

INDEX OF SHEETS 04-27-12 LETTING ITEM 035 STATE OF ILLINOIS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	1
		ILLINOIS	CONTRACT NO. 64H72	

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES, UTILITY NOTES & HIGHWAY STANDARDS
3-5	SUMMARY OF QUANTITIES
6-7	TYPICAL SECTIONS
8-10	SCHEDULE OF QUANTITIES
11	CROSSOVER PAVING DETAILS
12-16	TRAFFIC CONTROL PLAN - STAGE 1
17-21	TRAFFIC CONTROL PLAN - STAGE 2
22	TEMPORARY EROSION CONTROL PLAN
23-27	DISTRICT 2 STANDARDS
28-38	BRIDGE PLANS

DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

FAI ROUTE 39 (I-39)  
SECTION (201-1B)M-3  
PROJECT: NHI-039-1(031)116  
WINNEBAGO COUNTY  
BRIDGE & DECK REPAIRS

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

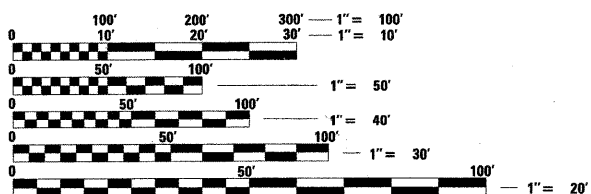
**UTILITY NOTE**

THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTION PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR, REROUTING, DISCONNECTION, PROTECTION, ETC. OF AN UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAIN THE SOLE RESPONSIBILITY OF THE CONTRACTOR



Know what's below.  
Call before you dig.

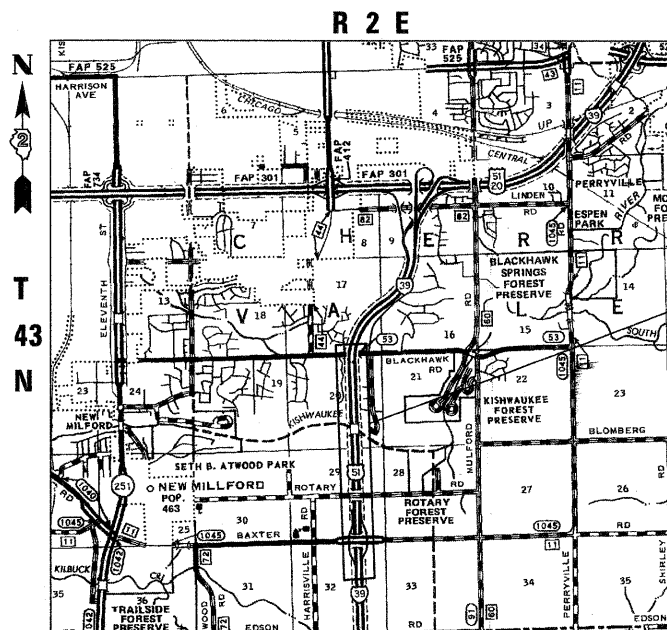
J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

PROJECT ENGINEER - JOHN M. HEYEN, P.E. 217-788-2450  
PROJECT MANAGER - JEFFREY J. TATAREK, P.E., S.E., 217-788-2450  
CONTRACT NO. 64H72

C-92-127-12



**AREA LOCATION PLAN**



GROSS LENGTH = 1350 FT. = 0.256 MILE (NB)  
= 1350 FT. = 0.256 MILE (SB)  
NET LENGTH = 1350 FT. = 0.256 MILE (NB)  
= 1350 FT. = 0.256 MILE (SB)

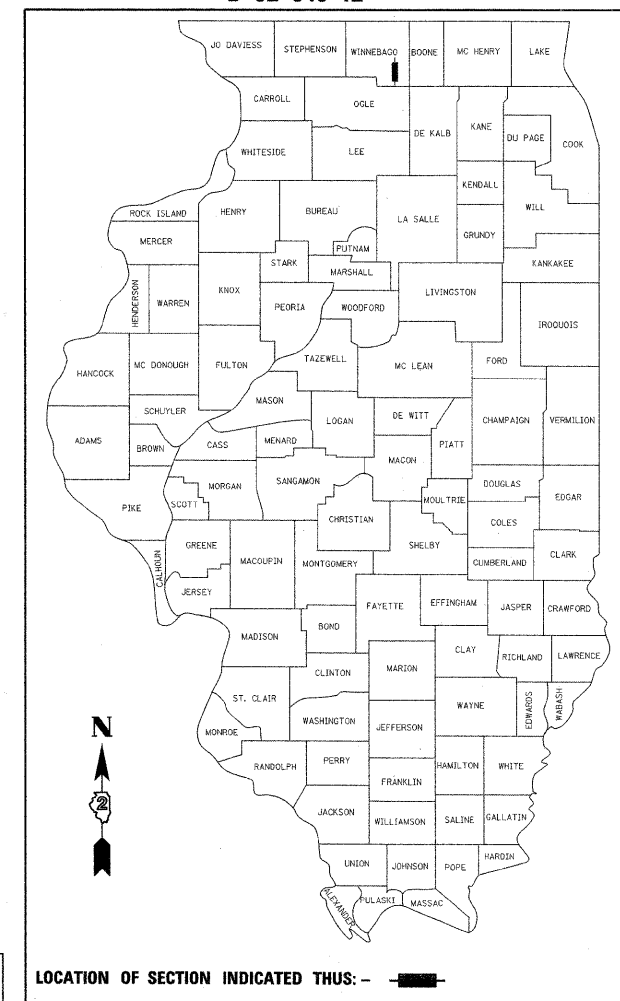
PROJECT LOCATION	SB	NB
IMPROVEMENT BEGINS:	2405 + 00	2397 + 50
SECTION BEGINS:	2418 + 30	2419 + 10
SECTION ENDS:	2431 + 55	2430 + 69
IMPROVEMENT ENDS:	2449 + 00	2447 + 11

SN 101-0133 (SB) STA. 2419 + 60 TO STA. 2431 + 30  
SN 101-0134 (NB) STA. 2418 + 81 TO STA. 2430 + 44

PROFESSIONAL ENGINEER  
JOHN M. HEYEN  
062-068721  
STATE OF ILLINOIS  
SIGNATURE: *John M. Heyen*  
DATE SIGNED: 2/9/12  
LICENSE EXP.: 11/30/2013  
SHEETS 1-22

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D-92-046-12



LOCATION OF SECTION INDICATED THUS: - ■ -

**FUNCTIONAL CLASSIFICATION**  
INTERSTATE  
2009 ADT = 29,100  
P.V. = 68.9% S.U. = 4.3% M.U. = 26.8%  
ROCKFORD & CHERRY VALLEY TOWNSHIPS  
SECTIONS 17, 20, 29, 32

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS  
SUBMITTED Feb 14 2012  
*Eric S. Therkildsen*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
March 23 2012  
*John D. Casanelli, PE*  
acting ENGINEER OF DESIGN AND ENVIRONMENT  
March 23 2012  
*William R. Freese*  
acting DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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**GENERAL NOTES**

- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- ALL AREAS DISTURBED BY THE CONTRACTOR OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE SEEDED AS DIRECTED BY THE ENGINEER, AT THE CONTRACTOR'S EXPENSE.
- IN ADDITION TO THE REQUIREMENTS OF ARTICLE 107.16, THE CONTRACTOR SHALL PROTECT THE SURFACE OF ALL BRIDGE DECKS AND BRIDGE APPROACH PAVEMENTS IN A MANNER SATISFACTORY TO THE ENGINEER BEFORE ANY EQUIPMENT IS ALLOWED TO CROSS THE STRUCTURE. PROTECTION SHALL BE PROVIDED FOR ALL EQUIPMENT, AS DEFINED IN ARTICLE 101.17, REGARDLESS IF TRACK MOUNTED OR WHEELED.
- THESE PLANS HAVE BEEN PREPARED USING STANDARD SYMBOLS AS INDICATED IN THESE PLANS, AND THEY SHALL TAKE PRECEDENCE OVER THOSE SHOWN ON STANDARD 000001 IF THERE IS A CONFLICT.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF EIGHT SAND BAGS PER BARRICADE.
- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR BITUMINOUS SURFACE BEFORE TRAFFIC IS ALLOWED ON THE PAVEMENT.
- FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:  
HOT-MIX ASPHALT - 112 LBS/SQ YD-IN (2.016 TON/CU YD)  
BITUMINOUS MATERIALS (PRIME COAT) - 0.00036 TON/SY (ON PAVEMENT)
- THE THICKNESS OF HOT MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT MIX ASPHALT MIXTURE IS PLACED.
- PRIOR TO PLACEMENT OF THE FINAL PAVEMENT MARKINGS, THE RESIDENT ENGINEER SHOULD CONTACT THE BUREAU OF OPERATIONS AND ARRANGE FOR INSPECTION AND APPROVAL OF THE PAVEMENT MARKING LAYOUT.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF TRAFFIC OPERATIONS 72 HOURS PRIOR TO THE SHIFTING OF THE NORTHBOUND OR SOUTHBOUND LANES.
- THE VERTICAL PANELS SHOWN ON STANDARD 701416 WILL NOT BE REQUIRED ON THE BRIDGE PARAPET. BARRIER WALL REFLECTORS, IN BOTH DIRECTIONS, SHALL BE INSTALLED IN LIEU OF THE VERTICAL PANELS. BARRIER WALL REFLECTORS SHALL ALSO BE INSTALLED ON THE TEMPORARY CONCRETE BARRIER AS SHOWN ON THE TYPICAL SECTIONS. THE PLACEMENT INTERVAL SHALL BE 40 FEET. THESE BARRIER WALL REFLECTORS WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCLUDED WITH THE COST OF "TRAFFIC CONTROL AND PROTECTION, STANDARD 701416."
- ANY TIME THE CONCRETE BARRIER IS NOT IN THE PROPER POSITION, FLAGGERS SHALL BE IN PLACE TO CONTROL TRAFFIC.
- THE LIMITS OF "SHOULDER RUMBLE STRIPS, 16 IN." TO BE CONSTRUCTED IN STAGE 2 SHALL BE IDENTICAL TO THOSE INDICATED FOR HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2".
- ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.
- THE AREA TO BE PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAYS PRODUCTIVITY, BUT NO MORE THAN FIVE DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
- INSTALL RUMBLE STRIPS IN ALL SHOULDERS IN ACCORDANCE WITH STATE STANDARD 642001. RUMBLE STRIPS SHALL BE PLACED ON SHOULDERS ON BOTH SIDES OF THE PAVEMENT.
- THIS WORK SHALL BE DONE IN ACCORDANCE WITH SECTION 704 OF THE STANDARD SPECIFICATIONS. TEMPORARY CONCRETE BARRIER WILL BE MEASURED IN FEET ALONG THE CENTERLINE OF THE BARRIER AND SHALL INCLUDE THE COST OF RENTING/OWNING THE BARRIER FOR THE TIME REQUIRED ON THE JOB PLUS HAULING TO AND FROM THE PROJECT SITE, AS WELL AS ONE PLACEMENT AND REMOVAL FROM THE ROADWAY IN ACCORDANCE WITH SECTION 704 OF THE STANDARD SPECIFICATION. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR TEMPORARY CONCRETE BARRIER.  
  
RELOCATE TEMPORARY CONCRETE BARRIER WILL BE PAID FOR IN FEET ALONG THE CENTERLINE OF THE BARRIER, AND WILL BE PAID FOR EACH TIME THE BARRIER IS REQUIRED BY STAGING TO BE PICKED UP AND MOVED TO A DIFFERENT LOCATION ON THE PROJECT, WHETHER IT IS TO ANOTHER LOCATION ON THE ROADWAY OR TO A STORAGE/STAGING LOCATION FOR THE PROJECT. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR RELOCATE TEMPORARY CONCRETE BARRIER.
- FERTILIZER NUTRIENTS SHALL BE APPLIED AT THE RATE SPECIFIED IN SECTIONS 250 AND 252 OF THE STANDARD SPECIFICATIONS. THIS SHALL BE INCLUDED IN THE COST OF THE SEEDING OR SODDING.

**GENERAL NOTES**

- PREVIOUSLY PUGMILLED STOCKPILES OF "TYPE A" OLDER THAN 1 MONTH WILL NOT BE APPROVED FOR USE UNTIL A MOISTURE CHECK IS RUN TO VERIFY MOISTURE CONTENT. MATERIAL SHIPPED TO PROJECTS WITHOUT BEING TESTED WILL NOT BE ACCEPTED.
- CONNECTING BANDS FOR CORRUGATED METAL PIPES SHALL BE METAL AND SHALL BE COATED WITH THE SAME MATERIAL AS THE PIPE SECTIONS. THE CONNECTING BANDS SHALL BE A MINIMUM OF 18" WIDE.
- CONTRACTOR SHALL LOCATE AERIAL CONDUCTORS TO LIGHTING CONTROLLER AS APPROVED BY THE ENGINEER.
- THE REMOVAL OF TEMPORARY RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE INCLUDED IN THE COST FOR THE PAY ITEM "TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER".
- FOR WORK REQUIRED TO UPGRADE THE CROSSOVER PAVEMENT, NO LANE CLOSURES OR TRAFFIC RESTRICTIONS WILL BE ALLOWED TO OCCUR ON FRIDAYS.
- COMMITMENTS:  
NONE AS OF APRIL 27, 2012

**UTILITY NOTES**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.
- MEMBERS OF JULIE KNOWN TO BE WITHIN OR IMMEDIATELY ADJACENT TO THE LIMITS OF THE IMPROVEMENT ARE:  
  
ELECTRIC:  
COMMONWEALTH EDISON COMPANY  
ATTN: DAVE SCHACHT 630-437-2129  
123 ENERGY AVENUE  
ROCKFORD, IL 61109  
  
CABLE TELEVISION:  
INSIGHT COMMUNICATIONS  
ATTN: MIKE OWENS, DONNA ZIES, TOM YUCCAS  
815-395-8977  
4450 KISHWAUKEE STREET  
ROCKFORD, IL 61109  
  
NATURAL GAS:  
NICOR GAS COMPANY  
ATTN: CONNIE LANE 630-983-8676  
1844 FERRY ROAD  
NAPERVILLE, IL 60563
- NON-MEMBERS OF JULIE KNOWN TO BE WITHIN OR IMMEDIATELY ADJACENT TO THE LIMITS OF THE IMPROVEMENT ARE:  
  
GOVERNMENT:  
IDOT - DISTRICT 2  
ATTN: KYLE LORENZ 815-284-5469  
819 DEPOT AVENUE  
DIXON, IL 61021
- ANY DAMAGE TO THE UNDERGROUND FACILITIES CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS. UTILITY ADJUSTMENTS (IF REQUIRED) SHALL BE MADE BY THE UTILITY COMPANIES UNLESS NOTED OTHERWISE.
- THE LOCATIONS OF EXISTING UTILITIES SHOWN ON THE PLANS ARE BASED ON FIELD INVESTIGATION AND THE BEST INFORMATION AVAILABLE. THE CONTRACTOR SHALL OBTAIN EXACT UTILITY LOCATIONS FROM THE UTILITY COMPANIES AND BY FIELD INSPECTION. THE FOLLOWING STANDARD UTILITY DEPTHS WERE USED IF INFORMATION WAS UNAVAILABLE:  
  
TELEPHONE/FIBER OPTIC - 2 FEET  
GAS - 2.5 FEET  
CABLE TV - 1.5 FEET  
ELECTRIC - 3 FEET  
SANITARY SEWER - 2.5 FEET  
WATER - 4 FEET
- ALL ELECTRIC LINES WILL REMAIN ENERGIZED DURING CONSTRUCTION UNLESS OTHERWISE COORDINATED WITH THE UTILITY COMPANY.

**HIGHWAY STANDARDS**

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-06	TEMPORARY EROSION CONTROL SYSTEMS
420001-07	PAVEMENT JOINTS
420101-04	24' JOINTED PCC PAVEMENT
420106-04	36' JOINTED PCC PAVEMENT
542511-02	INLET BOX TYPE 24 C
630001-10	STEEL PLATE BEAM GUARDRAIL
630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631031-10	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
638001-02	GLARE SCREEN BLADES
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701101-02	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' AWAY
701400-05	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701406-06	LANE CLOSURE, FREEWAY/EXPRESSWAY, DAY OPERATIONS ONLY
701411-08	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701416-07	LANE CLOSURE, FREEWAY/EXPRESSWAY, WITH CROSSOVER AND BARRIER
701426-04	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701901-02	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
780001-03	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
825001-01	LIGHTING CONTROLLER POLE MOUNTED, 240V

**HOT-MIX ASPHALT MIXTURE TABLES**

MIXTURE USE(S):	ROADWAY AND SHOULDER SURFACE
MIXTURE	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90
AC/PG:	PG 64-22
RAP % (MAX.):	0
DESIGN AIR VOIDS:	4.2% @ N90
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL-9.5 OR IL-12.5
FRICTION AGGREGATE:	D
20 YEAR ESAL	N/A
MIXTURE USE(S):	BRIDGE SURFACE
MIXTURE	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105
AC/PG:	SBS PG 70-22
RAP % (MAX.):	N/A
DESIGN AIR VOIDS:	4.0% @ N105
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL-9.5 OR IL-12.5
FRICTION AGGREGATE:	E
20 YEAR ESAL	46.2
MIXTURE USE(S):	CROSSOVER SHOULDER SURFACE
MIXTURE	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50
AC/PG:	PG 64-22
RAP % (MAX.):	10
DESIGN AIR VOIDS:	4.0% @ N50
MIXTURE COMPOSITION: (GRADATION MIXTURE)	IL-9.5 OR IL-12.5
FRICTION AGGREGATE:	C
20 YEAR ESAL	46.2
MIXTURE USE(S):	CROSSOVER SHOULDER
MIXTURE	HOT-MIX ASPHALT SHOULDER
AC/PG:	PG 58-22
RAP % (MAX.):	50
DESIGN AIR VOIDS:	4.0% @ N30
MIXTURE COMPOSITION: (GRADATION MIXTURE)	HMA SHOULDERS
FRICTION AGGREGATE:	NONE
20 YEAR ESAL	46.2

"Work on this project will be in progress at the same time as work on contract 64H84, Micro surfacing application on I-39 from Lee County Line to south of the Baxter Road ramps. The contractors shall inform each other of progress of the projects and give fair warning to the other contractor when a problem might be encountered."

## SUMMARY OF QUANTITIES

CONSTRUCTION CODE
90% FED 10% STATE
0014
URBAN

\*SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
20200100	EARTH EXCAVATION	CU YD	920
20400800	FURNISHED EXCAVATION	CU YD	370
25000210	SEEDING, CLASS 2A	ACRE	1.00
25100115	MULCH, METHOD 2	ACRE	2.00
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	100
28000305	TEMPORARY DITCH CHECKS	FOOT	180
28000400	PERIMETER EROSION BARRIER	FOOT	180
28000500	INLET AND PIPE PROTECTION	EACH	2
31100910	SUBBASE GRANULAR MATERIAL, TYPE A 12"	SQ YD	1571
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	3.9
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	112
40603345	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	TON	1,083
40603575	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N105	TON	1,140
42000501	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	SQ YD	1,382

## SUMMARY OF QUANTITIES

CONSTRUCTION CODE
90% FED 10% STATE
0014
URBAN

\*SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
44000100	PAVEMENT REMOVAL	SQ YD	492
44000155	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	SQ YD	11,048
44004250	PAVED SHOULDER REMOVAL	SQ YD	1,112
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	998
50300255	CONCRETE SUPERSTRUCTURE	CU YD	97.5
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	17,140
50105220	PIPE CULVERT REMOVAL	FOOT	13
542A0229	PIPE CULVERTS, CLASS A, TYPE 1 24"	FOOT	4
542D0217	PIPE CULVERTS, CLASS D, TYPE 1 12"	FOOT	83
54245205	INLET BOX, STANDARD 542511	EACH	1
*63301210	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	250.0
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	11,370
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	8
67100100	MOBILIZATION	L SUM	1

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		CHECKED - JMH	REVISED -
		DATE - 2/9/12	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
I-39 OVER KISHWAUKEE RIVER**

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	3
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				

## SUMMARY OF QUANTITIES

CONSTRUCTION CODE
90% FED 10% STATE
0014
<i>URBAN</i>

\*SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
70100410	TRAFFIC CONTROL AND PROTECTION, STANDARD 701416	EACH	1
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	49,055
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	16,351
70400100	TEMPORARY CONCRETE BARRIER	FOOT	4,525.0
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	4,525.0
70500100	TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	250
70500665	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	1
*72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1
*72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1
*78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	18,091
*78005130	EPOXY PAVEMENT MARKING - LINE 6"	FOOT	1,120
*78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	234

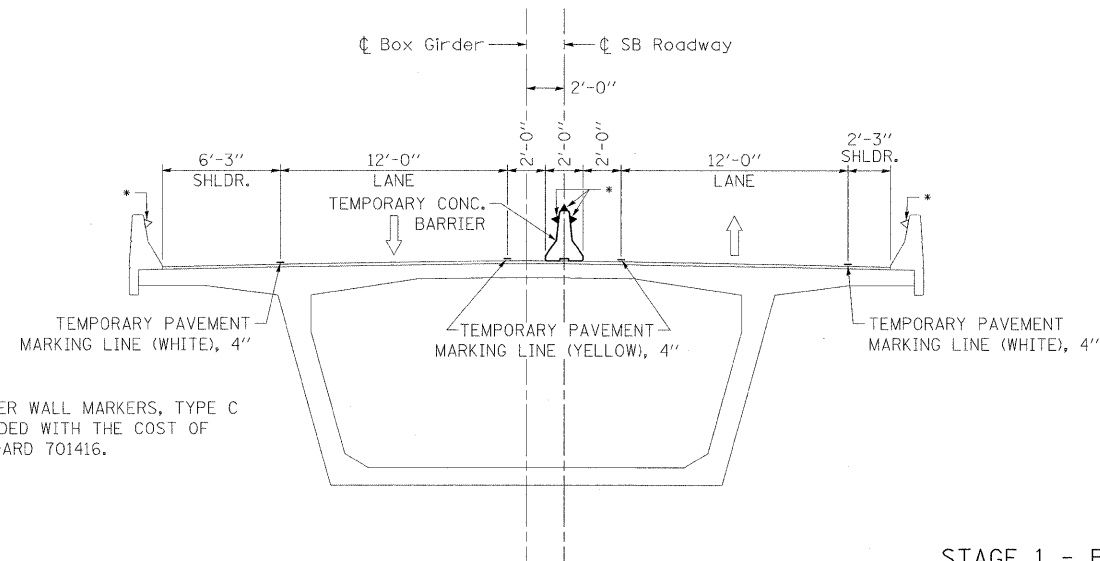
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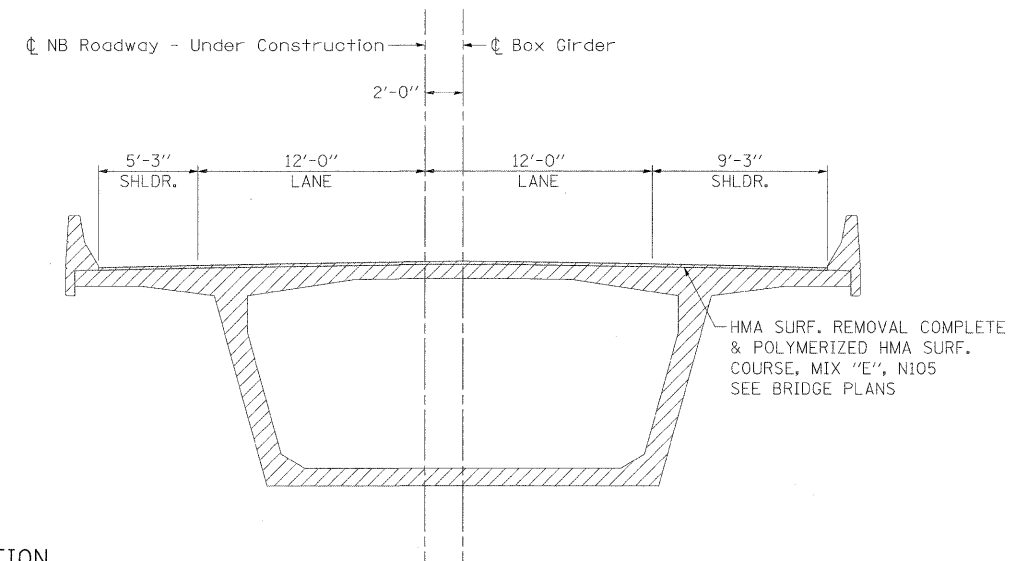
\*SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	29
78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	60
78100200	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	EACH	829
78200410	GUARDRAIL MARKERS, TYPE A	EACH	7
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1
78300100	PAVEMENT MARKING REMOVAL	SQ FT	5,626
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	58
X0322392	BEVELED PIPE AND GUARD	EACH	2
*X0325712	RELOCATE EXISTING ELECTRICAL SYSTEM	L SUM	1
X5810100	WATERPROOFING MEMBRANE SYSTEM (SPECIAL)	SQ YD	9,692
*X6330075	RELOCATE TRAFFIC BARRIER TERMINAL, (TEMPORARY)	EACH	2
X6380205	TEMPORARY MODULAR GLARE SCREEN	FOOT	450
*X7050169	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (FLARED)	EACH	1
*X8410102	TEMPORARY LIGHTING SYSTEM	L SUM	1



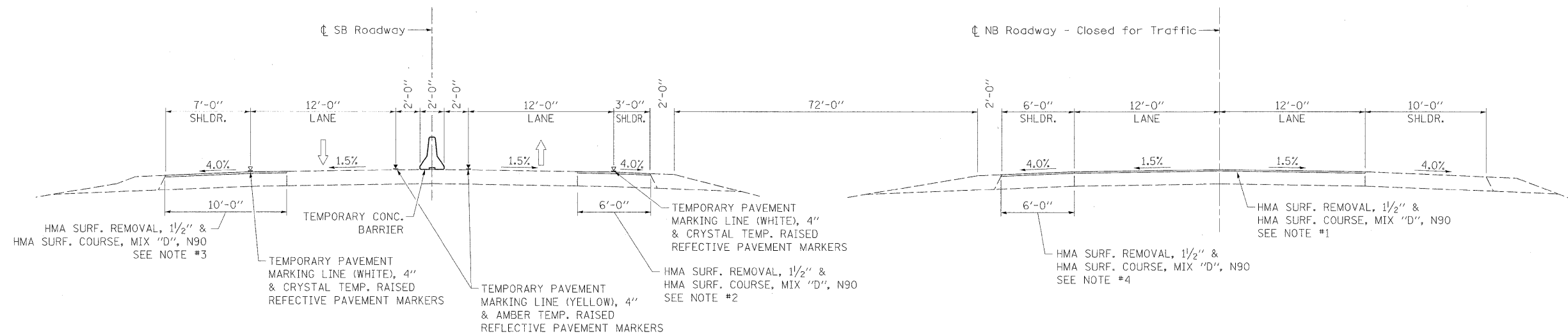


\*BARRIER WALL MARKERS, TYPE C INCLUDED WITH THE COST OF STANDARD 701416.



