

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

| F.A.P. RTE. | SECTION      | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|-------------|--------------|----------|--------------------|-----------|
| 690         | (15,15X)RS-3 | MADISON  | 17                 | 1         |
|             |              | ILLINOIS | CONTRACT NO. 76K88 |           |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

# PROPOSED HIGHWAY PLANS

FAP ROUTE 690 (IL 160)  
SECTION (15,15X)RS-3  
PROJECT STP-1P96(275)  
3P RESURFACING  
MADISON COUNTY

**TRAFFIC DATA**

FROM HIGHLAND ROAD TO ELLIS ROAD  
MINOR ARTERIAL

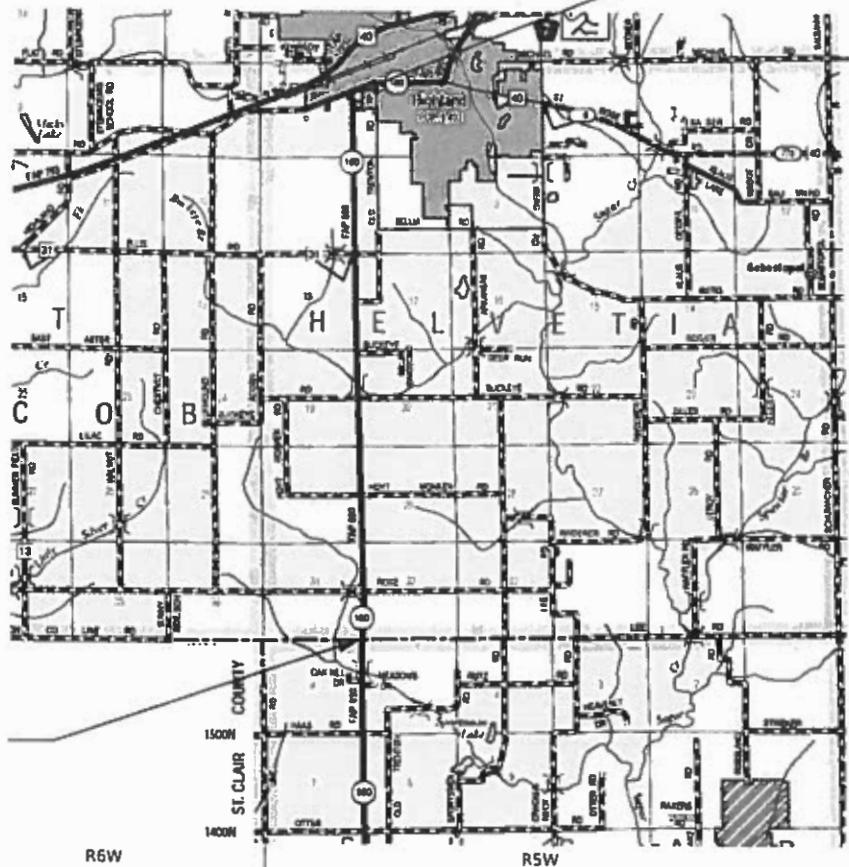
2017 ADT = 3950 2039 ADT = 4900  
PV=91.1% SU = 6.1% MU = 2.8%

FROM ELLIS ROAD TO CLINTON COUNTY LINE  
MINOR ARTERIAL

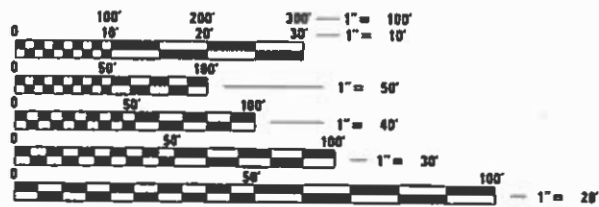
2017 ADT = 5500 2039 ADT = 6700  
PV=84.2% SU = 12.0% MU = 3.8%

C-98-214-18

END PROJECT  
STATION 314+41.50  
IL 160 AND HIGHLAND ROAD  
LAT: 38.73814  
LONG: -89.69000



LOCATION MAP  
NOT TO SCALE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT ENGINEER: HERVE GELIN (618) 346-3179  
PROJECT MANAGER: BILLIE OWEN (618) 346-3209

CONTRACT NO. 76K88

GROSS LENGTH = 30,051.5 FT. = 5.692 MILES  
NET LENGTH = 30,051.5 FT. = 5.692 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED *Jan 18, 2019*  
*Keith Roberts*  
REGIONAL ENGINEER

*March 22, 2019*  
*Paul P. Chif*  
ENGINEER OF DESIGN AND ENVIRONMENT

*March 22, 2019*  
*Paul P. Chif*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## INDEX OF SHEETS

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| 15-16 | SURVEY MONUMENTS  |
| 17    | CONSTRUCTION DETAILS  |

## HIGHWAY STANDARDS

|           |  |
|-----------|--|
| 000001-07 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                               |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT   |
| 406201-01 | MAILBOX TURNOUT  |
| 482001-02 | HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT                                 |
| 630001-12 | STEEL PLATE BEAM GUARDRAIL   |
| 630106-02 | LONG-SPAN GUARDRAIL OVER CULVERT   |
| 630301-09 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS                 |
| 642006    | SHOULDER RUMBLE STRIPS, 8 IN.  |
| 701006-05 | OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE  |
| 701011-04 | OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY                                 |
| 701201-05 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH                        |
| 701301-04 | LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS                                |
| 701306-04 | LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH |
| 701311-03 | LANE CLOSURE, 2L, 2W MOVING OPERATIONS - DAY ONLY                          |
| 701336-07 | LANE CLOSURE, 2L, 2W, WORK AREA IN SERIES, FOR SPEEDS ≥ 45 MPH             |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED                                      |
| 701901-08 | TRAFFIC CONTROL DEVICES  |
| 725001-01 | OBJECT AND TERMINAL MARKERS  |
| 780001-05 | TYPICAL PAVEMENT MARKINGS  |
| 781001-04 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS                    |
| 782006    | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS                      |

## GENERAL NOTES

- ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES WITHIN THE PROJECT AREA BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

| UTILITY  | ABOVE GROUND | BELOW GROUND |
|--|--------------|--------------|
| * AMEREN ILLINOIS (GAS & ELECTRIC)                     | X            | X            |
| * AT&T ILLINOIS (COMMUNICATIONS)                       | X            | X            |
| * CHARTER COMMUNICATIONS, INC. (CABLE TV)              | X            | X            |
| * CLINTON COUNTY ELECTRIC COOPERATIVE, INC. (ELECTRIC) |              | X            |
| * ENABLE MIDSTREAM PARTNERS (PIPELINE)                 |              | X            |
| * FRONTIER COMMUNICATIONS (COMMUNICATIONS)             | X            | X            |
| * CITY OF HIGHLAND (COMMUNICATIONS)                    |              | X            |
| * CITY OF HIGHLAND (WATER & SEWER)                     |              | X            |
| * CITY OF HIGHLAND (ELECTRIC)                          | X            |              |
| * KINDER MORGAN (NATURAL GAS PIPELINE)                 |              | X            |
| * VILLAGE OF ST. JACOB (WATER & SEWER)                 |              | X            |
| * TRI-TOWNSHIP WATER DISTRICT (WATER)                  |              | X            |

MEMBERS OF J.U.L.I.E. CALL TOLL FREE (800) 892-0123 OR 811 AND ARE INDICATED BY \*. NON- J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

- THE CONTRACTOR AND THE ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING, TOPOGRAPHY, AND QUANTITIES SHOWN IN THE PLANS WERE CREATED USING MICROFILM AND FIELD MEASUREMENTS. ALL SHALL BE ASSUMED TO BE APPROXIMATE. THE CONTRACTOR SHALL VERIFY DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE THICKNESS OF HOT-MIX ASPHALT SURFACE MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.
- AN ESTIMATED QUANTITY OF 12,400 TONS OF CUTTINGS FROM THE HOT-MIX ASPHALT SURFACE REMOVAL OPERATION IS ANTICIPATED.
- NO OVERNIGHT LANE CLOSURES WILL BE PERMITTED.
- THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.
- THE RESIDENT ENGINEER SHALL VERIFY THE EXISTENCE OF HIGHWAY LIGHTING AND/OR INTELLIGENT TRANSPORTATION SYSTEMS (I.T.S.) UTILITIES WITHIN THE PROJECT LIMITS. IF HIGHWAY LIGHTING AND/OR I.T.S. EXISTS WITHIN THE PROJECT LIMITS, AND IF THESE ITEMS REQUIRE LOCATING, THE CONTRACTOR SHALL BE DIRECTED TO DO SO ACCORDING TO SECTION 803 OF THE STANDARD SPECIFICATIONS.
- THE DEPARTMENT STRONGLY ENCOURAGES THE PRIME CONTRACTOR AND THEIR APPROVED SUB-CONTRACTORS TO HIRE MINORITY, WOMEN AND DISADVANTAGED INDIVIDUALS FROM ITS FEDERALLY FUNDED HIGHWAY CONSTRUCTION CAREERS TRAINING PROGRAM (HCCTP) TO HELP MEET WORKFORCE AND TRAINEE GOALS. THIS PROGRAM IS TRAINING MINORITIES, WOMEN AND DISADVANTAGED INDIVIDUALS IN HIGHWAY CONSTRUCTION-RELATED SKILLS, E.G., MATH FOR THE TRADES, JOB READINESS, TECHNICAL SKILLS COURSEWORK (CARPENTRY, CONCRETE FLATWORK, BLUEPRINT READING, SITE PLANS, SITE WORK, TOOLS USE, ETC.) AND OSHA 10 HOUR CERTIFICATION, TO PREPARE THEM FOR A CAREER IN THE HIGHWAY CONSTRUCTION TRADES. GRADUATES ARE WELL-TRAINED AND READY TO BECOME PRODUCTIVE ENTRY-LEVEL CONSTRUCTION WORKERS, CONTACT THE DISTRICT 8 EEO OFFICE AT 618-346-3360 AND/OR THE HCCTP COORDINATOR AT 618-874-6528 TO LEARN MORE ABOUT THE PROGRAM AND FOR ASSISTANCE IN MEETING WORKFORCE AND TRAINEE GOALS.
- ALL DISTURBED EMBANKMENT AREAS SHALL BE SEEDED WITH CLASS 2A SEED, FERTILIZED AND MULCHED AS PER SECTION 250.04 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THE COST FOR PERFORMING THIS WORK SHALL BE CONSIDERED INCLUDED IN THE UNIT PRICE BID FOR THE ITEM OF WORK INITIATING THE DISTURBANCE.
- FACTORS FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

|                                  |   |
|----------------------------------|---|
| HOT-MIX ASPHALT SURFACE COURSE   | 0.056 TON/SQ YD/IN  |
| AGGREGATE MATERIALS              | 2.05 TON/CU YD  |
| BITUMINOUS MATERIALS (TACK COAT) | 0.05 LB/SQ FT (MILLED HMA) AND 0.025 LB/SQ FT (BETWEEN LIFTS) |
- THE USE OF A MATERIAL TRANSFER DEVICE SHALL BE PERMITTED FOR THE PAVING OPERATIONS OF THIS PROJECT. HOWEVER, THE MATERIAL TRANSFER DEVICE SHALL BE EMPTY WHEN PASSING OVER THE FOLLOWING STRUCTURES:

|          |          |
|----------|----------|
| 060-0336 | 060-2425 |
| 060-2423 | 060-2426 |
| 060-2424 | 060-2427 |
- ONLY REMOVAL OF THE SHORT TERM PAVEMENT MARKING FROM THE FINAL SURFACE SHALL BE PAID FOR AS "SHORT TERM PAVEMENT MARKING REMOVAL".

