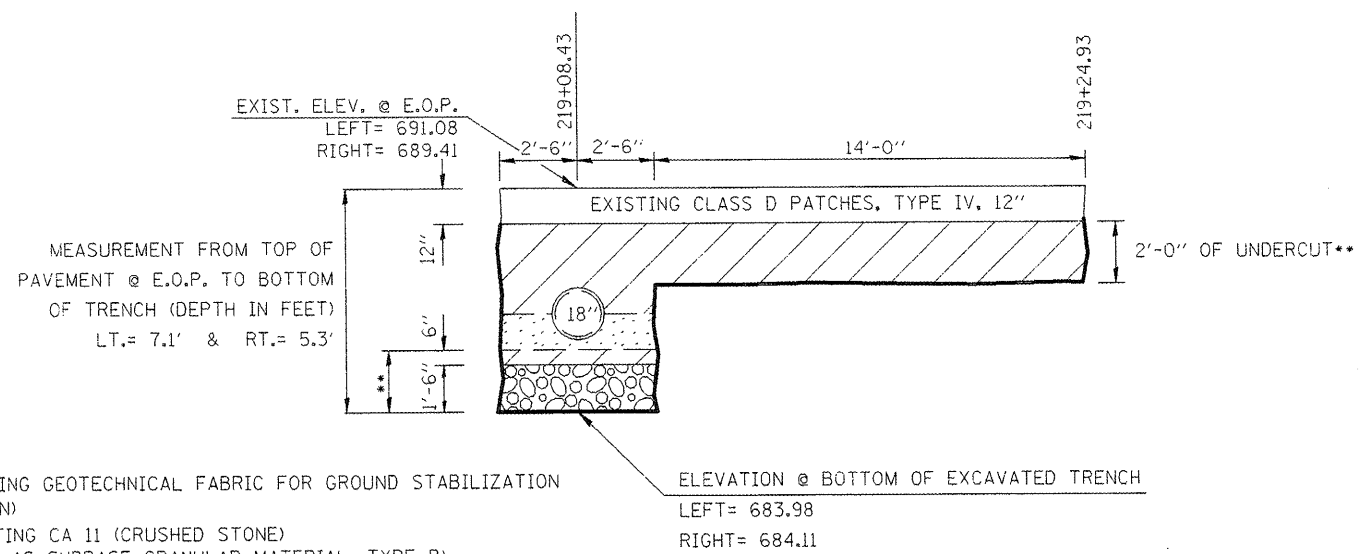


IL ROUTE 116 - MAINLINE
PAVEMENT AT RAILROAD VIADUCT



- EXISTING GEOTECHNICAL FABRIC FOR GROUND STABILIZATION (WOVEN)
- ▨ EXISTING CA 11 (CRUSHED STONE) (PAID AS SUBBASE GRANULAR MATERIAL, TYPE B)
- ⊘ EXISTING BREAKER RUN REJECT ROCK
- ⊘ EXISTING CA 11 (CRUSHED STONE) (INCLUDED IN THE COST OF STORM SEWERS, CLASS B TYPE 1 18") REFER TO ARTICLE 550.04 & 550.07 OF THE STANDARD SPECIFICATIONS

** UNDERCUT- FIELD VARIED BY DCP
DEPTH MAY NEED TO BE ADJUSTED IN FIELD
ADDITIONAL UNDERCUT AND MATERIALS, IF REQUIRED SHALL BE PAID
AS PER ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS

IL ROUTE 116 - MAINLINE
EXIST. CLASS D PATCH - FROM PREVIOUS CONTRACT PLANS
STA. 219+05.93 TO 219+24.93
(FOR INFORMATION ONLY)

EXISTING LEGEND

- ① 9-6-9 PCC PAVEMENT
- ② 5"± HMA OVERLAY'S
- ③ 8" HMA BASE CSE. WIDENING WITH 3" OVERLAY
- ④ 6" HMA SHOULDER CURRENTLY BEING USED AS WIDENING TO BE REMOVED (PAID AS PAVEMENT REMOVAL) ⊠
- ⑤ AGG. SHOULDER
- ⑥ BURIED GUTTER & HMA OVERLAY TO BE REMOVED (PAID AS PAVEMENT REMOVAL, SPECIAL) ▨
- ⑦ TYPE A MODIFIED GUTTER TO BE REMOVED
- ⑧ TYPE B MODIFIED GUTTER TO BE REMOVED
- ⑨ GUTTER TO BE REMOVED (UNKNOWN TYPE)
- ⑩ CURB & GUTTER, TY B-6.24 TO BE REMOVED
- ⑪ PAVEMENT REMOVAL ⊠ (SEE SPECIAL PROVISIONS)

NOTES

- (1'-0" OF THE ORIGINAL 9-6-9 PAVEMENT SHALL BE REMOVED AT LOCATIONS SPECIFIED TO ESTABLISH CLEAN EDGE FOR PROPOSED WIDENING.)
- HMA SURFACE REMOVAL IS SHOWN ON PROP. TYPICALS