

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MC HENRY	35	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 63138		

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

VILLAGE OF ALGONQUIN
McHENRY COUNTY
C-91-206-09

L.A. SECTION NO. 08-00075-00-RS
PROJECT No. M-9003(174)
RESURFACING

SQUARE BARN ROAD (FAU 3866) (VILLAGE LIMITS TO ALGONQUIN ROAD (FAU 3878)),
HANSON ROAD (FAU 4011) (HUNTINGTON DRIVE (FAU 94013) TO ALGONQUIN ROAD (FAU 3878)),
HUNTINGTON DRIVE (FAU 4013) (GOLDEN EAGLE DRIVE (FAU 94013) TO STONEGATE ROAD (FAU 1889)),
SANDBLOOM ROAD (FAU 4005) (VILLAGE LIMITS TO ALGONQUIN ROAD (FAU 3878))

INDEX OF SHEETS

DESCRIPTION	SHEET NO.
TITLE SHEET	1
SUMMARY OF QUANTITIES	2
GENERAL NOTES AND IDOT STANDARDS	3
EXISTING TYPICAL SECTIONS	4-7
PROPOSED TYPICAL SECTIONS	8-11
EXISTING CONDITIONS AND REMOVAL PLAN	12-20
PROPOSED PLAN	21-29
CONSTRUCTION DETAILS	30
BUTT JOINT AND HMA TAPER DETAILS	31
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	32
DISTRICT ONE TYPICAL PAVEMENT MARKINGS	33
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	34
PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	35

PROJECT ENDS
STA. 315 + 00

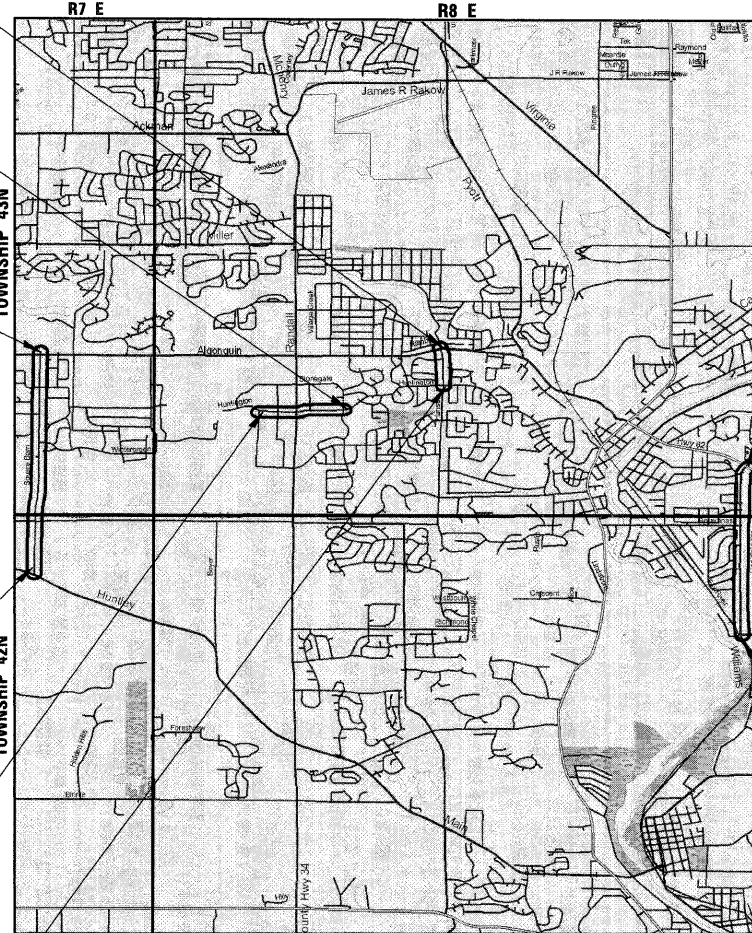
PROJECT ENDS
STA. 282 + 80

PROJECT ENDS
STA. 202 + 00

PROJECT BEGINS
STA. 122 + 00

PROJECT BEGINS
STA. 251 + 11.2

PROJECT BEGINS
STA. 304 + 80



LOCATION MAP

PROJECT ENDS
STA. 477 + 50

TRAFFIC DATA

SQUARE BARN ROAD
ADT (2008) = 12,045
SPEED LIMIT = 40 MPH

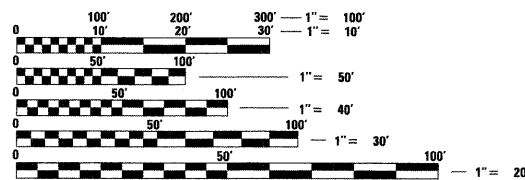
HANSON ROAD
ADT (2008) = 9,425
SPEED LIMIT = 30 MPH

HUNTINGTON DRIVE
ADT (2008) = 7,100
SPEED LIMIT = 30 MPH

SANDBLOOM ROAD
ADT (2008) = 13,325
SPEED LIMIT
(ALGONQUIN RD TO COMPTON AVE) = 35 MPH
(COMPTON AVE TO BOLZ RD) = 40 MPH

DESIGN DESIGNATION: COLLECTOR

PROJECT BEGINS
STA. 411 + 50



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
8-1-1 OR 1-800-892-0123

CONTRACT NO. 63138

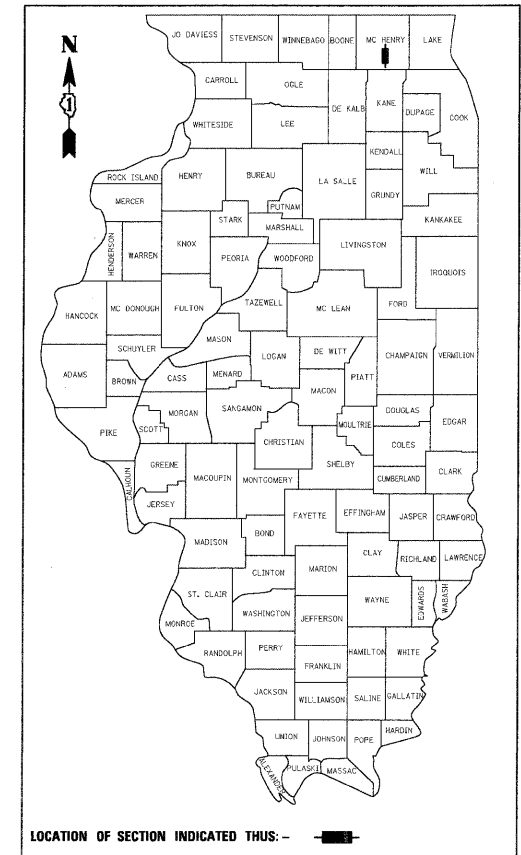
CB **CHRISTOPHER B. BURKE** ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018 (847) 823-0500

SQUARE BARN ROAD
GROSS LENGTH OF PROJECT = 8,000 LINEAL FEET (1.52 MILES)
NET LENGTH OF PROJECT = 8,000 LINEAL FEET (1.52 MILES)

HANSON ROAD
GROSS LENGTH OF PROJECT = 1,020 LINEAL FEET (0.19 MILES)
NET LENGTH OF PROJECT = 1,020 LINEAL FEET (0.19 MILES)

HUNTINGTON DRIVE
GROSS LENGTH OF PROJECT = 3,170 LINEAL FEET (0.60 MILES)
NET LENGTH OF PROJECT = 3,170 LINEAL FEET (0.60 MILES)

SANDBLOOM ROAD
GROSS LENGTH OF PROJECT = 6,600 LINEAL FEET (1.25 MILES)
NET LENGTH OF PROJECT = 6,600 LINEAL FEET (1.25 MILES)



LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED: *January 28, 2009*
[Signature]
PUBLIC WORKS DIRECTOR, VILLAGE OF ALGONQUIN

PASSED: *February 18, 2009*
[Signature]
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR
BID BASED ON
LIMITED REVIEW: *February 18, 2009*
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

LEE M. FELL
ILLINOIS REGISTRATION No. 062-053708
EXPIRATION DATE: 11/30/2009

[Signature] 2/14/09
ENGINEER DATE

FIELD ENGINEER: KEVIN STALLWORTH 847-705-4169

SUMMARY OF QUANTITIES

SPECIALTY ITEM	ITEM NUMBER	ITEM	UNIT	SQUARE BARN ROAD	HANSON ROAD	HUNTINGTON DRIVE	SANDBLOOM ROAD	TOTAL
				QUANTITY	QUANTITY	QUANTITY	QUANTITY	I000-2A QUANTITY
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	0	0	246	0	246
	20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	0	0	246	0	246
	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	0	0	1109	0	1109
	21301048	EXPLORATION TRENCH 48" DEPTH	FOOT	0	0	45	0	45
	25200200	SUPPLEMENTAL WATERING	UNIT	5	5	5	5	20
	35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	72	94	143	25	334
	35800100	PREPARATION OF BASE	SQ YD	0	0	6720	0	6720
	35800200	AGGREGATE BASE REPAIR	TON	0	0	388	0	388
	40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	20	20	20	20	80
	40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	3926	432	1519	1362	7239
	40600300	AGGREGATE (PRIME COAT)	TON	78	9	30	27	144
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	0	0	334	0	334
	40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	1540	169	0	559	2268
	40600895	CONSTRUCTING TEST STRIP	EACH	1	1	1	1	4
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	0	0	966	0	966
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	67	0	1962	25	2054
	40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5130	565	0	2698	8393
	42000300	PORTLAND CEMENT CONCRETE PAVEMENT 8"	SQ YD	115	0	150	45	310
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	0	0	148	83	231
	42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH, SPECIAL	SQ FT	70	0	1000	50	1120
	42400800	DETECTABLE WARNINGS	SQ FT	144	0	240	66	450
	44000151	HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"	SQ YD	4921	0	0	12970	17891
	44000154	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"	SQ YD	0	0	0	9665	9665
	44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	30772	3928	7750	0	42450
	44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	0	0	6720	0	6720
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	390	0	580	295	1265
	44000600	SIDEWALK REMOVAL	SQ FT	70	0	1000	50	1120
	44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1400	210	640	560	2810
	44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	35693	3928	7750	12970	60341
	44000100	PAVEMENT REMOVAL, 10"	SQ YD	115	0	150	45	310
	44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	891	98	380	571	1940
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	891	98	380	571	1940
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	891	98	380	571	1940
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	891	98	380	571	1940
	48101200	AGGREGATE SHOULDERS, TYPE B	TON	91	0	0	295	386
	67100100	MOBILIZATION	L SUM	0.25	0.25	0.25	0.25	1
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	0.25	0.25	0.25	0.25	1
	70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	0.25	0.25	0.25	0.25	1
	70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	0.25	0.25	0.25	0.25	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	0.25	0.25	0.25	0.25	1
	70300100	SHORT-TERM PAVEMENT MARKING	FOOT	9100	1020	3090	6600	19810
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	438	37	255	260	990
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	37200	4500	2600	27500	71800
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1985	180	2200	700	5065
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	900	0	1100	370	2370
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	100	60	305	115	580
*	85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	0	0	1	0	1
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	0	0	600	0	600
	XX002258	STRUCTURES TO BE ADJUSTED	EACH	10	5	5	10	30
	XX003535	AGGREGATE BASE COURSE, TYPE B, 2"	SQ YD	0	0	148	83	231
	X0325702	NIGHTTIME WORK ZONE LIGHTING	L SUM	0	0	1	0	1
	Z0013798	CONSTRUCTION LAYOUT	L SUM	0.25	0.25	0.25	0.25	1
	Z0017500	DRAINAGE & UTILITY STRUCTURE ADJUSTMENT (SPECIAL)	EACH	3	0	0	3	6
Δ	Z0076600	TRAINEES	HOUR	200	100	100	100	500

* SPECIALTY ITEMS
 Δ Y080

VILLAGE OF ALGONQUIN CONTACTS

PUBLIC WORKS DEPARTMENT
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 ALGONQUIN, IL 60102
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 WOODSTOCK, IL 60098

TRAFFIC ENGINEER
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 FAX: 815-334-4989
 CELL: 815-482-2867
 E-MAIL: BJCOSIN@CO.MCHENRY.IL.US

GENERAL NOTES

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, ITS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AND INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL.

PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, STRUCTURES TO BE ADJUSTED, DETECTOR LOOP REPLACEMENT, AND CLASS D PATCHES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001-05 INCLUDED IN THE SPECIFICATIONS).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES"

ADVANCED WARNING CHANGEABLE MESSAGE BOARDS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY 2 WEEKS PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION WITHIN THE VILLAGE'S RIGHT-OF-WAY NOTIFYING THE MOTORING PUBLIC OF THE UPCOMING WORK (ROAD CONSTRUCTION/LANE REDUCTION BEGINNING...)

FRESH OIL SIGNS SHALL BE POSTED AT BOTH ENDS OF THE ROADWAY AND ALL SIDE STREETS AS DIRECTED BY THE ENGINEER. CONSTRUCTION AHEAD SIGNS SHALL BE PLACED AT ALL SIDE STREETS AND BOTH ENDS OF THE ROADWAY WHILE CONSTRUCTION IS IN PROGRESS.

PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, THE WORK IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER SIGNS AND BARRICADES HAVE BEEN INSTALLED.

AT NO TIME SHALL LESS THAN HALF OF THE STREET BE AVAILABLE FOR PARKING.

ALL ROADS MUST HAVE ONLY ONE LONGITUDINAL JOINT WHILE PAVING.

SOD SALT TOLERANT AND TOP SOIL, 4" RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND SIDEWALK REMOVAL AND REPLACEMENT.

ALL WORK ON HUNTINGTON DRIVE MUST BE COMPLETED WHILE SCHOOL IS NOT IN SESSION, SPECIFICALLY BETWEEN JUNE 15TH AND AUGUST 1ST.

ALL WORK ON HUNTINGTON DRIVE WEST OF RANDALL SHALL BE COMPLETED DURING NIGHT TIME HOURS BETWEEN 9 PM AND 7 AM DUE TO HEAVY TRAFFIC.

ALL TRAFFIC SIGNAL DETECTOR LOOPS SHALL BE INSTALLED IN THE BINDER PRIOR TO THE INSTALLATION OF SURFACE COURSE.

SUBSURFACE EXPLORATION DATA

LIMITED INVESTIGATION OF SUBSURFACE CONDITIONS AT THE PROPOSED SITE OF WORK HAS BEEN MADE FOR THE PURPOSE OF DESIGN. VILLAGE OF ALGONQUIN, TESTING SERVICE CORPORATION, AND CHRISTOPHER B. BURKE ENGINEERING, LTD. ASSUME NO RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE SUFFICIENCY OR ACCURACY OF THESE PRELIMINARY INVESTIGATIONS.

UTILITY COORDINATION

Table with columns: UTILITY COMPANY, AT&T (SBC), MCHENRY COUNTY DIVISION OF TRANSPORTATION, COMCAST, COMMONWEALTH EDISON, NICOR GAS. Rows include contact persons, phone/fax numbers, addresses, and status on date of plan printing.

SITE INVESTIGATION AND CONDITIONS AFFECTING THE WORK

THE BIDDER ACKNOWLEDGES THAT, PRIOR TO SUBMISSION OF ITS BID, IT HAS TAKEN STEPS NECESSARY TO ASCERTAIN THE NATURE AND LOCATION OF THE WORK, AND THAT IT HAS INVESTIGATED, CONFIRMED, VERIFIED AS CORRECT AND SATISFIED ITSELF AS TO THE GENERAL AND LOCAL CONDITIONS WHICH CAN AFFECT THE WORK OR ITS COSTS.

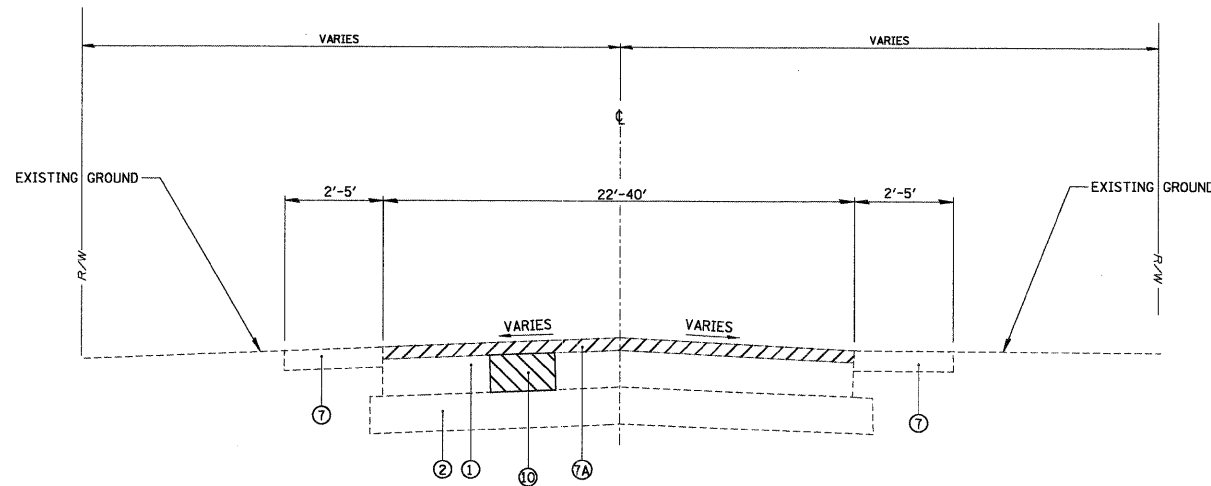
HIGHWAY STANDARDS

Table of Highway Standards with columns: Item Number, Description, and Notes. Includes items for symbols, curbs, patches, lane closures, and pavement markings.

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

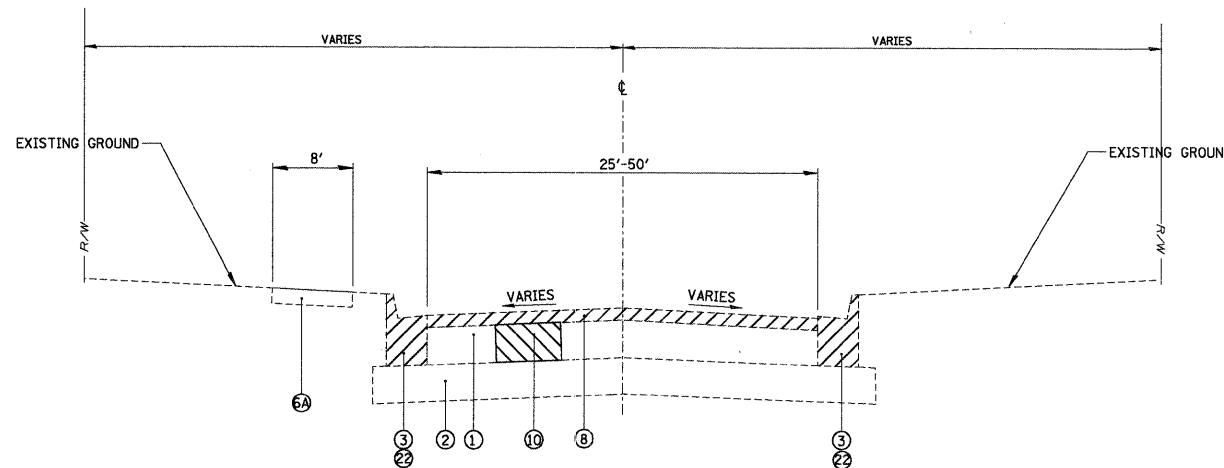
GENERAL NOTES AND IDOT STANDARDS

Project information form including file name, user name, design/revision details, scale, sheet number (35 of 3), and project location (FED. ROAD DIST. NO. 1 ILLINOIS).