## COMMITMENTS

NONE

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|                               |            |           |   |  |       |    |        |            |              |                  |              |           |
|-------------------------------|------------|-----------|---|--|-------|----|--------|------------|--------------|------------------|--------------|-----------|
| USER NAME = calcaterradm      | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>INDEX OF SHEETS, HIGHWAY STANDARDS,<br/>GENERAL NOTES &amp; COMMITMENTS</b> |       |    |        | F.A.P RTE. | SECTION      | COUNTY           | TOTAL SHEETS | SHEET NO. |
|                               | DRAWN -    | REVISED - |   |  |       |    |        | 690        | (15.15X)RS-3 | MADISON          | 17           | 2         |
| PLOT SCALE = 100.0000 ' / in. | CHECKED -  | REVISED - |   | CONTRACT NO. 76K88   |       |    |        | ILLINOIS   |              | FED. AID PROJECT |              |           |
| PLOT DATE = 1/15/2019         | DATE -     | REVISED - |   | SCALE:   | SHEET | OF | SHEETS | STA.       | TO           | STA.             |              |           |

| CODE NO. | ITEM  | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE        |                          |
|----------|---|-------|----------------|--------------------------|--------------------------|
|          |   |       |                | 80% FED<br>20% STATE     |                          |
|          |   |       |                | ROADWAY<br>0005<br>RURAL | ROADWAY<br>0005<br>URBAN |
| 20200500 | EARTH EXCAVATION (WIDENING)                           | CU YD | 7700           | 6464                     | 1236                     |
| 31102100 | SUBBASE GRANULAR MATERIAL, TYPE C 4"                  | SQ YD | 23099          | 19392                    | 3707                     |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT)                      | POUND | 54093          | 45513                    | 8580                     |
| 40600637 | LEVELING BINDER (MACHINE METHOD), 1L-9.5FG, N70       | TON   | 5610           | 4720                     | 890                      |
| 40600985 | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | SQ YD | 30             | 15                       | 15                       |
| 40600990 | TEMPORARY RAMP  | SQ YD | 258            | 238                      | 20                       |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70          | TON   | 6732           | 5664                     | 1068                     |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING                  | TON   | 42             | 34                       | 8                        |
| 44000160 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"               | SQ YD | 80137          | 67426                    | 12711                    |
| 44200168 | PAVEMENT PATCHING, TYPE II, 14 INCH                   | SQ YD | 445            | 310                      | 135                      |
| 44300200 | STRIP REFLECTIVE CRACK CONTROL TREATMENT              | FOOT  | 60103          | 50570                    | 9533                     |
| 48101200 | AGGREGATE SHOULDERS, TYPE B                           | TON   | 265            | 265                      |                          |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B                      | TON   | 4207           | 3522                     | 685                      |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8"                         | SQ YD | 19793          | 16615                    | 3178                     |

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|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET 1 OF 3 SHEETS STA. TO STA.

| F.A.P. RTE.        | SECTION      | COUNTY  | TOTAL SHEETS              | SHEET NO. |
|--------------------|--------------|---------|---------------------------|-----------|
| 690                | (15.15X)RS-3 | MADISON | 17                        | 3         |
| CONTRACT NO. 76K88 |              |         | ILLINOIS FED. AID PROJECT |           |

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| CODE NO.   | ITEM   | UNIT   | TOTAL QUANTITY | CONSTRUCTION CODE    |            |
|------------|--|--------|----------------|----------------------|------------|
|            |  |        |                | ROADWAY              | ROADWAY    |
|            |  |        |                | 0005 RURAL           | 0005 URBAN |
|            |  |        |                | 80% FED<br>20% STATE |            |
| * 6300003  | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS   | FOOT   | 1212.5         | 1212.5               |            |
| * 63000350 | LONG-SPAN GUARDRAIL OVER CULVERT, 12 FT 6 IN SPAN  | FOOT   | 112.5          | 112.5                |            |
| * 63000360 | LONG-SPAN GUARDRAIL OVER CULVERT, 18 FT 9 IN SPAN  | FOOT   | 175            | 175                  |            |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH   | 14             | 14                   |            |
| 63200310   | GUARDRAIL REMOVAL                                  | FOOT   | 1950           | 1950                 |            |
| 64200108   | SHOULDER RUMBLE STRIPS, 8 INCH                     | FOOT   | 56481          | 47466                | 9015       |
| 67000400   | ENGINEER'S FIELD OFFICE, TYPE A                    | CAL MO | 4              | 3                    | 1          |
| 67100100   | MOBILIZATION                                       | L SUM  | 1              | 0.8                  | 0.2        |
| 70100450   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201    | L SUM  | 1              | 1                    |            |
| 70100460   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306    | L SUM  | 1              | 1                    |            |
| 70100600   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701336    | L SUM  | 1              | 1                    |            |
| 70102620   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501    | L SUM  | 1              |                      | 1          |
| 70300100   | SHORT TERM PAVEMENT MARKING                        | FOOT   | 8196           | 6896                 | 1300       |
| 70300150   | SHORT TERM PAVEMENT MARKING REMOVAL                | SQ FT  | 911            | 766                  | 145        |
| 70300220   | TEMPORARY PAVEMENT MARKING - LINE 4"               | FOOT   | 73041          | 62316                | 10725      |

\* SPECIALTY ITEM

REV. - MS

|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: N.T.S. SHEET 2 OF 3 SHEETS STA. TO STA.

| F.A.P. RTE.        | SECTION      | COUNTY  | TOTAL SHEETS              | SHEET NO. |
|--------------------|--------------|---------|---------------------------|-----------|
| 690                | (15,15X)RS-3 | MADISON | 17                        | 4         |
| CONTRACT NO. 76K88 |              |         | ILLINOIS FED. AID PROJECT |           |

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| CODE NO.   | ITEM                                      | UNIT  | TOTAL QUANTITY | CONSTRUCTION CODE    |                    |
|------------|---|-------|----------------|----------------------|--------------------|
|            |   |       |                | ROADWAY 0005 RURAL   | ROADWAY 0005 URBAN |
|            |   |       |                | 80% FED<br>20% STATE |                    |
| * 72501000 | TERMINAL MARKER - DIRECT APPLIED          | EACH  | 14             | 14                   |                    |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4"  | FOOT  | 73041          | 62316                | 10725              |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER         | EACH  | 377            | 317                  | 60                 |
| * 78200005 | GUARDRAIL REFLECTORS, TYPE A              | EACH  | 32             | 32                   |                    |
| 78300200   | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH  | 377            | 317                  | 60                 |
| X7030005   | TEMPORARY PAVEMENT MARKING REMOVAL        | SQ FT | 24347          | 20772                | 3575               |
| Z0033700   | LONGITUDINAL JOINT SEALANT                | FOOT  | 30052          | 25285                | 4767               |
| Z0034105   | MATERIAL TRANSFER DEVICE                  | TON   | 12342          | 10384                | 1958               |
| Z0070100   | SURVEY MONUMENT COVER ASSEMBLY            | EACH  | 7              | 7                    |                    |
| ∅ Z0076600 | TRAINEES                                  | hour  | 1500           | 1500                 |                    |
| ∅ Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE        | hour  | 1500           | 1500                 |                    |