STAGE 1 - BRIDGE TYPICAL SECTION

Construction in this Stage



STAGE 1 - ROADWAY TYPICAL SECTION

**NOTE:**

SEE CROSSOVER PAVING PLAN SHEET FOR WIDENING DETAILS THRU THE CROSSOVERS. STA. 2405+33.9 TO STA. 2409+92.7

**NOTE #1**

STA. 2430+44.0 TO STA. 2430+69.0

**NOTE #2**

STA. 2409+92.7 TO STA. 2419+60.0  
STA. 2431+30.0 TO STA. 2438+07.4  
STA. 2440+32.3 TO STA. 2442+00.0

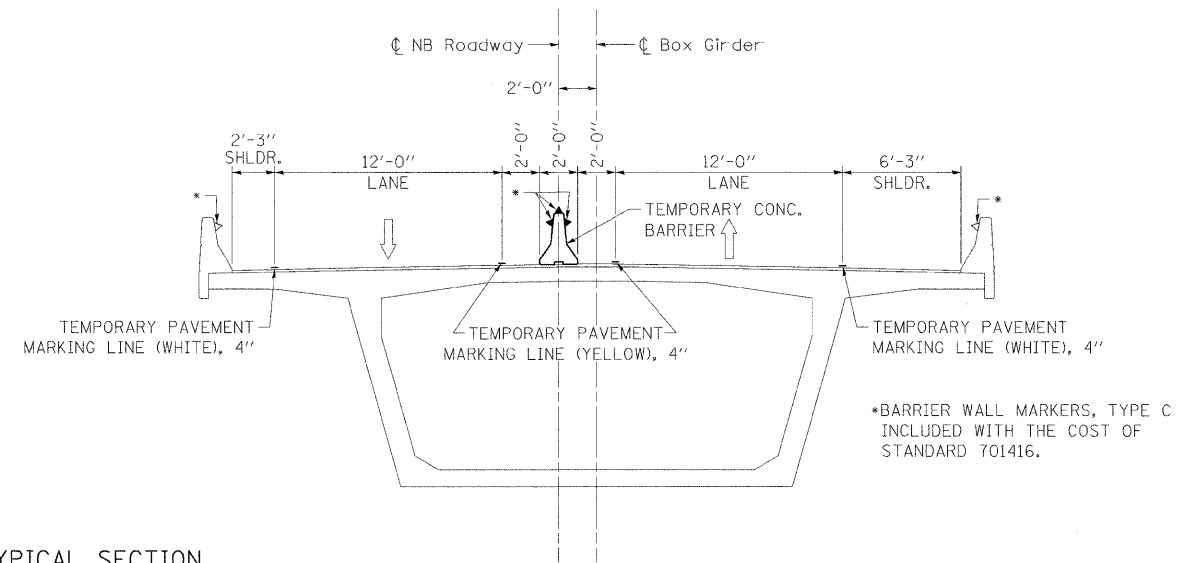
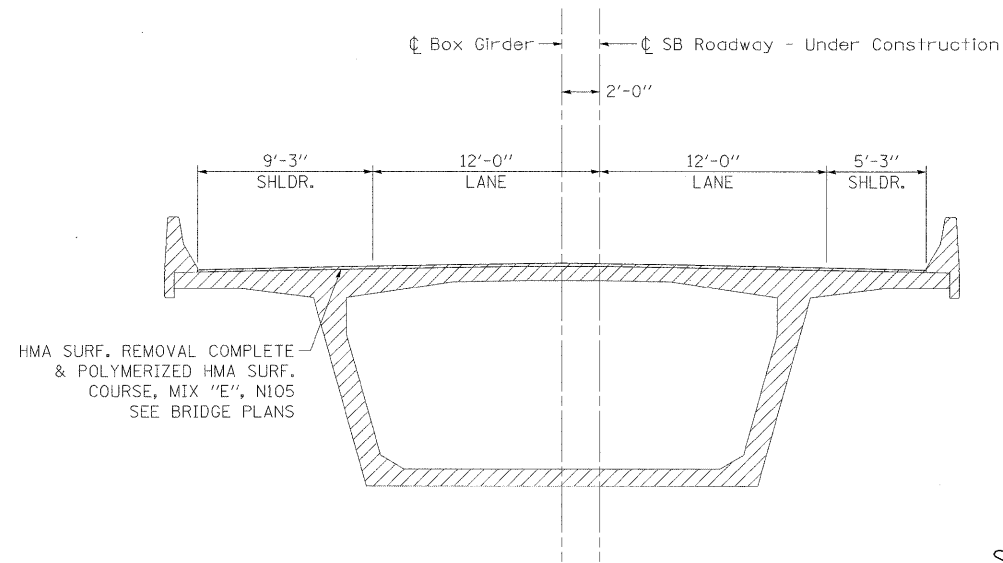
**NOTE #3**

STA. 2405+00.0 TO STA. 2419+60.0  
STA. 2431+30.0 TO STA. 2449+00.0

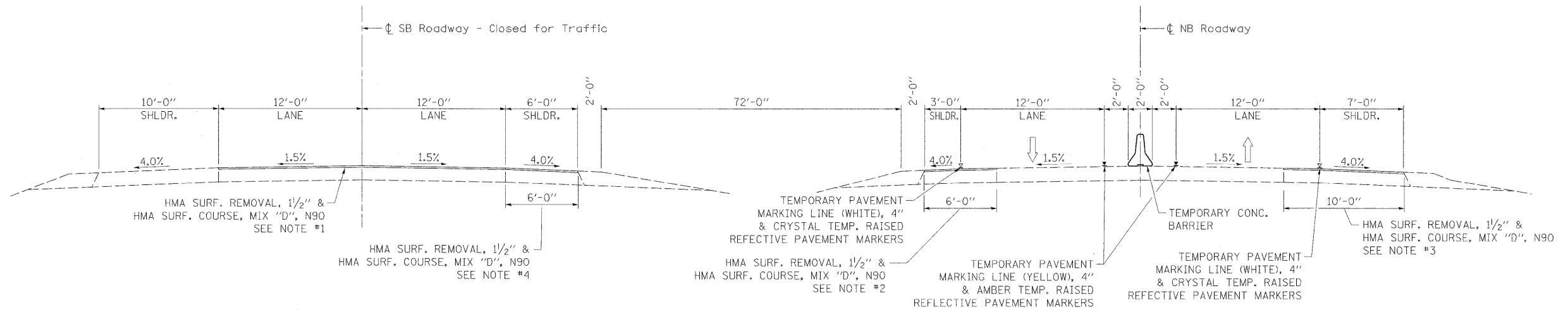
**NOTE #4**

STA. 2443+00.0 TO STA. 2447+11.0

FILE NAME =	USER NAME = pearc00397	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS - STAGE 1 CONSTRUCTION I-39 OVER KISHWAUKEE RIVER</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca:\prjse_work\do_not_delete\dms59254\0254472-svt-typical01.dgn		DRAWN - BJB	REVISED -		39	(201-1B)M-3	WINNEBAGO	38	6			
PLOT SCALE = 5.0000' / 1in.		CHECKED - JMH	REVISED -		CONTRACT NO. 64H72							
PLOT DATE = 02/09/2012		DATE - 2/9/12	REVISED -		ILLINOIS FED. AID PROJECT							



STAGE 2 - BRIDGE TYPICAL SECTION



STAGE 2 - ROADWAY TYPICAL SECTION

Construction in this Stage

NOTE #1

STA. 2418+30.0 TO STA. 2419+60.0  
STA. 2431+30.0 TO STA. 2431+55.0

NOTE #2

STA. 2409+92.7 TO STA. 2418+81.0  
STA. 2430+44.0 TO STA. 2438+07.4  
STA. 2440+32.3 TO STA. 2443+00.0

NOTE #3

STA. 2397+50.0 TO STA. 2418+81.0  
STA. 2430+44.0 TO STA. 2444+00.0

NOTE #4

STA. 2442+00.0 TO STA. 2447+11.0

FILE NAME =	USER NAME = pearc00397	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SECTIONS - STAGE 2 CONSTRUCTION I-39 OVER KISHWAUKEE RIVER</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cr:\pwise_work\do_not_delete\des59254\0264472-sh1-typical01.dgn		DRAWN - BJB	REVISED -		39	(201-1B)M-3	WINNEBAGO	38	7			
PLOT SCALE = 5.0000' / 1" =		CHECKED - JMH	REVISED -		CONTRACT NO. 64H72							
PLOT DATE = 02/09/2012		DATE - 2/9/12	REVISED -		SCALE:	SHEET 2 OF 2 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



EARTHWORK SCHEDULE					
LOCATION	20200100				20400800
	A	B	C	D	
	EARTH EXCAVATION	EXCAVATION TO BE USED IN EMBANKMENT, ADJUSTED FOR SHRINKAGE (25%)	EMBANKMENT	EARTHWORK BALANCE EXCESS (+) OR SHORTAGE (-)	
BEGIN STATION	END STATION	CU YD	CU YD	CU YD	CU YD
<b>STAGE 1</b>					
2406+00.00	2409+92.70	460	345	717	-370
2438+07.40	2442+00.00	460	345	717	-370
<b>TOTAL</b>		<b>920</b>	<b>345</b>	<b>717</b>	<b>-370</b>

EQUATIONS USED  
 B = A \* 0.75  
 D = B - C

SEEDING AND EROSION CONTROL SCHEDULE							
LOCATION	25000210	25100115	28000250	28000305	28000400	28000500	NOTE
	SEEDING, CLASS 2A	MULCH, METHOD 2	TEMPORARY EROSION CONTROL SEEDING	TEMPORARY DITCH CHECKS	PERIMETER EROSION BARRIER	INLET AND PIPE PROTECTION	
	ACRE	ACRE	POUND	FOOT	FOOT	EACH	
<b>STAGE 1</b>							
2406+00.00	2409+92.70	0.50	1.00	50			
2438+07.40	2442+00.00	0.50	1.00	50			
2408+00.00	2411+00.00				90		150' Spacing
2437+00.00	2440+00.00				90		150' Spacing
2410+00.00	2410+34.00					46	Crossover Right
2410+00.00	2410+34.00					44	Crossover Left
2437+66.00	2438+00.00					46	Crossover Right
2437+66.00	2438+00.00					44	Crossover Left
2406+36.00						1	
2442+10.00						1	
<b>TOTAL</b>		<b>1.00</b>	<b>2.00</b>	<b>100</b>	<b>180</b>	<b>180</b>	<b>2</b>

DRAINAGE SCHEDULE					
LOCATION	50105220	542A0229	542D0217	54245205	X0322392
	PIPE CULVERT REMOVAL	PIPE CULVERTS, CLASS A, TYPE 1 24"	PIPE CULVERTS, CLASS D, TYPE 1 12"	INLET BOX, STANDARD 542511	BEVELED PIPE AND GUARD
	FOOT	FOOT	FOOT	EACH	EACH
<b>STAGE 1</b>					
2406+50.00	Crossover			53	1
2441+50.00	Crossover			30	1
2408+00.00	RT	13	4	1	
<b>TOTAL</b>		<b>13</b>	<b>4</b>	<b>83</b>	<b>2</b>

REMOVAL SCHEDULE				
LOCATION	44000100	44004250	Z0042505	NOTE
	PAVEMENT REMOVAL	PAVED SHOULDER REMOVAL	POST REMOVAL AND REPLACEMENT	
	SQ YD	SQ YD	EACH	
<b>STAGE 1</b>				
2404+30.00	2404+85.00		4	Crossover
2405+15.00	2405+70.00		4	Crossover
2442+30.00	2442+85.00		4	Crossover
2443+15.00	2443+70.00		4	Crossover
2405+34.00	2409+92.00	535		Crossover Right
2438+07.00	2438+89.00	54		Crossover Left
2438+07.00	2438+89.00	54		Crossover Right
2438+89.00	2442+66.00	469		Crossover Left
2407+38.00	2409+93.00	150		Crossover Right
2407+38.00	2409+94.00	150		Crossover Left
2438+89.00	2440+33.00	96		Crossover Right
2438+89.00	2440+34.00	96		Crossover Left
<b>TOTALS</b>		<b>492</b>	<b>1,112</b>	<b>16</b>

PAVEMENT SCHEDULE									
LOCATION	31100910	40300200	40603310	40,603,345	42000501	44000155	48203021	64200116	NOTE
	SUBBASE GRANULAR MATERIAL, TYPE A 12"	BITUMINOUS MATERIALS (PRIME COAT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90	PORTLAND CEMENT CONCRETE PAVEMENT 10" (JOINTED)	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2"	HOT-MIX ASPHALT SHOULDERS, 6"	SHOULDER RUMBLE STRIPS, 16 INCH	
	SQ YD	TON	TON	TON	SQ YD	SQ YD	SQ YD	FOOT	
<b>STAGE 1</b>									
2405+00.00	2419+60.00		0.58		159	1,622			SB outside shld
2409+92.70	2419+60.00		0.23		63	645			SB inside shld
2430+44.00	2430+69.00		0.02		7	67			NB driving lanes
2431+30.00	2449+00.00		0.71		193	1,967			SB outside shld
2431+30.00	2438+07.40		0.16		44	452			SB inside shld
2440+32.30	2442+00.00		0.04		11	112			SB inside shld
2443+00.00	2447+11.00		0.10		27	274			NB inside shld
2405+34.00	2409+93.00	786							Crossover
2438+07.00	2440+34.00	785							Crossover
2405+67.00	2409+93.00			56					Crossover
2438+07.00	2442+33.00			56					Crossover
2405+67.00	2409+93.00							499	Crossover
2438+07.00	2442+33.00							499	Crossover
2407+38.00	2409+93.00				346				Crossover Right
2407+38.00	2409+94.00				346				Crossover Left
2438+89.00	2440+33.00				345				Crossover Right
2438+89.00	2440+34.00				345				Crossover Left
<b>SUBTOTAL</b>		<b>1,571</b>	<b>1.84</b>	<b>112</b>	<b>504</b>	<b>1,382</b>	<b>5,139</b>	<b>998</b>	<b>0</b>
<b>STAGE 2</b>									
2397+50.00	2418+81.00		0.85		232	2,368		2,131	NB outside shld
2409+92.70	2418+81.00		0.21		58	592		888	NB inside shld
2418+30.00	2419+60.00		0.12		34	347			SB driving lanes
2431+30.00	2431+55.00		0.02		7	67			SB driving lanes
2430+44.00	2438+07.40		0.18		50	509		763	NB inside shld
2430+44.00	2444+00.00		0.54		148	1,507		1,356	NB outside shld
2440+32.30	2443+00.00		0.06		17	178			NB inside shld
2442+00.00	2447+11.00		0.12		33	341			SB inside shld
2405+00.00	2419+60.00							1,460	SB outside shld
2409+92.70	2419+60.00							967	SB inside shld
2431+30.00	2438+07.40							677	SB inside shld
2431+30.00	2449+00.00							1,770	SB outside shld
2440+32.30	2447+11.00							679	NB inside shld
2440+32.30	2447+11.00							679	SB inside shld
<b>SUBTOTAL</b>		<b>0</b>	<b>2.10</b>	<b>0.0</b>	<b>579</b>	<b>0</b>	<b>5,909</b>	<b>0</b>	<b>11,370</b>
<b>TOTALS</b>		<b>1,571</b>	<b>3.94</b>	<b>112.0</b>	<b>1,083</b>	<b>1,382</b>	<b>11,048</b>	<b>998</b>	<b>11,370</b>

GUARDRAIL AND BARRIER SCHEDULE											
LOCATION	63301210	70400100	70400200	70500100	70500665	78200410	78201000	X6330075	X6380205	X7050169	NOTE
	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	TEMPORARY CONCRETE BARRIER	RELOCATE TEMPORARY CONCRETE BARRIER	TEMPORARY STEEL PLATE BEAM GUARDRAIL, TYPE A	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 6	GUARDRAIL MARKERS, TYPE A	TERMINAL MARKER - DIRECT APPLIED	RELOCATE TRAFFIC BARRIER TERMINAL (TEMPORARY)	TEMPORARY MODULAR GLARE SCREEN	TEMPORARY TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (FLARED)	
	FOOT	FOOT	FOOT	FOOT	EACH	EACH	EACH	EACH	FOOT	EACH	
<b>STAGE 1</b>											
2416+66.85	2419+16.85			250							
2401+13.00	2407+00.00		587.5								SB Lane Taper
2407+00.00	2446+30.00		3,937.5								SB Lane thru cross
2419+16.85	2419+60.00				1	7	1				SB lanes
2406+00.00	2410+50.00							450			SB lanes (NB lanes)
2416+16.85	2416+66.85									1	SB lanes
<b>SUBTOTAL</b>		<b>0</b>	<b>4,525.0</b>	<b>0.0</b>	<b>250</b>	<b>1</b>	<b>7</b>	<b>1</b>	<b>0</b>	<b>450</b>	<b>1</b>
<b>STAGE 2</b>											
2430+87.15	2433+37.15	250									
2401+07.50	2441+12.50			4012.5							NB lane thru cross
2441+12.50	2446+24.70			512.5							NB lane taper
2433+37.15	2433+87.15							1			Type 1 (Special)
2430+43.40	2430+87.15							1			Type 6
<b>SUBTOTAL</b>		<b>250</b>	<b>0.0</b>	<b>4,525.0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>
<b>TOTALS</b>		<b>250</b>	<b>4,525.0</b>	<b>4,525.0</b>	<b>250</b>	<b>1</b>	<b>7</b>	<b>1</b>	<b>2</b>	<b>450</b>	<b>1</b>

FILE NAME: c:\pwise\work\do\_no\_delete\dms59254\0264472-shl\tr\schedule01.dgn  
 USER NAME: brenn00248  
 DESIGNED: BJB  
 DRAWN: BJB  
 CHECKED: JMH  
 DATE: 2/9/12  
 REVISIONS:  
 REVISIONS:  
 REVISIONS:  
 REVISIONS:

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES  
 I-39 OVER KISHWAUKEE RIVER  
 SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE. 39  
 SECTION (201-1B)M-3  
 COUNTY WINNEBAGO  
 TOTAL SHEETS 38  
 SHEET NO. 8  
 CONTRACT NO. 64H72  
 ILLINOIS FED. AID PROJECT



PAVEMENT MARKING SCHEDULE														
LOCATION		70300220	70301000	78005110	78005130	78005140	78100100	78100105	78100200	78300100	78300200	COLOR	NOTE	LOCATION
		TEMPORARY PAVEMENT MARKING - LINE 4"	WORK ZONE PAVEMENT MARKING REMOVAL	EPOXY PAVEMENT MARKING - LINE 4"	EPOXY PAVEMENT MARKING - LINE 6"	EPOXY PAVEMENT MARKING - LINE 8"	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL			
BEGIN STATION	END STATION	FOOT	SQ FT	FOOT	FOOT	FOOT	EACH	EACH	EACH	SQ FT	EACH			
<b>STAGE 1</b>														
2361+00.00	2381+00.00	2000	667									White	Solid edge line	NB lane on RT
16+43.50	20+37.70	394	131									Yellow	Solid edge line	Baxter Rd on-ramp, LT
16+43.50	24+87.60	844	281									White	Solid edge line	Baxter Rd on-ramp, RT
2385+50.00	2444+87.00	5954	1985									White	Solid edge line	NB lane on RT thru crossover
2401+00.00	2447+00.00	4615	1538									Yellow	Solid edge line	NB lane on LT thru crossover
2405+35.00	2448+62.50	4328	1443									White	Solid edge line	SB lane on RT
2407+00.00	2473+50.00	6661	2220									Yellow	Solid edge line	SB lane on LT
2361+00.00	2371+00.00									83				NB center skip
2377+06.00	2378+23.00									78				NB gore
16+43.50	17+60.20									78				On ramp gore
2401+00.00	2404+00.00									100				NB edge LT
2443+00.00	2447+00.00									133				NB edge LT
2405+35.00	2448+62.50									1443				SB edge RT
2405+00.00	2442+00.00									1233				SB edge LT
2463+50.00	2473+50.00									83				SB center skip
2361+00.00	2371+00.00										13			NB center skip
2418+81.00	2430+69.00										15			NB bridge & approach
2463+50.00	2473+50.00										13			SB center skip
2361+00.00	2380+00.00								48					NB RT
2380+00.00	2381+00.00								3					NB RT
2385+50.00	2401+00.00								39					NB RT
16+43.50	19+37.20								7					Ent Ramp Lt
16+43.50	19+37.20								7					Ent Ramp Rt
19+37.20	20+38.00								3					Ent Ramp Lt
19+37.20	24+88.00								14					Ent Ramp Rt
2401+00.00	2409+00.00								40					NB LT
2401+00.00	2409+38.20								42					NB RT
2409+38.20	2410+00.00								2					NB RT
2405+35.00	2410+00.00								12					SB RT
2410+00.00	2416+54.40								16					NB RT
2410+00.00	2417+77.20								19					SB RT
2433+05.50	2438+61.90								14					NB RT
2433+35.20	2440+00.00								17					SB RT
2438+61.90	2440+00.00								7					NB RT
2440+00.00	2470+00.00								75					SB LT
2440+00.00	2448+62.50								22					SB RT
2440+00.00	2444+87.00								24					NB RT
<b>SUBTOTAL</b>		<b>24796</b>	<b>8265</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>411</b>	<b>3231</b>	<b>41</b>			