SQUARE BARN ROAD
EXISTING TYPICAL SECTION
STA. 122+00 TO STA. 135+00

LOCATION #1 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)



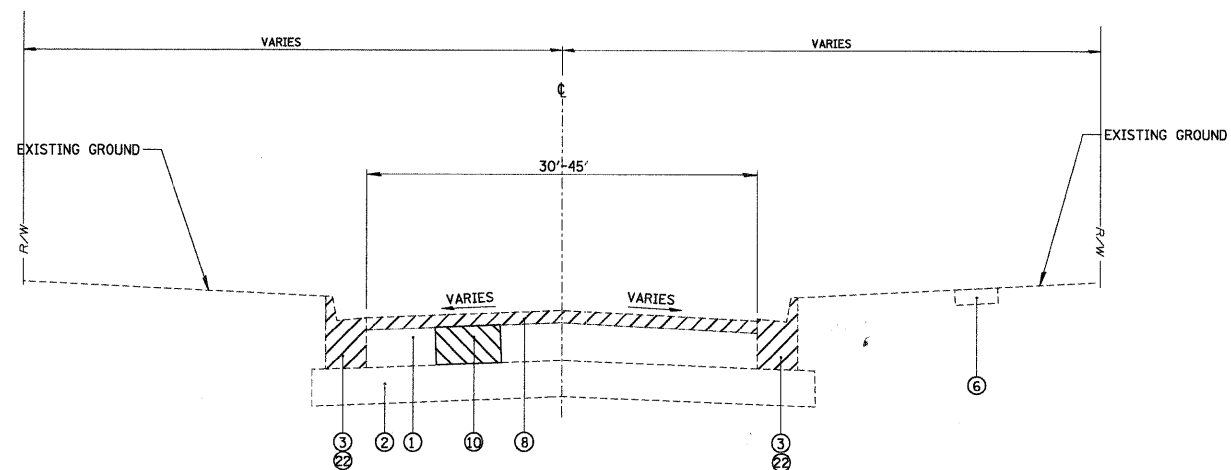
SQUARE BARN ROAD
EXISTING TYPICAL SECTION
STA. 135+00 TO STA. 202+00

LOCATION #2 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SQUARE BARN ROAD EXISTING TYPICAL SECTIONS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\VALCONQUIN\070273\070273.00019\Civil\SEC_TYPSSEC_SQUARE_BARN.dgn					DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	4
PLOT SCALE = NTS					CHECKED - LMF	REVISED -	CONTRACT NO. 63138					
PLOT DATE = 2/13/2009					DATE - 2/13/2009	REVISED -	SCALE: NTS			SHEET NO. 1 OF 4 SHEETS		
						STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



HANSON ROAD
EXISTING TYPICAL SECTION
STA. 304+80 TO STA. 315+00

LOCATION #3 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)

LEGEND

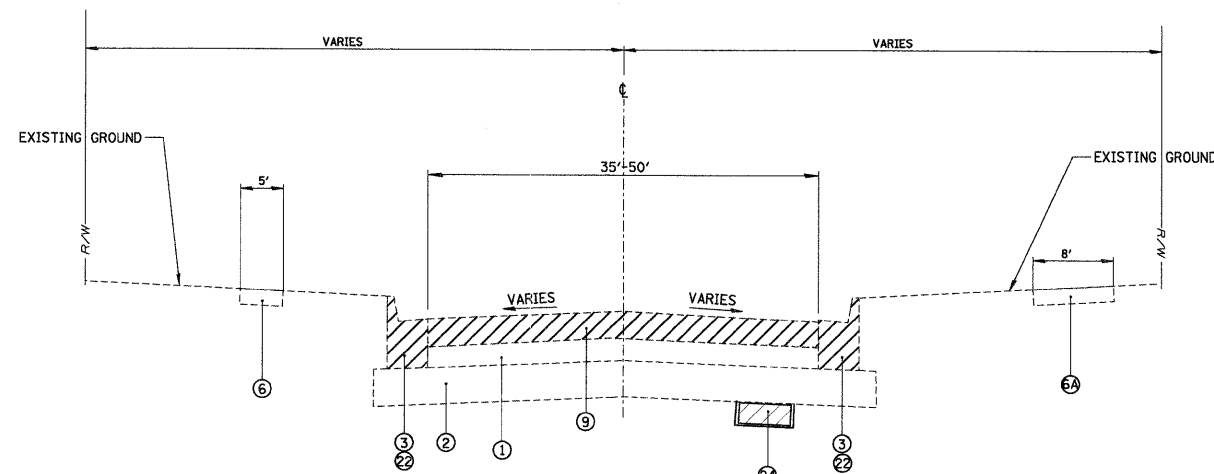
- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -
\\NALGONQUIN\070273\070273.00019\Civil\EX_TYPSSEC_HANSON.dgn		DRAWN - MDH	REVISED -
		CHECKED - LMF	REVISED -
		DATE - 2/13/2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

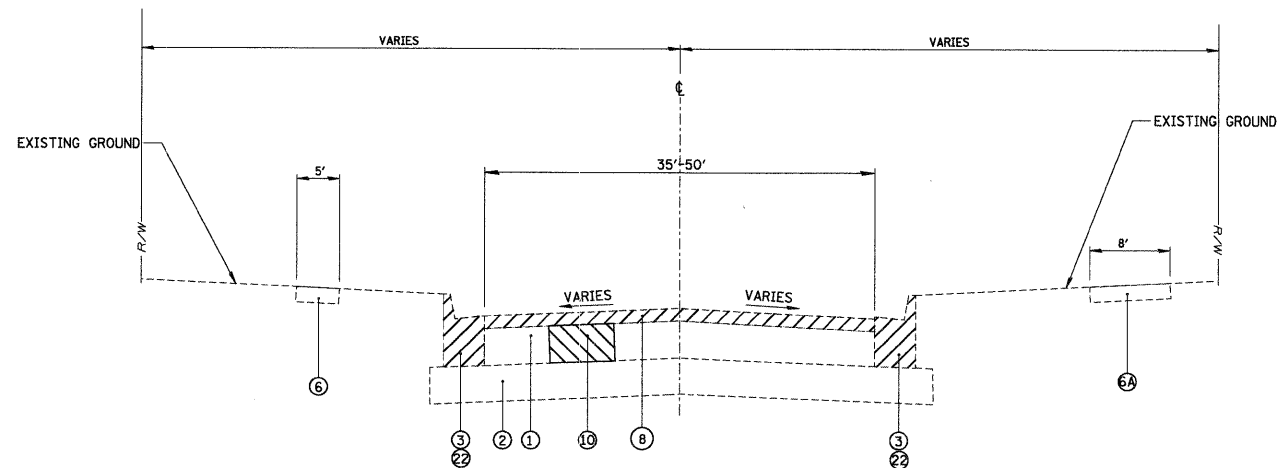
HANSON ROAD EXISTING TYPICAL SECTIONS	
SCALE: NTS	SHEET NO. 2 OF 4 SHEETS
STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	5
CONTRACT NO. 63138				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



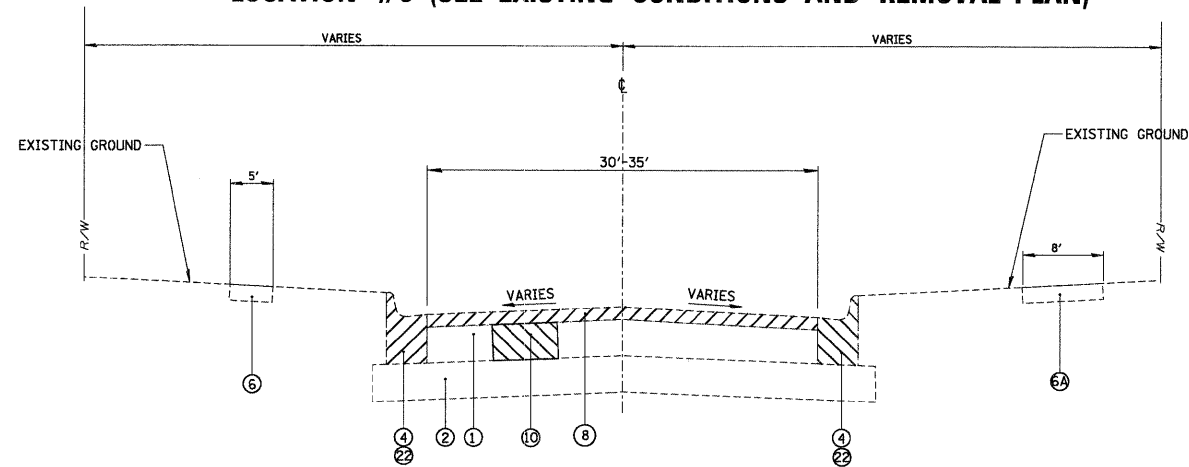
HUNTINGTON DRIVE
 EXISTING TYPICAL SECTION
 STA. 251+11.20 TO STA. 263+79.50
 (OMISSION AT RANDAL ROAD
 BETWEEN STA. 263+79.50 TO STA. 264+58.40)

LOCATION #4 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)



HUNTINGTON DRIVE
 EXISTING TYPICAL SECTION
 STA. 264+58.40 TO STA. 276+00

LOCATION #5 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)



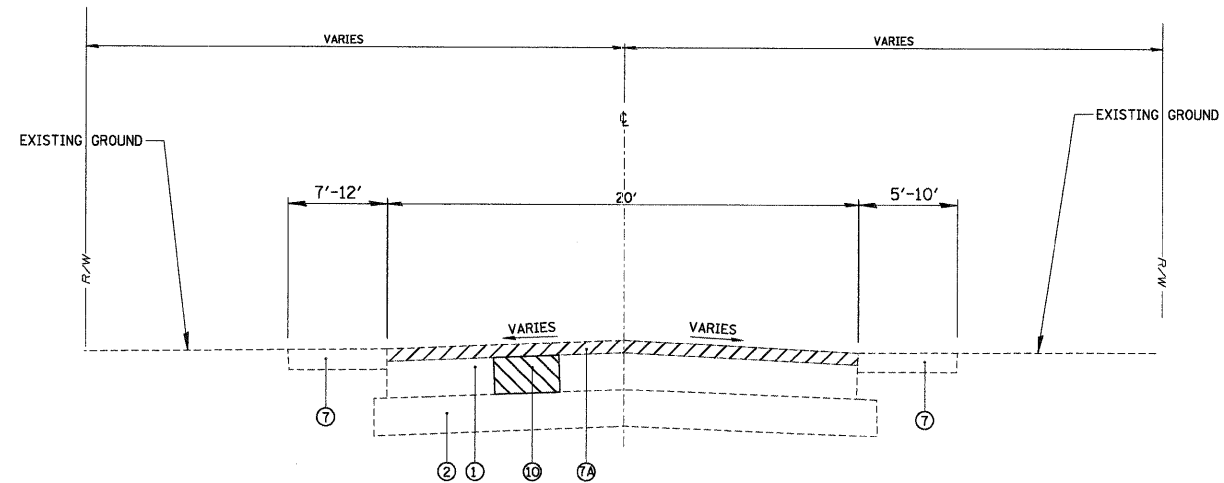
HUNTINGTON DRIVE
 EXISTING TYPICAL SECTION
 STA. 276+00 TO STA. 282+80

LOCATION #6 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)

LEGEND

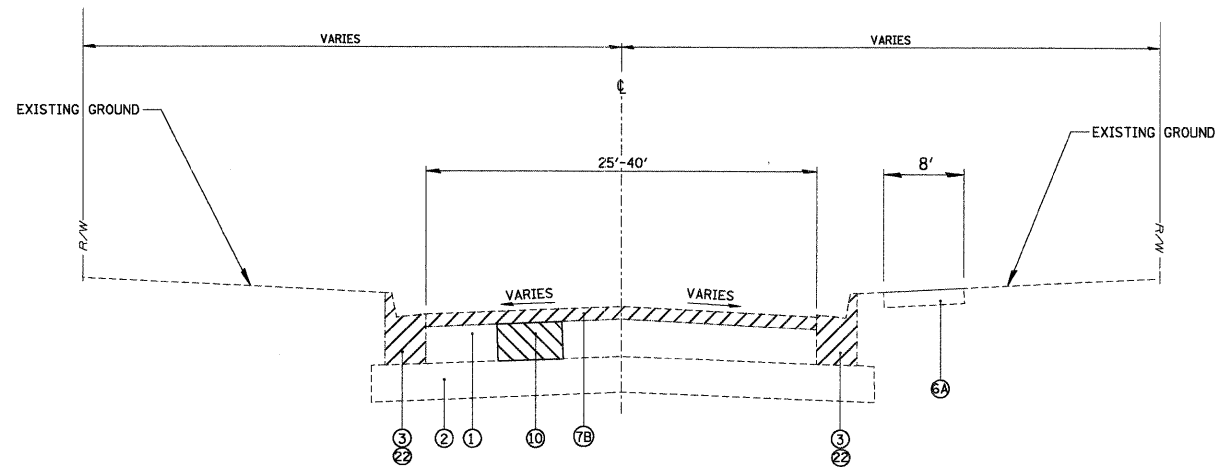
- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HUNTINGTON DRIVE EXISTING TYPICAL SECTIONS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\VALCONQUIN\070273\070273.00019\Civil\EX_TYPSEC_HUNTINGTON.dgn					DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	6
PLOT SCALE = NTS					CHECKED - LMF	REVISED -	CONTRACT NO. 63138					
PLOT DATE = 2/13/2009					DATE - 2/13/2009	REVISED -	SCALE: NTS			SHEET NO. 3 OF 4 SHEETS		



SANDBLOOM ROAD
 EXISTING TYPICAL SECTION
 STA. 411+50 TO STA. 426+55
 STA. 453+43 TO STA. 477+50

LOCATION #7 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)



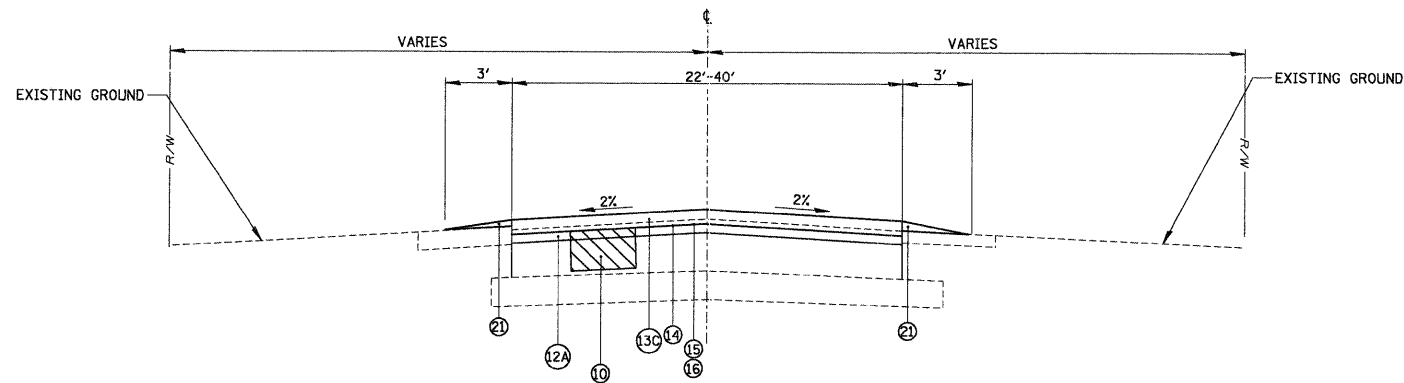
SANDBLOOM ROAD
 EXISTING TYPICAL SECTION
 STA. 426+55 TO STA. 453+43

LOCATION #8 (SEE EXISTING CONDITIONS AND REMOVAL PLAN)

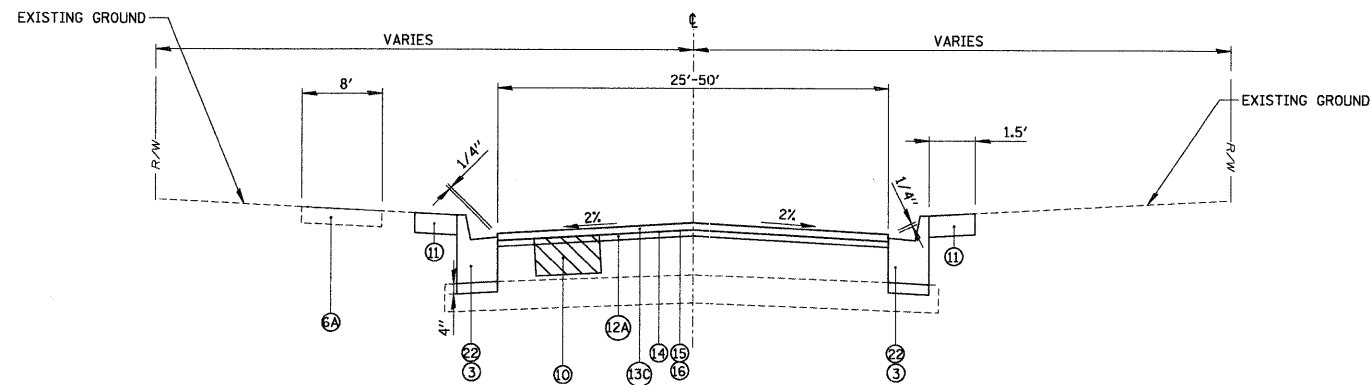
LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SANDBLOOM ROAD EXISTING TYPICAL SECTIONS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALCONQUIN\070273\070273.00019\Civil\EX_TYPSSEC_SANDBLOOM.dgn					DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	7
PLOT SCALE = NTS					CHECKED - LMF	REVISED -	CONTRACT NO. 63138					
PLOT DATE = 2/13/2009					DATE - 2/13/2009	REVISED -	SCALE: NTS SHEET NO. 4 OF 4 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



SQUARE BARN ROAD
 PROPOSED TYPICAL SECTION
 STA. 122+00 TO STA. 135+00
LOCATION #1 (SEE PROPOSED PLAN)



SQUARE BARN ROAD
 PROPOSED TYPICAL SECTION
 STA. 135+00 TO STA. 202+00
LOCATION #2 (SEE PROPOSED PLAN)

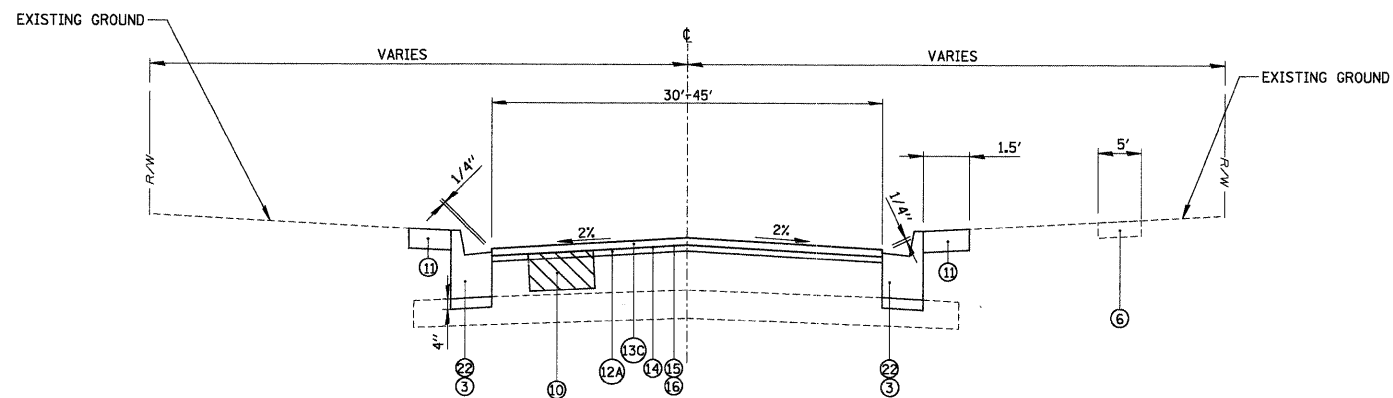
LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RED)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS ITEM	AC-TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm), 2" & 2 1/2"	PG 64-22	4%±50GYR.
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm), 1 1/2" & 2 1/2"	PG 64-22	4%±70GYR.
LEVELING BINDER (MACHINE METHOD), N50, 3/4"	PG 64-22*	4%±50GYR.
LEVELING BINDER (MACHINE METHOD), N70, 3/4"	PG 64-22*	4%±70GYR.
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N50, 2 1/2"	PG 64-22*	4%±50GYR.
CLASS D PATCHES, SPECIAL, 6" (HMA BINDER IL-19MM)	PG 64-22*	4%±70GYR.

NOTE:

- 1) THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
- 2) WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.
- 3) AGGREGATE BASE REPAIR - ANY NEW MATERIAL NECESSARY TO BRING THE EXISTING SUBBASE TO THE GRADE, MIN. DEPTH 9" CROSS SLOPE OR WIDTH SHOWN SHALL BE PAID FOR UNDER THIS ITEM.
- 4) ANY AGGREGATE BASE REMOVAL DUE TO PROPOSED ASPHALT SHALL BE CONSIDERED INCIDENTAL TO THE HOT-MIX ASPHALT SURFACE REMOVAL (FULL DEPTH)
- 5) POROUS GRANULAR EMBANKMENT (PGE) HAS BEEN PROVIDED AT THE LOCATIONS INDICATED FOR SOILS WHICH TEND TO BE UNSTABLE WHEN WET. ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGE WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER (BY USE OF A CONE PENETROMETER IN CONJUNCTION WITH THE IDOT SUBGRADE MANUAL). IF UNSUITABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGE AND GROUND FABRIC FOR GROUND STABILIZATION. NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.



