

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

MUN 4100 (107TH STREET), MUN 1120 (MENARD AVENUE),
AND MUN 4120 (108TH STREET & PLEASANT BOULEVARD)
PRINCESS AVENUE TO AUSTIN AVENUE,
107TH STREET TO 108TH STREET, AND
MENARD AVENUE TO CENTRAL AVENUE
RESURFACING

SECTION: 14-00053-00-RS
PROJECT: M-4003(443)
VILLAGE OF CHICAGO RIDGE
COOK COUNTY
JOB NO.: C-91-164-15

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00053-00-RS	COOK	17	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 61B31	

IDOT STANDARDS

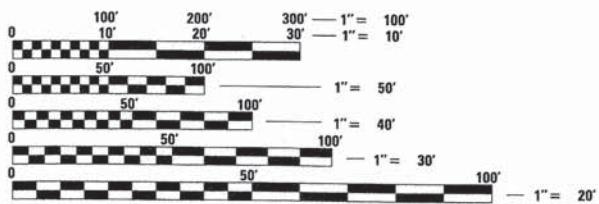
SEE SHEET 2

INDEX OF SHEETS

SEE SHEET 2

TRAFFIC DATA:

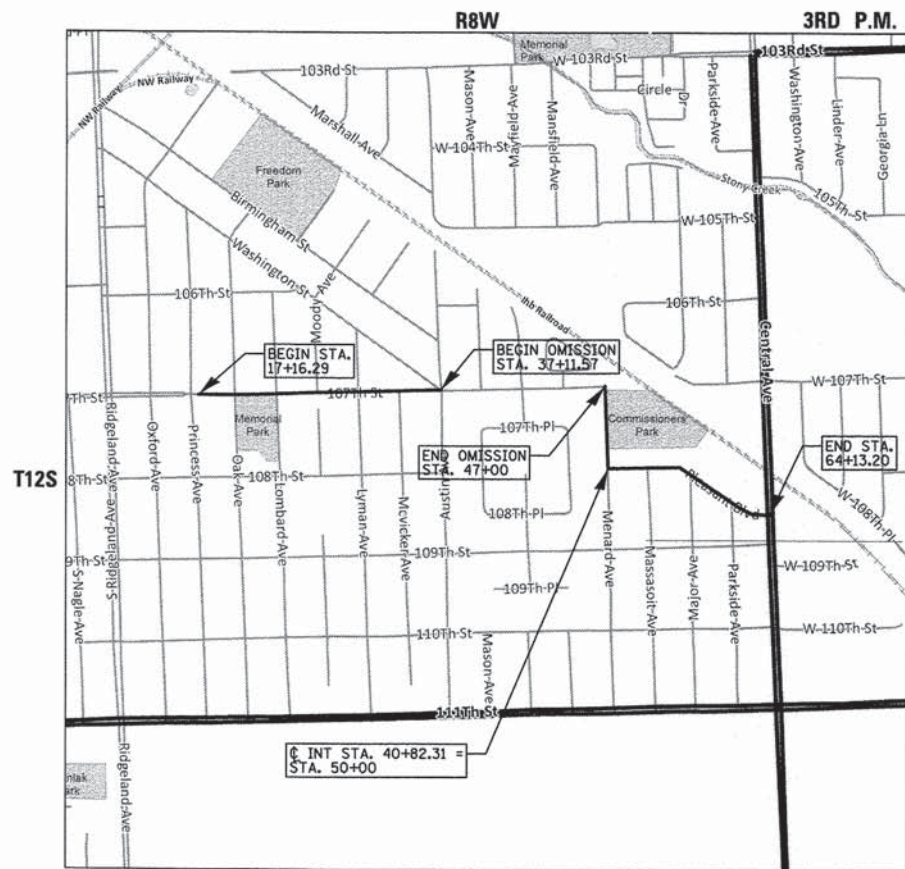
CLASSIFICATION - RESIDENTIAL COLLECTOR
ADT (2013) = 3950
POSTED SPEED = 20 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CONTRACT NO. 61B31



SECTION 17

GROSS LENGTH = 5,380 FT. = 1.03 MILE
NET LENGTH = 4,065 FT. = 0.77 MILE



February 6, 2015
Andrew M. Pufundt
ILLINOIS REGISTRATION No. 062-061729
EXPIRATION DATE: 11/30/15

CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-001175
EXPIRATION DATE: APRIL 30, 2015

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 2-2 2015
Charles E. Tolson
VILLAGE PRESIDENT, VILLAGE OF CHICAGO RIDGE

PASSED February 18, 2015
Christopher M. Holt
DISTRICT ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW February 18, 2015
John Fortmann
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

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OF THE STATE OF ILLINOIS**

FEDERAL AID PROGRAM ENGINEER: FAWAD AQUEEL, P.E., PTOE 847-705-4021, SCHAUMBURG, IL

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2015; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", (IMUTCD; THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", (SSCI), "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" MAY 1996 FIFTH EDITION, THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE LAWS AND GOVERNMENT AGENCY REGULATIONS AND RULES; AUTHORITIES HAVING JURISDICTION; OSHA REGULATIONS AND RULES; AND ANY APPLICABLE RULES AND REGULATIONS OF THE STATE OF ILLINOIS OR COOK COUNTY AGENCIES. FURTHERMORE, AND AS RELATED TO THE WORK, THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS, AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON THE SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTORS EXPENSE.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF THE VILLAGE AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED, AND SHALL BE AS INDICATED ON THE PLANS, ELEVATIONS SHOWN AT POINT OF CURVE, ETC. IS EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.

ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, ETC., ARE FROM THE PROPOSED BASE LINE OF CONSTRUCTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT. EXCEPT FOR PERIODS OF SHORT DURATION, THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AS AGGREGATE FOR TEMPORARY ACCESS.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSION AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED IN THE UNIT COST OF REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT. PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OF SURFACE COURSE, UNLESS OTHERWISE INDICATED.

BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. (ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL.)

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS". THIS WORK SHALL BE INCLUDED IN THE COST FOR PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH, SPECIAL.

ANY EXISTING PAVEMENT DAMAGED BY THE CONTRACTOR DURING THE CONSTRUCTION SHALL BE REPLACED/REPAIRED BY THE CONTRACTOR AT HIS/HERS OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER WITH NO ADDITIONAL COMPENSATION TO THE CONTRACTOR.

ALL TRANSITIONS IN CURB HEIGHT SHALL OCCUR OVER 3 FEET ALONG CURB LINE. AT LOCATIONS WHERE CURB TERMINATES, THE LAST 1 FOOT SHALL BE DEPRESSED. DEPRESSED CURB SHALL ALSO BE AT LOCATIONS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER.

GENERAL NOTES

THE OWNER, THE VILLAGE OF CHICAGO RIDGE SHALL BE NOTIFIED IN WRITING AT LEAST (3) FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

THE CONTRACTOR SHALL PROVIDE ALL NECESSARY PROTECTION FOR EXISTING UTILITIES IN CONFORMANCE WITH THE AFFECTED UTILITY COMPANIES REQUIREMENTS AS MAY BE REQUIRED TO PERFORM THE WORK OF THIS CONTRACT.

BEFORE BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY THE LINE AND GRADES SHOWN ON THE CONTRACT DRAWINGS. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONTRACT DRAWINGS, THE CONTRACTOR SHALL IMMEDIATELY REPORT SAME TO THE OWNER PRIOR TO PERFORMING WORK. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF WORK AS REQUIRED.

THE WORK PERFORMED UNDER THIS CONTRACT SHALL IN NO WAY INTERFERE WITH THE NORMAL OPERATION OF ANY EXISTING UTILITY SERVICE. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ITEMS OF EQUIPMENT REQUIRED TO MAINTAIN SUCH NORMAL OPERATION AT NO ADDITIONAL COST TO THE OWNER. THE COST ASSOCIATED FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.

SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SEED/MULCH APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

DETECTABLE WARNINGS, SIDEWALK, COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT SHALL BE AS DIRECTED BY ENGINEER.

STORM SEWER

WHENEVER, DURING CONSTRUCTION, OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST FOR CURB AND GUTTER.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AT NO COST TO THE VILLAGE.

FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES OF ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT, OR RECONSTRUCTION COST. IT IS THE CONTRACTORS RESPONSIBILITY TO DETERMINE STRUCTURE SIZE.

WHEN EXISTING DRAINAGE OR SEWERAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PUBLIC OR PRIVATE DRAINS, SEWERS, OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE ALL STORM WATER WHICH WOULD BE RECEIVED BY THESE FACILITIES AND DISCHARGE SAME. HE SHALL ALSO PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLANT, IF NECESSARY, AND A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME THAT PERMANENT CONNECTIONS WITH SEWERS ARE CONSTRUCTED AND IN SERVICE. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

DRAINAGE STRUCTURES CONSTRUCTED OVER EXISTING STORM SEWER SHALL INCLUDE THE COST TO REMOVE THE NECESSARY PORTION OF THE STORM SEWER.

ALL REMOVAL OR EXCAVATION ITEMS BEING DISPOSED OF AT AN UNCONTAMINATED SOIL FILL OPERATION OR CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) FILL SITE SHALL MEET THE REQUIREMENTS OF PUBLIC ACT 96-1416. ALL COSTS ASSOCIATED WITH MEETING THESE REQUIREMENTS SHALL BE INCLUDED IN THE UNIT PRICE COST FOR THE ASSOCIATED REMOVAL OR EXCAVATION ITEMS IN THE CONTRACT. THESE COSTS SHALL INCLUDE BUT ARE NOT LIMITED TO ALL REQUIRED TESTING, LAB ANALYSIS, CERTIFICATION BY A LICENSED PROFESSIONAL ENGINEER, AND STATE AND LOCAL TIPPING FEES.