PAVEMENT MARKING SCHEDULE													
LOCATION		70300220	70301000	78005110	78005130	78005140	78100100	78100105	78100200	78300100	78300200		
		TEMPORARY PAVEMENT MARKING - LINE 4"	WORK ZONE PAVEMENT MARKING REMOVAL	EPOXY PAVEMENT MARKING - LINE 4"	EPOXY PAVEMENT MARKING - LINE 6"	EPOXY PAVEMENT MARKING - LINE 8"	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	TEMPORARY RAISED REFLECTIVE PAVEMENT MARKER	PAVEMENT MARKING REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	COLOR	NOTE
BEGIN STATION	END STATION	FOOT	SQ FT	FOOT	FOOT	FOOT	EACH	EACH	EACH	SQ FT	EACH		
<b>STAGE 2</b>													
2361+00.00	2441+12.50	8013	2671									Yellow	Solid edge line NB lane on LT
2398+00.00	2443+62.50	4563	1521									White	Solid edge line NB lane on RT
2401+00.00	2447+00.00	4615	1538									Yellow	Solid edge line SB lane on LT thru crossover
2403+13.00	2473+50.00	7068	2356									White	Solid edge line SB lane on RT thru crossover
2398+00.00	2443+62.50			4563								White	NB outside edge
2401+00.00	2447+00.00			4600								Yellow	NB inside edge
2405+35.00	2448+62.50			4328								White	SB outside edge
2401+00.00	2447+00.00			4600								Yellow	SB inside edge
2361+00.00	2371+00.00				250							White	NB center skip
2419+11.00	2430+70.00				290							White	NB center skip
2418+30.00	2431+55.00				330							White	SB center skip
2463+50.00	2473+50.00				250							White	SB center skip
2377+06.00	2378+23.00					117						White	NB gore edge
16+43.50	17+60.20					117						White	Baster Ramp gore edge
2361+00.00	2371+00.00						13						NB lanes
2430+44.00	2430+69.00						1						NB lanes
2418+30.00	2419+60.00						2						SB lanes
2463+50.00	2473+50.00						13						SB lanes
2418+81.00	2430+44.00							30					NB Bridge
2419+60.00	2431+30.00							30					SB Bridge
2398+00.00	2419+11.00								704				NB edge RT
2430+01.00	2443+62.50								454				NB edge RT
2404+00.00	2419+11.00								504				NB edge LT
2430+01.00	2443+00.00								433				NB edge LT
2401+00.00	2405+00.00								133				SB edge LT
2442+00.00	2447+00.00								167				SB edge LT
2418+30.00	2431+55.00									17			SB bridge & approach
2361+00.00	2380+00.00							48					NB LT
2380+00.00	2410+00.00							75					NB LT
2398+00.00	2410+00.00							30					NB RT
2403+13.00	2409+38.20							31					SB RT
2409+38.20	2410+00.00							2					SB RT
2410+00.00	2415+97.70							15					NB RT
2410+00.00	2418+81.00							22					SB RT
2430+44.00	2440+00.00							24					NB RT
2433+37.20	2438+61.90							13					SB RT
2439+00.00	2440+00.00							5					SB LT
2438+61.90	2440+00.00							7					SB RT
2440+00.00	2443+62.50							9					NB RT
2440+00.00	2447+00.00							35					SB LT
2440+00.00	2447+00.00							35					SB RT
2447+00.00	2470+00.00							58					SB RT
2470+00.00	2473+50.00							9					SB RT
<b>SUBTOTAL</b>		<b>24259</b>	<b>8086</b>	<b>18091</b>	<b>1120</b>	<b>234</b>	<b>29</b>	<b>60</b>	<b>418</b>	<b>2395</b>	<b>17</b>		
<b>TOTALS</b>		<b>49055</b>	<b>16351</b>	<b>18091</b>	<b>1120</b>	<b>234</b>	<b>29</b>	<b>60</b>	<b>829</b>	<b>5626</b>	<b>58</b>		

FILE NAME =  
 USER NAME = brenn00248  
 DESIGNED - BJB  
 DRAWN - BJB  
 CHECKED - JMH  
 DATE - 2/9/12  
 PLOT SCALE = 1/8" = 1' / in.  
 PLOT DATE = 02/18/2012

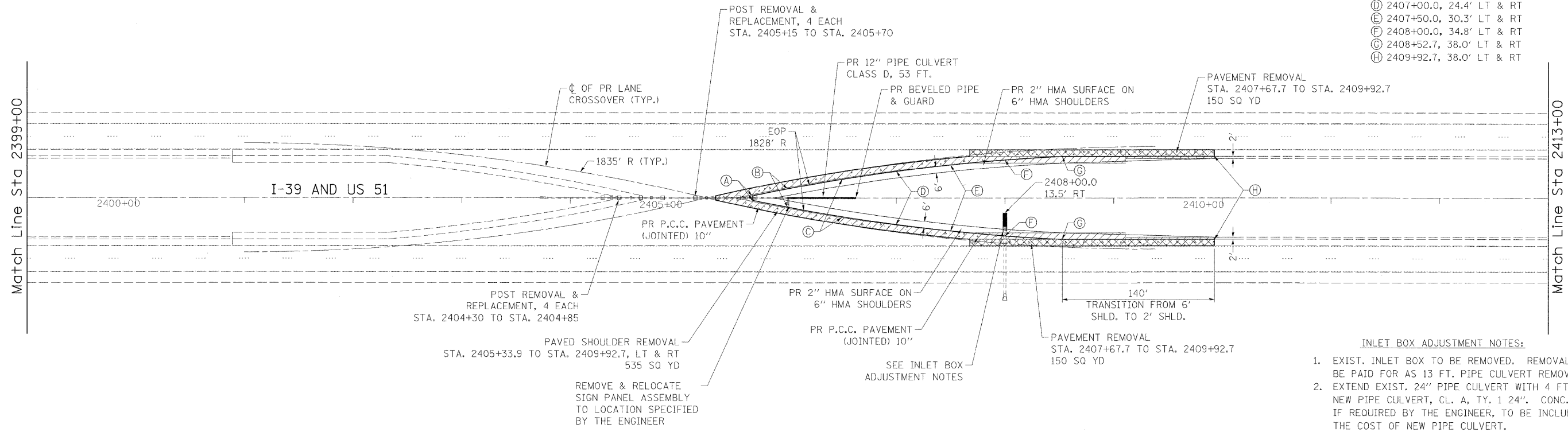
DESIGNED - BJB  
 DRAWN - BJB  
 CHECKED - JMH  
 DATE - 2/9/12  
 REVISED -  
 REVISED -  
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 REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES  
 I-39 OVER KISHWAUKEE RIVER**  
 SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	10
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				

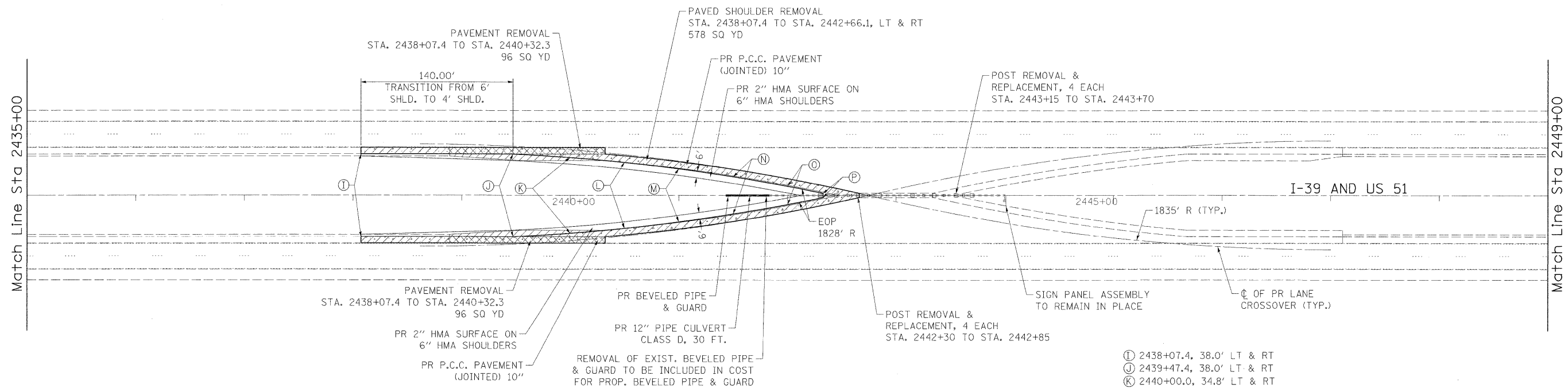
- Ⓐ 2405+67.4, 2.0' LT & RT
- Ⓑ 2406+00.0, 8.4' LT & RT
- Ⓒ 2406+50.0, 17.1' LT & RT
- Ⓓ 2407+00.0, 24.4' LT & RT
- Ⓔ 2407+50.0, 30.3' LT & RT
- Ⓕ 2408+00.0, 34.8' LT & RT
- Ⓖ 2408+52.7, 38.0' LT & RT
- Ⓗ 2409+92.7, 38.0' LT & RT



**SOUTH CROSSOVER**

- INLET BOX ADJUSTMENT NOTES:**
- EXIST. INLET BOX TO BE REMOVED. REMOVAL TO BE PAID FOR AS 13 FT. PIPE CULVERT REMOVAL.
  - EXTEND EXIST. 24" PIPE CULVERT WITH 4 FT. NEW PIPE CULVERT, CL. A, TY. 1 24". CONC. COLLAR, IF REQUIRED BY THE ENGINEER, TO BE INCLUDED IN THE COST OF NEW PIPE CULVERT.
  - NEW INLET BOX, STANDARD 542511. STA. 2408+00.0, 13.5' RT MATCH EXIST. INVERT ELEVATION

NOTE:  
1. SEE DISTRICT 2 STANDARD 88' SINGLE LANE MEDIAN CROSSOVER FOR DETAILS NOT SHOWN.



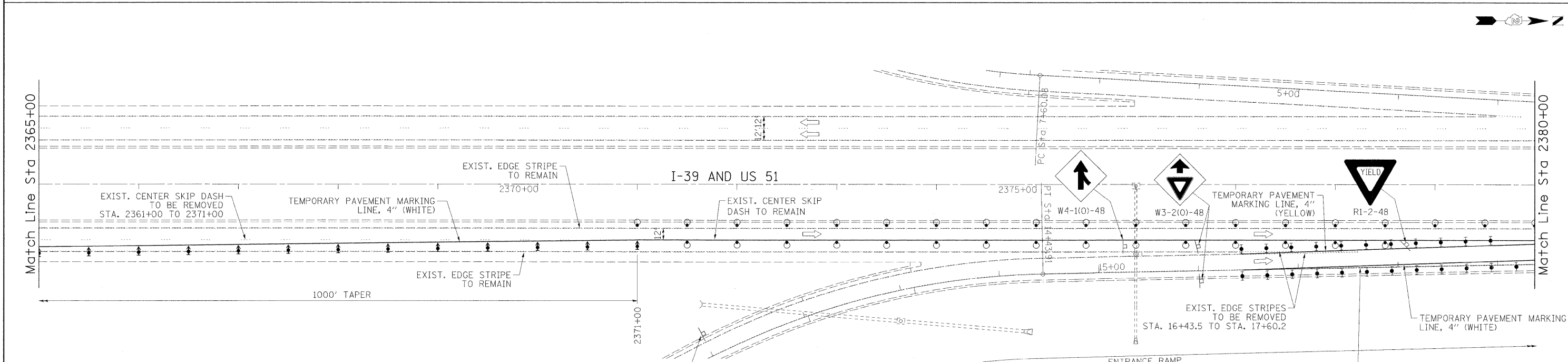
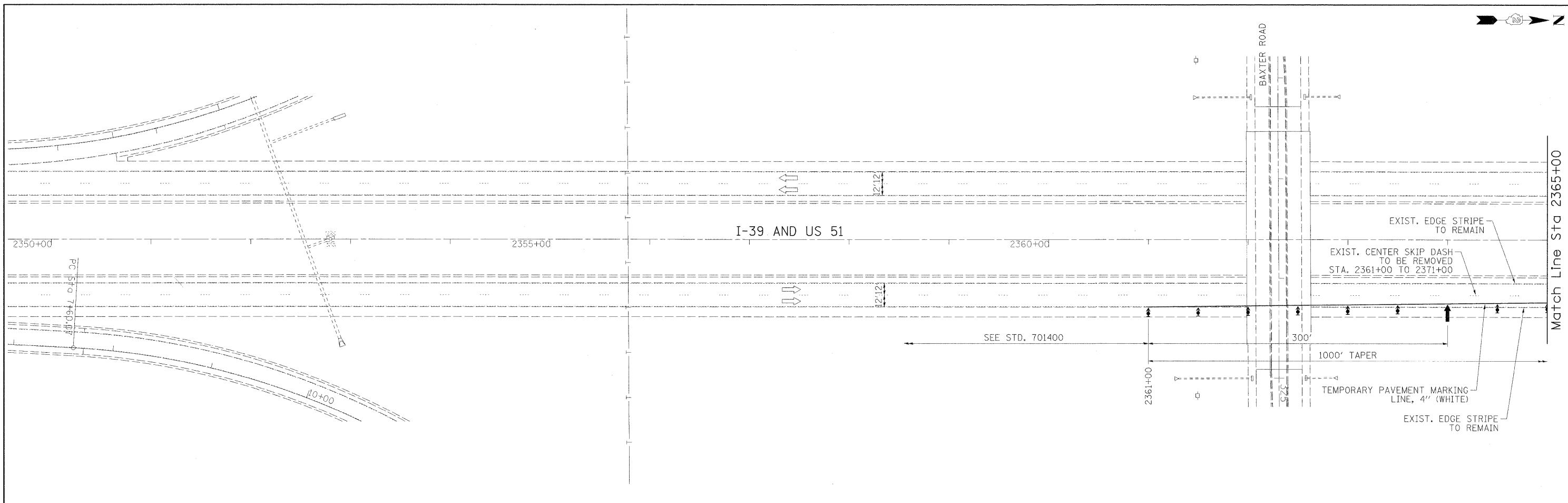
**NORTH CROSSOVER**

- Ⓘ 2438+07.4, 38.0' LT & RT
- Ⓝ 2439+47.4, 38.0' LT & RT
- Ⓚ 2440+00.0, 34.8' LT & RT
- Ⓛ 2440+50.0, 30.3' LT & RT
- Ⓜ 2441+00.0, 24.4' LT & RT
- Ⓝ 2441+50.0, 17.1' LT & RT
- Ⓞ 2442+00.0, 8.4' LT & RT
- Ⓟ 2442+32.7, 2.0' LT & RT

- LEGEND**
- PAVED SHOULDER REMOVAL
  - PAVEMENT REMOVAL
  - P.C.C. PAVEMENT (JOINTED) 10"

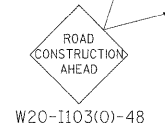


FILE NAME =	USER NAME = brenn00248	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CROSSOVER PAVING PLAN I-39 OVER KISHWAUKEE RIVER</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwise_work\do_not_delete\des59254\0264472-sh-t-plan01.dgn	PLCT SCALE = 50.0000' / in.	DRAWN - BJB	REVISED -		39	(201-1BIM-3)	WINNEBAGO	38	11			
PLCT DATE = 02/10/2012	DATE - 2/9/12	CHECKED - JMH	REVISED -		SCALE: 1" = 50'			SHEET 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 64H72	
		DATE - 2/9/12	REVISED -		ILLINOIS FED. AID PROJECT							



**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER



NOTE:  
REFER TO STANDARDS 701400, 701411 &  
701416 FOR ADDITIONAL REQUIREMENTS.

TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
40' INTERVAL:  
NB RT: STA. 2361+00.0 TO STA. 2380+00.0  
ENT. RAMP LT: STA. 16+43.5 TO STA. 19+37.2  
ENT. RAMP RT: STA. 16+43.5 TO STA. 19+37.2



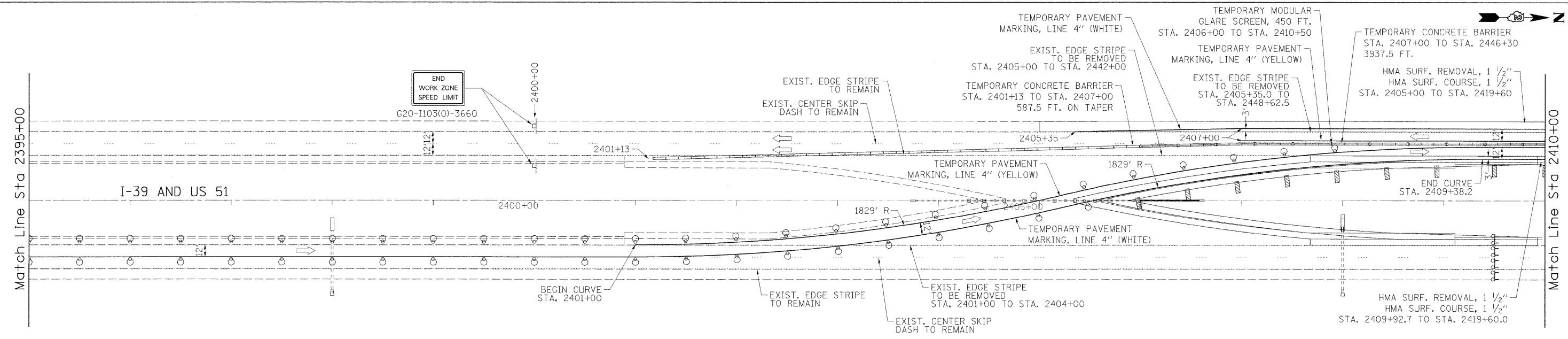
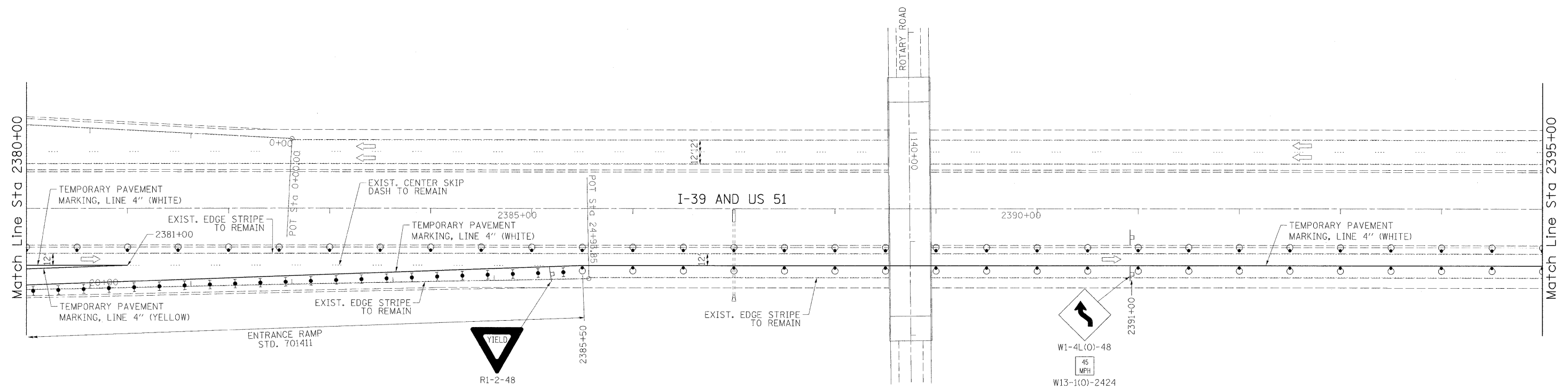
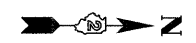
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PLOT DATE = 02/09/2012		DATE - 2/9/12	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN - STAGE 1  
I-39 OVER KISHWAUKEE RIVER**

SCALE: 1" = 50' SHEET 1 OF 5 SHEETS STA. 2350+00 TO STA. 2380+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNBAGO	38	12
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- TEMPORARY MODULAR GLARE SCREEN

**TEMP. RAISED REFLECTIVE PAVEMENT MARKERS**

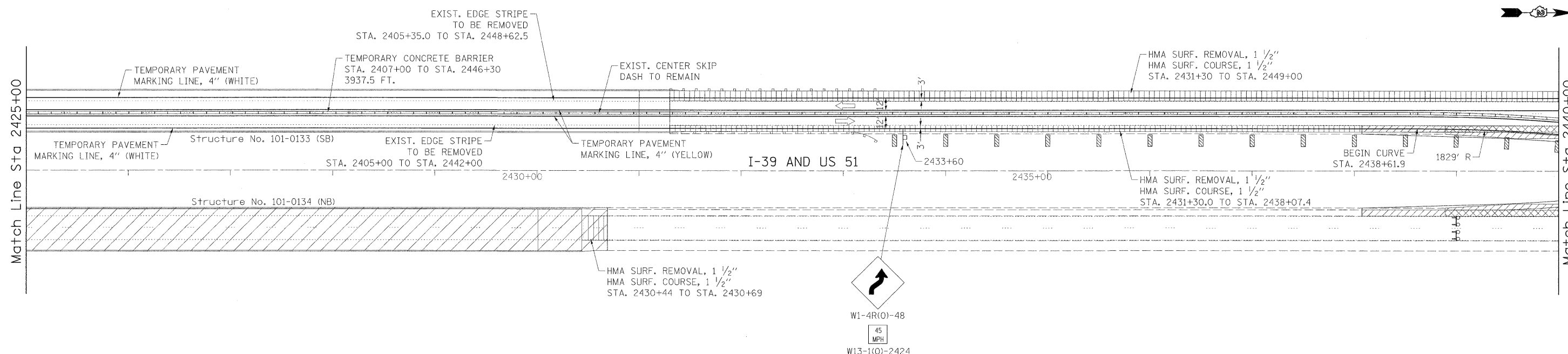
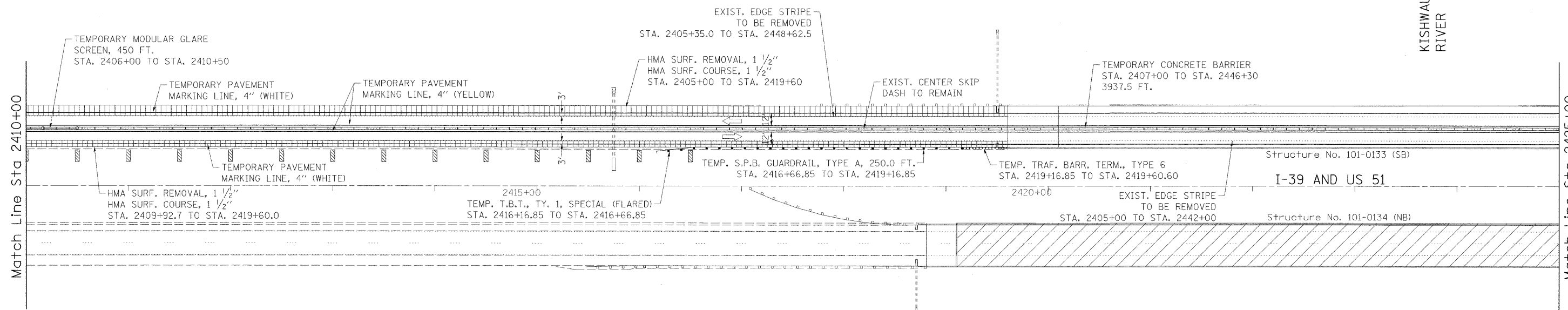
- 40' INTERVAL:
- NB RT: STA. 2380+00.0 TO STA. 2381+00.0
- NB RT: STA. 2385+50.0 TO STA. 2401+00.0
- NB RT: STA. 2409+38.2 TO STA. 2410+00.0
- SB RT: STA. 2405+35.0 TO STA. 2410+00.0
- ENT. RAMP LT: STA. 19+37.2 TO STA. 20+38.0
- ENT. RAMP RT: STA. 19+37.2 TO STA. 24+88.0
- 20' INTERVAL:
- NB LT: STA. 2401+00.0 TO STA. 2409+00.0
- NB RT: STA. 2401+00.0 TO STA. 2409+38.2

NOTE:  
 1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.  
 2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.



FILE NAME =	USER NAME = brenn00248	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL PLAN - STAGE 1 I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr:\pwise_work\do_not_delete\dms59254\0264472-shit-staging02.dgn	PLOT SCALE = 50.0000' / in.	CHECKED - JMH	REVISED -			39	(201-1B)M-3	WINNEBAGO	38	13	
PLOT DATE = 02/18/2012	DATE - 2/9/12	REVISED -				CONTRACT NO. 64H72					
						ILLINOIS FED. AID PROJECT					

KISHWAUKEE RIVER



LEGEND

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER

- TEMP. RAISED REFLECTIVE PAVEMENT MARKERS
- 40' INTERVAL:
- NB RT: STA. 2410+00.0 TO STA. 2416+54.4
  - NB RT: STA. 2433+05.5 TO STA. 2438+61.9
  - SB RT: STA. 2410+00.0 TO STA. 2417+77.2
  - SB RT: STA. 2433+35.2 TO STA. 2440+00.0
- 20' INTERVAL:
- NB RT: STA. 2438+61.9 TO STA. 2440+00.0

NOTE:

1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.
2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.