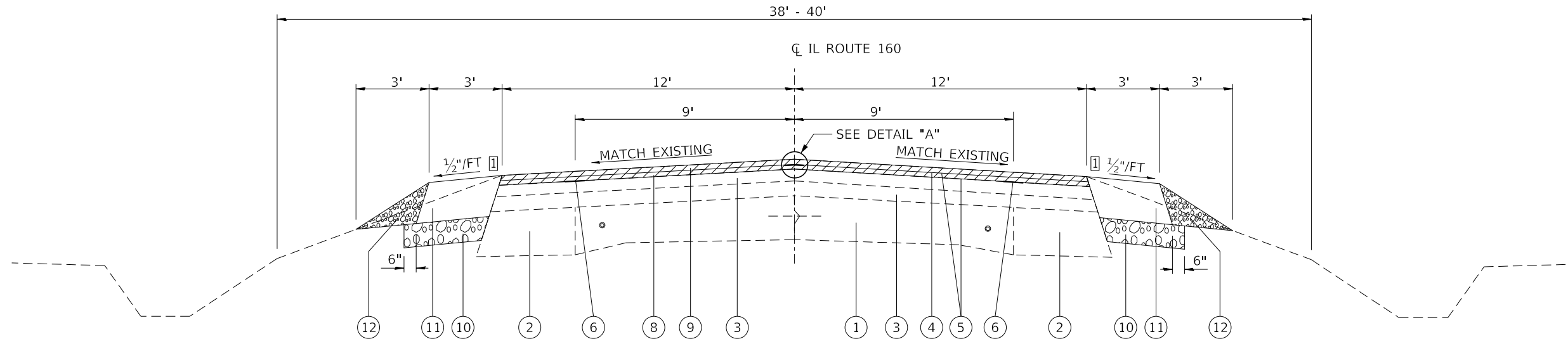
**\* SPECIALTY ITEM**

REV. - MS

|  |                          |                     |                    |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
|--|--------------------------|---------------------|--------------------|---------------------------|---------|-----------|------------------------------|-----------|-----------|-----------------------|--------|-----------|---|------------------------------|--|-------------|---------|--------|--------------|-----------|-----|--------------|---------|----|---|--|--|--|--------------------|--|
| <table border="1"> <tr> <td>USER NAME = calcaterradm</td> <td>DESIGNED -</td> <td>REVISED -</td> </tr> <tr> <td></td> <td>DRAWN -</td> <td>REVISED -</td> </tr> <tr> <td>PLOT SCALE = 100,0000' / in.</td> <td>CHECKED -</td> <td>REVISED -</td> </tr> <tr> <td>PLOT DATE = 1/15/2019</td> <td>DATE -</td> <td>REVISED -</td> </tr> </table> | USER NAME = calcaterradm | DESIGNED -          | REVISED -          |                           | DRAWN - | REVISED - | PLOT SCALE = 100,0000' / in. | CHECKED - | REVISED - | PLOT DATE = 1/15/2019 | DATE - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SUMMARY OF QUANTITIES</b> | <table border="1"> <tr> <td>F.A.P. RTE.</td> <td>SECTION</td> <td>COUNTY</td> <td>TOTAL SHEETS</td> <td>SHEET NO.</td> </tr> <tr> <td>690</td> <td>(15.15X)RS-3</td> <td>MADISON</td> <td>17</td> <td>5</td> </tr> <tr> <td colspan="3"></td> <td colspan="2">CONTRACT NO. 76K88</td> </tr> </table> | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | 690 | (15.15X)RS-3 | MADISON | 17 | 5 |  |  |  | CONTRACT NO. 76K88 |  |
| USER NAME = calcaterradm   | DESIGNED -               | REVISED -           |                    |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
|  | DRAWN -                  | REVISED -           |                    |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
| PLOT SCALE = 100,0000' / in.   | CHECKED -                | REVISED -           |                    |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
| PLOT DATE = 1/15/2019  | DATE -                   | REVISED -           |                    |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
| F.A.P. RTE.  | SECTION                  | COUNTY              | TOTAL SHEETS       | SHEET NO.                 |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
| 690  | (15.15X)RS-3             | MADISON             | 17                 | 5                         |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
|  |                          |                     | CONTRACT NO. 76K88 |                           |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |
| SCALE: N.T.S.  |                          | SHEET 3 OF 3 SHEETS | STA. TO STA.       | ILLINOIS FED. AID PROJECT |         |           |                              |           |           |                       |        |           |   |                              |  |             |         |        |              |           |     |              |         |    |   |  |  |  |                    |  |

**TYPICAL SECTION NOTES**

1 SHOULDER RUMBLE STRIPS, 8 INCH SHALL BE INSTALLED.

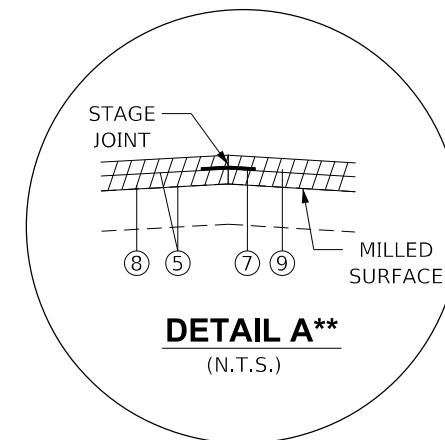


**TYPICAL SECTION MAIN LINE**

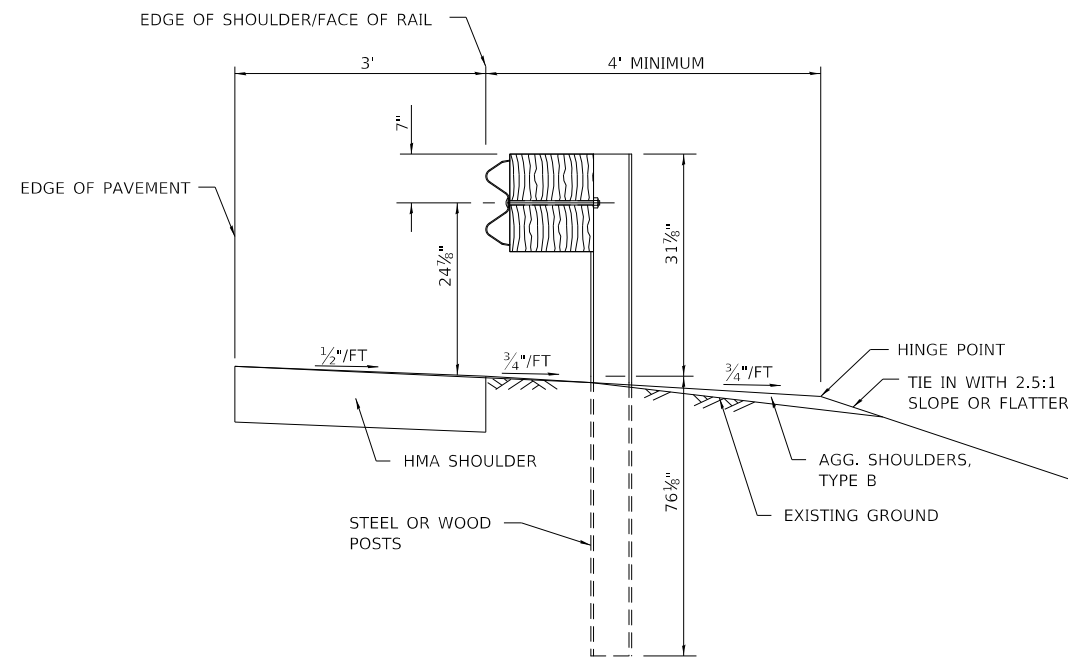
STA. 266+75.00 TO STA. 314+41.50 (URBAN)  
 STA. 13+90.00 TO STA. 266+75.00 (RURAL)

**LEGEND**

- 1 EXISTING 9' - 6 1/2" - 9" PCC PAVEMENT
- 2 EXISTING BITUMINOUS WIDENING 9"
- 3 EXISTING BITUMINOUS CONCRETE SURFACE
- 4 PROPOSED HMA SURFACE REMOVAL 2 3/4"
- 5 PROPOSED BITUMINOUS MATERIALS (TACK COAT)
- 6 PROPOSED STRIP REFLECTIVE CRACK CONTROL TREATMENT
- 7 PROPOSED LONGITUDINAL JOINT SEALANT
- 8 PROPOSED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N70, 1 1/4"
- 9 PROPOSED HMA SURFACE COURSE, MIX D, N70 1 1/2"
- 10 PROPOSED SUBBASE GRANULAR MATERIAL, TYPE C, 4"
- 11 PROPOSED HMA SHOULDERS, 8"
- 12 PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B



\*\*DETAIL "A" SHALL NOT BE APPLIED ALONG THE EDGE OF PAVEMENT



**TYPICAL GUARDRAIL SECTION**

**MIX CHART**

| MIXTURE USE                 | SURFACE               | LEVEL BINDER          | SHOULDERS ≥ 2.25"     | SHOULDERS < 2.25"     | PATCHING              |
|-----------------------------|-----------------------|-----------------------|-----------------------|-----------------------|-----------------------|
| AC/PG                       | PG 64-22              | PG 64-22              | PG 64-22              | PG 64-22              | PG 64-22              |
| RAP % (MAX)                 | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION |
| DESIGN AIR VOIDS            | 4.0% @ Ndes=70        | 4.0% @ Ndes=70        | 4.0% @ Ndes=30        | 4.0% @ Ndes=30        | 4.0% @ Ndes=70        |
| MIX COMPOSITION (GRADATION) | IL 9.5                | IL 9.5FG              | IL 19.0L              | IL 9.5L               | IL 19.0               |
| FRICTION AGG                | MIXTURE "D"           | MIXTURE "C"           |                       |                       | MIXTURE "B"           |
| QUALITY MGMT PROGRAM        | QCP                   | QCP                   | QC/QA                 | QC/QA                 | QC/QA                 |

PLAN QUANTITIES FOR HOT-MIX ASPHALT ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LB/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS).

MODEL: Default  
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|                               |            |           |
|-------------------------------|------------|-----------|
| USER NAME = calcaterradm      | DESIGNED - | REVISED - |
|                               | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019         | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.P RTE.         | SECTION      | COUNTY           | TOTAL SHEETS | SHEET NO. |
|--------------------|--------------|------------------|--------------|-----------|
| 690                | (15,15X)RS-3 | MADISON          | 17           | 6         |
| CONTRACT NO. 76K88 |              |                  |              |           |
| ILLINOIS           |              | FED. AID PROJECT |              |           |

PAVEMENT PATCHING SCHEDULE

| LOCATION MILE              | DIRECTION | LENGTH FT | WIDTH FT | AREA SQ YD | PAVEMENT PATCHING TYPE 11, 14" | COMMENTS     |
|----------------------------|-----------|-----------|----------|------------|--------------------------------|--------------|
| RURAL                      |           |           |          |            |                                |              |
| 17+94.3                    | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 17+94.3                    | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 72+64.4                    | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 72+64.4                    | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 84+73.5                    | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 84+73.5                    | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 95+98.1                    | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 95+98.1                    | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 122+75.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 122+75.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 150+41.8                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 150+41.8                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 156+33.2                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           | LONGITUDINAL |
| 157+54.6                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 157+54.6                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 161+29.5                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 161+29.5                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           | LONGITUDINAL |
| 161+29.5                   | SB        | 6.0       | 9.0      | 6.00       | 6.00                           |              |
| 162+08.7                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 162+08.7                   | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 162+35.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 162+35.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 168+52.9                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 168+52.9                   | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 173+43.9                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 173+43.9                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 185+05.5                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 185+05.5                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 203+32.4                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 203+32.4                   | SB        | 8.0       | 12.0     | 10.67      | 10.67                          |              |
| 207+17.8                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 207+17.8                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 230+04.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 230+04.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 238+38.3                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 239+38.6                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 239+38.6                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 241+12.9                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 241+12.9                   | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 241+28.7                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 241+28.7                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 242+13.2                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 242+13.2                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 243+82.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 243+82.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| URBAN                      |           |           |          |            |                                |              |
| 273+60.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 273+60.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 274+28.7                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 277+13.8                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 277+98.3                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 277+98.3                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 280+62.3                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 280+62.3                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 306+23.1                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 306+23.1                   | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 306+49.5                   | NB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 306+49.5                   | SB        | 6.0       | 12.0     | 8.00       | 8.00                           |              |
| 307+18.1                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 307+18.1                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 307+28.7                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 307+28.7                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 308+34.3                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 308+34.3                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 309+13.5                   | NB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| 309+13.5                   | SB        | 4.0       | 12.0     | 5.33       | 5.33                           |              |
| RURAL SUB-TOTAL            |           |           |          | 269.89     | 269.89                         |              |
| ANTICIPATED FAILURES (15%) |           |           |          |            | 40.48                          |              |
| RURAL TOTAL                |           |           |          |            | 310.374                        |              |
| URBAN SUB-TOTAL            |           |           |          | 117.28     | 117.28                         |              |
| ANTICIPATED FAILURES (15%) |           |           |          |            | 17.59                          |              |
| URBAN TOTAL                |           |           |          |            | 134.872                        |              |
| TOTAL (ROUNDED)            |           |           |          |            | 445                            |              |

