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ITEM NO: 1A

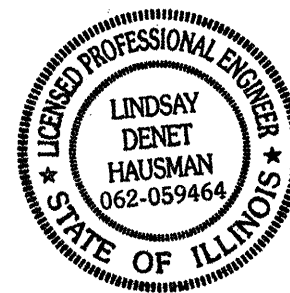
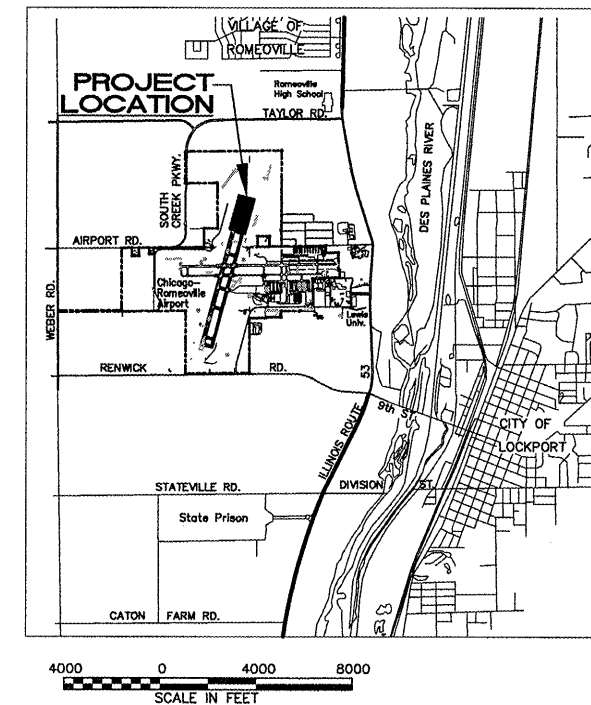
LE040
TOTAL SHEETS = 32

CONSTRUCTION PLANS
COMPLETE TAXIWAY G

JOLIET REGIONAL PORT DISTRICT
CHICAGO-ROMEOVILLE AIRPORT (LOT)
ROMEIOVILLE, WILL COUNTY, ILLINOIS

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

VICINITY MAP



SHEET NO. 1 OF 32

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

Seal EXP 11/30/09 Date of Plans
Lindsay Hausman
Lindsay D. Hausman, P.E. **JANUARY 30, 2009**

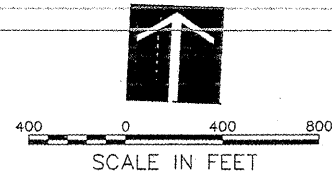
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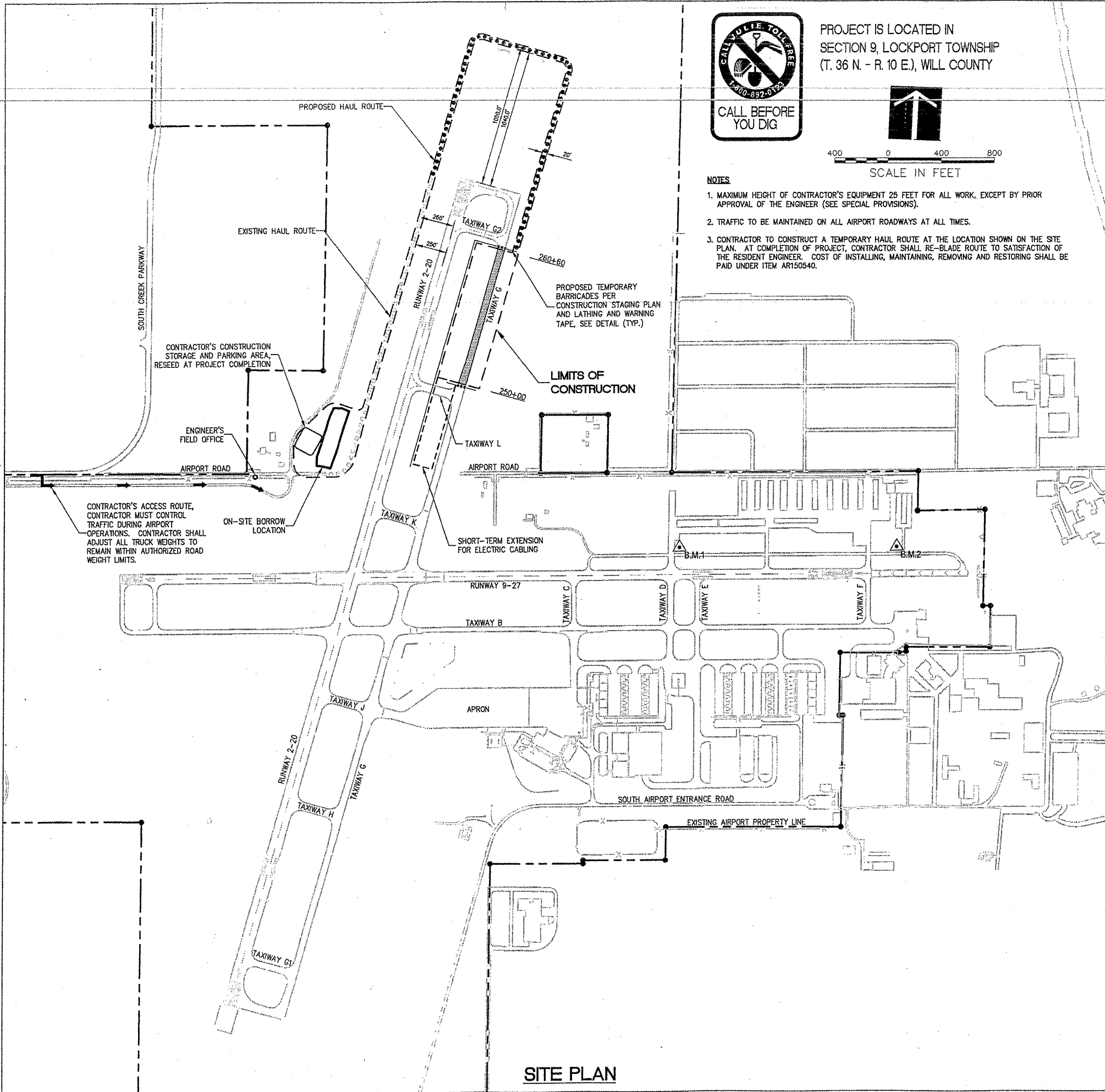


PROJECT IS LOCATED IN
SECTION 9, LOCKPORT TOWNSHIP
(T. 36 N. - R. 10 E.), WILL COUNTY



NOTES

1. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT 25 FEET FOR ALL WORK, EXCEPT BY PRIOR APPROVAL OF THE ENGINEER (SEE SPECIAL PROVISIONS).
2. TRAFFIC TO BE MAINTAINED ON ALL AIRPORT ROADWAYS AT ALL TIMES.
3. CONTRACTOR TO CONSTRUCT A TEMPORARY HAUL ROUTE AT THE LOCATION SHOWN ON THE SITE PLAN. AT COMPLETION OF PROJECT, CONTRACTOR SHALL RE-BLADE ROUTE TO SATISFACTION OF THE RESIDENT ENGINEER. COST OF INSTALLING, MAINTAINING, REMOVING AND RESTORING SHALL BE PAID UNDER ITEM AR150540.



GENERAL NOTES

PROJECT DESCRIPTION

THIS PROJECT IS TO CONSTRUCT A 1060' EXTENSION OF PARALLEL TAXIWAY G AT CHICAGO-ROMEDEVILLE AIRPORT INCLUDING, AMONG OTHER INCIDENTAL WORK, THE FOLLOWING ITEMS:

- PLACEMENT OF TEMPORARY SOIL EROSION CONTROL MEASURES
- EXTENSION OF EXISTING UNDERDRAIN PIPE SYSTEM
- CONSTRUCTION OF PARALLEL TAXIWAY G, PCC COMPOSITION
- PAVEMENT MARKING
- EXTENSION OF AIRFIELD LIGHTING
- INSTALLATION OF A LIGHTED SUPPLEMENTAL WIND CONE
- TOPSOILING, SEEDING AND MULCHING AROUND NEW PAVEMENTS

PROTECTION OF EXISTING AIRPORT FACILITIES

THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UNDERGROUND AND OVERHEAD UTILITIES AND LIGHTING EQUIPMENT, DRIVEWAY AND ROAD PAVEMENT AND SHOULDERS; RUNWAY, TAXIWAY AND APRON PAVEMENTS AND SHOULDERS; RUNWAY, TAXIWAY AND AIRPORT LIGHTING EQUIPMENT; AND SEEDED AND TURFED AREAS THAT ARE UTILIZED IN OR AFFECTED BY THE CONTRACTOR'S ACTIVITIES. ITEMS DAMAGED BY THE CONTRACTOR ARE TO BE REPAIRED AT CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE.

IN ADDITION WHEN CONDITIONS DICTATE OR AS DETERMINED BY THE AIRPORT MANAGER OR THE OWNER'S REPRESENTATIVE, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICK-UP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

CONTRACTOR'S ACCESS AND TEMPORARY FACILITIES

CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN ON THIS SHEET. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S OWNER.

THE CONTRACTOR IS TO PROVIDE A TEMPORARY HAUL ROUTE WITHIN THE CONSTRUCTION LIMIT LINES AS SHOWN. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN ON THIS SHEET. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN HE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. AT THE AIRPORT MANAGER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT MANAGER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

RESPONSIBILITY FOR EXISTING UTILITIES

THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.

EXISTING BENCHMARKS

PROJECT BENCHMARKS ARE AS FOLLOWS:

- ▲ B.M.1 N 1,800,236.82
E 1,051,080.54
ELEV. 666.67
- ▲ B.M.2 N 1,800,302.96
E 1,052,719.90
ELEV. 664.18

THIS DATA IS NOT ILLINOIS STATE PLANE COORDINATES.

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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

SITE PLAN AND GENERAL NOTES

08A0122
Project Number
LDH 06/18/08
Layout By Date
LDH 06/18/08
Designed By Date
RMH 01/30/09
Reviewed By Date
LDH
Drawn By
Sheet No.

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SITE PLAN

CONSTRUCTION AND SAFETY NOTES

SEQUENCE OF CONSTRUCTION

TO MINIMIZE DISRUPTIONS TO AIRPORT OPERATIONS, CONSTRUCTION OPERATIONS MUST BE CONTROLLED THROUGHOUT THE PROJECT'S DURATION AND WORK MUST BE COMPLETED EXPEDITIOUSLY. THE CONTRACTOR IS TO PREPARE AND FOLLOW A STAGING PLAN THAT COORDINATES HIS WORK WITH THE WORK OF HIS SUBCONTRACTORS AND THE WORK OF OTHER CONTRACTORS ON-FIELD. THIS STAGING PLAN MUST BE SATISFACTORY TO THE PROJECT ENGINEER AND THE AIRPORT DIRECTOR. THE CONTRACTOR SHALL EXPEDITE WORK AT THOSE STAGES WHEN ACTIVE RUNWAYS, TAXIWAYS, APRONS, ROADWAYS OR PARKING LOTS MUST BE CLOSED TO MINIMIZE THE LENGTH OF TIME THAT AIRPORT OPERATIONS ARE RESTRICTED.

LATHING AND WARNING TAPE

THE PROJECT WILL REQUIRE THE PLACEMENT OF LATHING AND WARNING TAPE TO DELINEATE THE WORK AREA FROM ACTIVE AIRPORT OPERATIONS AREAS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE, PLACE AND MAINTAIN LATHING AND WARNING TAPE SHOWN ON SHEET 3, AND IN DETAIL A, THIS SHEET, AND AS DIRECTED BY THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR. THE CONTRACTOR WILL FURNISH, PLACE, MAINTAIN AND RELOCATE THE LATHING AND WARNING TAPE AS REQUIRED. THE COST OF THESE ITEMS, AND THEIR MAINTENANCE, IS TO BE INCIDENTAL TO THE CONTRACT.

TEMPORARY BARRICADES ON AIRFIELD

THE PROJECT WILL REQUIRE THE PLACEMENT OF BARRICADES TO DELINEATE PORTIONS OF THE CONSTRUCTION AREA AND TO EFFECT TEMPORARY CLOSURES OF ACTIVE TAXIWAYS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH, PLACE AND MAINTAIN BARRICADES AS SHOWN ON SHEET 3, AND IN DETAIL B, THIS SHEET, AND AS DIRECTED BY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR. THE COST OF THESE ITEMS, AND THEIR MAINTENANCE, IS TO BE INCIDENTAL TO THE CONTRACT. ANY WORK THAT REQUIRES PORTIONS OF AN ACTIVE TAXIWAY OR APRON TO BE CLOSED MUST BE COMPLETED EXPEDITIOUSLY TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.

OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE DELINEATED WITH THE USE OF BARRICADES DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS. NO OPEN TRENCHES SHALL BE ALLOWED WITHIN AN ACTIVE RUNWAY SAFETY AREA (RSA) OR AN ACTIVE TAXIWAY SAFETY AREA (TSA) OVER NIGHT. THE RSA IS DEFINED AS 250 FEET FROM THE RUNWAY CENTERLINE AND 1,000 FEET FROM THE END OF THE RUNWAY. THE TSA IS MEASURED AT 65.5 FEET FROM THE TAXIWAY CENTERLINE. THE CONTRACTOR WILL HAVE STEEL PLATES ON-SITE TO ALLOW FOR THE RAPID COVERING OF TRENCHES IN AN ACTIVE RSA OR TSA IN THE EVENT OF UNEXPECTED WORK STOPPAGES FOR WEATHER OR AIRPORT EMERGENCIES.

RUNWAY CLOSURE

CLOSURE OF RUNWAY 2-20 OR RUNWAY 9-27 WILL NOT BE PERMITTED AT ANY TIME DURING THE PROJECT.

HAULROUTE

THE CONTRACTOR SHALL EXTEND AN EXISTING HAUL ROUTE AS SHOWN IN THE SITE PLAN AND DETAIL C THIS SHEET. THE COST OF CONSTRUCTING, MAINTAINING, AND REMOVING THE HAUL ROUTE AT PROJECT END SHALL BE PAID UNDER ITEM AR150540. AT THE RESIDENT ENGINEER'S OPTION, THE HAUL ROUTE CAN REMAIN.

VEHICULAR TRAFFIC CONTROL

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE AND PLACE ROAD WARNING SIGNS AND BARRICADES ON THE EXISTING ROADWAYS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE, INSTALL AND RELOCATE THE ITEMS AS REQUIRED. THE COST OF THIS WORK IS TO BE INCIDENTAL TO THE CONTRACT.

CONTRACTOR SHALL PROVIDE, INSTALL AND REMOVE ALL TRAFFIC CONTROL ITEMS WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 15 FEET OF AN ACTIVE ROADWAY EDGE OR AS REQUIRED BY THE SITE PLAN. COST OF THIS WORK IS TO BE INCIDENTAL TO THE CONTRACT.

AIRFIELD OPERATIONAL SAFETY DURING CONSTRUCTION

ALL CONSTRUCTION TRAFFIC AND PERSONNEL SHALL REMAIN WITHIN THE CONSTRUCTION LIMIT LINE SHOWN ON THE SITE PLAN FOR THE CURRENT WORK. CONTRACTOR'S PERSONNEL AND EQUIPMENT MUST REMAIN AT LEAST 200 FEET FROM THE CENTERLINE OF ACTIVE RUNWAYS, 1,000 FEET FROM THE END OF ACTIVE RUNWAYS, 65.5 FEET FROM THE CENTERLINE OF ACTIVE TAXIWAYS, 44.5 FEET FROM T-HANGAR TAXILANES AND 10 FEET FROM THE EDGE OF ACTIVE APRONS.

WHEN IT IS NECESSARY FOR CONSTRUCTION VEHICLES TO OPERATE ON OR WITHIN THESE LIMITS, THE RUNWAY, TAXIWAYS OR APRON MUST BE CLOSED. THE CONTRACTOR WILL PROVIDE POSITIVE CONTROL OF CONSTRUCTION VEHICLES USING RADIO-EQUIPPED FLAGGERS. ALL CONTRACTOR'S EQUIPMENT USED IN ACTIVE AIRPORT OPERATIONS AREAS SHALL BE EQUIPPED WITH A FAA-STANDARD FLAG, AS REFERENCED IN FAA AC 150/5370-2, CURRENT ISSUE. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY. CONSTRUCTION VEHICLES SHALL NOT CROSS AN ACTIVE RUNWAY. THE COST OF ALL TRAFFIC CONTROL, BOTH WITHIN AND OUTSIDE OF AIRPORT OPERATIONS AREAS, IS TO BE INCIDENTAL TO THE CONTRACT.