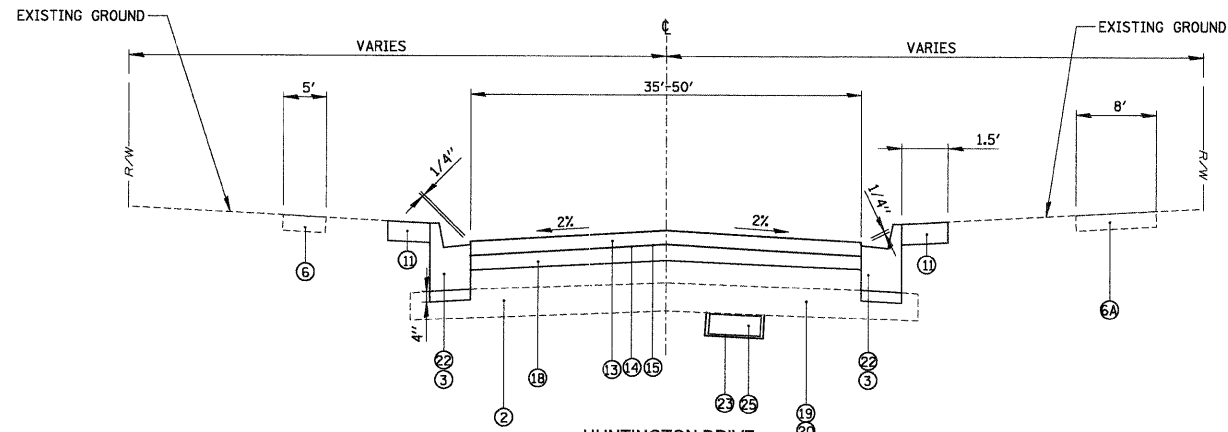
HANSON ROAD
 PROPOSED TYPICAL SECTION
 STA. 304+80 TO STA. 315+00

LOCATION #3 (SEE PROPOSED PLAN)

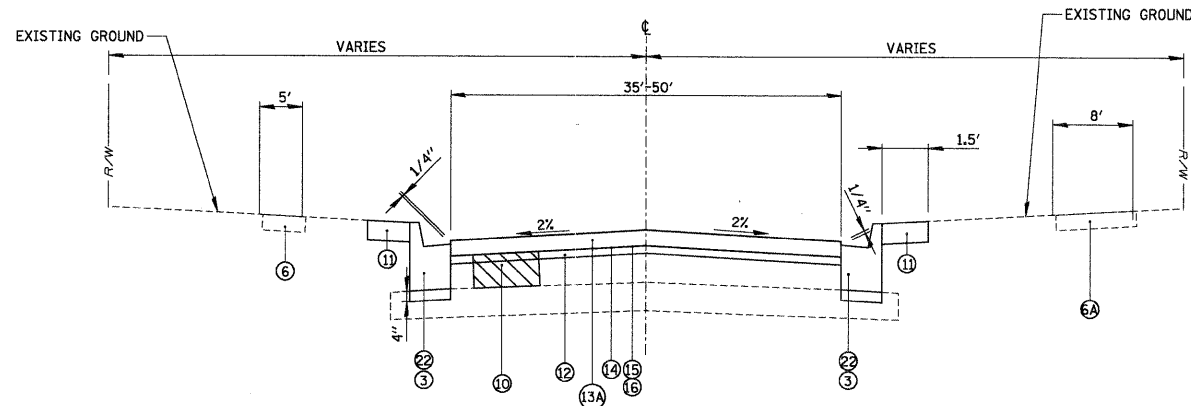
LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

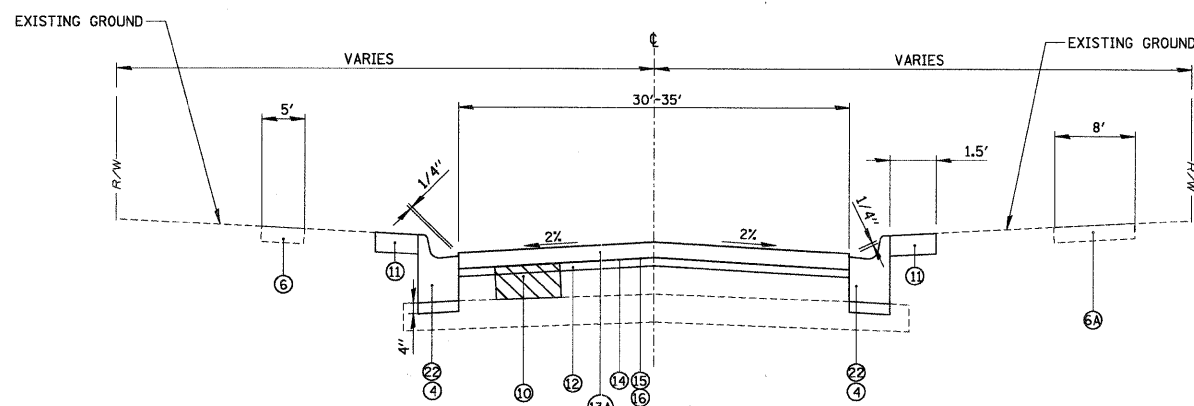
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HANSON ROAD PROPOSED TYPICAL SECTIONS			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALGONQUIN\070273\070273.00019\Civil\PL	TYPSEC.HANSON.dgn	DRAWN - MDH	REVISED -		SCALE: NTS			08-00075-00-RS	MCHENRY	35	9	
	PLUT SCALE = NTS	CHECKED - LMF	REVISED -		SHEET NO. 2 OF 4 SHEETS			CONTRACT NO. 63138				
	PLUT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -		STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HUNTINGTON DRIVE
 PROPOSED TYPICAL SECTION
 STA. 251+11.20 TO STA. 263+79.50
 (OMISSION AT RANDAL ROAD
 BETWEEN STA. 263+79.50 TO STA. 264+58.40)
LOCATION #4 (SEE PROPOSED PLAN)



HUNTINGTON DRIVE
 PROPOSED TYPICAL SECTION
 STA. 264+58 TO STA. 276+00
LOCATION #5 (SEE PROPOSED PLAN)

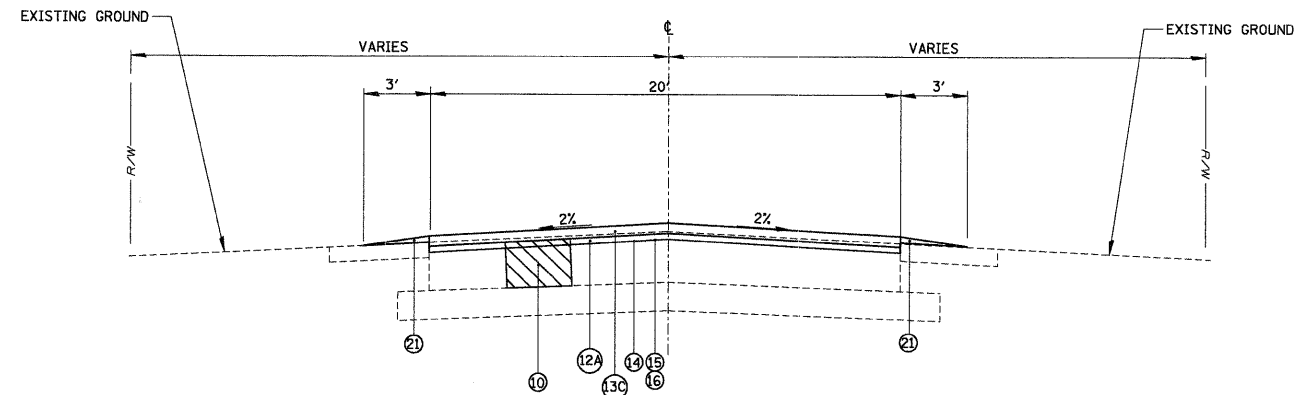


HUNTINGTON DRIVE
 PROPOSED TYPICAL SECTION
 STA. 276+00 TO STA. 282+80
LOCATION #6 (SEE PROPOSED PLAN)

LEGEND

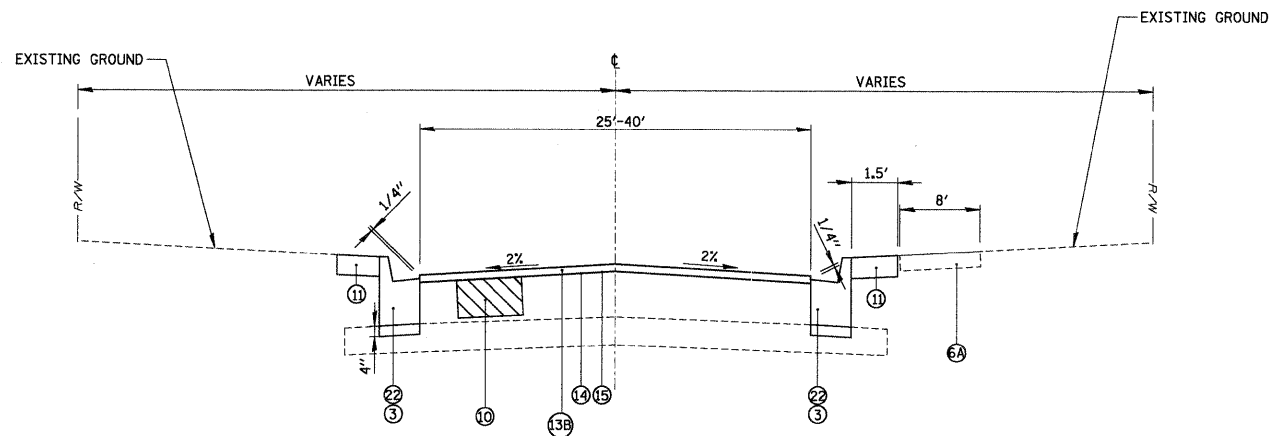
- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS D PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
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- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑲ AGGREGATE BASE REPAIR
- ⑲ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ⑳ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉑ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉒ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉓ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HUNTINGTON DRIVE PROPOSED TYPICAL SECTIONS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALCONQUIN\070273\070273\00019\Civil\PL\TYPSEC_HUNTINGTON.dgn		DRAWN - MDH	REVISED -		08-00075-00-RS	MCHENRY	35	10			
PLOT SCALE = NTS		CHECKED - LMF	REVISED -		CONTRACT NO. 63138						
PLOT DATE = 2/13/2009		DATE - 2/13/2009	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
				SCALE: NTS		SHEET NO. 3 OF 4 SHEETS		STA. TO STA.			



SANDBLOOM ROAD
 PROPOSED TYPICAL SECTION
 STA. 411+50 TO STA. 426+55
 STA. 453+43 TO STA. 477+50

LOCATION #7 (SEE PROPOSED PLAN)



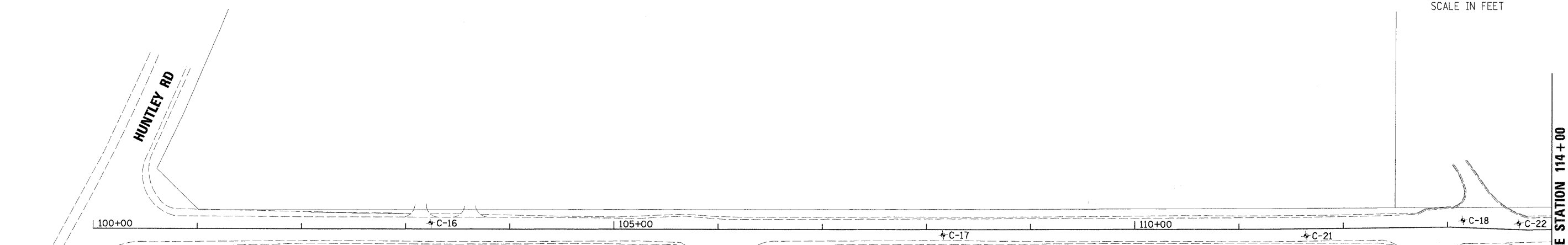
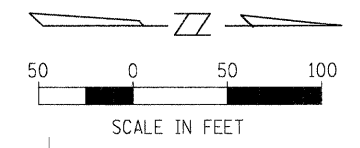
SANDBLOOM ROAD
 PROPOSED TYPICAL SECTION
 STA. 426+55 TO STA. 453+43

LOCATION #8 (SEE PROPOSED PLAN)

LEGEND

- ① EXISTING HOT-MIX ASPHALT PAVEMENT
- ② EXISTING AGGREGATE SUBBASE
- ③ EXISTING B6.12 CURB AND GUTTER
- ③A EXISTING B6.18 CURB AND GUTTER
- ④ EXISTING M6.12 CURB AND GUTTER
- ⑥ EXISTING PCC SIDEWALK
- ⑥A EXISTING HOT-MIX ASPHALT BIKE PATH
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑦A HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- ⑦B HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- ⑧ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- ⑩ CLASS 'D' PATCHES, 6" (LOCATION AS DIRECTED BY RE)
- ⑪ SODDING, SALT TOLERANT AS NECESSARY FOR CURB AND GUTTER REMOVAL AND REPLACEMENT (INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AND AGGREGATE SHOULDER)
- ⑫ PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- ⑫A PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- ⑬ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2"
- ⑬A PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 2 1/2"
- ⑬B PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- ⑬C PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- ⑭ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
- ⑮ PROPOSED AGGREGATE (PRIME COAT)
- ⑯ PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
- ⑰ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- ⑱ PREPARATION OF BASE
- ⑳ AGGREGATE BASE REPAIR
- ㉑ PROPOSED AGGREGATE SHOULDER, TYPE B (AS DIRECTED BY THE ENGINEER)
- ㉒ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER). INCLUDES 4" SUBBASE GRANULAR MATERIAL TYPE B.
- ㉓ PROPOSED GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- ㉔ REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- ㉕ POROUS GRANULAR EMBANKMENT, SUBGRADE

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SANDBLOOM ROAD PROPOSED TYPICAL SECTIONS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALGONDUI\070273\070273.00019\Civil\PR_TYPSPEC_SANDBLOOM.dgn		DRAWN - MDH	REVISED -		SCALE: NTS	SHEET NO. 4 OF 4 SHEETS	STA. TO STA.	08-00075-00-RS	MCHENRY	35	11
		CHECKED - LMF	REVISED -					CONTRACT NO. 63138			
		DATE - 2/13/2009	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

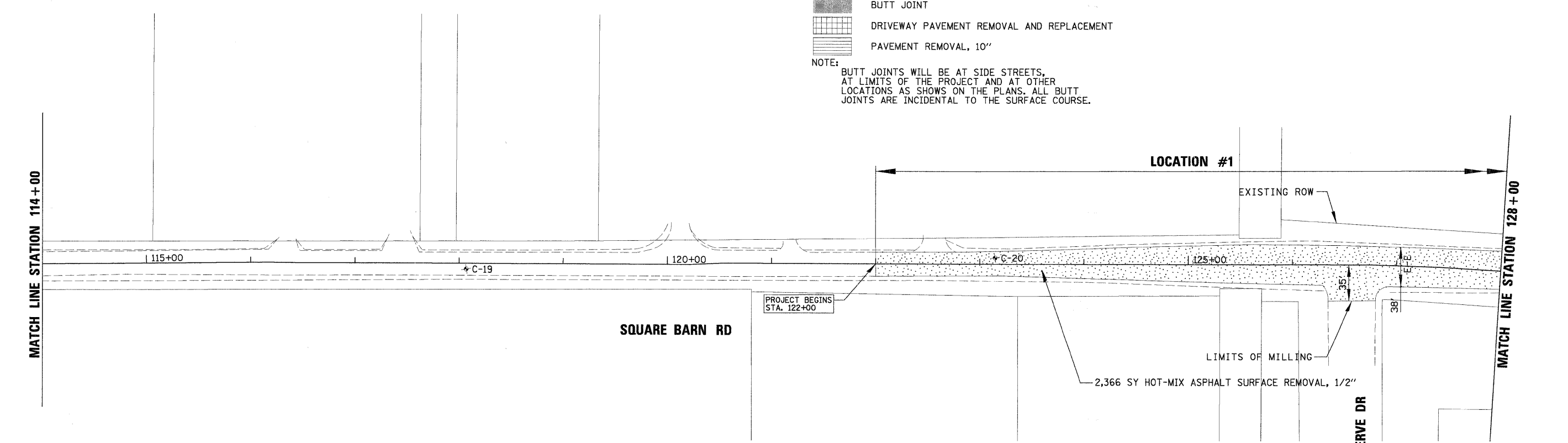


SQUARE BARN RD

LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- SIDEWALK REMOVAL
- BUTT JOINT
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWS ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.



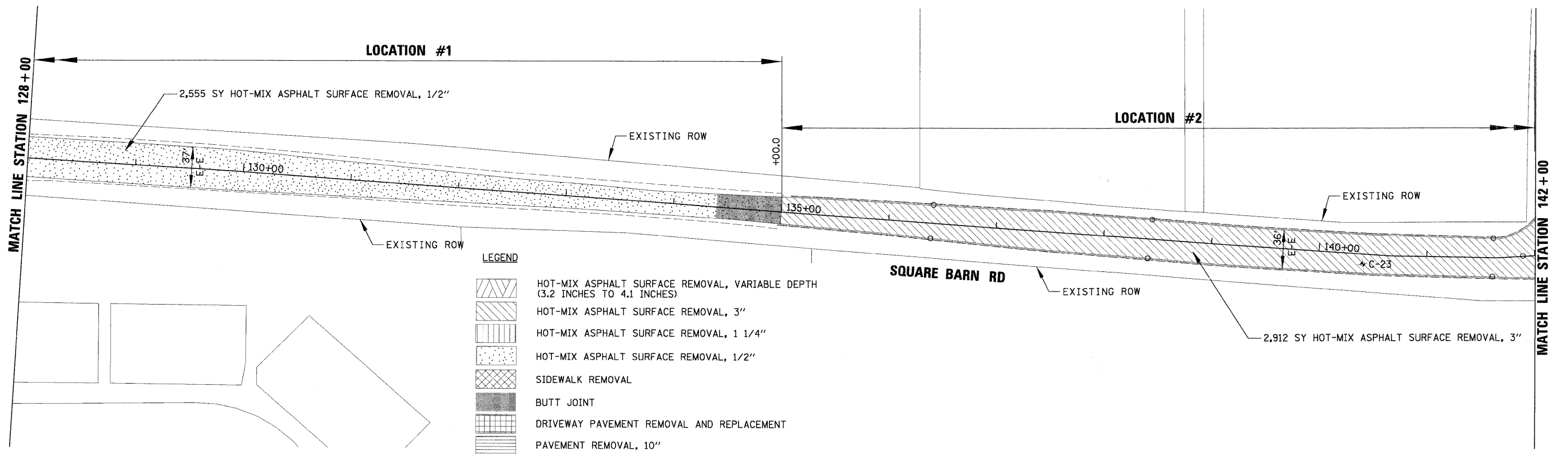
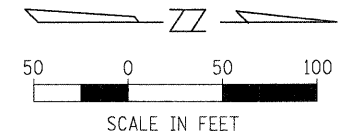
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N:\VALGONQUIN\070273\070273\00019\Civil\EM070273 LAPP Square Barn South-1.dgn		DRAWN - MDH	REVISED -
PLOT SCALE = 50'		CHECKED - LMF	REVISED -
PLOT DATE = 2/13/2009		DATE - 2/13/2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS AND REMOVAL PLAN
 SQUARE BARN RD - 1**

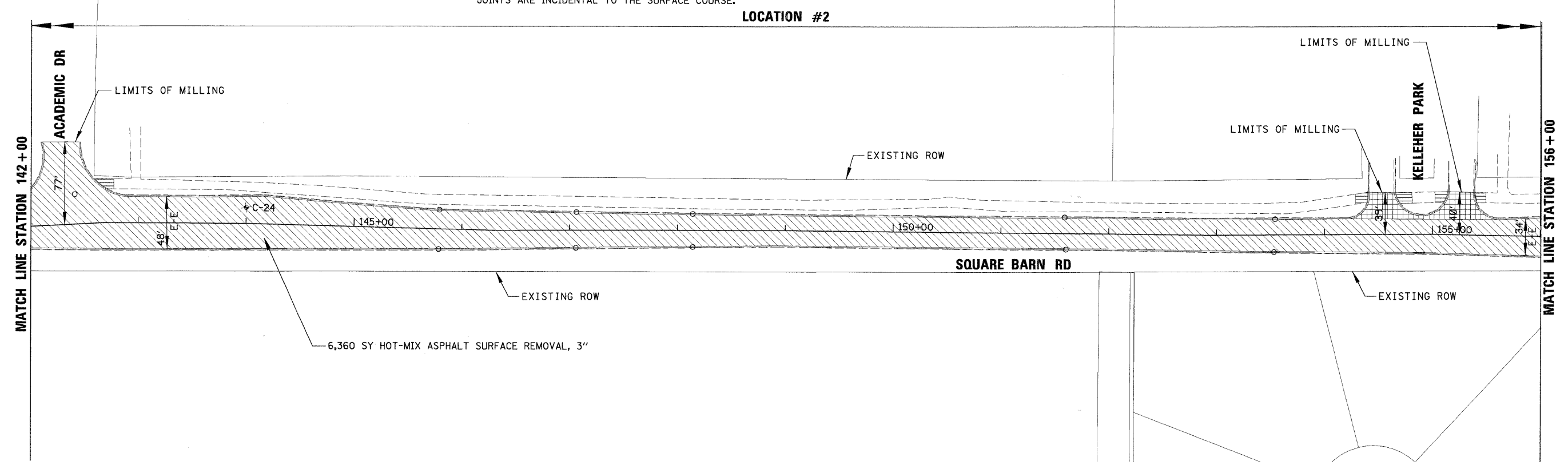
SCALE: 50' SHEET NO. 1 OF 9 SHEETS STA. 100+00 TO STA. 128+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	12
CONTRACT NO. 63138				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

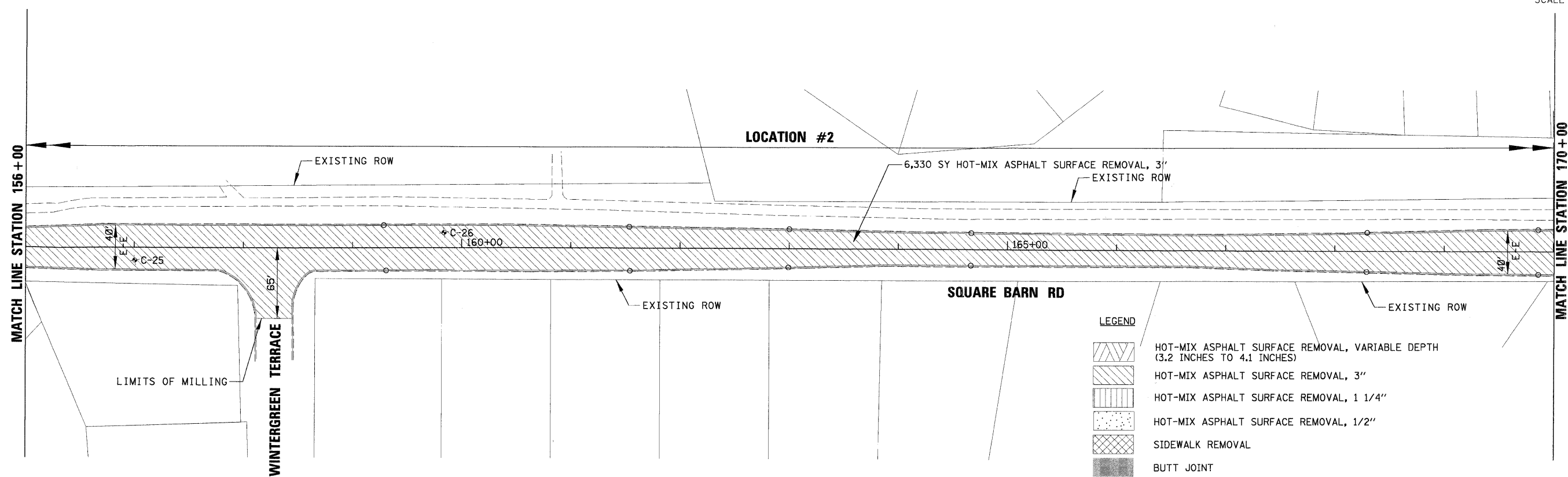
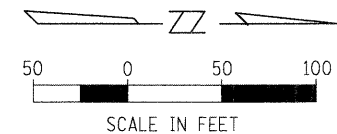


- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - SIDEWALK REMOVAL
 - BUTT JOINT
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS,
 AT LIMITS OF THE PROJECT AND AT OTHER
 LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT
 JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.