IDOT STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREV. & PATTERNS
- 424001-08 PERPENDICULAR CURB RAMP FOR SIDEWALKS
- 442201-03 CLASS C AND D PATCHES
- 602001-02 CATCH BASIN, TYPE A
- 602301-04 INLET - TYPE A
- 602601-03 PRECAST REINFORCED CONCRETE FLAT SLAB TOP
- 604001-04 FRAME AND LIDS, TYPE 1
- 6060001-06 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701311-03 LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
- 701501-06 URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
- 701801-05 SIDEWALK, CORNER OR CROSSWALK CLOSURE
- 701901-04 TRAFFIC CONTROL DEVICES
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-04 SIGN PANEL ERECTION DETAILS
- 729001-01 APPLICATIONS OF TYPES A&B METAL POSTS (FOR SIGNS AND MARKERS)
- 780001-05 TYPICAL PAVEMENT MARKINGS

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17	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)

FILE NAME =	USER NAME = esidorczuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
N:\CHICAGO\RIDGE\90617\079\Civil\1\NOT_9000611079_01.SHT	PLOT SCALE = 20'	DRAWN -	REVISED -			14-00053-00-RS	COOK	17	2		
Default	PLOT DATE = 2/9/2015	CHECKED -	REVISED -			CONTRACT NO. 61B31					
		DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO

SUMMARY OF QUANTITIES

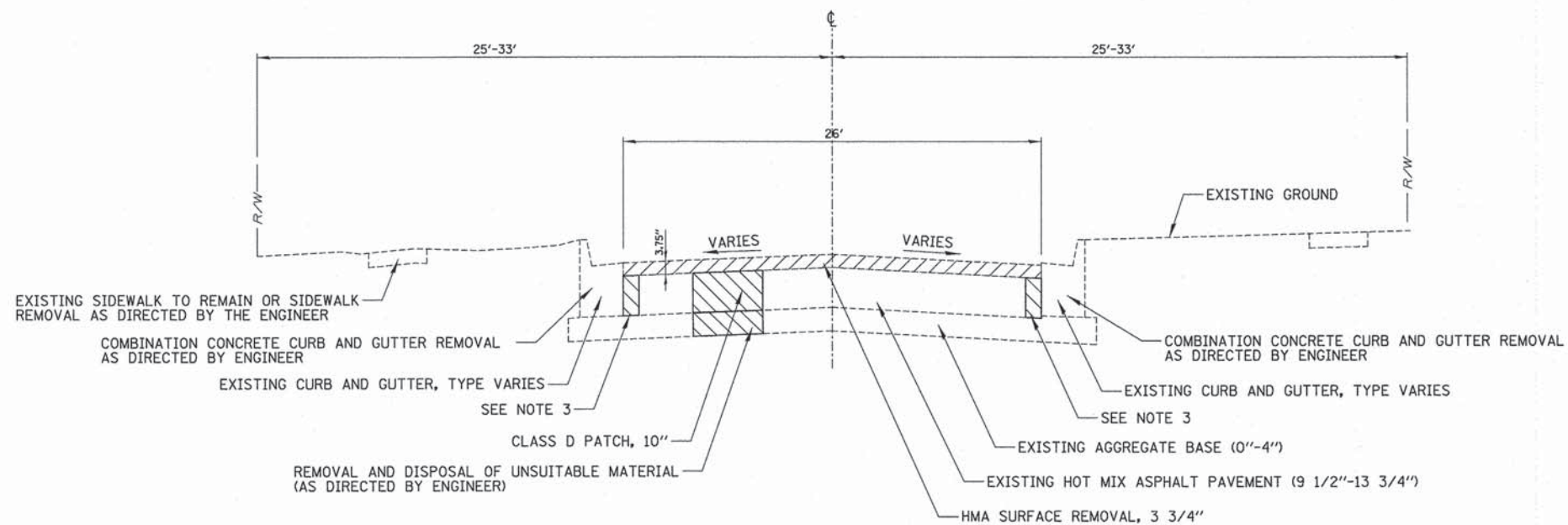
CODE NO.	ITEM	UNIT	TOTAL QUANTITY (0005) ROADWAY
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	680
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	2040
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	700
25200200	SUPPLEMENTAL WATERING	UNIT	50
25200110	SODDING, SALT TOLERANT	SQ YD	700
28000510	INLET FILTERS	EACH	45
*30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	680
35800100	PREPARATION OF BASE	SQ YD	10200
35800200	AGGREGATE BASE REPAIR	TON	535
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	300
*40600275	BITUMINOUS MATERIALS (PRIME COAT)	POUND	28350
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	350
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	2765
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1310
*42400800	DETECTABLE WARNINGS	SQ FT	380
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	4600
44000168	HOT-MIX ASPHALT SURFACE REMOVAL, 4 3/4"	SQ YD	10200
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	2000
44000600	SIDEWALK REMOVAL	SQ FT	3500
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	3
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	3
60500050	REMOVING CATCH BASINS	EACH	6

CODE NO.	ITEM	UNIT	TOTAL QUANTITY (0005) ROADWAY
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	700
60608562	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.12	FOOT	1300
67100100	MOBILIZATION	L SUM	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
72000100	SIGN PANEL - TYPE 1	SQ FT	402.75
72900200	METAL POST - TYPE B	FOOT	588
+ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1005
+ 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	452
+ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	343
+ 78300100	PAVEMENT MARKING REMOVAL	SQ FT	60
*X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	3100
*XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	300
*XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	600
*X4420484	CLASS D PATCHES, TYPE III, 6 INCH (SPECIAL)	SQ YD	200
*Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH	14
*Z0017500	DRAINAGE & UTILITY STRUCTURE ADJUSTMENT (SPECIAL)	EACH	18
*Z0017700	DRAINAGE & UTILITY STRUCTURES TO BE RECONSTRUCTED	EACH	1
*XX009013	CLASS D PATCHES, 10-INCH, SPECIAL	SQ YD	920

* INDICATES SPECIAL PROVISION
+ SPECIALTY ITEM

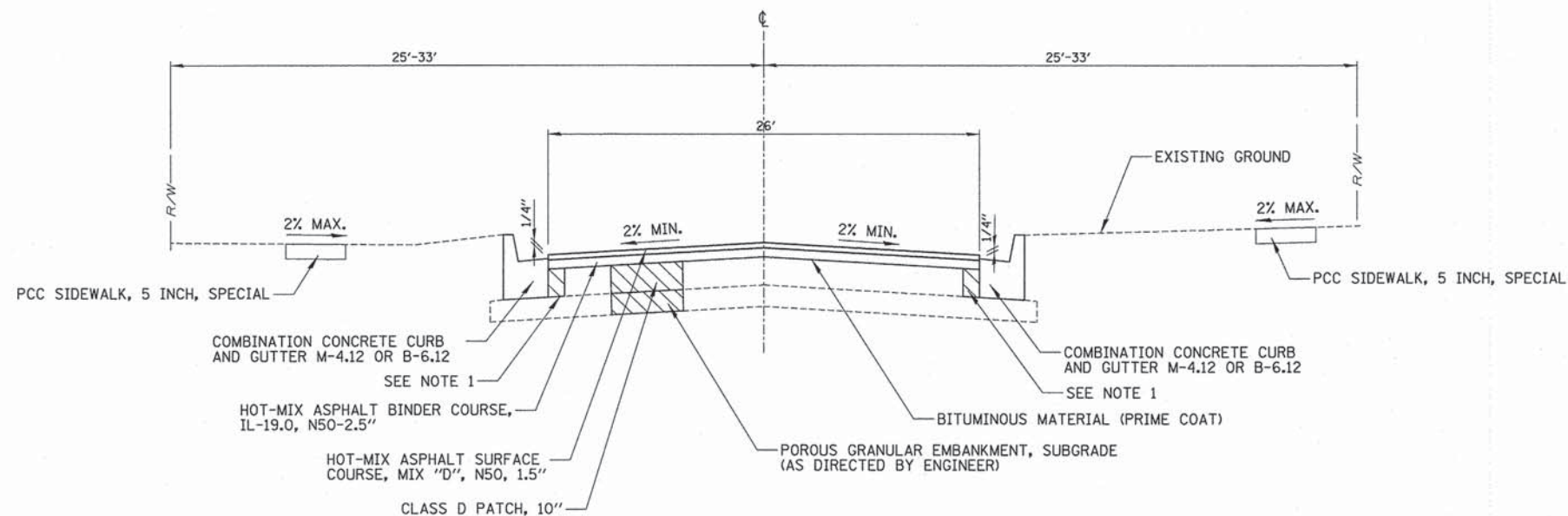
SIGNS TO BE REPLACED

SIGN	LOCATION	DIRECTION	APPROX. STATION(S)	SIZE (IN)	SIZE (IN)	QUANTITY	AREA (SF)	POST (FT)	
W11-8	FIRE TRUCK	PRINCESS - OAK	EASTBOUND	17+21	30	30	1	6.25	14
W11-8P	FIRE STATION	PRINCESS - OAK	EASTBOUND	17+21	24	18	1	3	
R7-1	NO PARKING ANY TIME	PRINCESS - OAK	EASTBOUND & WESTBOUND	17+05, 17+21, 18+30	12	18	3	4.5	
R2-1	PARK ZONE SPEED LIMIT 20 WHEN CHILDREN ARE PRESENT	PRINCESS - OAK	EASTBOUND	17+94	48	18	1	6	14
R7-1	NO PARKING ANY TIME	NEAR OAK AVE.	EASTBOUND	19+15	12	18	1	1.5	14
R1-1	STOP	AT OAK AVE.	ALL	19+72, 19+87, 20+29, 20+41	36	36	4	36	42
R1-3-4	4-WAY	AT OAK AVE.	ALL	19+72, 19+87, 20+29, 20+41	12	6	4	2	
W5-1	MEDIAN	PRINCESS - OAK	WESTBOUND	17+22	36	36	1	9	14
	PARK ZONE	OAK - LOMBARD	EASTBOUND	22+00	36	36	1	9	14
R7-1	NO PARKING ANY TIME	OAK - LOMBARD	EASTBOUND	22+00	12	18	1	1.5	
R1-1	STOP	AT LOMBARD AVE.	ALL	23+03, 23+21, 23+63, 23+76	36	36	4	36	42
R1-3-4	4-WAY	AT LOMBARD AVE.	ALL	23+03, 23+21, 23+63, 23+76	12	6	4	2	
R2-1	SPEED LIMIT 20	LOMBARD - MOODY	EASTBOUND	24+39	24	30	1	5	14
	SNOW ROUTE	LOMBARD - MOODY	EASTBOUND	24+39	18	24	1	3	
W11-8	FIRE TRUCK	LOMBARD - MOODY	WESTBOUND	25+17	30	30	1	6.25	
W11-8P	FIRE STATION	LOMBARD - MOODY	WESTBOUND	25+17	24	18	1	3	
R2-1	PARK ZONE SPEED LIMIT 20 WHEN CHILDREN ARE PRESENT	AT MOODY	WESTBOUND	26+36	48	18	1	6	14
R1-1	STOP	AT MOODY	SOUTHBOUND	26+53	36	36	1	9	14
	SNOW ROUTE	MOODY - LYMAN	WESTBOUND	28+50	18	24	1	3	
R1-1	STOP	AT LYMAN	ALL	29+73, 29+88, 30+31, 30+43	36	36	4	36	42
R1-3-4	4-WAY	AT LYMAN	ALL	29+73, 29+88, 30+31, 30+43	12	6	4	2	
	SNOW ROUTE	LYMAN - MCVICKER	WESTBOUND	31+65	18	24	1	3	
R1-1	STOP	AT MCVICKER	NORTHBOUND	33+63	36	36	1	9	14
R1-1	STOP	AT AUSTIN	ALL	36+15, 36+31, 36+58, 37+02, 37+17	36	36	5	45	70
R1-3-5	5-WAY	AT AUSTIN	ALL	36+15, 36+31, 36+58, 37+02, 37+17	12	6	5	2.5	
R12-1	WEIGHT LIMIT 5 TONS	AT AUSTIN	WESTBOUND	35+88	24	30	1	5	
R2-1	SPEED LIMIT 20	AT AUSTIN	WESTBOUND	35+88	24	30	1	5	
R7-1	NO PARKING ANY TIME	MENARD AT 107TH	SOUTHBOUND	46+94	12	18	1	1.5	14
R7-13	NO PARKING THIS SIDE OF STREET	107TH - 108TH	NORTHBOUND	45+88	12	18	1	1.5	14
R2-1	SPEED LIMIT 20	107TH - 108TH	SOUTHBOUND	45+34	24	30	1	5	
R7-1	NO PARKING ANY TIME	107TH - 108TH	SOUTHBOUND	45+34	12	18	1	1.5	
	PARK ZONE	107TH - 108TH	NORTHBOUND	44+55	36	36	1	9	14
R7-13	NO PARKING THIS SIDE OF STREET	107TH - 108TH	NORTHBOUND	43+00	12	18	1	1.5	
R2-1	PARK ZONE SPEED LIMIT 20 WHEN CHILDREN ARE PRESENT	NEAR 108TH	NORTHBOUND	41+40	48	18	1	6	14
R7-1	NO PARKING ANY TIME	NEAR 108TH	NORTHBOUND	41+40	12	18	1	1.5	
R1-1	STOP	MENARD AT 108TH	ALL	40+45, 41+17, 50+37	36	36	3	27	42
R1-3-3	3-WAY	MENARD AT 108TH	ALL	40+45, 41+17, 50+37	12	6	3	1.5	
R2-1	SPEED LIMIT 20	MENARD - MASSASOIT	EASTBOUND	51+62	24	30	1	5	
R7-13	NO PARKING THIS SIDE OF STREET	MENARD - MASSASOIT	EASTBOUND	51+62	12	18	1	1.5	
R7-1	NO PARKING ANY TIME	MENARD - MASSASOIT	WESTBOUND	51+50	12	18	1	1.5	14
R7-13	NO PARKING THIS SIDE OF STREET	AT MASSASOIT	WESTBOUND	53+21	12	18	1	1.5	14
R1-1	STOP	AT MASSASOIT	NORTHBOUND	53+54	36	36	1	9	14
R7-13	NO PARKING THIS SIDE OF STREET	MASSASOIT - MAJOR	EASTBOUND & WESTBOUND	53+79, 54+51, 55+75	12	18	3	4.5	42
W3-1	STOP AHEAD	MASSASOIT - MAJOR	EASTBOUND	55+00	30	30	1	6.25	
R1-1	STOP	AT MAJOR	ALL	56+65, 57+27, 57+30	36	36	3	27	42
R1-3-3	3-WAY	AT MAJOR	ALL	56+65, 57+27, 57+30	12	6	3	1.5	
R2-1	SPEED LIMIT 20	MAJOR - PARKSIDE	EASTBOUND	59+13	24	30	1	5	
R7-1	NO PARKING ANY TIME	MAJOR - PARKSIDE	EASTBOUND	59+13	12	18	1	1.5	
R1-1	STOP	AT PARKSIDE	NORTHBOUND	61+65	36	36	1	9	14
R10-9	NO THRU TRUCK TRAFFIC	AT PARKSIDE	SOUTHBOUND	61+09	24	30	1	5	14
R1-1	STOP	AT CENTRAL	EASTBOUND	64+11	36	36	1	9	14
							TOTALS =	402.75	588



