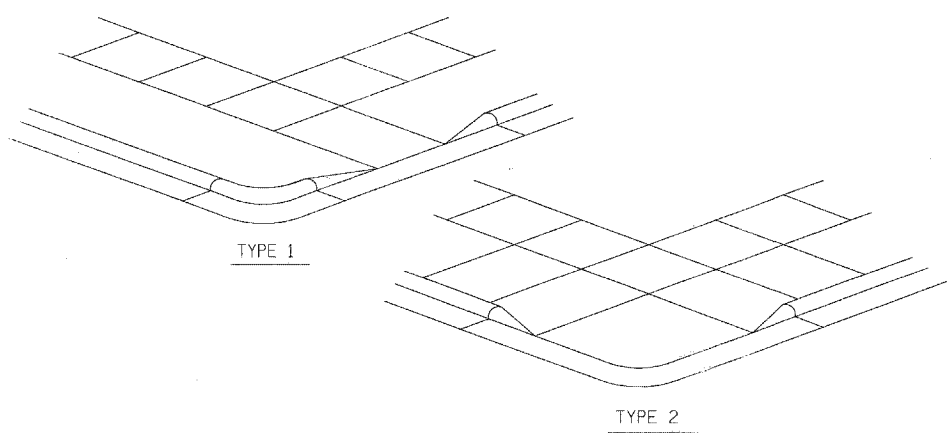
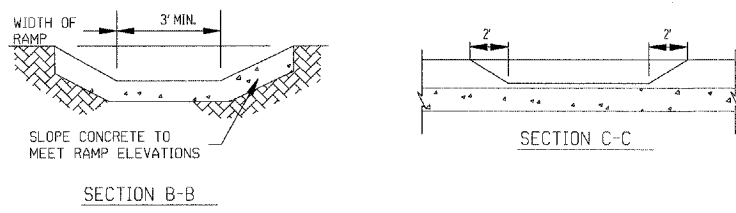
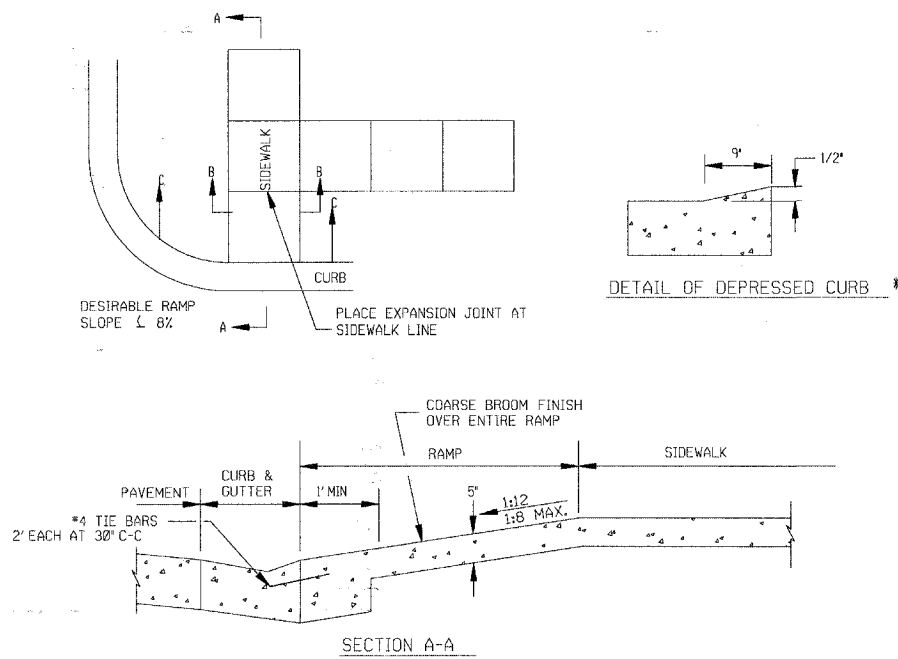


STANDARD DETAIL FOR CURB RAMPS



NOTES: CURB CUT RAMPS ARE TO BE LOCATED IN LINE WITH ALL CROSS WALKS OR AS DIRECTED BY THE ENGINEER

FINAL SURFACE TEXTURE OF THE RAMP SHALL BE THAT OBTAINED PLACING DETECTABLE WARNINGS WITH RAISED TRUNCATED DOMES IN THE FINISHED CONCRETE.

CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON THE RAMP, FREE OF SAGS AND SHORT GRADE CHANGES

IF POSSIBLE, DRAINAGE STRUCTURES SHALL NOT BE PLACED IN LINE WITH RAMPS, EXCEPT WHERE EXISTING DRAINAGE STRUCTURES ARE BEING UTILIZED IN THE NEW CONSTRUCTION.

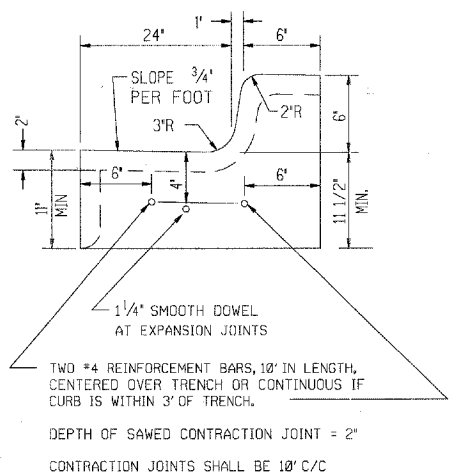
LOCATION OF THE RAMP SHOULD TAKE PRECEDENCE OVER LOCATION OF THE DRAINAGE STRUCTURE.

THE NORMAL GUTTER LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP.

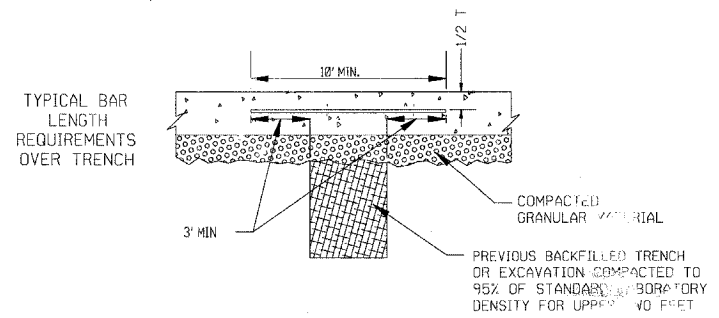
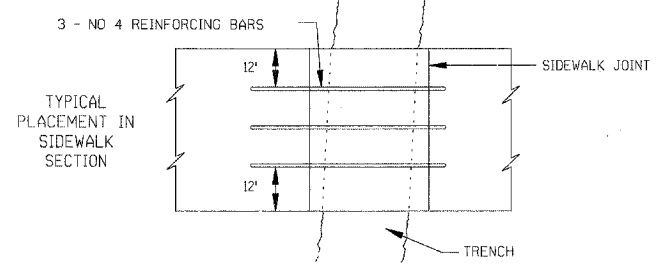
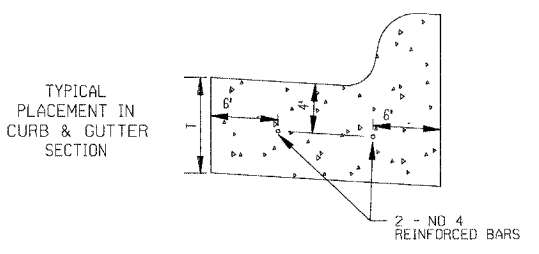
CROSSWALK AND STOP LINE MARKINGS (IF USED) SHALL BE SO LOCATED AS TO STOP TRAFFIC SHORT OF RAMP CROSSING.

DEPRESSED CURB IS NOT STANDARD. WHEN USED, THE DEPTH IS 1/2" INSTEAD OF 1-1/2".

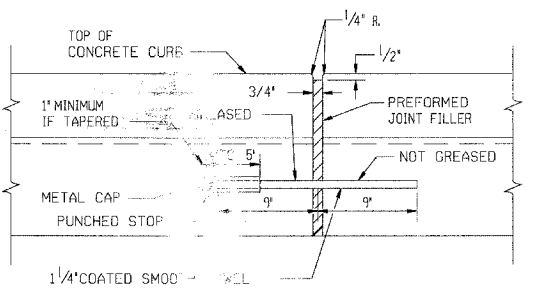
THE COST OF CONSTRUCTION OF CURB RAMPS, INCLUDING DEPRESSED CURB, SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR P.C.C. SIDEWALK AND COMBINATION CONCRETE CURB AND GUTTER OF THE TYPE SPECIFIED. DETECTABLE WARNINGS WILL BE PAID FOR SEPARATELY.



