

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 63144	

INDEX OF SHEETS

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STATE STANDARDS
SEE SHEET 2

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

FAU 126 (PINGREE ROAD)/TERRA COTTA ROAD AT FAU 116 (CRYSTAL LAKE AVENUE)

INTERSECTION IMPROVEMENTS

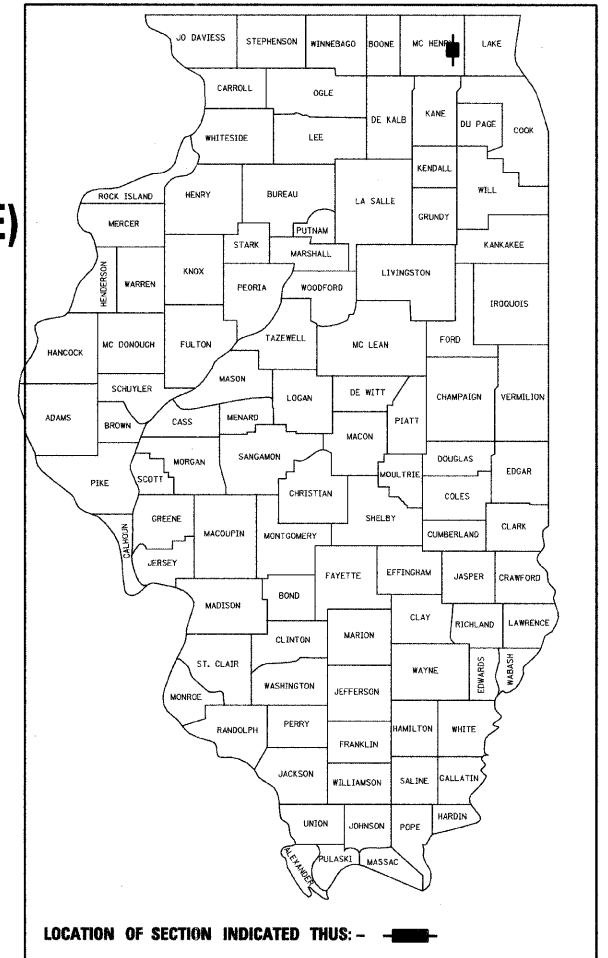
SECTION 08-00327-01-CH

PROJECT HD-M-8003(602)

ALGONQUIN TOWNSHIP & NUNDA TOWNSHIP

MCHENRY COUNTY

C-91-269-06



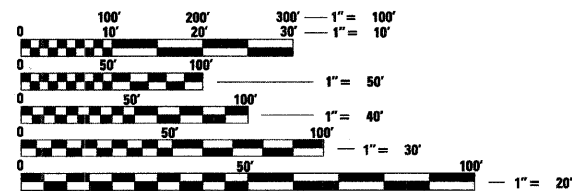
TRAFFIC DATA

ADT:
PINGREE RD./TERRA COTTA RD. 11,800 VPD (2007) 15,000 VPD (2030)
CRYSTAL LAKE AVE. 13,200 VPD (2007) 17,000 VPD (2030)

POSTED SPEED DESIGN SPEED
30 MPH (EXISTING) 35 MPH (PROPOSED)

CLASSIFICATION

PINGREE RD./TERRA COTTA RD. (FAU 0126) - URBAN STREET
CRYSTAL LAKE AVE. (FAU 0116) - URBAN STREET



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION CALL 811

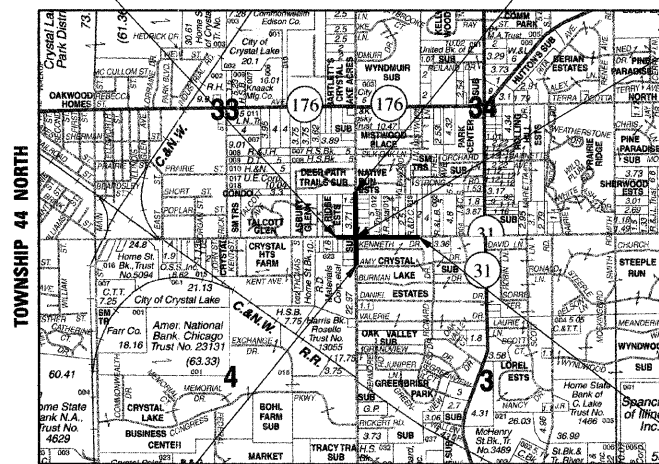
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PROJECT ENGINEER: JEFFREY STRZALKA
PROJECT MANAGER: JACK MELHUISH
CONTRACT NO. 63144

BEGIN PROJECT
CRYSTAL LAKE AVENUE
STA. 104+38

**PROJECT LOCATION MAP
N.T.S.
RANGE 8 EAST**



END PROJECT
TERRA COTTA ROAD
STA. 25+23

END PINGREE ROAD,
BEGIN TERRA COTTA ROAD
STA. 16+51.19 = STA. 106+97.76
CRYSTAL LAKE AVENUE

BEGIN PROJECT
PINGREE ROAD
STA. 10+00

END PROJECT
CRYSTAL LAKE AVENUE
STA. 113+02

PROJECT LENGTH

NET LENGTH OF PROJECT (PINGREE ROAD) = 651 FT (0.123 MI)
NET LENGTH OF PROJECT (TERRA COTTA ROAD) = 872 FT (0.165 MI)
NET LENGTH OF PROJECT (CRYSTAL LAKE AVENUE) = 864 FT (0.164 MI)

GROSS LENGTH OF PROJECT = 2,387 FT (0.452 MI)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED FEBRUARY 10 20 09
Joseph R. Korpalski Jr.
MCHENRY COUNTY DIVISION OF TRANSPORTATION

PASSED FEBRUARY 13 20 09
Charles H. Chastell
DISTRICT ONE ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW FEBRUARY 13 20 09
Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

PROFESSIONAL ENGINEER'S SIGN & SEAL

Jack Melhuish
JACK R. MELHUISH, JR. 02/13/2009
EXPIRES: 11-30-09

SEAL

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

COMPANY NAME: Smith
 PROJECT CONTACT: J. MELHUISH
 CLIENT: ALGONQUIN AND NUNDA TOWNSHIP
 DATE PLOTTED: 2/9/2009 5:29:29 PM
 PLOT DEVICE: pdf-processor
 PEN TABLE: standard-traffic

ASSOCIATE FEDERAL AID DESIGN ENGINEER: KEVIN STALLWORTH (847)-705-4169

GENERAL NOTES

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2007.
- ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
- A SOILS REPORT IS ON FILE AND AVAILABLE FOR REVIEW AT SEC GROUP, INC. MCHENRY OFFICE UPON REQUEST.
- PRIOR TO COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AFFECTING THEIR WORK WITH THE ACTUAL CONDITIONS AT THE JOB SITE. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, HE MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, STANDARD SPECIFICATIONS AND/OR SPECIAL DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED BY OMISSIONS OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE TRUE MEANING OF THE CONSTRUCTION PLANS OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN EXISTING FIELD CONDITIONS PRIOR TO BIDDING ON THE PROJECT.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE CITY OF CRYSTAL LAKE AT (815) 459-2020 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE BUTT JOINT AND BITUMINOUS TAPER DETAILS SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- ALL ELEVATIONS SHOWN ON THE PLANS ARE ON THE USGS DATUM.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN ALONG RETURNS AND AT POINTS OF CURVATURE, ETC. ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.0004 GALLONS / SQ YD.
- SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.
- DRIVEWAY OFFSETS ARE TO BE CONSTRUCTED TO THE R.O.W. OR UNLESS OTHERWISE NOTED.
- REMOVAL OF EXISTING COMBINATION CURB AND GUTTER, REGARDLESS OF CURB AND GUTTER TYPE, SHALL BE PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL".
- THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE NOTED.
- THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.10 GAL/SQ. YD..
- ALL SIGNS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE TOWNSHIPS AND SHALL BE RETURNED TO THE TOWNSHIPS UPON REMOVAL.

STORM SEWERS, WATER MAINS, AND UTILITIES

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 3 DAYS PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.

GENERAL NOTES (CONT.)

STORM SEWERS, WATER MAINS, AND UTILITIES (CONT.)

- UNLESS OTHERWISE NOTED ALL OFFSETS FOR ALL DRAINAGE STRUCTURES ARE TO THE CENTER OF THE STRUCTURE.
- STORM SEWER LENGTHS ARE MEASURED FROM INSIDE EDGE OF STRUCTURE TO THE INSIDE EDGE OF STRUCTURE, THEN ADDING 0.5' TO EACH END OF PIPE. SLOPE OF PIPE IS CALCULATED BY DIVIDING THE DIFFERENCE IN ELEVATION BY THE PIPE LENGTH.
- THE PAY ITEM FOR STORM SEWER REMOVAL SHALL INCLUDE ANY TRENCH BACKFILL REQUIRED TO FILL AREA OF SEWER REMOVAL WHERE THERE IS NO REPLACEMENT. IF PROPOSED STORM SEWER IS REPLACED IN THE SAME TRENCH, THEN TRENCH BACKFILL SHALL BE PAID FOR SEPARATELY.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN, IN AN OPERATING CONDITION, TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY THE EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- ALL FRAMES, GRATES, LIDS, AND BOXES SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY AND BE DELIVERED TO THE RESPECTIVE TOWNSHIP, AS APPLICABLE.
- ANY ITEMS DAMAGED DURING REMOVAL OPERATIONS SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, LIDS, OR BOXES AND/OR STOCKPILING THEM ON THE JOB SITE FOR DELIVERY TO ALGONQUIN AND NUNDA TOWNSHIP, AS APPLICABLE SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THE CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, INLET, VALVE VAULT OR METER VAULT SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM." ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURES SHALL BEAR THE WORD "WATER." ALL LIDS TO BE USED ON SANITARY SYSTEM STRUCTURES SHALL BEAR THE WORD "SANITARY." ANY ADDITIONAL COST FOR THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE FRAMES AND CLOSED LIDS PROVIDED.
- FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT OR RECONSTRUCTION COST.
- THE THICKNESSES OF HOT MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE HOT MIX ASPHALT MIXTURES ARE TO BE PLACED.
- PROTECTIVE COAT SHALL BE APPLIED TO ALL GUTTER FLAGS, FACE AND TOP OF CURB, PORTLAND CEMENT CONCRETE SIDEWALK AND AS DIRECTED BY THE ENGINEER.
- WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.
- TYPE "B" CURB RAMPS SHALL BE INSTALLED AT ALL INTERSECTING STREETS AND DRIVEWAYS PER CURRENT IDOT STANDARDS AT LOCATIONS WHERE SIDEWALK IS SHOWN ON THE PLAN.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL EXISTING MAILBOXES WHICH INTERFERE WITH CONSTRUCTION OPERATIONS AND AFTER COMPLETION OF ROADWAY CONSTRUCTIONS, TO SET THEM IN THEIR PERMANENT LOCATIONS PER UNITED STATES POST OFFICE AND TOWNSHIP HIGHWAY COMMISSIONER REQUIREMENTS. THIS WORK SHALL BE DONE IN CONFORMANCE WITH ARTICLE 107.20 OF THE STANDARDS SPECIFICATIONS, AND THE COST WILL BE CONSIDERED INCLUDED IN THE CONTRACT.
- THE INDISCRIMINATE USE OF FIRE HYDRANTS OR EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE ENGINEER PRIOR TO USE OF THE WATER.
- ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12" IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET AND VALVE VAULT STRUCTURES. COMMON BRICK WILL NOT BE ALLOWED. THE RINGS SHALL BE INCLUDED IN THE COST OF WORK BEING PERFORMED.

BACKFILL

- STORM SEWER, WATER MAIN, AND SANITARY SEWER SHALL BE BACK FILLED IN ACCORDANCE WITH ARTICLE 550.07, METHOD 1 ONLY.
- PROVIDE TRENCH BACKFILL FOR ALL UTILITY LINES WITHIN 2' OF PAVED AREAS. ALL TRENCH BACKFILL QUANTITIES FOR STORM SEWER HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PIPE SIZE AND INVERT DEPTH FROM SUBGRADE.
- COST OF ANY TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY ESTABLISHED, INCLUDING BEDDING MATERIAL, SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE BID.

SIGNING AND STRIPING

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.

GENERAL NOTES (CONT.)

- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH HIS CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- ALL SIGNS SHALL BE INSTALLED OR RELOCATED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. THIS WORK SHALL BE PAID FOR USING THE APPROPRIATE PAY ITEMS.
- ALL REMOVED SIGNS SHALL BE RETURNED TO ALGONQUIN AND NUNDA TOWNSHIPS, AS APPLICABLE.
- LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS. THIS WORK SHALL BE PAID FOR IN ACCORDANCE WITH SECTION 729 OF THE STANDARD SPECIFICATIONS.

EROSION CONTROL PLANS

- SEE EROSION CONTROL PLANS FOR GENERAL NOTES CONCERNING EROSION CONTROL.

TRAFFIC CONTROL

- SEE TRAFFIC CONTROL PLANS FOR GENERAL NOTES CONCERNING TRAFFIC CONTROL AND PROTECTION.

EARTHWORK

- PRIOR TO ANY EMBANKMENT PLACEMENT ALL VEGETATION AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL.
- SUBGRADE SHALL BE PREPARED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS BEFORE REMOVAL OF ANY UNSTABLE MATERIAL.

LANDSCAPING

- THE LOCATIONS OF PROPOSED TREES TO BE PLANTED WILL BE WITHIN THE PROJECT LIMITS AND DETERMINED BY THE RESIDENT ENGINEER.

STATE STANDARDS

STANDARD NO.	LIST OF DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERS
280001-04	TEMPORARY EROSION CONTROL SYSTEMS
424001-05	CURB RAMPS FOR SIDEWALK
442201-03	CLASS C AND D PATCHING
542301-02	PRECAST REINFORCED CONCRETE FLARED END SECTION
542401-01	METAL END SECTION FOR PIPE CULVERT
602001-01	CATCH BASIN TYPE A
602011-01	CATCH BASIN TYPE C
602301-02	INLET, TYPE A
602401-02	MANHOLE, TYPE A
602601-02	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	CAST IRON STEPS
604001-03	FRAME AND LIDS, TYPE 1
604011-04	FRAME AND GRATE, TYPE 3V
604036-02	GRATE, TYPE 8
604051-03	FRAME AND GRATE, TYPE 11
606001-04	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-05	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS (SIGNS, MARKERS, AND DELINEATORS)
720016-02	MAST ARM MOUNTED STREET NAME SIGNS
729001-01	APPLICATIONS FOR TYPE A AND B METAL POSTS
780001-02	PAVEMENT MARKINGS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-02	CONCRETE HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877011-04	STEEL COMBINATION MAST ARM ASSEMBLY AND POLE
878001-07	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS



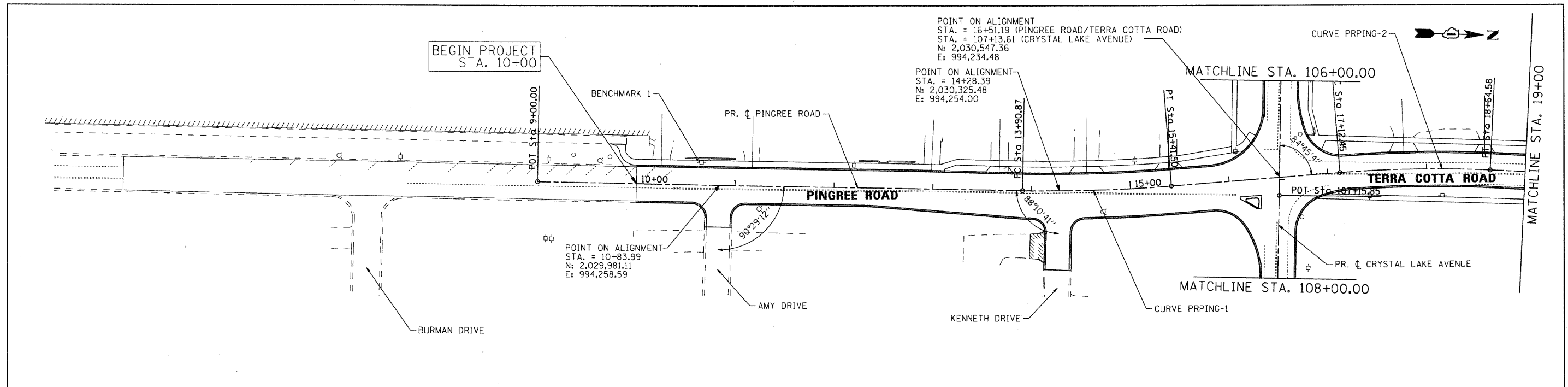
USER NAME = jmelhusah	DESIGNED - JRM	REVISED -
PLOT SCALE = N.T.S.	DRAWN - JPA	REVISED -
PLOT DATE = 3/5/2009	CHECKED - TCH	REVISED -
	DATE - 3/5/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**GENERAL NOTES, LIST OF HIGHWAY STANDARDS,
AND COMMITMENTS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	2
CONTRACT NO. 63144				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



PROP. CURVE PRPING-1

PI STA. = 14+66.24
 $\Delta = 5^\circ 21' 38''$ (LT)
 $D = 3^\circ 33' 31''$
 $R = 1,610.00'$
 $T = 75.37'$
 $L = 150.63'$
 $E = 1.76'$
 $e = N/A$
 $T.R. = N/A$
 $S.E. = N/A$
 $P.C. STA. = 13+90.87$
 $N = 2,030,287.97$
 $E = 994,254.89$
 $P.I. STA. = 14+66.19$
 $N = 2,030,363.34$
 $E = 994,253.98$
 $P.T. STA. = 15+41.50$
 $N = 2,030,438.29$
 $E = 994,246.04$

PROP. CURVE PRPING-2

PI STA. = 17+88.61
 $\Delta = 7^\circ 06' 55''$ (RT)
 $D = 4^\circ 40' 38''$
 $R = 1,225.00'$
 $T = 76.16'$
 $L = 152.13'$
 $E = 2.37'$
 $e = N/A$
 $T.R. = N/A$
 $S.E. = N/A$
 $P.C. STA. = 17+12.45$
 $N = 2,030,608.28$
 $E = 994,228.02$
 $P.I. STA. = 17+88.52$
 $N = 2,030,684.02$
 $E = 994,219.99$
 $P.T. STA. = 18+64.58$
 $N = 2,030,760.17$
 $E = 994,221.40$

PROP. CURVE PRPING-3

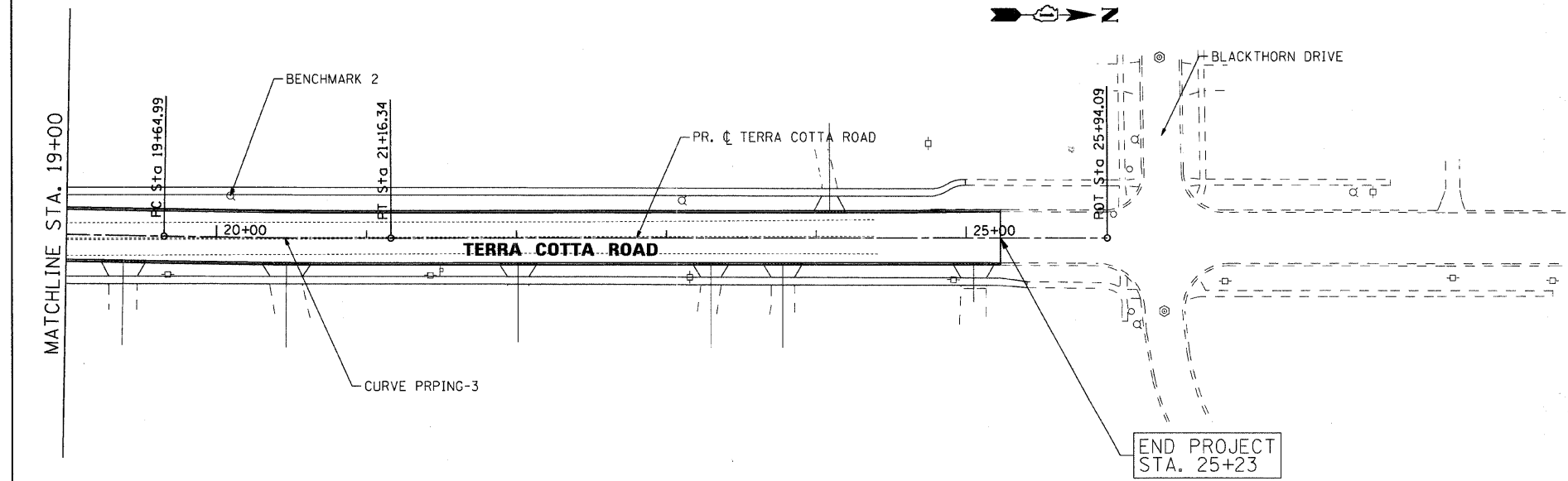
PI STA. = 20+40.67
 $\Delta = 1^\circ 09' 22''$ (LT)
 $D = 0^\circ 45' 50''$
 $R = 7,500.00'$
 $T = 75.67'$
 $L = 151.34'$
 $E = 0.38'$
 $e = N/A$
 $T.R. = N/A$
 $S.E. = N/A$
 $P.C. STA. = 19+64.99$
 $N = 2,030,860.57$
 $E = 994,223.27$
 $P.I. STA. = 20+40.66$
 $N = 2,030,936.23$
 $E = 994,224.68$
 $P.T. STA. = 21+16.34$
 $N = 2,030,011.90$
 $E = 994,224.56$

BENCHMARKS

- FOR BENCHMARK 1: RAILROAD SPIKE IN SOUTH FACE OF POWERPOLE LOCATED ON THE WEST SIDE OF PINGREE ROAD APPROXIMATELY 20 FEET SOUTH OF THE CENTERLINE OF AMY DRIVE. ELEVATION=910.66
- FOR BENCHMARK 2: ARROW BOLT ON TOP FLANGE OF HYDRANT LOCATED ON THE WEST SIDE OF TERRA COTTA ROAD APPROXIMATELY 357 FEET NORTH OF THE CENTERLINE OF CRYSTAL LAKE AVENUE. ELEVATION=911.61.

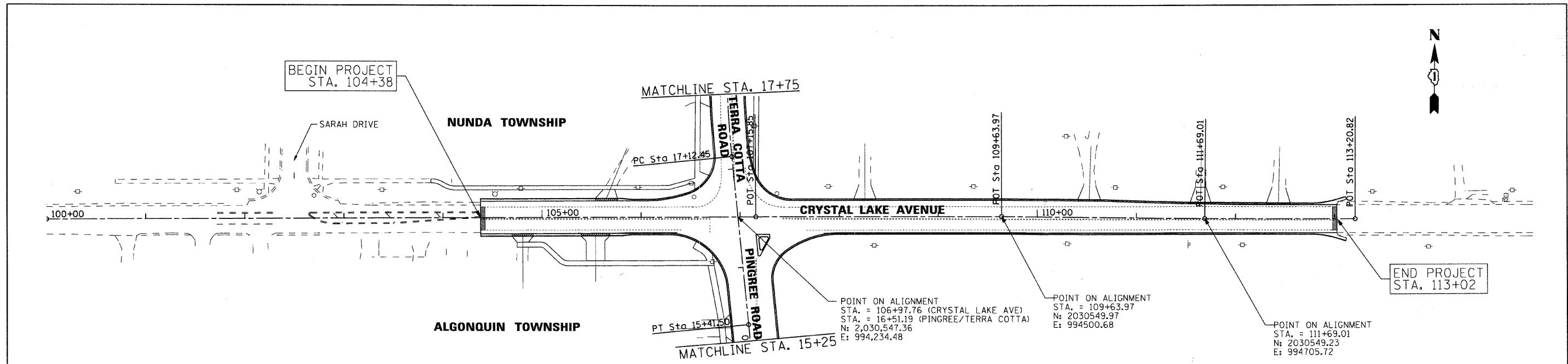
NOTE

- ALGONQUIN TOWNSHIP PROJECT LIMITS ARE FROM SOUTH OF CENTERLINE OF CRYSTAL LAKE AVENUE.
- NUNDA TOWNSHIP PROJECT LIMITS ARE FROM NORTH OF CENTERLINE OF CRYSTAL LAKE AVENUE.



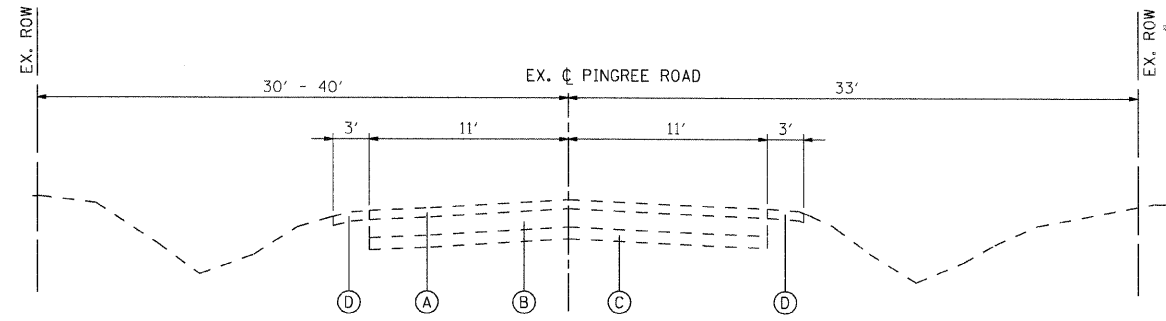
<p>SEC Group, Inc. Engineering Surveying Planning Landscape Architecture McHenry • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = chr-tnk	DESIGNED - JRM	REVISED -	MCHENRY COUNTY DIVISION OF TRANSPORTATION	ALIGNMENT, TIE AND BENCHMARKS PINGREE ROAD AND TERRA COTTA ROAD			F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 4	
	PLOT SCALE = 1" = 50'	DRAWN - JPA	REVISED -		SCALE: 1" = 50'	SHEET NO. 1 OF 2 SHEETS	STA. 10+00.00 TO STA. 25+22.88	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 63144			
	PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -										
		DATE - 2/13/09	REVISED -										

080154
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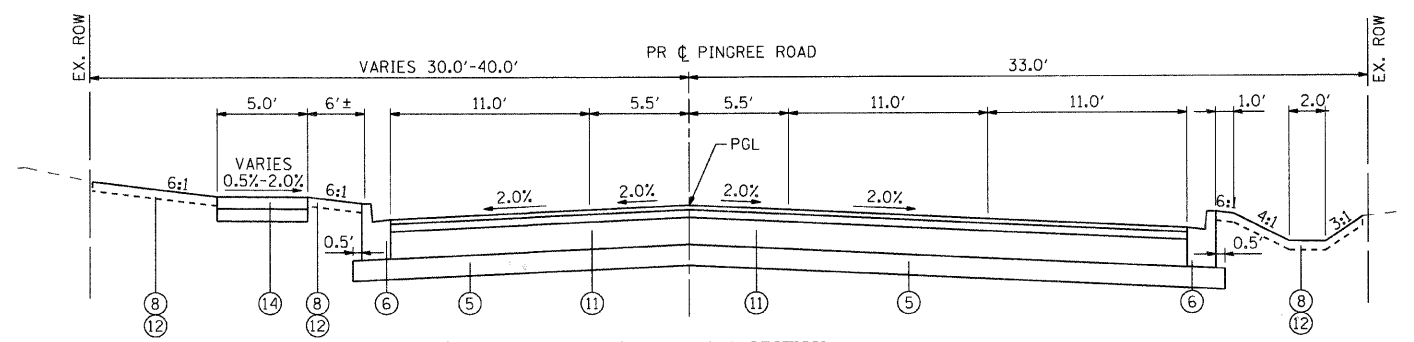


<p>SEC Group, Inc. <small>Since Professional Design Firm # 104-680108</small> Engineering Surveying Planning Landscape Architecture McHenry • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = charlie	DESIGNED - JRM	REVISED -	MCHENRY COUNTY DIVISION OF TRANSPORTATION	ALIGNMENT, TIE AND BENCHMARKS CRYSTAL LAKE AVENUE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1" = 50'	DRAWN - JPA	REVISED -		0116	08-00327-01-CH	MCHENRY	56	5			
	PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -		SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS STA. 104+38.00 TO STA. 113+02.22			CONTRACT NO. 63144				
		DATE - 2/13/09	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

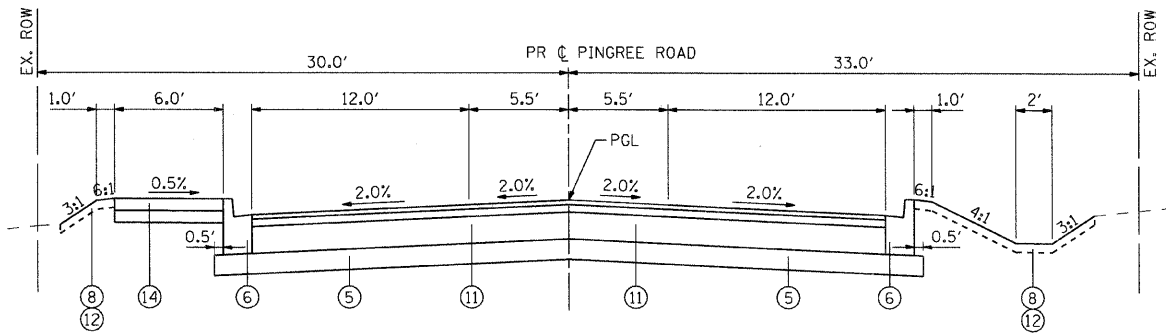
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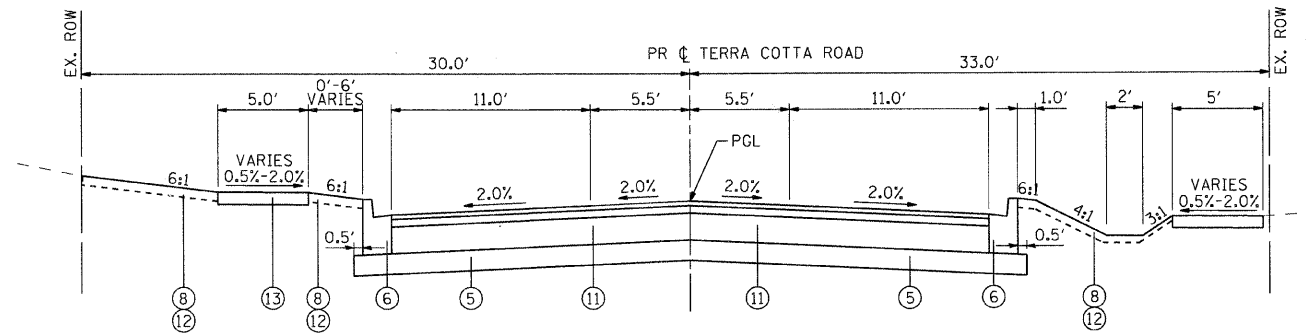
**EXISTING TYPICAL SECTION
PINGREE ROAD**
STA. 10+00 TO STA. 16+51.19
TERRA COTTA ROAD
STA. 16+51.19 TO STA. 25+23



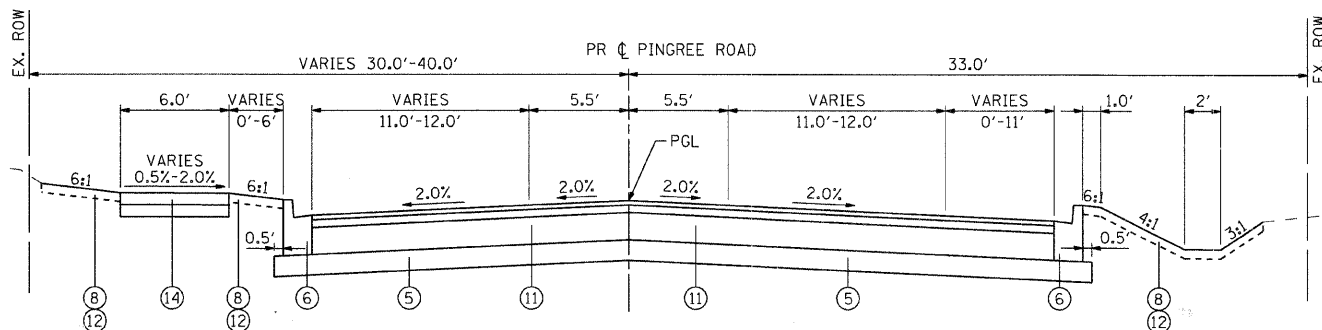
**PROPOSED TYPICAL SECTION
PINGREE ROAD**
STA. 13+91 TO STA. 16+51.19
(FULL SECTION NEAR INTERSECTION)



**PROPOSED TYPICAL SECTION
PINGREE ROAD**
STA. 10+00 TO STA. 12+36



**PROPOSED TYPICAL SECTION
TERRA COTTA ROAD**
STA. 16+51.19 TO STA. 25+23



**PROPOSED TYPICAL SECTION
PINGREE ROAD**
STA. 12+36 TO STA. 13+91
(TAPER FOR RIGHT TURN LANE)

EXISTING LEGEND

- (A) ASPHALT SURFACE COURSE, 3"±
- (B) ASPHALT BASE COURSE, 6"±
- (C) AGGREGATE SUBGRADE
- (D) AGGREGATE SHOULDER

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- (2) LEVELING BINDER (MACHINE METHOD), N70, 3/4" (MIN.)
- (3) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/2"
- (4) HOT-MIX ASPHALT BASE COURSE, 9"
- (5) SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- (6) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (7) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (8) TOPSOIL FURNISH AND PLACE, 6"
- (9) STRIP REFLECTIVE CRACK CONTROL TREATMENT
- (10) BITUMINOUS MATERIALS (PRIME COAT)
- (11) HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 13"
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 1 1/2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2"
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm); 9"
- (12) SEEDING, CLASS 2A
- (13) PCC SIDEWALK, 5"
- (14) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 3"
SUB-BASE GRANULAR MATERIAL, TYPE B 4"



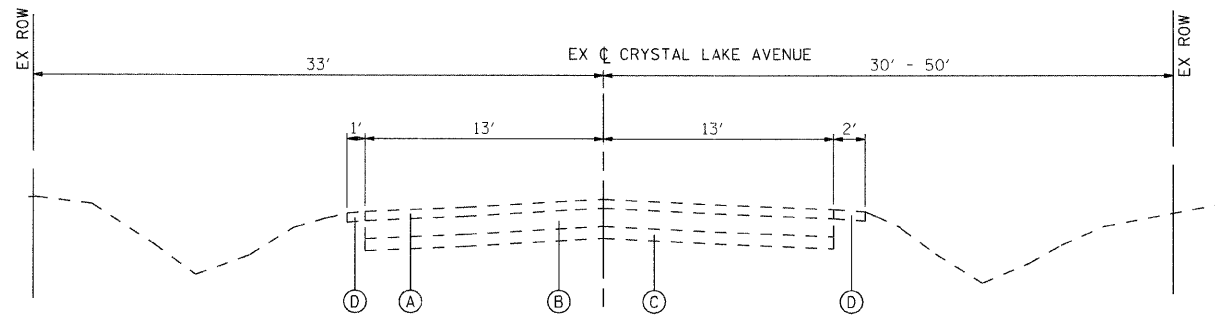
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	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

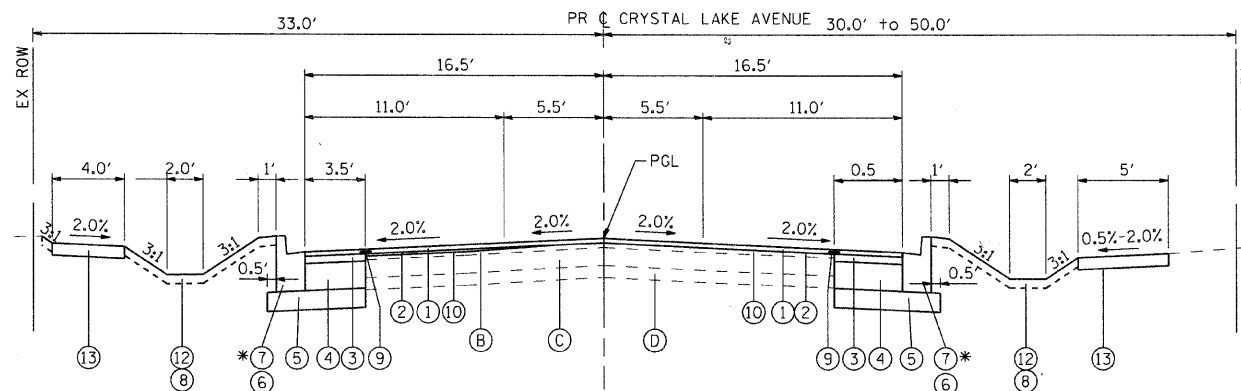
**TYPICAL SECTIONS
PINGREE ROAD AND TERRA COTTA ROAD**

SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	6
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 63144		

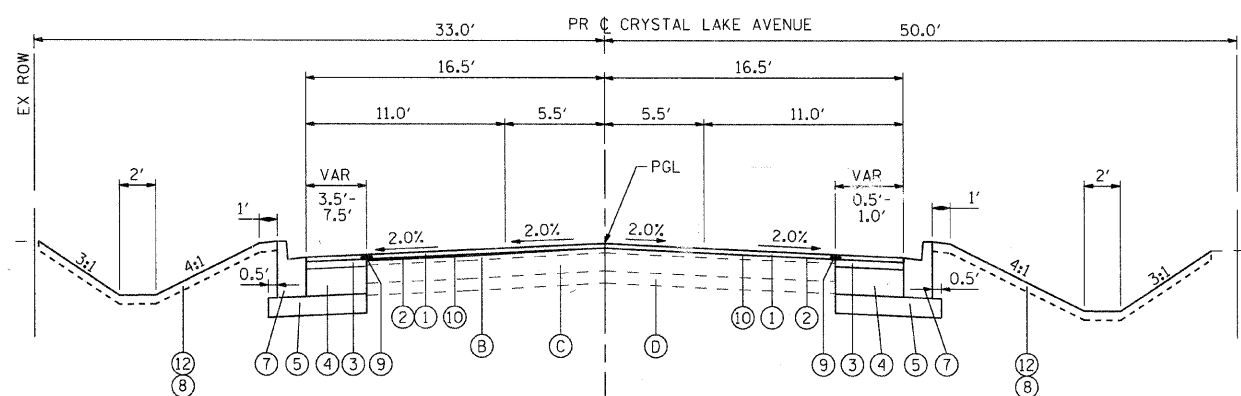


**EXISTING TYPICAL SECTION
CRYSTAL LAKE AVENUE**
STA. 104+38 TO STA. 113+02



* COMB C&G TYPE B-6.24 FROM
STA. 104+38 TO STA. 105+80

**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE**
STA. 104+38 TO STA. 107+00



**PROPOSED TYPICAL SECTION
CRYSTAL LAKE AVENUE**
STA. 107+00 TO STA. 113+02

EXISTING LEGEND

- Ⓐ ASPHALT SURFACE COURSE, 3"±
- Ⓑ ASPHALT BASE COURSE, 6"±
- Ⓒ AGGREGATE SUBGRADE, 4"
- Ⓓ AGGREGATE SHOULDER, 3"

PROPOSED LEGEND

- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ② LEVELING BINDER (MACHINE METHOD), N70, 3/4" (MIN.)
- ③ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/2"
- ④ HOT-MIX ASPHALT BASE COURSE, 9"
- ⑤ SUB-BASE GRANULAR MATERIAL, TYPE B 6"
- ⑥ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- ⑧ TOPSOIL FURNISH AND PLACE, 6"
- ⑨ STRIP REFLECTIVE CRACK CONTROL TREATMENT
- ⑩ BITUMINOUS MATERIALS (PRIME COAT)
- ⑪ HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 13"
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5mm); 1 1/2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70; 2 1/2"
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19mm); 9"
- ⑫ SEEDING, CLASS 2A
- ⑬ PCC SIDEWALK, 5"
- ⑭ HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 3"
SUB-BASE GRANULAR MATERIAL, TYPE B 4"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	VOIDS
HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 13"		
SURFACE COURSE, MIX "D", N70 1-1/2" (IL 9.5mm)	PG 64-22	4% @ 70 GYR.
BINDER COURSE, IL-19.0, N70 2-1/2"	PG 64-22*	4% @ 70 GYR.
HMA BASE COURSE (HMA BINDER IL-19mm)	PG 64-22*	4% @ 70 GYR.
PAVEMENT WIDENING		
SURFACE COURSE, MIX "D", N70 1-1/2" (IL 9.5mm)	PG 64-22	4% @ 70 GYR.
HMA BINDER COURSE, IL-19.0, N70 2-1/2"	PG 64-22*	4% @ 70 GYR.
HMA BASE COURSE (HMA BINDER IL-19mm)	PG 64-22*	4% @ 50 GYR.
LEVELING BINDER (MACHINE METHOD), N70	PG 64-22	4% @ 70 GYR.
DRIVEWAYS		
HMA SURFACE COURSE, MIX "C", N50 2"	PG 64-22	4% @ 50 GYR.
MULTI-USE PATH		
SURFACE COURSE, MIX "C", N50	PG 64-22	4% @ 50 GYR.
PATCHING		
CLASS D PATCHES, (HMA BINDER IL-19mm)	PG 64-22*	4% @ 70 GYR.

* WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.



USER NAME = char-tke	DESIGNED - JRM	REVISED -
PLOT SCALE = N.T.S.	DRAWN - JPA	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

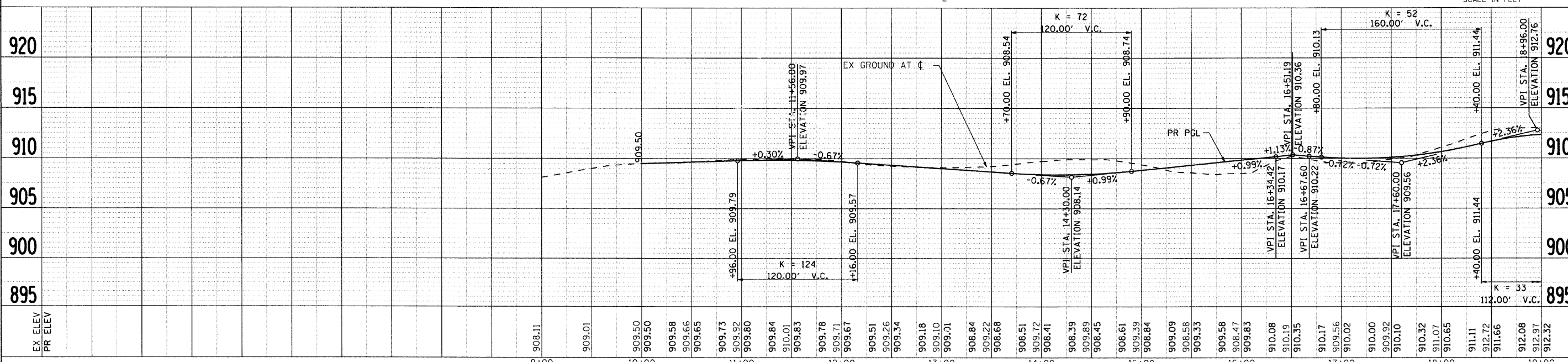
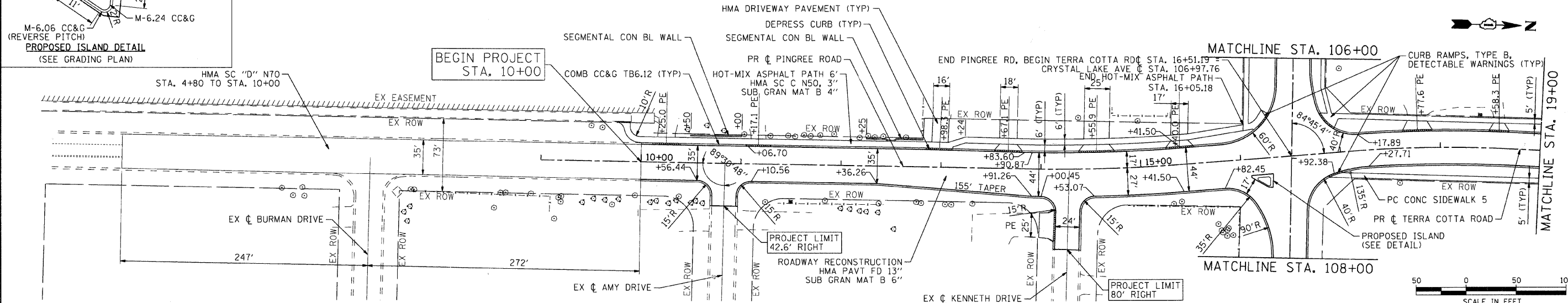
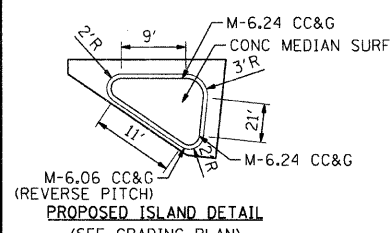
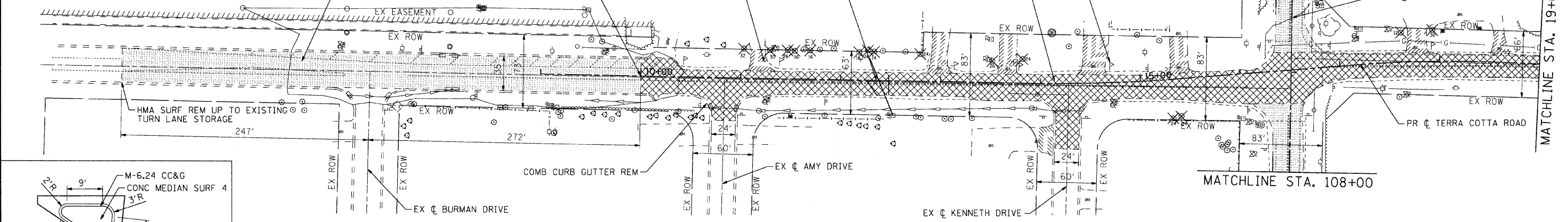
**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**TYPICAL SECTIONS
CRYSTAL LAKE AVENUE**

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	7
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63144				

- LEGEND**
- HMA SURFACE REMOVAL
 - PAVEMENT REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - TREE REMOVAL (UNITS)



DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	

DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	

COMPANY NAME: J. MELHUSH
 PROJECT CONTACT: ALGONQUIN AND NUNGA TOWNSHIP
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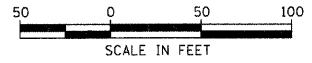
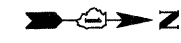
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PLOT SCALE = 1" = 50'	CHECKED - TCH	REVISED -
PLOT DATE = 2/13/2009	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

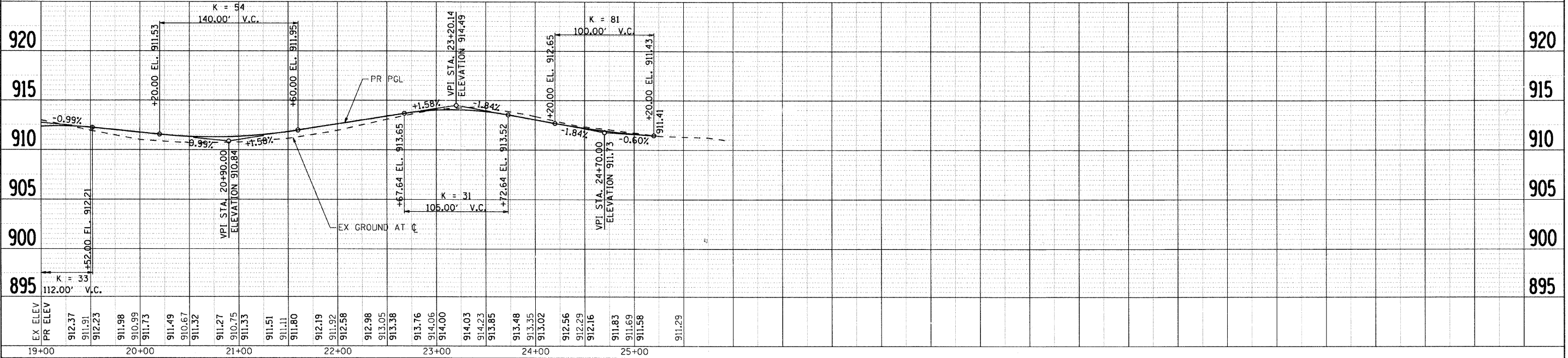
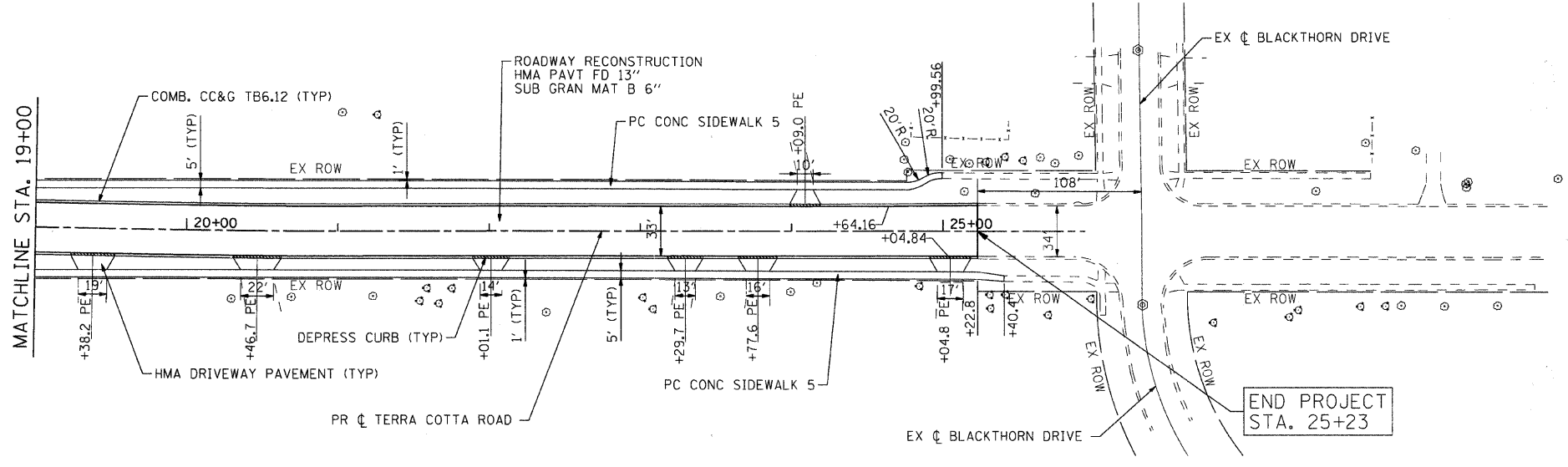
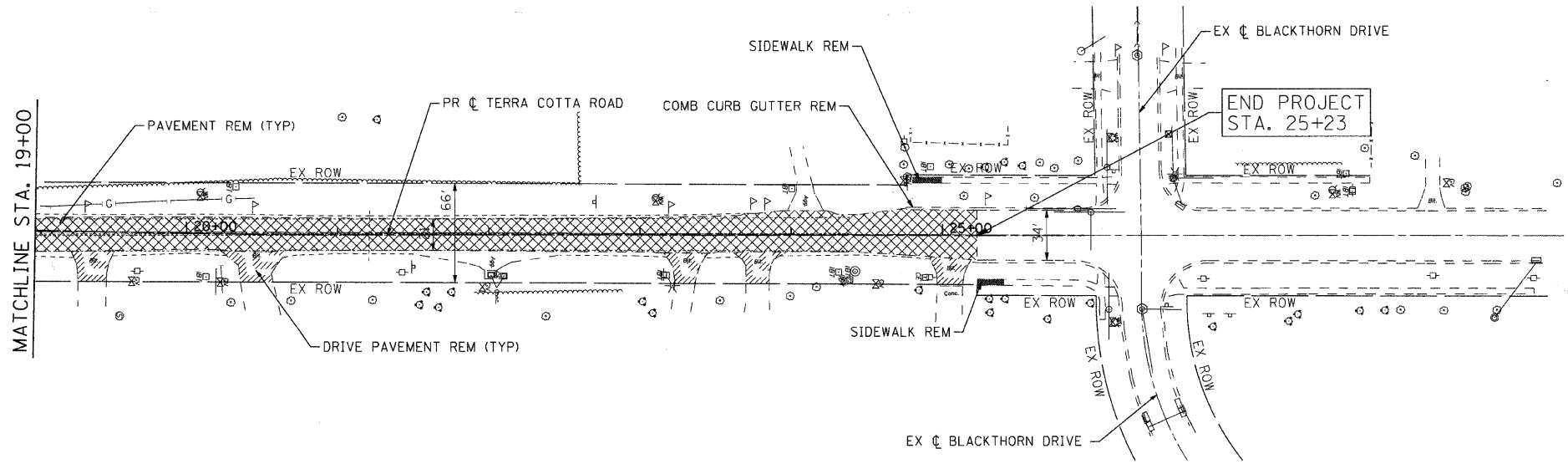
**PLAN AND PROFILE
PINGREE ROAD**

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. BEGIN TO STA. 19+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	8
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 63144



- LEGEND**
- HMA SURFACE REMOVAL
 - PAVEMENT REMOVAL
 - DRIVEWAY PAVEMENT REMOVAL
 - TREE REMOVAL (UNITS)



DATE	BY	DATE	BY

DATE	BY	DATE	BY

COMPANY NAME: J. MELNISH
PROJECT CONTACT: J. MELNISH AND NUNDA TOWNSHIP
CLIENT: J. MELNISH AND NUNDA TOWNSHIP
DATE PLOTTED: 2/13/2009 12:15:57 PM
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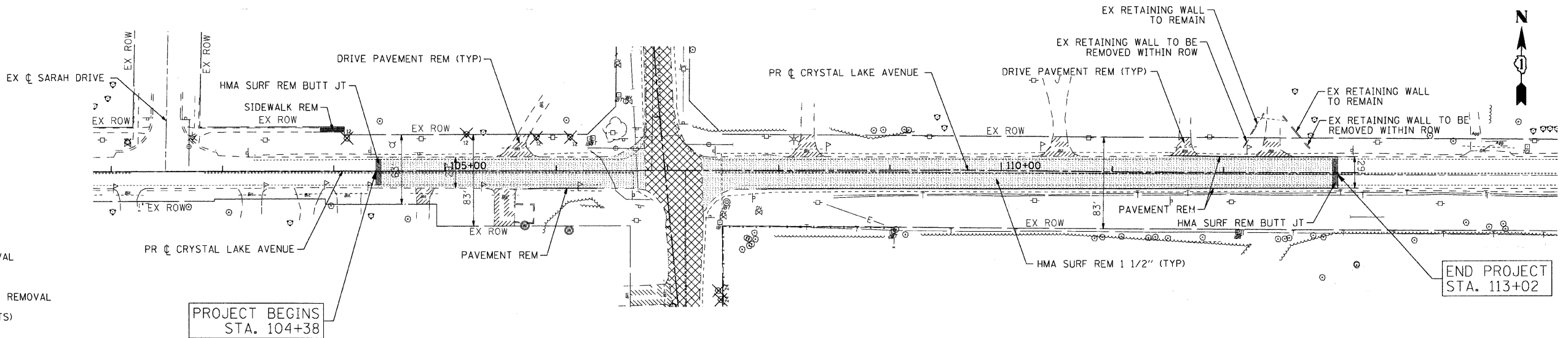
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PLOT DATE = 2/13/2009	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**PLAN AND PROFILE
TERRA COTTA ROAD**

SCALE: 1" = 50' SHEET NO. 2 OF 3 SHEETS STA. 19+00.00 TO STA. END

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	9
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 63144		

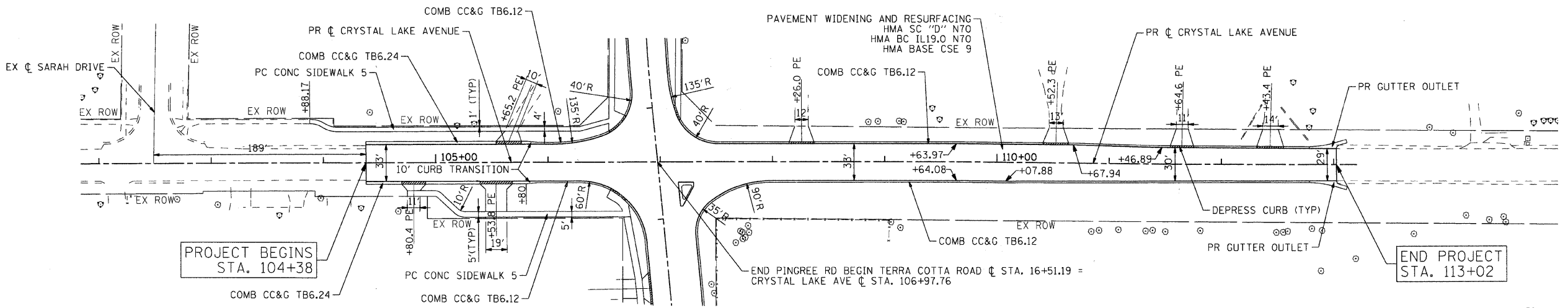


LEGEND

- HMA SURFACE REMOVAL
- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- TREE REMOVAL (UNITS)

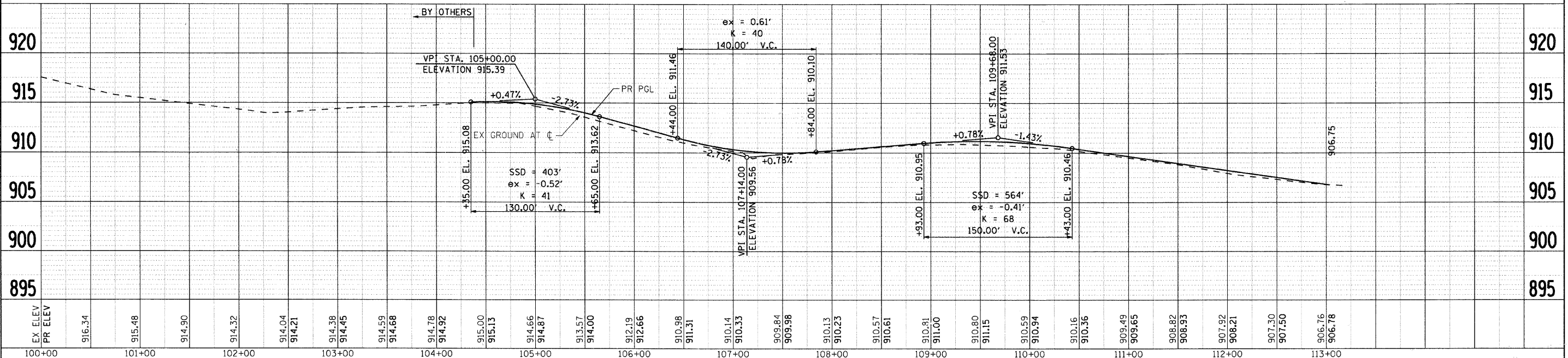
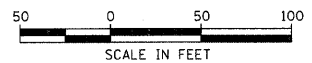
PROJECT BEGINS
STA. 104+38

END PROJECT
STA. 113+02



PROJECT BEGINS
STA. 104+38

END PROJECT
STA. 113+02



DATE: _____ BY: _____

REVISIONS:

NO.	DESCRIPTION

DATE: _____ BY: _____

REVISIONS:

NO.	DESCRIPTION

COMPANY NAME: Smith

PROJECT CONTACT: J. MEHREZ

CLIENT: ALGONQUIN AND NUNDA TOWNSHIP

DATE PLOTTED: 2/13/2009 12:21:17 PM

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PLOT DATE = 2/13/2009	DATE - 2/13/09	REVISED -

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

PLAN AND PROFILE
CRYSTAL LAKE AVENUE

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. BEGIN TO STA. END

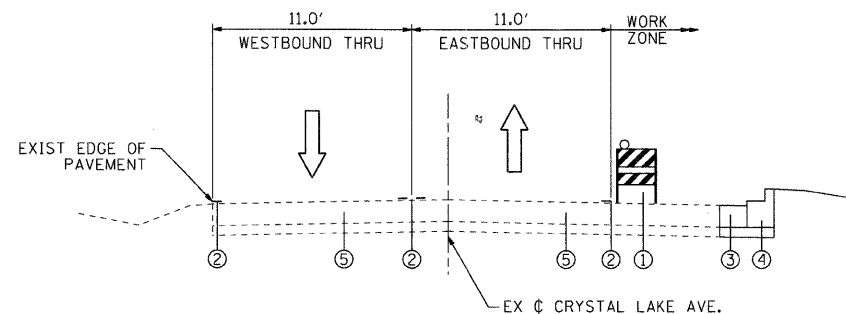
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0116	08-00327-01-CH	MCHENRY	56	10
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63144				

MAINTENANCE OF TRAFFIC GENERAL NOTES

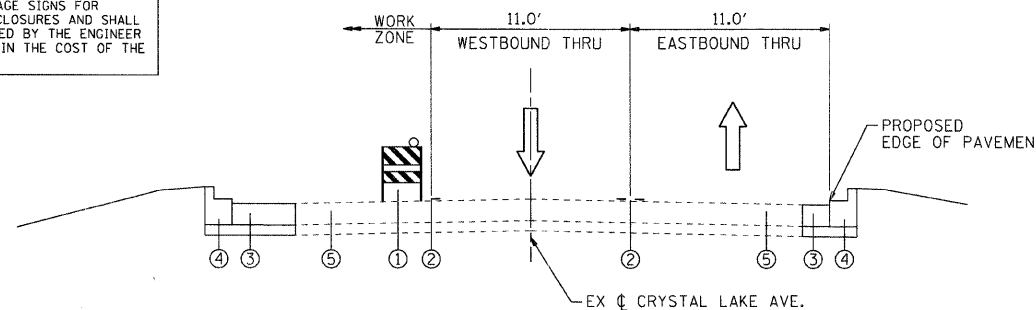
- THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN AND REMOVE ALL SIGNS, BARRICADES, FLAGGERS, PAVEMENT STRIPING AND OTHER TRAFFIC CONTROL DEVICES AS MAY BE NECESSARY FOR THE PURPOSE OF REGULATING, WARNING OR GUIDING TRAFFIC. PROPER PLACEMENT AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND MUST BE IN ACCORDANCE WITH THE APPLICABLE PARTS OF SECTION 701 OF THE STANDARD SPECIFICATIONS, THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS AND THE HIGHWAY STANDARDS. TRAFFIC CONTROL DEPICTED ON THE CONSTRUCTION STAGING PLANS IS THE MINIMUM REQUIREMENT. OTHER WORK OR SIGNING MAY BE REQUIRED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN ONE LANE OF TRAFFIC FLOW AT ALL TIMES DURING CONSTRUCTION ON CRYSTAL LAKE AVENUE. ALLOWABLE HOURS FOR LANE CLOSURES ARE 9:00 AM TO 3:00 PM.
- THE CONTRACTOR SHALL USE PAVEMENT MARKING TAPE TYPE III FOR TEMPORARY LANE MARKINGS ON ALL PERMANENT PAVEMENT. PAINT PAVEMENT MARKINGS MAY BE USED ON ALL SURFACES TO BE REMOVED OR RESURFACED.
- 4" SOLID LINES SHALL BE USED TO DEFINE OUTSIDE LANE LINES DURING MAINTENANCE OF TRAFFIC.
- ACCESS TO PROPERTIES AND SIDE STREETS SHALL BE PROVIDED AND MAINTAINED DURING CONSTRUCTION. ANY ACCESS CLOSURES SHALL BE COORDINATED WITH THE PROPERTY OWNERS AND SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "TEMPORARY ACCESS (PRIVATE ENTRANCE)."
- ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED OR COVERED BY MEANS APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR PER SQUARE FOOT OF "PAVEMENT MARKING REMOVAL".
- TYPE I OR II BARRICADES EQUIPPED WITH ONE-WAY FLASHING LIGHTS WILL BE REQUIRED AT ALL OPEN TRENCHES, EXCAVATIONS, OPEN OR EXPOSED SEWER TRENCHES, AND AT ANY OTHER LOCATIONS DESIGNATED BY THE ENGINEER.
- THE CONTRACTOR SHALL INFORM THE ENGINEER OF ANY STAGE CHANGE AT LEAST TWO WORKING DAYS IN ADVANCE OF THE CHANGE.
- EXISTING TRAFFIC CONTROL DEVICES ARE TO BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. ANY DAMAGE CAUSED BY HIS WORK SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- THE CONTRACTOR WILL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH BARRICADE USED. (TYPE I OR TYPE II (ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.) ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR (4) SANDBAGS PER BARRICADE.
- THE CONTRACTOR WILL BE REQUIRED TO COORDINATE THE MAINTENANCE OF TRAFFIC AND CONSTRUCTION WITH AN ADJACENT PROJECT ON THE WEST LEG CRYSTAL LAKE AVENUE. THE MAINTENANCE OF TRAFFIC WILL BE ADJUSTED AS DIRECTED BY THE ENGINEER TO MATCH THE ADJACENT PROJECT. ANY ADDITIONAL WORK WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- THE CONTRACTOR SHALL BE REQUIRED TO FURNISH UP TO FOUR (4) CHANGEABLE MESSAGE SIGNS FOR THIS PROJECT. THE SIGNS SHALL BE OPERATIONAL TWO WEEKS PRIOR TO ANY LANE CLOSURES AND SHALL BE LOCATED AS DIRECTED BY THE ENGINEER. ANY RELOCATION OF THE SIGNS DIRECTED BY THE ENGINEER DURING CONSTRUCTION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CHANGEABLE MESSAGE SIGN.

SUGGESTED CONSTRUCTION STAGING

- STAGE I - TRAFFIC
- PLACE TEMPORARY CONSTRUCTION SIGNAGE AND BARRICADES NECESSARY FOR STAGE I CONSTRUCTION
 - PLACE TRAFFIC DETOUR SIGNING
 - REMOVE ALL CONFLICTING PAVEMENT MARKINGS
 - SHIFT TRAFFIC AS INDICATED
- STAGE I - CONSTRUCTION
- CRYSTAL LAKE AVENUE - REMOVE EXISTING PAVEMENT AS INDICATED, PERFORM EARTHWORK AS REQUIRED, CONSTRUCT STORM SEWER, CURB AND GUTTER, PAVEMENT WIDENING, SIDEWALK AND DRIVEWAYS.
 - PINGREE ROAD - REMOVE EXISTING PAVEMENT AS INDICATED, PERFORM EARTHWORK AS REQUIRED, CONSTRUCT STORM SEWER, CURB AND GUTTER, PAVEMENT, SIDEWALK, AND DRIVEWAYS.
 - TERRA COTTA ROAD - NO WORK
- STAGE II - TRAFFIC
- PLACE/ADJUST TEMPORARY CONSTRUCTION SIGNAGE AND BARRICADES NECESSARY FOR STAGE II CONSTRUCTION.
 - REMOVE ALL CONFLICTING PAVEMENT MARKINGS.
 - SHIFT TRAFFIC AS INDICATED.
- STAGE II - CONSTRUCTION
- CRYSTAL LAKE AVENUE - REMOVE EXISTING PAVEMENT AS INDICATED, PERFORM EARTHWORK AS REQUIRED, CONSTRUCT STORM SEWER, CURB AND GUTTER, PAVEMENT WIDENING, SIDEWALK AND DRIVEWAYS.
 - TERRA COTTA ROAD - REMOVE EXISTING PAVEMENT AS INDICATED, PERFORM EARTHWORK AS REQUIRED, CONSTRUCT STORM SEWER, CURB AND GUTTER, PAVEMENT, SIDEWALK, AND DRIVEWAYS.
 - PINGREE ROAD - NO WORK
- STAGE III - TRAFFIC
- OPEN ROADWAYS TO TRAFFIC
 - UTILIZE DAILY LANE CLOSURES TO MAINTAIN ONE LANE OF TRAFFIC IN EACH DIRECTION.
- STAGE III - CONSTRUCTION
- RESURFACE PAVEMENT WITH BINDER AND SURFACE COURSES
 - PLACE PERMANENT PAVEMENT MARKINGS AND TRAFFIC SIGNS
 - PERFORM FINAL RESTORATION
 - REMOVE TEMPORARY CONSTRUCTION SIGNS AND BARRICADES



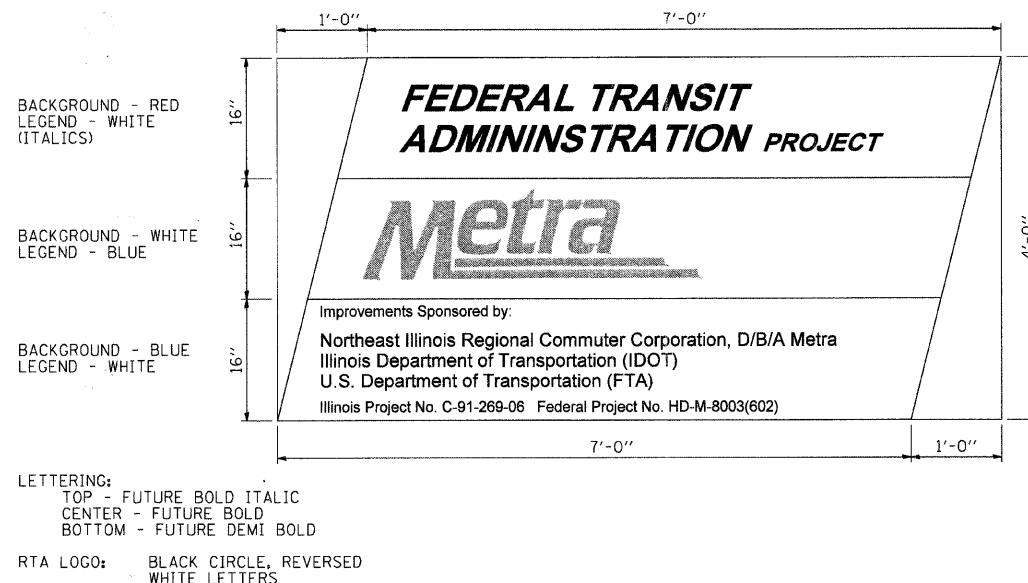
STAGE I - CRYSTAL LAKE AVENUE



STAGE II - CRYSTAL LAKE AVENUE

- LEGEND**
- ① TYPE II BARRICADES, SPACED PFR HWY STD.
 - ② TEMPORARY PAVEMENT MARKING LINE 4
 - ③ PROPOSED PAVEMENT WIDENING
 - ④ PROPOSED COMBINATION CONCRETE CURB AND GUTTER
 - ⑤ EXISTING PAVEMENT
 - ⇨ DIRECTION OF TRAFFIC

METRA SIGN DETAIL



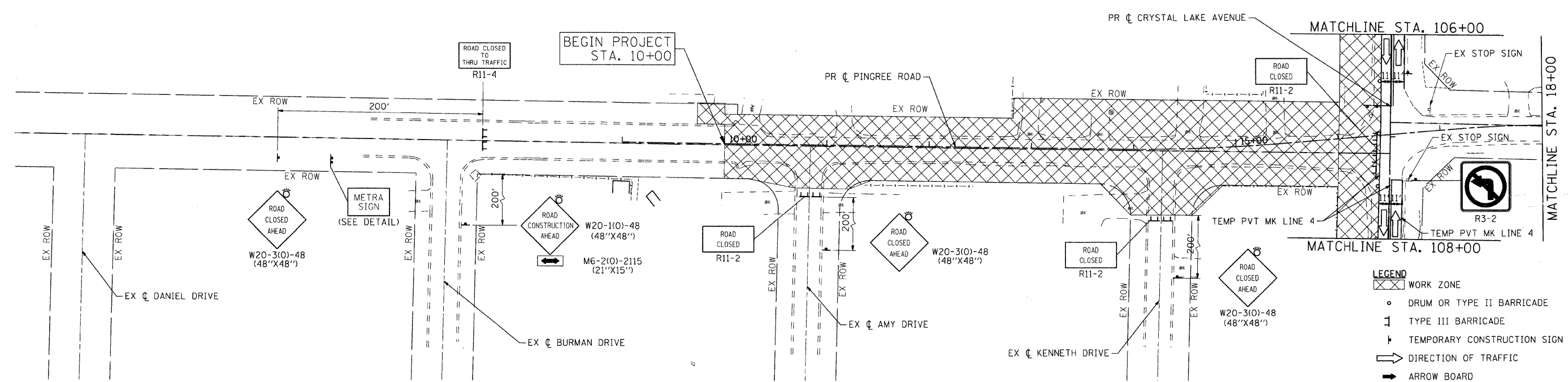
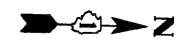
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	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

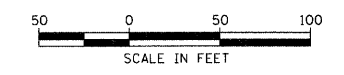
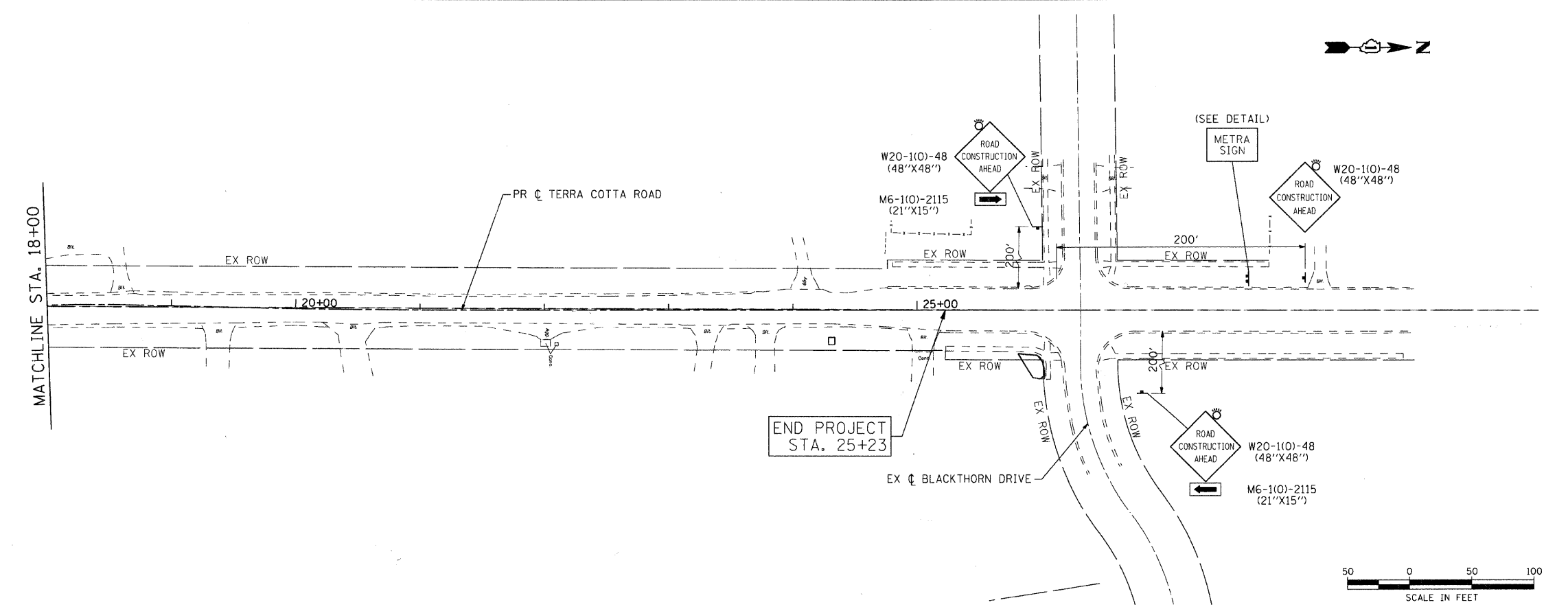
**CONSTRUCTION STAGING NOTES AND
MAINTENANCE OF TRAFFIC DETAILS**

SCALE: N.T.S. SHEET NO. 1 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	11
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	



- LEGEND**
- WORK ZONE
 - DRUM OR TYPE II BARRICADE
 - TYPE III BARRICADE
 - TEMPORARY CONSTRUCTION SIGN
 - DIRECTION OF TRAFFIC
 - ARROW BOARD



COMPANY NAME: Smith
 PROJECT CONTACT: J. MELRUSH
 DATE PLOTTED: 2/13/2009 15:21:44
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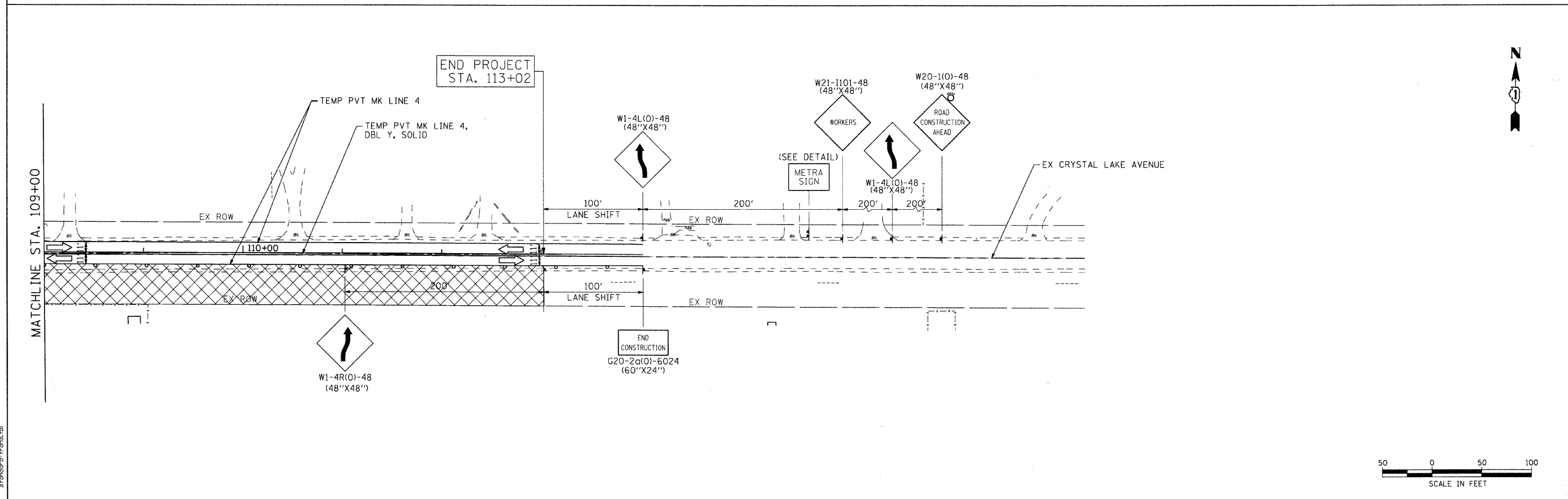
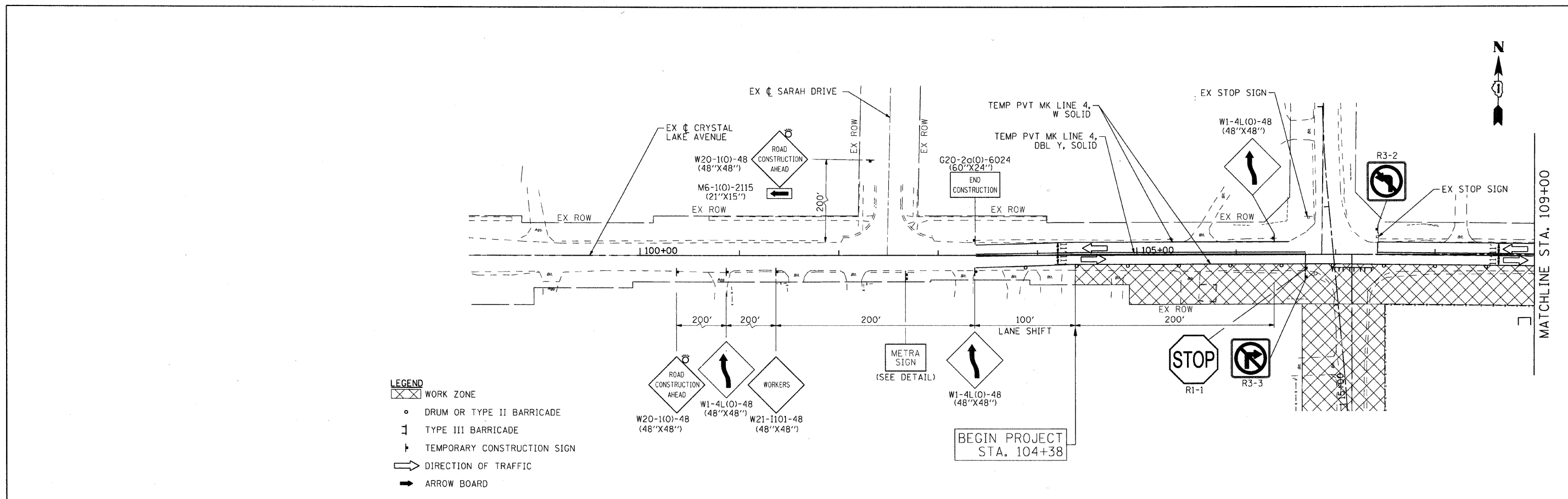
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	DATE - 2/13/09	REVISED