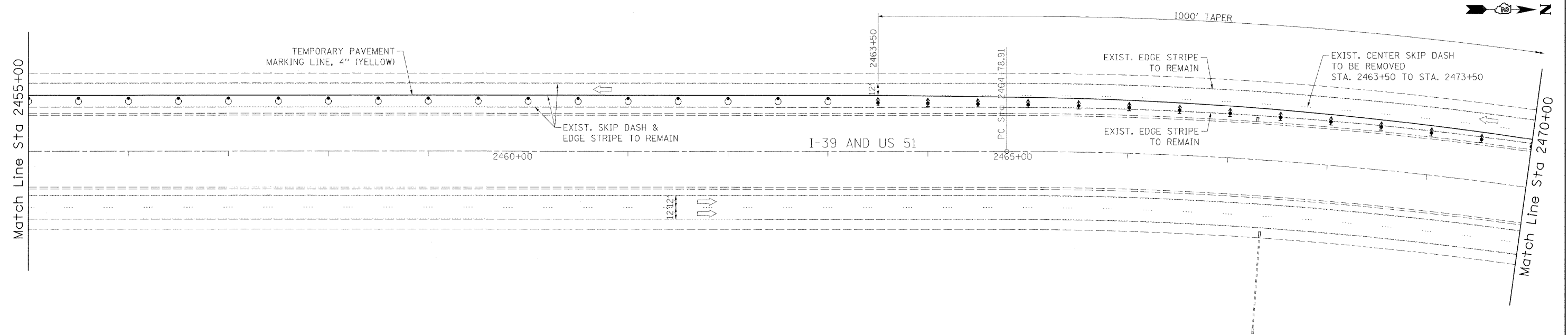
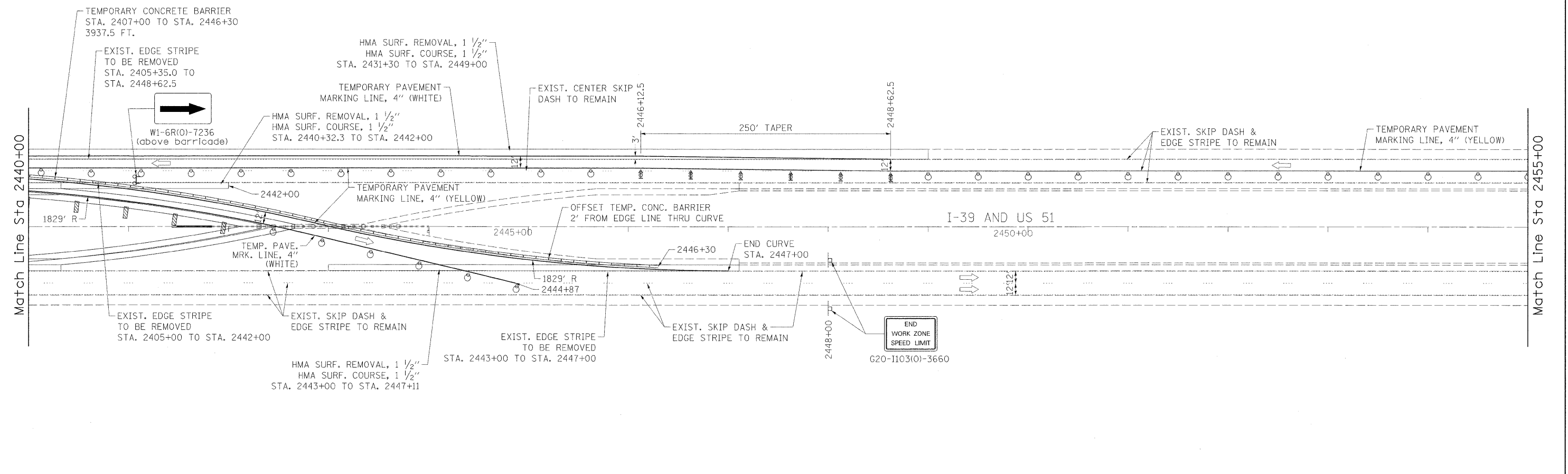
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PLOT DATE = 02/18/2012		DATE - 2/9/12	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN - STAGE 1  
I-39 OVER KISHWAUKEE RIVER

SCALE: 1" = 50' SHEET 3 OF 5 SHEETS STA. 2410+00 TO STA. 2440+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	14
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- ➔ ARROW BOARD
- ▨ WORK AREA
- ⊠ SIGN
- ➔ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⊠ TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▨ VERTICAL PANEL (BACK TO BACK)
- ⊠ TYPE III BARRICADE WITH FLASHING LIGHTS
- ▬ TEMPORARY CONCRETE BARRIER

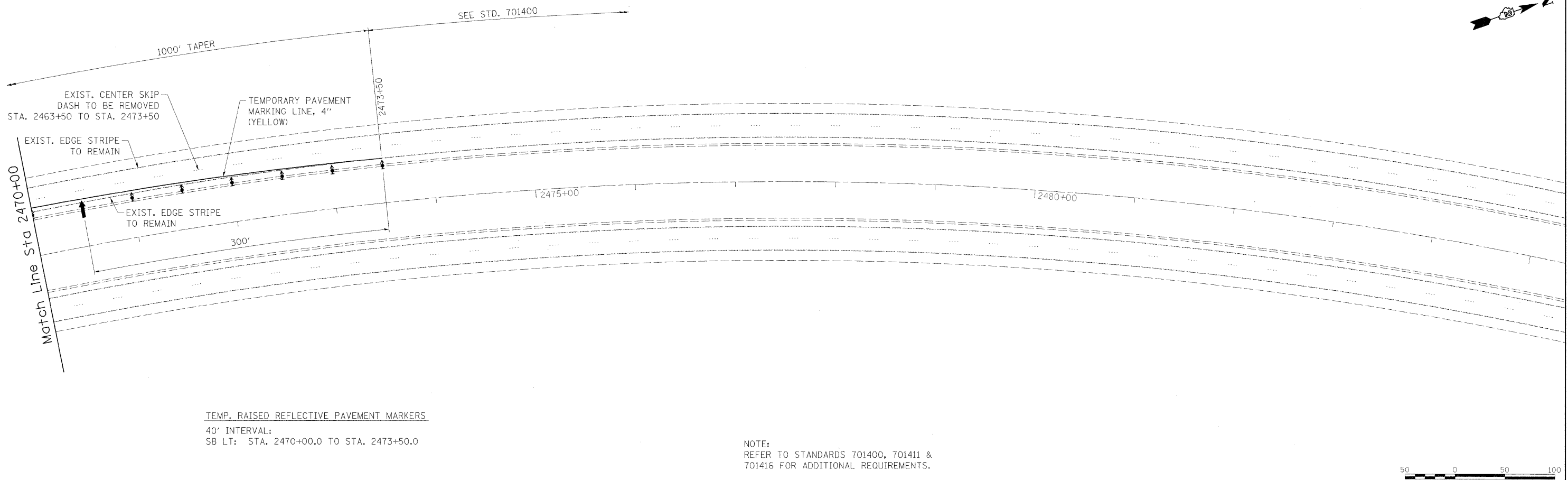
NOTE:  
 1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.  
 2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.

TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
 40' INTERVAL:  
 SB LT: STA. 2440+00.0 TO STA. 2470+00.0  
 SB RT: STA. 2440+00.0 TO STA. 2448+62.5  
 20' INTERVAL:  
 NB RT: STA. 2440+00.0 TO STA. 2444+87.0



FILE NAME =	USER NAME = brenn00248	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL PLAN - STAGE 1 I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE. 39	SECTION (201-1B)M-3	COUNTY WINNEBAGO	TOTAL SHEETS 38	SHEET NO. 15	
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PLOT DATE = 02/10/2012	DATE - 2/9/12	REVISED -	REVISED -			CONTRACT NO. 64H72					
ILLINOIS FED. AID PROJECT											



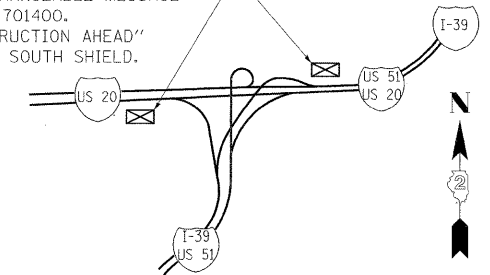


TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
 40' INTERVAL:  
 SB LT: STA. 2470+00.0 TO STA. 2473+50.0

NOTE:  
 REFER TO STANDARDS 701400, 701411 &  
 701416 FOR ADDITIONAL REQUIREMENTS.



ROAD CONSTRUCTION AHEAD, HIT A WORKER  
 AND THE PORTABLE CHANGEABLE MESSAGE  
 SHOWN ON STANDARD 701400.  
 MODIFY "ROAD CONSTRUCTION AHEAD"  
 SIGN BY ADDING I-39 SOUTH SHIELD.



CONSTRUCTION AHEAD SIGNAGE PLAN  
 Not to Scale

LEGEND

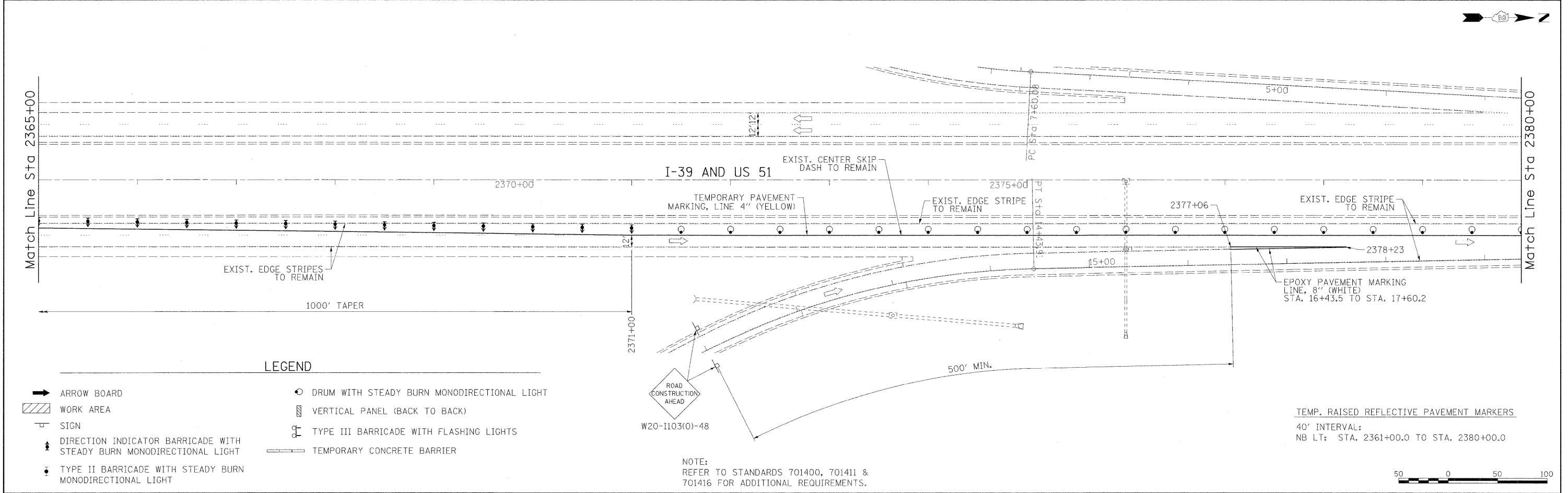
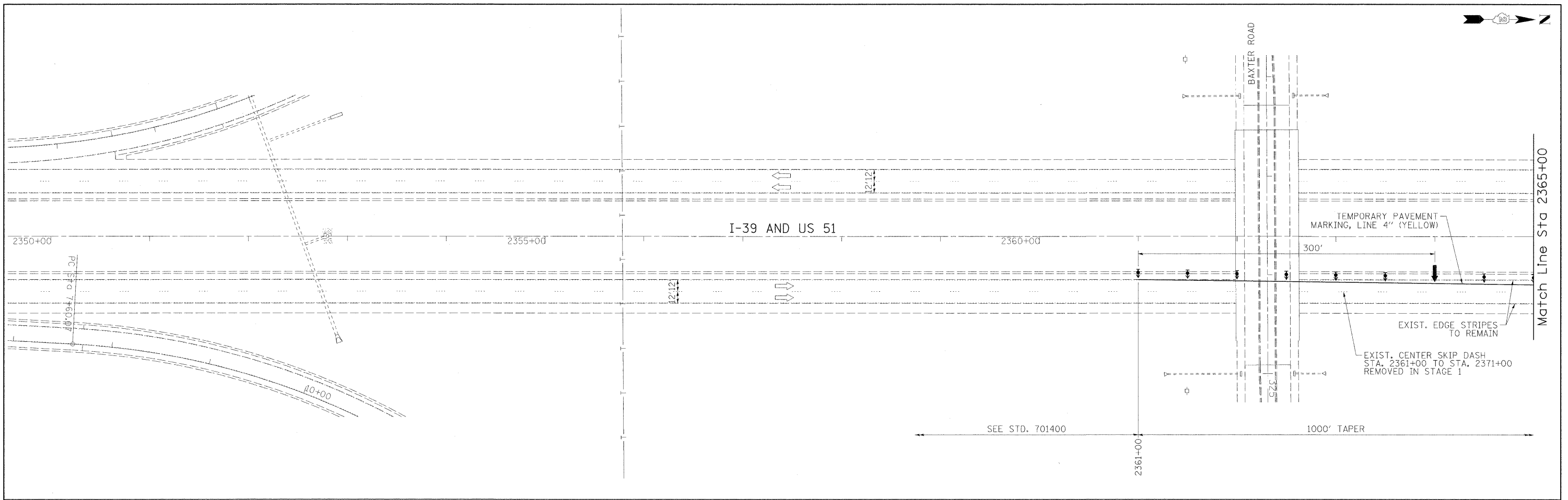
- ➔ ARROW BOARD
- ▨ WORK AREA
- ⊠ SIGN
- ↑ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ↓ TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▤ VERTICAL PANEL (BACK TO BACK)
- ⊠ TYPE III BARRICADE WITH FLASHING LIGHTS
- ▬ TEMPORARY CONCRETE BARRIER

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

<b>TRAFFIC CONTROL PLAN - STAGE 1</b>			
<b>I-39 OVER KISHWAUKEE RIVER</b>			
SCALE: 1" = 50'	SHEET 5	OF 5 SHEETS	STA. 2470+00 TO STA. 2485+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	16
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- ➔ ARROW BOARD
- ▨ WORK AREA
- ⊥ SIGN
- ↑ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▤ VERTICAL PANEL (BACK TO BACK)
- ⚡ TYPE III BARRICADE WITH FLASHING LIGHTS
- ▬ TEMPORARY CONCRETE BARRIER



NOTE:  
REFER TO STANDARDS 701400, 701411 &  
701416 FOR ADDITIONAL REQUIREMENTS.

TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
40' INTERVAL:  
NB LT: STA. 2361+00.0 TO STA. 2380+00.0



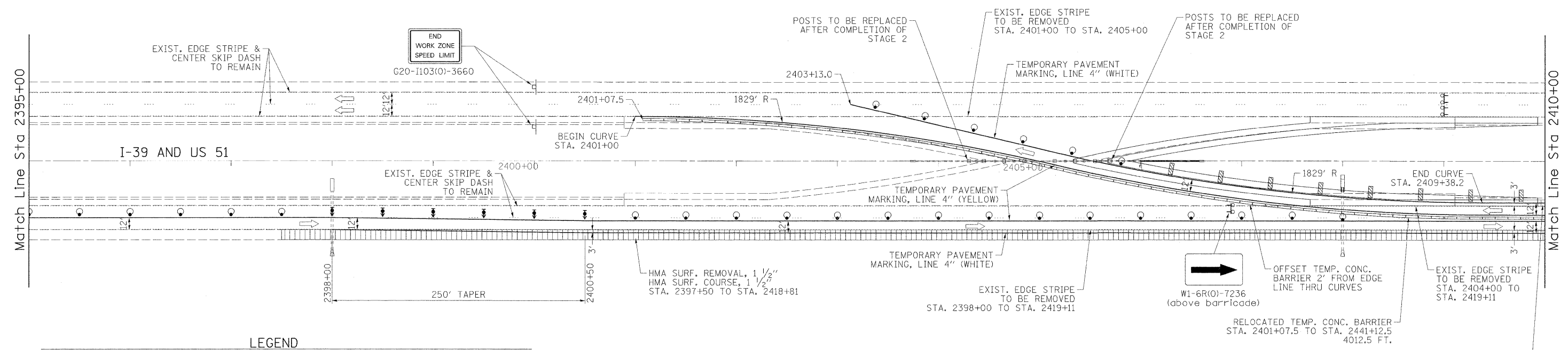
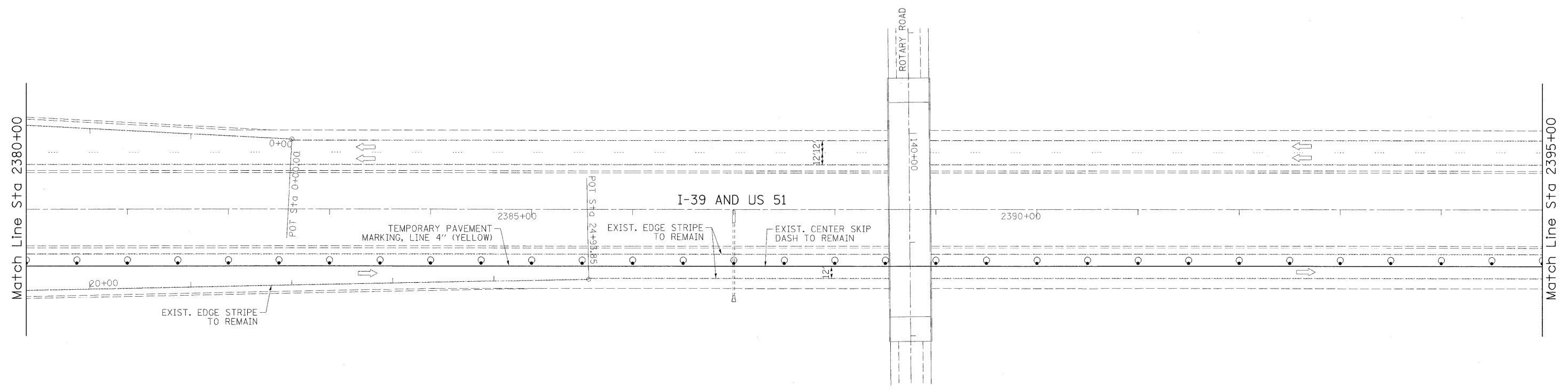
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PLOT DATE = 02/09/2012		DATE - 2/9/12	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN - STAGE 2  
I-39 OVER KISHWAUKEE RIVER**

SCALE: 1" = 50' SHEET 1 OF 5 SHEETS STA. 2350+00 TO STA. 2380+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNBAGO	38	17
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



**LEGEND**

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER

**NOTE:**  
 1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.  
 2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.

**TEMP. RAISED REFLECTIVE PAVEMENT MARKERS**  
 40' INTERVAL:  
 NB LT: STA. 2380+00.0 TO STA. 2410+00.0  
 NB RT: STA. 2398+00.0 TO STA. 2410+00.0  
 SB RT: STA. 2409+38.2 TO STA. 2410+00.0  
 20' INTERVAL:  
 SB RT: STA. 2403+13.0 TO STA. 2409+38.2



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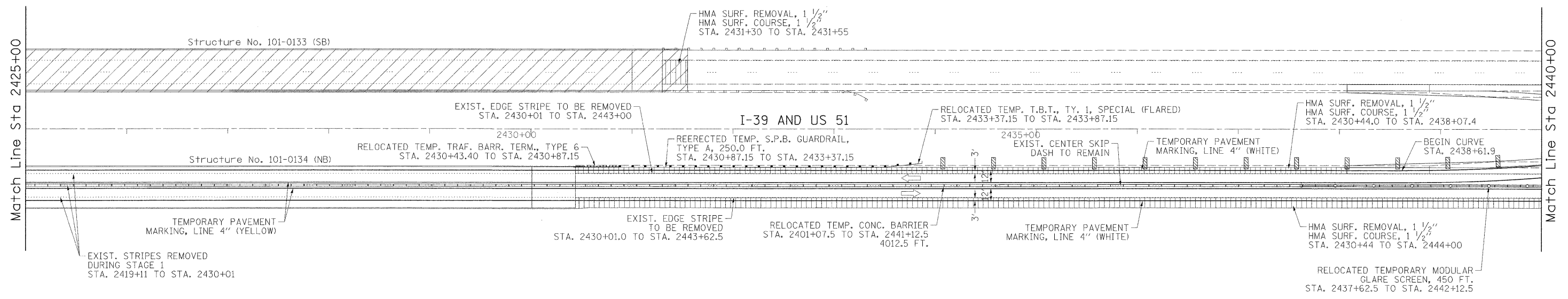
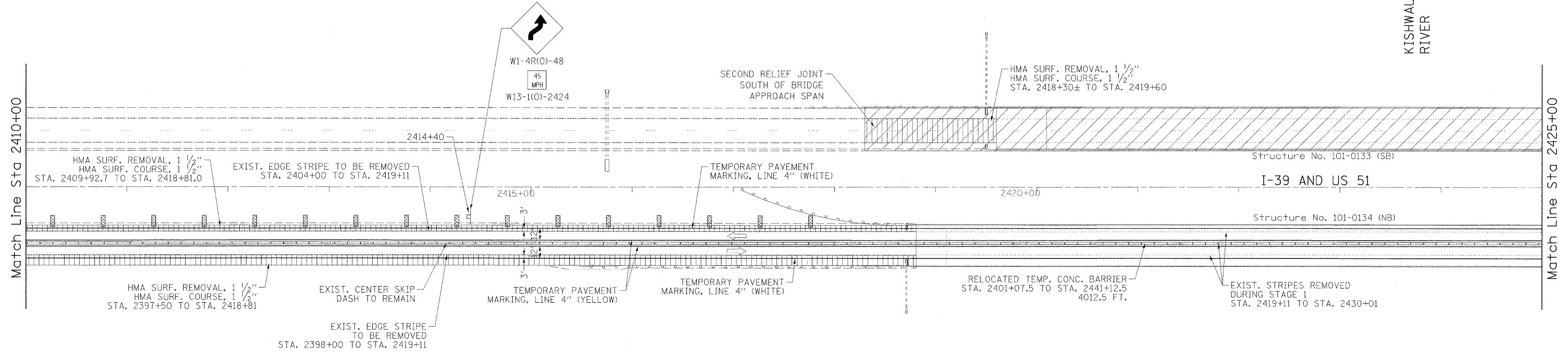
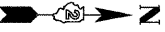
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN - STAGE 2  
I-39 OVER KISHWAUKEE RIVER**

SCALE: 1" = 50'    SHEET 2 OF 5 SHEETS    STA. 2380+00 TO STA. 2410+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	18
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				

KISHWAUKEE RIVER



LEGEND

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- TEMPORARY MODULAR GLARE SCREEN

NOTE:  
 1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.  
 2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.

TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
 40' INTERVAL:  
 NB RT: STA. 2410+00.0 TO STA. 2415+97.7  
 NB RT: STA. 2430+44.0 TO STA. 2440+00.0  
 SB RT: STA. 2410+00.0 TO STA. 2418+81.0  
 SB RT: STA. 2433+37.2 TO STA. 2438+61.9  
 20' INTERVAL:  
 SB LT: STA. 2439+00.0 TO STA. 2440+00.0  
 SB RT: STA. 2438+61.9 TO STA. 2440+00.0



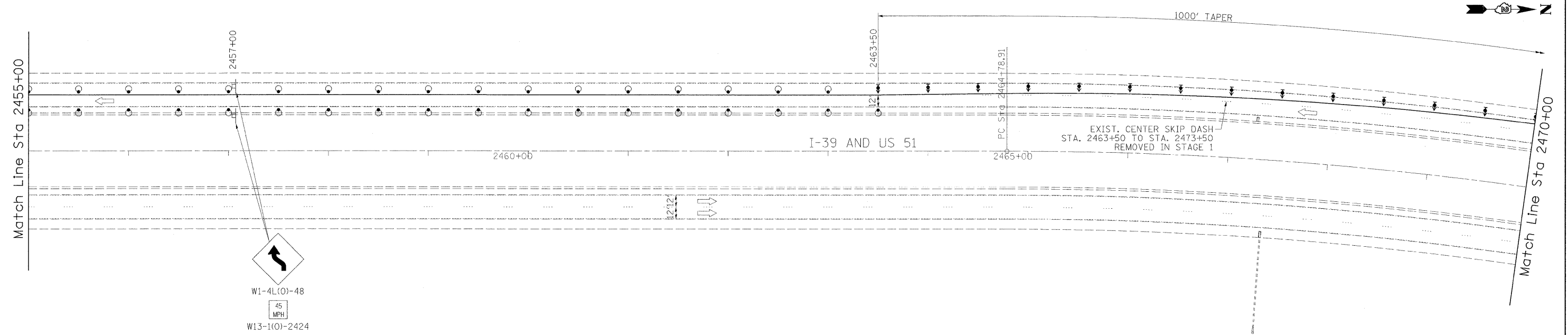
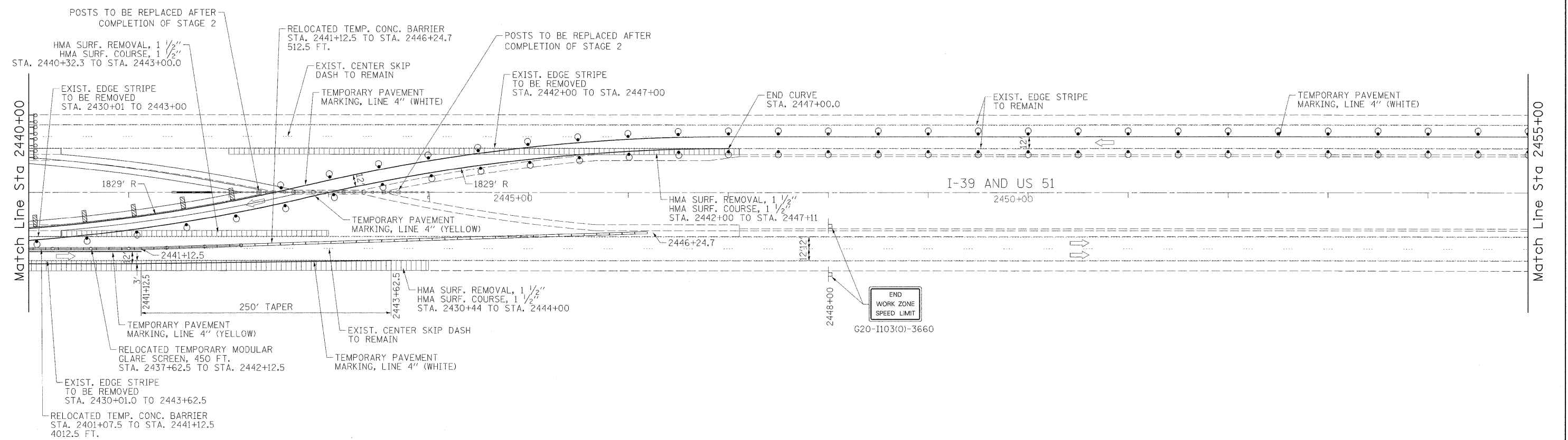
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PLCT DATE = 02/18/2012		DATE - 2/9/12	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN - STAGE 2  
 I-39 OVER KISHWAUKEE RIVER**

SCALE: 1" = 50'    SHEET 3 OF 5 SHEETS    STA. 2410+00 TO STA. 2440+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	19
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



**LEGEND**

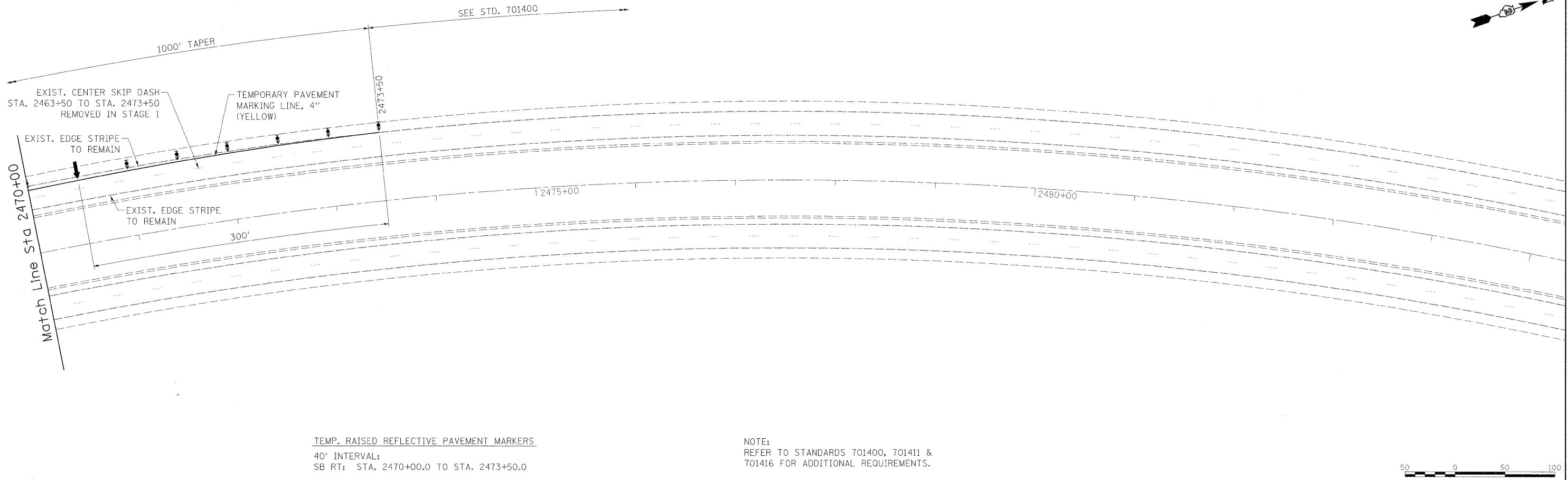
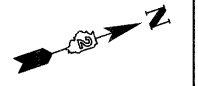
- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- VERTICAL PANEL (BACK TO BACK)
- TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY CONCRETE BARRIER
- TEMPORARY MODULAR GLARE SCREEN

NOTE:  
 1. REFER TO STANDARDS 701400, 701411 & 701416 FOR ADDITIONAL REQUIREMENTS.  
 2. SEE CROSSOVER PAVING PLAN SHEET FOR PAVEMENT WIDENING, PAVED SHOULDER REMOVAL & PAVEMENT REMOVAL.

TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
 40' INTERVAL:  
 NB RT: STA. 2440+00.0 TO STA. 2443+62.5  
 SB RT: STA. 2447+00.0 TO STA. 2470+00.0  
 20' INTERVAL:  
 SB LT: STA. 2440+00.0 TO STA. 2447+00.0  
 SB RT: STA. 2440+00.0 TO STA. 2447+00.0



FILE NAME =	USER NAME = brenn00248	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL PLAN - STAGE 2 I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT DATE = 02/18/2012	DATE - 2/9/12	CHECKED - JMH	REVISED -			CONTRACT NO. 64H72					
						ILLINOIS FED. AID PROJECT					

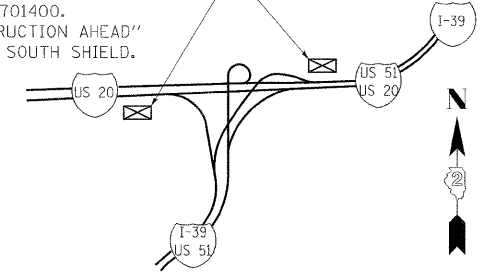


TEMP. RAISED REFLECTIVE PAVEMENT MARKERS  
 40' INTERVAL:  
 SB RT: STA. 2470+00.0 TO STA. 2473+50.0

NOTE:  
 REFER TO STANDARDS 701400, 701411 &  
 701416 FOR ADDITIONAL REQUIREMENTS.



ROAD CONSTRUCTION AHEAD, HIT A WORKER  
 AND THE PORTABLE CHANGEABLE MESSAGE  
 SHOWN ON STANDARD 701400.  
 MODIFY "ROAD CONSTRUCTION AHEAD"  
 SIGN BY ADDING I-39 SOUTH SHIELD.



CONSTRUCTION AHEAD SIGNAGE PLAN  
 Not to Scale

LEGEND

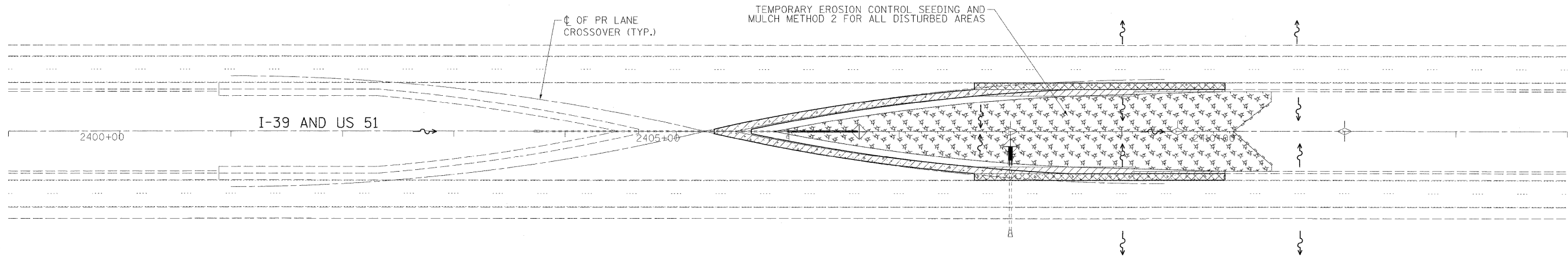
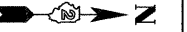
- ➔ ARROW BOARD
- ▨ WORK AREA
- ⊠ SIGN
- ⬆ DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⬇ TYPE II BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
- ⊙ DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
- ▩ VERTICAL PANEL (BACK TO BACK)
- ⊠ TYPE III BARRICADE WITH FLASHING LIGHTS
- ▬ TEMPORARY CONCRETE BARRIER

FILE NAME =	USER NAME = pearc00397	DESIGNED - BJB	REVISED -
c:\pwise_work\do_not_delete\dms59254\0264472-shit-staging08a.dgn		DRAWN - BJB	REVISED -
	PLOT SCALE = 50.0000' / 1" =	CHECKED - JMH	REVISED -
	PLOT DATE = 02/09/2012	DATE - 2/9/12	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

<b>TRAFFIC CONTROL PLAN - STAGE 2</b>			
<b>I-39 OVER KISHWAUKEE RIVER</b>			
SCALE: 1" = 50'	SHEET 5 OF 5 SHEETS	STA. 2470+00 TO STA. 2485+00	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	21
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



SOUTH CROSSOVER

**EROSION CONTROL LEGEND**

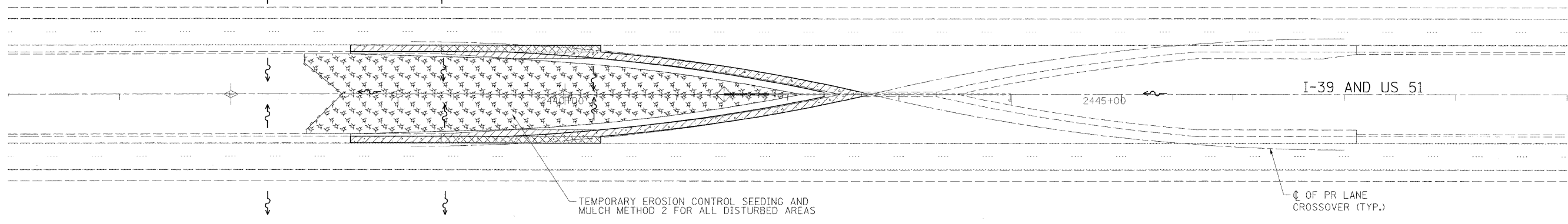
- TEMPORARY SEEDING/MULCH
- INLET AND PIPE PROTECTION
- TEMPORARY DITCH CHECK
- PERIMETER EROSION BARRIER
- DRAINAGE PATTERN DIRECTION

**INTENDED SEQUENCE**

1. PLACEMENT OF PERIMETER EROSION BARRIER PRIOR TO COMMENCEMENT OF ANY WORK, SEE STANDARD 280001.
2. PLACEMENT OF TEMPORARY SEEDING ON GRADED SURFACES NOT HAVING PERMANENT SEEDING APPLIED.
3. ONGOING MAINTENANCE OF EROSION CONTROL ELEMENTS PER THE SWPPP.
4. REMOVE TEMPORARY EROSION CONTROL ELEMENTS AFTER FINAL GRADING AND PERMANENT SEEDING ESTABLISHED AS PER THE SWPPP AND APPROVED BY THE ENGINEER.

**NOTES**

1. MAJOR GRADING SLOPES ALONG THE PROPOSED ROADWAY ARE 6:1 TYP.
2. SOILS DISTURBANCE SHALL ONLY OCCUR WITHIN THE AREAS SHOWN.
3. RECEIVING WATER FOR DRAINAGE FROM PROJECT IS THE KISHWAUKEE RIVER.



NORTH CROSSOVER



FILE NAME =	USER NAME = brenn00248	DESIGNED - BJB	REVISED -
o:\p\wise_work\do_not_delete\dms59254\0264472-shit-eros01.dgn		DRAWN - BJB	REVISED -
PLOT SCALE = 50.0000' / in.		CHECKED - JMH	REVISED -
PLOT DATE = 02/10/2012		DATE - 2/9/12	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

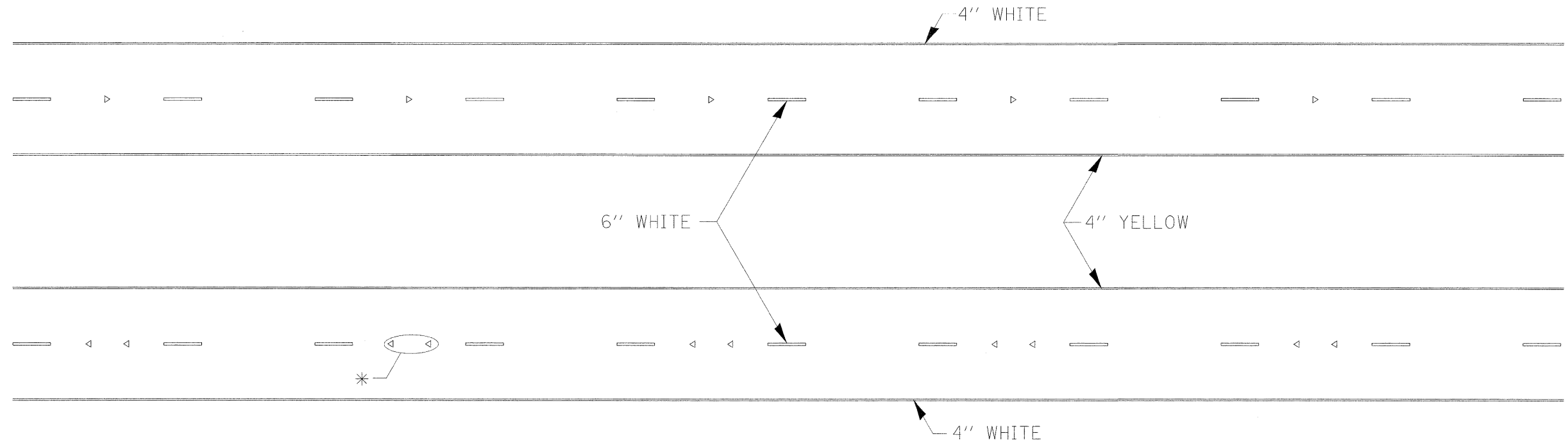
<b>TEMPORARY EROSION CONTROL PLAN</b>			
<b>I-39 OVER KISHWAUKEE RIVER</b>			
SCALE: 1" = 50'	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	22
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				



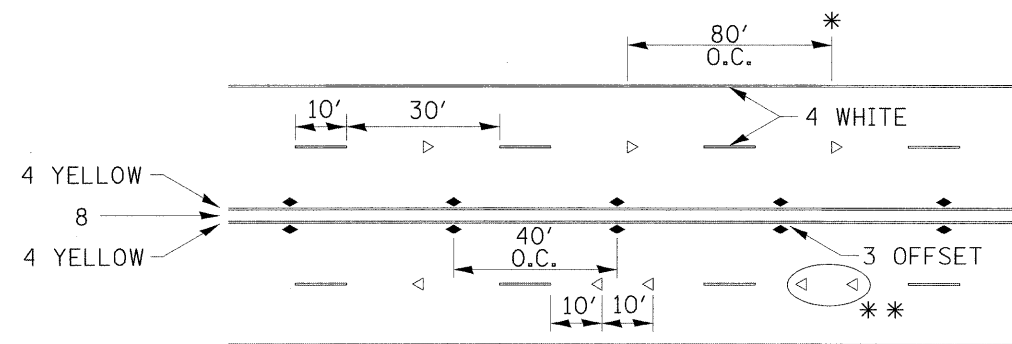


# TYPICAL PAVEMENT MARKINGS



\* SEE HIGHWAY STANDARD 781001 FOR SPACING DETAILS.  
USE DOUBLE MARKERS WHEN ADT  $\geq$  25,000.

## MULTI-LANE / DIVIDED

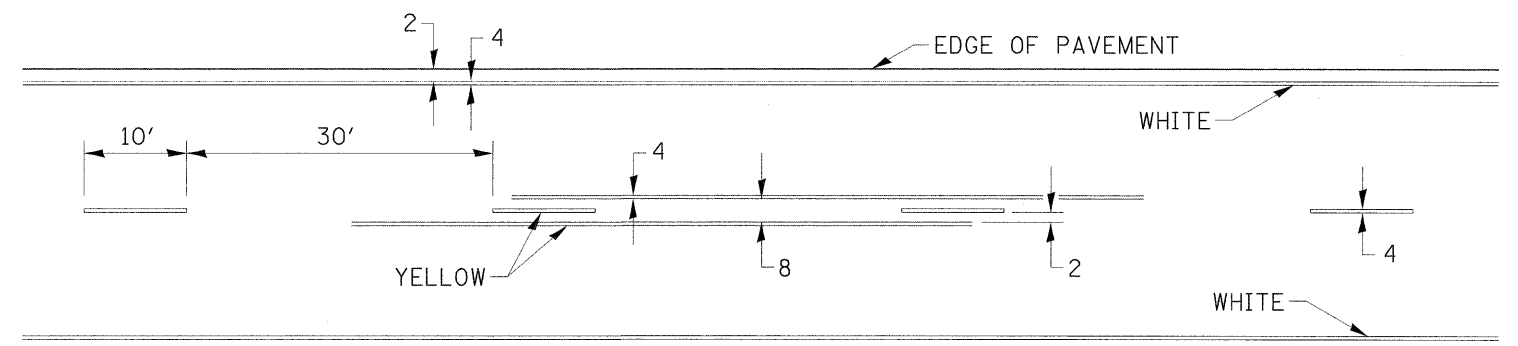


\* REDUCE TO 40' O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH LOWER THAN POSTED SPEEDS.  
\*\* USE DOUBLE MARKERS WHEN ADT  $\geq$  25,000

## MULTI-LANE / UNDIVIDED

(FOR MULTI-LANE UNDIVIDED HIGHWAYS USE THIS  
DETAIL NOT HIGHWAY STANDARD 781001)

## TYPICAL PAVEMENT MARKING FOR TWO LANE SECTION – NO PASSING ZONES



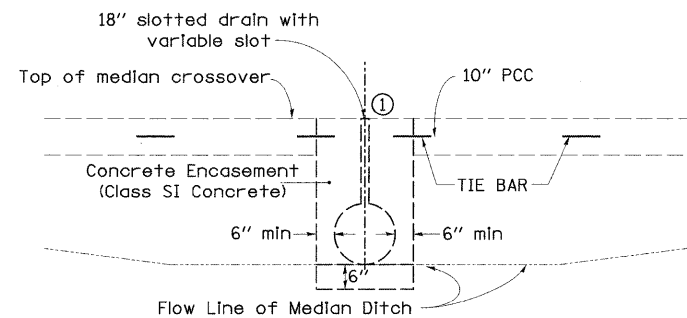
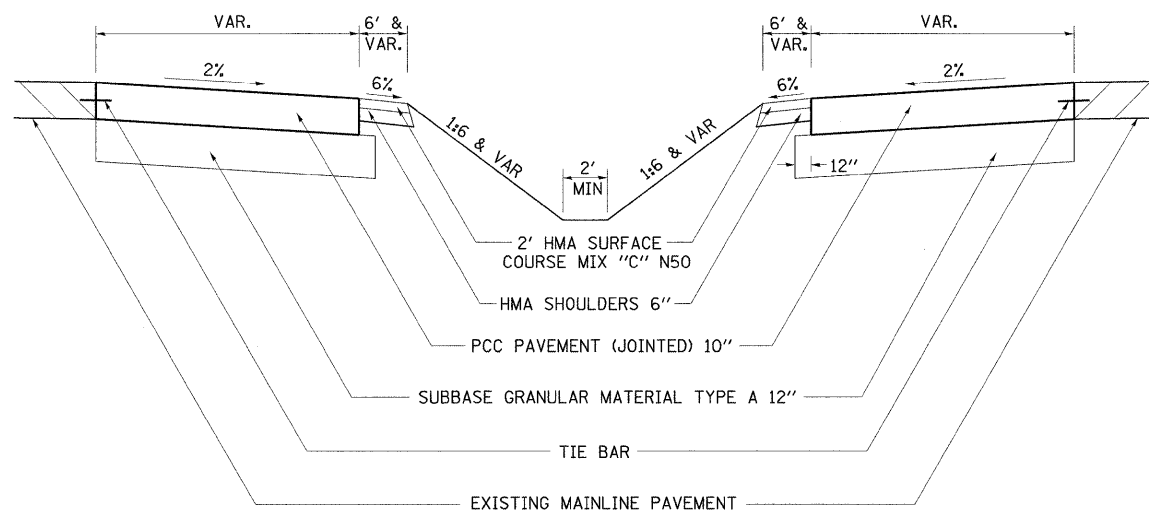
SYMBOLS

FILE NAME =	USER NAME = pearc00397	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 2 STANDARDS I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pwise_work\do_not_delete\dms59254\0224\4h72-shr-detail\02.dgn		DRAWN - BJB	REVISED -			39	(201-1BIM-3)	WINNEBAGO	38	24	
FLOT SCALE = 50.0000' / in.		CHECKED - JMH	REVISED -			CONTRACT NO. 64H72					
FLOT DATE = 02/09/2012		DATE - 2/9/12	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE:	SHEET 2 OF 5 SHEETS STA. TO STA.					

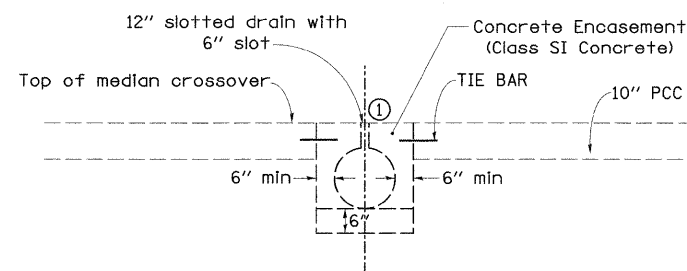
# 88' SINGLE LANE MEDIAN CROSSOVER

(POSTED SPEED LIMIT 65 MPH, WORK ZONE SPEED LIMIT 55 MPH)

TYPICAL SECTION



SECTION A-A  
(USE TO MAINTAIN MEDIAN DRAINAGE THROUGH THE CROSSOVER)



① Duct tape or wood blocks shall be used to cover slotted drain during construction of crossover paving

SECTION A-A  
(WHEN CROSSOVER IS AT MEDIAN HIGH POINT)

## GENERAL NOTES

Construction of median crossover shall conform to the requirement of current Standard Specifications.

Slotted drain shall be constructed of 14 or 16 gauge corrugated metal roadway pipe modified to accommodate slotted drain as shown.

Pavement, subbase, & shoulder quantities are:

(3704.06 Sq. Yds.)	SUBBASE GRANULAR MATERIAL TYPE A, 12"
(3535.98 Sq. Yds.)	P.C.C. PAVEMENT, (JOINTED) 10"
(96.79 Tons)	2" HMA SURFACE COURSE, MIX "C", N50
(864.23 Sq. Yds.)	HMA SHOULDERS 6"

Elbows and Caps shall be considered included to the SLOTTED DRAIN 12" WITH 6" SLOT.

See District Standard 61.2 or 68.1 for details for the slotted drain.

See District Standard 86.1 for details for the beveled pipe & guard.

The crossover is designed using a 55mph design speed.

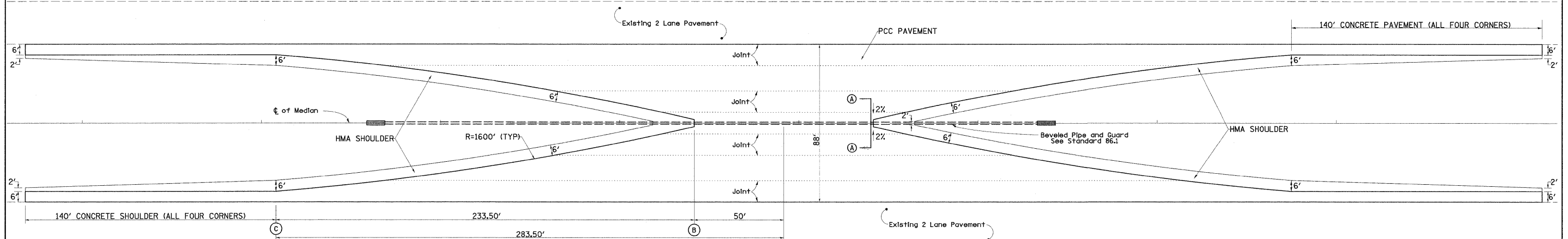
The end of the pipe guard shall be set where a minimum 1:4 front slope can be constructed from each side of pipe guard to the HMA shoulder.

The PCC Pavement (Jointed) 10" shall be constructed according to Section 420 of the Standard Specifications and Highway Standards 420001, 420101, & 420106

The PCC Pavement (Jointed) 10" shall be tied to adjacent existing concrete pavement and the concrete encasement for the slotted drain. The tie bars shall be No. 6 bars 24" long @ 30" cts, and installed according to the applicable portions of Article 420.05 (b) of the Standard Specifications. The cost of the bars to be included in the cost of the PCC Pavement (Jointed) 10".

TABLE OF OFFSETS AND DROPS

Distance feet from location station	0	50'	75'	100'	125'	150'	175'	200'	225'	250'	275'	283.50'
Offsets feet from inside edge of pavement	44'	42'	36.44'	31.30'	26.57'	22.25'	18.34'	14.83'	11.72'	9.01'	6.70'	6'
Drop feet from inside edge of pavement	0.88'	0.84'	0.73'	0.63'	0.53'	0.43'	0.37'	0.30'	0.23'	0.18'	0.13'	0.12'



TYPICAL PLAN

Unless otherwise specified, when the median crossover is to be removed, the Contractor shall be required to saw full-depth along the shoulder line 6' from edge of pavement). The 6' adjacent to the edge of pavement shall remain in place and be used as shoulders. The cost of Sawing shall be included in the Pavement Removal.

Longitudinal joints shall be sawed at a max 12' width. All joints shall be sealed.