PAVING SCHEDULE

| LOCATION               |    |           |          | HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4" | BITUMINOUS MATERIALS (TACK COAT) | TEMPORARY RAMP | LEVELING BINDER (MACHINE METHOD), 1L-9.5FG, N70 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | STRIP REFLECTIVE CRACK CONTROL TREATMENT | LONGITUDINAL JOINT SEALANT |
|------------------------|----|-----------|----------|---|----------------------------------|----------------|---|--|--|----------------------------|
| STA                    | TO | STA       | LENGTH   | (SQ YD)                                 | (POUND)                          | (SQ YD)        | (TON)   | (TON)  | (FOOT)                                   | (FOOT)                     |
| FAP ROUTE 690 (IL 160) |    |           |          |   |                                  |                |   |  |  |                            |
| RURAL                  |    |           |          |   |                                  |                |   |  |  |                            |
| 13+90.00               |    | 40+50.00  | 2,660.0  | 7,093.3                                 | 4,788.0                          | 66.6           | 496.5   | 595.8  | 5,320.0                                  | 2,660.0                    |
| 40+50.00               |    | 93+40.00  | 5,290.0  | 14,106.7                                | 9,522.0                          | 40.0           | 987.5   | 1,185.0                                      | 10,580.0                                 | 5,290.0                    |
| 93+40.00               |    | 146+70.00 | 5,330.0  | 14,213.3                                | 9,594.0                          | 51.1           | 994.9   | 1,193.9                                      | 10,660.0                                 | 5,330.0                    |
| 146+70.00              |    | 199+80.00 | 5,310.0  | 14,160.0                                | 9,558.0                          | 39.3           | 991.2   | 1,189.4                                      | 10,620.0                                 | 5,310.0                    |
| 199+80.00              |    | 226+20.00 | 2,640.0  | 7,040.0                                 | 4,752.0                          | 41.1           | 492.8   | 591.4  | 5,280.0                                  | 2,640.0                    |
| 226+20.00              |    | 266+75.00 | 4,055.00 | 10,813.3                                | 7,299.0                          |                | 756.9   | 908.3  | 8,110.0                                  | 4,055.0                    |
| URBAN                  |    |           |          |   |                                  |                |   |  |  |                            |
| 266+75.00              |    | 314+41.50 | 4,766.50 | 12,710.7                                | 8,579.7                          | 20.0           | 889.7   | 1,067.7                                      | 9,533.0                                  | 4,766.5                    |
| RURAL TOTAL:           |    |           |          | 67,426.6                                | 45,513.0                         | 238.1          | 4,719.8   | 5,663.8                                      | 50,570.0                                 | 25,285.0                   |
| URBAN TOTAL:           |    |           |          | 12,710.7                                | 8,579.7                          | 20.0           | 889.7   | 1,067.7                                      | 9,533.0                                  | 4,766.5                    |
| TOTALS:                |    |           |          | 80,137                                  | 54,093                           | 258            | 5,610   | 6,732  | 60,103                                   | 30,052                     |

MODEL: Default  
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|                               |            |           |
|-------------------------------|------------|-----------|
| USER NAME = calcaterradm      | DESIGNED - | REVISED - |
|                               | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019         | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: N.T.S. SHEET 1 OF 3 SHEETS STA. TO STA.

|                    |              |                  |              |           |
|--------------------|--------------|------------------|--------------|-----------|
| F.A.P. RTE.        | SECTION      | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 690                | (15.15X)RS-3 | MADISON          | 17           | 7         |
| CONTRACT NO. 76K88 |              |                  |              |           |
| ILLINOIS           |              | FED. AID PROJECT |              |           |

ENTRANCE & MAILBOX TURNOUT SCHEDULE

| LOCATION               | TYPE | WIDTH (W) | TOTAL AREA | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | INCIDENTAL HOT-MIX ASPHALT SURFACING |
|------------------------|------|-----------|------------|---|--------------------------------------|
|                        |      | (FOOT)    | (SQ FT)    | (SQ YD)   | (TON)                                |
| FAP ROUTE 690 (1L 160) |      |           |            |   |                                      |
| <b>RURAL</b>           |      |           |            |   |                                      |
| 16+43.85               | LT   | C.E.      | 55         | 177.8   | 1.1                                  |
| * 18+18.49             | RT   | P.E.      | 75         | 158.6   | 1.0                                  |
| * 19+43.75             | LT   | P.E.      | 76         | 162.1   | 1.0                                  |
| * 25+36.87             | LT   | P.E.      | 76         | 161.0   | 1.0                                  |
| * 28+54.36             | LT   | P.E.      | 51         | 110.5   | 0.7                                  |
| * 37+38.85             | LT   | P.E.      | 75         | 158.6   | 1.0                                  |
| * 38+92.16             | RT   | C.E.      | 78         | 163.5   | 1.0                                  |
| 39+08.81               | LT   | P.E.      | 22         | 48.2  | 0.3                                  |
| * 41+25.80             | LT   | P.E.      | 49         | 97.7  | 0.6                                  |
| * 42+16.04             | LT   | P.E.      | 52         | 103.4   | 0.6                                  |
| * 43+84.03             | LT   | P.E.      | 40         | 92.0  | 0.6                                  |
| * 44+53.99             | LT   | P.E.      | 65         | 140.3   | 0.9                                  |
| * 45+56.27             | LT   | P.E.      | 47         | 104.3   | 0.6                                  |
| 46+27.17               | LT   | P.E.      | 24         | 52.2  | 0.3                                  |
| * 48+87.75             | LT   | C.E.      | 64         | 135.9   | 0.8                                  |
| 66+35.14               | RT   | P.E.      | 27         | 58.2  | 0.4                                  |
| 66+71.03               | LT   | F.E.      | 25         | 52.1  | 0.3                                  |
| 79+90.57               | RT   | F.E.      | 19         | 40.5  | 0.3                                  |
| * 91+17.06             | RT   | P.E.      | 54         | 115.2   | 0.7                                  |
| 99+67.05               | RT   | F.E.      | 22         | 46.1  | 0.3                                  |
| 99+97.36               | LT   | F.E.      | 17         | 36.1  | 0.2                                  |
| 108+29.26              | RT   | P.E.      | 39         | 84.3  | 0.5                                  |
| 108+34.17              | LT   | MAILBOX   | -          | -   | -                                    |
| * 118+45.27            | LT   | P.E.      | 51         | 109.7   | 0.7                                  |
| 120+04.81              | RT   | P.E.      | 30         | 65.9  | 0.4                                  |
| 120+07.78              | LT   | MAILBOX   | -          | -   | -                                    |
| 126+22.70              | RT   | F.E.      | 27         | 56.1  | 0.3                                  |
| 126+83.22              | LT   | F.E.      | 27         | 56.1  | 0.3                                  |
| 133+08.84              | RT   | P.E.      | 50         | 101.4   | 0.6                                  |
| 133+54.75              | RT   | P.E.      | 32         | 67.6  | 0.4                                  |
| * 133+77.36            | LT   | P.E.      | 76         | 160.7   | 1.0                                  |
| * 137+25.67            | LT   | P.E.      | 63         | 134.3   | 0.8                                  |
| 138+21.67              | LT   | P.E.      | 25         | 52.1  | 0.3                                  |
| 141+10.85              | RT   | F.E.      | 31         | 64.1  | 0.4                                  |
| 145+30.35              | LT   | F.E.      | 38         | 78.1  | 0.5                                  |
| * 152+39.22            | LT   | P.E.      | 61         | 130.7   | 0.8                                  |
| 156+80.71              | LT   | F.E.      | 23         | 48.1  | 0.3                                  |
| 159+21.38              | RT   | P.E.      | 23         | 49.4  | 0.3                                  |
| 159+29.68              | LT   | MAILBOX   | -          | -   | -                                    |
| 163+42.71              | RT   | P.E.      | 32         | 67.4  | 7.5                                  |
| * 168+45.54            | LT   | P.E.      | 59         | 126.9   | 0.8                                  |
| * 173+07.16            | LT   | P.E.      | 57         | 122.5   | 0.8                                  |
| 173+09.94              | RT   | P.E.      | 53         | 113.0   | 0.7                                  |
| 186+44.56              | RT   | F.E.      | 21         | 44.1  | 0.3                                  |
| 186+71.21              | LT   | F.E.      | 22         | 47.4  | 0.3                                  |
| * 188+29.19            | LT   | P.E.      | 54         | 119.2   | 0.7                                  |
| 193+66.26              | LT   | F.E.      | 49         | 100.1   | 0.6                                  |
| * 199+70.77            | LT   | P.E.      | 81         | 170.5   | 1.1                                  |
| 202+71.08              | LT   | P.E.      | 27         | 59.0  | 0.4                                  |
| 205+63.09              | LT   | F.E.      | 41         | 84.1  | 0.5                                  |
| 222+12.05              | RT   | F.E.      | 25         | 52.1  | 0.3                                  |
| 224+77.10              | RT   | P.E.      | 29         | 62.2  | 0.4                                  |
| 226+79.20              | RT   | F.E.      | 31         | 64.1  | 0.4                                  |
| 226+84.39              | LT   | F.E.      | 41         | 85.2  | 0.5                                  |
| 244+76.75              | LT   | F.E.      | 25         | 52.1  | 0.3                                  |
| * 249+48.44            | LT   | P.E.      | 59         | 125.9   | 0.8                                  |
| 251+30.68              | LT   | C.E.      | 31         | 66.4  | 7.4                                  |
| * 253+00.41            | LT   | P.E.      | 70         | 144.3   | 0.9                                  |
| 253+42.30              | LT   | F.E.      | 28         | 56.8  | 0.4                                  |
| 257+82.36              | RT   | F.E.      | 21         | 45.0  | 0.3                                  |
| * 266+41.33            | LT   | P.E.      | 58         | 124.1   | 0.8                                  |

