

04-27-12 LETTING ITEM 019

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

FAU 1042 (WOODLAWN AVENUE)  
FAP 397 (SIBLEY BLVD) TO FAU 3593 (LINCOLN AVE)  
ROADWAY RESURFACING  
SECTION NO.: 11-00116-00-RS  
PROJECT NO.: M-9003 (908)  
VILLAGE of DOLTON  
COOK COUNTY  
JOB NO.: C-91-161-12

+1 = 12

F. A. S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	1
STA.	TO STA.			
FED. ROAD DIST. NO.	CLASS.	FED. AID PROJECT	M-9003(908)	

CONTRACT #63677

- INDEX OF SHEETS**
- COVER SHEET, INDEX OF SHEETS & STATE STANDARDS
  - SUMMARY OF QUANTITIES
  - TYPICAL CROSS SECTIONS
  - PAVEMENT PLAN
  - PAVEMENT MARKINGS
  - 6.-11. IDOT DISTRICT 1 STANDARD DETAILS

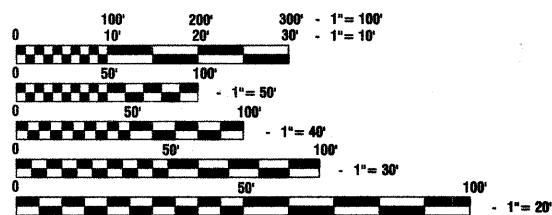
- HIGHWAY STANDARDS**
- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
  - 424001-06 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
  - 442201-03 CLASS C AND D PATCHES
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  - 606001-04 CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
  - 701501-06 URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
  - 701801-05 SIDEWALK, CORNER OR CROSSWALK CLOSURE
  - 701901-02 TRAFFIC CONTROL DEVICES
  - 886001-01 DETECTOR LOOP INSTALLATIONS
  - 886006-01 TYPICAL LAYOUT FOR DETECTION LOOPS

**WOODLAWN AVENUE**

2010 ADT -	2,400
2040 ADT -	2,400
POSTED SPEED LIMIT -	25 mph
DESIGN PERIOD -	20 YEARS
DESIGN SPEED LIMIT -	30 mph
STREET CLASSIFICATION -	COLLECTOR

**SCALES**

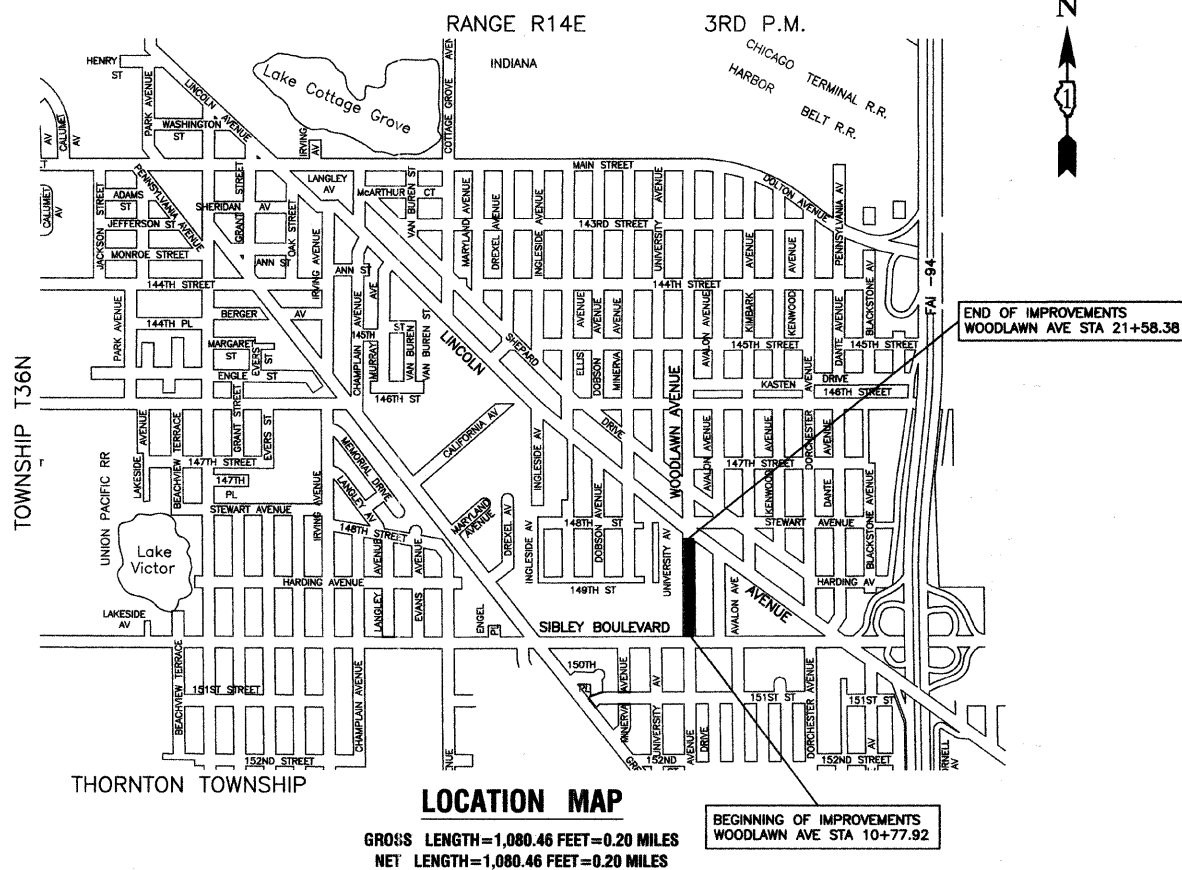
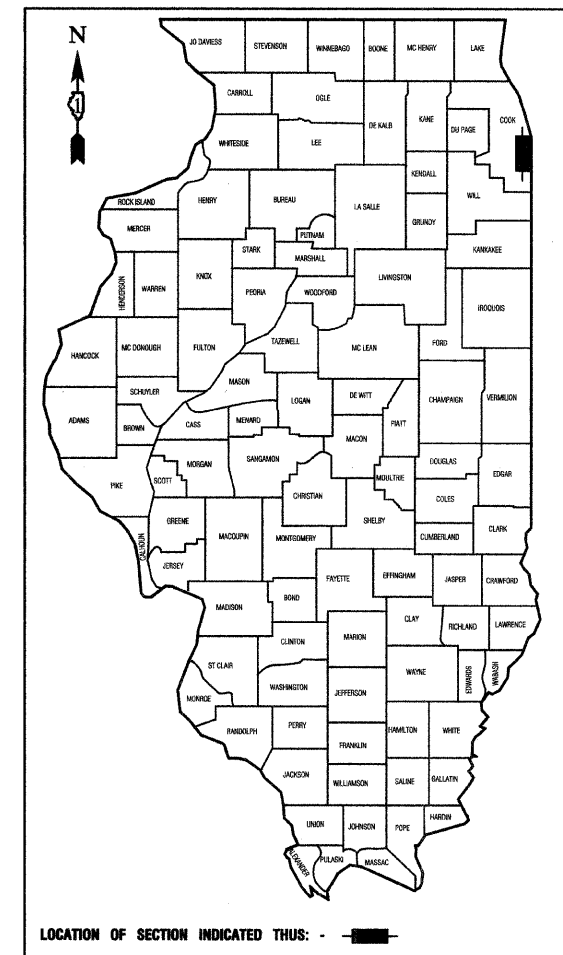
- PLAN - 1" = 50'
- PROFILE HORIZ. - 1" = 50'
- PROFILE VERT. - 1" = 5'
- CROSS SECTIONS - 1" = 10'



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J. U. L. I. E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123 or 811

CONTRACT NO. 63677



STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

Approved: 1-31-12  
*Hennie C. Lewis*  
President, Village of Dolton

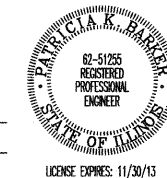
Passed: FEBRUARY 19, 2012  
*C. H. [Signature]*  
District 1 Engineer of Local Roads & Streets

Released for Bid Based on Limited Review: FEBRUARY 14, 2012  
*Deane M. O'Keefe*  
Deputy Director of Highways, Region 1 Engineer

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

PREPARED BY OR UNDER THE DIRECT SUPERVISION OF:

*Shirley Baker*  
1-31-12



PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, PE (847)705-4406, SCHAUMBURG, IL  
CONSULTANTS: ROBINSON ENGINEERING, LTD. 708-331-6700

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE
S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	0005
	20101700	SUPPLEMENTAL WATERING	UNIT	1	1
	25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4	4
	25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	4	4
	25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	4	4
	25200110	SODDING, SALT TOLERANT	SQ YD	100	100
	40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	700	700
	40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	220	220
	40600895	CONSTRUCTING TEST STRIP	EACH	1	1
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	100	100
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	340	340
	42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	40	40
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2110	2110
	42400800	DETECTABLE WARNINGS	SQ FT	16	16
	44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	3700	3700
	44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	40	40
	44000600	SIDEWALK REMOVAL	SQ FT	2110	2110
	44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	15	15
	44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	45	45
	44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	120	120
	44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1665	1665
	60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	1	1
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	6	6
	60255500	MANHOLES TO BE ADJUSTED	EACH	4	4
	60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1
	60257900	MANHOLES TO BE RECONSTRUCTED	EACH	1	1
	60500050	REMOVING CATCH BASINS	EACH	1	1
	67100100	MOBILIZATION	L SUM	1	1

\* - INDICATES SPECIALTY ITEMS

SUMMARY OF QUANTITIES					CONSTRUCTION TYPE CODE
S.I.	CODE NO.	PAY ITEM	UNIT	QUAN	0005
	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	220	220
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	300	300
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	90	90
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	100	100
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	40	40
*	88600600	DETECTOR LOOP REPLACEMENT	FOOT	200	200
	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	450	450

\* - INDICATES SPECIALTY ITEMS

#### GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 AND (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION REQUIRED)
- UTILITIES INDICATED ON THE PLANS ARE PROVIDED FOR THE CONTRACTORS USE AND ARE BASED UPON INFORMATION AVAILABLE AT THE TIME OF THE ADVERTISEMENT FOR BIDS. THE OWNER AND ENGINEER DO NOT GUARANTEE THE ACCURACY OF UTILITY INFORMATION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE THICKNESS OF HMA MIXTURE STATED IN THE SPECIFICATIONS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA SURFACE IS PLACED.
- ACCESS TO DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES BY LIMITING CURB AND GUTTER REPAIR TO ONE-HALF THE DRIVEWAY WIDTH AT ONE TIME AS WELL AS TEMPORARY AGGREGATE.
- THE REMOVAL AND/OR REPLACEMENT OF ANY DRIVEWAYS, PAVEMENT, CURB, SIDEWALK, ETC. SHALL BE ACCOMPLISHED BY MEANS OF A SAW CUT JOINT, AT THE DIRECTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE FOR THE VARIOUS ITEMS.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR OTHER DRAINAGE STRUCTURES SHALL BE REMOVED BY THE END OF EACH DAY BY THE CONTRACTOR AT THEIR EXPENSE.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS OTHERWISE NOTED ON THE PLAN.

FILE NAME = 11279-QUAN-01 - P01

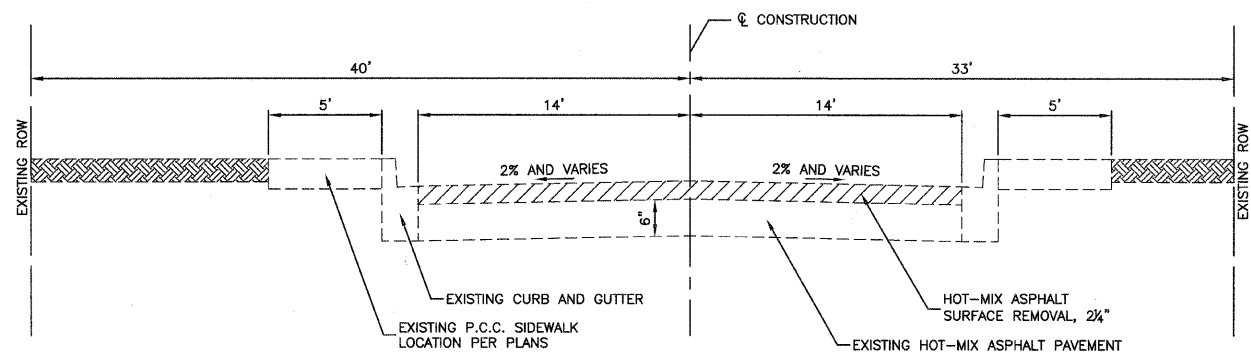
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PLOT SCALE =	CHECKED - PKB	REVISED -
PLOT DATE = 1-31-12	DRAWN - MED	REVISED -
	CHECKED - AG	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WOODLAWN AVENUE RESURFACING  
SIBLEY BLVD TO LINCOLN AVE  
SUMMARY OF QUANTITIES & GENERAL NOTES

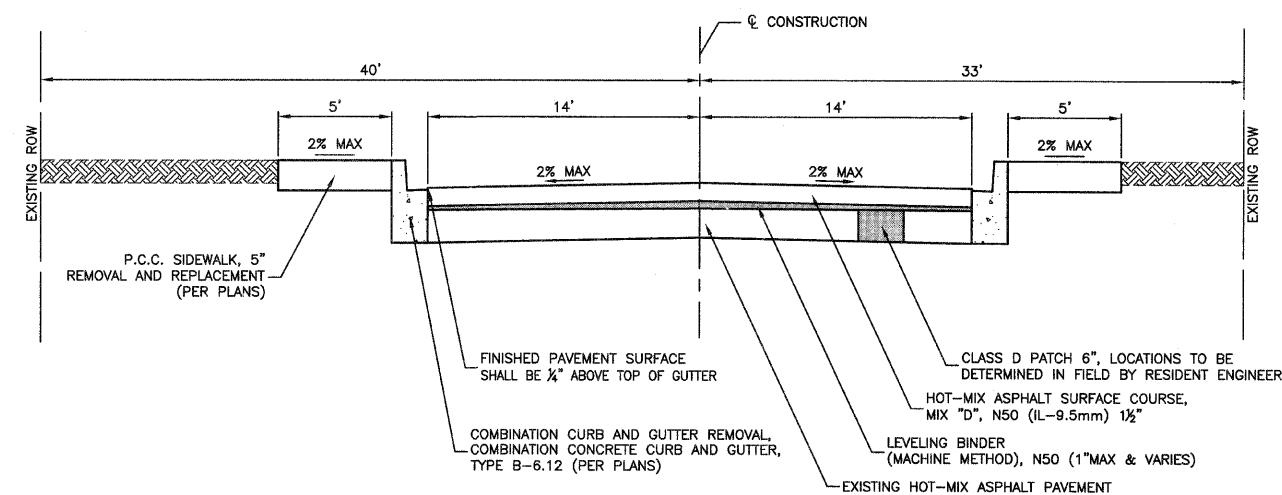
SCALE: NONE SHEET NO. 2 OF 11 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	2
FED. ROAD DIST. NO. 1 ILLINOIS			FED. AID PROJECT M-9003(908)	



**EXISTING TYPICAL SECTION**

WOODLAWN AVENUE  
SIBLEY BLVD TO LINCOLN AVE  
STA 10+77.92 TO STA 21+25.38



**PROPOSED TYPICAL SECTION**

WOODLAWN AVENUE  
SIBLEY BLVD TO LINCOLN AVE  
STA 10+77.92 TO STA 21+25.38

NOTE: CONTRACTOR SHALL MILL BEFORE PATCHING

**HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

MIXTURE TYPE	AIR VOIDS @ NDES
RESURFACING	
HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50(IL-9.5mm); 1 1/2"	4% @ 50 Gyr.
LEVELING BINDER (MACHINE METHOD), N50(IL-9.5mm), (1")	4% @ 50 Gyr.
PATCHING	
CLASS D PATCHES, TYPE I, II, III, IV (HMA BINDER IL-19.0mm), 6" (IN 3 LIFTS)	4% @ 70 Gyr.

- NOTES:
- THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
  - THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC" TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.
  - CONTRACTOR SHALL MILL BEFORE PATCHING.

FILE NAME = 11279-TYPX-01 - IDOT P01

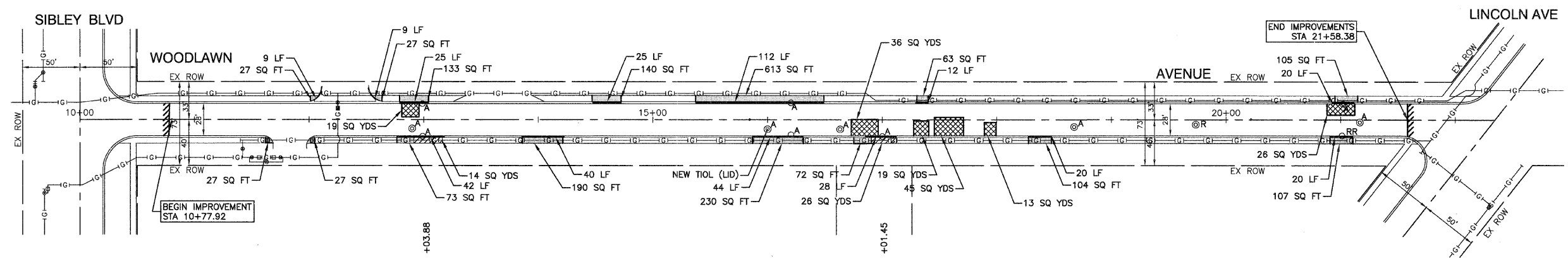
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PLOT DATE = 1-31-12	CHECKED -- QG	REVISED --

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WOODLAWN AVENUE RESURFACING  
SIBLEY BLVD TO LINCOLN AVE  
TYPICAL CROSS SECTIONS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	3
CONTRACT NO. 63677				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(808)				

SCALE: SHEET NO. 3 OF 11 SHEETS STA. TO STA.



**NOTES:**

1. DETECTABLE WARNINGS ARE TO BE INSTALLED AT LOCATIONS OF SIDEWALK REMOVAL AND REPLACEMENT. CONTRACTOR TO POSITION AND PLACE DETECTABLE WARNINGS AT THE DIRECTION OF THE ENGINEER IN ACCORDANCE WITH ADA REQUIREMENTS.
2. LIMIT OF CONSTRUCTION ON ALL SIDE STREETS IS THE RADIUS RETURN PLUS 10' BUTT JOINT OR ROW AS NOTED.
3. ACCESS TO ALL DRIVEWAYS SHALL BE MAINTAINED UPON COMPLETION OF EACH WORKDAY. RESIDENTS SHALL BE NOTIFIED 24 HOURS IN ADVANCE OF WHEN DRIVEWAY REPLACEMENT WILL TAKE PLACE.

**LEGEND**

- HMA PAVEMENT REPAIR
- HMA DRIVEWAY REMOVAL AND REPLACEMENT
- CONCRETE DRIVEWAY REMOVAL AND REPLACEMENT
- CONCRETE SIDEWALK REMOVAL AND REPLACEMENT
- HOT-MIX ASPHALT SURFACE REMOVAL- BUTT JOINT (10' UNLESS OTHERWISE INDICATED)
- CURB REMOVAL
- CATCH BASIN TO BE ADJUSTED, MANHOLE TO BE ADJUSTED, VALVE BOX TO BE ADJUSTED, FRAME & LID TO BE ADJUSTED (SPECIAL)
- CATCH BASIN TO BE REMOVED AND REPLACED, MANHOLE TO BE REMOVED AND REPLACED.
- CATCH BASIN TO BE RECONSTRUCTED, MANHOLE TO BE RECONSTRUCTED.
- DETECTABLE WARNINGS

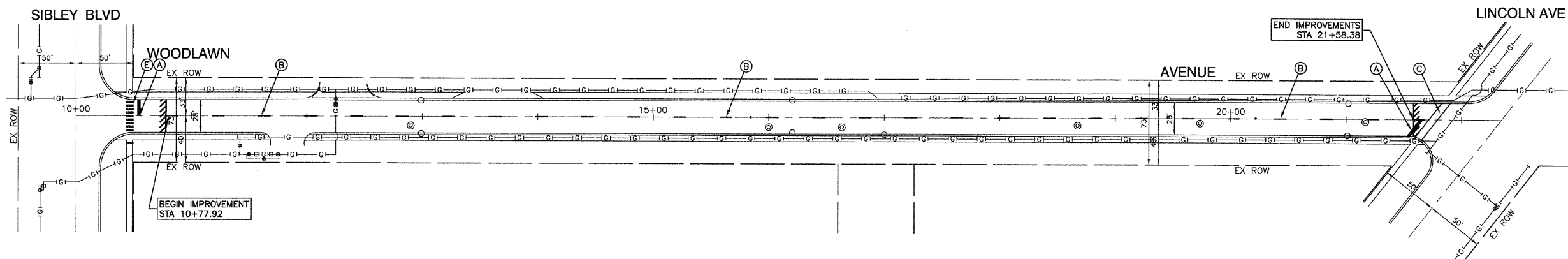
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	PLOT SCALE =	DRAWN -- MED	REVISED --
	PLOT DATE = 1-31-12	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

WOODLAWN AVENUE RESURFACING SIBLEY BLVD TO LINCOLN AVE PAVEMENT PLAN			
SCALE: 1"=50'	SHEET NO. 4	OF 11 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	4
CONTRACT NO. 63677				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)				

NOT DRAWN BY: RICHARD G. BROWN, CIVIL ENGINEER  
PLOT DATE BY: RICHARD G. BROWN, CIVIL ENGINEER



**LEGEND**

- (A) THERMOPLASTIC PAVEMENT MARKING  
WHITE, 24"
- (B) THERMOPLASTIC PAVEMENT MARKING  
YELLOW, LINE, 4", SKIP DASH (10' LINE--30' SPACE)
- (C) THERMOPLASTIC PAVEMENT MARKING  
WHITE, LINE, 6" (6' C-C)
- (D) THERMOPLASTIC PAVEMENT MARKING  
YELLOW, DOUBLE LINE, 4"
- (E) THERMOPLASTIC PAVEMENT MARKING  
WHITE, LINE, 12" (3' C-C)

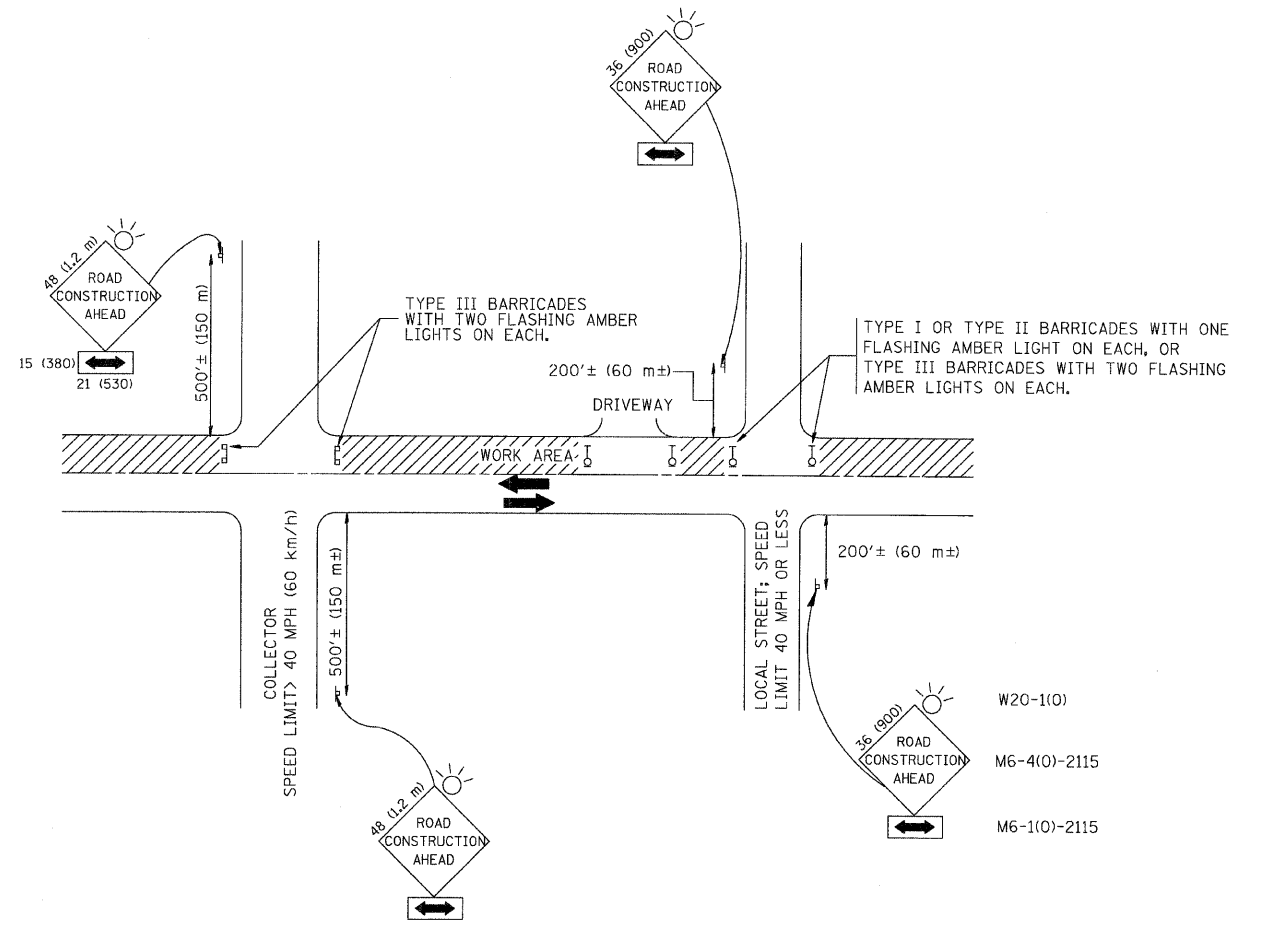
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		CHECKED -- PKB	REVISED --
	PLOT SCALE =	DRAWN -- MED	REVISED --
	PLOT DATE = 1-31-12	CHECKED -- AG	REVISED --

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>WOODLAWN AVENUE RESURFACING SIBLEY BLVD TO LINCOLN AVE PAVEMENT MARKINGS</b>			
SCALE: 1"=50'	SHEET NO. 5	OF 11 SHEETS	STA. TO STA.

F.A.U. RTE. 1042	SECTION 11-00116-00-RS	COUNTY COOK	TOTAL SHEETS 11	SHEET NO. 5
CONTRACT NO. 63677				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)				

LAST SAVED BY: PETERANTONIC 04/30/12  
PLOTTED BY: FILTER 05/04/12 09:05:19



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
  3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

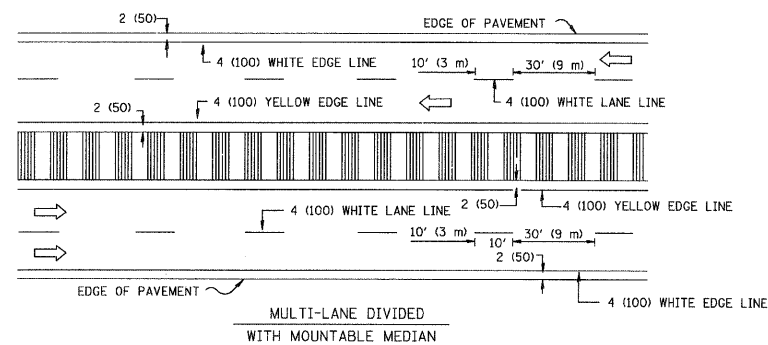
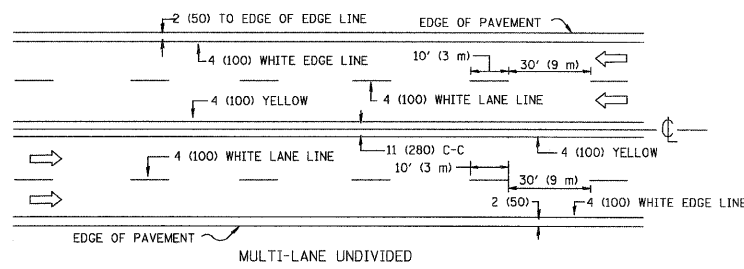
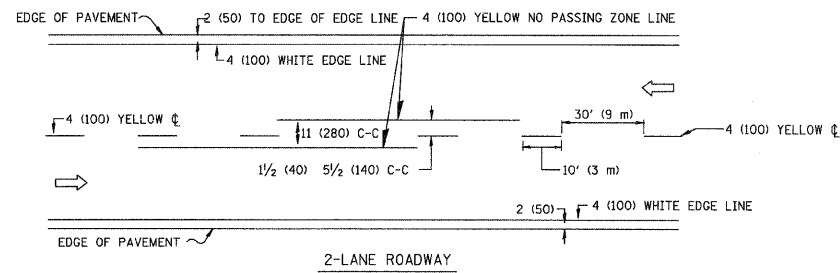
USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = 11279-DTLS-01 - TC-10	USER NAME = gagliardi	DESIGNED -- LHA	REVISED -- J. OBERLE 10-18-95
		CHECKED --	REVISED -- A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	DRAWN --	REVISED -- A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	CHECKED -- 06-89	REVISED -- T. RAMMACHER 01-06-00

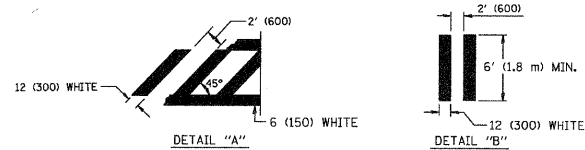
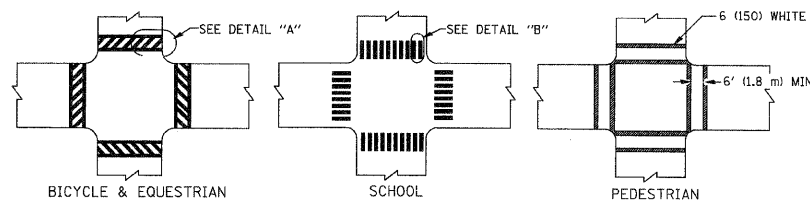
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

<b>TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS</b>			
SCALE:	SHEET NO. 6 OF 11 SHEETS	STA. N/A TO STA. N/A	
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS
1042	11-00116-00-RS	COOK	11 6
<b>TC-10</b>			CONTRACT NO. 63677
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT	M-9003(908)

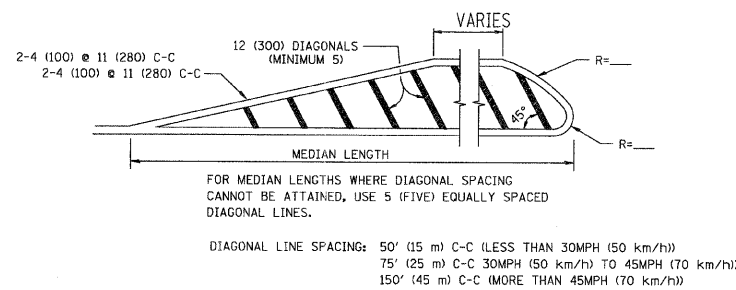
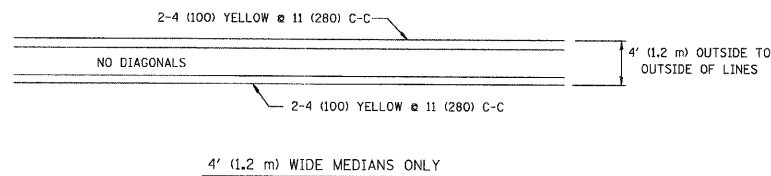


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

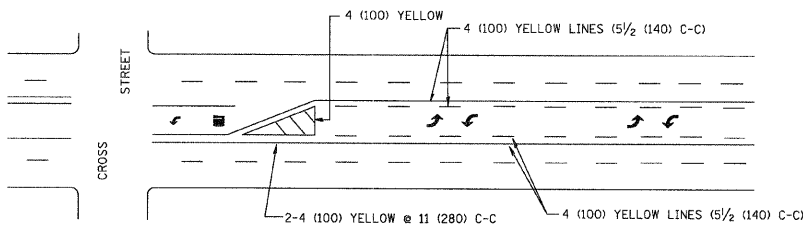
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

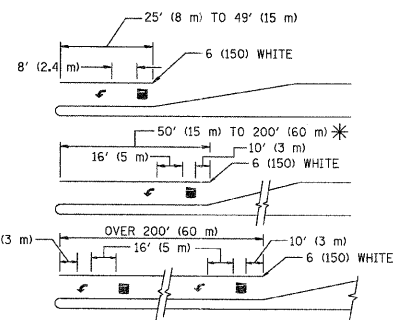


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

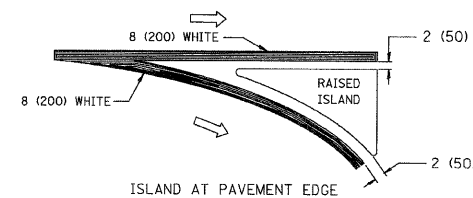
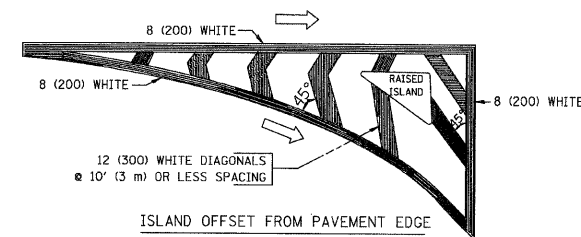
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
\* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL))	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = 11279-DTLS-02 -TC-13

USER NAME = dr-ivakcogn

DESIGNED - EVERS

REVISED - T. RAMMACHER 10-27-94

CHECKED -

DRAWN -

REVISED - C. JUCIUS 09-09-09

PLOT SCALE = 50,000' / IN.

CHECKED - 03-19-90

REVISED -

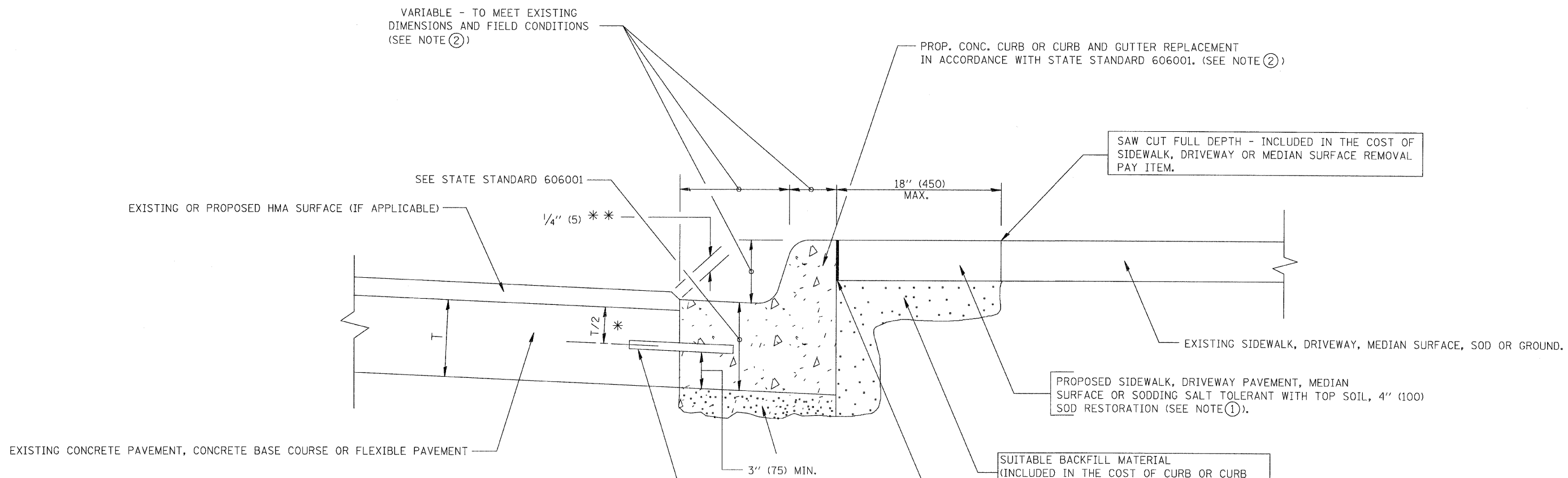
PLOT DATE = 9/9/2009

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	7
TC-13		CONTRACT NO. 63677		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT M-9003(906)		

SCALE: SHEET NO. 7 OF 11 SHEETS STA. N/A TO STA. N/A



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

- SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

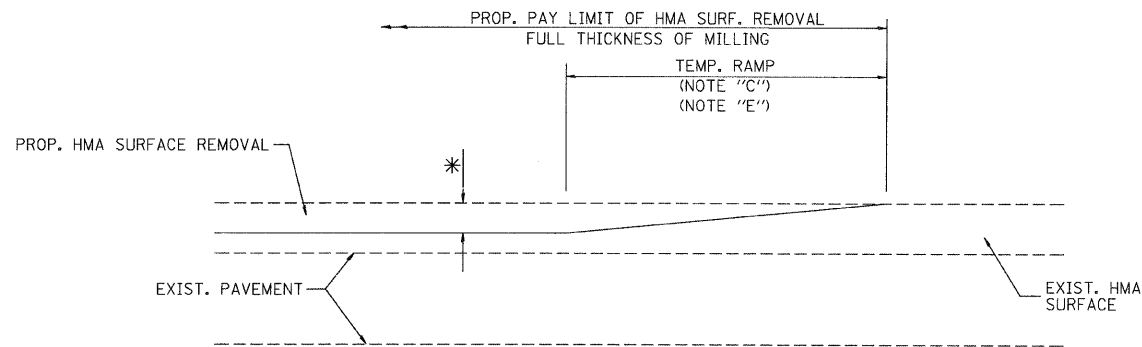
**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

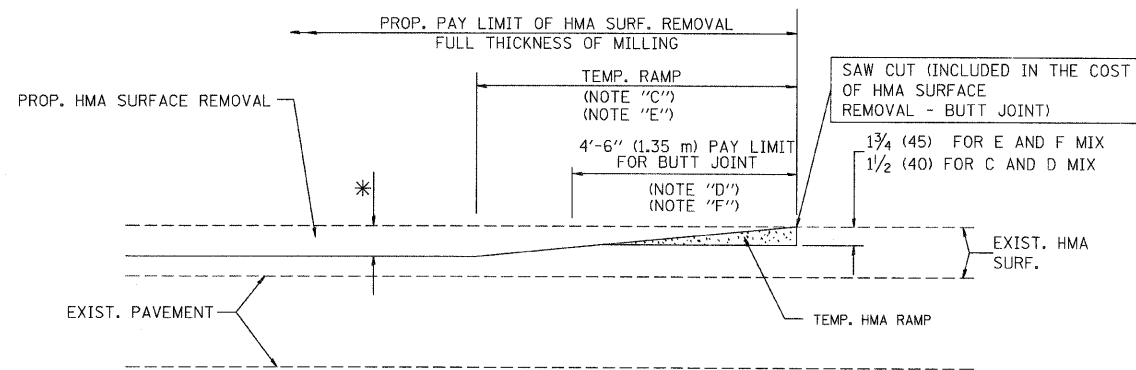
FILE NAME = 11279-DTUS-03 - BD-24	USER NAME = drivekaegn	DESIGNED -- A. HOUSEH	REVISED -- R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 5/8" = 1' IN.	CHECKED --	REVISED -- A. ABBAS 03-21-97			1042	11-00116-00-RS	COOK	11	8
	PLOT DATE = 12/15/2009	DRAWN --	REVISED -- M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		CONTRACT NO. 63677		
		CHECKED -- 03-11-94	REVISED -- R. BORO 12-15-09		SCALE:	SHEET NO. 8	OF 11 SHEETS	STA. N/A	TO STA. N/A	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-9003(908)





MILLED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

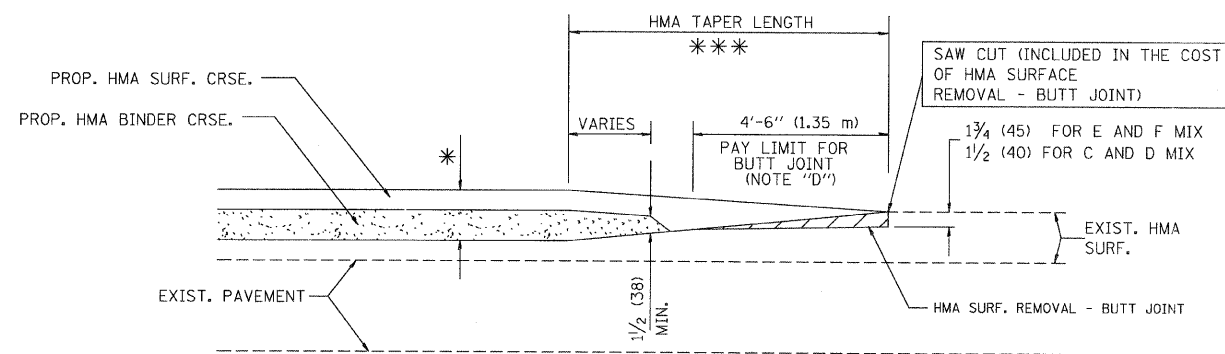
OPTION 1



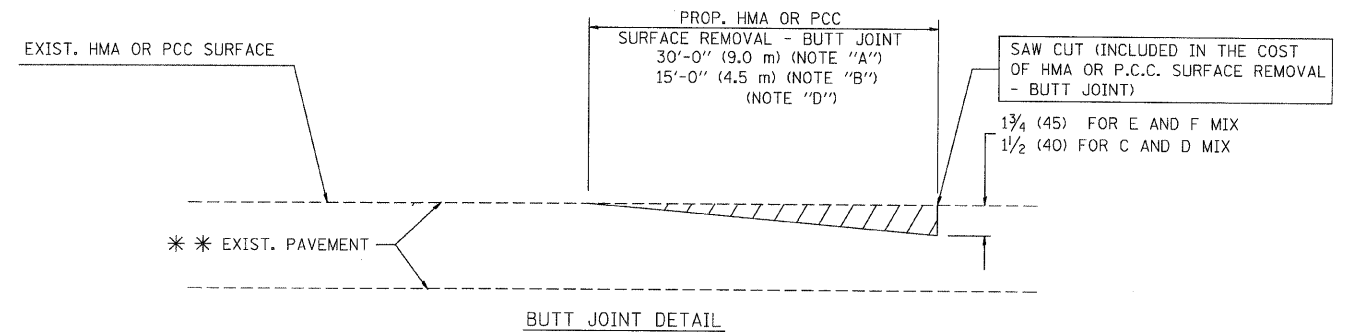
HMA CONSTRUCTED TEMPORARY RAMP  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

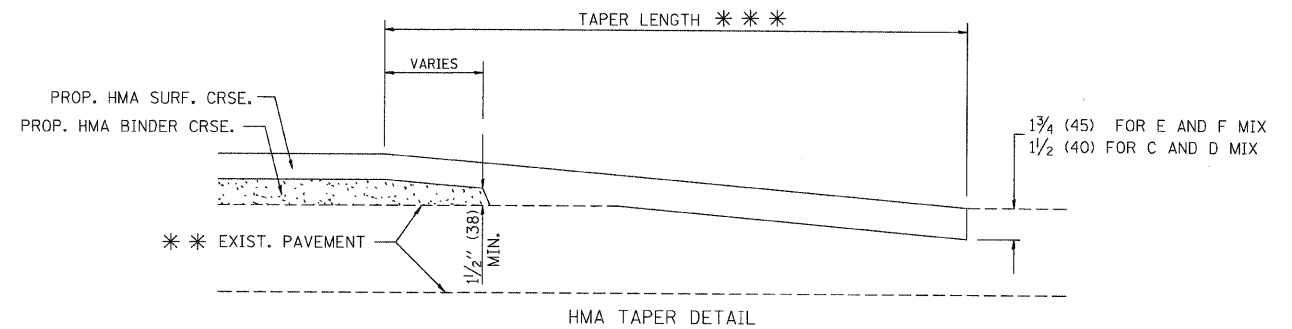
TYPICAL TEMPORARY RAMP



BUTT JOINT AND HMA TAPER  
TYPICAL BUTT JOINT AND HMA TAPER  
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER  
FOR RESURFACING ONLY

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

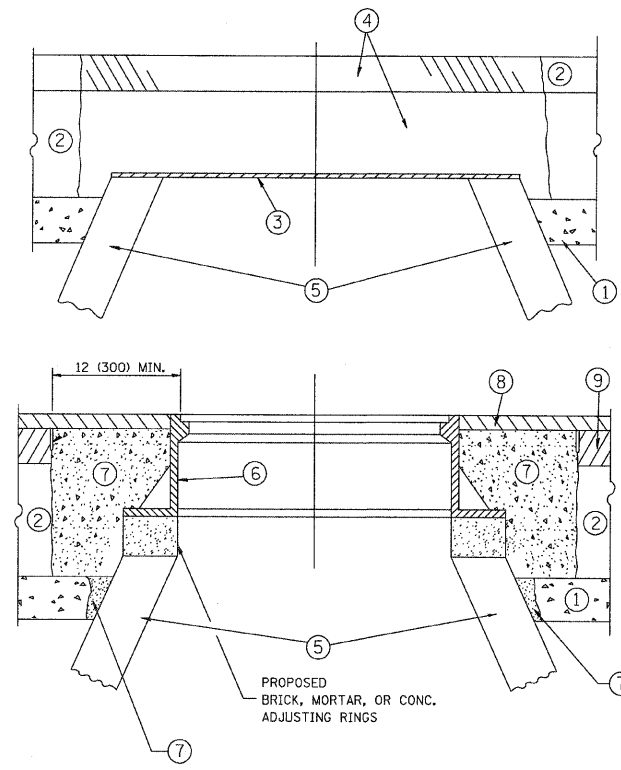
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 11279-DTIS-04 - BD-32	USER NAME = gegl1anobt	DESIGNED -- M. DE YONG	REVISED -- R. SHAH 10-25-94
		CHECKED --	REVISED -- A. ABBAS 03-21-97
	PLOT SCALE = 50,0000 ' / IN.	DRAWN --	REVISED -- M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	CHECKED -- 06-13-90	REVISED -- R. BORO 01-01-07

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		1042	11-00116-00-RS	COOK	11	9
SCALE:		SHEET NO. 9 OF 11 SHEETS		STA. N/A	TO STA. N/A	
		BD400-05 8032		CONTRACT NO. 63677		
		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER.

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

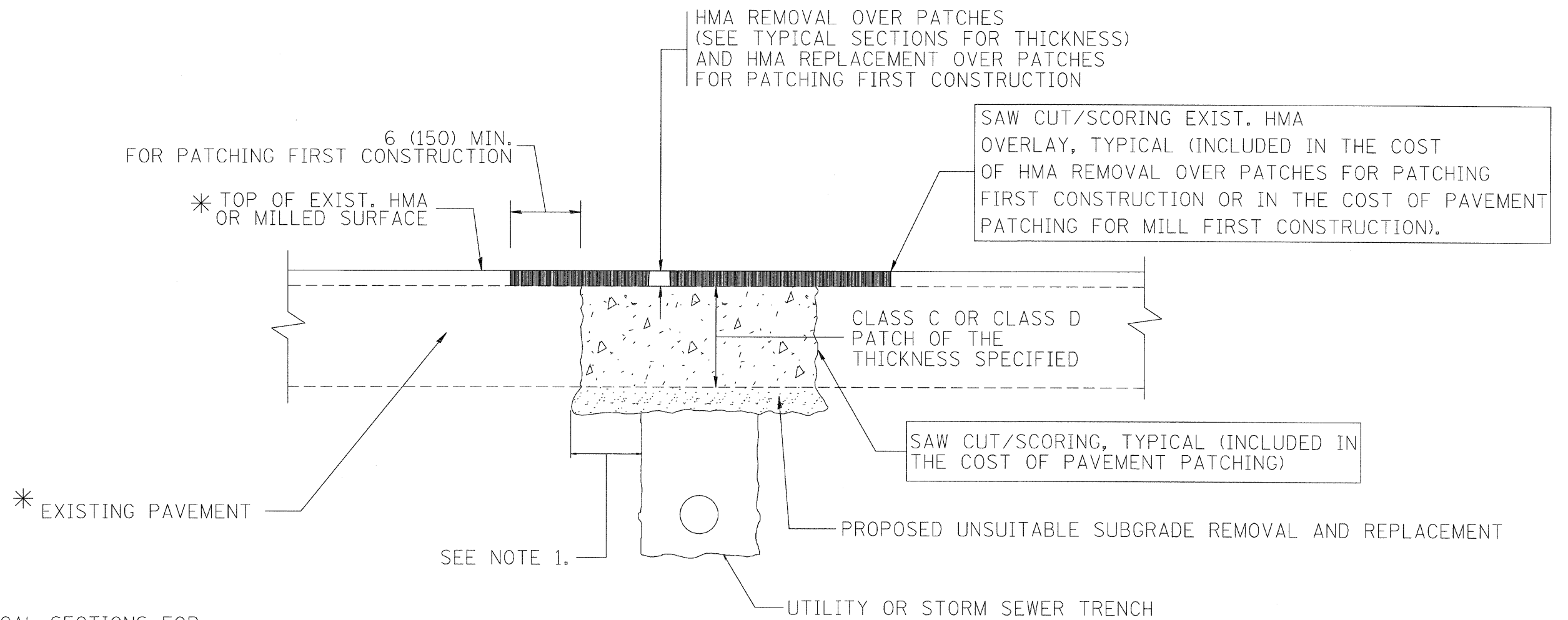
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION			
NAME	DATE	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
R. SHAH	10/25/94	SCALE: VERT. NONE HORIZ. NONE			
R. SHAH	01/30/95				
R. SHAH	03/10/95				
A. ABBAS	03/21/97				
R. WIEDEMAN	05/14/04				
R. BORO	01/01/07				
R. BORO	03/09/11				
R. BORO	12/06/11				
		DRAWN BY			
		CHECKED BY			
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
1042	11-00116-00-RS	COOK	11	10	
		<b>BD600-03 (BD-S)</b>		CONTRACT NO. 63677	
		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)			

FILE NAME = 11279-DTLS-05 - BD-08	USER NAME = goglianobt	DESIGNED — R. SHAH	REVISED —R. SHAH 03-10-95
		CHECKED —	REVISED — A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / 1" IN.	DRAWN —	REVISED —R. WIEDEMAN 05-14-04
	PLOT DATE = 1/4/2008	CHECKED — 10-25-94	REVISED — R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE:	SHEET NO. 10 OF 11 SHEETS	STA. N/A	TO STA. N/A
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\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

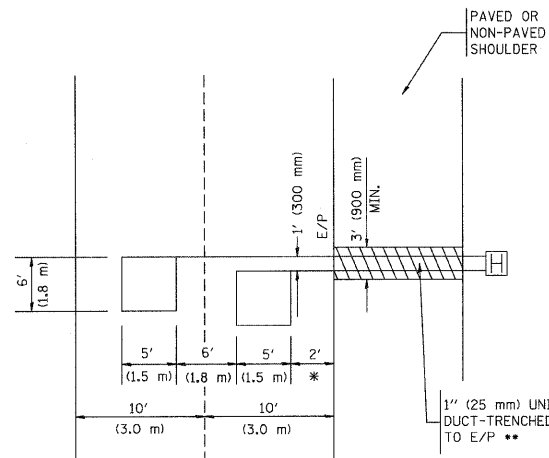
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = 11279-DTLS-06 - BD-22	USER NAME = bauerdl	DESIGNED -- R. SHAH	REVISED -- A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	CHECKED --	REVISOR -- R. BORO 01-01-07				1042	11-00116-00-RS	COOK	11	11	
	PLOT SCALE = 50,000' / IN.	DRAWN --	REVISOR -- R. BORO 09-04-07			<b>BD400-04 (BD-22)</b> CONTRACT NO. 63677					
	PLOT DATE = 10/27/2008	CHECKED -- 10-25-94	REVISOR -- K. ENG 10-27-08			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)					

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



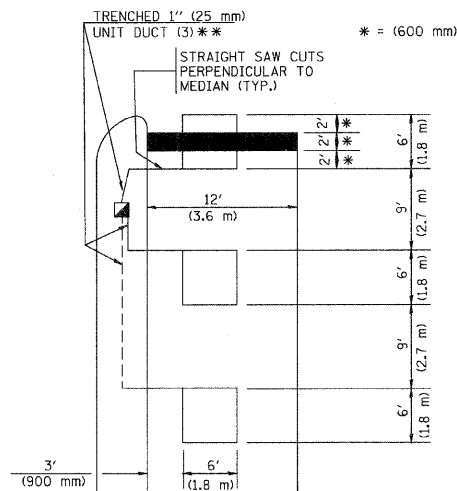
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD B14001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



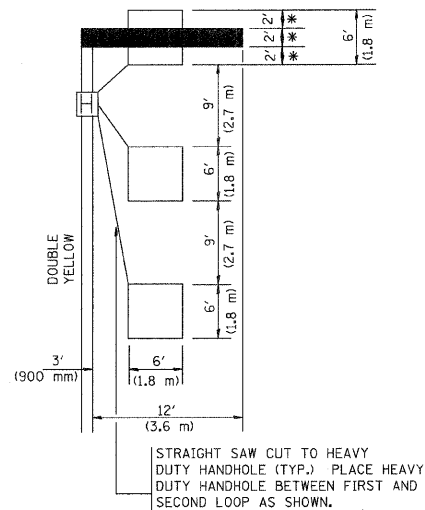
\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

\* = (600 mm)



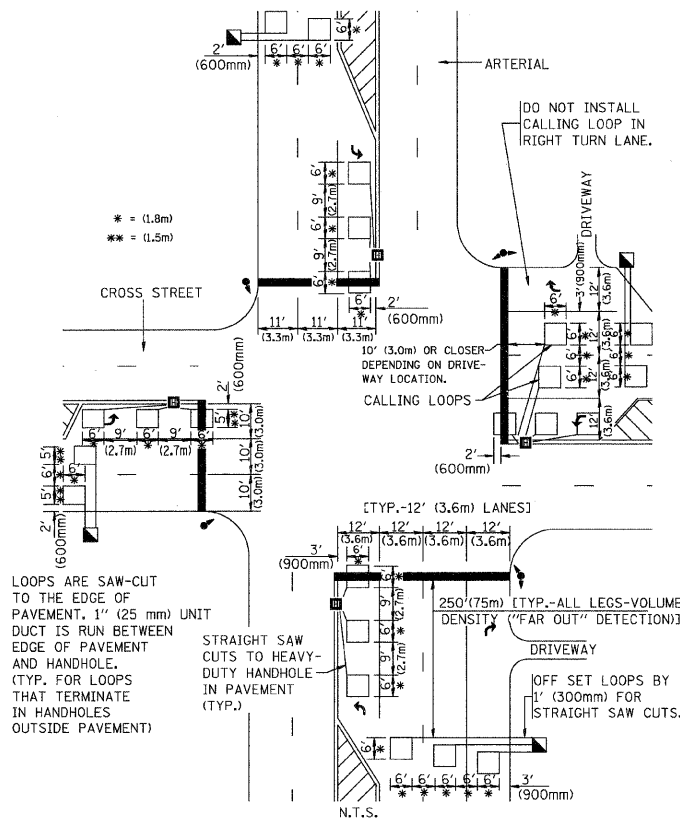
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

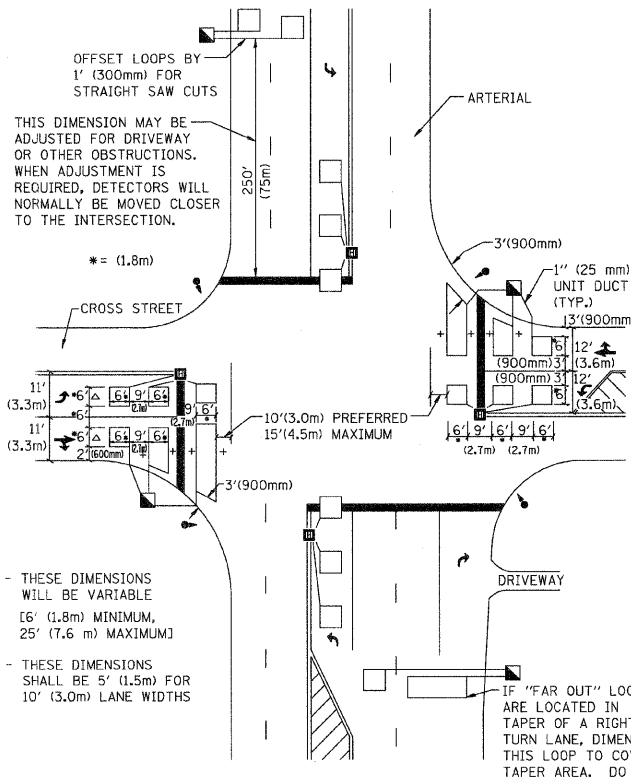
- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2  
N.T.S.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = 11279-DTSL-07-TS-07

USER NAME = gegljanob

DESIGNED —

REVISED —

CHECKED —

DRAWN — R.K.F.

REVISED —

PLOT SCALE = 5/8" = 1' IN.

CHECKED —

REVISED —

PLOT DATE = 1/4/2008

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: SHEET NO. 11A OF 11 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1042	11-00116-00-RS	COOK	11	11A
<b>TS-07</b>			CONTRACT NO. 63677	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-9003(908)				