EXISTING TYPICAL SECTION

**STA. 50+42 TO STA. 57+00, 108TH STREET
STA. 57+00 TO STA. 64+13.20, PLEASANT BOULEVARD**



PROPOSED TYPICAL SECTION

**STA. 50+42 TO STA. 57+00, 108TH STREET
STA. 57+00 TO STA. 64+13.20, PLEASANT BOULEVARD**

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS @ Ndes
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 1 1/2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50-2.5"-3.5"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19 MM) (IN 2 LIFTS)	4% @ 70 GYR
HMA DRIVEWAY	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3"	4% @ 50 GYR

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

NOTES:

1. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB AND GUTTER. (INCLUDED IN COST OF CURB REMOVAL)
2. ADDITIONAL AGGREGATE REQUIRED BETWEEN THE TOP OF THE EXISTING BASE AND BOTTOM OF PROPOSED ASPHALT SHALL BE PAID FOR AS AGGREGATE BASE REPAIR.
3. PAVEMENT REMOVAL (WIDTH VARIES AS NEEDED) ADJACENT TO PROPOSED CURB AND GUTTER INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
4. ALL WORK INCLUDING LANDSCAPE RESTORATION MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
5. FOR ROADWAYS WITH DIFFERING ELEVATIONS AT THE EDGE OF PAVEMENT, CONTRACTOR SHALL PAVE THE SIDE WITH THE HIGHER ELEVATION FIRST. WHEN GRADING THE AGGREGATE BASE AT THESE LOCATIONS, CONTRACTOR SHALL PROVIDE GRADE STAKES SHOWING FINISHED PAVEMENT GRADES AT 2% (MIN.) OFF HIGH SIDE OR THE CENTERLINE ELEVATIONS SHOWN ON THE CROSS SECTIONS (INCLUDED IN COST OF AGGREGATE BASE REPAIR).
6. CONTRACTOR SHALL MILL PRIOR TO PATCHING.
7. ENGINEER TO VERIFY MINIMUM 2% CROSS SLOPE PRIOR TO PLACING SURFACE COURSE. ANY CORRECTIONS MUST BE MADE BY THE CONTRACTOR AT HIS OWN EXPENSE.
8. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETRAMAT (FABRIC) ENCOUNTER DURING GRINDING OPERATIONS. REMOVAL OF PETRAMAT SHALL BE CONSIDERED INCLUDED IN COST OF HOT-MIX ASPHALT SURFACE REMOVAL.
9. GENERALLY, CONCRETE CURB AND GUTTER, TYPE IS FOUND AS FOLLOWS:
B-6.12: STA. 17+16.29 TO STA. 40+43.33 AND STA. 50+00.00 TO STA. 57+00.00
M-3.12: STA. 57+00.00 TO STA. 64+13.20

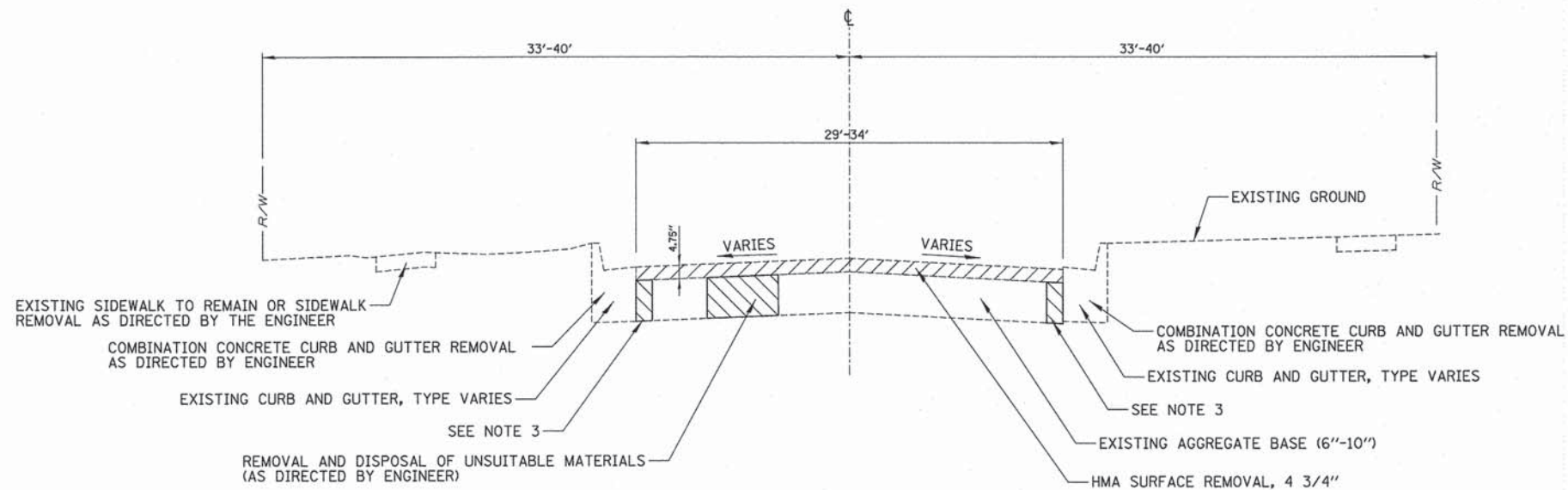
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Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 2/9/2015	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

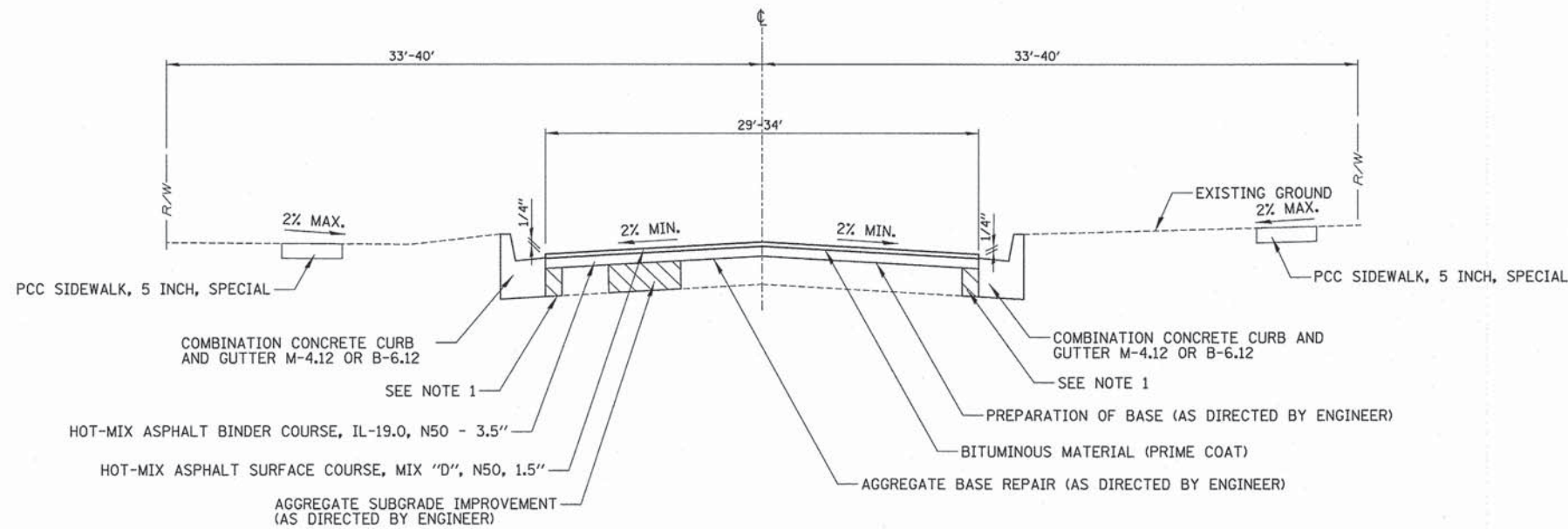
TYPICAL SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00053-00-RS	COOK	17	5
CONTRACT NO. 61B31			
ILLINOIS FED. AID PROJECT			