ENTRANCE & MAILBOX TURNOUT SCHEDULE

| LOCATION     | TYPE | WIDTH (W) | TOTAL AREA | PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT | INCIDENTAL HOT-MIX ASPHALT SURFACING |
|--------------|------|-----------|------------|---|--------------------------------------|
|              |      | (FOOT)    | (SQ FT)    | (SQ YD)   | (TON)                                |
| <b>URBAN</b> |      |           |            |   |                                      |
| 306+89.42    | RT   | C.E.      | 62         | 130.3   | 14.5                                 |
| 307+60.92    | LT   | C.E.      | 49         | 100.6   | 0.6                                  |
| 307+89.94    | RT   | P.E.      | 34         | 72.2  | 0.4                                  |
| 308+07.92    | LT   | P.E.      | 32         | 66.6  | 0.4                                  |
| 308+73.56    | RT   | P.E.      | 28         | 59.4  | 0.4                                  |
| 309+62.63    | LT   | C.E.      | 51         | 107.0   | 0.7                                  |
| 309+70.79    | RT   | P.E.      | 34         | 72.2  | 0.4                                  |
| 310+47.49    | RT   | P.E.      | 30         | 64.2  | 0.4                                  |
| 311+01.70    | LT   | C.E.      | 46         | 95.8  | 0.6                                  |
| 311+21.88    | RT   | P.E.      | 28         | 62.1  | 0.4                                  |
| 312+09.42    | LT   | P.E.      | 34         | 71.0  | 0.4                                  |
| 312+53.94    | LT   | C.E.      | 47         | 96.0  | 0.6                                  |
| 312+71.37    | RT   | F.E.      | 7          | 16.4  | 0.1                                  |
| 313+68.63    | LT   | C.E.      | 34         | 71.4  | 0.4                                  |
| 313+73.34    | RT   | P.E.      | 31         | 67.0  | 0.4                                  |
| URBAN TOTAL: |      |           |            | 15.0  | 8.0                                  |
| TOTALS:      |      |           |            | 30  | 42                                   |

NOTE: LOCATIONS AND QUANTITIES SHOWN ARE ESTIMATED ONLY, EXACT DIMENSIONS OF ENTRANCES WILL BE DETERMINED AT TIME OF CONSTRUCTION

\* DENOTES MAILBOX TURNOUT AT THIS LOCATION.

SHOULDER SCHEDULE

| LOCATION               | LENGTH | EARTH EXCAVATION (WIDENING) | SUBBASE GRANULAR MATERIAL, TYPE C 4" | AGGREGATE WEDGE SHOULDER, TYPE B | HOT-MIX ASPHALT SHOULDERS, 8" | SHOULDER RUMBLE STRIPS, 8 INCH |
|------------------------|--------|-----------------------------|--------------------------------------|----------------------------------|-------------------------------|--------------------------------|
|                        | (FOOT) | (CU YD)                     | (SQ YD)                              | (TON)                            | (SQ YD)                       | (FOOT)                         |
| FAP ROUTE 690 (1L 160) |        |                             |                                      |                                  |                               |                                |
| <b>RURAL</b>           |        |                             |                                      |                                  |                               |                                |
| 13+90.00 TO 40+50.00   | LT/RT  | 2,660.0                     | 679.8                                | 2,039.3                          | 356.3                         | 1,747.3                        |
| 40+50.00 TO 93+40.00   | LT/RT  | 5,290.0                     | 1,350.9                              | 4,052.8                          | 729.9                         | 3,472.3                        |
| 93+40.00 TO 146+70.00  | LT/RT  | 5,330.0                     | 1,359.2                              | 4,077.6                          | 731.6                         | 3,493.6                        |
| 146+70.00 TO 199+80.00 | LT/RT  | 5,310.0                     | 1,360.4                              | 4,081.2                          | 723.7                         | 3,497.0                        |
| 199+80.00 TO 226+20.00 | LT/RT  | 2,640.0                     | 672.6                                | 2,017.8                          | 386.0                         | 1,728.6                        |
| 226+20.00 TO 266+75.00 | LT/RT  | 4,055.0                     | 1,041.0                              | 3,122.9                          | 594.9                         | 2,676.1                        |
| <b>URBAN</b>           |        |                             |                                      |                                  |                               |                                |
| 266+75.0 TO 314+41.50  | LT/RT  | 4,766.5                     | 1,235.8                              | 3,707.3                          | 684.5                         | 3,177.7                        |
| RURAL TOTAL:           |        | 25,285.0                    | 6,463.9                              | 19,391.6                         | 3,522.4                       | 16,614.9                       |
| URBAN TOTAL:           |        | 4,766.5                     | 1,235.8                              | 3,707.3                          | 684.5                         | 3,177.7                        |
| TOTALS:                |        | 30,052                      | 7,700                                | 23,099                           | 4,207                         | 19,793                         |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

SCALE: N.T.S. SHEET 2 OF 3 SHEETS STA. TO STA.

|                    |              |                  |              |           |
|--------------------|--------------|------------------|--------------|-----------|
| F.A.P. RTE.        | SECTION      | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 690                | (15.15X)RS-3 | MADISON          | 17           | 8         |
| CONTRACT NO. 76K88 |              |                  |              |           |
| ILLINOIS           |              | FED. AID PROJECT |              |           |

|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN -    | REVISED - |
| PLOT DATE = 1/15/2019        | CHECKED -  | REVISED - |
|                              | DATE -     | REVISED - |



PAVEMENT MARKING SCHEDULE

| LOCATION               | DESCRIPTION | THERMOPLASTIC PAVEMENT MARKING |                 |                     | RAISED REFLECTIVE PAVEMENT MARKER | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | TEMPORARY PAVEMENT MARKING - LINE 4" | TEMPORARY PAVEMENT MARKING REMOVAL | SHORT TERM PAVEMENT MARKING | SHORT TERM PAVEMENT MARKING REMOVAL |       |
|------------------------|-------------|--------------------------------|-----------------|---------------------|-----------------------------------|---|--------------------------------------|------------------------------------|-----------------------------|-------------------------------------|-------|
|                        |             | 4" SOLID WHITE                 | 4" SOLID YELLOW | 4" SKIP-DASH YELLOW |                                   |   |                                      |                                    |                             |                                     |       |
|                        |             | (FOOT)                         | (FOOT)          | (FOOT)              |                                   |   |                                      |                                    |                             |                                     |       |
| FAP ROUTE 690 (IL 160) |             |                                |                 |                     |                                   |   |                                      |                                    |                             |                                     |       |
| RURAL                  |             |                                |                 |                     |                                   |   |                                      |                                    |                             |                                     |       |
| 13+90.00 TO 40+20.00   | LT          | EDGE LINE                      | 2,630.0         |                     |                                   |   | 2,630.0                              | 876.7                              |                             |                                     |       |
| 13+90.00 TO 266+75.00  | CL          | SKIP-DASH CENTERLINE           |                 |                     | 6,321.3                           | 317.0                                     | 317.0                                | 6,321.3                            | 2,107.1                     | 6,895.9                             |       |
| 13+90.00 TO 40+20.00   | RT          | EDGE LINE                      | 2,630.0         |                     |                                   |   | 2,630.0                              | 876.7                              |                             |                                     |       |
| 40+80.00 TO 93+05.00   | LT          | EDGE LINE                      | 5,225.0         |                     |                                   |   | 5,225.0                              | 1,741.7                            |                             |                                     |       |
| 40+80.00 TO 93+05.00   | RT          | EDGE LINE                      | 5,225.0         |                     |                                   |   | 5,225.0                              | 1,741.7                            |                             |                                     |       |
| 93+65.00 TO 146+35.00  | LT          | EDGE LINE                      | 5,270.0         |                     |                                   |   | 5,270.0                              | 1,756.7                            |                             |                                     |       |
| 93+65.00 TO 146+15.00  | RT          | EDGE LINE                      | 5,250.0         |                     |                                   |   | 5,250.0                              | 1,750.0                            |                             |                                     |       |
| 123+10.00 TO 133+30.00 | NB CL       | SOLID CENTERLINE               |                 | 1,020.0             |                                   |   |                                      | 1,020.0                            | 340.0                       |                                     |       |
| 133+30.00 TO 144+50.00 | SB CL       | SOLID CENTERLINE               |                 | 1,120.0             |                                   |   |                                      | 1,120.0                            | 373.3                       |                                     |       |
| 147+05.00 TO 199+50.00 | LT          | EDGE LINE                      | 5,245.0         |                     |                                   |   | 5,245.0                              | 1,748.3                            |                             |                                     |       |
| 147+05.00 TO 163+20.00 | RT          | EDGE LINE                      | 1,615.0         |                     |                                   |   | 1,615.0                              | 538.3                              |                             |                                     |       |
| 163+65.00 TO 199+50.00 | RT          | EDGE LINE                      | 3,585.0         |                     |                                   |   | 3,585.0                              | 1,195.0                            |                             |                                     |       |
| 170+50.00 TO 178+70.00 | NB CL       | SOLID CENTERLINE               |                 | 820.0               |                                   |   |                                      | 820.0                              | 273.3                       |                                     |       |
| 181+55.00 TO 190+05.00 | SB CL       | SOLID CENTERLINE               |                 | 850.0               |                                   |   |                                      | 850.0                              | 283.3                       |                                     |       |
| 191+70.00 TO 202+80.00 | NB CL       | SOLID CENTERLINE               |                 | 1,110.0             |                                   |   |                                      | 1,110.0                            | 370.0                       |                                     |       |
| 199+95.00 TO 225+95.00 | LT          | EDGE LINE                      | 2,600.0         |                     |                                   |   | 2,600.0                              | 866.7                              |                             |                                     |       |
| 199+95.00 TO 225+95.00 | RT          | EDGE LINE                      | 2,600.0         |                     |                                   |   | 2,600.0                              | 866.7                              |                             |                                     |       |
| 202+80.00 TO 214+30.00 | SB CL       | SOLID CENTERLINE               |                 | 1,150.0             |                                   |   |                                      | 1,150.0                            | 383.3                       |                                     |       |
| 226+50.00 TO 266+75.00 | LT          | EDGE LINE                      | 4,025.0         |                     |                                   |   | 4,025.0                              | 1,341.7                            |                             |                                     |       |
| 226+50.00 TO 266+75.00 | RT          | EDGE LINE                      | 4,025.0         |                     |                                   |   | 4,025.0                              | 1,341.7                            |                             |                                     |       |
| URBAN                  |             |                                |                 |                     |                                   |   |                                      |                                    |                             |                                     |       |
| 266+75.00 TO 314+41.50 | LT          | EDGE LINE                      | 4,766.5         |                     |                                   |   | 4,766.5                              | 1,588.8                            |                             |                                     |       |
| 266+75.00 TO 314+41.50 | CL          | SKIP-DASH CENTERLINE           |                 |                     | 1,191.6                           | 60.0                                      | 60.0                                 | 1,191.6                            | 397.2                       | 1,300.0                             |       |
| 266+75.00 TO 314+41.50 | RT          | EDGE LINE                      | 4,766.5         |                     |                                   |   | 4,766.5                              | 1,588.8                            |                             |                                     |       |
| RURAL TOTAL:           |             |                                | 49,925.0        | 6,070.0             | 6,321.3                           | 317.0                                     | 317.0                                | 62,316.3                           | 20,772.2                    | 6,895.9                             | 766.2 |
| URBAN TOTAL:           |             |                                | 9,533.0         | 0.0                 | 1,191.6                           | 60.0                                      | 60.0                                 | 10,724.6                           | 3,574.8                     | 1,300.0                             | 144.4 |
| TOTALS:                |             |                                |                 | 73,041              |                                   | 377                                       | 377                                  | 73,041                             | 24,347                      | 8,196                               | 911   |

