

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2012-0631	COOK	18	1
ILLINOIS			CONTRACT NO. 60V76	

D-91-066-13

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN THE CITY OF CHICAGO

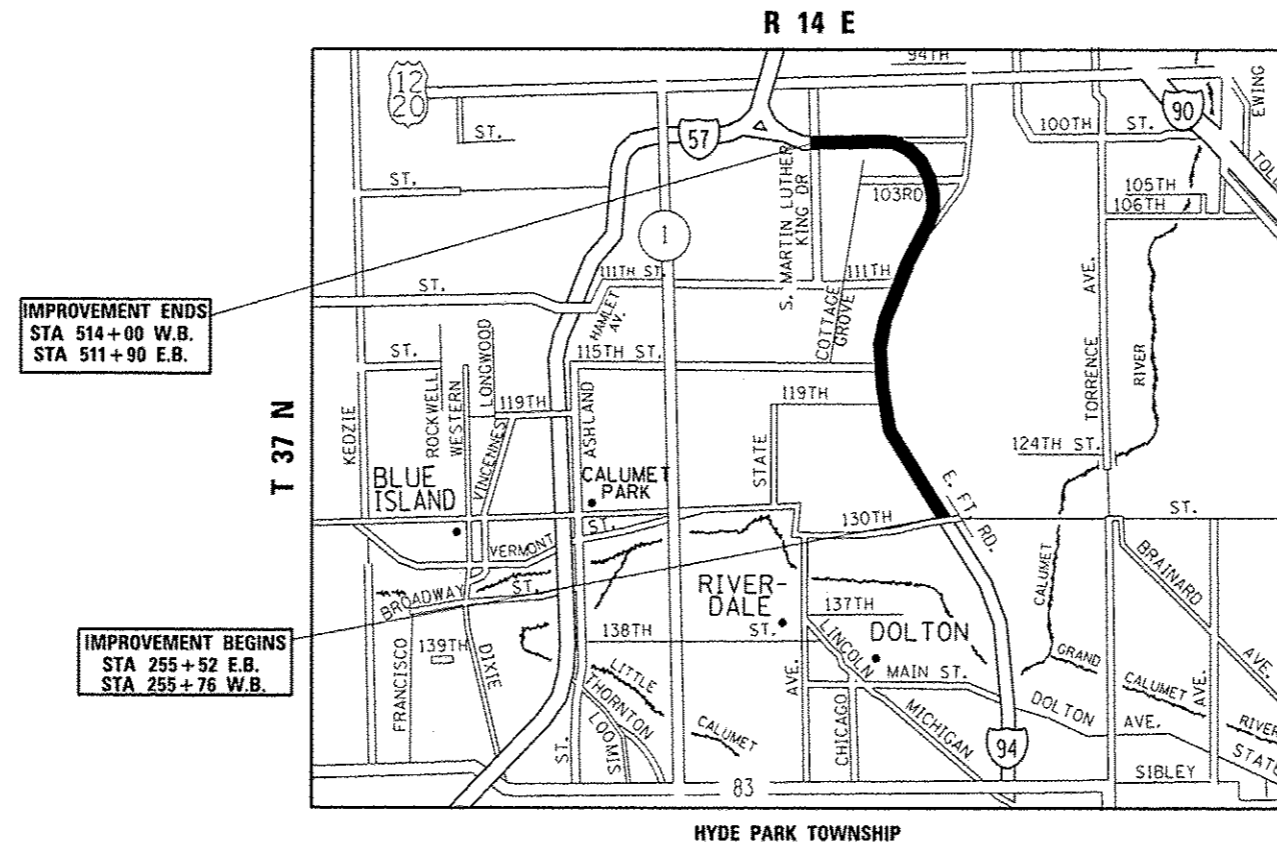
**PROPOSED
HIGHWAY PLANS**

F.A.I. ROUTE 94: I-94 (BISHOP FORD EXPY)
W OF MARTIN LUTHER KING DR TO 130th ST
SECTION: 2012-0631
CRACK AND JOINT SEALING
COOK COUNTY

C-91-114-07

TRAFFIC DATA

2010 ADT = 155800
POSTED SPEED LIMIT = 55 MPH



IMPROVEMENT ENDS
STA 514+00 W.B.
STA 511+90 E.B.

IMPROVEMENT BEGINS
STA 255+52 E.B.
STA 255+76 W.B.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

PROJECT ENGINEER MIDY J. ALAIN (847) 221-3056
PROJECT MANAGER ISSAM RAYYAN (847) 705-4178

GROSS LENGTH = 25824 FT. = 4.89 MILE

CONTRACT NO. 60V76

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED February 7 20 13
[Signature]
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 22 20 13
John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

March 22 20 13
Omer Osuwan, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
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STANDARDS

701400-06	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-07	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-08	LANE CLOSURE, MULTILANE AT ENTRANCE OR EXIT RAMP, FOR SPEEDS \geq 45 MPH
701426-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS \geq 45 MPH
701446-04	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701901-02	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACT SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4151 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT (708) 597-9800 A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

ALL CRACK ROUTING & CRACK SEALING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. CRACK ROUTING SHALL BE OMITTED WHERE VEHICLE DETECTOR LOOPS & RAISED REFLECTIVE PAVEMENT MARKERS ARE PRESENT IN THE PAVEMENT.

MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN THE EXISTING PAVEMENT MAY BE PLACED, SHAPED AND COMPACTED TO THE SATISFACTION OF THE ENGINEER ALONG EXISTING AGGREGATE SHOULDERS ADJACENT TO THE PAVEMENT. ALL MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITHOUT AGGREGATE SHOULDERS AND SURPLUS MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITH AGGREGATE SHOULDERS, WHERE ALL MATERIALS ARE NOT PLACED ALONG EXISTING AGGREGATE SHOULDERS, SHALL BE DISPOSED OF AS SPECIFIED IN ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. OLD SEALANTS REMOVED PRIOR TO ROUTING SHALL BE DISPOSED OF AS SPECIFIED IN ARTICLE 202.03. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

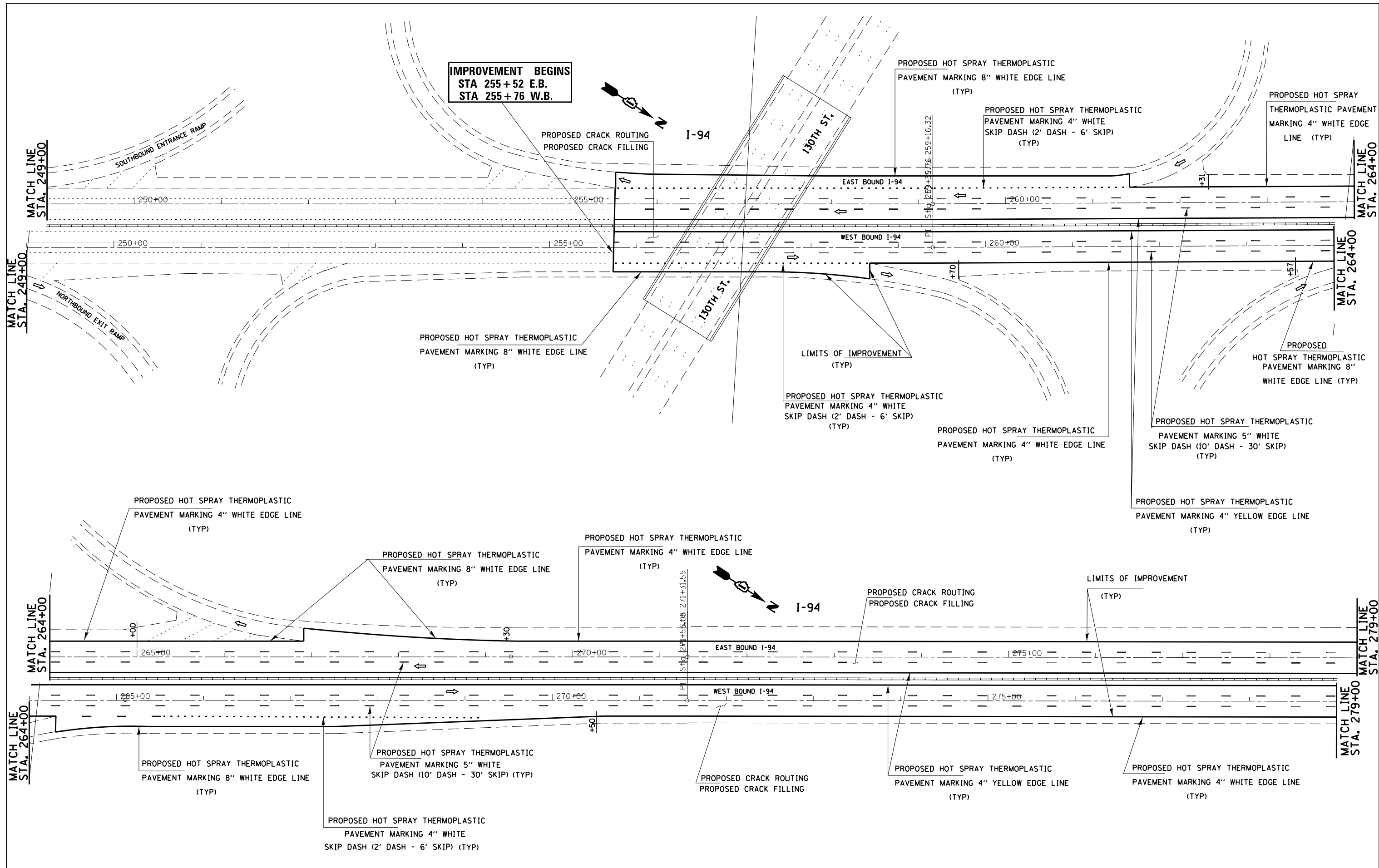
CONTACT IDOT ELECTRIC MAINTENANCE CONTRACTOR AT (773) 287-7600 TO LOCATE ANY ELECTRIC CABLES WITHIN STATE R.O.W.

FILE NAME *	USER NAME * pprzanowski	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT DATE * 2/7/2013	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO STA.

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE 0005						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	100% STATE TOTAL QUANTITIES	0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES						
45100100	CRACK ROUTING (PAVEMENT)	FOOT	191913	191913														
45100200	CRACK FILLING	POUND	115150	115150														
67100100	MOBILIZATION	L SUM	1	1														
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1														
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	436.8	436.8														
78300100	PAVEMENT MARKING REMOVAL	SO FT	436.8	436.8														
x7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1														
* x7800815	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 4 INCH	FOOT	91270	91270														
* x7800825	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 5 INCH	FOOT	21070	21070														
* x7800845	HOT SPRAY THERMOPLASTIC PAVEMENT MARKING LINE - 8 INCH	FOOT	14385	14385														

FILE NAME: c:\p\work\p1001\prj\2003\str\030519\00663-st	USER NAME: pyrzonawskirb	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	I-94 (W OF MARTIN LUTHER KING DR TO 130th ST) SUMMARY OF QUANTITIES	F.A.I. RTE. 94	SECTION 2012-0631	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 3
PLOT SCALE: 1000000' / in.	CHECKED: -	REVISED: -	SCALE:			SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
PLOT DATE: 2/7/2003	DATE: -	REVISED: -	CONTRACT NO. 60V76							



IMPROVEMENT BEGINS
 STA 255+52 E.B.
 STA 255+76 W.B.