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN
 PINGREE ROAD AND TERRA COTTA ROAD - STAGE I**

SCALE: 1" = 50' SHEET NO. 2 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	12
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
		CONTRACT NO. 63144		



COMPANY NAME: SMITH
 PROJECT CONTACT: J. MELROUSE
 CLIENT: MCHENRY COUNTY TOWNSHIP
 DRAWN BY: JPA
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	DATE - 2/13/09	REVISED

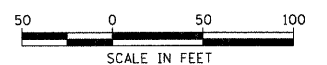
**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

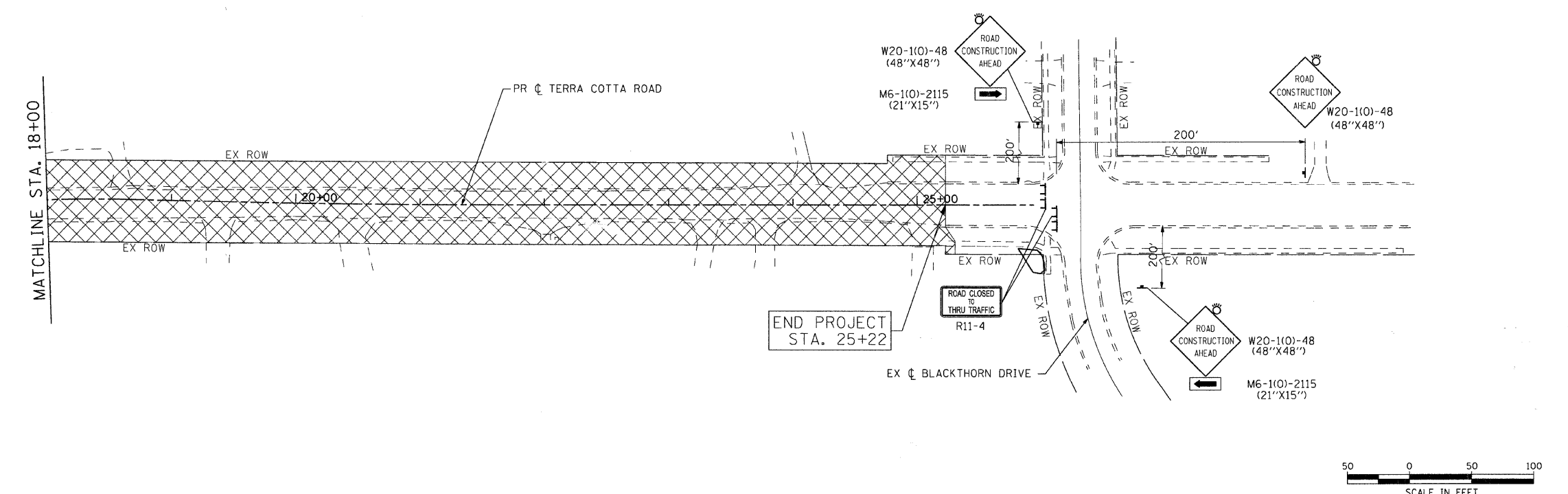
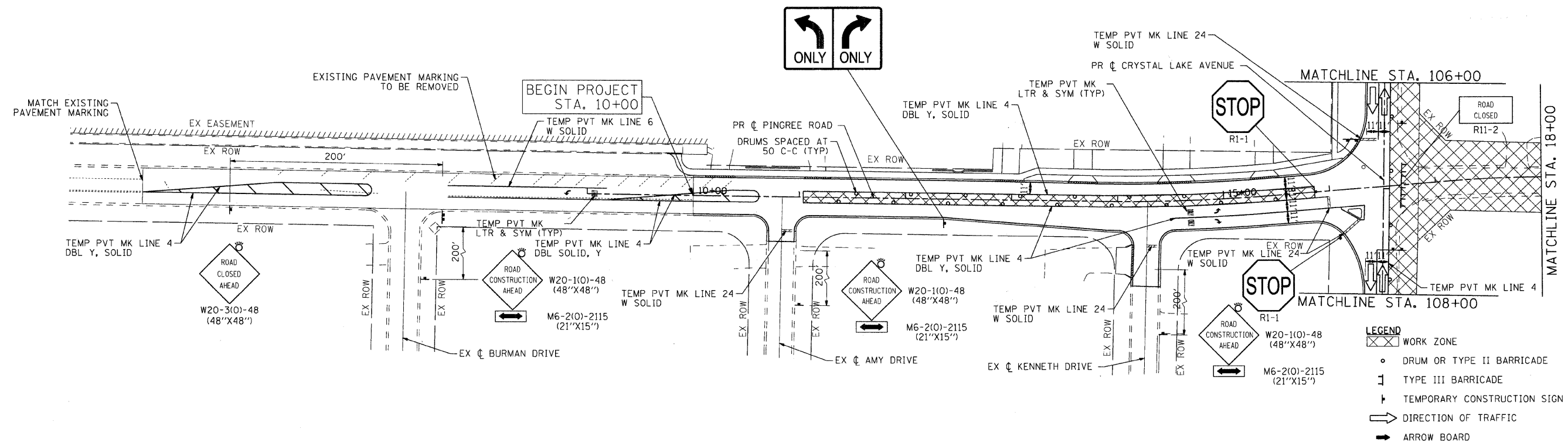
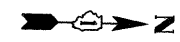
**MAINTENANCE OF TRAFFIC PLAN
 CRYSTAL LAKE AVE. - STAGE I**

SCALE: 1" = 50' SHEET NO. 3 OF 5 SHEETS STA. TO STA.

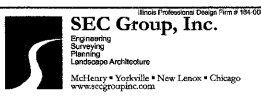
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0116	08-00327-01-CH	MCHENRY	56	13
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

CONTRACT NO. 63144





COMPANY NAME: **SEC Group, Inc.**
 PROJECT CONTACT: J. MELMISH
 DATE PLOTTED: 2/13/2009 11:31:46 AM
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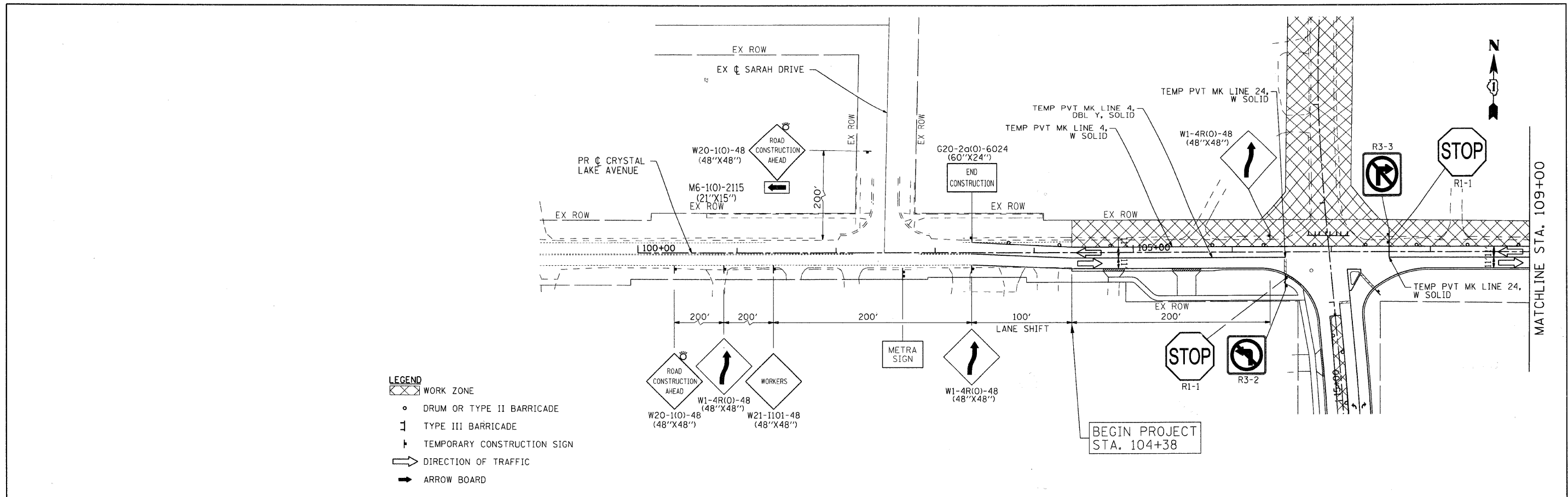
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MCHENRY COUNTY
DIVISION OF TRANSPORTATION

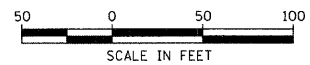
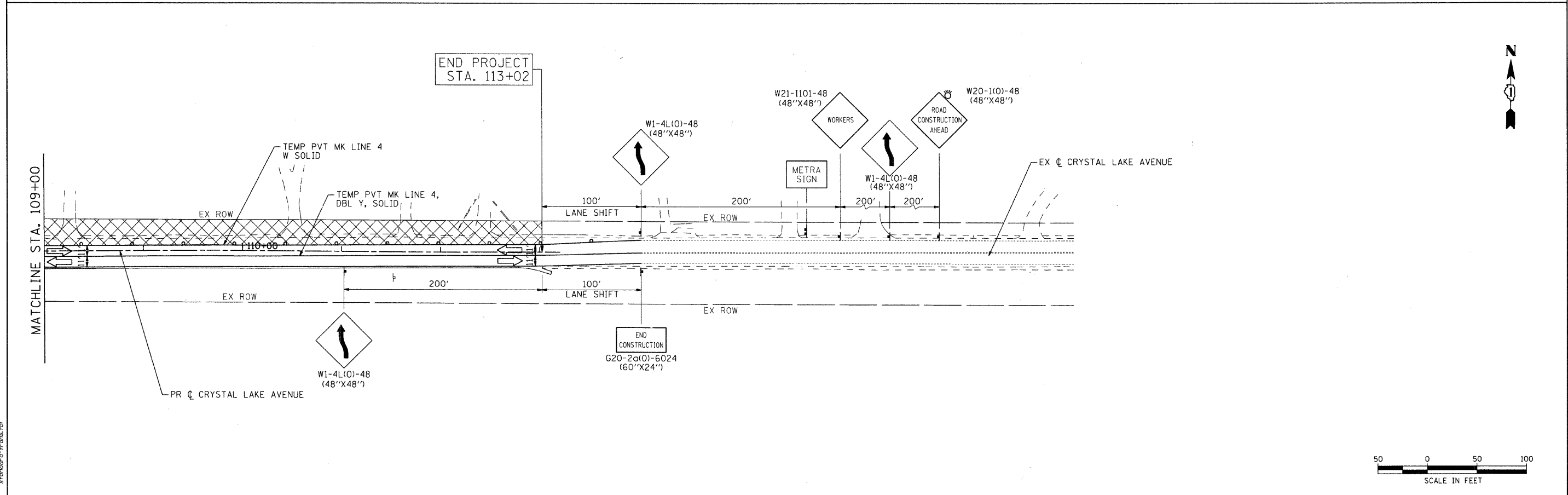
MAINTENANCE OF TRAFFIC PLAN
PINGREE ROAD AND TERRA COTTA ROAD - STAGE II

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 14
SCALE: 1" = 50'		SHEET NO. 4 OF 5 SHEETS		STA. TO STA.
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 63144



- LEGEND**
- WORK ZONE
 - DRUM OR TYPE II BARRICADE
 - TYPE III BARRICADE
 - TEMPORARY CONSTRUCTION SIGN
 - DIRECTION OF TRAFFIC
 - ARROW BOARD



COMPANY NAME: Smith
 PROJECT CONTACT: J. MELHUSH
 DATE PLOTTED: 2/13/2009 11:23:00 AM
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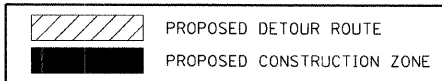
**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN
 CRYSTAL LAKE AVE. - STAGE II**

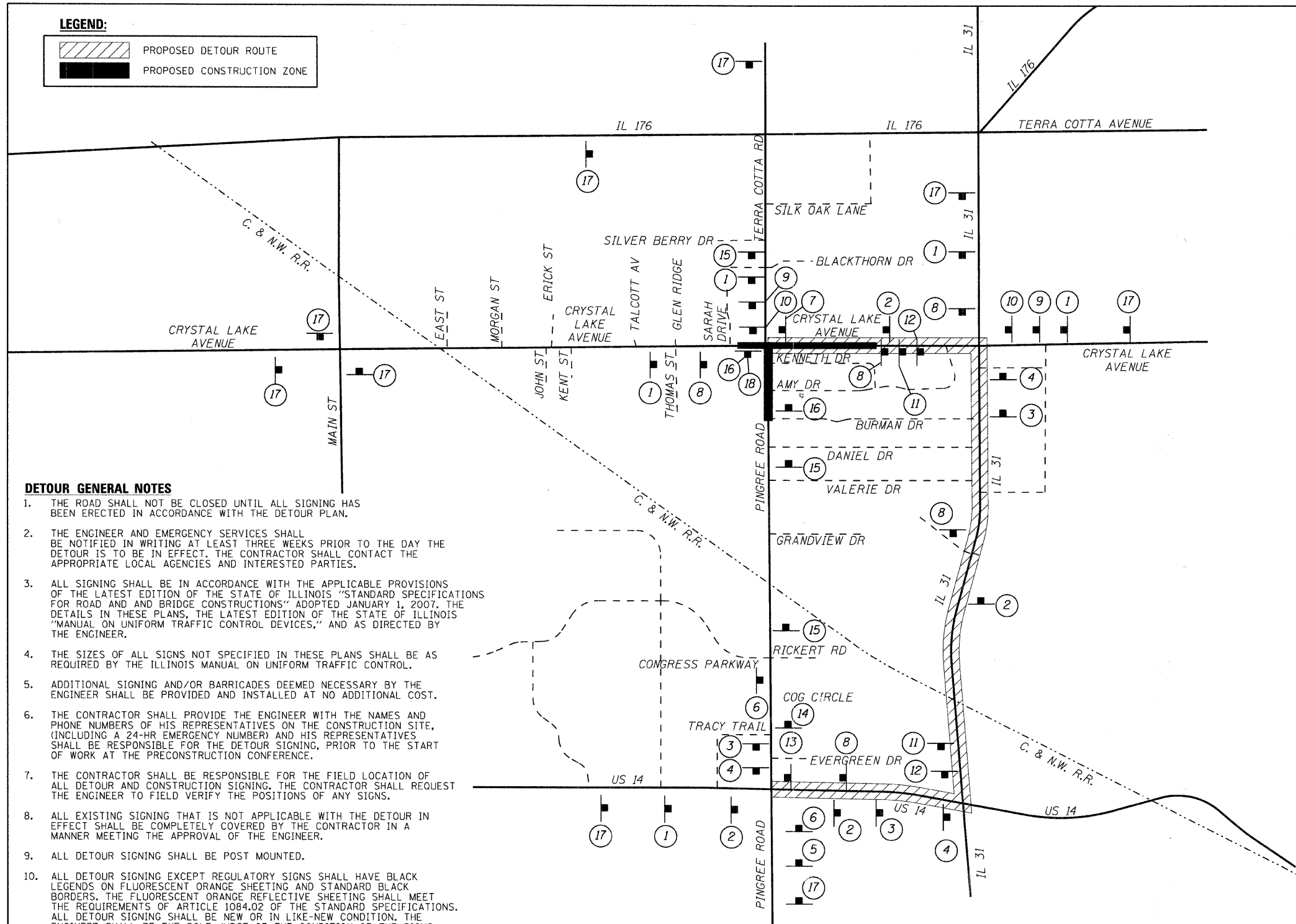
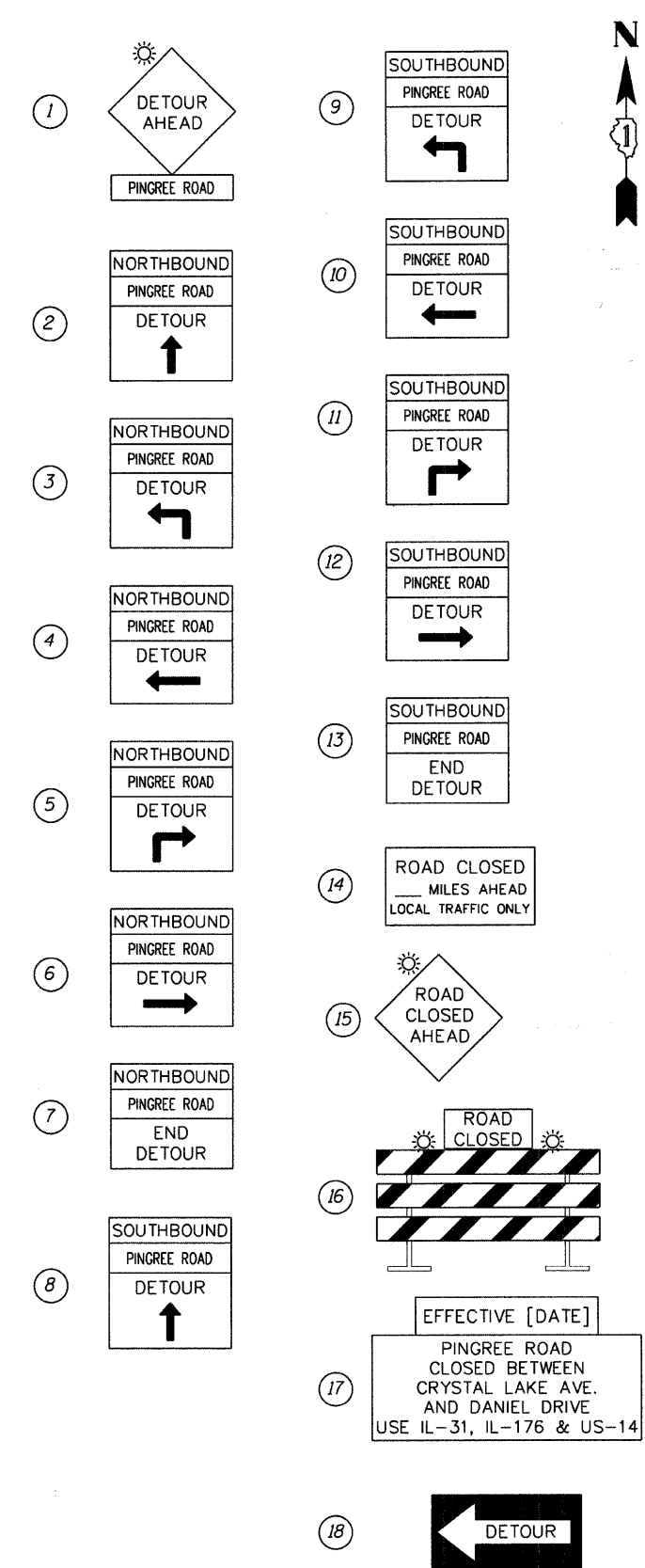
SCALE: 1" = 50' SHEET NO. 5 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	15
			CONTRACT NO. 63144	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

LEGEND:



SIGN LEGEND:



DETOUR GENERAL NOTES

1. THE ROAD SHALL NOT BE CLOSED UNTIL ALL SIGNING HAS BEEN ERECTED IN ACCORDANCE WITH THE DETOUR PLAN.
2. THE ENGINEER AND EMERGENCY SERVICES SHALL BE NOTIFIED IN WRITING AT LEAST THREE WEEKS PRIOR TO THE DAY THE DETOUR IS TO BE IN EFFECT. THE CONTRACTOR SHALL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
3. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE LATEST EDITION OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND AND BRIDGE CONSTRUCTIONS" ADOPTED JANUARY 1, 2007. THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE STATE OF ILLINOIS "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES," AND AS DIRECTED BY THE ENGINEER.
4. THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS SHALL BE AS REQUIRED BY THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL.
5. ADDITIONAL SIGNING AND/OR BARRICADES DEEMED NECESSARY BY THE ENGINEER SHALL BE PROVIDED AND INSTALLED AT NO ADDITIONAL COST.
6. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE NAMES AND PHONE NUMBERS OF HIS REPRESENTATIVES ON THE CONSTRUCTION SITE, (INCLUDING A 24-HR EMERGENCY NUMBER) AND HIS REPRESENTATIVES SHALL BE RESPONSIBLE FOR THE DETOUR SIGNING, PRIOR TO THE START OF WORK AT THE PRECONSTRUCTION CONFERENCE.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FIELD LOCATION OF ALL DETOUR AND CONSTRUCTION SIGNING. THE CONTRACTOR SHALL REQUEST THE ENGINEER TO FIELD VERIFY THE POSITIONS OF ANY SIGNS.
8. ALL EXISTING SIGNING THAT IS NOT APPLICABLE WITH THE DETOUR IN EFFECT SHALL BE COMPLETELY COVERED BY THE CONTRACTOR IN A MANNER MEETING THE APPROVAL OF THE ENGINEER.
9. ALL DETOUR SIGNING SHALL BE POST MOUNTED.
10. ALL DETOUR SIGNING EXCEPT REGULATORY SIGNS SHALL HAVE BLACK LEGENDS ON FLUORESCENT ORANGE SHEETING AND STANDARD BLACK BORDERS. THE FLUORESCENT ORANGE REFLECTIVE SHEETING SHALL MEET THE REQUIREMENTS OF ARTICLE 1084.02 OF THE STANDARD SPECIFICATIONS. ALL DETOUR SIGNING SHALL BE NEW OR IN LIKE-NEW CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE CONDITION OF THE SIGNS.
11. THE ROAD NAME SIGN SHALL BE A BLACK LEGEND ON ORANGE REFLECTIVE SHEETING. THE SIGN BLANK SHALL BE VARIABLE WITH DESIGN SERIES C LETTERS. THE CAPITAL LETTERS SHALL BE 6".
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS, AND OTHER DEVICES INSTALLED BY HIM ARE IN PLACE AND OPERATING 24 HOURS EACH DAY, INCLUDING SUNDAYS AND HOLIDAYS
13. THE "ROAD CLOSED" (R11-2) AND (R11-4) SIGNS SHALL BE MOUNTED ABOVE THE TOPS OF THE TYPE III BARRICADES. ALL TYPE III BARRICADES SHALL HAVE TWO AMBER TYPE A-LOW INTENSITY FLASHING LIGHTS SPACED NEAR THE CENTERLINES OF THE SUPPORTS.

DETOUR GENERAL NOTES (CONT.)

14. THE TYPE III BARRICADES USED AT POINTS OF CLOSURE TO THRU TRAFFIC ONLY SHALL NOT EXCEED 8 FEET IN WIDTH EACH FOR A SINGLE APPROACH LANE. ALL BARRICADES AT THESE LOCATIONS SHALL HAVE REFLECTORIZED STRIPING ON THE BACK SIDES OF THE BARRICADES.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE VISIBILITY OF ALL DETOUR AND CONSTRUCTION SIGNS, INCLUDING BRUSHING BACK VEGETATION IF DEEMED NECESSARY BY THE ENGINEER.
16. THE ENGINEER SHALL BE NOTIFIED AT LEAST 24 HOURS BEFORE THE ROAD IS TO BE REOPENED TO TRAFFIC. THE ENGINEER WILL CONTACT THE APPROPRIATE LOCAL AGENCIES AND INTERESTED PARTIES.
17. ALL WORK REQUIRED FOR DETOUR SIGNING SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR LUMP SUM AS "TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR."



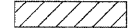

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**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**


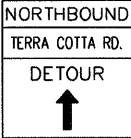
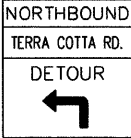
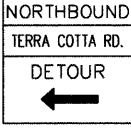
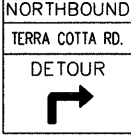
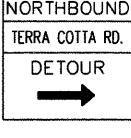
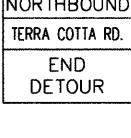
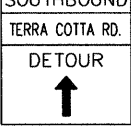
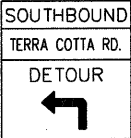
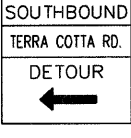
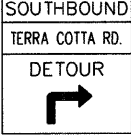
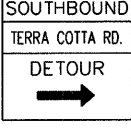
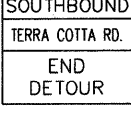
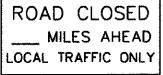


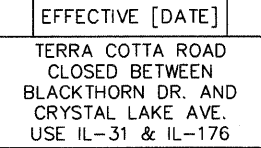

DETOUR PLAN - STAGE 1 PINGREE ROAD	
SCALE: 1" = 700'	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

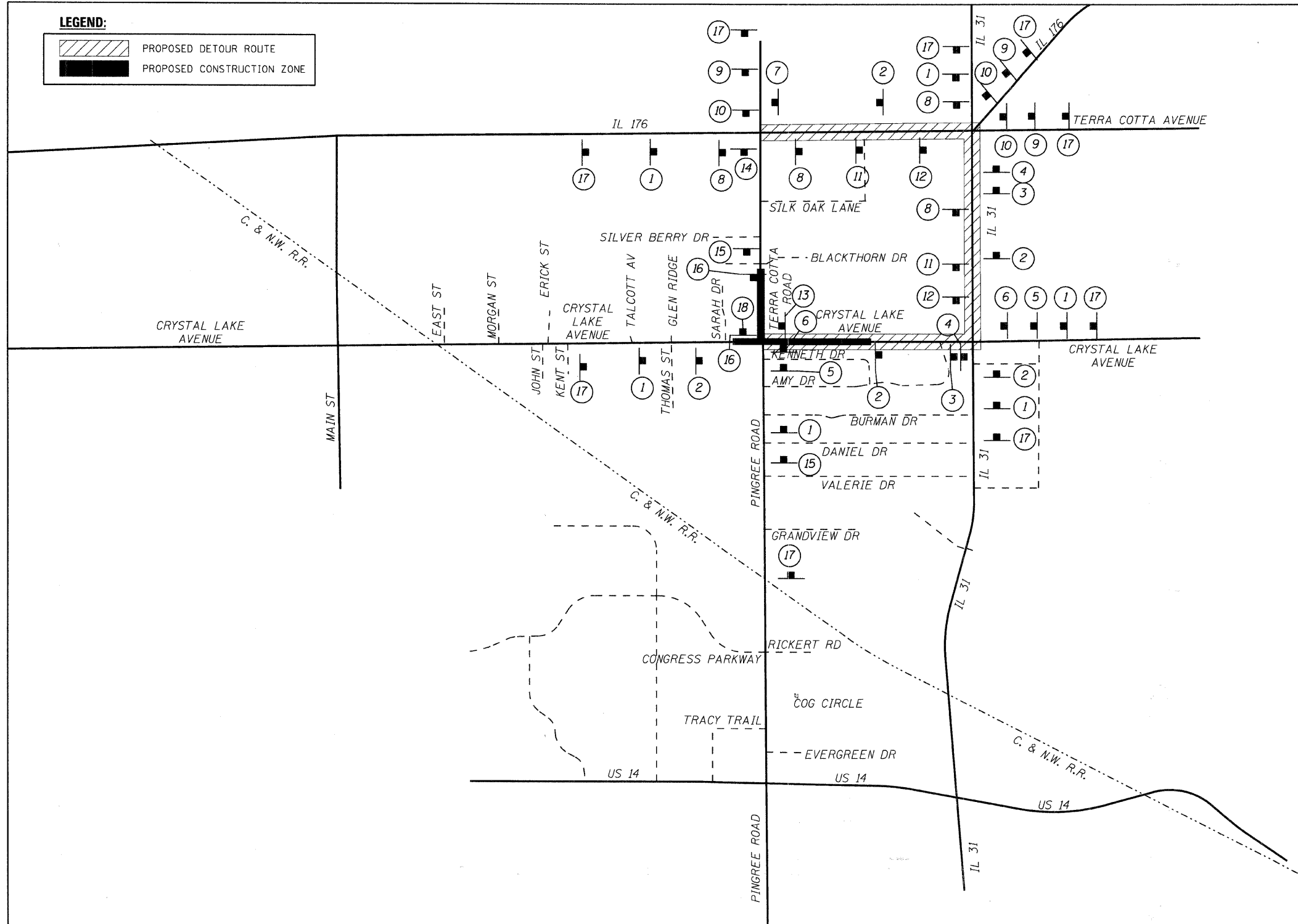
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0116	08-00327-01-CH	MCHENRY	56	14
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	

LEGEND:

-  PROPOSED DETOUR ROUTE
-  PROPOSED CONSTRUCTION ZONE

SIGN LEGEND:

- 1  DETOUR AHEAD
TERRA COTTA RD.
- 2  NORTHBOUND
TERRA COTTA RD.
DETOUR
↑
- 3  NORTHBOUND
TERRA COTTA RD.
DETOUR
↶
- 4  NORTHBOUND
TERRA COTTA RD.
DETOUR
↶
- 5  NORTHBOUND
TERRA COTTA RD.
DETOUR
↷
- 6  NORTHBOUND
TERRA COTTA RD.
DETOUR
↷
- 7  NORTHBOUND
TERRA COTTA RD.
END
DETOUR
- 8  SOUTHBOUND
TERRA COTTA RD.
DETOUR
↑
- 9  SOUTHBOUND
TERRA COTTA RD.
DETOUR
↶
- 10  SOUTHBOUND
TERRA COTTA RD.
DETOUR
↶
- 11  SOUTHBOUND
TERRA COTTA RD.
DETOUR
↷
- 12  SOUTHBOUND
TERRA COTTA RD.
DETOUR
↷
- 13  SOUTHBOUND
TERRA COTTA RD.
END
DETOUR
- 14  ROAD CLOSED
MILES AHEAD
LOCAL TRAFFIC ONLY
- 15  ROAD CLOSED
AHEAD
- 16  ROAD CLOSED
END
ROAD CLOSED
- 17  EFFECTIVE [DATE]
TERRA COTTA ROAD
CLOSED BETWEEN
BLACKTHORN DR. AND
CRYSTAL LAKE AVE.
USE IL-31 & IL-176
- 18  DETOUR



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 Engineering
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USER NAME = char-ktk	DESIGNED - JRM	REVISED -
PLOT SCALE = 1" = 700'	DRAWN - JPA	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**DETOUR PLAN - STAGE 2
 TERRA COTTA ROAD**

SCALE: 1" = 700' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	17
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63144				

080154
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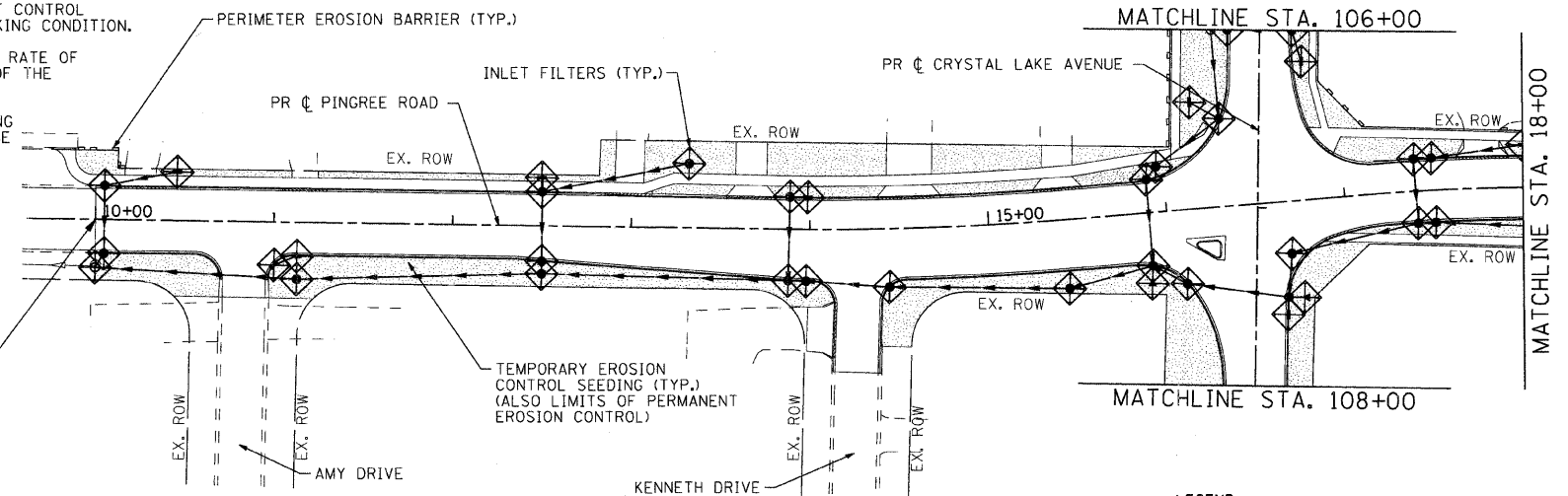
EROSION CONTROL GENERAL NOTES

- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
- ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
- ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION-SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.

EROSION CONTROL GENERAL NOTES (CONT.)

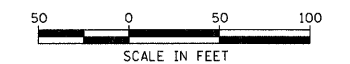
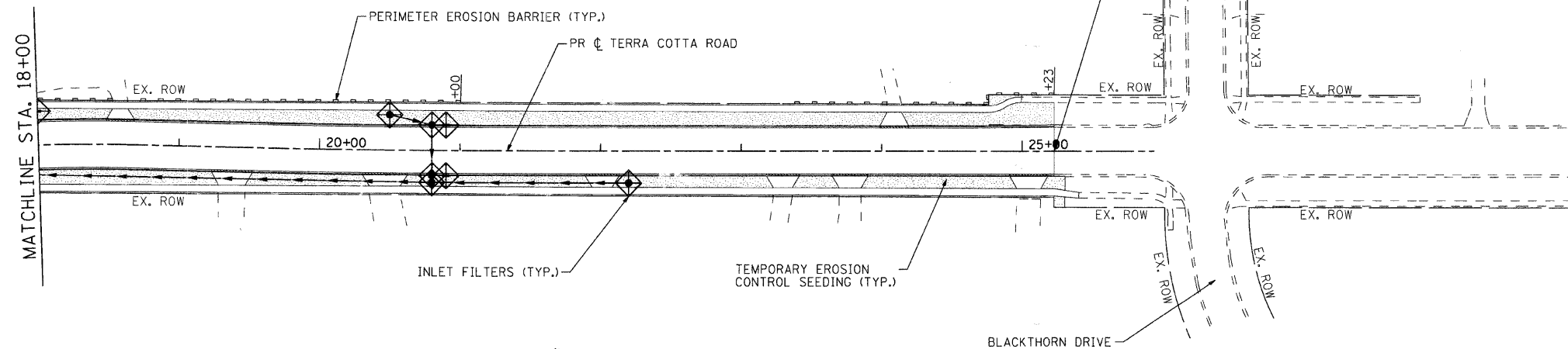
- THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO AND DEVELOPMENT SITE, CHANNEL, WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
- ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
- TEMPORARY EROSION CONTROL SEEDING APPLIED AT THE RATE OF 100 POUNDS PER ACRE AS STATED IN ARTICLE 280.04 OF THE STANDARD SPECIFICATIONS.
- PERMANENT EROSION CONTROL SHALL INCLUDE FURNISHING AND PLACING TOPSOIL, 6" AND SEEDING CLASS 2A TO BE PROVIDED IN ACCORDANCE WITH ARTICLE 250 OF THE STANDARD SPECIFICATIONS.
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE STORM WATER POLLUTION PREVENTION PLAN FOR THIS PROJECT.