GUARDRAIL SCHEDULE

| LOCATION               | GUARDRAIL REMOVAL | STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS | LONG-SPAN GUARDRAIL OVER CULVERT, 12 FT 6 IN SPAN | LONG-SPAN GUARDRAIL OVER CULVERT, 18 FT 9 IN SPAN | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | GUARDRAIL REFLECTORS, TYPE A | TERMINAL MARKER - DIRECT APPLIED | AGGREGATE SHOULDERS, TYPE B |        |
|------------------------|-------------------|--|---|---|--|------------------------------|----------------------------------|-----------------------------|--------|
|                        |                   |  |   |   |  |                              |                                  |                             | (FOOT) |
| FAP ROUTE 690 (IL 160) |                   |  |   |   |  |                              |                                  |                             |        |
| RURAL                  |                   |  |   |   |  |                              |                                  |                             |        |
| LOCATION #1            | RT                | 325.00   | 187.50  | 37.50   |  | 2                            | 5                                | 2                           | 32.0   |
| LOCATION #2            | LT                | 287.50   | 193.75  |   | 43.75  | 2                            | 5                                | 2                           | 23.0   |
| LOCATION #2            | RT                | 175.00   | 206.25  |   | 43.75  | 2                            | 5                                | 2                           | 56.0   |
| LOCATION #3            | LT                | 250.00   | 62.50   | 37.50   |  | 2                            | 3                                | 2                           | 53.0   |
| LOCATION #3            | RT                | 312.50   | 175.00  | 37.50   |  | 2                            | 4                                | 2                           | 21.0   |
| LOCATION #5            | LT                | 350.00   | 193.75  |   | 43.75  | 2                            | 5                                | 2                           | 28.0   |
| LOCATION #5            | RT                | 250.00   | 193.75  |   | 43.75  | 2                            | 5                                | 2                           | 52.0   |
| RURAL TOTAL:           |                   | 1,950.0  | 1,212.5   | 112.5   | 175.0  | 14.0                         | 32.0                             | 14.0                        | 265.0  |
| TOTALS:                |                   | 1,950  | 1,212.5   | 112.5   | 175  | 14                           | 32                               | 14                          | 265    |

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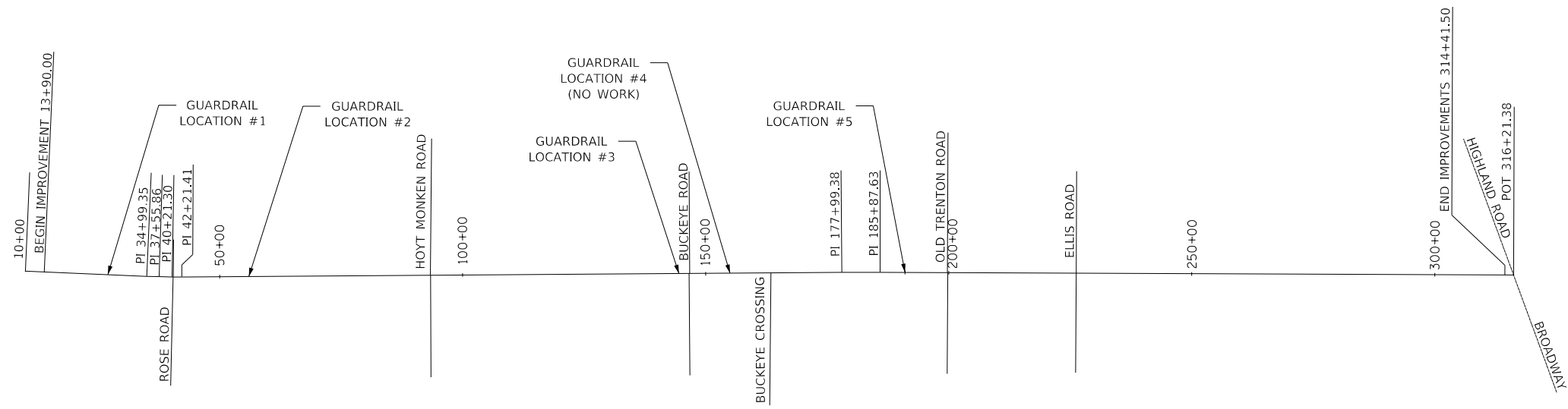
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| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES

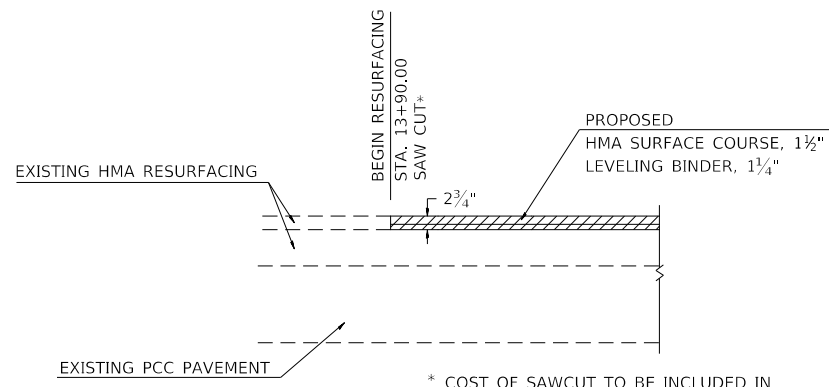
SCALE: N.T.S. SHEET 3 OF 3 SHEETS STA. TO STA.

|                    |             |                  |              |           |
|--------------------|-------------|------------------|--------------|-----------|
| F.A.P. RTE.        | SECTION     | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 690                | (15.15)RS-3 | MADISON          | 17           | 9         |
| CONTRACT NO. 76K88 |             |                  |              |           |
| ILLINOIS           |             | FED. AID PROJECT |              |           |



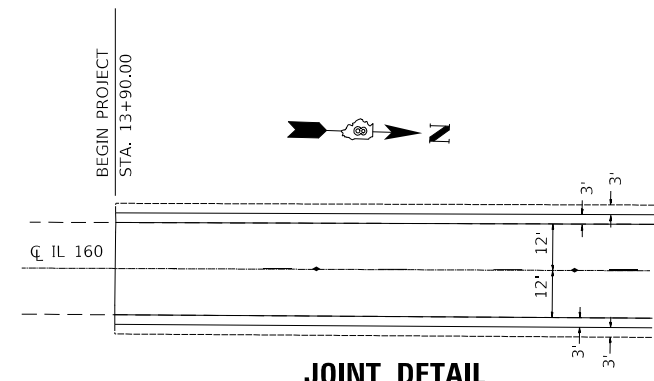
**LOCATION**

- STA 49+43.62 - ROSE ROAD EAST
- STA 40+45.98 - ROSE ROAD WEST
- STA 98+37.09 - HOYT MONKEN ROAD WEST
- STA 98+41.20 - HOYT MONKEN ROAD EAST
- STA 146+59.61 - BUCKEYE ROAD EAST
- STA 146+72.12 - BUCKEYE ROAD WEST
- STA 163+42.91 - BUCKEYE CROSSING
- STA 199+74.24 - OLD TRENTON ROAD WEST
- STA 199+78.77 - OLD TRENTON ROAD EAST
- STA 226+21.20 - ELLIS ROAD EAST
- STA 226+23.09 - ELLIS ROAD WEST
- STA 316+21.38 - HIGHLAND ROAD/BROADWAY

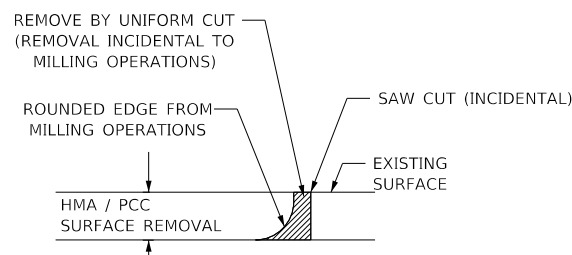


**JOINT DETAIL**  
ELEVATION VIEW

\* COST OF SAWCUT TO BE INCLUDED IN HOT-MIX ASPHALT SURFACE REMOVAL 2 3/4"

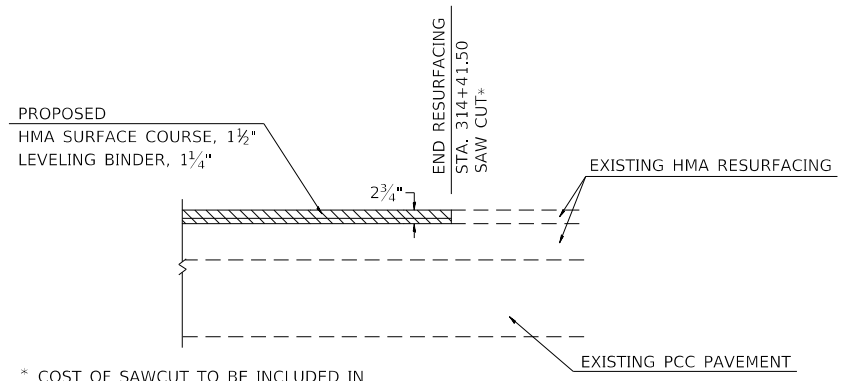


**JOINT DETAIL**  
PLAN VIEW



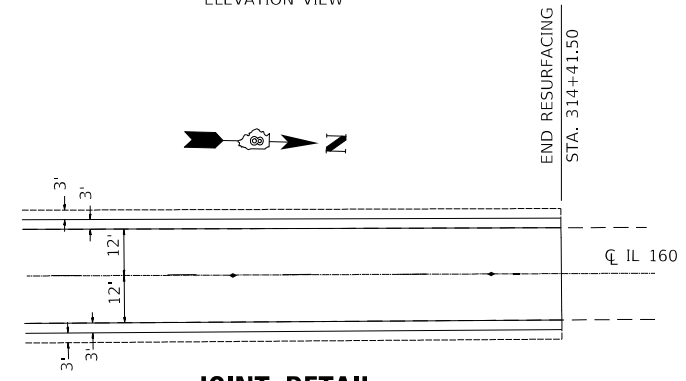
NOTE:  
WHEN MILLING OPERATIONS PRODUCE A ROUNDED EDGE, THEN A SAW CUT SHALL BE USED TO MANUFACTURE A PERPENDICULAR EDGE AS SHOWN IN THE DETAIL. THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE USE OF THIS DETAIL.

