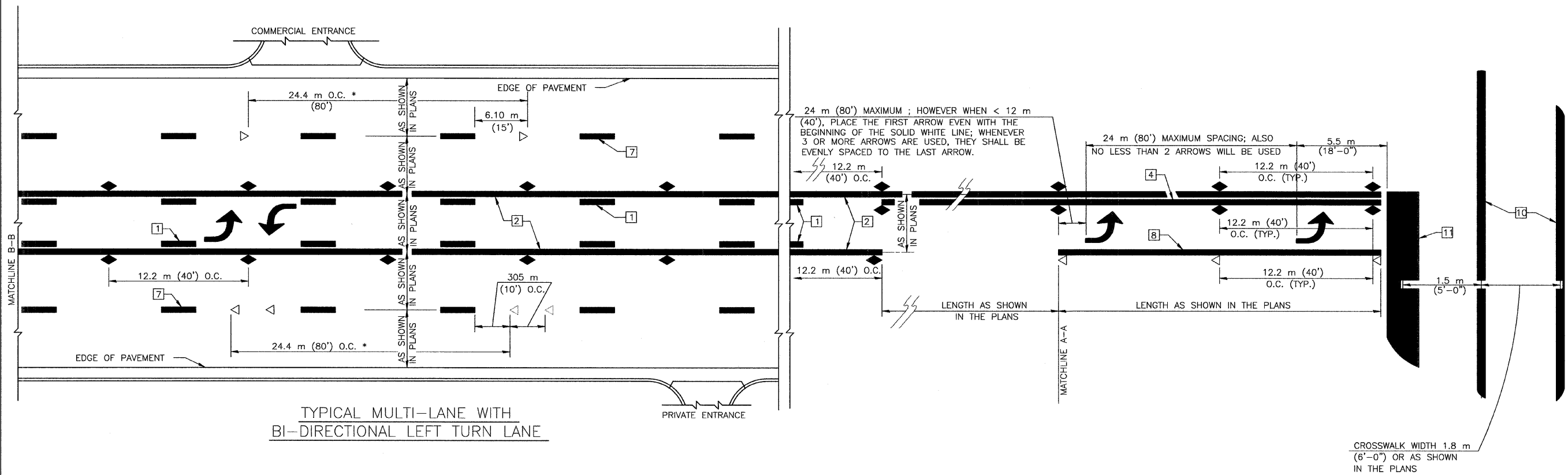




TYPICAL APPLICATIONS OF URBAN PAVEMENT MARKINGS AND MARKERS



TYPICAL MULTI-LANE WITH BI-DIRECTIONAL LEFT TURN LANE

TYPICAL MULTI-LANE TRANSITION FROM BI-DIRECTIONAL LEFT TURN LANE TO LEFT TURN LANE

SPECIAL NOTES:
 TURN ARROW PAIRS SHALL BE PLACED AT 75 m (250') INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
 THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
 THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ABOVE.
 * REDUCE TO 12.2 METERS (40 FEET) O.C. IN NO PASSING ZONES AND ON CURVES WHERE ADVISORY SPEEDS ARE 15 kph (10 MPH) LOWER THAN POSTED SPEEDS.
 ** WHERE DOUBLE LANE LINE MARKERS ARE SPECIFIED, THEY SHALL BE SPACED AS SHOWN ABOVE.

All dimensions are in millimeters (inches) unless otherwise shown.

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