TRAFFIC CONTROL STANDARD 701416 IS TO BE USED WITH THIS DETAIL

FILE NAME =	USER NAME = parr-00397	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 2 STANDARDS I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
cr:\pwise_work\do_not_delete\dms59254\024472-shr-detail1a03.dgn	PLOT SCALE = 50.0000' / in.	DRAWN - BJB	REVISED -			39	(201-1B)M-3	WINNEBAGO	38	25	
PLOT DATE = 02/09/2012	DATE - 2/9/12	CHECKED - JMH	REVISED -			CONTRACT NO. 64H72					
						ILLINOIS FED. AID PROJECT					

# BEVELED PIPE & GUARD DETAIL FOR MEDIAN CROSSOVER

## GENERAL NOTES:

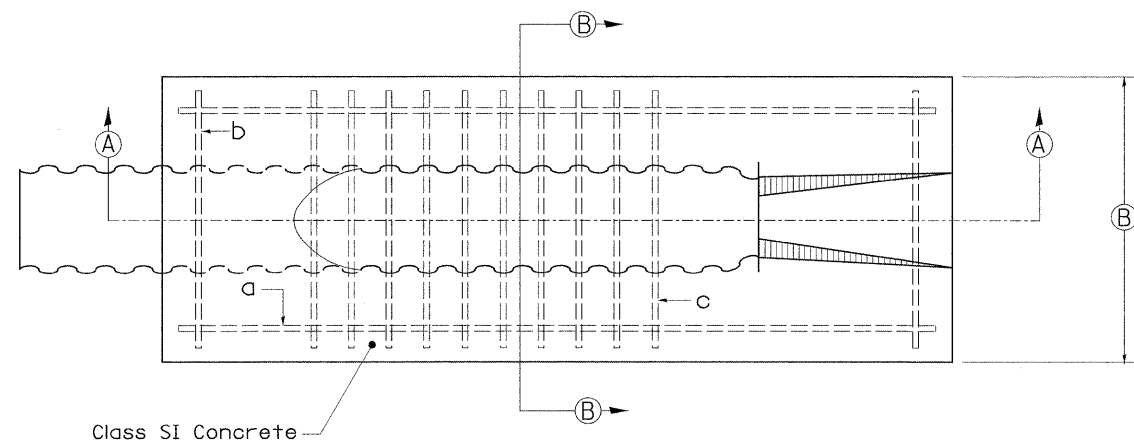
Details shown hereon are for the construction of beveled pipe and guard. Alternate designs, methods of construction or materials may be submitted to the Engineer for approval. All methods of construction and materials involved shall conform to current Standard Specifications.

Reinforcing steel used in construction of "Beveled Pipe and Guard" shall be deformed bars meeting the requirements of Article 1006.10 of the Standard Specifications. All steel bars shall be hot-dip galvanized in accordance with ASTM A 123 specifications.

Concrete used in construction of the beveled pipe and guard shall be Class "SI" Concrete.

The corrugated metal pipe shall be cut to fit the 1:8 foreslope. Slots shall be cut into the C.M.P. for placement of the 'b' and 'c' bars. After the foreslope has been placed, the 'b' and 'c' bars shall be fitted into the slots cut in the C.M.P. so they will be in proper position when the concrete collar is poured.

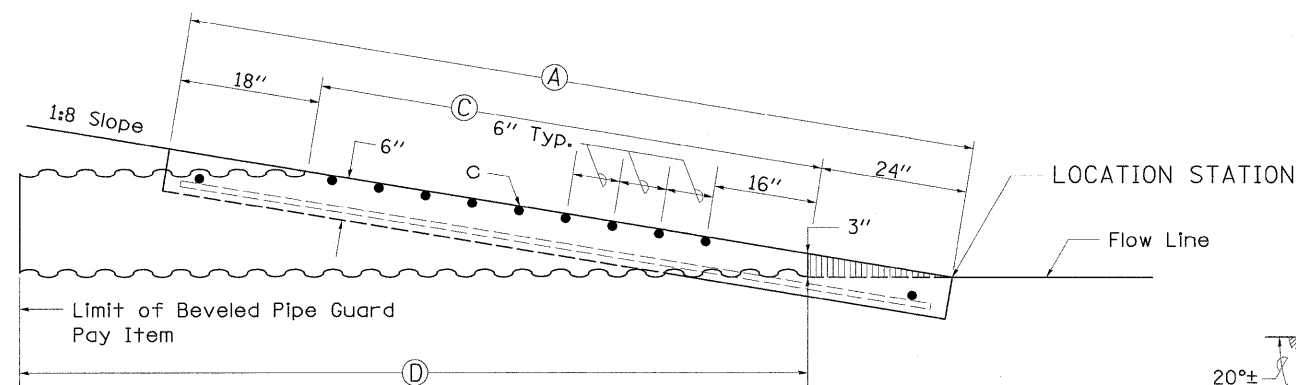
This work shall be paid for at the contract unit price per Each for "Beveled Pipe and Guard", as shown hereon and as directed by the Engineer.



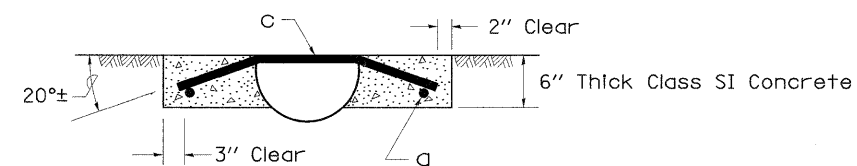
PLAN VIEW

12 PIPE REINFORCING SCHEDULE			
Mark Req'd	Bar Size	Length	No.
a	5	110	2
b	5	32	2
c	8	34	10

18 PIPE REINFORCING SCHEDULE			
Mark Req'd	Bar Size	Length	No.
a	5	162	2
b	5	38	2
c	8	40	18



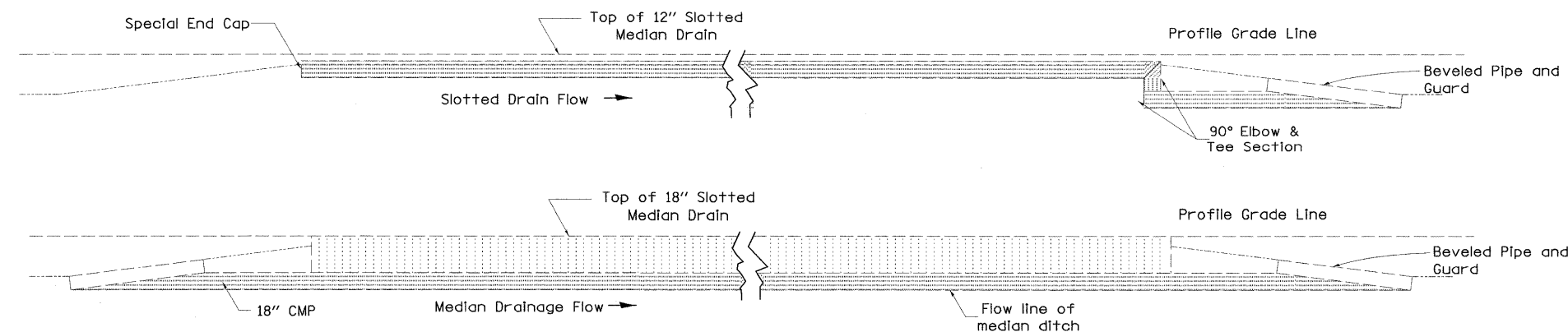
SECTION A-A



SECTION B-B

TABLE OF DIMENSIONS				
PIPE SIZE	A	B	C	D
12	9' - 6"	36	6'	10'
18	13' - 10"	42	10' - 4"	14' - 10"

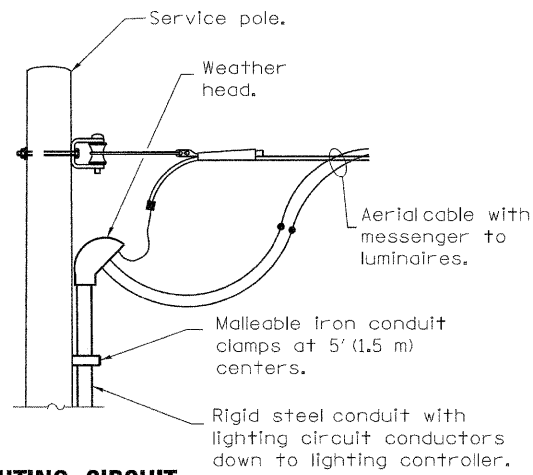
## TYPICAL SECTION THRU CENTERLINE OF MEDIAN CROSSOVER



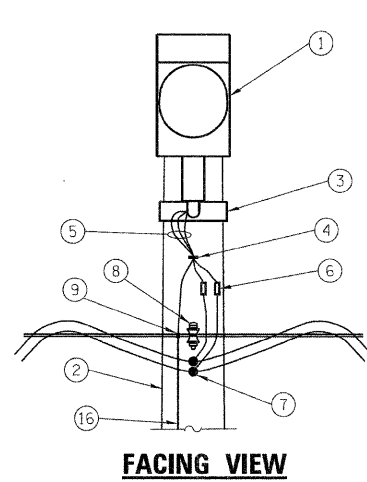
ALL DIMENSIONS ARE IN INCHES UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = pearc00397	DESIGNED - BJB	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 2 STANDARDS I-39 OVER KISHWAUKEE RIVER</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwise\work\do_not_delete\dms59254\0264472-shr-details04.dgn	PLOT SCALE = 50.0000' / in.	DRAWN - BJB	REVISED -			39	(201-1B)M-3	WINNEBAGO	38	26	
PLOT DATE = 02/09/2012	DATE - 2/9/12	CHECKED - JMH	REVISED -			CONTRACT NO. 64H72					
						ILLINOIS FED. AID PROJECT					

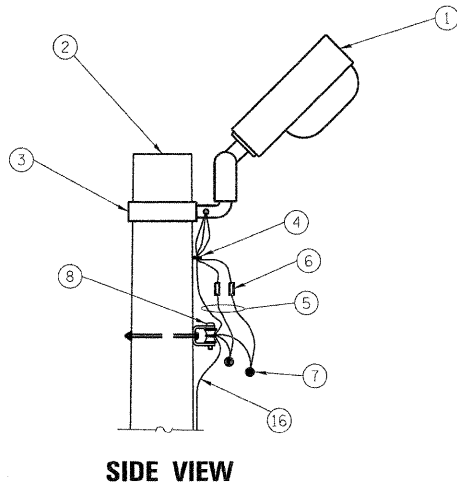
# TEMPORARY ROADWAY LIGHTING



**LIGHTING CIRCUIT AT SERVICE/CONTROLLER**  
See standard 825001 for service installation.

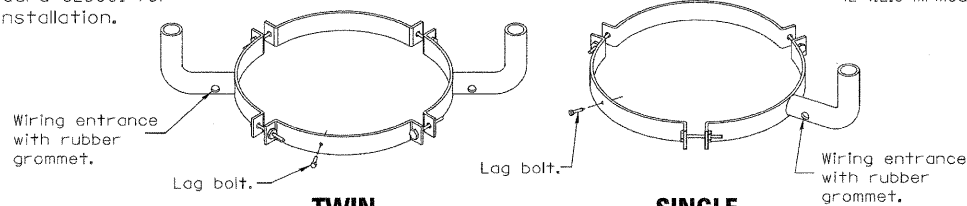


**FACING VIEW**

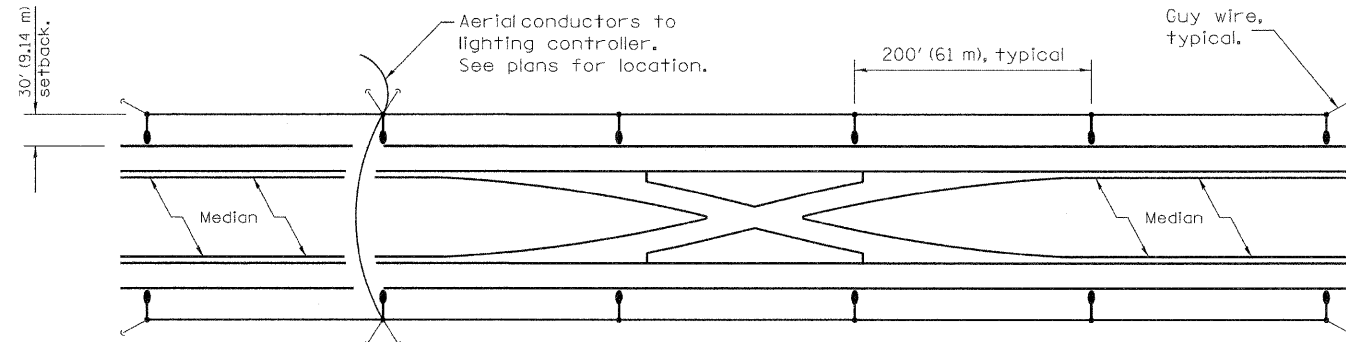


**SIDE VIEW**

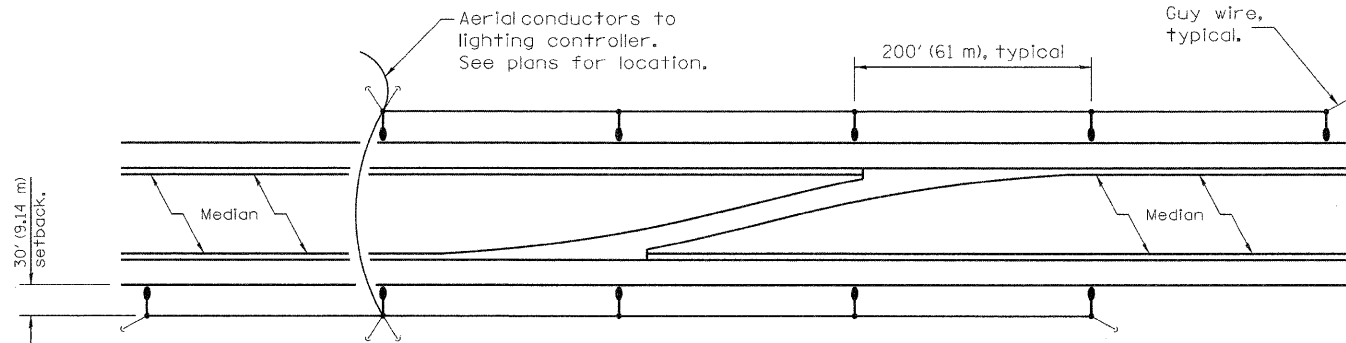
**LUMINAIRE MOUNTING DETAILS**  
42' (12.8 m) mounting height.



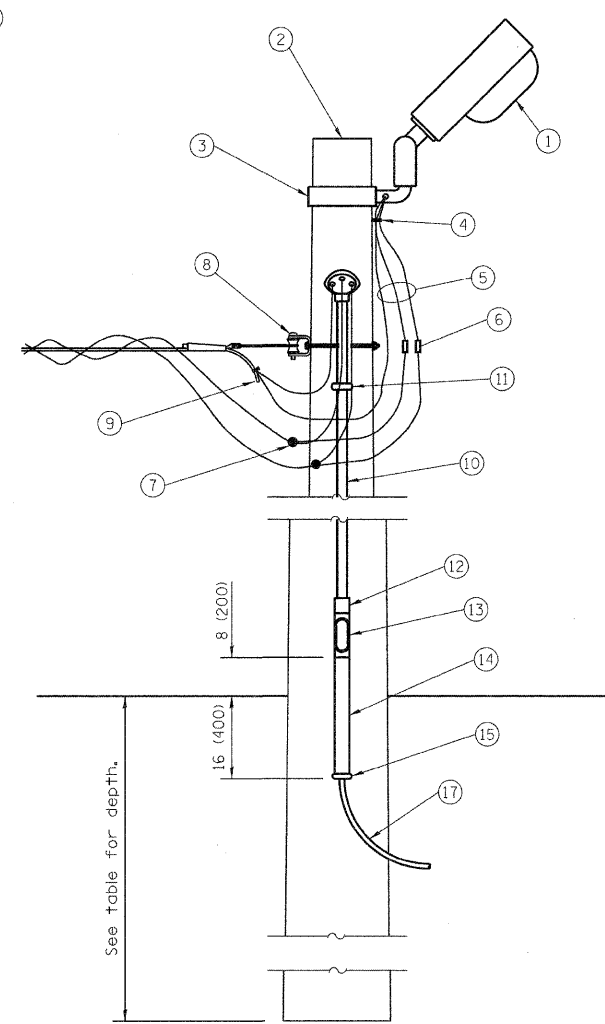
**MOUNTING BRACKET DETAILS**



**DUAL CROSSOVER**



**SINGLE CROSSOVER**



**LIGHT POLE WITH CIRCUIT ROUTED UNDERGROUND**

- ① Luminaire.
- ② Wood light pole.
- ③ Luminaire mounting bracket.
- ④ Cable clamps on 24 (600) centers.
- ⑤ Three #10 XLP-USE cable.
- ⑥ Waterproof fuse holder with fuse.
- ⑦ Waterproof insulation piercing tap connector.
- ⑧ Heavy duty insulated pulley devis with mounting bolt and hardware.
- ⑨ Ground clamp.
- ⑩ 1 (25) rigid steel conduit.\*
- ⑪ Malleable iron conduit clamps, 5' (1.5 m) intervals.
- ⑫ Threaded conduit reducer.
- ⑬ "C" conduit, threaded.
- ⑭ 1/2 (40) rigid steel conduit.
- ⑮ Conduit bushing.
- ⑯ Bare copper ground wire, every third light pole.
- ⑰ Unit duct.

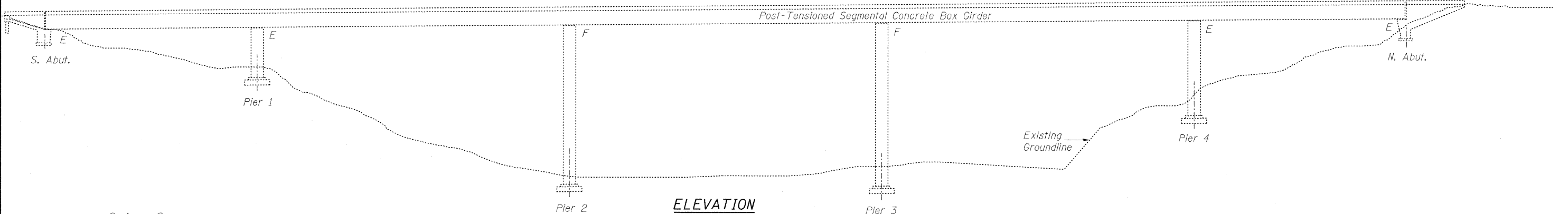
Pole Length	Depth in ground
65'	12'
(19.8 m)	(3.6 m)
60'	10'
(18.0 m)	(3.0 m)
55'	9'
(16.8 m)	(2.7 m)
50'	8'
(16.0 m)	(2.4 m)
45'	7'
(13.7 m)	(2.1 m)
40'	6.5'
(12.0 m)	(2.0 m)
35'	6'
(10.7 m)	(1.8 m)
30'	5.5'
(9.0 m)	(1.7 m)

**GENERAL NOTES**

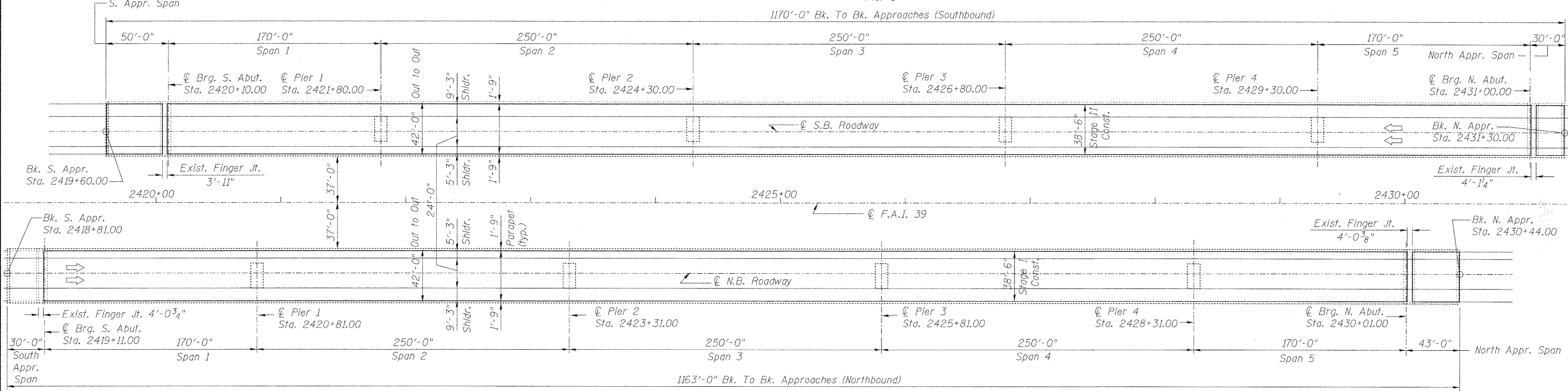
See plans for wire and unitduct sizes not shown.  
All dimensions are in inches (millimeters) unless otherwise shown.

Existing Structure

S.N. 101-0133 (S.B.) and S.N. 101-0134 (N.B.)  
 Built as F.A. Route 412, Section 201-1B in 1980. The Southbound and Northbound superstructures each consist of a 1090'-0" post-tensioned segmental concrete box girder.  
 In 2008 the Structure was reconstructed. The post-tensioned segmental concrete box girders were retrofitted with external post-tensioning, waterproofing membrane and wearing surface were removed and replaced, and a FAST system installed.  
 Traffic shall be maintained on existing S.N. 101-0133 during reconstruction of S.N. 101-0134 and on S.N. 101-0134 during reconstruction on S.N. 101-0133 by use of crossovers.  
 Salvage - FAST system spray disks and piping on Bridge. Materials shall be boxed, labeled and placed inside Pump Station Building, located on-site.



**ELEVATION**



**PLAN**

**PROPOSED SCOPE OF WORK**

1. Replace existing deck overlay and waterproofing membrane
2. Remove Fixed Anti-icing Spray Technology (FAST System) Spray Disks on bridge, remove & reinstall sensors on bridge, remove & reinstall spray disks on approach spans.
3. Modify Electrical System to install Concrete Deviator Restraints.
4. Install Concrete Deviator Restraints

**DESIGN SPECIFICATIONS**

**NEW CONSTRUCTION**

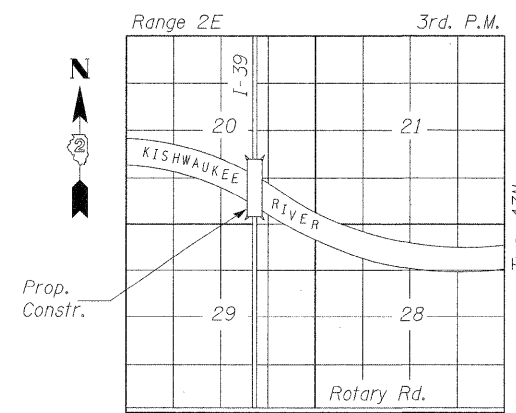
2010 AASHTO LRFD Bridge Design Specifications, 5th Edition with 2010 Interims.

**DESIGN STRESSES**

**FIELD UNITS (NEW CONST.)**

$f'_c = 5,500$  psi (Superstructure - Self Consolidating Concrete)  
 $f_y = 60,000$  psi (Reinf)

**LOADING HS 20-44 (EXIST. CONST.)**



**LOCATION SKETCH**

**APPROVED**  
 FOR STRUCTURAL ADEQUACY ONLY  
 ENGINEER OF BRIDGES AND STRUCTURES



Michael N. Mendenhall  
 SIGNATURE  
 02-08-2012  
 DATE  
 LIC. EXP. DATE: 11-30-2012

**GENERAL PLAN**  
**I-39 (FAI 39) OVER KISHWAUKEE RIVER**  
**SECTION (201-1B)M-3**  
**WINNEBAGO COUNTY**  
**STATION 2425+00**  
**STRUCTURE NOS. 101-0133 (S.B.) & 101-0134 (N.B.)**

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LAYOUT  
 DRAWN  
 REVIEWED

LAYOUT	///
DRAWN	///
REVIEWED	///

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISED
		CHECKED - TEH	REVISED
		DRAWN - Rod (Parson's)	REVISED
		CHECKED - MNM	REVISED

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

SHEET NO. 01 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	28
CONTRACT NO. 64H72			ILLINOIS FED. AID PROJECT	

**GENERAL NOTES**

Reinforcement bars designated (E) shall be epoxy coated.

Access to the box girder interiors is provided by a 3'x3' access opening in the bottom slab of the box girder near each abutment and an entry door at the N. Abut. (SB) & S. Abut. (NB) bridge.

All construction joints shall be bonded in accordance with Section 503.09(b), the surface of existing concrete shall have the cement paste removed to create a prepared surface. The Surface shall be Prepared by washing with water under pressure or by sandblasting to expose clean, well bonded aggregate C. All box girder faces where the concrete deviator restraint is cast against, shall be bonded. The prepared surface of the existing concrete shall be wetted according to the Standard Specifications.

