

GENERAL NOTES FOR STAGE CONSTRUCTION AND MAINTENANCE OF TRAFFIC

- TRAFFIC CONTROL AND PROTECTION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, HIGHWAY STANDARDS AND CONTRACT SPECIAL PROVISIONS, AND AS DIRECTED BY THE ENGINEER. TRAFFIC CONTROL SHOWN IN THESE PLANS REPRESENTS A GUIDE FOR THE SAFE MANAGEMENT OF TRAFFIC DURING THE EXECUTION OF THE WORK. MODIFICATIONS MAY BE NECESSARY DUE TO LOCAL CONDITIONS AT THE TIME OF CONSTRUCTION. ANY PROPOSED CHANGES BY THE CONTRACTOR SHALL BE APPROVED BY THE ENGINEER IN WRITING PRIOR TO BEING IMPLEMENTED. ANY MODIFICATIONS OR ADDITIONS REQUIRED BY THE ENGINEER WILL BE INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION, SPECIAL, UNLESS A SEPARATE PAY ITEM HAS BEEN ESTABLISHED FOR THE WORK.
- ANY EXISTING OR TEMPORARY PAVEMENT MARKINGS WHICH CONFLICT WITH MARKINGS REQUIRED FOR THE IMMEDIATELY FOLLOWING CONSTRUCTION STAGE SHALL BE REMOVED ACCORDING TO SECTION 783 OF THE STANDARD SPECIFICATIONS.
- IL ROUTE 31 (FIRST STREET) SHALL HAVE A MINIMUM OF ONE THROUGH LANE OPEN IN EACH DIRECTION AND LEFT-TURN LANES TO IL ROUTE 38 (STATE STREET) AT ALL TIMES, EXCEPT AS NOTED HEREIN OR APPROVED BY THE ENGINEER. A SHORT-TERM SINGLE LANE CLOSURE MAY BE REQUIRED ON IL ROUTE 38 (STATE STREET). THIS LANE CLOSURE SCHEDULE WILL REQUIRE APPROVAL BY IDOT AND THE CITY. MINIMUM LANE WIDTHS OF 10 FEET SHALL BE PROVIDED AT ALL TIMES WITHIN THE PROJECT LIMITS, EXCEPT AS SHOWN HEREIN OR APPROVED BY THE ENGINEER.
- ACCESS TO ADJACENT PROPERTIES AND SIDE STREETS SHALL BE MAINTAINED AT ALL TIMES.
- THE FOLLOWING TEMPORARY PAVEMENT MARKINGS SHALL BE USED IN EACH OF THE VARIOUS STAGES OF CONSTRUCTION AS REQUIRED:
 - 4" WHITE EDGE LINE - EACH EDGE
 - 4" DOUBLE YELLOW - MEDIANS AND BETWEEN OPPOSING LANES
 - 6" WHITE SKIP-DASH (6' SPACE - 2' DASH) - TURN BAY TAPER
 - 6" WHITE LANE LINE - STORAGE AREA TURN BAY
 - 12" YELLOW DIAGONALS
 - 24" WHITE STOP BAR - ALL LOCATIONS
 - WHITE LETTERS AND SYMBOLS - TURN LANES
- TEMPORARY PAVEMENT MARKING APPLIED TO FINAL PAVEMENT SURFACES AND EXISTING PAVEMENT SURFACES TO REMAIN SHALL BE PAVEMENT MARKING TAPE, TYPE III.
- PLATING AND/OR TEMPORARY DRAINAGE STRUCTURE ADJUSTMENTS MAY BE REQUIRED DUE TO THE STAGING OF CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, SPECIAL.
- THE CONTRACTOR SHALL NOTIFY THE IDOT TRAFFIC CONTROL SUPERVISOR FOR ARTERIALS AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

EROSION CONTROL NOTES:

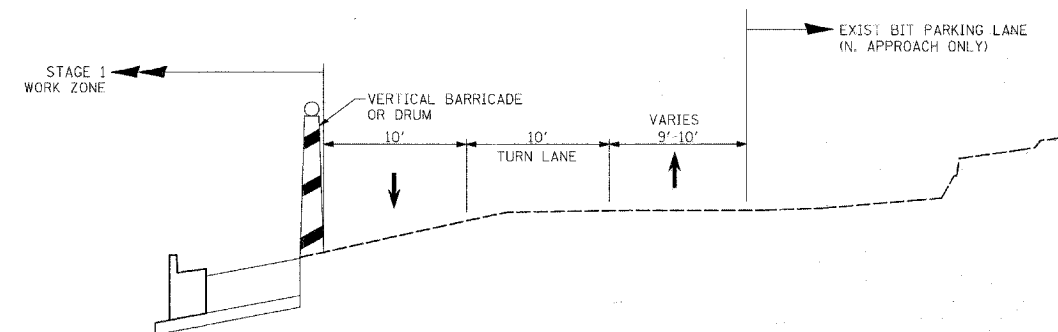
THE FOLLOWING WAS ESTABLISHED AND INCLUDED IN THESE PLANS TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS.