FILE NAME = N:\ALGONQUIN\070273\070273.00\019\Civil\EM070273 LAPP Square Barn South-2.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN SQUARE BARN RD -- 2			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5/8"	DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 2 OF 9 SHEETS	STA. 128+00 TO STA. 156+00	08-00075-00-RS	MCHENRY	35	13	
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -									
		DATE - 2/13/2009	REVISED -						FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	

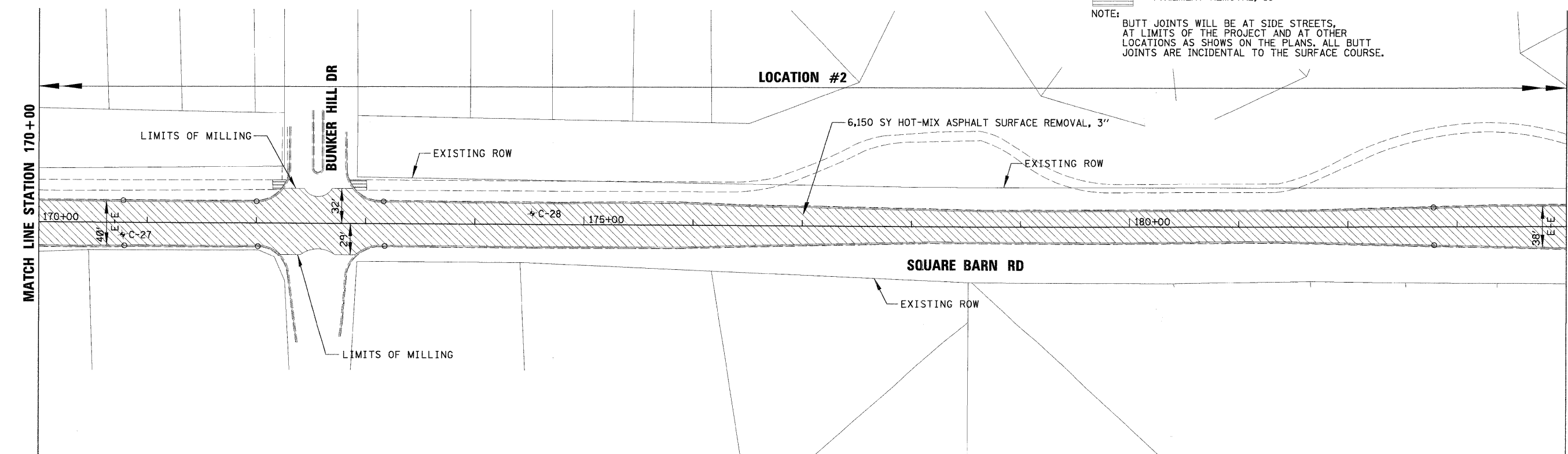


LEGEND

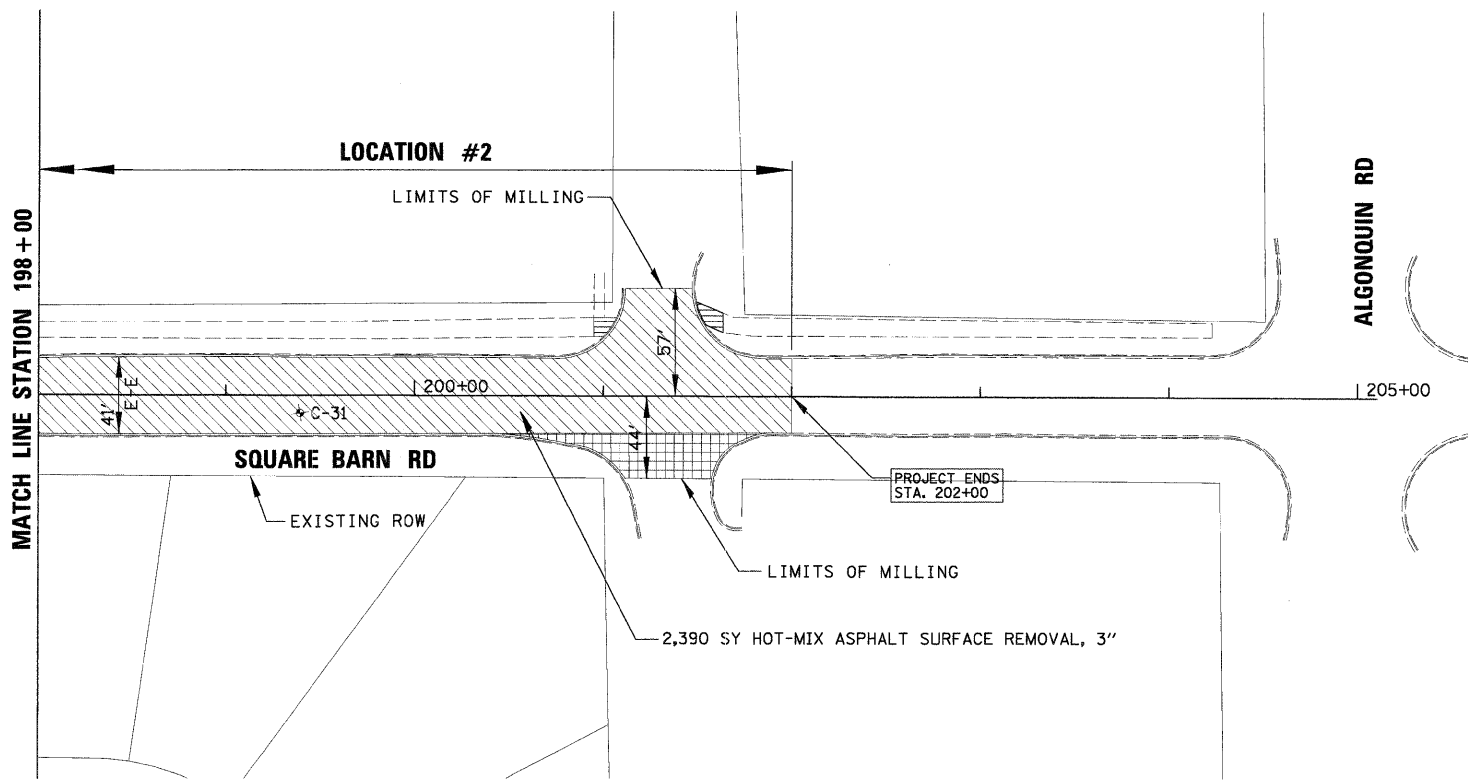
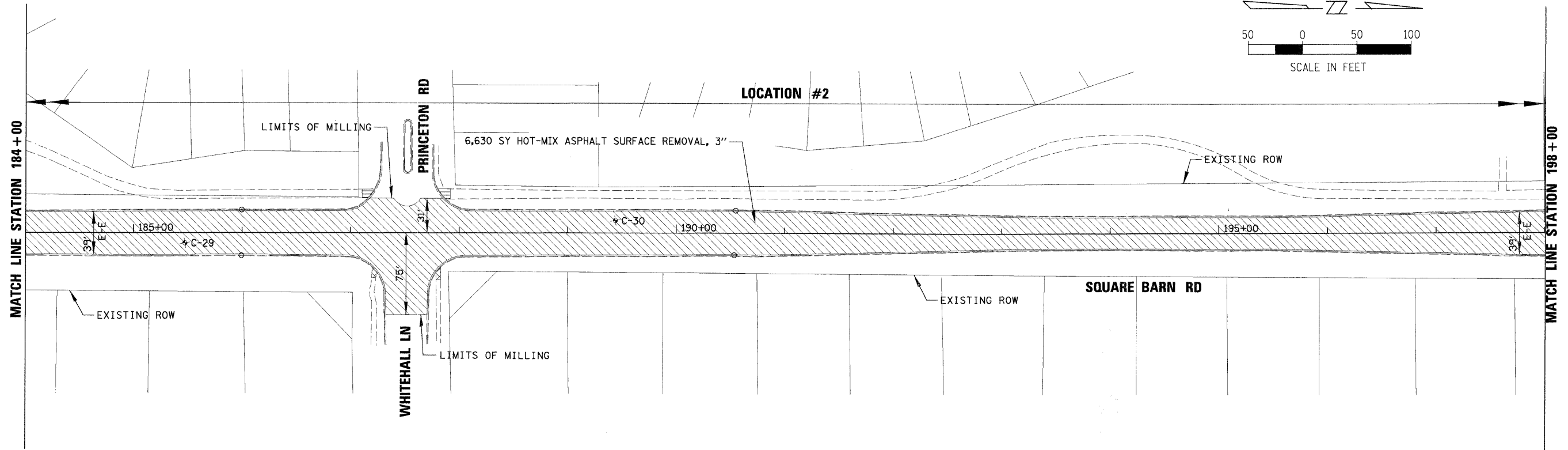
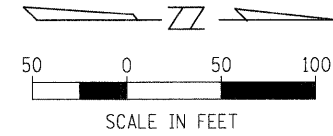
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- SIDEWALK REMOVAL
- BUTT JOINT
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PAVEMENT REMOVAL, 10"

NOTE:

BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.



FILE NAME = N:\ALCONQUIN\070273\070273\00019\Civil\REM070273 LAPP Square Barn North-3.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN SQUARE BARN RD - 3			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50'	DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 3 OF 9 SHEETS	STA. 156+00 TO STA. 184+00	08-00075-00-RS	MCHENRY	35	14	
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -					CONTRACT NO. 63138				
		DATE - 2/13/2009	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- SIDEWALK REMOVAL
- BUTT JOINT
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS,
 AT LIMITS OF THE PROJECT AND AT OTHER
 LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT
 JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

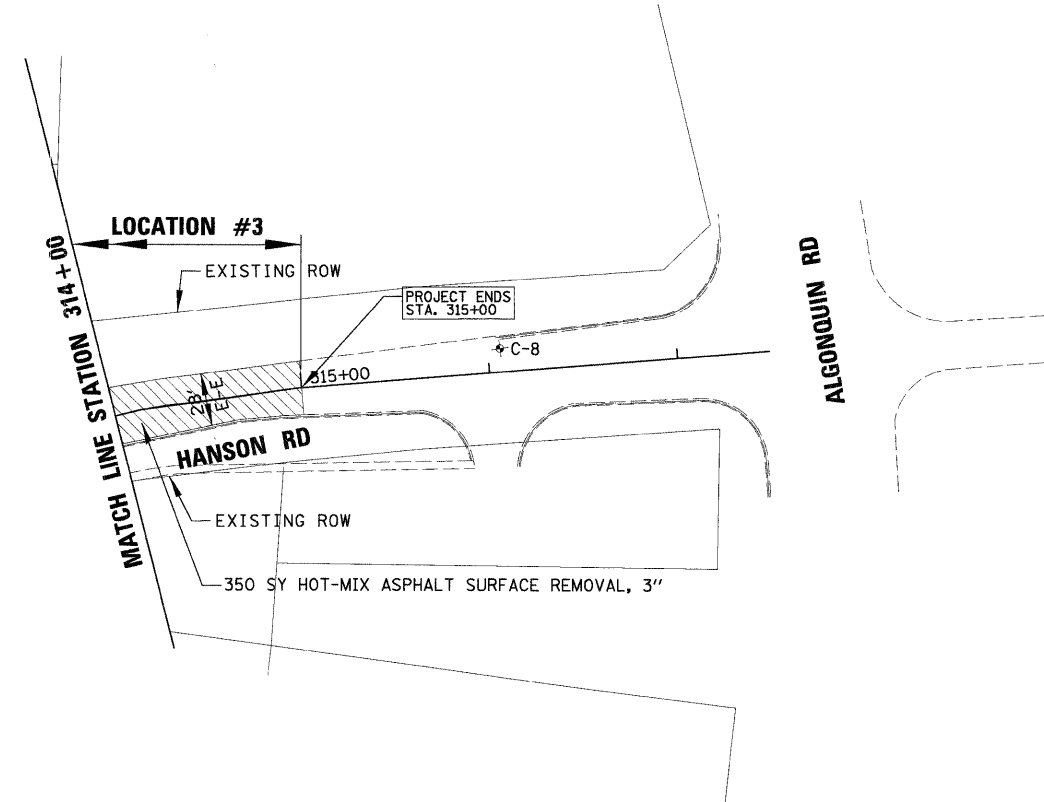
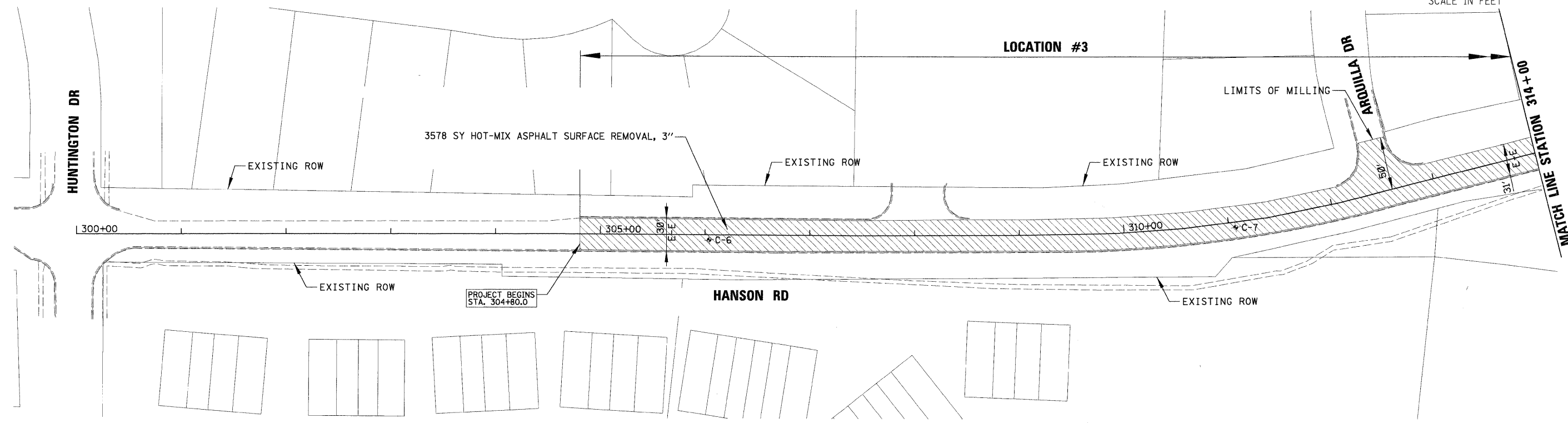
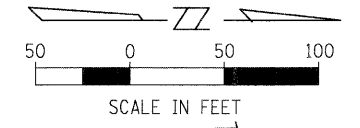
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -
N:\ALGONQUIN\070273\070273.00019\Civil\RE	EM070273 LAPP Square Barn North-4.dgn	DRAWN - MDH	REVISED -
	PLOT SCALE = 50'	CHECKED - LMF	REVISED -
	PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS AND REMOVAL PLAN
 SQUARE BARN RD - 4**

SCALE: 50' SHEET NO. 4 OF 9 SHEETS STA. 184+00 TO STA. 205+00

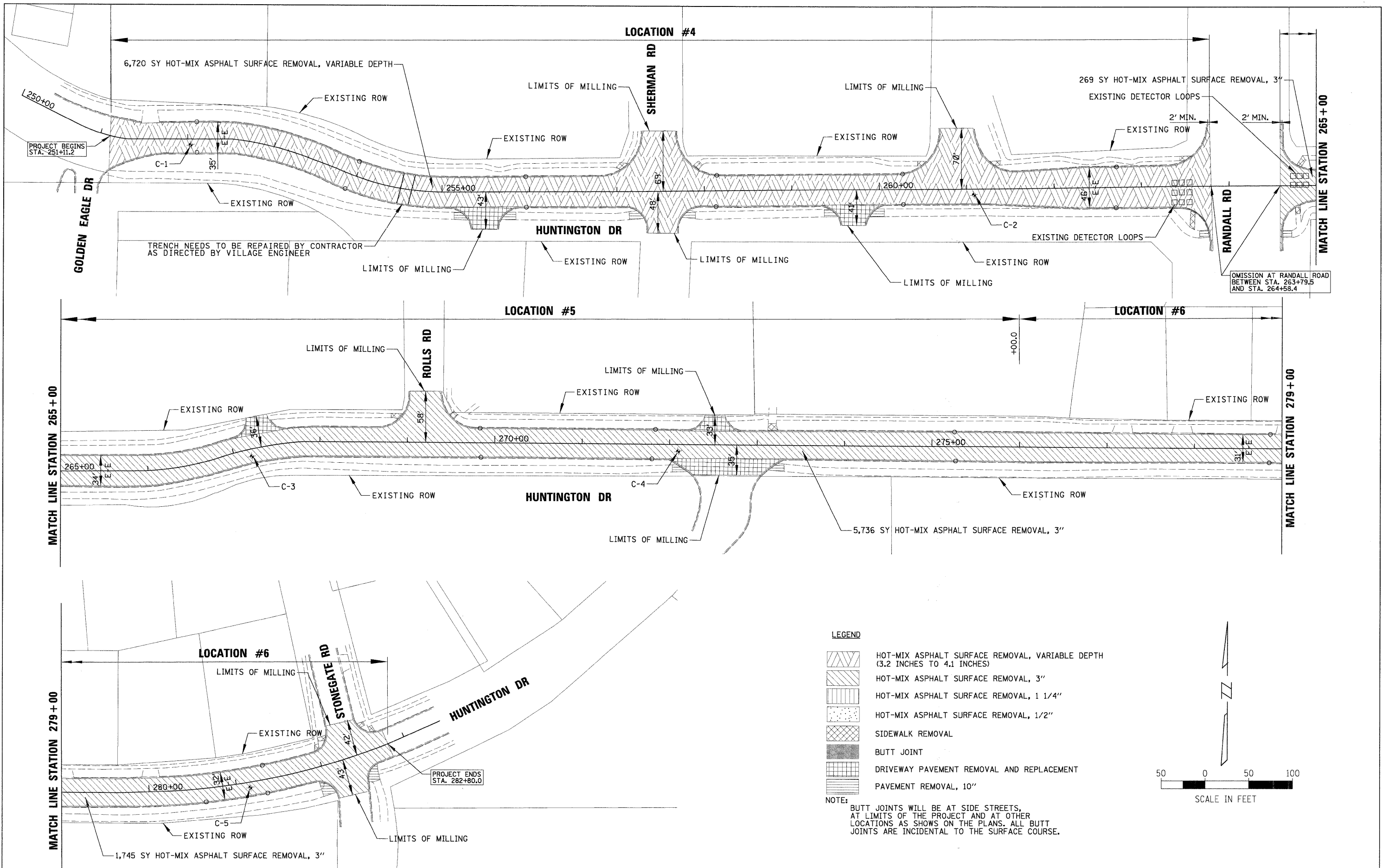
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	15
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63138	



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - SIDEWALK REMOVAL
 - BUTT JOINT
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

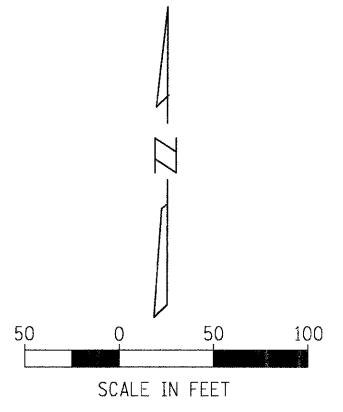
FILE NAME = N:\ALGONQUIN\070273\070273.00019\Civil\EM070273 LAPP Hanson.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN HANSON ROAD		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50'	DRAWN - MDH	REVISED -		08-00075-00-RS	MCHENRY	35	16			
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -		CONTRACT NO. 63138						
	DATE - 2/13/2009	REVISED -	STA. 300+00 TO STA. 315+00		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



LEGEND

- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
- HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
- SIDEWALK REMOVAL
- BUTT JOINT
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.



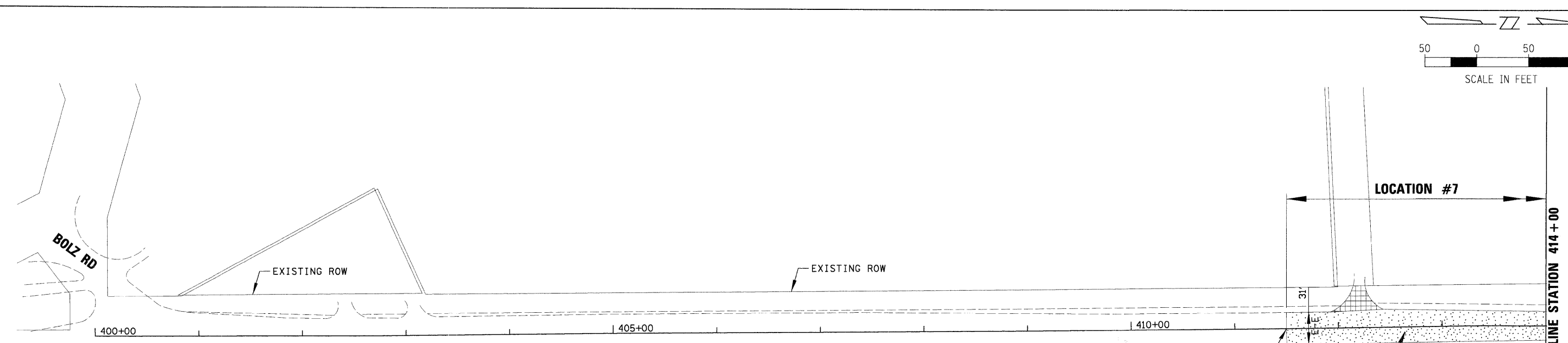
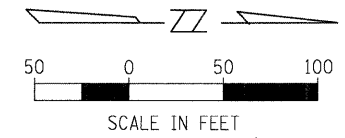
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PLOT SCALE = 50'		CHECKED - LMF	REVISED -
PLOT DATE = 2/13/2009		DATE - 2/13/2009	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING CONDITIONS AND REMOVAL PLAN
 HUNTINGTON DR**

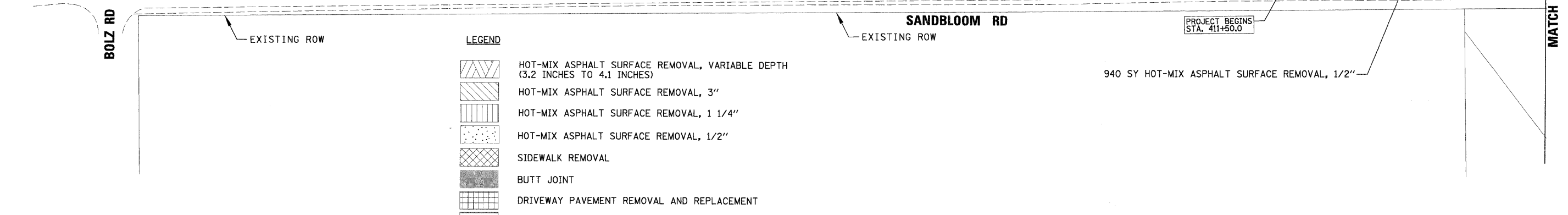
SCALE: 50' SHEET NO. 6 OF 9 SHEETS STA. 250+00 TO STA. 283+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	17
CONTRACT NO. 63138				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

