skokie Road to Illinois Avenue

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Skokie Rd	Lake Avenue	SB	1	12	30	360	40
		NB	1	12	74	888	99
		NB	1	12	17	204	23
		C/L	-	2	174	348	39
		SB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	12	144	16
		NB	1	12	58	696	77
		SB	1	12	22	264	29
		C/L	-	2	176	352	39
		NB	1	12	21	252	28
		SB	1	12	35	420	47
		NB	1	12	55	660	73
		SB	1	12	17	204	23
		NB	1	12	17	204	23
		C/L	-	2	100	212	22
		NB	1	12	7	84	9
		NB	1	12	2	24	3
		SB	1	12	2	24	3
		Lake Avenue	Walden St.	NB	1	12	19
NB	1			12	2	24	3
SB	1			12	2	24	3
C/L	-			2	300	600	67
NB	1&2			2	53	106	12
SB	1&2			2	53	106	12
NB	1			12	10	120	13
NB	1			12	10	120	13
SB	1			12	10	120	13
C/L	-			2	590	1180	131
Walden St.	Sprucewood	NB	1	12	6	72	8
		NB	1	12	6	72	8
		SB	1	12	6	72	8
		SB	2	12	6	72	8
		NB	1	12	2	24	3
		NB	1	12	4	48	5
		SB	1	12	4	48	5
		NB	1	12	4	48	5
Sprucewood	Illinois Ave.	C/L	-	2	84	168	19
		NB	1	12	10	120	13
		NB	1	12	4	48	5

TOTALS: 2370 FT 1071 SY



ROUTE: **IL Rte 53 West Frontage from IL 68 to Rand Rd**

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
IL Rte 68	Carol	NB B	1	12	80	960	107
		SB B	1	12	80	960	107
Rand Intersection	Rand	SB B	1	12	200	2400	267
		NB B	1	12	200	2400	267
		SB B	2	12	200	2400	267

TOTALS: 760 FT 1015 SY

ROUTE: **IL 53 West Frontage: Rand Rd to Palatine Rd.**

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Rand Rd	Limit of Conc Pavement	SB B	1	12	12	144	16
		SB B	1	12	20	240	27
		NB B	1	12	100	1200	133
		SB B	1	12	15	180	20
		NB B	1	12	100	1200	133
		NB B	1	12	80	960	107
		SB B	1	12	60	720	80
		SB B	1	12	60	720	80
		NB B	1	12	110	1320	147
		NB B	1	12	80	960	107
		SB B	1	12	120	1440	160
		NB B	1	12	100	1200	133
		NB B	1	12	200	2400	267
		SB B	1	12	12	144	16
		SB B	1	12	6	72	8
		NB B	1	12	6	72	8

TOTALS: 1081 FT 1442 SY







ROUTE: **IL 72 EB (0.1 mile E/O Meacham to 0.2 mile E/O Meacham)**

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
0.1 mile E/O Meacham	0.2 Mile E/O Meacham	EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3

TOTALS: 26 35  
FT SY



ROUTE: Oakton St. EB & WB (Elmhurst Rd. to 0.1 mile W/O Elmhurst Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Elmhurst Rd.	0.1 mile W of Elmhurst Rd.	WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	600	7200	800
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	600	7200	800
		EB	1	12	600	7200	800
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	2	12	600	7200	800
		EB	2	12	2	24	3
		EB	Lt Turn	8	400	3200	356
		EB	Lt Turn	12	200	2400	267

TOTALS: 3024 FT 3854  
SY

ROUTE: Touhy Avenue (McCormick Blvd to Kedzie Ave)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
McCormick Blvd	Kedzie Ave.	EB	1	12	4	48	5
		WB	1	12	24	288	32
		WB	2	12	24	288	32
		EB	1	12	24	288	32
		EB	2	12	24	288	32
		EB	1	12	7	84	9
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	17	204	23
		EB	2	12	17	204	23
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	4	48	5
		EB	2	12	4	48	5
		WB	1	12	4	48	5
		WB	2	12	4	48	5
		WB	2	12	30	360	40
		WB	1	12	12	144	16
		WB	2	12	32	384	43
		EB	2	12	32	384	43

TOTAL

287  
FT

382  
SY

FILE NAME = c:\pwwork\pww\DOT\SMITHK\ld0125091\Desk\dgn	USER NAME = smithk1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE</b> <b>TOUHY AVE.</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 1/8" = 1' / IN.	CHECKED -	REVISED -	VAR.					2009-024 PP	COOK	33	18	
PLOT DATE = 2/9/2009	DATE -	REVISED -					<b>CONTRACT NO. 60C23</b>					
			SCALE:		SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

ROUTE: West Lake Ave. (Pfingsten Rd. to Greenwood)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Pfingsten	Greenwood	EB	1	2	10	20	2
		EB	1	12	10	120	13
		EB	1	2	30	60	7
		EB	1	2	15	30	3
		EB	1	12	15	180	20
		EB	1	12	10	120	13
		EB	1	2	10	20	2
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	30	60	7
		EB	1	12	15	180	20
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	50	100	11
		EB	1	12	10	120	13
		EB	1	12	40	480	53
		EB	1	2	12	24	3
		EB	1	12	100	1200	133
		EB	1	12	100	1200	133
		WB	1	12	15	180	20
		WB	1	12	10	120	13
		WB	1	2	20	40	4
		WB	1	2	40	80	9
		WB	1	2	10	20	2
		WB	1	12	20	240	27
		WB	1	12	100	1200	133
		WB	1	12	50	600	67
		WB	1	2	20	40	4
		WB	1	2	15	30	3

TOTALS: 817 FT 732 SY

ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Landwehre Rd.	Pfungsten	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
Pfungsten	Greenwood	EB	2	2	36	70	8
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
Greenwood	Shermer	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
Shermer	Ravine	EB	2	12	15	180	20
		EB	2	12	15	180	20
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
Ravine	Waukegan	EB	2	12	12	144	16
		EB	2	12	12	144	16
		EB	2	12	12	144	16
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	49	98	11
		EB	2			0	0

ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)		
FROM	TO								
Waukegan	Ravine	WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	250	500	56		
		WB	2	2	250	500	56		
		Ravine	Shermer	WB	2	2	12	24	3
		Shermer	Greenwood	WB	2	2	12	24	3
				WB	2	2	60	120	13
		Greenwood	Pfungsten	WB	2	2	15	180	20
				WB	2	2	12	24	3
				WB	2	2	12	24	3
				WB	2	2	12	24	3
				WB	2	2	12	24	3
				WB	2	2	12	24	3
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
WB	2			2	12	24	3		
Pfungsten	Landwehre	WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
		WB	2	2	12	24	3		
Landwehre	Pfungsten	EB	1	2	40	80	9		
		EB	1	2	30	60	7		
		EB	1	2	40	80	9		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
Pfungsten	Greenwood	EB	1	2	150	300	33		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		
		EB	1	2	12	24	3		

CONTINUED ON THE NEXT SHEET

FILE NAME =	USER NAME = smithk1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE</b> <b>WILLOW RD.</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pwwork\pwwork\SMITHKL\08125091\Design	DRAWN -	REVISED -	VAR.			2009-024 PP	COOK	33	20	
PLOT SCALE = 103.7051' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60623							
PLOT DATE = 2/9/2009	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)		
FROM	TO								
Greenwood	Shermer	EB	1	2	200	400	44		
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	2	12	24	24	3	
		EB	1	12	12	144	144	16	
		EB	1	12	12	144	144	16	
		EB	1	2	400	800	800	89	
		Shermer	Ravine	EB	1	2	350	700	78
				EB	1	2	1200	2400	267
		EB	1	2	200	400	44		
Ravine	Waukegan	EB	1	2	600	1200	133		
Waukegan	Ravine	WB	1	2	400	800	89		
		WB	1	2	300	600	67		
		WB	1	2	200	400	44		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
Ravine	Shermer	WB	1	2	1000	2000	222		
		WB	1	2	40	80	9		
		WB	1	2	250	500	56		
Shermer	Greenwood	WB	1	12	12	144	16		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
Greenwood	Pfingsiten	WB	1	2	100	200	22		
		WB	1	2	400	800	89		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		
		WB	1	2	12	24	3		

ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)	
FROM	TO							
Pfingsten	Landwehre	WB	1	2	12	24	3	
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	12	24	24	3
		WB	1	2	150	300	33	
		WB	1	2	15	30	3	
		WB	1	2	50	100	11	

TOTALS: 9205 FT 2228 SY

FILE NAME =  
 es:\pwwork\PWIDOT\SMITHKL\0125091\Desig...