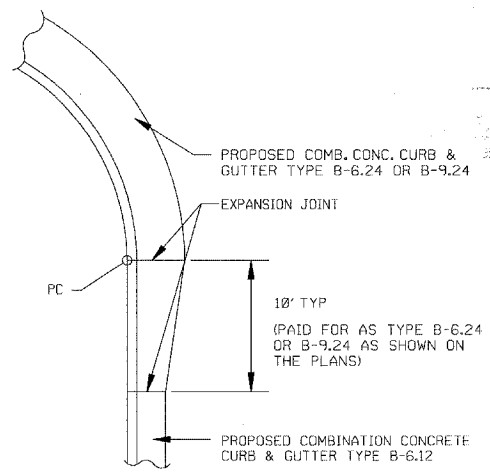
SAWED CONTRACTION JOINT DETAIL



REINFORCING CURB & GUTTER OR SIDEWALK INSTALLED OVER TRENCH

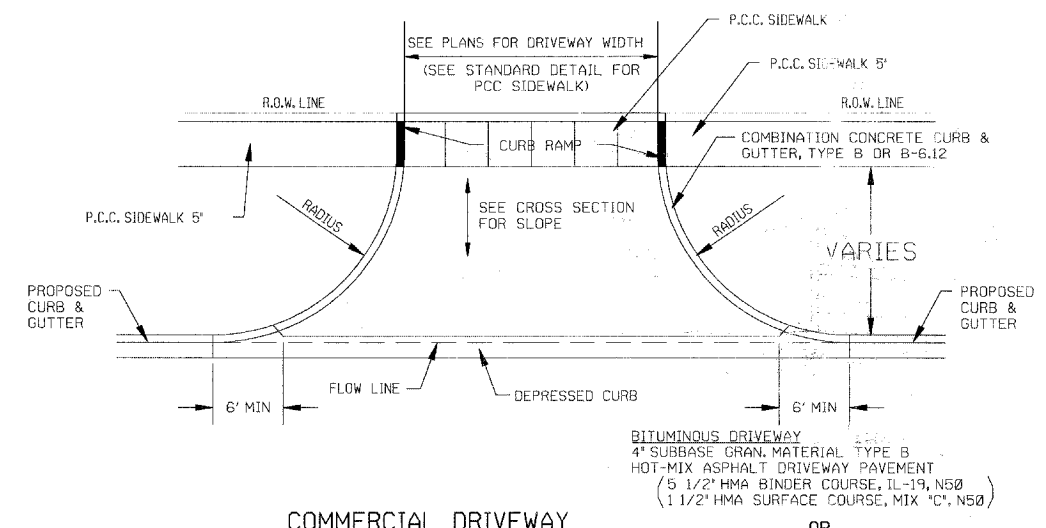


CURB EXPANSION JOINT DETAIL



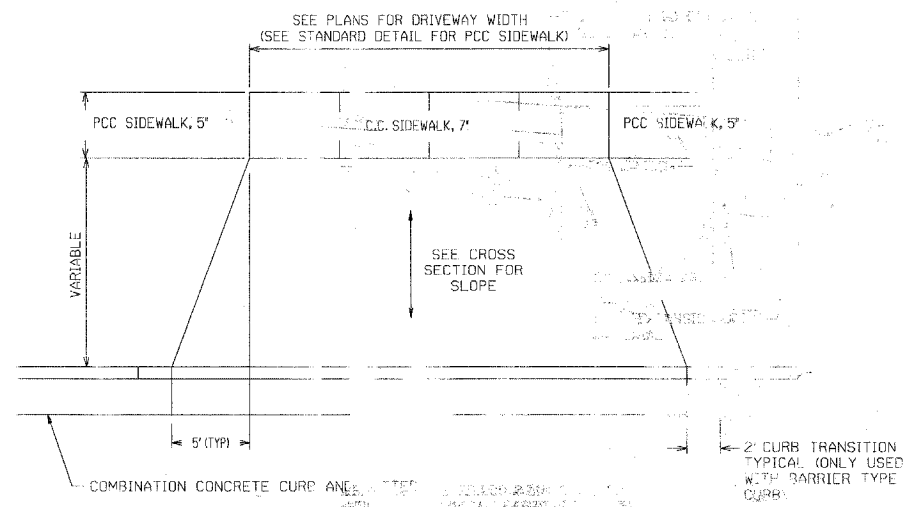
CURB AND GUTTER TRANSITION DETAIL

(N.T.S.)



COMMERCIAL DRIVEWAY

(N.T.S.)



RESIDENTIAL DRIVEWAY