EXISTING TYPICAL SECTION
STA. 17+16.29 TO STA. 37+11.57, 107TH STREET
STA. 40+43.33 TO STA. 47+00, MENARD AVENUE

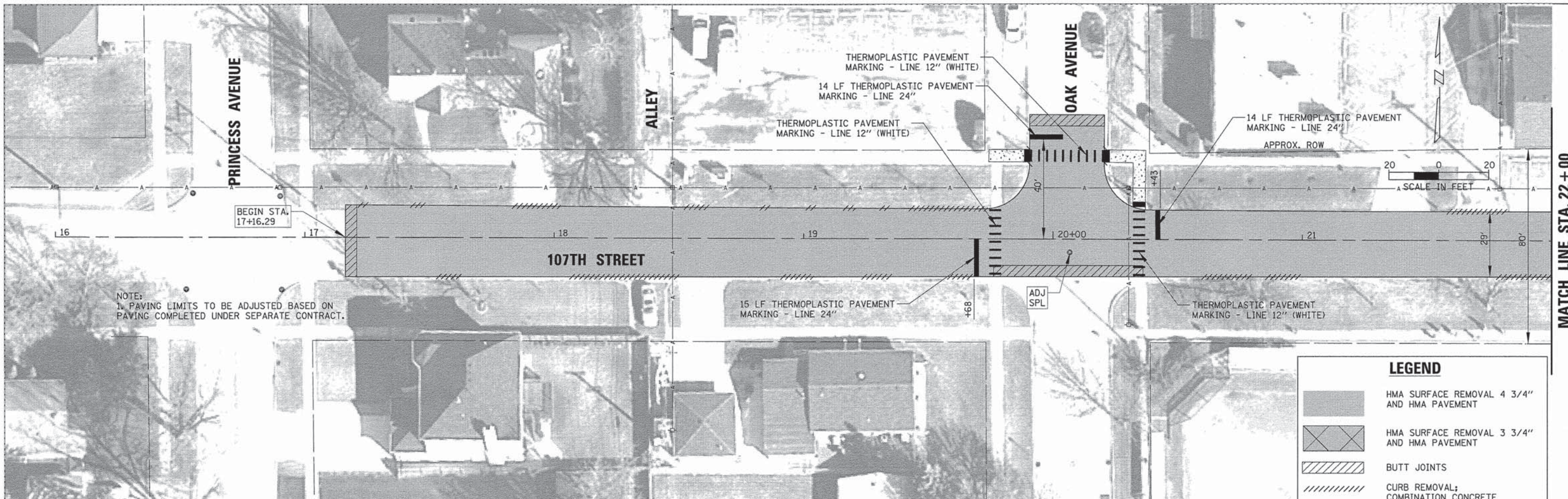


PROPOSED TYPICAL SECTION
STA. 17+16.29 TO STA. 37+11.57, 107TH AVENUE
STA. 40+43.33 TO STA. 47+00, MENARD AVENUE

NOTES:

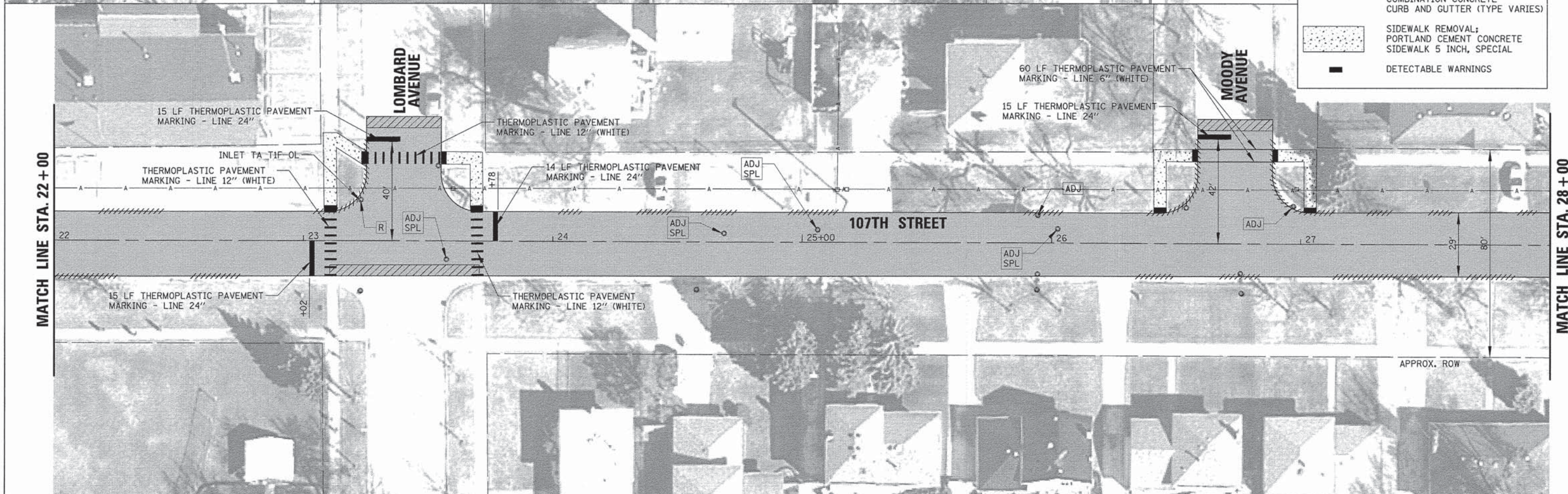
1. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB AND GUTTER. (INCLUDED IN COST OF CURB REMOVAL)
2. ADDITIONAL AGGREGATE REQUIRED BETWEEN THE TOP OF THE EXISTING BASE AND BOTTOM OF PROPOSED ASPHALT SHALL BE PAID FOR AS AGGREGATE BASE REPAIR.
3. PAVEMENT REMOVAL (WIDTH VARIES AS NEEDED) ADJACENT TO PROPOSED CURB AND GUTTER INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
4. ALL WORK INCLUDING LANDSCAPE RESTORATION MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
5. FOR ROADWAYS WITH DIFFERING ELEVATIONS AT THE EDGE OF PAVEMENT, CONTRACTOR SHALL PAVE THE SIDE WITH THE HIGHER ELEVATION FIRST. WHEN GRADING THE AGGREGATE BASE AT THESE LOCATIONS, CONTRACTOR SHALL PROVIDE GRADE STAKES SHOWING FINISHED PAVEMENT GRADES AT 2% (MIN.) OFF HIGH SIDE OR THE CENTERLINE ELEVATIONS SHOWN ON THE CROSS SECTIONS (INCLUDED IN COST OF AGGREGATE BASE REPAIR).
6. CONTRACTOR SHALL MILL PRIOR TO PATCHING.
7. ENGINEER TO VERIFY MINIMUM 2% CROSS SLOPE PRIOR TO PLACING SURFACE COURSE. ANY CORRECTIONS MUST BE MADE BY THE CONTRACTOR AT HIS OWN EXPENSE.
8. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETRAMAT (FABRIC) ENCOUNTERED DURING GRINDING OPERATIONS. REMOVAL OF PETRAMAT SHALL BE CONSIDERED INCLUDED IN COST OF HOT-MIX ASPHALT SURFACE REMOVAL.
9. GENERALLY, CONCRETE CURB AND GUTTER, TYPE IS FOUND AS FOLLOWS:
 B-6.12: STA. 17+16.29 TO STA. 40+43.33 AND STA. 50+00.00 TO STA. 57+00.00
 M-3.12: STA. 57+00.00 TO STA. 64+13.20

FILE NAME =	USER NAME = esidorozuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTIONS		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\CHICAGO\RDG\9061T\079\Civ11\TYP_9000	11079_03.SHT	DRAWN -	REVISED -				14-00053-00-RS	COOK	17	6	
Default	PLOT SCALE = 20'	CHECKED -	REVISED -		CONTRACT NO. 61B31						
	PLOT DATE = 2/9/2015	DATE -	REVISED -		ILLINOIS FED. AID PROJECT						
					SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

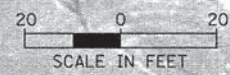
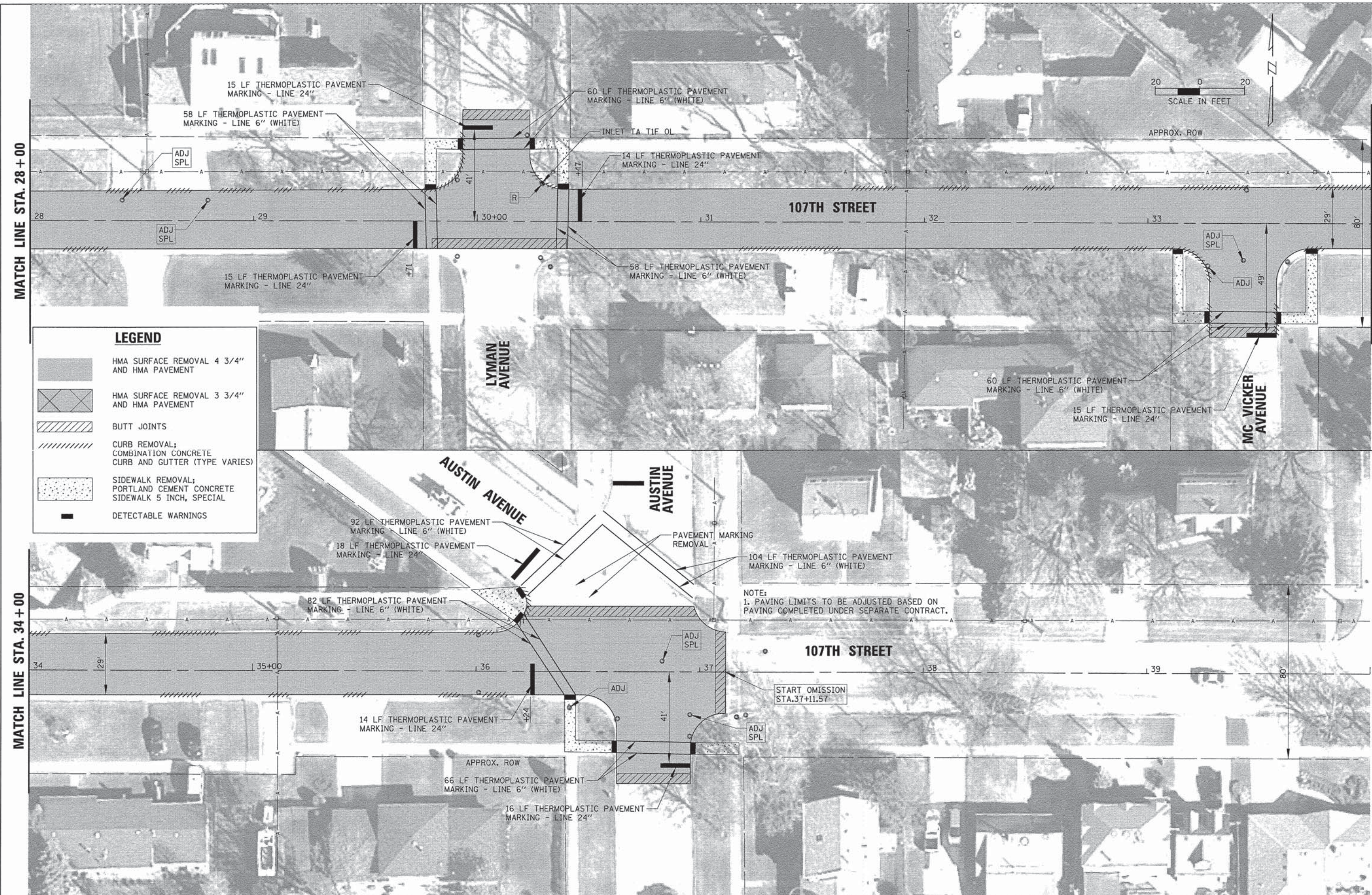


NOTE:
1. PAVING LIMITS TO BE ADJUSTED BASED ON PAVING COMPLETED UNDER SEPARATE CONTRACT.