USER NAME = smithkl  
 PLOT SCALE = 1/83.7851" / IN.  
 PLOT DATE = 2/9/2009

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PATCHING SCHEDULE  
WILLOW RD.**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	21
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	CONTRACT NO. 60G23	

ROUTE: Wolf Rd. (Milwaukee Ave. to Palatine Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
IL 21	Picardy Ln.	SB	1	12	12	144.0	16.0
	Longit. Jt.	SB	1-2	2	20	40.0	4.4
Picardy Ln.	Mayer Ave.	SB	1	12	12	144.0	16.0
	Widening Jt.	SB	2	2	150	300.0	33.3
Mayer Ave.	Strong St.	SB Edge	1	2	50	100.0	11.1
		SB Edge	1	2	12	24.0	2.7
Strong St.	Capri Ter.	SB	1	12	6	72.0	8.0
Capri Ter.	Dundee Rd.	SB Edge	2	2	50	100.0	11.1
Dundee Rd.	Jeffrey Ave.	SB Edge	2	2	60	120.0	13.3
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB Edge		6	15	90.0	10.0
		SB Edge		2	15	30.0	3.3
		SB Edge		2	10	20.0	2.2
		SB	1	12	6	72.0	8.0
		SB Edge		2	30	60.0	6.7
		SB	1	12	6	72.0	8.0
		SB Edge		2	10	20.0	2.2
		SB Edge		2	15	30.0	3.3
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB Edge		2	15	30.0	3.3
		SB Edge		2	15	30.0	3.3
		SB Edge		2	30	60.0	6.7
		SB Edge		2	15	30.0	3.3
		SB Edge		2	25	50.0	5.6
Jeffrey Ave.	Manchester Dr.	SB Edge	2	2	50	100.0	11.1
		SB Edge	1	12	6	72.0	8.0
		SB Edge	2	2	25	50.0	5.6
		SB Edge	2	2	50	100.0	11.1
		SB	1	12	12	144.0	16.0
		SB Edge		2	20	40.0	4.4
		SB Edge		2	15	30.0	3.3
Manchester Dr.	Crescent Dr.	NB	1	12	6	72.0	8.0
		NB	1	12	20	240.0	26.7
		NB	1	12	6	72.0	8.0
		NB Edge		2	20	40.0	4.4
		NB Edge		2	30	60.0	6.7
		NB	1	12	6	72.0	8.0
Crescent Dr.	Dundee Rd.	NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB Edge		2	25	50.0	5.6
		NB	1	12	6	72.0	8.0
		NB	1	12	12	144.0	16.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB Edge		2	25	50.0	5.6
		NB Edge		2	30	60.0	6.7
		NB Edge		2	25	50.0	5.6
		NB	1	12	12	144.0	16.0
		NB Edge		2	50	100.0	11.1
Dundee Rd.	Strong St.	NB Edge	2	2	25	50.0	5.6
		NB Edge	2	2	50	100.0	11.1
	Longit. Jt.	CL		2	50	100.0	11.1
Strong St.	IL 21	NB Edge		2	30	60.0	6.7
		NB Edge		2	15	30.0	3.3
		NB Edge		2	15	30.0	3.3
		NB	1	12	6	72.0	8.0
		NB	2	12	6	72.0	8.0
		1-2	2	15	30.0	3.3	
Palatine Rd.	Marquardt Dr.	NB	1	12	12	144.0	16.0
		NB	1	6	12	72.0	8.0
	Longit. Jt.	NB	1-2	2	20	40.0	4.4
		NB	1	12	18	216.0	24.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
	Longit. Jt.	NB	1-2	2	25	50.0	5.6
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB	1	12	50	600.0	66.7
		NB	1	12	12	144.0	16.0
		NB	1	12	12	144.0	16.0

ROUTE: Wolf Rd. (Milwaukee Ave. to Palatine Rd.)

CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	LANE NO. (1, 2, 3)	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA (SQ FT)	REPAIR AREA (SQ YD)
FROM	TO						
Marquardt Dr.	Palatine Rd.	SB				0.0	0.0
	Longit. Jt. @ Messner	SB RHTB		6	80	480.0	53.3
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	1	12	15	180.0	20.0
		SB Edge	1	2	16	32.0	3.6
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
Widening area for Palatine Rd.		SB	1-2	2	200	400.0	44.4
		SB	1	12	6	72.0	8.0
		SB	2	12	12	144.0	16.0
		SB	1	12	6	72.0	8.0

TOTAL 1864 FT 916 SY

NOTE:

NO PATCHING IS TO BE DONE ON MARQUARDT DR. TO PALATINE RD. THESE LIMITS WILL BE RESURFACED AT A LATER DATE.

ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

CROSS STREETS		DIRECTION (EB/WB)	LANE NO.	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA	REPAIR AREA	
FROM	TO							
Grosse Pointe Rd	Skokie Rd	WB	Lt Turn	12	43	516	57	
		WB	Lt Turn	2	100	200	22	
	EB	1	12	4	48	5		
	EB	1	12	4	48	5		
	EB	2	12	4	48	5		
	WB	1	12	4	48	5		
Skokie Rd	Niles Center Rd	WB	2	12	4	48	5	
		THIS SECTION IS PCC PAVEMENT						
Niles Center Rd	Kenton Av	C/L	-	2	60	120	13	
Kenton Av	Kolmar Av	EB	1	12	2	24	3	
		EB	2	12	2	24	3	
	WB	1	12	2	24	3		
	WB	2	12	2	24	3		
	WB	1&2	2	250	500	56		
	C/L	-	2	40	80	9		
Kolmar Av	Kilbourn Av	C/L	-	2	100	200	22	
		WB	1	12	6	72	8	
	WB	2	12	6	72	8		
	C/L	-	2	123	246	27		
	WB	1&2	2	123	246	27		
	WB	1&2	2	75	150	17		
Kilbourn Av	Kenneth Av	C/L	-	2	75	150	17	
		WB	2	12	150	1800	200	
	C/L	-	2	150	300	33		
	WB	1&2	2	40	80	9		
	WB	1	12	6	72	8		
	WB	2	12	6	72	8		
Kenneth Av	Kostner Av	EB	1	12	6	72	8	
		EB	2	12	6	72	8	
		EB	1&2	2	40	80	9	
		WB	1&2	2	40	80	9	
		C/L	-	2	110	220	23	
		WB	1&2	2	47	94	9	
	Kostner Av	Lowell Av	C/L	-	2	100	200	22
			C/L	-	2	388	76	8
		WB	1&2	2	100	200	22	
		C/L	-	2	73	146	16	
		EB	1	12	6	72	8	
		C/L	-	2	140	280	31	
Lowell Av	Kildare Av	C/L	-	2	225	450	50	
		EB	1	12	6	72	8	
		C/L	-	2	94	188	21	
Kildare Av	Tripp Av	WB	1&2	2	87	174	19	
		C/L	-	2	195	390	43	
		WB	1&2	2	110	220	24	
Tripp Av	Keeler Av	EB	1	12	4	48	5	
		EB	2	12	4	48	5	
	WB	1	12	4	48	5		
	WB	2	12	4	48	5		
	C/L	-	2	18	36	4		
	WB	1&2	2	78	156	17		
		EB	1&2	2	27	54	6	
		C/L	-	2	67	134	15	

ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

CROSS STREETS		DIRECTION (EB/WB)	LANE NO.	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA	REPAIR AREA
FROM	TO						
Tripp Av	Keeler Av (CONT'D)	WB	1&2	2	42	84	9
		EB	2	12	6	72	8
		C/L	-	2	10	20	2
Keeler Av	Kedvale Av	EB	1&2	2	25	50	6
		C/L	-	2	144	288	32
		C/L	-	2	24	48	5
		C/L	-	2	77	154	17
Kedvale Av	Karlov Av	C/L	-	2	109	218	24
		WB	1&2	4	22	88	10
		WB	1&2	2	39	78	9
		C/L	-	2	174	348	37
Karlov Av	Keystone Av	WB	1&2	2	63	126	14
		WB	1	12	23	276	31
		EB	1&2	2	34	68	7
		C/L	-	2	320	640	71
		WB	1&2	2	25	50	6
		EB	1	12	6	72	8
Keystone Av	Crawford Av	EB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2-Jan	12	6	72	8
		WB	1	2	25	50	6
		WB	1	4	12	48	5
		WB	2	4	12	48	5
		C/L	-	2	177	354	39
		WB	1	12	10	120	13
		C/L	-	2	119	238	26
		WB	1&2	2	33	66	7
Crawford Av	Harding Av	WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2	12	6	72	8
Harding Av	Springfield Av	C/L	-	2	50	100	11
		EB	1	12	6	72	8
		C/L	-	2	15	30	3
Springfield Av	Avers Av	EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	1&2	2	38	76	8
		WB	2	12	6	72	8
Avers Av	Samoset Av	WB	2	12	6	72	8
		CL	-	2	48	96	11
		EB	1	12	6	72	8
		EB	1&2	2	43	86	11
Samoset Av	Hamlin Av	C/L	-	2	35	70	8
		WB	1	12	10	120	13
		WB	2	12	10	120	13
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
Hamlin Av	Ridgeland Av. (SB)	EB	2	12	6	72	8
		WB	1&2	2	60	120	13
		WB	1	12	6	72	8
Ridgeland Av. (SB)	Ridgeland Av. (NB)	WB	1	12	6	72	8

CONTINUED ON THE NEXT SHEET

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE CHURCH ST.</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
os:\pwork\p\1001\SMITHKL\10125091\Desig	DRAWN -	REVISED -	VAR.			2009-024 PP	COOK	33	23	
PLOT SCALE = 103.7851' / IN.	CHECKED -	REVISED -	CONTRACT NO. 60G23							
PLOT DATE = 2/9/2009	DATE -	REVISED -	SCALE:			SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

CROSS STREETS		DIRECTION (EB/WB)	LANE NO.	PAVEMENT PATCH WIDTH	PAVEMENT PATCH LENGTH	REPAIR AREA	REPAIR AREA
FROM	TO						
Ridgeland Av. (NB)	Lawndale Av.	WB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
Lawndale Av.	Central Park Av.	EB	1&2	2	27	54	6
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	1	12	4	48	5
		WB	2	12	4	48	5
		EB	1	12	4	48	5
		EB	2	12	4	48	5
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	1-Jan	12	6	72	8
		C/L	-	2	54	108	12
WB	1	12	6	72	8		
WB	2	12	6	72	8		
EB	1	12	6	72	8		
EB	2	12	6	72	8		
Central Park Av.	Drake Av.	WB	2	2	200	400	44
		WB	1	12	6	72	8
Drake Av.	Lincolnwood Av.	WB	2	12	6	72	8
		WB	1	12	8	96	11
		WB	2	12	8	96	11
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	2	12	10	120	13
		EB	1	12	6	72	8
		EB	2	12	6	72	8
Lincolnwood Av.	Ewing Av.	EB	1	12	6	72	8
		EB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
Ewing Av.	McCormick Blvd	EB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8

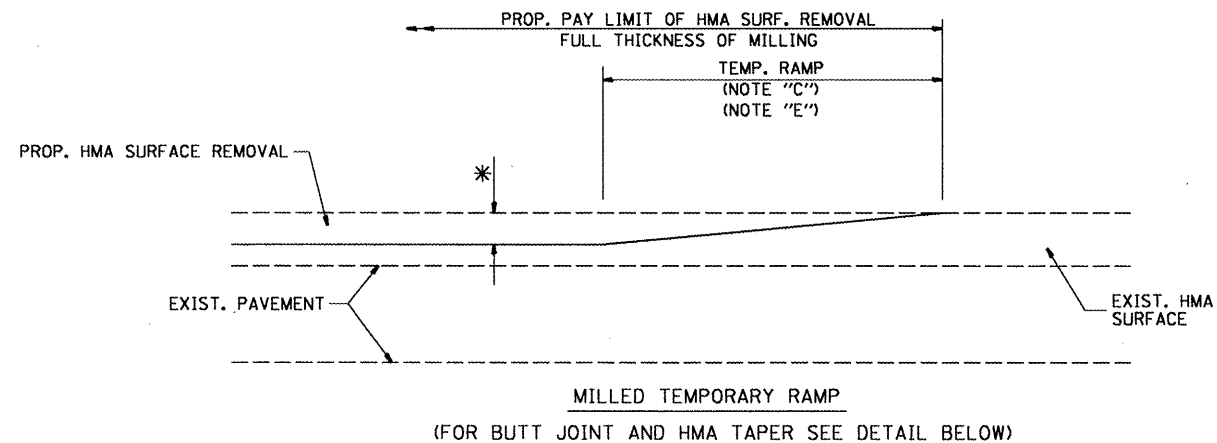
TOTALS: 5853 FT 2023 SY



ROUTE: North Cook County - Various Routes and Limits

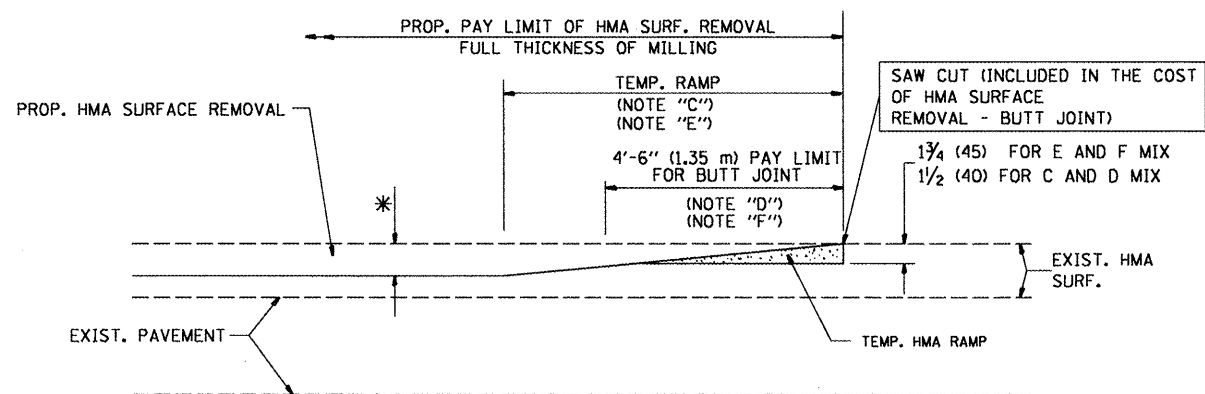
ROUTE	CROSS STREETS		DIRECTION (EB/WB) (NB/SB)	REPAIR AREA (SQ YD)
	FROM	TO		
LINCOLN AVE.	DEVON AVE.	TOUHY AVE.	NB/SB	2400
MILWAUKEE AVE.	GOLF RD.	PALATINE RD.	NB/SB	4200
RAND RD.	WOLF RD.	ELK BLVD.	EB/WB	1100

FILE NAME = c:\p\work\PWIDDT\SMITHKL\0125091\Desig	USER NAME = smithkl	DESIGNED -	REVISED - 7/17/08 - DPW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PATCHING SCHEDULE VARIOUS ROUTES</b>				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	VAR.	2009-024 PP	COOK	33 25
		CHECKED -	REVISED -								<b>CONTRACT NO. 60G23</b>			
		DATE -	REVISED -								FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			