WHEN NOT IN USE AND DURING NONWORKING HOURS, CONTRACTOR'S EQUIPMENT SHALL BE PARKED WITHIN THE CONTRACTOR'S EQUIPMENT STORAGE AND PARKING AREAS. THE EQUIPMENT STORAGE AND PARKING AREAS ARE TO BE LOCATED AS SHOWN ON THE SITE PLAN, SHEET 3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE CONSTRUCTION ENTRANCE IN GOOD CONDITION. THE COST OF MAINTAINING THE CONSTRUCTION ENTRANCE IS TO BE INCIDENTAL TO THE CONTRACT.

AT NO TIME SHALL THE CONTRACTOR OPERATE OR PARK EQUIPMENT SO AS TO OBSTRUCT AN ACTIVE RUNWAY APPROACH SURFACE.

BEFORE REOPENING TEMPORARILY CLOSED RUNWAYS, TAXIWAYS OR ROADWAYS, THE CONTRACTOR SHALL INSPECT AND CLEAN, AS NECESSARY, THE PAVEMENT TO ASSURE THAT NO MATERIALS OR OBJECTS THAT MAY DAMAGE AIRCRAFT OR VEHICLES REMAIN. ANY REQUIRED CLEANING SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT OWNER AND IS INCIDENTAL TO THE CONTRACT.

ALL CONTRACTOR EQUIPMENT IS LIMITED TO A HEIGHT OF 25 FEET.

NOTIFICATIONS BY CONTRACTOR

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT OWNER 7 DAYS IN ADVANCE OF THE CONTRACTOR'S CLOSING OF ACTIVE TAXIWAYS. THE DATE, TIME AND SCHEDULED DURATION OF THE CLOSING MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE AIRPORT OWNER. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT OWNER 72 HOURS IN ADVANCE OF THE CONTRACTOR'S CLOSING OF OTHER ACTIVE ROADWAYS, AIRFIELD OR ROADWAY LIGHTING CIRCUITS, OR OTHER AIRPORT FACILITIES.

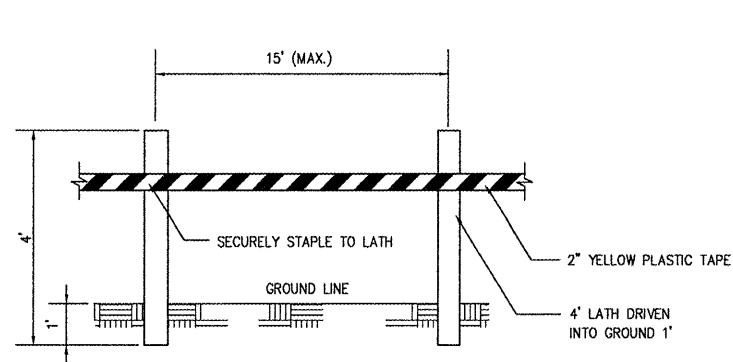
CONTRACTOR'S USE OF SITE

THE CONTRACTOR SHALL NOT OPERATE WITHIN, ENCRUCH ON OR OBSTRUCT AIRPORT OPERATIONAL AREAS, INCLUDING ACTIVE RUNWAY, TAXIWAYS AND APRON SAFETY AREAS, OBJECT AND OBSTACLE FREE ZONES, RUNWAY PROTECTION ZONES AND AIRPORT IMAGINARY SURFACES AS DEFINED IN FEDERAL AVIATION REGULATIONS (FAR) PART 77, "OBJECTS AFFECTING NAVIGABLE AIRSPACE".

THE CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF THE WORK AREA PRIOR TO BEGINNING WORK AT A NEW LOCATION.

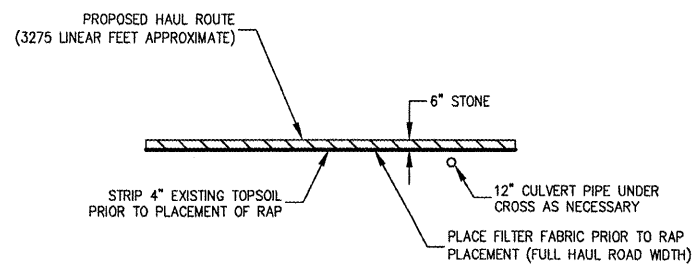
UTILITY OUTAGES AND SHUTDOWNS

THE CONTRACTOR SHALL PROVIDE 72 HOURS PRIOR NOTICE OF ANY OUTAGES OR SHUTDOWNS TO THE OWNER AND THE AGENCY OWNING THE AFFECTED UTILITY. THE CONTRACTOR SHALL PROVIDE ANY TEMPORARY CONNECTIONS OR OTHER MEASURES AS MAY BE REQUIRED TO MAINTAIN SERVICE AS MAY BE REQUIRED BY THE OWNING AGENCY AT NO COST TO THE OWNER.

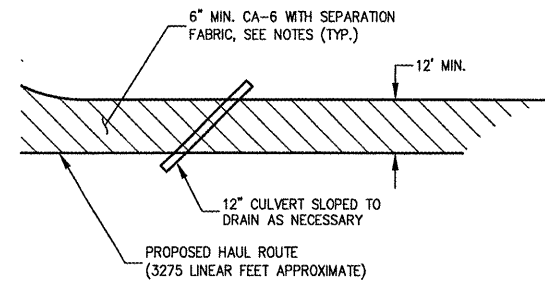


DETAIL A
LATHING AND WARNING TAPE

MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE IS TO BE INCIDENTAL TO THE CONTRACT.



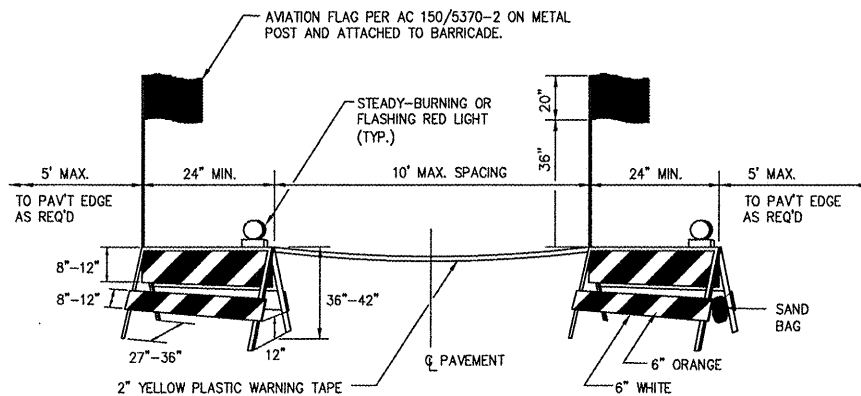
CROSS SECTION



PLAN

NOTES:

1. STRIP 4" OF EXISTING TOPSOIL PRIOR TO PLACEMENT OF STONE.
2. STONE SHALL BE 2-INCH SIZE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT, OR RECYCLED ASPHALT.
3. HAUL ROUTE THICKNESS SHALL NOT BE LESS THAN SIX INCHES.
4. HAUL ROUTE WIDTH SHALL BE 12 FEET MINIMUM.
5. SURFACE WATER FLOWING OR DIVERTED SHALL BE CARRIED IN CULVERT (CMP, STEEL OR HDPE).
6. PLACE SEPARATION FABRIC PRIOR TO STONE PLACEMENT FOR FULL WIDTH OF HAUL ROUTE. FABRIC TO BE MIRAFI 160N OR APPROVED EQUAL, COST INCIDENTAL TO ITEM AR150540.
7. THE HAUL ROUTE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL AGGREGATE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURE USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAYS MUST BE REMOVED IMMEDIATELY.
8. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
9. HAUL ROUTE TO BE REMOVED AT PROJECT END. AREA TO BE RESTORED AND RESEED (SEE LANDSCAPING PLAN) AND LEFT IN A CONDITION SATISFACTORY TO THE RESIDENT ENGINEER. AT RESIDENT ENGINEER'S OPTION, THE HAUL ROUTE CAN REMAIN.
10. COST OF INSTALLING, MAINTAINING, REMOVING AND RESTORING HAUL ROUTE SHALL BE PAID UNDER ITEM AR150540.



DETAIL B
PAVEMENT BARRICADES

BARRICADES ARE TO BE OF IDOT TYPE I. A STEADY-BURNING OR FLASHING RED LIGHT FACING PASSING TRAFFIC IS TO BE MOUNTED ABOVE THE TOP OF EACH BARRICADE FRAME. THE BARRICADE IS TO BE STABILIZED FROM WIND BY SANDBAGS PLACED ON THE FRAME OR OTHER METHODS APPROVED BY THE RESIDENT ENGINEER. NO PART OF THE REFLECTORIZED PORTION OF THE BARRICADE IS TO BE OBSTRUCTED IN ANY MANNER. COST OF FURNISHING, INSTALLING, RELOCATING, MAINTAINING AND REMOVING BARRICADES IS TO BE INCIDENTAL TO THE CONTRACT.

DETAIL C
HAUL ROUTE

DETAILS SHOWN ARE NOT TO SCALE

LE040



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COMPLETE TAXIWAY G

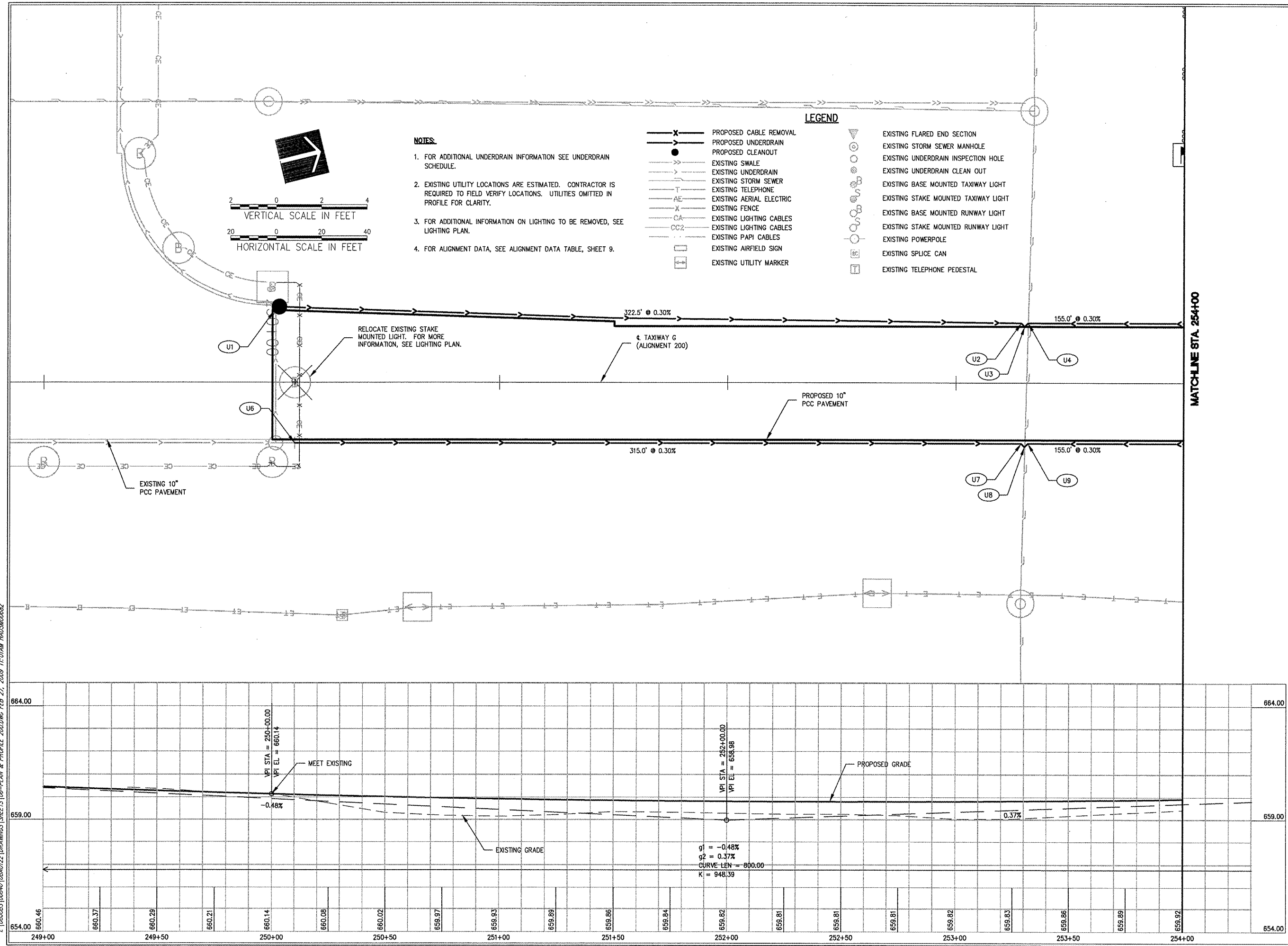
AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date: **JANUARY 30, 2009**
Sheet Title:

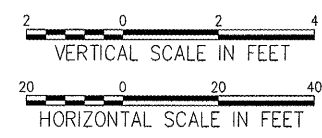
CONSTRUCTION AND SAFETY NOTES AND DETAILS

08A0122	Project Number
LDH	06/18/08
LDH	06/18/08
RMH	01/30/09
LDH	---



- NOTES:**
1. FOR ADDITIONAL UNDERDRAIN INFORMATION SEE UNDERDRAIN SCHEDULE.
 2. EXISTING UTILITY LOCATIONS ARE ESTIMATED. CONTRACTOR IS REQUIRED TO FIELD VERIFY LOCATIONS. UTILITIES OMITTED IN PROFILE FOR CLARITY.
 3. FOR ADDITIONAL INFORMATION ON LIGHTING TO BE REMOVED, SEE LIGHTING PLAN.
 4. FOR ALIGNMENT DATA, SEE ALIGNMENT DATA TABLE, SHEET 9.

- LEGEND**
- PROPOSED CABLE REMOVAL
 - PROPOSED UNDERDRAIN
 - PROPOSED CLEANOUT
 - EXISTING SWALE
 - EXISTING UNDERDRAIN
 - EXISTING STORM SEWER
 - EXISTING TELEPHONE
 - EXISTING AERIAL ELECTRIC
 - EXISTING FENCE
 - EXISTING LIGHTING CABLES
 - EXISTING LIGHTING CABLES
 - EXISTING PAPI CABLES
 - EXISTING AIRFIELD SIGN
 - EXISTING UTILITY MARKER
 - EXISTING FLARED END SECTION
 - EXISTING STORM SEWER MANHOLE
 - EXISTING UNDERDRAIN INSPECTION HOLE
 - EXISTING UNDERDRAIN CLEAN OUT
 - EXISTING BASE MOUNTED TAXIWAY LIGHT
 - EXISTING STAKE MOUNTED TAXIWAY LIGHT
 - EXISTING BASE MOUNTED RUNWAY LIGHT
 - EXISTING STAKE MOUNTED RUNWAY LIGHT
 - EXISTING POWERPOLE
 - EXISTING SPLICE CAN
 - EXISTING TELEPHONE PEDESTAL



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COMPLETE TAXIWAY G
 AIP PROJECT NO. 3-17-0140-B43
 IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date: JANUARY 30, 2009
 Sheet Title:

**PLAN AND PROFILE
 TAXIWAY G
 STA 249+00 THRU
 STA. 254+00**

08A0122
 Project Number
 LDH 06/23/08
 Layout By Date
 LDH 06/23/08
 Designed By Date
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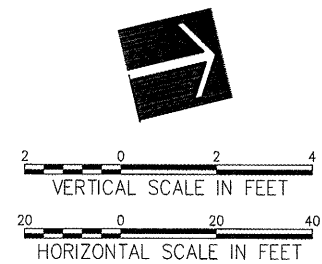
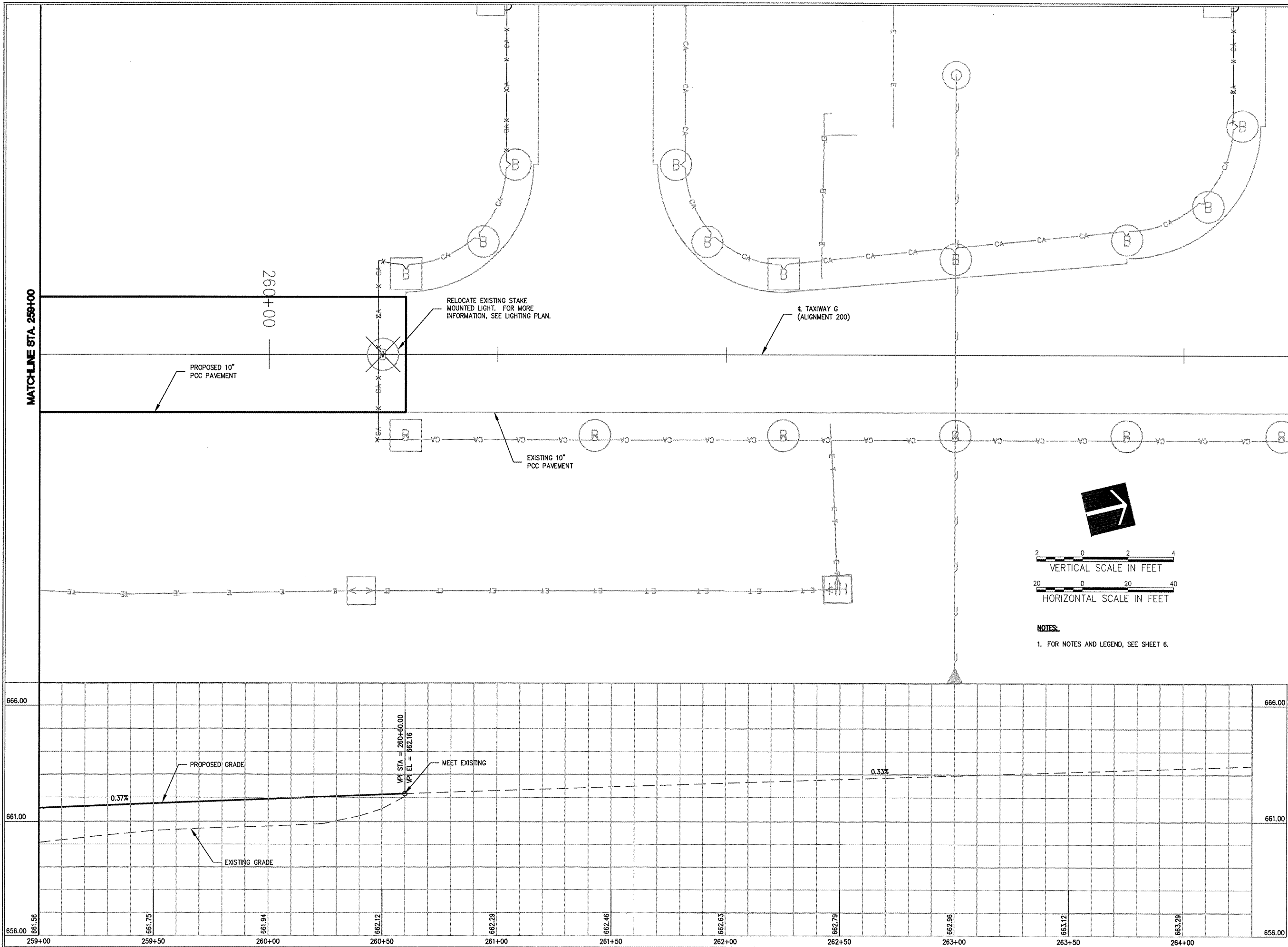


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Telephone: 815.838.9497
Fax: 815.838.9524

COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



NOTES:
1. FOR NOTES AND LEGEND, SEE SHEET 6.

No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

**PLAN AND RPROFILE
TAXIWAY G
STA. 259+00 THRU
STA. 264+00**

08A0122
Project Number
LDH 06/24/08
Layout By Date
LDH 06/24/08
Designed By Date
RMH 01/30/09
Reviewed By Date
Drawn By _____
Sheet No. _____

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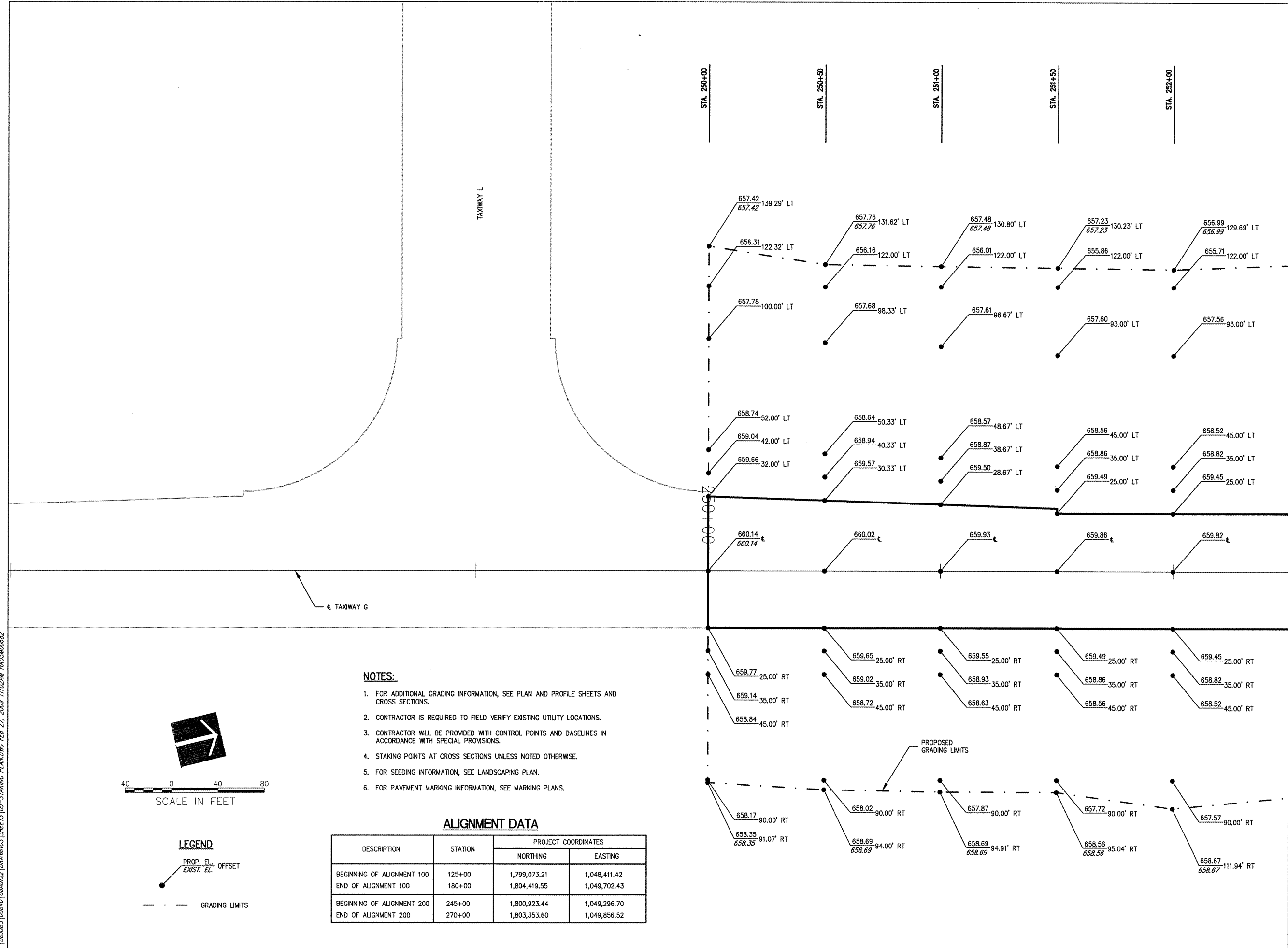


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COMPLETE TAXIWAY G

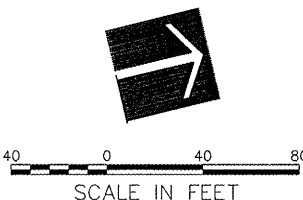
AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



MATCH-LINE STA. 252+50

NOTES:

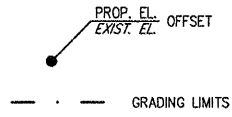
1. FOR ADDITIONAL GRADING INFORMATION, SEE PLAN AND PROFILE SHEETS AND CROSS SECTIONS.
2. CONTRACTOR IS REQUIRED TO FIELD VERIFY EXISTING UTILITY LOCATIONS.
3. CONTRACTOR WILL BE PROVIDED WITH CONTROL POINTS AND BASELINES IN ACCORDANCE WITH SPECIAL PROVISIONS.
4. STAKING POINTS AT CROSS SECTIONS UNLESS NOTED OTHERWISE.
5. FOR SEEDING INFORMATION, SEE LANDSCAPING PLAN.
6. FOR PAVEMENT MARKING INFORMATION, SEE MARKING PLANS.



ALIGNMENT DATA

DESCRIPTION	STATION	PROJECT COORDINATES	
		NORTHING	EASTING
BEGINNING OF ALIGNMENT 100	125+00	1,799,073.21	1,048,411.42
END OF ALIGNMENT 100	180+00	1,804,419.55	1,049,702.43
BEGINNING OF ALIGNMENT 200	245+00	1,800,923.44	1,049,296.70
END OF ALIGNMENT 200	270+00	1,803,353.60	1,049,856.52

LEGEND



No.	Drawing Issue Description	Date	By

Date: JANUARY 30, 2009

Sheet Title: STAKING PLAN TAXIWAY G STA. 247+00 THRU STA. 252+50

08A0122
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 LDH 06/26/08
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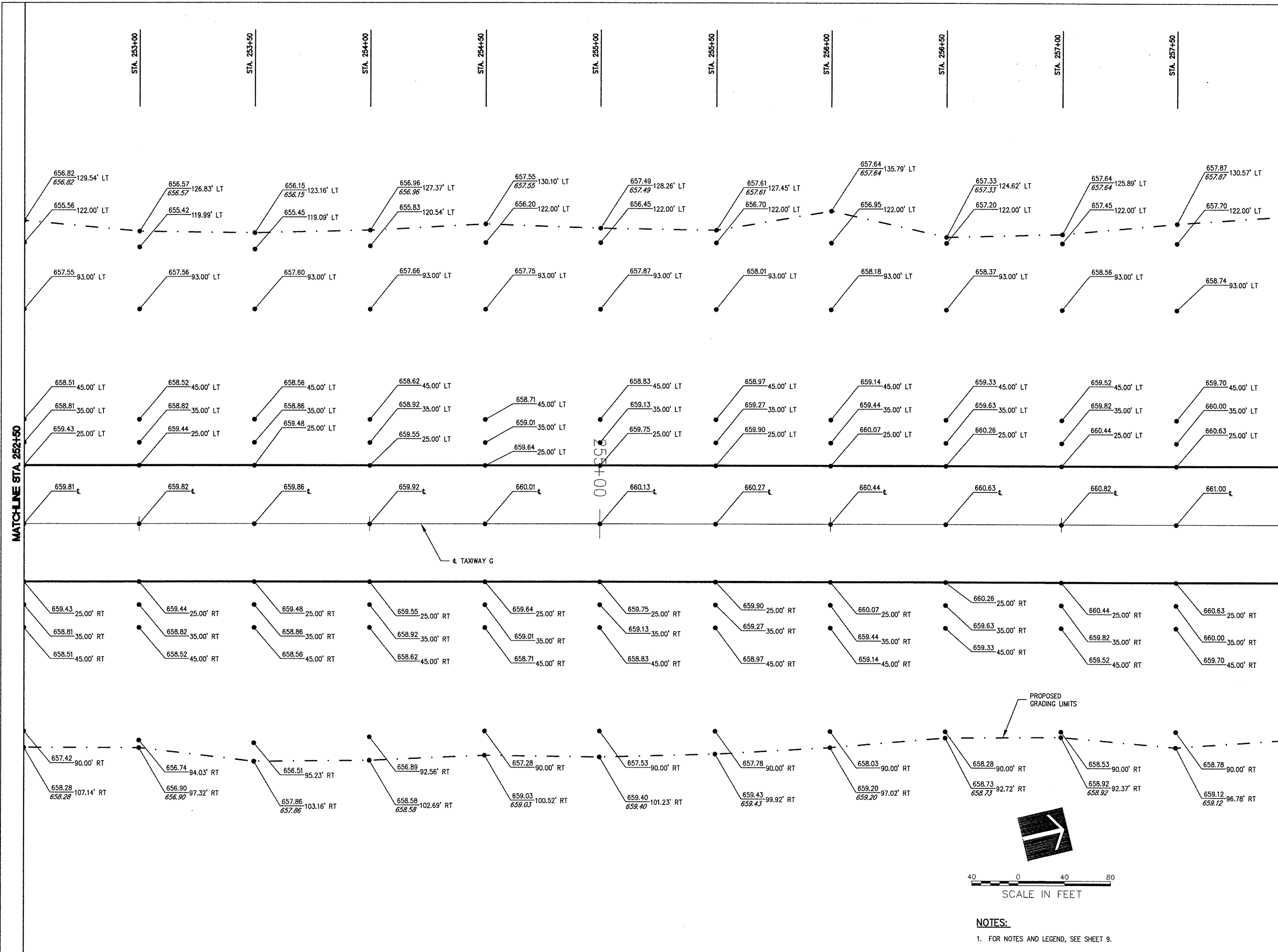


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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



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MATCHLINE STA. 258+00

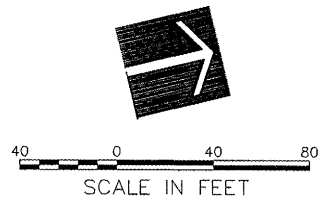
No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

**STAKING PLAN
TAXIWAY G
STA. 252+50 THRU
STA. 258+00**

08A0122
Project Number
LDH 06/27/08
Layout By Date
LDH 06/27/08
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Drawn By Sheet No.

10



NOTES:
1. FOR NOTES AND LEGEND, SEE SHEET 9.



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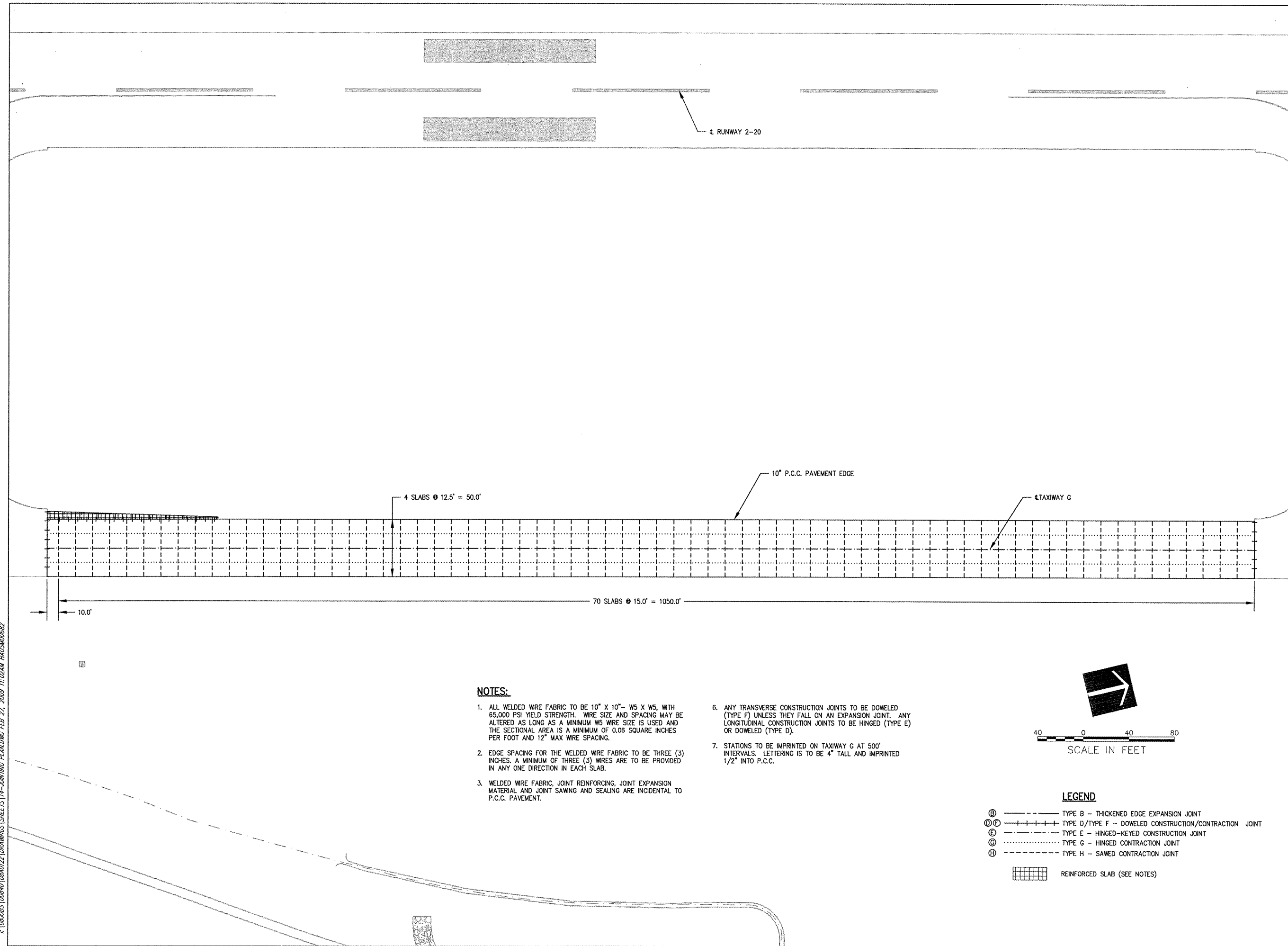


