

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-RS	COOK/KANE	30	1
ILLINOIS			CONTRACT NO. 62J96	

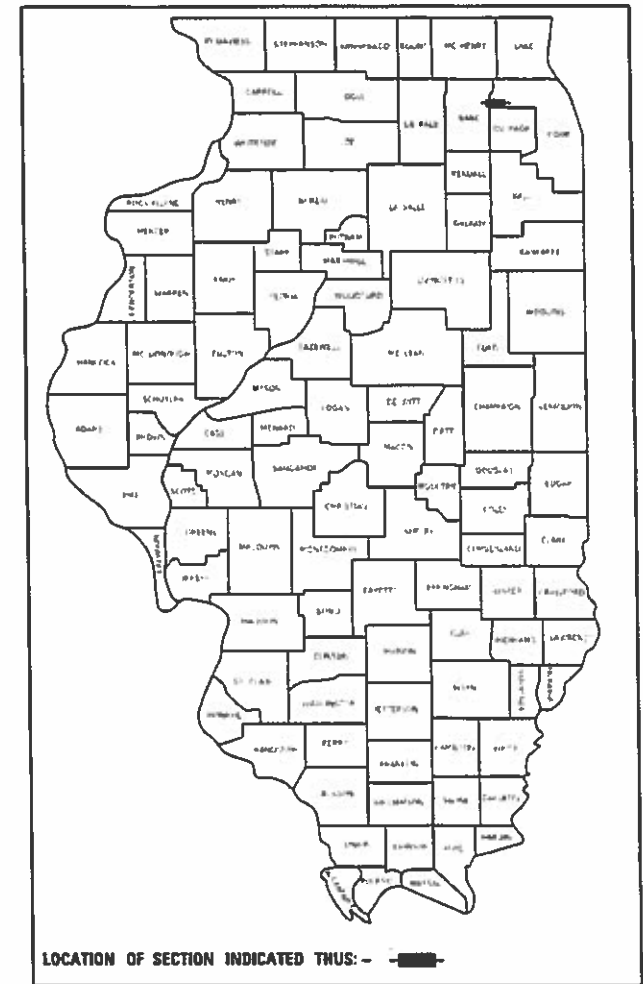
D-91-250-20

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF ELGIN

**PROPOSED
HIGHWAY PLANS**
ROUTE: F.A.P. 345 (US ROUTE 20)
PLANK RD. TO SHALES PKWY.
SECTION: 2019-090-RS
PROJECT: NHPP-JSOA(992)
CENTERLINE JOINT REPAIR AND PATCHING
COOK AND KANE COUNTIES

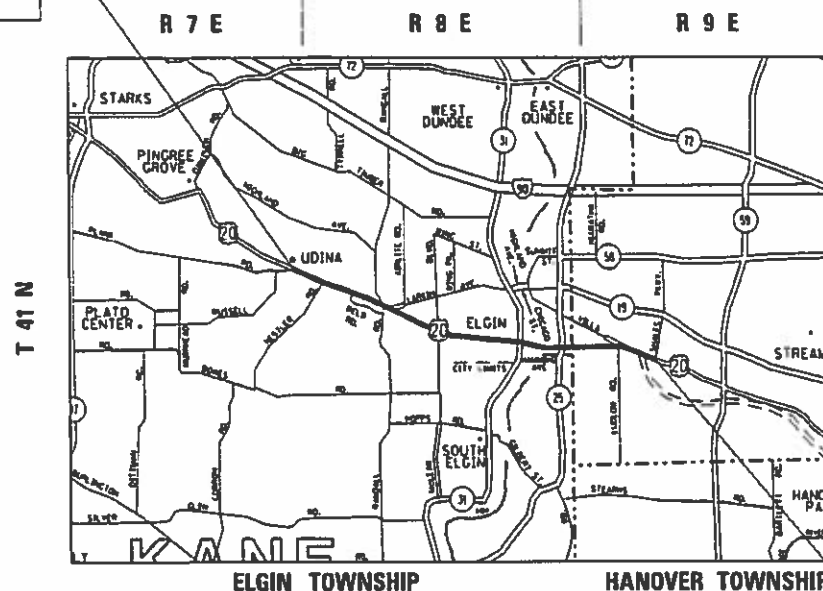
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TRAFFIC DATA:

2019 AADT=51,100
SPEED LIMIT
= 55 MPH

PROJECT STARTS
STA 20+75

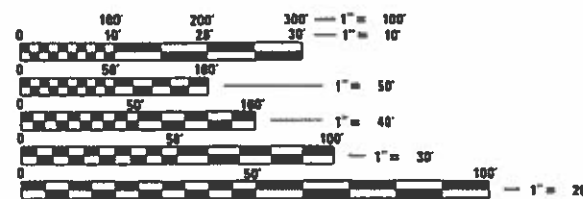


LOCATION MAP (NOT TO SCALE)

OMISSIONS:

- STA. 175+68 TO STA. 245+93
- STA. 302+57 TO STA. 309+16
- STA. 312+48 TO STA. 313+85
- STA. 322+90 TO STA. 324+48
- STA. 326+27 TO STA. 327+72
- STA. 345+88 TO STA. 347+05
- STA. 383+11 TO STA. 385+33

PROJECT ENDS
STA 411+77



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847) 705-4240
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

GROSS LENGTH = 39,102 FT. = 7.41 MILES
NET LENGTH = 30,639 FT. = 5.80 MILES

CONTRACT NO. 62J96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 29, 2020
Anthony J. Dwyer REGIONAL ENGINEER
March 20, 2020
[Signature] ENGINEER OF DESIGN AND ENVIRONMENT
March 20, 2020
[Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
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2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES
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23	BD-08: DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
24	BD-22: PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
25	TC-10: TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
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28	TC-14: TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
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30	TC-22: ARTERIAL ROAD INFORMATION SIGN

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS, DAY ONLY, FOR SPEEDS \geq 45 MPH
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701336-07	LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES, FOR SPEEDS \geq 45 MPH
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS \geq 45 MPH TO 55 MPH
701422-10	LANE CLOSURE, MULTILANE, FOR SPEEDS \geq 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS \geq 45 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, CITY OF ELGIN.
3. THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
5. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
6. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING.
7. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
8. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
9. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
10. CATCH BASINS, MANHOLES, INLETS, DRAINAGE STRUCTURES AND VALVE VAULTS ADJUSTMENT AND/OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN.
11. THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, VIA E-MAIL AT DON.CHIARUGI@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
12. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
13. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
14. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
15. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
17. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER / TECHNICIAN, AND AS SHOWN IN THE PLANS.
18. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
19. GAPS BETWEEN SUCCESSIVE LANE CLOSURES SHALL NOT BE LESS THAN 2 MILES (3 KM) IN LENGTH ACCORDING TO ARTICLE 701.05 MAXIMUM LENGTH OF LANE CLOSURE
20. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
21. THE CONTRACTOR SHALL USE CARE IN CONSTRUCTION OPERATIONS NEAR ANY EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**U.S. 20 LAKE ST. (PLANK RD. TO SHALES PKWY.)
INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	2
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 COOK CTY	80% FED 20% STATE 0005 KANE CTY			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	13557	4128	9429			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	46	14	32			
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1526	382	1144			
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SO YD	13619	3405	10214			
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SO YD	3320	830	2490			
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SO YD	1788	447	1341			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	1.5	4.5			
67100100	MOBILIZATION	L SUM	1	0.25	0.75			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	0.25	0.75			
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1	0.25	0.75			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.25	0.75			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.25	0.75			

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005 COOK CTY	80% FED 20% STATE 0005 KANE CTY			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	13788	4719	9069			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	3064	1048	2016			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	14226	4325	9901			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	3447	1180	2267			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	14226	4325	9901			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1300	436	864			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1300	436	864			
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SO FT	4738	1440	3298			
X4405020	LONGITUDINAL PARTIAL DEPTH REMOVAL 2"	FOOT	49517	17300	32217			
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	1849	646	1203			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	5	2	3			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	4738	1440	3298			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	25.7	25.7			
Ø Z0076600	TRAINEES	HOUR	500	500				REV. - MS
Ø Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500				

* = SPECIALTY ITEMS
Ø 0042

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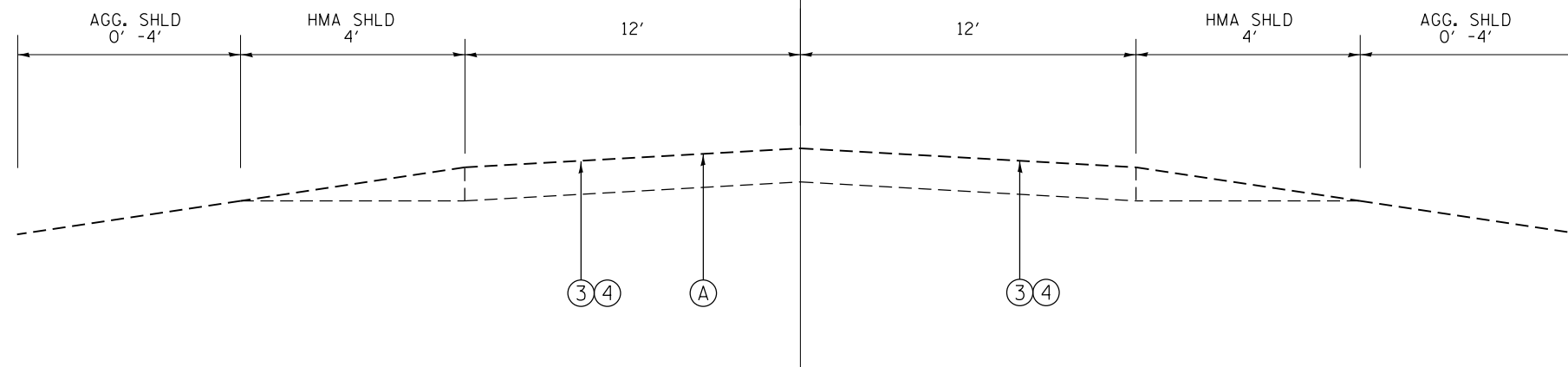
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-RS	COOK/KANE	30	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J96	

U.S. 20 LAKE STREET

CL OF MEDIAN
0' & VARIES



U.S. 20 LAKE STREET
EXISTING TYPICAL SECTION
STA. 20+75 TO STA. 91+61

LEGEND - EXISTING:

(A) HOT-MIX ASPHALT PAVEMENT, +/- 16"

LEGEND - PROPOSED

- (1) LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"
- (2) LONGITUDINAL PARTIAL DEPTH PATCHING, 2"
- (3) INTERMITTENT RESURFACING, HMA SC D 9.5 N70; 2"
- (4) CLASS D PAVEMENT PATCHING, 16"

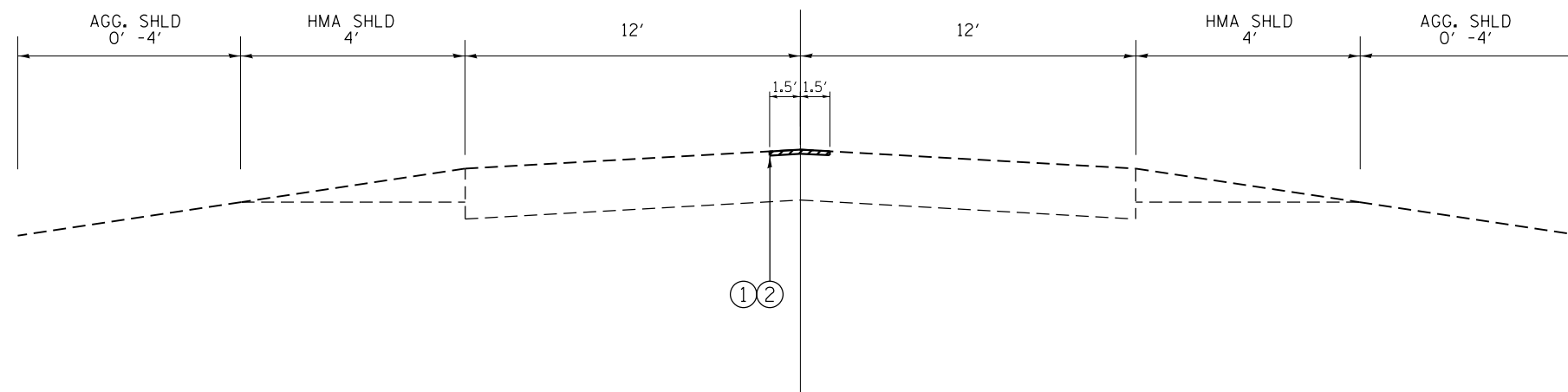
NOTES:

- 1.) THE CONTRACTOR SHALL DO CLASS D PATCHES BEFORE INTERMITTENT RESURFACING.
- 2.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
PATCHING (INTERMITTENT RESURFACING)		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70; 2"	4.0% AT 70 GYR	QC/QA
PATCHING		
CLASS D PATCHES (HOT-MIX ASPHALT BINDER IL-19.0)	4.0% AT 70 GYR	QC/QA
LONGITUDINAL PARTIAL DEPTH PATCHING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D" N70; 2"	4.0% AT 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

U.S. 20 LAKE STREET

CL OF MEDIAN
0' & VARIES



U.S. 20 LAKE STREET
PROPOSED TYPICAL SECTION
STA. 23+75 TO 30+13
STA. 60+40 TO 65+08

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

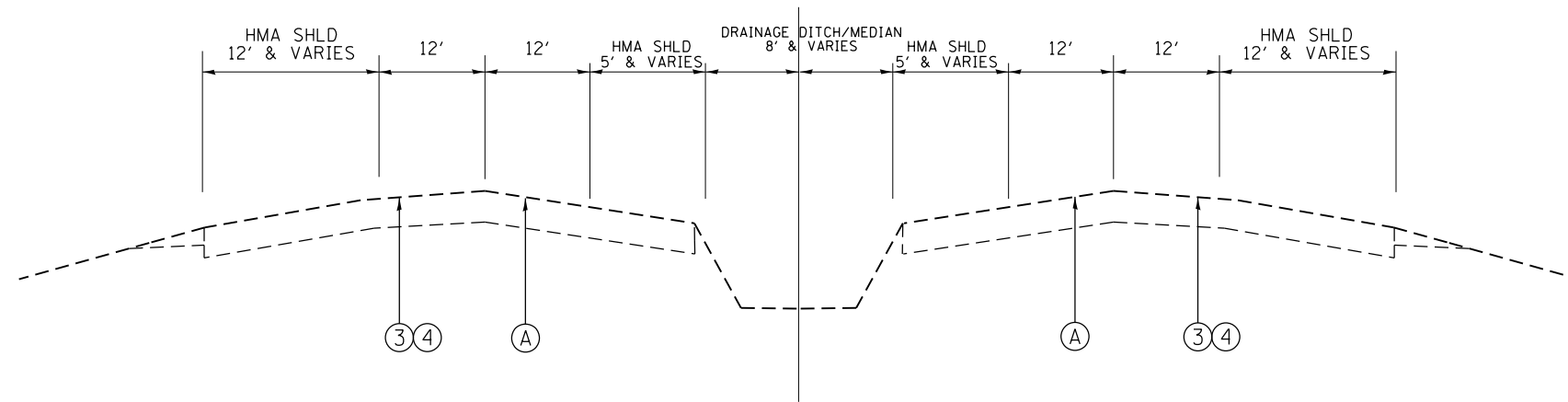
TYPICAL SECTIONS
U.S. 20 LAKE ST. PLANK RD. TO SHALES PKWY.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-RS	COOK/KANE	30	4
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

U.S. 20 LAKE STREET

☐ OF MEDIAN DITCH



U.S. 20 LAKE STREET
EXISTING TYPICAL SECTION
STA. 91+61 TO STA. 396+23

LEGEND - EXISTING:

(A) HOT-MIX ASPHALT PAVEMENT, +/- 16"