**OPTION 1**

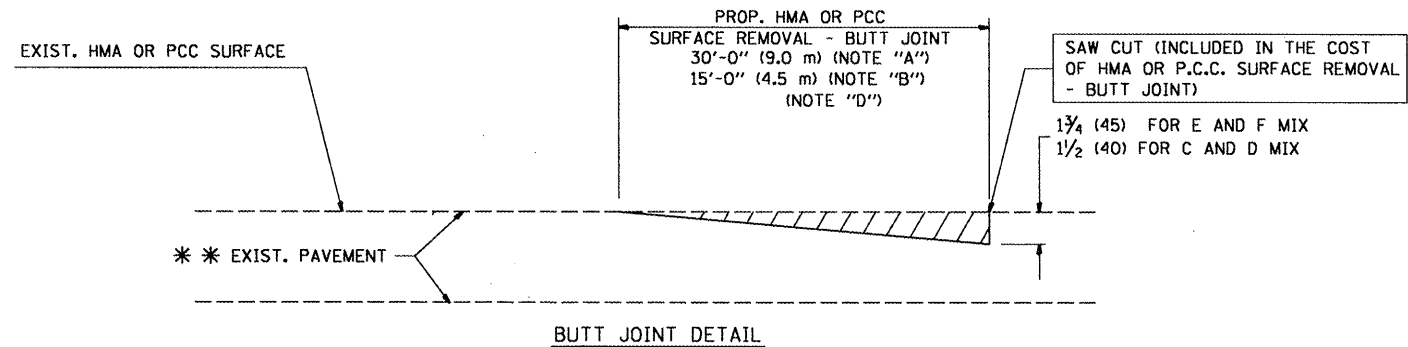
MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)



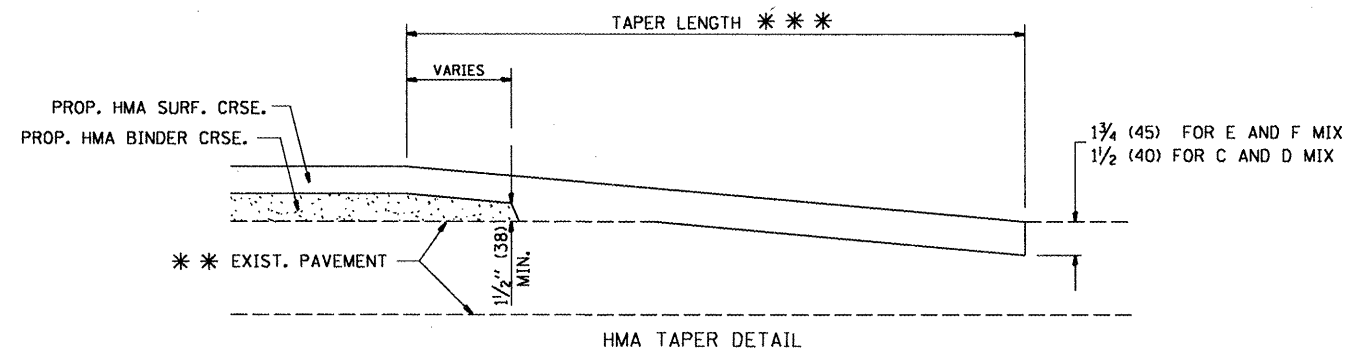
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

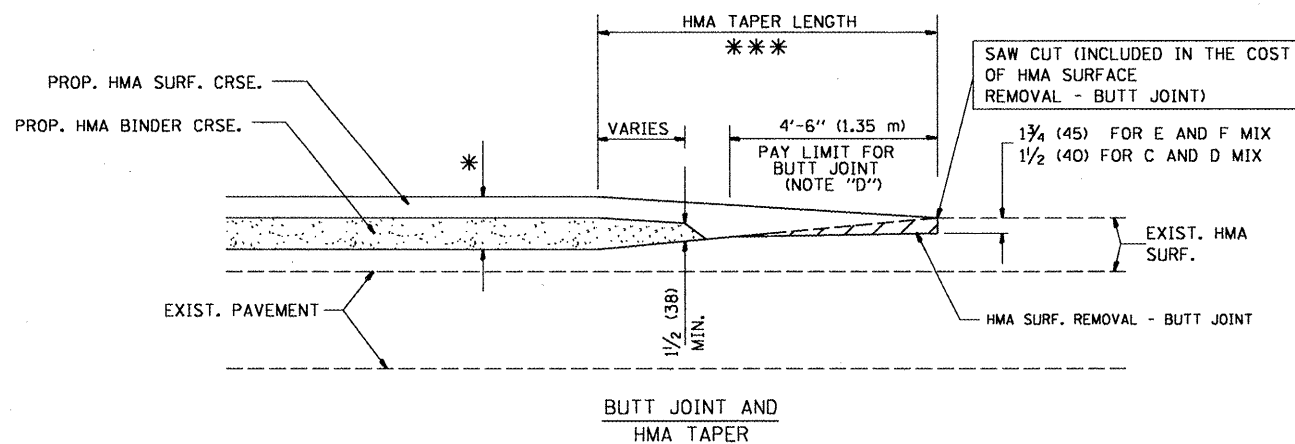
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

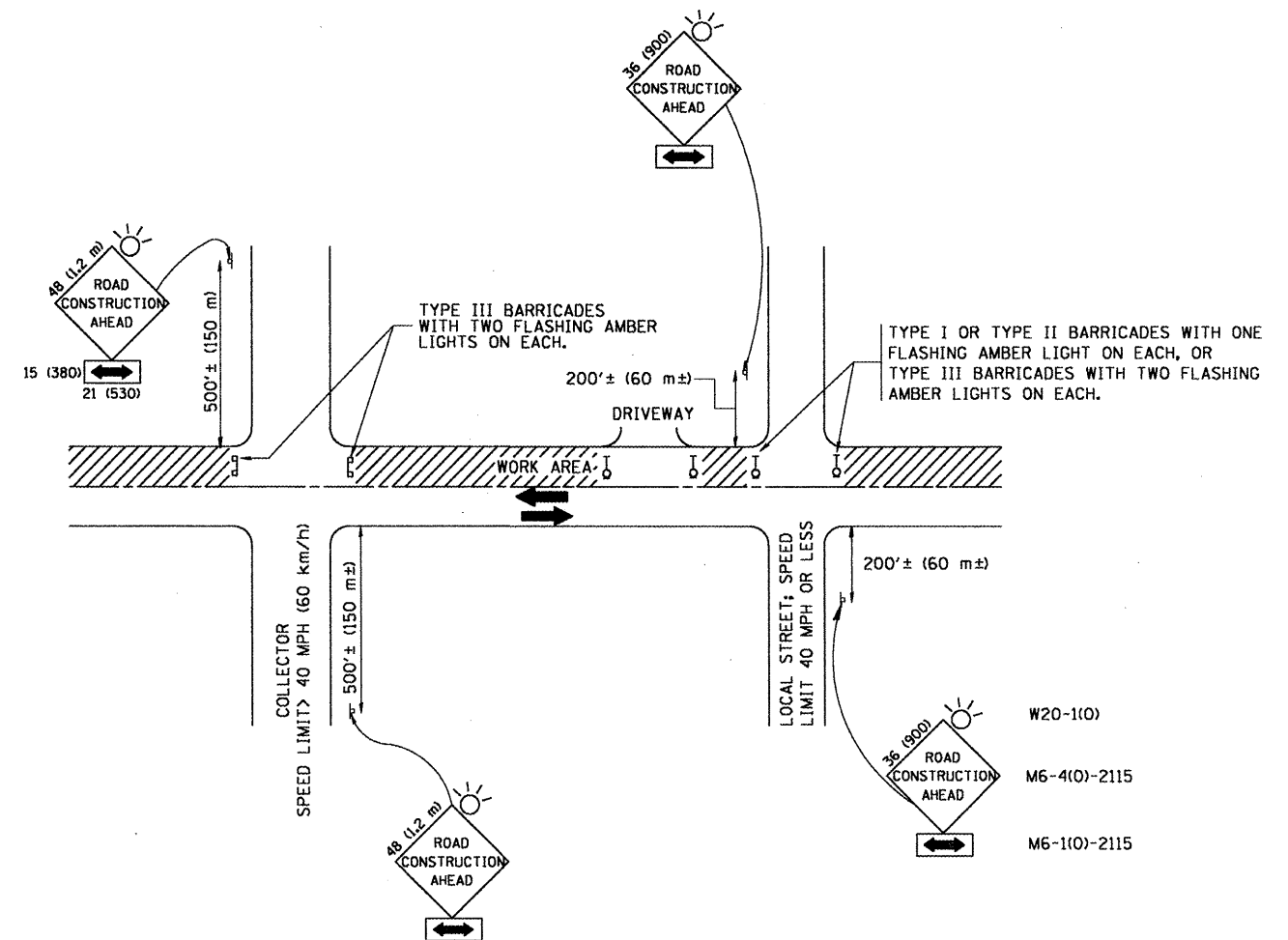
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND  
HMA TAPER DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	26
BD400-05 BD32			CONTRACT NO. 60G23	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