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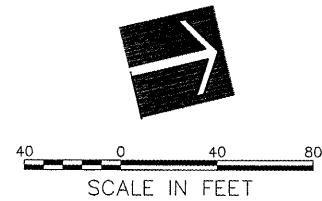
COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



NOTES:

- ALL WELDED WIRE FABRIC TO BE 10" X 10"- W5 X W5, WITH 65,000 PSI YIELD STRENGTH. WIRE SIZE AND SPACING MAY BE ALTERED AS LONG AS A MINIMUM W5 WIRE SIZE IS USED AND THE SECTIONAL AREA IS A MINIMUM OF 0.06 SQUARE INCHES PER FOOT AND 12" MAX WIRE SPACING.
- EDGE SPACING FOR THE WELDED WIRE FABRIC TO BE THREE (3) INCHES. A MINIMUM OF THREE (3) WIRES ARE TO BE PROVIDED IN ANY ONE DIRECTION IN EACH SLAB.
- WELDED WIRE FABRIC, JOINT REINFORCING, JOINT EXPANSION MATERIAL AND JOINT SAWING AND SEALING ARE INCIDENTAL TO P.C.C. PAVEMENT.
- ANY TRANSVERSE CONSTRUCTION JOINTS TO BE DOWELED (TYPE F) UNLESS THEY FALL ON AN EXPANSION JOINT. ANY LONGITUDINAL CONSTRUCTION JOINTS TO BE HINGED (TYPE E) OR DOWELED (TYPE D).
- STATIONS TO BE IMPRINTED ON TAXIWAY G AT 500' INTERVALS. LETTERING IS TO BE 4" TALL AND IMPRINTED 1/2" INTO P.C.C.



LEGEND

- (B) --- TYPE B - THICKENED EDGE EXPANSION JOINT
- (D)(F) --- TYPE D/TYPE F - DOWELED CONSTRUCTION/CONTRACTION JOINT
- (E) --- TYPE E - HINGED-KEYED CONSTRUCTION JOINT
- (G) --- TYPE G - HINGED CONTRACTION JOINT
- (H) --- TYPE H - SAWED CONTRACTION JOINT
- [Grid Pattern] REINFORCED SLAB (SEE NOTES)

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No.	Drawing Issue Description	Date	By

Date
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**JOINTING PLAN
TAXIWAY G**

08A0122
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LDH 06/18/08
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LDH 06/18/08
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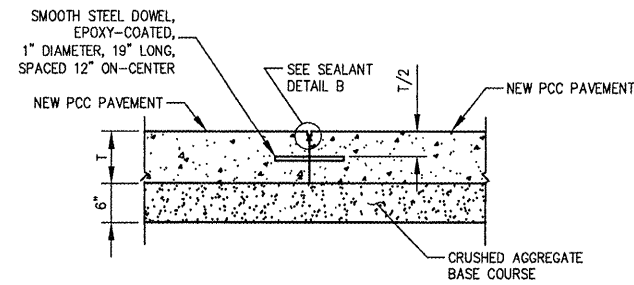


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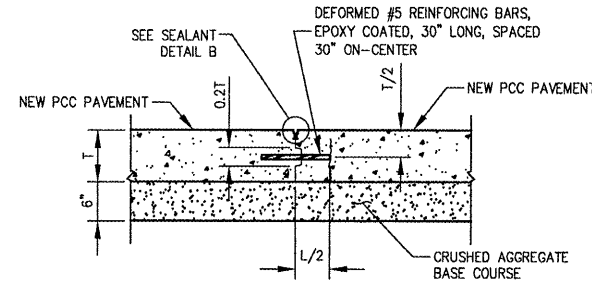
1 George Michas Drive
Romeoville, Illinois 60446
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COMPLETE TAXIWAY G

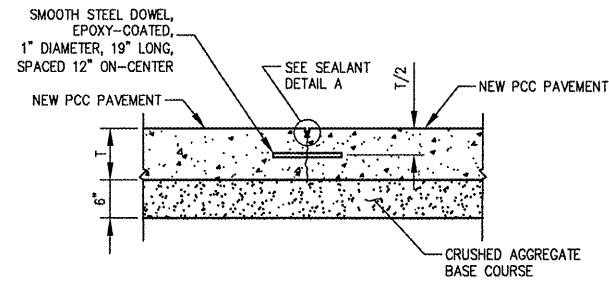
AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



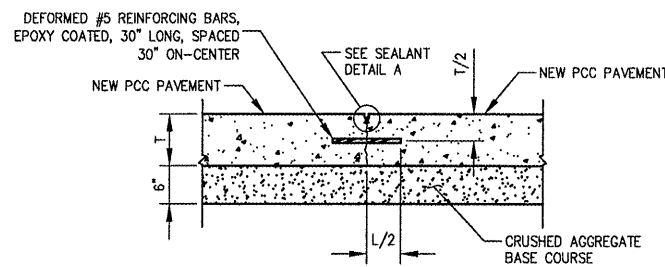
**TYPE D - DOWELED
CONSTRUCTION JOINT**



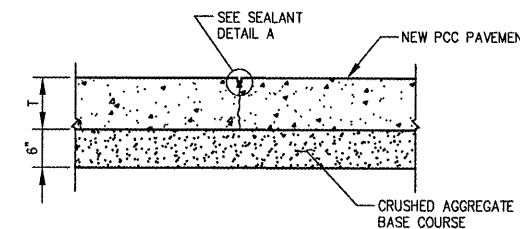
**TYPE E - HINGED-KEYED
CONSTRUCTION JOINT**



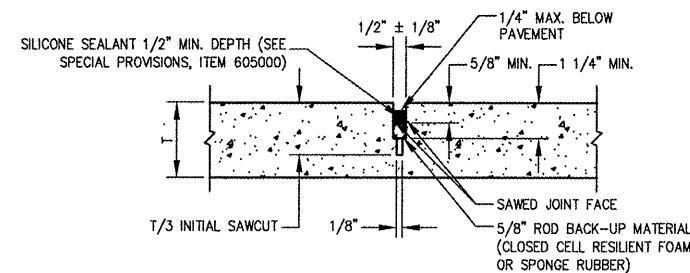
**TYPE F - DOWELED
CONTRACTION JOINT**



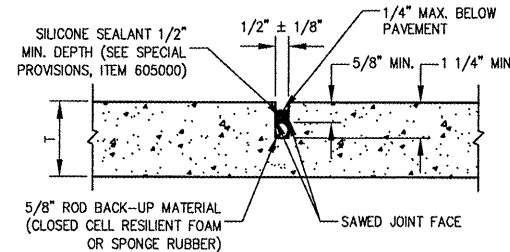
**TYPE G - HINGED
CONTRACTION JOINT**



**TYPE H - SAWED
CONTRACTION JOINT**



DETAIL A - SEALANT



DETAIL B - SEALANT

DETAILS SHOWN ARE NOT TO SCALE

No.	Drawing Issue Description	Date	By

Date

JANUARY 30, 2009

Sheet Title

**P.C.C. PAVEMENT AND
JOINTING DETAILS**

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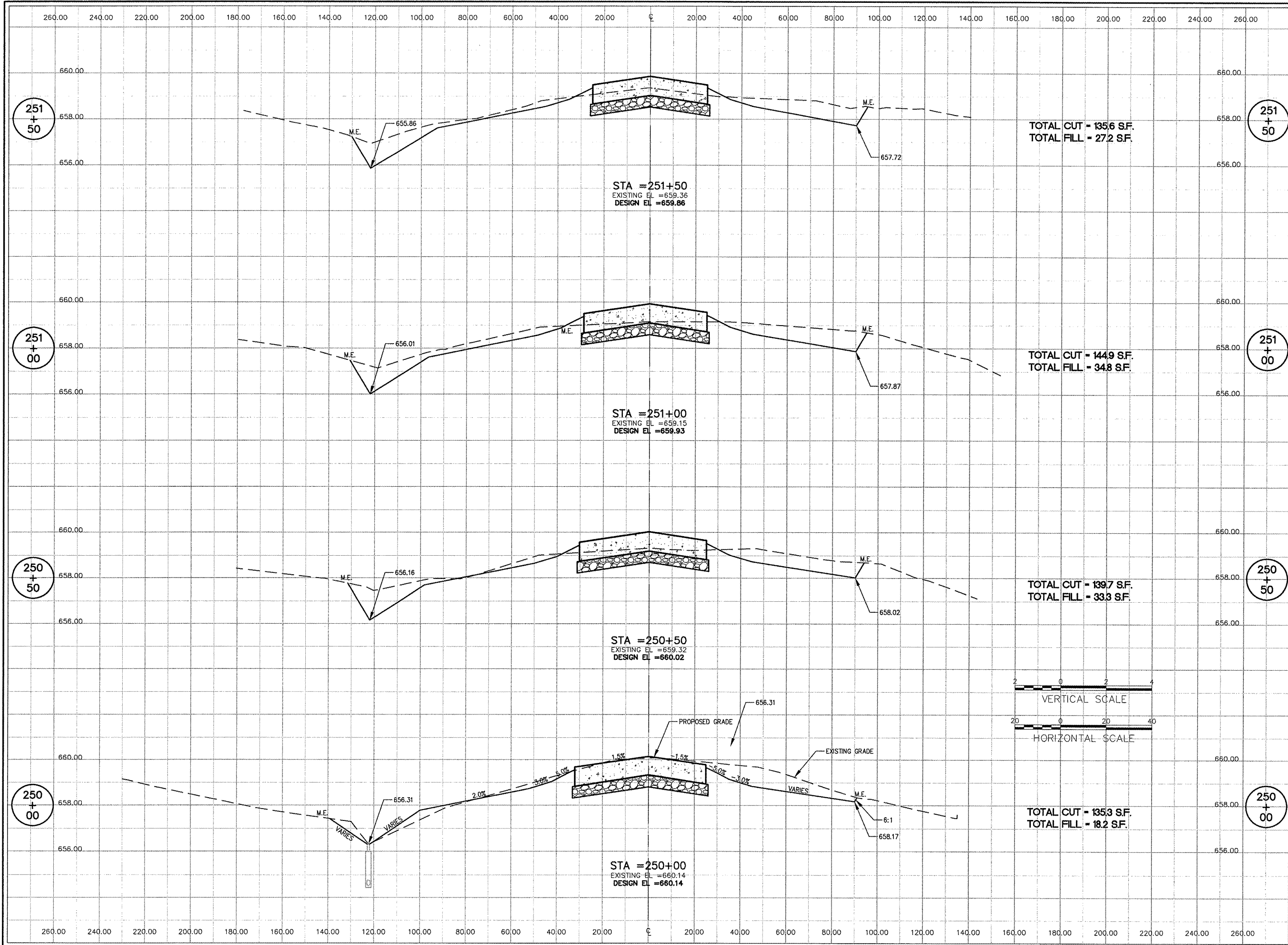
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IDA PROJECT NO. LOT-3883

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Date
JANUARY 30, 2009
Sheet Title

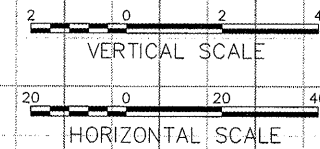
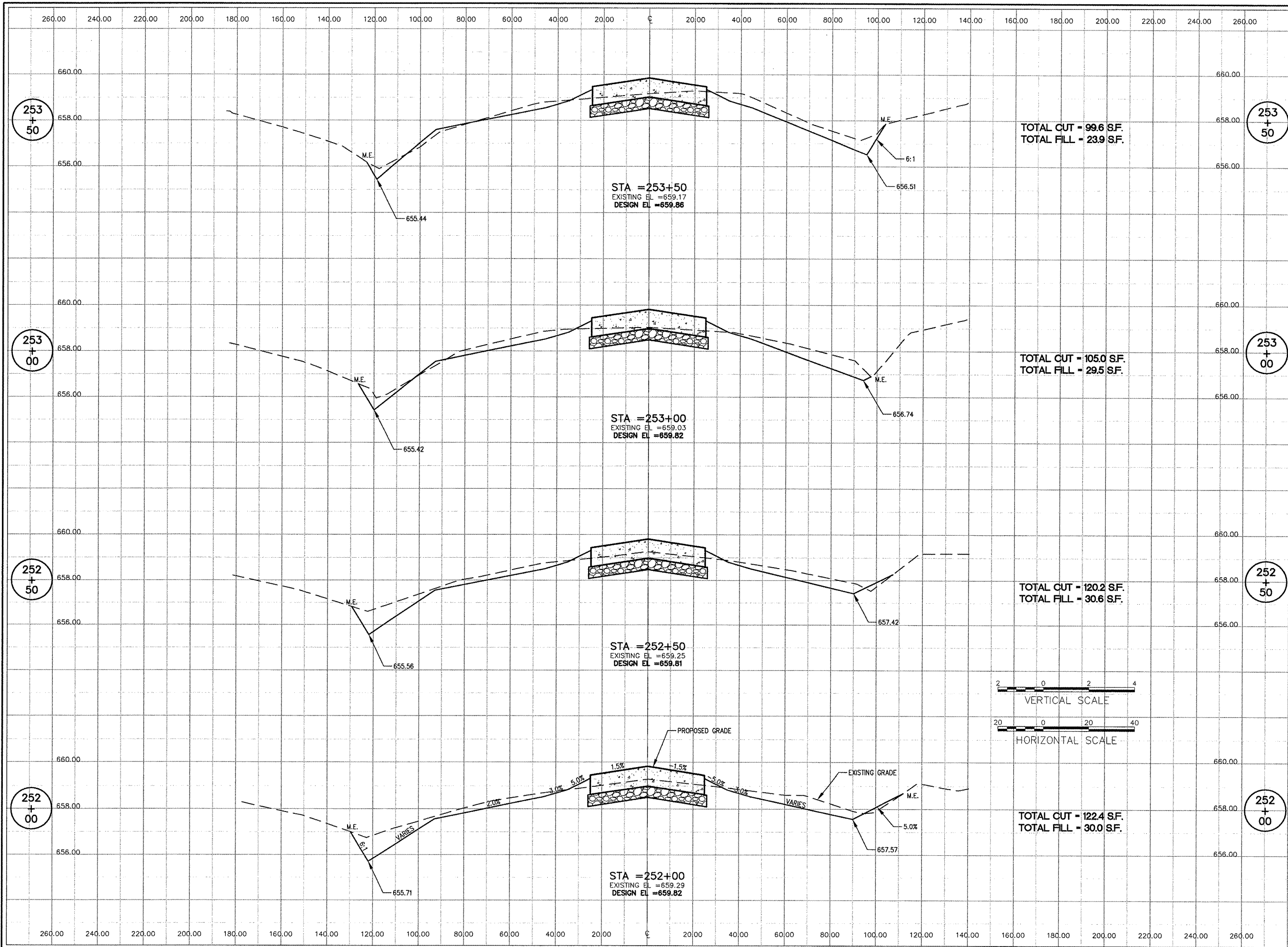
TAXIWAY G
CROSS SECTIONS
250+00 - 251+50

08A0122
Project Number
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LDH 01/12/09
Designed By Date
RMH 01/30/09
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AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

