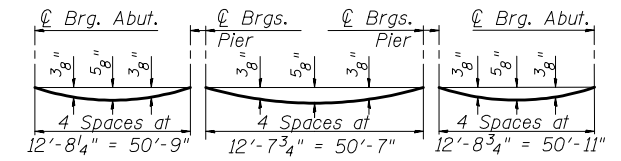


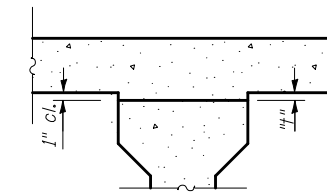
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



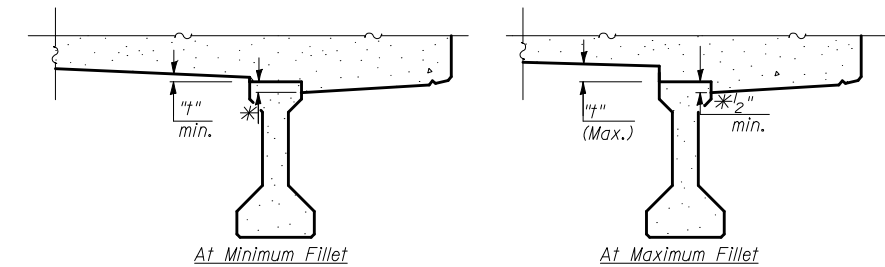
DEAD LOAD DEFLECTION DIAGRAM

(Includes weight of concrete, excluding beams).

Note:
The above deflections are not to be used in the field if the Engineer is working from the "Theoretical Grade Elevations Adjusted for Dead Load Deflections and Grinding" as shown below and on Sheet 6 & 7 of 27.



INTERIOR BEAMS



* Variable (not less than 1/2")

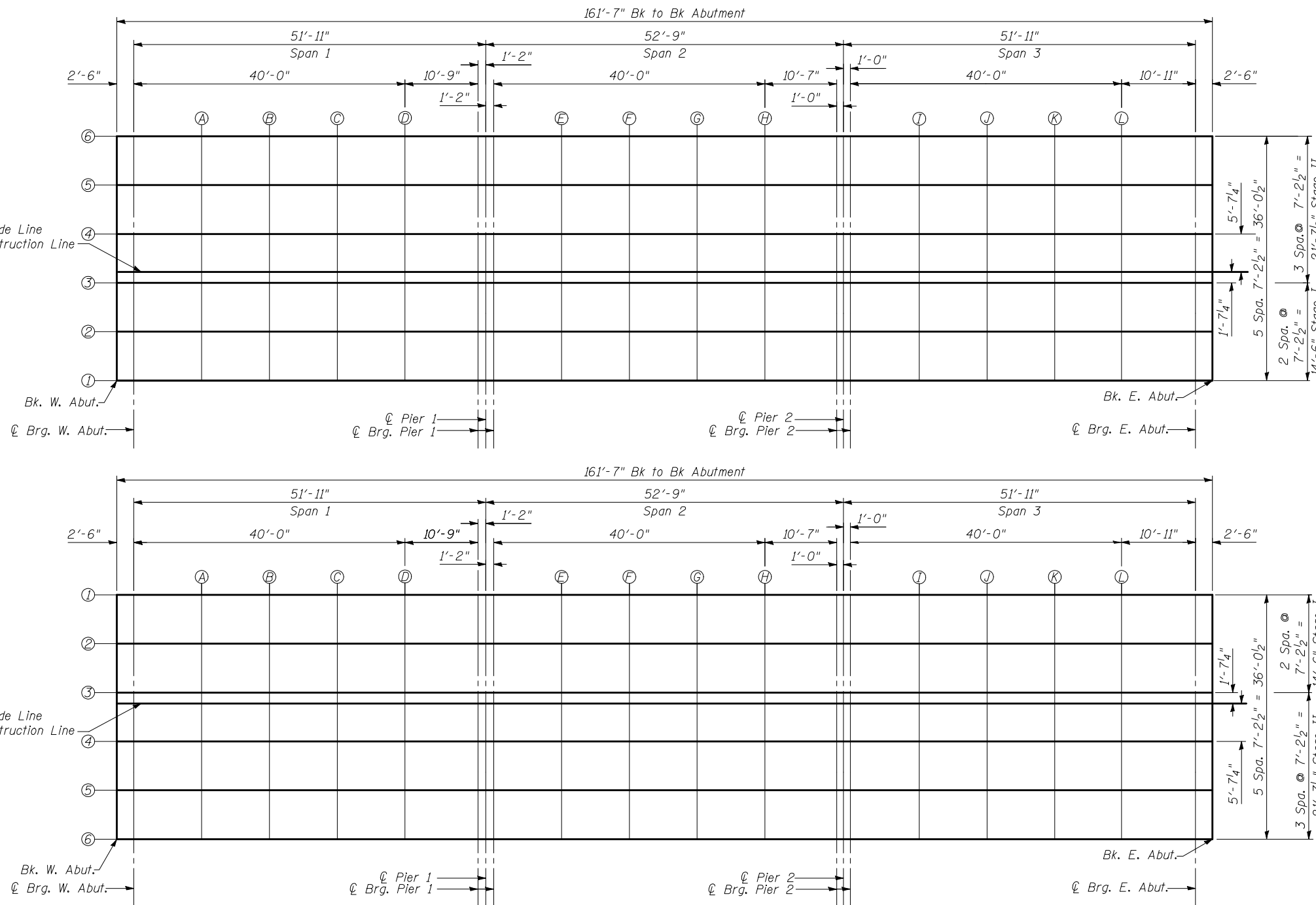
EXTERIOR BEAMS

To determine "t": After all precast prestressed beams have been erected, elevations of the top flanges of the beams shall be taken at intervals shown on the plans. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflections and Grinding" shown on Sheets 6 & 7 of 27, minus 8 1/4" deck thickness, equals the fillet heights "t" above top flanges of beams.

The slab is to be ground after curing to achieve smoothness but the slab is not to be ground to elevations below the "Theoretical Grade Elevations" shown on sheets 6 & 7 of 27. For grinding the deck, see Special Provisions.

**TOP OF SLAB ELEVATIONS
I-74 OVER LITTLE KICKAPOO CREEK
STRUCTURE NO. 057-0120
AND 057-0121**

STRAND ASSOCIATES, INC.



☉ Roadway
Profile Grade Line
Stage Construction Line

☉ Roadway
Profile Grade Line
Stage Construction Line



DESIGNED	RRD
CHECKED	AJS
DRAWN	KAS
CHECKED	JAR

SHEET NO. 5 27 SHEETS	F.A.I. RTE. 74	SECTION (57-21B)BR	COUNTY MCLEAN	TOTAL SHEETS 61	SHEET NO. 17
	TOP OF SLAB ELEVATIONS (1 OF 5)			CONTRACT NO. 70641	
FED. ROAD DIST. NO. 7 ILLINOIS FED. AID PROJECT					