FILE NAME =	USER NAME = smthkl	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
cr:\pwork\pwork\SMITHKL\08125091\01stfz.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / IN.	CHECKED -		REVISED - M. GOMEZ 04-06-01
PLDT DATE = 2/3/2009	DATE - 06-13-90		REVISED - R. BORO 01-01-07



**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

**NOTES:**

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
    1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
      - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
      - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
    2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
      - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
      - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
  - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
  - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

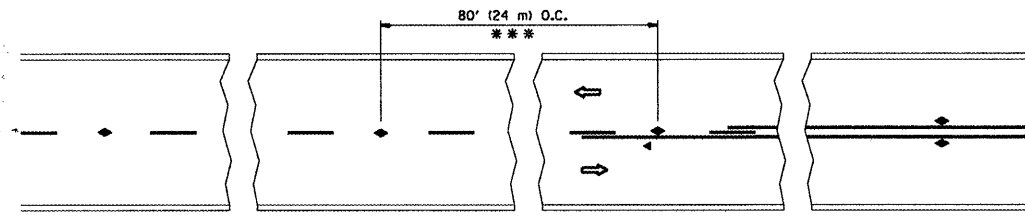
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		CHECKED -	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 1/8" = 1'-0"		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 2/3/2009			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

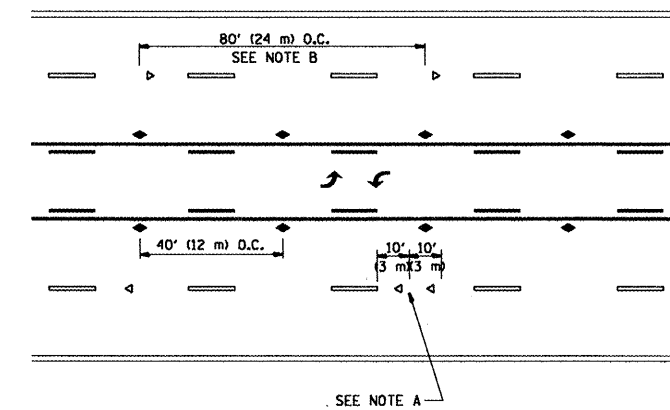
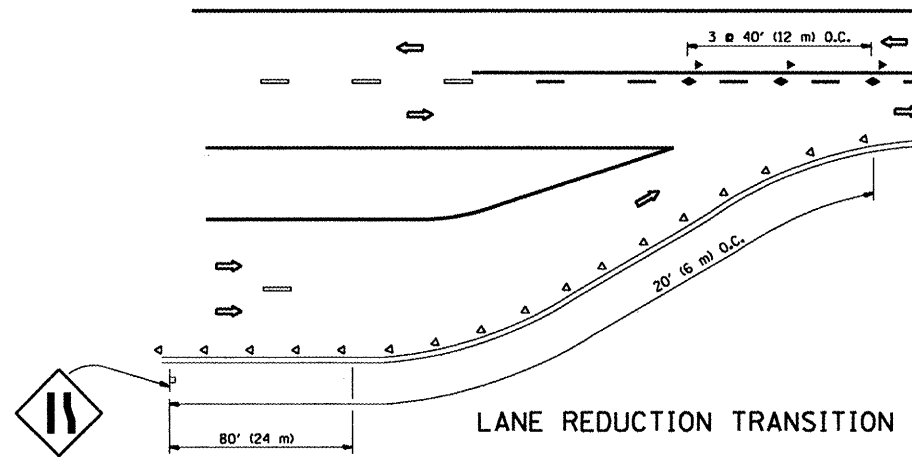
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	27
TC-10		CONTRACT NO. 60C23		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

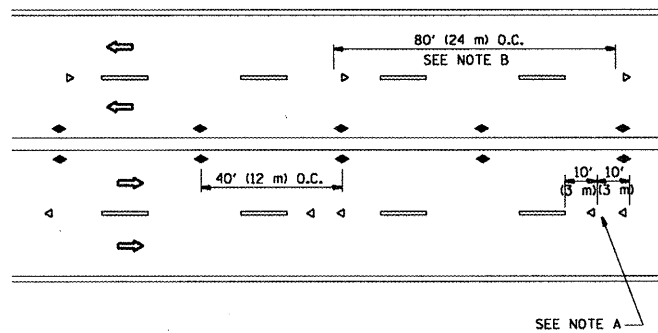


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

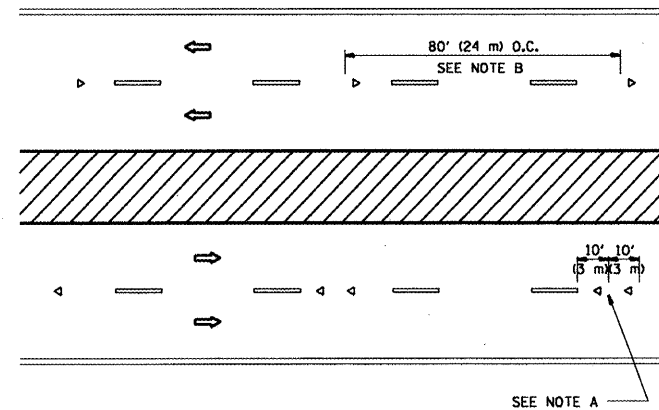
TWO-LANE/TWO-WAY



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

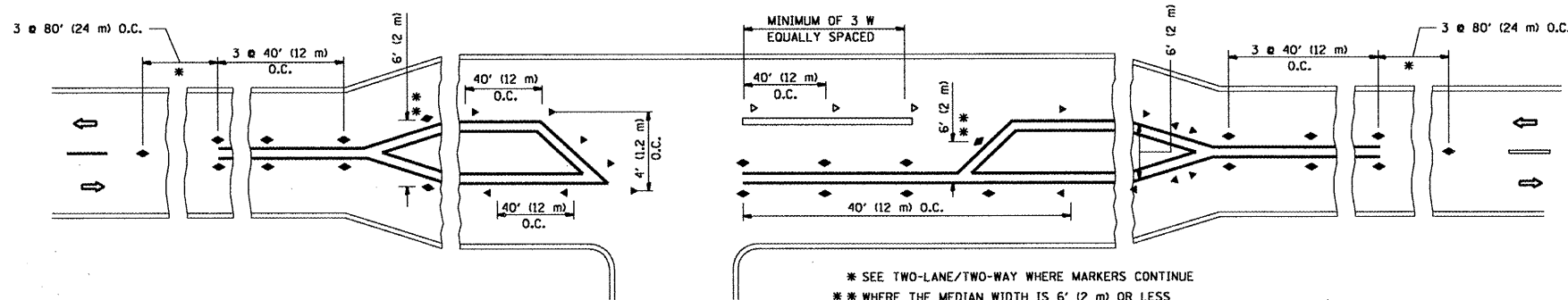
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

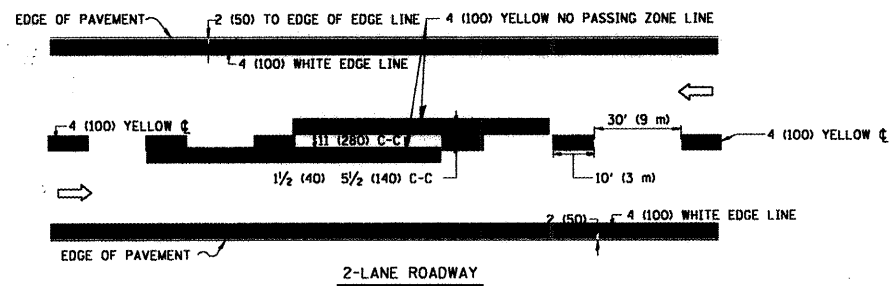
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
...	...	DRAWN -	REVISED - T. RAMMACHER 03-12-99
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00	
PLOT DATE = 2/3/2009	DATE -	REVISED -	