LEGEND	
	HMA SURFACE REMOVAL 4 3/4" AND HMA PAVEMENT
	HMA SURFACE REMOVAL 3 3/4" AND HMA PAVEMENT
	BUTT JOINTS
	CURB REMOVAL; COMBINATION CONCRETE CURB AND GUTTER (TYPE VARIES)
	SIDEWALK REMOVAL; PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL
	DETECTABLE WARNINGS



FILE NAME =	USER NAME = esidorczuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN			SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\CHICAGO\DRIDGE\9061T\079\Civil\PLN_9000617079_01.SHT	PLOT SCALE = 28'	DRAWN -	REVISED -					14-00053-00-RS	COOK	17	7
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		DATE -	REVISED -		ILLINOIS FED. AID PROJECT						

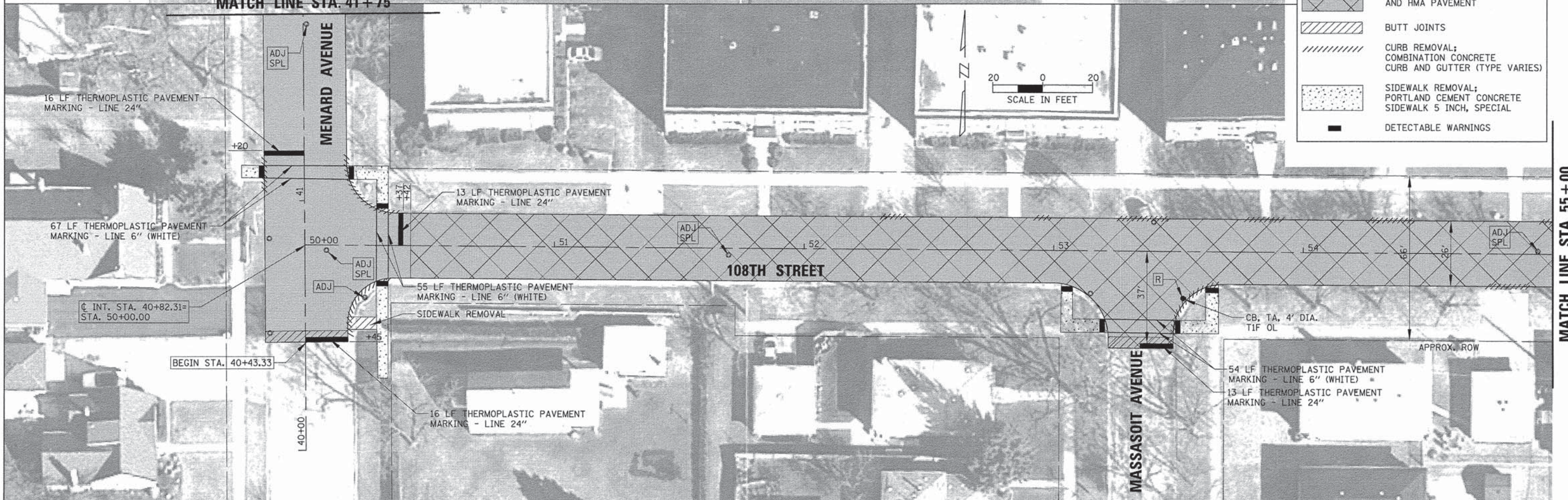
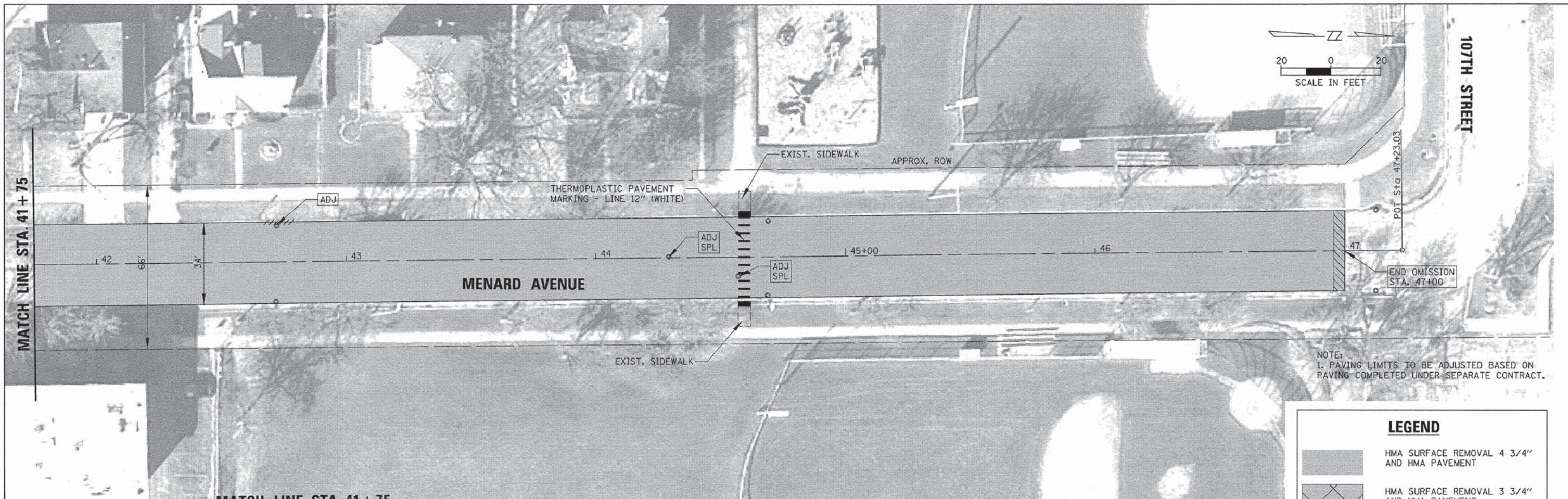


MATCH LINE STA. 28+00

MATCH LINE STA. 34+00

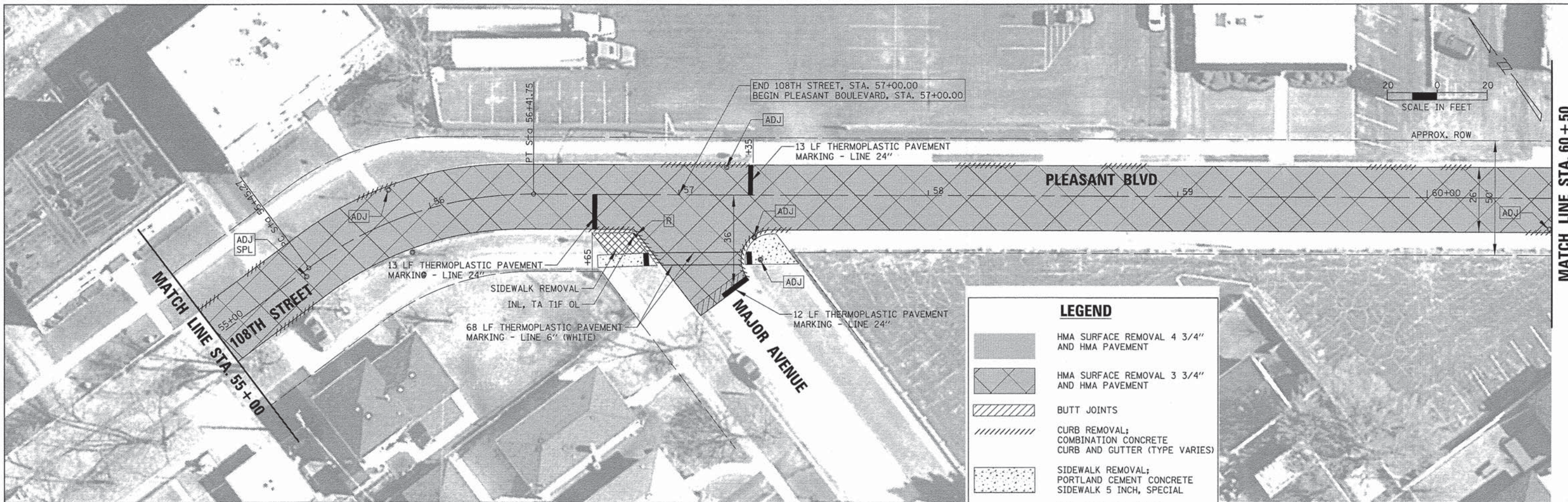
MATCH LINE STA. 34+00

FILE NAME =	USER NAME = esidorczuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\CHICAGO\RDG\9061T\079\Civ11\PLN\9000117079_02.SHT	PLOT SCALE = 20'	DRAWN -	REVISED -			14-00053-00-RS	COOK	17	8
Default	PLOT DATE = 2/9/2015	CHECKED -	REVISED -			CONTRACT NO. 61B31			
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT			



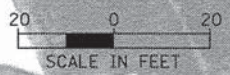
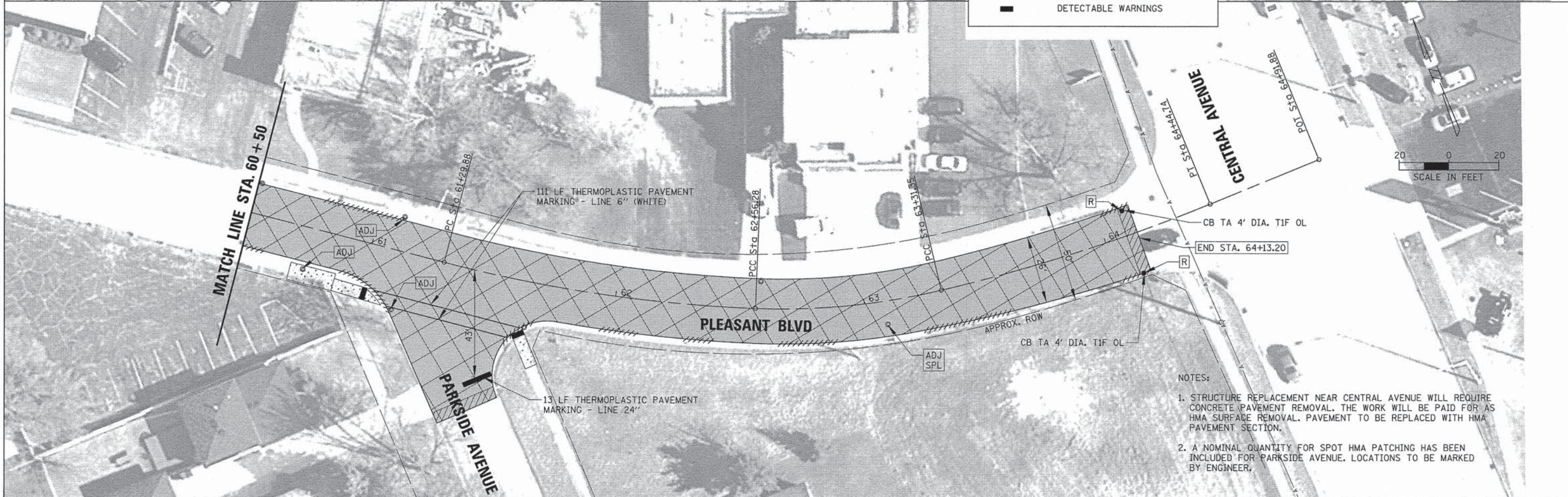
LEGEND	
	HMA SURFACE REMOVAL 4 3/4" AND HMA PAVEMENT
	HMA SURFACE REMOVAL 3 3/4" AND HMA PAVEMENT
	BUTT JOINTS
	CURB REMOVAL; COMBINATION CONCRETE CURB AND GUTTER (TYPE VARIES)
	SIDEWALK REMOVAL; PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL
	DETECTABLE WARNINGS