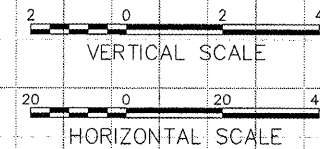
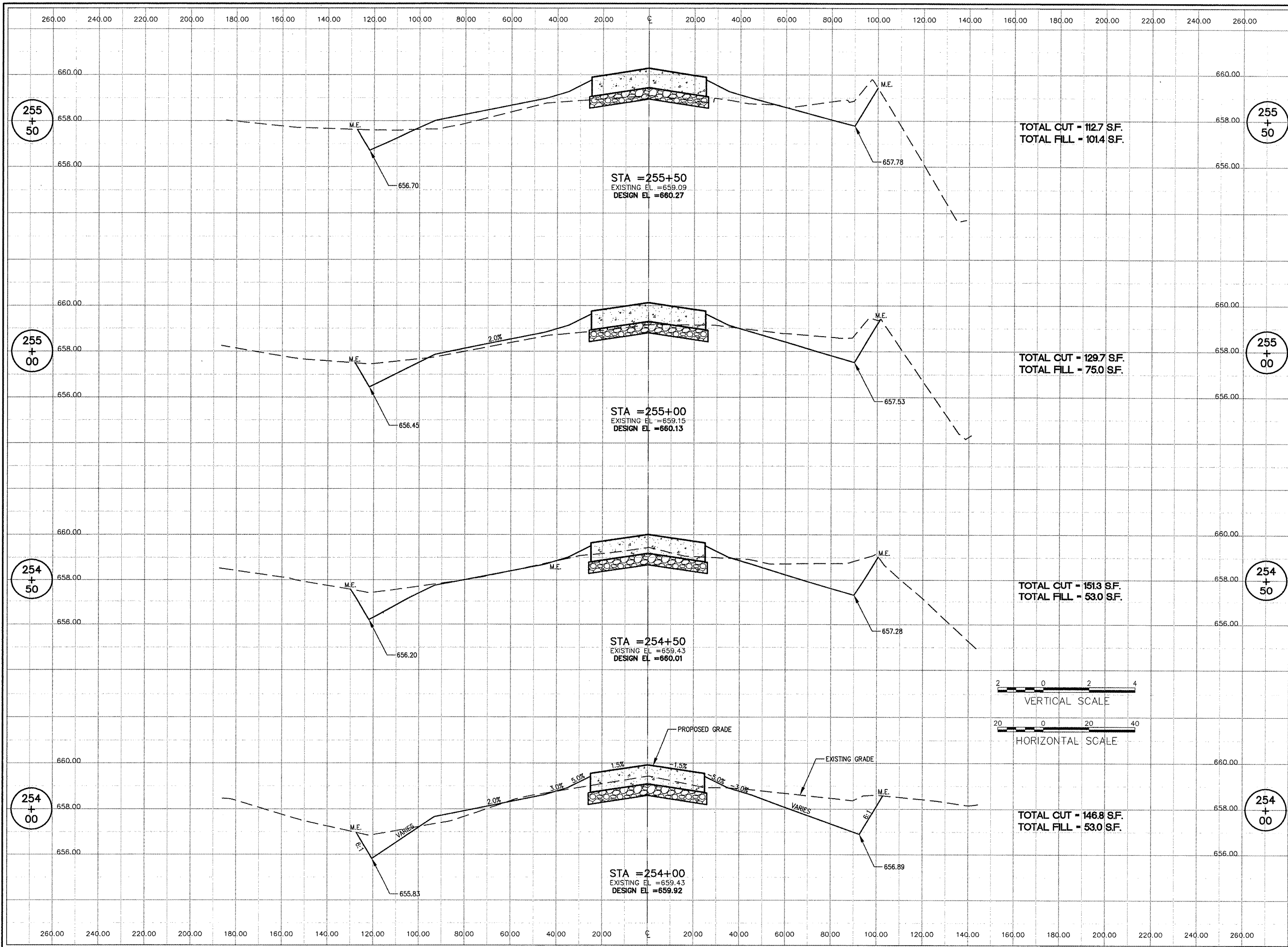
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Date: JANUARY 30, 2009
Sheet Title

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CROSS SECTIONS
252+00 - 253+50

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Project Number
LDH 01/12/09
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LDH 01/12/09
Designed By Date
RMH 01/30/09
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 IDA PROJECT NO. LOT-3883

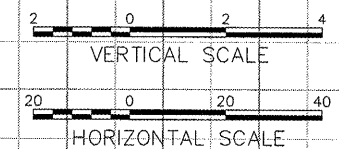
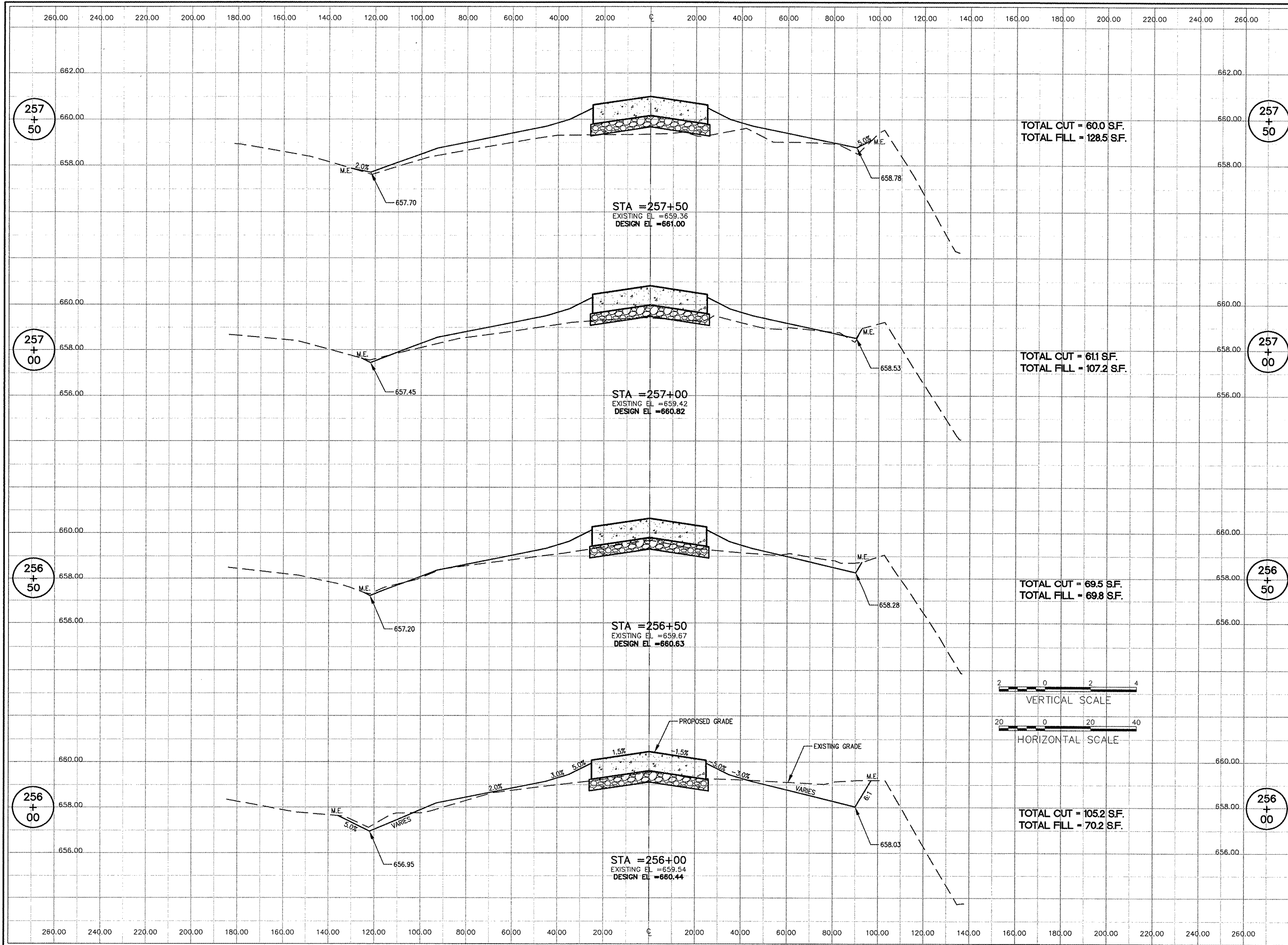
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Date: JANUARY 30, 2009
 Sheet Title:

**TAXIWAY G
 CROSS SECTIONS
 254+00 - 255+50**

08A0122
 Project Number
 LDH 01/12/09
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 IDA PROJECT NO. LOT-3883

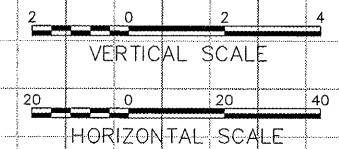
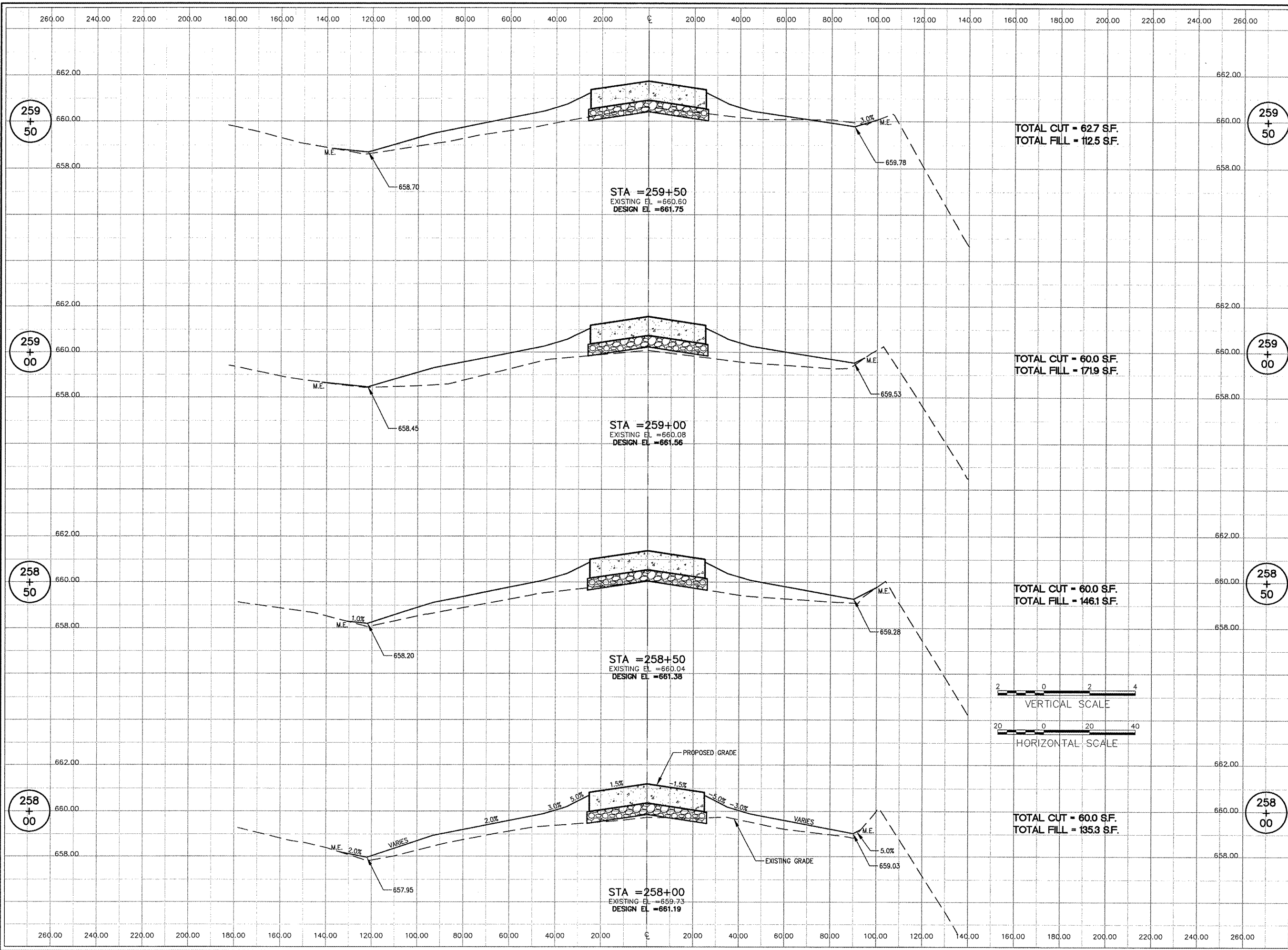
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Date
 JANUARY 30, 2009
 Sheet Title

TAXIWAY G
 CROSS SECTIONS
 256+00 - 257+50

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 Project Number
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 LDH 01/12/09
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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
 IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date: JANUARY 30, 2009

Sheet Title: TAXIWAY G CROSS SECTIONS 258+00 - 259+50

**TAXIWAY G
 CROSS SECTIONS
 258+00 - 259+50**

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 LDH 01/12/09
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 RMH 01/30/09
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Sheet No. _____

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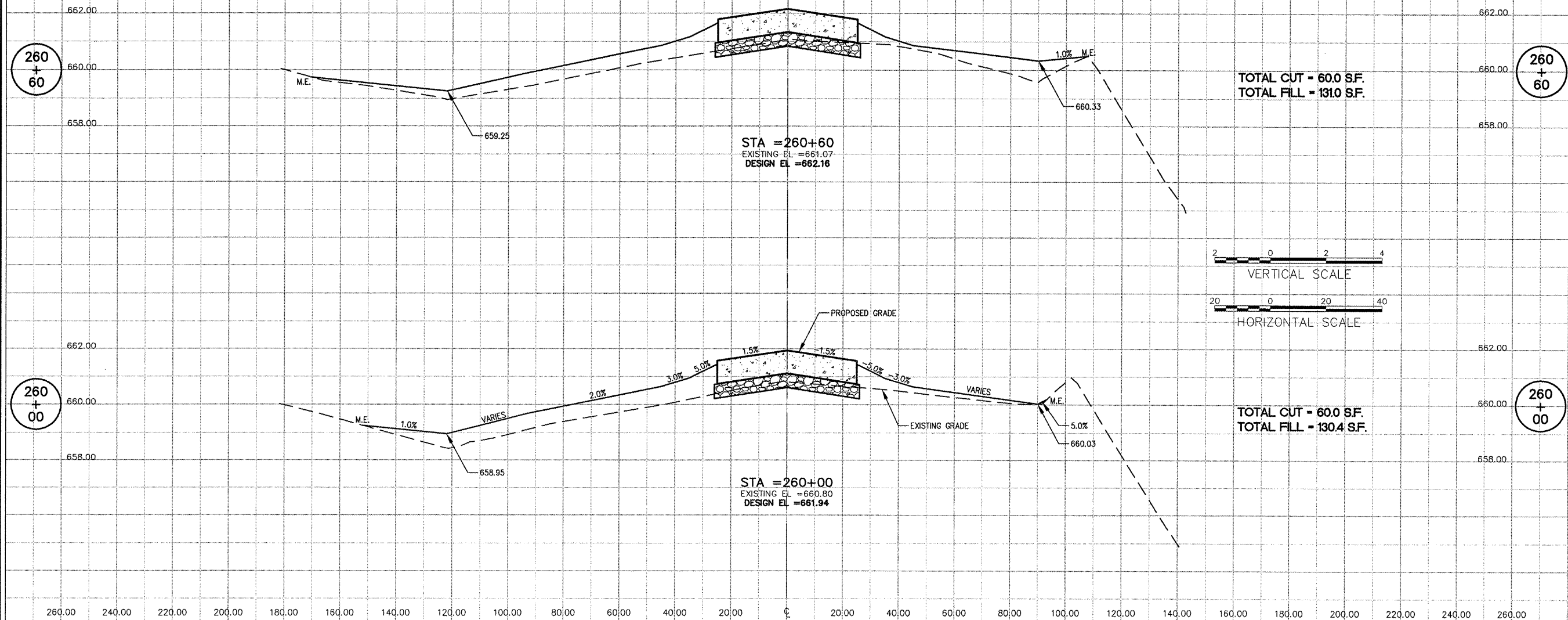


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AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009

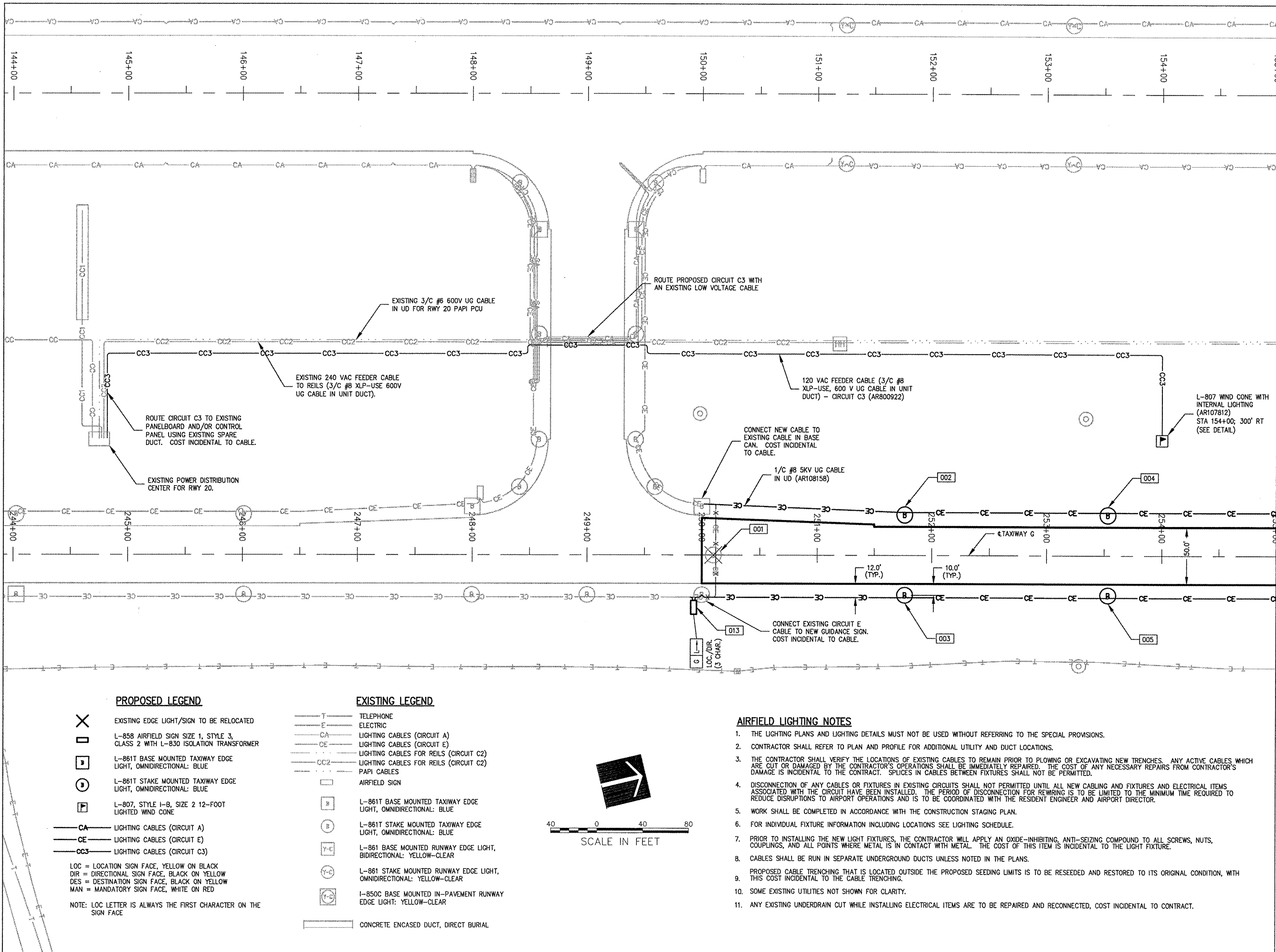
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**TAXIWAY G
CROSS SECTIONS
260+00 - 260+60**

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LDH 01/12/09
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LDH 01/12/09
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MATCH LINE STA. 255+00

PROPOSED LEGEND

- EXISTING EDGE LIGHT/SIGN TO BE RELOCATED
- L-858 AIRFIELD SIGN SIZE 1, STYLE 3, CLASS 2 WITH L-830 ISOLATION TRANSFORMER
- L-861T BASE MOUNTED TAXIWAY EDGE LIGHT, OMNIDIRECTIONAL: BLUE
- L-861T STAKE MOUNTED TAXIWAY EDGE LIGHT, OMNIDIRECTIONAL: BLUE
- L-807, STYLE I-B, SIZE 2 12-FOOT LIGHTED WIND CONE
- LIGHTING CABLES (CIRCUIT A)
- LIGHTING CABLES (CIRCUIT E)
- LIGHTING CABLES (CIRCUIT C3)
- LOC = LOCATION SIGN FACE, YELLOW ON BLACK
- DIR = DIRECTIONAL SIGN FACE, BLACK ON YELLOW
- DES = DESTINATION SIGN FACE, BLACK ON YELLOW
- MAN = MANDATORY SIGN FACE, WHITE ON RED
- NOTE: LOC LETTER IS ALWAYS THE FIRST CHARACTER ON THE SIGN FACE

EXISTING LEGEND

- TELEPHONE
- ELECTRIC
- LIGHTING CABLES (CIRCUIT A)
- LIGHTING CABLES (CIRCUIT E)
- LIGHTING CABLES FOR REILS (CIRCUIT C2)
- LIGHTING CABLES FOR REILS (CIRCUIT C2)
- PAPI CABLES
- AIRFIELD SIGN
- L-861T BASE MOUNTED TAXIWAY EDGE LIGHT, OMNIDIRECTIONAL: BLUE
- L-861T STAKE MOUNTED TAXIWAY EDGE LIGHT, OMNIDIRECTIONAL: BLUE
- L-861 BASE MOUNTED RUNWAY EDGE LIGHT, BIDIRECTIONAL: YELLOW-CLEAR
- L-861 STAKE MOUNTED RUNWAY EDGE LIGHT, OMNIDIRECTIONAL: YELLOW-CLEAR
- L-850C BASE MOUNTED IN-PAVEMENT RUNWAY EDGE LIGHT: YELLOW-CLEAR
- CONCRETE ENCASED DUCT, DIRECT BURIAL

AIRFIELD LIGHTING NOTES

1. THE LIGHTING PLANS AND LIGHTING DETAILS MUST NOT BE USED WITHOUT REFERRING TO THE SPECIAL PROVISIONS.
2. CONTRACTOR SHALL REFER TO PLAN AND PROFILE FOR ADDITIONAL UTILITY AND DUCT LOCATIONS.
3. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF EXISTING CABLES TO REMAIN PRIOR TO PLOWING OR EXCAVATING NEW TRENCHES. ANY ACTIVE CABLES WHICH ARE CUT OR DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE IMMEDIATELY REPAIRED. THE COST OF ANY NECESSARY REPAIRS FROM CONTRACTOR'S DAMAGE IS INCIDENTAL TO THE CONTRACT. SPLICES IN CABLES BETWEEN FIXTURES SHALL NOT BE PERMITTED.
4. DISCONNECTION OF ANY CABLES OR FIXTURES IN EXISTING CIRCUITS SHALL NOT BE PERMITTED UNTIL ALL NEW CABLING AND FIXTURES AND ELECTRICAL ITEMS ASSOCIATED WITH THE CIRCUIT HAVE BEEN INSTALLED. THE PERIOD OF DISCONNECTION FOR REWIRING IS TO BE LIMITED TO THE MINIMUM TIME REQUIRED TO REDUCE DISRUPTIONS TO AIRPORT OPERATIONS AND IS TO BE COORDINATED WITH THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
5. WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE CONSTRUCTION STAGING PLAN.
6. FOR INDIVIDUAL FIXTURE INFORMATION INCLUDING LOCATIONS SEE LIGHTING SCHEDULE.
7. PRIOR TO INSTALLING THE NEW LIGHT FIXTURES, THE CONTRACTOR WILL APPLY AN OXIDE-INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS, COUPLINGS, AND ALL POINTS WHERE METAL IS IN CONTACT WITH METAL. THE COST OF THIS ITEM IS INCIDENTAL TO THE LIGHT FIXTURE.
8. CABLES SHALL BE RUN IN SEPARATE UNDERGROUND DUCTS UNLESS NOTED IN THE PLANS.
9. PROPOSED CABLE TRENCHING THAT IS LOCATED OUTSIDE THE PROPOSED SEEDING LIMITS IS TO BE RESEED AND RESTORED TO ITS ORIGINAL CONDITION, WITH THIS COST INCIDENTAL TO THE CABLE TRENCHING.
10. SOME EXISTING UTILITIES NOT SHOWN FOR CLARITY.
11. ANY EXISTING UNDERDRAIN CUT WHILE INSTALLING ELECTRICAL ITEMS ARE TO BE REPAIRED AND RECONNECTED, COST INCIDENTAL TO CONTRACT.

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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

**LIGHTING PLAN
TAXIWAY G
STA. 244+00 THRU
STA. 255+00**

08A0122
Project Number
LDH 06/18/08
Layout By Date
LDH 06/18/08
Designed By Date
RMH 01/30/09
Reviewed By Date
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22



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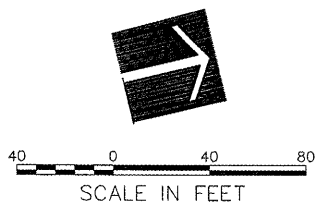
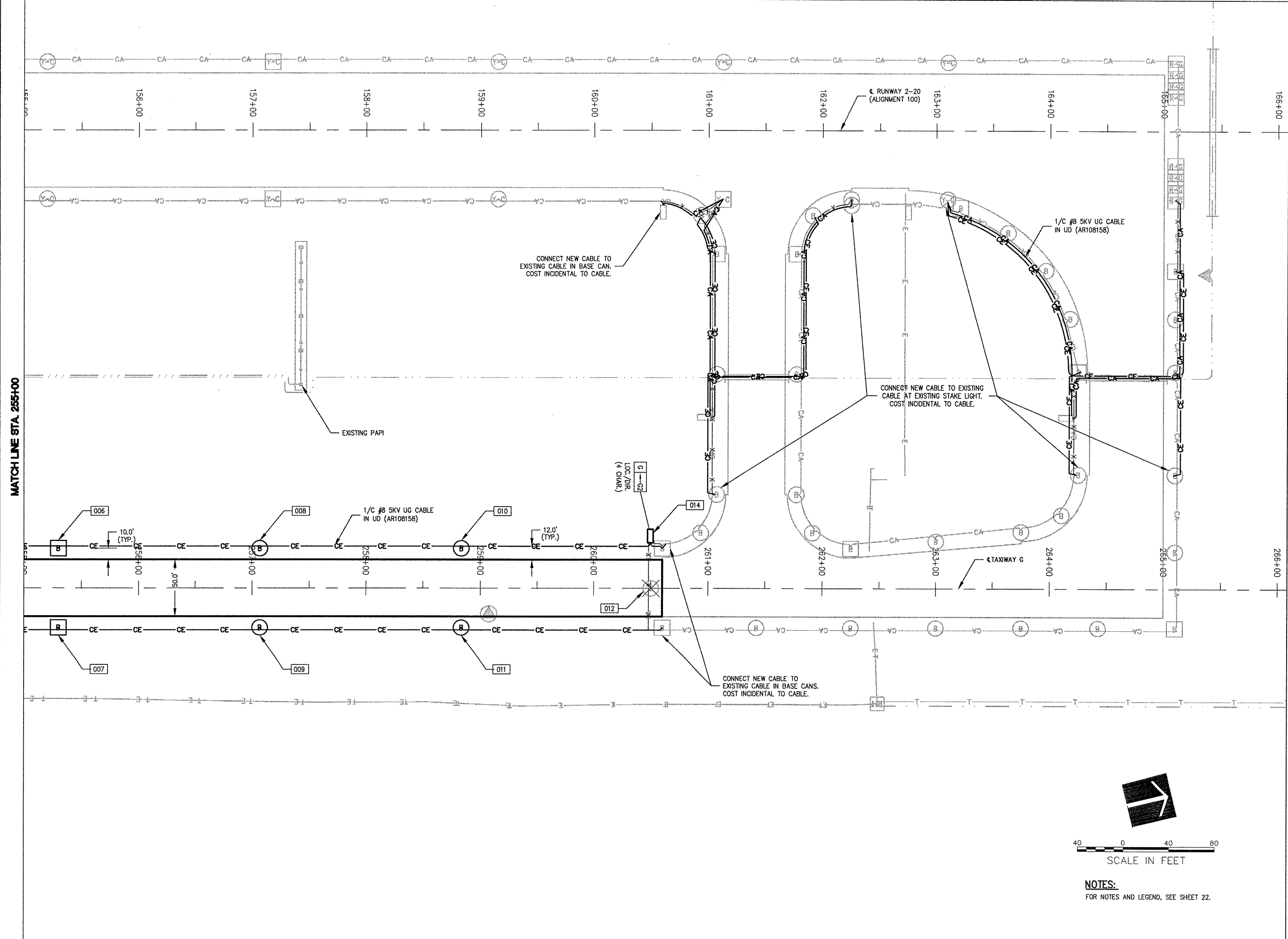
COMPLETE TAXIWAY G
AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

LIGHTING PLAN
TAXIWAY G
STA. 255+00 THRU
STA. 266+00

08A0122
Project Number
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Layout By Date
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NOTES:
FOR NOTES AND LEGEND, SEE SHEET 22.

AIRFIELD LIGHTING NOTES

1. UNLESS OTHERWISE NOTED, ALL UNDERGROUND AIRFIELD LIGHTING SERIES CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE FAA APPROVED 5000 VOLT L-824 TYPE. ALL UNDERGROUND FIELD POWER LOW VOLTAGE (600 VOLT & BELOW) CIRCUIT CONDUCTORS WHETHER DEB OR IN DUCT/CONDUIT SHALL BE UL LISTED 600 VOLT, TYPE XLP-USE-2 COPPER CONDUCTORS. CONDUCTOR SIZES SHALL BE AS SPECIFIED, HEREIN.
2. NO COMPONENTS OF PRIMARY CIRCUIT SUCH AS CABLE, CONNECTORS AND TRANSFORMERS SHALL BE BROUGHT ABOVE GROUND AT EDGE LIGHTS, SIGNS, REIL, PAPI, ETC.
3. THERE SHALL BE NO EXPOSED POWER/CONTROL CABLES BETWEEN THE POINT WHERE THEY LEAVE THE UNDERGROUND (DEB OR L-867 BASES) AND WHERE THEY ENTER THE EQUIPMENT (SUCH AS TAXIWAY SIGNS, PAPI, REIL, ETC.) ENCLOSURES. THESE CABLES SHALL BE ENCLOSED IN RIGID CONDUIT OR IN FLEXIBLE, WATERTIGHT CONDUIT WITH BREAKABLE COUPLING(S) AT THE GRADE OR THE HOUSING COVER, AS SHOWN IN APPLICABLE DETAILS.
4. THE JOINTS OF THE L-823 PRIMARY CONNECTORS SHALL BE WRAPPED WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF THE JOINT, AS SHOWN ON SHEET NO. 9.
5. THE CABLE ENTRANCE INTO THE FIELD--ATTACHED L-823 CONNECTORS SHALL BE ENCLOSED BY A HEAT-SHRINKABLE TUBING WITH CONTINUOUS INTERNAL ADHESIVE, AS SHOWN ON SHEET NO. 9.
6. L-823 TYPE II, TWO-CONDUCTOR SECONDARY CONNECTORS SHALL BE CLASS 'A' (FACTORY MOLDED).
7. THERE SHALL BE NO SPLICES IN THE SECONDARY CABLE(S) WITHIN THE STEMS OF A RUNWAY/TAXIWAY EDGE/THRESHOLD LIGHTING FIXTURE AND THE WIREWAYS LEADING TO TAXIWAY SIGNS AND PAPI/REIL EQUIPMENT.
8. ELECTRICAL INSULATING GREASE SHALL BE APPLIED WITHIN THE L-823, SECONDARY, TWO CONDUCTOR CONNECTORS TO PREVENT WATER ENTRANCE. THESE CONNECTORS SHALL NOT BE TAPED.
9. DEB ISOLATION TRANSFORMERS SHALL BE BURIED AT A DEPTH OF TEN (10") INCHES ON A LINE CROSSING THE LIGHT AND PERPENDICULAR TO THE RUNWAY/TAXIWAY CENTERLINE AT A LOCATION TWELVE (12") INCHES FROM THE LIGHT OPPOSITE FROM THE RUNWAY/TAXIWAY.
10. A SLACK OF THREE (3') FEET, MINIMUM, SHALL BE PROVIDED IN THE PRIMARY CABLE AT EACH TRANSFORMER/CONNECTOR TERMINATION. AT STAKE-MOUNTED LIGHTS, THE SLACK SHALL BE LOOSELY COILED IMMEDIATELY BELOW THE ISOLATION TRANSFORMER.
11. DIRECTION OF PRIMARY CABLES SHALL BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING LIGHT WITH BACK TO PAVEMENT, CABLE TO THE LEFT IS CODED RED AND CABLE TO RIGHT IS CODED BLUE. THIS APPLIES TO STAKE MOUNTED LIGHTS AND BASE MOUNTED LIGHTS WHERE THE BASE HAS ONLY ONE ENTRANCE.
12. L-867 BASES SHALL BE SIZE B, 24" DEEP, CLASS I, UNLESS OTHERWISE NOTED.
13. BASE MOUNTED BREAKABLE COUPLINGS SHALL NOT HAVE WEEP HOLES TO THE OUTSIDE. PLUGGED UP HOLES SHALL NOT BE ACCEPTABLE. IT SHALL BE A 1/4" DIAMETER, MINIMUM, OR EQUIVALENT OPENING FOR DRAINAGE FROM THE SPACE AROUND THE SECONDARY CONNECTOR INTO THE L-867 BASE.
14. THE ELEVATION OF THE BREAKABLE COUPLING GROOVE SHALL NOT EXCEED 1-1/2" ABOVE THE EDGE OF THE COVER IN CASE OF BASE MOUNTED COUPLINGS, OR THE TOP OF THE STAKE IN CASE OF STAKE MOUNTED COUPLINGS.
15. WHERE THE BREAKABLE COUPLING IS NOT AN INTEGRAL PART OF THE LIGHT FIXTURE STEM OR MOUNTING LEG, A BEAD OF SILICON SEAL SHALL BE APPLIED COMPLETELY AROUND LIGHT STEM OR WIREWAY AT BREAKABLE COUPLING TO PROVIDE A WATERTIGHT SEAL.
16. TOPS OF THE STAKES SUPPORTING LIGHT FIXTURES SHALL BE FLUSH WITH THE SURROUNDING GRADE.
17. PLASTIC LIGHTING FIXTURE COMPONENTS, SUCH AS LAMP HEADS, STEMS, BREAKABLE COUPLINGS, BASE COVERS, BRACKETS, STAKES, SHALL NOT BE ACCEPTABLE.
18. THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE: ONE (1) INCH. IN CASE OF STAKE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE STAKE AND THE TOP OF THE LENS. IN CASE OF BASE MOUNTED LIGHTS, THE SPECIFIED LIGHTING FIXTURE HEIGHT SHALL BE MEASURED BETWEEN THE TOP OF THE BASE FLANGE AND THE TOP OF THE LENS, THUS INCLUDING THE BASE COVER, THE FRANGIBLE COUPLING, THE STEM, THE LAMP HOUSING AND THE LENS.