BEGIN PROJECT STA. 10+00



- LEGEND**
- TEMPORARY EROSION CONTROL SEEDING (ACRES)
 - INLET FILTERS (EA)
 - PERIMETER EROSION BARRIER (FT)

END PROJECT STA. 25+23



COMPANY NAME: J. MELNIK
 PROJECT CONTACT: J. MELNIK
 DATE PLOTTED: 2/13/2009 11:53:33 AM
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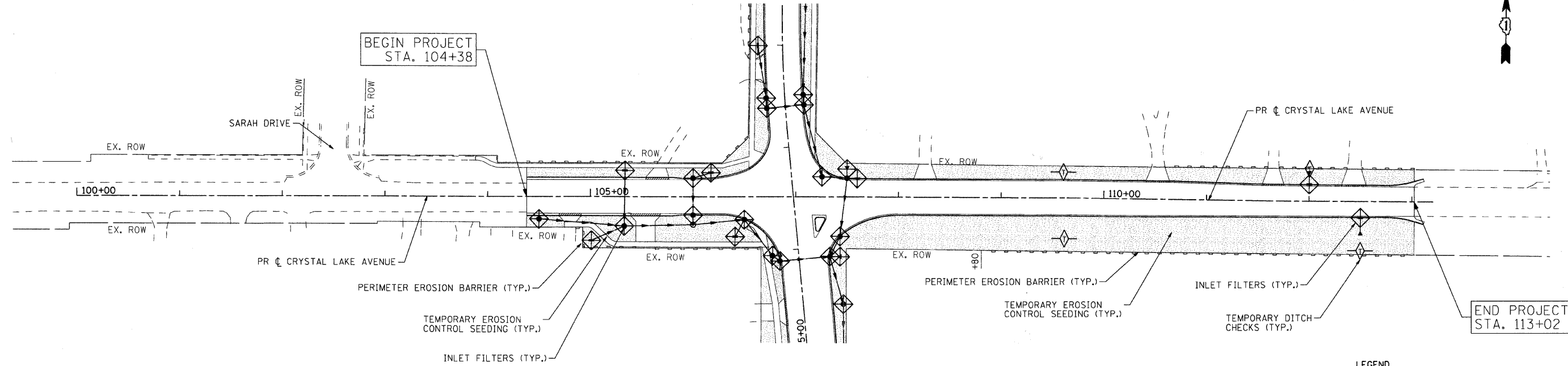


USER NAME = char-ke	DESIGNED - JJS	REVISED
PLOT SCALE = 1" = 50'	DRAWN - JJS	REVISED
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED
	DATE - 2/13/09	REVISED

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

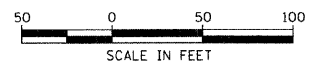
**EROSION CONTROL PLAN
 PINGREE ROAD AND TERRA COTTA ROAD**

SCALE: 1" = 50'	SHEET NO. 1 OF 2 SHEETS	STA. BEGIN TO STA. END	F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 18
			FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT CONTRACT NO. 63144		



LEGEND

	TEMPORARY EROSION CONTROL SEEDING (ACRE)
	INLET FILTERS (EA)
	PERIMETER EROSION BARRIER (FT)



COMPANY NAME: J. McHenry
 PROJECT CONTACT: J. McHenry
 CLIENT: MCHENRY COUNTY
 DATE: 2/13/2009 11:28:48 AM
 FILE NAME: 080154-08-02.dwg
 PLOT DRIVER: pcf-greyscale_raster.plt
 PEN TABLE: standard-trans.tbl

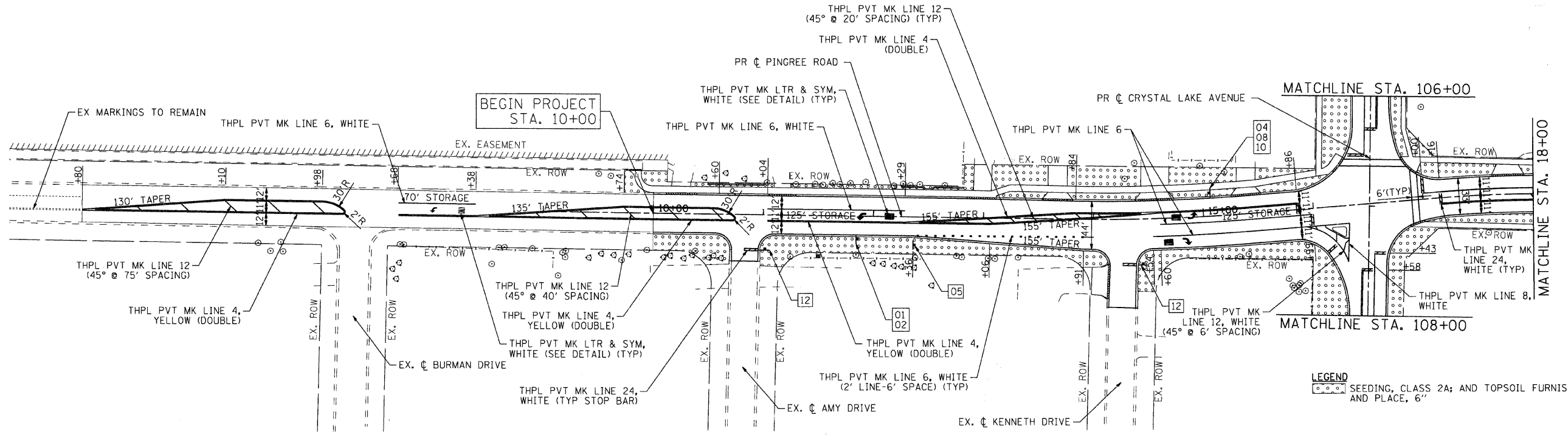
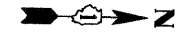


USER NAME = char-tke	DESIGNED - JJS	REVISED
PLOT SCALE = 1" = 50'	DRAWN - JJS	REVISED
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED
	DATE - 2/13/09	REVISED

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

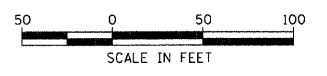
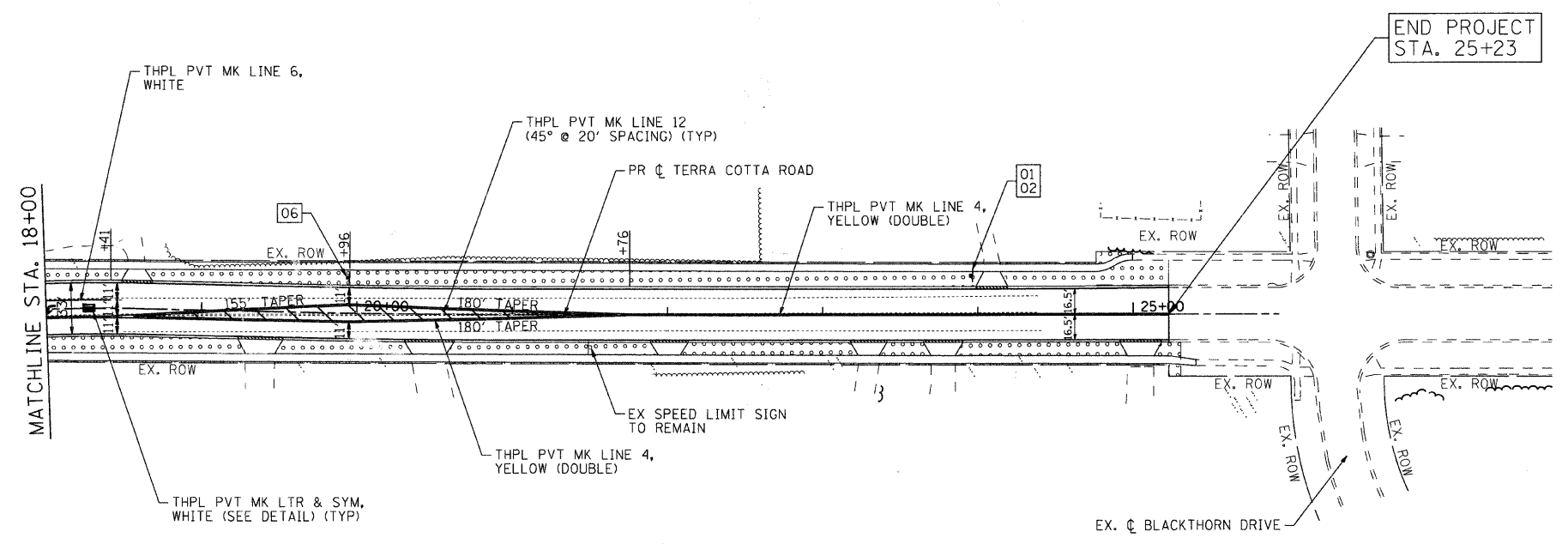
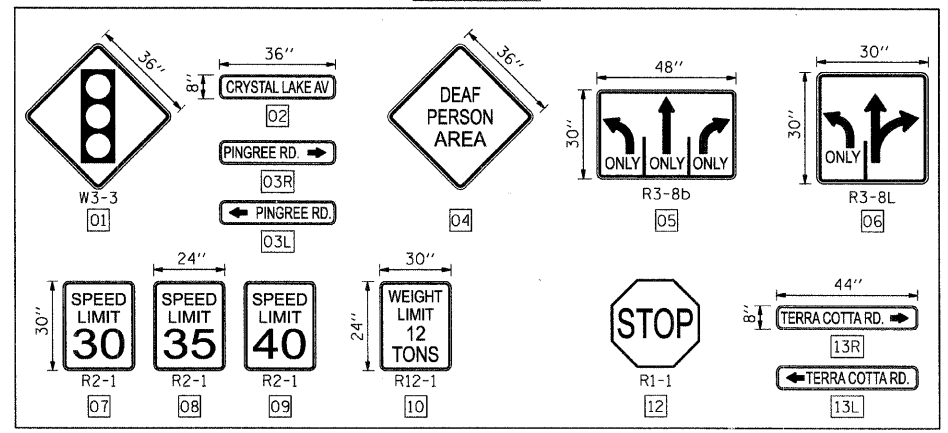
EROSION CONTROL PLAN CRYSTAL LAKE AVENUE	
SCALE: 1" = 50'	SHEET NO. 2 OF 2 SHEETS
STA. BEGIN	TO STA. END

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	19
CONTRACT NO. 63144				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



LEGEND
 SEEDING, CLASS 2A; AND TOPSOIL FURNISH AND PLACE, 6"

SIGN LEGEND



COMPANY NAME: Smith
 PROJECT CONTACT: J. MELHUSH
 DATE PLOTTED: 2/13/2009 11:51:01 AM
 FILE NAME: 080156-pm-01.dgn
 PLOT DRIVER: pdf2epscale_raster-plot
 PEN TABLE: standard-trans.tbl



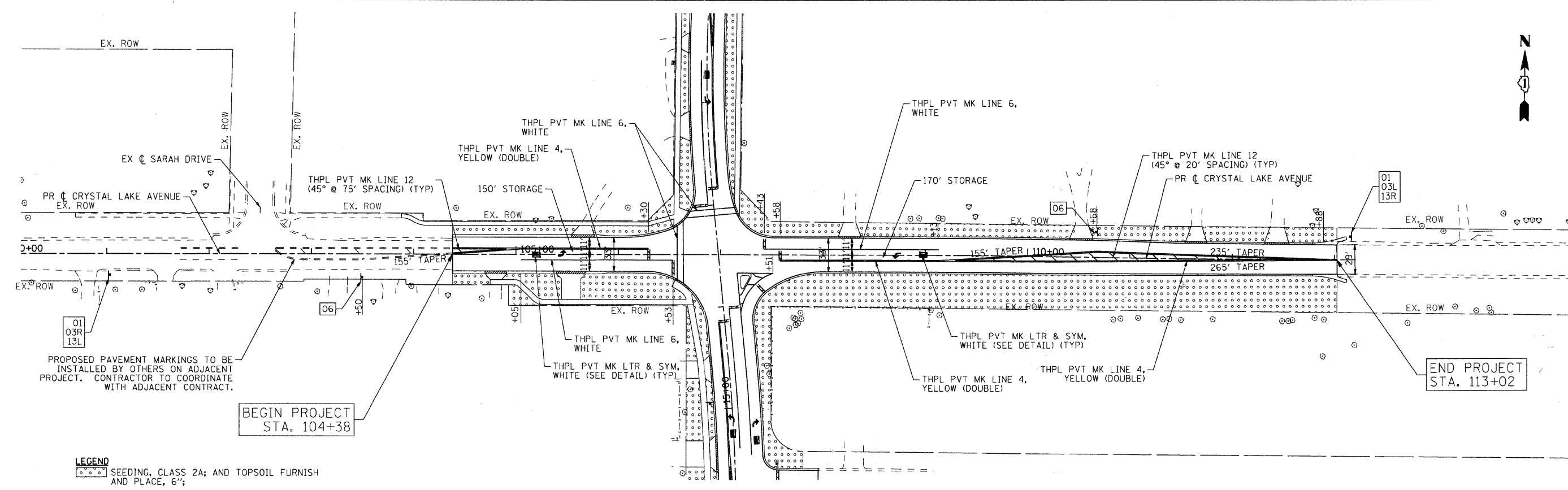
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PLOT SCALE = 1" = 50'	DRAWN - JPA	REVISED
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED
	DATE - 2/13/09	REVISED

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

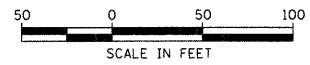
**PAVEMENT MARKING AND SIGNING PLAN
 PINGREE ROAD AND TERRA COTTA ROAD**

SCALE: 1" = 50' SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	20
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	



LEGEND
 ○○○○ SEEDING, CLASS 2A; AND TOPSOIL FURNISH AND PLACE, 6";



COMPANY NAME: J. McINISH
 PROJECT CONTACT: J. McINISH
 CLIENT: MCHENRY COUNTY
 FILE NAME: 080154-08-02.dgn
 PLOT DRIVER: pdf-grayscale-aster.dlt
 PLOT TABLE: standard-trans.tbl



USER NAME = char-cke	DESIGNED - JRM	REVISED
PLOT SCALE = 1" = 50'	DRAWN - JPA	REVISED
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED
	DATE - 2/13/09	REVISED

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**PAVEMENT MARKING AND SIGNING PLAN
 CRYSTAL LAKE AVENUE**

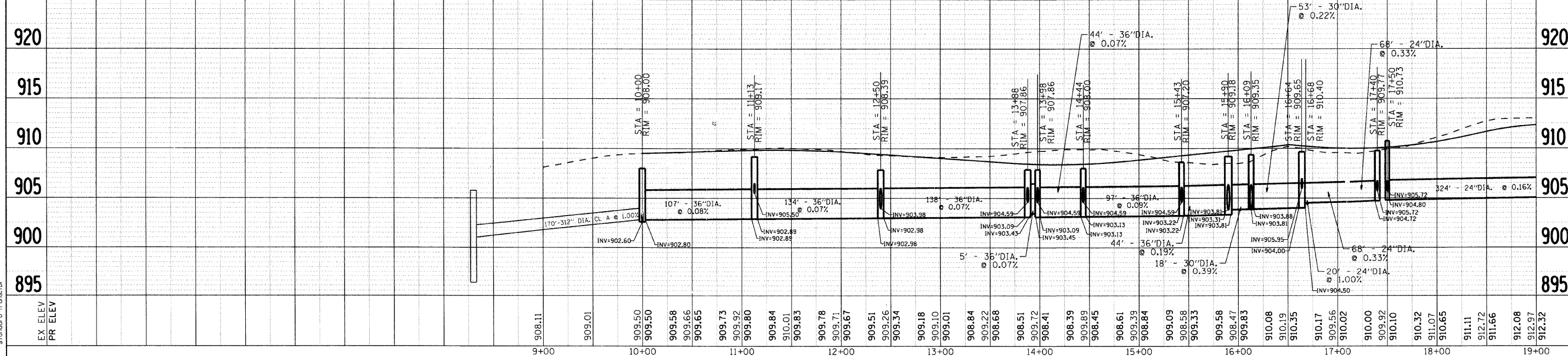
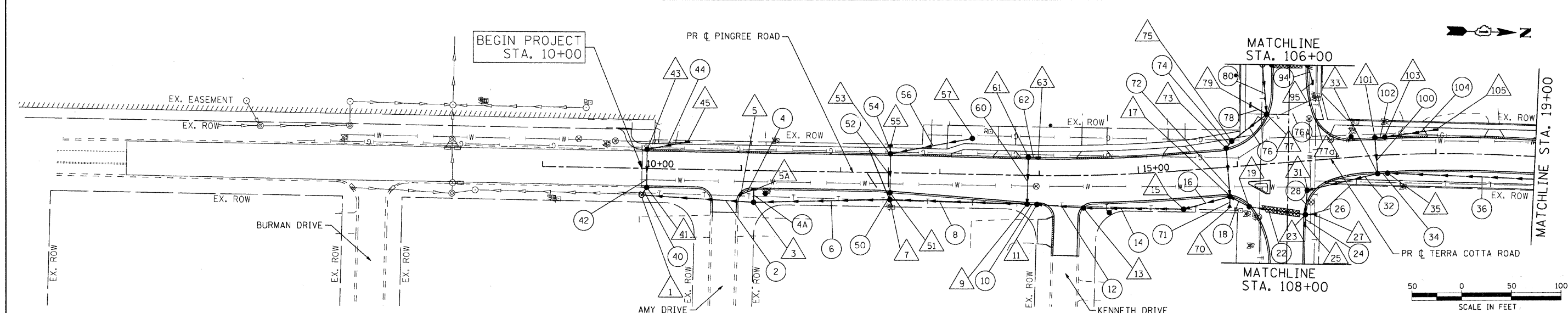
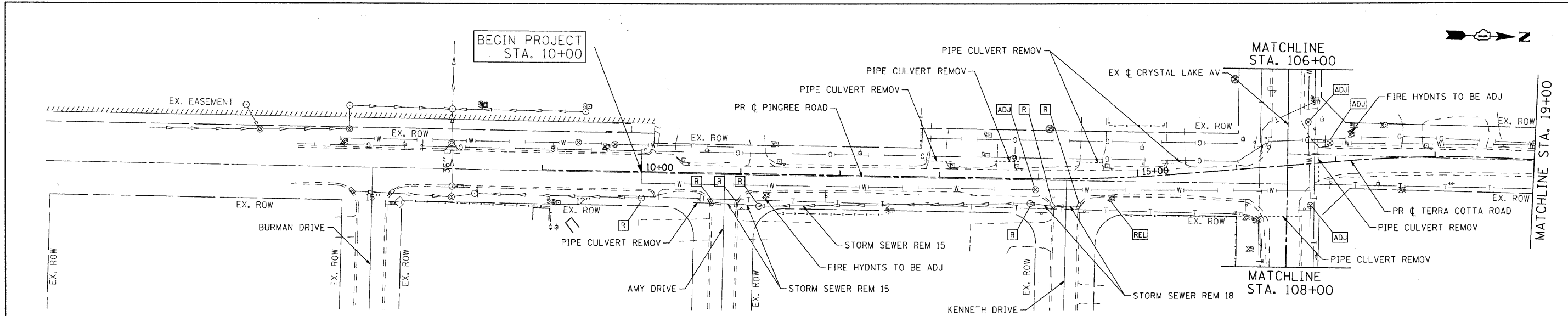
SCALE: 1" = 50' SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	21
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	

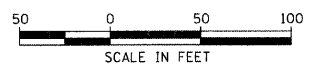
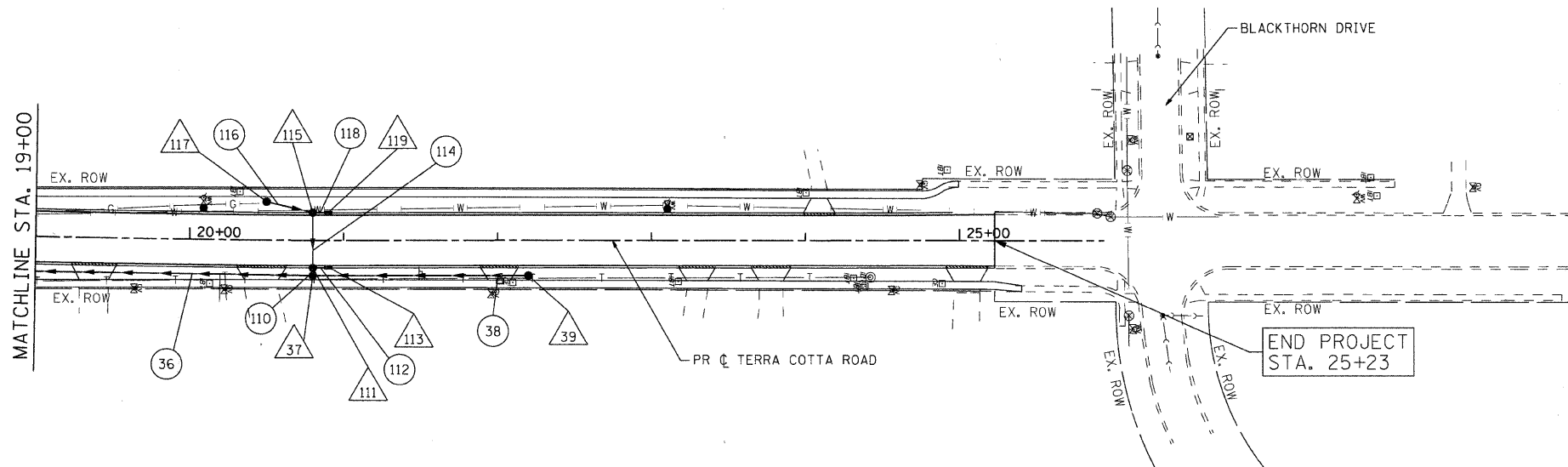
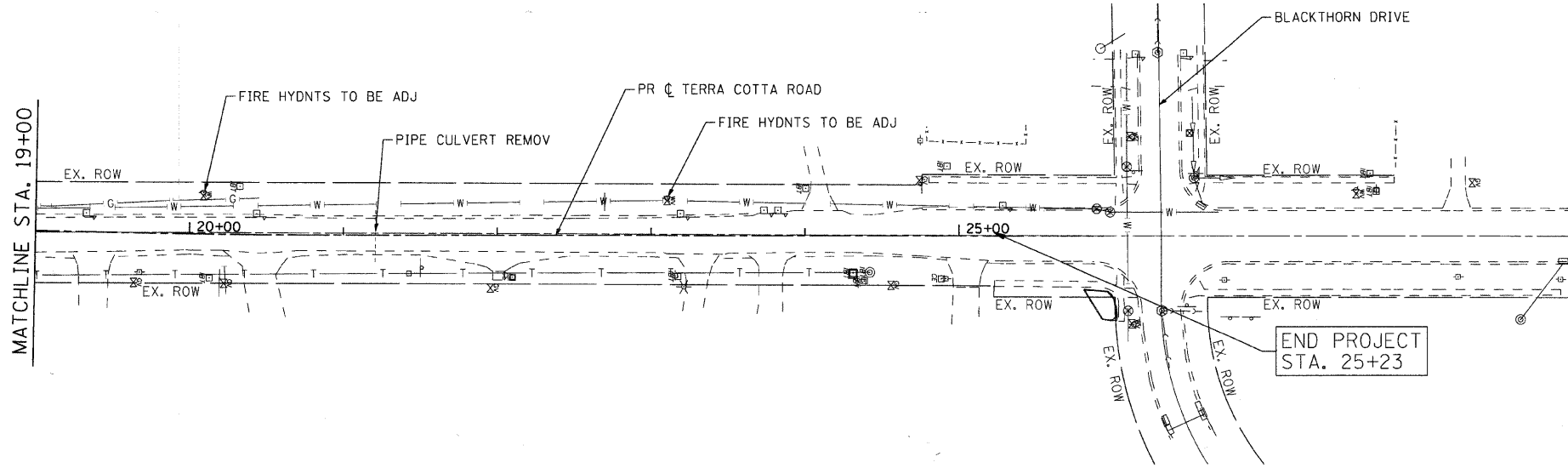
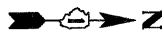
DATE	BY
REVISION	
PLANNING	
DESIGN	
CHECKED	
DRAWN	
IN CHARGE	

DATE	BY
REVISION	
PROFILING	
EX. ROW	

COMPANY NAME: **SEC Group, Inc.**
 PROJECT CONTACT: **J. MELRIUSH**
 CLIENT: **ALCONGWIN AND MUNGA TOWNSHIP**
 DATE PLOTTED: **2/13/2009 10:32:28 AM**
 FILE NAME: **080154-dpp-01.dgn**
 PEN TABLE: **Standard Trans.tbl**



MCHENRY COUNTY DIVISION OF TRANSPORTATION				DRAINAGE AND UTILITY PLAN PINGREE ROAD				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
SCALE: 1" = 50'				SHEET NO. 1 OF 5 SHEETS				STA. BEGIN TO STA. 19+00.00		0116	08-00327-01-CH	MCHENRY	56 22
									CONTRACT NO. 63144			ILLINOIS FED. AID PROJECT	



PLAN	DATE
BY	
REVISIONS	
NO.	
DESCRIPTION	
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PROFILE	DATE
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REVISIONS	
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DESCRIPTION	
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REVISIONS	
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DESCRIPTION	
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Smith
 COMPANY NAME: J. MELUJISH
 PROJECT CONTACT: ALGOMQUIN AND MANITA TOWNSHIP
 CLIENT: 2/13/2009 11:35:43 AM
 DATE PLOTTED: 080154-dpp-02.dgn
 FILE NUMBER: s:\roads-francs.tbl
 PEN TABLE:



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FILE NAME	= 080154-dpp-02.dgn	DRAWN	- JJS	REVISED	-
PLOT SCALE	= 1" = 50'	CHECKED	- TCH	REVISED	-
PLOT DATE	= 2/13/2009	DATE	- 2/13/09	REVISED	-

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

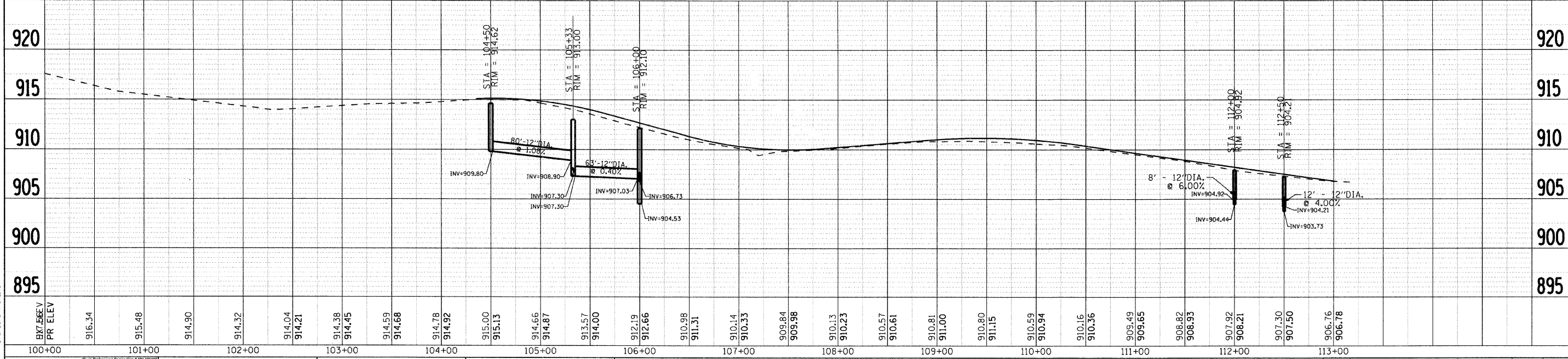
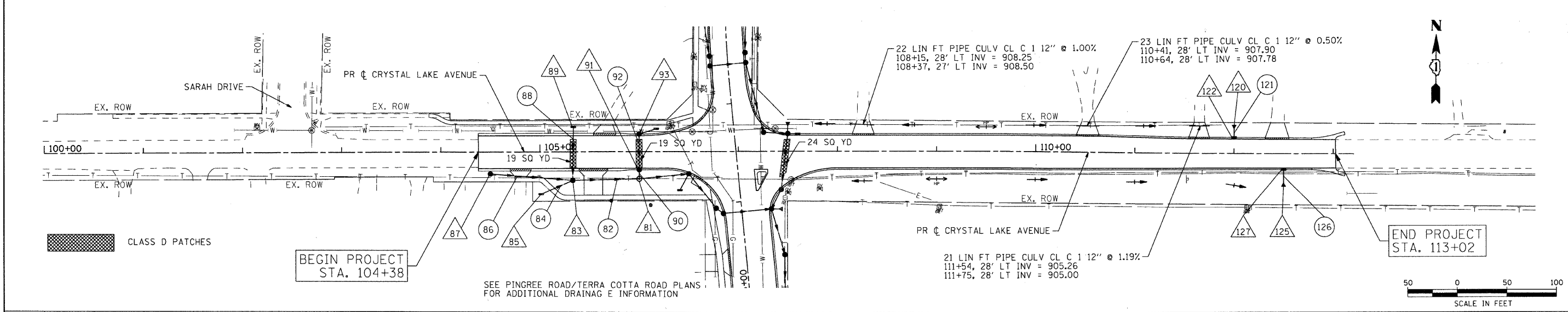
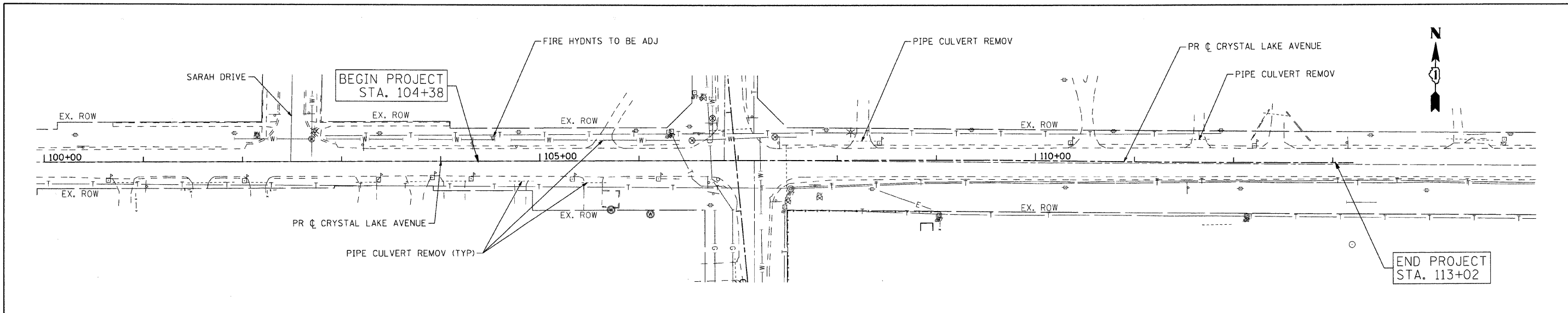
**DRAINAGE AND UTILITY PLAN
 TERRA COTTA ROAD**
 SCALE: 1" = 50' SHEET NO. 2 OF 5 SHEETS STA. 19+00.00 TO STA. END

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	23
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	

DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	
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REVISIONS	
NO.	
DESCRIPTION	
DATE	
BY	
REVISIONS	
NO.	
DESCRIPTION	

COMPANY NAME: J. MELNIISH
 PROJECT CONTACT: ALGOMQUIN AND WANDA TOWNSHIP
 CLIENTS: 2/13/2009 10:55:57 AM
 DATE PLOTTED: 08/24/09 08:09
 FILE NAME: 08024-dpp-03.dgn
 PLOT SCALE: 1" = 50'
 PLOT DATE: 2/13/2009



PR ELEV	916.34	915.48	914.90	914.32	914.04	914.21	914.38	914.45	914.59	914.68	914.78	914.92	915.00	915.13	914.66	914.87	913.57	914.00	912.19	912.66	910.98	911.31	910.14	910.33	909.84	909.98	910.13	910.23	910.57	910.61	910.81	911.00	910.80	911.15	910.59	910.94	910.16	910.36	909.49	909.65	908.82	908.93	907.92	908.21	907.30	907.50	906.76	906.78				
STA	100+00	101+00	102+00	103+00	104+00	105+00	106+00	107+00	108+00	109+00	110+00	111+00	112+00	113+00																																						

SEC Group, Inc.
 Engineering
 Planning
 Landscaping/Architecture
 Mchenry • Yorkville • New Lenox • Chicago
 www.secgroup.com

USER NAME = char-tke	DESIGNED - JJS	REVISED -
FILE NAME = 080154-dpp-03.dgn	DRAWN - JJS	REVISED -
PLOT SCALE = 1" = 50'	CHECKED - TCH	REVISED -
PLOT DATE = 2/13/2009	DATE - 2/13/09	REVISED -

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

SCALE: 1" = 50'		SHEET NO. 3 OF 5 SHEETS		STA. BEGIN TO STA. END	
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	24
CONTRACT NO. 63144			ILLINOIS FED. AID PROJECT	

PROPOSED DRAINAGE STRUCTURE SCHEDULE

STRUCTURE NUMBER	STATION	OFFSET (FT)	STRUCTURE TYPE						DIA. (FT.)	FRAME & GRATE	INVERTS								RIM ELEVATION
			MH TYPE A	CB TYPE A OPEN BOTTOM	CB TYPE A	INL TYPE A	CB TYPE C	FES			NORTH	NORTHWEST	WEST	SOUTHWEST	SOUTH	SOUTHEAST	EAST	NORTHEAST	
1	10+00.00	27	1						6	T1CL	902.80	902.60			902.67				908.00
3	11+13.00	32		1					5	T8	902.89		905.43		902.89				909.17
5	11+00.00	24						1	2	T11		905.56							909.36
5A	11+13.00	19						1	2	T11						905.51	905.47		909.48
7	12+50.00	27		1					5	T8	902.98		903.98		902.98				907.84
9	13+88.00	29	1						5	T11	903.08		904.59		903.08				907.89
11	13+98.00	29		1					5	T8	903.09				903.09				907.86
13	14+44.00	33			1				5	T11	903.13				903.13				907.87
15	15+43.00	39		1					5	T8		903.22			903.22				908.60
17	15+90.00	30			1				6	T11			903.81			903.31	905.90	903.81	909.35
19	16+08.55	42			1				6	T11	903.88			903.88					909.46
23	16+64.00	54			1				5	T11	905.42		904.30		904.00		905.95		909.80
25	107+60.00	-18						1	2	T11			906.03						909.85
27	107+50.00	-27					1		2	T8					905.49				907.20
31	16+68.00	30	1						5	T1CL	904.50						904.50		909.92
33	17+40.00	18		1					5	T8	904.72		905.72		904.72				909.81
35	17+50.00	18			1				5	T11	904.80				904.80				909.85
37	20+80.00	23		1					5	T8	906.31		906.31		905.31				909.43
39	22+20.00	23			1				4	T8					907.13				911.40
41	10+05.00	19			1				4	T11			904.40			902.80			909.17
43	10+05.00	-19			1				4	T11		904.50					904.50		909.17
45	10+46.00	-27					1		2	T8						904.60			908.65
51	12+50.00	20			1				5	T11			904.07				904.07		908.98
53	12+50.00	-19			1				4	T11		904.13	905.24				904.13		908.99
55	12+50.00	-27					1		2	T8							905.26		908.00
57	13+32.00	-36			1				4	T8						904.22			907.45
61	13+89.00	-18			1				4	T11	904.69						904.69		908.11
63	13+99.00	-18						1	2	T11					904.73				908.08
70	15+89.00	41							1	NA			906.00						
73	15+90.00	-19			1				5	T11		903.88					903.88		909.95
75	15+96.00	-25		1					5	T8		904.41				903.91			908.90
77	106+50.00	22			1				6	T11	905.22		904.47	905.22		904.47			911.07
79	106+41.00	39					1		2	T8							905.26		910.17
81	106+00.00	27	1						5	T1CL	906.73		907.03				904.53		912.10
83	105+33.00	29		1					5	T8	907.30		908.90	909.00			907.30		913.00
85	105+00.00	43					1		2	T8							909.70		914.28
87	104+50.00	22			1				4	T8							909.80		914.62
89	105+34.00	-25					1		2	T8					907.60				912.39
91	106+00.00	18			1				4	T11	906.94				906.82				912.49
93	106+00.00	-18			1				4	T11					907.07		907.57		912.49
95	106+17.00	-16					1		2	T8			907.65						910.77
101	17+40.00	-18			1				4	T11	905.94						905.94		909.72
103	17+50.00	-18			1				4	T11		906.01			906.01				909.76
105	18+00.00	-24					1		2	T8					906.31				909.72
111	20+80.00	18			1				4	T11	907.09		906.49				906.36		910.90
113	20+90.00	18						1	2	T11					907.75				910.95
115	20+80.00	-18			1				4	T11	907.09			906.59			906.59		910.90
117	20+50.00	-25			1				4	T8							906.67		909.38
119	20+90.00	-18						1	2	T11					907.25				910.95
120	112+00.00	-29							1	NA	904.44								
122	112+00.00	-16						1	2	T11	904.92								907.92
125	112+50.00	33							1	NA					903.73				
127	112+50.00	16						1	2	T11					904.21				907.21



USER NAME = jmelhuish	DESIGNED - JJS	REVISED -
PLOT SCALE = N.T.S.	DRAWN - JJS	REVISED -
PLOT DATE = 3/5/2009	CHECKED - TCH	REVISED -
	DATE - 3/5/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**DRAINAGE AND UTILITY PLAN
PROPOSED DRAINAGE STRUCTURE SCHEDULE**

SCALE: N.T.S. SHEET NO. 4 OF 5 SHEETS STA. TO STA.

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 25
CONTRACT NO. 63144				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

080154
080124-01.dgn

PROPOSED DRAINAGE PIPE SCHEDULE

PIPE NO.	FROM STRUCT.	TO STRUCT.	STORM SEWER, CLASS A, TYPE 2						SLOPE (%)	TBF (CY)
			12"	15"	18"	24"	30"	36"		
2	3	1						107	0.08	93.00
4	5	5A	12						0.40	4.00
4A	5A	3	9						0.40	3.00
6	7	3						134	0.07	100.00
8	9	7						138	0.07	89.00
10	11	9						5	0.40	3.00
12	13	11						44	0.07	27.00
14	15	13						97	0.09	68.00
16	17	15						44	0.19	36.00
18	19	17					18		0.39	20.00
22	23	19					53		0.22	45.00
24	25	23	7						1.14	2.00
26	27	23	7						0.78	2.00
28	31	23				20			1.00	16.00
32	33	31				68			0.33	19.00
34	35	33				8			0.80	7.00
36	37	35				324			0.16	228.00
38	39	37	137						0.55	44.00
40	41	1	6						2.00	5.00
42	43	41	34						0.26	12.00
44	45	43	40						0.24	15.00
50	51	7				4			2.25	3.00
52	53	51				36			0.15	25.00
54	55	53	6						0.25	2.00
56	57	53				80			0.11	28.00
60	61	9			44				0.21	12.00
62	63	61	8						0.50	2.00
71	70	17			5				0.20	
72	73	17					44		0.14	32.00
74	75	73					5		0.33	4.00
76	77	75				41			0.13	35.00
76A	77A	77	44						1.00	25.00
78	79	77		17					0.21	11.00
80	81	77				46			0.12	52.00
82	83	81	63						0.40	48.00
84	85	83	33						2.00	12.00
86	87	83	80						1.08	48.00
88	89	83	52						0.56	38.00
90	91	81	4						1.00	3.00
92	93	91	33						0.36	25.00
94	95	93	16						0.44	5.00
100	101	33	33						0.61	11.00
102	103	101	8						0.70	3.00
104	105	103	50						0.58	15.00
110	111	37	2						2.50	3.00
112	113	111	8						2.00	2.00
114	115	111	33						0.28	13.00
116	117	115	28						0.26	8.00
118	119	115	8						2.00	3.00
121	122	120	8						6.00	2.00
126	127	125	12						4.00	3.00



USER NAME : cher-ka
 PLOT SCALE = N.T.S.
 PLOT DATE = 2/13/2009

DESIGNED - JJS
 DRAWN - JJS
 CHECKED - TCH
 DATE - 2/13/09

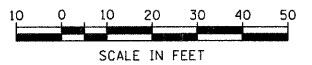
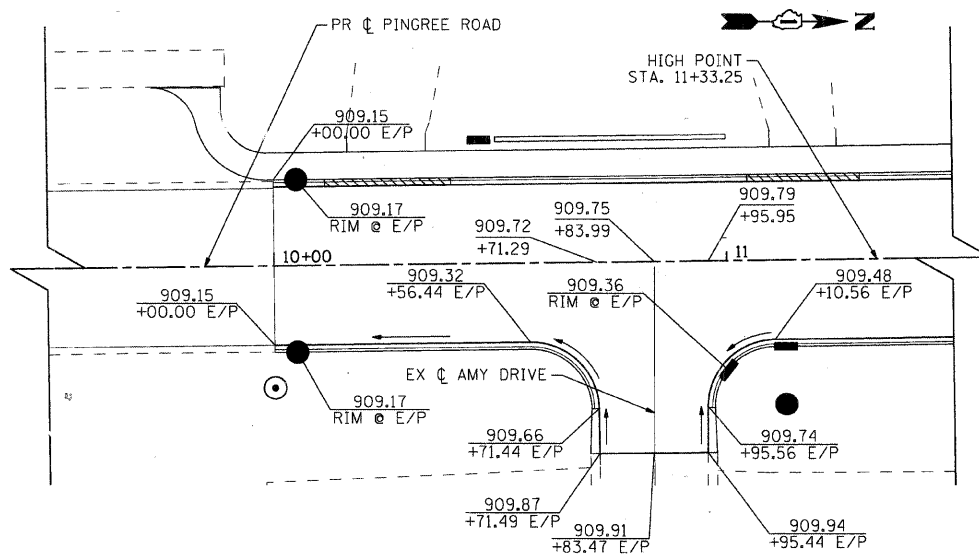
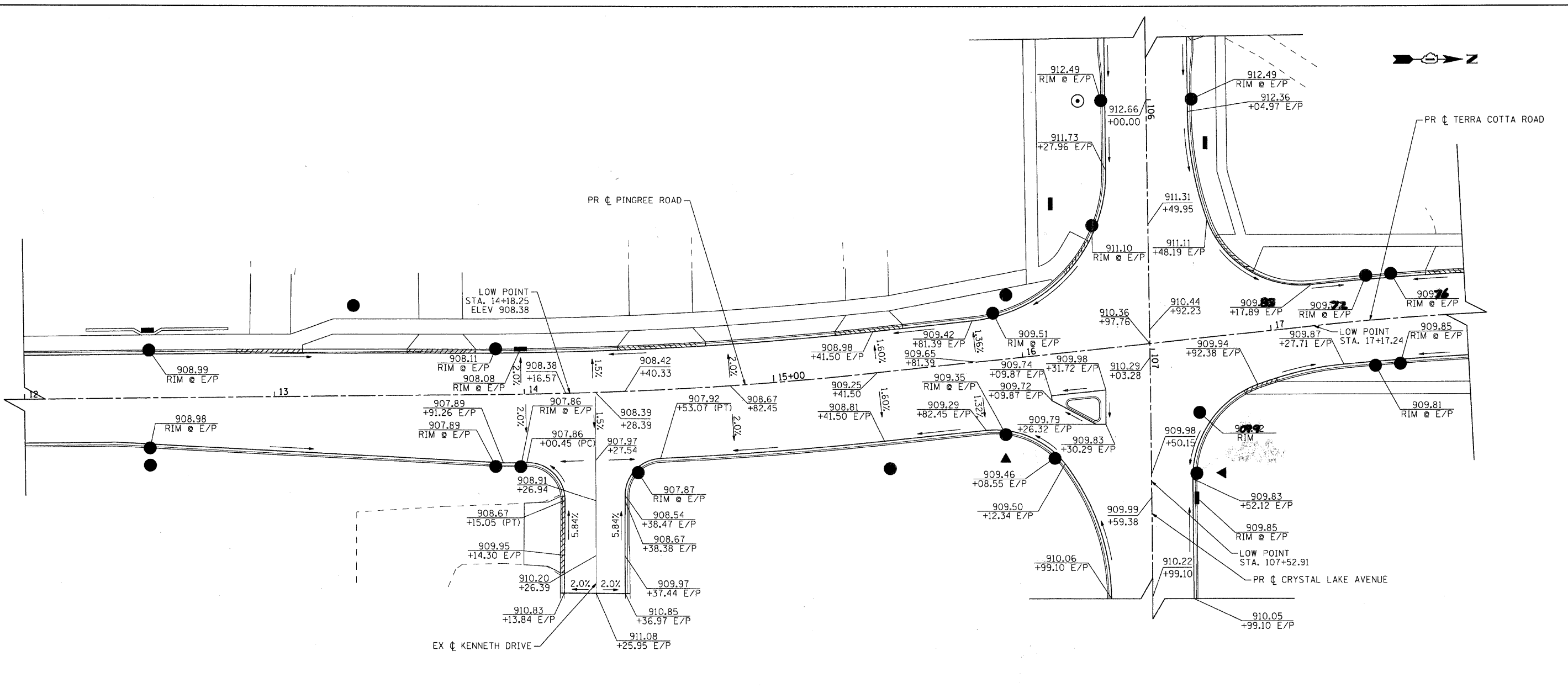
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 REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**DRAINAGE AND UTILITY PLAN
 PROPOSED DRAINAGE PIPE SCHEDULE**