LEGEND - PROPOSED

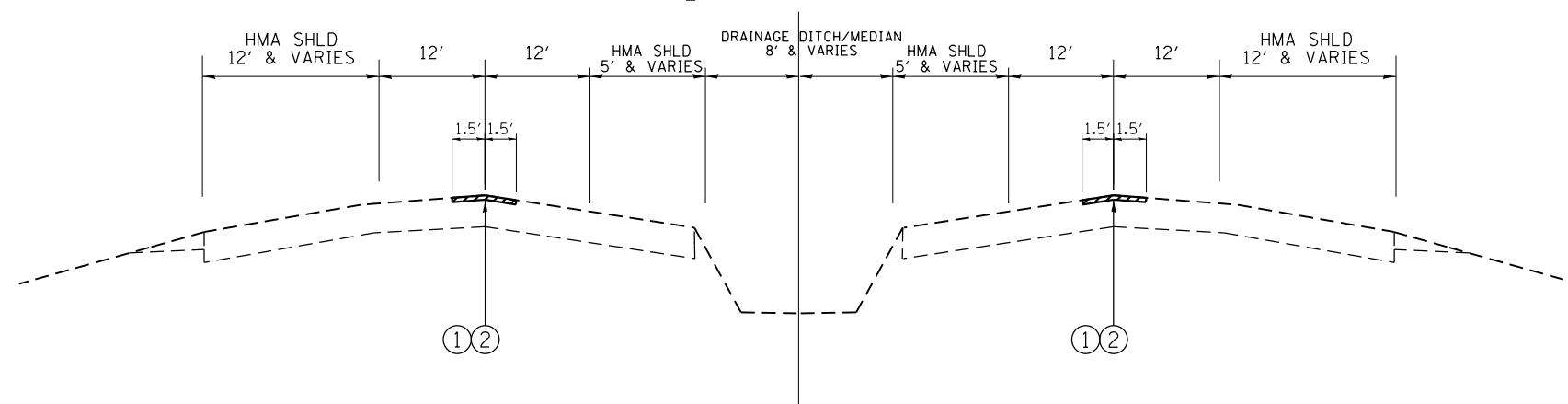
- (1) LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"
- (2) LONGITUDINAL PARTIAL DEPTH PATCHING, 2"
- (3) INTERMITTENT RESURFACING, HMA SC D 9.5 N70; 2"
- (4) CLASS D PAVEMENT PATCHING, 16"

NOTES:

- 1.) THE CONTRACTOR SHALL DO CLASS D PATCHES BEFORE INTERMITTENT RESURFACING.
- 2.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

U.S. 20 LAKE STREET

☐ OF MEDIAN DITCH



U.S. 20 LAKE STREET
PROPOSED TYPICAL SECTION
STA. 93+33 TO 175+68 RT STA. 324+48 TO 326+27
STA. 100+11 TO 175+68 LT STA. 327+72 TO 345+88
STA. 245+93 TO 302+57 STA. 347+05 TO 383+11 LT
STA. 309+16 TO 312+48 STA. 385+33 TO 396+23 LT
STA. 313+85 TO 322+90 STA. 347+05 TO 396+23 RT

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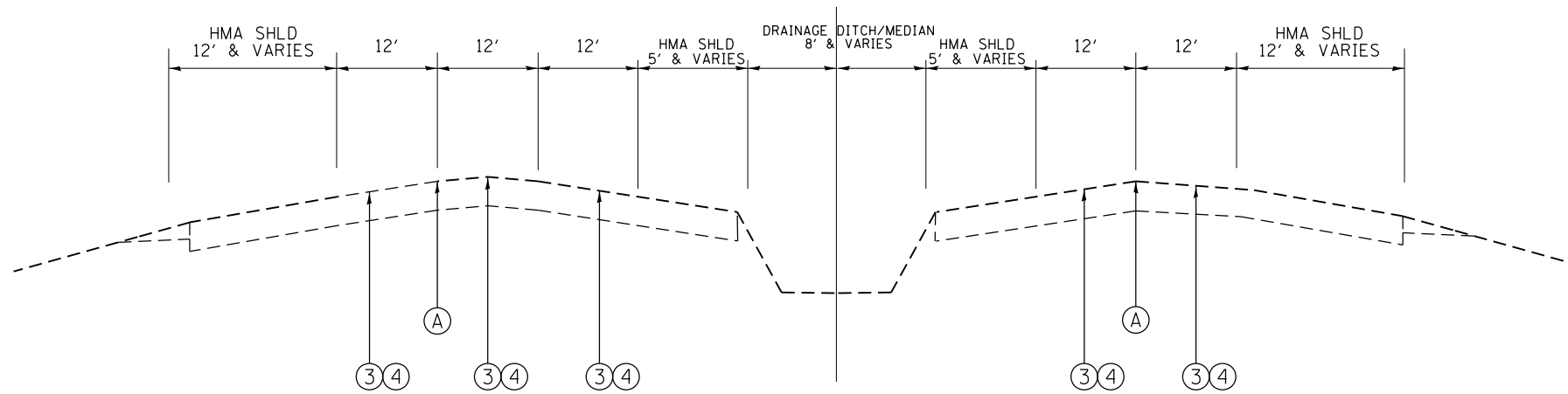
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
U.S. 20 LAKE ST. PLANK RD. TO SHALES PKWY.			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-RS	COOK/KANE	30	5
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

U.S. 20 LAKE STREET

☐ OF MEDIAN DITCH



U.S. 20 LAKE STREET
EXISTING TYPICAL SECTION
STA. 396+23 TO STA. 411+77

LEGEND - EXISTING:

(A) HOT-MIX ASPHALT PAVEMENT, +/- 16"

LEGEND - PROPOSED

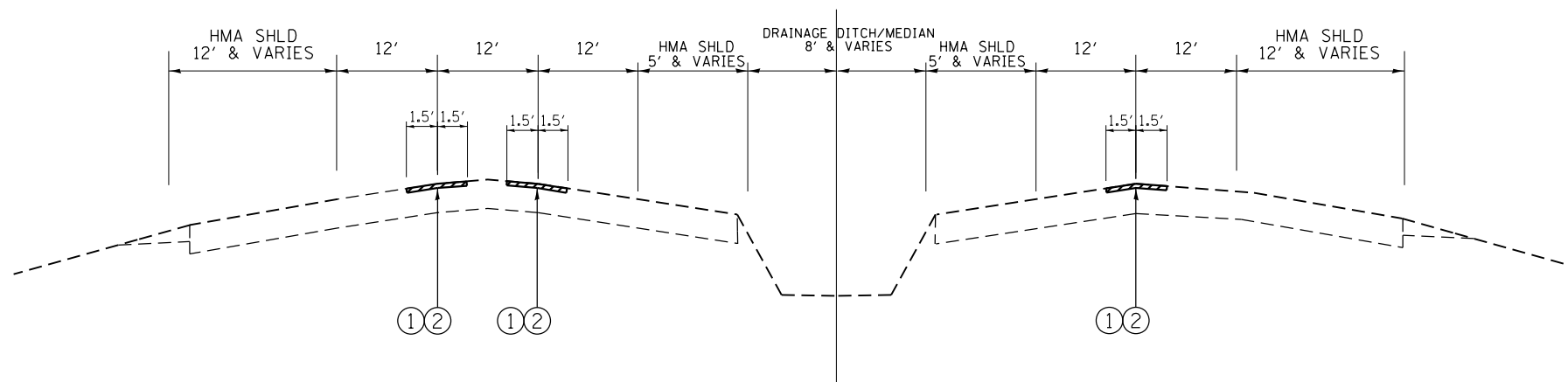
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- (3) INTERMITTENT RESURFACING, HMA SC D 9.5 N70; 2"
- (4) CLASS D PAVEMENT PATCHING, 16"

NOTES:

- 1.) THE CONTRACTOR SHALL DO CLASS D PATCHES BEFORE INTERMITTENT RESURFACING.
- 2.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

U.S. 20 LAKE STREET

☐ OF MEDIAN DITCH



U.S. 20 LAKE STREET
PROPOSED TYPICAL SECTION
STA. 396+23 TO STA. 411+74

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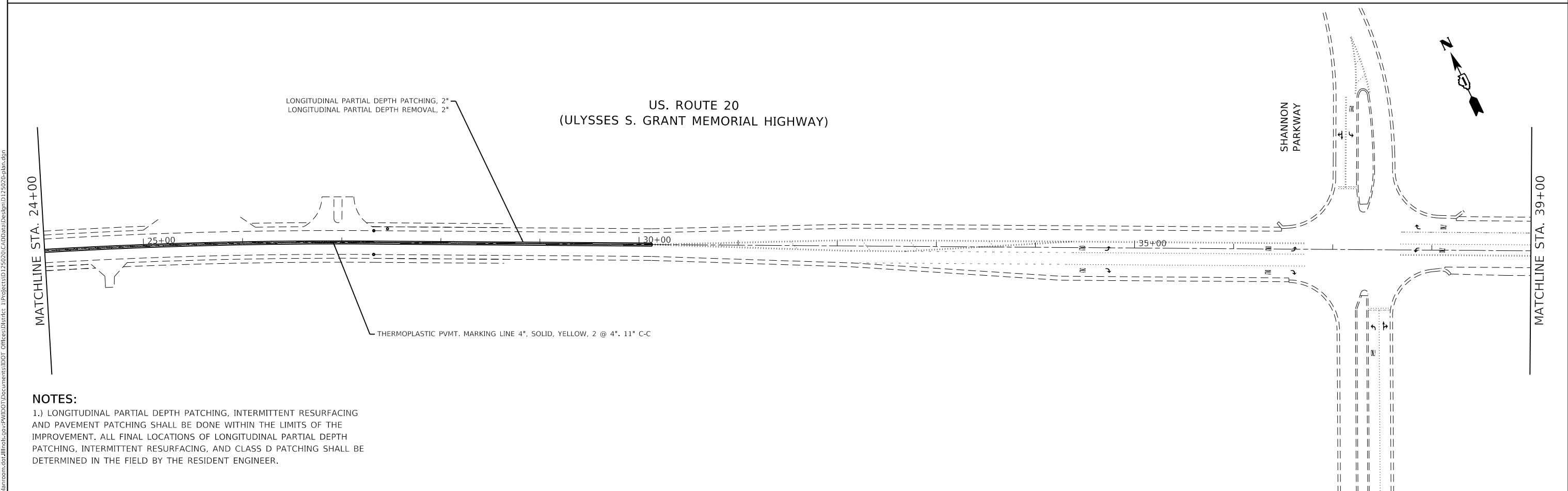
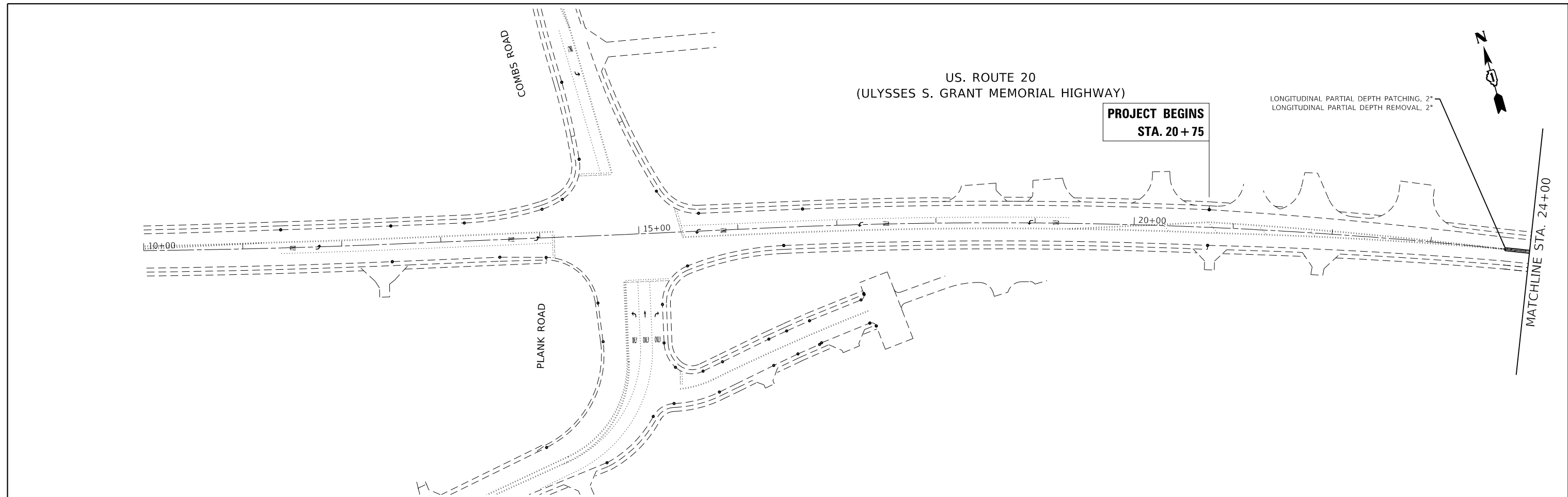
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
U.S. 20 LAKE ST. PLANK RD. TO SHALES PKWY.

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	6
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

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NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

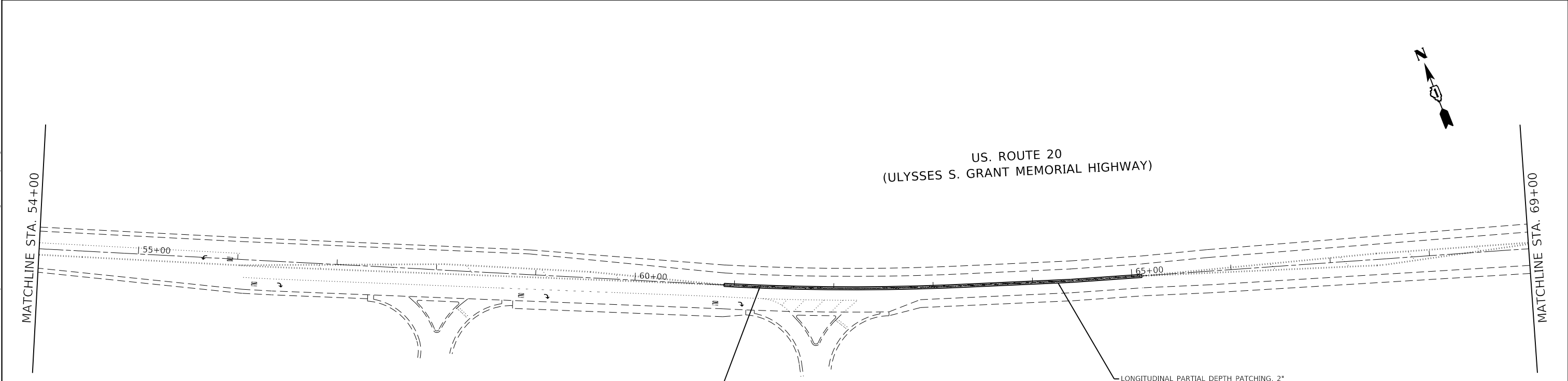
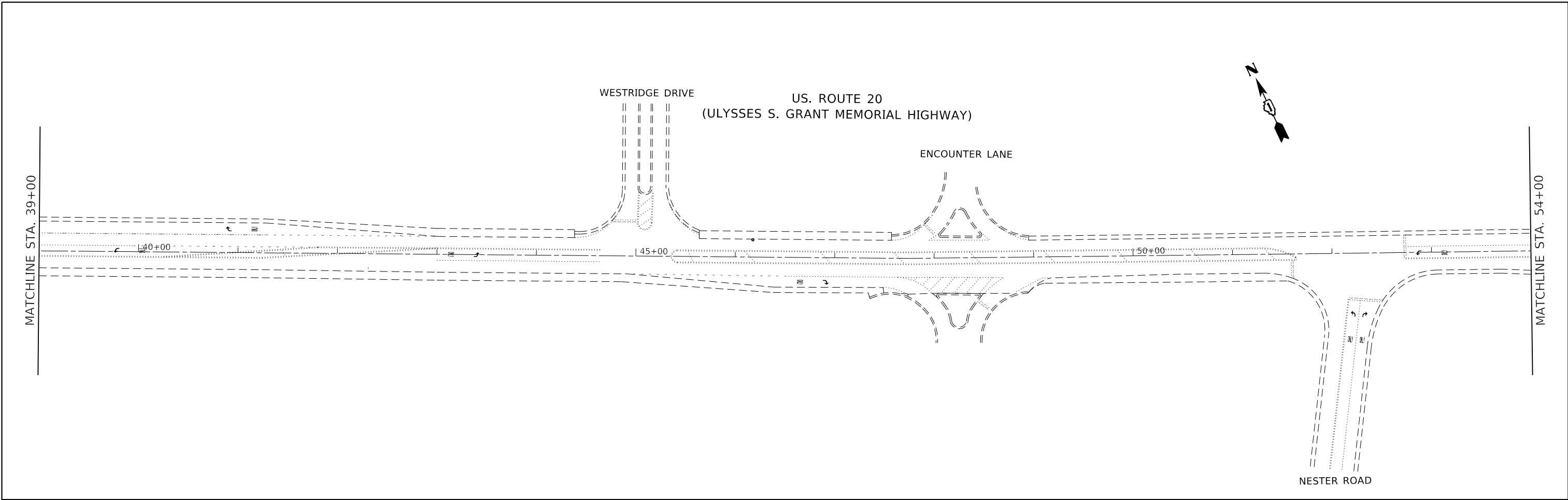
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	7
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