Type I and Type II Deviator Restraints shall be self-consolidating concrete and shall be poured and/or pumped through optional hole locations shown in these plans. The Contractor may propose a new location that shall be approved by the Engineer. The Contractor shall verify all hole locations used, miss existing reinforcement, and existing post-tensioning bars. This also includes all optional locations shown in plans. Any necessary repairs shall be carried out as directed by the Engineer using an approved non-shrink grout. Cored holes shall be completely filled with an approved non-shrink grout. This work shall be included in the cost of Concrete Superstructures.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

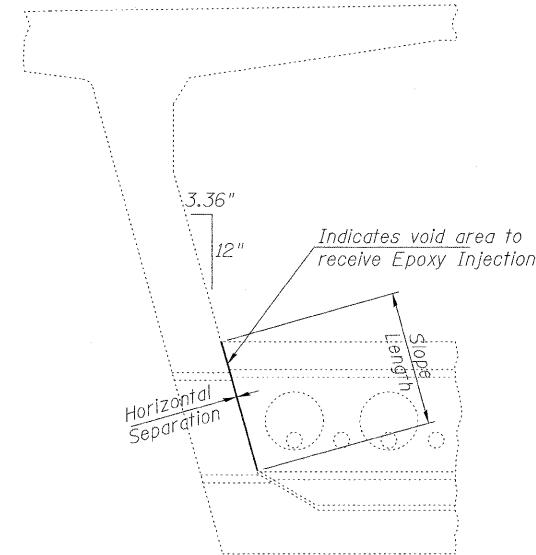
The Contractor shall miss existing post-tensioning components and reinforcement when drilling holes in the existing structure. Proposed Epoxy Grouted Bars shall have a minimum clearance gap of 1" to existing conventional reinforcement and a minimum clearance gap of 1/2" to existing post-tensioning bars. The location of the post-tensioning bars and reinforcement shall be determined by the contractor using non-destructive methods approved by the Engineer. No drilling is permitted without approval by the Engineer. Holes drilled in existing concrete shall have the epoxy grouted reinforcement bars immediately installed. This work shall be in accordance with Section 584 of the Standard Specifications.

**INDEX of SHEETS**

DRAWING NO.	TITLE
1	GENERAL PLAN
2	GENERAL DATA
3	STAGE CONSTRUCTION DETAILS
4	TEMPORARY CONCRETE BARRIER
5	DEVIATORS - LAYOUT and MODIFICATION LOCATIONS
6	TYPE I DEVIATOR RESTRAINT DETAILS
7	TYPE II DEVIATOR RESTRAINT DETAILS
8	DECK OVERLAY - REPLACEMENT
9	FAST SYSTEM SITE PLAN
10	FAST SYSTEM DETAILS
11	INTERIOR ELECTRICAL LIGHTING SYSTEM MODIFICATIONS

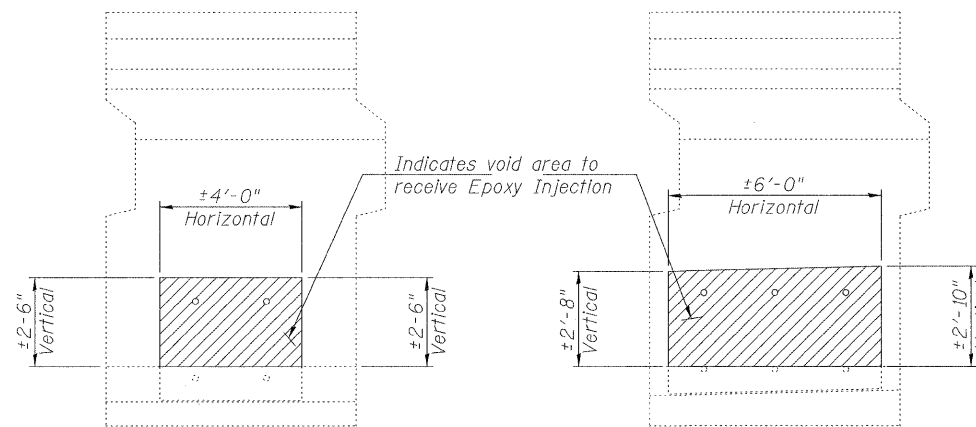
**TOTAL BILL of MATERIAL**

ITEM	UNIT	STAGE 1	STAGE 2	TOTAL
		S.N. 101-0134 NORTHBOUND	S.N. 101-0133 SOUTHBOUND	
Polymerized Hot-Mix Asphalt Surface Course, Mix "E", NI05	Ton	562	578	1,140
Concrete Superstructure	Cu. Yd.	48.8	48.7	97.5
Reinforcement Bars, Epoxy Coated	Pound	8,570	8,570	17,140
Relocate Existing Electrical System	L. Sum	0.5	0.5	1
Waterproofing Membrane System (Special)	Sq. Yd.	4,831	4,861	9,692
Hot-Mix Asphalt Surface Removal Complete	Sq. Yd.	4,831	4,966	9,797
Partially Remove and Reinstall Fixed Anti-Icing Spray Technology (FAST) System	L. Sum	0.5	0.5	1
Epoxy Injection	Each	4	16	20



Note:  
Only Deviator Ends Listed w/ Horizontal Separations shall be Epoxy Injected. Horizontal Separation Field Measurements shown shall be for estimating purposes only. Any deviation from these measurements at the time of injection shall not be a cause for additional compensation, but shall be included in the unit price of "Epoxy Injection".

**PARTIAL DEVIATOR END ELEVATION  
EPOXY INJECTION PLACEMENT**



**DEVIATOR TYPE I  
DEVIATOR TYPE II  
DEVIATOR END VIEWS - EPOXY INJECTION PLACEMENT**

**BILL OF MATERIAL**

Item	Unit	S.B.	N.B.	Total Quantity
Epoxy Injection	Each	16	4	20

**Epoxy Injection S.B.L. I-39**

Deviator Number	Deviator Type	Horizontal Separation	
		West End	East End
1	I	1/16"	1/8"
2	I	1/16"	3/16"
3	II	1/8"	-
4	II	-	1/8"
5	I	-	3/16"
6	I	-	1/8"
7	II	-	1/8"
8	II	-	-
9	I	-	1/8"
10	I	-	1/16"
11	II	-	-
12	II	-	-
13	I	-	-
14	I	-	-
15	II	-	1/8"
16	II	-	1/8"
17	I	3/16"	3/8"
18	I	1/8"	-

**Epoxy Injection N.B.L. I-39**

Deviator Number	Deviator Type	Horizontal Separation	
		West End	East End
1	I	-	-
2	I	1/8"	-
3	II	-	-
4	II	-	-
5	I	3/16"	-
6	I	-	-
7	II	-	-
8	II	-	-
9	I	1/8"	-
10	I	-	-
11	II	-	-
12	II	-	-
13	I	1/8"	-
14	I	-	-
15	II	-	-
16	II	-	-
17	I	-	-
18	I	-	-



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02/07/2012  
nrc/2012

LAYOUT	/
DRAWN	/
REVIEWED	/

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISED
		CHECKED - TEH	REVISED
		DRAWN - Rod (Parson's)	REVISED
		PLOT DATE =	REVISED

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL DATA  
STRUCTURE NO. 101-0133 and 101-0134**  
SHEET NO. 02 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	29
CONTRACT NO. 64H72				
ILLINOIS FED. AID PROJECT				

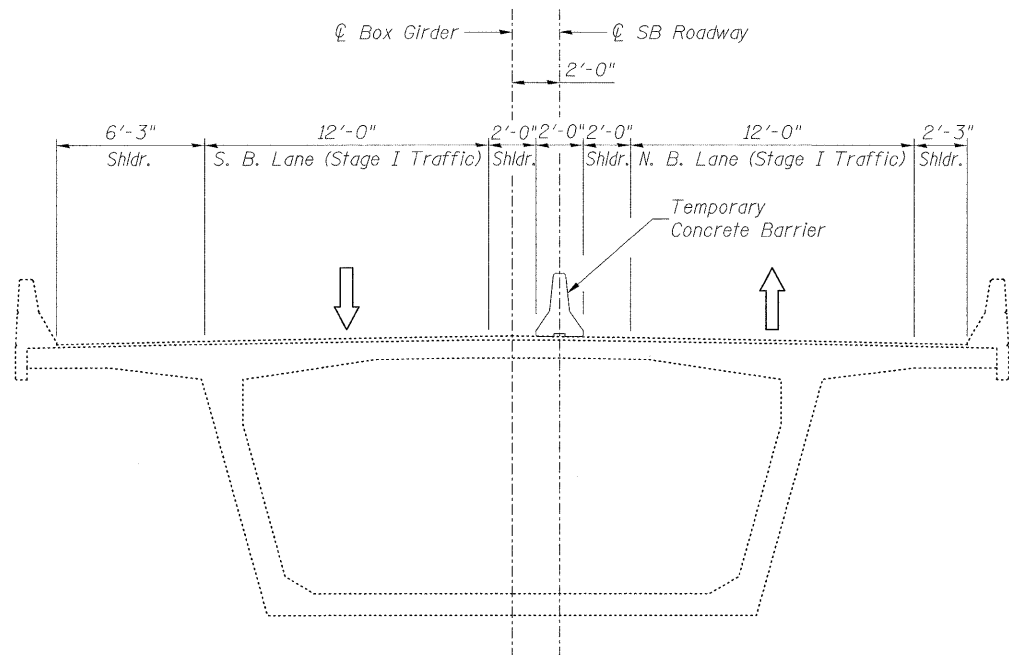




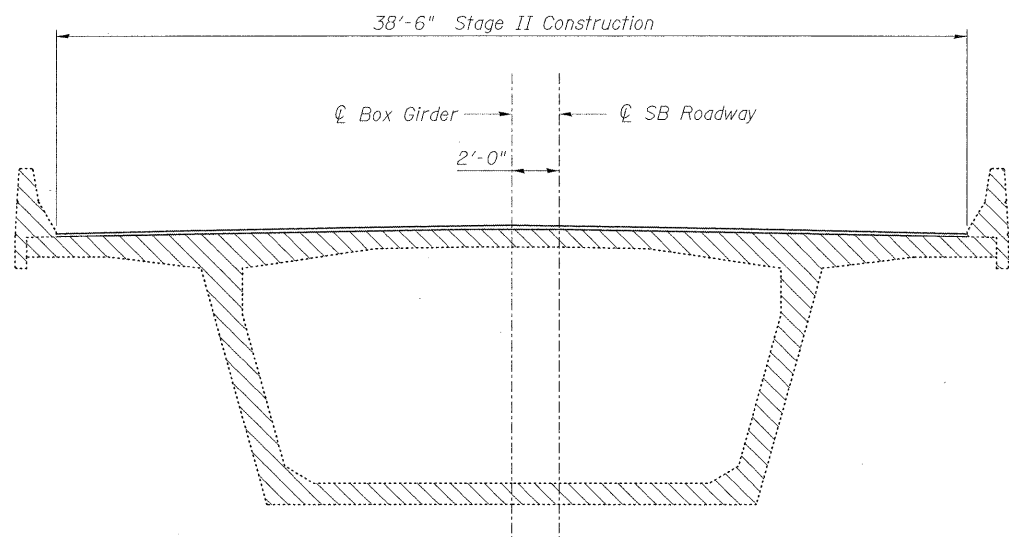
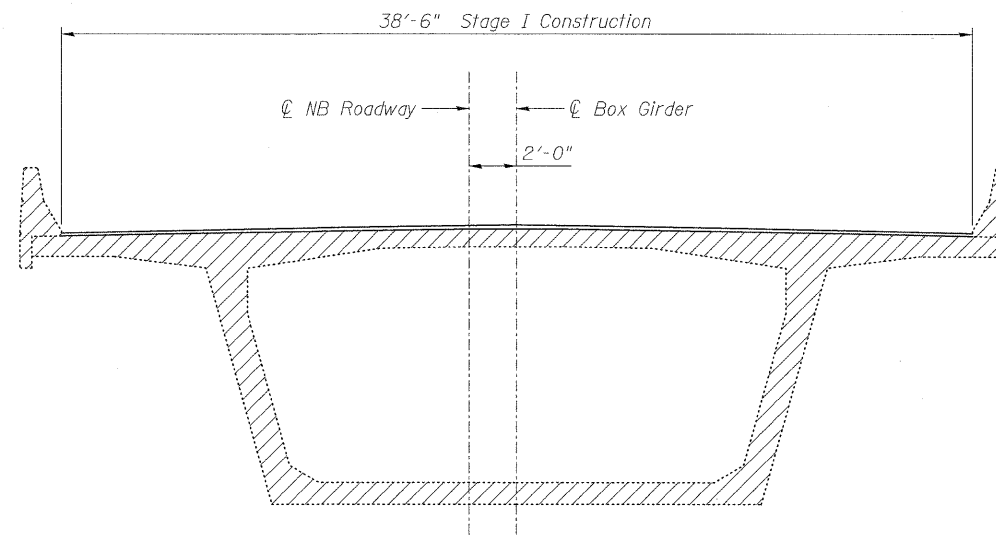
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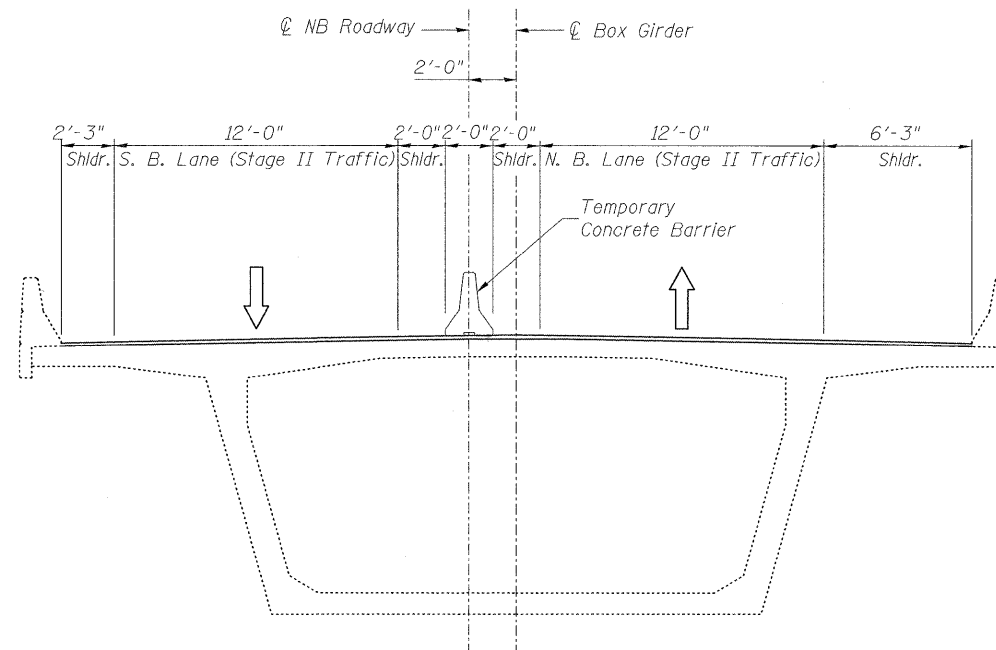
LAYOUT  
 DRAWN  
 REVIEWED



**STAGE I - CONSTRUCTION**  
 (Looking North)



**STAGE II - CONSTRUCTION**  
 (Looking North)



**LEGEND:**

Construction in this Stage

**NOTES:**  
 Details for Temporary Concrete Barrier are on sheet 4 of 11.  
 See Roadway Plans for Temporary Concrete Barrier Quantities.

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISED
		CHECKED - TEH	REVISED
		DRAWN - Red (Parson's)	REVISED
		CHECKED - MNM	REVISED

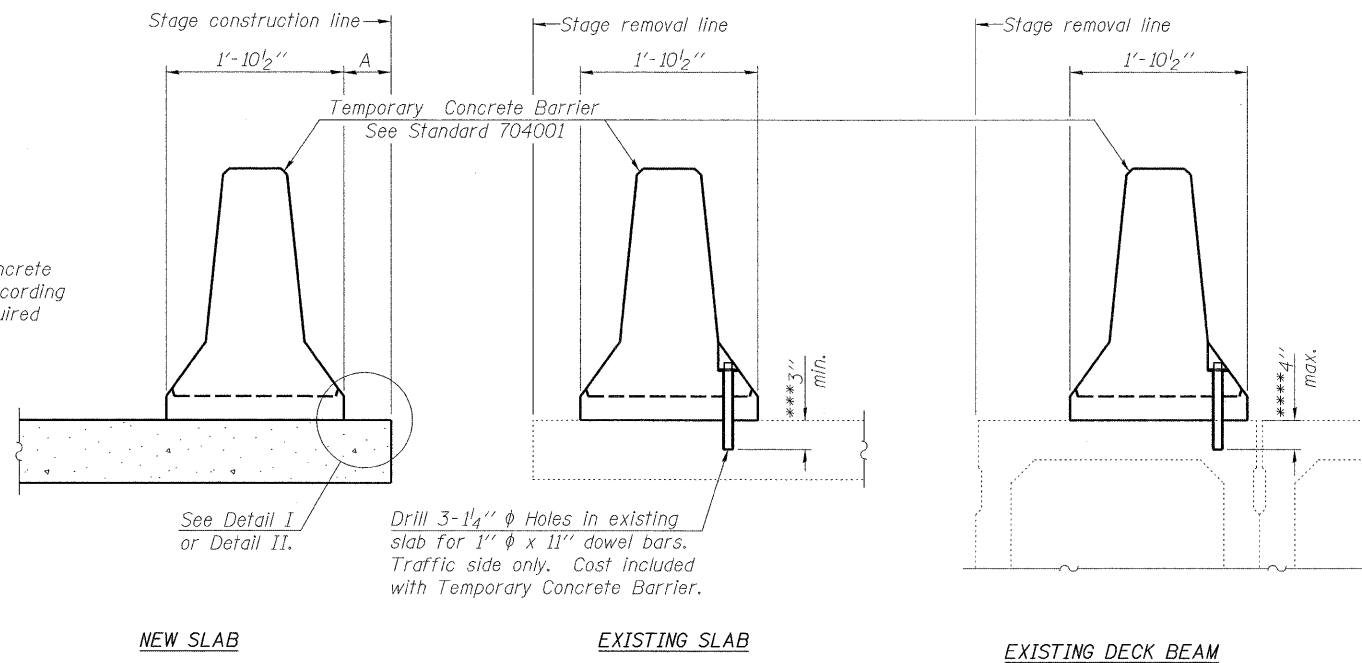
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS  
 STRUCTURE NO. 101-0133 and 101-0134**

SHEET NO. 03 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B)M-3	WINNEBAGO	38	30
CONTRACT NO. 64H72			ILLINOIS FED. AID PROJECT	

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



**SECTIONS THRU SLAB OR DECK BEAM**

**NOTES**

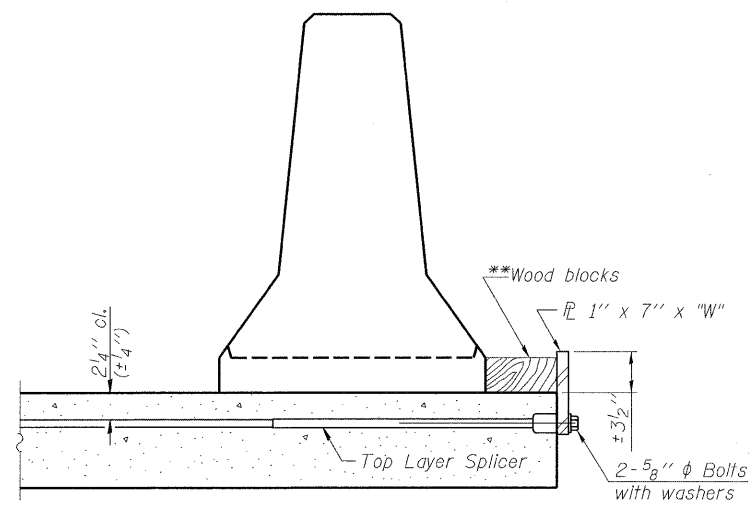
Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1" x 7" x "W" steel  $\bar{P}$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\bar{C}$  of each barrier panel.

Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1" x 7" x "W" steel  $\bar{P}$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\bar{C}$  of each barrier panel.

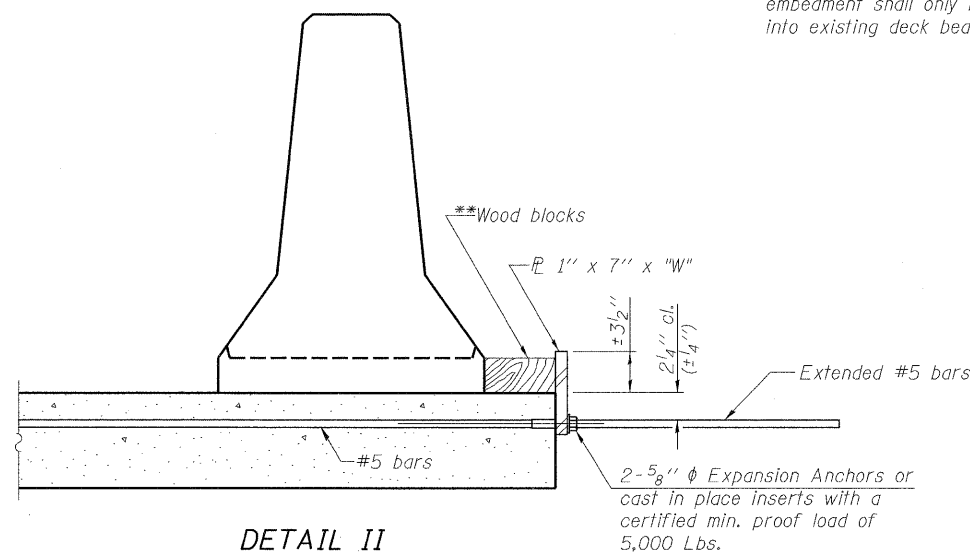
Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x "W" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

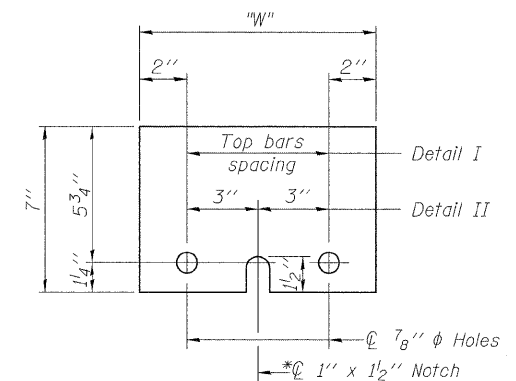
\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



**DETAIL I**



**DETAIL II**



**STEEL RETAINER  $\bar{P}$  1" x 7" x "W"**

\* Required only with Detail II

\*\* Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

"W" = Top bars spacing + 4"

R-27

7-1-10

LAYOUT	✓
DRAWN	✓
REVIEWED	✓

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISED _____
		CHECKED - TEH	REVISED _____
		DRAWN - Rod (IDOT)	REVISED _____
		CHECKED - MNM	REVISED _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

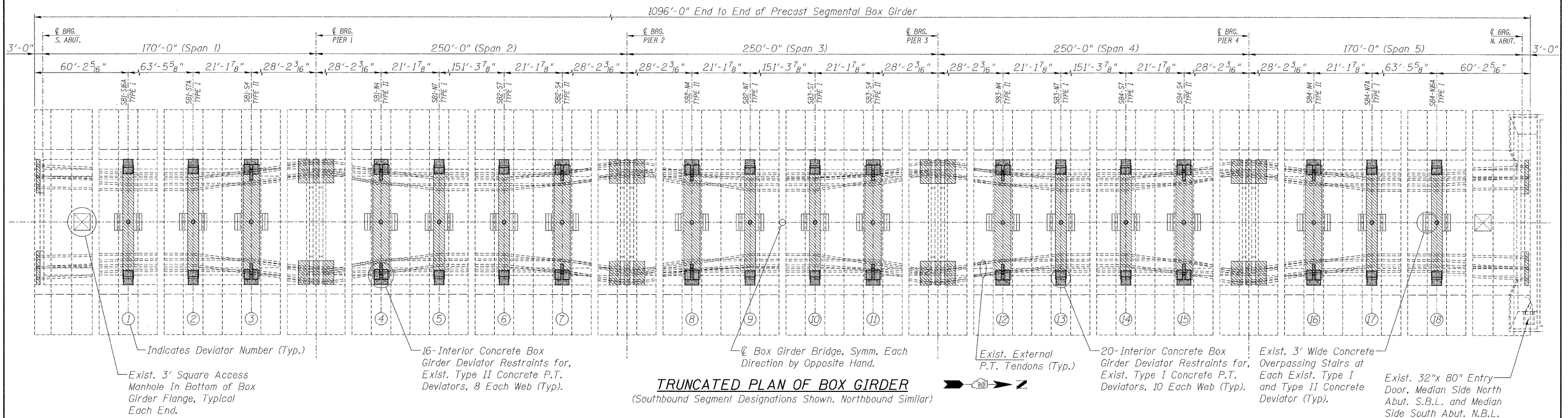
**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION  
STRUCTURE NO. 101-0133 AND 101-0134**