SCALE: N.T.S. SHEET NO. 5 OF 5 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	26
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	



SEC Group, Inc.
 Engineering
 Surveying
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 Landscape Architecture
 McHenry • Yorkville • New Lenox • Chicago
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USER NAME = char-tke	DESIGNED - JPA	REVISED -
PLOT SCALE = 1" = 20'	DRAWN - JPA	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

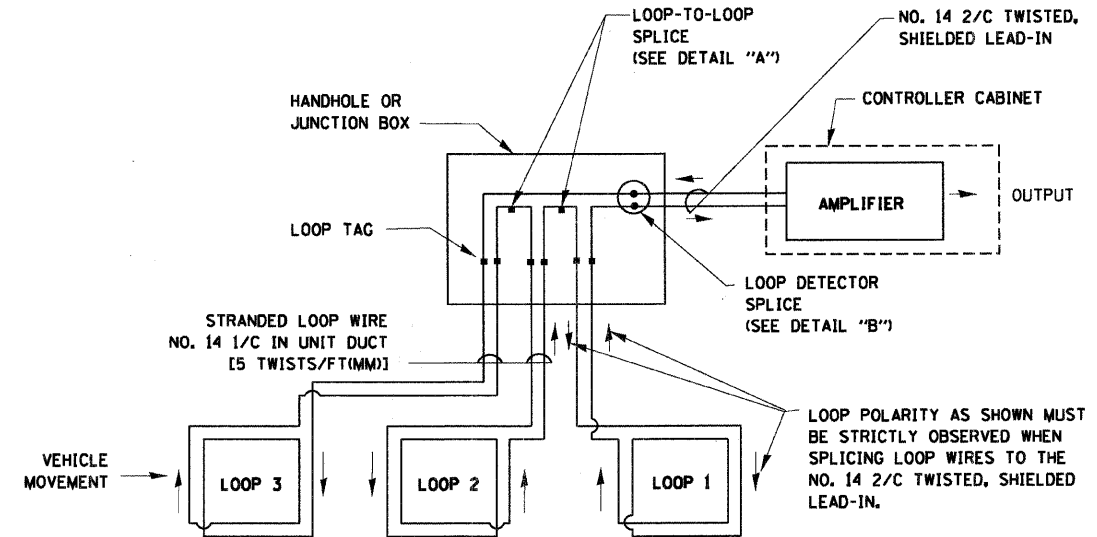
**INTERSECTION GRADING
 PLAN**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	27
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63144				

080154
 C-81-069-06
 080154-p10-01.dgn

LOOP DETECTOR NOTES

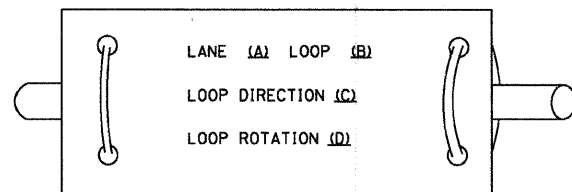
- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



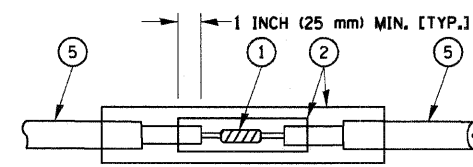
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

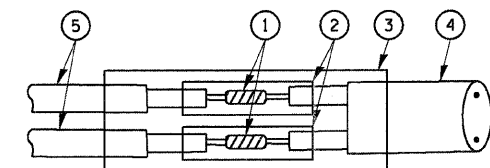
LOOP LEAD-IN CABLE TAG



- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

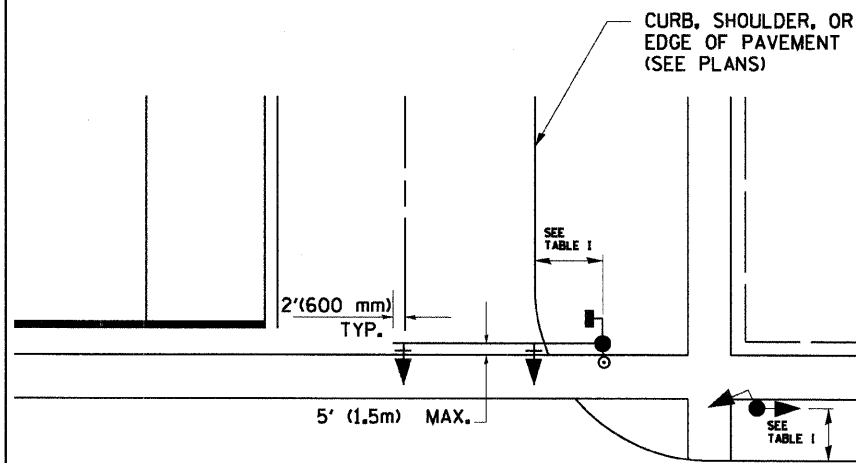
LOOP DETECTOR SPLICE

- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

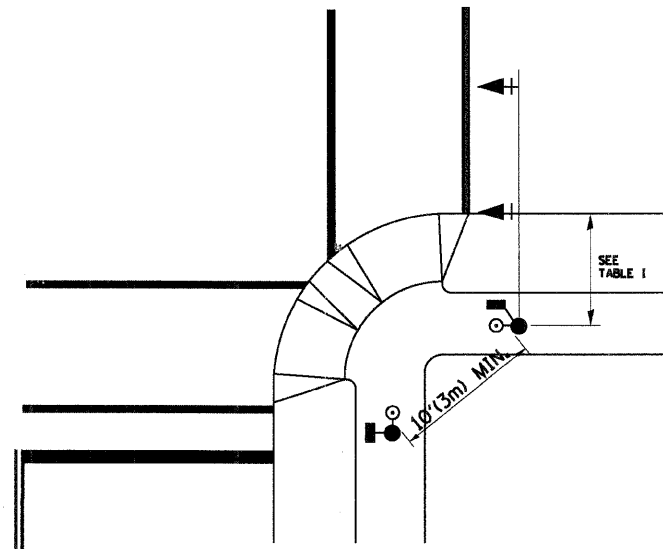
<p>SEC Group, Inc. Engineering Surveying Planning Landscape Architecture McHenry • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = char-tke PLOT SCALE = N.T.S. PLOT DATE = 2/13/2009	DESIGNED - D.A.D. DRAWN - R.W.P. CHECKED - D.A.Z. DATE - 2/13/09	REVISED - 11-12-01 REVISED - BUR, TRAFFIC 1-2-02 REVISED - REVISED -	MCHENRY COUNTY DIVISION OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A. RTE. 0116 SECTION 08-00327-01-CH COUNTY MCHENRY TOTAL SHEETS 56 SHEET NO. 28	CONTRACT NO. 63144 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
	SCALE: N.T.S. SHEET NO. 1 OF 7 SHEETS STA. TO STA.						

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.
- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.
- PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:
 - A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.
 - B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.
 - C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
 - D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).
 - E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

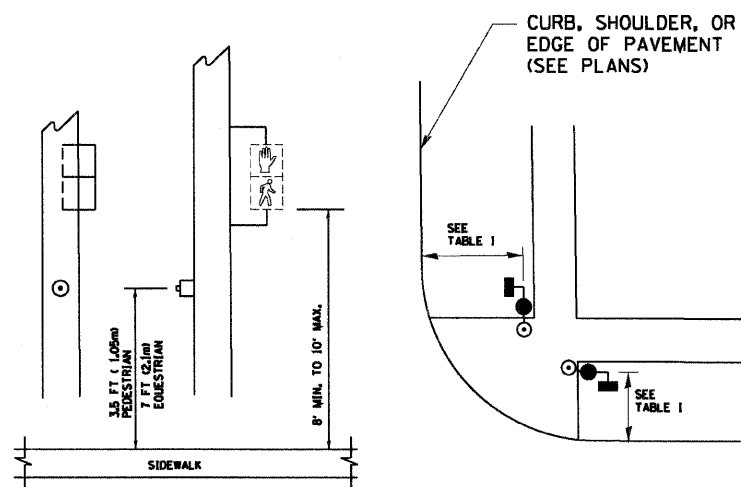
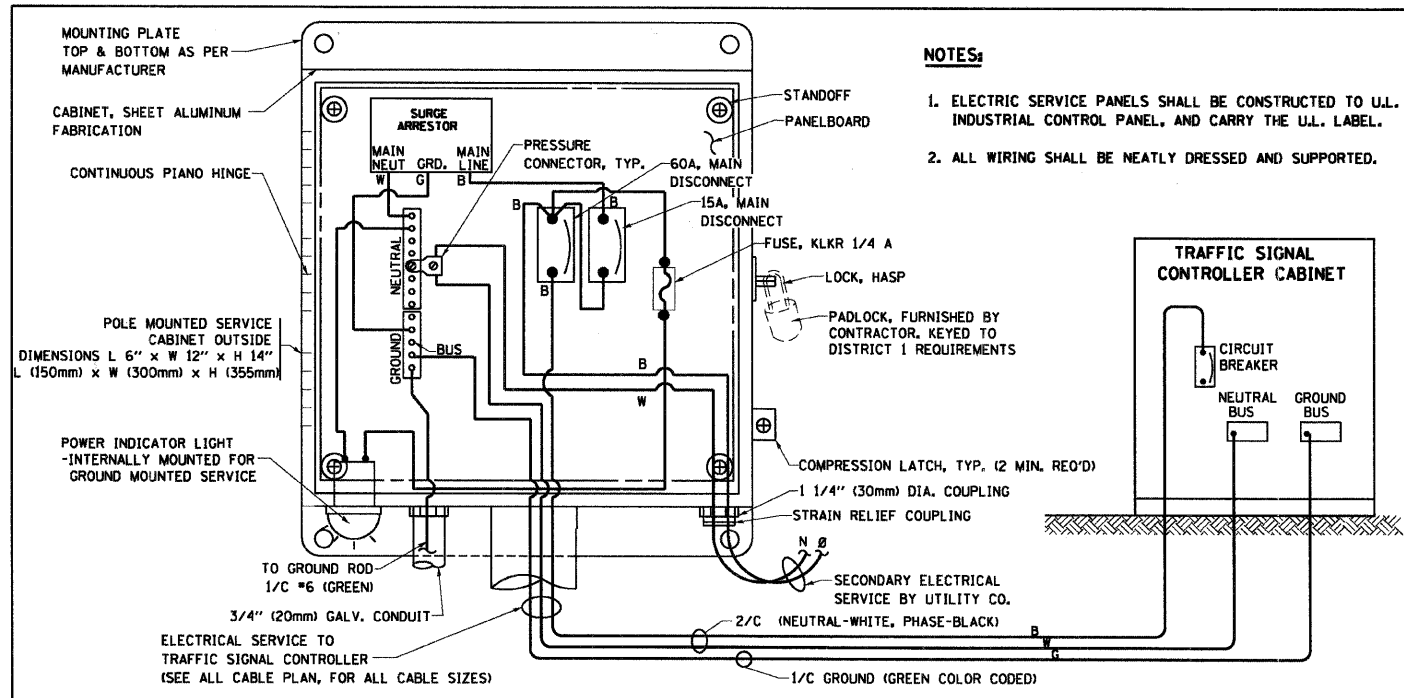


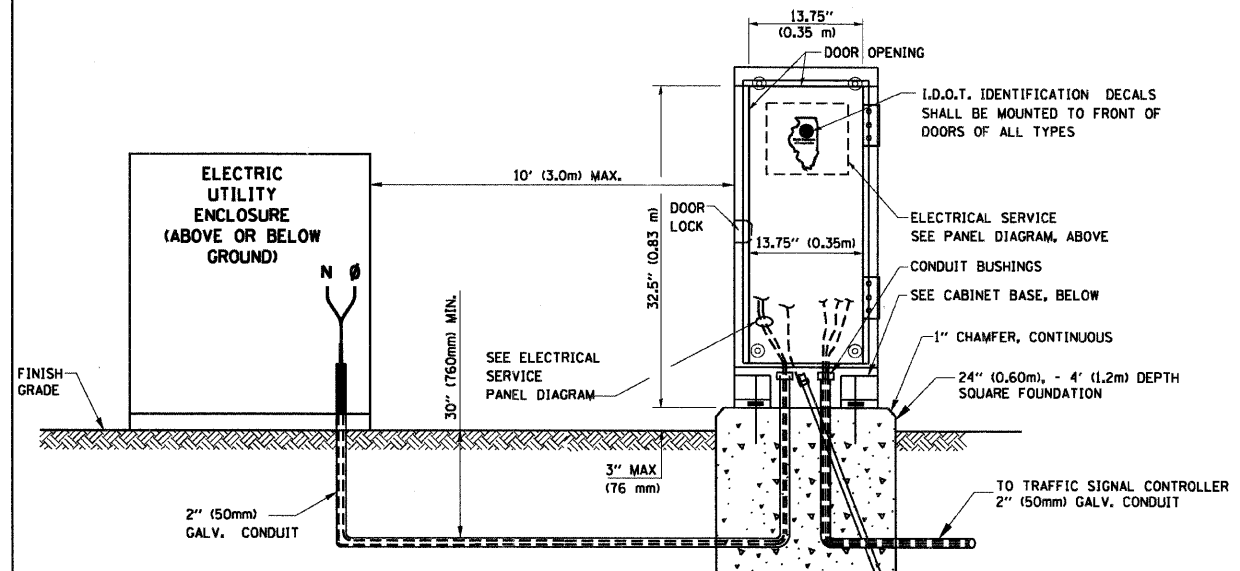
TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

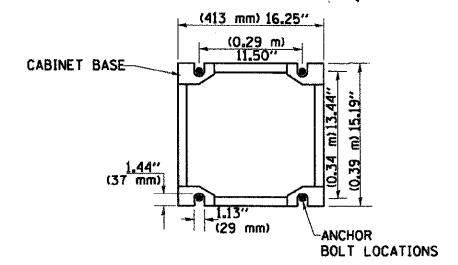


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

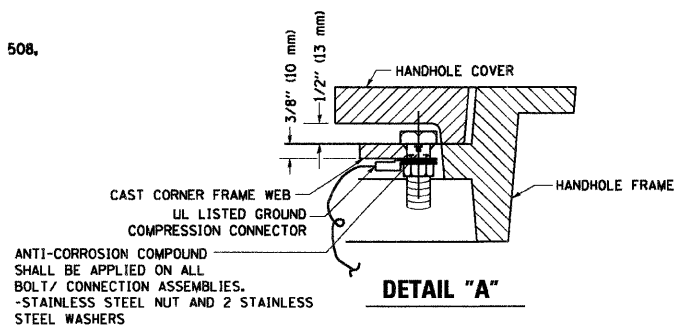
SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



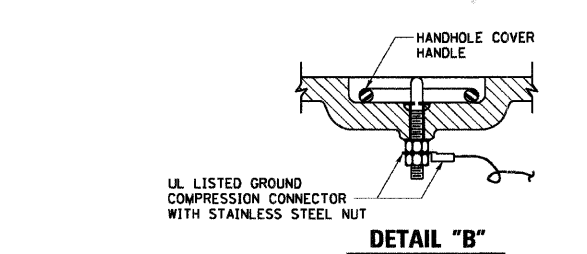
SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)



CABINET - BASE BOLT PATTERN (NOT TO SCALE)



DETAIL "A"

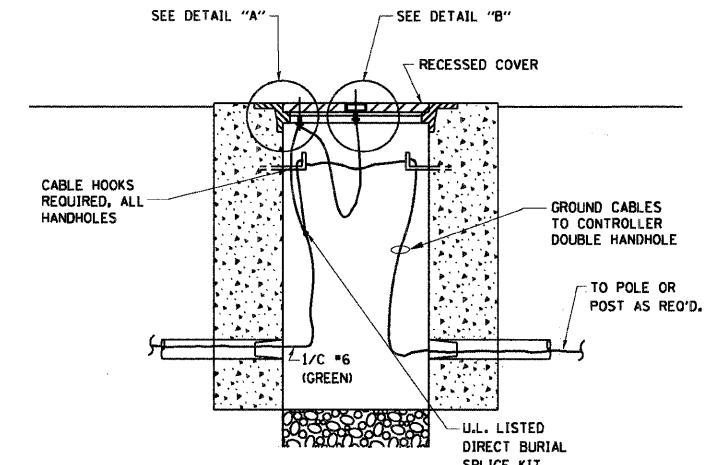


DETAIL "B"

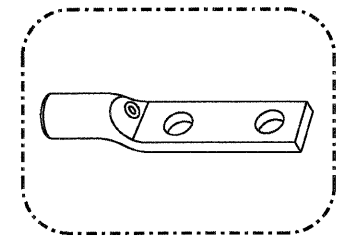
NOTES:

GROUNDING SYSTEM

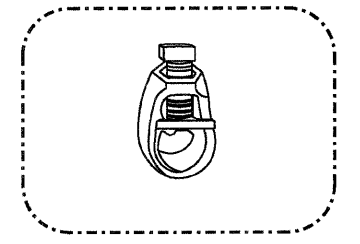
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.), GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION, AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)

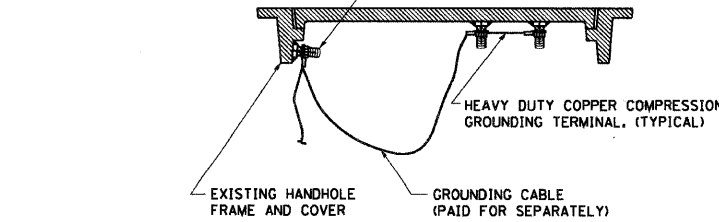


3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EQUAL)

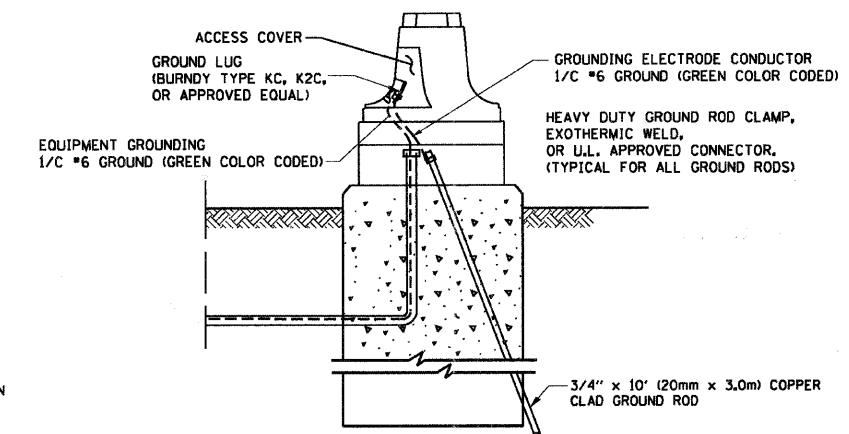
NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

(2) 1/2" x 1 1/4" STAINLESS STEEL BOLT WITH SPLIT LOCK WASHER AND NYLON INSERT LOCKOUT WELDED TO FRAME AND TO COVER. (TYPICAL)



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)



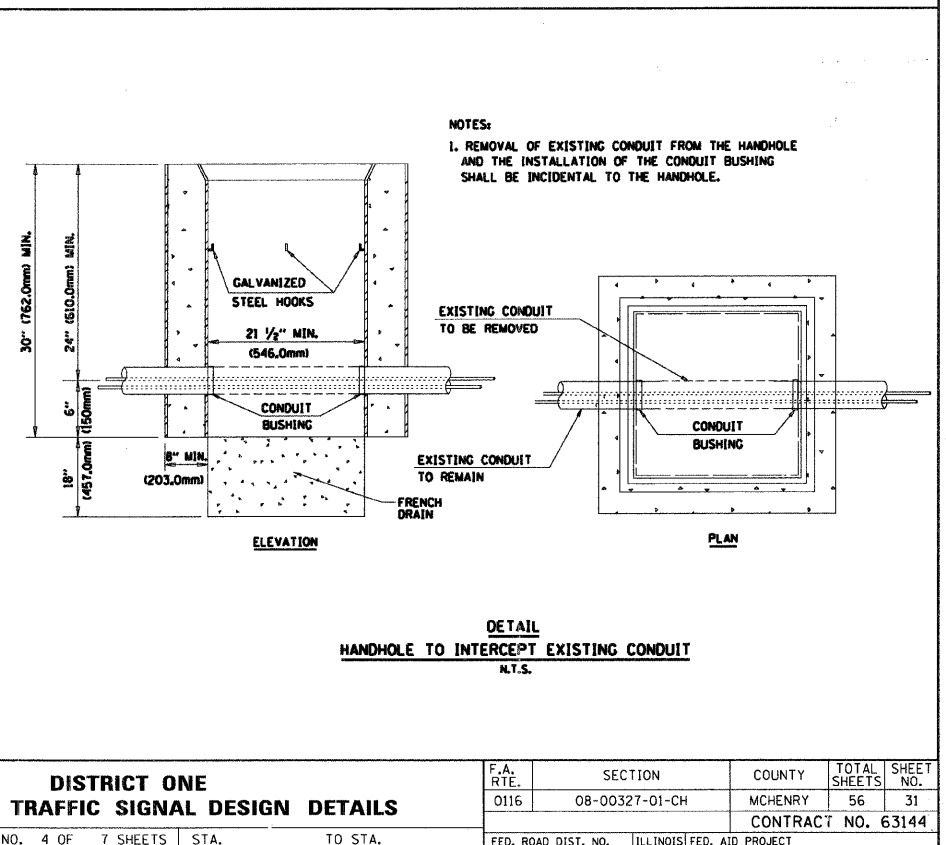
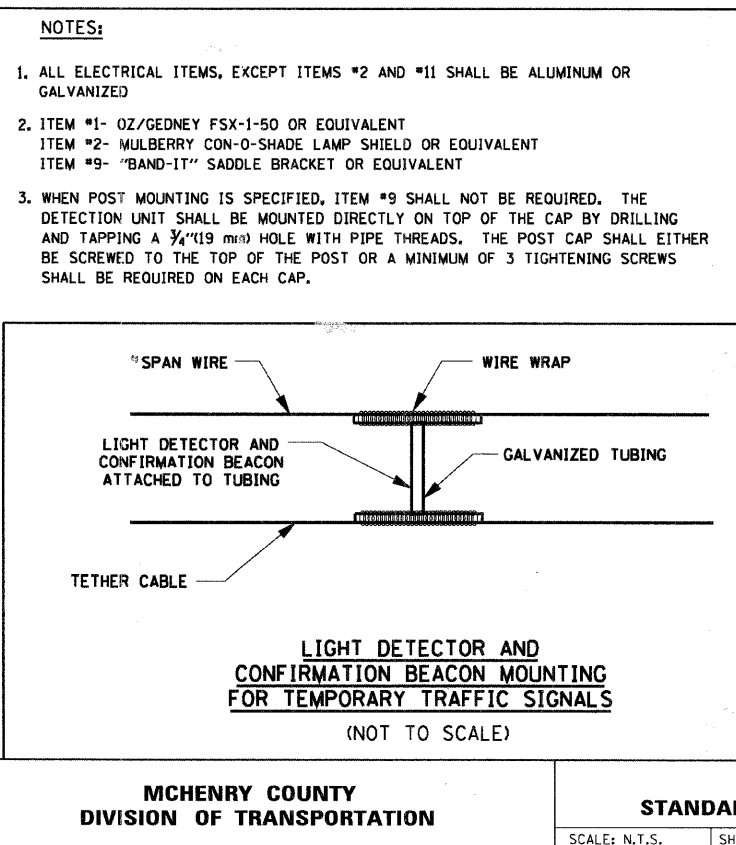
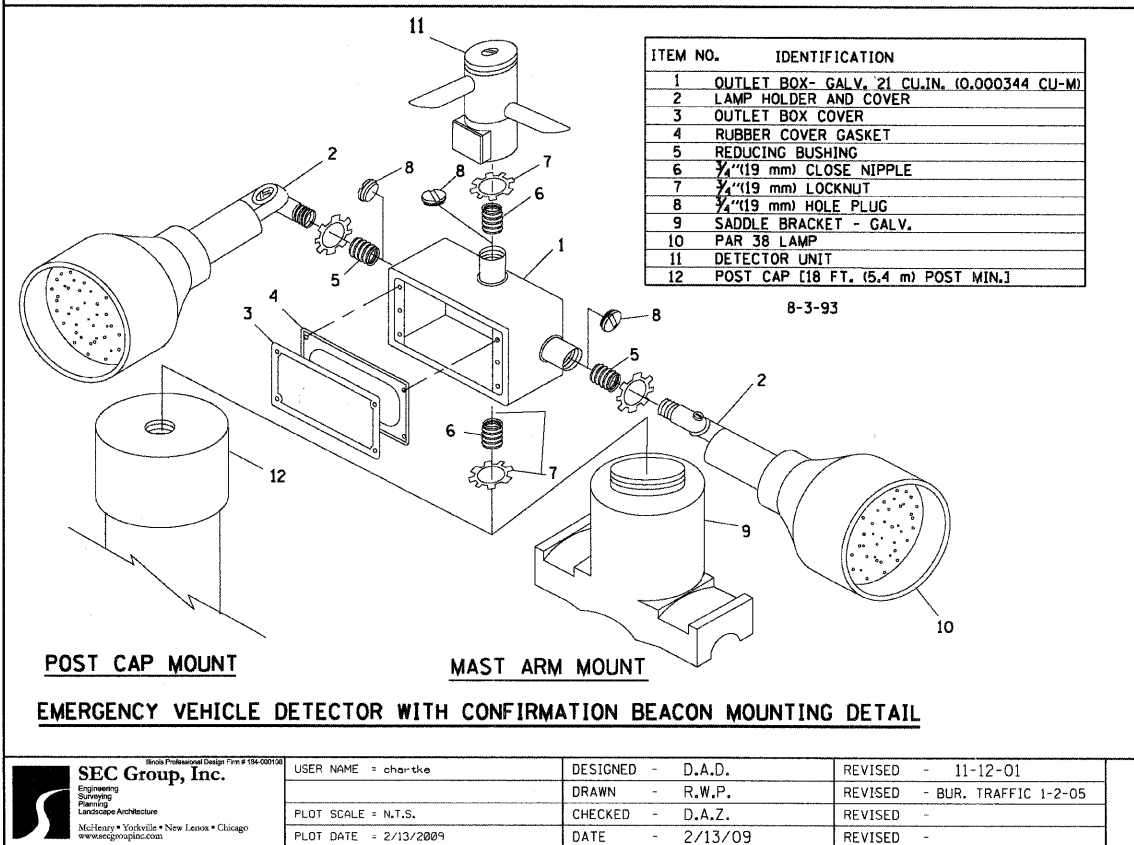
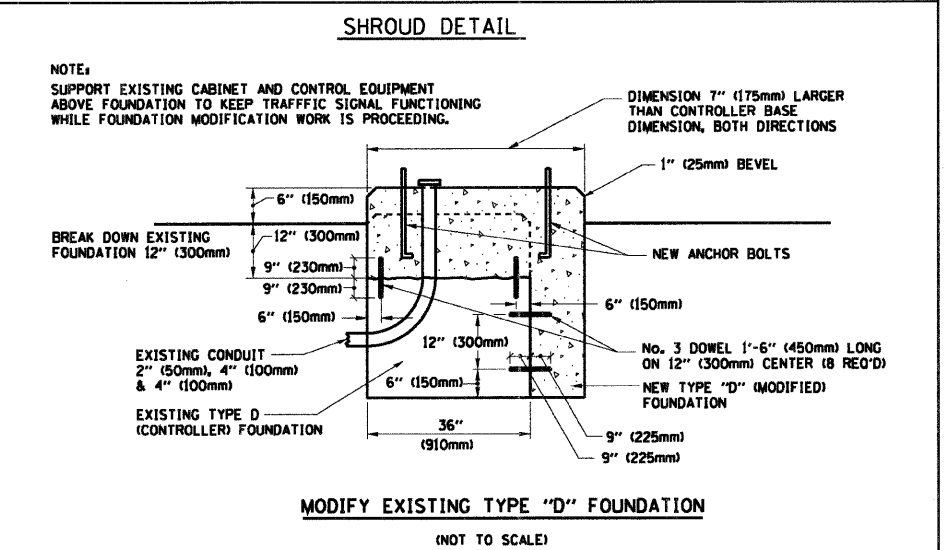
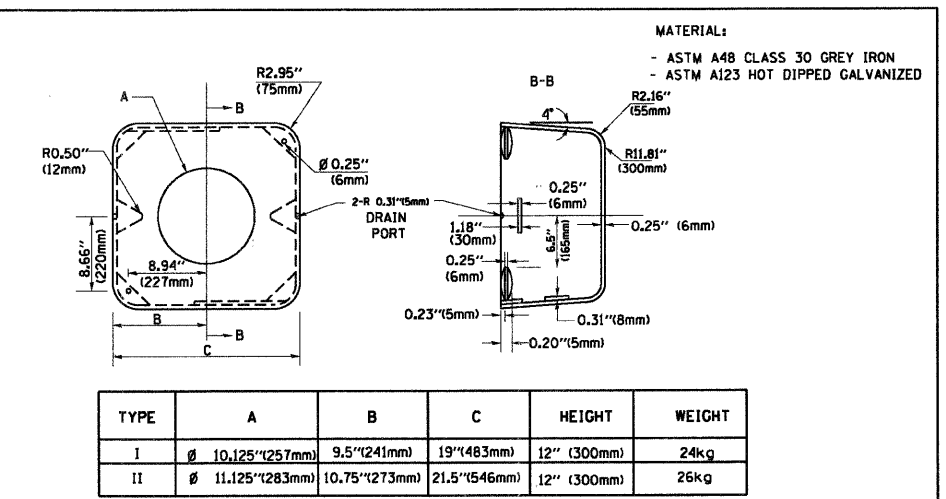
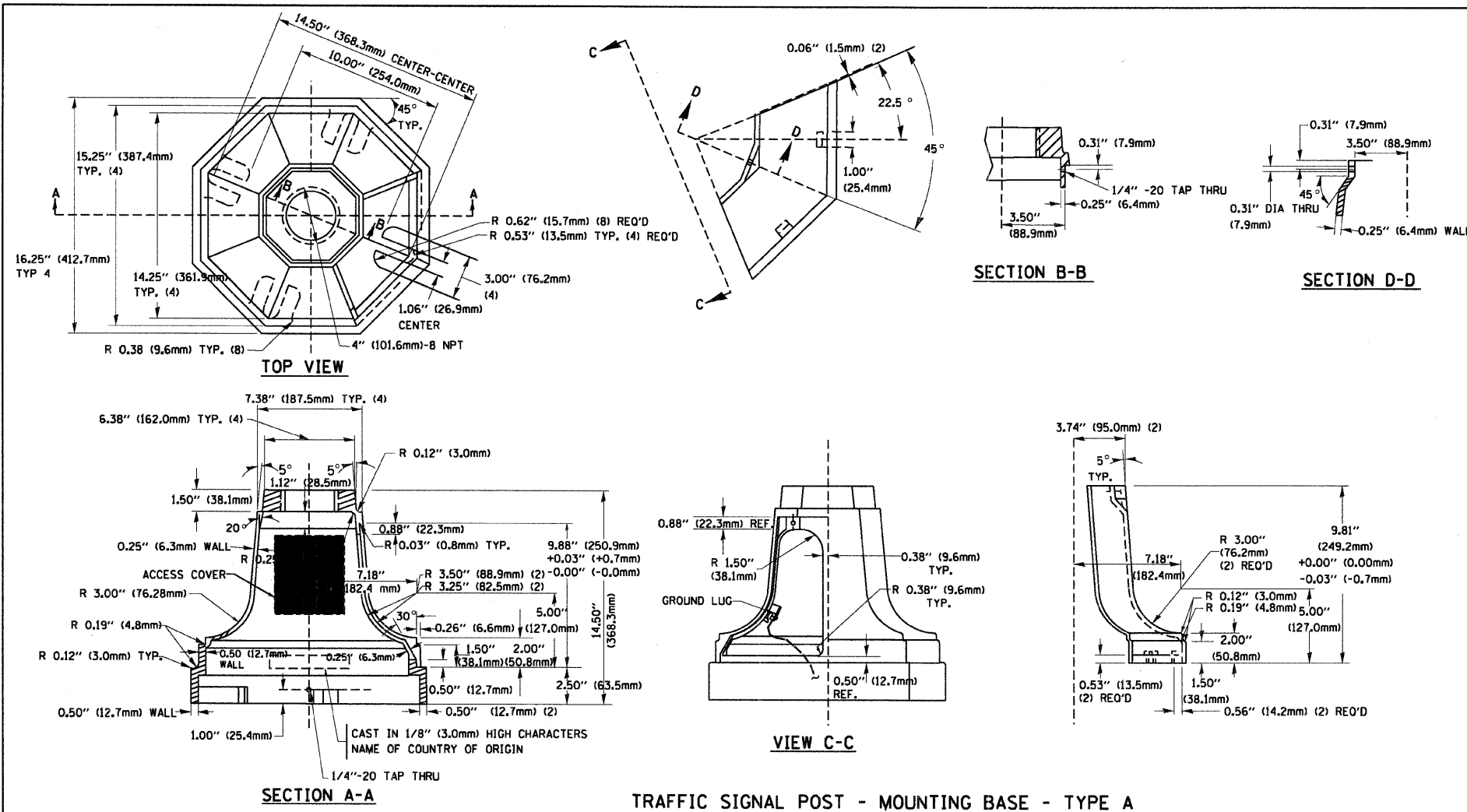
USER NAME = char-uke	DESIGNED - D.A.D.	REVISED - 11-12-01
PLOT SCALE = N.T.S.	DRAWN - R.W.P.	REVISED - BUR, TRAFFIC 1-2-04
PLOT DATE = 2/13/2009	CHECKED - D.A.Z.	REVISED -
	DATE - 2/13/09	REVISED -

MCHENRY COUNTY DIVISION OF TRANSPORTATION

DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	30
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 63144				

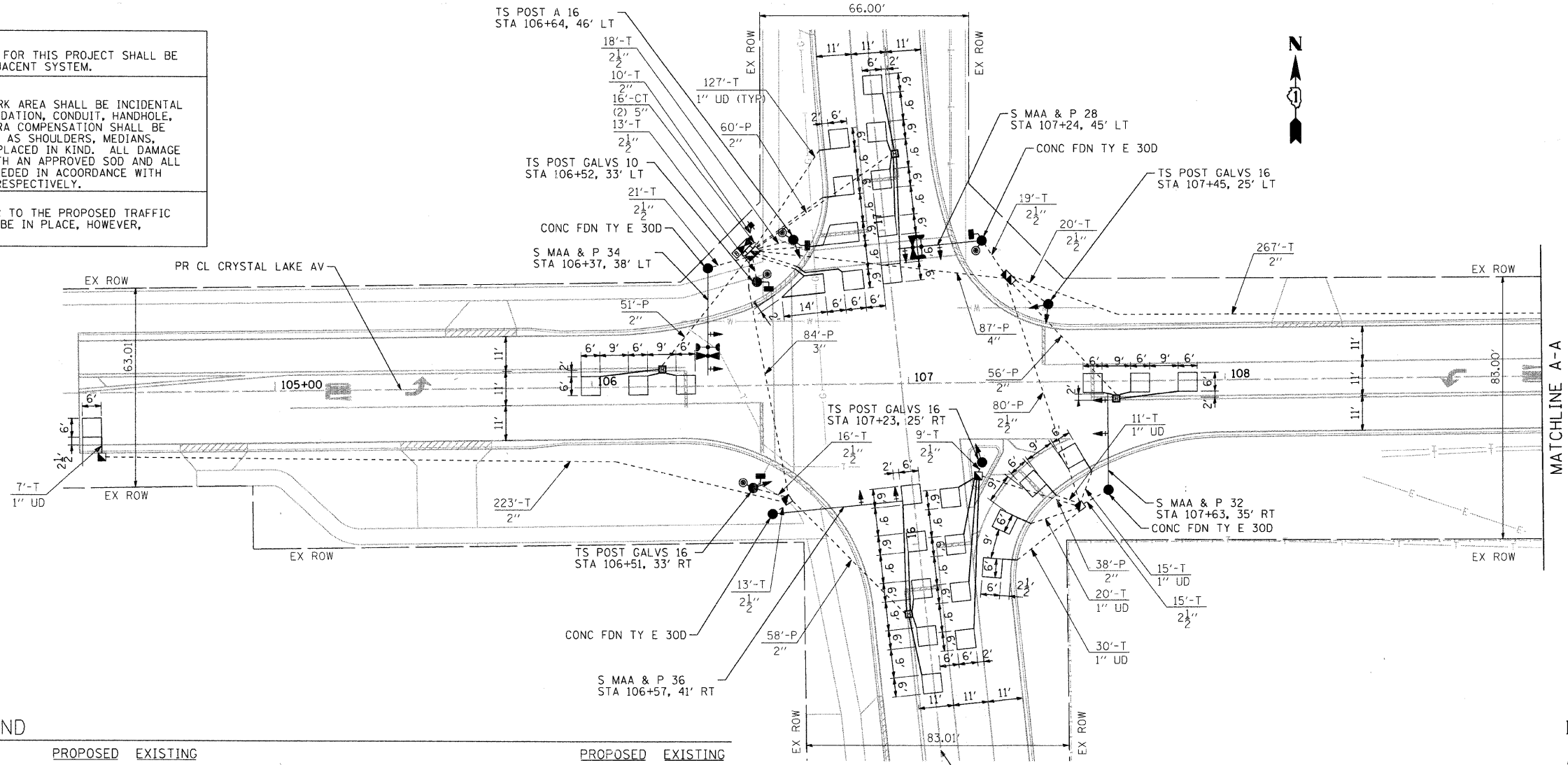
SCALE: N.T.S. SHEET NO. 3 OF 7 SHEETS STA. TO STA.



NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

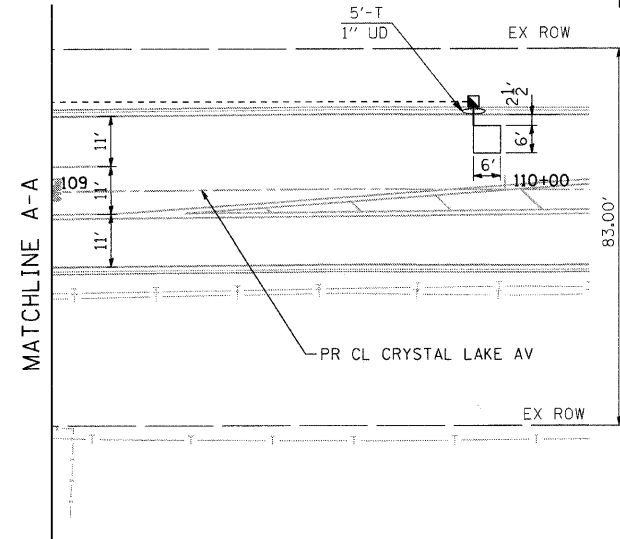
RESTORATION OF WORK AREA
RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

NOTE:
STOP BARS ARE NOT TO BE PLACED PRIOR TO THE PROPOSED TRAFFIC SIGNALS BEING OPERATIONAL. THEY MUST BE IN PLACE, HOWEVER, AT THE TIME OF TURN-ON.