FILE NAME =	USER NAME = esidorezuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN			SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\CHICAGO\RDGE\9061T\079\Civ11\PLN_900011079_03.SHT	PLOT SCALE = 20'	DRAWN -	REVISED -					14-00053-00-RS	COOK	17	9
Default	PLOT DATE = 2/9/2015	CHECKED -	REVISED -		CONTRACT NO. 61B31						
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT						



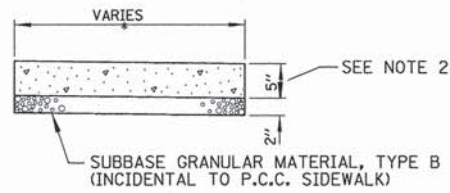
MATCH LINE STA. 60 + 50

LEGEND	
	HMA SURFACE REMOVAL 4 3/4" AND HMA PAVEMENT
	HMA SURFACE REMOVAL 3 3/4" AND HMA PAVEMENT
	BUTT JOINTS
	CURB REMOVAL; COMBINATION CONCRETE CURB AND GUTTER (TYPE VARIES)
	SIDEWALK REMOVAL; PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL
	DETECTABLE WARNINGS



- NOTES:
1. STRUCTURE REPLACEMENT NEAR CENTRAL AVENUE WILL REQUIRE CONCRETE PAVEMENT REMOVAL. THE WORK WILL BE PAID FOR AS HMA SURFACE REMOVAL. PAVEMENT TO BE REPLACED WITH HMA PAVEMENT SECTION.
 2. A NOMINAL QUANTITY FOR SPOT HMA PATCHING HAS BEEN INCLUDED FOR PARKSIDE AVENUE. LOCATIONS TO BE MARKED BY ENGINEER.

FILE NAME =	USER NAME = esdorczyk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED PLAN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\CHICAGO\RDGE\9061T\079\Civil\PLN_900061T079_04.SHT	PLOT SCALE = 20'	DRAWN -	REVISED -			14-00053-00-RS	COOK	17	10
Defaults	PLOT DATE = 2/9/2015	CHECKED -	REVISED -			CONTRACT NO. 61B31			
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT			

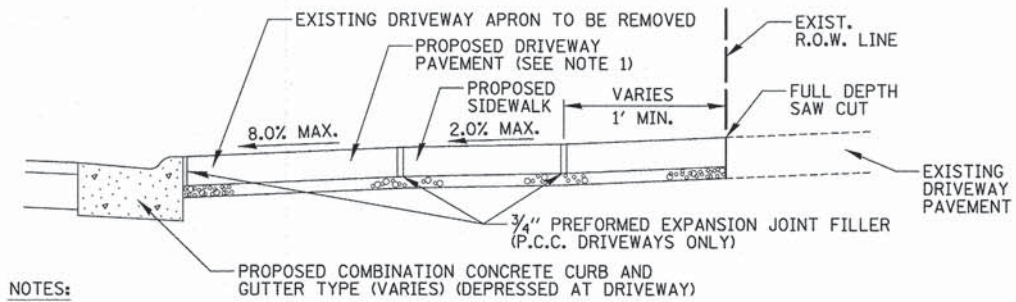


• CROSS SLOPE 0.5% (MIN.) TO 2% (MAX.)

NOTES:

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 5 INCH, SPECIAL.
2. THICKNESS SHALL BE INCREASED TO 7" WHERE SIDEWALK IS ADJACENT TO A DRIVEWAY (NO WIRE MESH). (COST INCIDENTAL).
3. IN LOCATIONS WHERE SIDEWALK IS REMOVED AND REPLACED THROUGH DRIVEWAYS, DRIVEWAYS SHALL BE SAWCUT AND PATCHED A MINIMUM OF 1' ON EITHER SIDE OF THE WALK. THIS WORK SHALL BE PAID FOR PER SQUARE YARD AT THE CONTRACT UNIT PRICE FOR CONCRETE DRIVEWAY REPLACEMENT OR HOT-MIX ASPHALT DRIVEWAY REPLACEMENT.
4. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
5. ALL LANDSCAPE RESTORATION (TOPSOIL, SODDING AND SUPPLEMENTAL WATERING) SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.

**P.C.C. SIDEWALK, 5 INCH SPECIAL
DETAIL**

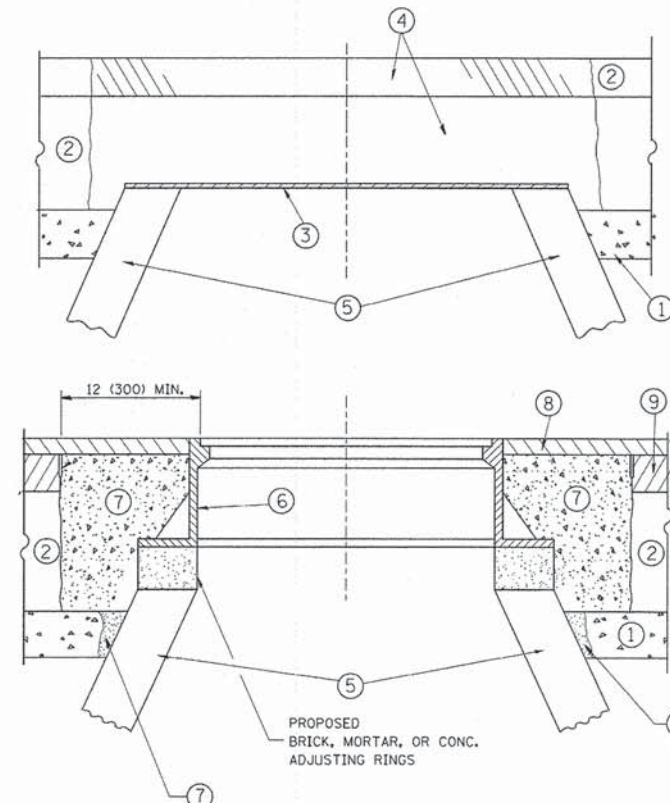


NOTES:

1. THE PROPOSED DRIVEWAY PAVEMENT SHALL CONSIST OF:
 - A. 7" PORTLAND CEMENT CONCRETE AND 3" OF AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS PORTLAND CEMENT CONCRETE, OR
 - B. 3" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, AND 6" AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS HMA OR AGGREGATE.
2. PREFORMED EXPANSION JOINT FILLER SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT.
3. ALL REQUIRED EARTH EXCAVATION AND DRIVEWAY REMOVAL REQUIRED TO CONSTRUCT DRIVES SHALL BE INCIDENTAL TO DRIVEWAY REMOVAL & REPLACEMENT, OF THE TYPE SPECIFIED.

DRIVEWAY REMOVAL AND REPLACEMENT DETAIL

FILE NAME =	USER NAME = esidorozuk	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CONSTRUCTION DETAILS				SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\CHICAGO\RDGE\9061T\079\Civil\DET_900001\1079-01.SHT	PLOT SCALE = 20'	DRAWN -	REVISED -						14-00053-00-RS	COOK	17	11
Default	PLOT DATE = 2/9/2015	CHECKED -	REVISED -		CONTRACT NO. 61B31							
		DATE -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

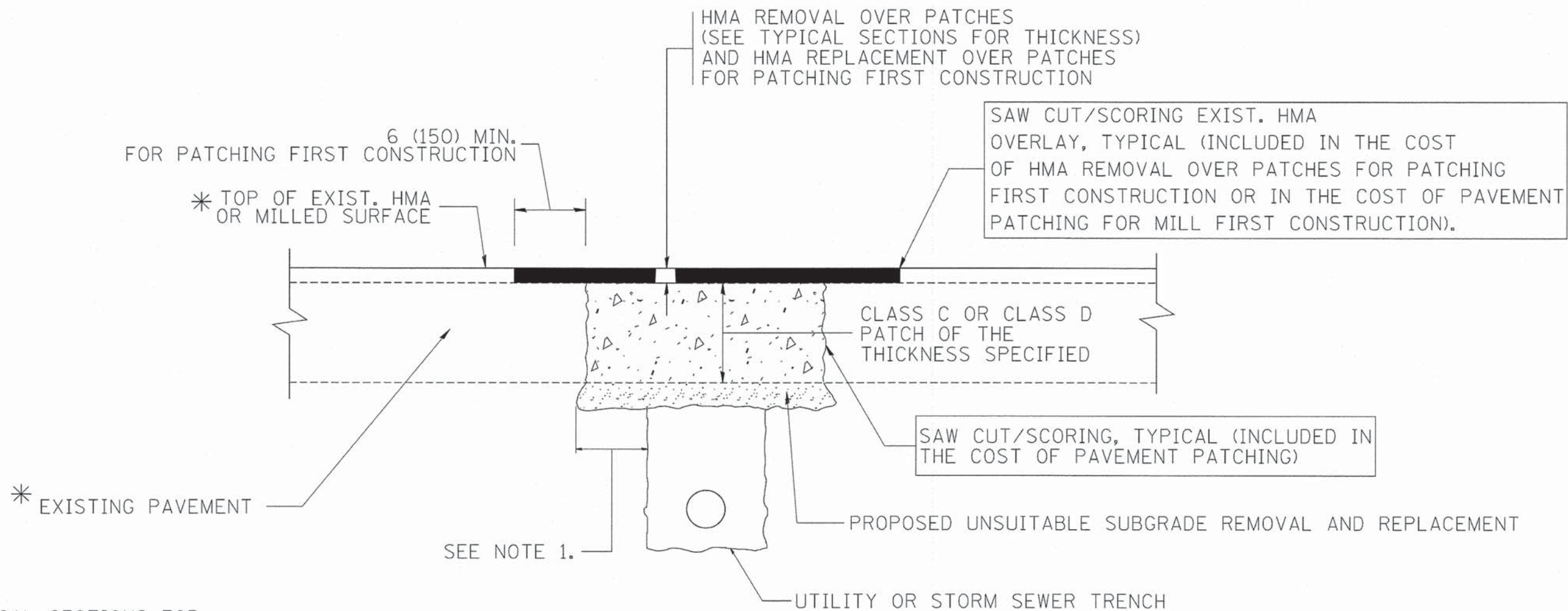
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = beuerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
al\pw\work\psidot\beuerdl\d0108315\bd08.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLDT SCALE = 1/8" = 1'-0"	CHECKED -	REVISED - R. BORO 03-09-11
	PLDT DATE = 12/6/2011	DATE = 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00053-00-RS	COOK	17	12
BD600-03 (BD-8)		CONTRACT NO. 61831	
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>			