THERMOPLASTIC PVMT, MARKING LINE 4", SOLID, YELLOW, 2 @ 4'. 11" C-C

LONGITUDINAL PARTIAL DEPTH PATCHING, 2"
LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

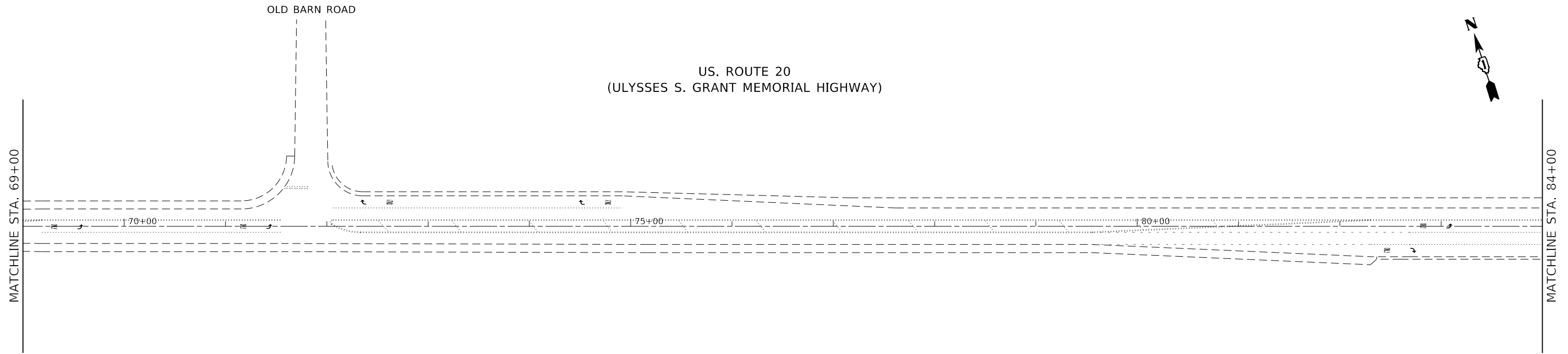
**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

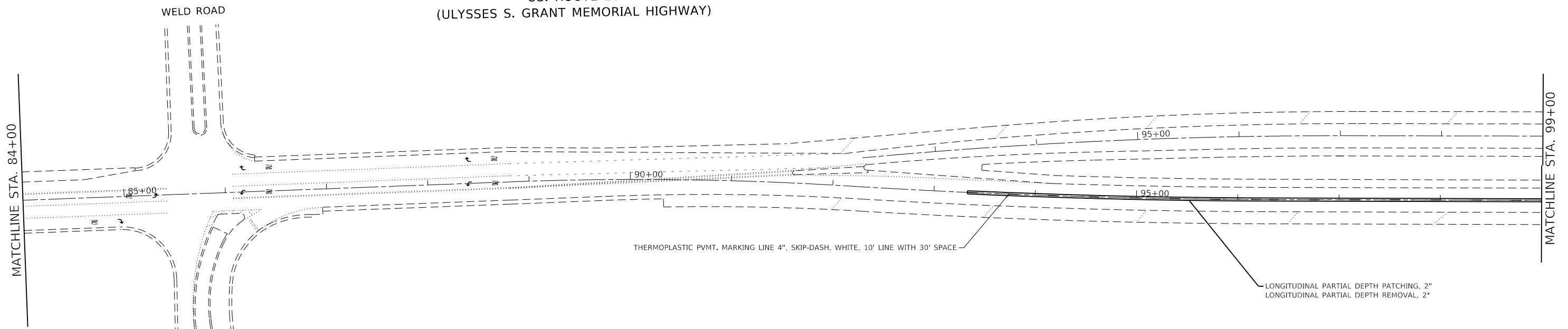
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	8
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 62196

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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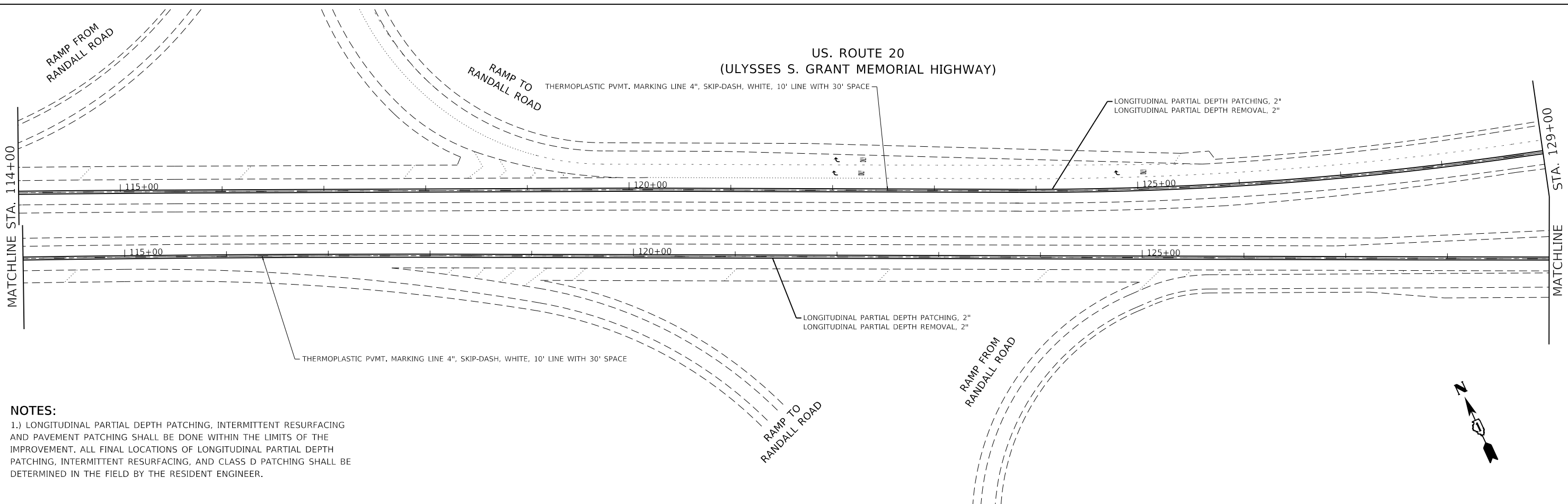
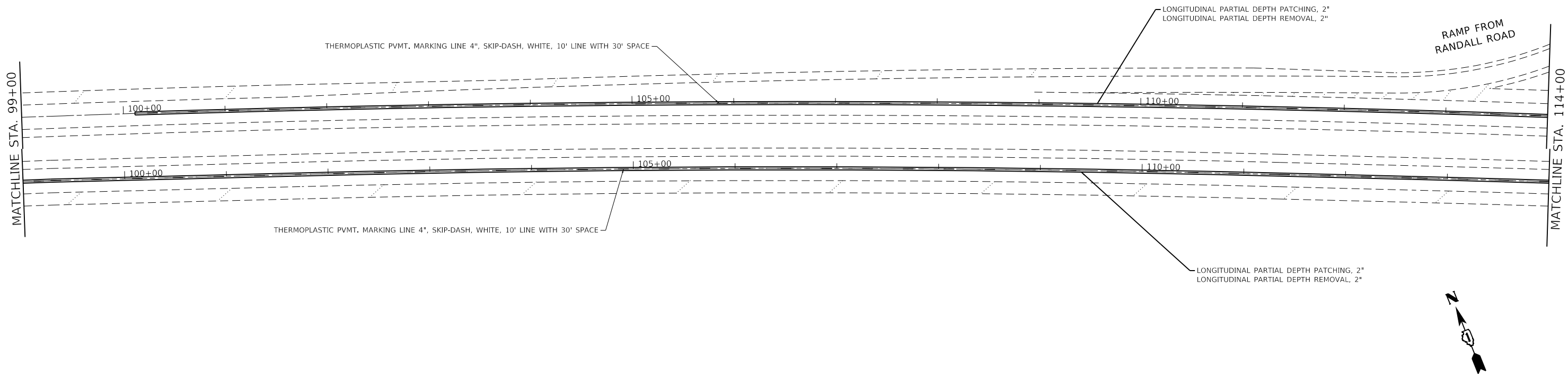
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	9
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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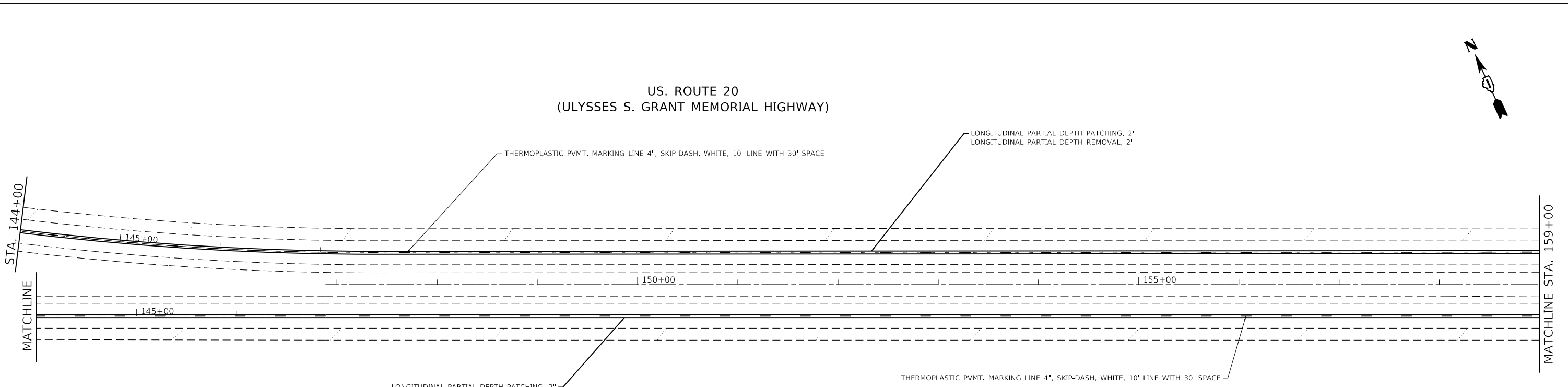
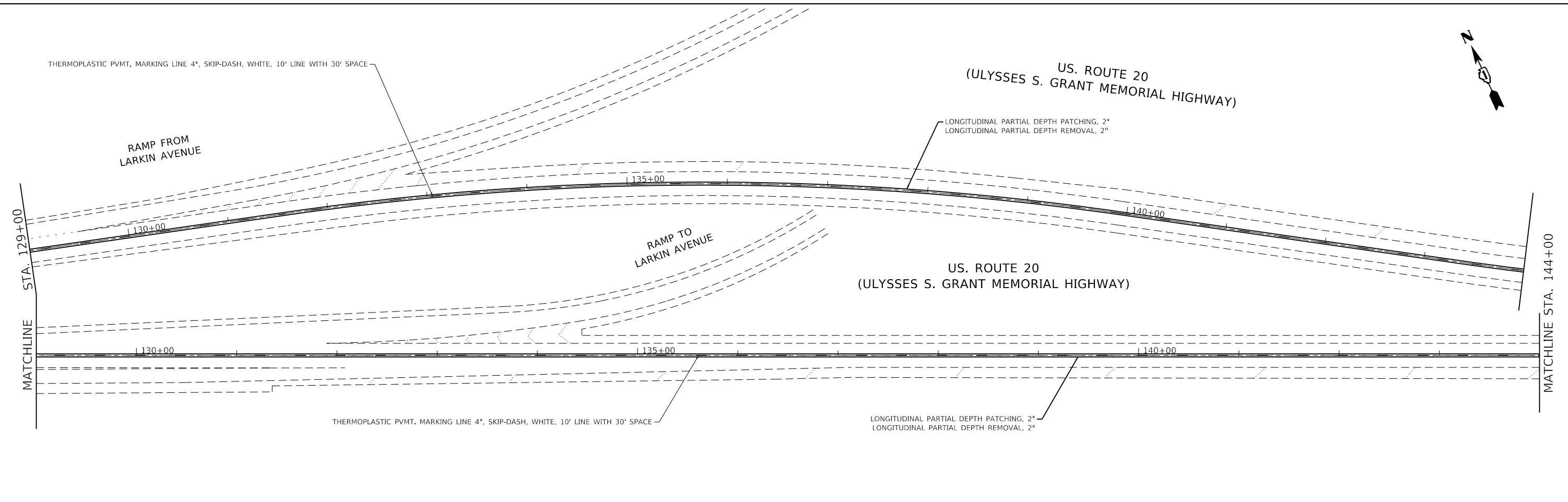
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	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	10
				CONTRACT NO. 62J96
				ILLINOIS FED. AID PROJECT



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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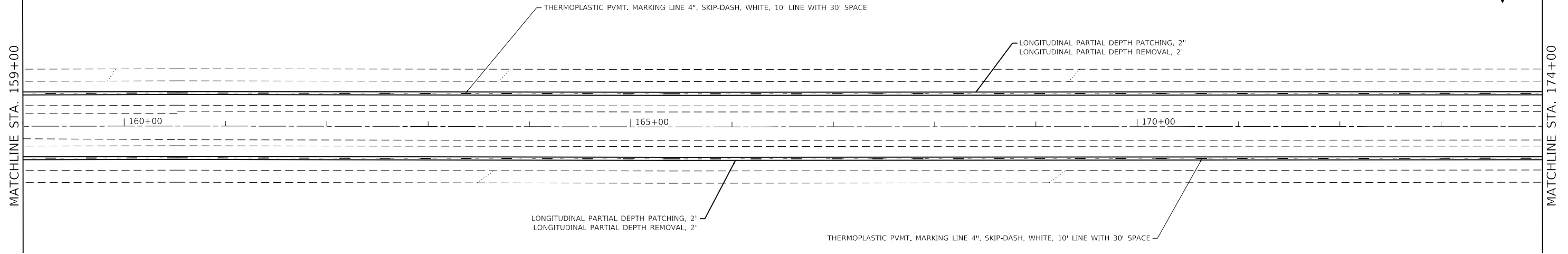
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
 U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

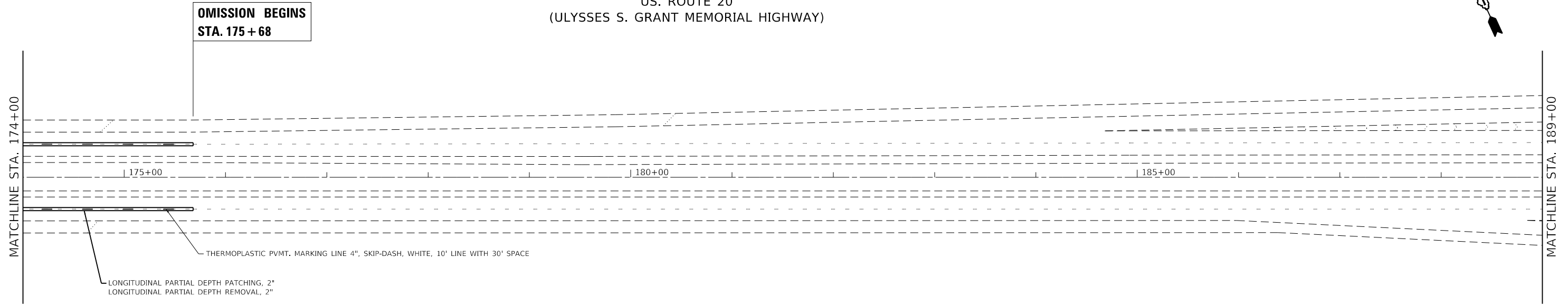
SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	11
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	12
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)