GROUNDING NOTES FOR AIRFIELD LIGHTING

19. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ONE (1) INCH. THIS ALSO APPLIES AT INTERSECTIONS TO LATERAL SPACING BETWEEN LIGHTS OF A RUNWAY/TAXIWAY AND THE INTERSECTING RUNWAY/TAXIWAY.
20. ENTRANCES INTO L-867 BASES SHALL HAVE CONDUIT COUPLINGS OR REDUCERS TO INTERFACE UNIT DUCT/CONDUIT TO L-867 BASE HUBS, OR SHALL BE SEALED WITH HEAT SHRINK AS SHOWN IN DETAIL "B" ON SHEET NO. 8.
21. GALVANIZED/PAINTED EQUIPMENT/COMPONENT SURFACES SHALL NOT BE DAMAGED BY DRILLING, FILING, ETC. DRAIN HOLES IN METAL TRANSFORMER HOUSINGS SHALL BE MADE BEFORE GALVANIZING.
22. EDGE LIGHT NUMBERING TAGS SHALL BE FACING THE PAVEMENT.
23. CABLE/SPLICE/DUCT MARKERS SHALL BE PRECAST CONCRETE OF THE SIZE SHOWN. LETTERS/NUMBERS/ARROWS FOR THE LEGEND TO BE IMPRESSED INTO THE TOPS OF THE MARKERS SHALL BE PRE-ASSEMBLED AND SECURED IN THE MOLD BEFORE THE CONCRETE IS POURED. LEGEND INSCRIBED BY HAND IN WET CONCRETE SHALL NOT BE ACCEPTABLE.
24. ALL UNDERGROUND CABLE RUNS SHALL BE IDENTIFIED BY CABLE MARKERS AT 200 FEET MAXIMUM SPACING, WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS SHALL BE INSTALLED IMMEDIATELY ABOVE THE CABLES.
25. THERE SHALL BE NO SPLICES BETWEEN THE ISOLATION TRANSFORMERS. L-823 CONNECTORS ARE ALLOWED AT TRANSFORMER CONNECTIONS ONLY, UNLESS OTHERWISE SHOWN.
26. APPLY AN OXIDE INHIBITING, ANTI-SEIZING COMPOUND TO ALL SCREWS, NUTS AND BREAKAGE COUPLING THREADS.
27. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS SHALL BE IDENTIFIED BY DUCT MARKERS.
28. WHERE A PARALLEL, CONSTANT VOLTAGE PAPI SYSTEM IS PROVIDED, THE "T" SPLICES SHALL BE OF THE CAST TYPE.
29. CONCRETE USED FOR SLABS, FOOTINGS, BACKFILL AROUND TRANSFORMER HOUSINGS, MARKINGS, ETC. SHALL BE 3000 PSI, AIR-ENTRAINED.
30. ALL POWER AND CONTROL CABLES IN MAN/HAND HOLES SHALL BE TAGGED. USE EMBOSSED COPPER STRIPS TO BE ATTACHED AT BOTH ENDS TO THE CABLE BY THE USE OF PLASTIC STRAPS. MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MAN/HAND HOLE--ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT.
31. THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 1-800-892-0123. ALSO CONTACT AIRPORT MANAGER AND/OR RESPECTIVE AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. CONTACT FAA FOR ASSISTANCE IN LOCATING THEIR CABLES.
32. WHEN PREPARING CABLE FOR SPLICES, THE CONTRACTOR SHALL USE A CABLE STRIPPER/PENCILLER WHENEVER CABLE CONNECTIONS ARE MADE.

1. GROUNDING FOR RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS SHALL BE AS DETAILED ON THE PLANS AND AS SPECIFIED HEREIN. PER FAA AC 150/5340-30C DESIGN AND INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS, CHAPTER 12, PART 12.6; A SAFETY GROUND MUST BE INSTALLED AT EACH LIGHT FIXTURE. THE PURPOSE OF THE SAFETY GROUND IS TO PROTECT PERSONNEL FROM POSSIBLE CONTACT WITH AN ENERGIZED LIGHT BASE OR MOUNTING STAKE AS THE RESULT OF A SHORTED CABLE OR ISOLATION TRANSFORMER. A SAFETY GROUND SHALL BE INSTALLED AT EACH TRANSFORMER BASE/LIGHT CAN ASSOCIATED WITH RUNWAY LIGHTS, TAXIWAY LIGHTS, AND LIGHTED TAXI GUIDANCE SIGNS. A SAFETY GROUND SHALL ALSO BE INSTALLED AT EACH STAKE MOUNTED LIGHT FIXTURE. THE SAFETY GROUND SHALL BE A #6 AWG BARE COPPER CONDUCTOR CONNECTED TO THE GROUND LUG ON THE RESPECTIVE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE AND A 5/8-INCH DIAMETER BY 8-FOOT LONG (MINIMUM) UL LISTED COPPER CLAD GROUND ROD. CONNECTIONS TO GROUND LUGS ON THE L-867 TRANSFORMER BASE/LIGHT CAN OR MOUNTING STAKE SHALL BE WITH A UL LISTED GROUNDING CONNECTOR. CONNECTIONS TO GROUND RODS SHALL BE MADE WITH EXOTHERMIC WELD TYPE CONNECTORS, CADWELD BY ERICO PRODUCTS, INC., SOLON, OHIO, (PHONE: 800-248-9353), THERMOWELD BY CONTINENTAL INDUSTRIES, INC., TULSA, OKLAHOMA (PHONE: 918-663-1440) OR ULTRAWELD BY HARGER, GRAYSLAKE, ILLINOIS (PHONE: 800-842-7437). EXOTHERMIC WELD CONNECTIONS SHALL BE INSTALLED IN CONFORMANCE WITH THE RESPECTIVE MANUFACTURER'S DIRECTIONS USING MOLDS AS REQUIRED FOR EACH RESPECTIVE APPLICATION. BOLTED CONNECTIONS WILL NOT BE PERMITTED AT GROUND RODS. TOP OF GROUND RODS SHALL BE BURIED 12 INCHES MINIMUM BELOW GRADE, UNLESS SPECIFIED OTHERWISE HEREIN, FOR RESPECTIVE APPLICATIONS.
2. CLEAN ALL METAL SURFACES BEFORE MAKING GROUND CONNECTIONS. METALLIC SURFACES TO BE JOINED SHALL BE PREPARED BY THE REMOVAL OF ALL NON-CONDUCTIVE MATERIAL PER 2008 NATIONAL ELECTRICAL CODE ARTICLE 250-12.
3. PER FAA 150/5340-30C THE RESISTANCE TO GROUND OF THE RESPECTIVE MOUNTING STAKE OR LIGHT BASE (WITH GROUND ROD CONNECTED) MUST BE 25 OHMS OR LESS.

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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

No.	Drawing Issue Description	Date	By

Date
JANUARY 30, 2009
Sheet Title

ELECTRICAL NOTES

08A0122
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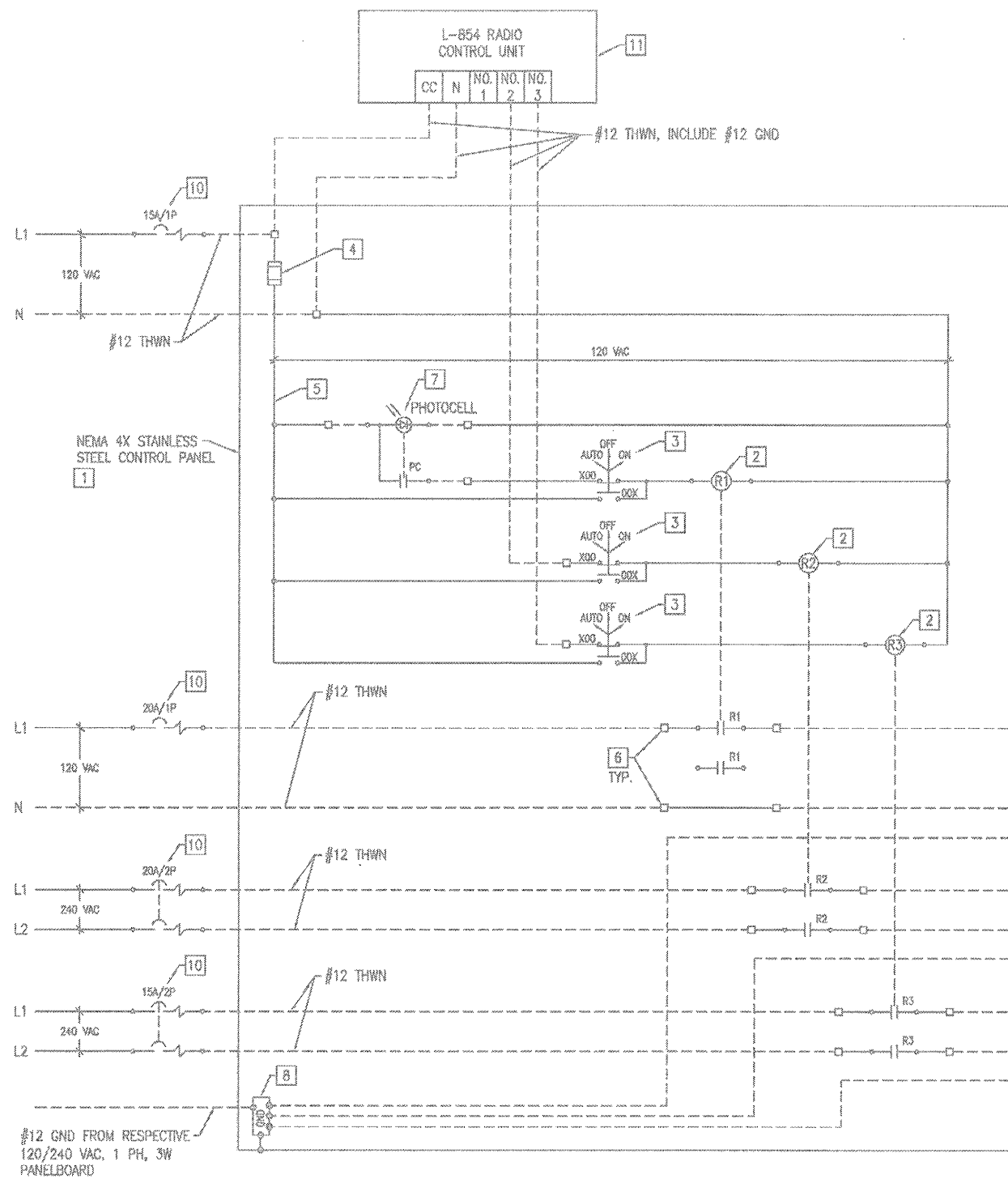


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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883



KEYED NOTES

- 1 EXISTING UL LISTED NEMA 4X STAINLESS STEEL CONTROL PANEL ENCLOSURE ADEQUATELY SIZED TO HOLD THE RESPECTIVE COMPONENTS AND EQUIPMENT. PROVIDE INNER DOOR TO MOUNT SELECTOR SWITCHES. INCLUDE LEGEND PLATES LABELED "RELAY CONTROL PANEL FOR WIND CONE, PAPI, AND REILS". INCLUDE ADDITIONAL LEGEND PLATE TO IDENTIFY THE RESPECTIVE RUNWAY (RUNWAY 2 OR RUNWAY 20).
- 2 EXISTING 30 AMP, 600 VAC, 2 POLE ELECTRICALLY HELD LIGHTING CONTACTOR WITH 120 VAC COIL, SQUARE D CLASS 8903, TYPE SMO1V02, OR APPROVED EQUAL.
- 3 EXISTING THREE-POSITION MAINTAINED "HAND-OFF-AUTO" SELECTOR SWITCH, HEAVY DUTY, WATERTIGHT/OIL TIGHT (NEMA 4/13), SQUARE D CLASS 9001, TYPE KS43FBH13 OR APPROVED EQUAL. INCLUDE LEGEND PLATE LABELED "AUTO-OFF-ON". MOUNT SELECTOR SWITCH ON PANEL ENCLOSURE INNER DOOR.
- 4 EXISTING FUSING FOR CONTROL WIRING SHALL BE 10 AMP, 600 VAC, BUSSMANN CATALOG FNQ-R-10, OR APPROVED EQUAL, WITH FUSE BLOCKS, WITH BOX LUG TERMINALS, SIZED AS REQUIRED FOR THE RESPECTIVE APPLICATION. INCLUDE HARDWARE FOR MOUNTING. PROVIDE ONE BOX (5 MINIMUM QUANTITY) OF EACH TYPE AND SIZE OF FUSE, UPON COMPLETION OF THE JOB FOR USE AS SPARES.
- 5 EXISTING CONTROL WIRING SHALL BE SIZED AS REQUIRED PER NEC MINIMUM #12 AWG TYPE MTW, THW, OR THWN, COPPER. TERMINAL BLOCKS FOR CONTROL WIRING SHALL BE 600 VOLT, WITH AMPERAGE RATINGS IN CONFORMANCE WITH NEC TABLE 310-16 USING 75 DEGREE C WIRE FOR THE RESPECTIVE WIRE LUG RANGE, BOX LUG TYPE, SQUARE D CLASS 9080, TYPE G, OR APPROVED EQUAL.
- 6 EXISTING TERMINAL BLOCKS FOR BRANCH CIRCUITS SHALL BE SQUARE D CLASS 9080, TYPE G, OR APPROVED EQUAL SIZED AS REQUIRED FOR THE RESPECTIVE CONDUCTORS.
- 7 EXISTING PHOTOCELL RATED 2000 WATTS AT 120 VAC, WITH OFF DELAY, AND -40 DEGREE C TO 60 DEGREE C OPERATING TEMPERATURE RANGE, TORK MODEL NO. 2101, OR APPROVED EQUAL.
- 8 EXISTING EQUIPMENT GROUNDING BAR: PROVIDE A GROUNDING BAR MOUNTED AND BONDED INSIDE THE PANEL ENCLOSURE, ADEQUATELY SIZED TO ACCOMMODATE ALL GROUND CONDUCTORS TO OR FROM THE CONTROL PANEL.
- 9 EXISTING LEGEND PLATES: LEGEND PLATES SHALL BE REQUIRED FOR THE CONTROL PANEL, ALL RELAYS, SELECTOR SWITCHES, AND TO IDENTIFY THE RESPECTIVE INPUT AND OUTPUT BRANCH CIRCUITS. LEGEND PLATES SHALL BE WEATHERPROOF AND ABRASION RESISTANT PHENOLIC MATERIALS. LETTERING SHALL BE BLACK ON WHITE BACKGROUND, UNLESS OTHERWISE NOTED.
- 10 EXISTING BRANCH CIRCUIT BREAKER FROM THE RESPECTIVE PANELBOARD.
- 11 EXISTING L-854 RADIO CONTROL UNIT IN A NEMA 4 WEATHERPROOF ENCLOSURE WITH ANTENNA.