**BUTT JOINT DETAIL**



**JOINT DETAIL**  
ELEVATION VIEW

\* COST OF SAWCUT TO BE INCLUDED IN HOT-MIX ASPHALT SURFACE REMOVAL 2 3/4"



**JOINT DETAIL**  
PLAN VIEW

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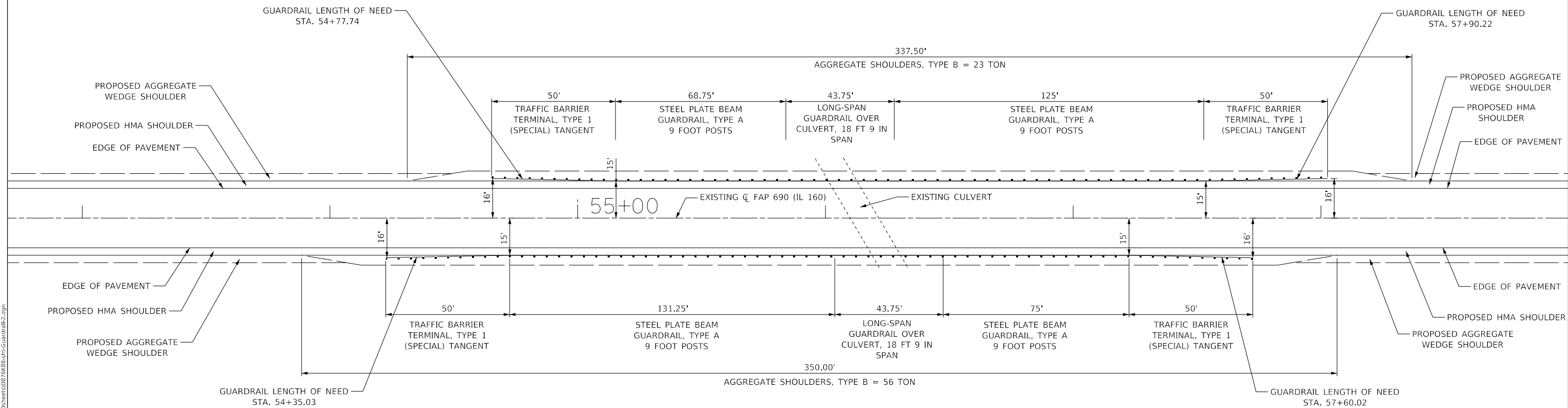
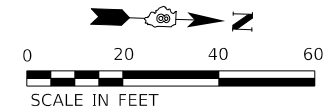
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| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                      |                     |
|----------------------|---------------------|
| <b>LOCATION PLAN</b> |                     |
| SCALE:               | SHEET 1 OF 1 SHEETS |
| STA.                 | TO STA.             |

|                    |              |                  |              |           |
|--------------------|--------------|------------------|--------------|-----------|
| F.A.P RTE.         | SECTION      | COUNTY           | TOTAL SHEETS | SHEET NO. |
| 690                | (15.15X)RS-3 | MADISON          | 17           | 10        |
| CONTRACT NO. 76K88 |              |                  |              |           |
| ILLINOIS           |              | FED. AID PROJECT |              |           |





**NOTE:**

CONTRACTOR SHALL CENTER LONG-SPAN GUARDRAIL OVER CULVERT, 12 FOOT 6 INCH SPAN OVER EXISTING CULVERT AND THEN CONSTRUCT GUARDRAIL IN BOTH DIRECTIONS FROM THAT POINT.

ALL DIMENSIONS SHOWN ARE TO THE FACE OF GUARDRAIL.

ALL EXISTING GUARDRAIL SHALL BE REMOVED, HOWEVER ONLY PROPOSED GUARDRAIL IS SHOWN. SEE GUARDRAIL SCHEDULE.

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|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

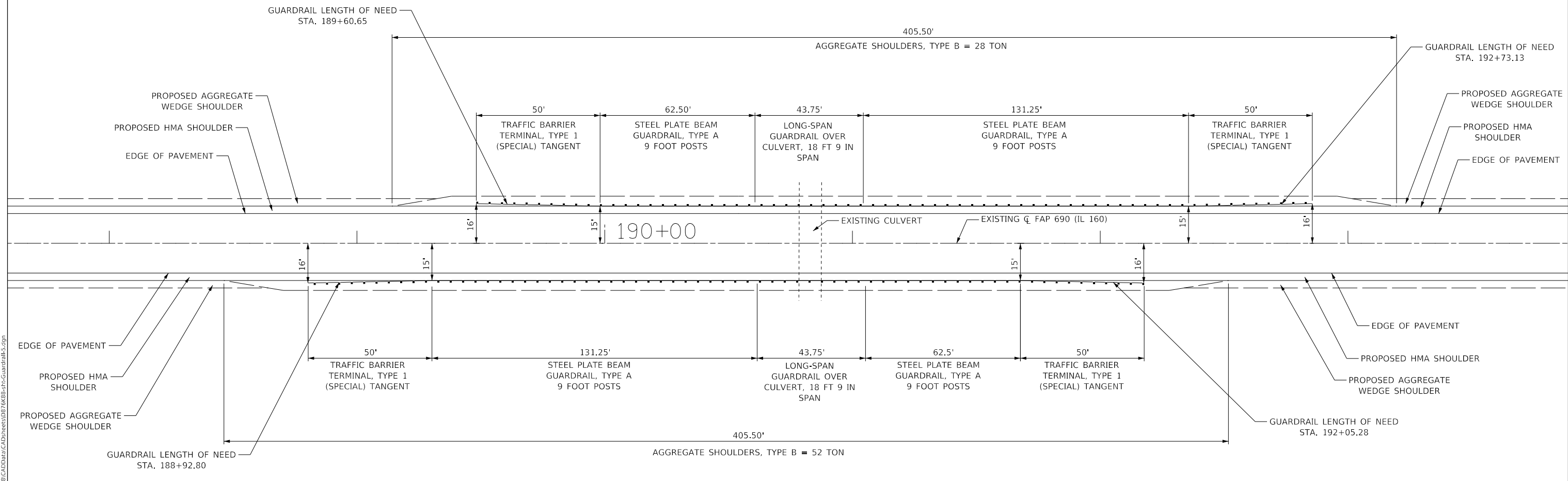
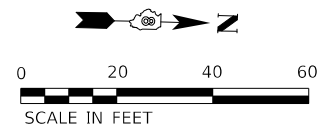
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GUARDRAIL PLAN  
LOCATION #2**

SCALE: 1"=20'      SHEET 2      OF 4      SHEETS      STA. 53+00      TO STA. 59+00

| F.A.P. RTE.        | SECTION      | COUNTY  | TOTAL SHEETS              | SHEET NO. |
|--------------------|--------------|---------|---------------------------|-----------|
| 690                | (15.15X)RS-3 | MADISON | 17                        | 12        |
| CONTRACT NO. 76K88 |              |         | ILLINOIS FED. AID PROJECT |           |





**NOTE:**  
 CONTRACTOR SHALL CENTER LONG-SPAN GUARDRAIL OVER CULVERT, 12 FOOT 6 INCH SPAN OVER EXISTING CULVERT AND THEN CONSTRUCT GUARDRAIL IN BOTH DIRECTIONS FROM THAT POINT.  
 ALL DIMENSIONS SHOWN ARE TO THE FACE OF GUARDRAIL.  
 ALL EXISTING GUARDRAIL SHALL BE REMOVED, HOWEVER ONLY PROPOSED GUARDRAIL IS SHOWN. SEE GUARDRAIL SCHEDULE.