RAMP FROM
MCLEAN BOULEVARD



MATCHLINE STA. 189+00

MATCHLINE STA. 204+00

190+00

195+00

200+00

RAMP TO
MCLEAN BOULEVARD

RAMP TO
MCLEAN BOULEVARD

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



MATCHLINE STA. 204+00

MATCHLINE STA. 219+00

205+00

210+00

215+00

RAMP FROM
MCLEAN BOULEVARD

NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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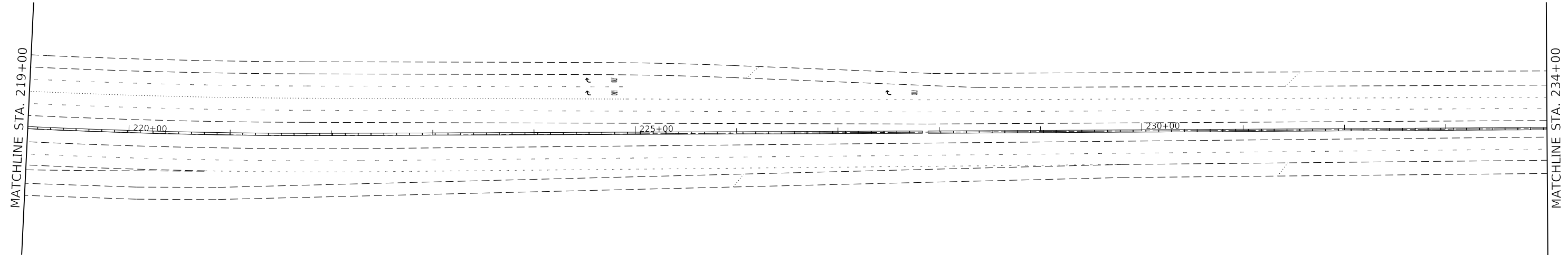
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

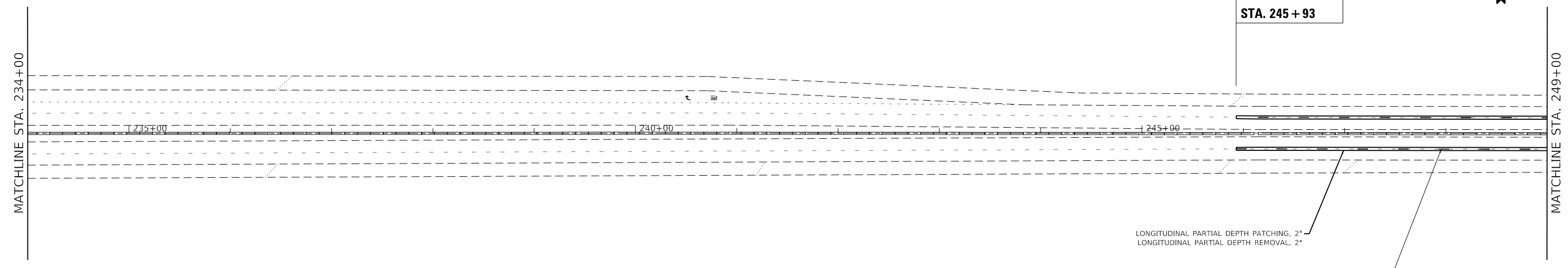
SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	13
			CONTRACT NO. 62J96	
		ILLINOIS	FED. AID PROJECT	

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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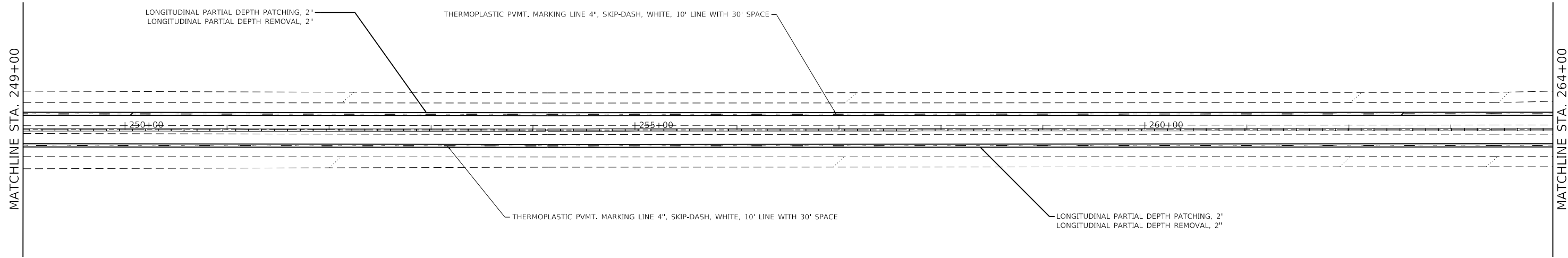
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

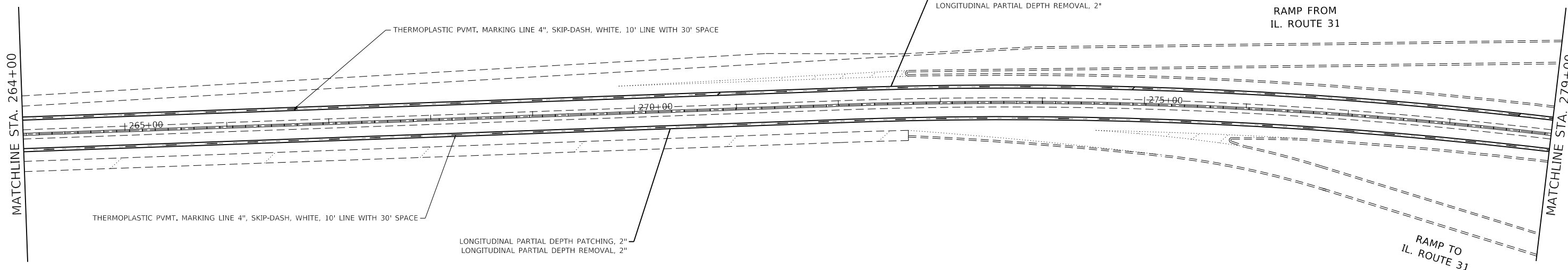
ROADWAY PLAN	
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)	
SCALE: 1" = 50'	SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	14
CONTRACT NO. 62J96				
ILLINOIS		FED. AID PROJECT		

US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

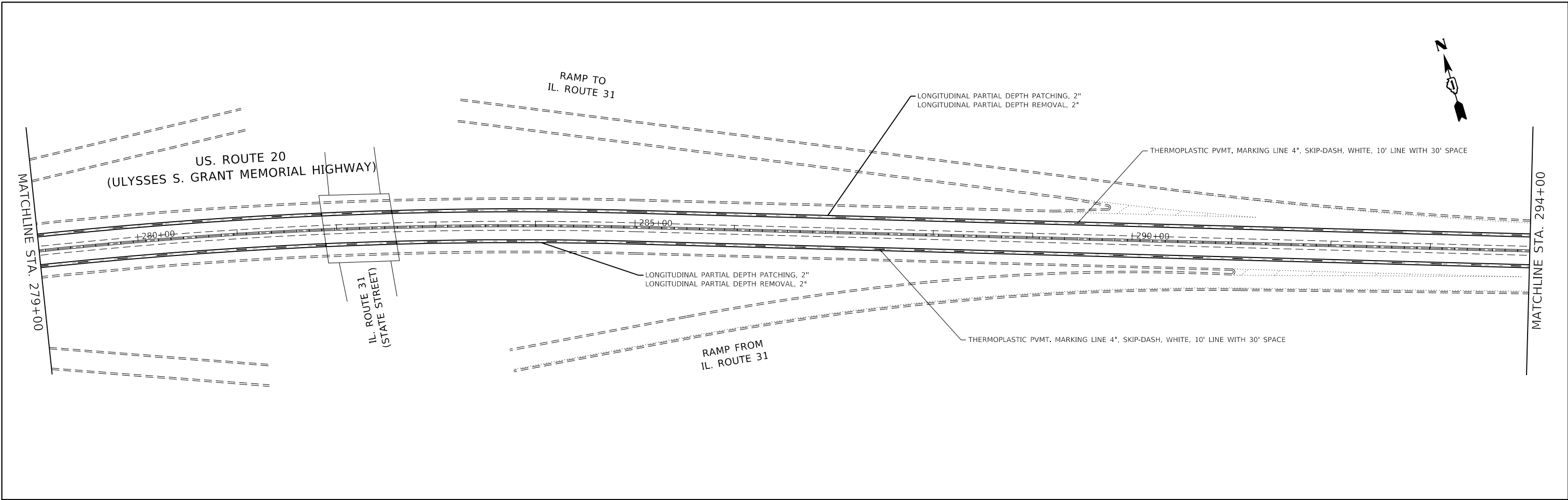
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

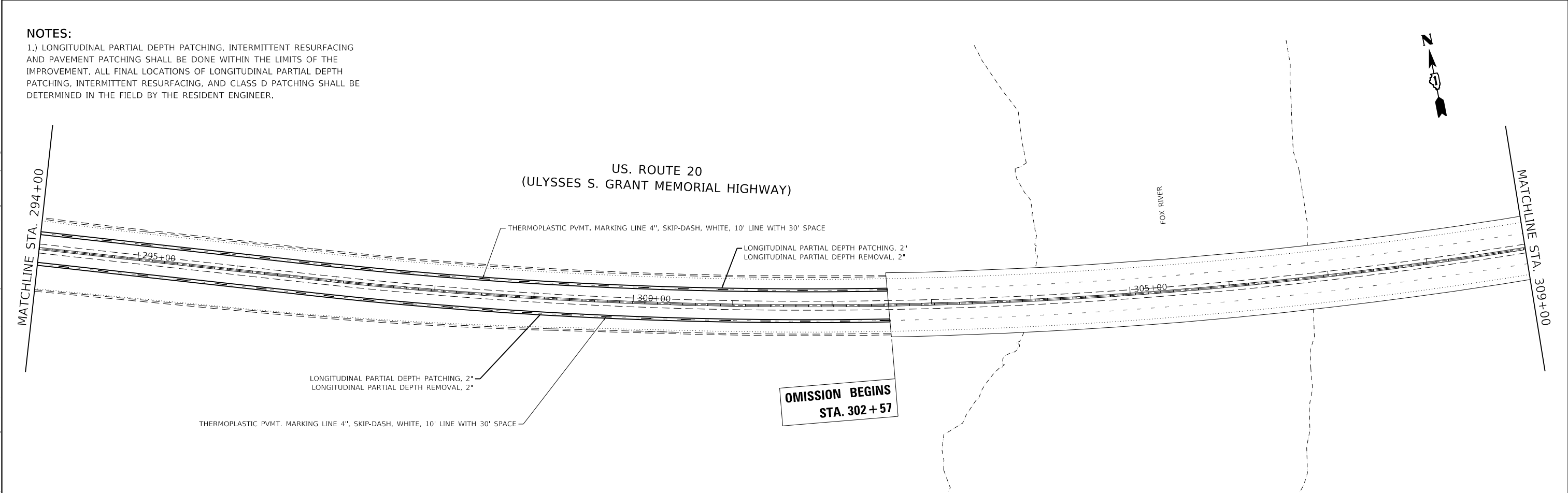
**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**
 SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	15
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.



**OMISSION BEGINS
STA. 302 + 57**

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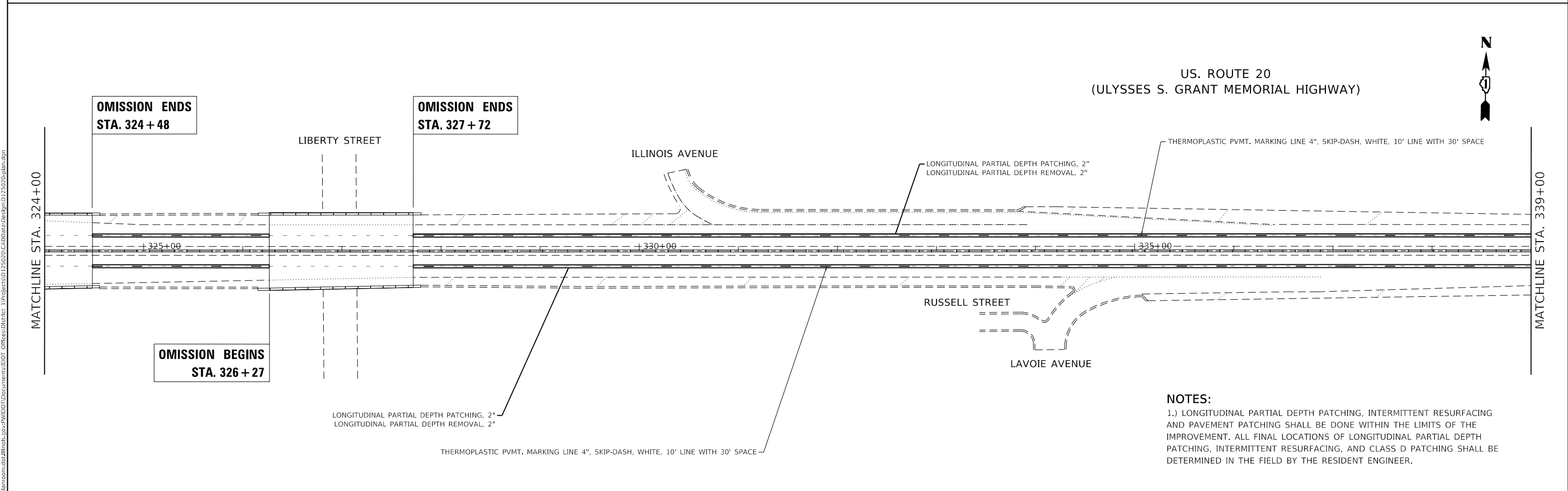
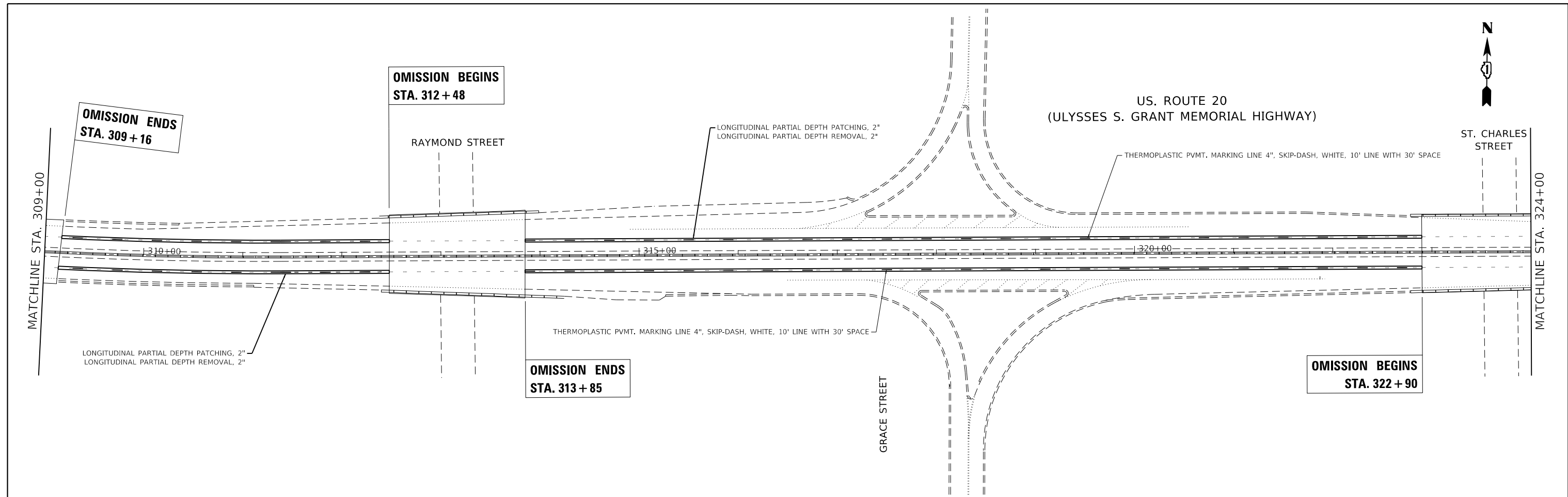
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PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	16
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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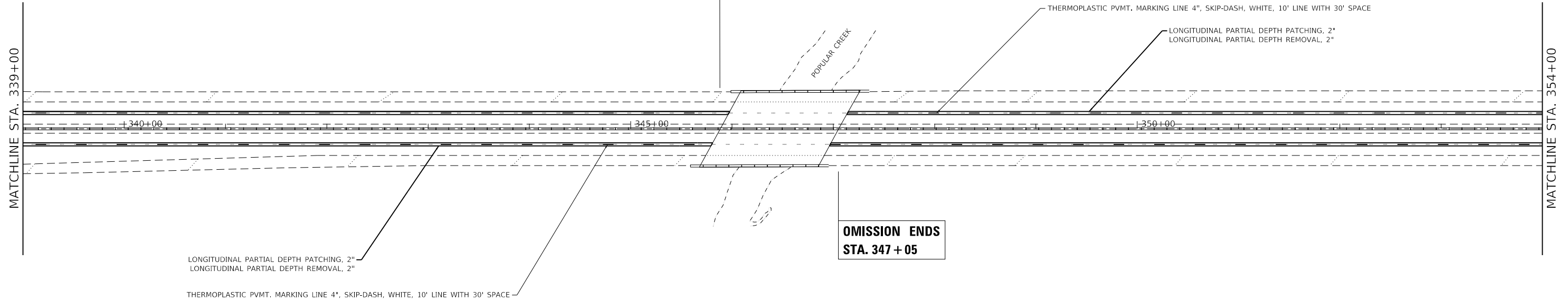
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)			
SCALE: 1"= 50'	SHEET	OF SHEETS	STA. TO STA.