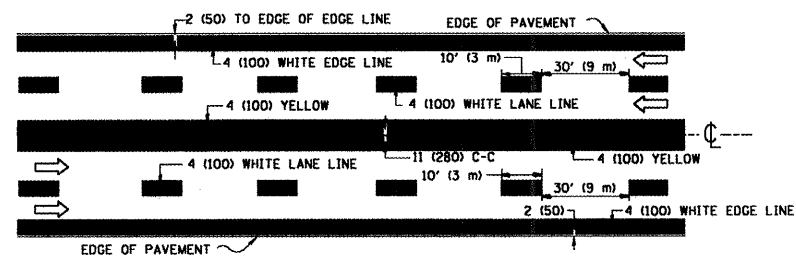
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLow RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

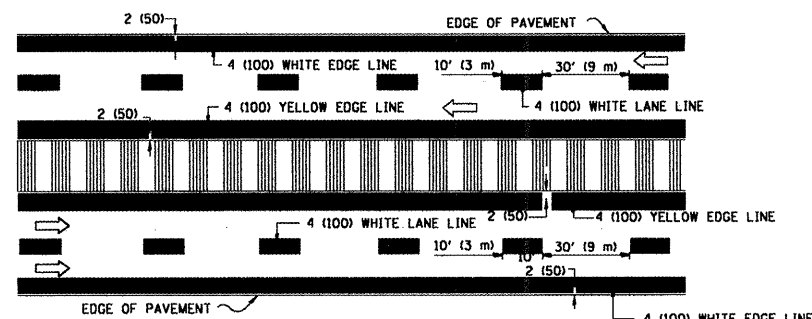
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	28
TC-11			CONTRACT NO. 60623	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY



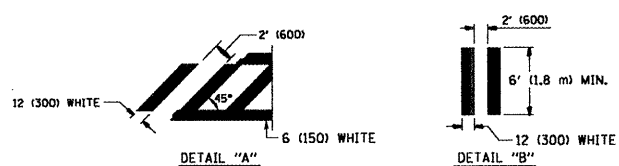
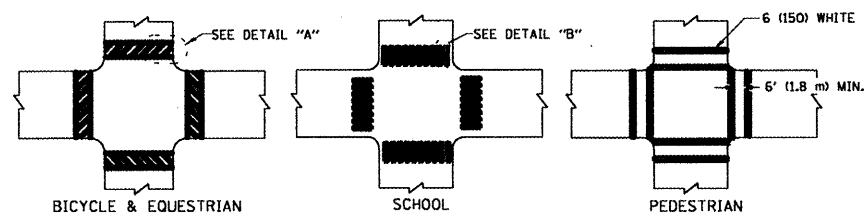
MULTI-LANE UNDIVIDED



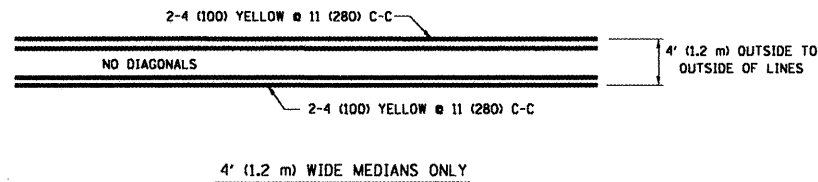
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIUM

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

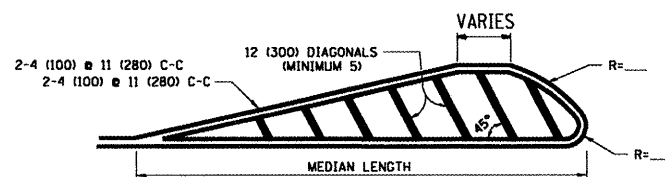
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



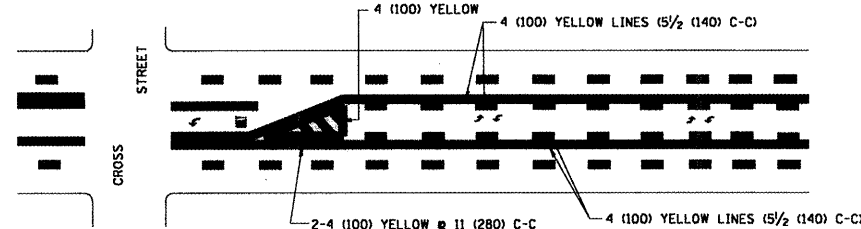
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

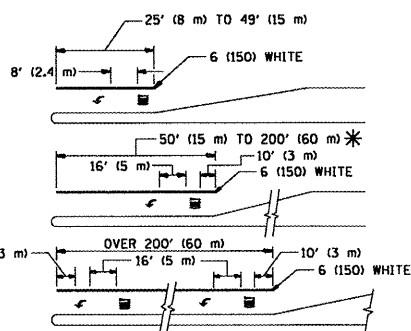
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

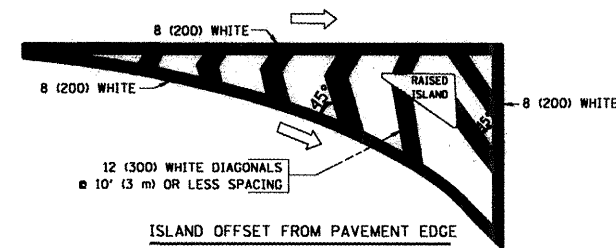


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

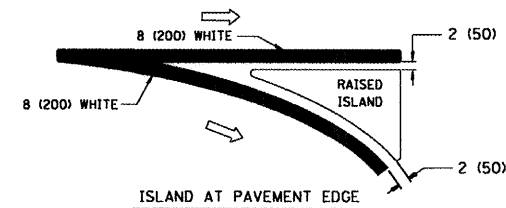
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

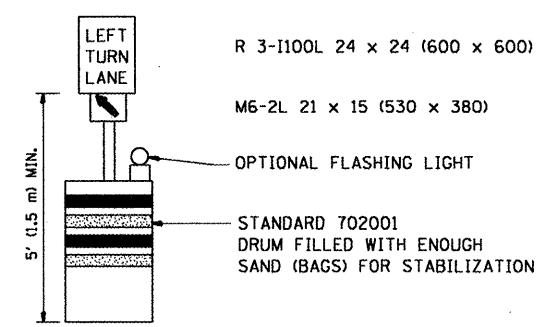
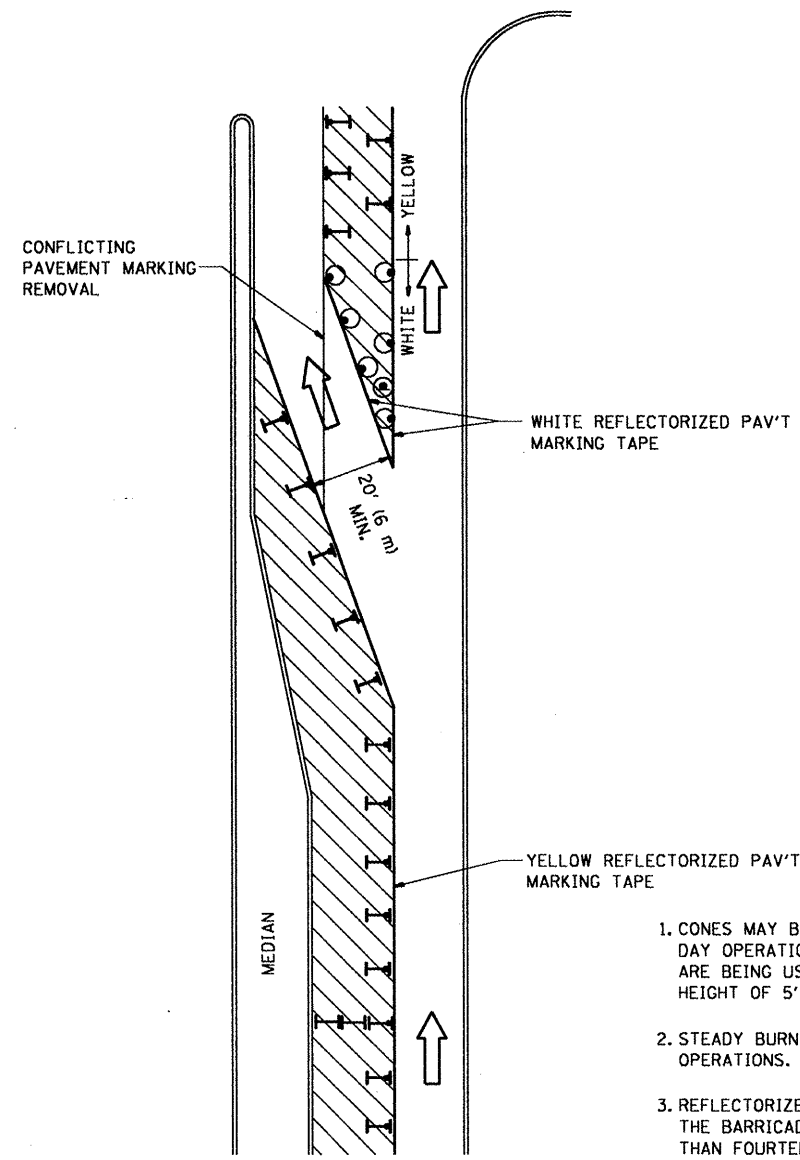
All dimensions are in inches (millimeters) unless otherwise shown.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	29
TC-13		CONTRACT NO. 60G23		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