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|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

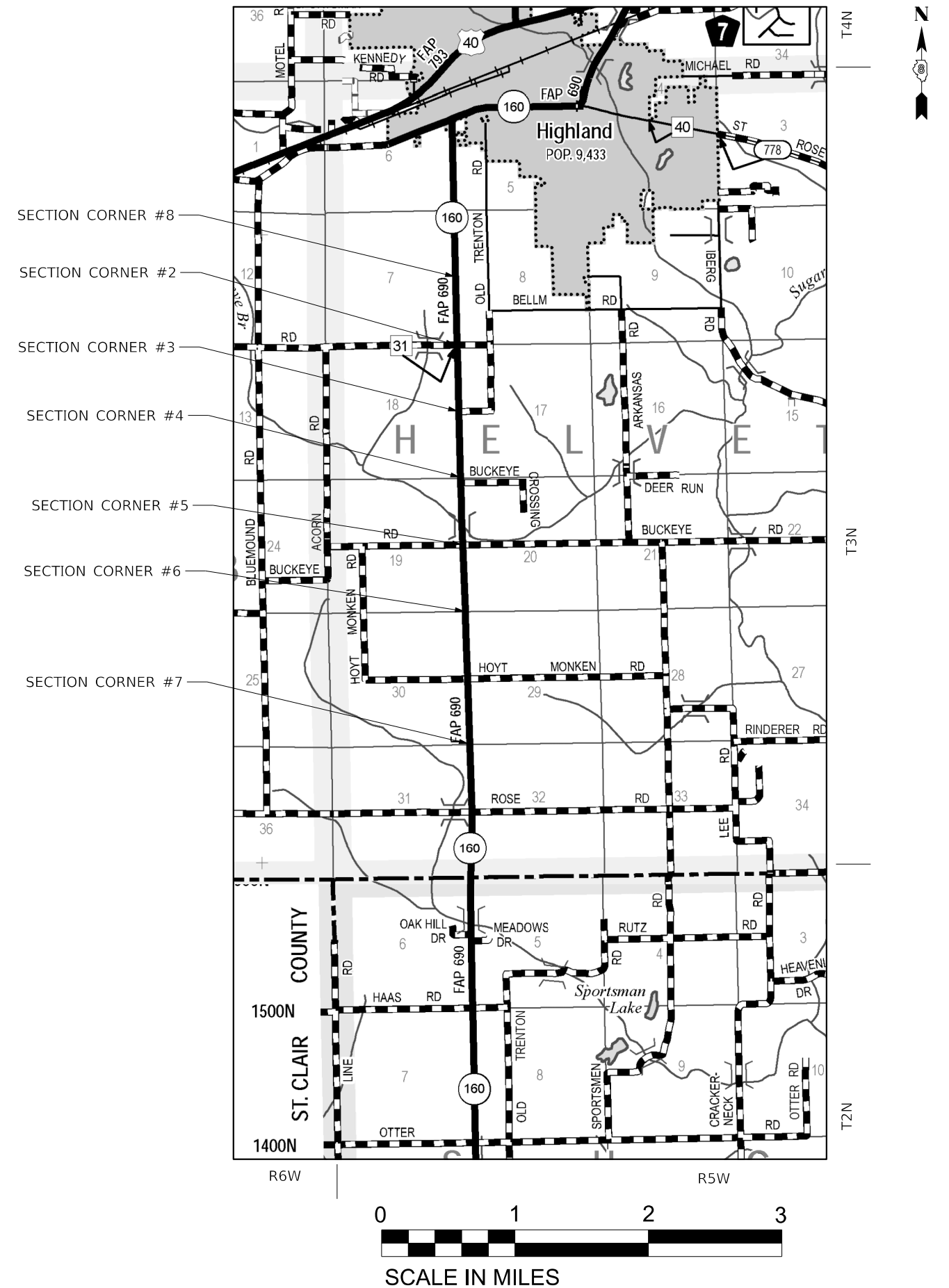
**GUARDRAIL PLAN  
 LOCATION #5**

SCALE: 1"=20'      SHEET 4 OF 4 SHEETS      STA. 188+00 TO STA. 194+00

| F.A.P. RTE.        | SECTION      | COUNTY  | TOTAL SHEETS              | SHEET NO. |
|--------------------|--------------|---------|---------------------------|-----------|
| 690                | (15.15X)RS-3 | MADISON | 17                        | 14        |
| CONTRACT NO. 76K88 |              |         | ILLINOIS FED. AID PROJECT |           |

| SECTION CORNER COORDINATE TABLE |  |  |
|---------------------------------|--|--|
| POINT #                         | DESCRIPTION  | CORNER   |
| SECTION CORNER #8               | FOUND IRON PIN WITH CAP FLUSH                              | WEST QUARTER CORNER SECTION 8, T.3.N., R.5.W.  |
| SECTION CORNER #2               | FOUND IRON PIN WITH CAP BURIED 6" INSIDE OF BRASS CYLINDER | SW CORNER SECTION 8, T.3.N., R.5.W.            |
| SECTION CORNER #3               | FOUND IRON PIN WITH CAP BURIED 6" INSIDE OF BRASS CYLINDER | WEST QUARTER CORNER SECTION 17, T.3.N., R.5.W. |
| SECTION CORNER #4               | FOUND IRON PIN WITH CAP BURIED 6"                          | NW CORNER SECTION 20, T.3.N., R.5.W.           |
| SECTION CORNER #5               | FOUND IRON PIN WITH CAP BURIED 6"                          | WEST QUARTER CORNER SECTION 20, T.3.N., R.5.W. |
| SECTION CORNER #6               | FOUND IRON PIN WITH CAP BURIED 6" INSIDE OF BRASS CYLINDER | NE CORNER SECTION 30, T.3.N., R.5.W.           |
| SECTION CORNER #7               | FOUND IRON PIN WITH CAP BURIED 6" INSIDE OF BRASS CYLINDER | SE CORNER SECTION 30, T.3.N., R.5.W.           |

NOTES: SECTION CORNERS SHALL BE SET IN ACCORDANCE WITH SURVEY MONUMENT COVER ASSEMBLY SPECIAL PROVISION.  
SECTION CORNER #1 IS LOCATED OUTSIDE THE PROJECT LIMITS AND THUS IS NOT PART OF THIS PROJECT.



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|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

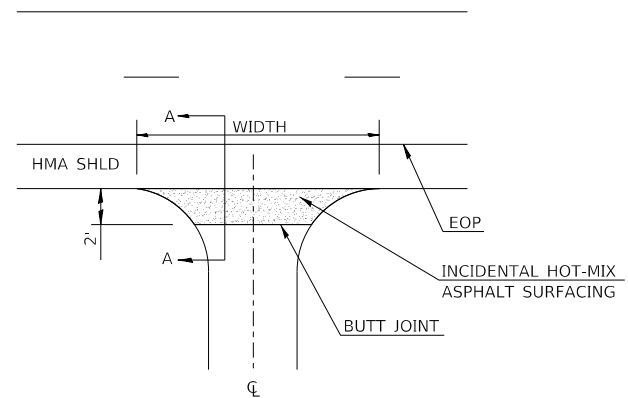
**SURVEY MONUMENTS**

SCALE: 1"=1 MILE SHEET 1 OF 2 SHEETS STA. TO STA.

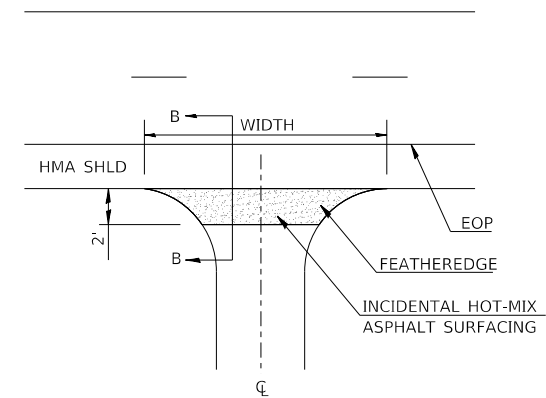
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| F.A.P. RTE.        | SECTION      | COUNTY   | TOTAL SHEETS     | SHEET NO. |
| 690                | (15,15X)R5-3 | MADISON  | 17               | 15        |
| CONTRACT NO. 76K88 |              |          |                  |           |
|                    |              | ILLINOIS | FED. AID PROJECT |           |



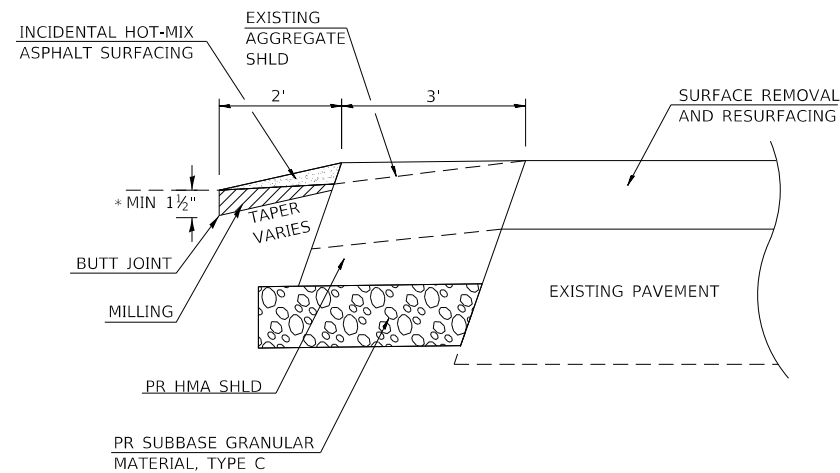




**CONCRETE ENTRANCE DETAIL**

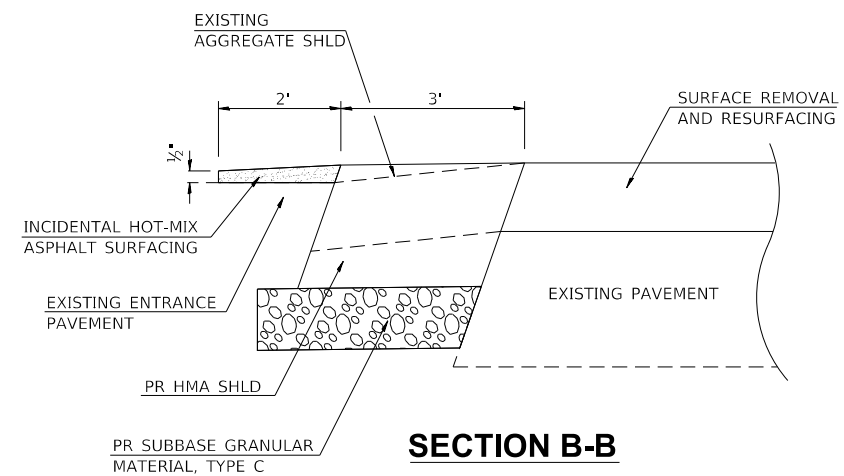


**HMA, AGG, AND FIELD ENTRANCE DETAIL  
(FEATHEREDGE DETAIL)**



**SECTION A-A**

\* MILLING DEPTH SHALL BE A MAX OF 1 1/2" WHEN SIDEROAD OR ENTRANCE IS CONCRETE.



**SECTION B-B**

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|                              |            |           |
|------------------------------|------------|-----------|
| USER NAME = calcaterradm     | DESIGNED - | REVISED - |
|                              | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/15/2019        | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS**

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.P. RTE.               | SECTION      | COUNTY  | TOTAL SHEETS | SHEET NO. |
|---------------------------|--------------|---------|--------------|-----------|
| 690                       | (15.15X)RS-3 | MADISON | 17           | 17        |
| CONTRACT NO. 76K88        |              |         |              |           |
| ILLINOIS FED. AID PROJECT |              |         |              |           |