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

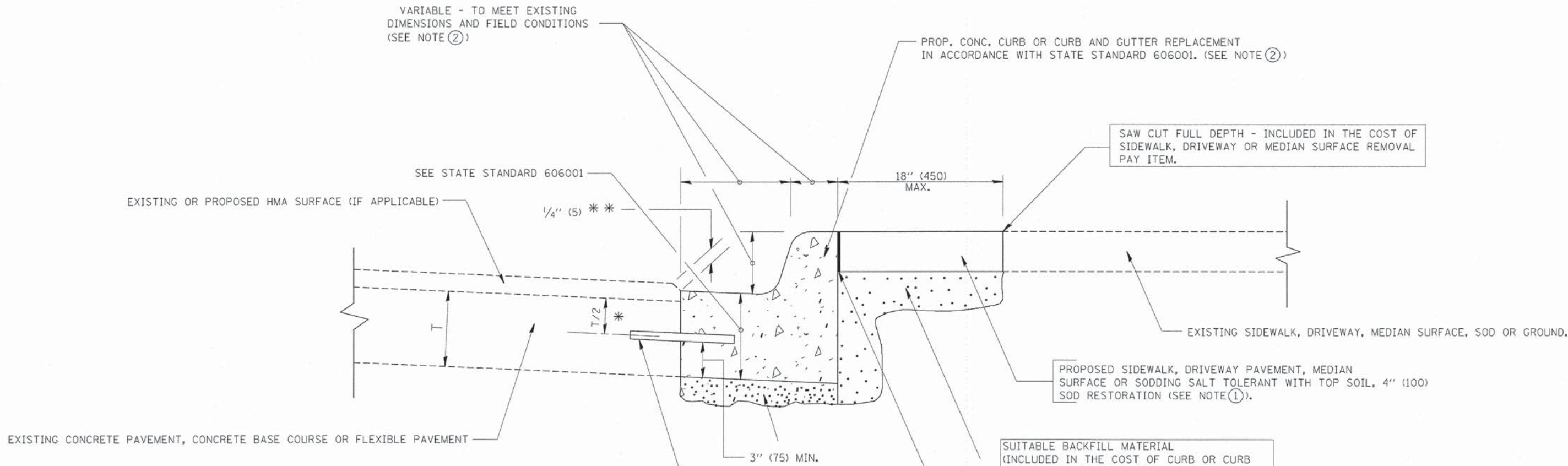
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\dsets\td22x34\bd22.dgn	USER NAME = bowerd	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	14-00053-00-RS	COOK	17 13
		CHECKED -	REVISED - R. BORO 09-04-07						BD400-04 (BD-22)	CONTRACT NO. 61831	
		DATE - 10-25-94	REVISED - K. ENG 10-27-08						FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

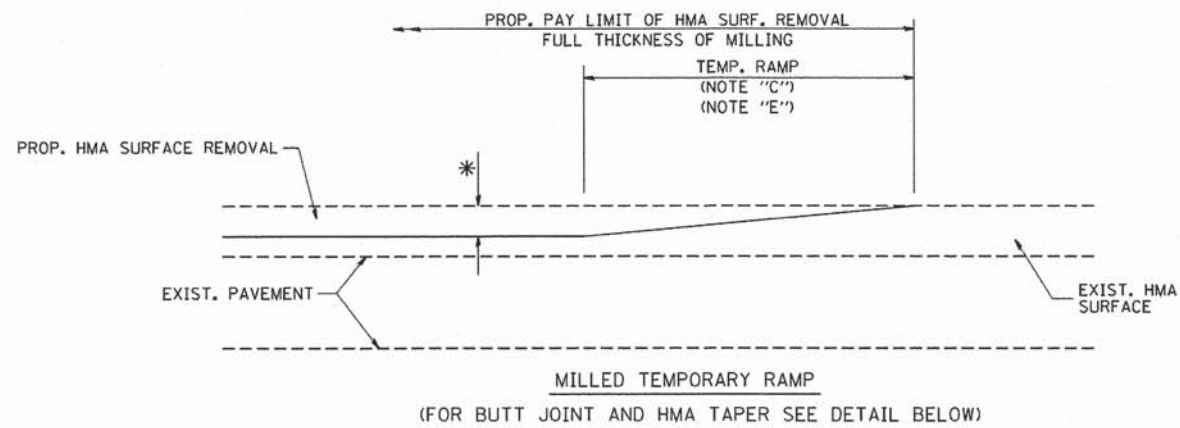
- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

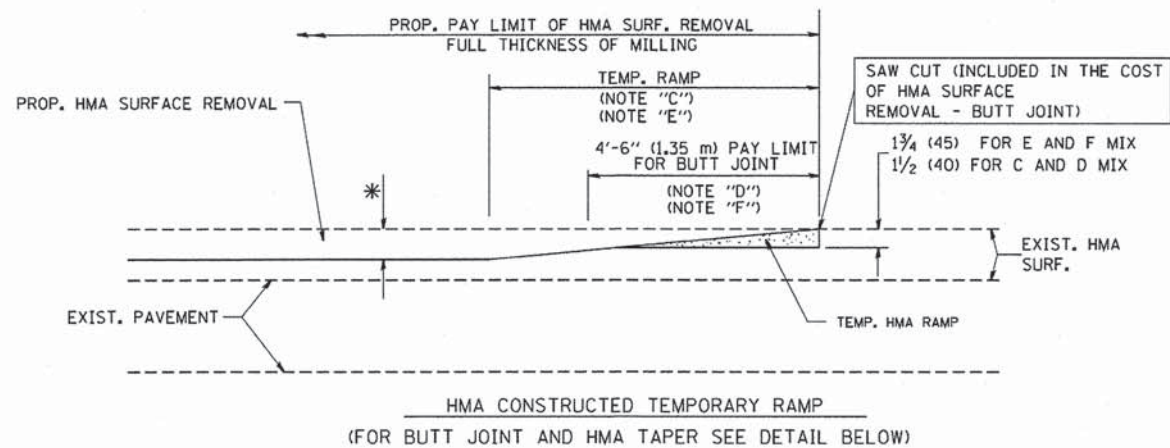
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

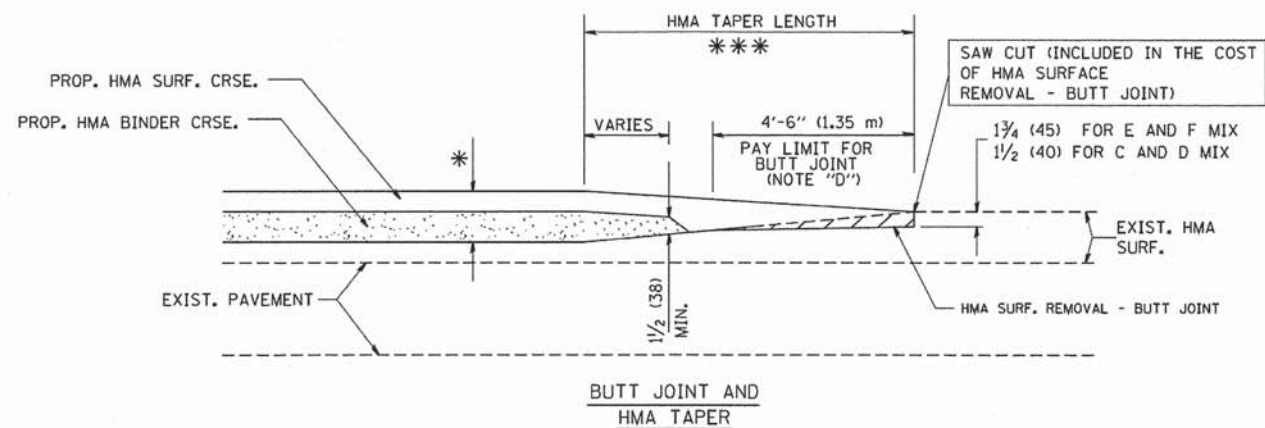
FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAN 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
et\pv_wor\p\dot\drivakosgn\d8108315\bc24.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97	14-00053-00-RS			COOK	17	14	
PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 01-22-01	BD600-06 (BD-24)			CONTRACT NO. 61831			
PLOT DATE = 12/15/2009	DATE - 03-11-94	REVISED - R. BORO 12-15-09	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						
			SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	



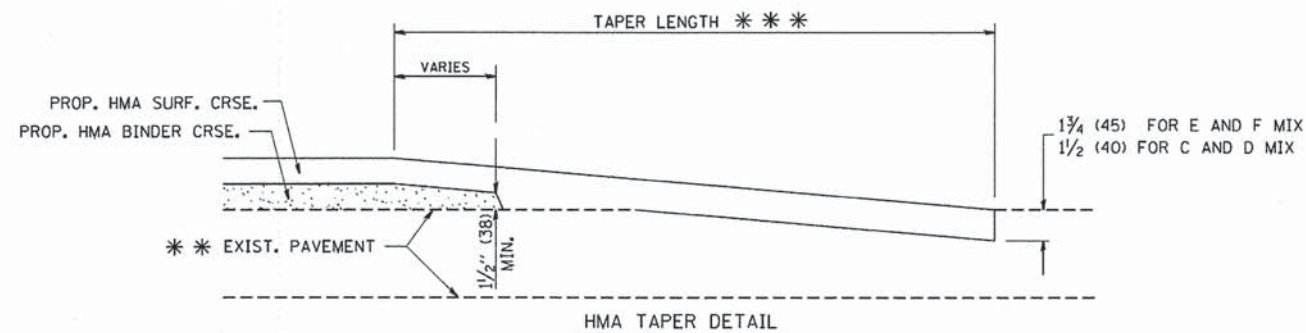
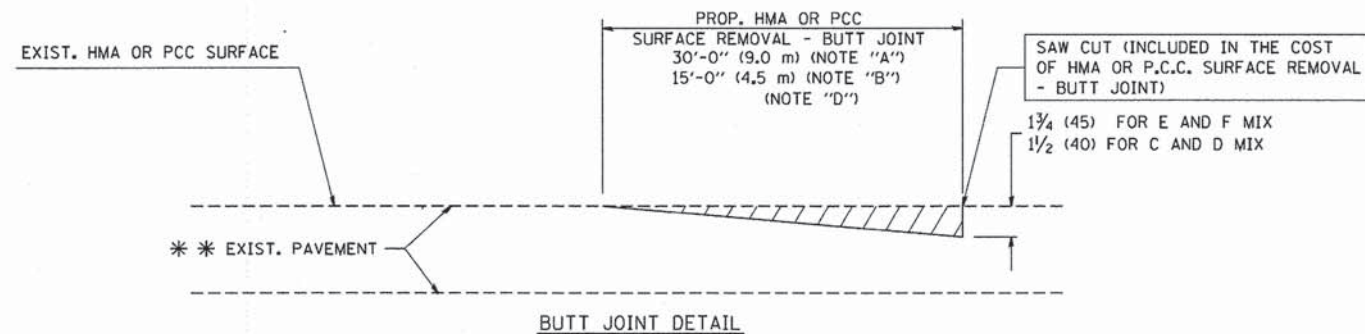
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