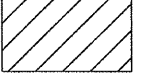







**GENERAL NOTES**

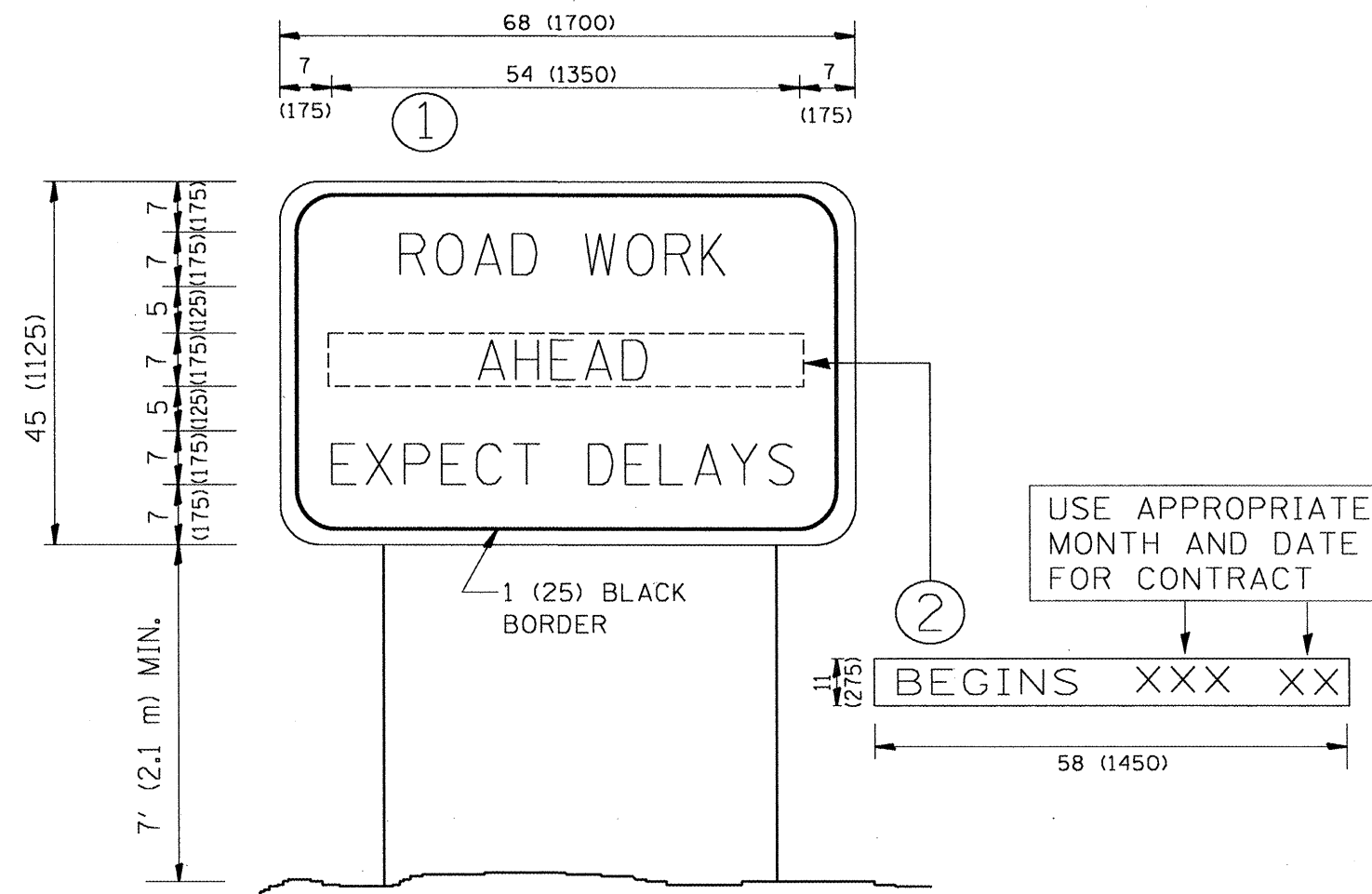
1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

**LEGEND**

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -T. RAMMACHER 09-08-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cat\pwork\PWIDOT\SMITHKL\d0125091\01st.dgn		DRAWN -	REVISED - A. HOUSEH 11-07-95			VAR.	2009-024 PP	COOK	33	30
		CHECKED -	REVISED - A. HOUSEH 10-12-96			<b>TC-14</b>		<b>CONTRACT NO. 60G23</b>		
		DATE -	REVISED -T. RAMMACHER 01-06-00			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

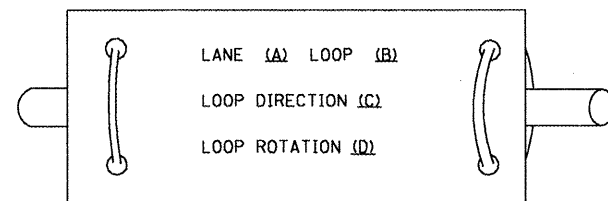
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = amthkl	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ct:\pw_work\PWIDOT\SMITHKL\d0125091\Dist.td.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	VAR.			2009-024 PP	COOK	33	31	
PLOT SCALE = 1/80.0000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 60623				
PLOT DATE = 2/3/2009	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT	

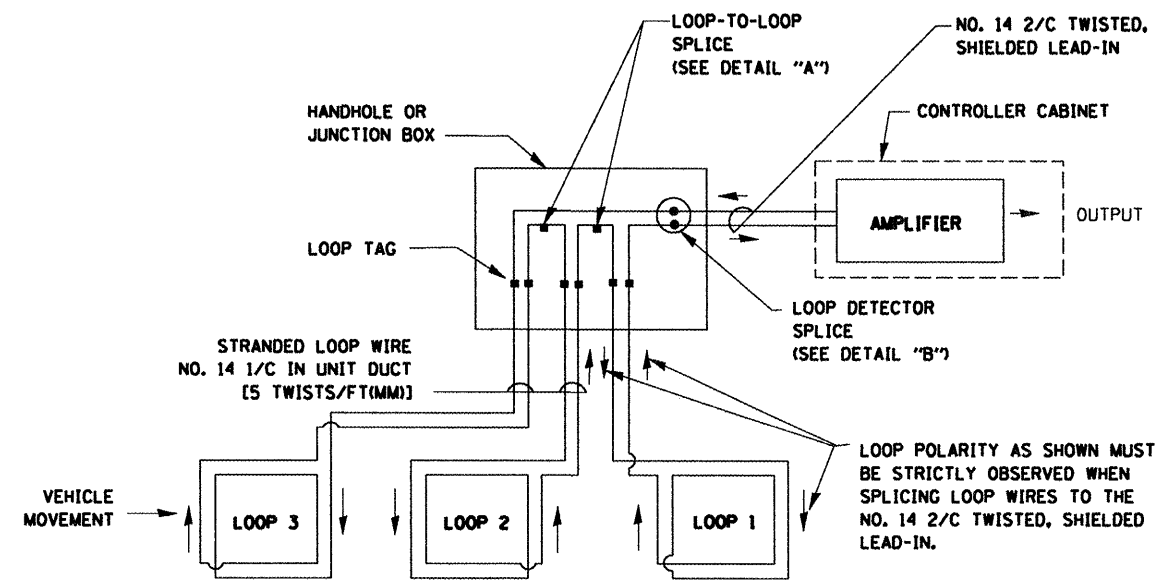
## LOOP DETECTOR NOTES

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PERFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

### LOOP LEAD-IN CABLE TAG

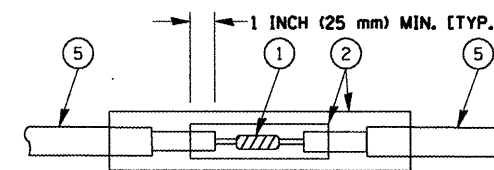


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

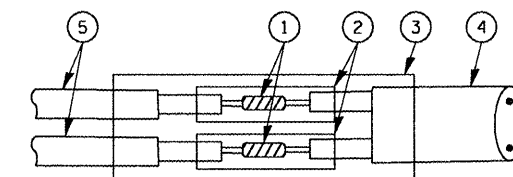


### DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

### LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

FILE NAME =	USER NAME = smthkl	DESIGNED - D.A.D.	REVISED - 11-12-01
cd:\pw\work\PW100T\SMITHKL\08125091\Dist1	td.dgn	DRAWN - R.W.P.	REVISED - BUR, TRAFFIC 01-01-02
PLOT SCALE = 100.0000' / IN.	CHECKED - D.A.Z.	REVISED -	
PLOT DATE = 2/3/2009	DATE - 05-30-00	REVISED -	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

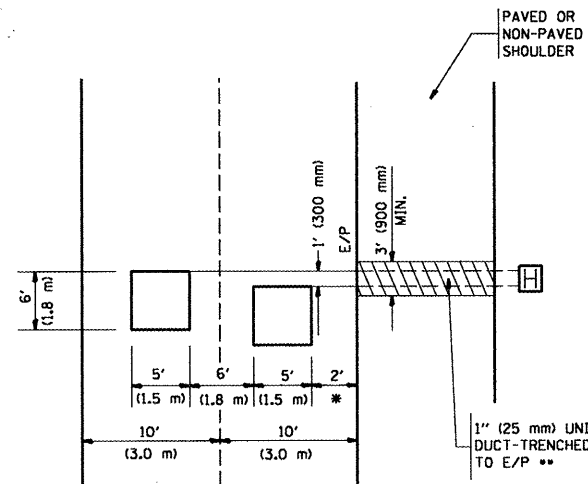
SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	32
TS-05			CONTRACT NO. 60G23	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

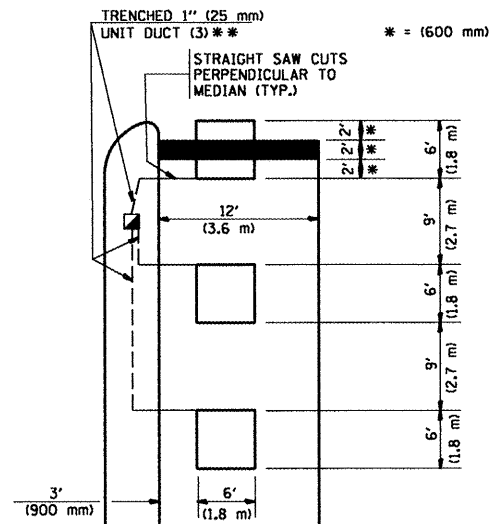


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)

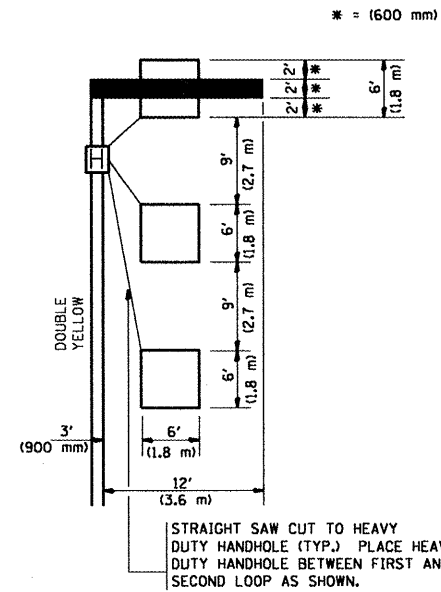
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

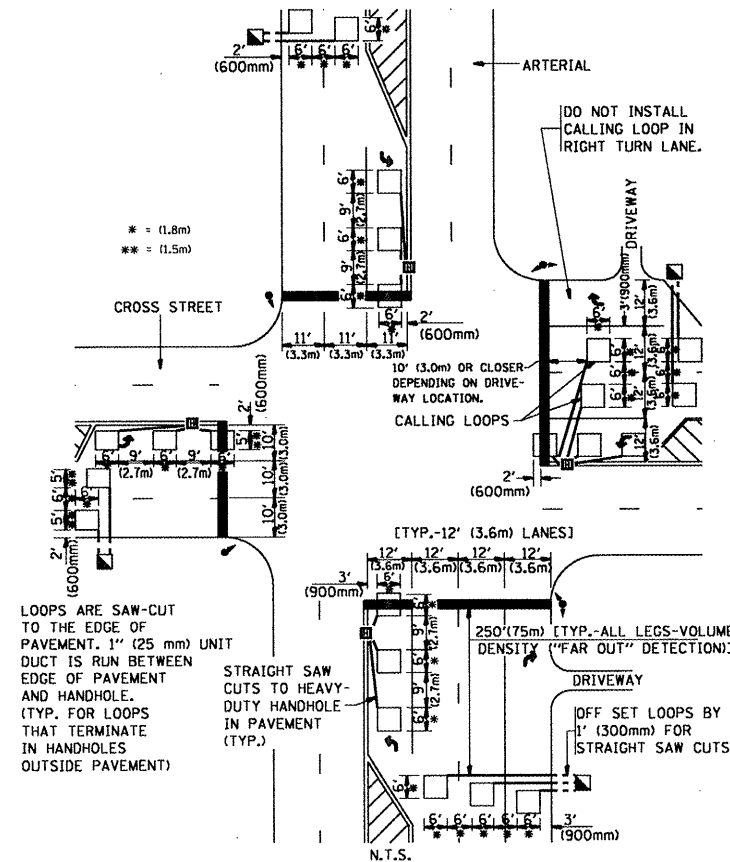
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

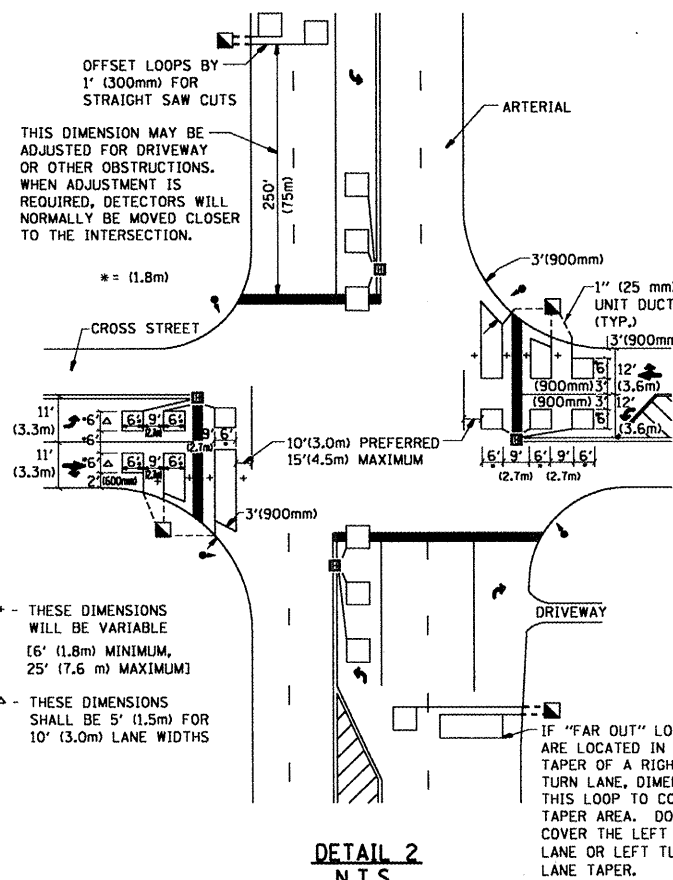
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2  
N.T.S.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
VAR.	2009-024 PP	COOK	33	33
TS-07			CONTRACT NO. 60G23	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				