THE PURPOSE OF THE PROJECT EROSION CONTROL SYSTEMS IS TO MINIMIZE SILTATION WITHIN THE CONSTRUCTION ZONE AND TO ELIMINATE SEDIMENTS FROM ENTERING AND LEAVING THE CONSTRUCTION ZONE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

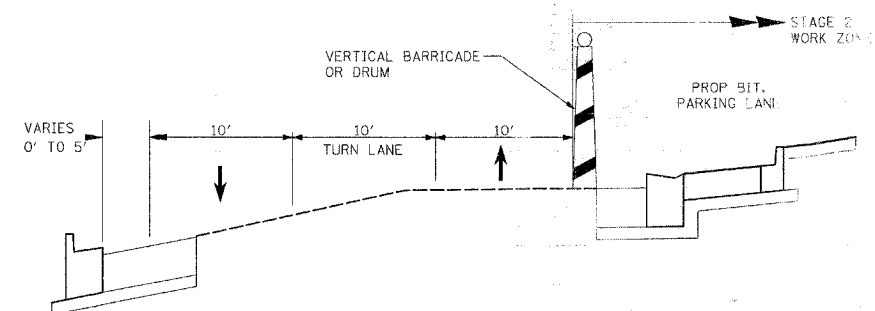
CERTAIN ITEMS, AS SHOWN IN THIS PLAN, SHALL BE PLACED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE PLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION RESULTING FROM THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

NPDES STORM WATER PERMIT REQUIREMENTS ARE NOT APPLICABLE TO THIS PROJECT.

- ALL WORK PROPOSED ON THE EROSION CONTROL PLAN SHALL BE DONE IN ACCORDANCE WITH THE "ILLINOIS URBAN MANUAL" (LATEST EDITION), THE STANDARD SPECIFICATIONS, SPECIAL PROVISIONS, AND DETAILS AS SHOWN IN THE PLANS. THE CONTRACTOR IS DIRECTED TO THE CONTRACT SPECIAL PROVISIONS FOR THE APPLICABLE CONSTRUCTION STANDARD AND SUPPLEMENTAL INFORMATION. MAINTENANCE, CLEANING, REPLACEMENT, AND FINAL REMOVAL OF THE EROSION CONTROL ITEMS SHALL BE INCLUDED IN THE COST OF THE ITEM.
- THE CONSTRUCTION LIMITS WILL BE IDENTIFIED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGED CONSTRUCTION LIMITS.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY OR PERMANENT MEASURES. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF SOIL DISTURBANCE. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY.
- EROSION CONTROL MEASURES INDICATED IN THESE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY. ANY PROPOSED CHANGES BY THE CONTRACTOR SHALL BE APPROVED BY THE ENGINEER PRIOR TO BEING IMPLEMENTED. ANY MODIFICATIONS OR ADDITIONS REQUIRED BY THE ENGINEER SHALL BE INCLUDED IN THE COST OF THE EROSION CONTROL ITEM.
- THE TEMPORARY EROSION CONTROL SYSTEMS MAY BE UTILIZED IN MULTIPLE CONSTRUCTION STAGES. THESE SYSTEMS SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION OR AS DIRECTED BY THE ENGINEER.
- ALL EROSION CONTROL PRODUCTS FURNISHED SHALL BE SPECIFICALLY RECOMMENDED BY THE MANUFACTURER FOR THE USE SPECIFIED IN THESE PLANS. PRIOR TO APPROVAL AND USE, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER CERTIFICATION BY THE PRODUCER STATING THE INTENDED USE OF THE PRODUCT AND THAT THE PHYSICAL PROPERTIES REQUIRED FOR THIS APPLICATION ARE MET OR EXCEEDED. THE CONTRACTOR SHALL PROVIDE MANUFACTURER INSTALLATION PROCEDURES TO THE ENGINEER UPON REQUEST.
- SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED ON SITE. THIS COST SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- DISTURBED AREAS SHALL BE PERMANENTLY SODDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED WITHIN 14 CALENDAR DAYS FROM INITIAL DISTURBANCE OR RE-DISTURBANCE.
- CONSTRUCTION EQUIPMENT SHALL BE STORED, FUELED AND WASHED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL AND OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS.
- THE CONTRACTOR SHALL INSPECT ALL SOIL EROSION CONTROL MEASURES ON A WEEKLY BASIS OR AFTER A 1/2" RAINFALL AND REPLACE, REPAIR OR CLEAN THEM WITHIN 24 HOURS.
- DURING DEWATERING OPERATIONS, WATER WILL BE PUMPED ONLY INTO SEDIMENT BASINS OR SILT TRAPS. DEWATERING DIRECTLY INTO NATURAL DRAINAGE WAYS, FIELD TILES OR STORM WATER STRUCTURES THAT DO NOT DRAIN INTO SEDIMENT BASINS OR SILT TRAPS IS PROHIBITED.
- QUANTITIES OF EROSION CONTROL ITEMS HAVE BEEN INCLUDED IN THESE PLANS. THE ACTUAL NEED FOR ANY EROSION CONTROL SYSTEM WILL BE DETERMINED ON-SITE AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ANY QUANTITIES FOR EROSION CONTROL ITEMS NOT USED WILL BE DEDUCTED IN ACCORDANCE WITH ARTICLE 109.03 OF THE STANDARD SPECIFICATIONS.



STAGE 1 TYPICAL SECTION
IL ROUTE 31 (FIRST ST) AT IL ROUTE 38



STAGE 2 TYPICAL SECTION
IL ROUTE 31 (FIRST ST) AT IL ROUTE 38