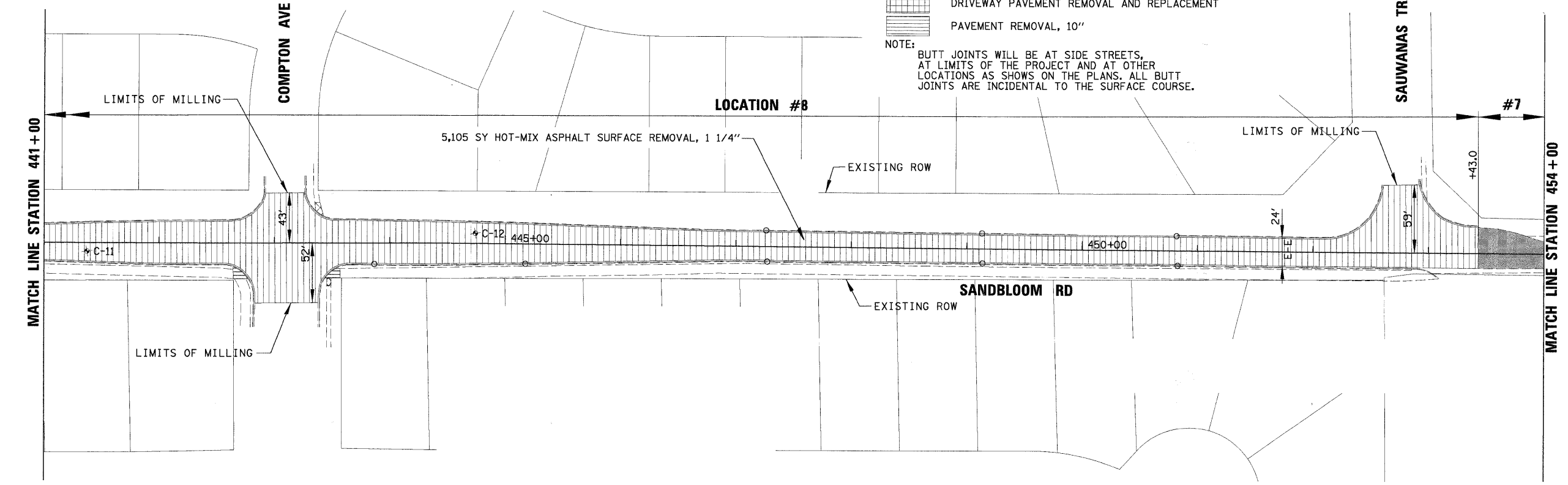
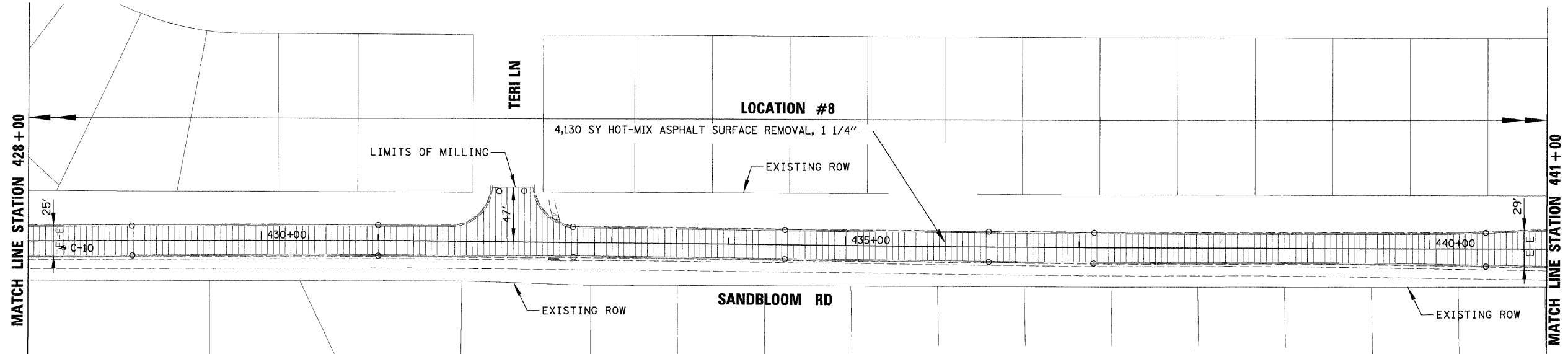
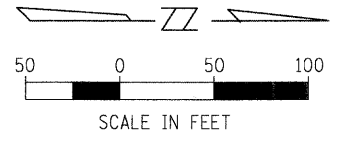


- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - SIDEWALK REMOVAL
 - BUTT JOINT
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PAVEMENT REMOVAL, 10"

NOTE:
 BUTT JOINTS WILL BE AT SIDE STREETS,
 AT LIMITS OF THE PROJECT AND AT OTHER
 LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT
 JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

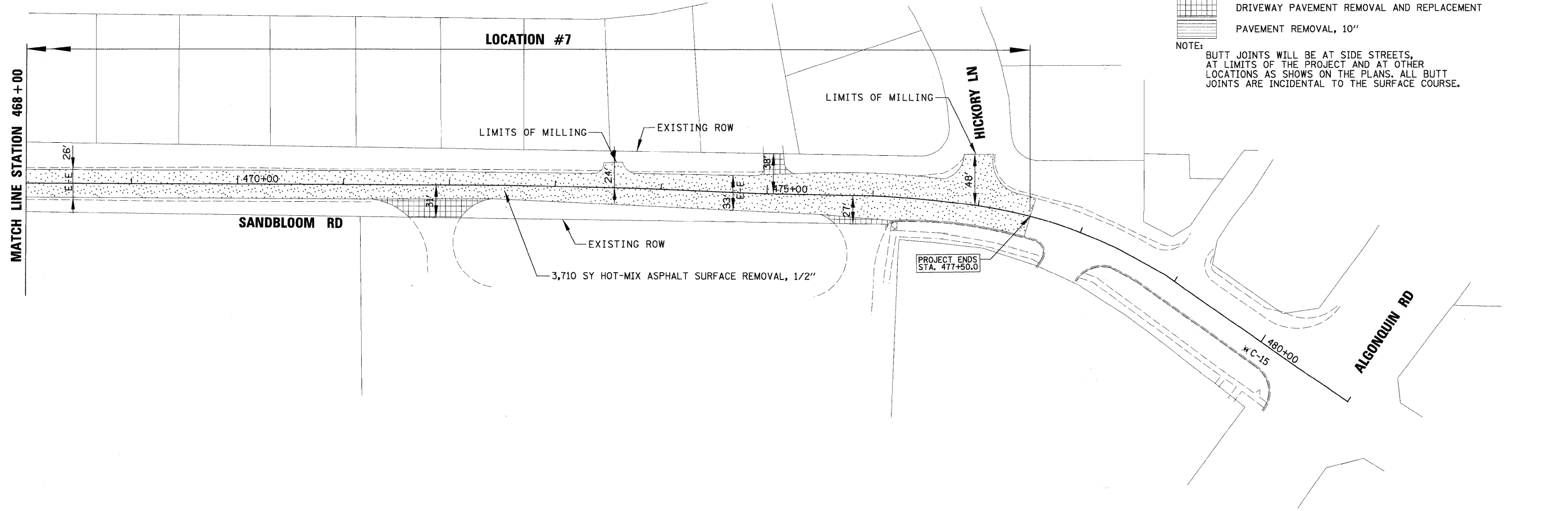
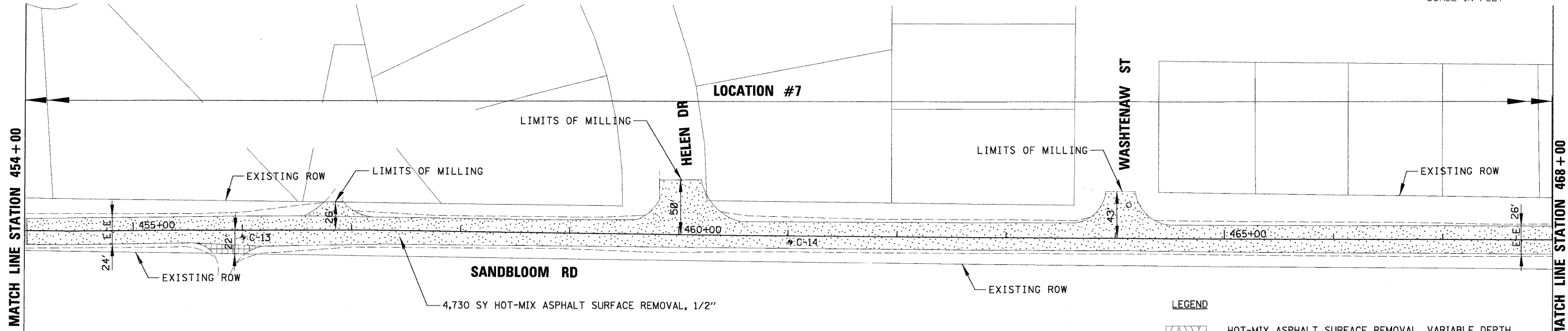
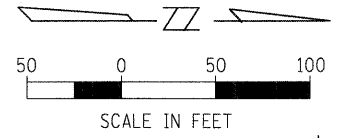


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	PLOT SCALE = 50'	DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 7 OF 9 SHEETS	STA. 400+00 TO STA. 428+00	08-00075-00-RS	MCHENRY	35	18	
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -		CONTRACT NO. 63138							
		DATE - 2/13/2009	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							



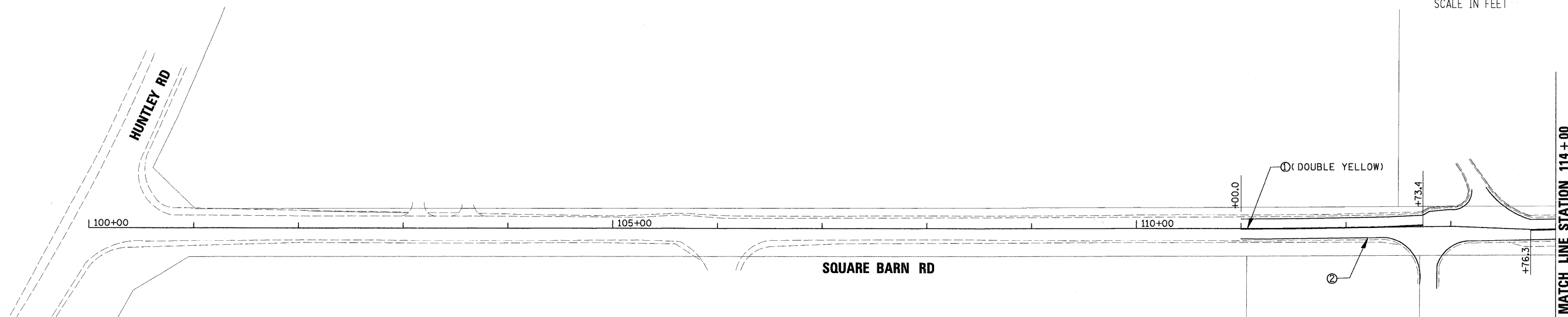
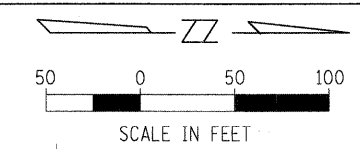
- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - SIDEWALK REMOVAL
 - BUTT JOINT
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PAVEMENT REMOVAL, 10"
- NOTE:**
 BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

FILE NAME = N:\ALGONGUIN\070273\0702\3.00\019\C:\I\REM070273 LAPP Sandbloom-2.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING CONDITIONS AND REMOVAL PLAN SANDBLOOM RD - 2				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50'	DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 8 OF 9 SHEETS	STA. 428+00 TO STA. 454+00	08-00075-00-RS	MCHENRY	35	19		
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -		FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 63138					
		DATE - 2/13/2009	REVISED -										



- LEGEND**
- HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (3.2 INCHES TO 4.1 INCHES)
 - HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"
 - HOT-MIX ASPHALT SURFACE REMOVAL, 1/2"
 - SIDEWALK REMOVAL
 - BUTT JOINT
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PAVEMENT REMOVAL, 10"
- NOTE:**
 BUTT JOINTS WILL BE AT SIDE STREETS, AT LIMITS OF THE PROJECT AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS. ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

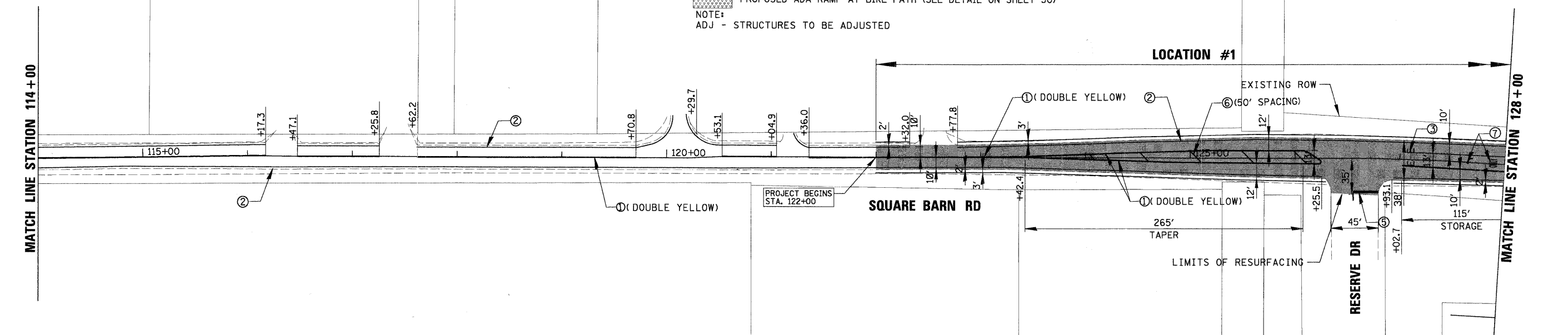
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N:\ALGONQUIN\070273\070273.00019\Civil1\NEM070273_LAPP_Sandbloom-3.dgn	DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	20		
PLOT SCALE = 50'	CHECKED - LMF	REVISED -	CONTRACT NO. 63138							
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
					SCALE: 50'	SHEET NO. 9 OF 9 SHEETS		STA. 454+00 TO STA. 481+00		



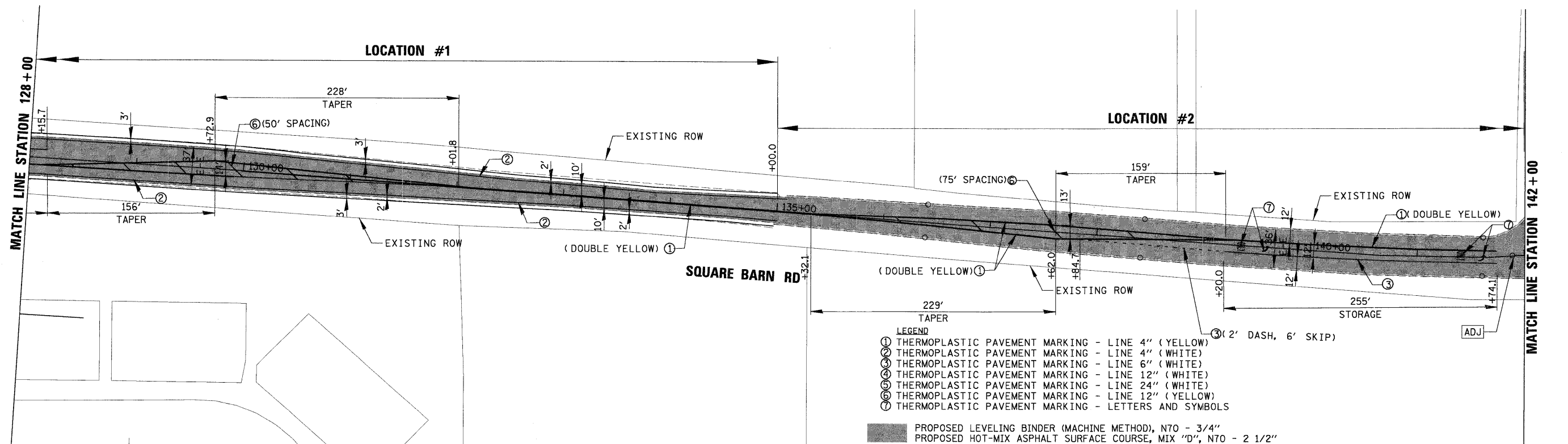
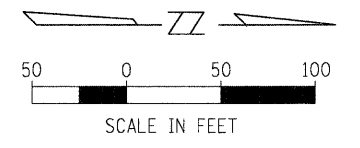
- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)

NOTE:
ADJ - STRUCTURES TO BE ADJUSTED



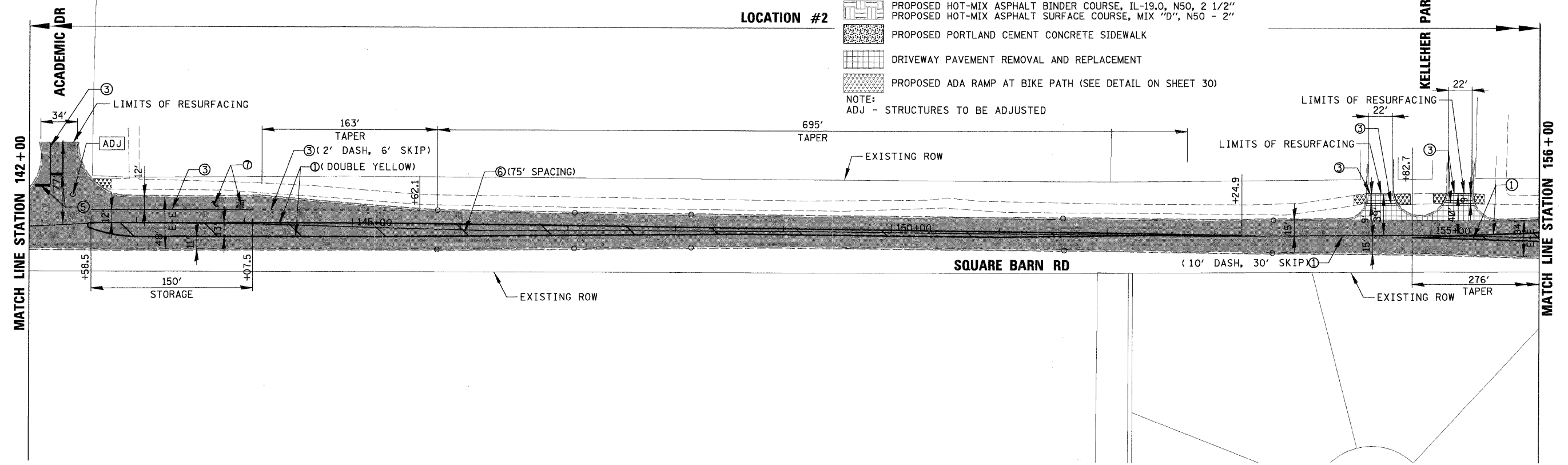
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SQUARE BARN RD - 1	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\VALGONDOUN\070273\070273.0001\9\Civil\PRO070273 LAPP Square Barn South-1.dgn	DRAWN - MDH	REVISED -				08-00075-00-RS	MCHENRY	35	21	
PLOT SCALE = 50'	CHECKED - LMF	REVISED -				CONTRACT NO. 63138				
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -				FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				
						SCALE: 50' SHEET NO. 1 OF 9 SHEETS STA. 100+00 TO STA. 128+00				



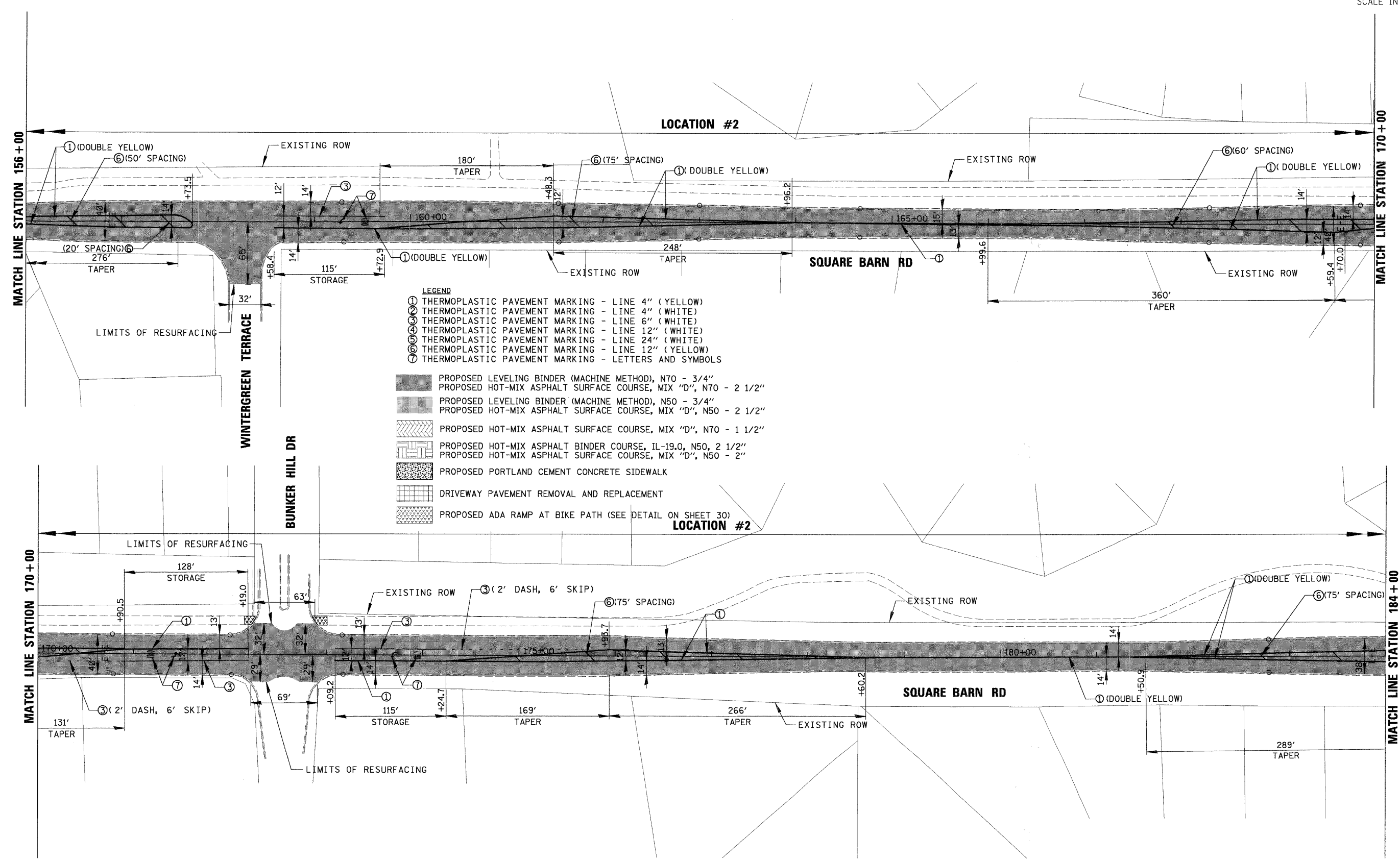
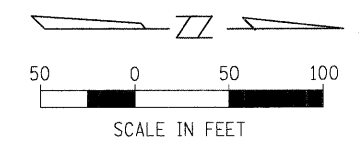
- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)