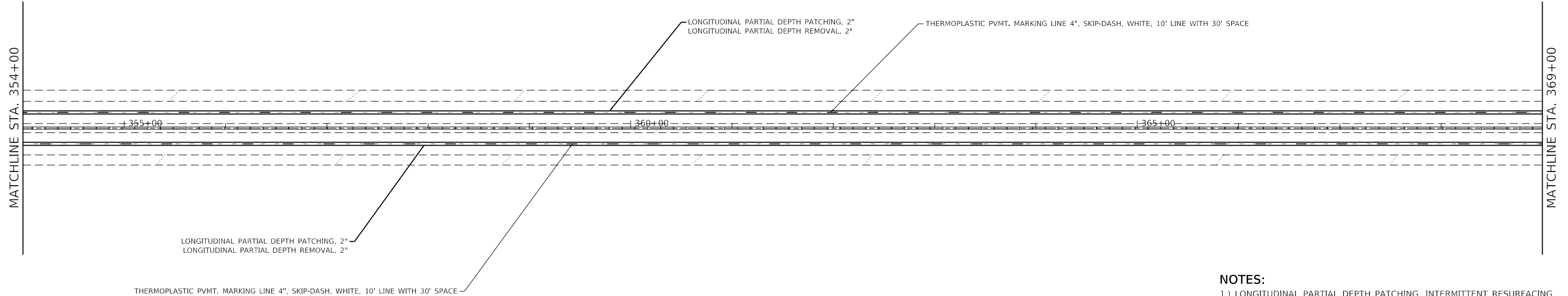
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345	2019-090-R5	COOK/KANE	30	17
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



US. ROUTE 20
(ULYSSES S. GRANT MEMORIAL HIGHWAY)



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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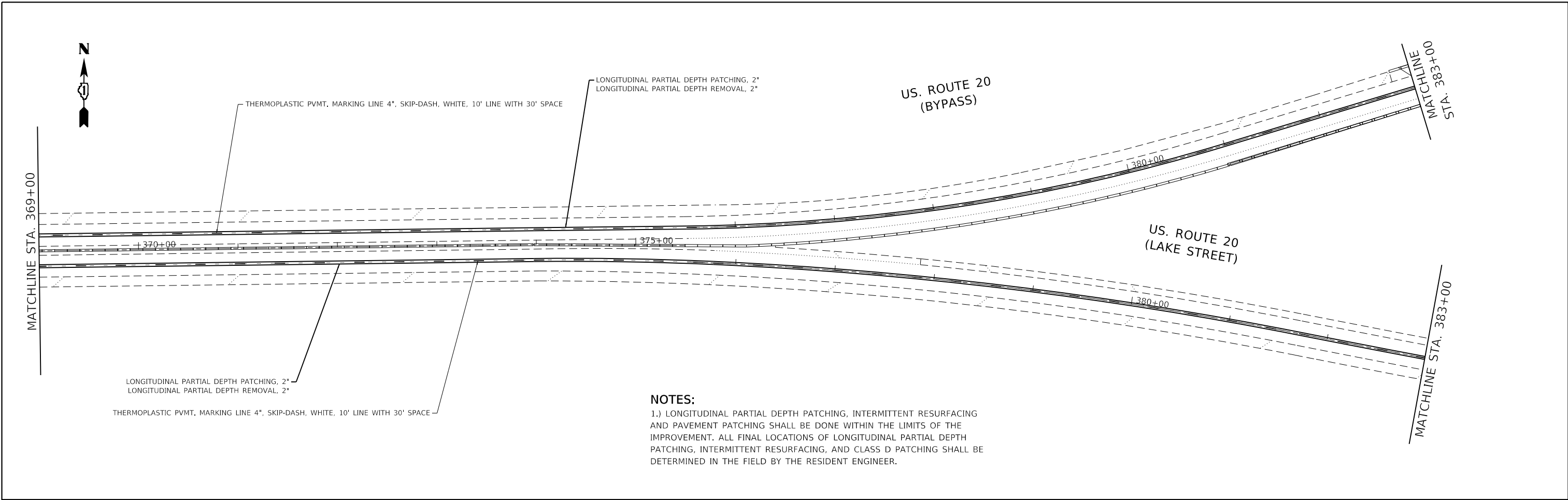
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	18
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

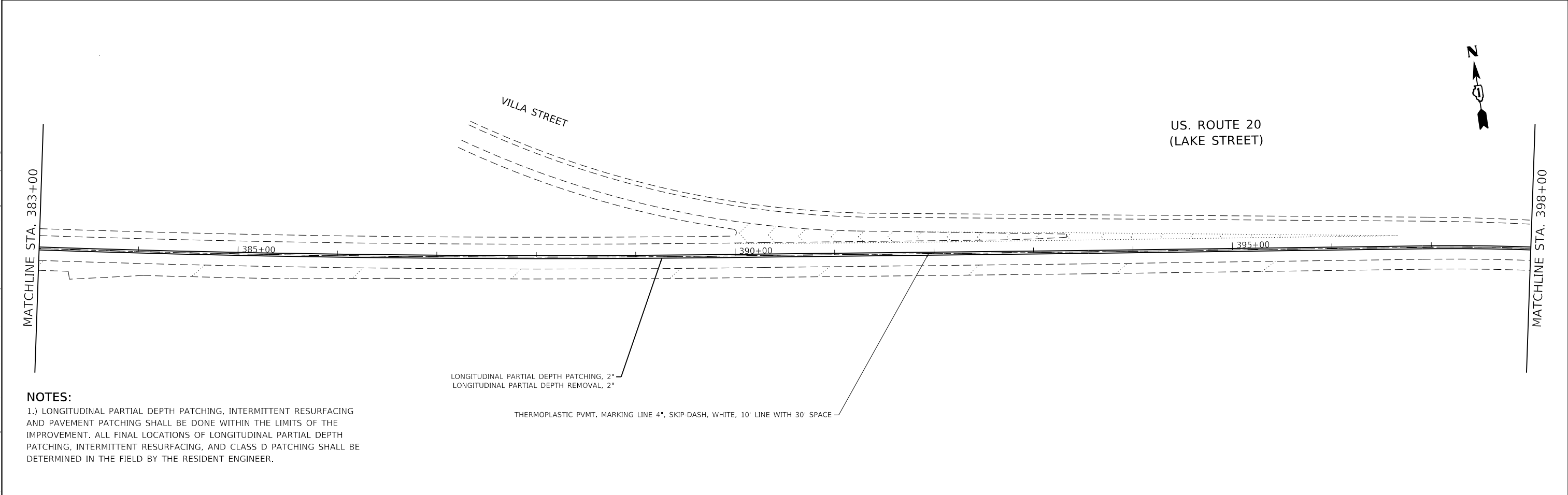
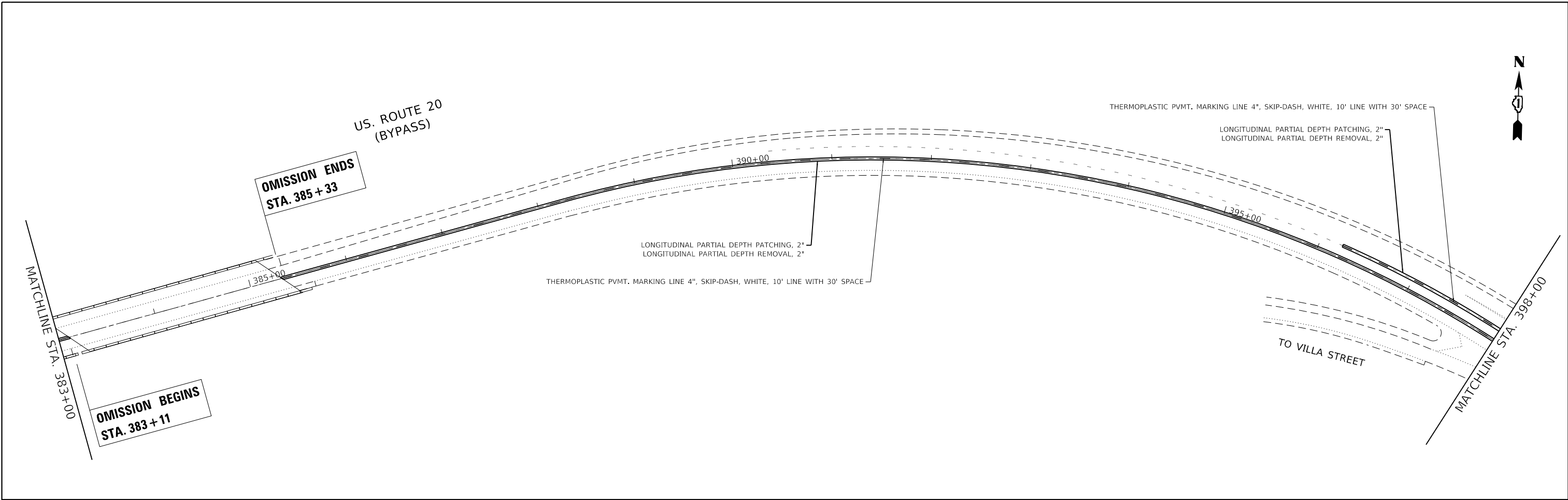
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PLOT DATE = 1/31/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)			
SCALE: 1"= 50'	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	19
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



NOTES:
 1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

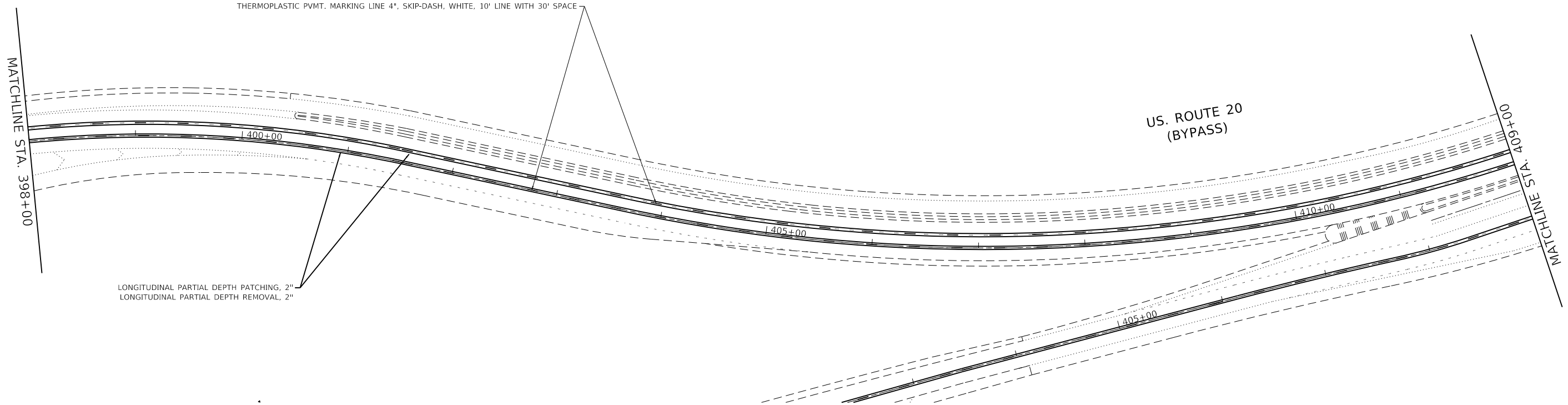
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	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

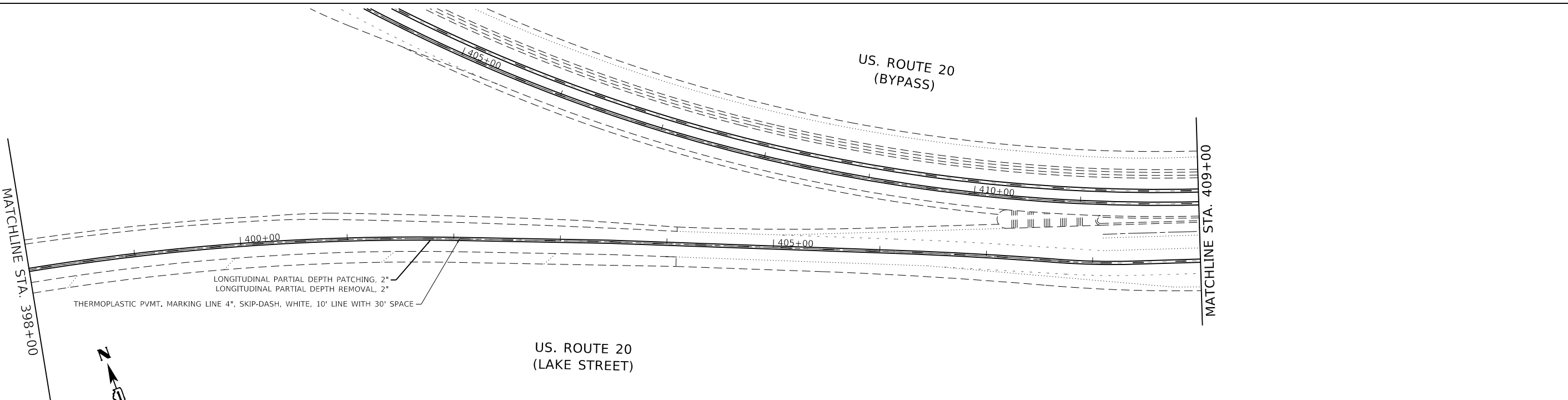
ROADWAY PLAN			
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)			
SCALE: 1"= 50'	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	20
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



LONGITUDINAL PARTIAL DEPTH PATCHING, 2"
LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"

US. ROUTE 20
(BYPASS)



LONGITUDINAL PARTIAL DEPTH PATCHING, 2"
LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"

US. ROUTE 20
(LAKE STREET)