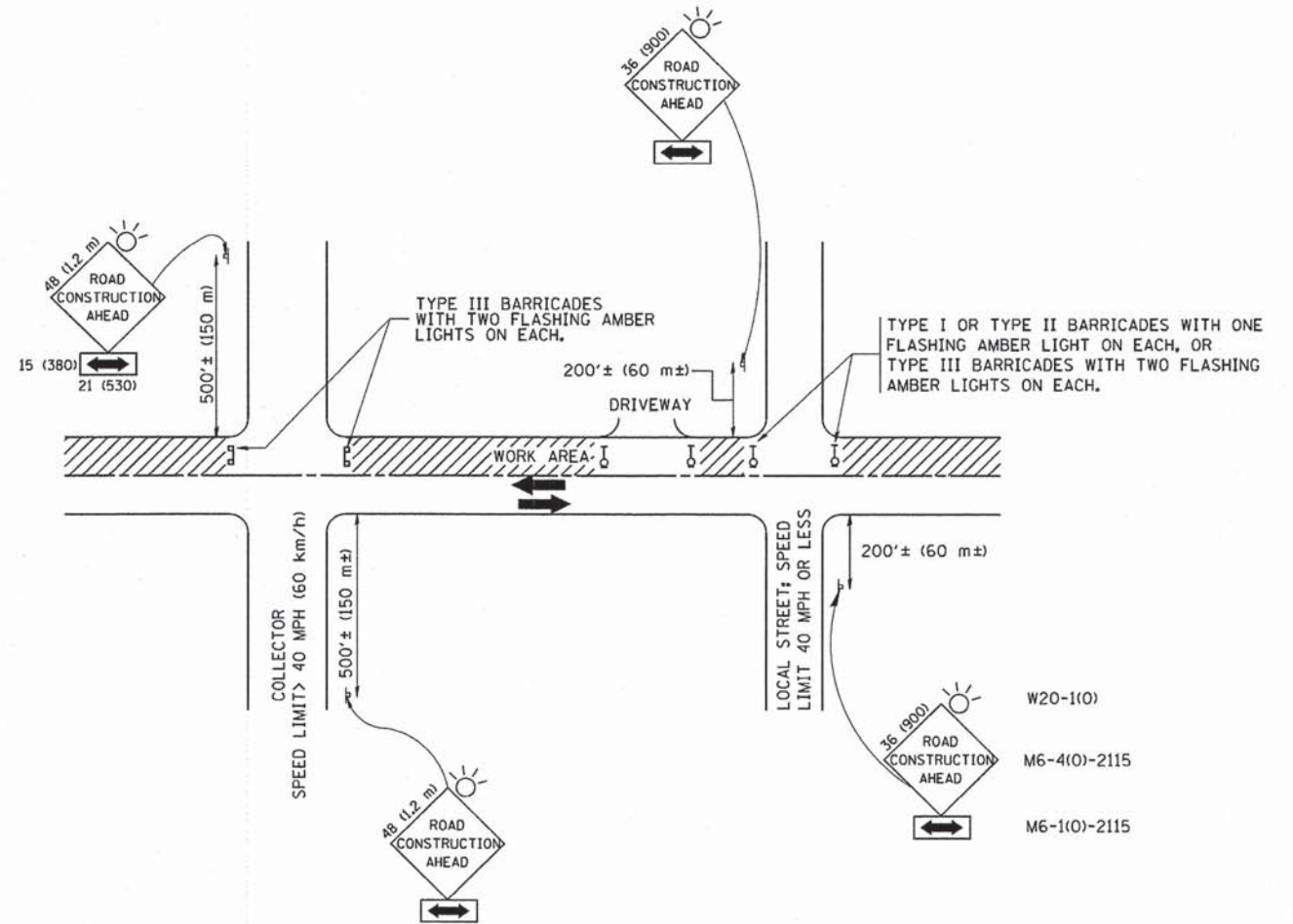
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		14-00053-00-RS	COOK	17	15
		BD400-05 BD32	CONTRACT NO. 61831		
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 70150L, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

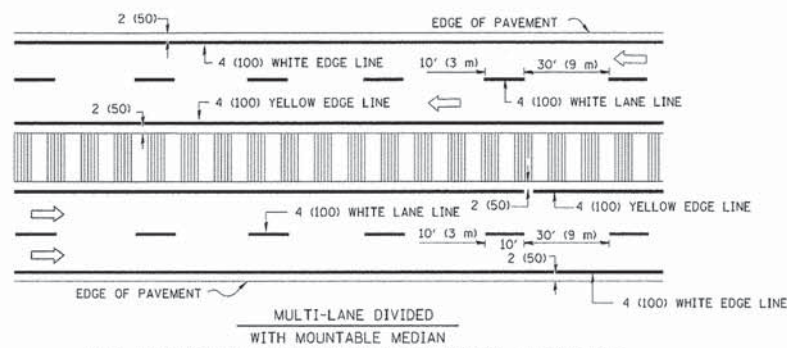
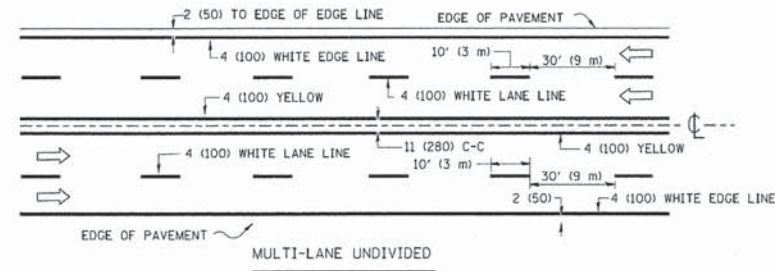
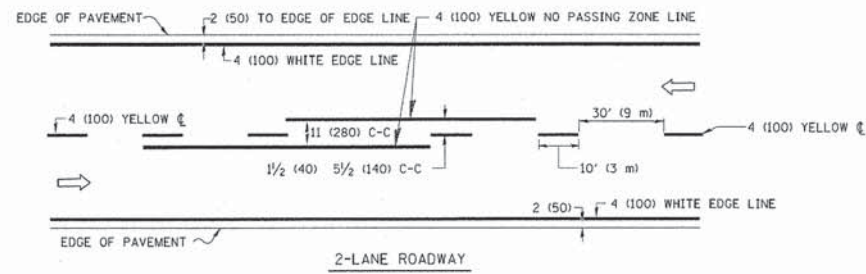
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	PLOT SCALE = 50,000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2000	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

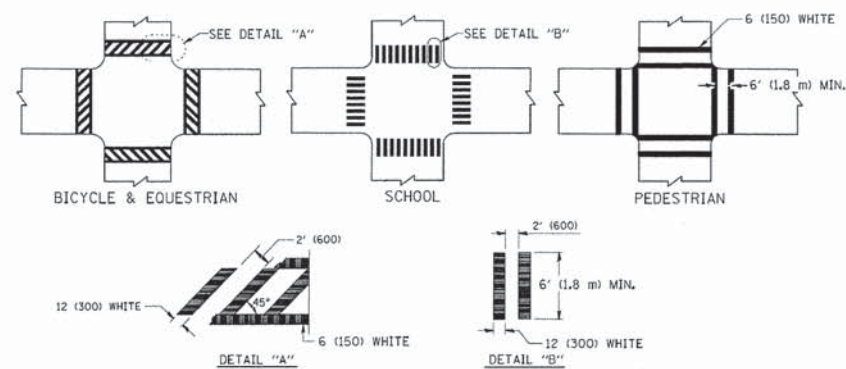
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SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10		CONTRACT NO. 61B31	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

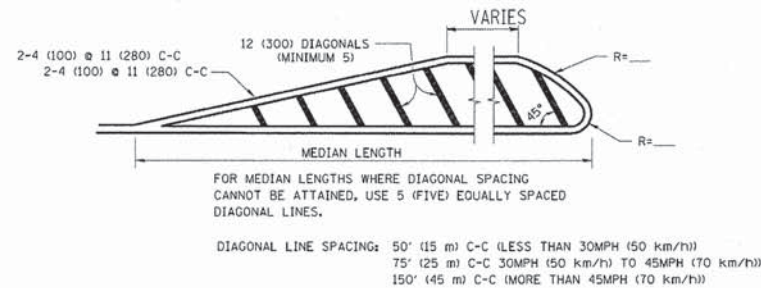
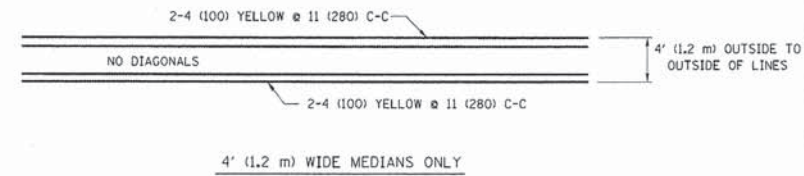


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

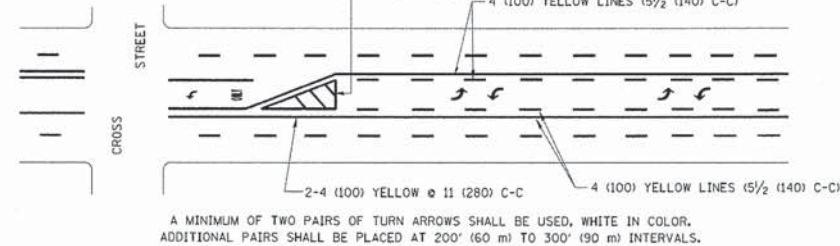
TYPICAL LANE AND EDGE LINE MARKING



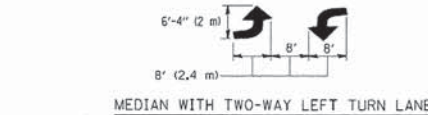
TYPICAL CROSSWALK MARKING



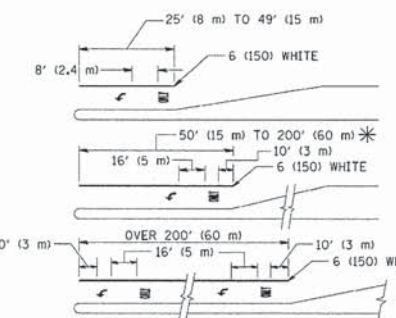
MEDIANS OVER 4' (1.2 m) WIDE



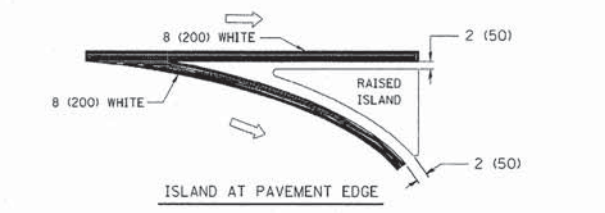
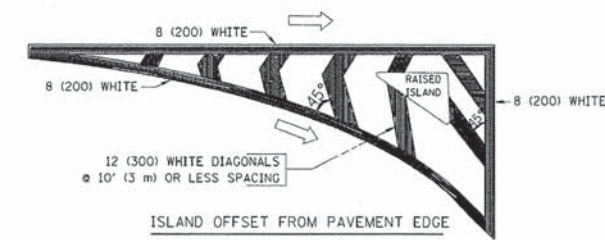
TYPICAL PAINTED MEDIAN MARKING



TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2' (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2' (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
al:\pwork\psidot\drvakosgn\0810315\td3.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50,000 * / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
14-00053-00-RS	COOK	17	17
TC-13		CONTRACT NO. 61B31	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT			