

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	1

#1  
= 69

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

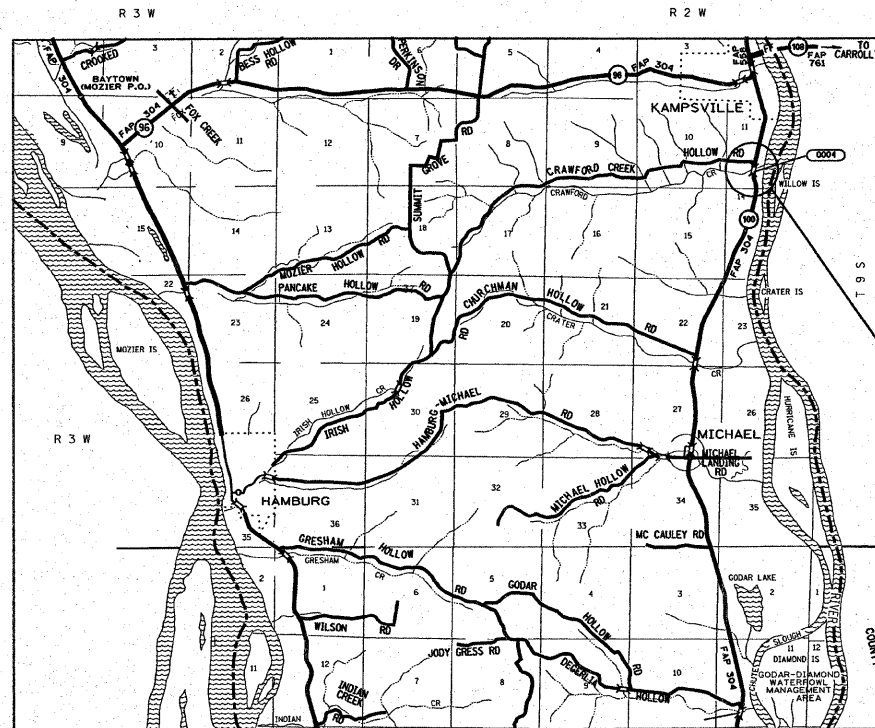
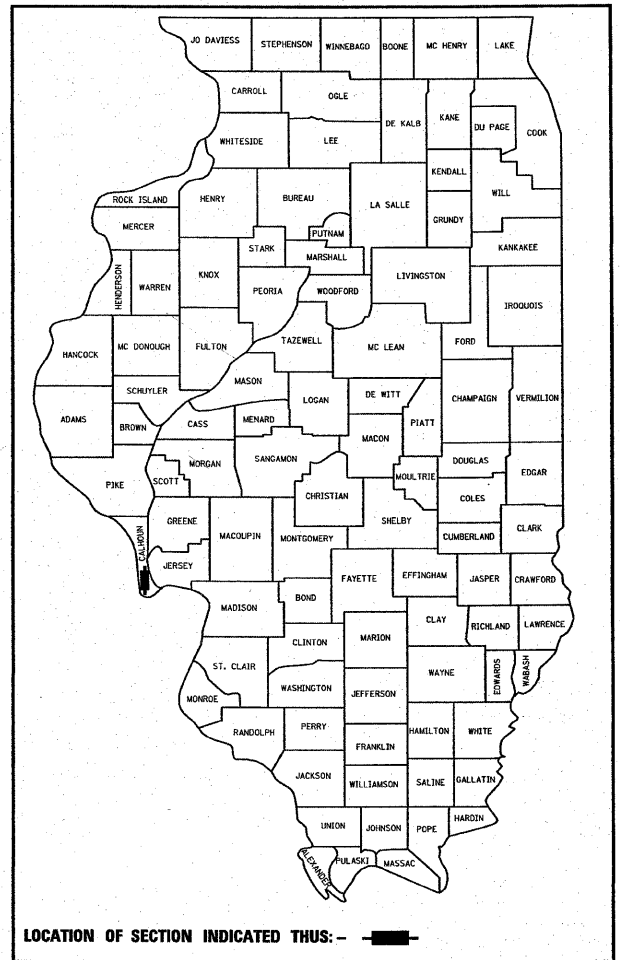
FAP ROUTE 304 (IL 100)  
SECTION 5BR-2  
**STRUCTURE REPLACEMENT OVER CRAWFORD CREEK**  
CALHOUN COUNTY

C-98-115-05

PROJECT: BRF-0304(036)

FOR INDEX OF SHEETS, SEE SHEET NO. 2

D-98-090-05



PROJECT LOCATION  
IL 100 OVER CRAWFORD CREEK  
SN 007-0004(E) 0027(P)  
STA 467+59.00

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED Jan 31, 20 08  
Mr. C. [Signature]  
DEPUTY DIRECTOR OF HIGHWAYS  
REGION FIVE ENGINEER

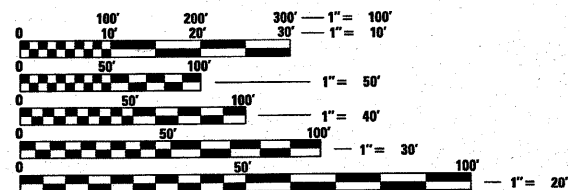
March 21, 20 08  
Eric E. [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

March 21, 20 08  
Christine M. [Signature]  
DIRECTOR, DIVISION OF HIGHWAYS

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: PATTI LEBEAU (618) 346-3179  
SQUAD CONTACT: ART MUEHLFELD (618) 346-3209

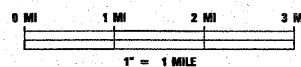
MICROFILMED \_\_\_\_\_  
REEL NUMBER \_\_\_\_\_  
AWARDED \_\_\_\_\_  
RESIDENT ENGINEER \_\_\_\_\_  
AS BUILT CHANGES WERE MADE  
ON THE FOLLOWING SHEETS



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

TRAFFIC DATA:  
ADT: 1450 (2008)  
1650 (2028)  
SU: 2.9%  
MU: 4.3%



LATITUDE: 39.2813 LONGITUDE: 90.6098

GROSS LENGTH (ABUT. TO ABUT.) = 116.03 FT

CONTRACT NO. 76948



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	SBR-2	CALHOUN	68	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

# SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A FED 80% STATE 20%	X071 -2A FED 80% STATE 20%
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	99	99	
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	824	824	
20200100	EARTH EXCAVATION	CU YD	200	200	
20200500	EARTH EXCAVATION (WIDENING)	CU YD	80	80	
20700400	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	122		122
28100107	STONE RIPRAP, CLASS A4	SQ YD	3742	3742	
28100109	STONE RIPRAP, CLASS A5	SQ YD	1650		1650
28200200	FILTER FABRIC	SQ YD	2818	1168	1650
35600712	HOT-MIX ASPHALT BASE COURSE WIDENING, 9"	SQ YD	306	306	
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.5	0.5	
40600300	AGGREGATE (PRIME COAT)	TON	2.1	2.1	
40600990	TEMPORARY RAMP	SQ YD	60	60	
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	370	370	
40603315	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N70	TON	118	118	
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	11	11	
42001165	BRIDGE APPROACH PAVEMENT	SQ YD	247	247	
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	50	50	
44000100	PAVEMENT REMOVAL	SQ YD	620	620	
44000198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	605	605	
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	380	380	
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	365	365	
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1		1
50105220	PIPE CULVERT REMOVAL	FOOT	70	70	
50200100	STRUCTURE EXCAVATION	CU YD	424		424
50300100	FLOOR DRAINS	EACH	7		7
50300225	CONCRETE STRUCTURES	CU YD	167.7		167.7
50300255	CONCRETE SUPERSTRUCTURE	CU YD	155.4		155.4
50300260	BRIDGE DECK GROOVING	SQ YD	402		402
50300280	CONCRETE ENCASMENT	CU YD	10.8		10.8
50300300	PROTECTIVE COAT	SQ YD	576		576
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1		1
50500505	STUD SHEAR CONNECTORS	EACH	2376		2376
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	52010		52010
50800515	BAR SPLICERS	EACH	513		513

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000-2A FED 80% STATE 20%	X071 -2A FED 80% STATE 20%
51201600	FURNISHING STEEL PILES HP12X53	FOOT	575		575
51201900	FURNISHING STEEL PILES HP14X89	FOOT	615		615
51202305	DRIVING PILES	FOOT	1190		1190
51203600	TEST PILE STEEL HP12X53	EACH	2		2
51203900	TEST PILE STEEL HP14X89	EACH	2		2
51205200	TEMPORARY SHEET PILING	SQ FT	673		673
51500100	NAME PLATES	EACH	1		1
52100520	ANCHOR BOLTS, 1"	EACH	48		48
54213453	END SECTIONS 18"	EACH	2	2	
542D1063	PIPE CULVERTS, CLASS D, TYPE 2 18"	FOOT	72.5	72.5	
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	79		79
60109580	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	106		106
* 63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	650	650	
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4	4	
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4	4	
63200310	GUARDRAIL REMOVAL	FOOT	968.5	968.5	
* 63300205	REMOVAL AND REINSTALLATION OF EXISTING STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	601	601	
66600105	FURNISHING AND ERECTING RIGHT-OF-WAY MARKERS	EACH	16	16	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	7	7	
67100100	MOBILIZATION	L SUM	1	1	
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1	
70101205	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)	EACH	1	1	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1		1
70106700	TEMPORARY RUMBLE STRIP	EACH	6	6	
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6	
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	4442	4442	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1620	1620	
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	72	72	
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1625	1625	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	870	870	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	750	750	
* 72000100	SIGN PANEL - TYPE 1	SQ FT	10	10	
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	24	24	
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3574	3574	

\* SPECIALTY ITEMS

# SUMMARY OF QUANTITIES

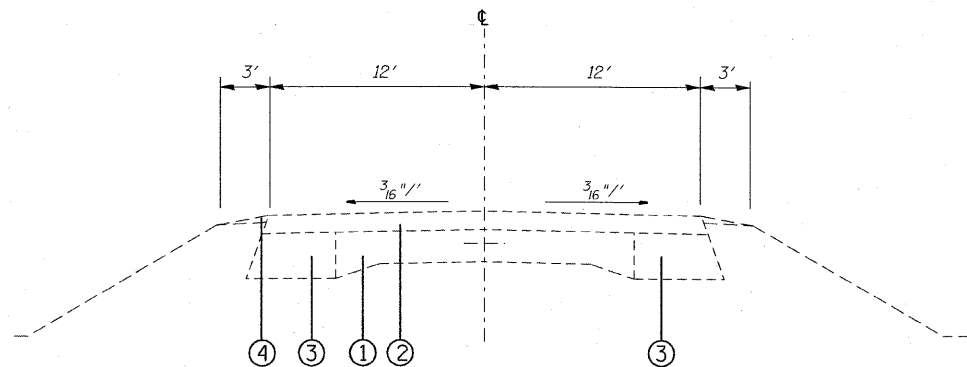
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	4
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE	
CODE NO	ITEM	UNIT		I000-2A FED 80% STATE 20%	X071 -2A FED 80% STATE 20%
* 7800650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	20	20	
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	705		705
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	12	12	
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	2		2
* 78200200	BIDIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	4	4	
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	12	12	
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4	
78300100	PAVEMENT MARKING REMOVAL	SQ FT	840	840	
X5020501	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 1	EACH	1		1
X5020502	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 2	EACH	1		1
X7200200	WIDE LOAD SIGNING	L SUM	1	1	
Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	

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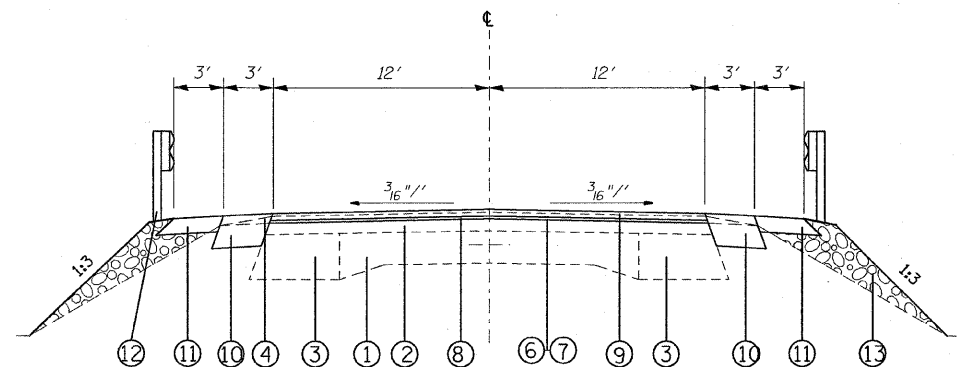
\*SPECIALTY ITEMS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



**EXISTING TYPICAL SECTION**

STA. 463+70.00 TO STA. 471+65.00



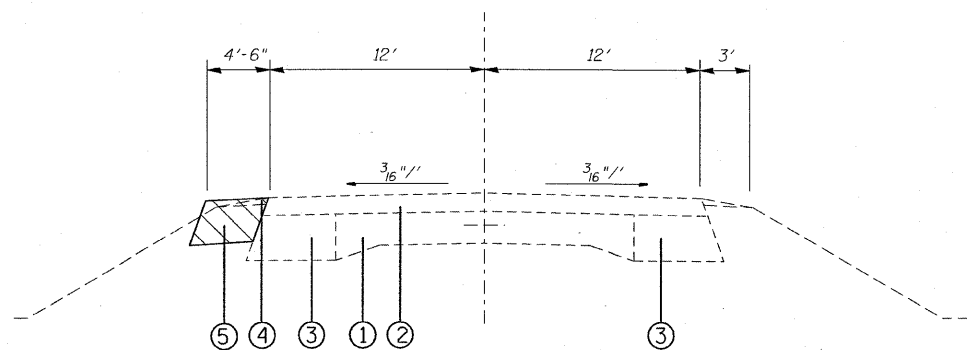
**PROPOSED TYPICAL SECTION**

STA. 464+15.00 TO STA. 470+15.00

**STONE RIPRAP:**

STA. 463+58.00 TO STA. 471+13.00 (LT)

STA. 464+15.00 TO STA. 471+05.00 (RT)



**PROPOSED TYPICAL SECTION**

STA. 463+70.00 TO STA. 471+21.00 - LT

PAVEMENT REMOVAL

**LEGEND**

- ① EXISTING P.C.C. PAVEMENT 9-7-9
- ② EXISTING BITUMINOUS OVERLAY 6" (±)
- ③ EXISTING BASE COURSE WIDENING 8"
- ④ EXISTING AGGREGATE SHOULDERS
- ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING, 9"
- ⑥ PROPOSED BITUMINOUS MATERIALS (PRIME COAT)
- ⑦ PROPOSED AGGREGATE (PRIME COAT)
- ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE VARIABLE DEPTH (0.75" TO 14.3")
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, 1 1/2"
- ⑩ PROPOSED HOT-MIX ASPHALT SHOULDER, 8 "
- ⑪ PROPOSED AGGREGATE SHOULDER, TYPE B 6"
- ⑫ PROPOSED GUARDRAIL
- ⑬ PROPOSED STONE RIPRAP

**MIXTURE REQUIREMENTS**

MIXTURE USE	SURFACE	INCIDENTAL SURF	BINDER	WIDENING COURSE	SHOULDERS	TOP LIFT SHOULDERS
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22	PG 58-22	PG 58-22
RAP % (MAX)	10%	10%	15%	15%	30%	30%
DESIGN AIR VOIDS	4.0% @ Ndes= 70	4.0% @ Ndes=70	4.0% @ Ndes= 70	4.0% @ Ndes= 70	2.0% @ Ndes=30	**2.0% @ Ndes=30
MIX COMPOSITION						
(GRADATION MIXTURE)						
FRICITION AGG	MIXTURE "D"	MIXTURE "C"	MIXTURE "B"	MIXTURE "B"	BAM	BAM

\*\* TOP LIFT SHOULDERS - DESIGN THIS MIX AT 2.0% VOIDS AND ADD ASPHALT TO REDUCE VOIDS TO 1.5%.

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS).

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TYPICAL SECTIONS**  
**MIXTURE REQUIREMENT**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	6
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

RESURFACING SCHEDULE

STATION	RT/LT	HOT-MIX ASPHALT BS WIDENING 9" (SQ YD)	AGG. PRIME COAT (TON)	BIT. MAT'L PRIME COAT (TON)	INCIDENTAL HMA SURF. (TON)	BIT. CONC. BINDER SUPER., MIX "B" (TON)	BIT. CONC. SURF. CSE., SUPER., MIX "C", N70 (TON)	HMA SHOULDERS 8" (SQ YD)	AGG. SHLD TYPE B 6" (SQ YD)
463+70.00 TO 466+71.50	LT	150.75							
464+15.00 TO 466+71.39	RT/LT		1.03	0.21			57.43	170.93	170.93
464+15.00 TO 465+04.86	RT/LT					10.06			
465+04.86 TO 466+71.39	RT/LT					156.67			
468+11.50 TO 471+21.00	LT	154.75							103.17
468+42.80 TO 471+19.00	LT							92.07	
468+47.42 TO 471+15.00	RT/LT		1.07	0.22			59.94		
468+47.42 TO 470+11.81	RT/LT					191.48			
470+11.81 TO 471+15.00	RT/LT					11.56			
468+47.42 TO 471+65.00	RT								105.86
468+59.78 TO 471+65.00	RT							101.74	
471+44.97 SIDE RD	LT				10.33				
TOTAL		305.50	2.10	0.43	10.33	369.77	117.37	364.74	379.96

RIPRAP SCHEDULE

LOCATION					RIPRAP A4 (SQ YD)	RIPRAP A5 (SQ YD)	FILTER FABRIC (SQ YD)
STA	463+58.00	TO	STA	466+81.00	LT	1081.19	191.88
STA	465+15.00	TO	STA	466+99.00	RT	623.42	204.24
BRIDGE						1650	
STA	468+19.00	TO	STA	471+25.00	LT	1264.01	695.42
STA	468+38.90	TO	STA	471+05.00	RT	772.88	75.94
TOTAL					3741.51	1650.00	1167.47

TEMPORARY PAVEMENT MARKING SCHEDULE

STATION	RT/LT	STOP BARS	PAVEMENT MARKING			WORK ZONE PVMT REMOVAL (SQ FT)	PVMT MRKG REMOVAL (SQ FT)
			LINE 4" PAVEMENT (FT)	LINE 6" (TEMP. BARR.) (FT)	LINE 24" PAVEMENT (FT)		
462+30.00	RT	STOP BARS			12	24.0	
462+30.00 TO 473+48.00		STAGE 1	2236.00			745.3	
462+30.00 TO 473+50.00	RT/LT						746.7
462+30.00 TO 473+50.00	CL						93.3
463+30.00 TO 472+00.00		STAGE 1		870			
463+57.50 TO 471+17.50		STAGE 2		750			
473+48.00	LT	STOP BARS			12.0	24.0	
462+47.00	RT	STOP BARS			12	24.0	
462+47.00 TO 473+50.00		STAGE 2	2206.00			735.3	
473+50.00	LT	STOP BARS			12.0	24.0	
471+44.97	SIDE ROAD	LT	STOP BARS		24.0	48.0	
SUB-TOTAL							
TOTAL			4442.0	1620.0	72.0	1624.7	840.0

GUARDRAIL SCHEDULE

STATION	RT/LT	SPBGR (FT)	TBT - T1 (SPECIAL) (EA)	TBT-T6 (EA)	GUARDRAIL MRKS TY-A (EA)	PRISMATIC BAR. REFLEC. (EA)
463+75.25 TO 466+94.00	LT	225.0	1	1	4	
466+94.00 TO 468+09.50	LT					2
468+09.50 TO 471+03.25	LT	200.0	1	1	4	
465+02.25 TO 467+08.50	RT	112.5	1	1	3	
467+08.50 TO 468+26.00	RT					2
468+26.00 TO 470+32.25	RT	112.5	1	1	3	
TOTAL		650.0	4	4	14	4

PAVEMENT MARKING SCHEDULE

STATION	RT/LT	PAVEMENT					BRIDGE		
		4" WHITE LINE (FT)	4" YELLOW LINE (FT)	YELLOW SKIP DASH LINE 4" (FT)	4" DBL YELLOW LINE (FT)	DOUBLE AMBER RSD REFL PMK (EA)	4" WHITE LINE (FT)	4" DBL YELLOW LINE (FT)	DOUBLE AMBER RSD REFL PMK (EA)
462+30.00 TO 466+71.39	RT/LT	882.78				6			
462+30.00 TO 465+00.00	CL		270.00	67.50					
465+00.00 TO 466+71.39	CL				342.78				
466+71.39 TO 468+47.42	RT/LT					352.06	352.06	2	
468+47.42 TO 473+50.00	RT/LT	1005.16							
468+47.42 TO 473+50.00	CL				1005.16	6			
SUB-TOTAL		1887.94	270.00	67.50	1347.94	12	352.06	352.06	2
TOTAL			3573.38			12	704.12	2	

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION SCHEDULE OF QUANTITIES
NAME	DATE	
		FAP ROUTE 304 SECTION 5BR-2 CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1832	5BR-2	CALHOUN	68	7
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

TREE REMOVAL SCHEDULE

STATION	OFFSET	RT/LT	6 TO 15 UNITS					OVER 15 UNITS							
465+23.24	22.3	LT										38			
465+43.75	43.6	RT			15										
465+48.14	23.6	LT										38			
465+58.37	41.2	RT			15										
465+85.23	26.5	LT									36				
466+42.07	49.0	RT		12											
466+56.60	46.2	RT											48		
466+79.85	40.6	RT											48		
467+03.00	45.3	RT												60	
467+04.57	39.3	LT											48		
467+20.40	43.1	RT										40			
467+21.94	37.4	RT					30								
468+02.03	28.4	LT											48		
468+77.21	44.4	RT											48		
469+21.33	41.5	RT					24								
469+27.01	42.1	RT					24								
469+36.35	46.2	RT						30							
469+44.92	46.2	RT		12											
469+60.89	45.0	RT		10											
469+64.99	45.3	RT					24								
469+70.25	44.6	RT					24								
469+77.47	44.4	RT						30							
469+86.29	44.3	RT							32						
469+94.04	46.2	RT			15										
470+04.75	43.0	RT					24								
470+08.14	43.3	RT						30							
470+13.43	42.6	RT					20								
470+20.74	43.1	RT					20								
470+26.27	42.8	RT		8											
470+75.46	40.3	RT					20								
470+79.62	40.9	RT			12										
470+94.52	42.7	RT										40			
SUB-TOTAL			8	10	36	45	60	120	120	32	36	76	80	240	60
TOTAL			99					824							

EARTHWORK SCHEDULE

LOCATION	EARTH EXCAVATION (CU YD)	EARTH EXCAVATION ADJTD FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)
STA. 469+50.00 TO STA. 471+15.00	196.8	147.6	0.0	147.6
TOTAL	196.6	147.6	0.0	147.6

EARTHWORK SCHEDULE (WIDENING)

LOCATION	EARTH EXCAVATION (CU YD)	EARTH EXCAVATION ADJTD FOR SHRINKAGE (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)
STA. 463+70.00 TO STA. 466+71.50	37.7	28.3	0.0	28.3
STA. 468+11.50 TO STA. 471+21.00	38.7	29.0	0.0	29.0
TOTAL	76.4	57.3	0.0	57.3

REMOVAL SCHEDULE

LOCATION					PVMT REMOVAL		HMA SURF REMOVAL VAR. DEPTH (SQ YD)	SPBGR (FT)
FROM	OFFSET	TO	OFFSET	RT/LT	MAINLINE REMOVAL (SQ YD)	WIDENING REMOVAL (SQ YD)		
463+70.00		466+71.50		LT		150.75		
463+71.25		466+71.75		LT			300.50	
464+15.00		465+04.86		RT/LT			239.63	
465+02.25		467+04.25		RT			202.00	
466+71.39		467+18.86		RT/LT	146.00			
467+91.45		468+47.42		RT/LT	168.02			
468+10.50		471+11.00		LT			300.50	
468+11.50		471+21.00		LT		154.75		
468+30.50		469+96.00		RT			165.50	
470+11.81		471+15.00		RT/LT			275.17	
471+44.97		SIDE ROAD		LT			89.79	
TOTAL					314.02	305.50	604.59	968.50
					619.52			

RIGHT-OF-WAY MARKERS SCHEDULE

LOCATION			ROW MARKERS (EA)
STATION	RT/LT	OFFSET	
458+69.65	RT	128.72	1
458+70.43	RT	28.72	1
459+50.00	RT	POC	1
460+16.75	RT	129.77	1
460+17.34	LT	45.23	1
462+86.09	LT	45.39	1
462+91.47	RT	129.53	1
463+66.65	LT	67.24	1
465+50.00	RT	POC	1
467+00.00	LT	POC	1
467+92.64	RT	87.06	1
468+12.17	RT	127.14	1
469+50.00	RT	POC	1
470+83.62	LT	65.79	1
470+92.63	RT	89.32	1
471+07.27	LT	95.87	1
TOTAL			16

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SCHEDULE OF QUANTITIES**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

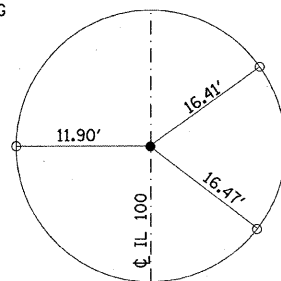
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	8
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

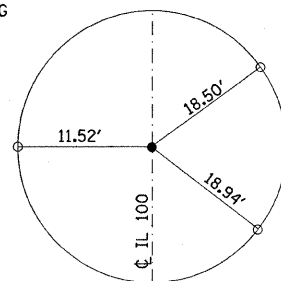


ALL TIES MAG  
NAILIN EOP



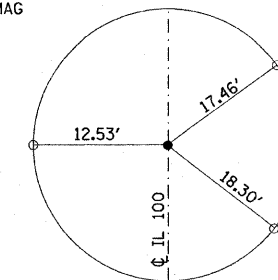
TIE POINT  
PT STA 460+04.56  
MAG NAIL (SET)

ALL TIES MAG  
NAILIN EOP



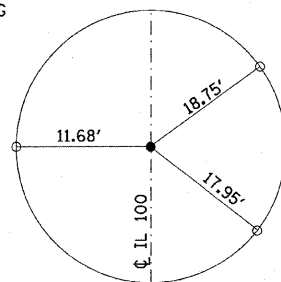
TIE POINT  
PT STA 462+20.81  
MAG NAIL (SET)

ALL TIES MAG  
NAILIN EOP



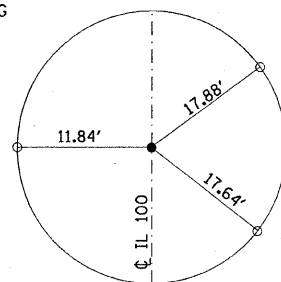
TIE POINT  
PT STA 467+16.30  
MAG NAIL (SET)

ALL TIES MAG  
NAILIN EOP



TIE POINT  
PT STA 463+25.21  
MAG NAIL (SET)

ALL TIES MAG  
NAILIN EOP



TIE POINT  
PT STA 463+66.65  
MAG NAIL (SET)

BENCHMARKS

BM 205 - RR SPIKE IN WEST SIDE OF GUY POLE ON EAST SIDE OF  
RT 100 @ STA 493+50  
ELEVATION = 435.89

BM 206 - CHISELED " " ON TOP OF THE NORTH END OF WEST  
CONCRETE BRIDGE CURB FOR RTE 100 BRIDGE (SN  
007-0004) OVER CRAWFORD CREEK  
ELEVATION = 438.26

NOTE: ALL TIES PULLED DIRECT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**TIES & BENCHMARKS**  
FAP ROUTE 304  
SECTION 5BR-2  
CALHOUN COUNTY

SCALE: VERT.  
HORIZ.  
DATE

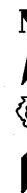
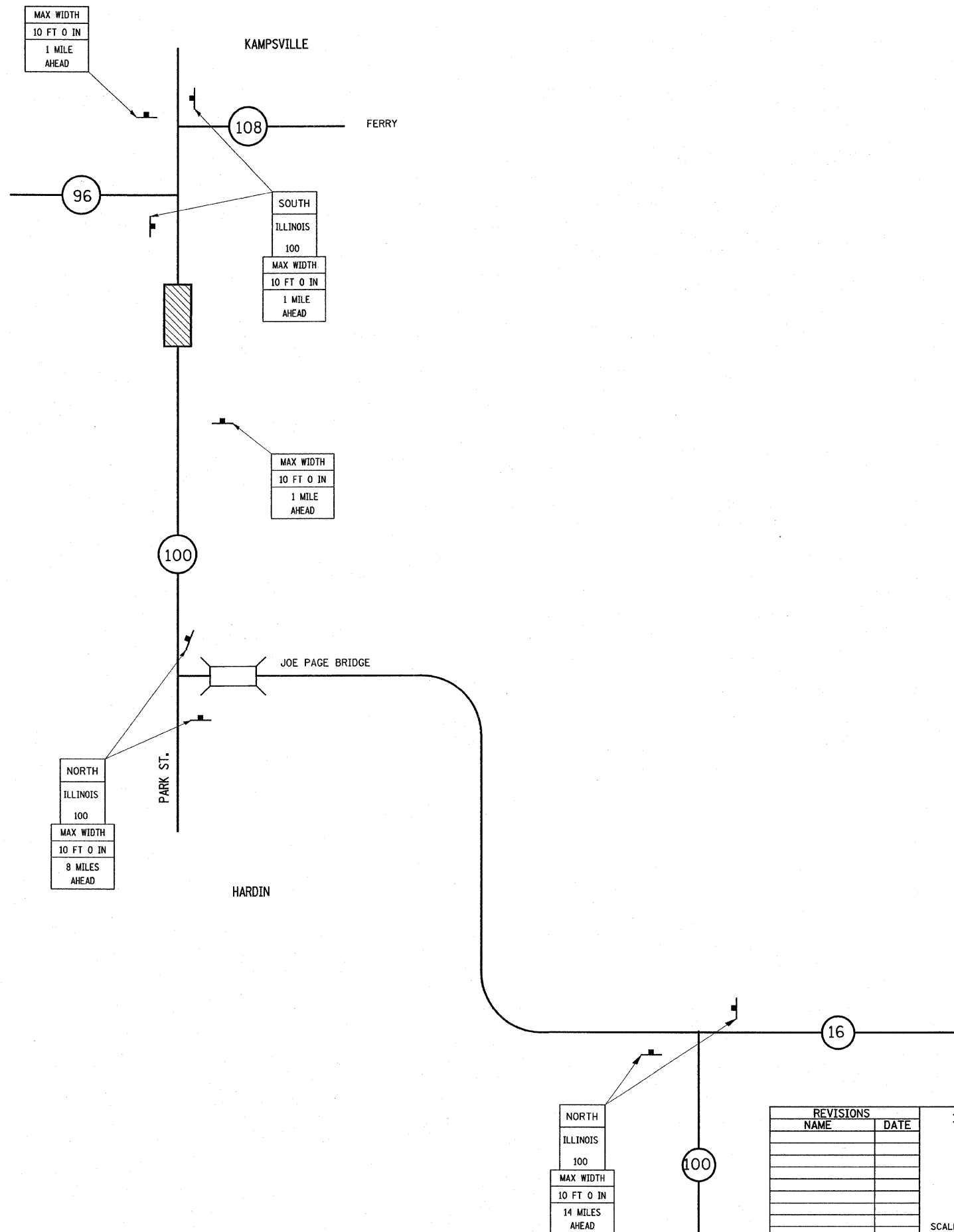
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	9
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

**NOTES**

1. ALL SIGNS REQUIRED WILL BE SUPPLIED TO THE CONTRACTOR BY I.D.O.T.
2. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THIS SHEET, AS DIRECTED BY THE RE/RT. THE POSTS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL GIVE ILLINOIS DEPARTMENT OF TRANSPORTATION, BUREAU OF OPERATIONS TWO WEEKS NOTICE FOR SIGNS. THE CONTRACTOR SHALL PICK UP THE SIGNS AT THE T.M. BUILDING IN FAIRVIEW HEIGHTS, AND RETURN THEM UPON COMPLETION OF THE CONTRACT. CONTACT JEAN SLAPE @ (618) 346-3289.
4. THE ABOVE NOTED WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE, LUMP SUM, FOR WIDE LOAD SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. SIGN SPACING WILL BE 400' OR TO FIT FIELD CONDITIONS.
6. THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT BE LESS THAN 6'.



SIGNS REQUIRED			
MAX WIDTH 10 FT 0 IN 1 MILE AHEAD	(4)	NORTH	(4)
MAX WIDTH 10 FT 0 IN 8 MILES AHEAD	(2)	SOUTH	(2)
MAX WIDTH 10 FT 0 IN 14 MILES AHEAD	(2)	ILLINOIS 100	(6)

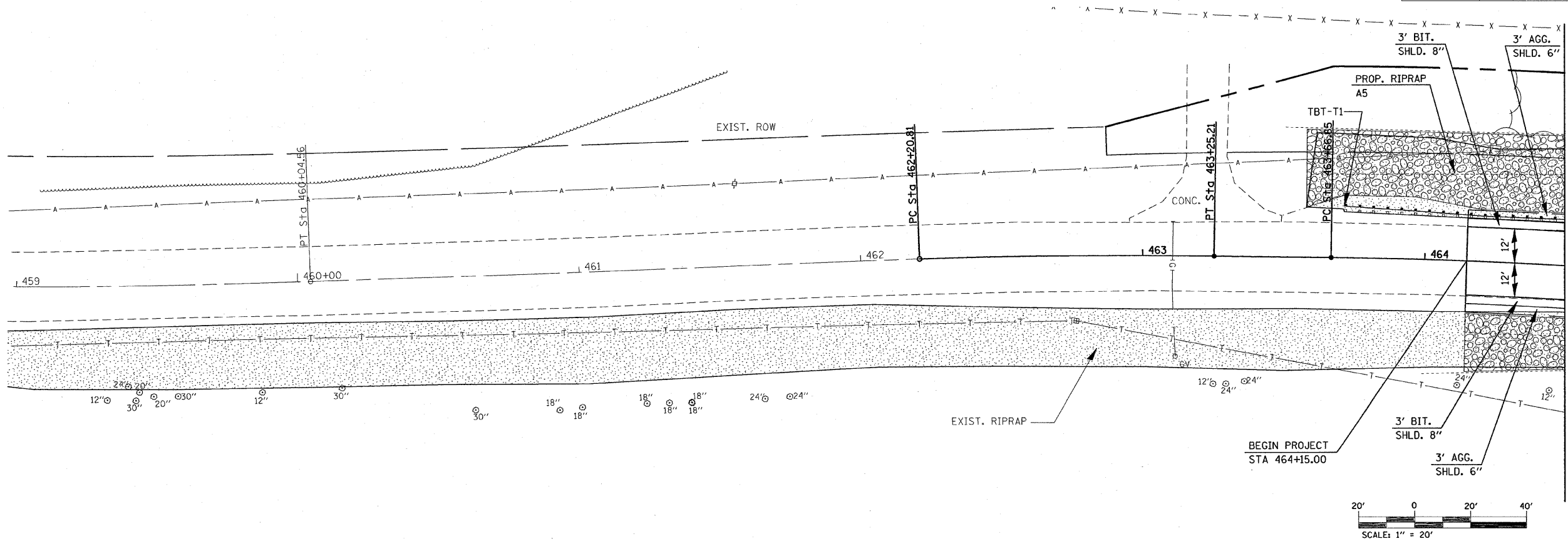
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**WIDE LOAD SIGNING**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

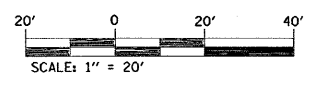
SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	10
STA. 459+00.00 TO STA. 464+50.00				
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	



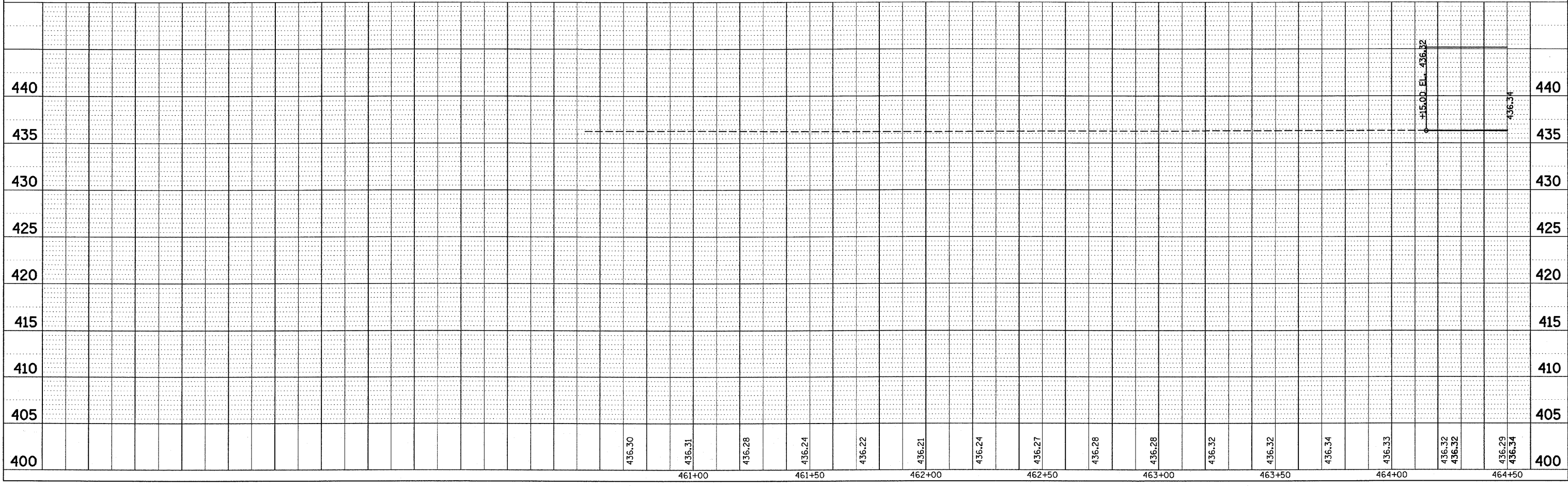
MATCHLINE STA 464+50.00



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	CHECKED		
NO.	BY		
	DATE		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	CHECKED		
NO.	BY		
	DATE		

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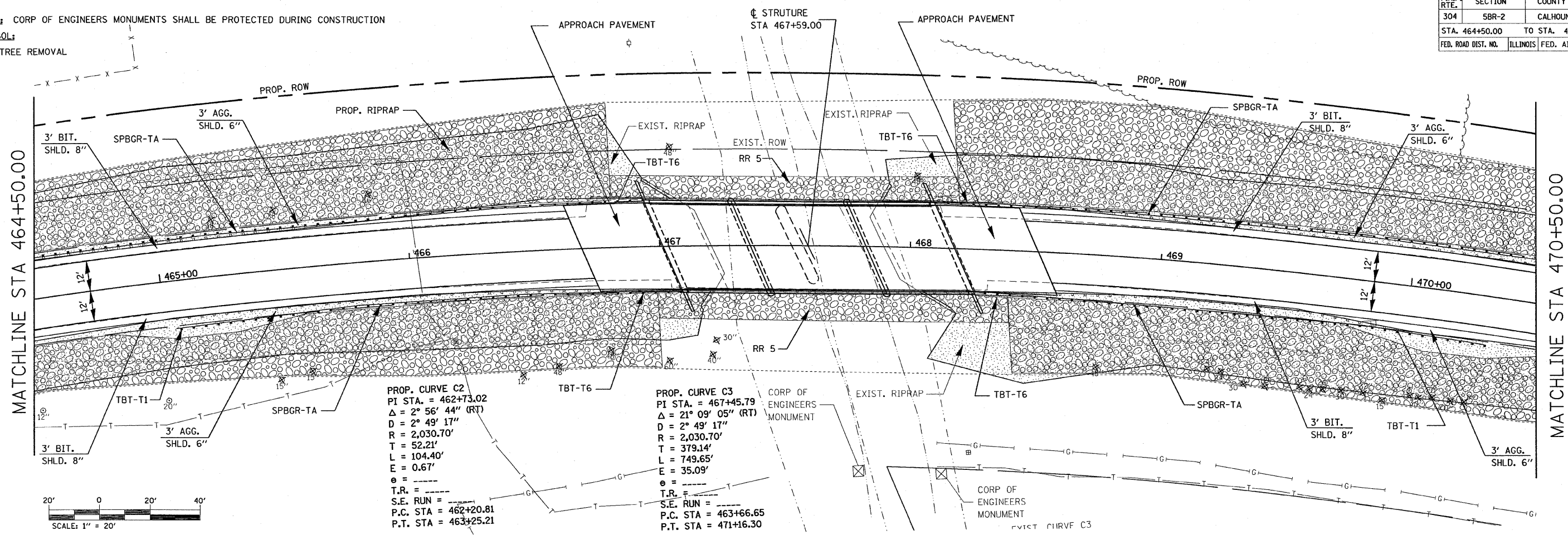


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	58R-2	CALHOUN	68	11
STA. 464+50.00		TO STA. 470+50.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

NOTE: CORP OF ENGINEERS MONUMENTS SHALL BE PROTECTED DURING CONSTRUCTION

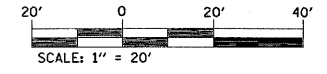
SYMBOL:

✕ TREE REMOVAL



PROP. CURVE C2  
 PI STA. = 462+73.02  
 $\Delta = 2^\circ 56' 44''$  (RT)  
 $D = 2^\circ 49' 17''$   
 $R = 2,030.70'$   
 $T = 52.21'$   
 $L = 104.40'$   
 $E = 0.67'$   
 $\theta =$   
 $T.R. =$   
 $S.E. RUN =$   
 $P.C. STA = 462+20.81$   
 $P.T. STA = 463+25.21$

PROP. CURVE C3  
 PI STA. = 467+45.79  
 $\Delta = 21^\circ 09' 05''$  (RT)  
 $D = 2^\circ 49' 17''$   
 $R = 2,030.70'$   
 $T = 379.14'$   
 $L = 749.65'$   
 $E = 35.09'$   
 $\theta =$   
 $T.R. =$   
 $S.E. RUN =$   
 $P.C. STA = 463+66.65$   
 $P.T. STA = 471+16.30$



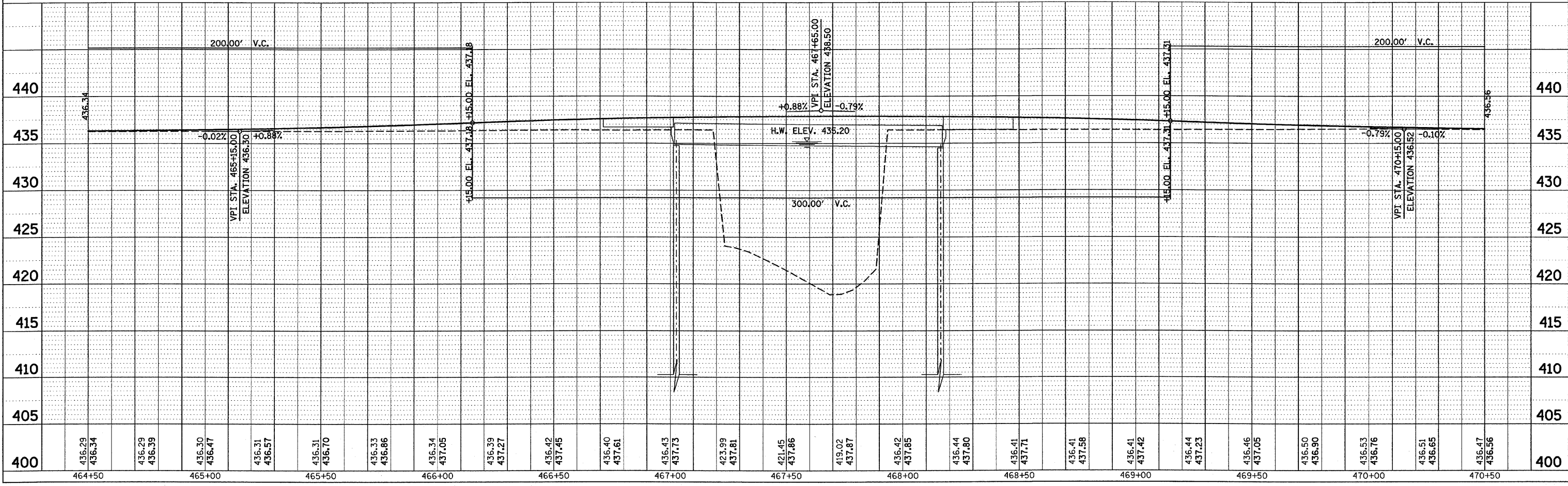
PLAN

DATE	
BY	
SURVEYED	
ALIGNED	
CHECKED	
RT. OF WAY CHECKED	
NOTE BOOK NO.	
DATE FILED	
FILE NAME	

PROFILE

DATE	
BY	
SURVEYED	
GRADES CHECKED	
STRUCTURE NOTING OK'D	
NOTE BOOK NO.	
DATE FILED	
FILE NAME	

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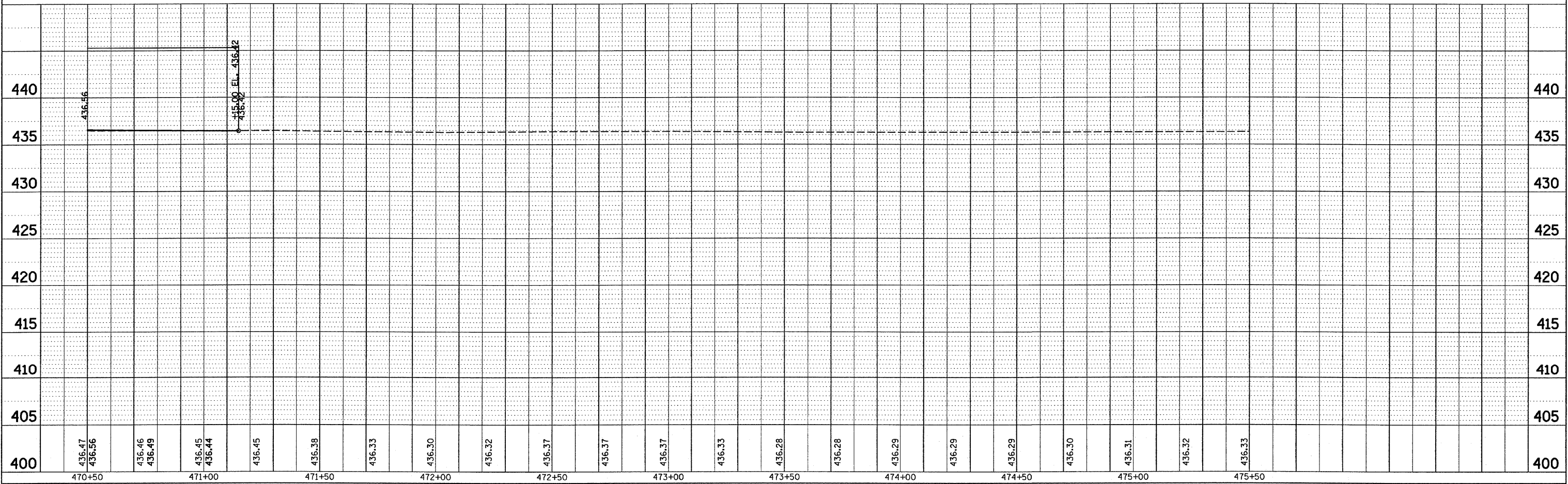
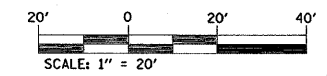
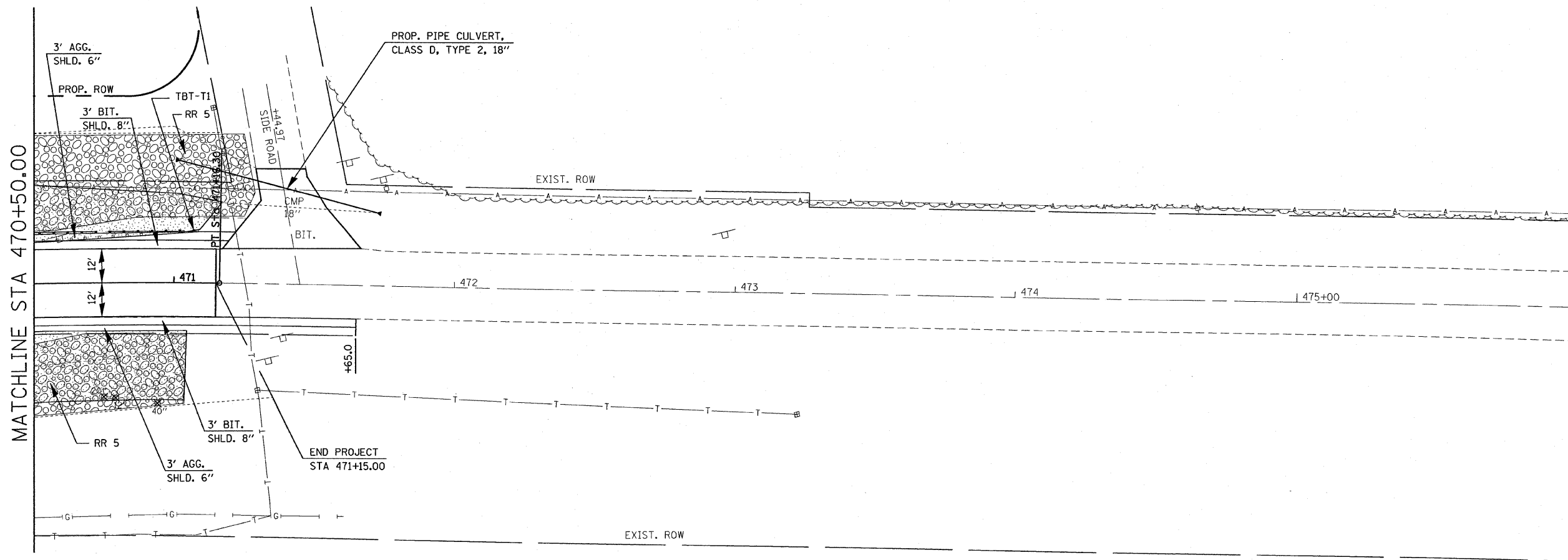


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	12
STA. 470+50.00		TO STA. 475+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		
	PROJ. FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	B.M. NOTED		
	STRUCTURE NOTATIONS CHECKED		

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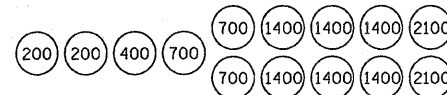
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	13
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		

PRE-STAGE I CONSTRUCTION:

- PRE-STAGE I CONSTRUCTION SHALL CONSIST OF THE RELOCATION OF THE EXISTING GUARD RAIL AND CONSTRUCTION OF THE 4 1/2' PAVEMENT WIDENING ON THE NORTHEAST AND NORTHWEST CORNERS OF THE STRUCTURE. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, STANDARD 701326.

STAGE I CONSTRUCTION:

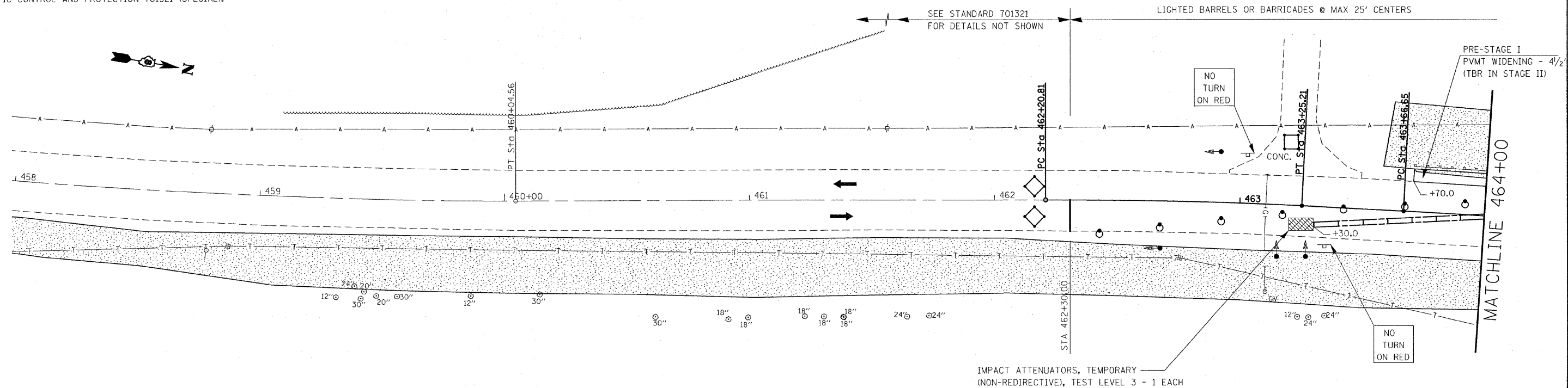
- STAGE I CONSTRUCTION SHALL CONSIST OF STAGE I REMOVAL OF THE EXISTING STRUCTURE, AND STAGE I CONSTRUCTION OF THE REPLACEMENT STRUCTURE, PAVEMENT REMOVAL, GRADING, RESURFACING THE RIGHT SIDE PAVEMENT NORTH AND SOUTH OF THE STRUCTURE, GUARDRAIL, RIPRAP, ETC. STAGE I CONSTRUCTION SHALL BE DONE ACCORDING TO STAGE CONSTRUCTION AS DETAILED IN THE BRIDGE PLANS. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STANDARD 701321 AND AS DETAILED IN THE STAGE CONSTRUCTION PLANS. THIS TRAFFIC CONTROL SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION 701321 (SPECIAL).



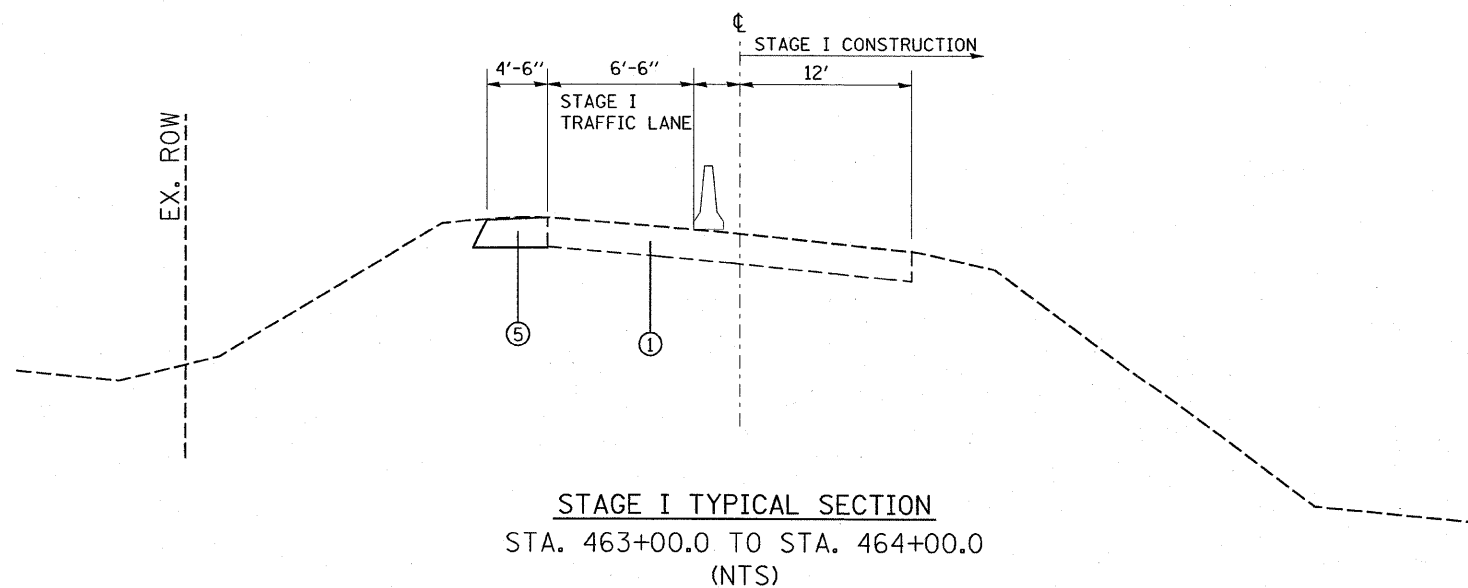
SAND MODULE IMPACT ATTENUATOR LAYOUT (IF OPTION USED)

NOTES:

- THE CONTRACTOR SHALL MAINTAIN ACCESS TO PRIVATE AND FIELD ENTRANCES LOCATED WITHIN THE LIMITS OF THE PROJECT.
- TRAFFIC CONTROL & PROTECTION, STANDARD 701321 (SPECIAL) INCLUDES BOTH STAGE I & II AND ANY ADDITIONAL SIGNING OR TRAFFIC CONTROL DEVICES SHOWN ON THE STAGE CONSTRUCTION PLANS.
- ALL ADDITIONAL TRAFFIC SIGNAL HEADS, LOOP DETECTORS AND ASSOCIATED EQUIPMENT REQUIRED TO MAINTAIN ACCESS AT THE FIELD AND DRIVEWAY ENTRANCES SHALL BE INCLUDED IN THE COST OF "TEMPORARY BRIDGE TRAFFIC SIGNALS"
- THE COST OF "BARRICADES, TYPE III" SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".
- ALL SIDEROADS AND ENTRANCES WITHIN TRAFFIC CONTROL SHALL HAVE "NO RIGHT TURN ON RED" AND "STOP HERE ON RED" SIGNS. THE COST SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".



IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3 - 1 EACH

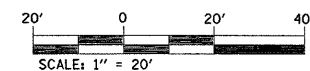


STAGE I TYPICAL SECTION  
STA. 463+00.0 TO STA. 464+00.0  
(NTS)

**LEGEND:**

	STRUCTURE REMOVAL
	PAVEMENT REMOVAL
	TEMPORARY CONCRETE BARRIER
	IMPACT ATTENUATOR
	INDUCTION LOOP DETECTOR
	DRUM WITH STEADY BURNING LIGHT
	SIGNALIZED TWO-WAY TRAFFIC LANE
	TEMPORARY BRIDGE TRAFFIC SIGNAL
	TYPE III BARRICADE

- LEGEND**
- ① EXISTING PAVEMENT
  - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE I CONSTRUCTION**  
FAP ROUTE 304  
SECTION 5BR-2  
CALHOUN COUNTY  
SCALE: VERT. HORIZ.  
DATE  
DRAWN BY  
CHECKED BY

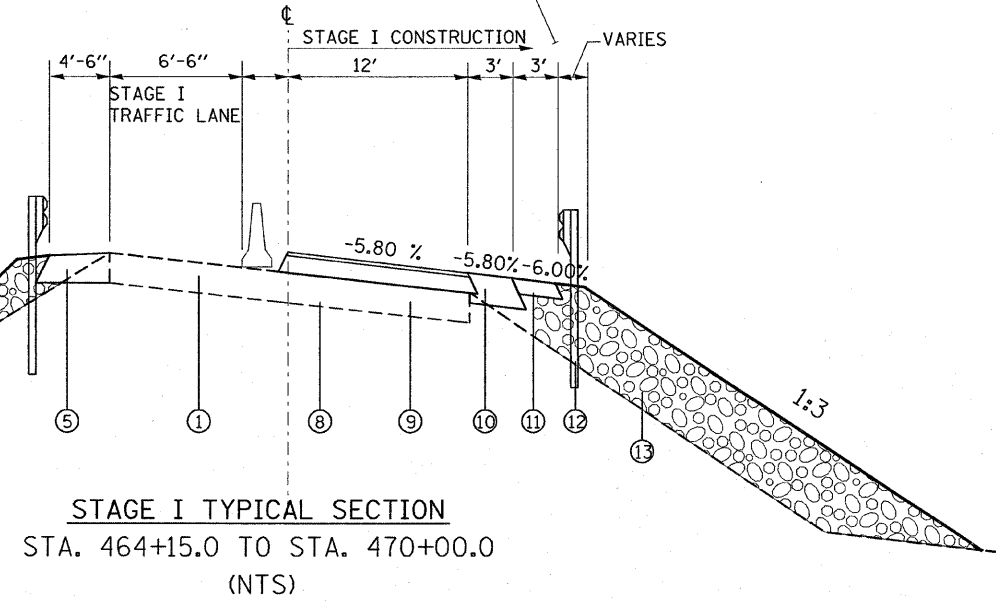
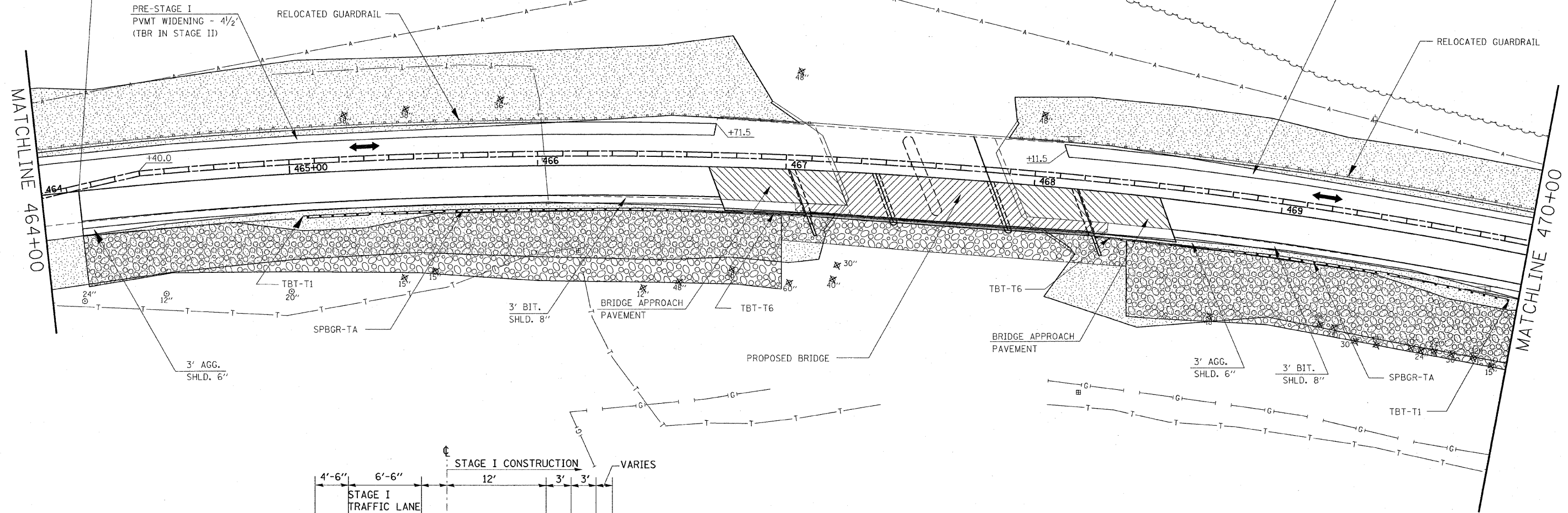
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	14
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

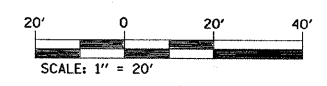
**LEGEND:**

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE

BEGIN PROJECT  
STA 464+15.00



- LEGEND**
- ① EXISTING PAVEMENT
  - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
  - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
  - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
  - ⑪ PROPOSED AGGREGATE SHOULDER 6"
  - ⑫ PROPOSED GUARDRAIL
  - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE I CONSTRUCTION**  
FAP ROUTE 304  
SECTION 5BR-2  
CALHOUN COUNTY

SCALE: VERT. HORIZ.  
DATE

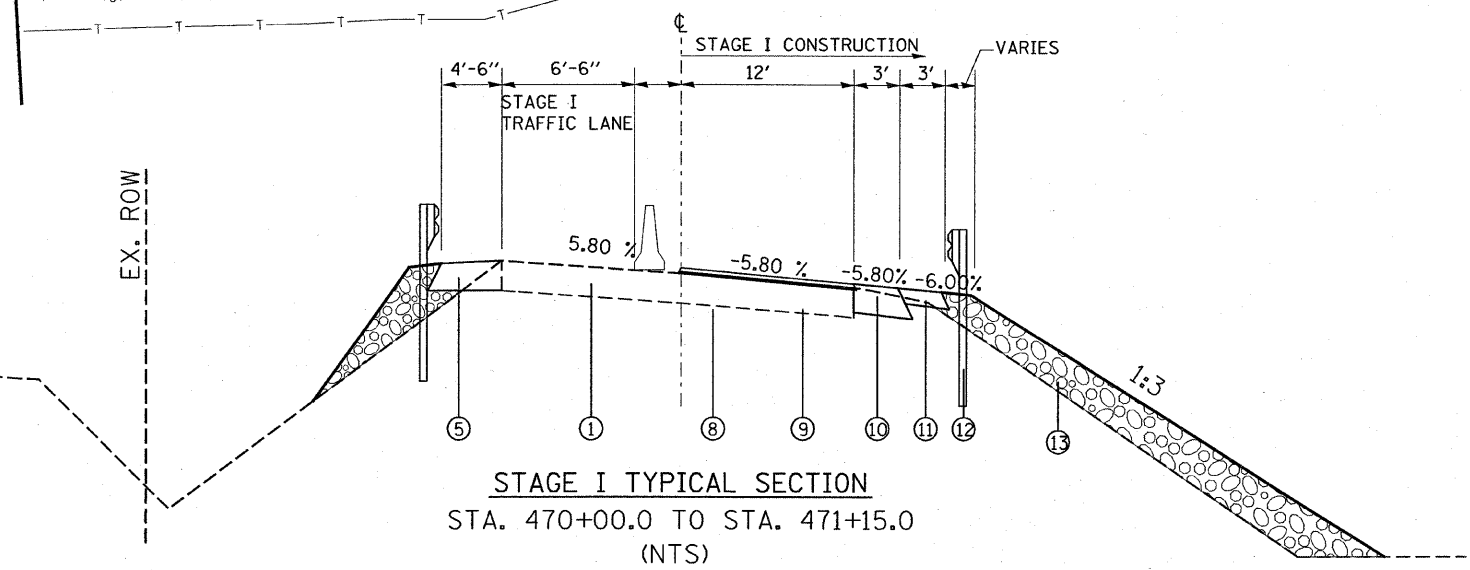
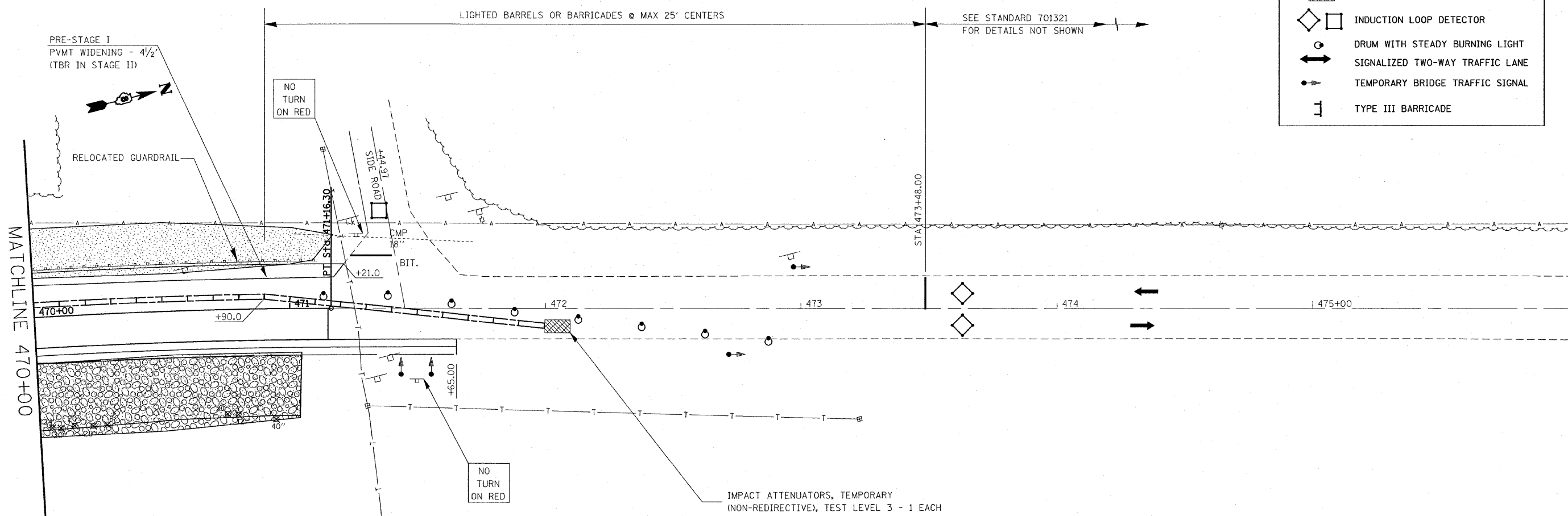
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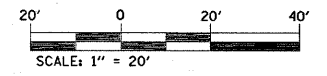
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	15
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

**LEGEND:**

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
  - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
  - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
  - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
  - ⑪ PROPOSED AGGREGATE SHOULDER 6"
  - ⑫ PROPOSED GUARDRAIL
  - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE I CONSTRUCTION**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

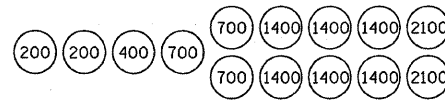
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 REFERENCE = 4REF

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	16
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

STAGE II CONSTRUCTION:

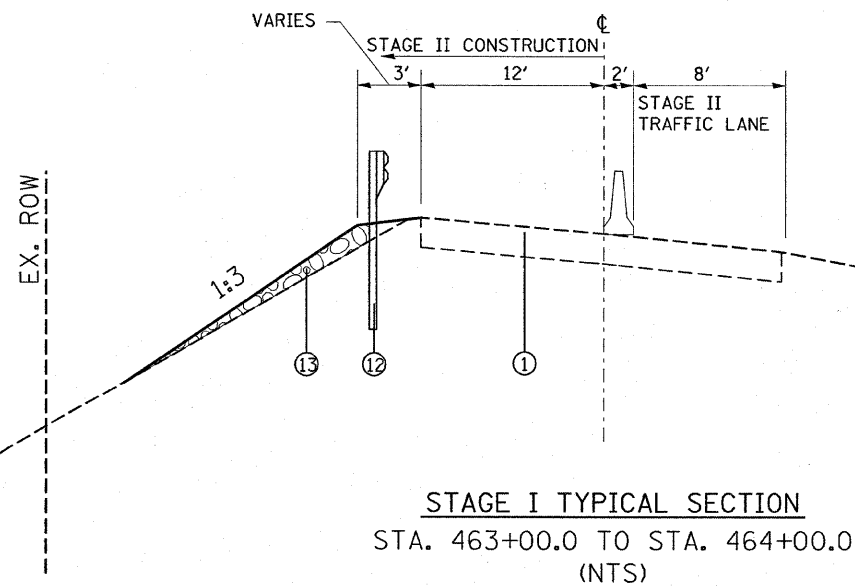
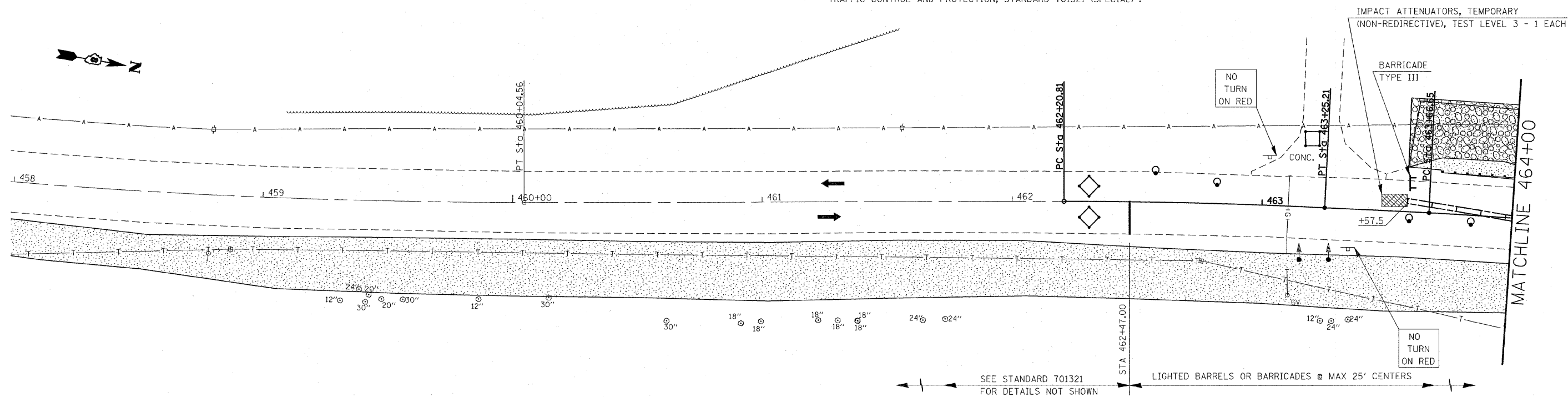
- STAGE II CONSTRUCTION SHALL CONSIST OF STAGE II REMOVAL OF THE EXISTING STRUCTURE, AND STAGE II CONSTRUCTION OF THE REPLACEMENT STRUCTURE, REMOVE PRE-STAGE I PAVEMENT WIDENING, PAVEMENT REMOVAL, GRADING, RESURFACING THE LEFT SIDE PAVEMENT NORTH AND SOUTH OF THE STRUCTURE, GUARDRAIL, RIPRAP, ETC. STAGE I CONSTRUCTION SHALL BE DONE ACCORDING TO STAGE CONSTRUCTION AS DETAILED IN THE BRIDGE PLANS. TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF STANDARD 701321 AND AS DETAILED IN THE STAGE CONSTRUCTION PLANS. THIS TRAFFIC CONTROL SHALL BE PAID FOR AS TRAFFIC CONTROL AND PROTECTION 701321 (SPECIAL).



SAND MODULE IMPACT ATTENUATOR LAYOUT (IF OPTION USED)

NOTES:

- THE CONTRACTOR SHALL MAINTAIN ACCESS TO PRIVATE AND FIELD ENTRANCES LOCATED WITHIN THE LIMITS OF THE PROJECT.
- TRAFFIC CONTROL & PROTECTION, STANDARD 701321 (SPECIAL) INCLUDES BOTH STAGE I & II AND ANY ADDITIONAL SIGNING OR TRAFFIC CONTROL DEVICES SHOWN ON THE STAGE CONSTRUCTION PLANS.
- ALL ADDITIONAL TRAFFIC SIGNAL HEADS, LOOP DETECTORS AND ASSOCIATED EQUIPMENT REQUIRED TO MAINTAIN ACCESS AT THE FIELD AND DRIVEWAY ENTRANCES SHALL BE INCLUDED IN THE COST OF "TEMPORARY BRIDGE TRAFFIC SIGNALS"
- THE COST OF "BARRICADES, TYPE III" SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".
- ALL SIDEROADS AND ENTRANCES WITHIN TRAFFIC CONTROL SHALL HAVE "NO RIGHT TURN ON RED" AND "STOP HERE ON RED" SIGNS. THE COST SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 (SPECIAL)".



STAGE I TYPICAL SECTION  
STA. 463+00.0 TO STA. 464+00.0  
(NTS)

- LEGEND
- ① EXISTING PAVEMENT
  - ⑫ PROPOSED GUARDRAIL
  - ⑬ PROPOSED RIPRAP

- LEGEND:
- [Hatched Box] STRUCTURE REMOVAL
  - [Dotted Box] PAVEMENT REMOVAL
  - [Dashed Line] TEMPORARY CONCRETE BARRIER
  - [Cross-hatched Box] IMPACT ATTENUATOR
  - [Diamond] INDUCTION LOOP DETECTOR
  - [Circle with Light] DRUM WITH STEADY BURNING LIGHT
  - [Arrow] SIGNALIZED TWO-WAY TRAFFIC LANE
  - [Triangle] TEMPORARY BRIDGE TRAFFIC SIGNAL
  - [T-shape] TYPE III BARRICADE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE II CONSTRUCTION**  
FAP ROUTE 304  
SECTION 5BR-2  
CALHOUN COUNTY

SCALE: VERT. / HORIZ.  
DATE / DRAWN BY / CHECKED BY



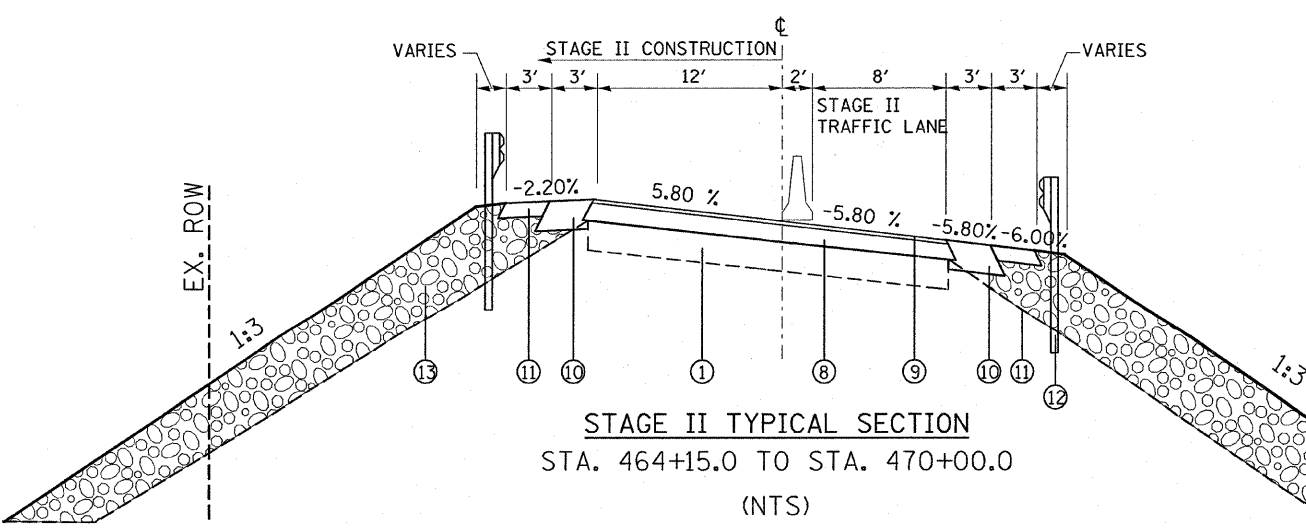
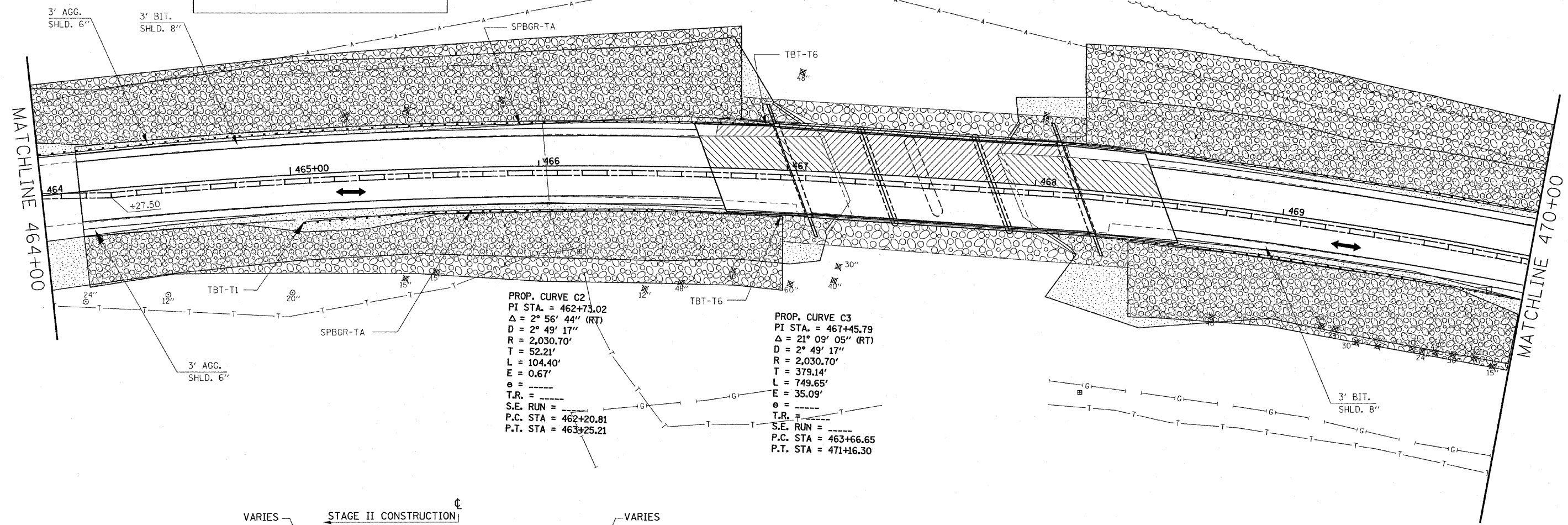
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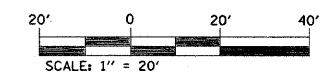
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	17
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

**LEGEND:**

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
  - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
  - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
  - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
  - ⑪ PROPOSED AGGREGATE SHOULDER 6"
  - ⑫ PROPOSED GUARDRAIL
  - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE II CONSTRUCTION**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

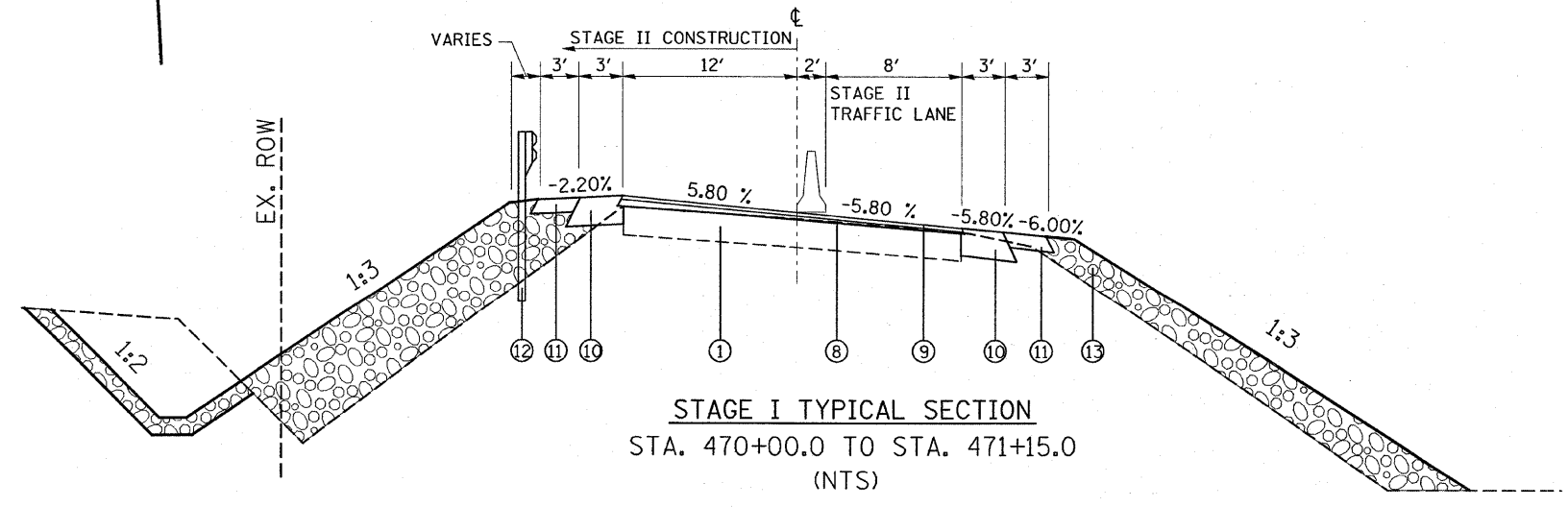
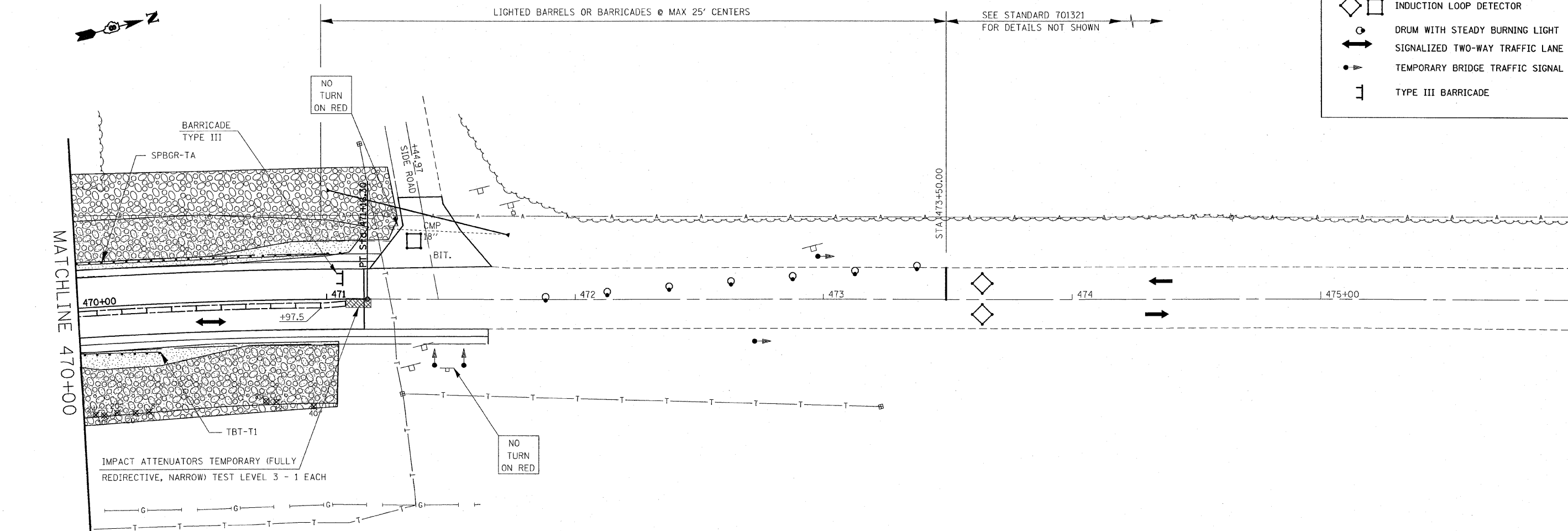
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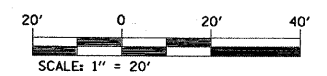
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	18
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

**LEGEND:**

- STRUCTURE REMOVAL
- PAVEMENT REMOVAL
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR
- INDUCTION LOOP DETECTOR
- DRUM WITH STEADY BURNING LIGHT
- SIGNALIZED TWO-WAY TRAFFIC LANE
- TEMPORARY BRIDGE TRAFFIC SIGNAL
- TYPE III BARRICADE



- LEGEND**
- ① EXISTING PAVEMENT
  - ⑤ PROPOSED HOT-MIX ASPHALT BASE COURSE WIDENING 9"
  - ⑧ PROPOSED HOT-MIX ASPHALT BINDER COURSE (VARIES 0.75" TO 15")
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE COURSE 1 1/2"
  - ⑩ PROPOSED HOT-MIX ASPHALT SHOULDERS, 8"
  - ⑪ PROPOSED AGGREGATE SHOULDER 6"
  - ⑫ PROPOSED GUARDRAIL
  - ⑬ PROPOSED RIPRAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SUGGESTED STAGE II CONSTRUCTION**  
FAP ROUTE 304  
SECTION 5BR-2  
CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

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REFERENCE = 4/12/08

PART OF THE S.W. 1/4 OF SECTION 11 T. 9 S., R. 2 W., OF THE 4RD PM, CALHOUN COUNTY, ILLINOIS

BEARINGS SHOWN HEREON ARE BASED ON SURVEY CONTROL DATA AS PROVIDED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

EXISTING CENTERLINE OF ROW COORDINATE TABLE				
STATION	OFFSET	NORTH	EAST	
STA. 460+17.19 (PT STA. 461+20.00)	0.23' LT (EXIST. $\frac{1}{4}$ ROW)	951713.1924	171346.6443	
STA. 462+87.38 (PC STA. 463+90.20)	0.41' LT (EXIST. $\frac{1}{4}$ ROW)	951979.3722	171300.2287	
STA. 466+90.56 (PI STA. 467+99.80)	46.09' LT (N/A)			
STA. 470+93.70 (PT STA. 471+97.48)	0.67' LT (EXIST. $\frac{1}{4}$ ROW)	952780.2911	171329.1183	

COORDINATE TABLE				
STATION	OFFSET	NORTH	EAST	
STA. 458+69.65	128.72' RT	951582.8370	171496.7814	
STA. 458+70.43	28.72' RT	951571.6212	171397.4121	
STA. 460+16.75	129.77' RT	951735.5244	171474.7118	
STA. 460+17.34	45.23' LT	951705.4621	171302.3133	
STA. 462+86.09	45.39' LT	951971.6419	171255.8976	
STA. 462+91.47	129.53' RT	952001.7042	171428.2961	
STA. 463+66.65	67.24' LT	952049.6787	171223.7128	
STA. 465+03.42	67.97' LT	952190.3285	171210.1602	
STA. 467+44.53	68.10' LT	952439.3686	171210.5626	
STA. 467+73.04	68.01' LT	952468.7590	171212.6621	
STA. 467+92.64	87.06' RT	952475.7552	171368.7955	
STA. 468+12.17	127.14' LT	952490.5822	171410.3746	
STA. 470+83.62	65.79' LT	952785.2310	171263.3874	
STA. 470+92.63	89.32' RT	952758.4828	171416.4361	
STA. 471+07.27	95.87' RT	952816.1327	171239.8338	
STA. 482+55.11	30.00' LT			

CONSTRUCTION CENTERLINE COORDINATE TABLE				
STATION	OFFSET	NORTH	EAST	
PC STA. 455+00.00	CONST. $\frac{1}{4}$			
PI STA. 457+53.20	N/A			
PT STA. 460+04.56	CONST. $\frac{1}{4}$			
PC STA. 462+20.81	CONST. $\frac{1}{4}$	951913.7095	171311.2132	
PI STA. 462+73.02	N/A			
PT STA. 463+25.21	CONST. $\frac{1}{4}$	952016.9224	171295.5825	
PC STA. 463+66.65	CONST. $\frac{1}{4}$	952059.0350	171290.4335	
PI STA. 467+45.79	N/A			
PT STA. 471+16.30	CONST. $\frac{1}{4}$	952802.0976	171335.1211	
POT STA. 480+00.00	CONST. $\frac{1}{4}$	953659.5010	171549.0970	

SECTION CORNER COORDINATE TABLE			
CORNER DESCRIPTION	NORTH	EAST	
N.W. COR. E. 1/2 S.W. 1/4 SEC. 11	953807.7731	170864.6536	

**PROP. CURVE C1**  
 PI STA. = 457+53.20  
 $\Delta = 11^\circ 55' 28''$  (LT)  
 $D = 2^\circ 21' 48''$   
 $R = 2,424.37'$   
 $T = 253.20'$   
 $L = 504.56'$   
 $E = 13.19'$   
 P.C. STA. = 455+00.00  
 P.T. STA. = 460+04.56

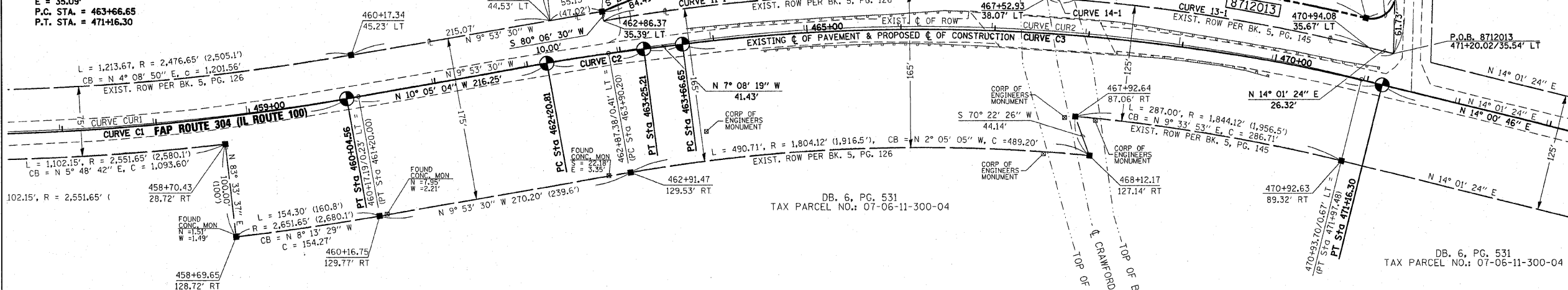
**EXIST. CURVE CUR1**  
 (PI STA. = 455+14.81)  
 $\Delta = 28^\circ 04' 39''$  (LT) ( $28^\circ 04'$ )  
 $D = 2^\circ 16' 20''$  ( $2^\circ 15'$ )  
 $R = 2,521.65'$  (2,550.1')  
 $T = 630.53'$  (636.5')  
 $L = 1,235.72'$  (1,247.4')  
 $E = 77.64'$  (78.3')  
 (P.C. STA. = 448+84.28)  
 (P.T. STA. = 461+20.00)

**PROP. CURVE C2**  
 PI STA. = 462+73.02  
 $\Delta = 2^\circ 56' 44''$  (RT)  
 $D = 2^\circ 49' 17''$   
 $R = 2,030.70'$   
 $T = 52.21'$   
 $L = 104.40'$   
 $E = 0.67'$   
 P.C. STA. = 462+20.81  
 P.T. STA. = 463+25.21

**EXIST. CURVE CUR2**  
 (PI STA. = 467+99.80)  
 $\Delta = 23^\circ 54' 53''$  (RT) ( $23^\circ 50'$ )  
 $D = 2^\circ 57' 45''$  ( $2^\circ 48'$ )  
 $R = 1,934.12'$  (2,046.5')  
 $T = 409.61'$  (431.9')  
 $L = 807.29'$  (851.2')  
 $E = 42.90'$  (45.0')  
 (P.C. STA. = 463+90.20)  
 (P.T. STA. = 471+97.48)

**PROP. CURVE C3**  
 PI STA. = 467+45.79  
 $\Delta = 21^\circ 09' 05''$  (RT)  
 $D = 2^\circ 49' 17''$   
 $R = 2,030.70'$   
 $T = 379.14'$   
 $L = 749.65'$   
 $E = 35.09'$   
 P.C. STA. = 463+66.65  
 P.T. STA. = 471+16.30

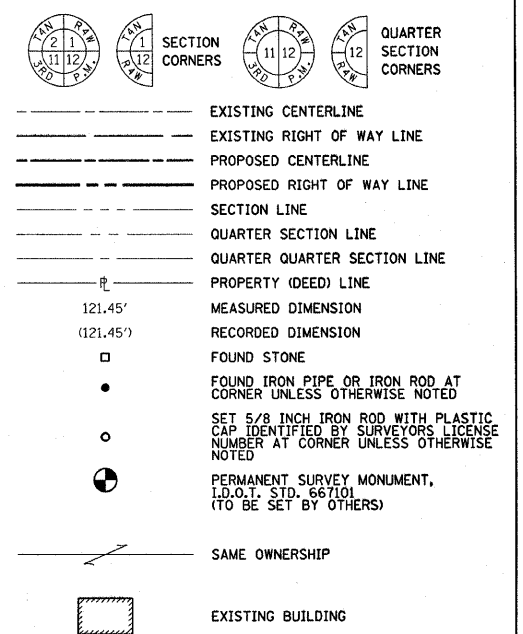
SW 1/4  
 SEC 11-9S-2W



FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	19

CONTRACT NO.:

LEGEND



STAKING OF PROPOSED RIGHT OF WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

STATE OF KENTUCKY )  
 ) SS  
 COUNTY OF McCRACKEN )

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED \_\_\_\_\_  
 RONALD S. BACON, PLS NO. 035-003586  
 LICENSE EXPIRATION DATE: 11/30/2008

**GEOTECH**  
 ENGINEERING & TESTING, INC.

500 South 17th Street  
 Paducah, Kentucky 42003  
 Phone: (270) 443-1995  
 Fax: (270) 443-1904

401 North Market  
 Marlon, IL 62958  
 Phone: (618) 997-9190

REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PLAT OF HIGHWAYS  
 FAP ROUTE 304 (IL ROUTE 100)  
 SECTION 5BR-2  
 CALHOUN COUNTY  
 JOB NO. R-98-012-07  
 STA. 457+00 TO STA. 473+00

CURVE TABLE				
CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
11-1	1,969.12' (2,081.5')	221.46'	N 6° 40' 10" W	221.35'
11-2	1,999.12'	141.33'	S 5° 30' 14" E	141.30'
12-1	1,969.12' (2,081.5')	253.03'	S 0° 14' 01" W	252.86'
12-2	1,999.12'	249.20'	N 0° 05' 33" E	249.04'
13-1	1,969.12' (2,081.5')	318.16'	S 9° 23' 40" W	317.81'
13-2	1,999.12'	320.86'	N 9° 06' 22" E	320.51'
13-3	25.00'	44.50'	N 37° 18' 54" W	38.85'
14-1	1,969.12' (2,081.5')	29.24'	N 4° 20' 25" E	29.24'
14-2	1,999.12'	29.47'	S 4° 05' 09" W	29.47'

COMPLETION DATE OF FIELD WORK PERFORMED  
 LAND SURVEY: 5/23/07  
 RIGHT OF WAY STAKING:

PARCEL NO.	OWNER	TOTAL HOLDING ACRES	FEE SIMPLE ACQUISITION						PERMANENT TAX NUMBER	PROPERTY ACQUIRED BY	
			GROSS ACRES	SQ. FT.	PREVIOUSLY DEDICATED ACRES	SQ. FT.	NET ACRES	SQ. FT.			
8712011	VILLAGE OF KAMPSVILLE BOOK 24, PAGE 457 TITLE REPORT NUMBER CN-227	4.9970	0.1339	5,831	0	0	0.1339	5,831	4,8631	07-06-11-300-002-A	
8712012	SANDRA A. CLENDENNY, A MARRIED PERSON BOOK 48, PAGE 203 TITLE REPORT NUMBER CN-226	72.2148	0.1729	7,533	0	0	0.1729	7,533	72,0418	07-06-11-300-002	
8712013	DONNA JANE RICHTER AS TRUSTEE OF THE DONNA JANE RICHTER DECLARATION OF TRUST DATED NOVEMBER 1, 2006 DOCUMENT NO. 60388 TITLE REPORT NUMBER CN-225	3.8727	0.2447	10,661	0	0	0.2447	10,661	3,6280	07-06-11-300-006	
8712014	THE ESTATE OF EMIEL HEIDENREICH, DECEASED TITLE REPORT NUMBER CN-330	0.3046	0.0202	882	0	0	0.0202	882	0,2844	NO PIN ASSIGNED	

SCALE: 1" = 60'  
 SHEET 2 OF 4

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS/DISTRICT 8  
 1102 EASTPORT PLAZA DRIVE  
 COLLINGSVILLE, ILLINOIS 62234-6198

PART OF THE S.W. 1/4 OF SECTION 11 T. 9 S., R. 2 W., OF THE 4RD PM, CALHOUN COUNTY, ILLINOIS

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	20
STA. 455+00		TO STA. 480+00		
CONTRACT NO.:				

LEGEND

- QUARTER SECTION CORNERS
- SECTION CORNERS
- QUARTER QUARTER SECTION LINE
- SECTION LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED CENTERLINE
- EXISTING RIGHT OF WAY LINE
- EXISTING CENTERLINE
- PROPERTY (DEED) LINE
- MEASURED DIMENSION
- RECORDED DIMENSION
- FOUND STONE
- FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED
- SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED
- PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 66701 (TO BE SET BY OTHERS)
- SET 5/8 INCH IRON ROD AS SURVEY CONTROL UNLESS OTHERWISE NOTED
- FOUND CUT CROSS
- SET CUT CROSS
- SAME OWNERSHIP
- EXISTING BUILDING

STAKING OF PROPOSED RIGHT OF WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN, IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

STATE OF KENTUCKY )  
 ) SS  
 COUNTY OF McCRACKEN )

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED \_\_\_\_\_  
 RONALD S. BACON, PLS NO. 035-003586  
 LICENSE EXPIRATION DATE: 11/30/2008

**GEOTECH**  
 ENGINEERING & TESTING, INC.

500 South 17th Street  
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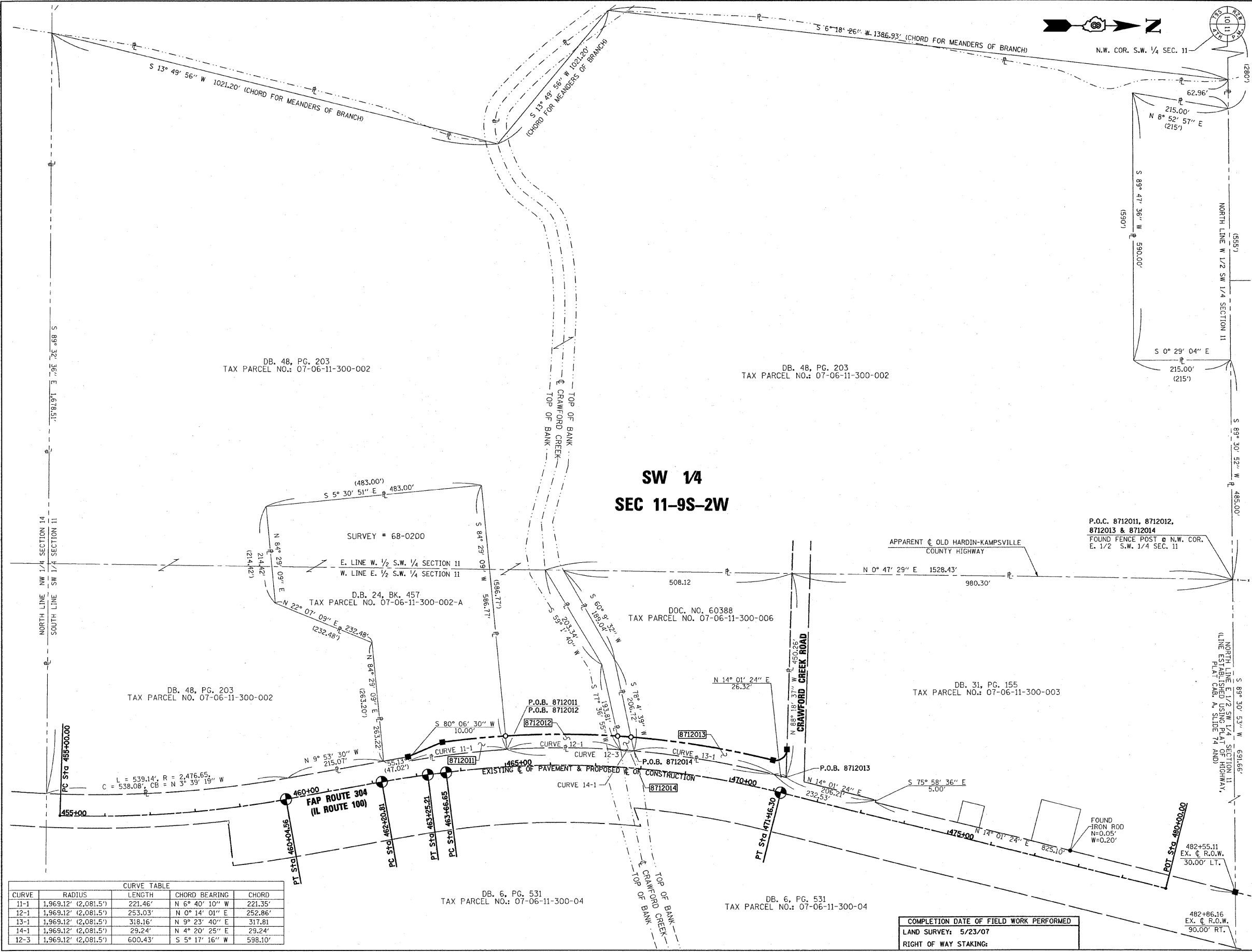
REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAT OF HIGHWAYS**  
 FAP ROUTE 304 (IL ROUTE 100)  
 SECTION 5BR-2  
 CALHOUN COUNTY  
 JOB NO. R-98-012-07  
 STA. 455+00 TO STA. 480+00

100' 0 100' 200'  
 SCALE: 1" = 100' SHEET 3 OF 4

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DIVISION OF HIGHWAYS/DISTRICT 9  
 1102 EASTPORT PLAZA DRIVE  
 COLLINSVILLE, ILLINOIS 62234-6198  
 SHEET 1 IS A COVER SHEET

NO.	DATE	DESCRIPTION	BY



CURVE	RADIUS	LENGTH	CHORD BEARING	CHORD
11-1	1,969.12' (2,081.5')	221.46'	N 6° 40' 10" W	221.35'
12-1	1,969.12' (2,081.5')	253.03'	N 0° 14' 01" E	252.86'
13-1	1,969.12' (2,081.5')	318.16'	N 9° 23' 40" E	317.81'
14-1	1,969.12' (2,081.5')	29.24'	N 4° 20' 25" E	29.24'
12-3	1,969.12' (2,081.5')	600.43'	S 5° 17' 16" W	598.10'

COMPLETION DATE OF FIELD WORK PERFORMED  
 LAND SURVEY: 5/23/07  
 RIGHT OF WAY STAKING:

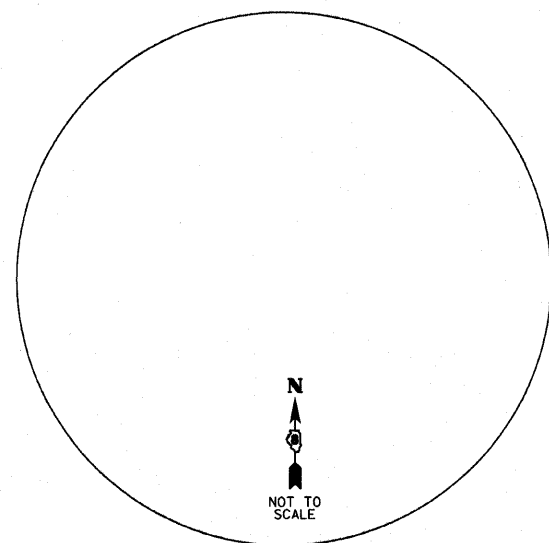
9/20/07  
 1/20/2008  
 RSE: Ronald S. Bacon  
 \*REF-  
 \*REF-  
 \*REF-

FAP ROUTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	20A
STA. N/A		TO STA. N/A		
CONTRACT NO.:				

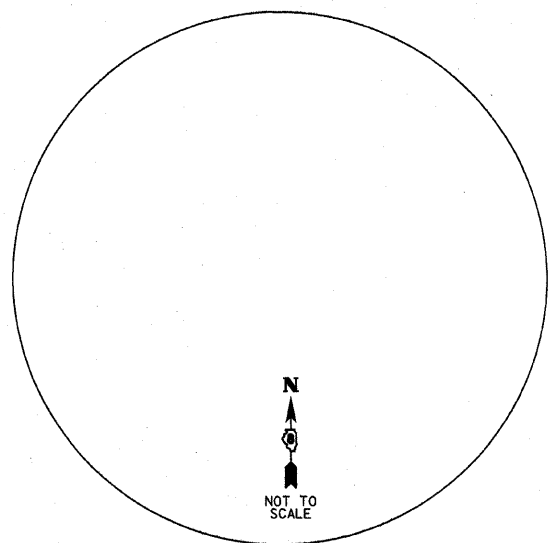
**PROPOSED CENTERLINE OF CONSTRUCTION CONTROL TIES**

**LEGEND**

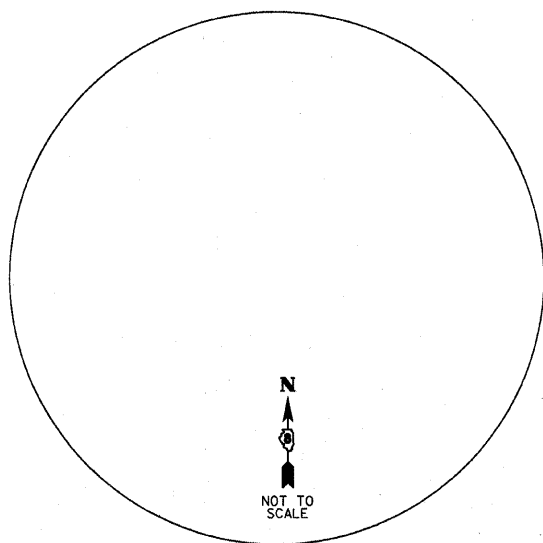
SECTION CORNERS  
 QUARTER SECTION CORNERS  
 EXISTING CENTERLINE  
 EXISTING RIGHT OF WAY LINE  
 PROPOSED CENTERLINE  
 PROPOSED RIGHT OF WAY LINE  
 SECTION LINE  
 QUARTER SECTION LINE  
 QUARTER QUARTER SECTION LINE  
 PROPERTY (DEED) LINE  
 MEASURED DIMENSION  
 RECORDED DIMENSION  
 FOUND STONE  
 FOUND IRON PIPE OR IRON ROD AT CORNER UNLESS OTHERWISE NOTED  
 SET 5/8 INCH IRON ROD WITH PLASTIC CAP IDENTIFIED BY SURVEYORS LICENSE NUMBER AT CORNER UNLESS OTHERWISE NOTED  
 PERMANENT SURVEY MONUMENT, I.D.O.T. STD. 667101 (TO BE SET BY OTHERS)  
 SAME OWNERSHIP  
 EXISTING BUILDING



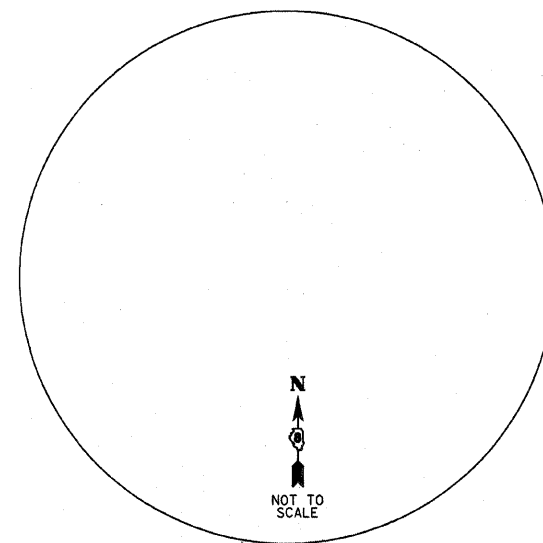
P.T. STA. 460+04.56 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



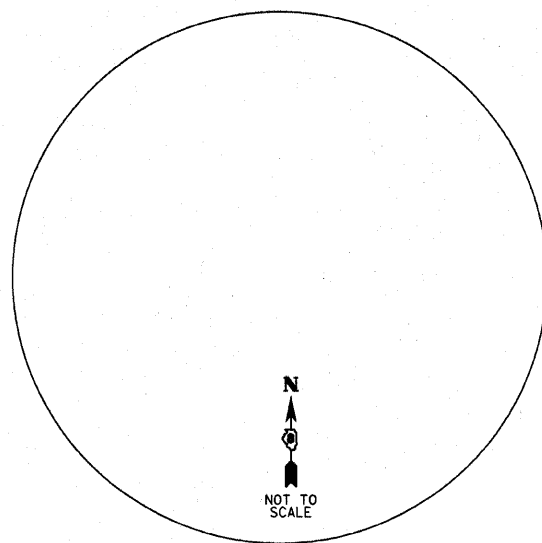
P.C. STA. 462+20.81 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



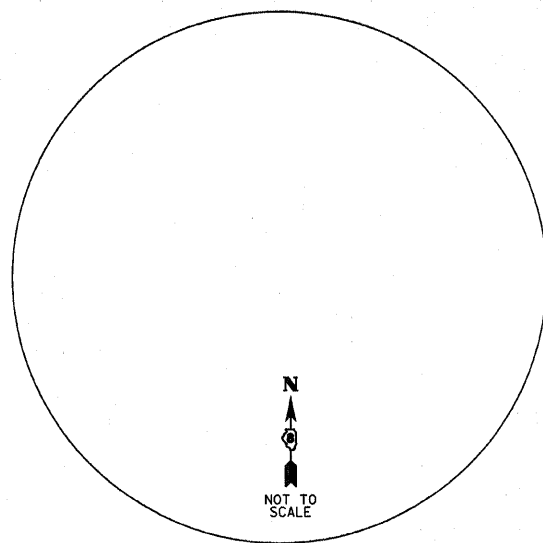
P.I. STA. 462+73.02 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



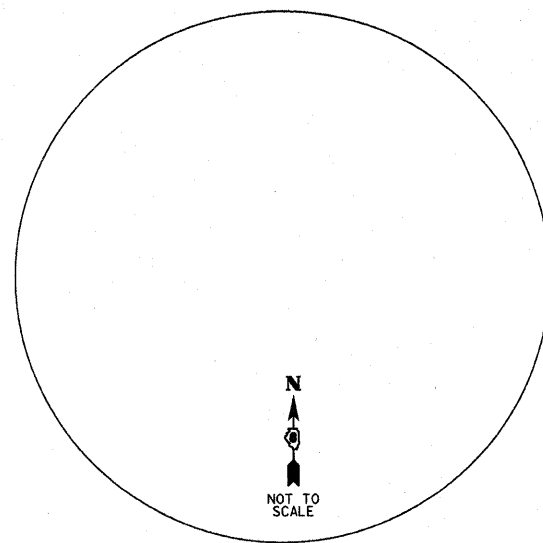
P.T. STA. 463+25.21 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



P.C. STA. 463+66.65 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



P.I. STA. 467+45.79 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL



P.C. STA. 471+16.30 PROPOSED  $\text{C}$  OF CONSTRUCTION  
FAP ROUTE 304 / IL RT. 100  
SET MAGNAIL

■ STAKING OF PROPOSED RIGHT OF WAY AND PERMANENT EASEMENT CORNERS. SET 5/8 INCH METAL ROD WITH DIVISION OF HIGHWAY SURVEY ALUMINUM CAP TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS LICENSE NUMBER. (PROPOSED RIGHT OF WAY CORNERS SET IN CULTIVATED AREAS SHALL BE A MINIMUM OF 20 INCHES BELOW THE GROUND SURFACE).

STATE OF KENTUCKY )  
                                  ) SS  
COUNTY OF McCRACKEN )

I, RONALD S. BACON, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, CERTIFY THAT I HAVE SURVEYED THE PLAT OF HIGHWAY SHOWN HEREON AND THAT THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY FOR THE PROPOSED PARCELS TO BE ACQUIRED BY THE STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, SHOWN HEREON.

DATED \_\_\_\_\_

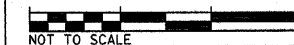
RONALD S. BACON, PLS NO. 035-003586  
LICENSE EXPIRATION DATE: 11/30/2008

**GEOTECH**  
ENGINEERING & TESTING, INC.

500 South 17th Street      401 North Market  
Paducah, Kentucky 42003      Marion, IL 62959  
Phone: (270) 443-1995      Phone: (618) 997-9190  
Fax: (270) 443-1904

REGISTRATION NO. 184-003258

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAT OF HIGHWAYS**  
FAP ROUTE 304 (IL ROUTE 100)  
SECTION 5BR-2  
CALHOUN COUNTY  
JOB NO. R-98-012-07  
TIE SHEET



NOT TO SCALE SHEET 4 OF 4

COMPLETION DATE OF FIELD WORK PERFORMED  
LAND SURVEY: 5/23/07  
RIGHT OF WAY STAKING:

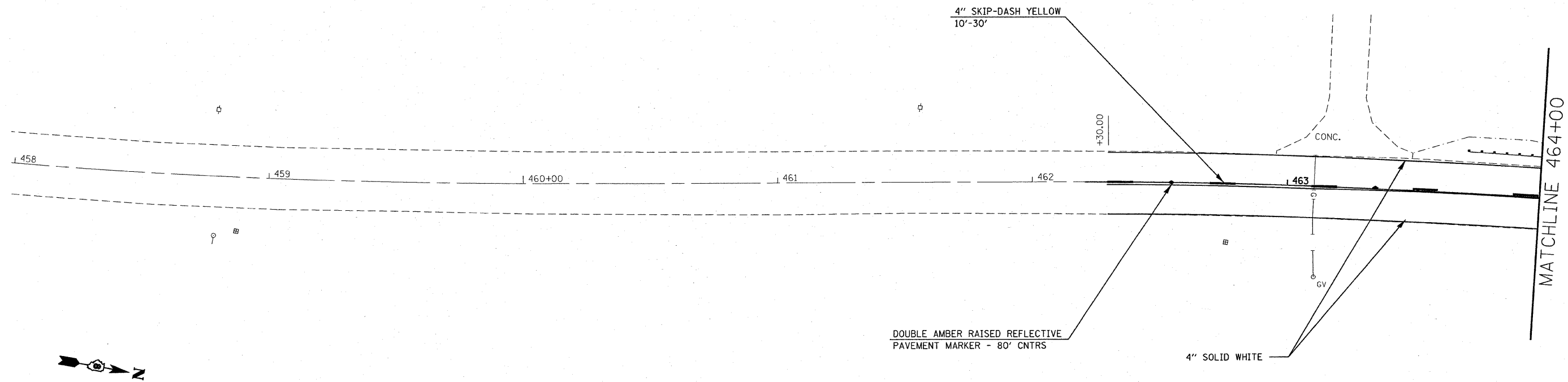
ILLINOIS DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS/DISTRICT 8  
1102 EASTPORT PLAZA DRIVE  
COLLINSVILLE, ILLINOIS 62234-6198  
SHEET 1 IS A COVER SHEET

NO.	DATE	BY	DESCRIPTION

#DATE##  
 P:\General\Projects\Geotech\IL 188\7-18-07\Submittal\SH10204.dgn  
 \*REF-  
 \*REF-  
 \*REF-

PLOT DATE: 1/30/2008

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	21
STA. 458+00.00		TO STA. 464+00.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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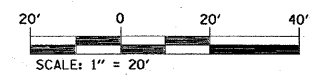
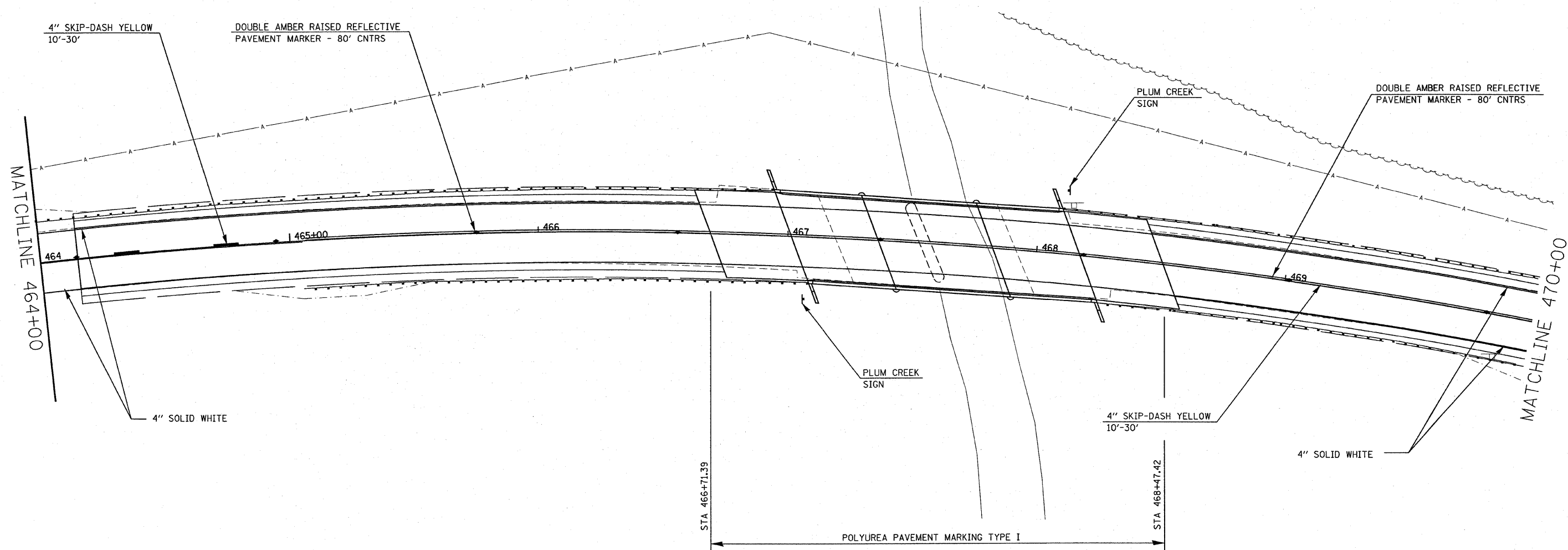
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING DETAILS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	22
STA. 464+00.00		TO STA. 470+00.00		
FED. ROAD DIST. NO. ILLINOIS		FED. AID PROJECT		



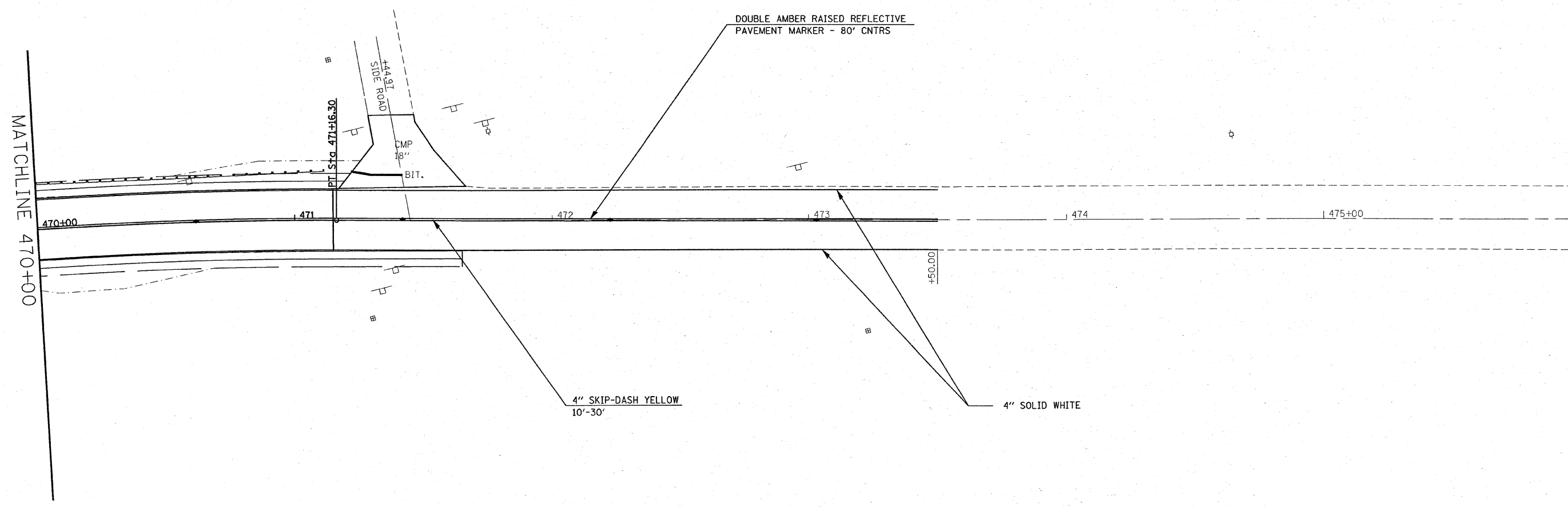
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING DETAILS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

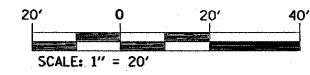
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 DATE / DRAWN BY / CHECKED BY

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 PLOT SCALE = 28.0000 / IN.  
 REFERENCE = REF

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	23
STA. 470+00.00		TO STA. 475+00.00		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



MATCHLINE 470+00



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PAVEMENT MARKING DETAILS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CALHOUN COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

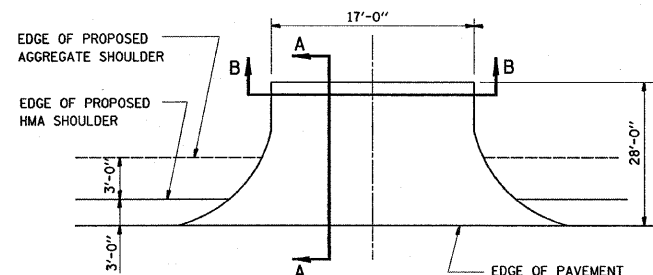
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 REFERENCE = #REF#



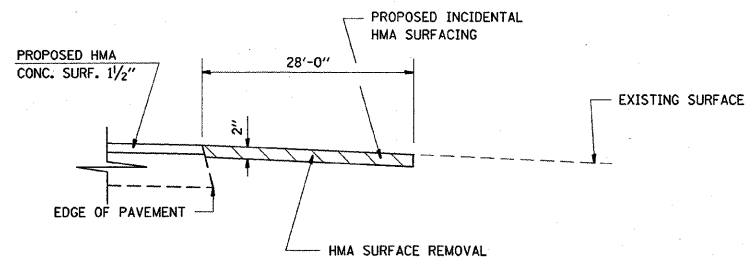
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CLINTON	68	24
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

**DETAIL OF HMA ENTRANCES**

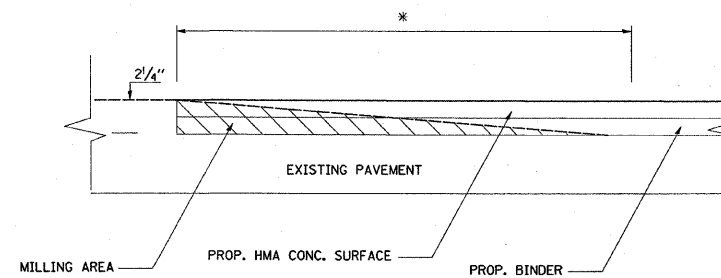
P.E. AT STA. 471+44.97



**PLAN**

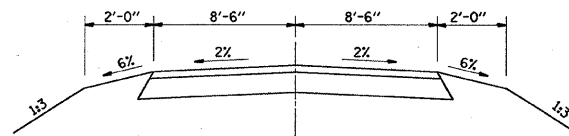


**SECTION A-A WITH NEGATIVE GRADE**



**HMA SURFACE REMOVAL DETAIL**

\* BEGINNING STA. 464+15.00 TO STA. 465+04.86  
 ENDING STA. 470+11.81 TO STA. 471+15.00



**SECTION B-B**

PLOT DATE = 1/30/2008  
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 PLOT SCALE = 3/8" = 1'-0"  
 REFERENCE = #REF#

DRAWING NOT TO SCALE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SIDE ROAD & JOINT DETAILS**

FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY

SCALE: VERT.  
 HORIZ.  
 DATE

DRAWN BY  
 CHECKED BY

Bench Mark: BM 206, chiseled square on top of north end of west concrete curb on existing structure. Elevation 438.261

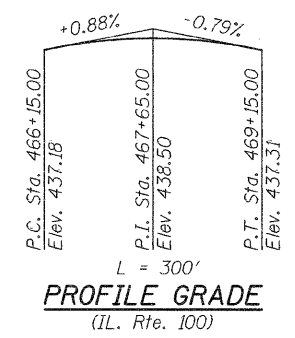
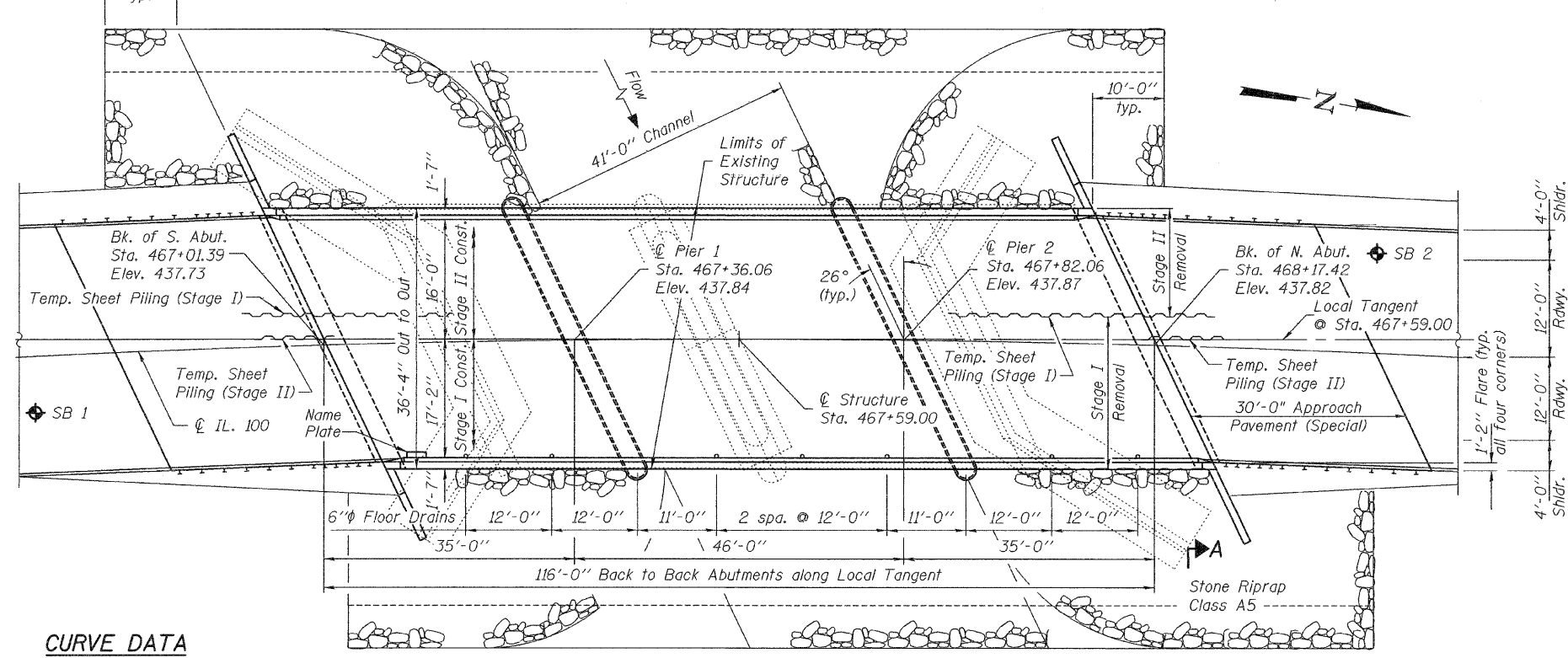
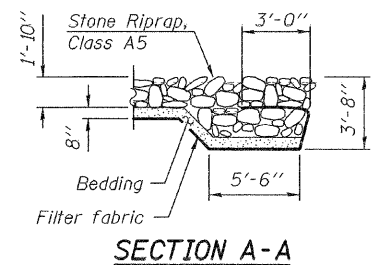
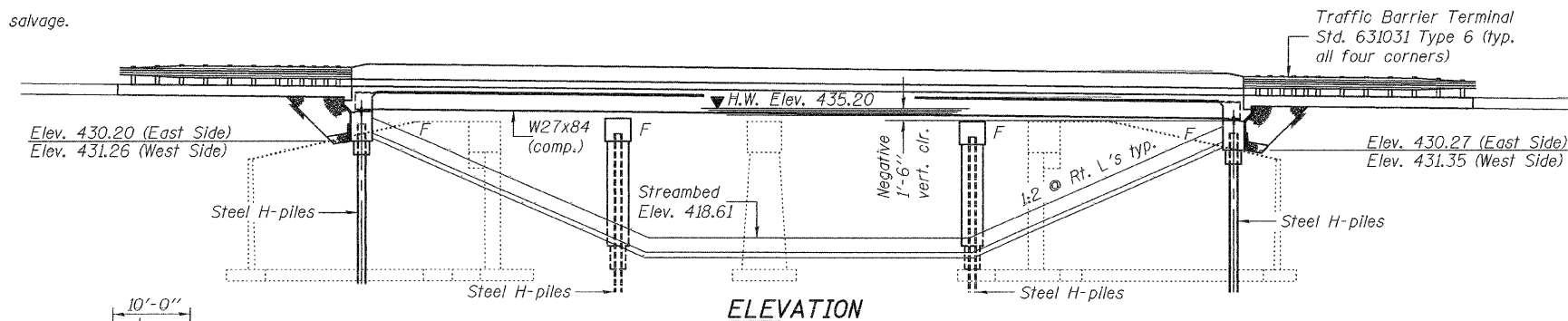
Existing Structure: S.N. 007-0004, originally built in 1924 as S.B.I. Route 38, Section 5B. The superstructure was replaced and the structure was widened in 1981. The existing structure is a two span deck beam bridge supported on closed abutments and a solid wall pier all on pile supported footings. The back to back abutment length is 72'-7" and the out to out bridge width is 36'-0". Traffic is to be maintained utilizing stage construction.

No salvage.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	JOB SHEETS	SHEET NO.	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	25	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948



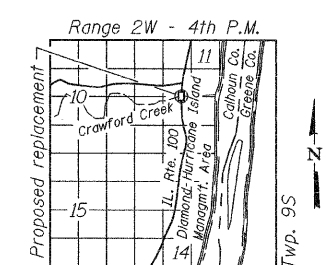
**INDEX OF SHEETS**

- 1 General Plan and Elevation
- 2 General Structure Data
- 3 Foundation Layout
- 4 Stage Construction Details
- 5 Temporary Concrete Barrier For Stage Construction
- 6-9 Top of Slab Elevations
- 10 Top of S. Approach Slab Elevations
- 11 Top of N. Approach Slab Elevations
- 12 Superstructure
- 13 Superstructure Details
- 14 Diaphragm Details
- 15 Framing Plan and Design Data
- 16 Beam Details
- 17 Bearing Details
- 18 South Abutment Details
- 19 North Abutment Details
- 20 Pier 1 Details
- 21 Pier 2 Details
- 22 Miscellaneous Pier Details
- 23 Steel H-Pile Details
- 24 Bar Splicer Assembly Details
- 25 Cantilever Forming Brackets for Superstructure
- 26 Concrete Parapet Slipforming Option
- 27-28 Boring Logs

STATION 467+59.00  
BUILT 200 BY  
STATE OF ILLINOIS  
F.A.P. RT. 304 SEC. 5BR-2  
LOADING HL93  
STRUCTURE NO. 007-0027

**NAME PLATE**  
See Std. 515001

**APPROVED**  
FOR STRUCTURAL ADEQUACY ONLY  
*Robert E. Anderson*  
ENGINEER OF BRIDGES AND STRUCTURES



**LOADING HL-93**  
Allow 50#/sq. ft. for future wearing surface.

**DESIGN SPECIFICATIONS**  
2007 AASHTO LRFD Bridge Design Specifications, 4th. Edition

**DESIGN STRESSES**  
 $f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (reinforcement)  
 $f_y = 50,000$  psi (M270 Grade 50 structural steel)

**SEISMIC DATA**  
Seismic Performance Zone (SPZ) = 1  
Bedrock Acceleration Coefficient (A) = 0.054g  
Site Coefficient (S) = 2.0

**GENERAL PLAN AND ELEVATION**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

**CURVE DATA**  
P.I. Sta. = 467+41.98  
 $\Delta = 20^\circ-56'-30''$  (RT)  
 $D = 2^\circ-49'-17''$   
 $R = 2,030.70'$   
 $T = 375.30'$   
 $L = 742.22'$   
 $E = 34.39'$   
 $S.E. = 5.8\%$   
P.C. Sta. = 463+66.68  
P.T. Sta. = 471+08.91

**DESIGN SCOUR ELEVATION TABLE**

Design Scour Elevation (feet)	S. Abut.	Pier 1	Pier 2	N. Abut.
	430.0	416.6	416.6	430.1

**WATERWAY INFORMATION**

Drainage Area = 5.57 sq. mi. Low Grade Elev. 436.21 @ Sta. 462+00

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Head - Ft.		Headwater El.		
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	
Design	50	3,081	743.82	997.32	435.20	0.30	0.15	435.50	435.35
Base	100	3,664	743.82	997.32	435.20	0.43	0.22	435.63	435.42
Overtopping	N/A	-	-	-	-	-	-	0.00	0.00
Max. Calc.	500	5,107	743.82	997.32	435.19	0.83	0.43	436.02	435.62
Scour	10	1,848	743.82	997.32	435.20	0.11	0.06	435.31	435.26

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



*David W. Petermeier*  
**DAVID W. PETERMEIER**  
EDWARDSVILLE, ILLINOIS  
ILLINOIS LICENSED STRUCTURAL  
ENGINEER NO. 081-005642  
EXPIRES NOV. 30, 2008

02/27/08

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 2
F.A.P. 304	5BR-2	CALHOUN	68	26	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948

GENERAL NOTES

Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts  $\frac{7}{8}$  in.  $\phi$ , holes  $\frac{15}{16}$  in.  $\phi$ , unless otherwise noted.  
Calculated weight of Structural Steel = 60,570 lbs. of Grade 50 and 5,150 lbs. of Grade 36.  
No field welding is permitted except as specified in the contract documents.  
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60 (1L Modified). See Special Provisions  
Reinforcement bars designated (E) shall be epoxy coated.  
If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.  
Bearing seat surfaces shall be constructed or adjusted to their designated elevations within a tolerance of  $\frac{1}{8}$  inch (0.01 ft.). Adjustment shall be made either by grinding the surface or by shimming the bearings.  
The Organic Zinc Rich Primer / Epoxy / Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasteners and damaged areas shall be touched up in the field. The color of the final finish coat for all interior steel surfaces shall be Gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5 G 4/8. See Special Provision for "Cleaning and Painting New Metal Structures".  
Layout of slope protection system may be varied in the field to suit ground conditions as directed by the Engineer.  
The embankment configuration shown shall be the minimum that must be placed and compacted prior to construction of the abutments.  
The Contractor shall drive test piles to 110% of the nominal required bearing specified in production locations at substructures specified or approved by the Engineer before ordering the remainder of piles.  
The Contractor is advised that the existing PPC Deck Beams are in a deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the beams when developing construction procedures for removal and replacement of the superstructure.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. Yd.		126	126
Stone Riprap, Class A5	Sq. Yd.		1,479	1,479
Filter Fabric	Sq. Yd.		1,479	1,479
Removal of Existing Structures	Each			1
Structure Excavation	Cu. Yd.		424	424
Floor Drains	Each	7		7
Concrete Structures	Cu. Yd.		168.0	168.0
Concrete Superstructure	Cu. Yd.	155.4		155.4
Bridge Deck Grooving	Sq. Yd.	402		402
Concrete Encasement	Cu. Yd.		10.8	10.8
Protective Coat	Sq. Yd.	576		576
Furnishing and Erecting Structural Steel	L. Sum	1		1
Stud Shear Connectors	Each	2,376		2,376
Reinforcement Bars, Epoxy Coated	Pound	35,670	16,480	52,150
Bar Splicers	Each	415	102	517
Furnishing Steel Piles HP 12x53	Foot		575	575
Furnishing Steel Piles HP 14x89	Foot		620	620
Driving Piles	Foot		1,195	1,195
Test Pile Steel HP 12x53	Each		2	2
Test Pile Steel HP 14x89	Each		2	2
Temporary Sheet Piling	Sq. Ft.		673	673
Name Plates	Each	1		1
Anchor Bolts, 1"	Each		48	48
Geocomposite Wall Drain	Sq. Yd.		79	79
Pipe Underdrains for Structures 4"	Foot		106	106
Underwater Structure Excavation Protection, Location #1	Each		1	1
Underwater Structure Excavation Protection, Location #2	Each		1	1
Asbestos Bearing Pad Removal	Each	48		48

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



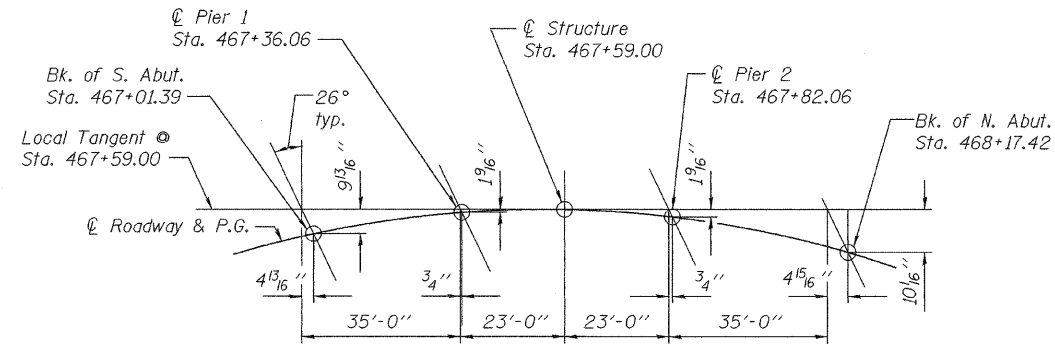
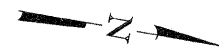
02/27/08

GENERAL STRUCTURE DATA  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

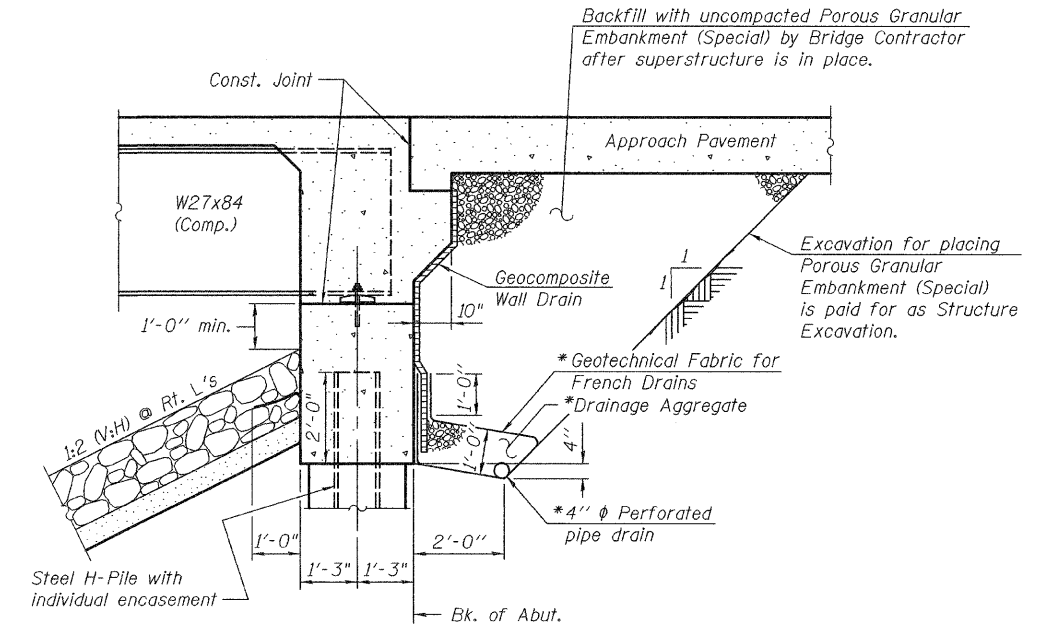
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 3
F.A.P. 304	5BR-2	CALHOUN	68	27	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948



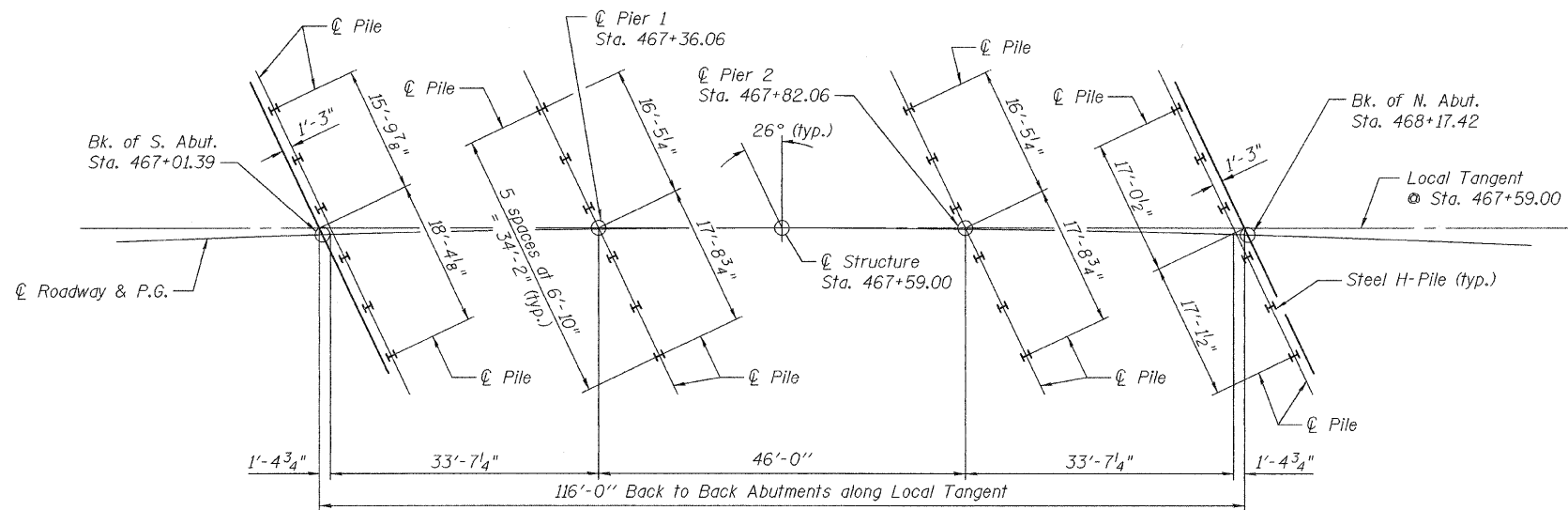
**GEOMETRIC LAYOUT**



**SECTION THRU INTEGRAL ABUTMENT**  
(Horiz. dim. @ Rt. L's)

\* Included in the cost of Pipe Underdrains for Structures 4".

Note:  
All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101).



**FOUNDATION LAYOUT**

Note:  
Pile encasement not shown for clarity.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



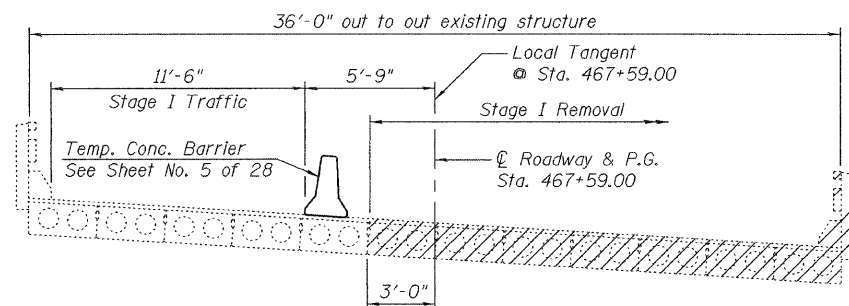
02/27/08

**FOUNDATION LAYOUT**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

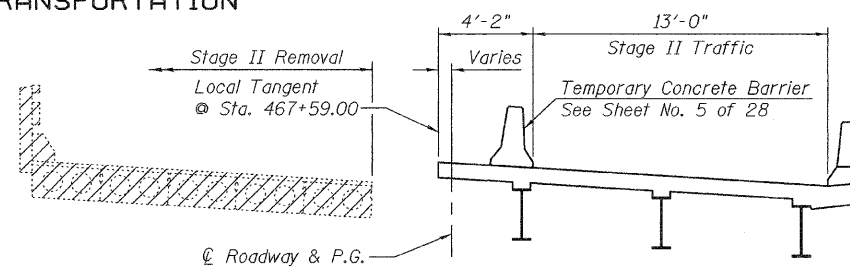
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 4 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	08	28	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

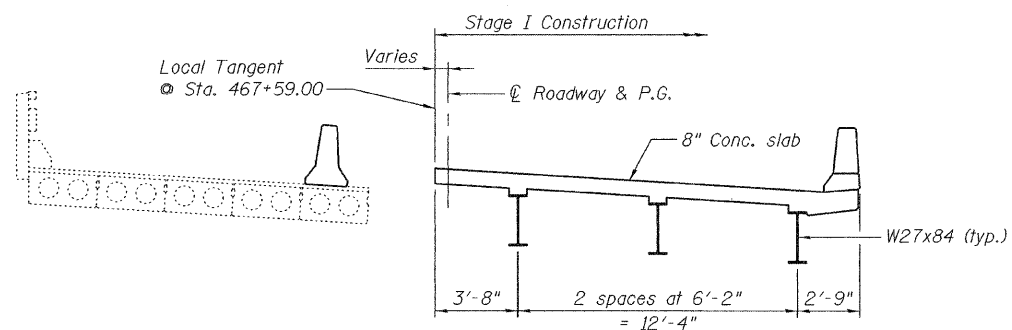
Contract #76948



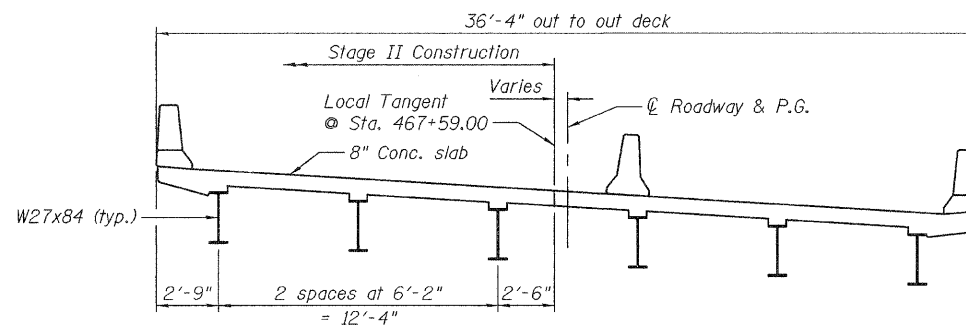
**STAGE I REMOVAL**  
(Looking upstation)



**STAGE II REMOVAL**  
(Looking upstation)



**STAGE I CONSTRUCTION**  
(Looking upstation)



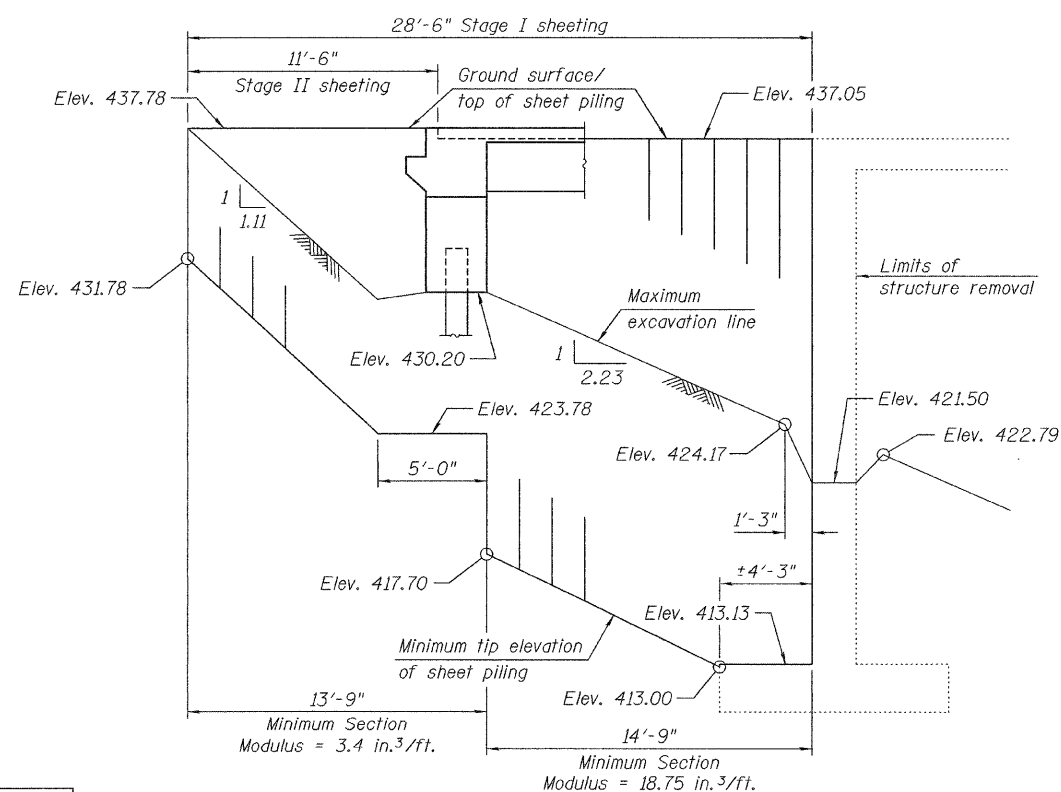
**STAGE II CONSTRUCTION**  
(Looking upstation)

Notes:

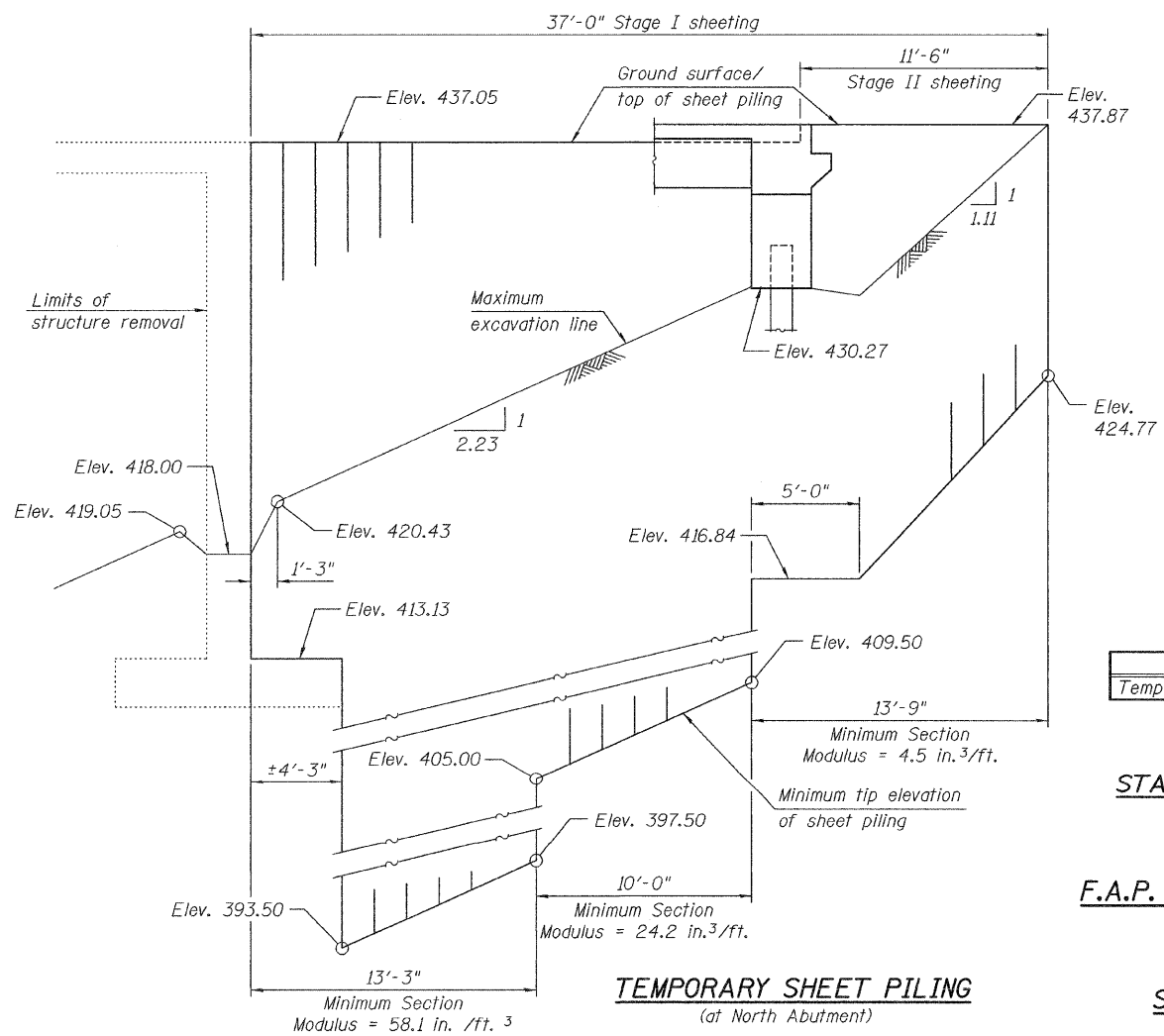
Excavation behind existing abutment walls shall be performed to balance front and back soil pressure before removing the existing superstructure. The Contractor shall sawcut the upper portion of the existing abutment at the stage removal line before Stage I removal to ensure the remaining portion will not be prematurely damaged.

If the Contractor chooses to alter the temporary cantilevered sheet piling design requirements shown on the plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.

The Contractor shall connect the first sheet to the existing abutment wall to ensure stability of sheets driven to the top of the existing footing. This connection shall be reviewed and accepted by the Engineer and included in the cost for Temporary Sheet Piling.



**TEMPORARY SHEET PILING**  
(at South Abutment)



**TEMPORARY SHEET PILING**  
(at North Abutment)

**BILL OF MATERIAL**

Item	Unit	Total
Temporary Sheet Piling	Sq. Ft.	673

**STAGE CONSTRUCTION DETAILS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



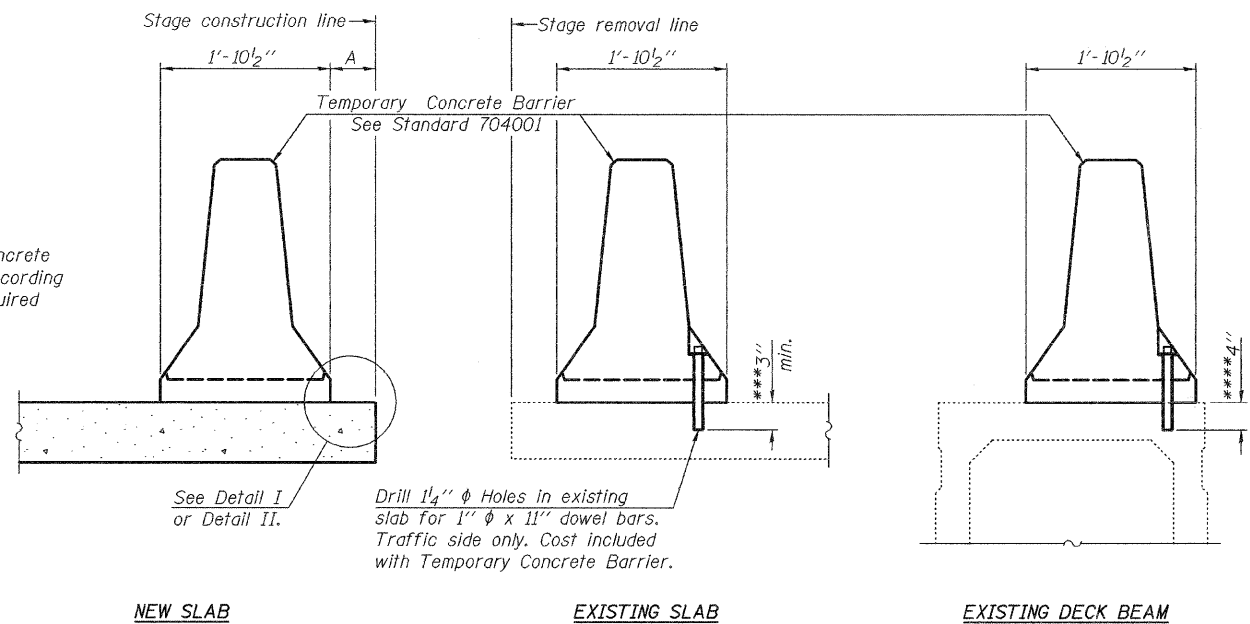
02/27/08

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 5 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	29	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76948

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



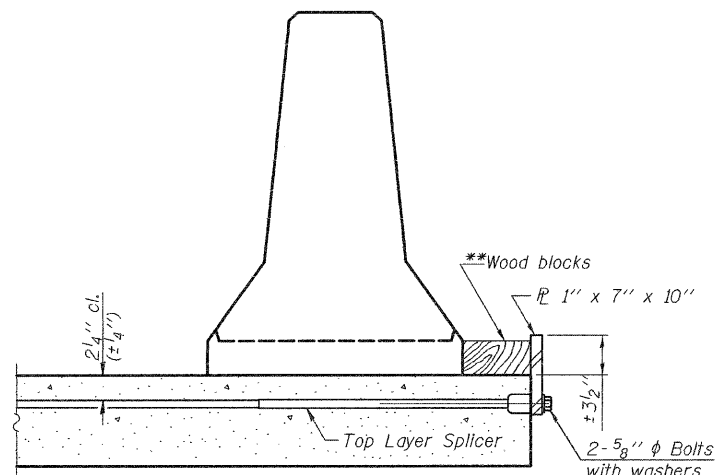
Drill 1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

**NOTES**

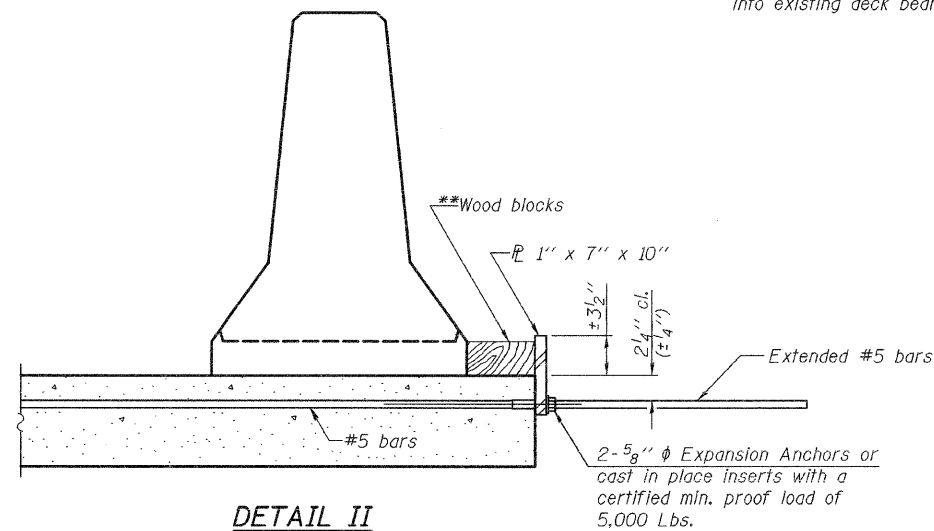
- Detail I - With Bar Splicer or Couplers:**  
Connect one (1) 1"x7"x10" steel  $\bar{L}$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\bar{C}$  of each barrier panel.
- Detail II - With Extended Reinforcement Bars:**  
Connect one (1) 1"x7"x10" steel  $\bar{L}$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\bar{C}$  of each barrier panel.
- Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.  
See Roadway Plans for quantity of Temporary Concrete Barrier.

**SECTIONS THRU SLAB OR DECK BEAM**

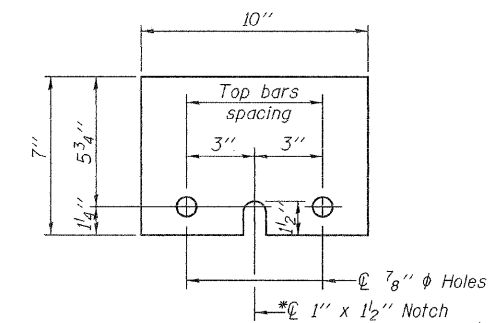
- \*\*\*Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.
- \*\*\*If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



**DETAIL I**



**DETAIL II**



**STEEL RETAINER  $\bar{L}$  1" x 7" x 10"**

\* Required only with Detail II

\*\*Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM

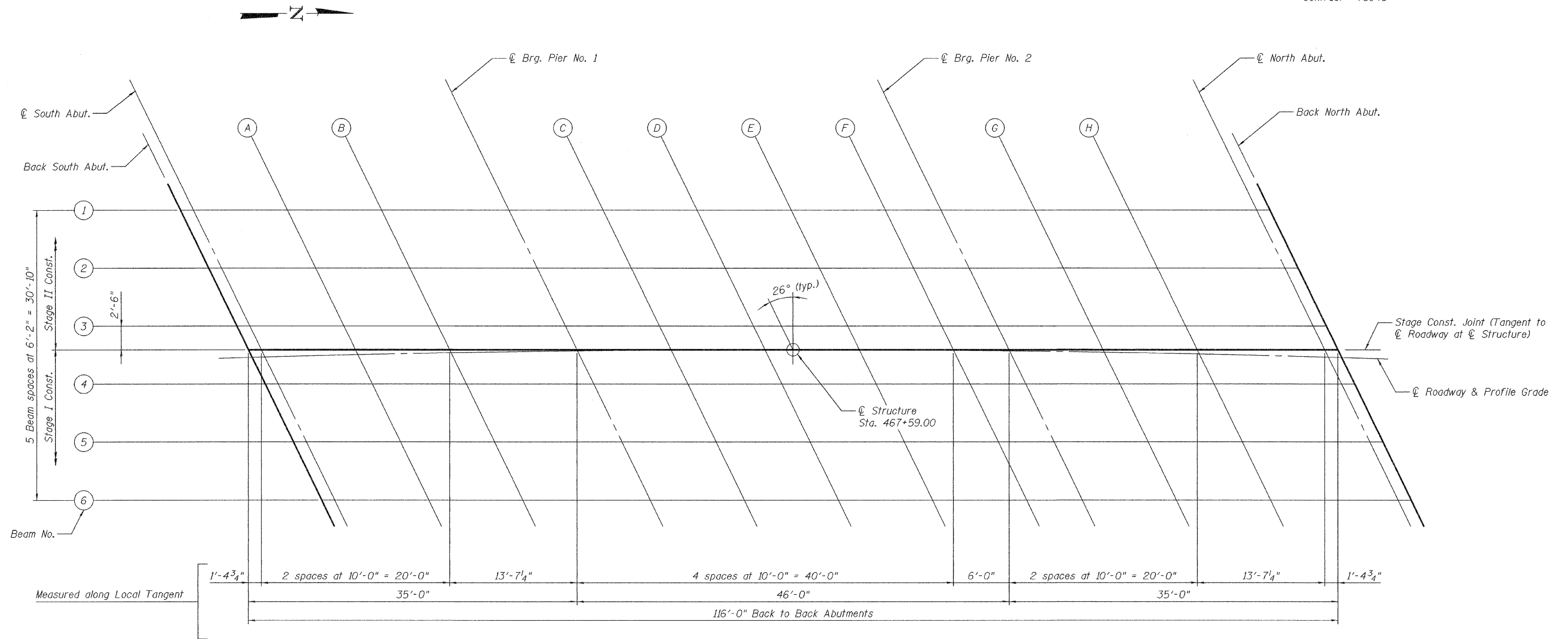


**TEMPORARY CONCRETE BARRIER  
FOR STAGE CONSTRUCTION  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 30	SHEET NO. 6 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT-	

Contract #76948



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

TOP OF SLAB ELEVATIONS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 7 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	31	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76948

**BEAM 1**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+94.26	-15.87	438.62	438.62
⊕ South Abut.	466+95.64	-15.83	438.63	438.63
A	467+05.56	-15.54	438.65	438.66
B	467+15.48	-15.30	438.67	438.68
⊕ Brg. Pier No. 1	467+28.99	-15.06	438.69	438.69
C	467+38.91	-14.93	438.71	438.72
D	467+48.84	-14.86	438.72	438.74
E	467+58.77	-14.83	438.73	438.75
F	467+68.69	-14.86	438.74	438.74
⊕ Brg. Pier No. 2	467+74.65	-14.89	438.74	438.74
G	467+84.58	-15.00	438.74	438.75
H	467+94.50	-15.15	438.74	438.75
⊕ North Abut.	468+08.01	-15.43	438.74	438.74
Bk. North Abut.	468+09.39	-15.46	438.74	438.74

**BEAM 2**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+97.06	-9.62	438.27	438.27
⊕ South Abut.	466+98.44	-9.57	438.28	438.28
A	467+08.39	-9.30	438.30	438.31
B	467+18.34	-9.08	438.32	438.32
⊕ Brg. Pier No. 1	467+31.89	-8.85	438.34	438.34
C	467+41.85	-8.74	438.36	438.37
D	467+51.80	-8.68	438.37	438.38
E	467+61.76	-8.67	438.37	438.39
F	467+71.72	-8.71	438.38	438.39
⊕ Brg. Pier No. 2	467+77.69	-8.75	438.38	438.38
G	467+87.65	-8.87	438.38	438.39
H	467+97.60	-9.04	438.38	438.39
⊕ North Abut.	468+11.15	-9.34	438.38	438.38
Bk. North Abut.	468+12.53	-9.38	438.38	438.38

**BEAM 3**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	466+99.87	-3.36	437.92	437.92
⊕ South Abut.	467+01.26	-3.32	437.92	437.92
A	467+11.24	-3.06	437.95	437.95
B	467+21.22	-2.85	437.97	437.97
⊕ Brg. Pier No. 1	467+34.81	-2.64	437.99	437.99
C	467+44.80	-2.55	438.00	438.01
D	467+54.79	-2.50	438.01	438.03
E	467+64.77	-2.51	438.02	438.03
F	467+74.76	-2.56	438.02	438.03
⊕ Brg. Pier No. 2	467+80.75	-2.62	438.03	438.03
G	467+90.74	-2.75	438.03	438.03
H	468+00.72	-2.93	438.02	438.03
⊕ North Abut.	468+14.31	-3.25	438.02	438.02
Bk. North Abut.	468+15.70	-3.29	438.02	438.02

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

**TOP OF SLAB ELEVATIONS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 8 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	32	
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76948

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+01.02	-0.83	437.78	437.78
☉ South Abut.	467+02.41	-0.79	437.78	437.78
A	467+12.40	-0.53	437.80	437.81
B	467+22.39	-0.33	437.82	437.83
☉ Brg. Pier No. 1	467+36.00	-0.13	437.84	437.84
C	467+46.00	-0.04	437.86	437.87
D	467+56.00	0.00	437.87	438.89
E	467+66.00	-0.01	437.87	438.89
F	467+76.00	-0.07	437.88	437.88
☉ Brg. Pier No. 2	467+81.98	-0.13	437.88	437.88
G	467+92.00	-0.27	437.88	437.88
H	468+01.99	-0.46	437.88	437.89
☉ North Abut.	468+15.59	-0.79	437.87	437.87
Bk. North Abut.	468+16.98	-0.83	437.87	437.87

☉ ROADWAY & P.G.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+01.39	0.00	437.73	437.73
☉ South Abut.	467+02.76	0.00	437.74	437.74
A	467+12.64	0.00	437.77	437.78
B	467+22.55	0.00	437.80	437.81
☉ Brg. Pier No. 1	467+36.06	0.00	437.84	437.84
C	467+46.02	0.00	437.86	437.87
D	467+56.00	0.00	437.87	438.89
E	467+66.01	0.00	437.87	438.89
F	467+76.03	0.00	437.88	437.88
☉ Brg. Pier No. 2	467+82.06	0.00	437.87	437.87
G	467+92.13	0.00	437.87	437.87
H	468+02.23	0.00	437.85	437.86
☉ North Abut.	468+16.05	0.00	437.82	437.82
Bk. North Abut.	468+17.42	0.00	437.82	437.82

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+02.70	2.89	437.57	437.57
☉ South Abut.	467+04.09	2.93	437.57	437.57
A	467+14.11	3.17	437.59	437.60
B	467+24.12	3.37	437.61	437.62
☉ Brg. Pier No. 1	467+37.75	3.56	437.63	437.63
C	467+47.77	3.64	437.65	437.66
D	467+57.79	3.67	437.66	437.67
E	467+67.80	3.65	437.66	437.68
F	467+77.82	3.58	437.67	437.67
☉ Brg. Pier No. 2	467+83.83	3.52	437.67	437.67
G	467+93.85	3.37	437.67	437.67
H	467+03.86	3.17	437.67	437.67
☉ North Abut.	468+17.49	2.83	437.66	437.66
Bk. North Abut.	468+18.88	2.79	437.66	437.66

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



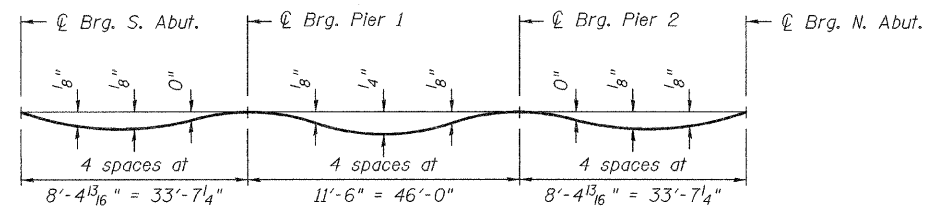
02/27/08

TOP OF SLAB ELEVATIONS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEET NO.	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68 33	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-		

Contract #76948

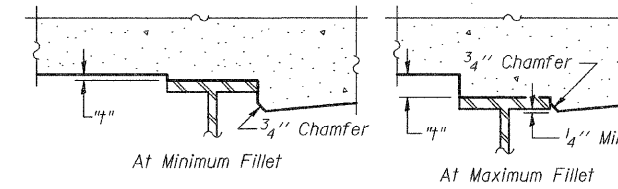


**DEAD LOAD DEFLECTION DIAGRAM**

(Includes weight of concrete only.)

Note:

The above deflections are not to be used in the field if the engineer is working from the grade elevations adjusted for dead load deflections as shown on Sheet Nos. 7 thru 9 of 28.



To determine "t": After all structural steel has been erected, elevations of the top flanges of the beams shall be taken at intervals shown on Sheet No. 6 of 28. These elevations subtracted from the "Theoretical Grade Elevations Adjusted for Dead Load Deflection" shown on Sheet Nos. 7 thru 9 of 28, minus slab thickness, equals the fillet heights "t" above top flange of beams.

**FILLET HEIGHTS**

**BEAM 5**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+05.55	9.13	437.22	437.22
☉ South Abut.	467+06.95	9.17	437.22	437.22
A	467+16.99	9.40	437.24	437.25
B	467+27.03	9.58	437.26	437.27
☉ Brg. Pier No. 1	467+40.71	9.75	437.28	437.28
C	467+50.76	9.82	437.29	437.30
D	467+60.80	9.83	437.30	437.32
E	467+70.85	9.80	437.31	437.32
F	467+80.90	9.72	437.31	437.32
☉ Brg. Pier No. 2	467+86.93	9.64	437.31	437.31
G	467+96.98	9.48	437.31	437.31
H	468+07.02	9.27	437.31	437.31
☉ North Abut.	468+20.68	8.90	437.30	437.30
Bk. North Abut.	468+22.08	8.86	437.29	437.29

**BEAM 6**

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Bk. South Abut.	467+08.42	15.37	436.87	436.87
☉ South Abut.	467+09.82	15.41	436.87	436.87
A	467+19.89	15.63	436.89	436.90
B	467+29.97	15.79	436.91	436.91
☉ Brg. Pier No. 1	467+43.68	15.94	436.93	436.93
C	467+53.76	15.99	436.94	436.95
D	467+63.84	15.99	436.95	436.96
E	467+73.92	15.95	436.95	436.97
F	467+84.00	15.85	436.95	436.96
☉ Brg. Pier No. 2	467+90.05	15.76	436.95	436.95
G	468+00.12	15.59	436.95	436.95
H	468+10.20	15.36	436.95	436.95
☉ North Abut.	468+23.90	14.97	436.94	436.94
Bk. North Abut.	468+25.30	14.93	436.93	436.93

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

**TOP OF SLAB ELEVATIONS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 10 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	34	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+64.97	-16.00	438.48
A	466+74.50	-16.45	438.56
B	466+84.03	-16.94	438.64
Bk. South Abut.	466+93.54	17.48	438.71

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+66.69	-12.00	438.26
A	466+76.44	-12.00	438.31
B	466+86.20	-12.00	438.36
Bk. South Abut.	466+95.99	-12.00	438.41

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+71.05	-1.91	437.70
A	466+81.04	-1.50	437.73
B	466+91.03	-1.14	437.75
Bk. South Abut.	467+01.02	-0.83	437.78

℄ ROADWAY & P.G.

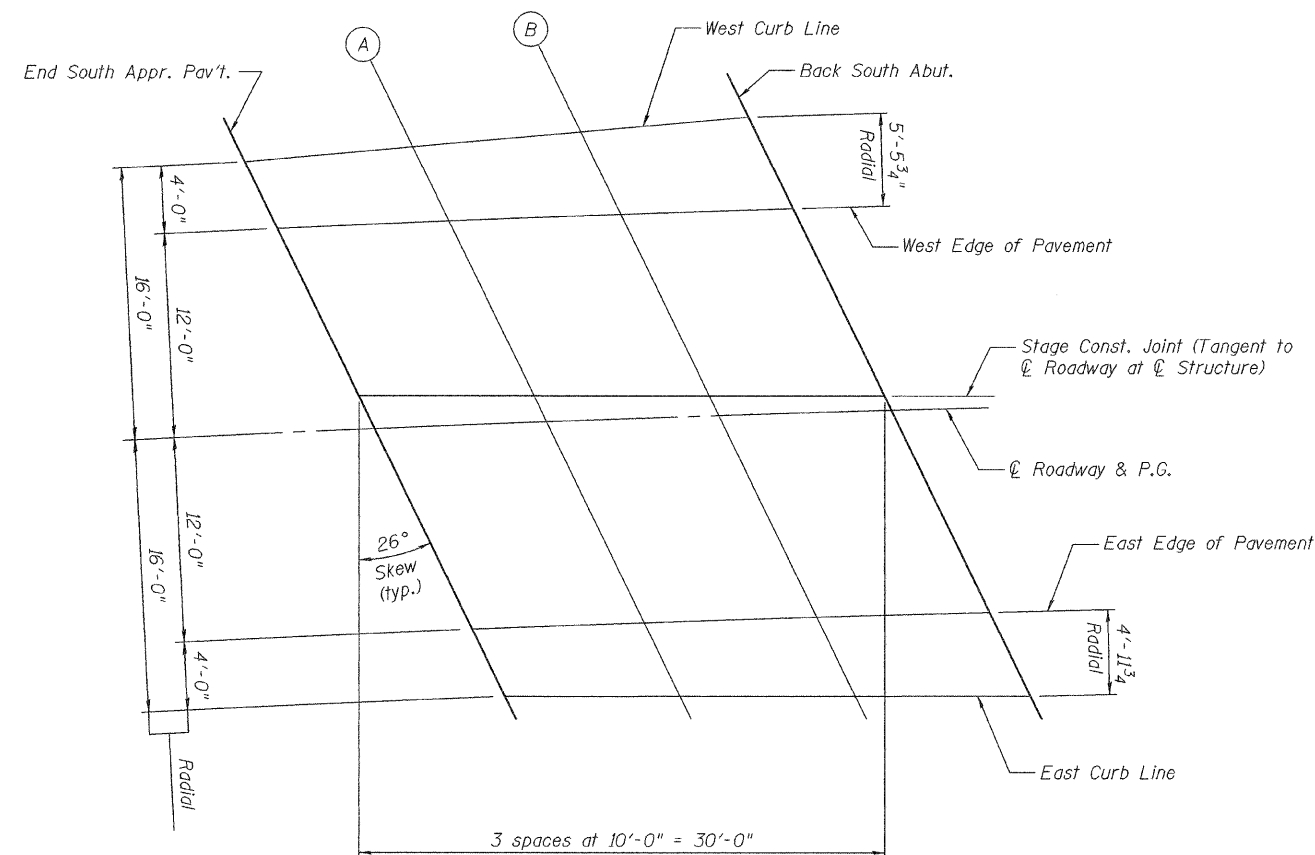
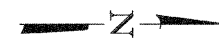
Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+71.88	0.00	437.59
A	466+81.70	0.00	437.64
B	466+91.53	0.00	437.69
Bk. South Abut.	467+01.39	0.00	437.73

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+77.15	12.00	436.92
A	466+87.03	12.00	436.97
B	466+96.94	12.00	437.02
Bk. South Abut.	467+06.86	12.00	437.06

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End South Appr. Pav't.	466+78.92	16.00	436.70
A	466+88.99	16.38	436.73
B	466+99.07	16.70	436.75
Bk. South Abut.	467+09.15	16.98	436.78



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



02/27/08

TOP OF SOUTH APPROACH  
SLAB ELEVATIONS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	68	35
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 11  
28 SHEETS

Contract #76948

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+08.58	-17.03	438.83
A	468+18.84	-16.63	438.78
B	468+29.10	-16.29	438.73
End North Appr. Pav't.	468+39.37	-16.00	438.68

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+11.17	-12.00	438.53
A	468+21.26	-12.00	438.51
B	468+31.37	-12.00	438.48
End North Appr. Pav't.	468+41.50	-12.00	438.44

STAGE CONSTRUCTION JOINT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+16.98	-0.83	437.87
A	468+26.97	-1.14	437.86
B	468+36.96	-1.50	437.85
End North Appr. Pav't.	468+46.94	-1.91	437.83

☉ ROADWAY & P.G.

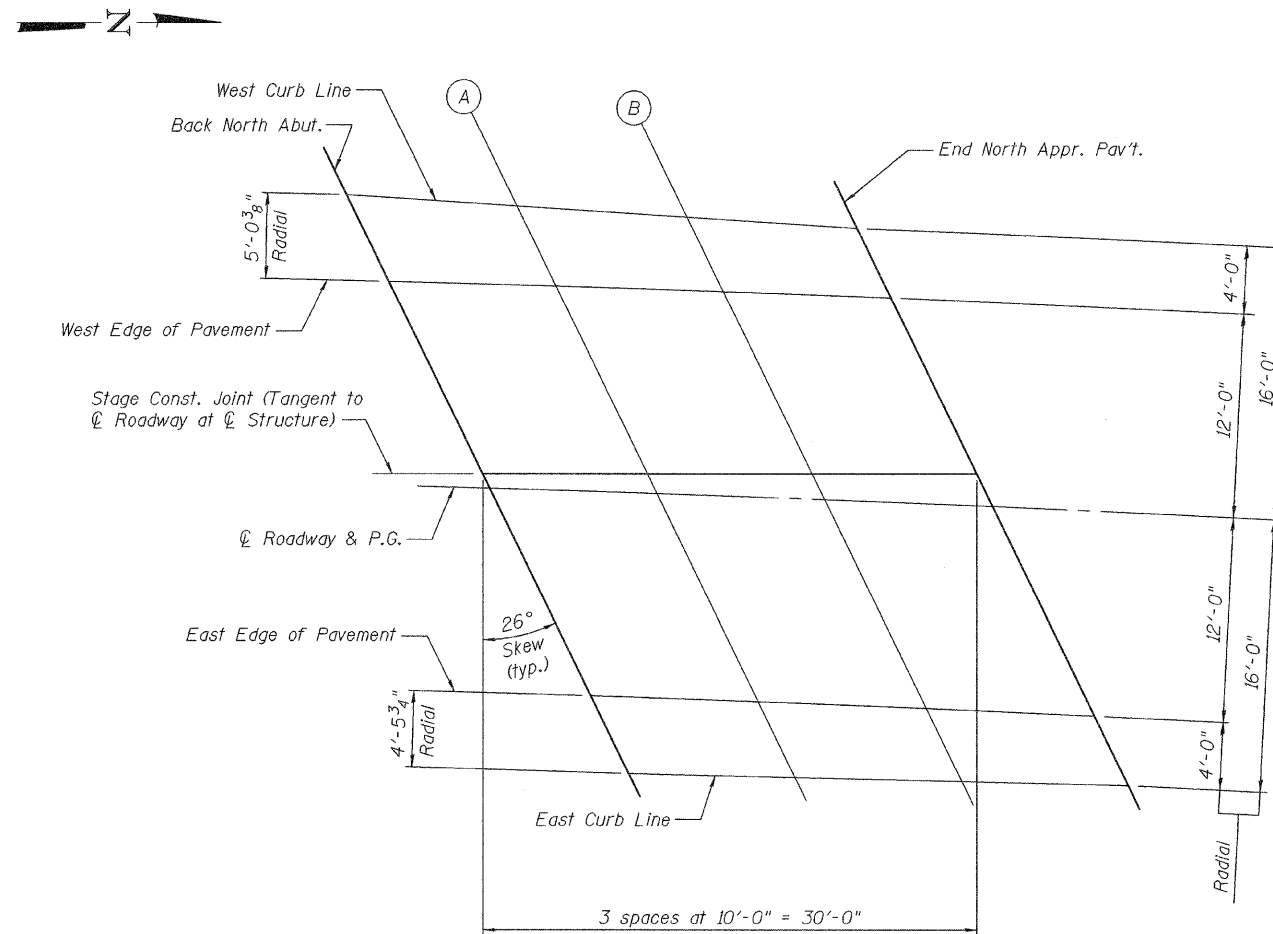
Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+17.42	0.00	437.82
A	468+27.58	0.00	437.79
B	468+37.76	0.00	437.76
End North Appr. Pav't.	468+47.98	0.00	437.72

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+23.75	12.00	437.11
A	468+33.98	12.00	437.08
B	468+44.25	12.00	437.04
End North Appr. Pav't.	468+54.54	12.00	436.99

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
Bk. North Abut.	468+26.13	16.48	436.84
A	468+36.34	16.37	436.81
B	468+46.55	16.21	436.79
End North Appr. Pav't.	468+56.75	16.00	436.75



PLAN

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



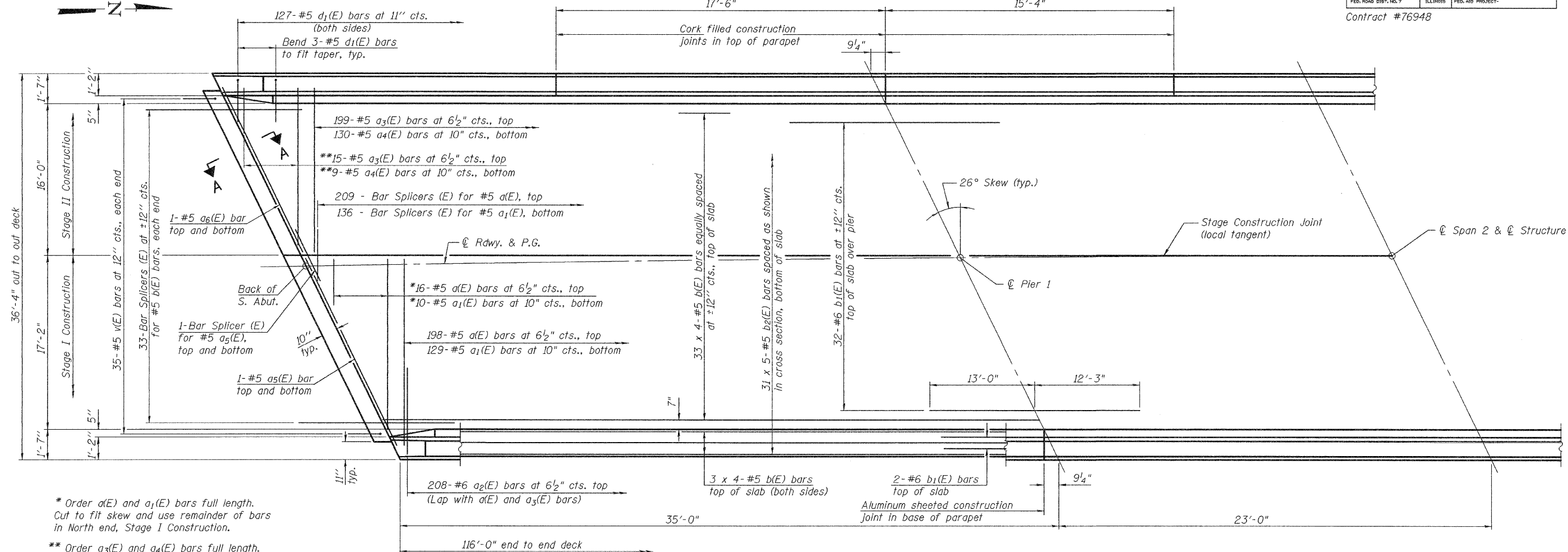
02/27/08

TOP OF NORTH APPROACH  
SLAB ELEVATIONS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 30	SHEET NO. 12 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT-	

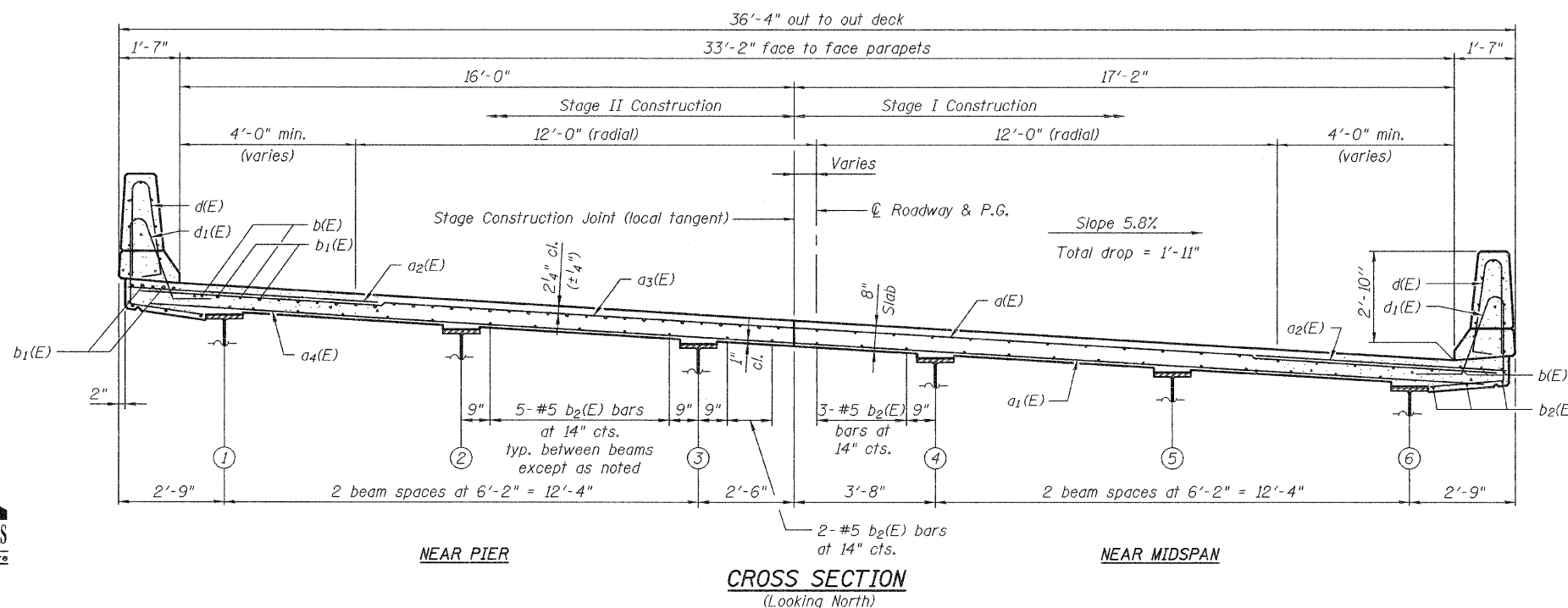
Contract #76948



\* Order a(E) and a<sub>1</sub>(E) bars full length. Cut to fit skew and use remainder of bars in North end, Stage I Construction.

\*\* Order a<sub>3</sub>(E) and a<sub>4</sub>(E) bars full length. Cut to fit skew and use remainder of bars in North end, Stage II Construction.

**PARTIAL PLAN**



**MINIMUM BAR LAP**  
(Slab)

#5 bar = 1'-8"

Notes:  
See Sheet 13 of 28 for superstructure details and Superstructure Bill of Material.  
Bars indicated thus 33 x 4-#5 etc. indicates 33 lines of bars with 4 lengths per line.  
See Sheet 13 of 28 for parapet reinforcement.  
For details of Section A-A, see Sheet No. 14 of 28.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



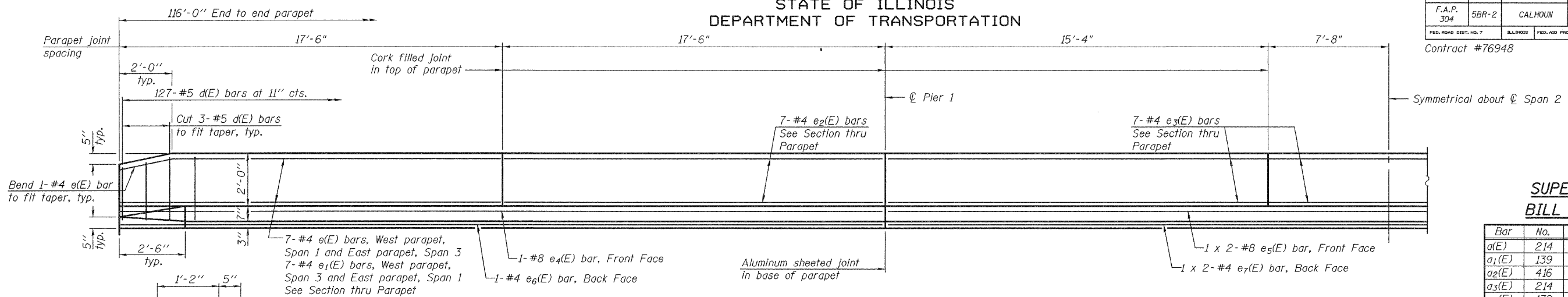
02/27/08

**SUPERSTRUCTURE**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

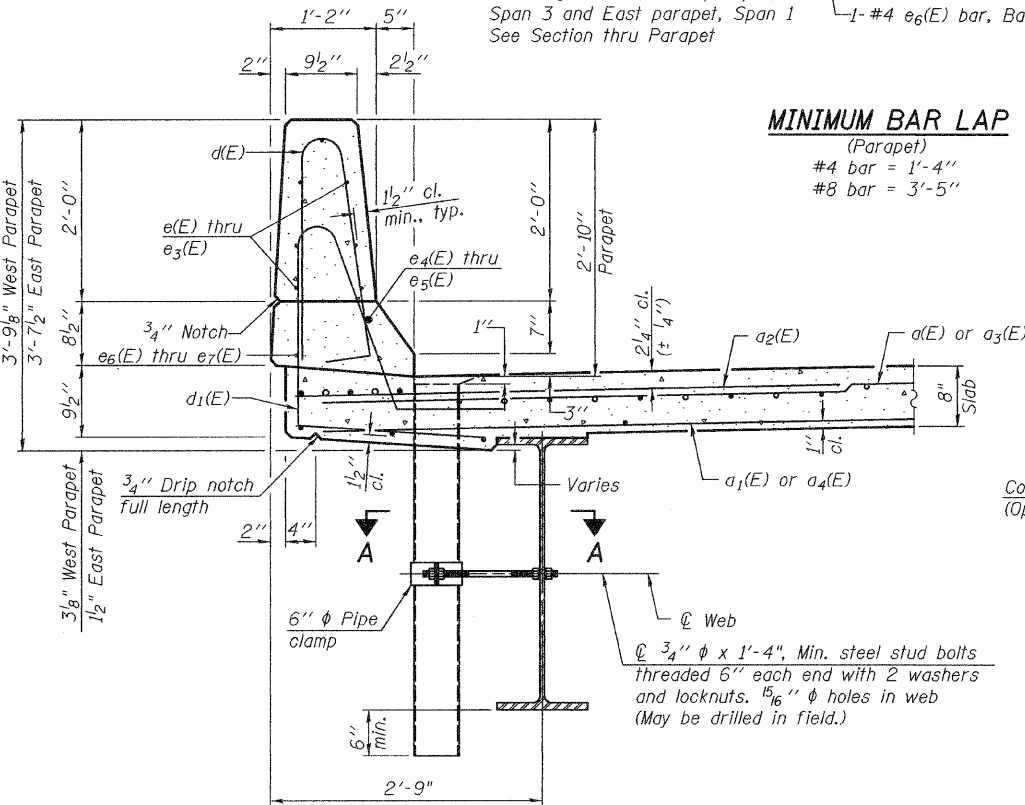
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 13
F.A.P. 304	5BR-2	CALHOUN	68	37	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948



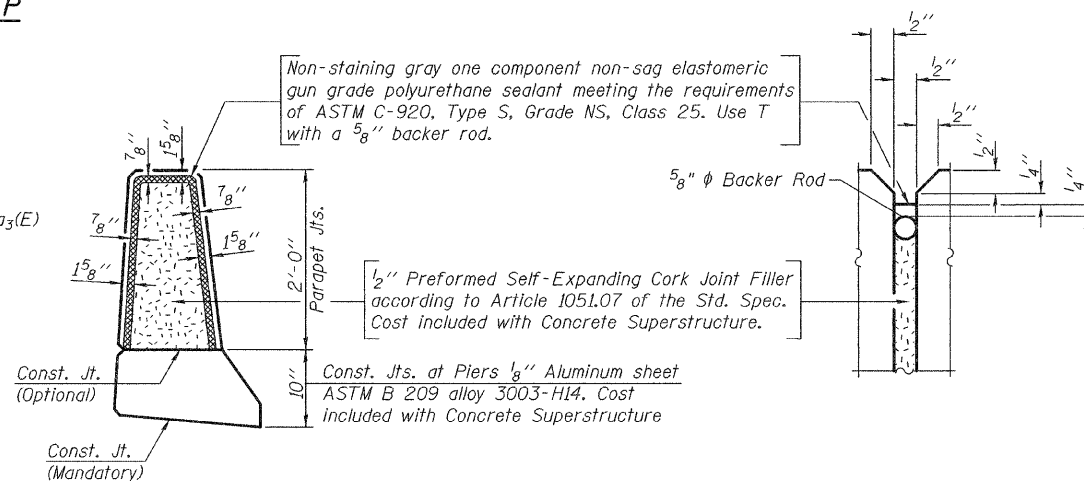
INSIDE ELEVATION OF PARAPET



SECTION THRU PARAPET

MINIMUM BAR LAP

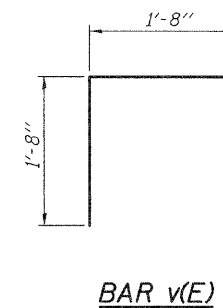
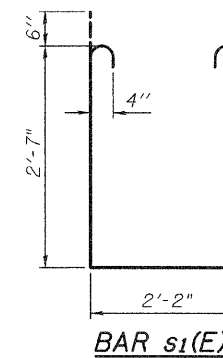
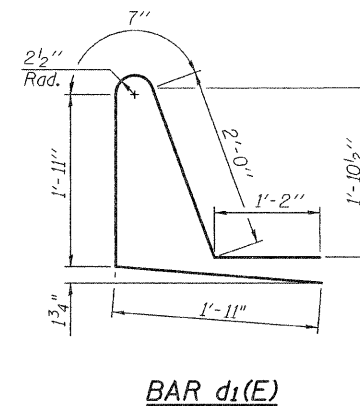
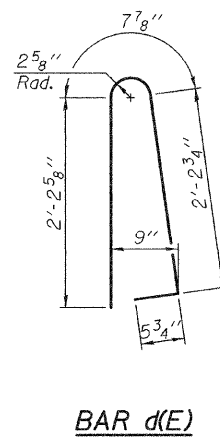
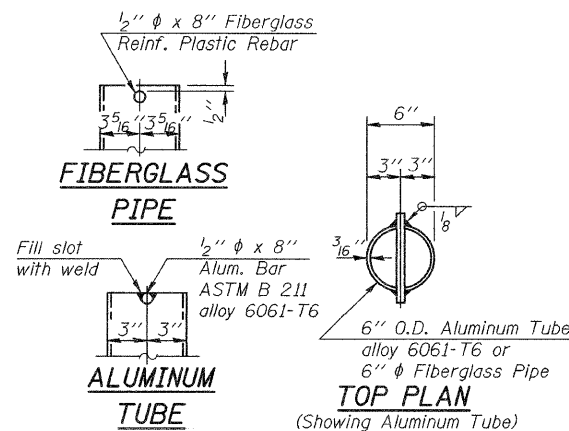
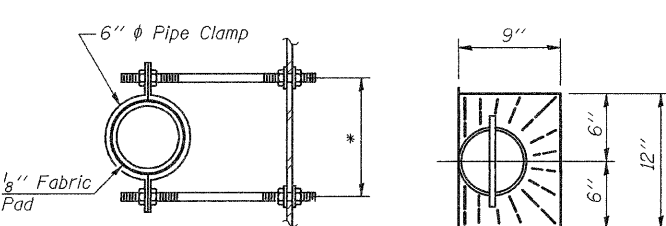
(Parapet)  
#4 bar = 1'-4"  
#8 bar = 3'-5"



PARAPET JOINT DETAILS

Notes:

All edges of concrete structures shall have a standard 3/4" chamfer, except as noted.  
The exterior surfaces of the floor drains shall be painted with the finish coat as specified in the special provisions for Cleaning and Painting New Metal Structures. The exterior surfaces of the drains shall be cleaned according to Steel Structures Painting Council's Spec. SSPC-SP1 prior to painting.  
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.  
The clamping device and inserts shall be galvanized according to AASHTO M 232.  
For concrete parapet slipforming option, see Sheet No. 26 of 28.



SUPERSTRUCTURE  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	214	#5	18'-2"	—
a <sub>1</sub> (E)	139	#5	17'-5"	—
a <sub>2</sub> (E)	416	#6	6'-0"	—
a <sub>3</sub> (E)	214	#5	16'-11"	—
a <sub>4</sub> (E)	139	#5	16'-2"	—
a <sub>5</sub> (E)	4	#5	19'-5"	—
a <sub>6</sub> (E)	4	#5	18'-0"	—
b(E)	156	#5	30'-3"	—
b <sub>1</sub> (E)	72	#6	25'-3"	—
b <sub>2</sub> (E)	155	#5	24'-6"	—
d(E)	254	#5	5'-7"	—
d <sub>1</sub> (E)	254	#5	7'-7"	—
e(E)	14	#4	17'-8"	—
e <sub>1</sub> (E)	14	#4	16'-10"	—
e <sub>2</sub> (E)	28	#4	17'-3"	—
e <sub>3</sub> (E)	42	#4	15'-1"	—
e <sub>4</sub> (E)	4	#8	34'-9"	—
e <sub>5</sub> (E)	4	#8	24'-7"	—
e <sub>6</sub> (E)	4	#4	34'-9"	—
e <sub>7</sub> (E)	4	#4	23'-7"	—
m(E)	4	#6	19'-5"	—
m <sub>1</sub> (E)	6	#6	20'-7"	—
m <sub>2</sub> (E)	12	#6	8'-9"	—
m <sub>3</sub> (E)	8	#6	6'-6"	—
m <sub>4</sub> (E)	4	#6	2'-9"	—
m <sub>5</sub> (E)	2	#6	3'-9"	—
m <sub>6</sub> (E)	4	#6	18'-1"	—
m <sub>7</sub> (E)	6	#6	19'-4"	—
m <sub>8</sub> (E)	12	#6	8'-4"	—
m <sub>9</sub> (E)	2	#6	2'-5"	—
s(E)	72	#5	6'-9"	—
s <sub>1</sub> (E)	62	#4	8'-4"	—
v(E)	70	#5	3'-4"	—
Reinforcement Bars, Epoxy Coated		Pound		35,670
Concrete Superstructure		Cu. Yds.		155.4

Bars indicated thus 1 x 2-#4 etc. indicates 1 line of bars with 2 lengths per line.  
For details of bar splicers, see Sheet No. 24 of 28.

SUPERSTRUCTURE DETAILS

IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM

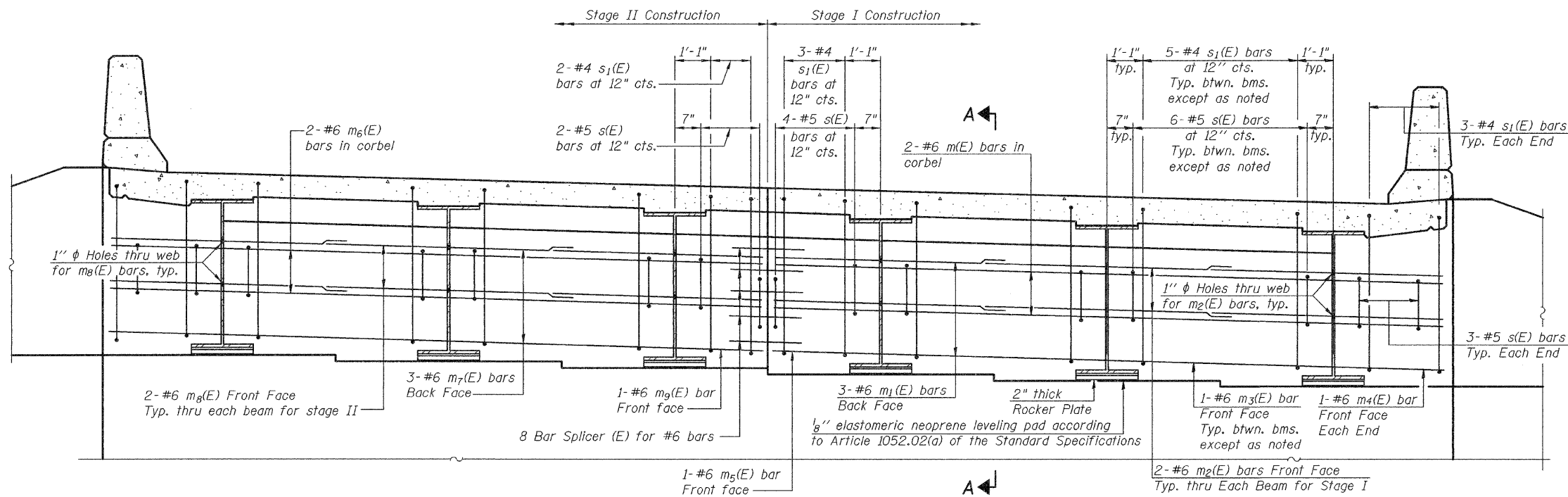


02/27/08

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 38	SHEET NO. 14 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS		FED. AID PROJECT-	

Contract #76948



**DIAPHRAGM ELEVATION AT ABUTMENT**

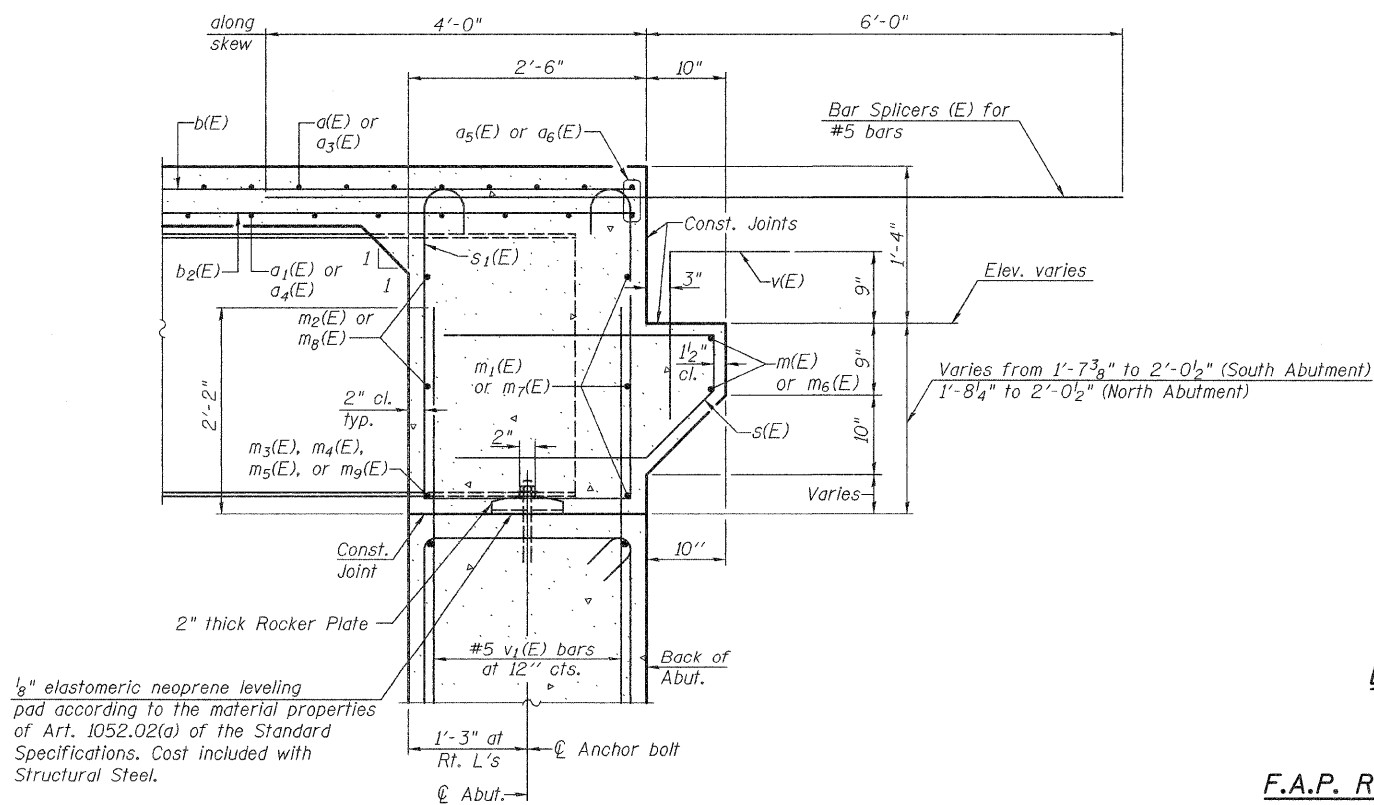
(North Abutment shown, looking North. South Abutment similar)

Notes:  
Reinforcement bars in diaphragm are billed with superstructure on Sheet No. 13 of 28.  
Concrete in diaphragm is included with Concrete Superstructure on Sheet No. 13 of 28.  
For details of bars s(E) & s<sub>1</sub>(E) see Sheet No. 13 of 28.  
The s(E) and s<sub>1</sub>(E) bars shall be placed parallel to the beams. Spacing for these bars shall be at right angles to the beams.

**MIN. BAR LAP**

(Diaphragms)

#6 bar = 2'-9"



**SECTION A-A**

Dimensions at right angles to abutment, except as shown.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	YSS



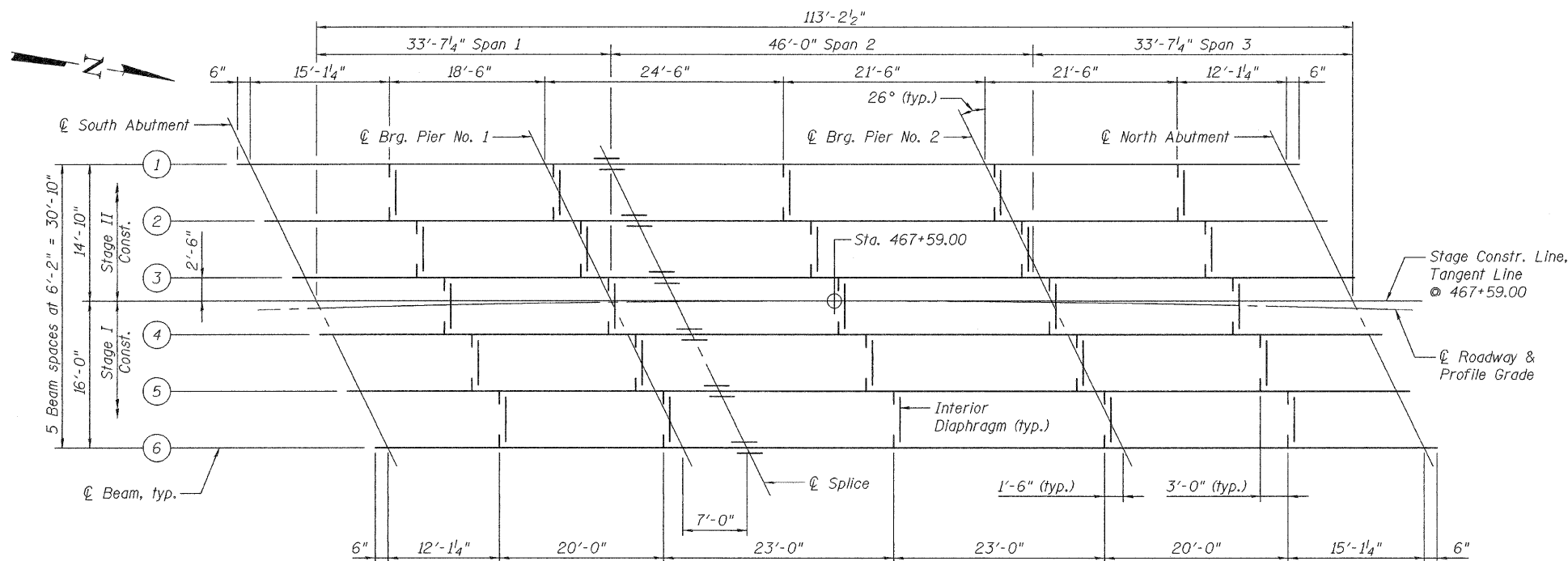
02/27/08

**DIAPHRAGM DETAILS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 39	SHEET NO. 15 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

Contract #76948



**FRAMING PLAN**

Note:  
For Beam Elevation, Field Splice and Interior Diaphragm Details, See Sheet No. 16 of 28.  
For Details of Diaphragms at the Abutments, see Sheet No. 14 of 28.

	0.4 Sp. 1 0.6 Sp. 3	Pier 1 or Pier 2	0.5 Sp. 2
$I_s$	(in <sup>4</sup> ) 2,850	2,850	2,850
$I_c(n)$	(in <sup>4</sup> ) 8,549	-	8,625
$I_c(3n)$	(in <sup>4</sup> ) 6,390	-	6,472
$S_s$	(in <sup>3</sup> ) 213	213	213
$S_c(n)$	(in <sup>3</sup> ) 331	-	332
$S_c(3n)$	(in <sup>3</sup> ) 300	-	301
Z	(in <sup>3</sup> ) -	-	-
DC1	(k/')	0.728	0.728
M <sub>DC1</sub>	(k)	51	73
DC2	(k/')	0.149	0.149
M <sub>DC2</sub>	(k)	11	15
DW	(k/')	0.308	0.308
M <sub>DW</sub>	(k)	22	31
M <sub>ℓ + imp</sub>	(k)	310	327
M <sub>u</sub> (Strength I)	(k)	653	729
φ <sub>r</sub> M <sub>n</sub> , φ <sub>r</sub> M <sub>nc</sub>	(k)	1,741	1,723
f <sub>s</sub> DC1	(ksi)	2.873	4.113
f <sub>s</sub> DC2	(ksi)	0.620	0.845
f <sub>s</sub> DW	(ksi)	0.880	1.236
f <sub>s</sub> 1.3(ℓ+I)	(ksi)	14.610	15.365
f <sub>s</sub> (Service II)	(ksi)	18.983	21.559
f <sub>s</sub> (Total)(Strength I)	(ksi)	25.354	28.735
V <sub>r</sub>	(k)	16.2	16.8

	S. Abut. or N. Abut.	Pier 1 or Pier 2
R <sub>DC1</sub>	(k) 8.7	32.5
R <sub>DC2</sub>	(k) 1.8	6.7
R <sub>DW</sub>	(k) 3.7	13.8
R <sub>ℓ + imp</sub>	(k) 56.3	80.0
R <sub>Total</sub>	(k) 117.2	209.7

- $I_s, S_s$ : Non-composite moment of inertia and section modulus of the steel section used for computing  $f_s$  (Total-Strength I, and Service II) due to non-composite dead loads (in<sup>4</sup> and in<sup>3</sup>).
- $I_c(n), S_c(n)$ : Composite moment of inertia and section modulus of the steel and deck based upon the modular ratio, "n", used for computing  $f_s$  (Total-Strength I, and Service II) due to short-term composite live loads (in<sup>4</sup> and in<sup>3</sup>).
- $I_c(3n), S_c(3n)$ : Composite moment of inertia and section modulus of the steel and deck based upon 3 times the modular ratio, "3n", used for computing  $f_s$  (Total-Strength I, and Service II) due to long-term composite (superimposed) dead loads (in<sup>4</sup> and in<sup>3</sup>).
- Z: Plastic section modulus of the steel section in non-composite areas. Omit line in Moment Table if not used in design calculations (in<sup>3</sup>).
- DC1: Un-factored non-composite dead load (kips/ft.).
- M<sub>DC1</sub>: Un-factored moment due to non-composite dead load (kip-ft.).
- DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).
- M<sub>DC2</sub>: Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).
- DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).
- M<sub>DW</sub>: Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).
- M<sub>ℓ + imp</sub>: Un-factored live load moment plus dynamic load allowance (impact)(kip-ft.).
- M<sub>u</sub> (Strength I): Factored design moment (kip-ft.).  
1.25 (M<sub>DC1</sub> + M<sub>DC2</sub>) + 1.5 M<sub>DW</sub> + 1.75 M<sub>ℓ + imp</sub>
- φ<sub>r</sub>M<sub>n</sub>: Compact composite positive moment capacity computed according to Article 6.10.7.1 (kip-ft.).
- φ<sub>r</sub>M<sub>nc</sub>: Non-Compact non-composite negative moment capacity computed according to Article A6.1.1 (kip-ft.).
- f<sub>s</sub> (Service II): Sum of stresses as computed from the moments below (ksi).  
M<sub>DC1</sub> + M<sub>DC2</sub> + M<sub>DW</sub> + 1.3 M<sub>ℓ + imp</sub>
- f<sub>s</sub> (Total)(Strength I): Sum of stresses as computed from the moments below on non-compact section (ksi).  
1.25 (M<sub>DC1</sub> + M<sub>DC2</sub>) + 1.5 M<sub>DW</sub> + 1.75 M<sub>ℓ + imp</sub>
- V<sub>r</sub>: Factored shear range computed according to Article 6.10.10.

Notes:  
All diaphragms shall be installed as steel is erected and secured with erection pins and bolts except as otherwise noted. Individual diaphragms at supports may be temporarily disconnected to install bearing anchor rods.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



**FRAMING PLAN AND DESIGN DATA**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

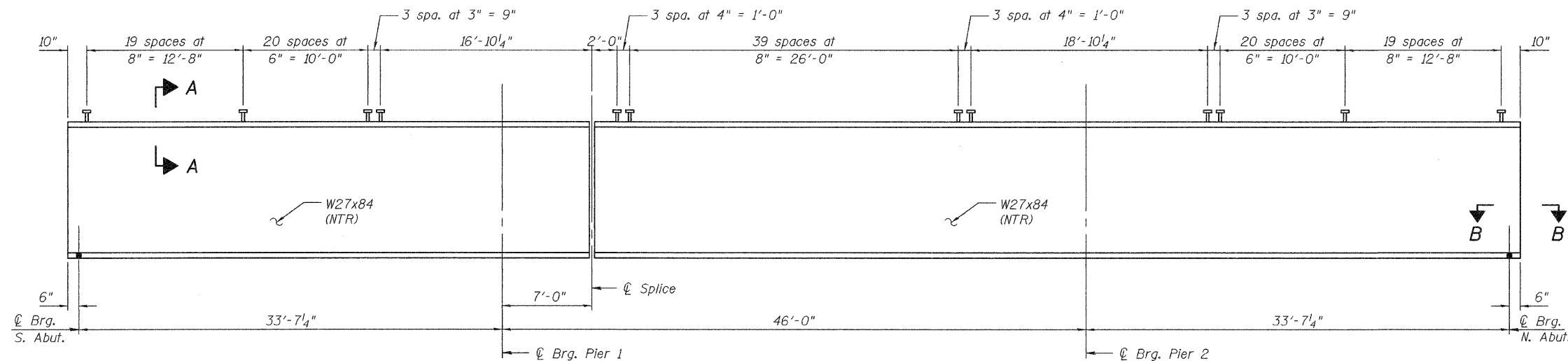
02/27/08



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 16
F.A.P. 304	5BR-2	CALHOUN	68	40	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT			

Contract #76948

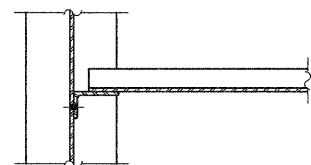


**BEAM ELEVATION**  
(6 Required)

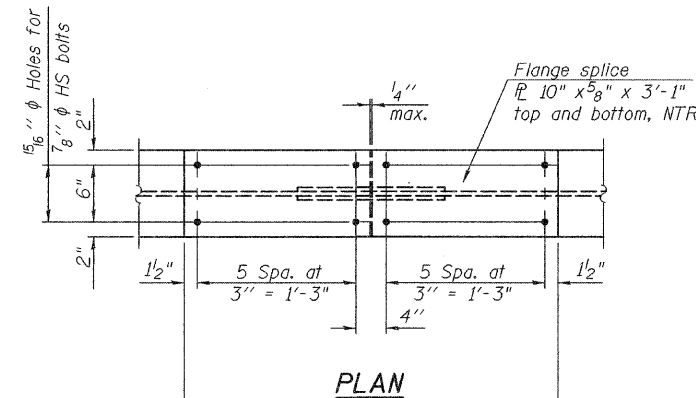
**\*TOP OF BEAM ELEVATIONS**

Location	Gdr. #1	Gdr. #2	Gdr. #3	Gdr. #4	Gdr. #5	Gdr. #6
☉ Brg. at South Abut.	437.87	437.52	437.16	436.81	436.46	436.11
☉ Brg. at Pier 1	437.93	437.58	437.23	436.87	436.52	436.17
☉ Splice	437.94	437.59	437.24	436.88	436.53	436.17
☉ Brg. at Pier 2	437.98	437.62	437.27	436.91	436.55	436.19
☉ Brg. at North Abut.	437.98	437.62	437.26	436.90	436.54	436.18

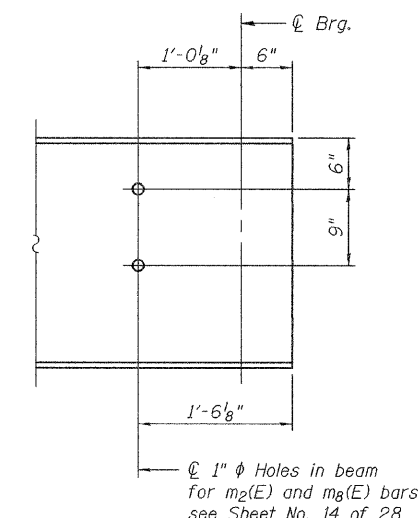
\* For fabrication only



**SECTION C-C**

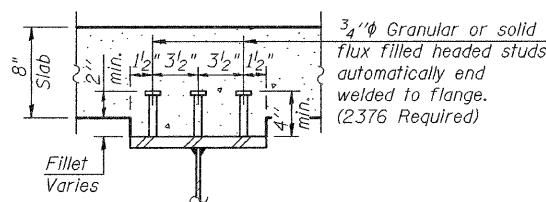


**PLAN**

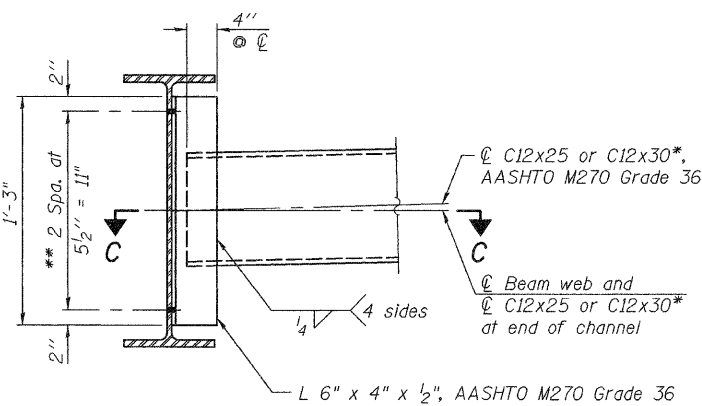


**END OF GIRDER ELEVATION**

(N. Abut. end shown  
S. Abut. end similar)

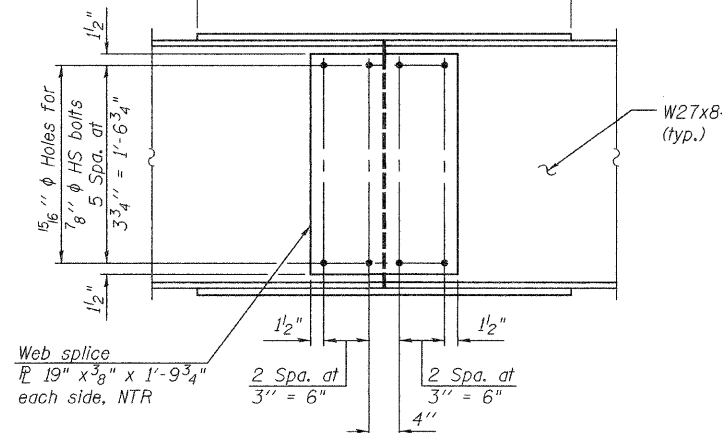


**SECTION A-A**



**INTERIOR DIAPHRAGM**  
(25 Required)

Notes:  
Two hardened washers required for each set of oversized holes with one washer on each side of the connection.  
See the Framing Plan on Sheet No. 15 of 28 for locations of interior diaphragms.  
\* Alternate channels are permitted to facilitate material acquisition. Calculated weight of structural steel is based on the lighter section.  
\*\* 3/4" φ HS bolts, 15/16" φ holes



**ELEVATION**

**SPLICE DETAIL**  
(6 Required)

Note:  
Load carrying components designated "NTR" shall conform to the Supplemental Requirements for Notch Toughness, Zone 2.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



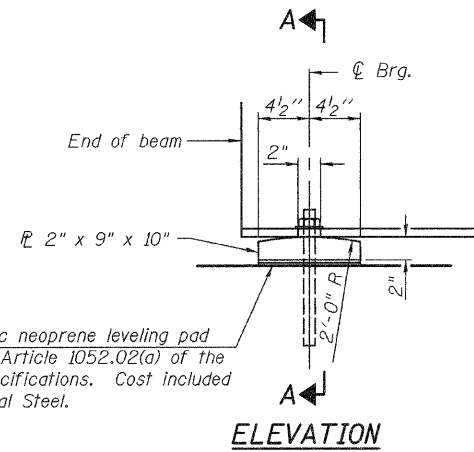
02/27/08

**BEAM DETAILS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 17
F.A.P. 304	5BR-2	CALHOUN	48	41	28 SHEETS
FED. ROAD DIST. NO. 7	ELLINOIS	FED. AID PROJECT-			

Contract #76948

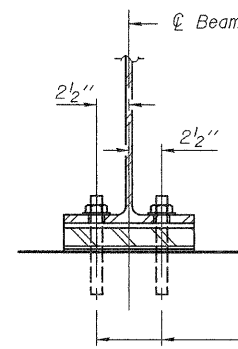


1/8" elastomeric neoprene leveling pad according to Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION

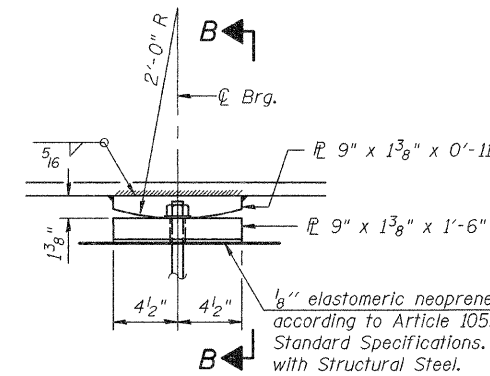
**FIXED BEARING AT ABUTMENT**

(12 Required)



1"  $\phi$  x 12" anchor bolts (Grade 36) with 2 1/4" x 2 1/4" x 5/16" PL washer under nut. 1 3/8" x 2" slotted hole in flange. 1/2"  $\phi$  holes in bearing plate.

SECTION A-A

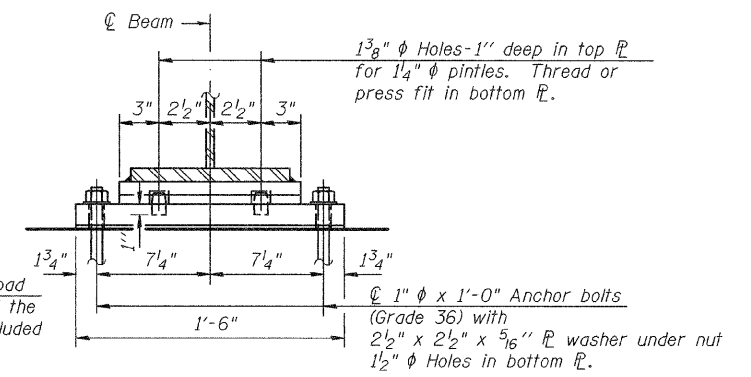


1/8" elastomeric neoprene leveling pad according to Article 1052.02(a) of the Standard Specifications. Cost included with Structural Steel.

ELEVATION

**FIXED BEARING AT PIER**

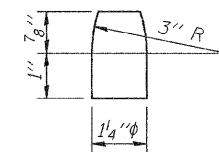
(12 Required)



1 3/8"  $\phi$  Holes-1" deep in top PL for 1/4"  $\phi$  pintles. Thread or press fit in bottom PL.

1"  $\phi$  x 1'-0" Anchor bolts (Grade 36) with 2 1/2" x 2 1/2" x 5/16" PL washer under nut. 1/2"  $\phi$  Holes in bottom PL.

SECTION B-B



PINTLE

(24 Required)

Notes:

Bearing plates and pintles shall be AASHTO M270 Grade 50.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

**BILL OF MATERIAL**

Item	Unit	Total
Anchor Bolts, 1"	Each	48

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	YSS



02/27/08

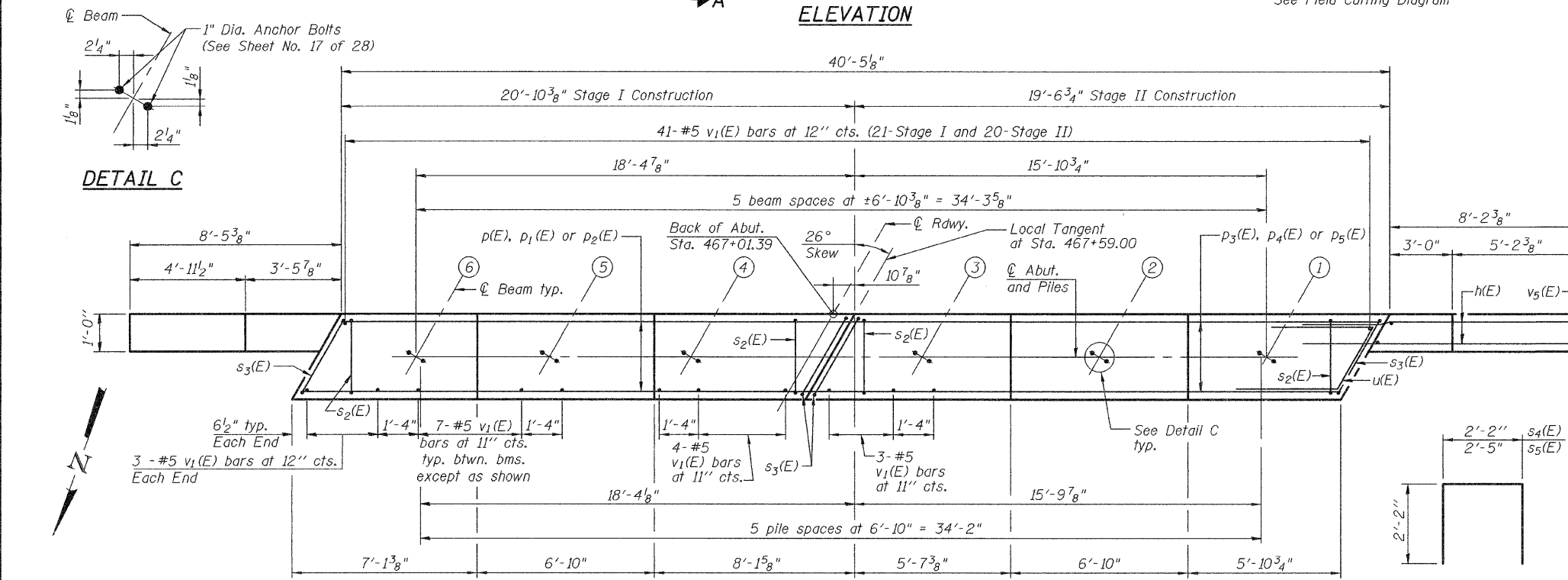
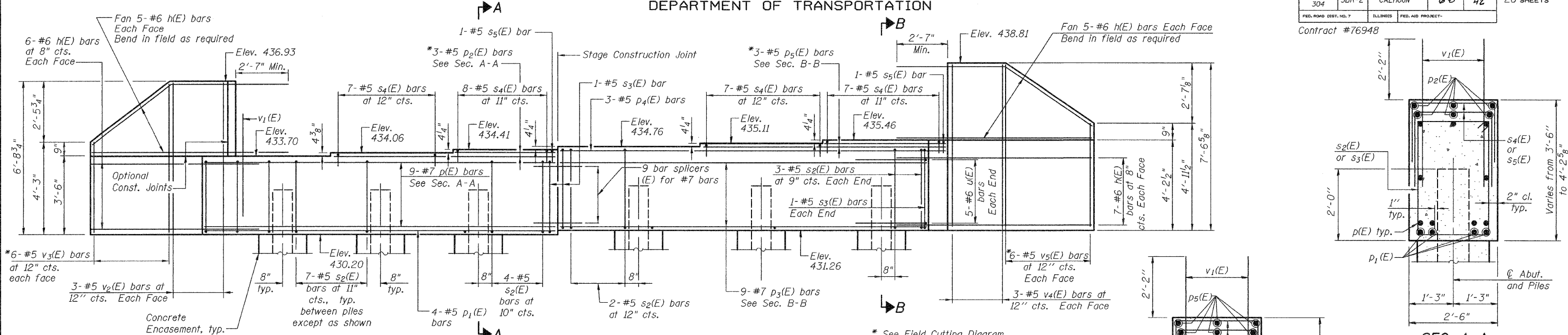
**BEARING DETAILS**  
**IL. ROUTE 100 OVER**  
**CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

Notes: Four steps monolithically with cap.  
Space reinforcement in cap to miss anchor bolts.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 18 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	42	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

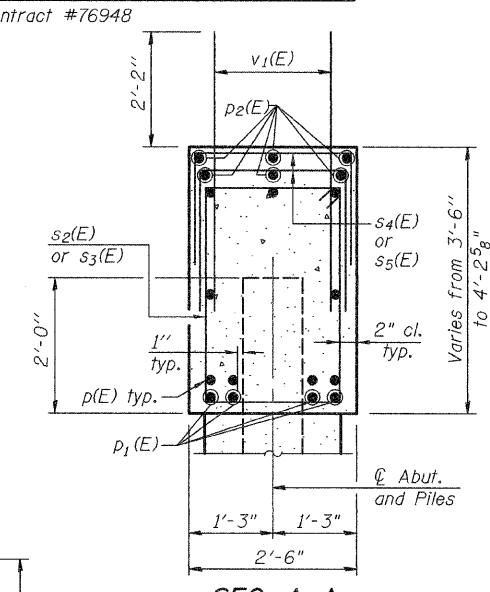
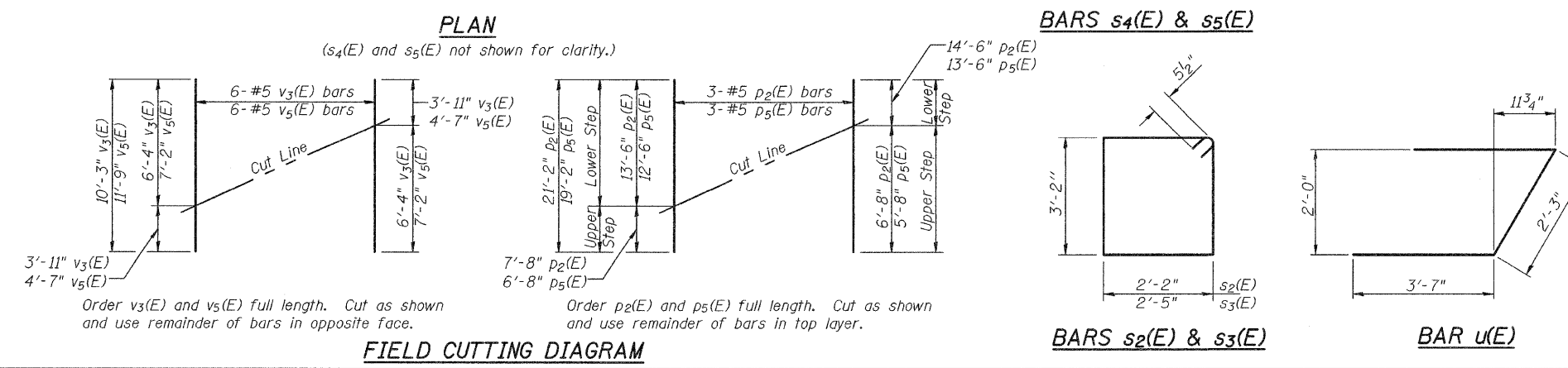
Contract #76948



**PILE DATA**

Type: Steel HP 12x53  
Nominal Required Bearing: 340 kips  
Factored Resistance Available: 170 kips  
Est. Length: 53 ft  
No. Production Piles: 5  
No. Test Piles: 1

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	YSS



**SEC. A-A**

**BILL OF MATERIAL**

Bar No.	Size	Length	Shape
h(E)	46 #6	11'-7"	
p(E)	9 #7	20'-6"	
p1(E)	4 #5	20'-6"	
p2(E)	3 #5	21'-2"	
p3(E)	9 #7	19'-3"	
p4(E)	3 #5	19'-3"	
p5(E)	3 #5	19'-2"	
s2(E)	40 #5	11'-7"	
s3(E)	4 #5	12'-1"	
s4(E)	29 #5	6'-6"	
s5(E)	2 #5	6'-9"	
u(E)	10 #6	9'-5"	
v1(E)	82 #5	4'-4"	
v2(E)	6 #5	6'-5"	
v3(E)	6 #5	10'-3"	
v4(E)	6 #5	7'-3"	
v5(E)	6 #5	11'-9"	

Structure Excavation	Cu. Yd.	159
Concrete Structures	Cu. Yd.	18.4
Reinforcement Bars, Epoxy Coated	Pound	3,290
Furnishing Steel Piles HP 12x53	Foot	265
Driving Piles	Foot	265
Test Pile Steel HP 12x53	Each	1
Concrete Encasement	Cu. Yd.	2.1

For details of Bar Splicers, see Sheet No. 24 of 28.  
For details of piles and Concrete Encasement, see Sheet No. 23 of 28.

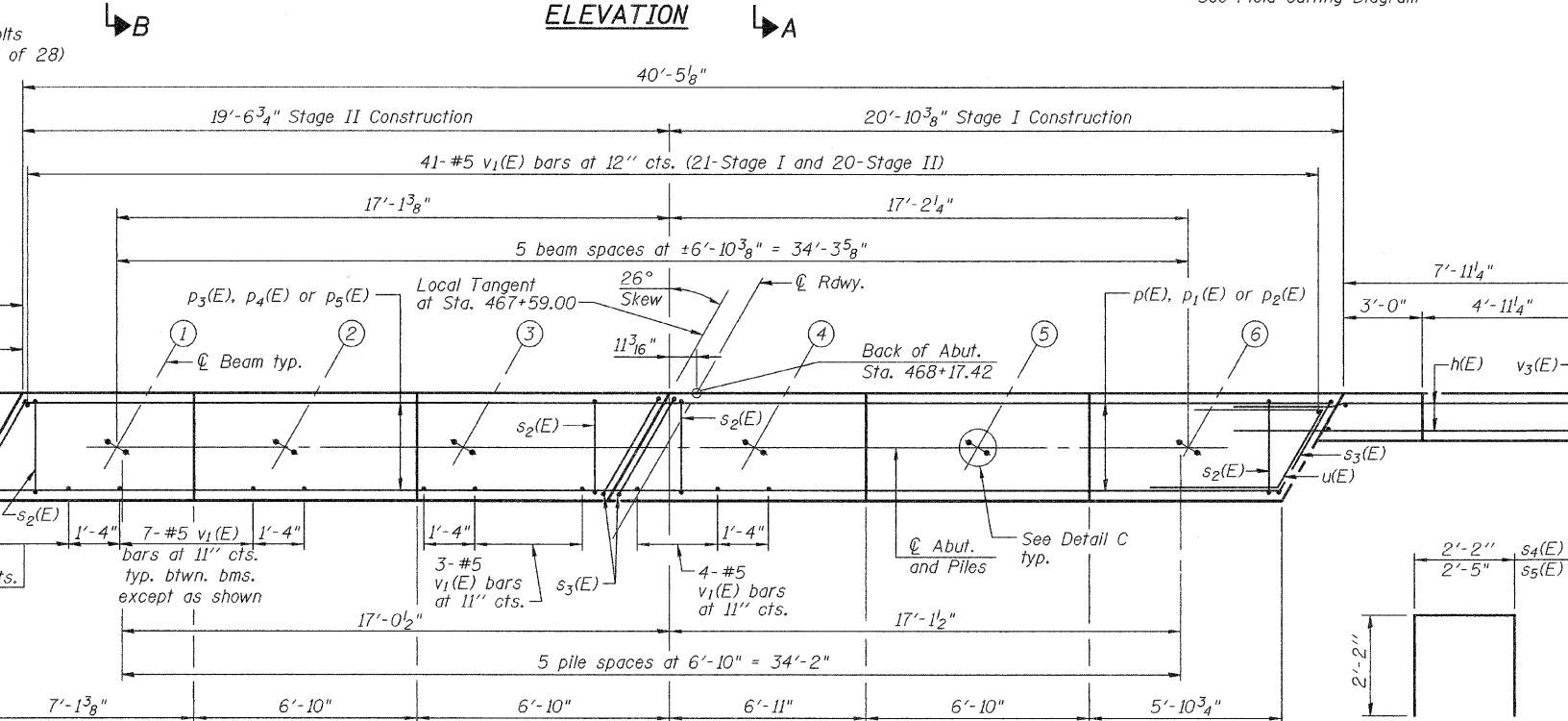
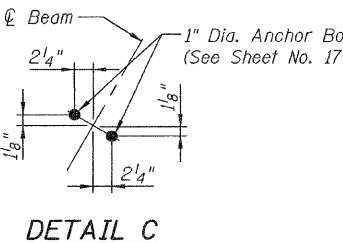
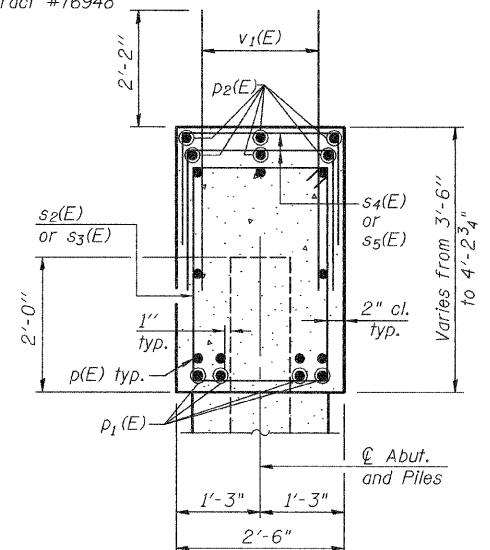
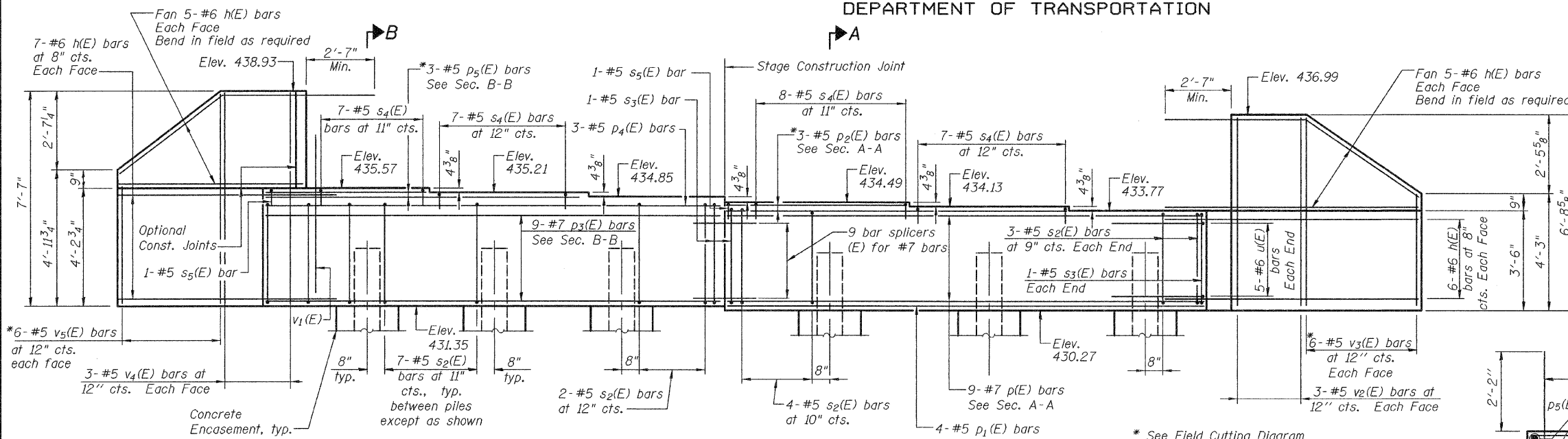
**SOUTH ABUTMENT DETAILS**  
**IL. ROUTE 100 OVER CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

Notes: Four steps monolithically with cap.  
Space reinforcement in cap to miss anchor bolts.

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 19
F.A.P. 304	5BR-2	CALHOUN	48	43	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948



PILE DATA

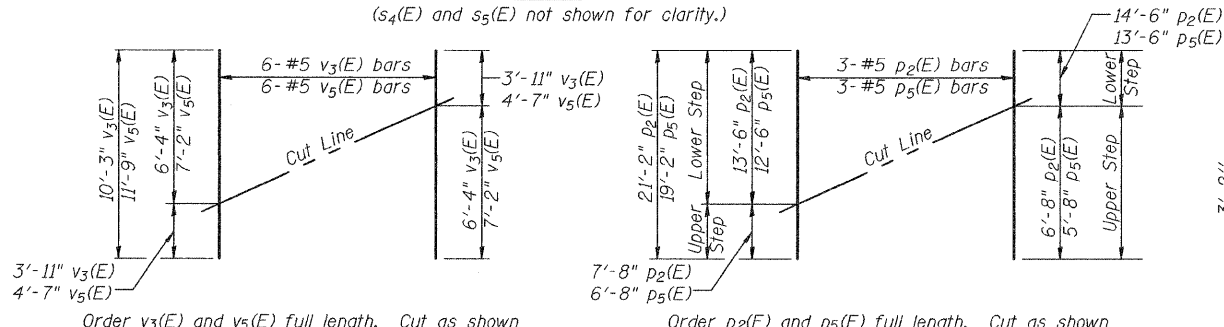
Type: Steel HP 12x53  
Nominal Required Bearing: 340 kips  
Factored Resistance Available: 170 kips  
Est. Length: 62 ft  
No. Production Piles: 5  
No. Test Piles: 1

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	YSS

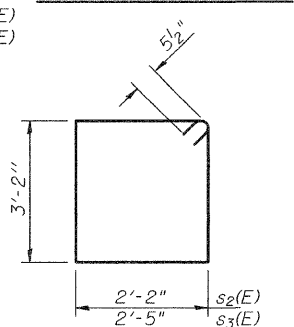


02/27/08

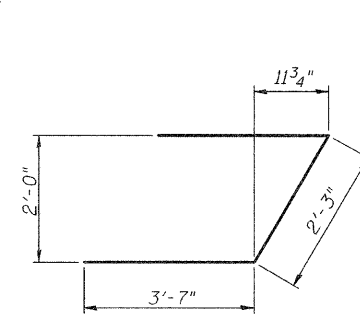
PLAN



BARS s4(E) & s5(E)



BARS s2(E) & s3(E)



BAR u(E)



SEC. A-A  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h(E)	46	#6	11'-7"	—
p(E)	9	#7	20'-6"	—
p1(E)	4	#5	20'-6"	—
p2(E)	3	#5	21'-2"	—
p3(E)	9	#7	19'-3"	—
p4(E)	3	#5	19'-3"	—
p5(E)	3	#5	19'-2"	—
s2(E)	40	#5	11'-7"	□
s3(E)	4	#5	12'-1"	□
s4(E)	29	#5	6'-6"	□
s5(E)	2	#5	6'-9"	□
u(E)	10	#6	9'-5"	└
v1(E)	82	#5	4'-4"	—
v2(E)	6	#5	6'-5"	—
v3(E)	6	#5	10'-3"	—
v4(E)	6	#5	7'-3"	—
v5(E)	6	#5	11'-9"	—

Structure Excavation	Cu. Yd.	158
Concrete Structures	Cu. Yd.	18.5
Reinforcement Bars, Epoxy Coated	Pound	3,290
Furnishing Steel Piles HP 12x53	Foot	310
Driving Piles	Foot	310
Test Pile Steel HP 12x53	Each	1
Concrete Encasement	Cu. Yd.	2.1

For details of Bar Splicers, see Sheet No. 24 of 28.  
For details of piles and Concrete Encasement, see Sheet No. 23 of 28.

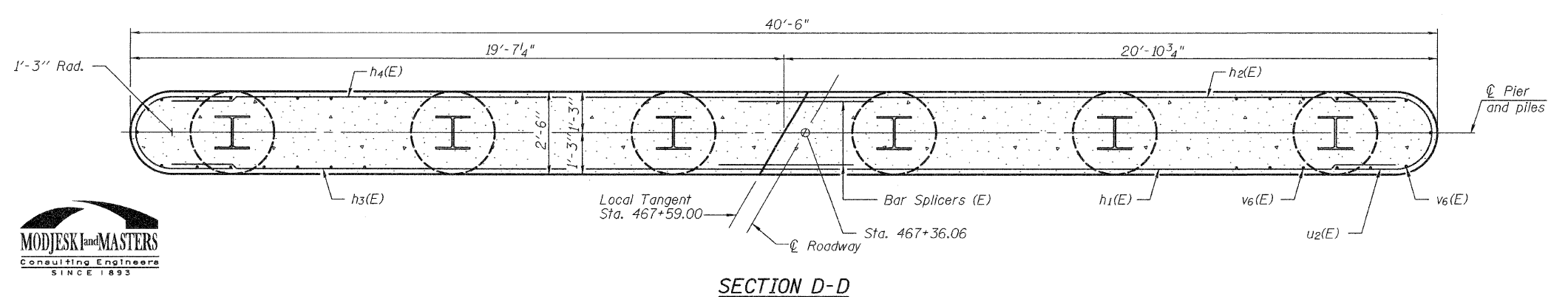
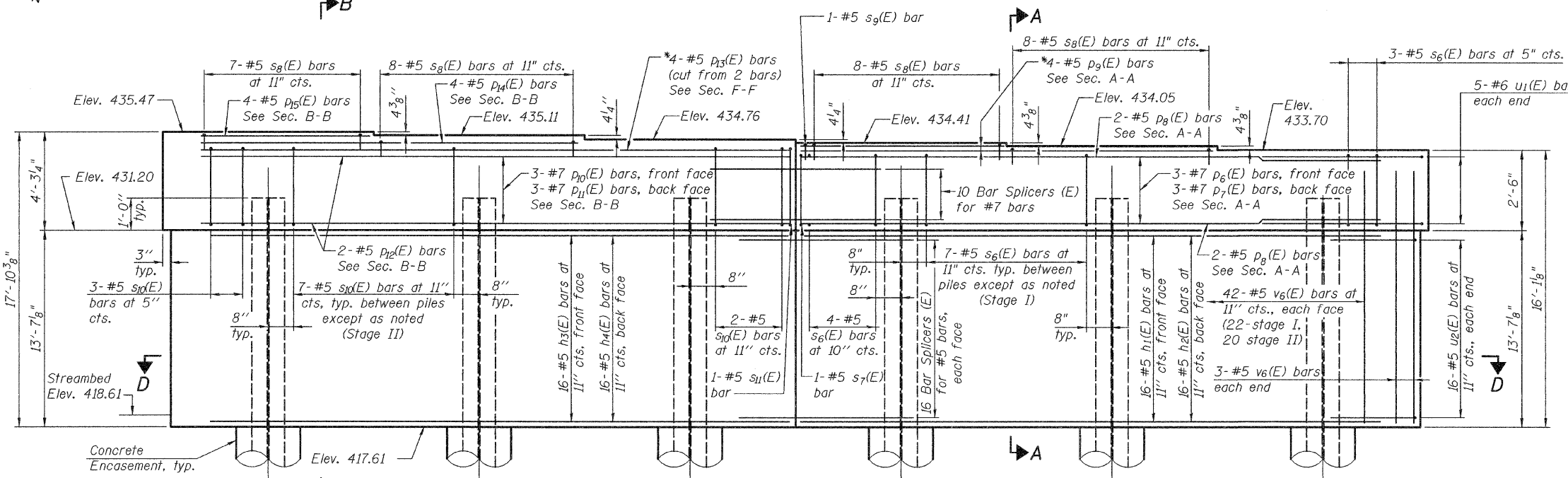
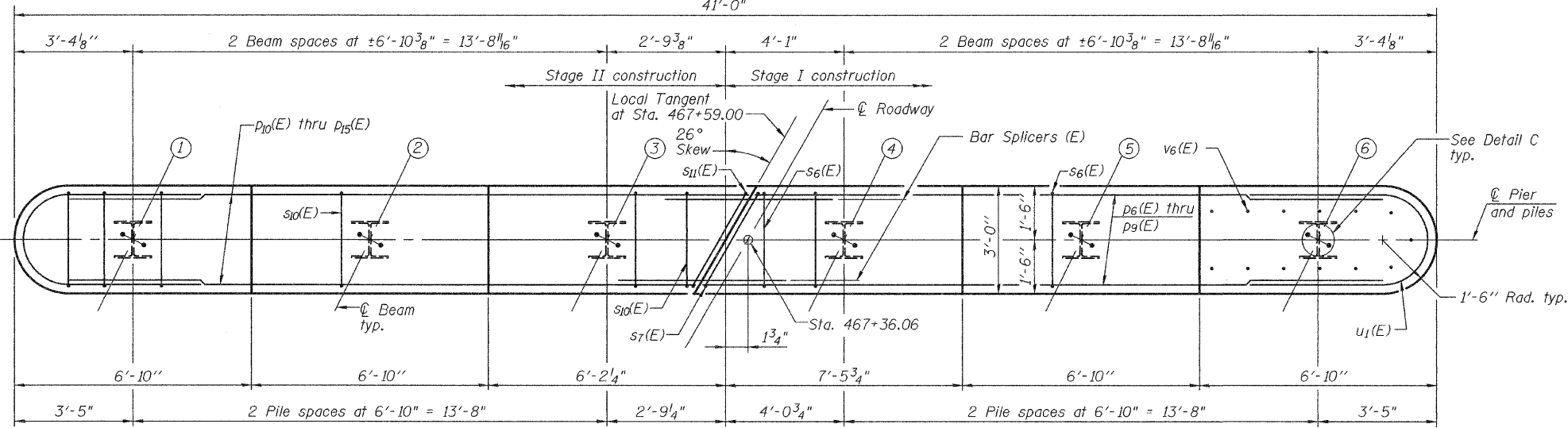
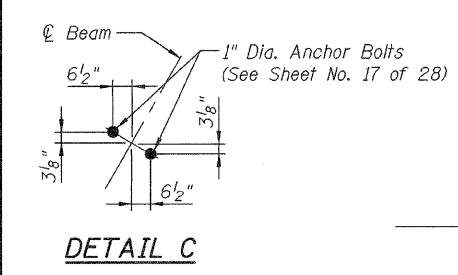
**NORTH ABUTMENT DETAILS**  
**IL. ROUTE 100 OVER CRAWFORD CREEK**  
**F.A.P. ROUTE 304 - SECTION 5BR-2**  
**CALHOUN COUNTY**  
**STATION 467+59.00**  
**STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 20
F.A.P. 304	5BR-2	CALHOUN	68	44	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948

Notes:  
Pour steps monolithically with cap.  
Space reinforcement in cap to miss anchor bolts.



BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h <sub>1</sub> (E)	16	#5	20'-0"	—
h <sub>2</sub> (E)	16	#5	19'-0"	—
h <sub>3</sub> (E)	16	#5	17'-9"	—
h <sub>4</sub> (E)	16	#5	18'-9"	—
p <sub>6</sub> (E)	3	#7	20'-1"	—
p <sub>7</sub> (E)	3	#7	18'-11"	—
p <sub>8</sub> (E)	4	#7	19'-4"	—
p <sub>9</sub> (E)	*4	#5	21'-4"	—
p <sub>10</sub> (E)	3	#7	17'-7"	—
p <sub>11</sub> (E)	3	#7	18'-10"	—
p <sub>12</sub> (E)	4	#7	18'-10"	—
p <sub>13</sub> (E)	*2	#5	36'-5"	—
p <sub>14</sub> (E)	4	#5	12'-1"	—
p <sub>15</sub> (E)	4	#5	5'-3"	—
s <sub>6</sub> (E)	21	#5	10'-7"	□
s <sub>7</sub> (E)	1	#5	11'-3"	□
s <sub>8</sub> (E)	31	#5	7'-0"	□
s <sub>9</sub> (E)	1	#5	7'-4"	□
s <sub>10</sub> (E)	19	#5	12'-9"	□
s <sub>11</sub> (E)	1	#5	13'-5"	□
u <sub>1</sub> (E)	10	#6	11'-1"	U
u <sub>2</sub> (E)	32	#5	10'-7"	U
v <sub>6</sub> (E)	90	#5	15'-1"	—
Structure Excavation			Cu. Yd.	70
Concrete Structures			Cu. Yd.	65.5
Reinforcement Bars, Epoxy Coated			Pound	4,950
Furnishing Steel Piles HP 14x89			Foot	310
Driving Piles			Foot	310
Test Pile Steel HP 14x89			Each	1
Concrete Encasement Underwater Structure			Cu. Yd.	3.3
Excavation Protection Location #1			Each	1

For details of bar splicer, see Sheet No. 24 of 28.  
For details of piles and concrete encasement, see Sheet No. 23 of 28.  
For details of Section A-A and B-B, see Sheet No. 22 of 28.  
For bar dimensions, see Sheet No. 22 of 28.

PILE DATA

Type: Steel HP 14x89  
Nominal Required Bearing: 600 kips  
Allowable Resistance Available: 240 kips  
Est. Length: 62 ft  
No. Production Piles: 5  
No. Test Piles: 1

PIER 1 DETAILS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



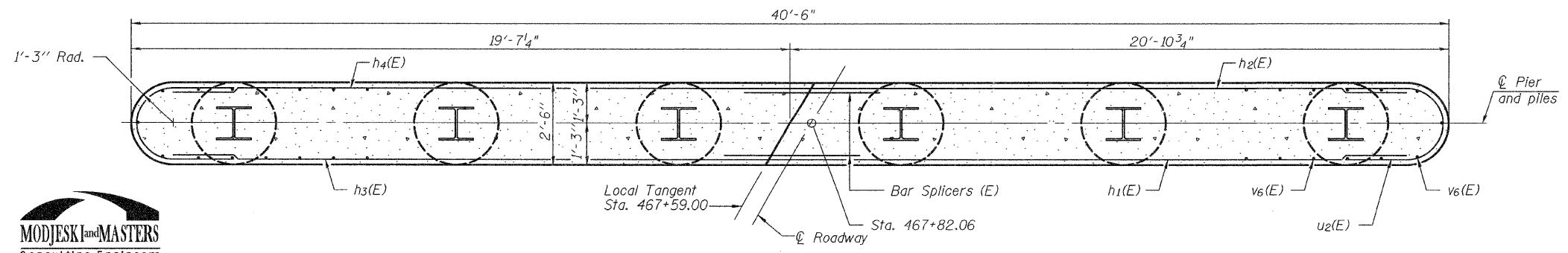
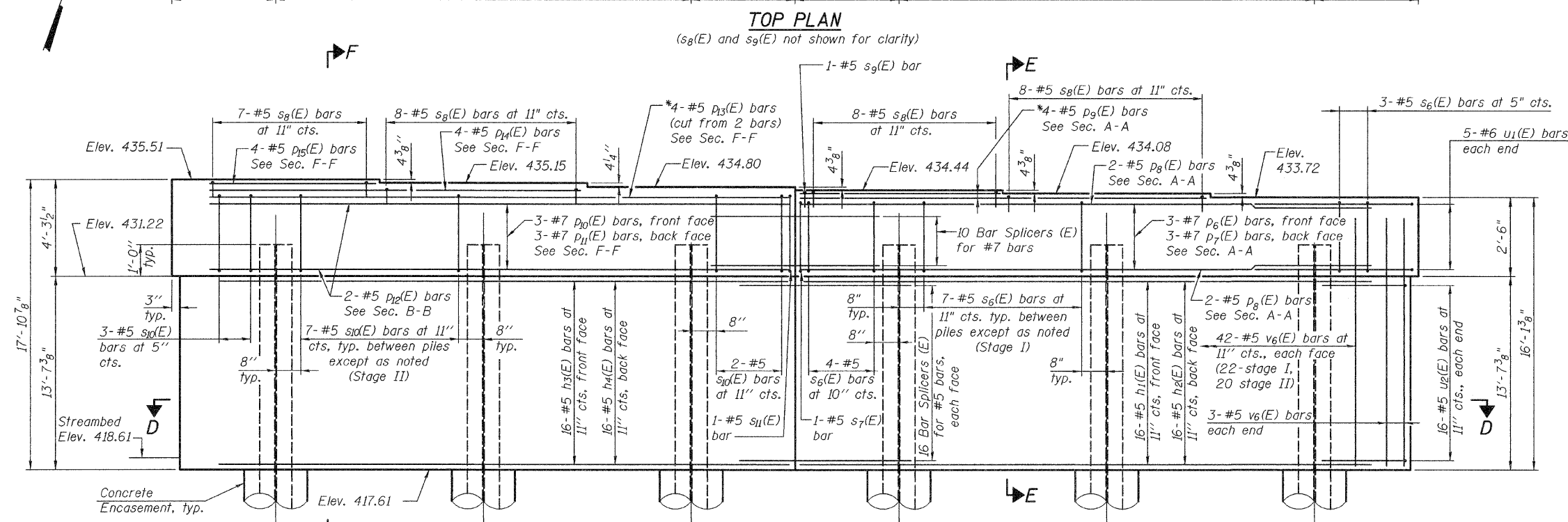
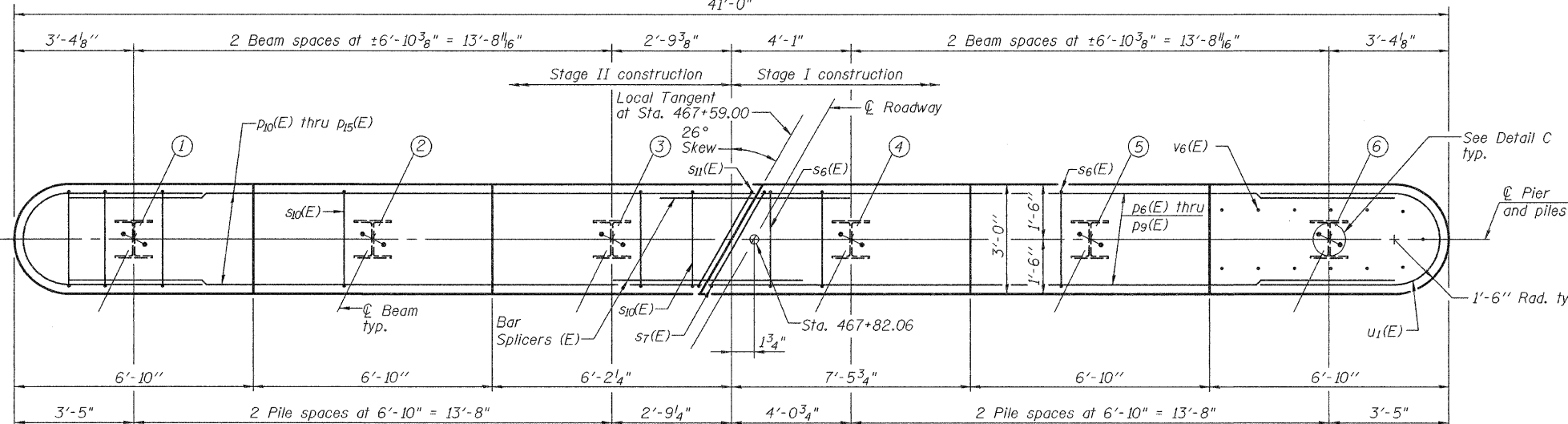
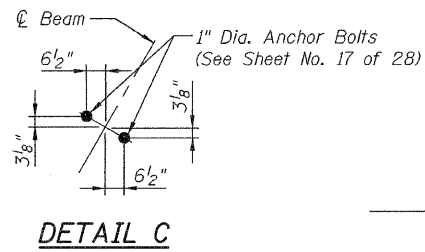
02/27/08

Notes:  
 Pour steps monolithically with cap.  
 Space reinforcement in cap to miss anchor bolts.

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 41'-0"

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 21
F.A.P. 304	5BR-2	CALHOUN	48	45	28 SHEETS
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-			

Contract #76948



**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h <sub>1</sub> (E)	16	#5	20'-0"	—
h <sub>2</sub> (E)	16	#5	19'-0"	—
h <sub>3</sub> (E)	16	#5	17'-9"	—
h <sub>4</sub> (E)	16	#5	18'-9"	—
p <sub>6</sub> (E)	3	#7	20'-1"	—
p <sub>7</sub> (E)	3	#7	18'-11"	—
p <sub>8</sub> (E)	4	#7	19'-4"	—
p <sub>9</sub> (E)	*4	#5	21'-4"	—
p <sub>10</sub> (E)	3	#7	17'-7"	—
p <sub>11</sub> (E)	3	#7	18'-10"	—
p <sub>12</sub> (E)	4	#7	18'-0"	—
p <sub>13</sub> (E)	*2	#5	36'-5"	—
p <sub>14</sub> (E)	4	#5	12'-1"	—
p <sub>15</sub> (E)	4	#5	5'-3"	—
s <sub>6</sub> (E)	21	#5	10'-7"	□
s <sub>7</sub> (E)	1	#5	11'-3"	□
s <sub>8</sub> (E)	31	#5	7'-0"	□
s <sub>9</sub> (E)	1	#5	7'-4"	□
s <sub>10</sub> (E)	19	#5	12'-9"	□
s <sub>11</sub> (E)	1	#5	13'-5"	□
u <sub>1</sub> (E)	10	#6	11'-1"	U
u <sub>2</sub> (E)	32	#5	10'-7"	U
v <sub>6</sub> (E)	90	#5	15'-1"	—
Structure Excavation		Cu. Yd.	37	
Concrete Structures		Cu. Yd.	65.6	
Reinforcement Bars, Epoxy Coated		Pound	4,950	
Furnishing Steel Piles HP 14x89		Foot	310	
Driving Piles		Foot	310	
Test Pile Steel HP 14x89		Each	1	
Concrete Encasement Underwater Structure Excavation Protection-Location #2		Cu. Yd.	3.3	
		Each	1	

For details of bar splicer, see Sheet No. 24 of 28.  
 For details of piles and concrete encasement, see Sheet No. 23 of 28.  
 For details of Section E-E and F-F, see Sheet No. 22 of 28.  
 For bar dimensions, see Sheet No. 22 of 28.

**PILE DATA**

Type: Steel HP 14x89  
 Nominal Required Bearing: 600 kips  
 Allowable Resistance Available: 240 kips  
 Est. Length: 62 ft  
 No. Production Piles: 5  
 No. Test Piles: 1

**PIER 2 DETAILS**  
 IL. ROUTE 100 OVER  
 CRAWFORD CREEK  
 F.A.P. ROUTE 304 - SECTION 5BR-2  
 CALHOUN COUNTY  
 STATION 467+59.00  
 STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM

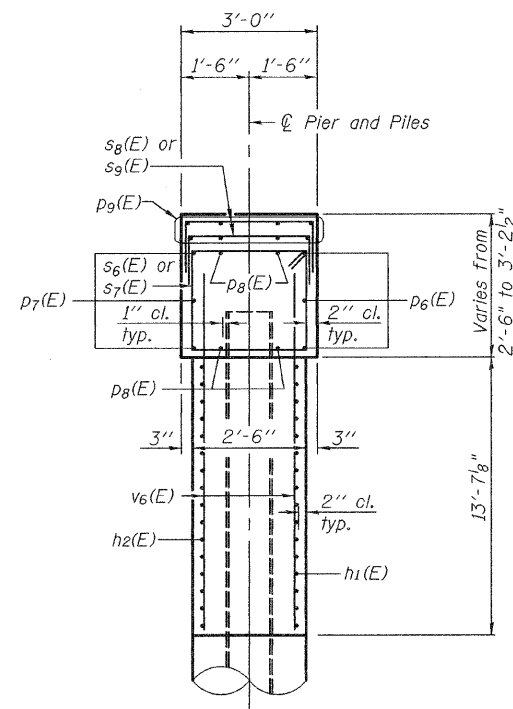


02/27/08

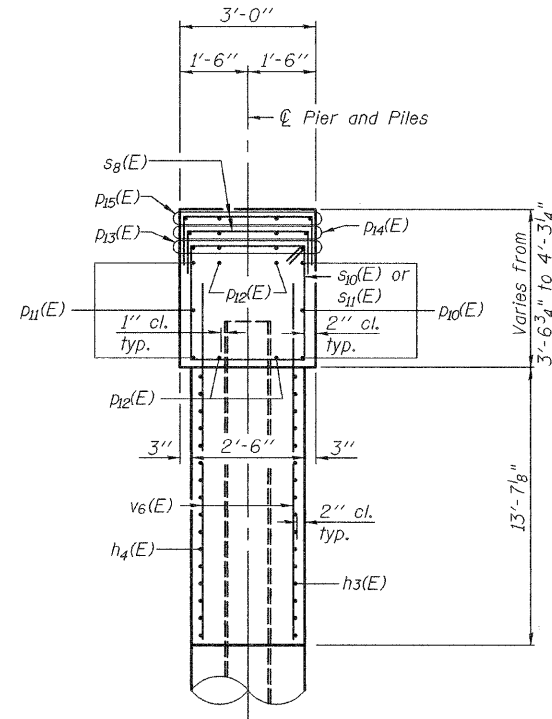
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 68	SHEET NO. 46	SHEET NO. 22 28 SHEETS
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-		

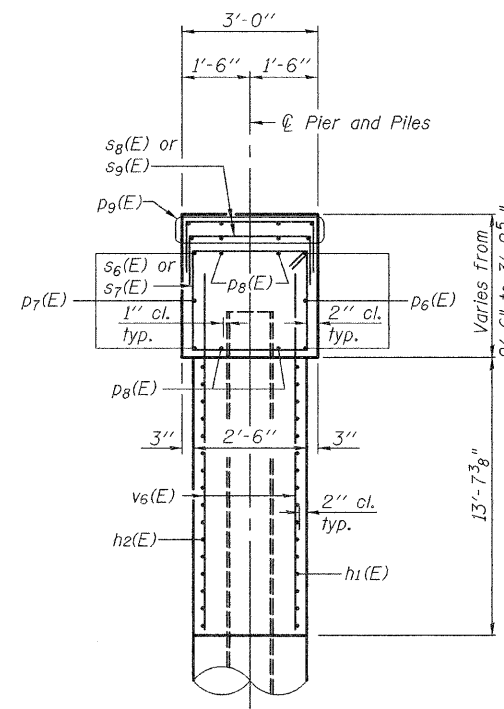
Contract #76948



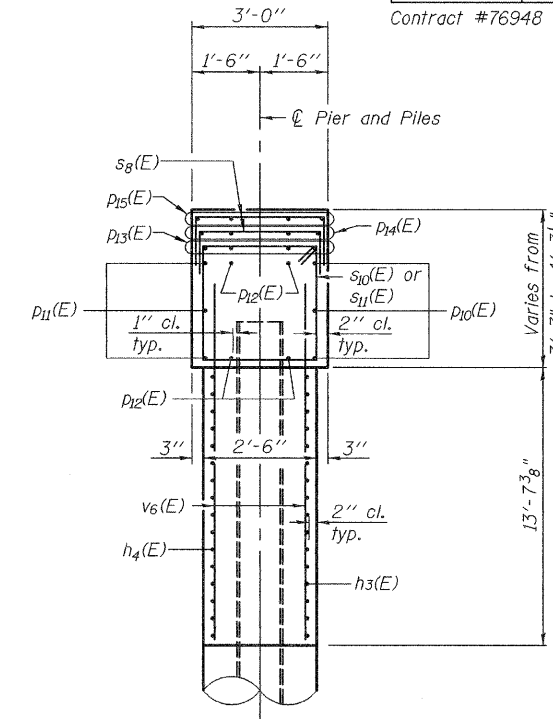
SECTION A-A



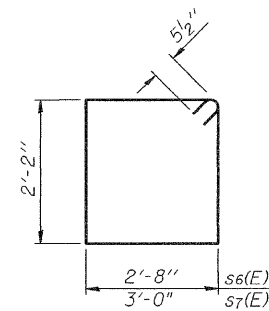
SECTION B-B



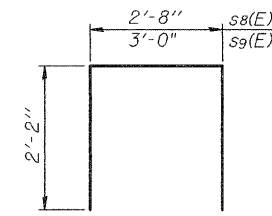
SECTION E-E



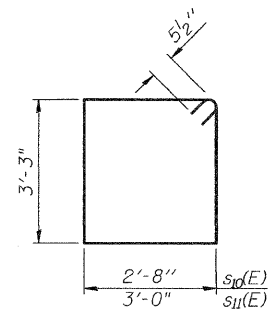
SECTION F-F



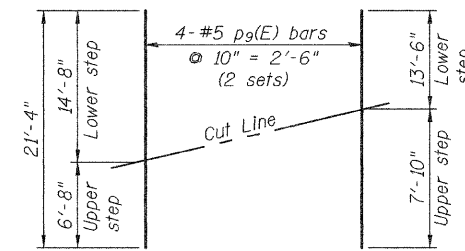
BARS s6(E) & s7(E)



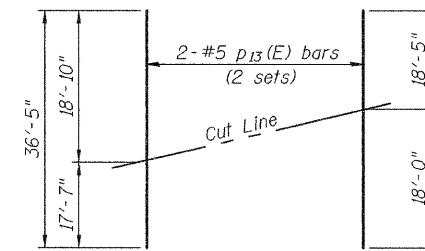
BARS s8(E) & s9(E)



BARS s10(E) & s11(E)



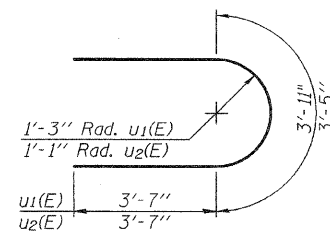
Order p9(E) full length. Cut as shown and place one set of bars as shown in Section A-A and one set of bars as shown in Section E-E (Stage I Constr.).



Order p13(E) full length. Cut as shown and place one set of bars as shown in Section B-B and one set of bars as shown in Section F-F (Stage II Constr.).

FIELD CUTTING DIAGRAM

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



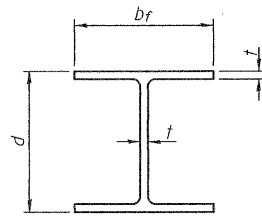
BARS u1(E) & u2(E)

MISCELLANEOUS PIER DETAILS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

02/27/08

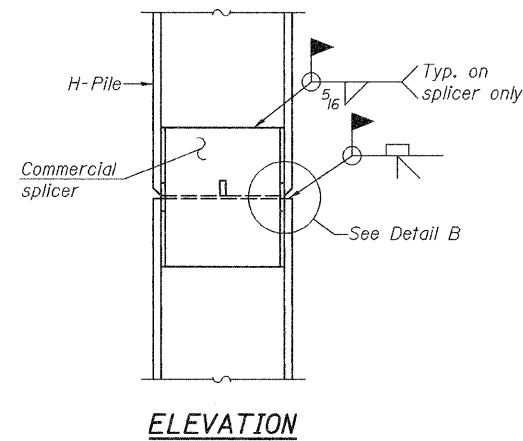
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 23 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	08	47	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-	Contract #76948		

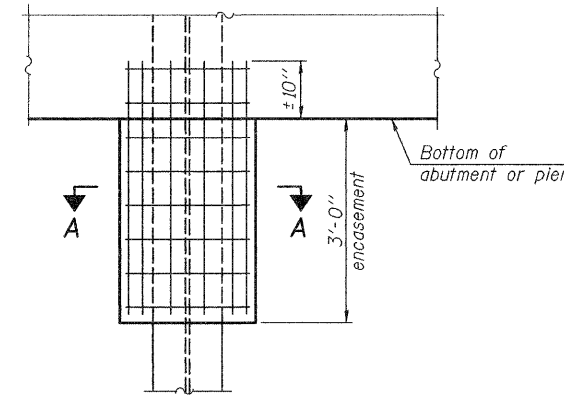


STEEL PILE TABLE

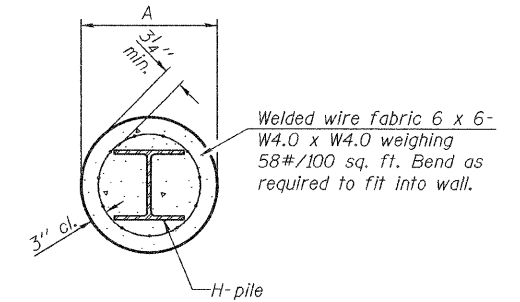
Designation	Depth d	Flange width bf	Web and Flange thickness t	Encasement diameter A
HP 14x117	14 1/4"	14 7/8"	13/16"	30"
x102	14"	14 3/4"	1/16"	30"
x89	13 7/8"	14 3/4"	5/8"	30"
x73	13 5/8"	14 5/8"	1/2"	30"
HP 12x84	12 1/4"	12 1/4"	1/16"	24"
x74	12 1/8"	12 1/4"	5/8"	24"
x63	12"	12 1/8"	1/2"	24"
x53	11 3/4"	12"	7/16"	24"
HP 10x57	10"	10 1/4"	9/16"	24"
x42	9 3/4"	10 1/8"	7/16"	24"
HP 8x36	8"	8 1/8"	7/16"	18"



ELEVATION



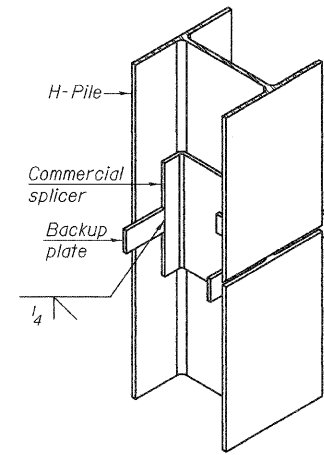
ELEVATION



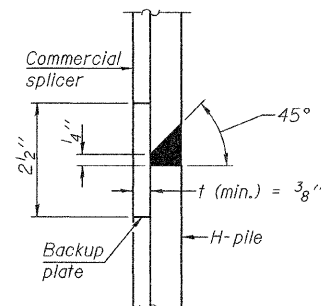
SECTION A-A

Note:  
Forms for encasement may be omitted when soil conditions permit.

PILE ENCASEMENT

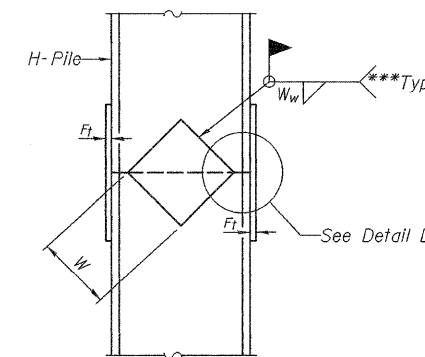


ISOMETRIC VIEW

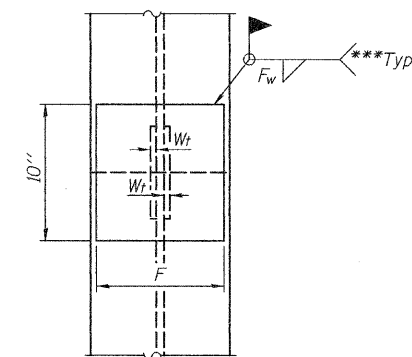


DETAIL "B"

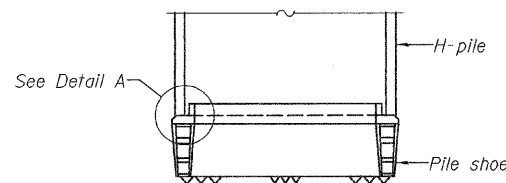
WELDED COMMERCIAL SPLICE



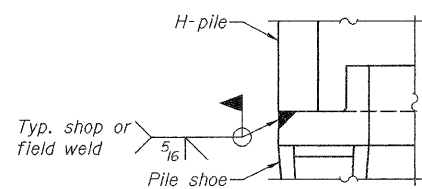
ELEVATION



END VIEW

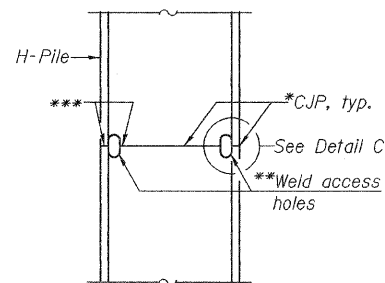


ELEVATION



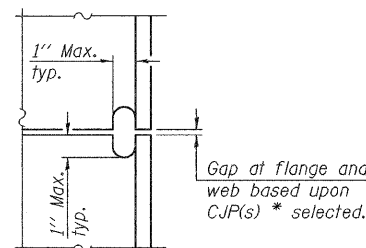
DETAIL A

H-PILE SHOE ATTACHMENT

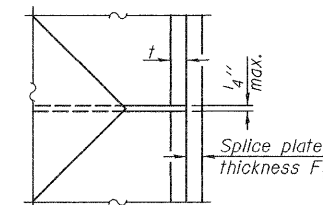


ELEVATION

COMPLETE PENETRATION WELD SPLICE



DETAIL C



DETAIL D

WELDED PLATE FIELD SPLICE

Designation	F	Ft	Fw	W	Wt	Ww
HP 14x117	12 1/2"	1"	7/8"	7 3/4"	5/8"	1/2"
x102	12 1/2"	7/8"	3/4"	7 3/4"	5/8"	1/2"
x89	12 1/2"	3/4"	1/16"	7 3/4"	5/8"	1/2"
x73	12 1/2"	5/8"	9/16"	7 3/4"	5/8"	1/2"
HP 12x84	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x74	10"	7/8"	1/16"	6 1/2"	5/8"	1/2"
x63	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
x53	10"	5/8"	1/2"	6 1/2"	1/2"	3/8"
HP 10x57	8"	3/4"	9/16"	5 1/4"	1/2"	3/8"
x42	8"	5/8"	9/16"	5 1/4"	1/2"	3/8"
HP 8x36	7"	5/8"	7/16"	4 1/4"	1/2"	3/8"

STEEL H-PILE DETAILS

IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



9-3-07

F-HP

\*Use joint conforming to Figure 3.4 in AWS D1.1, Structure Welding Code - Steel.

\*\*Preparation per Fig. 5.2 in AWS D1.1, Structure Welding Code - Steel.

\*\*\*Interrupt welds 1/4" from end of each pile.

Note:  
The steel H-piles shall be according to AASHTO M270 Grade 50.



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO. F.A.P. 304	SECTION 5BR-2	COUNTY CALHOUN	TOTAL SHEETS 08	SHEET NO. 48
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

SHEET NO. 24  
28 SHEETS

Contract #76948

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.

Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length. All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.

Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
- ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$

Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.

$A_t$  = Tensile stress area of lapped reinforcement bars.

\* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-0"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8

The diameter of this part is equal or larger than the diameter of bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



\*\* ONE PIECE

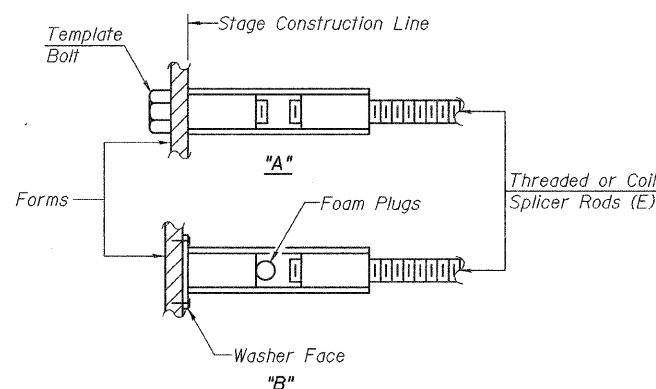
Wire Connector



WELDED SECTIONS

**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.

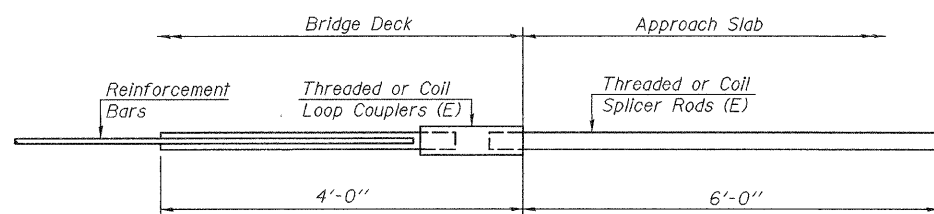


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

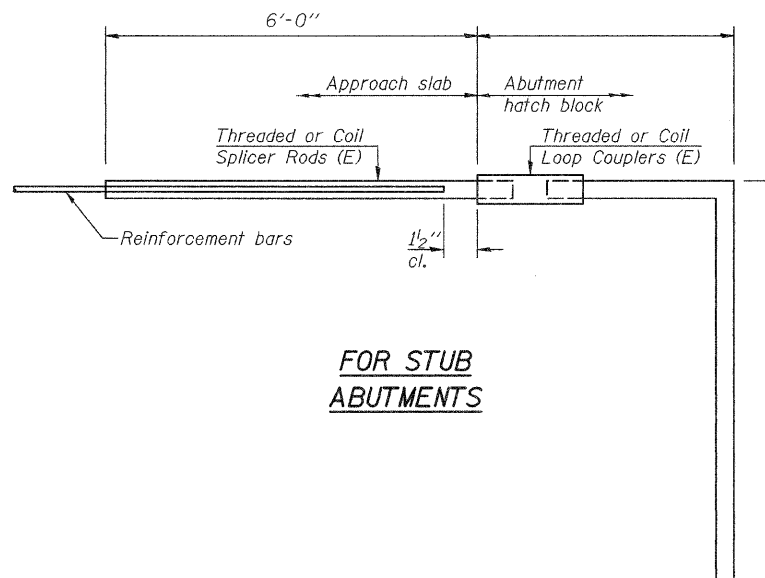
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



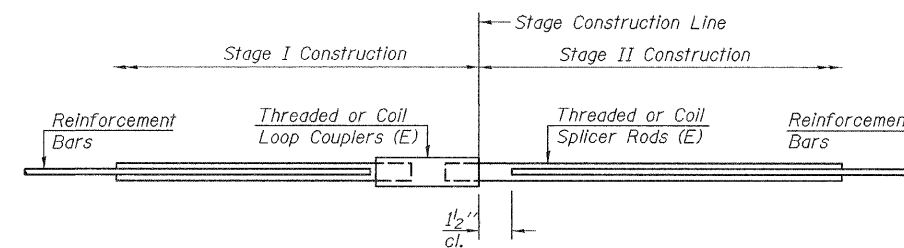
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 66



**FOR STUB ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 0



**STANDARD**

Bar Size	No. Assemblies Required	Location
#5	349	Slab
#7	9	S. Abut.
#7	9	N. Abut.
#5	32	Pier 1
#7	10	Pier 1
#5	32	Pier 2
#7	10	Pier 2

**BAR SPLICER ASSEMBLY DETAILS**

IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM

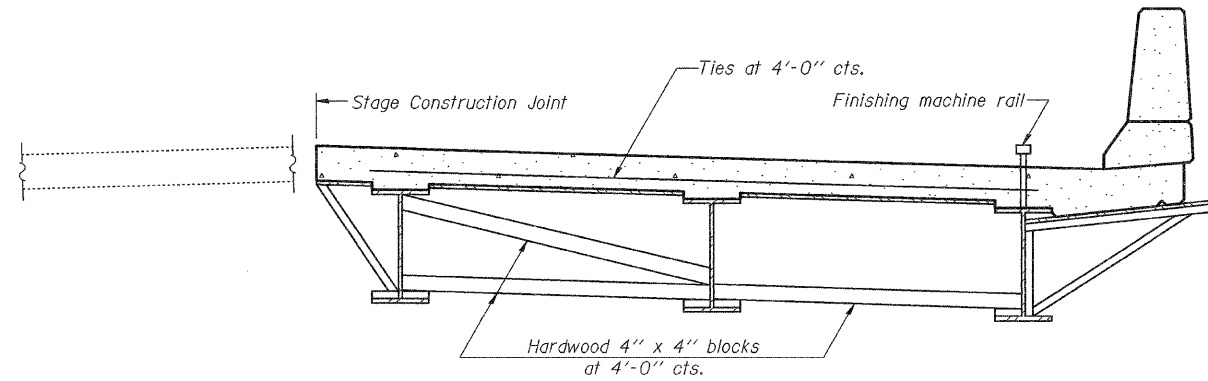


BSD-1

11-1-06

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	SHEET NO. 25 28 SHEETS
F.A.P. 304	5BR-2	CALHOUN	68	49	
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT-	Contract #76948		



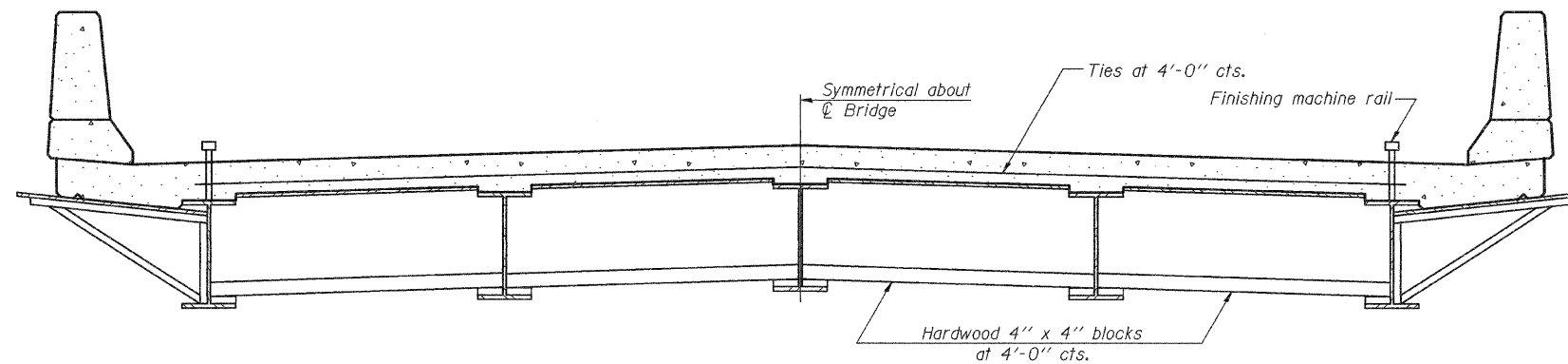
**FORM BRACES FOR  
STAGE CONSTRUCTION**

When cantilever forming brackets are used, the work shall be done according to Article 503.06(b) of the Standard Specifications, except as modified below and in the details shown on this sheet.

The finishing machine rails shall be placed on the top flange of the exterior beams.

The beams or girders, supporting cantilever forming brackets, shall be tied together at 4 foot intervals.

For Standard construction, or Stage Construction the Hardwood bracing materials shall be placed as shown between webs of beams in each bay. See Sheet No. 4 of 28 for Stage Construction Details.



**FORM BRACES FOR  
STANDARD CONSTRUCTION**

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



SB-1

11-1-06

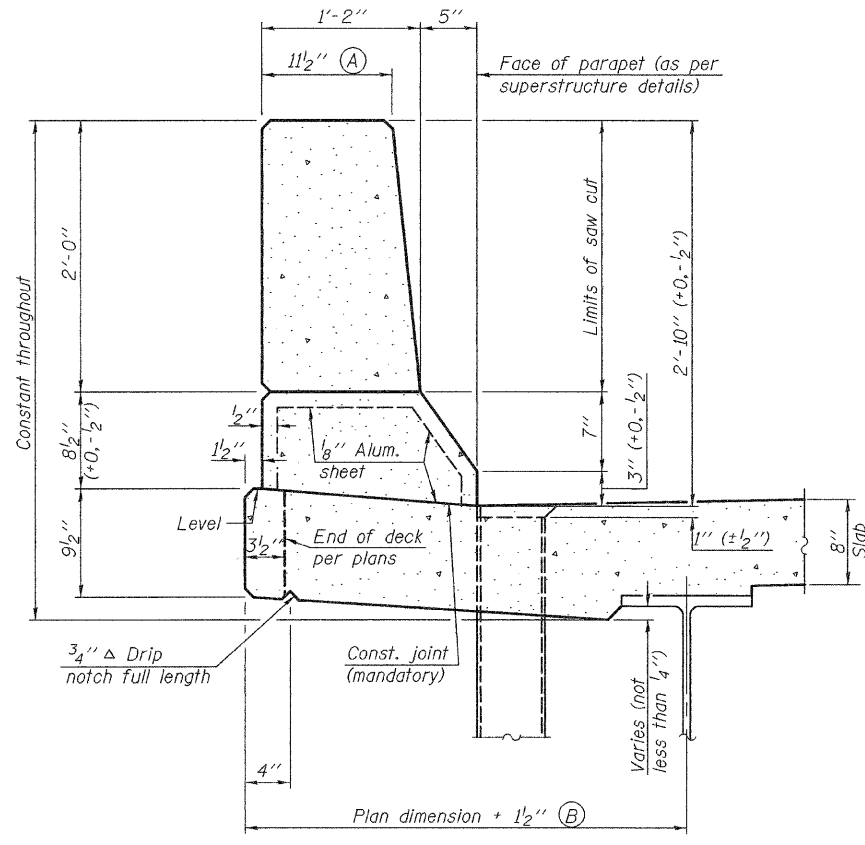
**CANTILEVER FORMING BRACKETS  
FOR SUPERSTRUCTURE  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

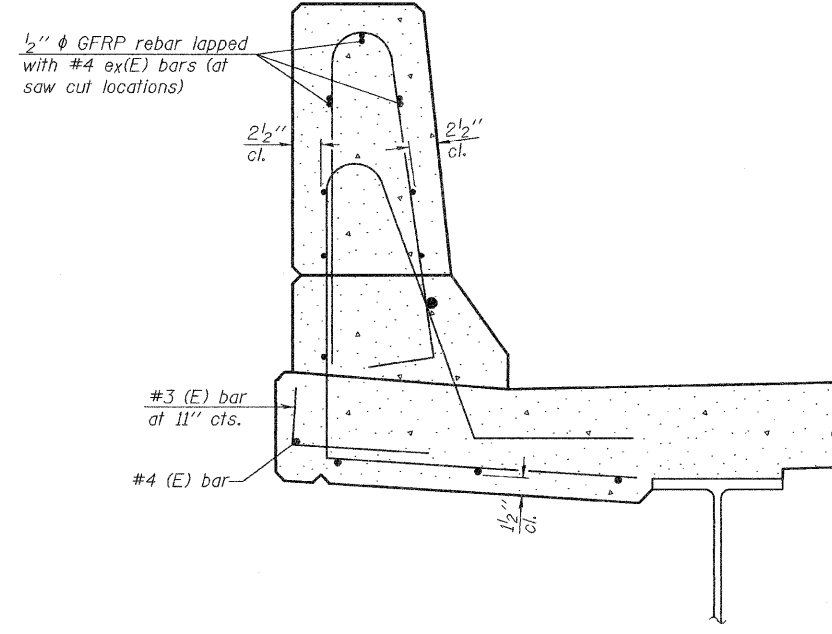
ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
F.A.P. 304	5BR-2	CALHOUN	08	50
FED. ROAD DIST. NO. 7		ILLINOIS	FED. AID PROJECT-	

Contract #76948

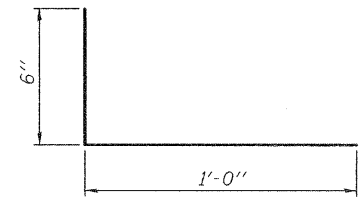
SHEET NO. 26  
28 SHEETS



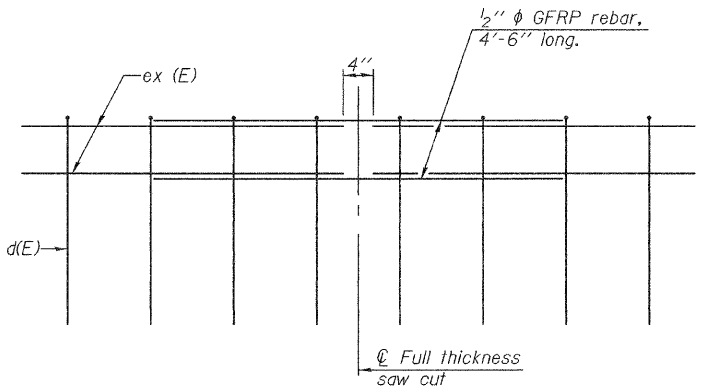
**SECTION**  
(Showing dimensions)



**SECTION**  
(Showing reinforcement clearances for slip forming and additional reinforcement bars)



**#3 (E) BAR**



**GFRP REBAR STIFFENING DETAIL**  
(Place as shown in parapet section at each parapet joint location.)

**GENERAL NOTES**  
All dimensions shall remain the same as shown on contract plans, except dimensions A and B which are to be revised as shown to provide additional clearance. Additional concrete needed to revise dimension A and B= 0.0165 cu. yds./ft. of parapet.  
Place aluminum sheet in curb portion at and near piers. Full thickness saw cut at all joint locations in lieu of cork joint filler.

DESIGNED	YSS
CHECKED	RLM
DRAWN	PRC
CHECKED	RLM



SFP-34 9-3-07

**CONCRETE PARAPET  
SLIPFORMING OPTION  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**SOIL BORING LOG** Page 1 of 3  
Date 11/8/06

ROUTE FAP 304 DESCRIPTION IL 100 over Crawford Creek LOGGED BY S. Wiszka  
SECTION 5BR-2 LOCATION SW 1/4, SEC. 11, TWP. 9S, RNG. 2W, 4 PM  
COUNTY Calhoun DRILLING METHOD Hollow Stem Auger HAMMER TYPE 140# Automatic

STRUCT. NO. 007-0004 (E) / 007-0027 (P)  
Station 467+55  
BORING NO. SB 1 S. Abut  
Station 467+58  
Offset 12.00ft Right  
Ground Surface Elev. 438.0 ft

Depth (ft)	D E P T H	B L U C S	U C S	M O I S T U R E	Surface Water Elev. _____ ft	Stream Bed Elev. _____ ft	Groundwater Elev. _____ ft	SPT				
								(ft)	(in)	(bl)	(%)	
424.5	7							2	0.59	29		
	5							2				
	6							4	0.52	37		
	5							5				
	4							3				
	4							2	3.83	20		
	5							3				
	5							4	0.89	20		
	6							10				
	5							1				
	6							5	0.78	36		
	7							5				
422.0	2											
	3											
	4											
	3							7				
	5							15				
	6							24				
417.5	4											
	3											
	3											

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T268)

**SOIL BORING LOG** Page 2 of 3  
Date 11/8/06

ROUTE FAP 304 DESCRIPTION IL 100 over Crawford Creek LOGGED BY S. Wiszka  
SECTION 5BR-2 LOCATION SW 1/4, SEC. 11, TWP. 9S, RNG. 2W, 4 PM  
COUNTY Calhoun DRILLING METHOD Hollow Stem Auger HAMMER TYPE 140# Automatic

STRUCT. NO. 007-0004 (E) / 007-0027 (P)  
Station 467+55  
BORING NO. SB 1 S. Abut  
Station 467+58  
Offset 12.00ft Right  
Ground Surface Elev. 438.0 ft

Depth (ft)	D E P T H	B L U C S	U C S	M O I S T U R E	Surface Water Elev. _____ ft	Stream Bed Elev. _____ ft	Groundwater Elev. _____ ft	SPT				
								(ft)	(in)	(bl)	(%)	
395.5	4											
	3											
	1.37											
	6											
	8											
	8							2	1.58	28		
	8											
	3											
	5							1				
	6							5	0.78	36		
	5											
382.5												
	4											
	5											
	7											
	7											
379.0												
379.0												

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)  
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T268)

**ROCK BORING LOG** Page 3 of 3  
Date 11/8/06

ROUTE FAP 304 DESCRIPTION IL 100 over Crawford Creek LOGGED BY S. Wiszka  
SECTION 5BR-2 LOCATION SW 1/4, SEC. 11, TWP. 9S, RNG. 2W, 4 PM  
COUNTY Calhoun CORING METHOD \_\_\_\_\_

STRUCT. NO. 007-0004 (E) / 007-0027 (P)  
Station 467+55  
CORING BARREL TYPE & SIZE \_\_\_\_\_  
CORING DIAMETER \_\_\_\_\_ in  
CORE NO. \_\_\_\_\_  
TOP OF ROCK ELEV. \_\_\_\_\_ ft  
BEGIN CORE ELEV. \_\_\_\_\_ ft

Depth (ft)	R E C O R D E D C O R E L E N G T H	C O R E D I A M E T E R	C O R E Q U A L I T Y	C O R E S T R E N G T H	C O R E S T R E N G T H
378.00	1	88	51	5	
	1	88	51	5	
	1	88	51	9	
373.00	1	88	51	17	

End of Boring and Rock Core  
Moved Boring Location off Structure & Approach Pavement due to Concrete

Color pictures of the cores  Yes  
Cores will be stored for examination until Indefinitely  
The "Strength" column represents the uniaxial compressive strength of the core sample (ASTM D-2938)  
BBS, Form 137 (Rev. 8-95)

DESIGNED	YSS
CHECKED	RLM
DRAWN	AEC
CHECKED	RLM



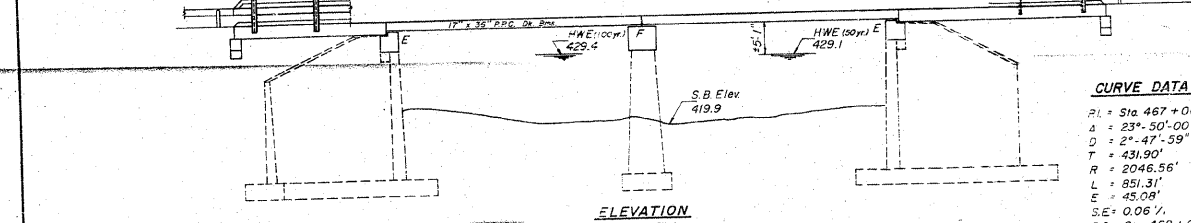
02/27/08

**BORING LOGS  
IL. ROUTE 100 OVER  
CRAWFORD CREEK  
F.A.P. ROUTE 304 - SECTION 5BR-2  
CALHOUN COUNTY  
STATION 467+59.00  
STRUCTURE NO. 007-0027**



DESIGNER	CHECKED	DATE	BY	DATE
Z	SBR	CALHOUN	ZS	AS

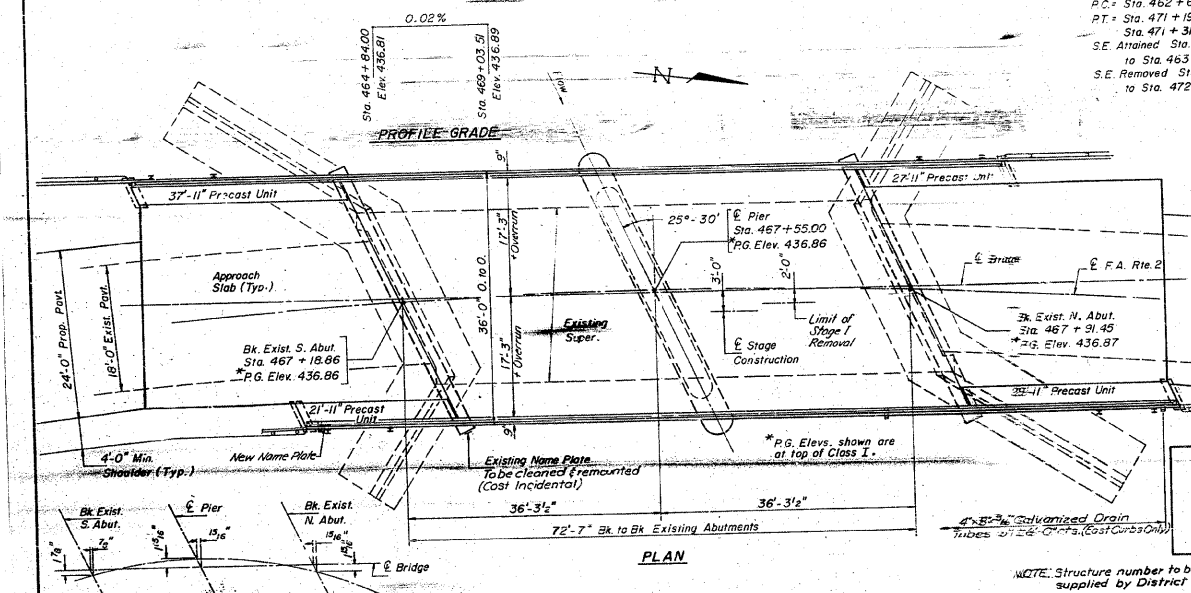
B.M.: RR Spike in Power Pole (Gen) South of Bridge  
Elev. 429.45  
Existing Structure: Superstructure consists of a Two Span (non-continuous) R.C. Thru Girder with R.C. Beams and Deck Slab. Substructure consists of two R.C. Closed Abutments and one unreinforced Solid Pier. Built as S.B.I. Rte. 38, Section 5B, Station 467+55 in 1924. Existing Superstructure to be removed and replaced utilizing Stage Construction and maintaining one lane traffic at all times. Existing Abutments and Pier to be widened using Reinforced Concrete Cap Extensions. No Salvage.  
Bridge No. 007-0004



**CURVE DATA**  
 P.I. = Sta. 467+00.00  
 a = 23° 50' 00"  
 D = 2° 47' 59"  
 T = 431.90'  
 R = 2046.56'  
 L = 851.31'  
 E = 45.08'  
 S.E. = 0.06 %  
 P.C. = Sta. 462+68.10  
 P.T. = Sta. 471+19.41 (Back)  
 = Sta. 471+31.90 (Ahead)  
 S.E. Attained Sta. 461+34.77  
 to Sta. 463+34.77  
 S.E. Removed Sta. 470+52.74  
 to Sta. 472+65.23

**GENERAL NOTES**

All structural steel shall be shop painted with two coats of basic lead silico chromate paint, except as noted.  
 Plan dimensions and details relative to existing structure have been taken from existing plans and are subject to nominal construction variations. It shall be the responsibility of the Contractor to verify such dimensions and details in the field and make the necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in the scope of work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
 The top surface of the beams shall be finished in accordance with Article 605.06 of the Standard Specifications except that the surface shall not be roughened by brooming. The finished surface shall be free of depressions or high spots with sharp corners.  
 Protective Coat shall not be applied to surfaces to which Waterproofing Membrane System is applied.  
 Reinforcement bars shall conform to the requirements of AASHTO M-31 or M-33 Grade 60, except as noted.  
 Shoulder transition to wingwall shall be shaped with broken concrete. Cost incidental.  
 Expansion bolts shall consist of self-drilling expansion anchors and 3/4" hooked bolts. Hooked bolts shall extend a minimum of 12" into new concrete, unless otherwise shown.



**TOTAL BILL OF MATERIAL**

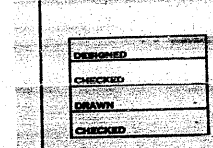
Item	Unit	Super	Sub	Total
Portland Cement Mortar Facing Course	Lin. Ft.	800		800
S.I. Conc. Surface Course, Mix D, Class I	Tons	24		24
Waterproofing Membrane System	Sq. Yds.	285		285
Protective Coat	Sq. Yds.	45		45
Concrete Removal	Cu. Yds.		19.9	19.9
Expansion Bolts 3/4" #	Each		108	108
Class X Concrete	Cu. Yds.	14.8		52.7
Precast Concrete Bridge Slab	Sq. Ft.	441		441
Precast Prestresser Conc. DK Bms (17' Depth)	Sq. Ft.	2616		2616
Steel Railing, Type T	Lin. Ft.	267		267
Reinforcement Bars	Pounds	650	7510	8160
Removal of Existing Superstructures	Each			1
Name Plates	Each			1
Reformed Joint Seal (2 1/2")	Lin. Ft.	80		80
Temporary Bridge Rail	Lin. Ft.	156		336
Structural Steel	Pounds	5360		5360
Epoxy Crack Sealing	Lin. Ft.		68	68
Temporary Support System	Each			1

STATION 467+55  
 REBUILT 198 BY  
 STATE OF ILLINOIS  
 F.A. RTE. 2 SEC. 5BR-1  
 PROJECT BR-F-2 (S.I.)  
 LOADING HS 20  
 STR. NO.

NAME PLATE  
 See Std 2113

NOTE: Structure number to be supplied by District

**OFFSET SKETCH**

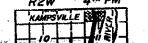


**WATERWAY INFORMATION**

Drainage Area S.B.I. Sq.Mi.	Low Grade Elev. 435.7 @ Sta. 468+50	Flow	Yr.	C.F.S.	Exist.	Prop.	H.W.E. Exist.	Prop.	Exist.	Prop.	Min. Water Elev.
Design	50	4200	468	458	429.1	2.0	2.0	431.1	431.1		
Base	100	4860	468	468	429.4	2.0	2.0	432.0	432.0		
Overtopping	500	6420	531	531	430.1	4.2	4.2	434.3	434.3		
Max. Calc.	500										

**DESIGN STRESSES**

FIELD UNITS: f<sub>c</sub> = 5500 psi  
 f<sub>s</sub> = 60,000 psi  
 PRECAST PRESTRESSER UNITS: f<sub>c</sub> = 4000 psi  
 f<sub>s</sub> = 270,000 psi (1/2" # Strands)  
 f<sub>s</sub> = 185,000 psi (3/8" # Strands)  
 PRECAST UNITS: f<sub>c</sub> = 4500 psi  
 f<sub>s</sub> = 20,000 psi  
 f<sub>c</sub> = 1800 psi  
 Design Specifications 1977 AASHTO and 1978 & 1979 Interims.  
 LOADING HS 20-44\*\*\*  
 Allow 25#/sq. ft. for Future W.S.  
 \*\*\* New Construction



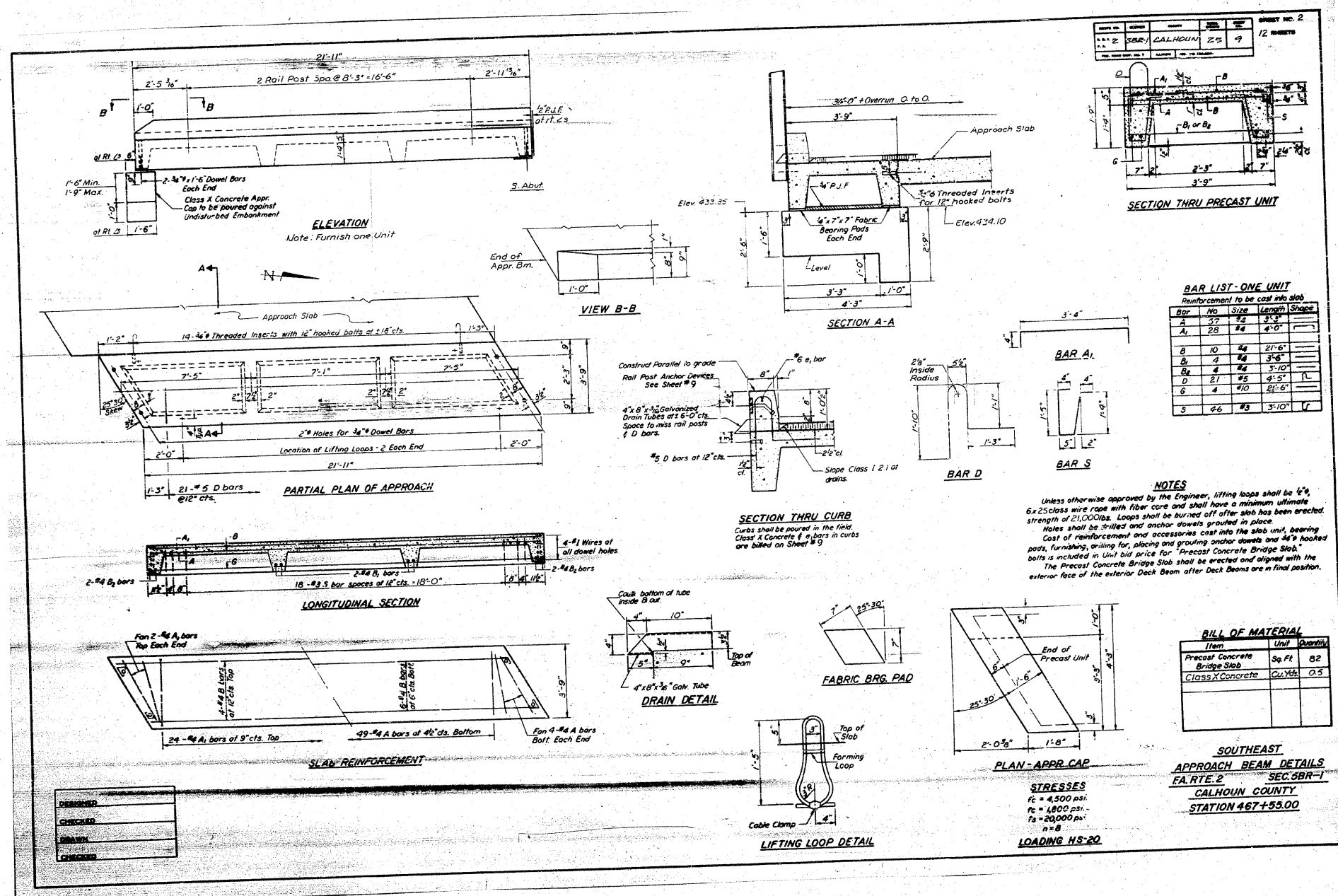
**GENERAL PLAN & ELEVATION**

F.A. RTE. 2 OVER CRAWFORD CREEK  
 F.A. ROUTE 2 SECTION 5BR-1  
 CALHOUN COUNTY  
 STATION 467+55.00

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

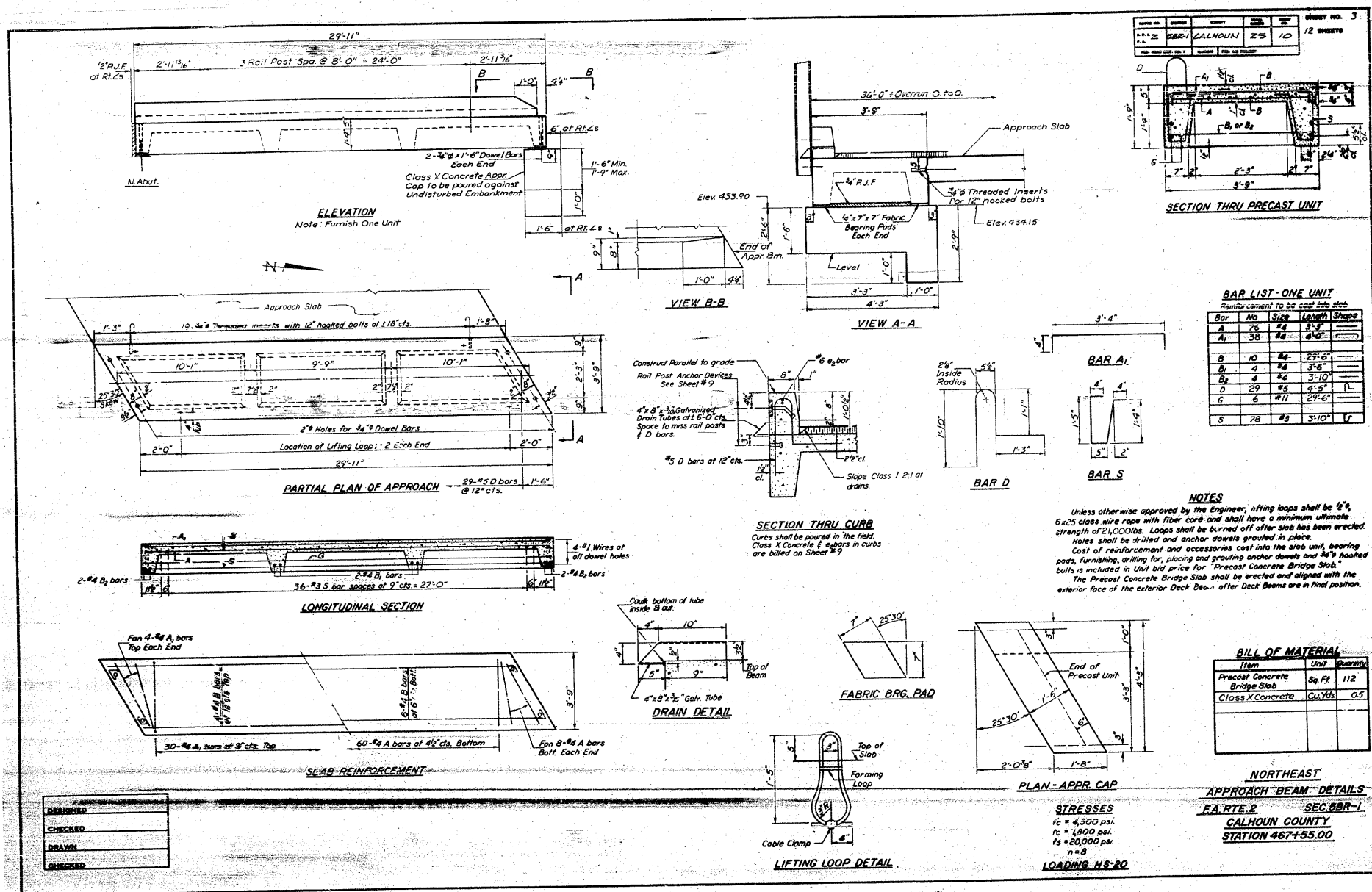
FOR INFORMATION ONLY



FOR INFORMATION ONLY

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 REFERENCE = #REF#

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. DATE  
 HORIZ. DATE  
 DRAWN BY  
 CHECKED BY



FOR INFORMATION ONLY

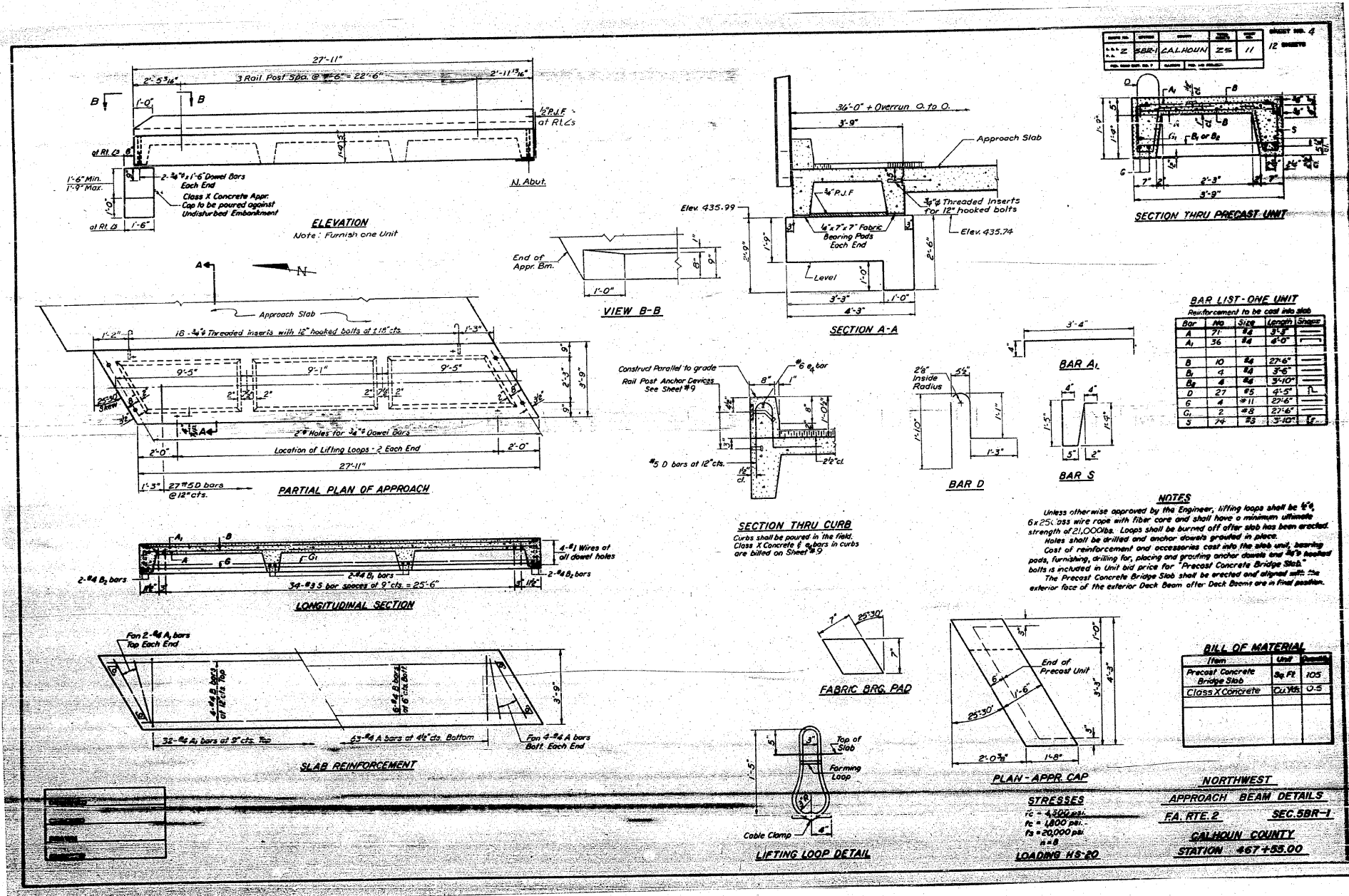
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 EXISTING STRUCTURE PLANS  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. DATE  
 HORIZ. DATE  
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 CHECKED BY

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 REFERENCE = BRG#



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	56
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

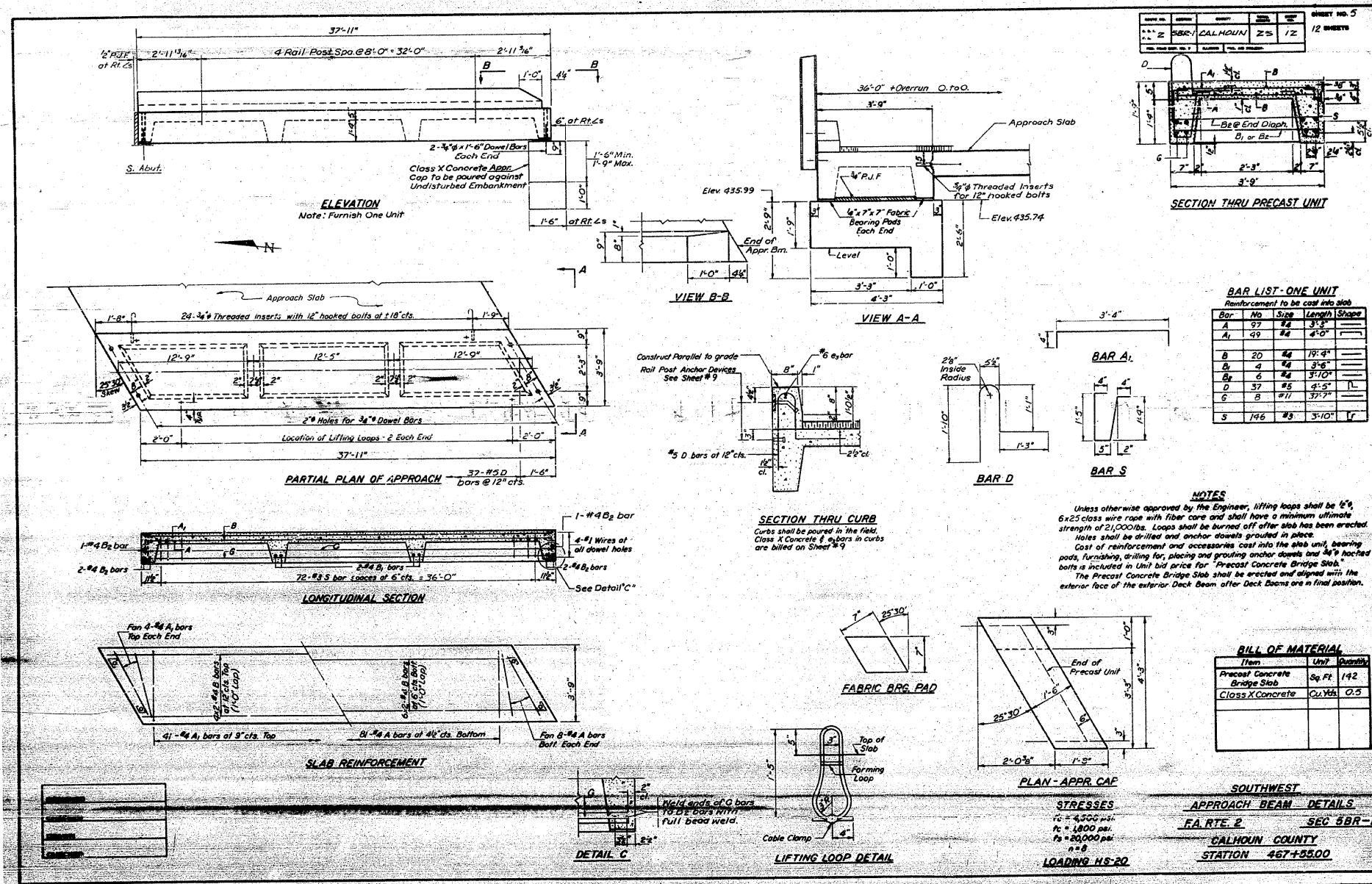


FOR INFORMATION ONLY

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REFERENCE = #REF#

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
FAP ROUTE 304  
SECTION 5BR-2  
CLINTON COUNTY  
SCALE: VERT. DATE  
HORIZ. DATE  
DRAWN BY  
CHECKED BY

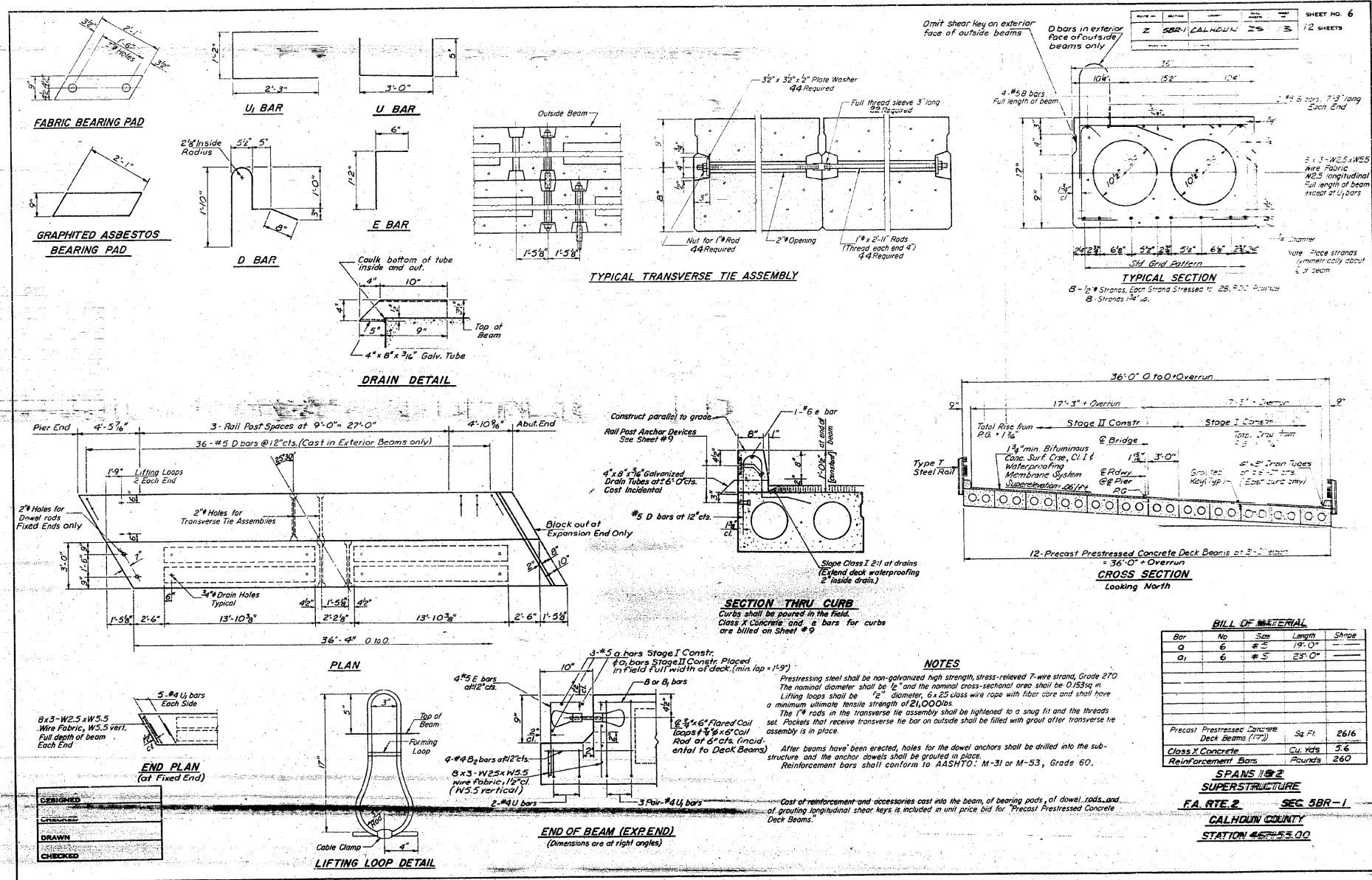


FOR INFORMATION ONLY

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>EXISTING STRUCTURE PLANS</b>  FAP ROUTE 304 SECTION 5BR-2 CLINTON COUNTY  SCALE: VERT. HORIZ. DATE:                      DRAWN BY:                      CHECKED BY:

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 REFERENCE = #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	58
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



SHEET NO. 6  
 Z 5BR-1 CALHOUN 25 5 12 SHEETS

**BILL OF MATERIAL**

Bar	No	Size	Length	Shape
0	6	#5	19'-0"	
01	6	#5	23'-0"	
Precast Prestressed Concrete Deck Beams (17')				
Class X Concrete				
Reinforcement Bars				
			Sq. Ft.	Pounds
			2616	5.6
				260

**NOTES**

Pressing steel shall be non-galvanized high strength, stress-relieved 7-wire strand, Grade 270. The nominal diameter shall be 1/2" and the nominal cross-sectional area shall be 0.153sq in. Lifting loops shall be 1/2" diameter, 6 x 25 class wire rope with fiber core and shall have a minimum ultimate tensile strength of 21,000lbs.

The 1/4" rods in the transverse tie assembly shall be tightened to a snug fit and the threads set. Pockets that receive transverse tie bar on outside shall be filled with grout after transverse tie assembly is in place.

After beams have been erected, holes for the dowel anchors shall be drilled into the sub-structure and the anchor dowels shall be grouted in place.

Reinforcement bars shall conform to AASHTO: M-31 or M-53, Grade 60.

**SPANS 1 & 2**  
**SUPERSTRUCTURE**  
**F.A. RTE. 2** **SEC. 5BR-1**  
**CALHOUN COUNTY**  
**STATION 457+55.00**

DESIGNED
CHECKED
DRAWN
CHECKED

FOR INFORMATION ONLY

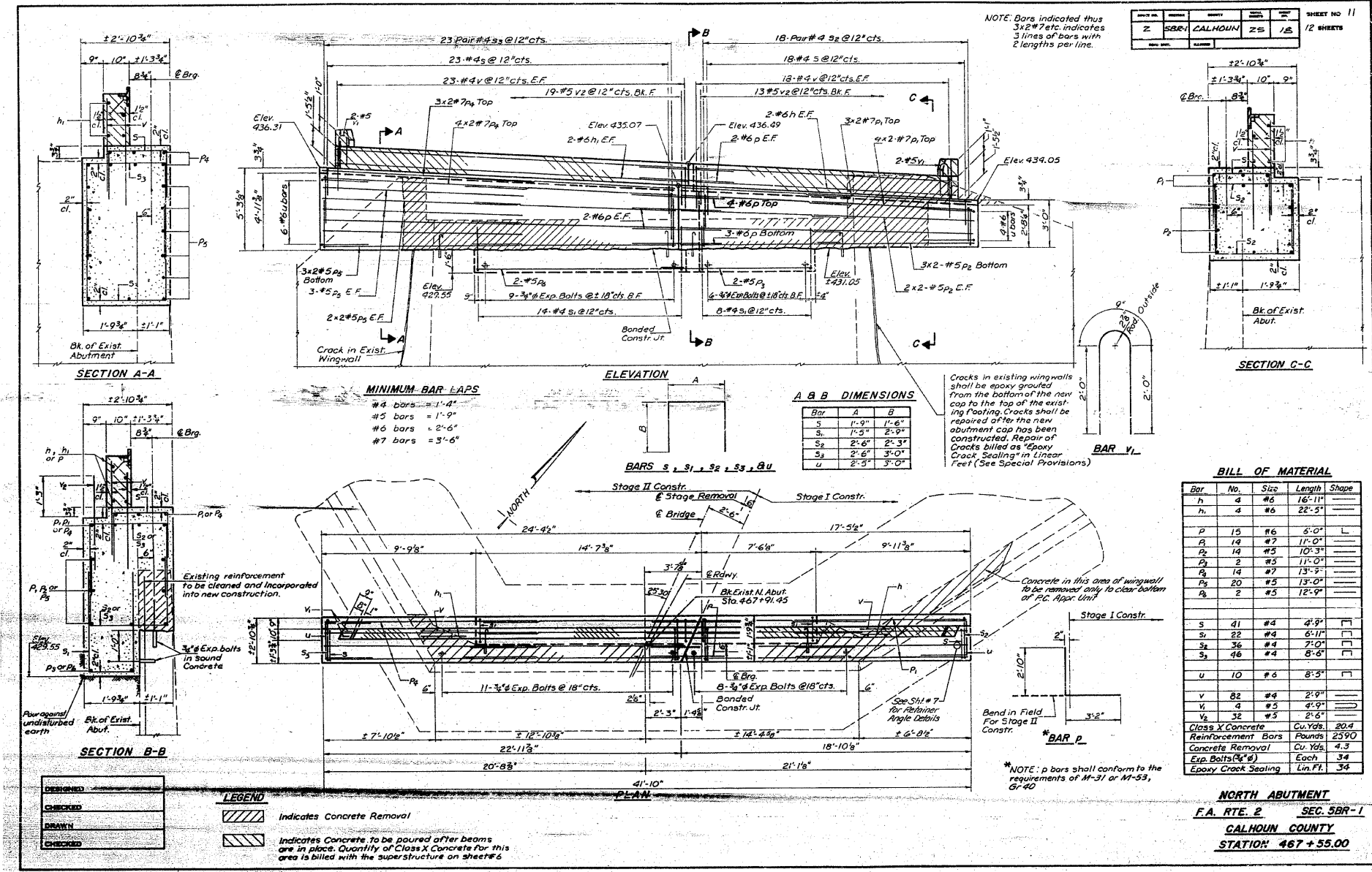
**REVISIONS**

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. DATE  
 HORIZ. DATE  
 DRAWN BY  
 CHECKED BY

PLOT DATE = 1/24/2008  
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 PLOT SCALE = 5/8"=1'-0"  
 REFERENCE = #REF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	SBR-2	CALHOUN	68	59
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



SHEET NO 11

2	SBR-1	CALHOUN	ZS	1B	12 SHEETS
---	-------	---------	----	----	-----------

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h	4	#6	16'-11"	
h	4	#6	22'-5"	
P	15	#6	5'-0"	L
P	14	#7	17'-0"	
P <sub>2</sub>	14	#5	10'-3"	
P <sub>3</sub>	2	#5	11'-0"	
P <sub>4</sub>	14	#7	13'-9"	
P <sub>5</sub>	20	#5	13'-0"	
P <sub>6</sub>	2	#5	12'-9"	
S	41	#4	4'-9"	□
S <sub>1</sub>	22	#4	6'-11"	□
S <sub>2</sub>	36	#4	7'-0"	□
S <sub>3</sub>	46	#4	8'-6"	□
u	10	#6	8'-5"	□
v	82	#4	2'-9"	□
V <sub>1</sub>	4	#5	4'-9"	□
V <sub>2</sub>	32	#5	2'-5"	□
Class X Concrete		Cu Yds.	20.4	
Reinforcement Bars		Pounds	2590	
Concrete Removal		Cu Yds.	4.3	
Exp. Bolts (#6)		Each	34	
Epoxy Crack Sealing		Lin. Ft.	34	

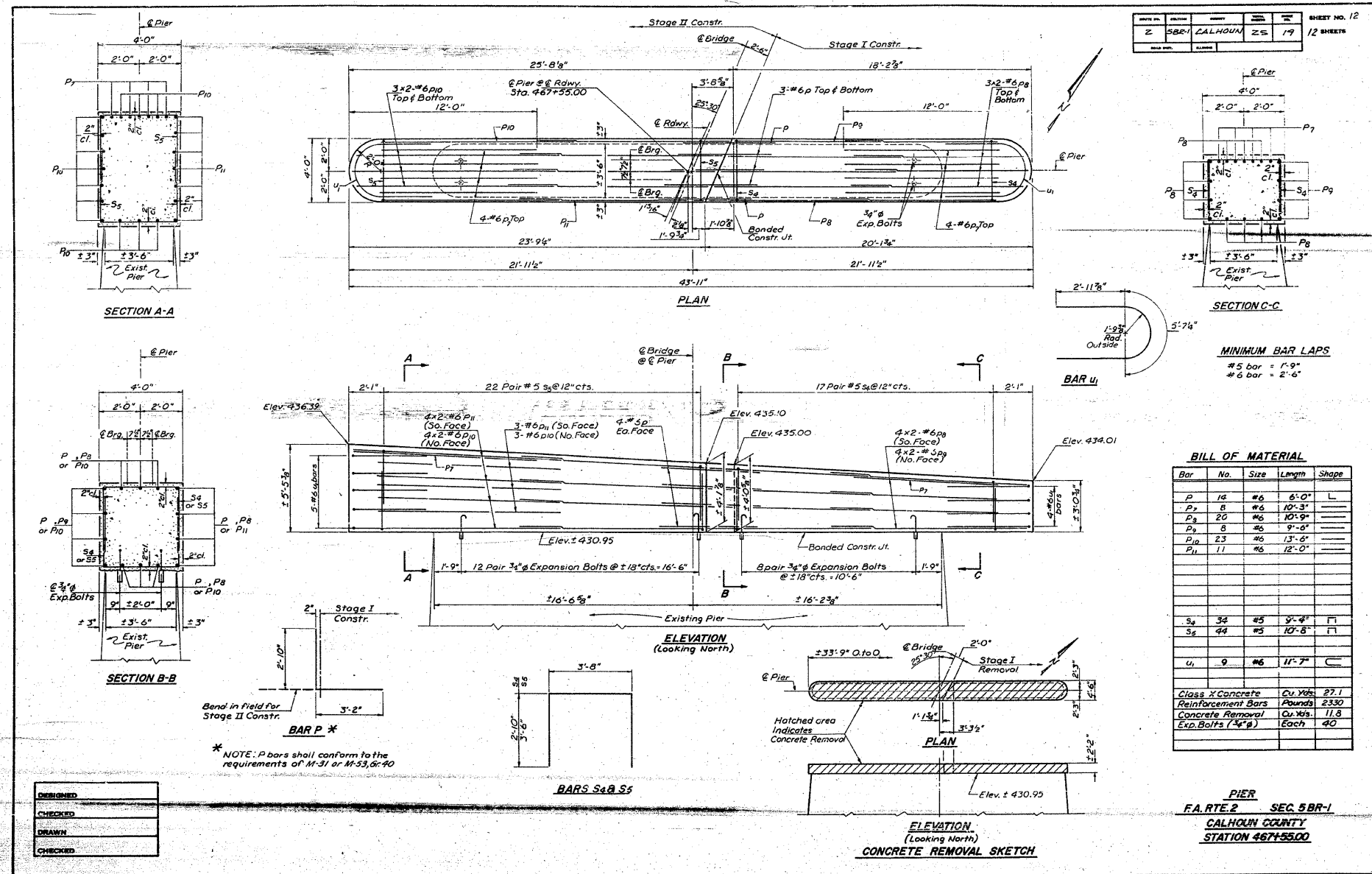
**NORTH ABUTMENT**  
**F.A. RTE. 2 SEC. 5BR-1**  
**CALHOUN COUNTY**  
**STATION 467+55.00**

FOR INFORMATION ONLY

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		<b>EXISTING STRUCTURE PLANS</b>  FAP ROUTE 304 SECTION 5BR-2 CLINTON COUNTY  SCALE: VERT. HORIZ. DATE:                      DRAWN BY:                      CHECKED BY:

PLOT DATE = 1/24/2008  
 PLOT SCALE = 60' = 1" IL  
 REFERENCE = BREF#

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	60
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



FOR INFORMATION ONLY

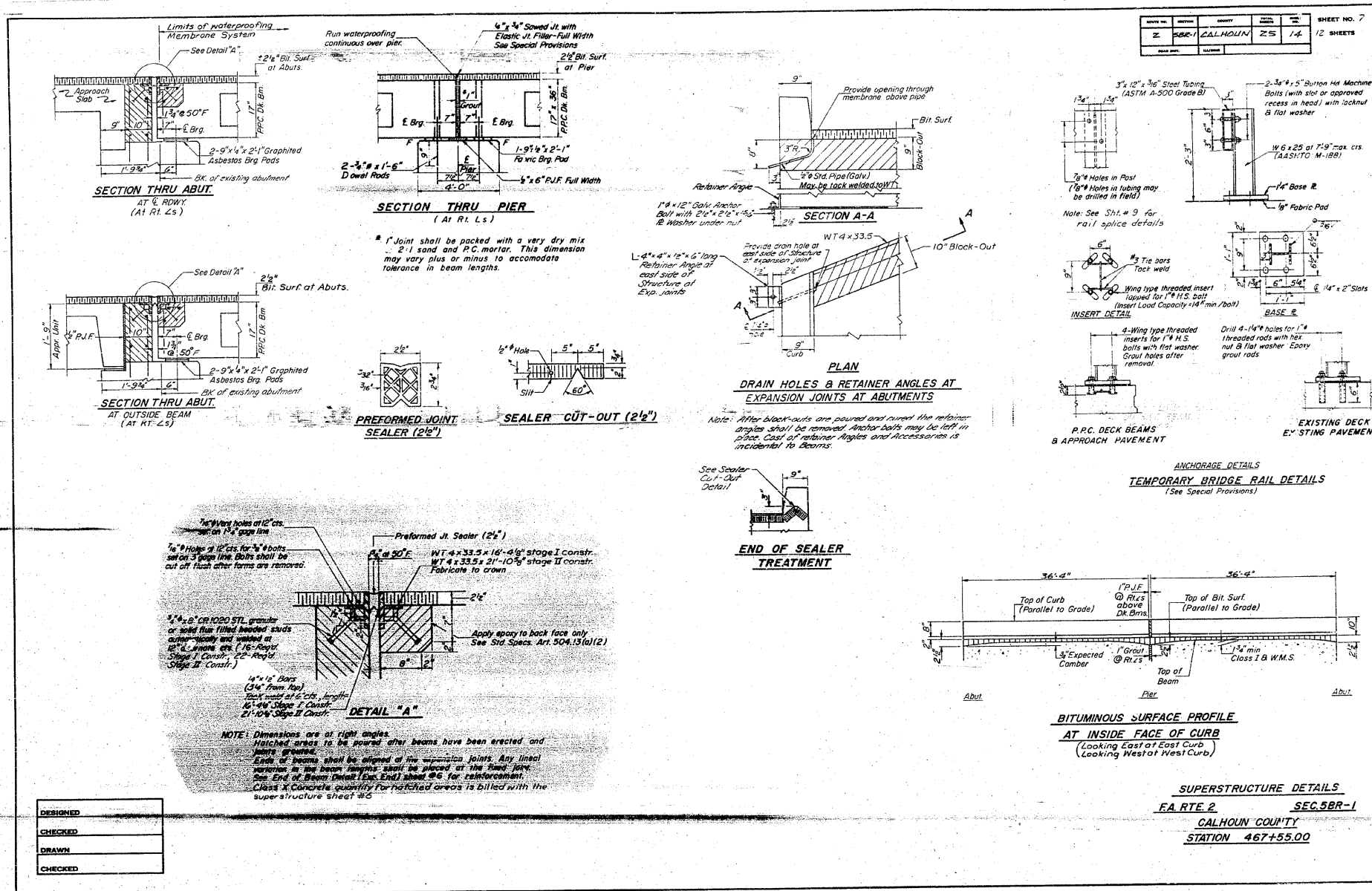
REVISIONS		DATE
NAME		

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLT DATE = 1/24/2008  
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 PLOT SCALE = 50.0000 / 1 IN.  
 REFERENCE = NONE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	61
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

DESIGNED BY	CHECKED BY	DRAWN BY	CHECKED BY
Z	SEC-1	CALHOUN	ZS
SHEET NO. 7			
12 SHEETS			



DESIGNED	
CHECKED	
DRAWN	
CHECKED	

FA RTE 2 SEC 5BR-1  
CALHOUN COUNTY  
STATION 467+55.00

FOR INFORMATION ONLY

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		EXISTING STRUCTURE PLANS

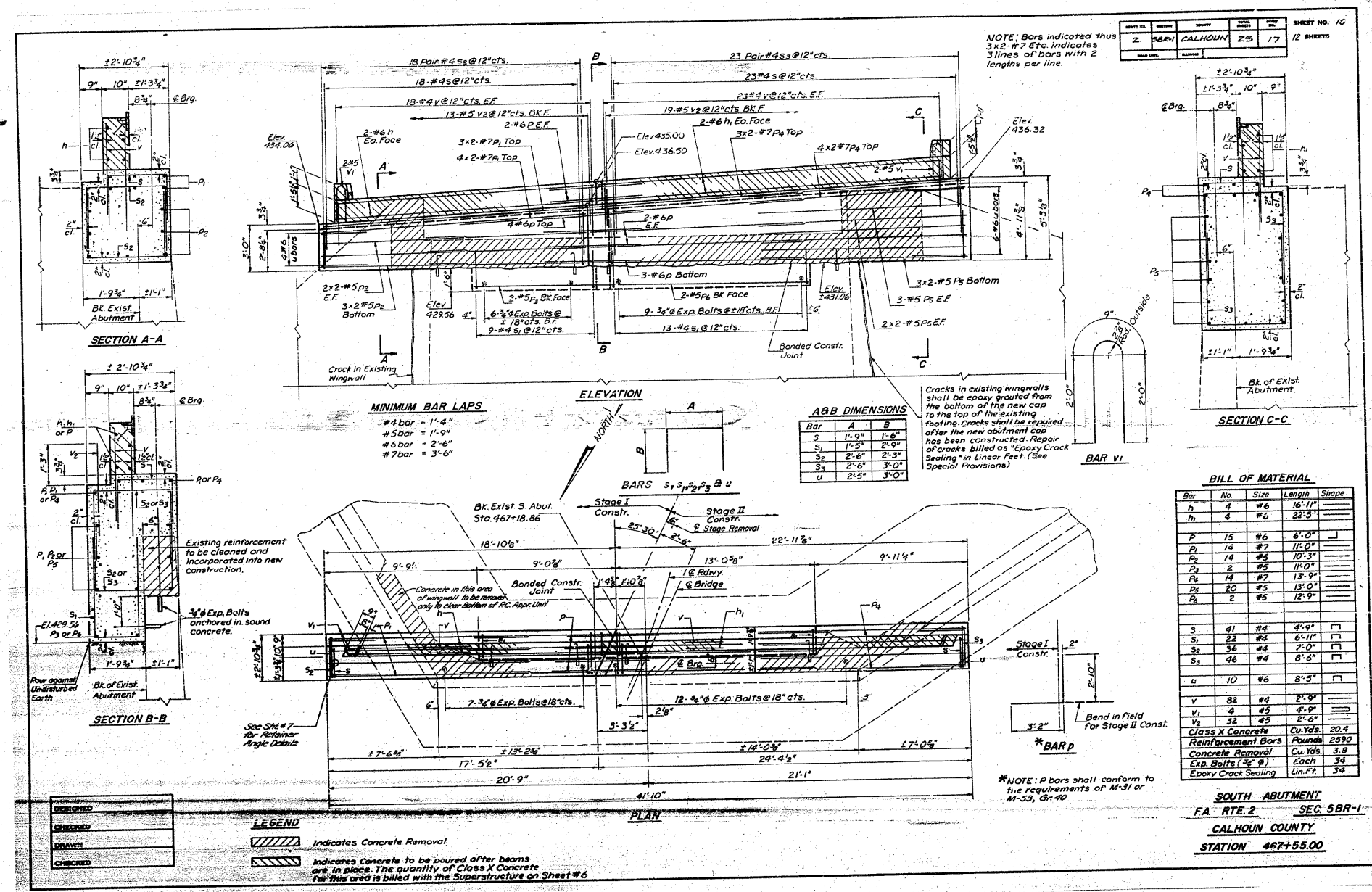
FAP ROUTE 304  
SECTION 5BR-2  
CLINTON COUNTY

SCALE: VERT. / HORIZ.  
DATE

DRAWN BY  
CHECKED BY

PLOT DATE = 1/24/2008  
FILE NAME = c:\pcc\projects\76948\5br\5br61.dgn  
PLOT SCALE = 5/8" = 1' IN.  
REFERENCE = # REF #

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	62
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

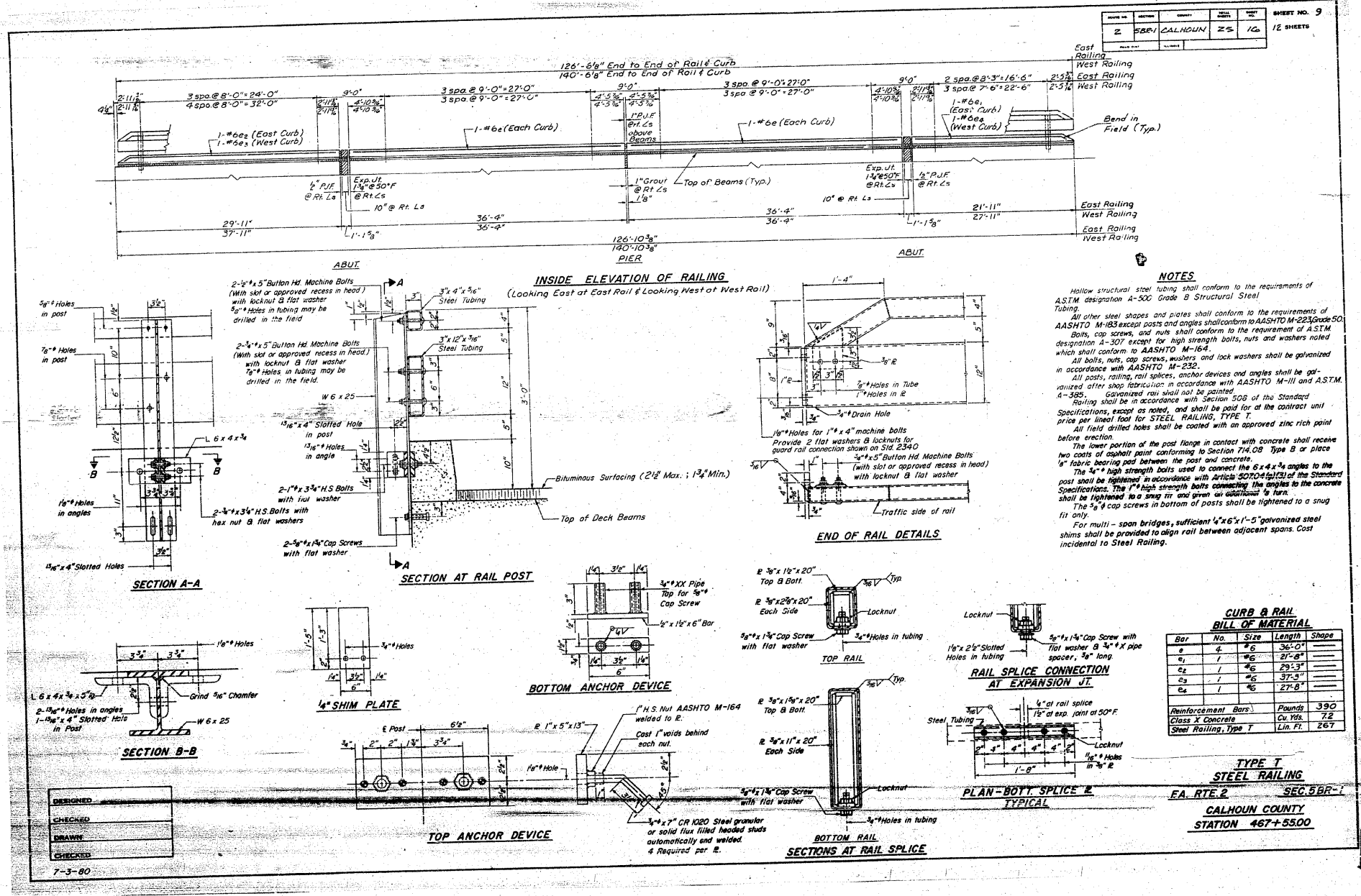


FOR INFORMATION ONLY

PLOT DATE = 1/24/2008  
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 PLOT SCALE = 56.00000 / IN.  
 REFERENCE = #REF#

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT.     DRAWN BY  
 HORIZ.             CHECKED BY  
 DATE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	63
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



**NOTES**

1. Hollow structural steel tubing shall conform to the requirements of ASTM designation A-500 Grade B Structural Steel Tubing.

2. All other steel shapes and plates shall conform to the requirements of AASHTO M-183 except posts and angles shall conform to AASHTO M-223 Grade 50.

3. Bolts, cap screws, and nuts shall conform to the requirements of ASTM designation A-307 except for high strength bolts, nuts and washers noted which shall conform to AASHTO M-164.

4. All bolts, nuts, cap screws, washers and lock washers shall be galvanized in accordance with AASHTO M-232.

5. All posts, railing, rail splices, anchor devices and angles shall be galvanized after shop fabrication in accordance with AASHTO M-111 and ASTM. Galvanized rail shall not be painted.

6. Railing shall be in accordance with Section 506 of the Standard Specifications, except as noted, and shall be paid for at the contract unit price per linear foot for STEEL RAILING, TYPE T.

7. All field drilled holes shall be coated with an approved zinc rich paint before erection.

8. The lower portion of the post flange in contact with concrete shall receive two coats of asphalt paint conforming to Section 714.08 Type B or place 1/2" fabric bearing pad between the post and concrete.

9. The 1/2" high strength bolts used to connect the 6 x 4 x 3/4 angles to the post shall be tightened in accordance with Article 507.04 of the Standard Specifications. The 1" high strength bolts connecting the angles to the concrete shall be tightened to a snug fit and given an additional 1/4 turn.

10. The 3/8" cap screws in bottom of posts shall be tightened to a snug fit only.

11. For multi-span bridges, sufficient 1/2" x 6" x 1/2" galvanized steel shims shall be provided to align rail between adjacent spans. Cost incidental to Steel Railing.

**CURB & RAIL BILL OF MATERIAL**

Bar No.	Size	Length	Shape
a	1/2"	36'-0"	
b	1/2"	27'-0"	
c	1/2"	29'-3"	
d	1/2"	37'-5"	
e	1/2"	27'-0"	

Reinforcement Bars Pounds 390  
 Glass X Concrete Cu Yds 7.2  
 Steel Railing, Type T Lin. Ft. 267

**TYPE T STEEL RAILING**  
 FA. RTE. 2 SEC. 5BR-2  
 CALHOUN COUNTY  
 STATION 467+55.00

DESIGNED	
CHECKED	
DRAWN	
CHECKED	

7-3-80

FOR INFORMATION ONLY

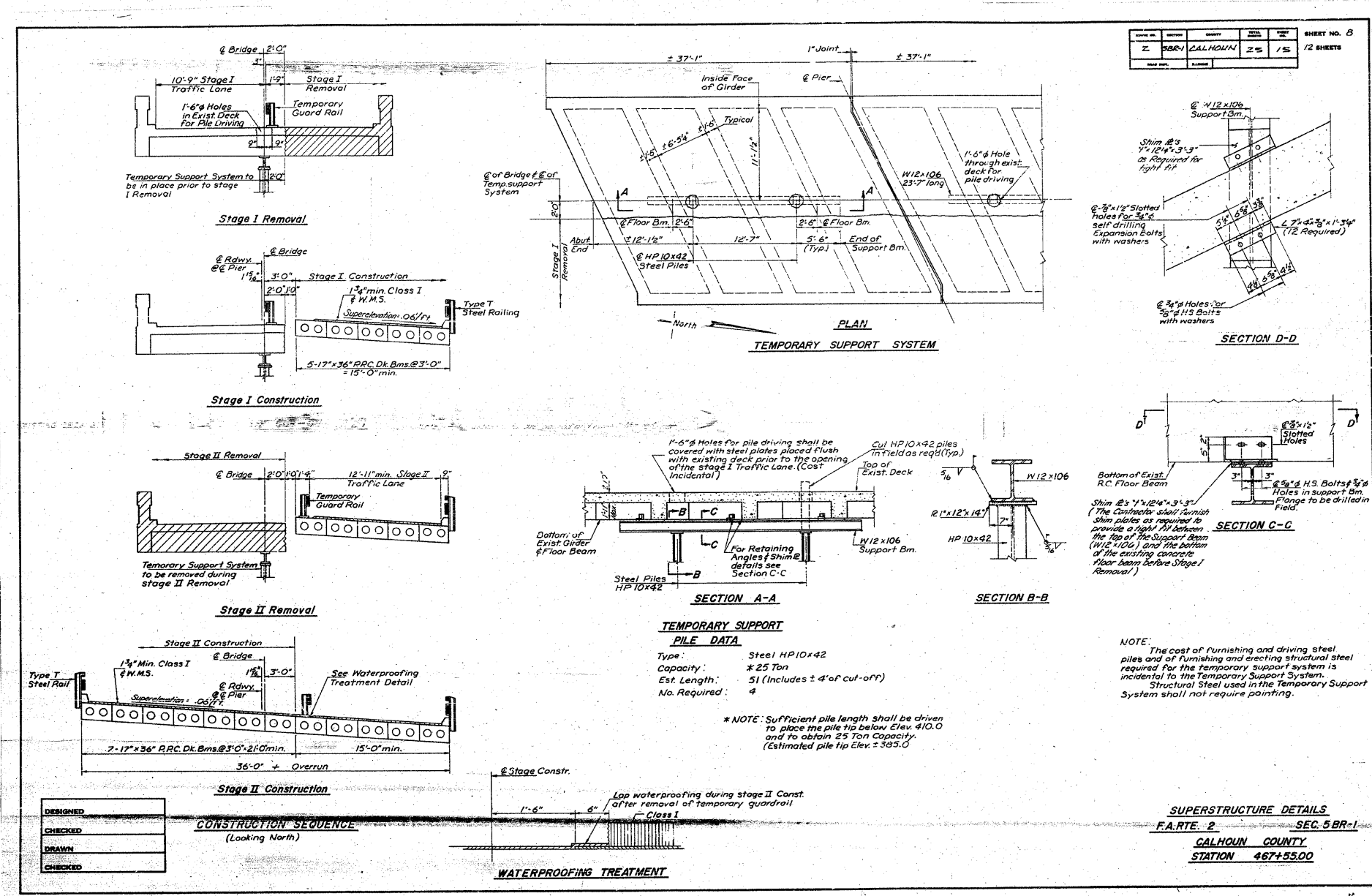
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**EXISTING STRUCTURE PLANS**  
 FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 DATE \_\_\_\_\_ HORIZ. \_\_\_\_\_  
 DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

PLOT DATE = 1/24/2009  
 FILE NAME = c:\projects\76948\5br2\5br2.dwg  
 PLOT SCALE = 1/8" = 1'-0"  
 REFERENCE = 5BR2



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	64
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



FOR INFORMATION ONLY

PLOT DATE = 1/24/2008  
 FILE NAME = c:\pwork\jess\veg9805\plan\p10905.dgn  
 PLOTTER = HP DesignJet 500 / IN  
 REFERENCE = REF#

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
		EXISTING STRUCTURE PLANS

FAP ROUTE 304  
 SECTION 5BR-2  
 CLINTON COUNTY

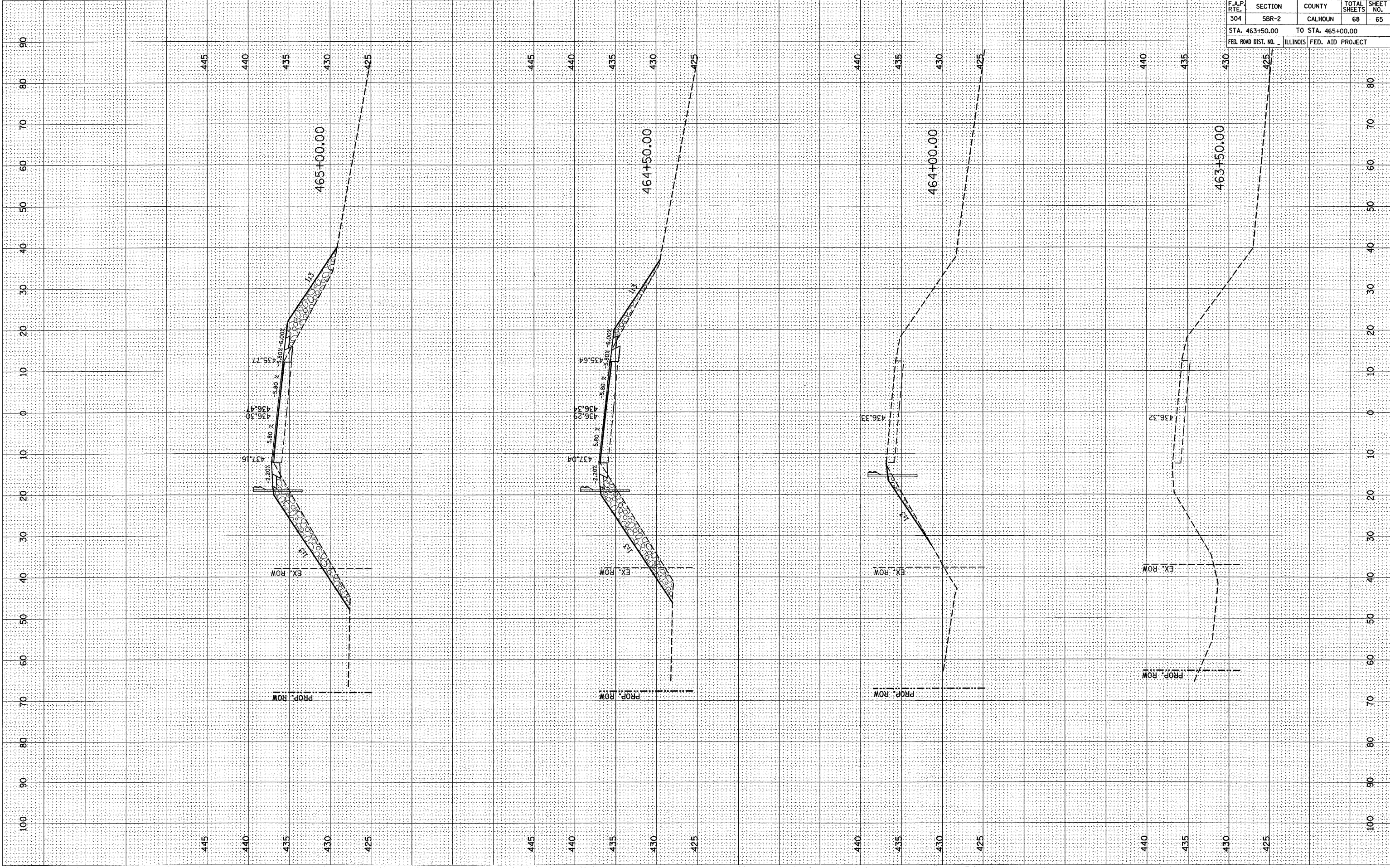
SCALE: VERT. DATE  
 HORIZ. DATE

DRAWN BY  
 CHECKED BY

PLOT DATE = 1/20/2008  
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 PLOT SCALE = 10.0000 / IN  
 USER NAME = gclink

ORIGINAL SURVEY  
 BY  
 DATE

FINAL SURVEY  
 BY  
 DATE



CONTRACT NO. 76948

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	65

STA. 463+50.00 TO STA. 465+00.00  
 ILLINOIS FED. AID PROJECT

PLOT DATE = 1/30/2008  
 PLOT SCALE = 1/4" = 100.00' / IN.  
 USER NAME = gmluh

ORIGINAL SURVEY  
 CHECKED  
 PLOTTED  
 TEMPLATE  
 AREAS CHECKED

FINAL SURVEY  
 CHECKED  
 PLOTTED  
 TEMPLATE  
 AREAS CHECKED

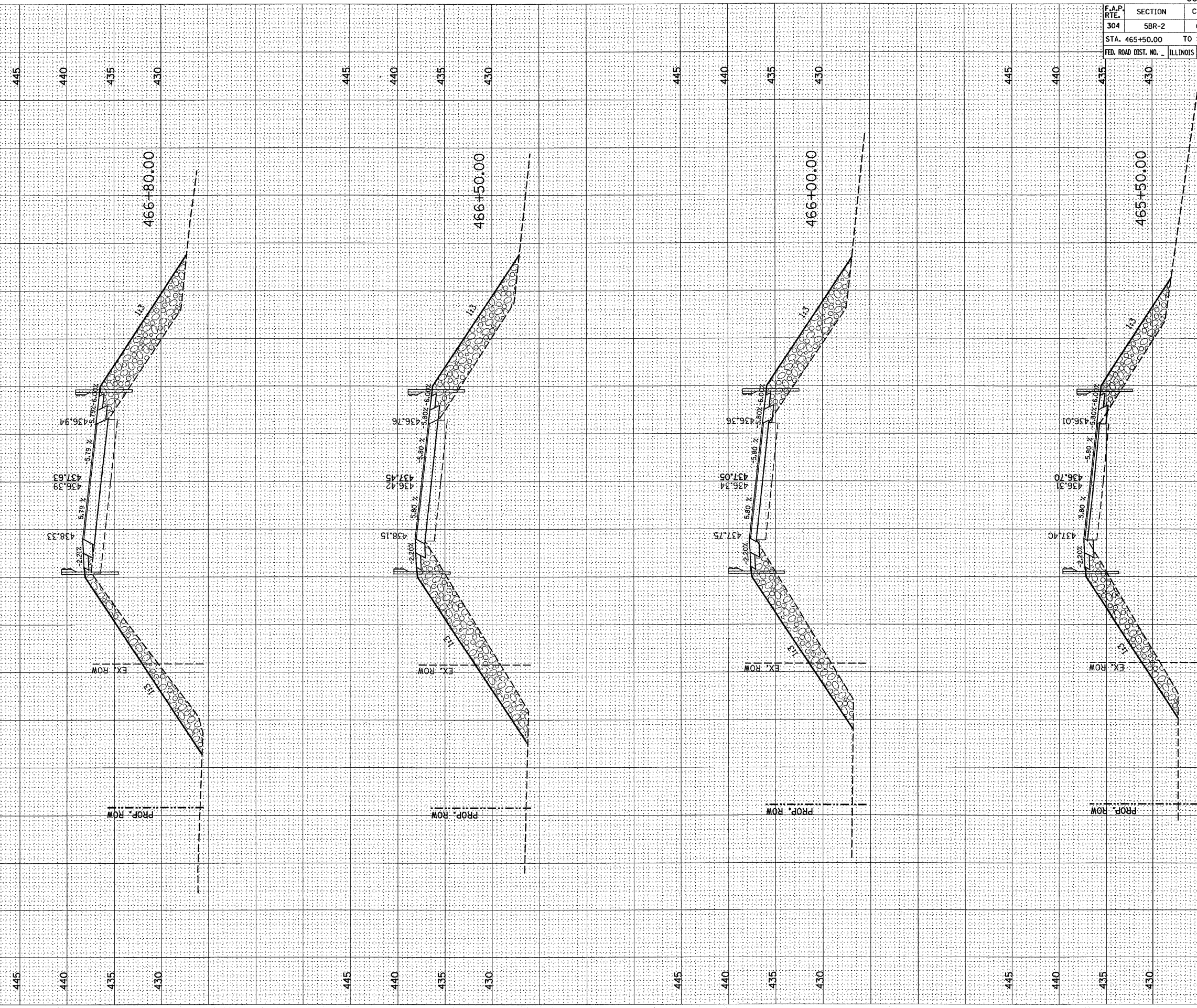
BY DATE

BY DATE

CONTRACT NO. 76948

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	58R-2	CALHOUN	68	66
STA. 465+50.00		TO STA. 466+80.00		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

Ø STRUCTURE @ STA. 467+59.00

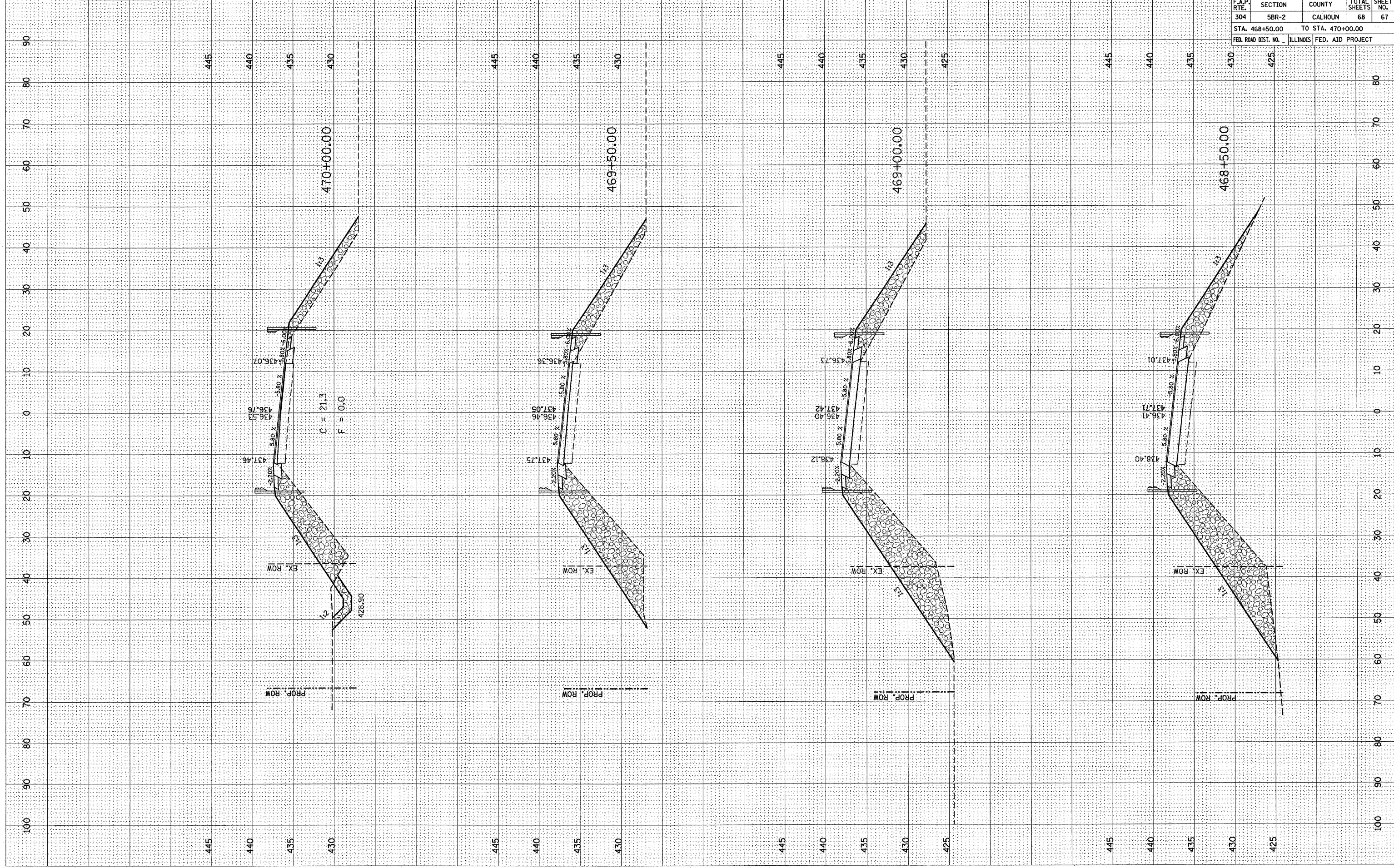


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USER NAME = gaur

ORIGINAL SURVEY  
SURVEY NO.  
DATED  
TEMP. PLATE NO.  
AREAS CHECKED  
AREAS CHECKED

FINAL SURVEY  
SURVEY NO.  
DATED  
TEMP. PLATE NO.  
AREAS CHECKED  
AREAS CHECKED

BY  
DATE



CONTRACT NO. 76948

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	67

STA. 468+50.00 TO STA. 470+00.00  
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

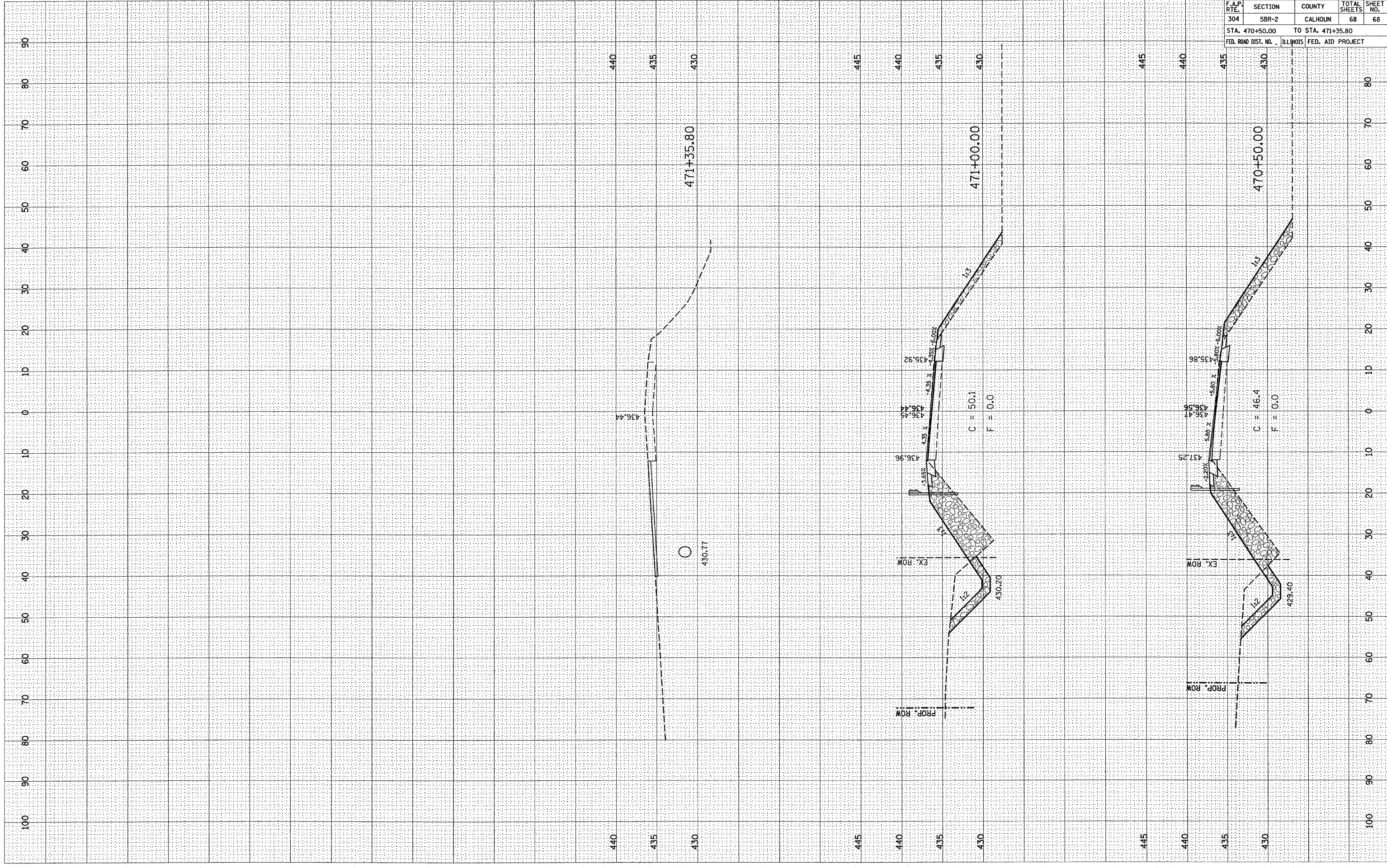
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 USER NAME = galuh

ORIGINAL SURVEYED  
 SURVEYED  
 SURVEYED  
 SURVEYED  
 SURVEYED  
 SURVEYED

FINAL SURVEYED  
 SURVEYED  
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BY \_\_\_\_\_

DATE \_\_\_\_\_



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
304	5BR-2	CALHOUN	68	68
STA. 470+50.00		TO STA. 471+35.80		
FED. ROAD DIST. NO. _____		ILLINOIS FED. AID PROJECT		