NOTE:
ADJ - STRUCTURES TO BE ADJUSTED

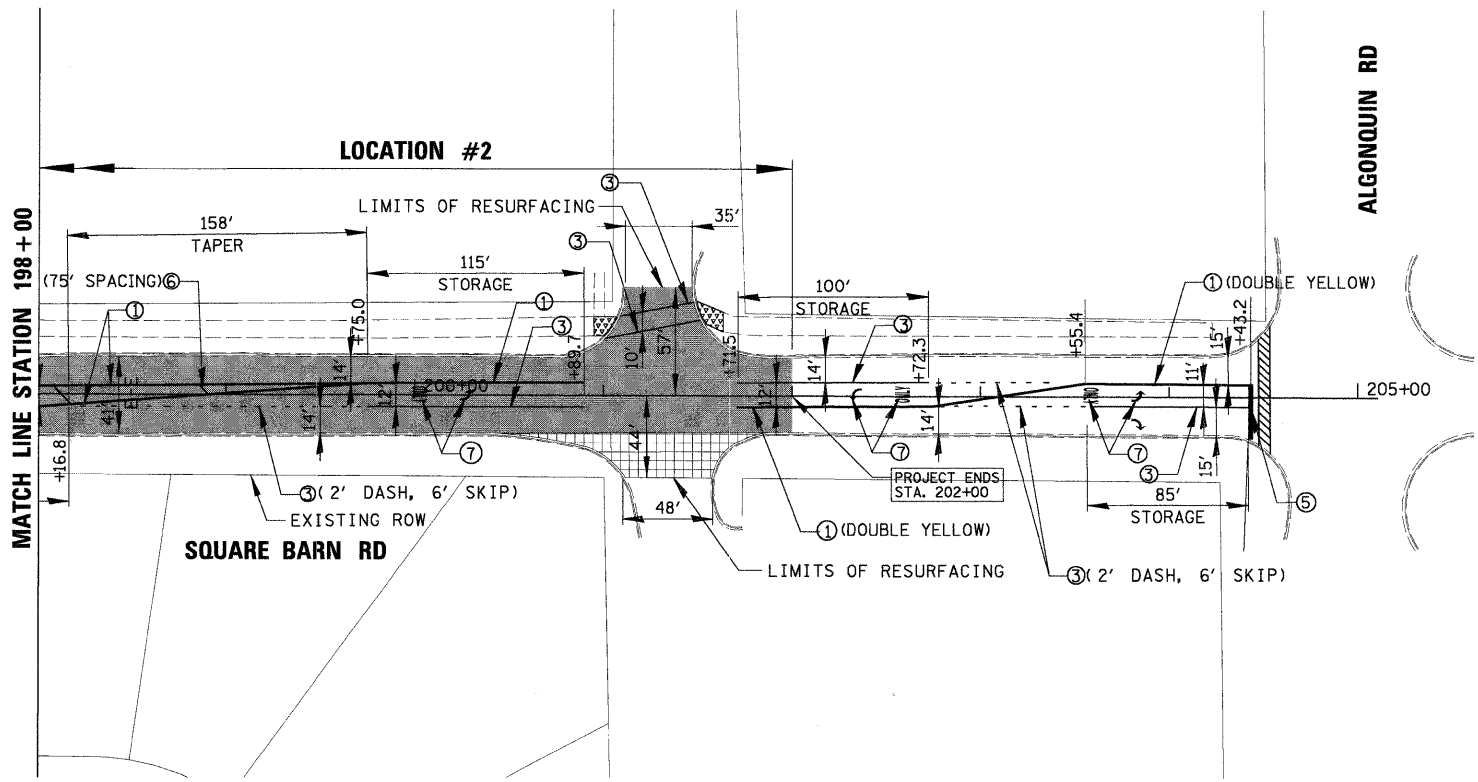
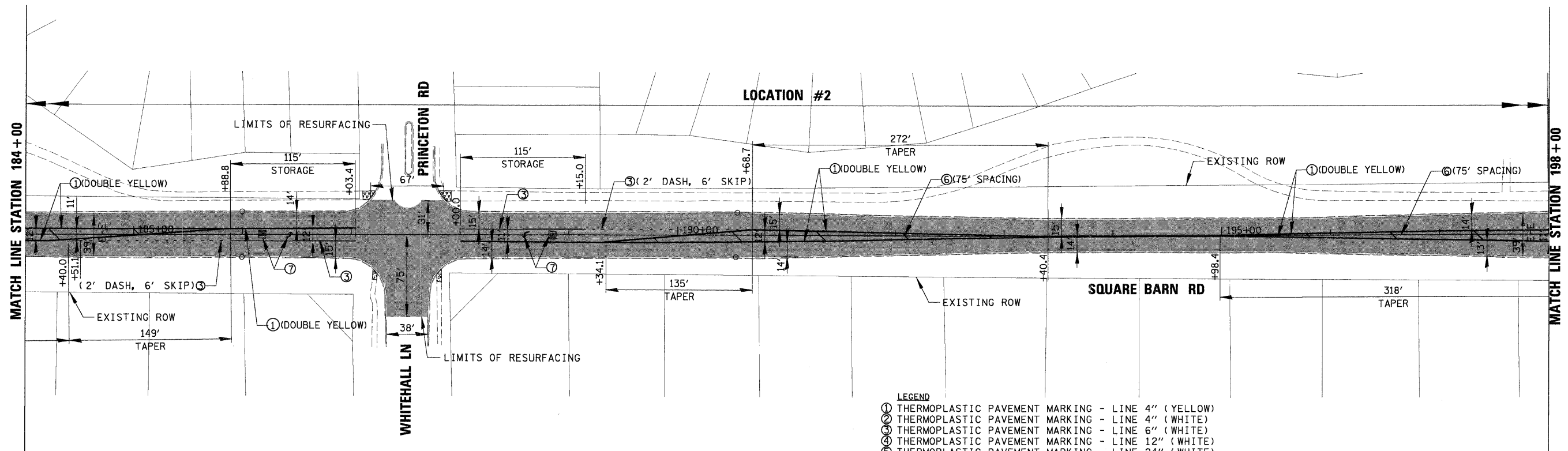


FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SQUARE BARN RD - 2	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\VALGONDWIN\072273\070273.00019\Civil1\F0070273 LAPP Square Barn South-2.dgn	DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	22		
PLOT SCALE = 50'	CHECKED - LMF	REVISED -	CONTRACT NO. 63138							
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
SCALE: 50'						SHEET NO. 2 OF 9 SHEETS		STA. 128+00 TO STA. 156+00		



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- [Hatched Box] PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
 - [Hatched Box] PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
 - [Hatched Box] PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
 - [Hatched Box] PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
 - [Hatched Box] PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 - [Hatched Box] PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - [Hatched Box] PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
 - [Hatched Box] PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
 - [Hatched Box] DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - [Hatched Box] PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)

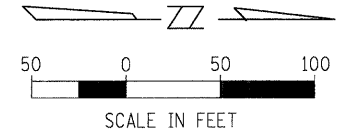
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SQUARE BARN RD - 3			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\VALGONDWIN\070273\070273.00019\Civil\PROJ\070273 LAPP Square Barn North-3.dgn	PLOT SCALE = 50'	DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 3 OF 9 SHEETS	STA. 156+00 TO STA. 184+00	08-00075-00-RS	MCHENRY	35	23	
	PLOT DATE = 2/13/2009	CHECKED - LMF	REVISED -									
		DATE - 2/13/2009	REVISED -					FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
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 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)

NOTE:
ADJ - STRUCTURES TO BE ADJUSTED



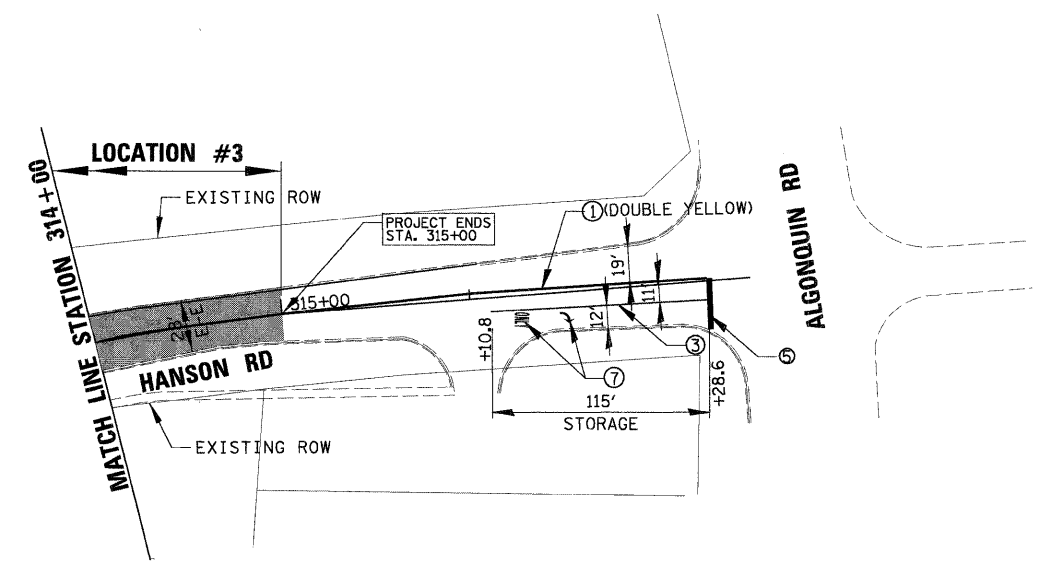
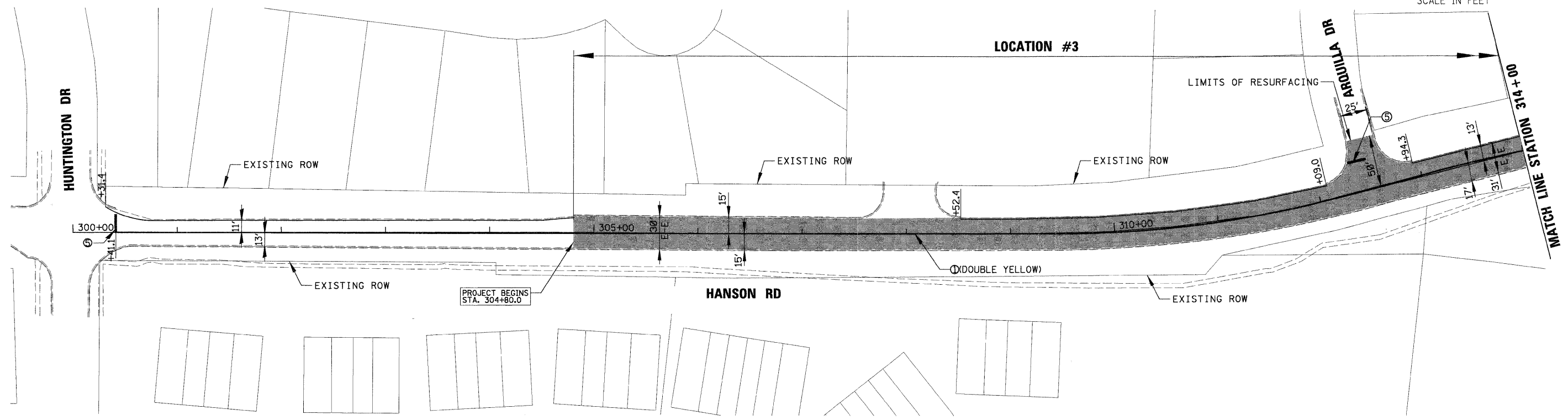
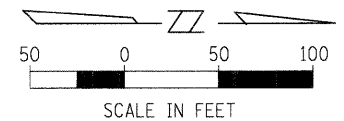
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -
N:\ALGONQUIN\070273\070273.00019\Civil\RD0070273 LAPP Square Barn North-4.dgn		DRAWN - MDH	REVISED -
PLOT SCALE = 50'		CHECKED - LMF	REVISED -
PLOT DATE = 2/13/2009		DATE - 2/13/2009	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN
SQUARE BARN RD - 4**

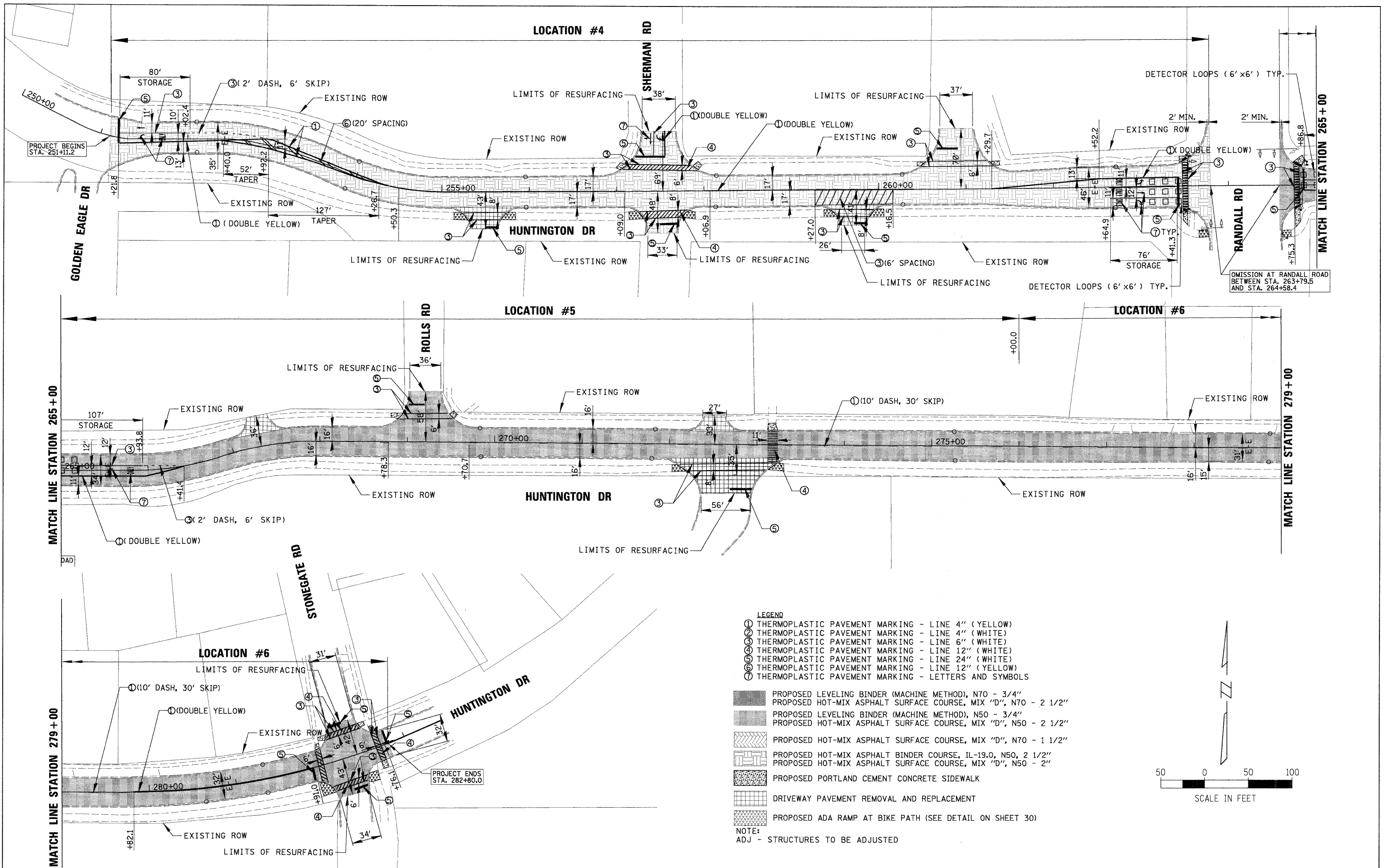
SCALE: 50' SHEET NO. 4 OF 9 SHEETS STA. 184+00 TO STA. 205+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	24
CONTRACT NO. 63138				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
 - PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 - PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
 - PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)
- NOTE:**
 ADJ - STRUCTURES TO BE ADJUSTED

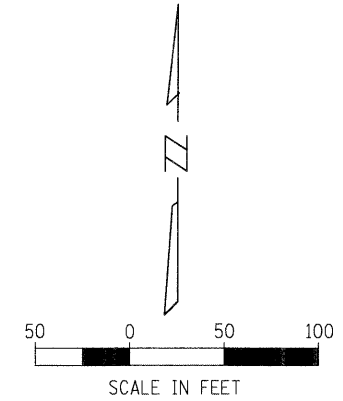
FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN HANSON ROAD	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
M:\ALGONQUIN\070273\070273.00019\Civil\08070273 LAPP Hanson.dgn	DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	25		
PLOT SCALE = 50'	CHECKED - LMF	REVISED -	CONTRACT NO. 63138							
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT							
				SCALE: 50'	SHEET NO. 5 OF 9 SHEETS	STA. 300+00 TO STA. 315+00				



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

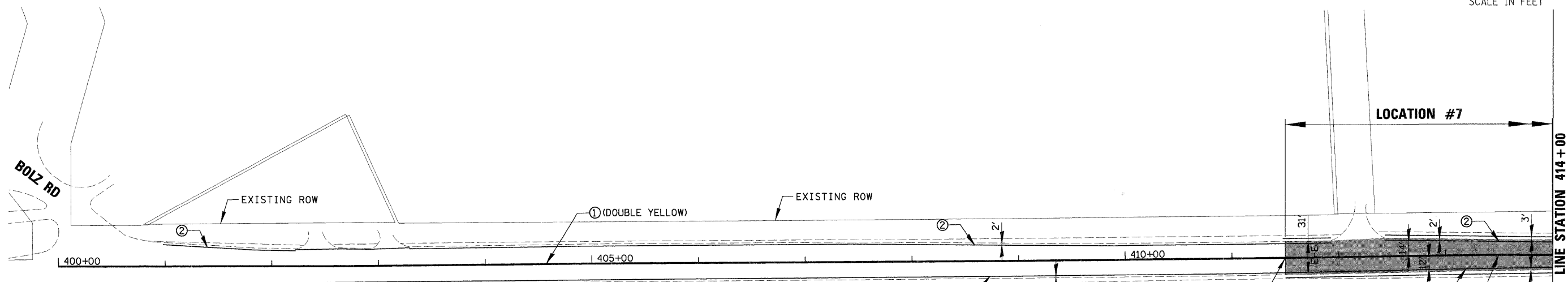
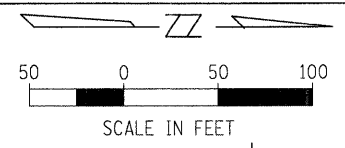
- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
- PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
- PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
- PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
- PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
- DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
- PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)

NOTE:
ADJ - STRUCTURES TO BE ADJUSTED

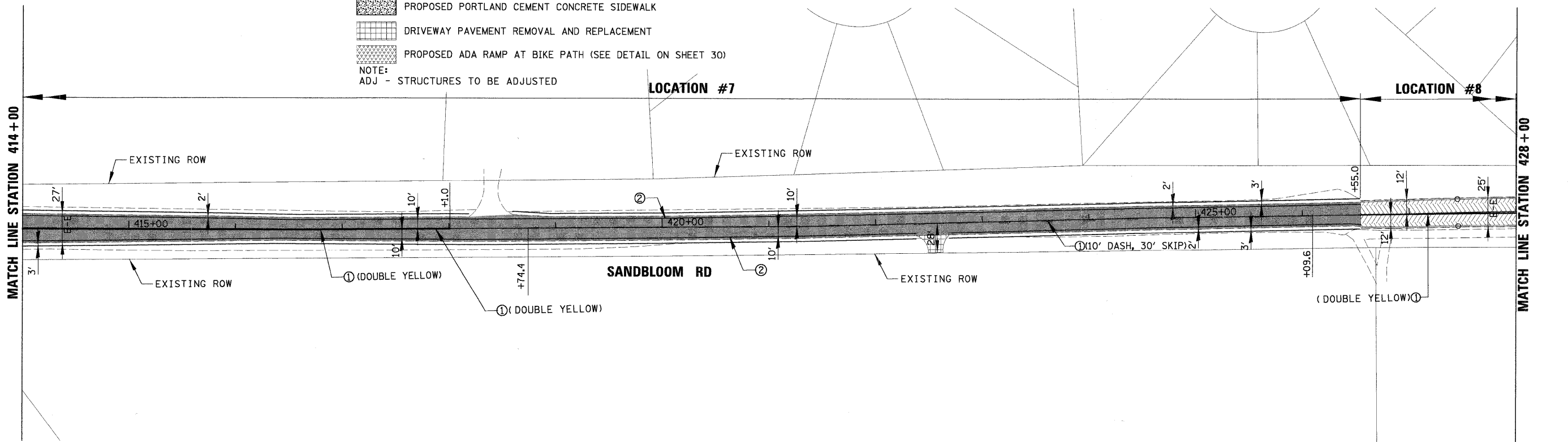


FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN HUNTINGTON DR	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALGONDWIN\070273\070273\00019\Civil\FR0070273 LAPP Huntington 01.dgn	PLOT SCALE = 50'	DRAWN - MDH	REVISED -			08-00075-00-RS	MCHENRY	35	26	
PLOT DATE = 2/13/2009	DATE - 2/13/2009	CHECKED - LMF	REVISED -			CONTRACT NO. 63138				
			REVISED -			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