TRAFFIC SIGNALS LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER	☒	☒	EMERGENCY VEHICLE LIGHT DETECTOR	⚡	⚡
SERVICE INSTALLATION	+	+	CONFIRMATION BEACON	⚡	⚡
SIGNAL HEAD	➔	➔	SIGNAL HEAD OPTICALLY PROGRAMMED	➔ "P"	➔ "P"
SIGNAL HEAD WITH BACKPLATE	➔	➔	VIDEO DETECTION CAMERA	📹	📹
SIGNAL HEAD, PEDESTRIAN	➔	➔	TELEPHONE CONNECTION	☐	☐
SIGNAL POST	●	○	ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"	☑	☑ "E"
MAST ARM ASSEMBLY AND POLE, STEEL	●	○	ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"	☑	☑ "E"
MAST ARM ASSEMBLY AND POLE, ALUMINUM	●	○	CONDUIT SPLICE	⚡	⚡
COMBINATION MAST ARM ASSEMBLY AND POLE, STEEL WITH LUMINAIRE	●	○	WOOD POLE	⊙	⊙ "E"
HANDHOLE	☐	☐	RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II	⎓	⎓ "E"
HEAVY DUTY HANDHOLE	☐	☐	VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE	⎓	⎓
DOUBLE HANDHOLE	☐	☐	RAILROAD CONTROL CABINET	☒	☒ "E"
G.S. CONDUIT IN TRENCH (T) OR PUSHED (P)	—	—	UNINTERRUPTABLE POWER SUPPLY	⊞	⊞
PEDESTRIAN PUSHBUTTON DETECTOR	●	●			
DETECTOR LOOP	☐	☐			
CAST IRON JUNCTION BOX	⊙	⊙ "E"			
COMMON TRENCH	CT				
UNIT DUCT	UD				



<p>SEC Group, Inc. Engineering Surveying Planning Landscape Architecture Midwest • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = char-ike	DESIGNED - JJS	REVISED -	MCHENRY COUNTY DIVISION OF TRANSPORTATION	TRAFFIC SIGNAL INSTALLATION PLAN CRYSTAL LAKE AV AT PINGREE RD			F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 32
	PLOT SCALE = 1" = 20'	CHECKED - CP	REVISED -		SCALE: 1" = 20'	SHEET NO. 5 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 63144		
	PLOT DATE = 2/13/2009	DATE - 2/13/09	REVISED -									

080154
C:\21-269-06
080154-10-01.dgn

SCHEDULE OF QUANTITIES

72000100	SIGN PANEL - TYPE 1	SQ FT	33
72000200	SIGN PANEL - TYPE 2	SQ FT	30
81000600	CONDUIT IN TRENCH, 2" DIA, GALVANIZED STEEL	FOOT	500
81000700	CONDUIT IN TRENCH, 2 1/2" DIA, GALVANIZED STEEL	FOOT	142
81001100	CONDUIT IN TRENCH, 5" DIA, GALVANIZED STEEL	FOOT	16
81018500	CONDUIT PUSHED, 2" DIA, GALVANIZED STEEL	FOOT	263
81018600	CONDUIT PUSHED, 2 1/2" DIA, GALVANIZED STEEL	FOOT	80
81018700	CONDUIT PUSHED, 3" DIA, GALVANIZED STEEL	FOOT	84
81018900	CONDUIT PUSHED, 4" DIA, GALVANIZED STEEL	FOOT	87
81400100	HANDHOLE	EACH	5
81400200	HEAVY-DUTY HANDHOLE	EACH	4
81400300	DOUBLE HANDHOLE	EACH	2
81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	986
85700205	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 2/C	FOOT	425
87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 3/C	FOOT	791
87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 5/C	FOOT	833
87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL, NO. 14 7/C	FOOT	1,675
87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1,797
87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2/C	FOOT	49
87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 10 FT.	EACH	1
87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 16 FT.	EACH	4
87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1
87700200	STEEL MAST ARM ASSEMBLY AND POLE, 32 FT.	EACH	1
87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1
87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	1
87800100	CONCRETE FOUNDATION, TYPE A	FOOT	24
87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4
87800400	CONCRETE FOUNDATION, TYPE E, 30-INCH DIAMETER	FOOT	51
88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	4
88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	5
88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	4
88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	8
88500100	INDUCTIVE LOOP DETECTOR	EACH	11
88600100	DETECTOR LOOP, TYPE 1	FOOT	1,129
88700200	LIGHT DETECTOR	EACH	4
88700300	LIGHT DETECTOR AMPLIFIER	EACH	1
88800100	PEDESTRIAN PUSH BUTTON	EACH	4
X8050010	SERVICE INSTALLATION, GROUND MOUNTED	EACH	1
X8620020	UNINTERRUPTIBLE POWER SUPPLY	EACH	1
X8730027	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1/C	FOOT	1,346
XX005660	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED SHIELDED	FOOT	333

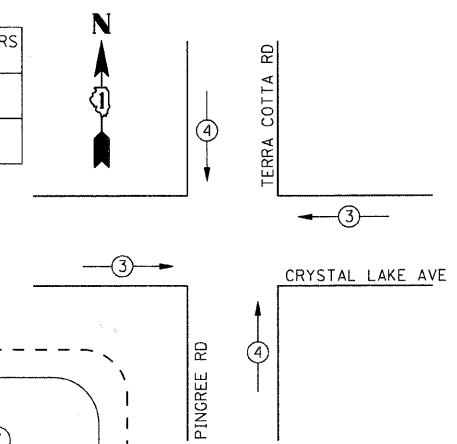
NOTE:
THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

PROPOSED EMERGENCY VEHICLE PREEMPTORS

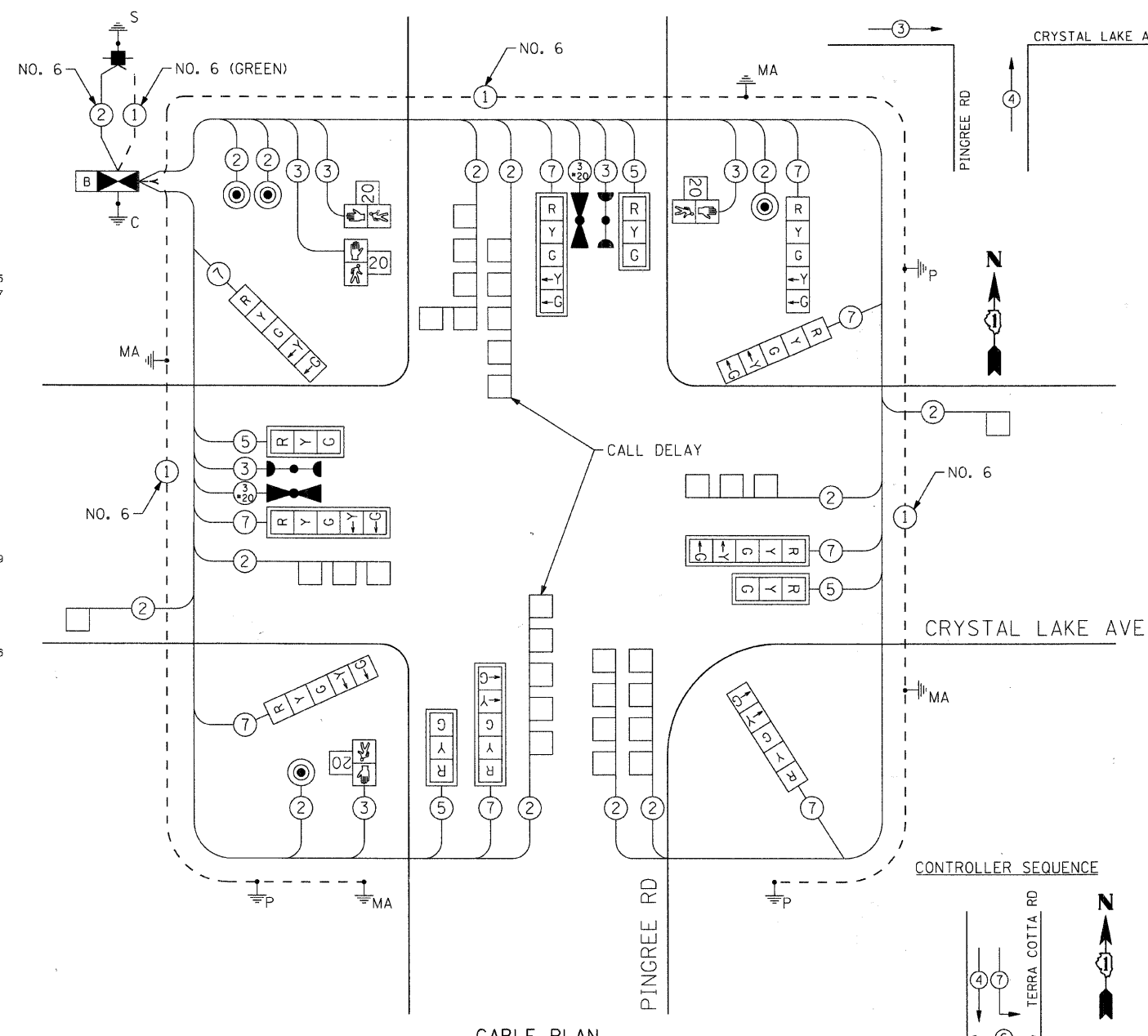
PROPOSED EMERGENCY VEHICLE PREEMPTORS	3	4
MOVEMENT	←	↑

EMERGENCY VEHICLE PREEMPTION SEQUENCE



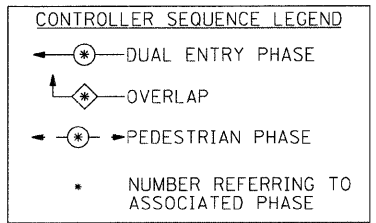
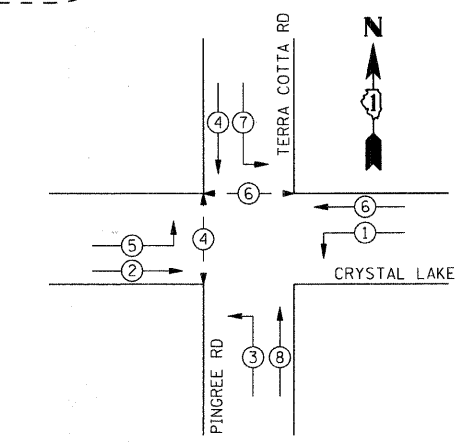
CABLE PLAN LEGEND

- | | | |
|----------|----------|---|
| EXISTING | PROPOSED | |
| (R) | (R) | 12" TRAFFIC SIGNAL SECTION |
| (W) | (W) | 12" PEDESTRIAN SIGNAL SECTION |
| (20) | (20) | 12" PEDESTRIAN SIGNAL SECTION WITH COUNTDOWN TIMER |
| (C) | (C) | CONTROLLER CABINET |
| (S) | (S) | SERVICE INSTALLATION |
| (T) | (T) | TELEPHONE INSTALLATION |
| (V) | (V) | VEHICLE DETECTOR, INDUCTION LOOP |
| (M) | (M) | MAGNETIC DETECTOR |
| (E) | (E) | EMERGENCY VEHICLE LIGHT DETECTOR |
| (B) | (B) | CONFIRMATION BEACON |
| (P) | (P) | PUSHBUTTON DETECTOR |
| (2) | (2) | 2 DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED. |
| (1) | (1) | 1 GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN) |
| (24) | (24) | 24 FIBER OPTIC CABLE IN CONDUIT NO. 62.5/125 2-MM12F SM12F |
| (R) | (R) | RAILROAD CONTROL CABINET |
| (V) | (V) | VIDEO DETECTION CAMERA |
| (R) | (R) | SIGNAL FACE WITH BACKPLATE |
| (P) | (P) | "P" INDICATES PROGRAMMED HEAD |
| (MA/P) | (MA/P) | GROUND ROD AT POST (P) OR MAST ARM POLE (MA) |
| (H/C) | (H/C) | GROUND ROD AT HANDHOLE (H), DOUBLE HANDHOLE (H), OR CONTROLLER (C) |
| (S) | (S) | GROUND ROD AT ELECTRIC SERVICE INSTALLATION |
| (B) | (B) | UNINTERRUPTIBLE POWER SUPPLY |



CABLE PLAN

CONTROLLER SEQUENCE



I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	13	17	0.50		110.5
(YELLOW)	13	25	0.25		81.25
(GREEN)	13	15	0.25		48.75
ARROW (YELLOW)	9	12	0.10		10.8
ARROW (GREEN)	9	12	0.10		10.8
PEDESTRIAN SIGNAL	4	25	1.00		100
CONTROLLER	1	100	1.00		100
UPS	1	25	1.00		25
TOTAL =					487.1

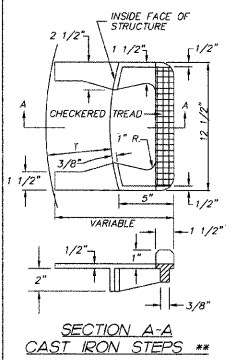
FOUNDATION (DEPTH)	FT. (m)	CABLE SLACK	FT. (m)	VERTICAL	FT. (m)
TYPE A - POST	4 (1.2)	HANDHOLE	6.5 (2.0)	ALL FOUNDATIONS	3.5 (1.0)
C - CONTROLLER	4 (1.2)	DOUBLE HANDHOLE	13 (4.0)	MAST ARM (L) POLE	20'+L-2=
E - M.A. LENGTH		SIGNAL POST	2 (1.0)	BRACKET MOUNTED	(6m+L-0.6m)
<30'	30" (750mm)	CONTROLLER CAB.	1 (0.5)	PED. PUSHBUTTON	4 (1.2)
<40'	30" (750mm)	FIBER OPTIC	13 (4.0)	ELECTRIC SERVICE	13.5 (4.1)
<40'	36" (900mm)	ELECTRIC SERVICE	1 (0.5)	SERVICE TO GROUND	13.5 (4.1)
<50'	36" (900mm)	GROUND CABLE	1 (0.5)	POST MOUNTED	6 (1.8)
>50'	36" (900mm)				

ENERGY COSTS TO: CITY OF CRYSTAL LAKE
P.O. BOX 597
CRYSTAL LAKE, IL 60039

ENERGY SUPPLY CONTACT: KATHY NYSTROM
PHONE: (815) 477-5251
COMPANY: COMMONWEALTH EDISON

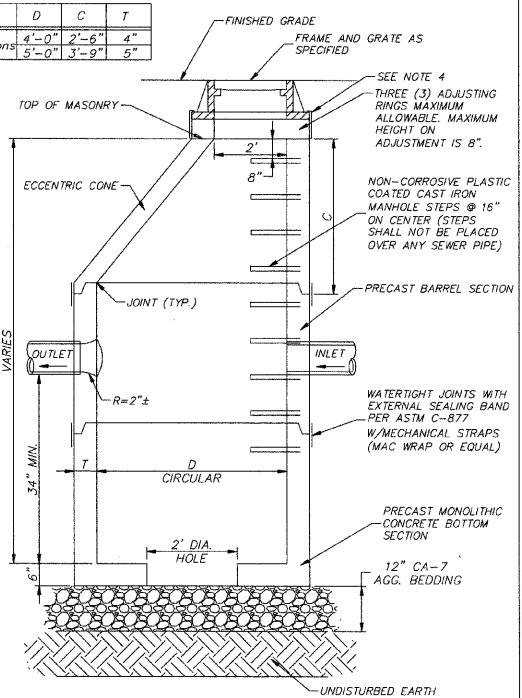
08/154
C-31-269-06
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MATERIALS FOR STRUCTURE	D	C	T
Precast Reinforced Concrete Sections	4'-0"	2'-6"	4"
	5'-0"	3'-9"	5"



**SECTION A-A
CAST IRON STEPS**

** The Cast Iron Steps As Detailed
• Dimension "C" For Precast Reinforced Hereon Are Typical Steps Of Other Design And Material That Will Conform To The Minimum Requirements Of The Steps Shown. May Be Used When Approved By The Engineer.
Non-corrosive Plastic Coated Cast Iron Steps Shall Be Gray Iron Conforming To The Requirements Of Art. 1006.14 Of The Standard Specifications.
Steps Shall Be Embedded Into Wall A Minimum Of 3 Inches. Steps Shall Not Be Extended On The Outside.



1. All Catch Basins shall be 4'-0" in diameter unless otherwise noted on plans.
2. In addition to the requirements of Art. 602.15 of the standard Specifications the contract unit price for Catch Basins, Type A shall include the sand cushion when required, furnishing and placing steps when required, and furnishing and compacting the specified backfill material.
3. Dimension "C" for Precast Reinforced Concrete sections may vary from the dimension given to plus 6 inches.
4. External chimney seal to be installed on catch basin frame/masonry cone joint. (Not required for catch basins in grassy areas).

Algonquin Twp.
**OPEN BOTTOM CATCH
BASIN TYPE A**



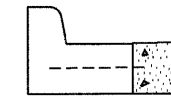
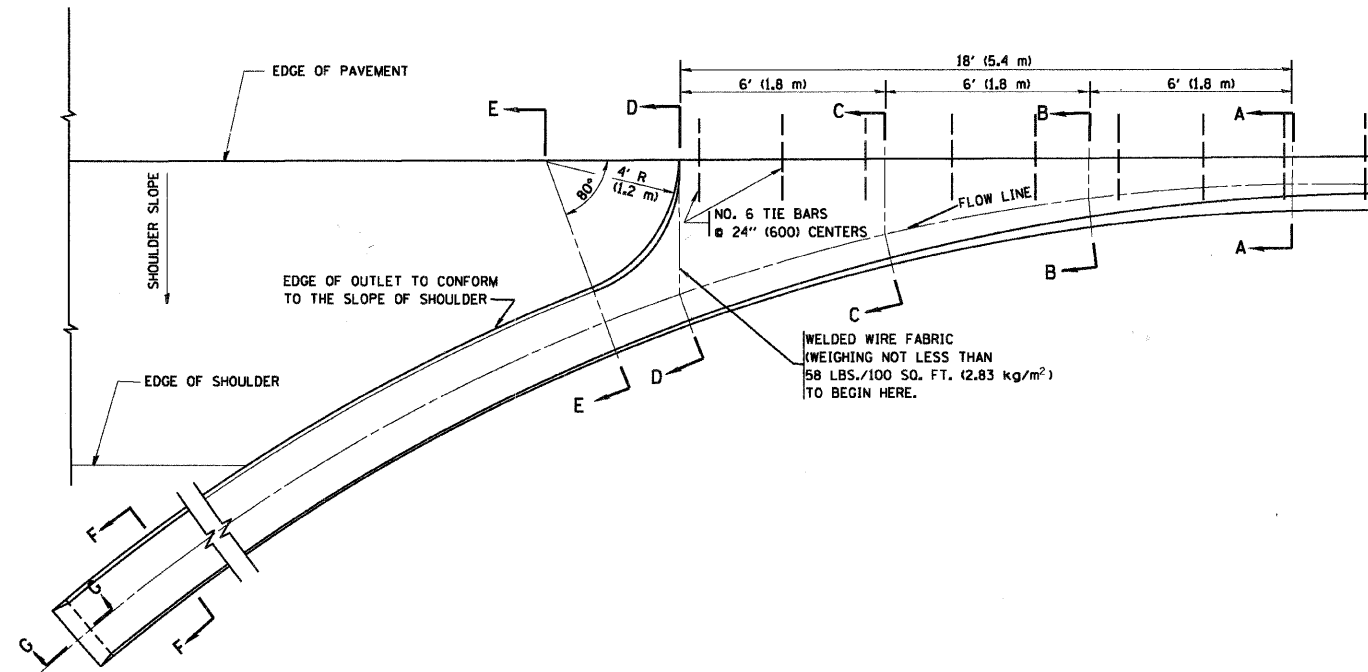
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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - JNK	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

DRAINAGE DETAILS

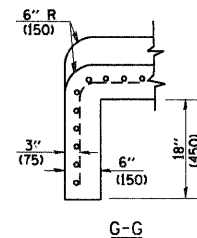
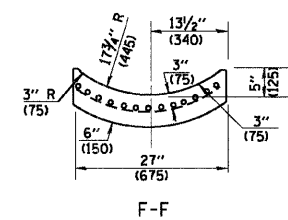
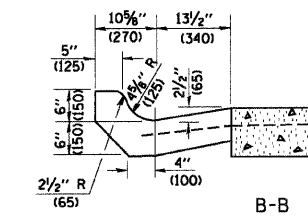
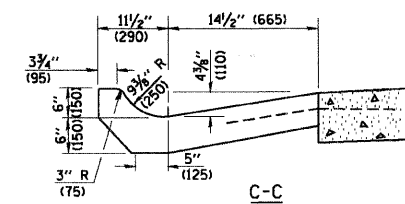
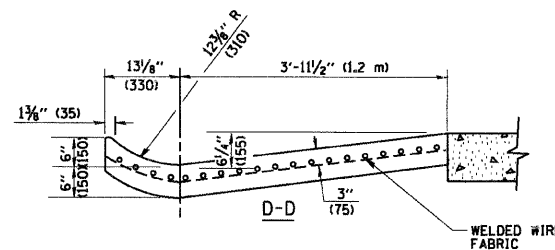
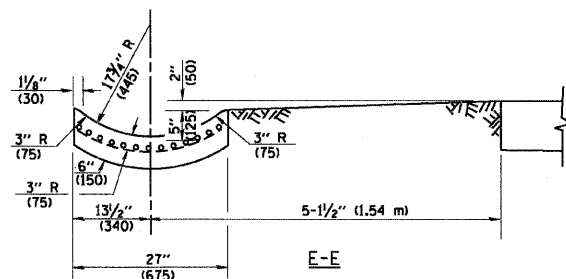
SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	35
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	



A-A *

* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



GENERAL NOTES

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24" (600) CENTERS UNLESS OTHERWISE SHOWN.

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

QUANTITIES

FOR SECTION A-A TO E-E AND CURTAIN WALL =
 1.25 CU. YDS. (0.96 m³) CLASS S1 CONCRETE (OUTLET) FOR 9" (225) PAV'T.
 1.27 CU. YDS. (0.96 m³) CLASS S1 CONCRETE (OUTLET) FOR 10" (250) PAV'T.
 FOR SECTION F-F =
 0.045 CU. YDS. (0.03 m³) CLASS S1 CONCRETE PER FT. (M).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



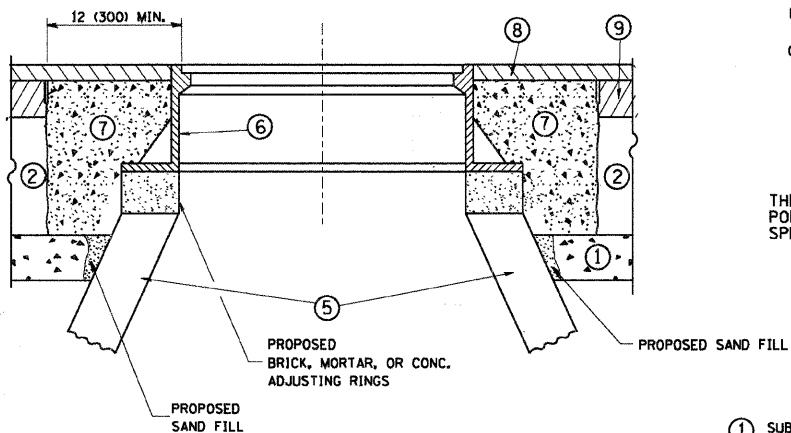
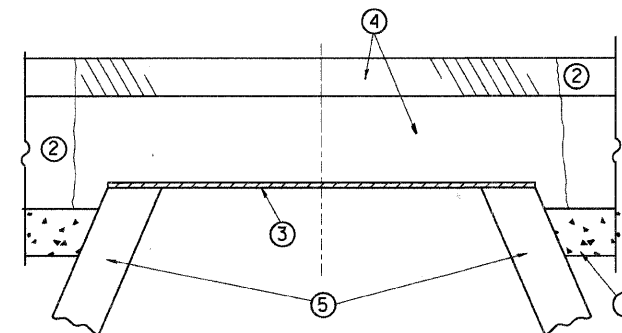
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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED -	REVISED -
	DATE = 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**OUTLET FOR CONCRETE
CURB AND GUTTER**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 36
BD600-01 (BD-03)		CONTRACT NO. 63144		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS S1 CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS S1 CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

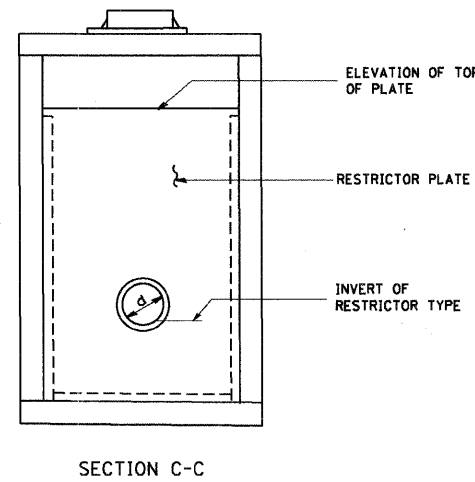
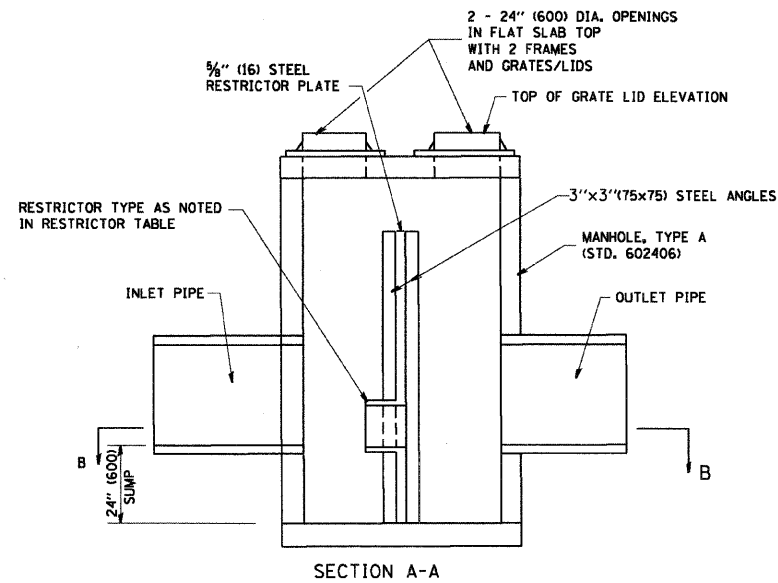
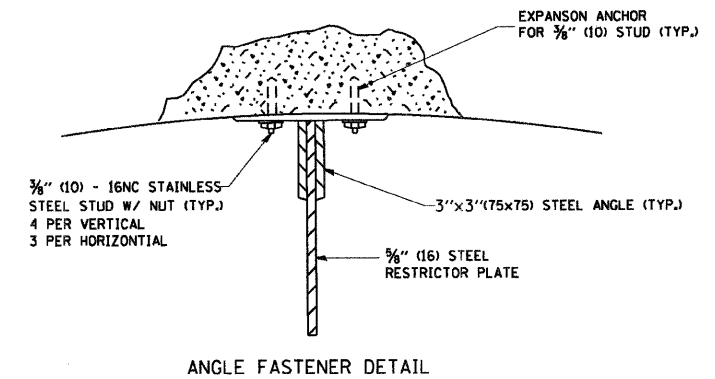
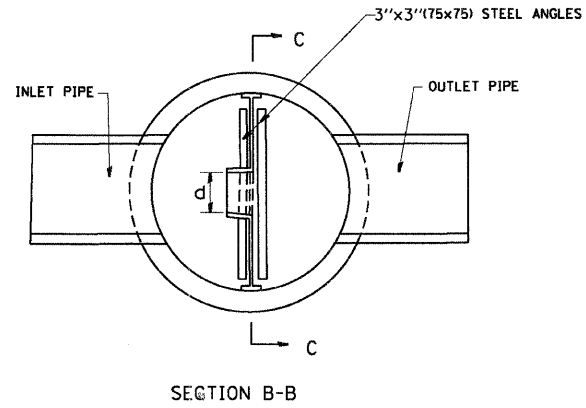
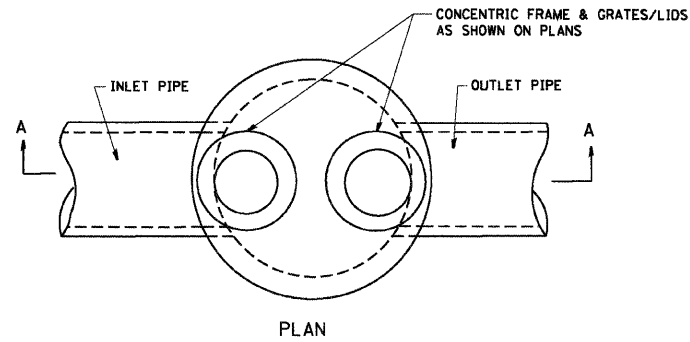
<p>SEC Group, Inc. Engineering Surveying Planning Landscape Architecture McHenry • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = chertke	DESIGNED -	REVISED -
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	PLOT DATE = 2/13/2009	CHECKED -	REVISED -
		DATE = 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

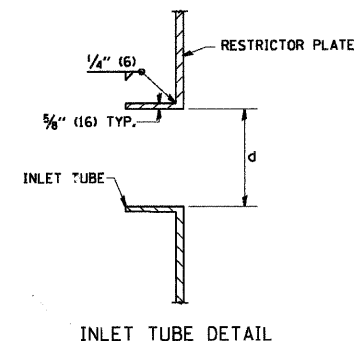
**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

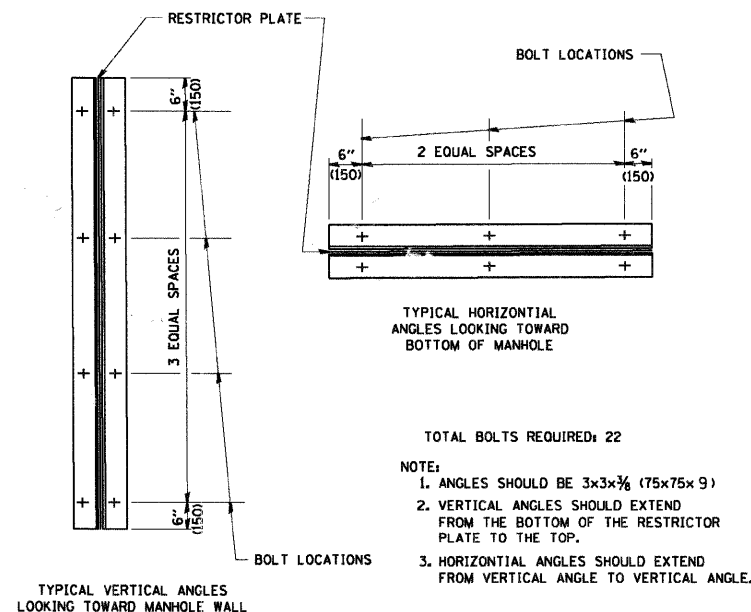
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	37
BD600-03 (BD-8)		CONTRACT NO. 63144		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
 2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
 3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 M)-DIAMETER, TYPE I FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
10+00.0	6'	T1 CL	3	12"	902.67	905.50



- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 3x3x3/8 (75x75x9)
 2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
 3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

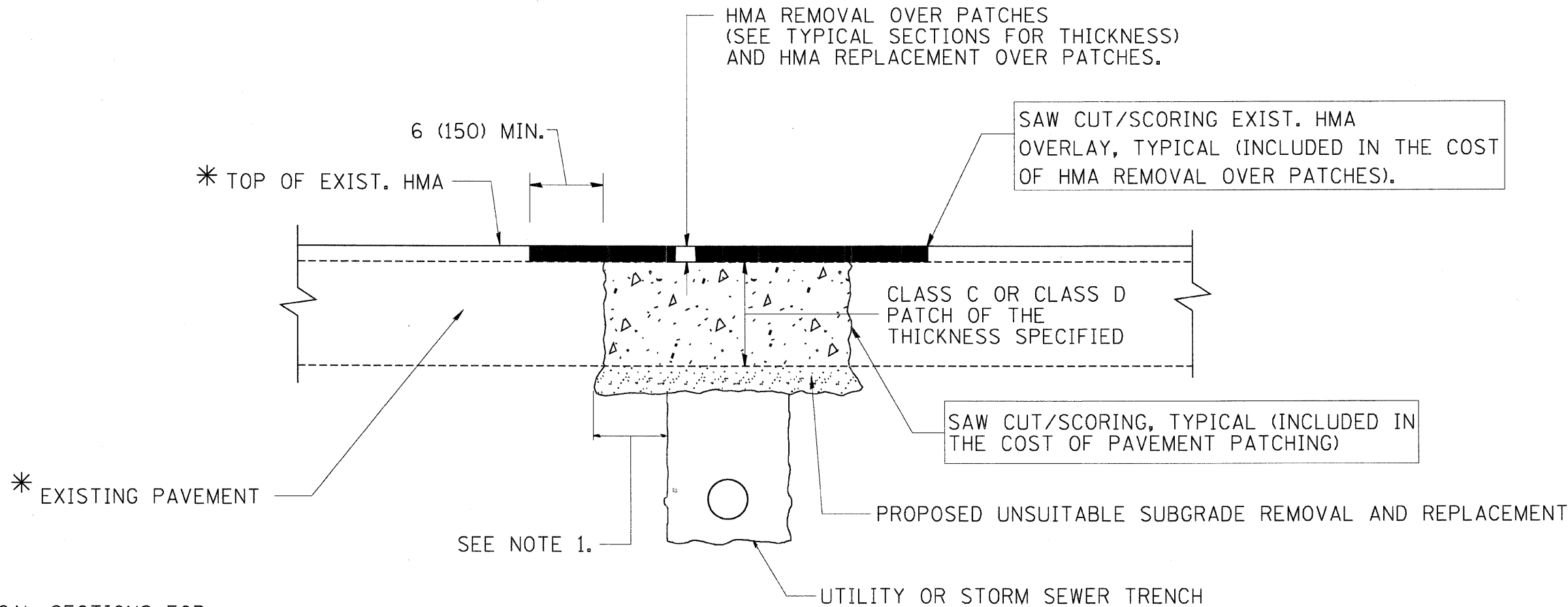
RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENFRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENFRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

<p>SEC Group, Inc. Engineering Surveying Planning Landscape Architecture McHenry • Yorkville • New Lenox • Chicago www.secgroupinc.com</p>	USER NAME = charlie	DESIGNED -	REVISED -	MCHENRY COUNTY DIVISION OF TRANSPORTATION	MANHOLE WITH RESTRICTOR PLATE	F.A. RTE. =	SECTION =	COUNTY =	TOTAL SHEETS =	SHEET NO. =
	PLOT SCALE = N.T.S.	DRAWN -	REVISED -			0116	08-00327-01-CH	MCHENRY	56	38
	PLOT DATE = 2/13/2009	CHECKED -	REVISED -			BD600-04 (BD-12)		CONTRACT NO. 63144		
		DATE = 2/13/09	REVISED -			SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

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C-01-099-06
080154-ds7-06.dgn



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



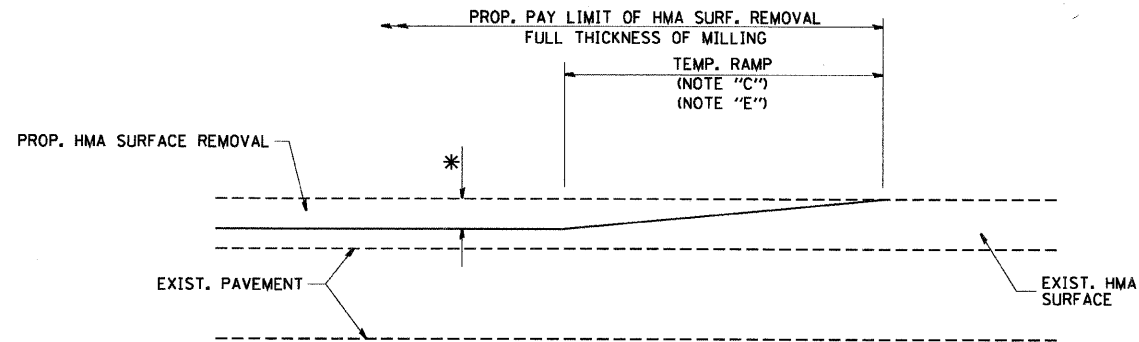
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PLOT DATE = 2/13/2009	CHECKED -	REVISED -
	DATE = 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

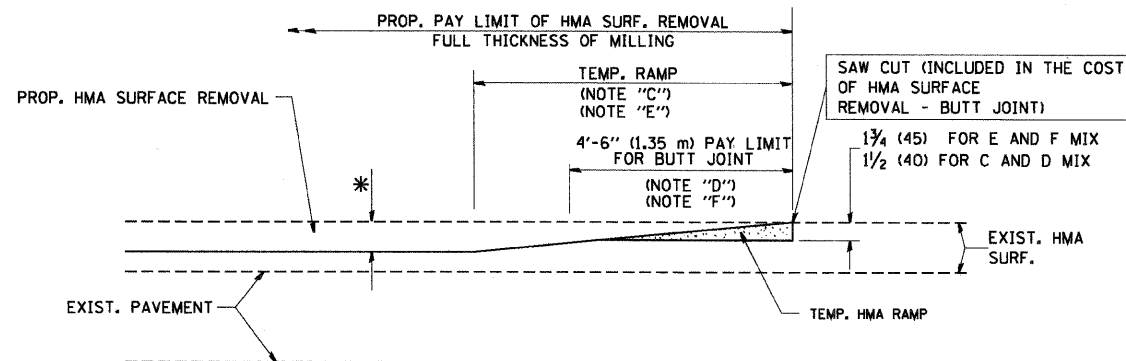
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BD400-04 (BD-22)		CONTRACT NO. 63144		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



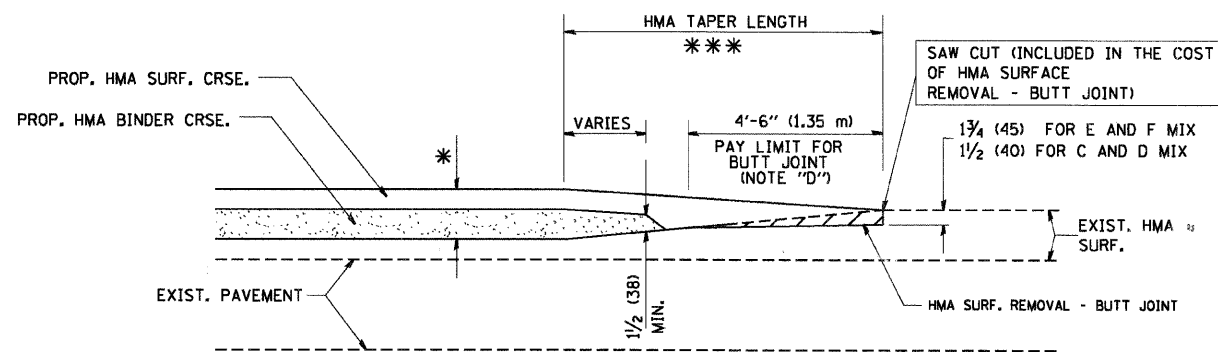
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



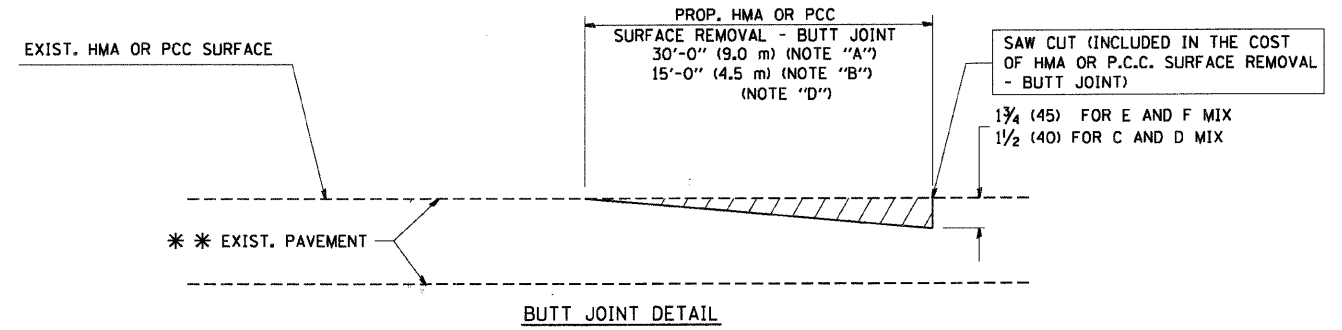
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

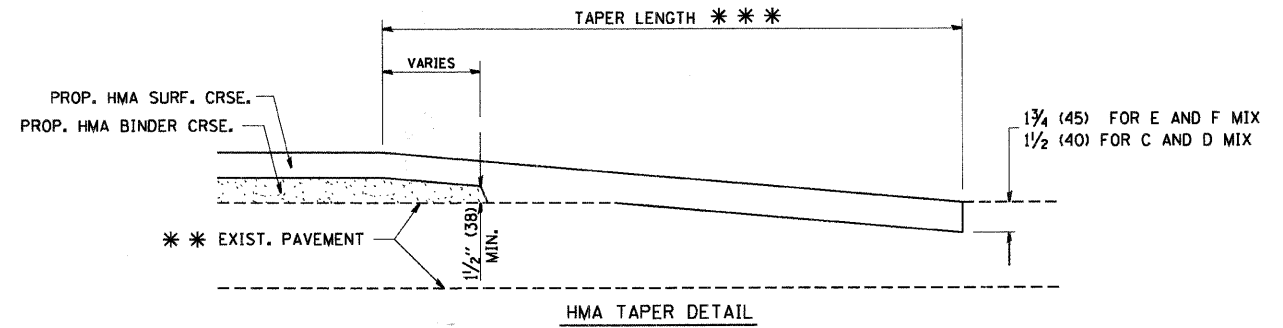


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



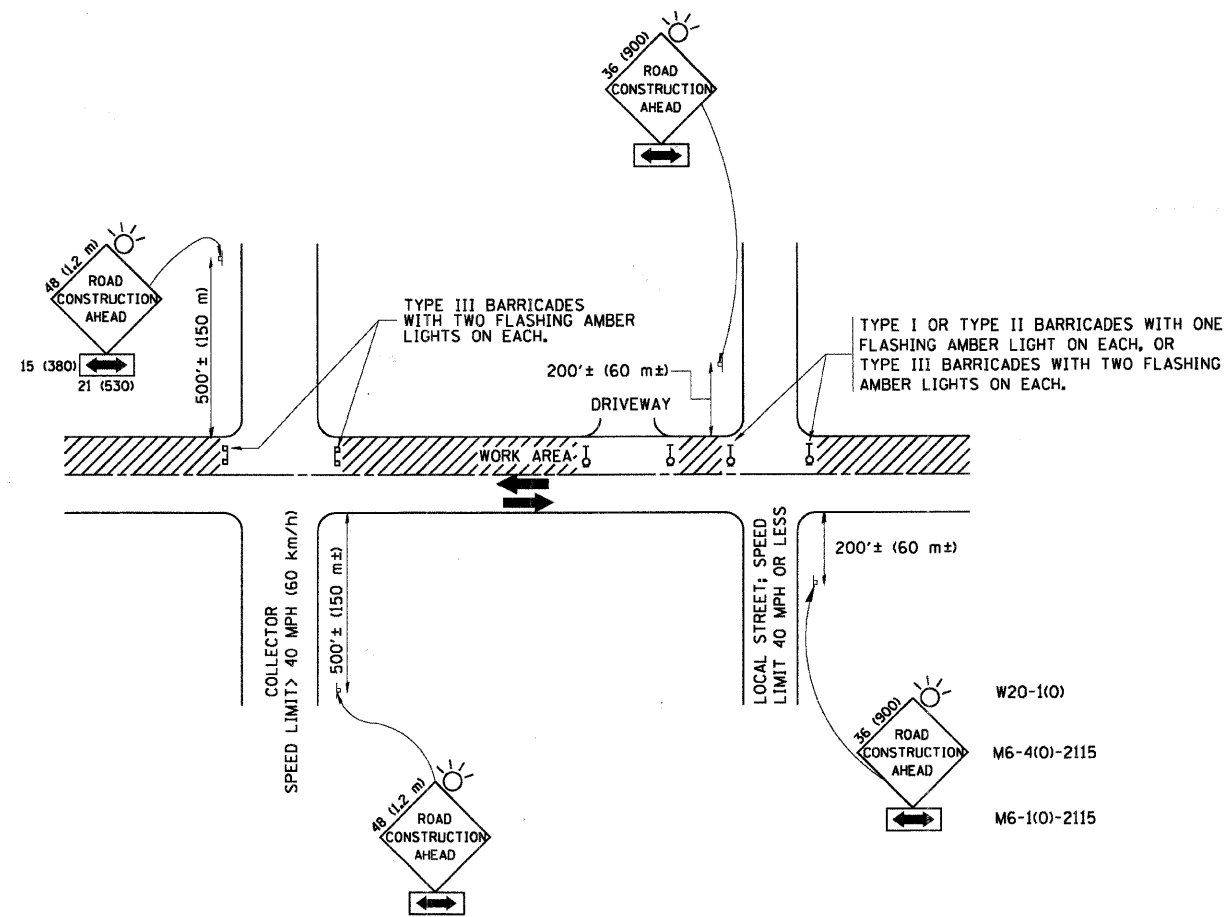
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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED -	REVISED -
	DATE = 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: N.T.S. SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	40
BD400-05 BD32		CONTRACT NO. 63144		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



W20-1(0)
M6-4(0)-2115
M6-1(0)-2115

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.



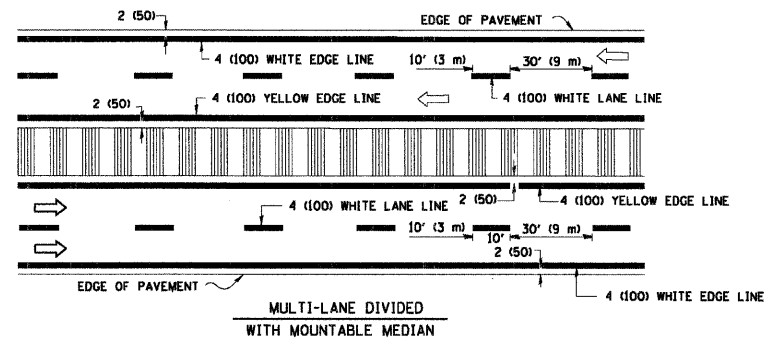
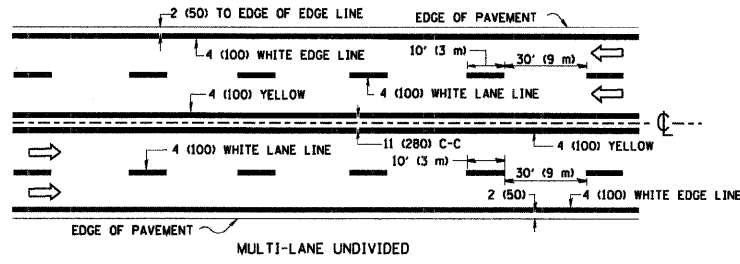
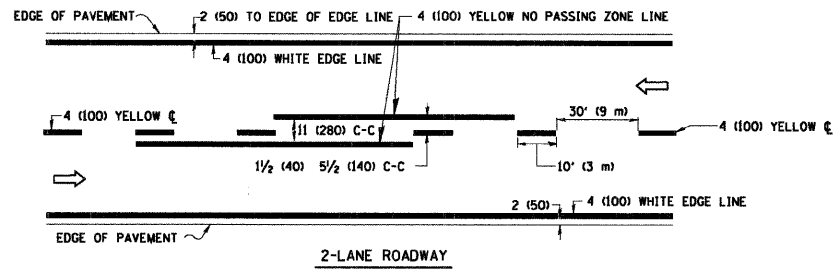
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PLOT SCALE = N.T.S.	DRAWN -	REVISED -
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MCHENRY COUNTY
DIVISION OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

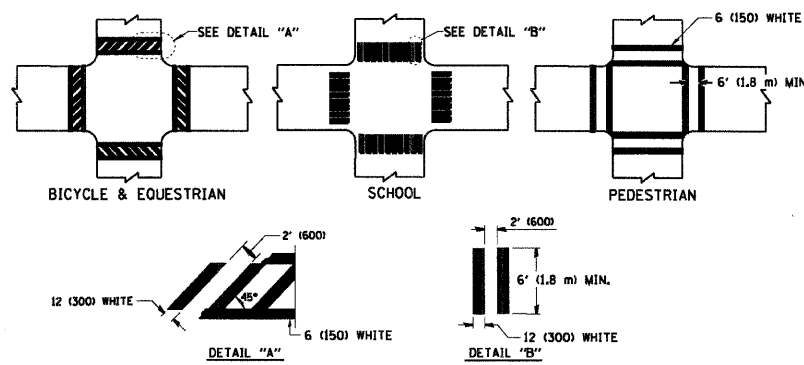
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 63144		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

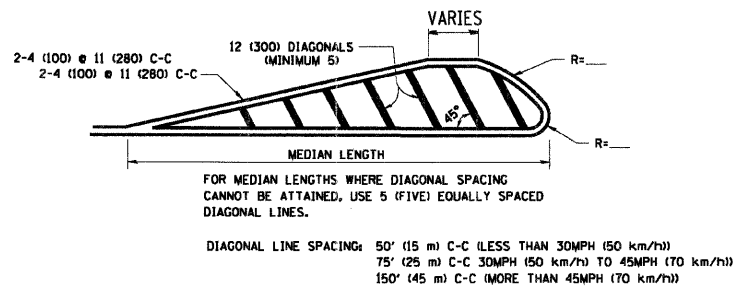
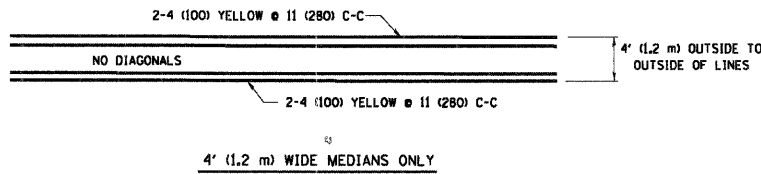


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

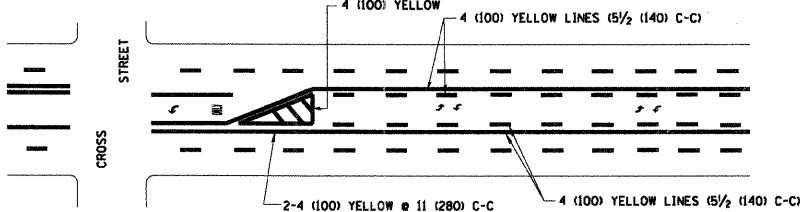
TYPICAL LANE AND EDGE LINE MARKING



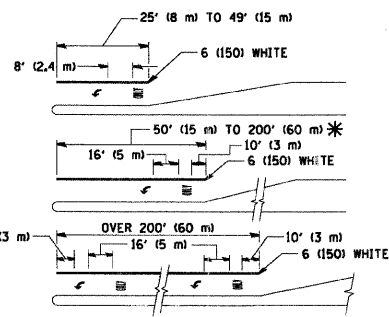
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

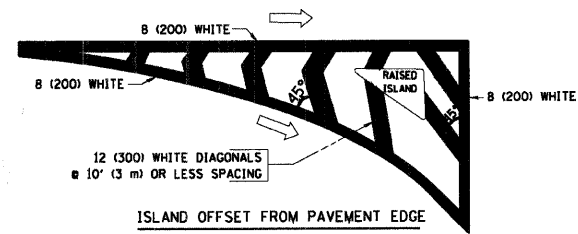


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²); AREA = 20.8 SQ. FT. (1.9 m²)

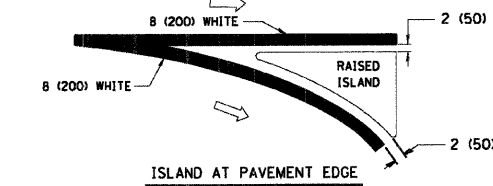
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

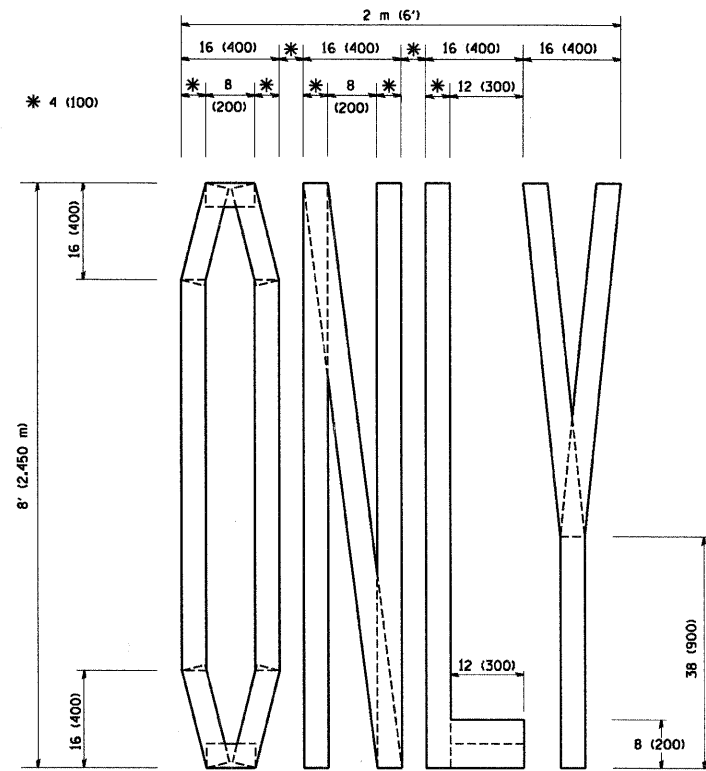


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PLOT DATE = 2/13/2009	CHECKED -	REVISED -
	DATE = 2/13/09	REVISED -

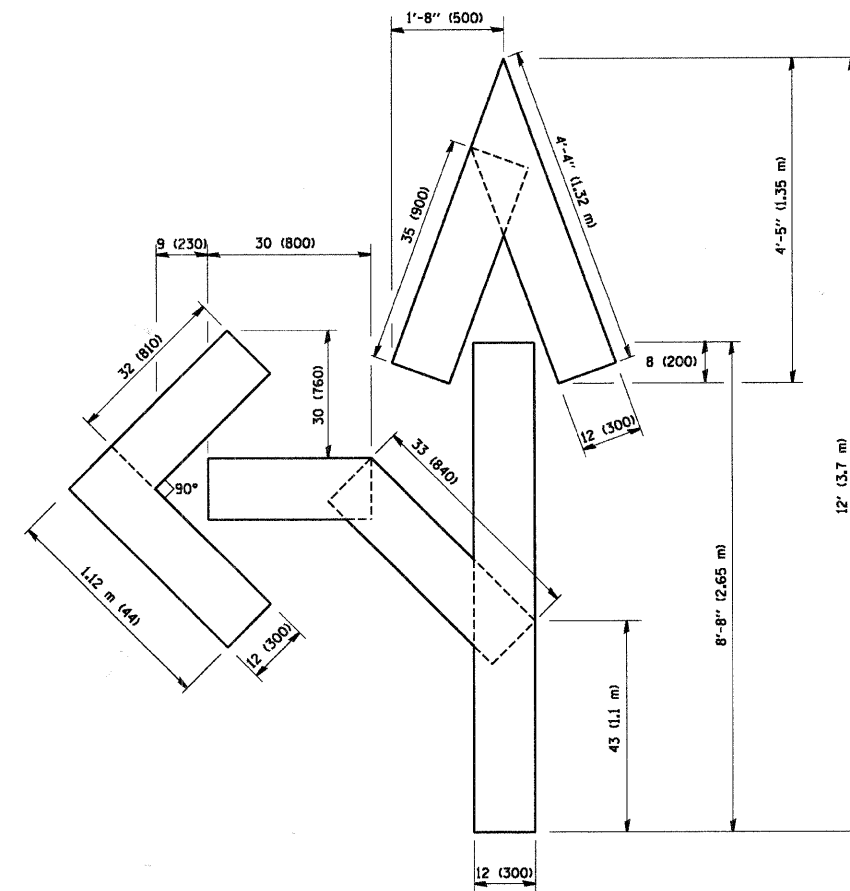
MCHENRY COUNTY DIVISION OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

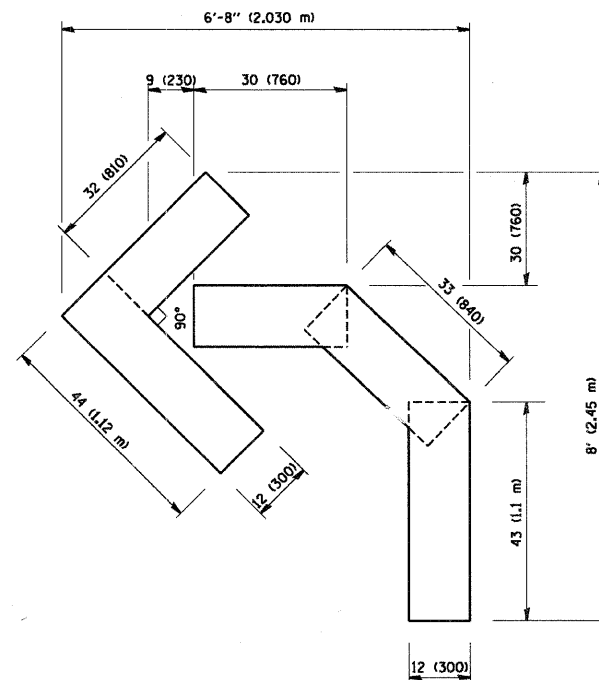
F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 42
SCALE: N.T.S.		CONTRACT NO. 63144		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.



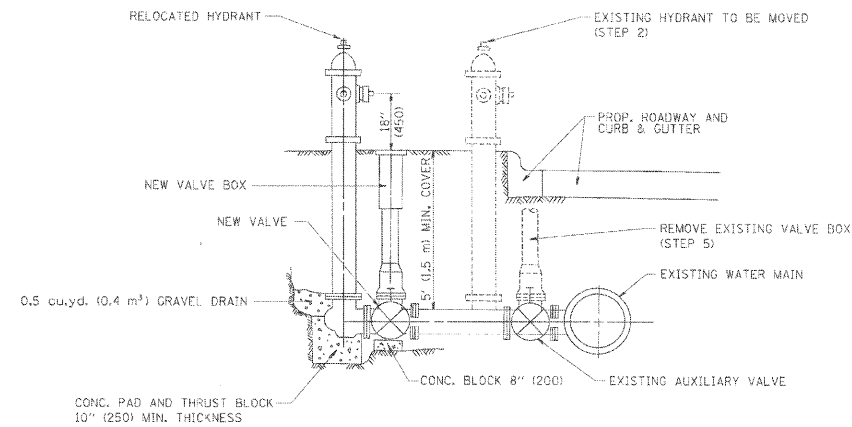
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PLOT DATE = 2/13/2009	CHECKED -	REVISED -
	DATE = 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

**PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING**

SCALE: N.T.S.	SHEET NO.	OF SHEETS	STA.	TO STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	43
TC-16		CONTRACT NO. 63144		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



- SEQUENCE OF CONSTRUCTION**
1. CITY TO CLOSE EXISTING VALVE.
 2. REMOVE EXISTING HYDRANT.
 3. INSTALL HYDRANT EXTENSION AND NEW VALVE.
 4. CHLORINATE HYDRANT, VALVE AND EXTENSION BY SWABBING.
 5. RELOCATE EXISTING HYDRANT.
 6. CONTACT CITY TO CHARGE HYDRANT AND INSPECT FOR LEAKS.
 7. BACKFILL.
 8. CITY TO FLUSH HYDRANT A MINIMUM OF 24 HOURS AFTER #5 (ONLY CITY TO OPERATE VALVE TO LIVE MAIN).
 9. CITY TO FLUSH AND CONTRACTOR TO OBTAIN WATER SAMPLE FOR BACTERIOLOGICAL TESTING.
 10. CITY TO TURN ON HYDRANT UPON NEGATIVE WATER TEST RESULTS ARE RECEIVED. CONTRACTOR TO REMOVE OLD BOX.

FIRE HYDRANT TO BE RELOCATED

NOTE:
ALL MANHOLES MUST BE DAMP PROOF PER IDOT SPECIFICATIONS 1080 "WATER PROOFING MATERIALS"

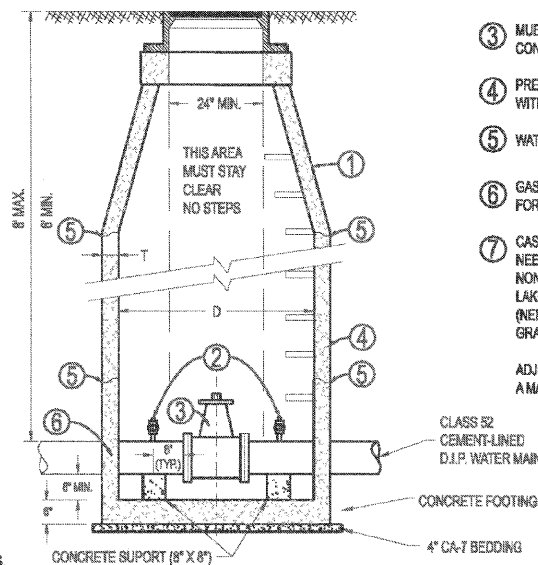
ALL LIDS TO BE USED ON WATER SYSTEM STRUCTURE SHALL BEAR THE WORDS "CITY OF CRYSTAL LAKE WATER".

DRAIN FOR VALVE VAULT SHALL BE CONSTRUCTED ONLY WHEN SHOWN ON THE PLANS.

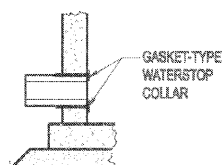
FRAME AND RING(S) TO BE SET IN MORTAR BED OR SEALED WITH A PREFORMED BITUMEN SEAL (E-Z-STICK OR APPROVED EQUAL).

FOR PRESSURE CONNECTION, SEE DETAIL UW-2.

DIAMETER OF WATER MAIN	D	T
8 INCHES AND OVER	5 FT.	6"



- 1 CONCENTRIC CONES REQUIRED
 - 2 1" (PREFERRED, 3/4" MIN.) MUELLER, A.Y. McDONALD, OR FORD CORPORATION STOP EACH SIDE OF VALVE MUST BE A COMPRESSION FITTING.
 - 3 MUELLER 2380-23-9020 OR AMERICAN FLOW CONTROL 2500-1 RESILIENT SEAT GATE VALVE
 - 4 PRECAST CONCRETE MANHOLE SECTIONS WITH A MINIMUM THICKNESS OF 6"
 - 5 WATERTIGHT JOINT
 - 6 GASKET-TYPE WATERSTOP COLLAR FOR ALL WATER MAIN CONNECTIONS
 - 7 CAST IRON MANHOLE FRAME & COVER SIMILAR TO NEENAH R-1712, OR EQUAL WITH STANDARD DUTY, NON-ROCKING TYPE LIDS WITH "CITY OF CRYSTAL LAKE WATER" CAST INTO LID. (NEENAH R-2636, OR EQUAL MAY BE USED IN GRASSY AREAS)
- ADJUSTING RING HEIGHT NOT TO EXCEED 8" WITH A MAXIMUM OF THREE (3) ADJUSTMENT RINGS.



Approved: City Engineer

Victor C. Ramirez
Victor C. Ramirez, P.E.
Director of Engineering and Building

Drawing Name

STANDARD VALVE VAULT

Drawing Number

UW-01

Date: 4/15/2007

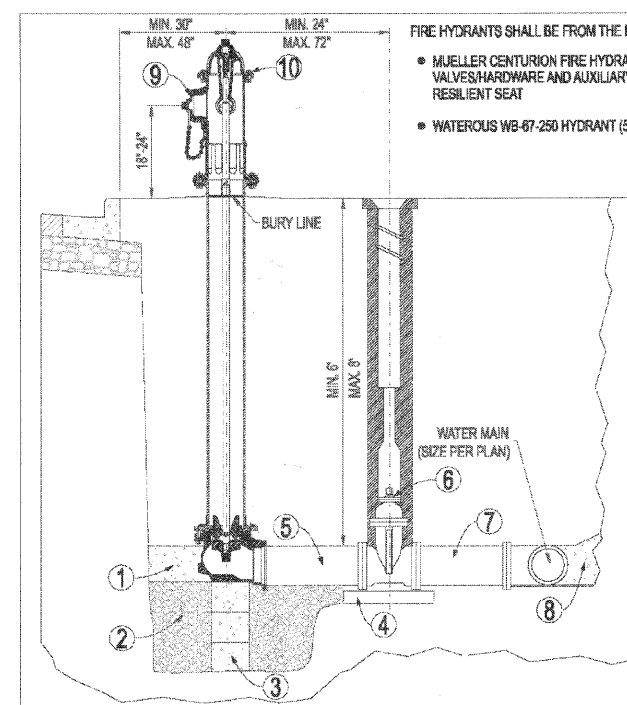
Drawn: EM

Checked: JN



ENGINEERING AND BUILDING DIVISION

UW-01



FIRE HYDRANTS SHALL BE FROM THE FOLLOWING LIST, AS APPROVED BY THE CITY:

- MUELLER CENTURION FIRE HYDRANT, OPT-004 (5 1/4" BARREL) WITH MUELLER VALVES/HARDWARE AND AUXILIARY VALVE 8" GATE VALVE NO. 2360-23-9020 MODIFIED WEDGE RESILIENT SEAT
- WATEROUS WB-67-250 HYDRANT (5 1/4" PACER) WITH SERIES 2500-1 RESILIENT WEDGE GATE VALVE

- 1 PROVIDE CLASS SI CONCRETE BASE AND BLOCKING AGAINST UNDISTURBED EARTH
- 2 DRAIN SUMP TO BE 3/4 CUBIC YARD OF 3/4" WASHED STONE
- 3 CONCRETE SUPPORT
- 4 CONCRETE BLOCK OR BRICK SUPPORT
- 5 PIPE AS REQUIRED TO MAINTAIN 24" SEPARATION AS SHOWN (DIRECT MECHANICAL JOINT CONNECTION IF APPROVED BY CITY)
- 6 RUBBER VALVE BOX STABILIZER
- 7 USE "COR-TEN" STEEL TIE RODS BETWEEN AUXILIARY VALVE AND WATER MAIN (STAINLESS STEEL MAY BE REQUIRED BY THE CITY ENGINEER). ANY DISTANCE GREATER THAN 30" SHALL BE RODDED TO MEGA-LUG FLANGE. NO COUPLINGS ARE PERMITTED IN RODS.
- 8 CONCRETE BLOCKING CAST IN PLACE 3000 P.S.I.
- 9 4 1/2" PORT TO FACE PAVEMENT OR AS DIRECTED BY CITY ENGINEER
- 10 ALL NEWLY INSTALLED HYDRANTS MUST BE TOP COATED WITH RUST-OLEUM FIRE HYDRANT ENAMEL (COLOR = FIRE HYDRANT RED)
- 11 MIN. 48" BETWEEN HYDRANT AND ANY VERTICAL OBSTRUCTIONS.
- 12 MIN. 72" BETWEEN HYDRANT AND ANY LANDSCAPING WITH A MATURE HEIGHT GREATER THAN 12".
- 13 ALL VALVE AND HYDRANT HARDWARE MUST BE STAINLESS STEEL INCLUDING NUTS, BOLTS, AND WASHERS.

Approved: City Engineer

Victor C. Ramirez
Victor C. Ramirez, P.E.
Director of Engineering and Building

Drawing Name

FIRE HYDRANT

Drawing Number

UW-06

Date: 11/2/2007

Drawn: EM

Checked: JN



ENGINEERING AND BUILDING DIVISION

UW-06



USER NAME = charlke
PLOT SCALE = N.T.S.
PLOT DATE = 2/13/2009

DESIGNED - JRM
DRAWN - CCL
CHECKED - TCH
DATE - 2/13/09

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DIVISION OF TRANSPORTATION**

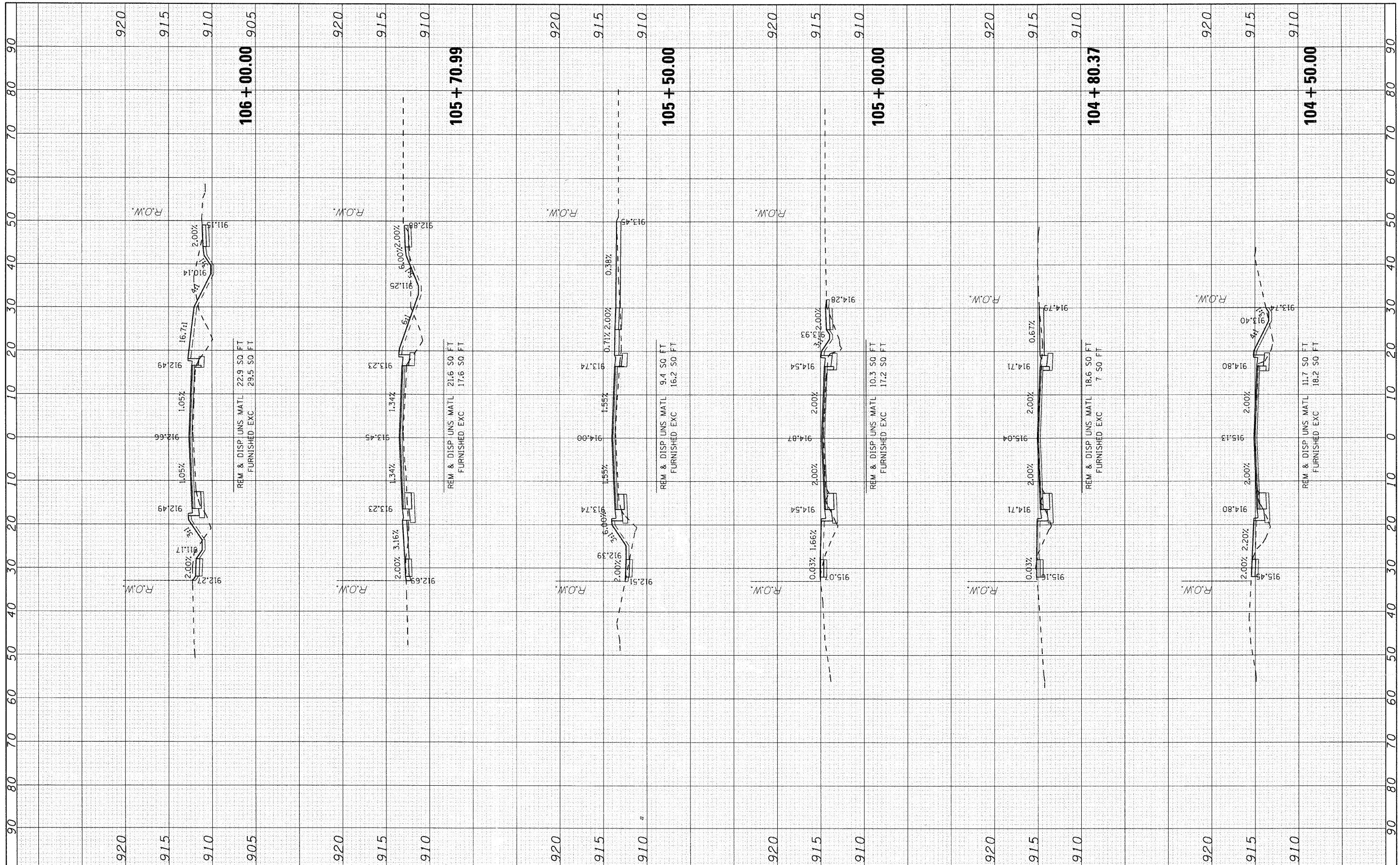
**CITY OF CRYSTAL LAKE
DETAILS**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	

FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		



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	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

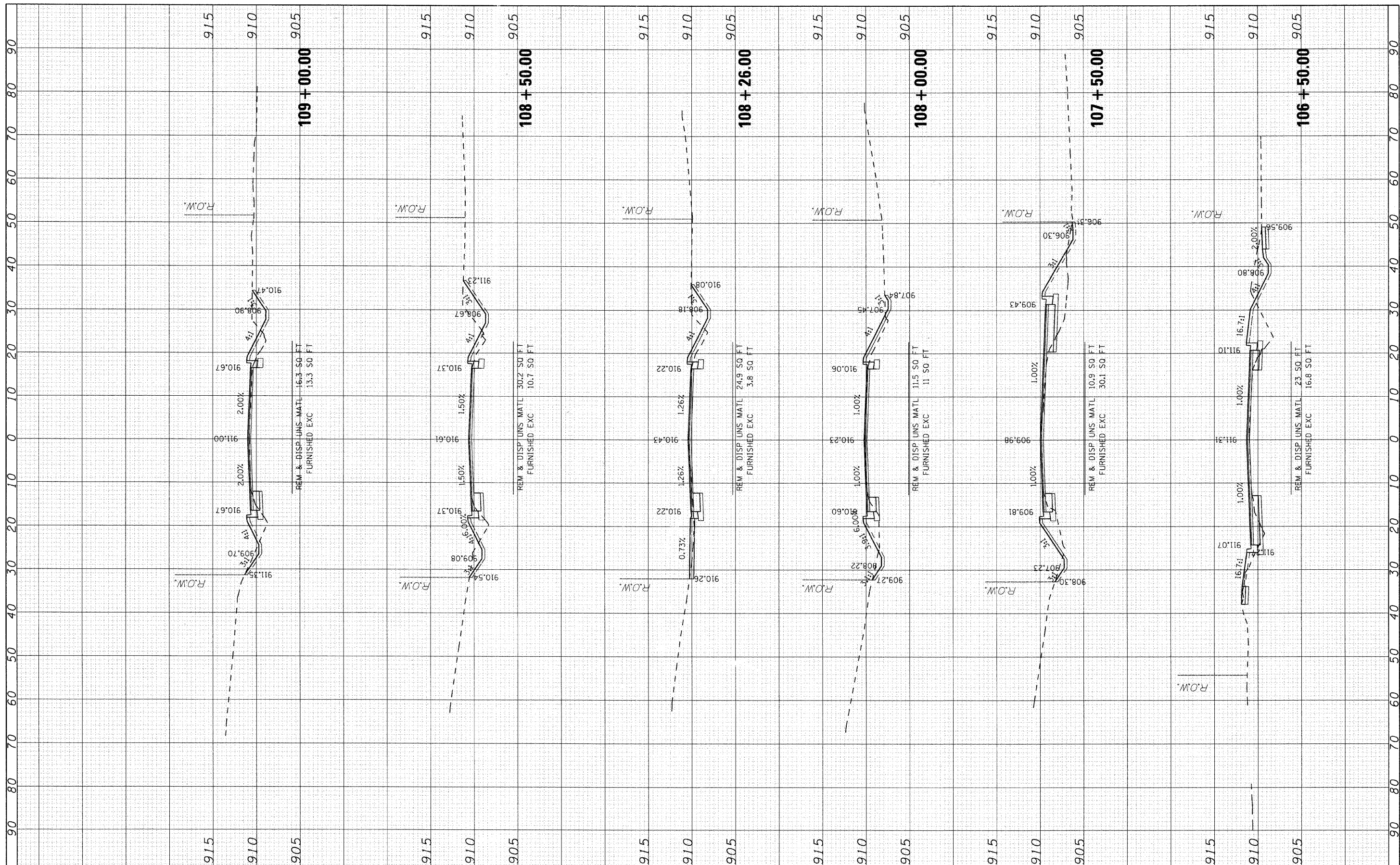
CROSS SECTIONS - CRYSTAL LAKE AVENUE

SCALE: SHEET NO. 1 OF 4 SHEETS STA. 104+50.00 TO STA. 106+00.00

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 45
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		CONTRACT NO. 63144		

FINAL SURVEY	REVISED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY	REVISED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
	CHECKED	



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PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
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**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

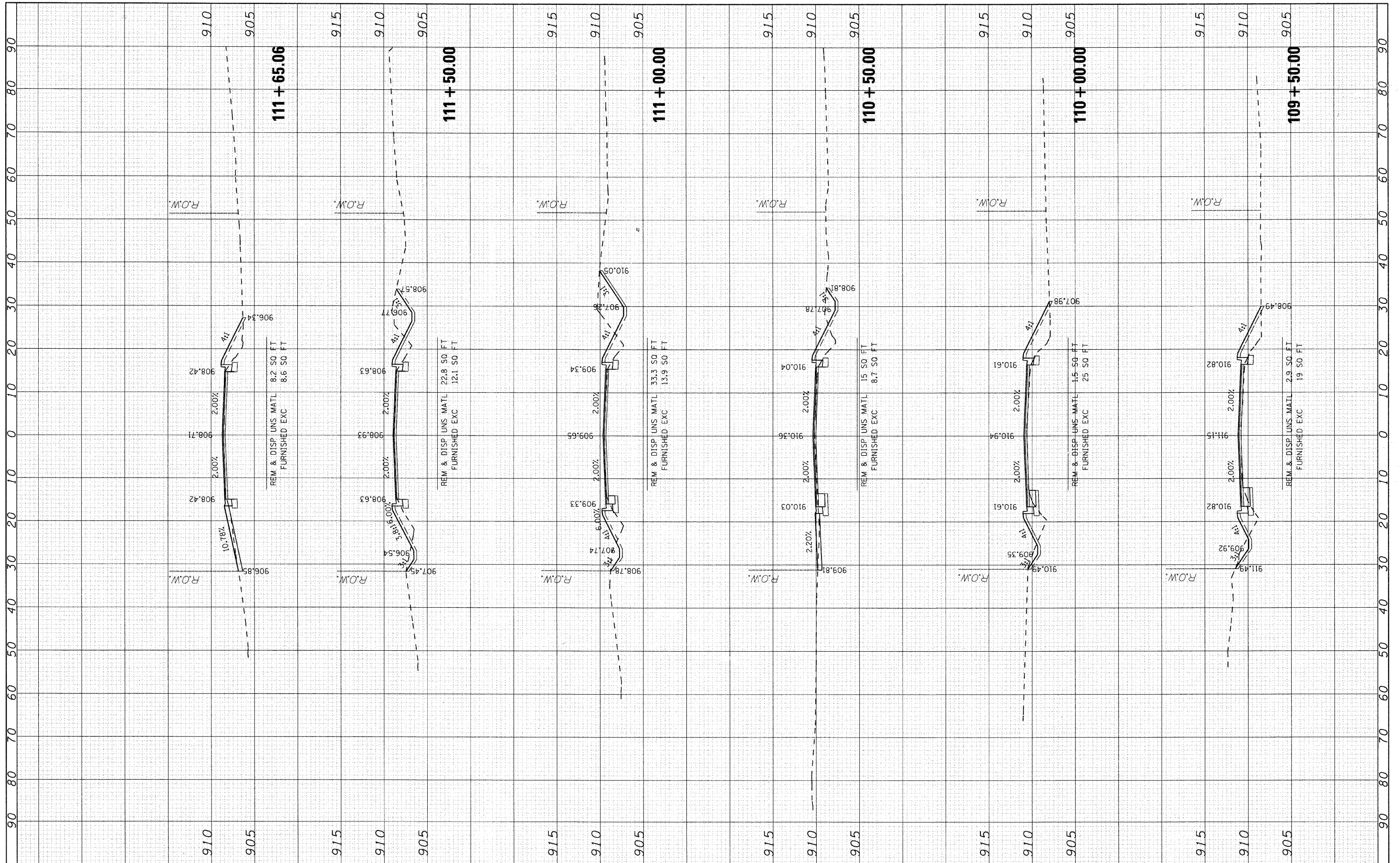
CROSS SECTIONS - CRYSTAL LAKE AVENUE

SCALE: SHEET NO. 2 OF 4 SHEETS STA. 106+50.00 TO STA. 109+00.00

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 46
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 63144				

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
	CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	TEMPLATE	
	AREAS	
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PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
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**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