NOTES:
1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

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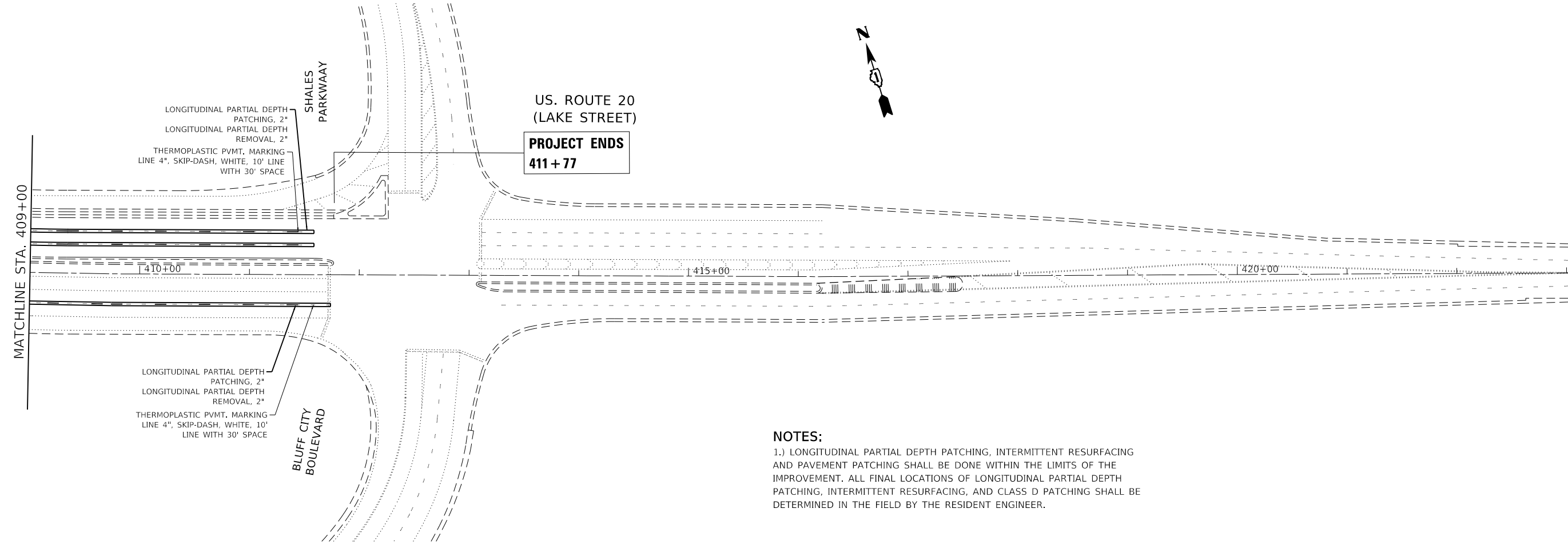
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	21
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



LONGITUDINAL PARTIAL DEPTH
PATCHING, 2"
LONGITUDINAL PARTIAL DEPTH
REMOVAL, 2"
THERMOPLASTIC PVMT, MARKING
LINE 4", SKIP-DASH, WHITE, 10'
LINE WITH 30' SPACE

SHALES
PARKWAY

US. ROUTE 20
(LAKE STREET)

PROJECT ENDS
411+77

MATCHLINE STA. 409+00

410+00

415+00

420+00

LONGITUDINAL PARTIAL DEPTH
PATCHING, 2"
LONGITUDINAL PARTIAL DEPTH
REMOVAL, 2"
THERMOPLASTIC PVMT, MARKING
LINE 4", SKIP-DASH, WHITE, 10'
LINE WITH 30' SPACE

BLUFF CITY
BOULEVARD

NOTES:

1.) LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING AND PAVEMENT PATCHING SHALL BE DONE WITHIN THE LIMITS OF THE IMPROVEMENT. ALL FINAL LOCATIONS OF LONGITUDINAL PARTIAL DEPTH PATCHING, INTERMITTENT RESURFACING, AND CLASS D PATCHING SHALL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

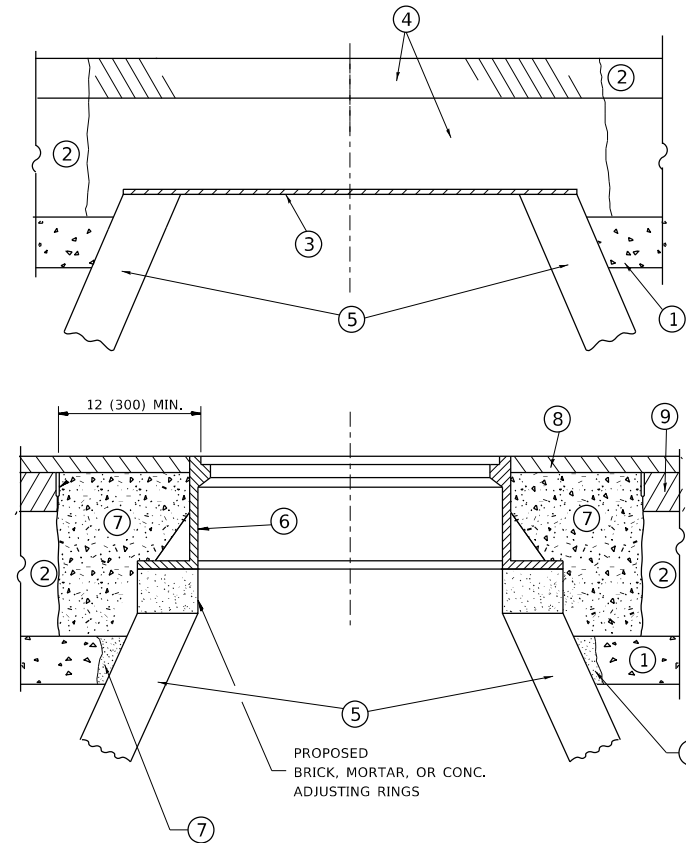
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USER NAME = skinnerl	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
U.S. ROUTE 20 (PLANK ROAD TO SHALES PARKWAY)			
SCALE: 1"= 50'	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	22
CONTRACT NO. 62J96				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 * CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT *THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1 *CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR *FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

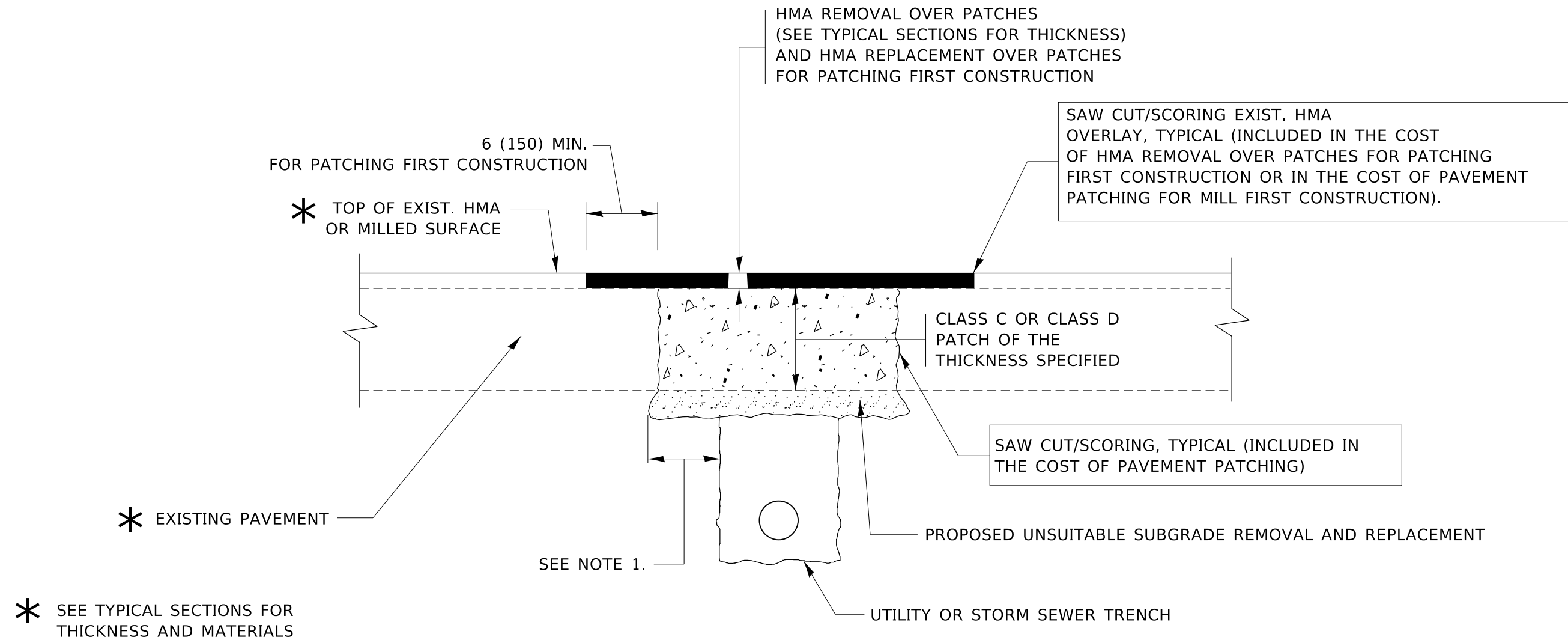
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	23
BD600-03 (BD-8)		CONTRACT NO. 62J96		
ILLINOIS FED. AID PROJECT				

USER NAME = skinneral	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 1/31/2020	DATE - 10-25-94	REVISED - R. BORO 12-06-11



NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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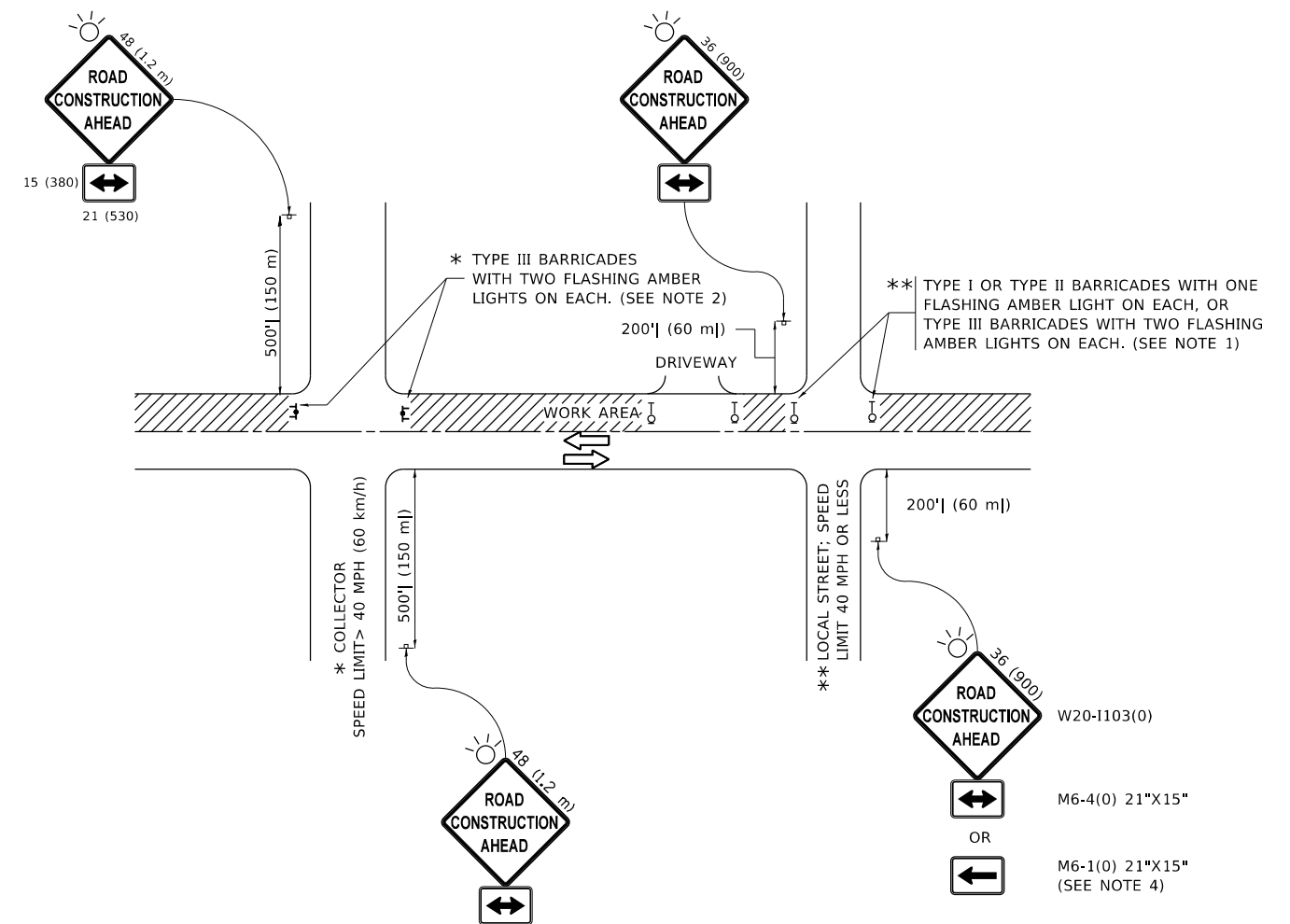
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	DRAWN -	REVISED - R. BORO 01-01-07
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PLOT DATE = 1/31/2020	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	24
BD400-04 (BD-22)			CONTRACT NO. 62J96	
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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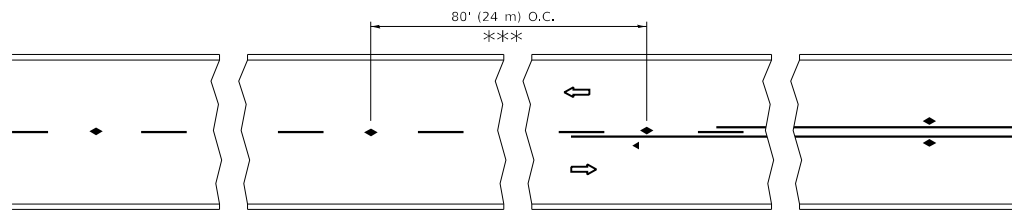
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 1/31/2020	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

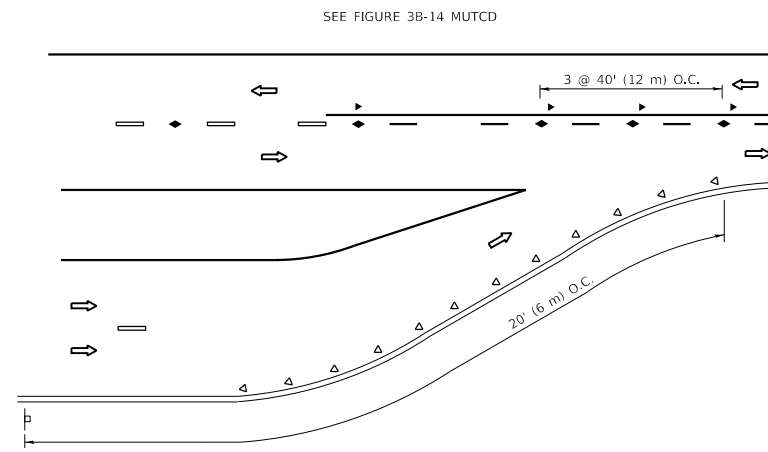
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TC-10		CONTRACT NO. 62J96		
ILLINOIS FED. AID PROJECT				

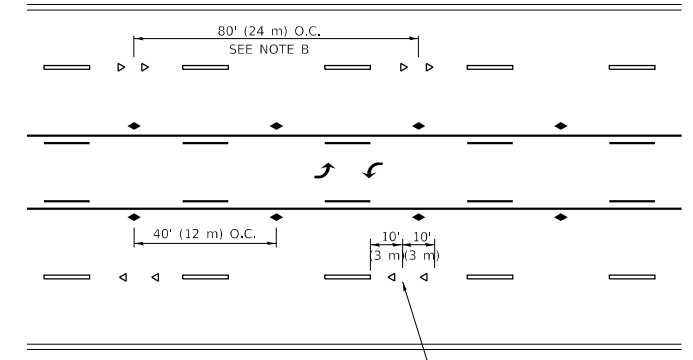


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

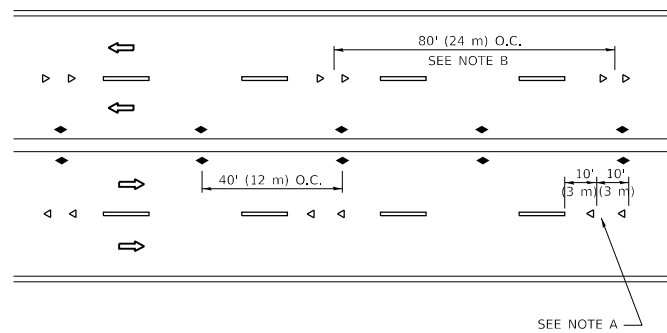
TWO-LANE/TWO-WAY



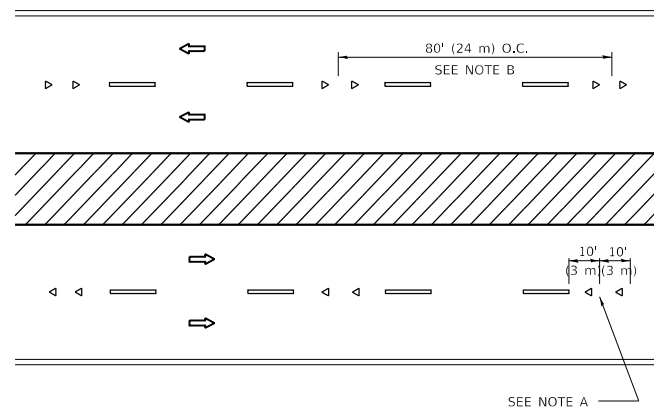
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