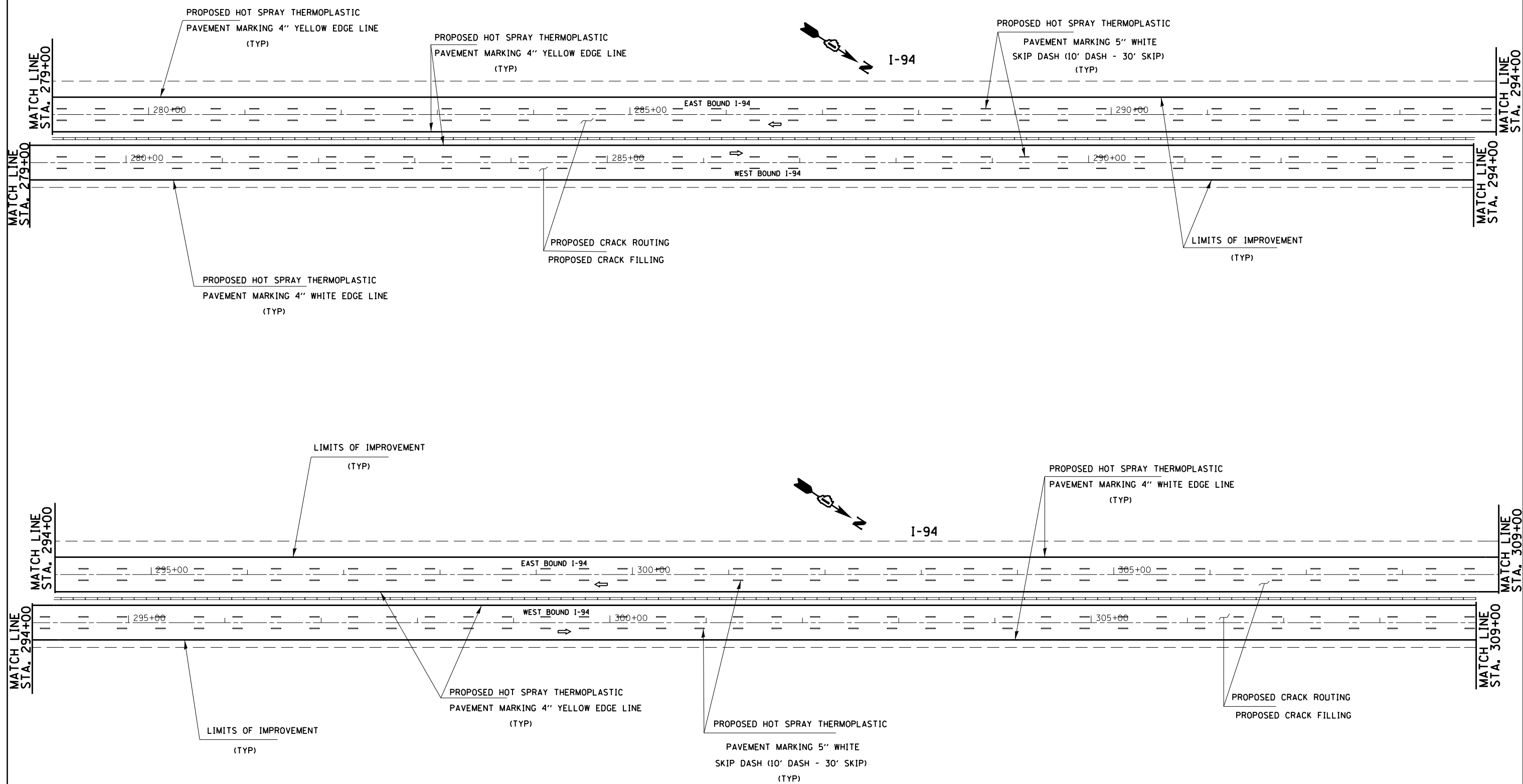
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**I-94 (W OF MARTIN LUTHER KING DR TO 130th ST)
 PROPOSED PLANS AND PAVEMENT MARKING**

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DATE = 12/13/2013	DATE -	REVISED -

F.A.I. RTE. 94	SECTION 2012-0631	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 4
CONTRACT NO. 60V76			ILLINOIS FED. AID PROJECT	

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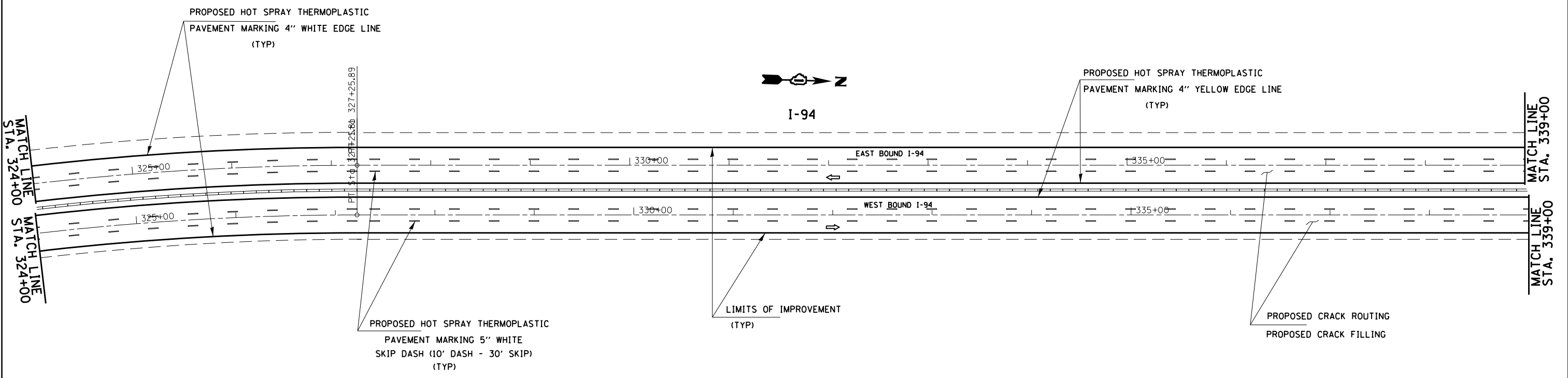
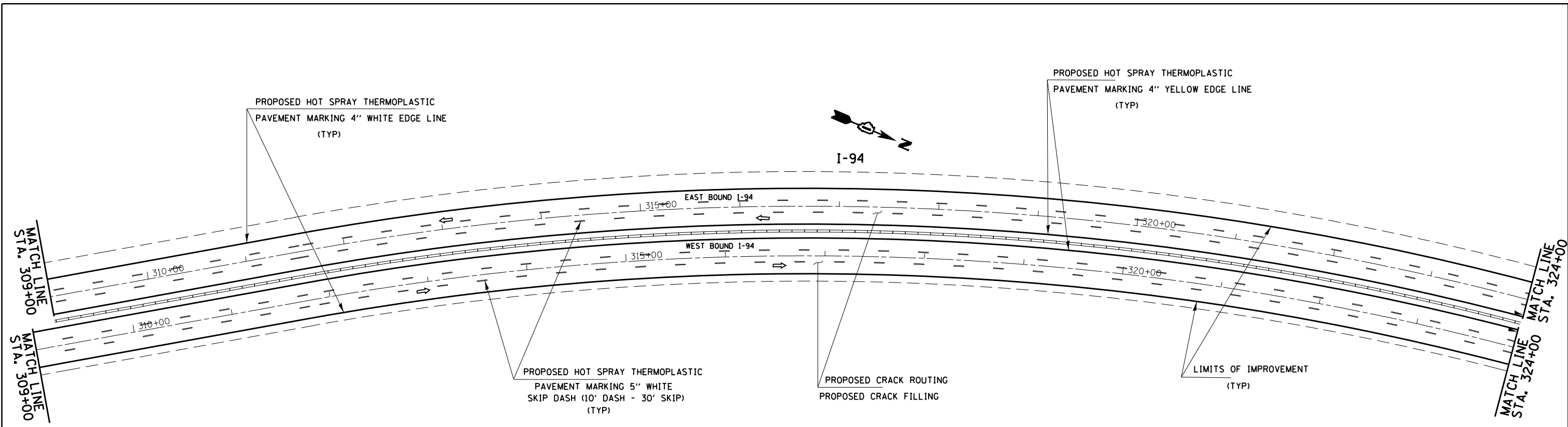
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

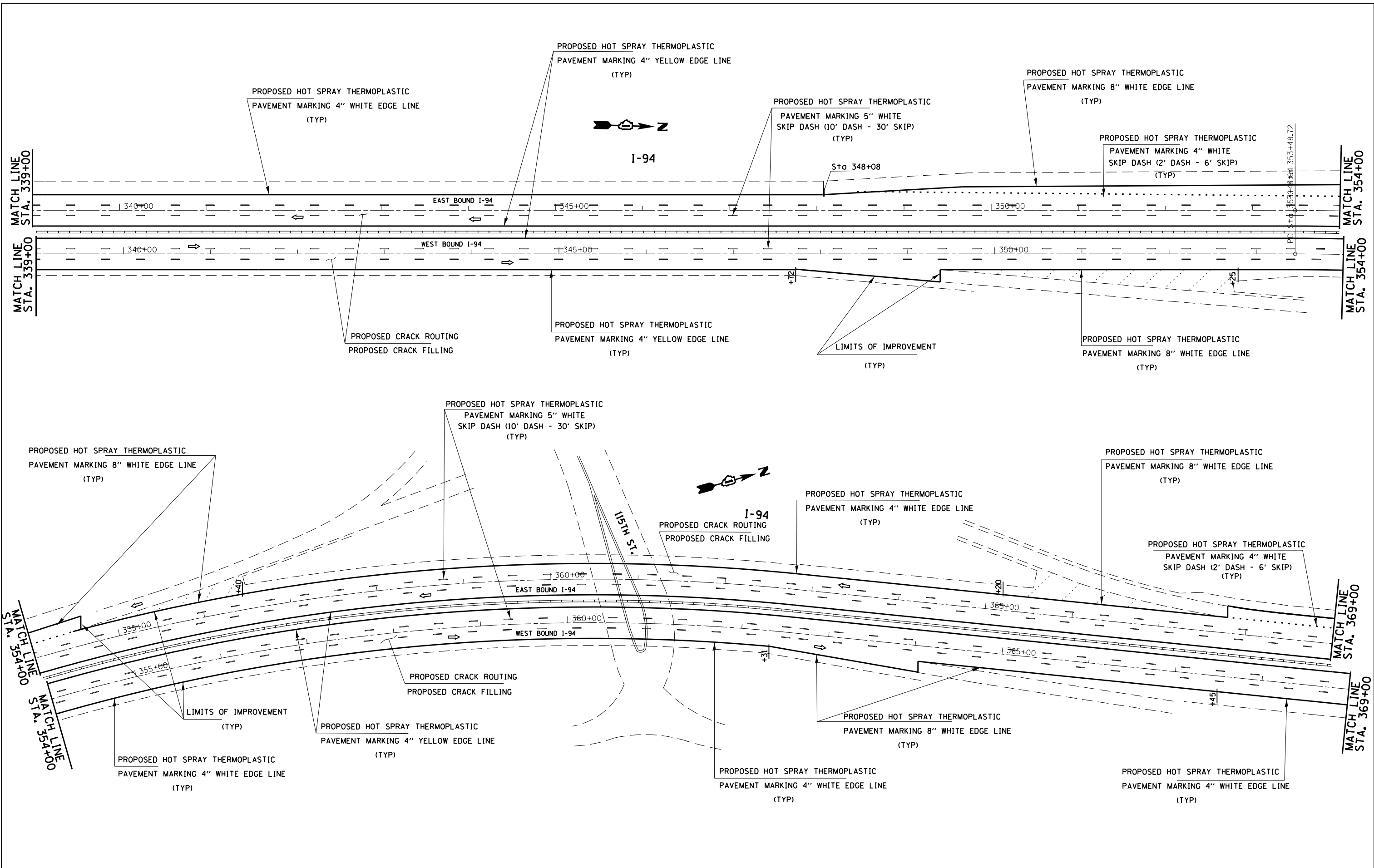
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PROPOSED PLANS AND PAVEMENT MARKING**