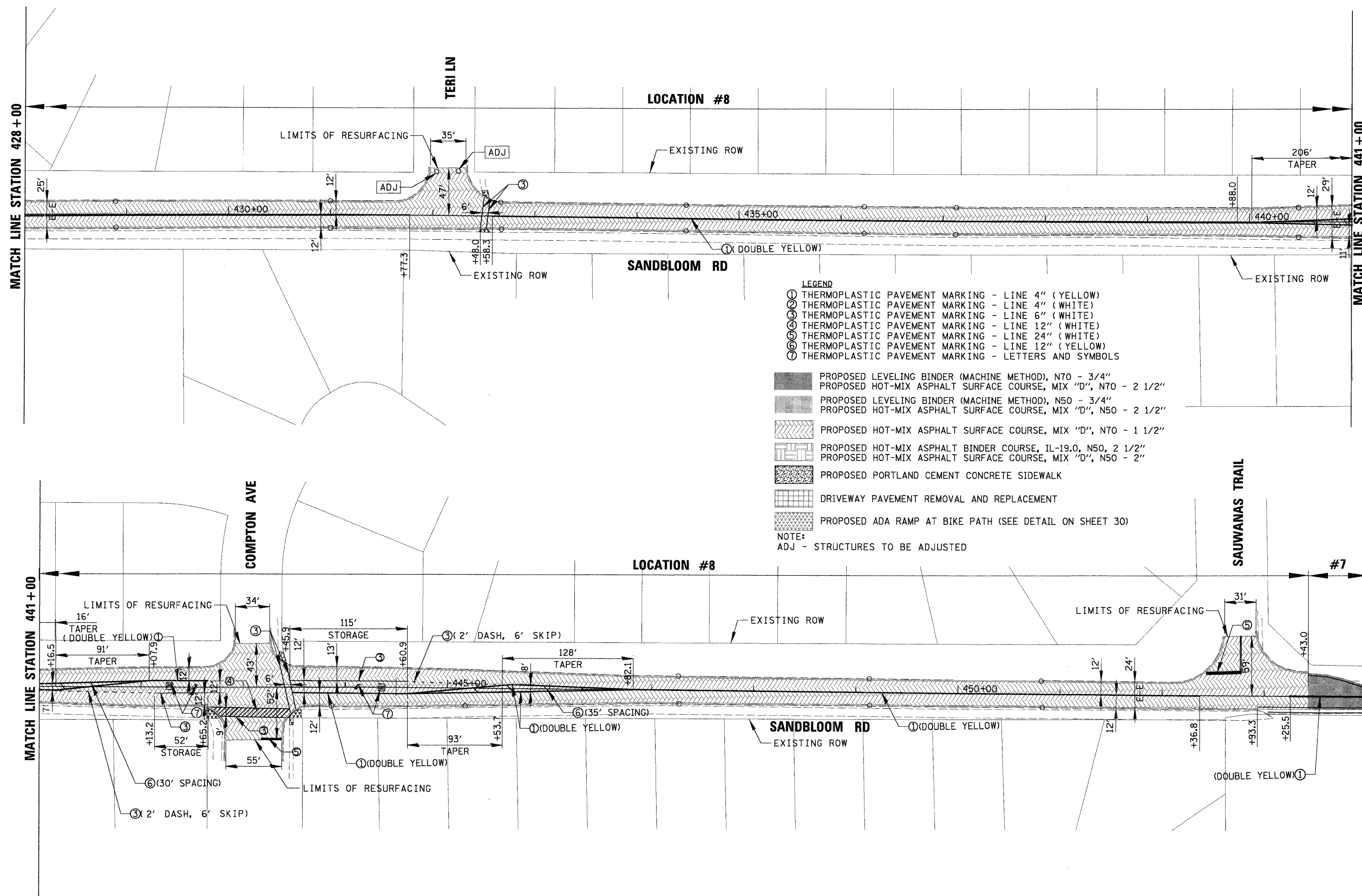
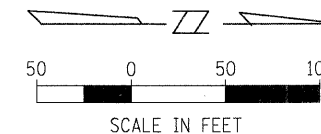
SCALE: 50' SHEET NO. 6 OF 9 SHEETS STA. 250+00 TO STA. 283+00



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
 - PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 - PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
 - PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)
- NOTE:**
ADJ - STRUCTURES TO BE ADJUSTED

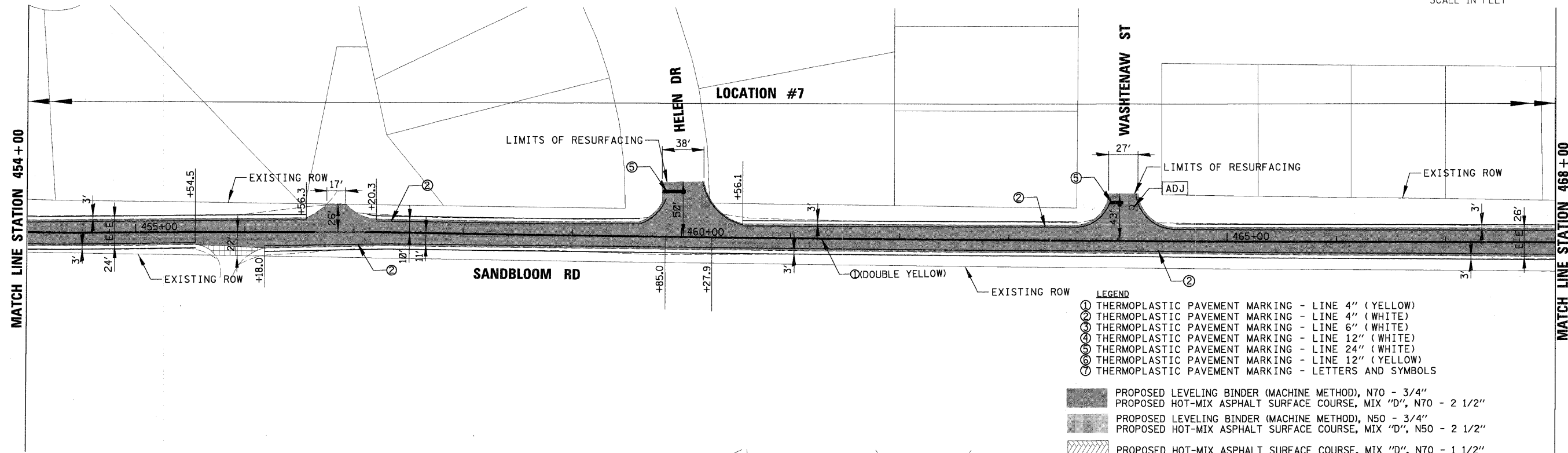
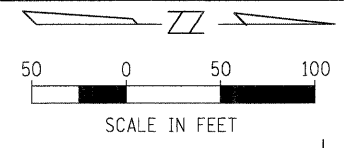


FILE NAME = N:\VALGONGUIN\070273\070273.00019\Civil\PRO070273 LAPP Sandbloom-1.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SANDBLOOM RD - 1		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50'		DRAWN - MDH	REVISED -		SCALE: 50'	SHEET NO. 7 OF 9 SHEETS	STA. 400+00 TO STA. 428+00	08-00075-00-RS	MCHENRY	35	27
PLOT DATE = 2/13/2009		CHECKED - LMF	REVISED -					CONTRACT NO. 63138			
		DATE - 2/13/2009	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

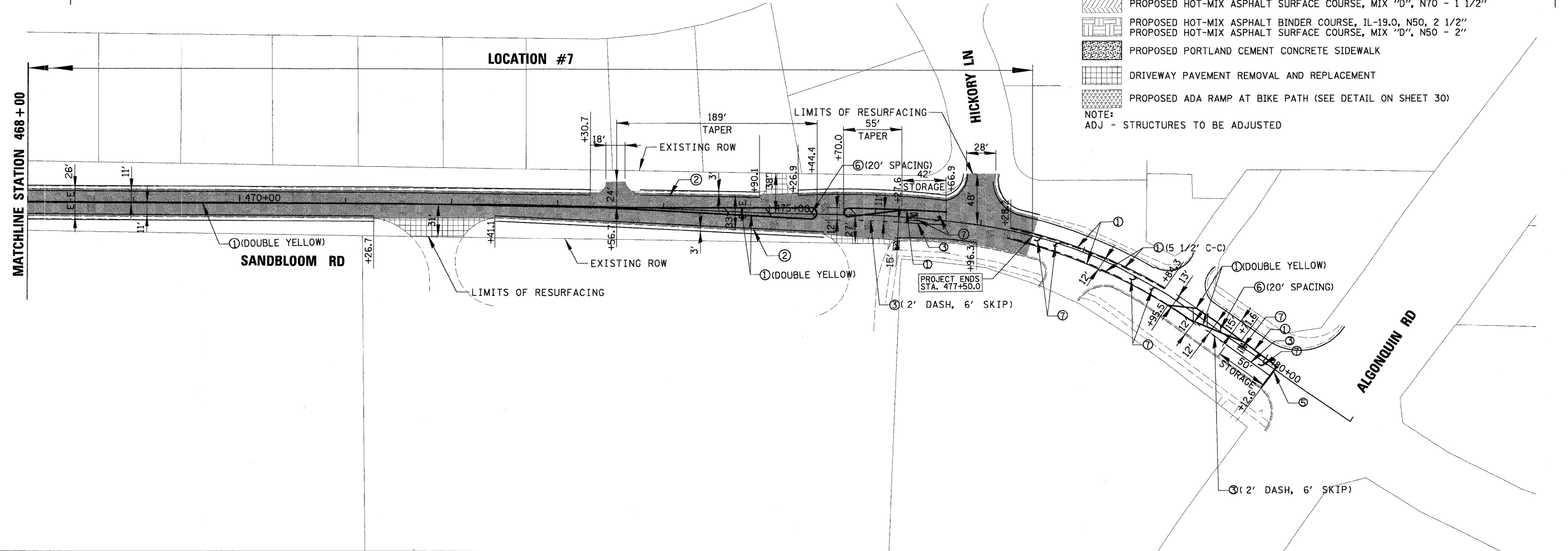


- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
 - ④ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
 - PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 - PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
 - PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)
- NOTE:**
ADJ - STRUCTURES TO BE ADJUSTED

FILE NAME = N:\VALGONGUIN\070273\070273\00019\Civil\08070273 LAMP Sandbloom-2.dgn	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SANDBLOOM RD - 2		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50'	CHECKED - LMF	REVISED -		SCALE: 50'	SHEET NO. 8 OF 9 SHEETS	STA. 428+00 TO STA. 454+00	08-00075-00-RS	MCHENRY	35	28
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -	REVISED -							CONTRACT NO. 63138	
							FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- LEGEND**
- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW)
 - ② THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
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 - ⑤ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)
 - ⑥ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW)
 - ⑦ THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
- PROPOSED LEVELING BINDER (MACHINE METHOD), N70 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 2 1/2"
 - PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 - 1 1/2"
 - PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2 1/2"
 - PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"
 - PROPOSED PORTLAND CEMENT CONCRETE SIDEWALK
 - DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT
 - PROPOSED ADA RAMP AT BIKE PATH (SEE DETAIL ON SHEET 30)
- NOTE:**
 ADJ - STRUCTURES TO BE ADJUSTED



FILE NAME =	USER NAME = MAGLOEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN SANDBLOOM RD - 3	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALGONQUIN\070273\070273\00019\Civil\PC070273 LAPP Sandbloom-3.dgn	DRAWN - MDH	REVISED -	08-00075-00-RS			MCHENRY	35	29		
PLOT SCALE = 50'	CHECKED - LMF	REVISED -	CONTRACT NO. 63138							
PLOT DATE = 2/13/2009	DATE - 2/13/2009	REVISED -	ILLINOIS FED. AID PROJECT							
					SCALE: 50'	SHEET NO. 9 OF 9 SHEETS		STA. 454+00 TO STA. 481+00		

CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2" THICK HOT-MIX MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- A) REMOVE THE HOT-MIX MATERIAL AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

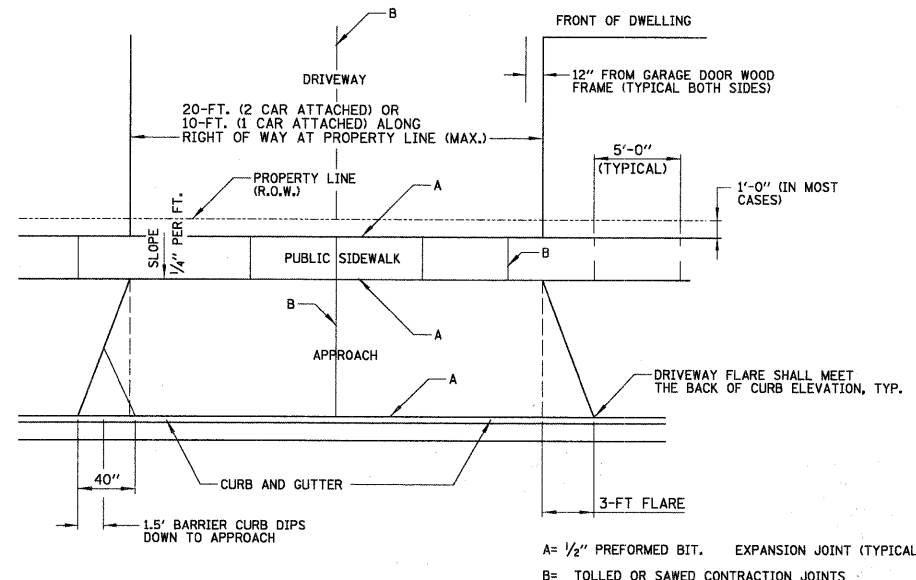
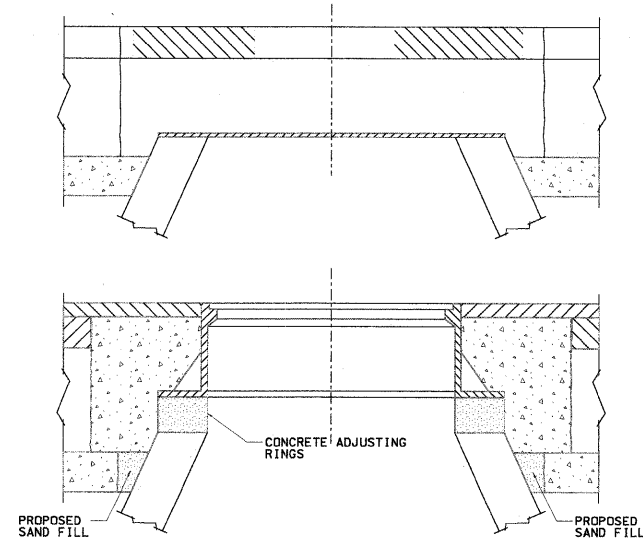
BASIS OF PAYMENT

STRUCTURE TO BE ADJUSTED.

NOTES

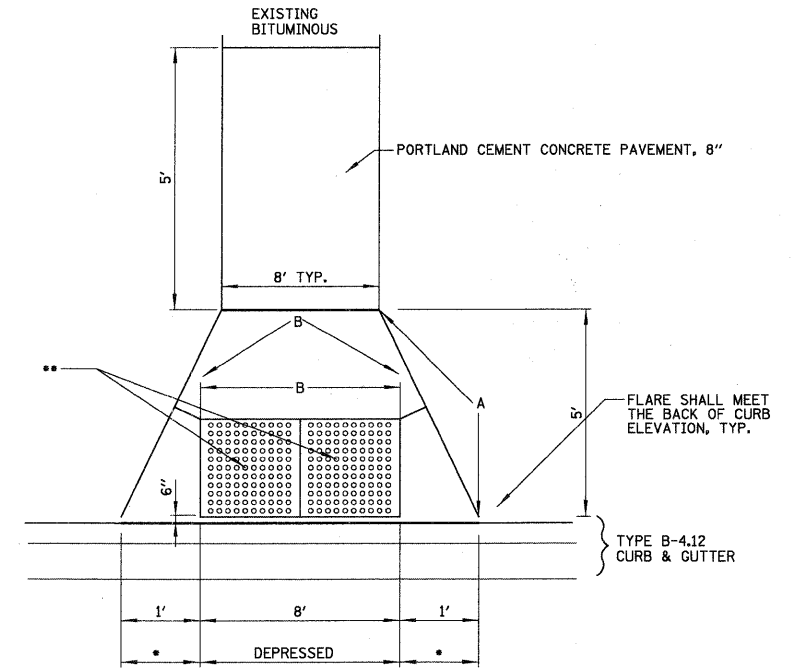
1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
4. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE PROVIDED AND SHALL CONSIST OF A RUBBER SLEEVE, COMPRESSION BAND AND EXTENSION SKIRT. RUBBER SLEEVE SHALL BE HIGH GRADE RUBBER COMPOUND CONFORMING TO ASTM C293 WITH A HARDNESS OF 45 PLUS OR MINUS 5. COMPRESSION BANDS SHALL BE 16 GAUGE TYPE 304 STAINLESS STEEL WITH A MINIMUM WIDTH OF 1 INCH. EXTENSION WEIGHT OF 12 OUNCES PER SQUARE YARD. EXTERNAL MANHOLE CHIMNEY SEAL SHALL BE MANUFACTURED BY CANUSA, INFISHIELD OR APPROVED EQUAL.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING



DRIVEWAY WITH A CURB AND GUTTER

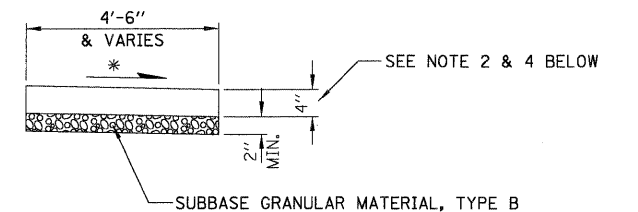
- GENERAL NOTES:**
1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 6%.
 2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 6%.
 3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED, (95% PROCTOR)
 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS. (NO WIRE MESH)
 5. MINIMUM THICKNESS FOR APPROACH, (NO WIRE MESH), THIS WILL BE PAID FOR BY THE FOLLOWING ITEMS:
 - A. 7" THK. P.C. CONCRETE ON 2" AGGREGATE BASE COURSE TYPE B OR
 - B. 3" THK. HOT-MIX ASPHALT SURFACE, MIX "D" NSO ON 6" AGGREGATE BASE COURSE TYPE B
 6. SALT TOLERANT SOD AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF DRIVEWAY REMOVAL AND REPLACEMENT.



- 1' TRANSITION FROM DEPRESSED CURB AND GUTTER TO FULL HEIGHT CURB AND GUTTER
- EAST JORDAN IRON WORKS (EJIW) OR APPROVED EQUAL DETECTABLE WARNING PLATES PRODUCT # 00700542 DIPPED (SEE DETAIL), COLOR SELECTED BY VILLAGE
- A = 1/2" PREFORMED EXPANSION JOINT (TYP.)
- B = TOOLED OR SAWED CONTRACTION JOINT

ADA RAMP AT BIKE PATH

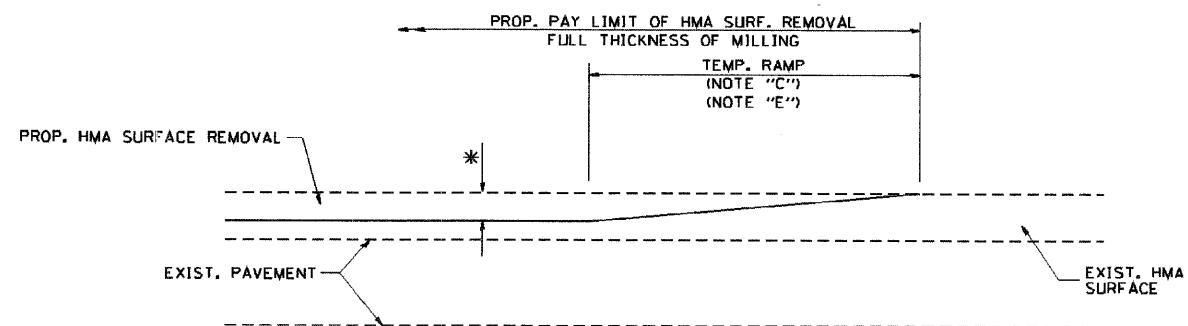
NOT TO SCALE



- CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS
- 1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 4 INCH, REMOVE AND REPLACE
- 2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFIELD WITHIN 24 HOURS.
- 3. SALT TOLERANT SOD AND TOPSOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF SIDEWALK REMOVAL AND REPLACEMENT.
- 4. PUBLIC SIDEWALK SHALL BE 6" AT RESIDENTIAL DRIVEWAYS AND 8" AT COMMERCIAL/INDUSTRIAL DRIVEWAYS.

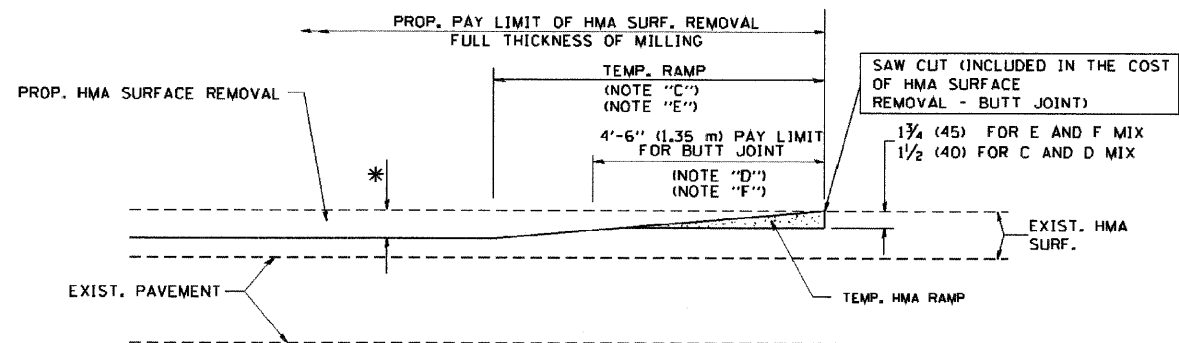
P.C.C. SIDEWALK DETAIL

FILE NAME =	USER NAME = MAGOLDEN	DESIGNED - LMF	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\ALCONQUIN\070273\070273.00019\Civil\070273_00019-01.dwg	PLOT SCALE = 5'	DRAWN - MDH	REVISED -			08-00075-00-RS	MCHENRY	35	30	
PLOT DATE = 2/13/2009	DATE - 2/13/2009	CHECKED - LMF	REVISED -			CONTRACT NO. 63138				
						SCALE: 5'	SHEET NO.	OF	SHEETS	STA.



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

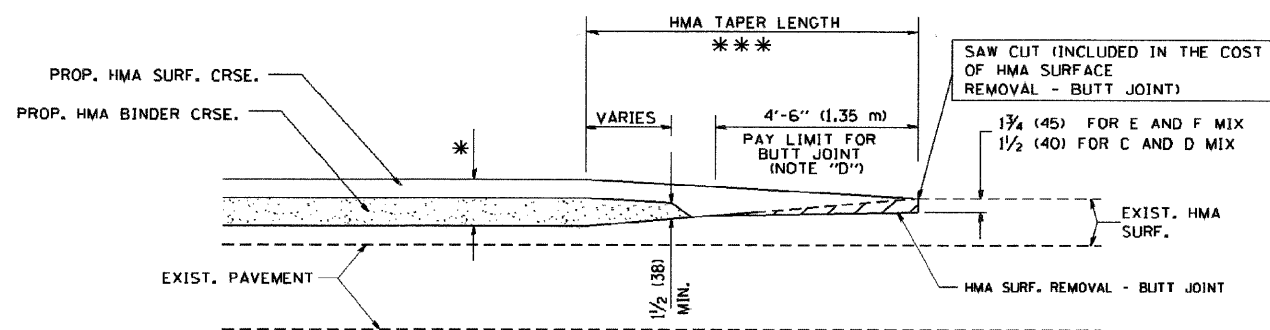
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

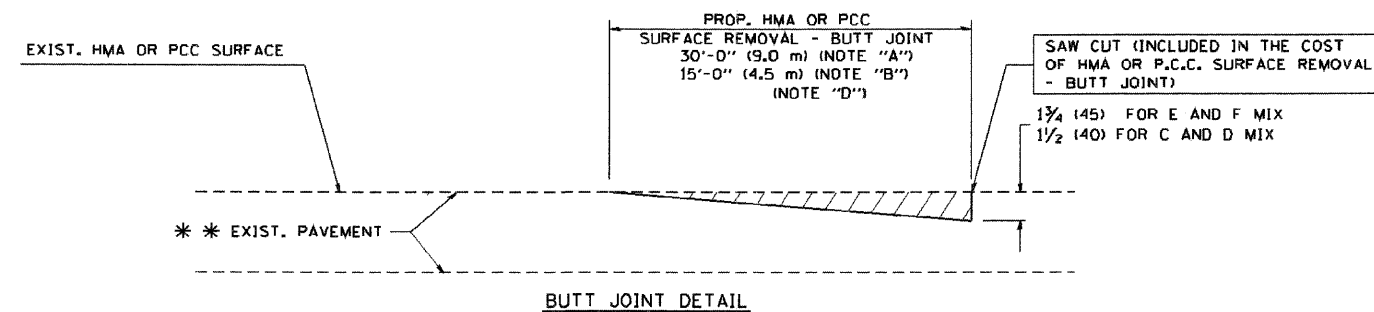
OPTION 2

TYPICAL TEMPORARY RAMP

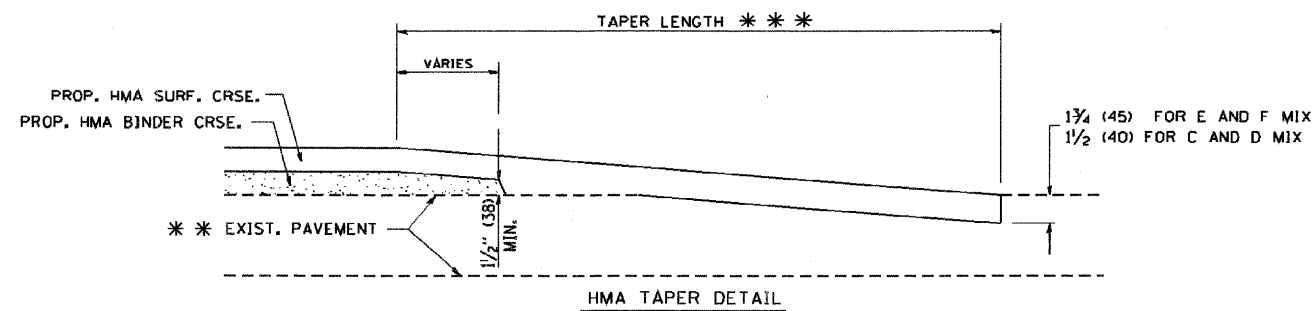


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

ALL BUTT JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =
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USER NAME = goglianobt
PLOT SCALE = 50.0000 / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	08-00075-00-RS	MCHENRY	35	31
BD400-05 BD32			CONTRACT NO. 63138	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001

18" (450) MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND/OR SIDEWALK REMOVAL AND REPLACEMENT.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

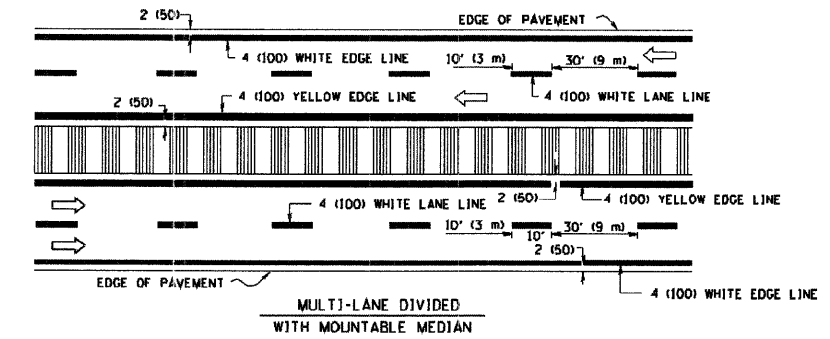
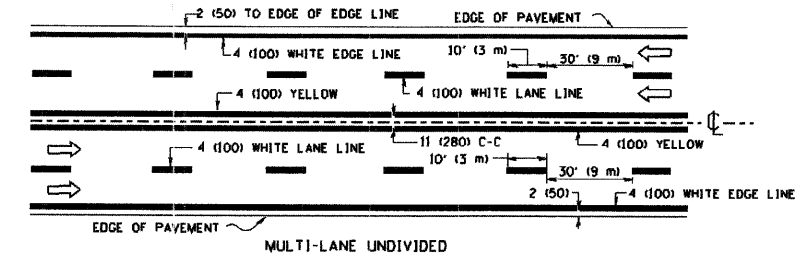
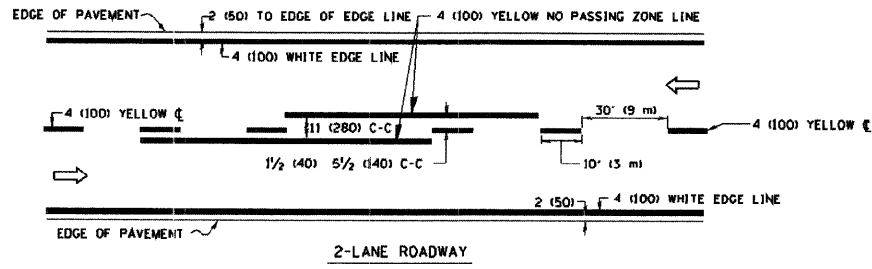
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

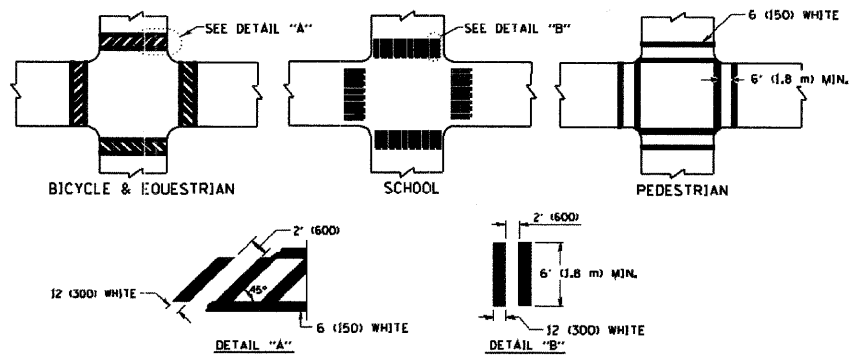
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd24.dgn	USER NAME = gajlienobt	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	08-00075-00-R5	MCHENRY	35	32	
		PLOT SCALE = 50.000 / IN.	REVISED - M. GOMEZ 01-22-01						BD600-06 (BD-24)				
		PLOT DATE = 1/4/2008	REVISED - R. BORO 01-01-07						CONTRACT NO. 63138				

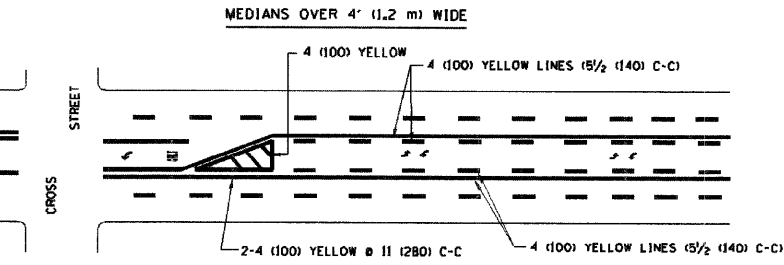
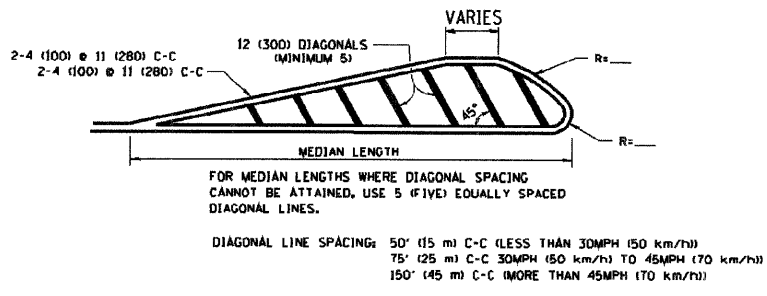
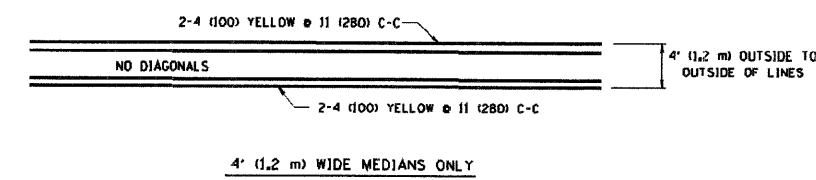
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



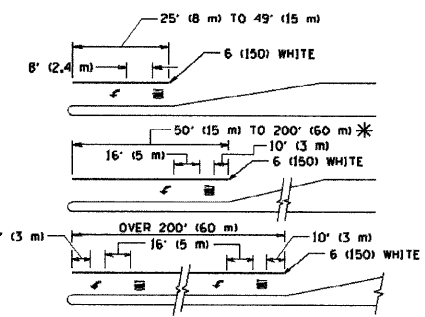
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

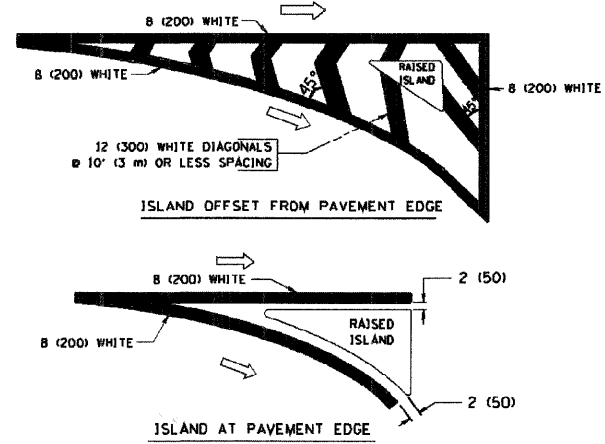


TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL TURN LANE MARKING

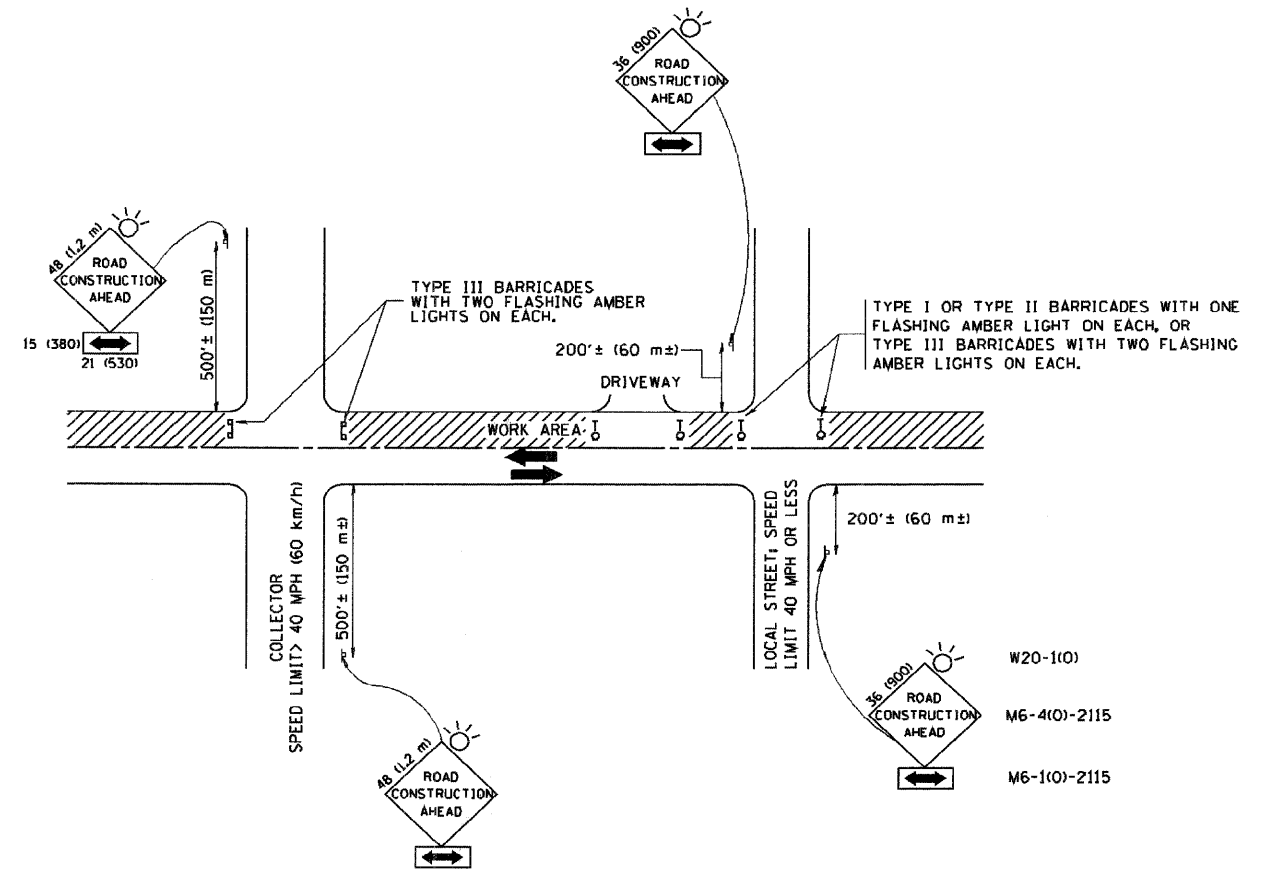


TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 6 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW. EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD T80001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD T80001.

All dimensions are in inches (millimeters) unless otherwise shown.



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

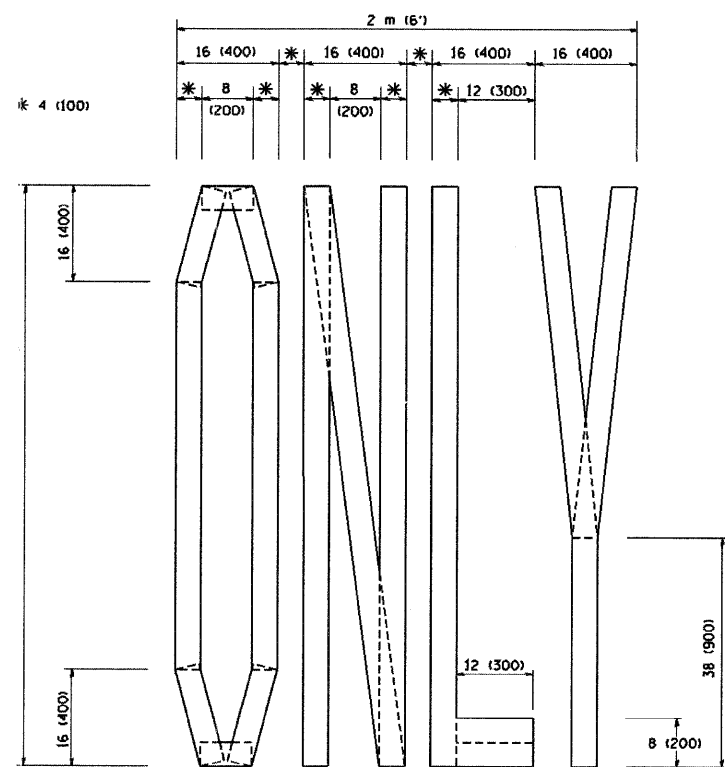
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = W:\diststd\22x34\101R.dgn	USER NAME = gaglianab	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

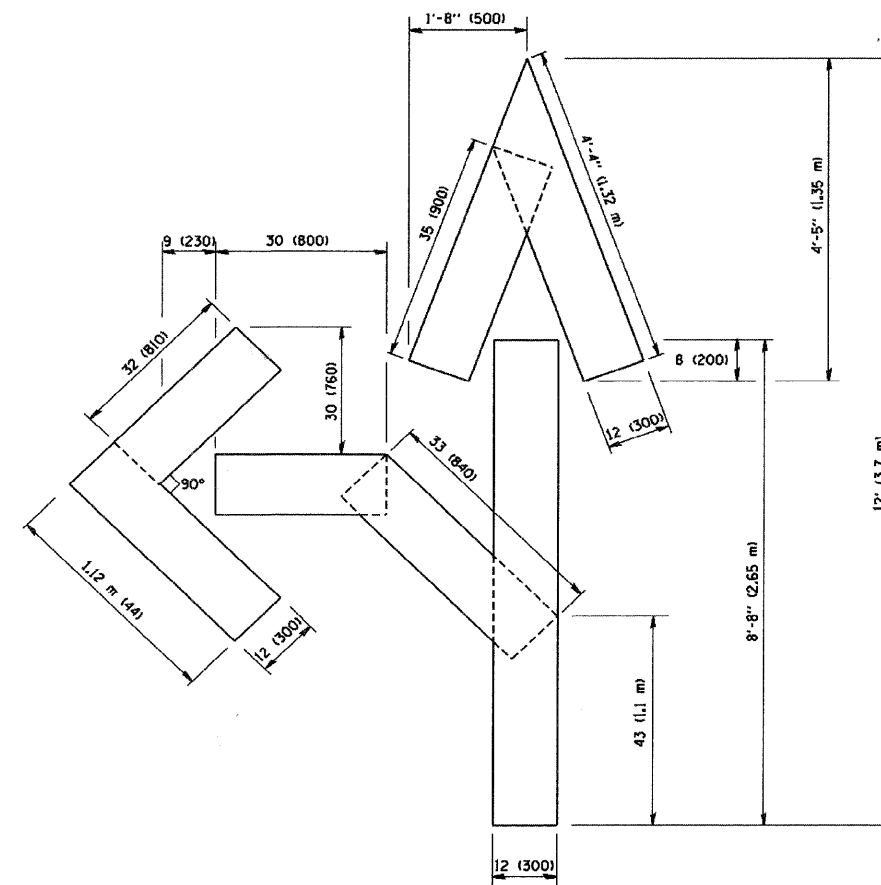
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

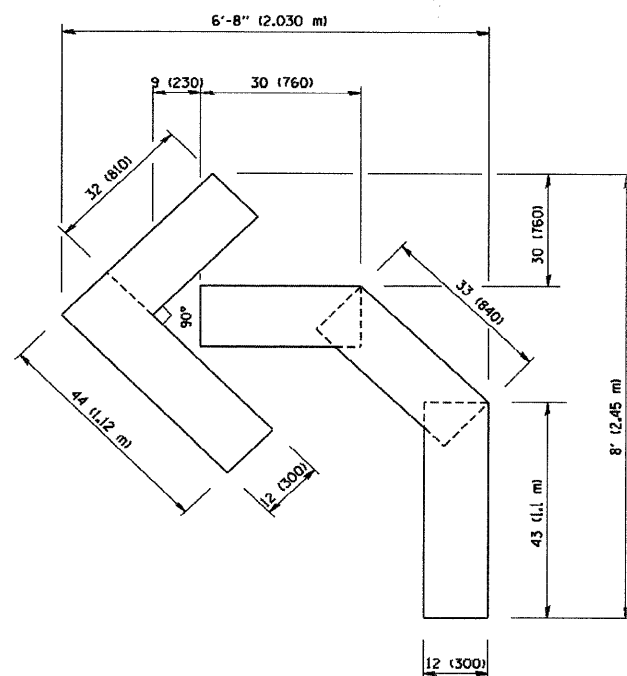
F.A. - RTE.	SECTION 08-00075-00-RS	COUNTY MCHENRY	TOTAL SHEETS 35	SHEET NO. 34
TC-10			CONTRACT NO. 6313B	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\diststd\22x34\tcl6.dgn	USER NAME = gaqlianobt	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING		F.A. -	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000 / IN.	DRAWN -	REVISED -T. RAMMACHER 11-04-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	08-00075-00-RS	MCHENRY	35	35
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -T. RAMMACHER 03-02-98					TC-16		CONTRACT NO. 63138	
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			