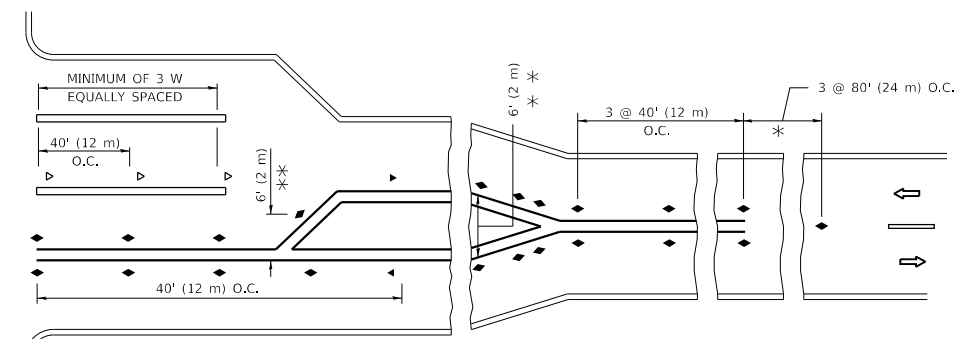
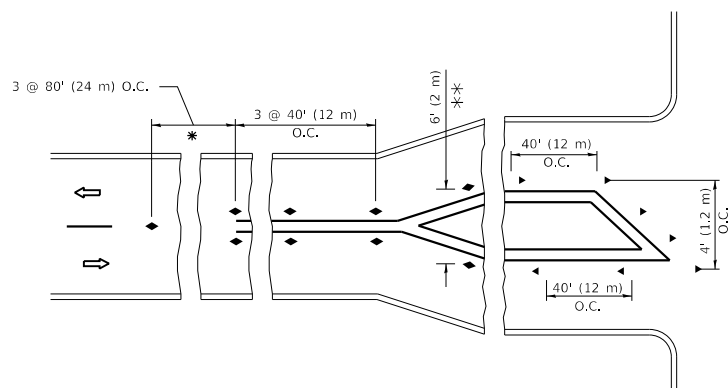
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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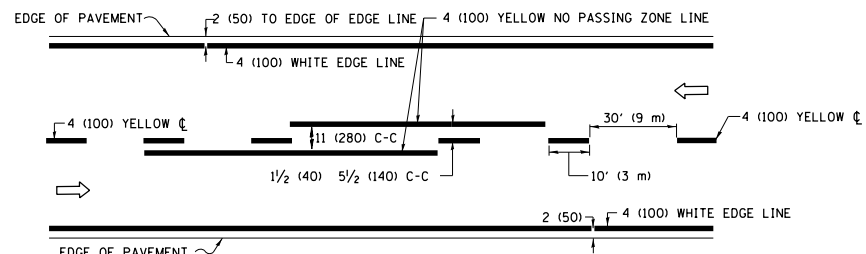
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

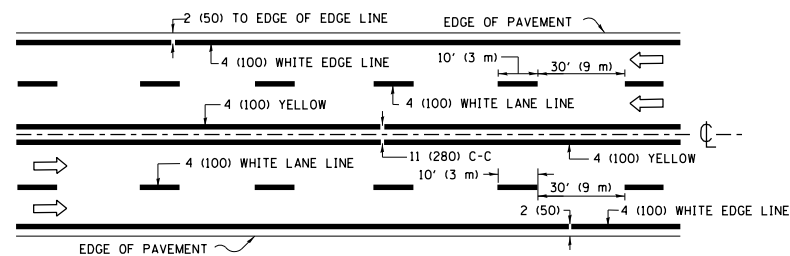
**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

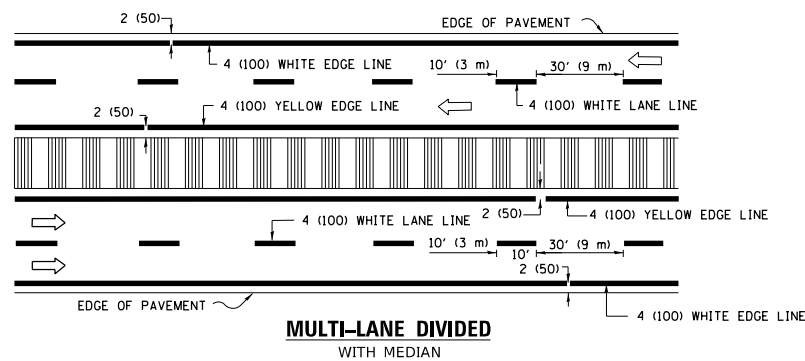
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	26
TC-11		CONTRACT NO. 62J96		
ILLINOIS		FED. AID PROJECT		



2-LANE ROADWAY

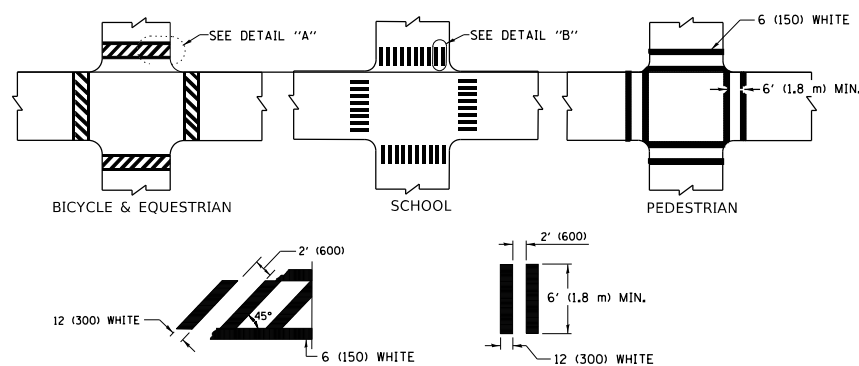


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

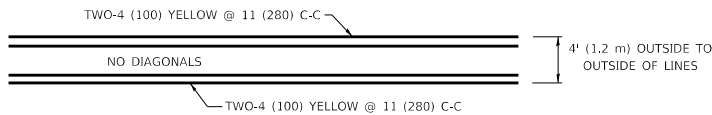


DETAIL "A"

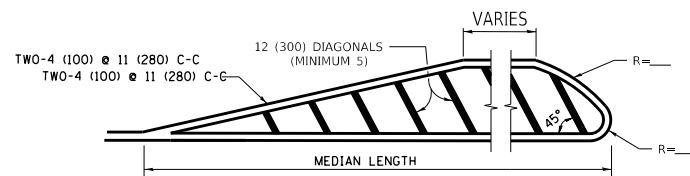
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

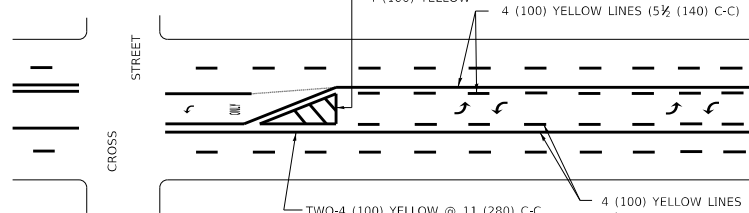


4' (1.2 m) WIDE MEDIANS ONLY



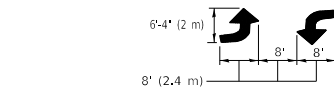
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

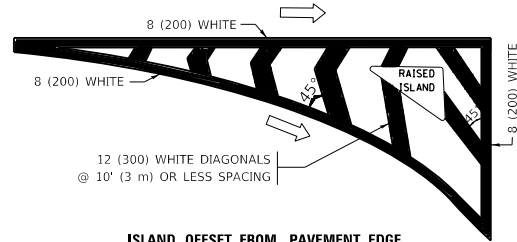
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



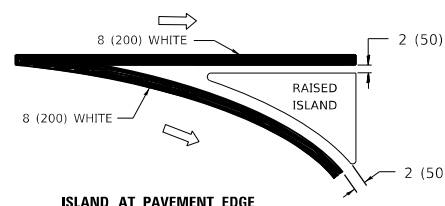
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

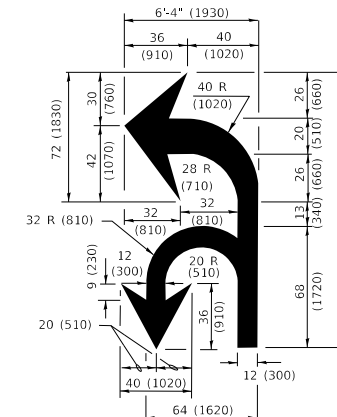


ISLAND OFFSET FROM PAVEMENT EDGE

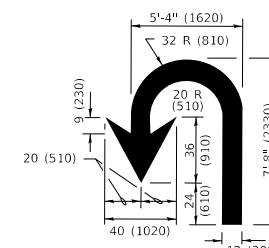


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES *RR* IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR *X*	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
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USER NAME = skinneral	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT DATE = 1/31/2020	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	27
TC-13		CONTRACT NO. 62196		
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

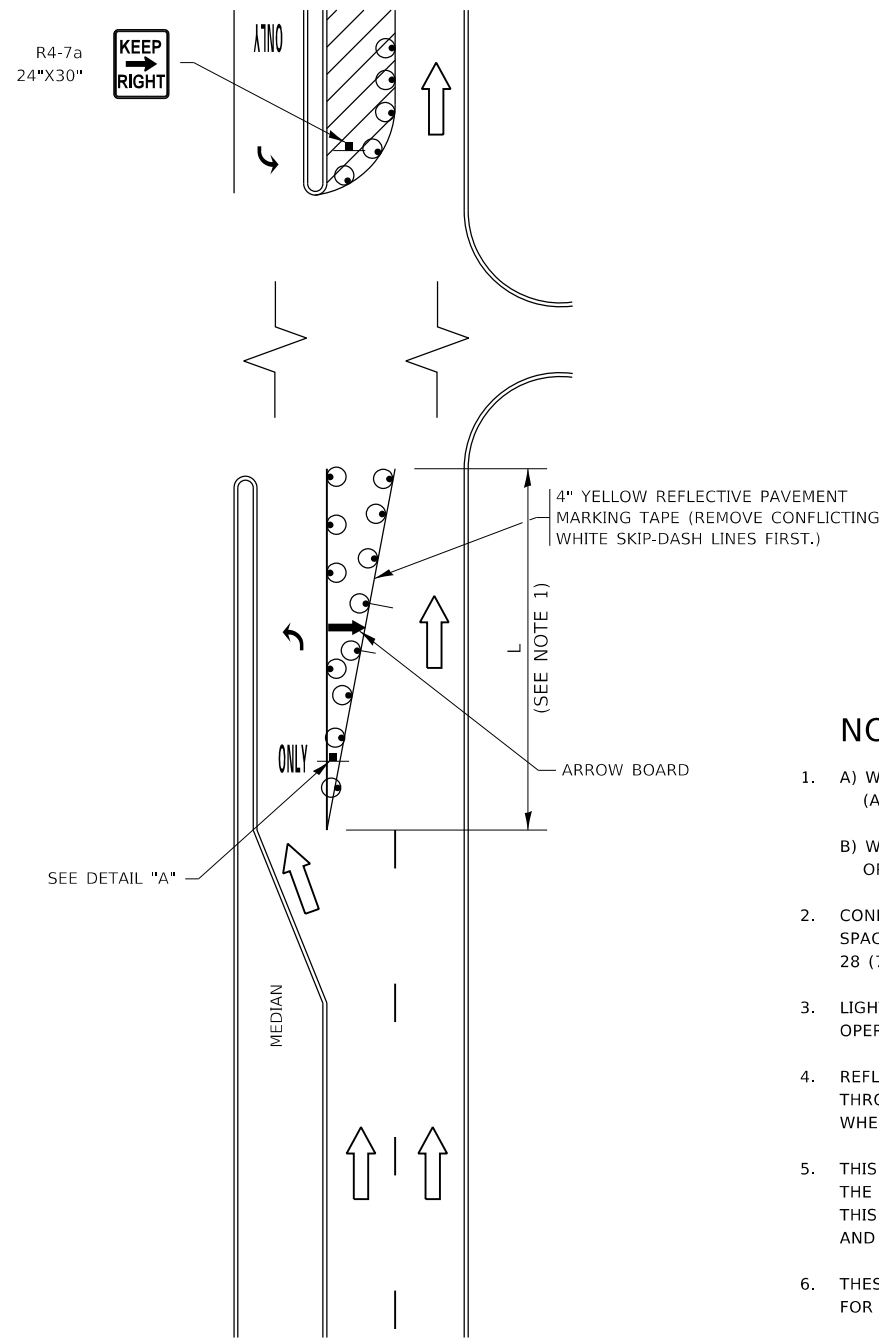


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

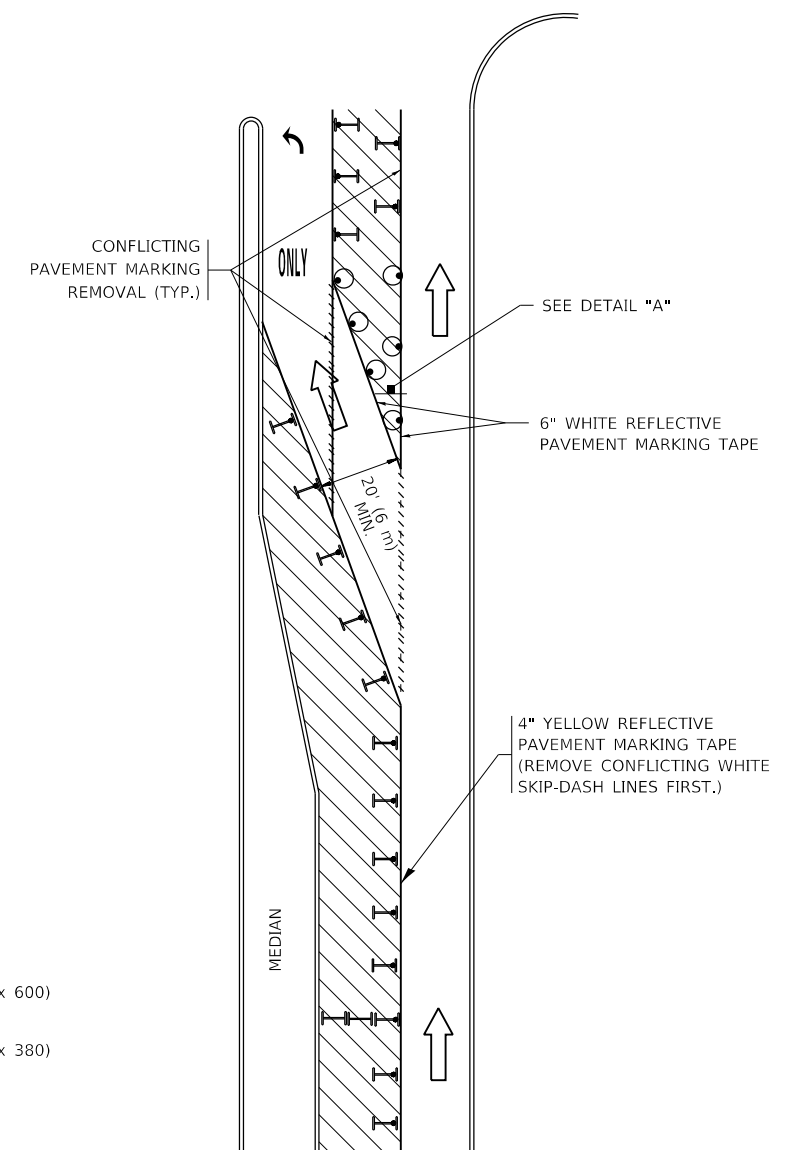
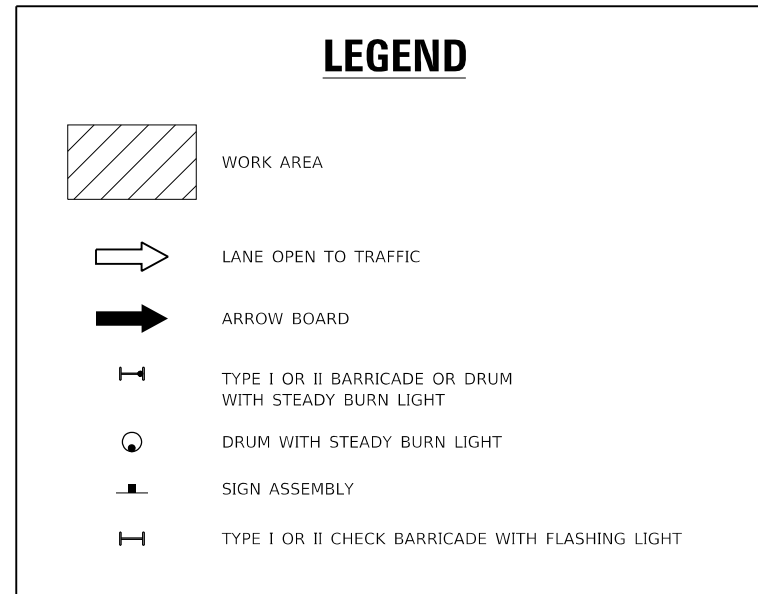
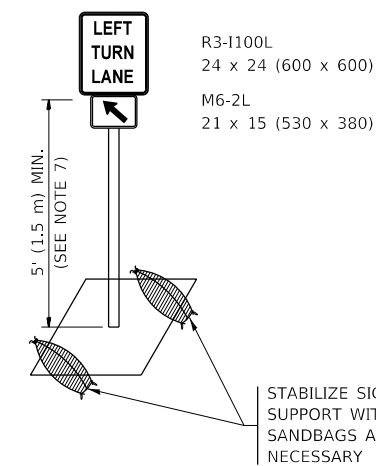