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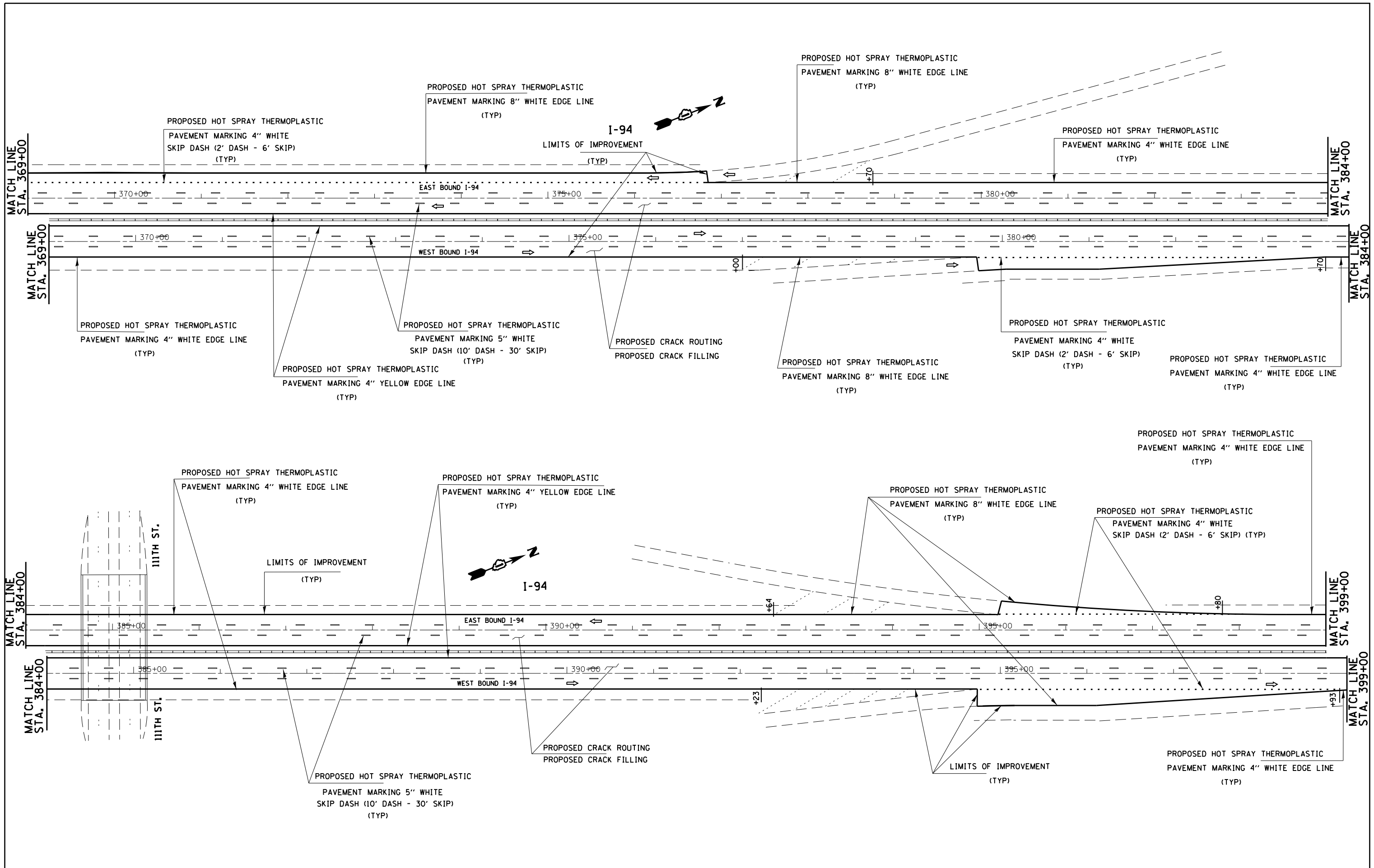
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CONTRACT NO. 60V76				
ILLINOIS FED. AID PROJECT				



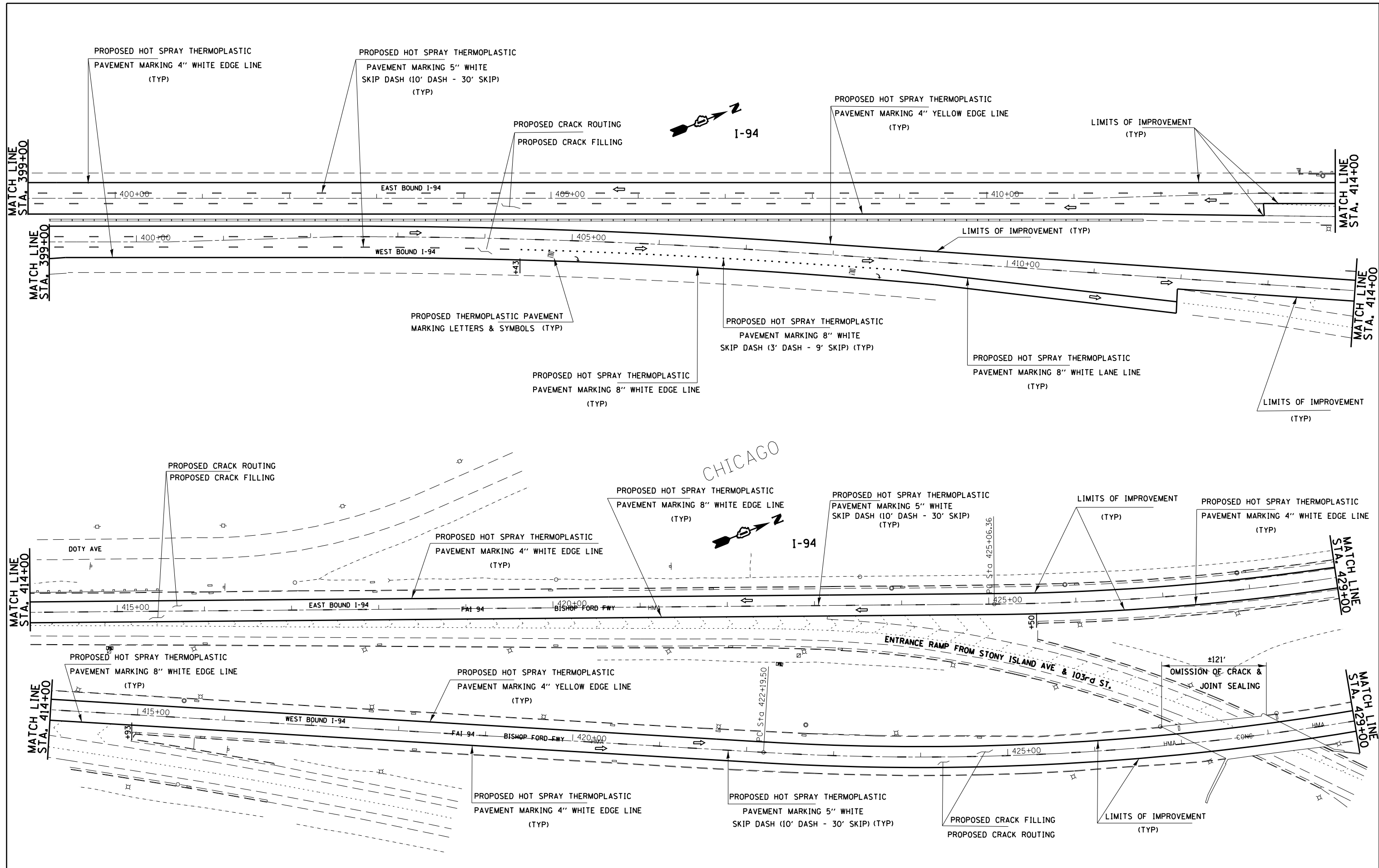
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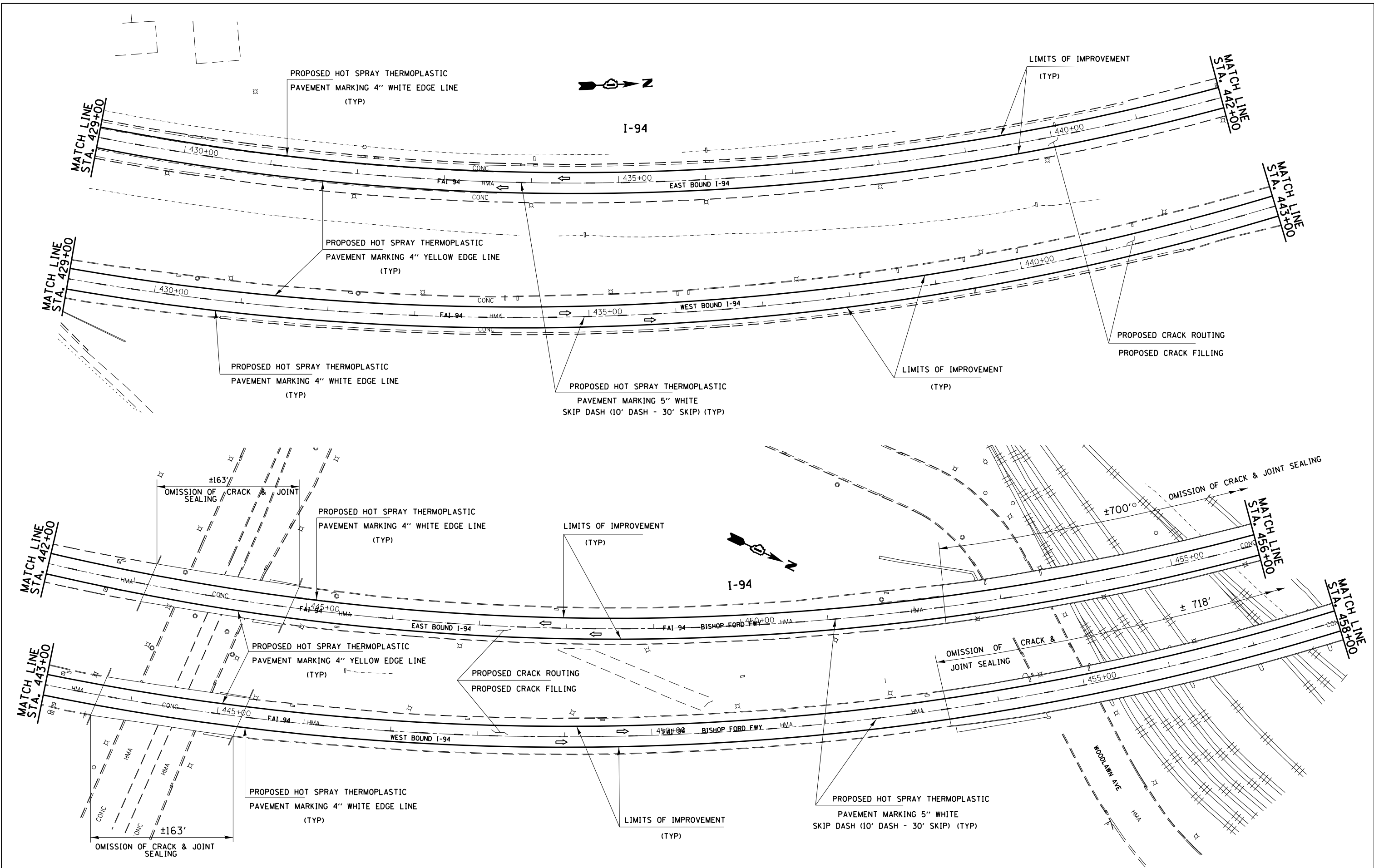
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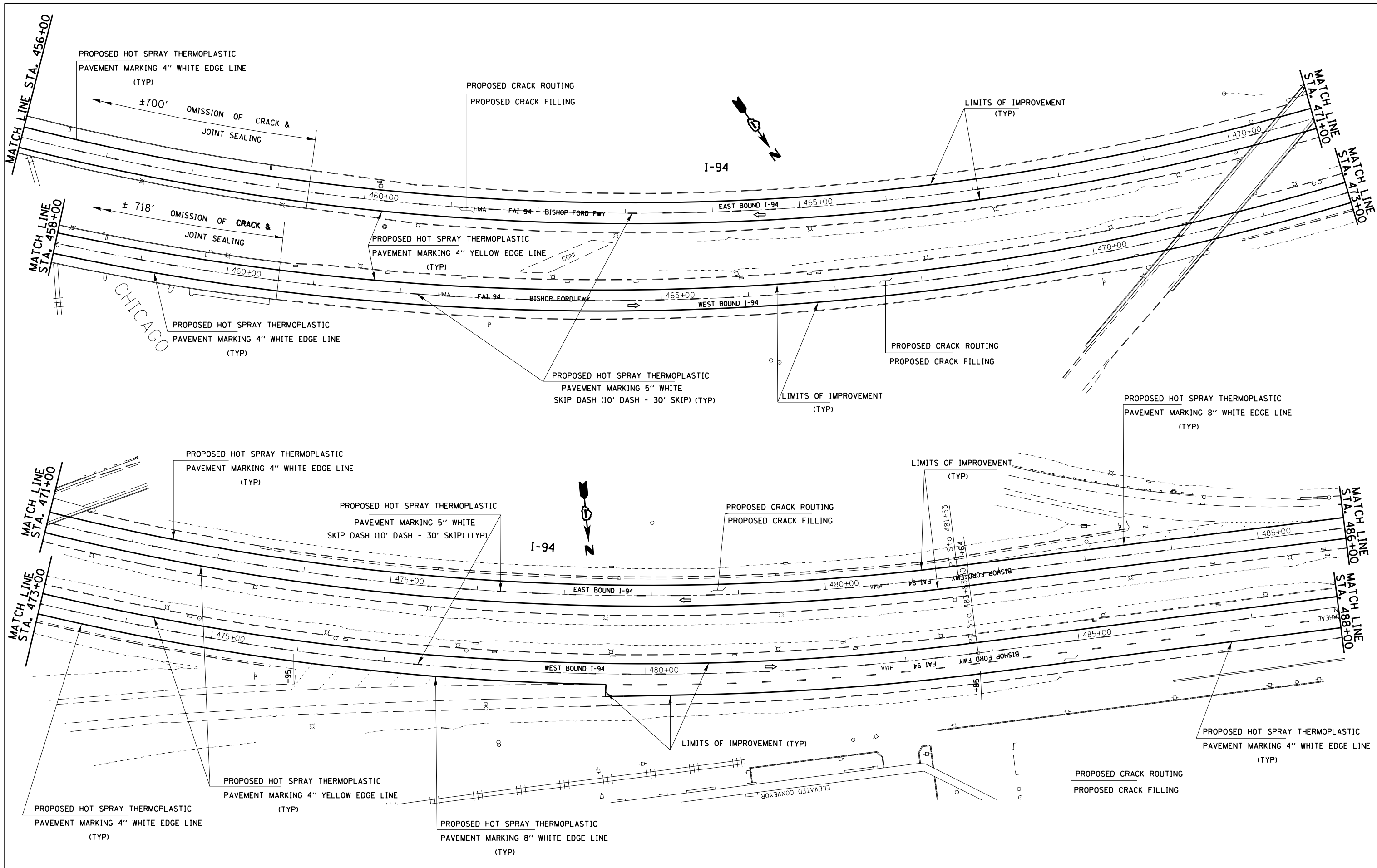
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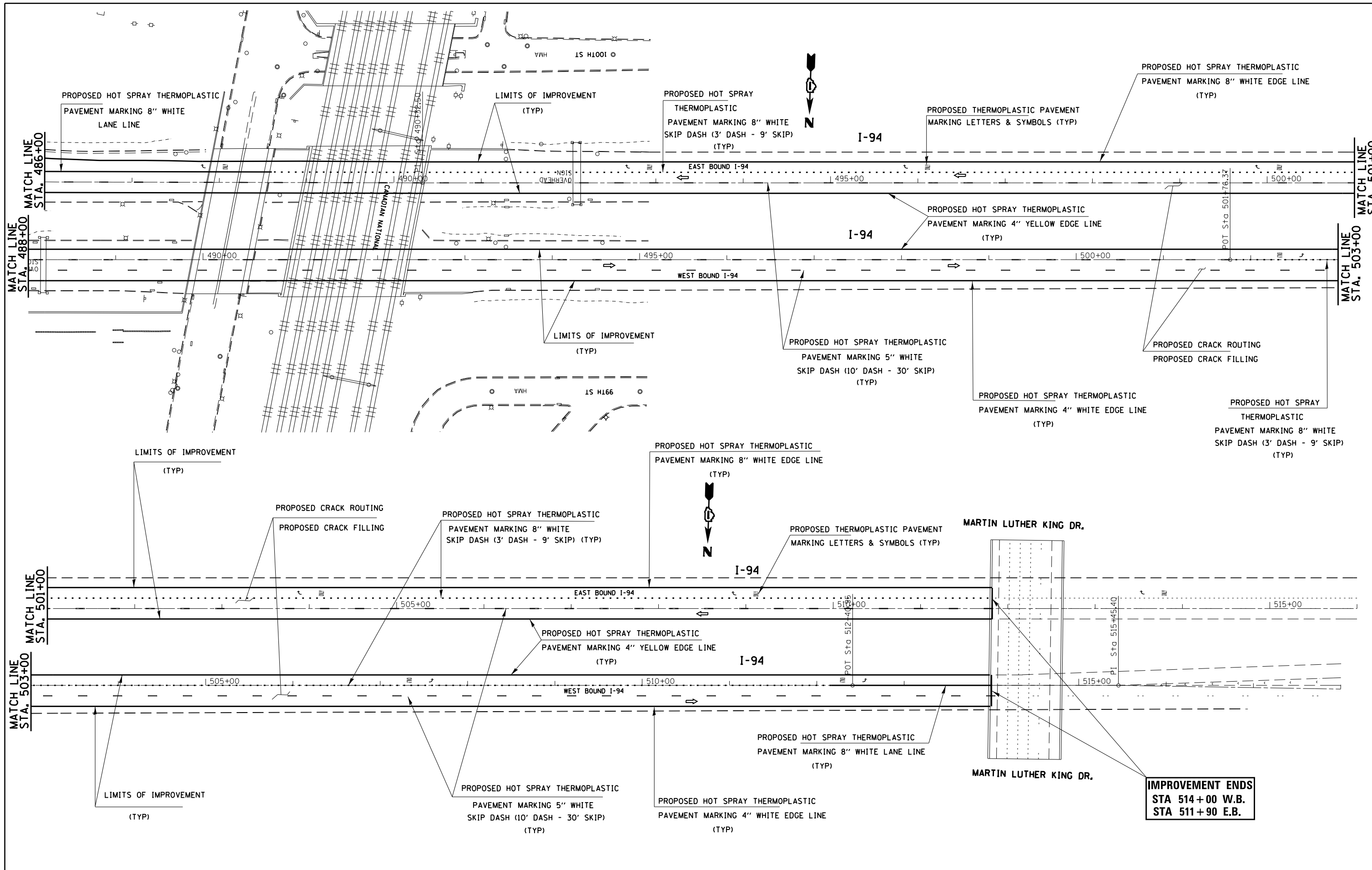
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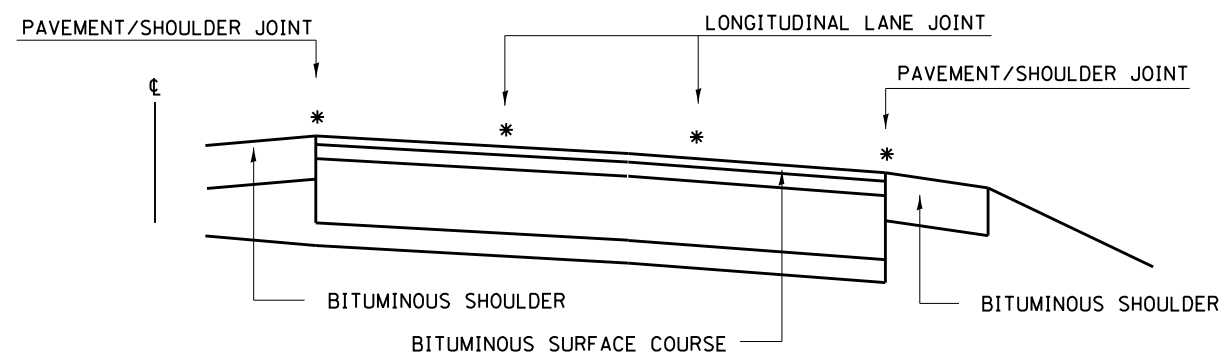
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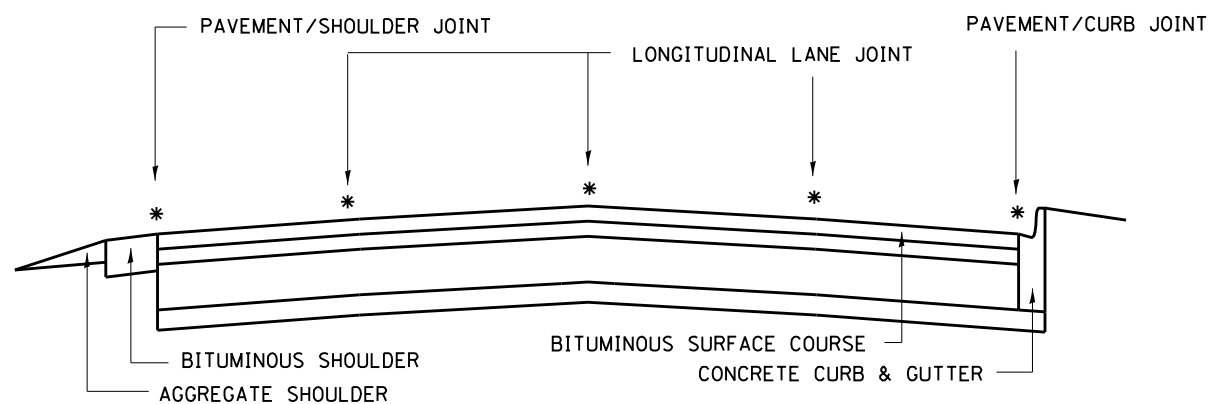
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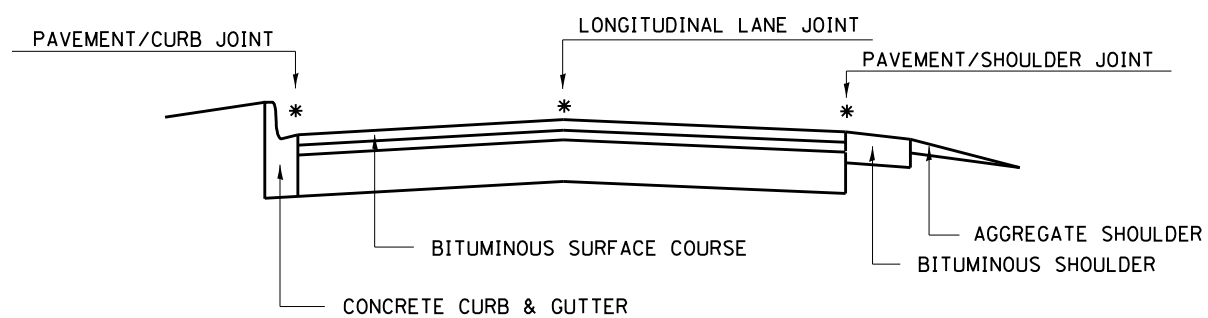
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	PLOT DATE = 2/7/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



MULTI-LANE DIVIDED PAVEMENT



MULTI-LANE UNDIVIDED PAVEMENT

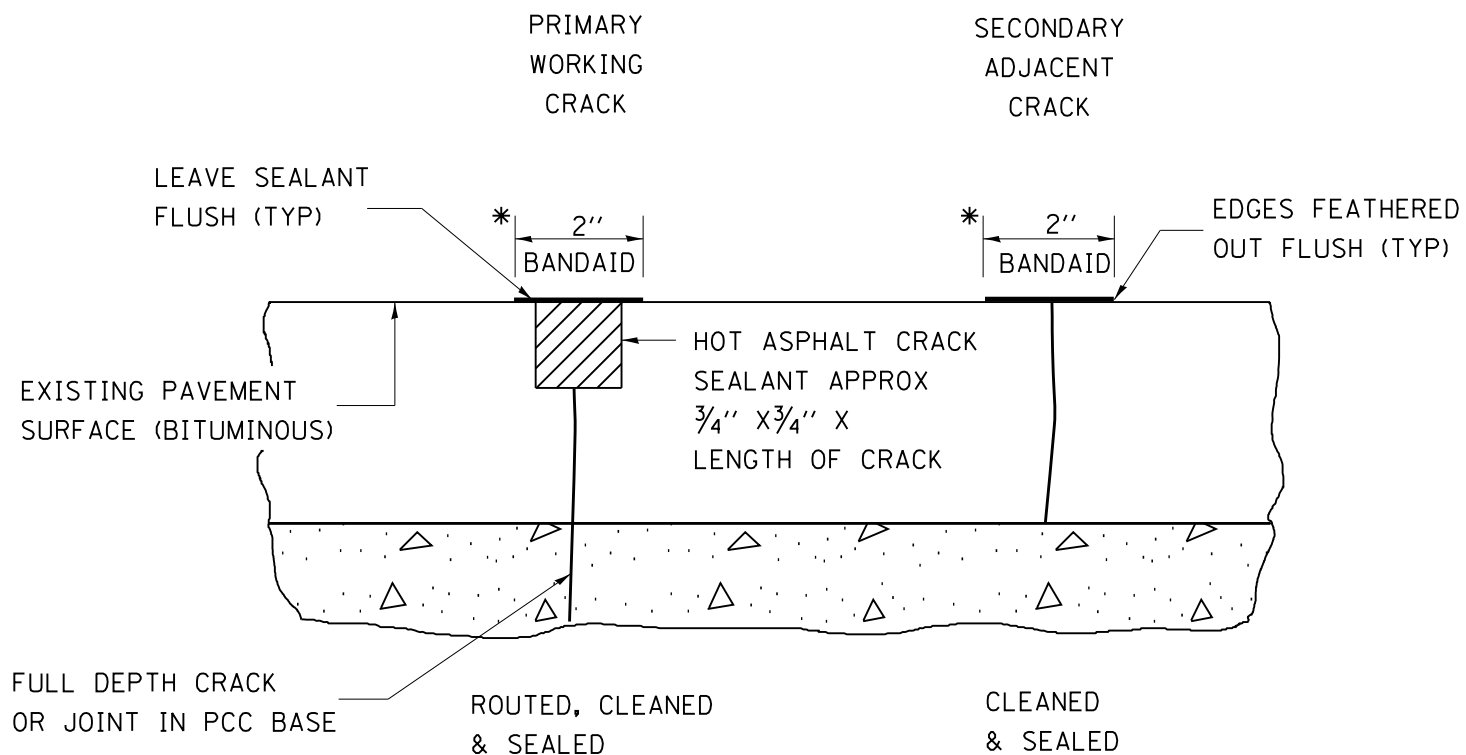


TWO-LANE PAVEMENT

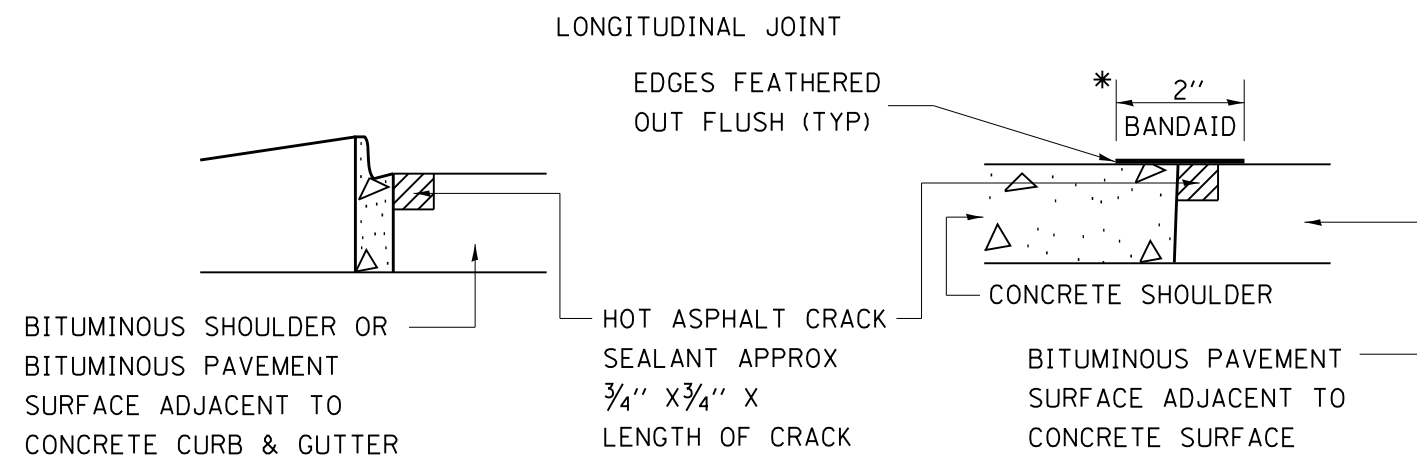
PROPOSED TRANSVERSE CRACK ROUTING (PAVEMENT) & CRACK FILLING LOCATIONS NOT SHOWN

* PROPOSED LONGITUDINAL CRACK ROUTING (PAVEMENT) & CRACK FILLING LOCATIONS

GENERAL EXISTING TYPICAL SECTIONS



* IN ALL LOCATIONS WHERE THE LONGITUDINAL CRACK CONFLICTS WITH EXISTING PAVEMENT MARKINGS (EITHER EDGE LINE OR LANE LINE) THE CRACK SHALL BE ROUTED & THE RESULTING RESERVOIR FILLED WITH SEALANT. HOWEVER, THE 2" BANDAID SEALANT STRIP SHALL BE DELETED TO PREVENT UNNECESSARY DAMAGE TO THE EXISTING PAVEMENT MARKINGS. ALL DAMAGES TO EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR. ALL EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS SHALL BE RESTORED WITH HOT SPRAY THERMOPLASTIC PAVEMENT MARKINGS.



CRACK & JOINT SEALING DETAIL

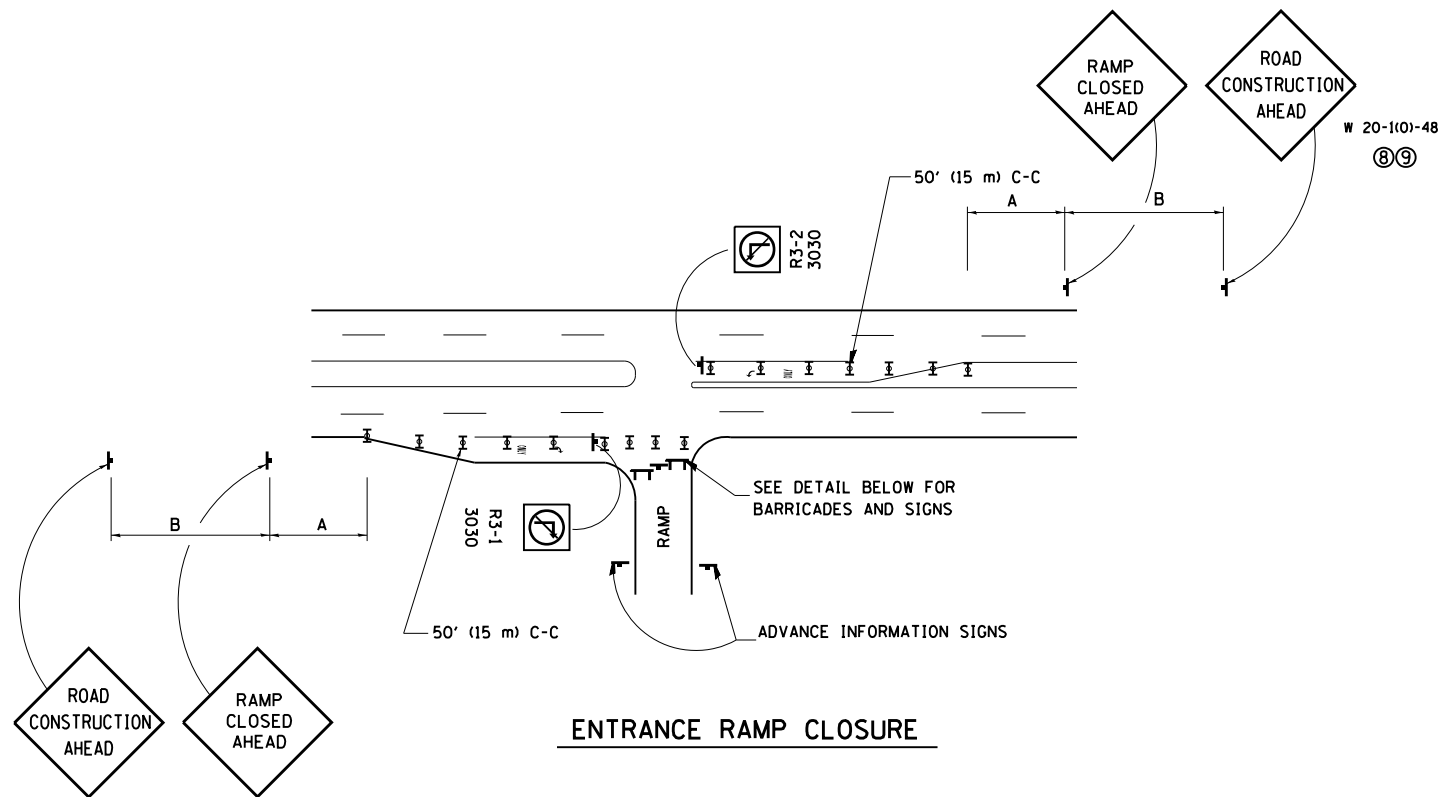
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	PLOT DATE = 2/7/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-94 (BISHOP FORD EXPY) (130th ST TO US (159th ST))
CRACK & JOINT SEALING DETAIL

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE. 94	SECTION 2012-0641	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 13
CONTRACT NO. 60V77				
ILLINOIS FED. AID PROJECT				

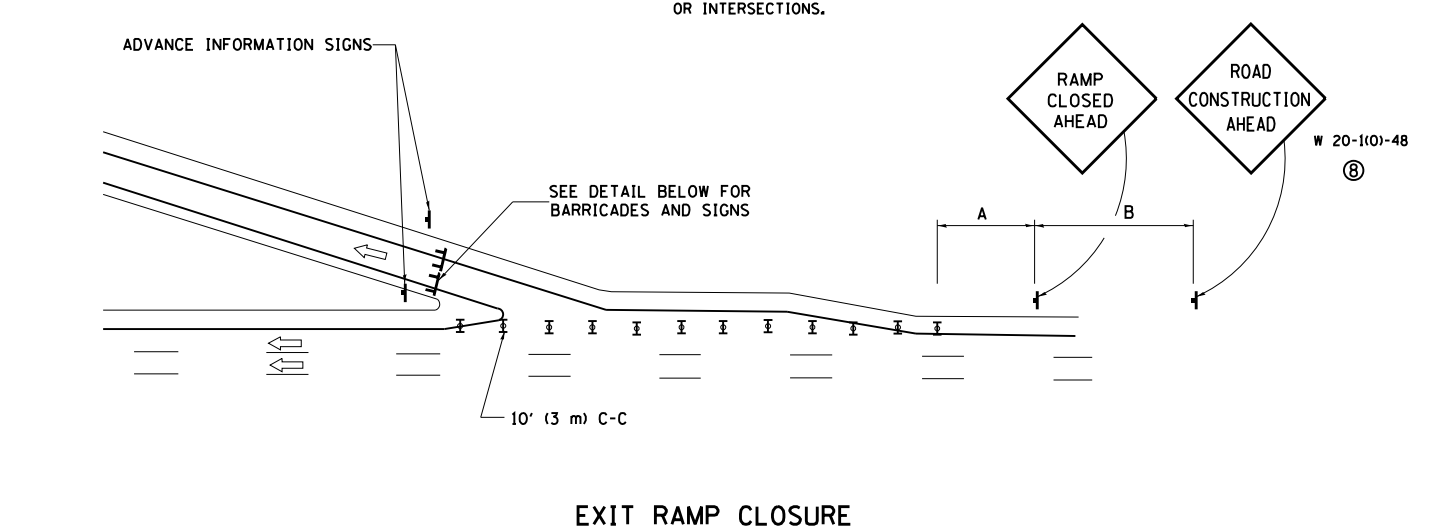


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

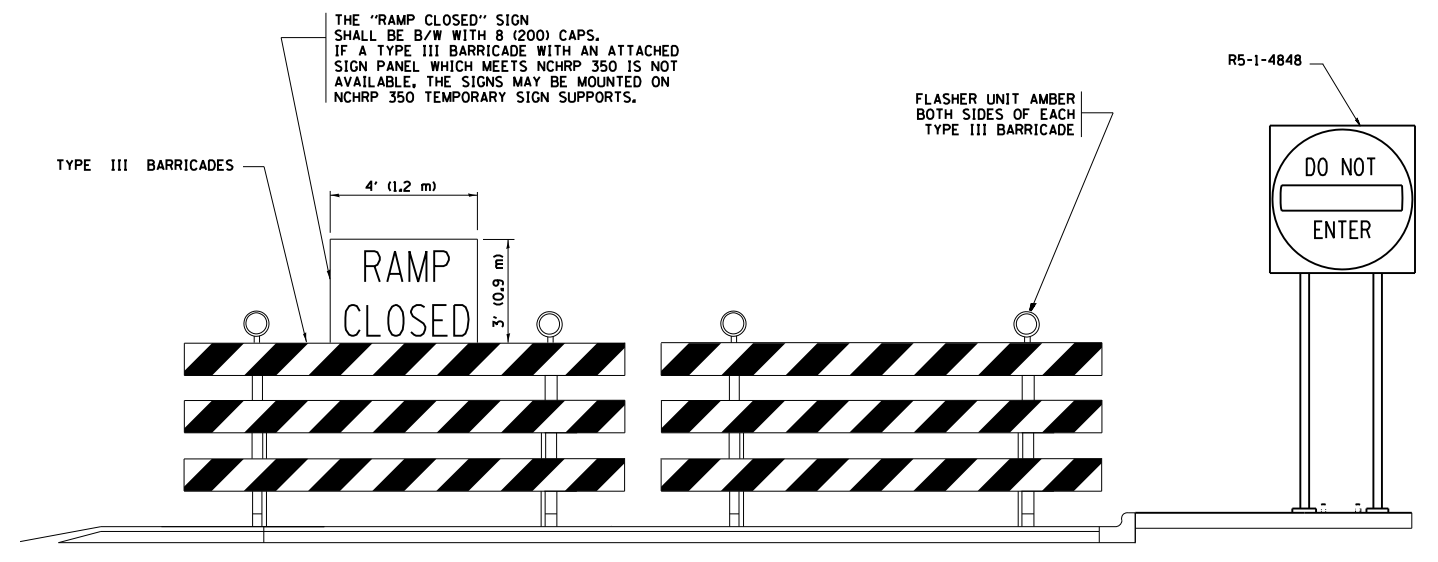
FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL >45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	150' (45 m)	150' (45 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

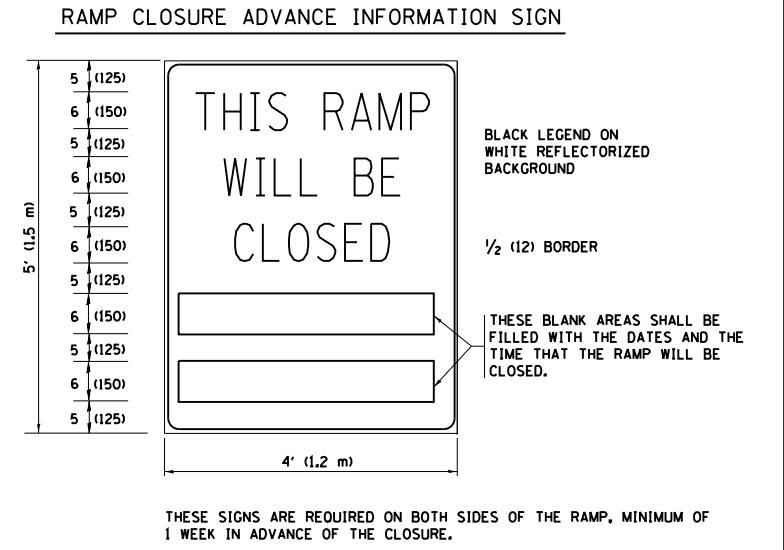
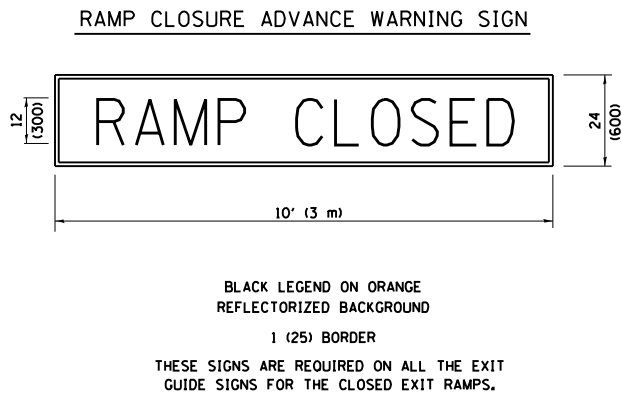


EXIT RAMP CLOSURE

- SYMBOLS**
- ⊥ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ⊓ TYPE III BARRICADE WITH FLASHING LIGHT



DETAIL FOR REQUIRED BARRICADES & SIGNS



GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED ON CLOSURES LESS THAN 24 HOURS IN DURATION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

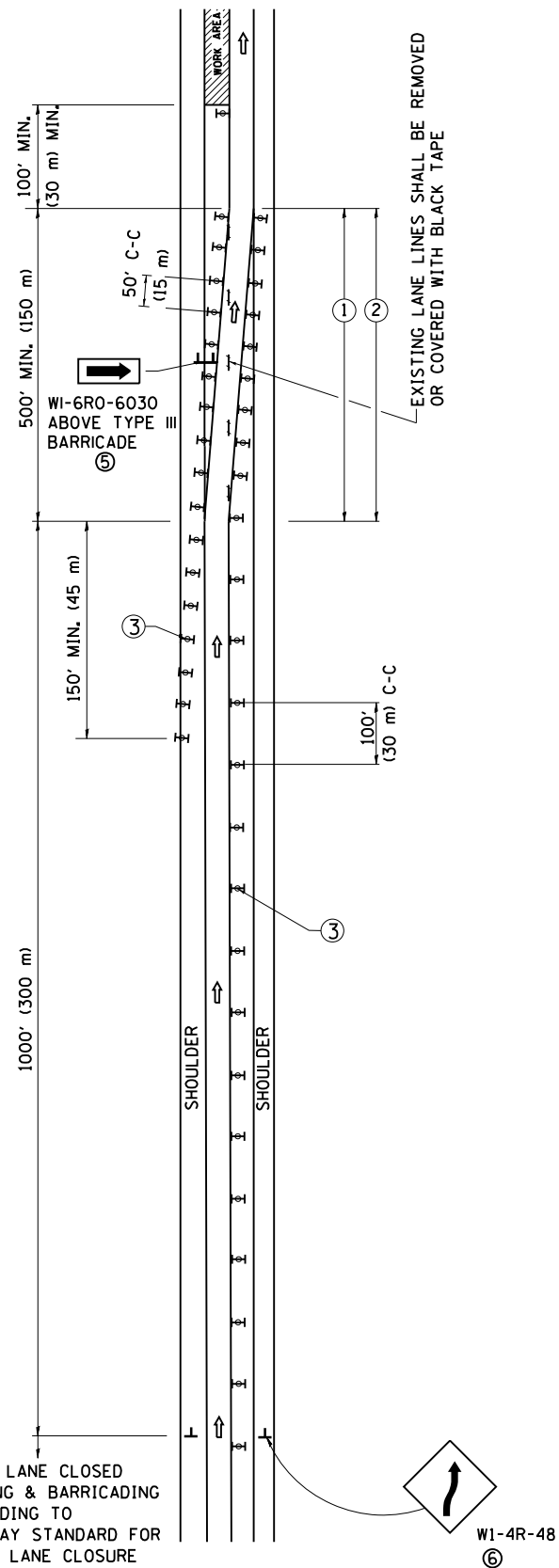
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

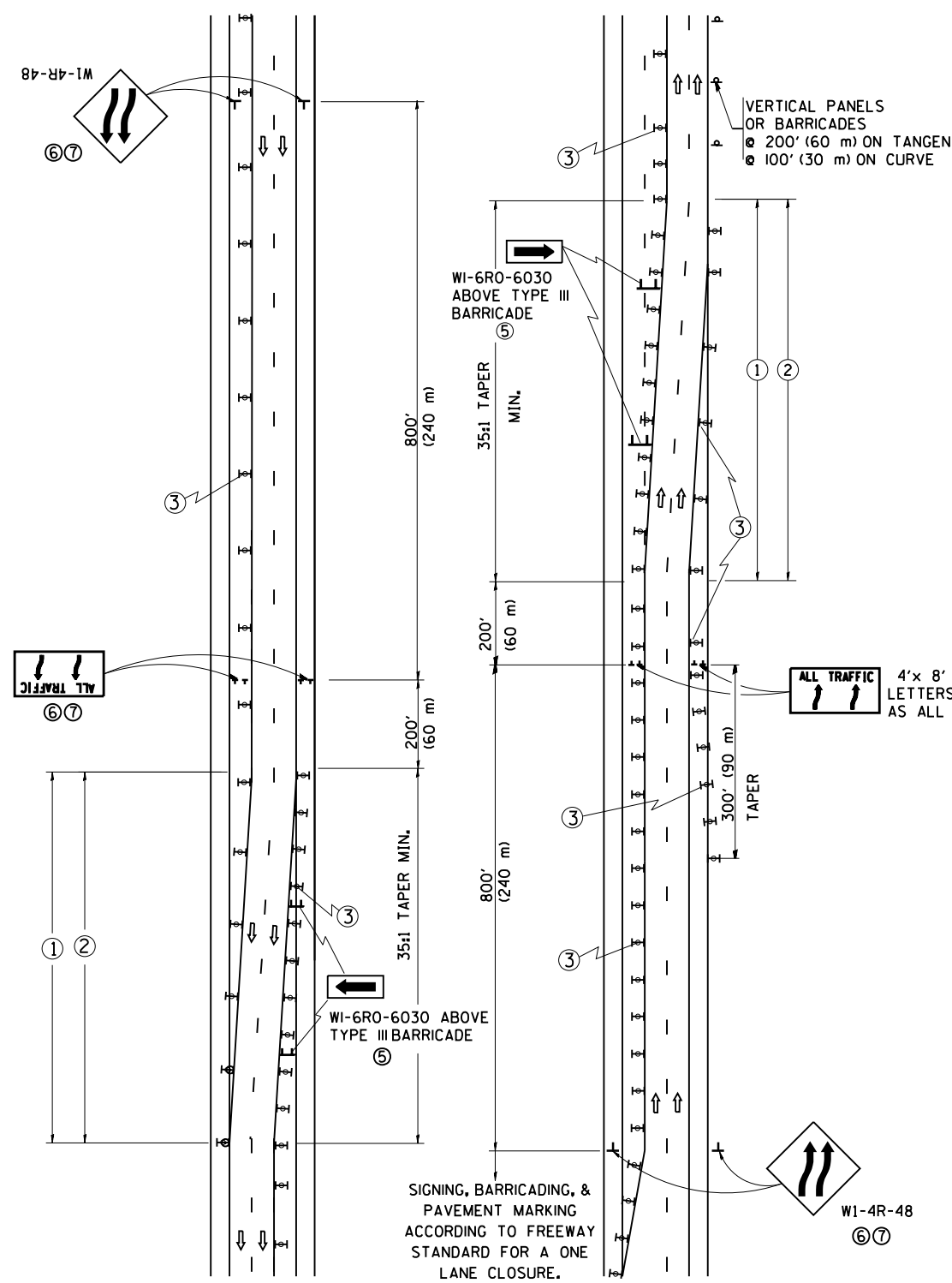
FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	2012-0631	COOK	18	14
TC-08		CONTRACT NO. 60V76		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



- GENERAL NOTES**
- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 24 HOURS IN DURATION.
 - CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
 - PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
 - ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
 - IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS. TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
 - WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
 - THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

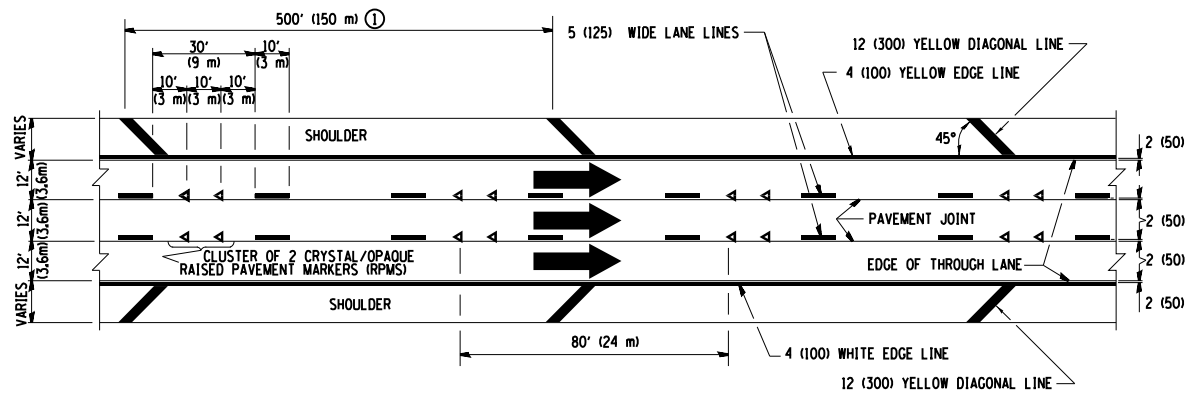
SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT

W24-1-48
⑦

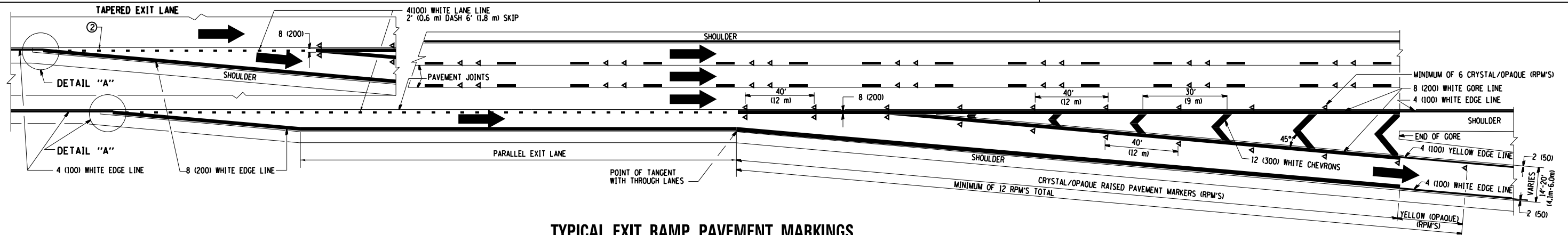
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = pyzranowskirb	DESIGNED - DWS	REVISED - JAF 01-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A.I. RTE. 94	SECTION 2012-0631	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 15
et:\pw\work\p\idot\pyzranowskirb\d0318579\0106613-sht-plan.dgn	DRAWN -	CHECKED -	REVISED - JAF 02-06		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-09		CONTRACT NO. 60V76		
PLOT SCALE = 100.0000' / 1"	DATE - 02-87	REVISOR - SPB 01-07	REVISED - SPB 12-09		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
PLOT DATE = 2/7/2013												

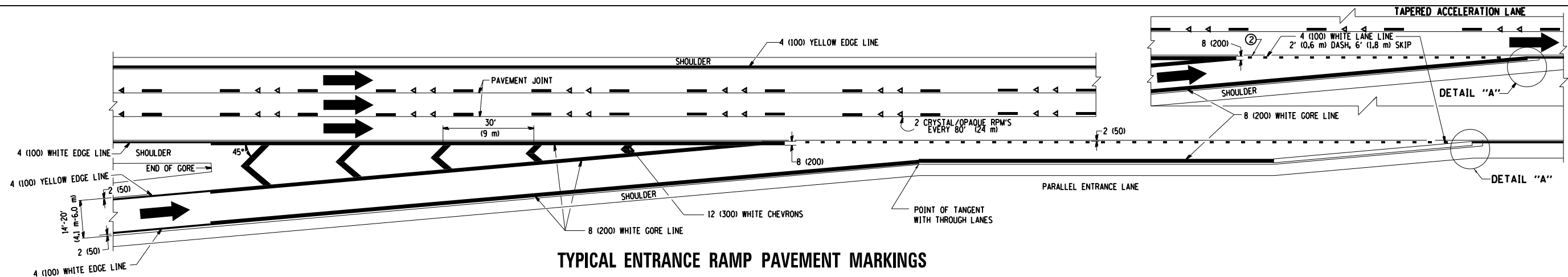


TYPICAL EDGE LINES & LANE LINES

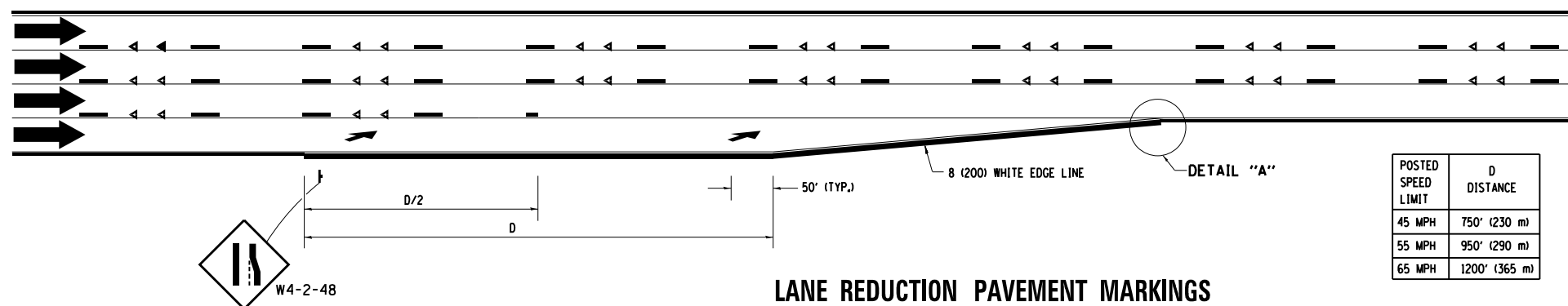
- PAVEMENT MARKING MATERIALS**
1. THERMO PLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR THE EDGE LINES, GORE LINES, AND DIAGONAL LINES ON BITUMINOUS PAVEMENT ONLY.
 2. PREFORMED PLASTIC TYPE B PAVEMENT MARKING LINE SHALL BE USED FOR ALL LANE LINES ON BITUMINOUS PAVEMENT.
 3. POLYUREA PAVEMENT MARKING SHALL BE USED FOR ALL MARKINGS ON PCC.



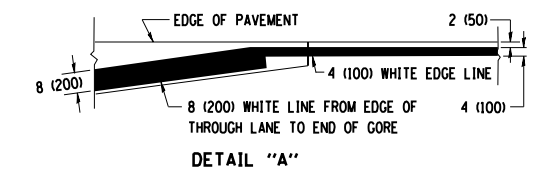
TYPICAL EXIT RAMP PAVEMENT MARKINGS



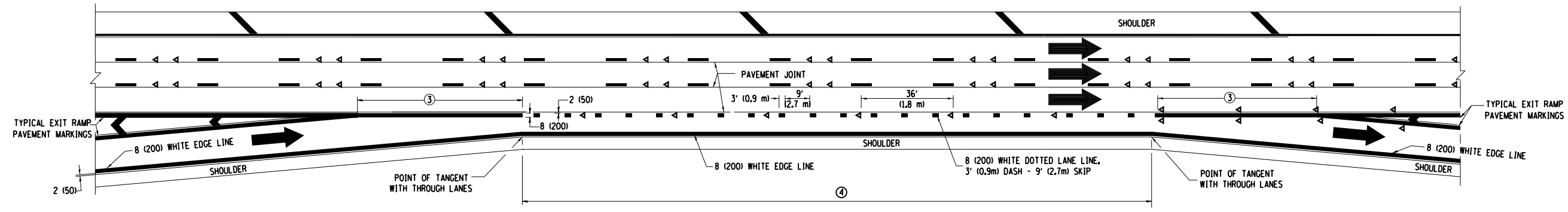
TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



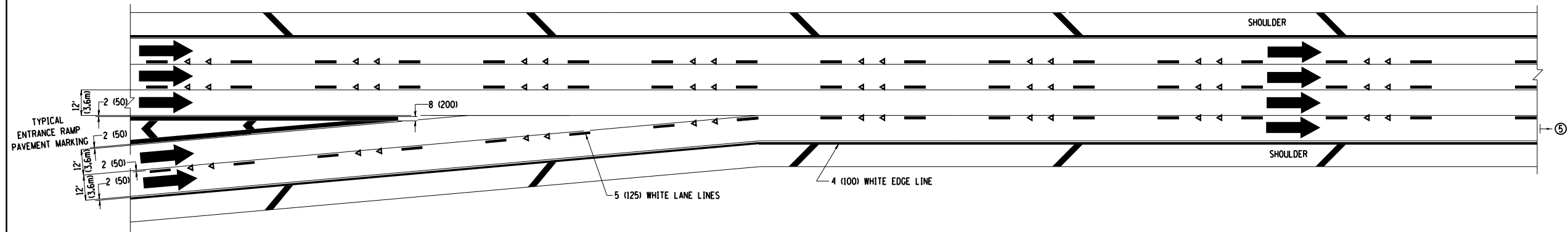
LANE REDUCTION PAVEMENT MARKINGS



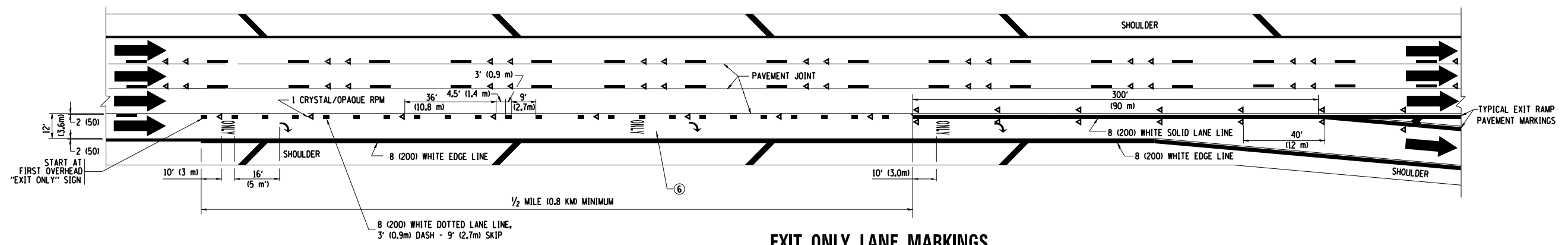
- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.



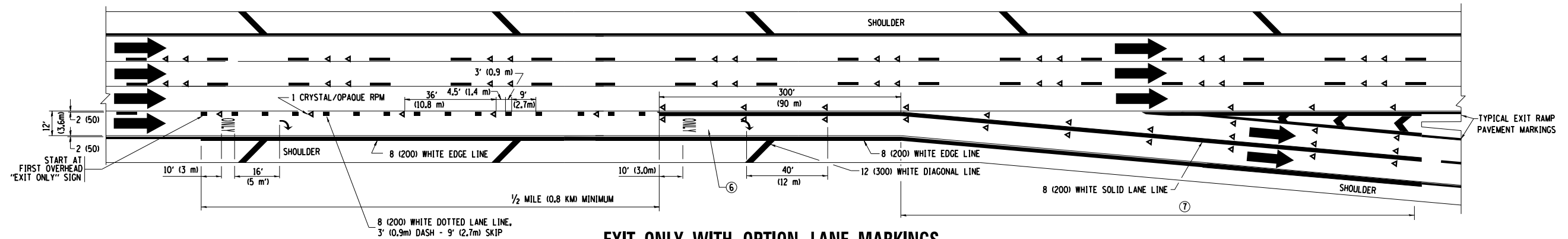
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS

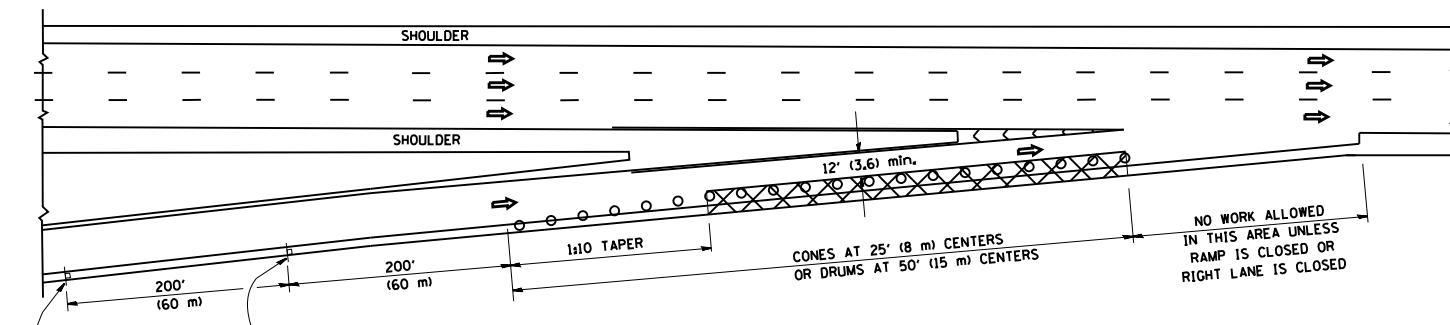


EXIT ONLY WITH OPTION LANE MARKINGS

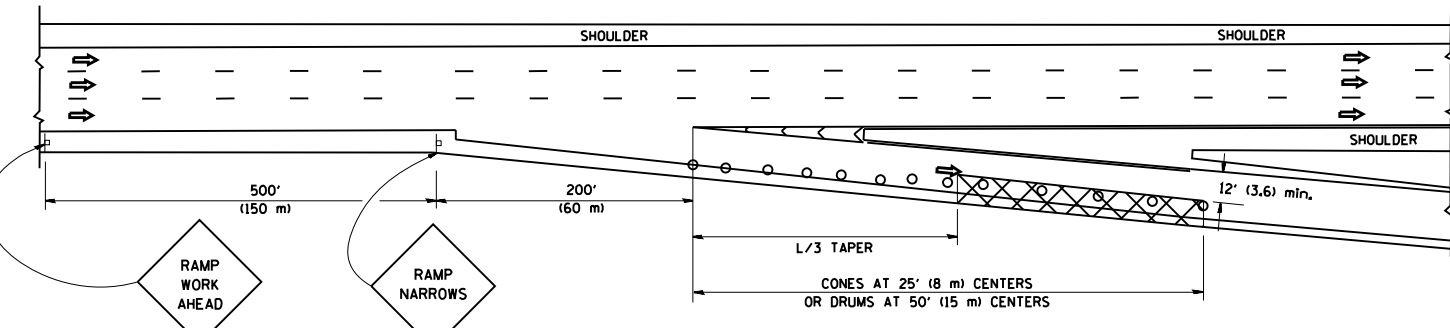
- NOTES**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

FILE NAME =	USER NAME = pyrzanowski	DESIGNED - D.W.S.	REVISED - D.W.S. 07-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS		F.A.I. RTE. 94	SECTION 2012-0631	COUNTY COOK	TOTAL SHEETS 18	SHEET NO. 17	
ca:\pwork\pwork\pyrzanowski\0106613-sht-plan.dgn	PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED - J.A.F. 02-06		SCALE: NONE	SHEET NO. 2 OF 2 SHEETS	STA. TO STA.	TC-12		CONTRACT NO. 60V76		
	PLOT DATE = 2/7/2013	CHECKED -	REVISED - S.P.B. 01-07									
		DATE - 01-90	REVISED - S.P.B. 01-10		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

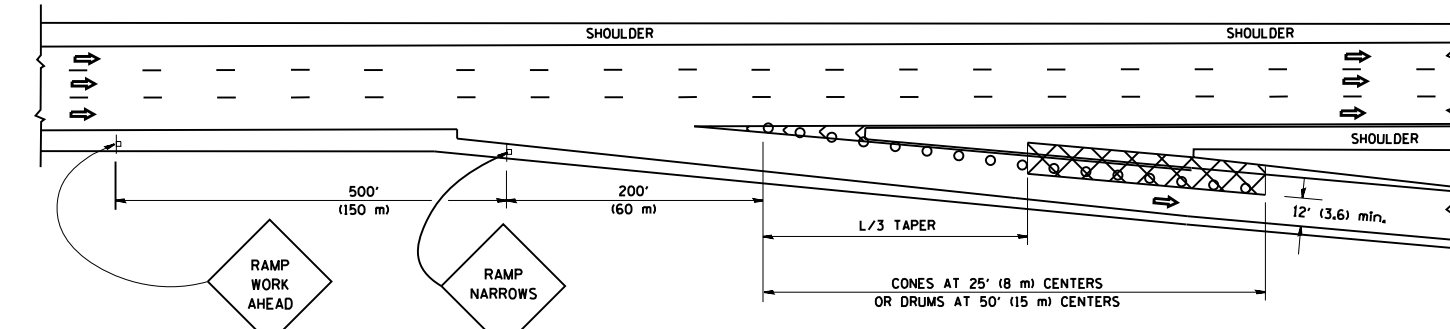
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

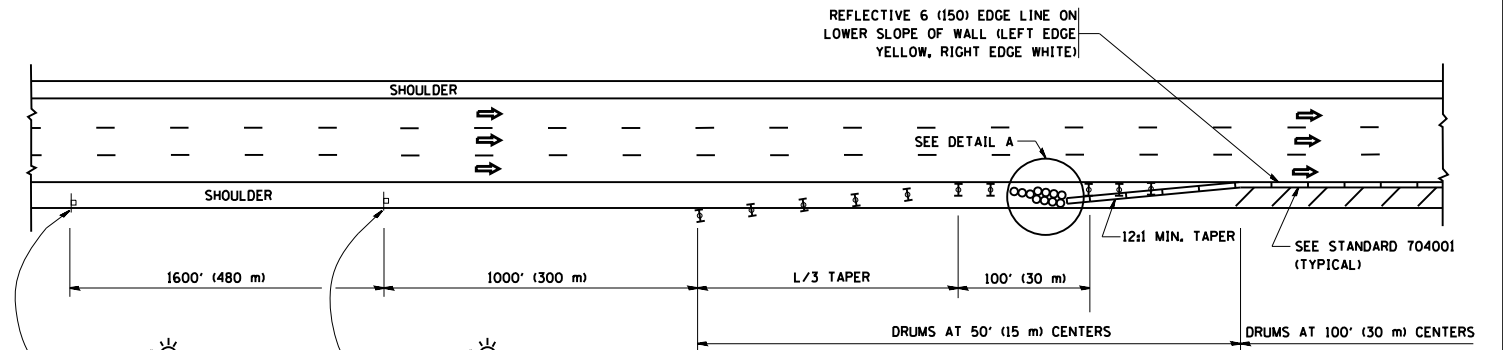
GENERAL NOTES

1. THE "L" DISTANCE EQUALS:

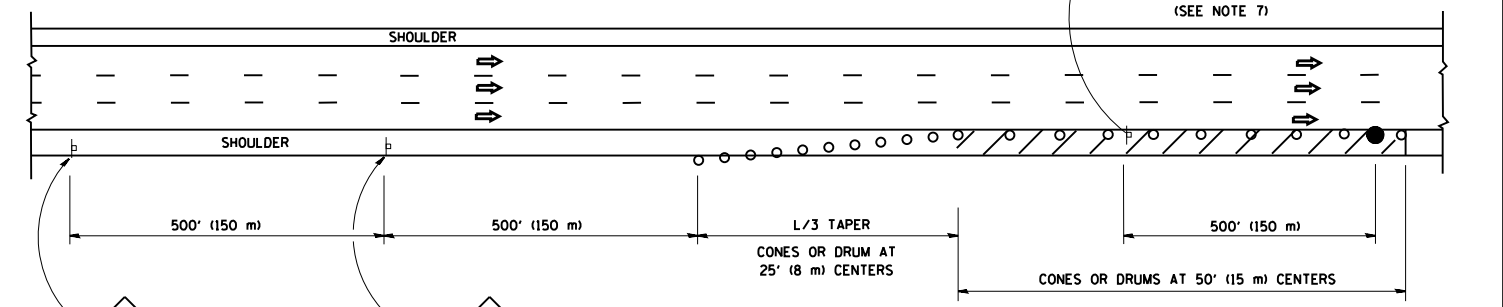
SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH
	$L=0.65(WXS)$ $L=(WXS)$

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)
2. PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

SHOULDER CLOSURE DETAILS

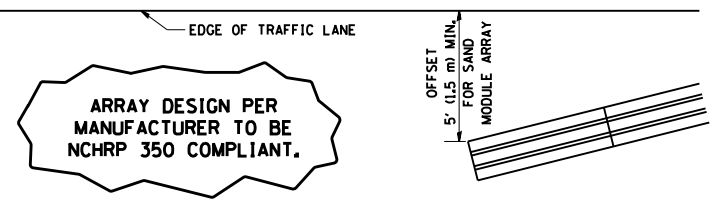


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCRANCH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



**DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)**

5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCRANCHMENT INTO THE LANE OPEN TO TRAFFIC.

THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = pyrzanowskirb	DESIGNED -	REVISED - 04-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\pw\work\p\idot\pyrzanowskirb\d0318579\106613-sht-plan.dgn	DI06613-sht-plan.dgn	DRAWN - D.W.S.	REVISED - J.A.F. 12-06		94	2012-0631	COOK	18	18			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - S.P.B. 01-07		TC-17			CONTRACT NO. 60V076				
PLOT DATE = 2/7/2013		DATE - 11-96	REVISED - S.P.B. 12-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				