#8 XLP-USE, 600V CABLES IN UNIT DUCT. CABLE INSULATION SHALL BE COLOR CODED PHASE A - BLACK, NEUTRAL - WHITE, GROUND - GREEN. INTERFACE NEW CABLE TO EXISTING LIGHTING CONTACTOR/RELAY CONTROL PANEL. ITEM AR800922.

TO WIND CONE
EXISTING #8 FAA L-824, TYPE C, 600V CABLES IN UNIT DUCT
TO PAPI
EXISTING #8 FAA L-824, TYPE C, 600V CABLES IN UNIT DUCT
TO REILS

NOTE

1. WIND CONE SHALL BE ACTIVATED BY PHOTOCELL IN AUTOMATIC MODE.

EXISTING RELAY CONTROL PANEL FOR WIND CONE, PAPI'S, & REILS

L 10840851 (08/04) (08/01) (2) DRAWINGS SHEETS (29) CONTROL DETAILS.DWG FEB 27, 2009 11:04AM HAUSMOR02

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Date
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WIND CONE CONTROL DETAILS

08A0122

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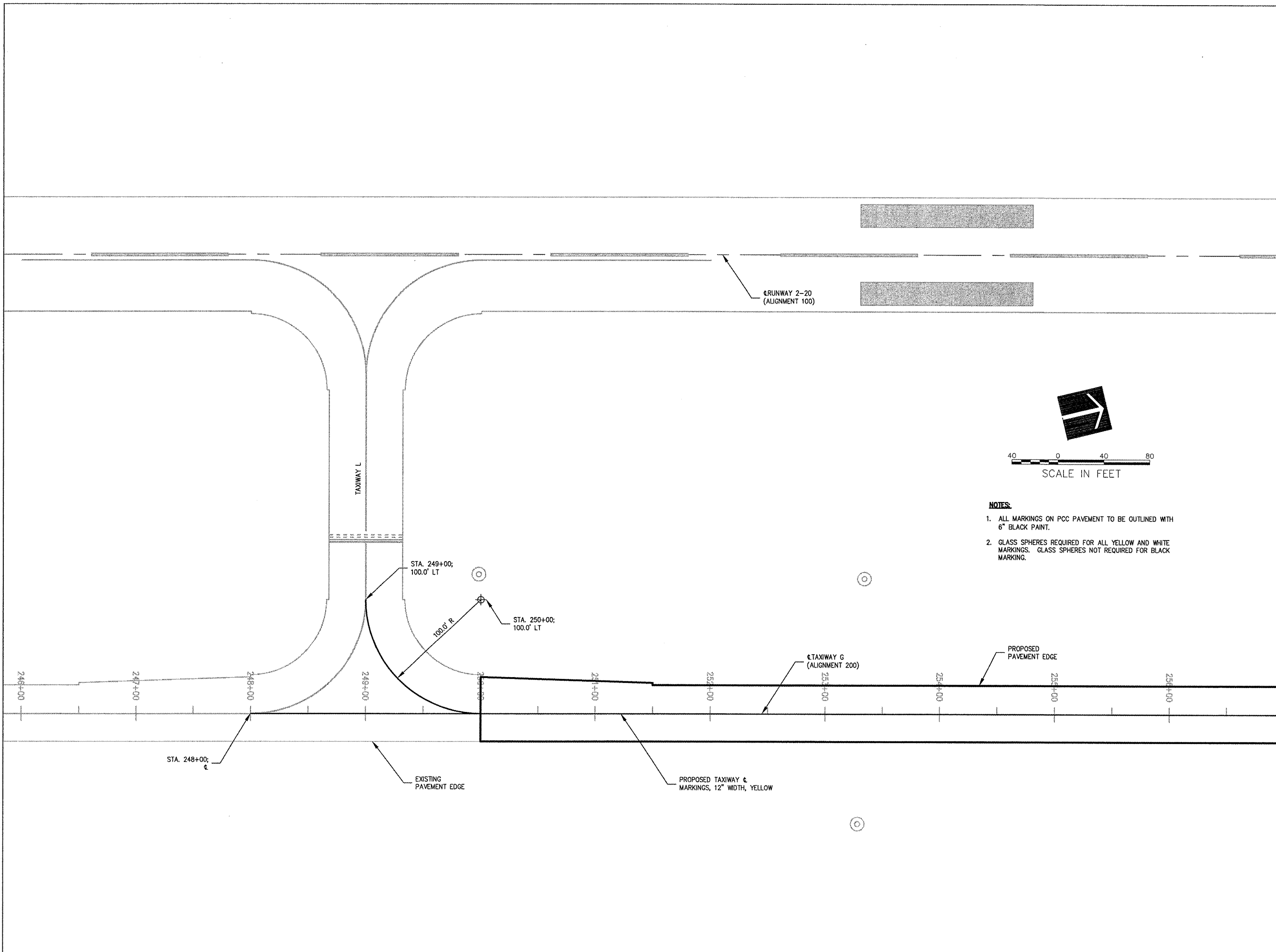
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COMPLETE TAXIWAY G

AIP PROJECT NO. 3-17-0140-B43
IDA PROJECT NO. LOT-3883

MATCHLINE STA. 257+00



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**MARKING PLAN
TAXIWAY G
STA. 246+00 THRU
STA. 257+00**

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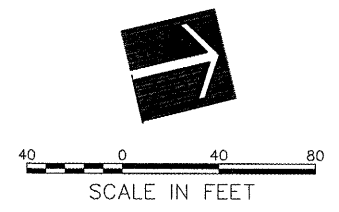
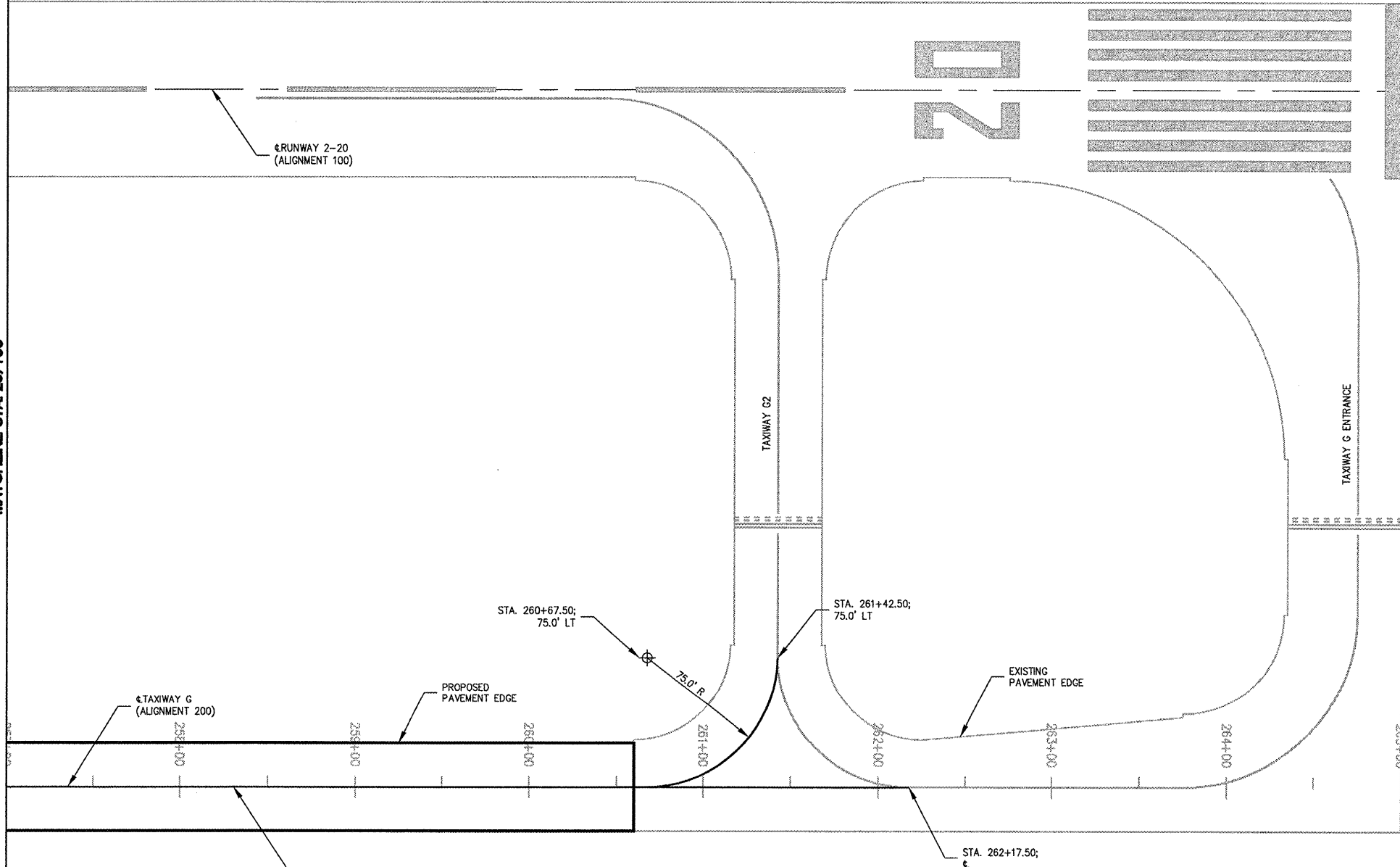
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IDA PROJECT NO. LOT-3883

MATCHLINE STA. 257+00



NOTES:

1. ALL MARKINGS ON PCC PAVEMENT TO BE OUTLINED WITH 6" BLACK PAINT.
2. GLASS SPHERES REQUIRED FOR ALL YELLOW AND WHITE MARKINGS. GLASS SPHERES NOT REQUIRED FOR BLACK MARKING.

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JANUARY 30, 2009
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**MARKING PLAN
TAXIWAY G
STA. 257+00 THRU
STA. 268+00**

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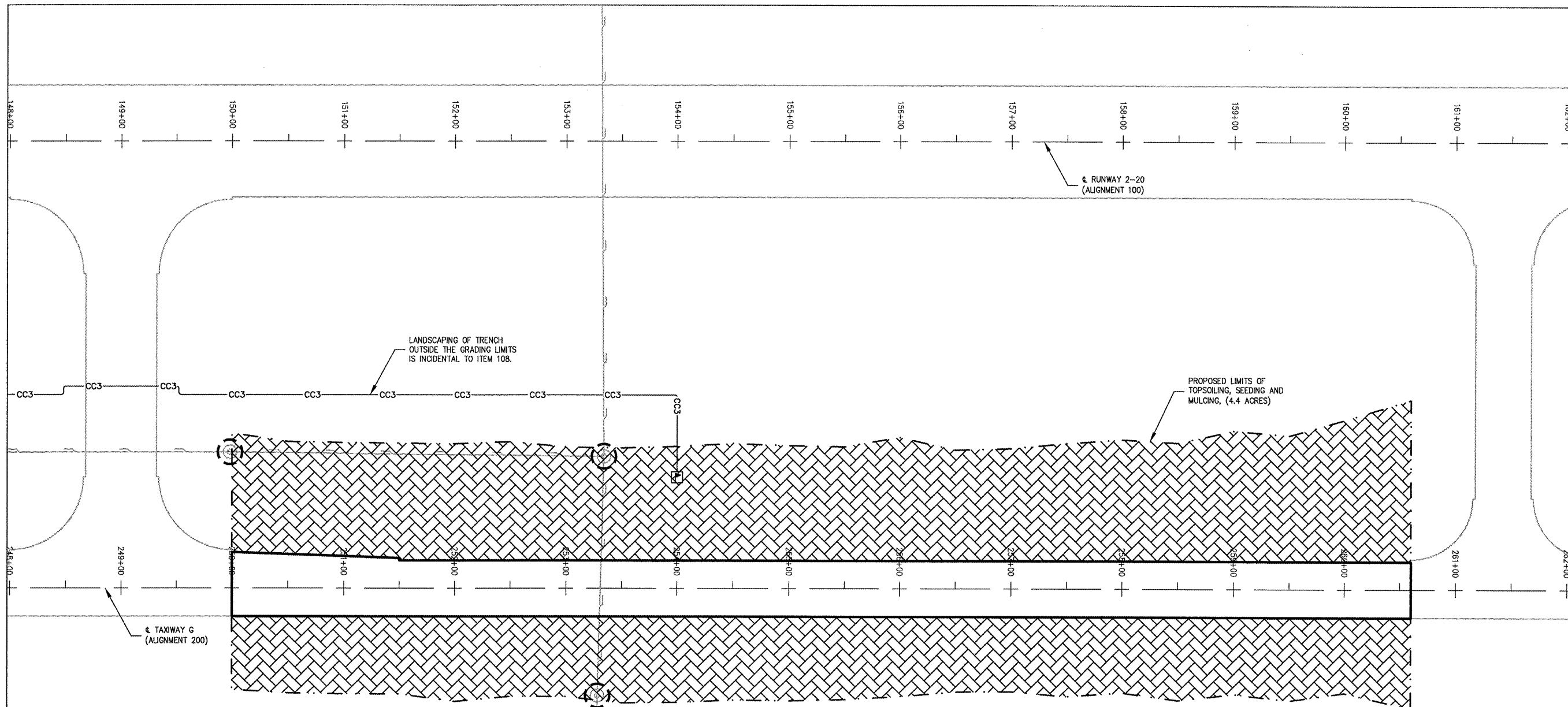


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LANDSCAPING OF TRENCH
OUTSIDE THE GRADING LIMITS
IS INCIDENTAL TO ITEM 108.

PROPOSED LIMITS OF
TOPSOILING, SEEDING AND
MULCHING, (4.4 ACRES)

INLET PROTECTION AT
EXISTING MANHOLE, SEE
DETAIL (TYP., 3 LOCATIONS)

STORM WATER POLLUTION PREVENTION NOTES

GENERAL

THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

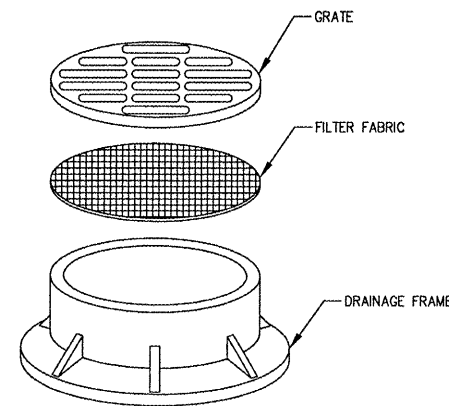
POLLUTION PREVENTION MEASURES

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHERE EVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

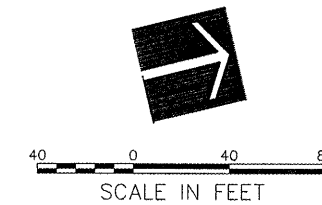
ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.



INLET PROTECTION - DRAINAGE STRUCTURE FILTER WRAP

NOTES:

1. FILTER WRAP TO BE PLACED IN ALL MANHOLES AS SHOWN.
2. FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR SILT FENCE.
3. FABRIC SHALL OVERLAY FRAME BY 2 INCHES (MINIMUM).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
6. COST OF FILTER WRAP SHALL BE INCIDENTAL TO INLET PROTECTION.



LANDSCAPING NOTES:

1. SEE CROSS SECTIONS AND STAKING PLANS FOR ADDITIONAL CONSTRUCTION LIMITS INFORMATION.

LEGEND

- PROPOSED SEEDING
- LIMITS OF TOPSOILING AND SODDING
- PROPOSED INLET PROTECTION (3 EACH)

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No.	Drawing Issue Description	Date	By

Date: JANUARY 30, 2009
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LANDSCAPING AND STORM WATER POLLUTION PREVENTION PLAN

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