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
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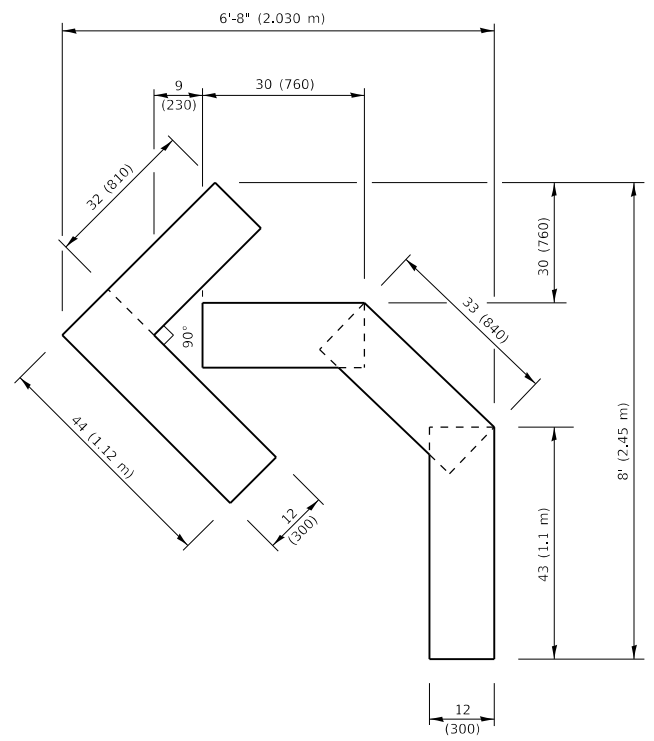
USER NAME = skinner	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 1/31/2020	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS
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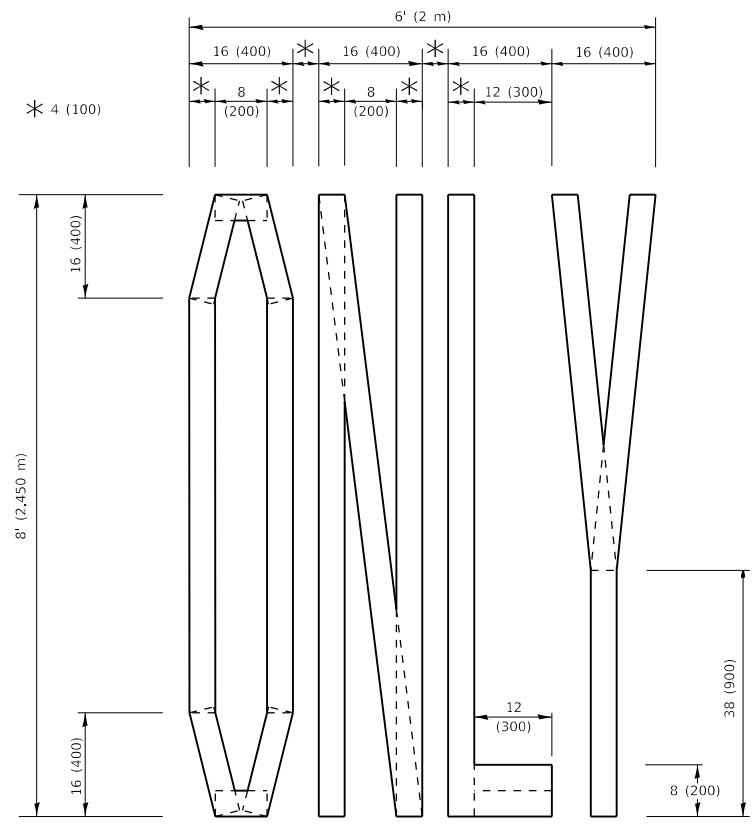
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

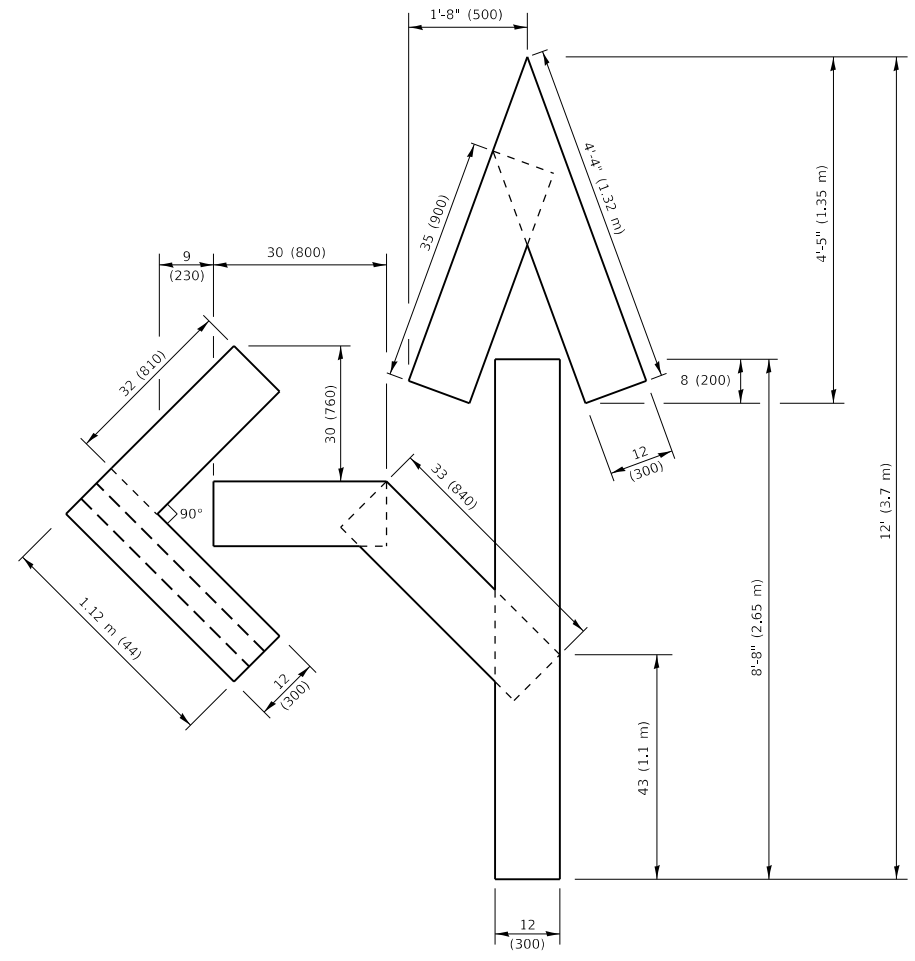
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	28
TC-14		CONTRACT NO. 62J96		
ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

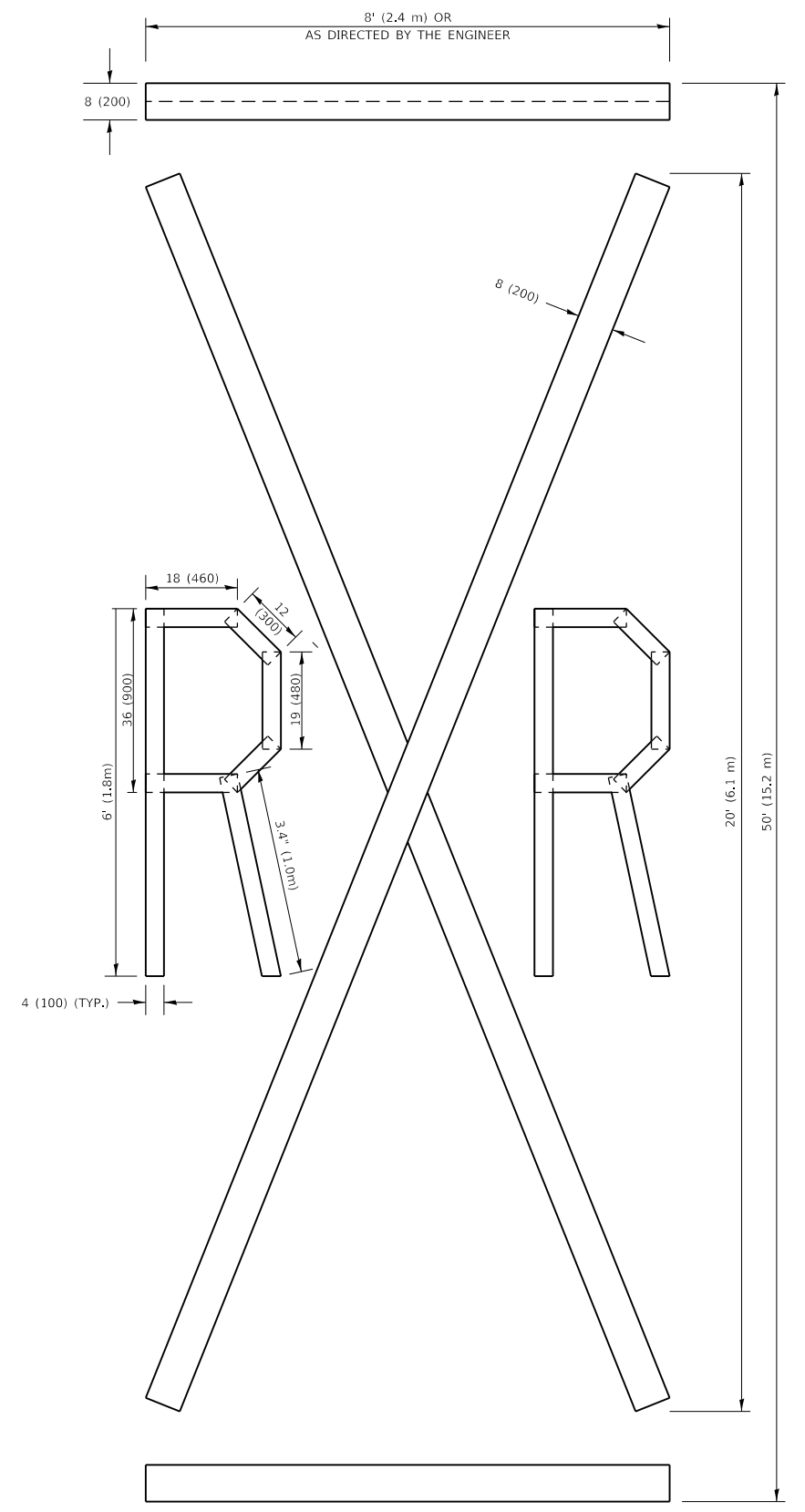


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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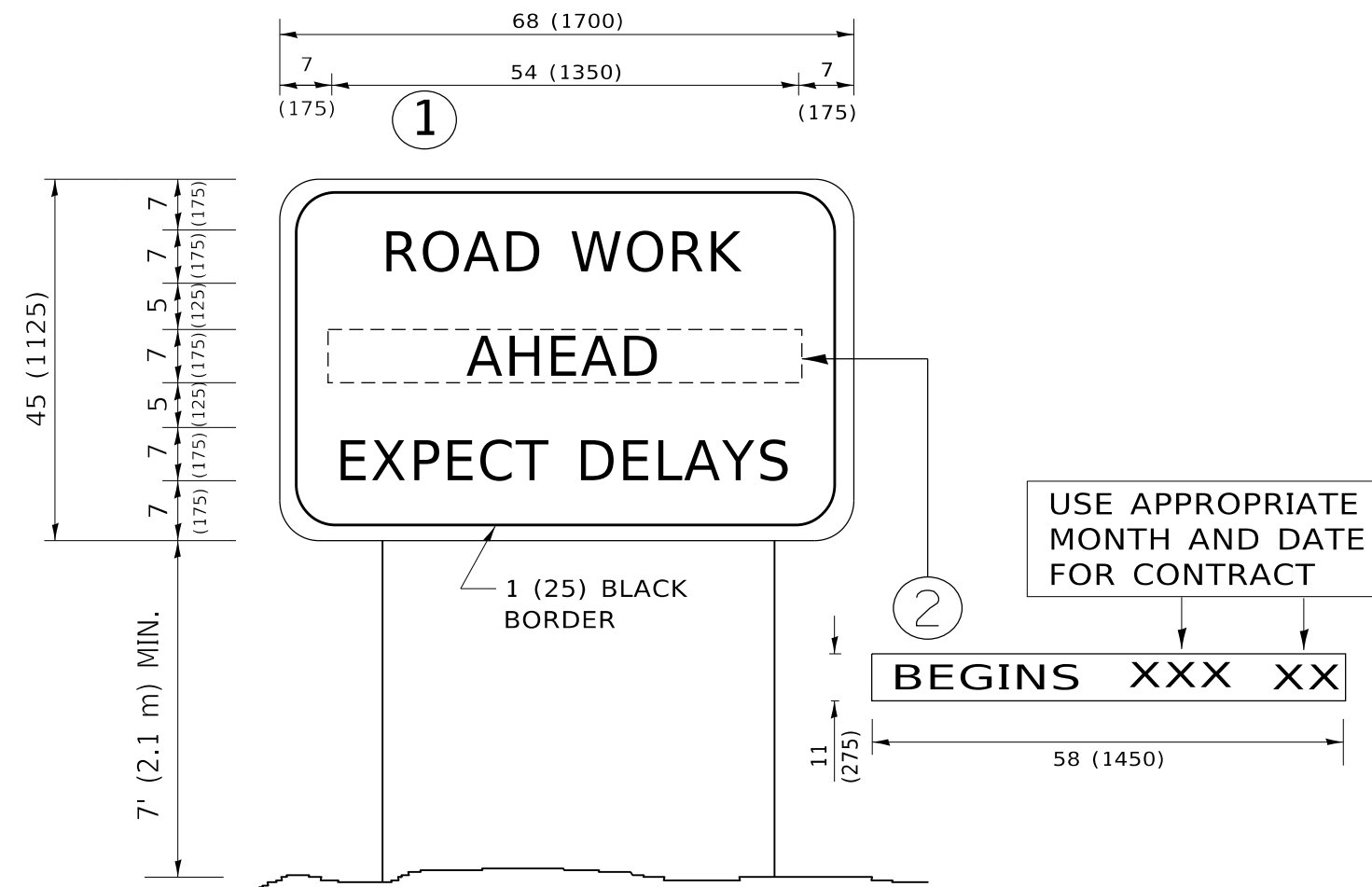
USER NAME = skinnerl	DESIGNED -	REVISED - T. RAMMACH 03-02-98
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 1/31/2020	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	29
TC-16		CONTRACT NO. 62J96		
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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USER NAME = skinnerl	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 1/31/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
345	2019-090-R5	COOK/KANE	30	30
TC-22		CONTRACT NO. 62J96		
ILLINOIS FED. AID PROJECT				