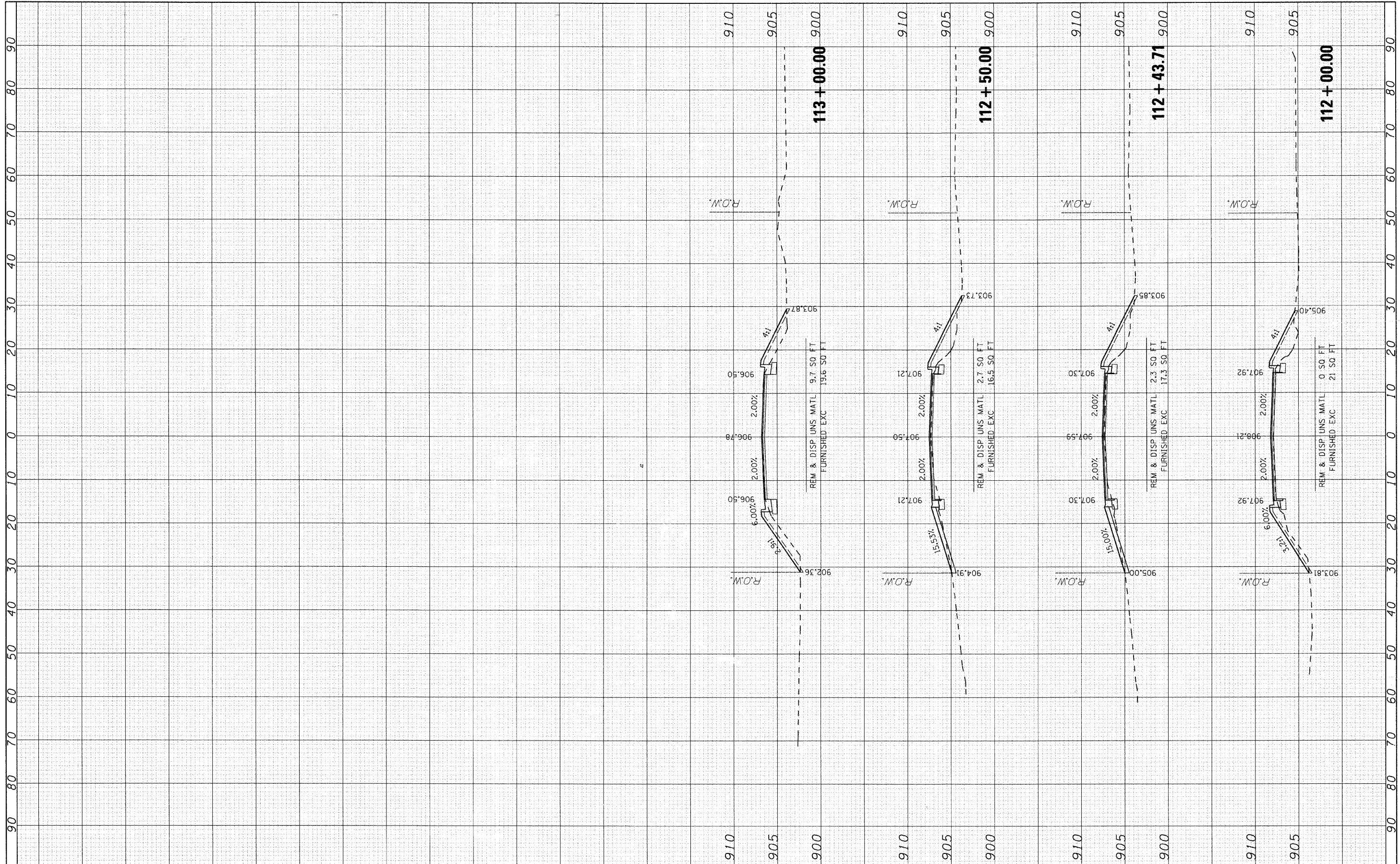
CROSS SECTIONS - CRYSTAL LAKE AVENUE

SCALE: SHEET NO. 3 OF 4 SHEETS STA. 109+50.00 TO STA. 111+65.06

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 47
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	

FINAL SURVEY	BY	DATE
SUBMITTED		
PLOTTED		
NOTE BOOK		
NO.		
AREAS CHECKED		

ORIGINAL SURVEY	BY	DATE
SUBMITTED		
PLOTTED		
NOTE BOOK		
NO.		
AREAS CHECKED		



USER NAME = char-ke	DESIGNED - JRM	REVISED -
PLOT SCALE = 1/8"=1'-0"	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

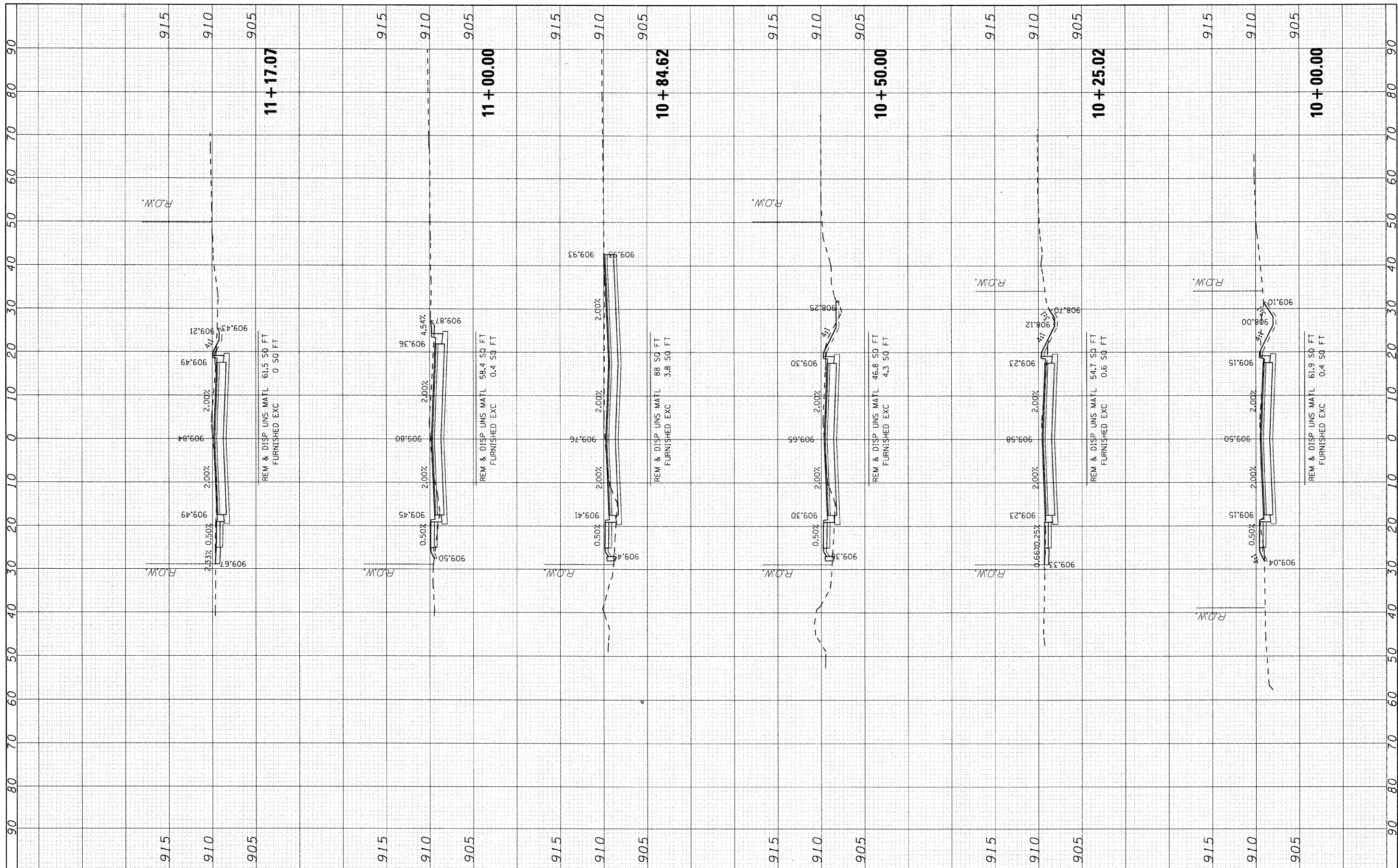
**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

CROSS SECTIONS - CRYSTAL LAKE AVENUE	
SCALE:	SHEET NO. 4 OF 4 SHEETS STA. 112+00.00 TO STA. 113+00.00

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 48
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 63144				

FINAL SURVEY NO.	DATE
EMPHASIZED	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	

ORIGINAL SURVEY NO.	DATE
EMPHASIZED	BY
PLOTTED	
TEMPLATE	
AREAS CHECKED	



USER NAME = char-ke	DESIGNED - JRM	REVISED -
PLOT SCALE = 10'H=5'V	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

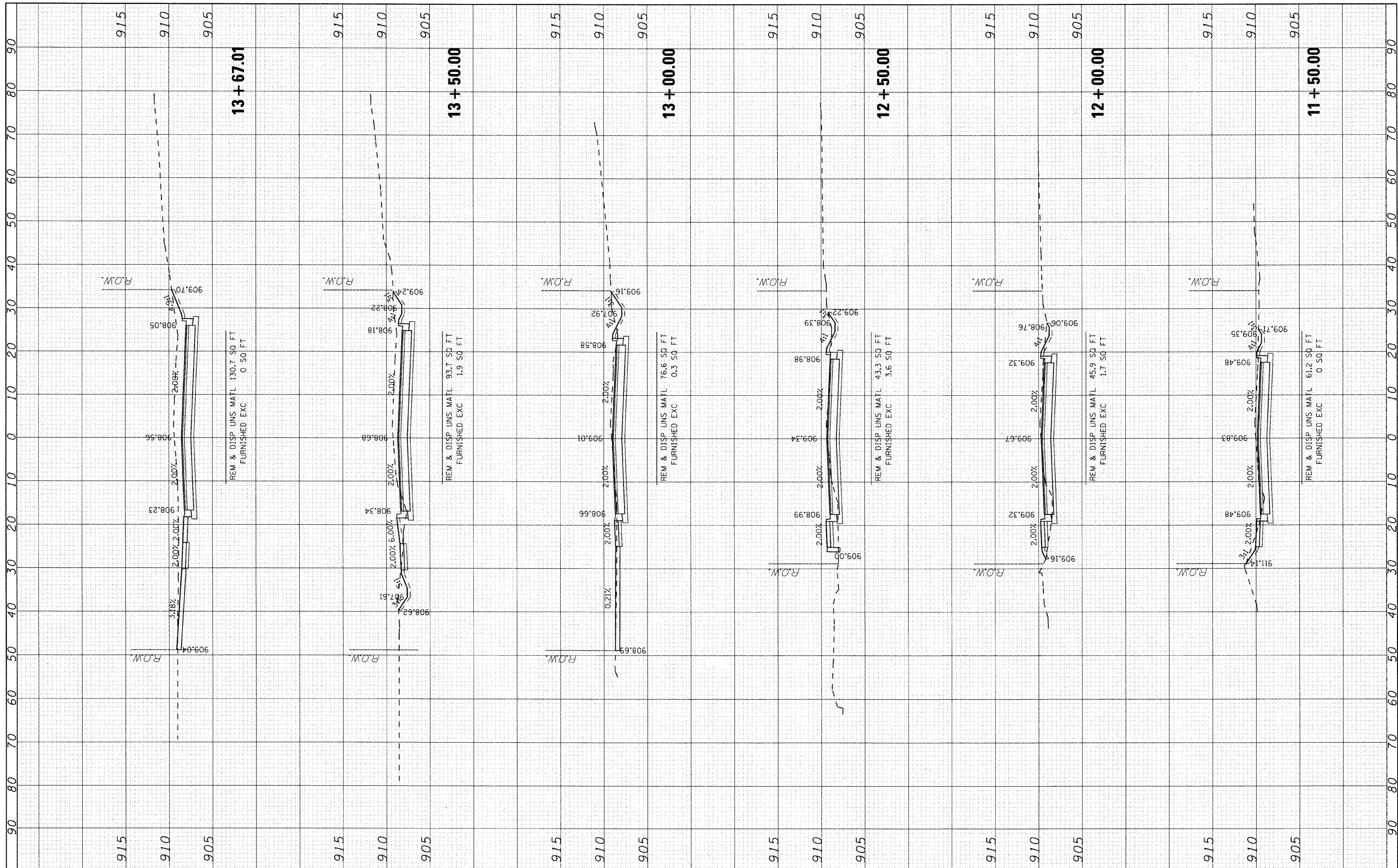
CROSS SECTIONS - PINGREE ROAD

SCALE: 10'H=5'V SHEET NO. 1 OF 8 SHEETS STA. 9+00.00 TO STA. 11+17.07

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	49
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	

FINAL SURVEY NO.	DATE
REVISED SURVEY NO.	DATE
PLOTTED TEMPLATE AREAS CHECKED	
NO.	

ORIGINAL SURVEY NO.	DATE
REVISED SURVEY NO.	DATE
PLOTTED TEMPLATE AREAS CHECKED	
NO.	



USER NAME = char:tkc	DESIGNED - JRM	REVISED -
PLOT SCALE = 10'H=5'V	DRAWN - TCH	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

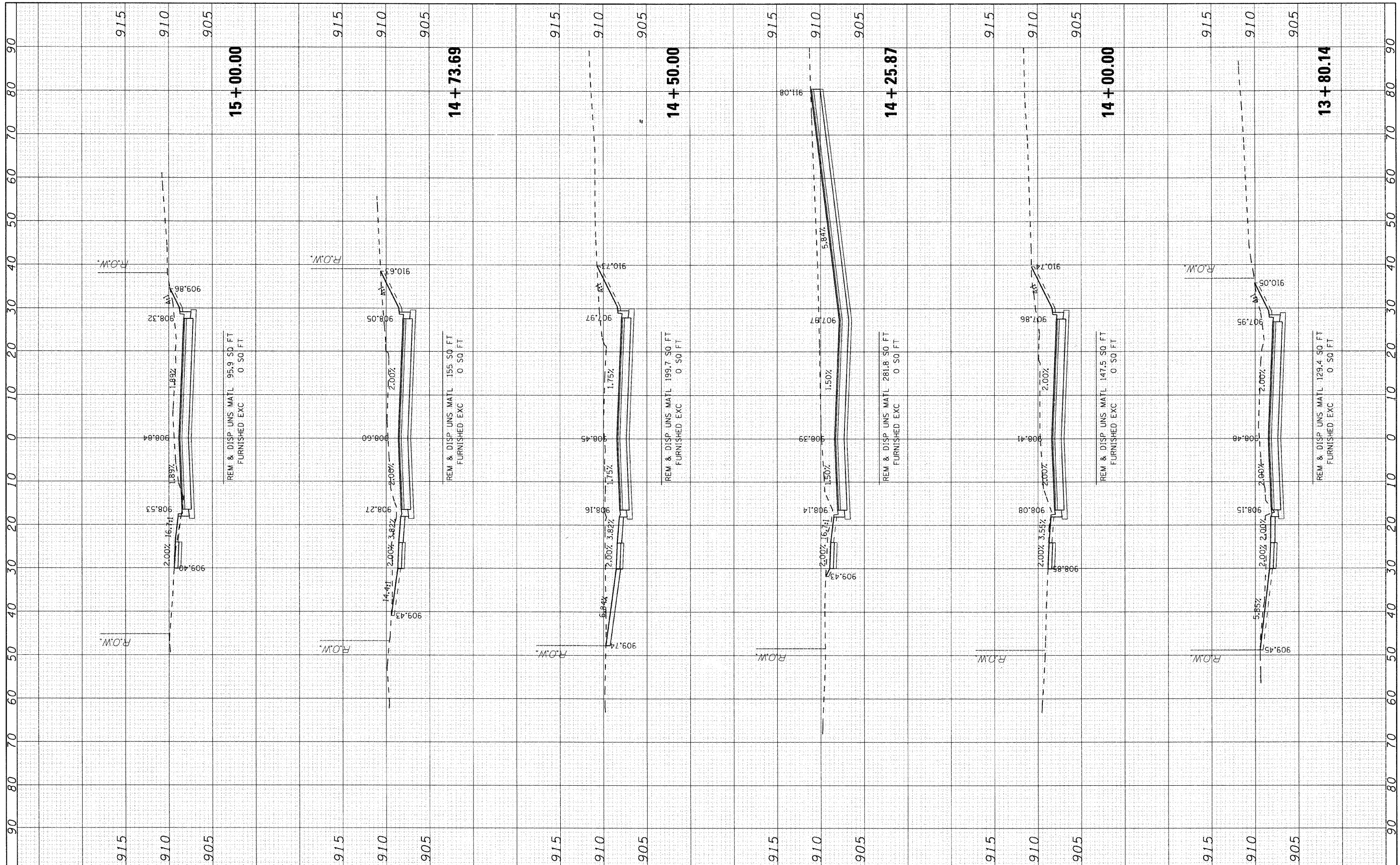
CROSS SECTIONS - PINGREE ROAD

SCALE: 10'H=5'V SHEET NO. 2 OF 8 SHEETS STA. 11+50.00 TO STA. 13+67.01

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	50
FED. ROAD DIST. NO.			CONTRACT NO. 63144	
ILLINOIS FED. AID PROJECT				

FINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
NO.	
AREAS CHECKED	

ORIGINAL SURVEY	DATE
SURVEYED	BY
PLOTTED	
NOTE BOOK	
NO.	
AREAS CHECKED	



USER NAME = char-01a	DESIGNED - JRM	REVISED -
PLLOT SCALE = 10'H=5'V	DRAWN -	REVISED -
PLLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

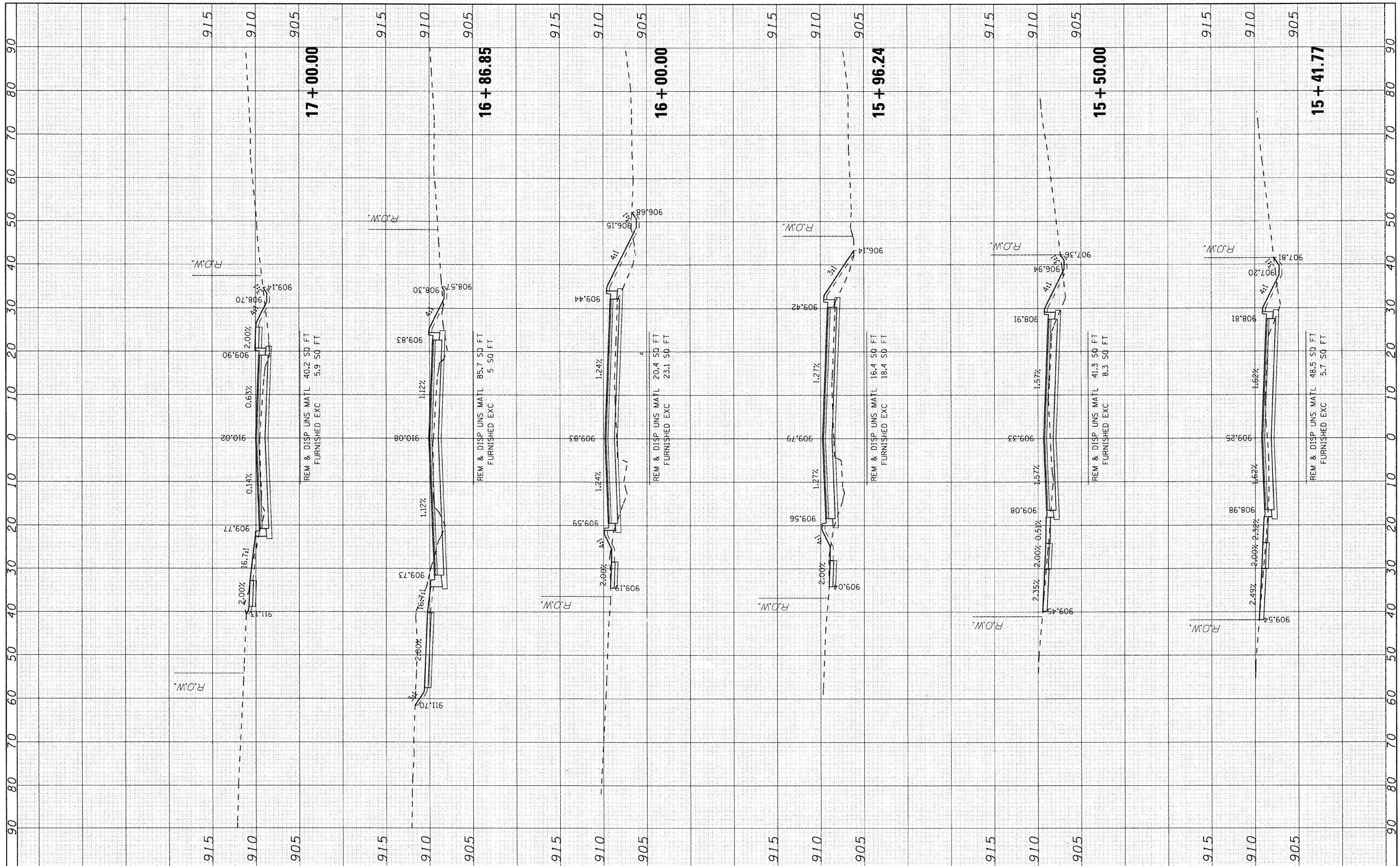
CROSS SECTIONS - PINGREE ROAD

SCALE: 10'H=5'V SHEET NO. 3 OF 8 SHEETS STA. 13+80.14 TO STA. 15+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	51
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	

FINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	AREAS CHECKED	

ORIGINAL SURVEY	SURVEYED	DATE
NOTE BOOK	PLOTTED	BY
NO.	AREAS CHECKED	



USER NAME = char tke
 PLOT SCALE = 18"=H5'V
 PLOT DATE = 2/13/2009

DESIGNED -	JRM	REVISED -	
DRAWN -		REVISED -	
CHECKED -	TCH	REVISED -	
DATE -	2/13/09	REVISED -	

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

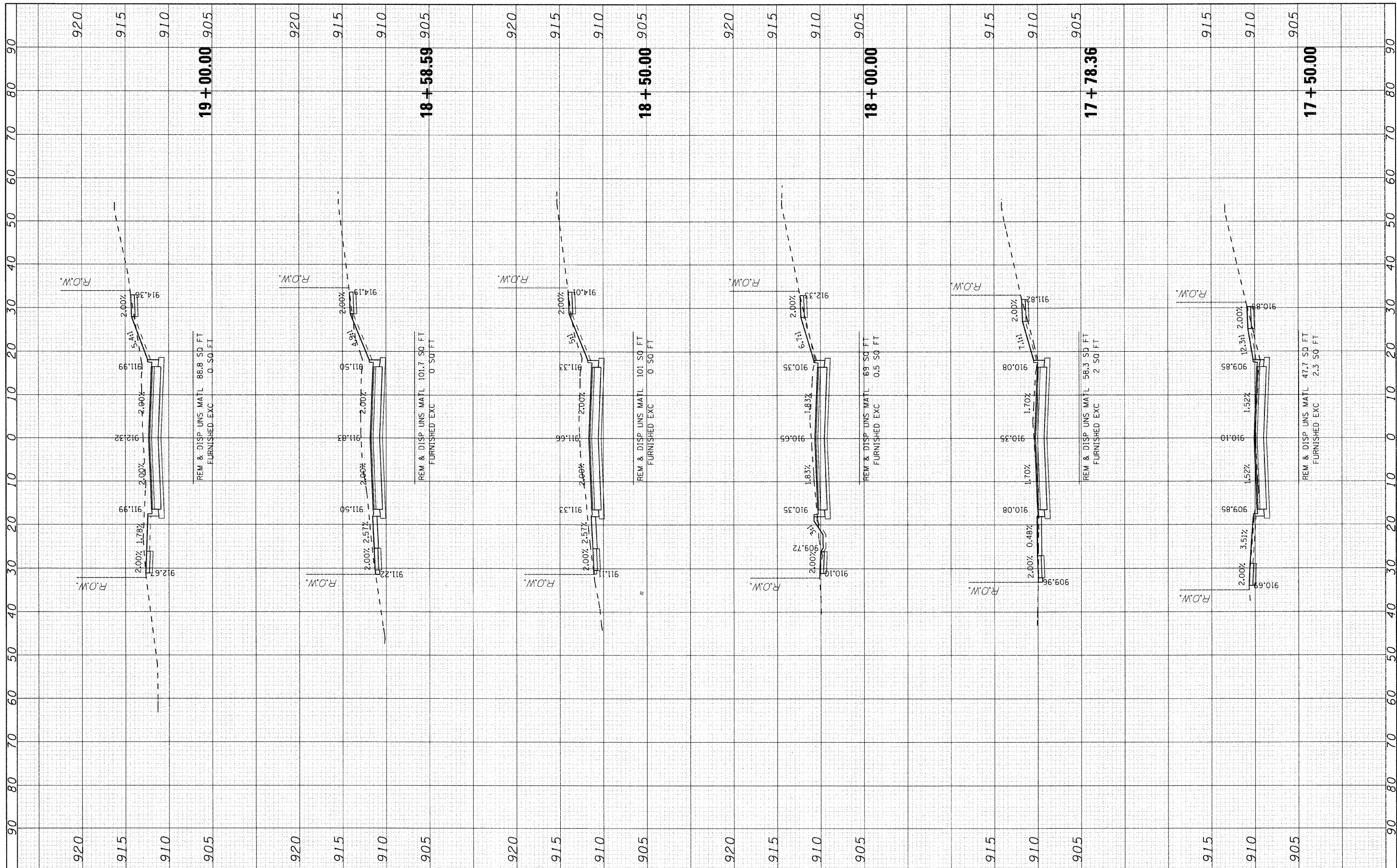
CROSS SECTIONS - PINGREE ROAD

SCALE: 10"=H5'V SHEET NO. 4 OF 8 SHEETS STA. 15+41.77 TO STA. 17+00.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	52
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63144	

FINAL SURVEY NO.	DATE
REVISIONS	BY
NO.	

ORIGINAL SURVEY NO.	DATE
NO.	



REM & DISP UNS MATL 88.8 SQ FT
FURNISHED EXC 0 SQ FT

REM & DISP UNS MATL 101.7 SQ FT
FURNISHED EXC 0 SQ FT

REM & DISP UNS MATL 101 SQ FT
FURNISHED EXC 0 SQ FT

REM & DISP UNS MATL 69 SQ FT
FURNISHED EXC 0.5 SQ FT

REM & DISP UNS MATL 58.3 SQ FT
FURNISHED EXC 2 SQ FT

REM & DISP UNS MATL 47.7 SQ FT
FURNISHED EXC 2.3 SQ FT



USER NAME = char_ske	DESIGNED - JRM	REVISED -
PLOT SCALE = 10'H=5'V	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

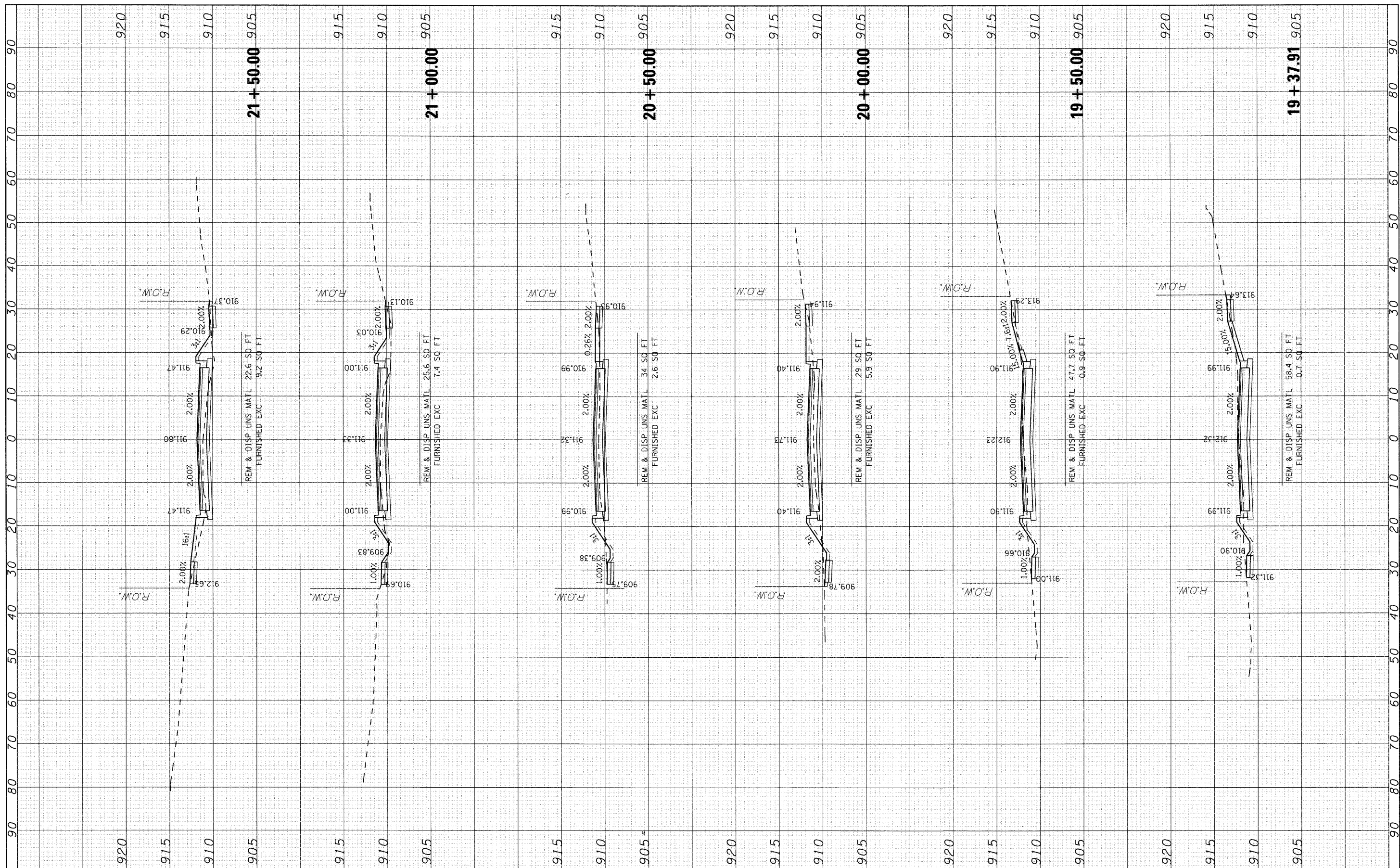
CROSS SECTIONS - TERRA COTTA ROAD

SCALE: 10'H=5'V SHEET NO. 5 OF 8 SHEETS STA. 17+50.00 TO STA. 19+00.00

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 53
CONTRACT NO. 63144				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

FINAL SURVEY	DATE	BY	DATE
SURVEY PLOTTED			
NOTE BOOK			
AREAS CHECKED			
NO.			

ORIGINAL SURVEY	DATE	BY	DATE
SURVEY PLOTTED			
NOTE BOOK			
AREAS CHECKED			
NO.			



USER NAME = char like	DESIGNED - JRM	REVISED -
PLOT SCALE = 10'H=5'V	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
DIVISION OF TRANSPORTATION**

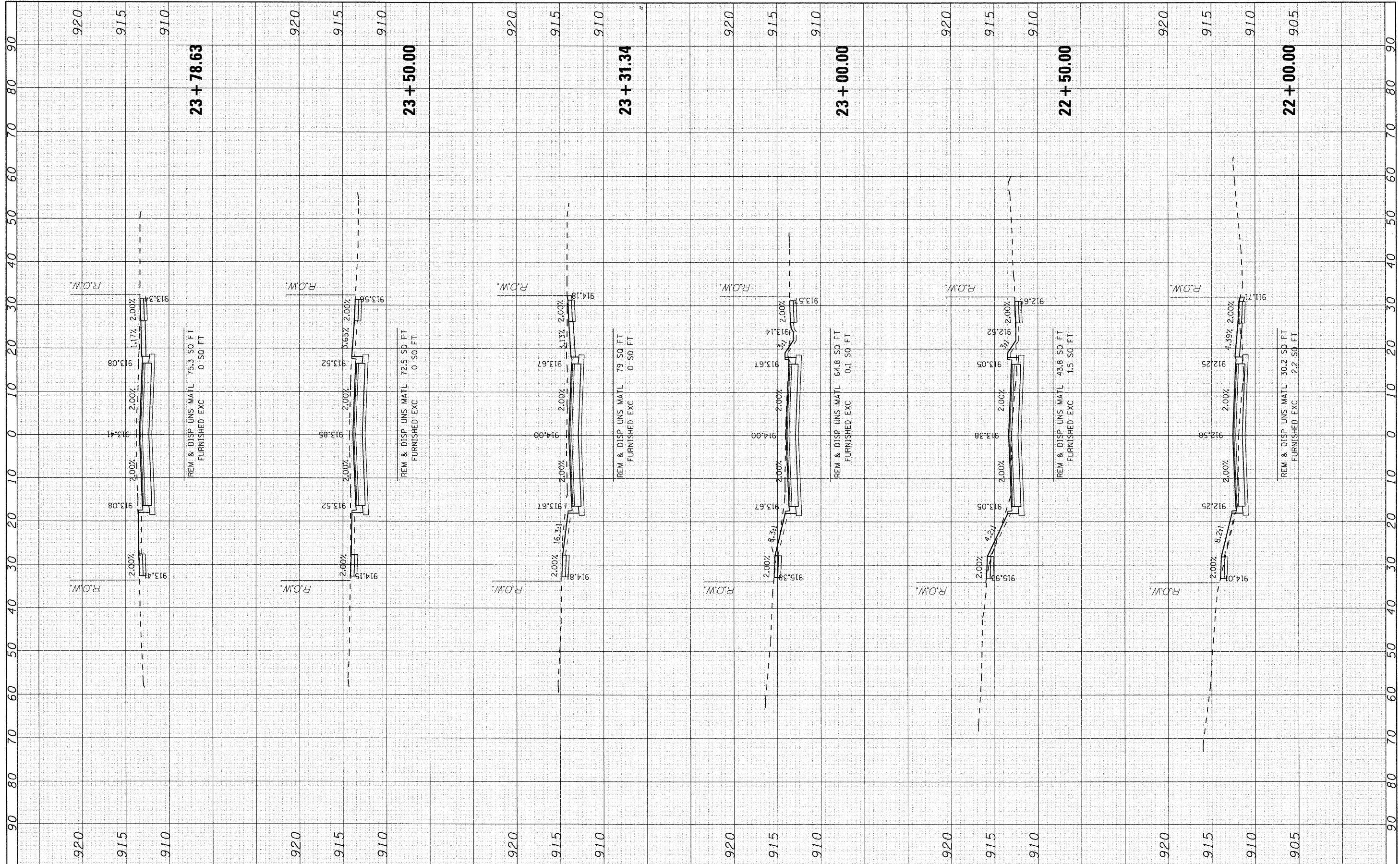
CROSS SECTIONS - TERRA COTTA ROAD

SCALE: 10'H=5'V SHEET NO. 6 OF 8 SHEETS STA. 19+37.91 TO STA. 21+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	54
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	

FINAL	SURVISED	BY	DATE
NO.	NO.		
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
CHECKED	AREAS		

ORIGINAL	DATE
NO.	
NOTE BOOK	
AREAS	
CHECKED	



USER NAME = charlke
 DESIGNED - JRM
 DRAWN -
 CHECKED - TCH
 DATE - 2/13/09
 PLOT SCALE = 10'H=5'V
 PLOT DATE = 2/13/2009

DESIGNED - JRM
 DRAWN -
 CHECKED - TCH
 DATE - 2/13/09
 REVISED -
 REVISED -
 REVISED -
 REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

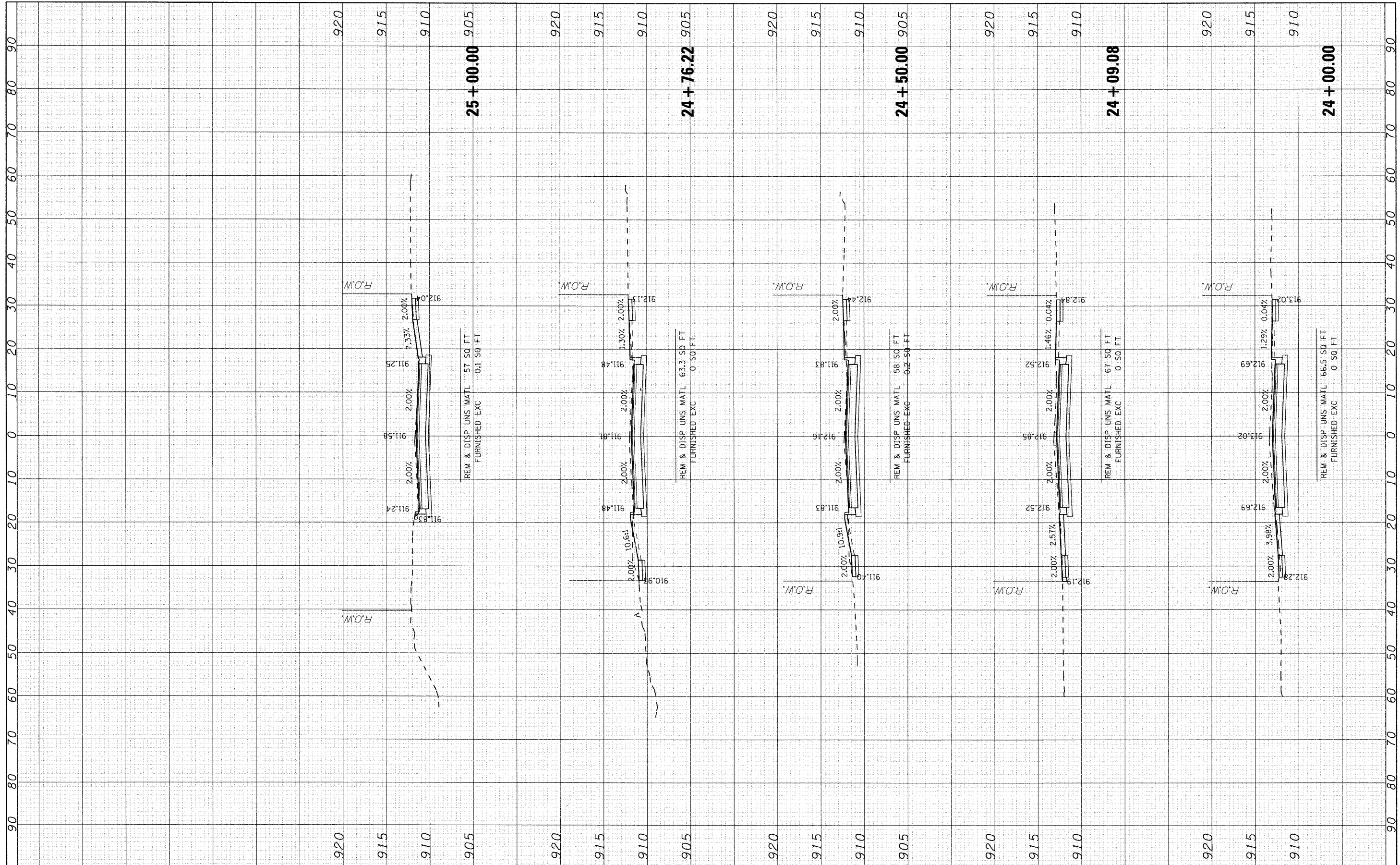
CROSS SECTIONS - TERRA COTTA ROAD

SCALE: 10'H=5'V SHEET NO. 7 OF 8 SHEETS STA. 22+00.00 TO STA. 23+78.63

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0116	08-00327-01-CH	MCHENRY	56	55
CONTRACT NO. 63144				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

FINAL	CHECKED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
AREAS	CHECKED		
NO.			

ORIGINAL	CHECKED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
AREAS	CHECKED		
NO.			



USER NAME = zhar-tke	DESIGNED - JRM	REVISED -
PLOT SCALE = 10'HS'V	DRAWN -	REVISED -
PLOT DATE = 2/13/2009	CHECKED - TCH	REVISED -
	DATE - 2/13/09	REVISED -

**MCHENRY COUNTY
 DIVISION OF TRANSPORTATION**

CROSS SECTIONS - TERRA COTTA ROAD			
SCALE: 10'HS'V	SHEET NO. 8 OF 8 SHEETS	STA. 24+00.00 TO STA. 25+00.00	

F.A. RTE. 0116	SECTION 08-00327-01-CH	COUNTY MCHENRY	TOTAL SHEETS 56	SHEET NO. 56
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63144	