SHEET NO. 04 OF 11 SHEETS

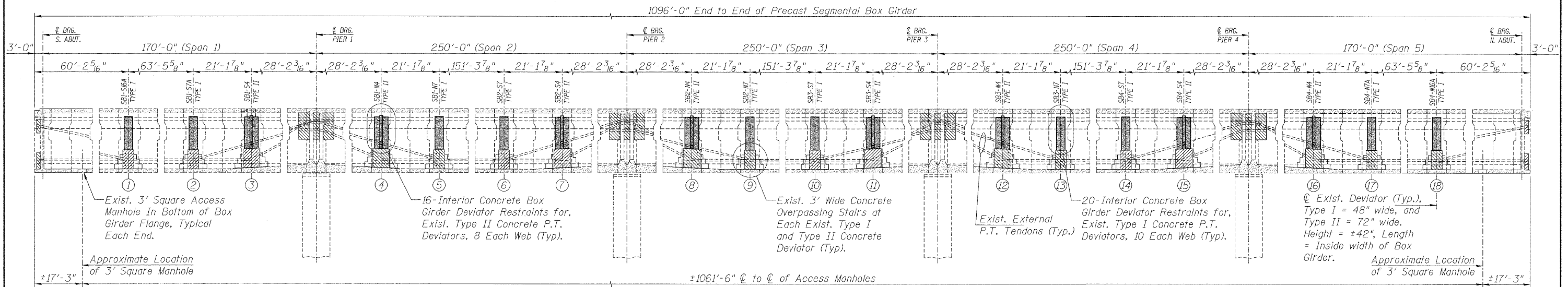
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1BIM-3)	WINNEBAGO	38	31
CONTRACT NO. 64H72			ILLINOIS FED. AID PROJECT	



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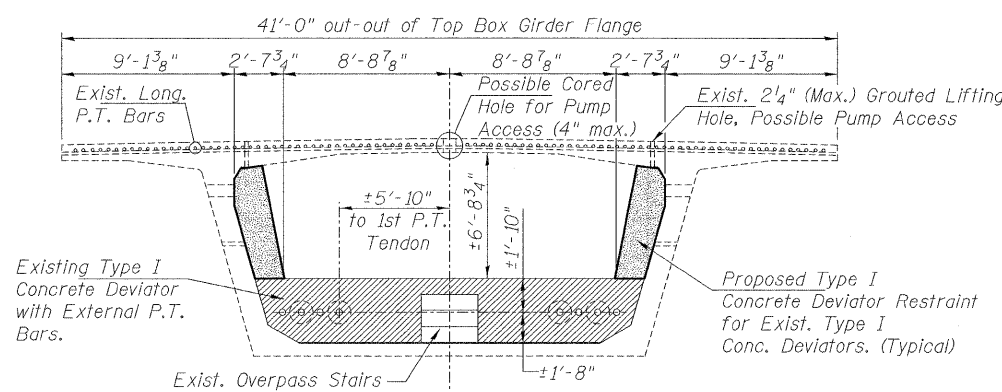


**TRUNCATED PLAN OF BOX GIRDER**  
(Southbound Segment Designations Shown, Northbound Similar)

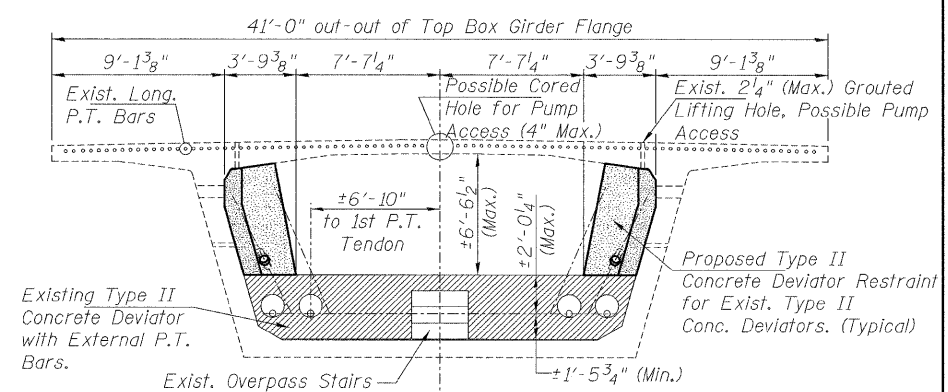


**TRUNCATED LONGITUDINAL SECTION THRU BOX GIRDER**  
(Southbound Segment Designations Shown, Northbound Similar)

- Notes:
- Supplemental External P.T. Tendons and Concrete Deviators (Hatched) added to the Interior of the Existing Box Girder in 2008.
  - Structural Modifications made in 2008:
    - Upper and Lower Concrete Anchorages at Abutments.
    - Upper Concrete Deviator/Anchorages at Piers
    - Lower Concrete Deviators Types I & II in all spans.
    - Concrete Overpass Stairs at all added Concrete Deviators.
    - Transverse Post Tension Bars in added Conc. Deviators.
  - Concrete Deviator Restraints to be added to all Existing Lower Concrete Type I & II Deviators.
  - Note that the exact location of P.T. Tendons that pass over the Type II Deviators and pass through the Type II Deviator Restraints may vary from what is shown. The Contractor may need to make Reinforcement and Forming Adjustments, any necessary adjustments shall be submitted to and approved by the Engineer prior to making adjustment.
  - SB1-S16A Indicates Original Shop Drawing Labeled Segment Piece Numbers, NB=Northbound and SB=Southbound (Typ.)



**TYPICAL SECTION AT DEVIATOR - TYPE I**  
(Typical for 10 Locations, Each Structure)



**TYPICAL SECTION AT DEVIATOR - TYPE II**  
(Typical for 8 Locations, Each Structure)

LAYOUT	///
DRAWN	///
REVIEWED	///

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISED
		CHECKED - TEH	REVISED
		DRAWN - Rod	REVISED
		CHECKED - MNM	REVISED

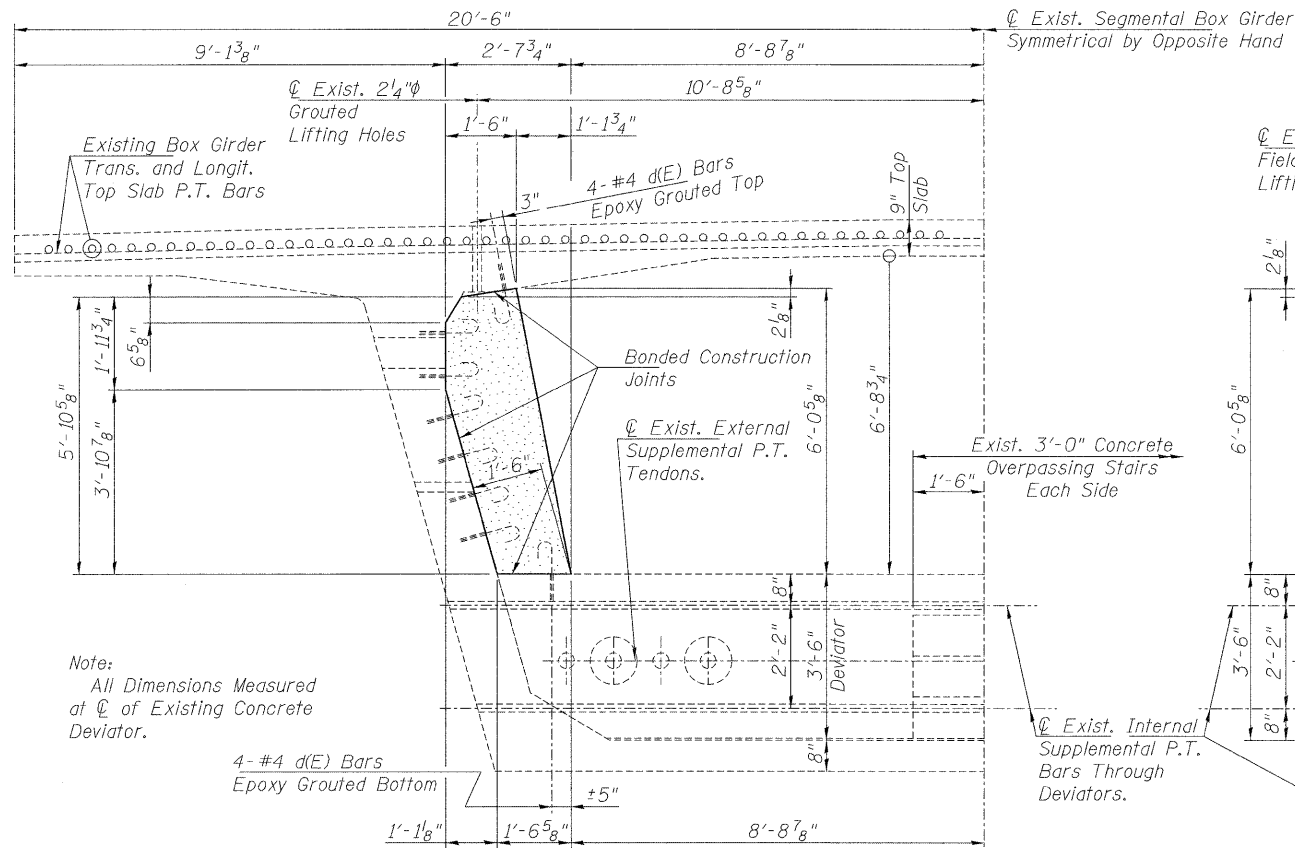
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DEVIATORS - LAYOUT and MODIFICATION LOCATIONS  
STRUCTURE NO. 101-0133 and 101-0134

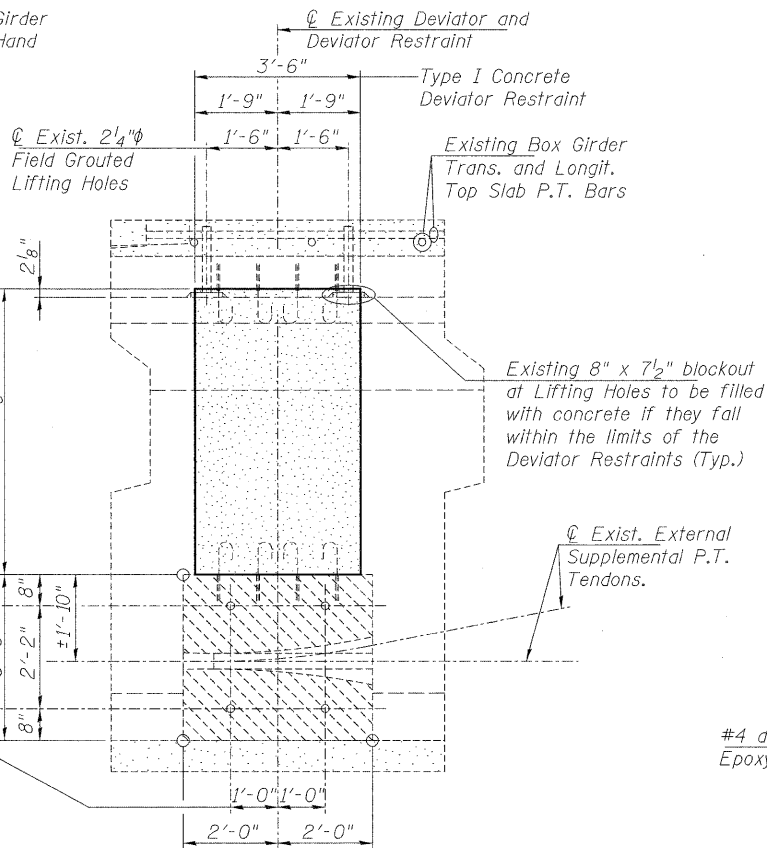
SHEET NO. 05 OF 11 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-1B1M-3)	WINNEBAGO	38	32
CONTRACT NO. 64H72				

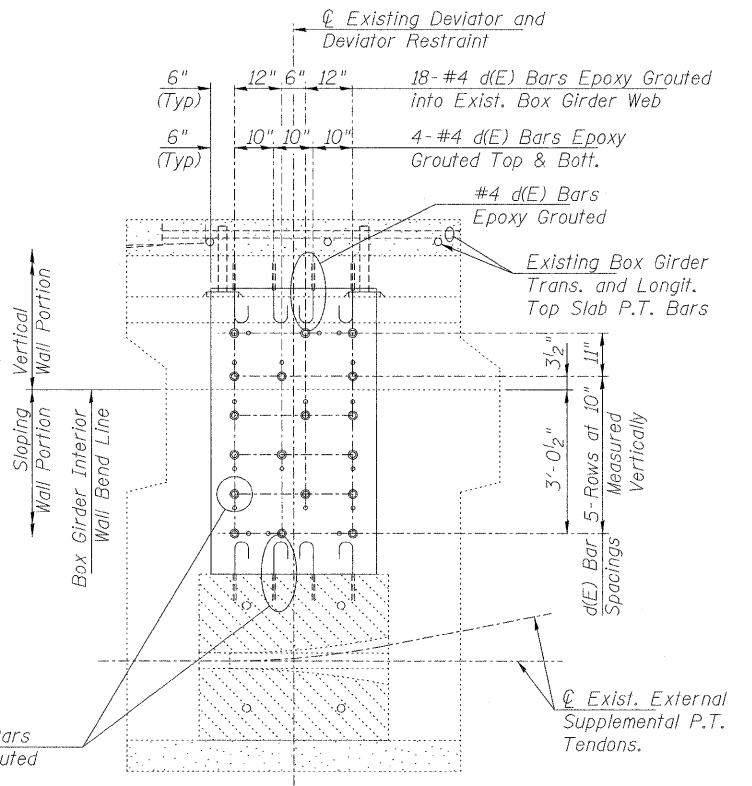
ILLINOIS FED. AID PROJECT



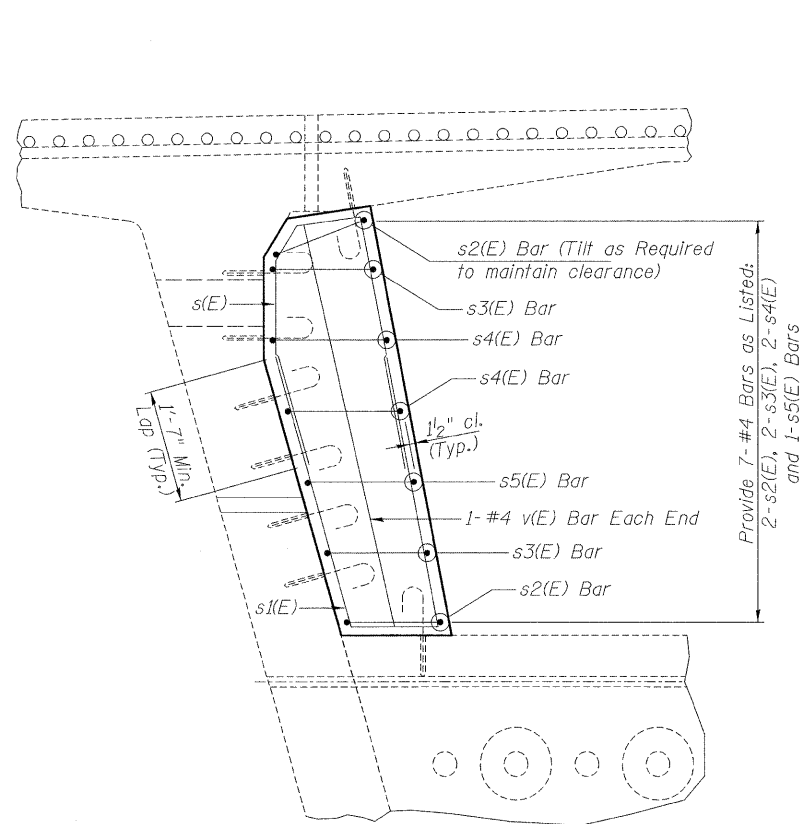
**HALF BOX GIRDER SECTION - DEVIATOR RESTRAINT - TYPE I**  
(Showing Concrete Deviator Restraint at Exist. Deviator 2, Looking North)



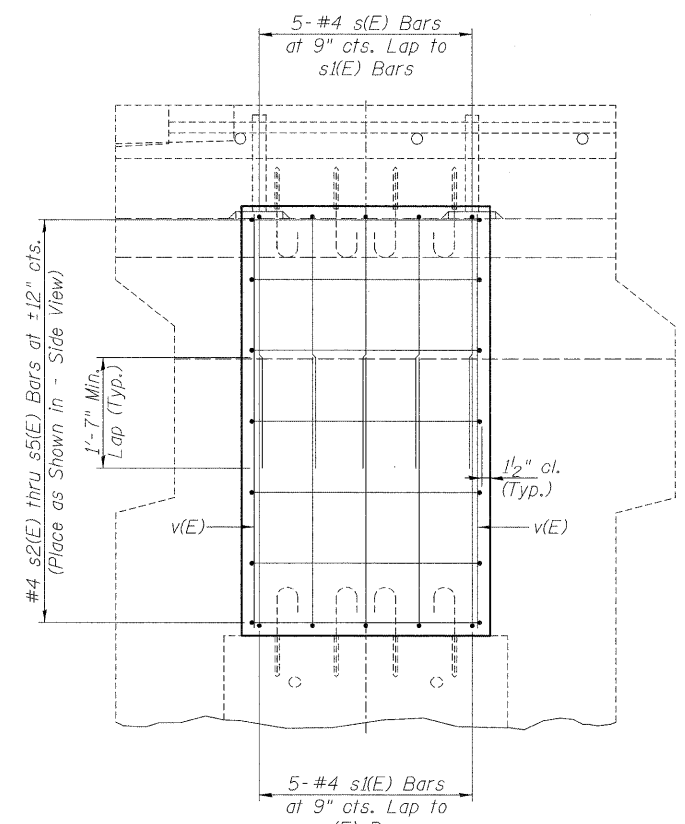
**ELEVATION VIEW - DEVIATOR RESTRAINT - TYPE I**  
(Showing Concrete Deviator Restraint at Exist. Deviator 2, Looking West)



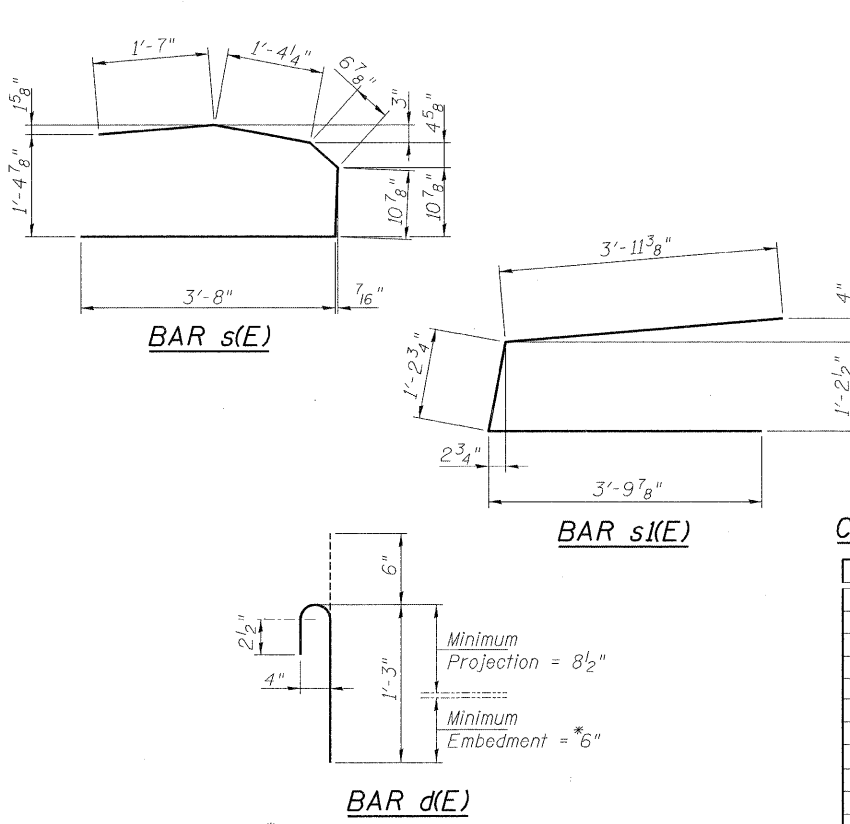
**ELEVATION VIEW - BOX GIRDER WEB - TYPE I**  
(Showing Epoxy Grouted Dowel Bar Locations, Exist. Deviator 2, Looking West)



**SIDE VIEW - DEVIATOR RESTRAINT - TYPE I**  
(Showing Deviator Restraint Reinforcement, Exist. Deviator 2, Looking North)



**ELEVATION - DEVIATOR RESTRAINT - TYPE I**  
(Showing Deviator Restraint Reinforcement, Exist. Deviator 2, Looking West)



**BARS s2(E) thru s5(E)**

Bar	A	B
s2(E)	3'-3"	1'-4"
s3(E)	3'-3"	1'-5"
s4(E)	3'-3"	1'-7 1/2"
s5(E)	3'-3"	1'-6"

**BILL OF MATERIAL**  
**40 - TYPE I**  
**CONCRETE DEVIATOR RESTRAINTS**

Bar	Number	Size	Length	Shape
*d(E)	1040	#4	1'-9"	[Shape]
s(E)	200	#4	8'-1"	[Shape]
s(I(E))	200	#4	9'-0"	[Shape]
s2(E)	80	#4	9'-11"	[Shape]
s3(E)	80	#4	10'-1"	[Shape]
s4(E)	80	#4	10'-6"	[Shape]
s5(E)	40	#4	10'-3"	[Shape]
v(E)	80	#4	5'-9"	[Shape]
Concrete Superstructure			C.Y.	52.8
Reinforcement Bars, Epoxy Coated			Lb.	5710

**BAR d(E)**  
\*Bars epoxy grouted shall have an embedment sufficient to obtain 1.25 x the yield strength of the reinforcing bar.









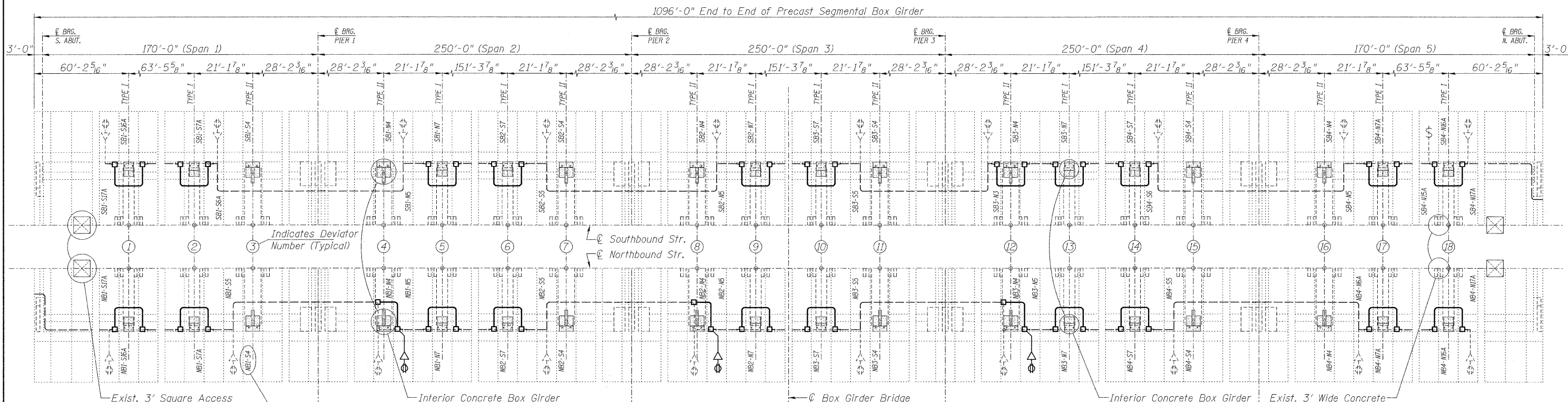




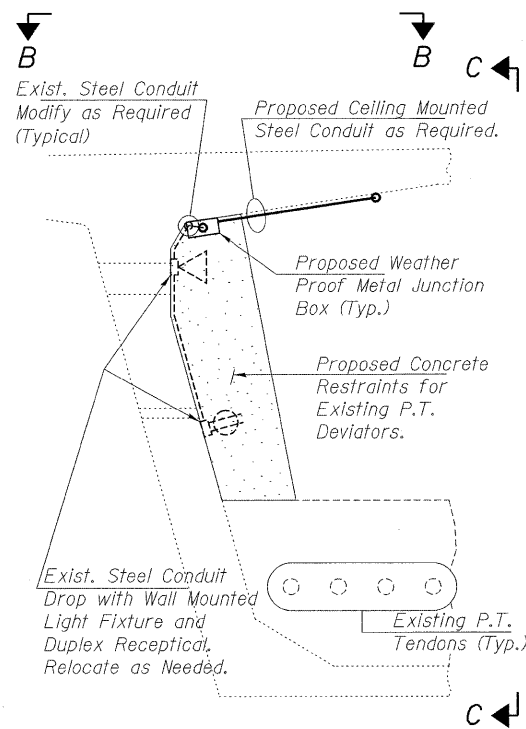


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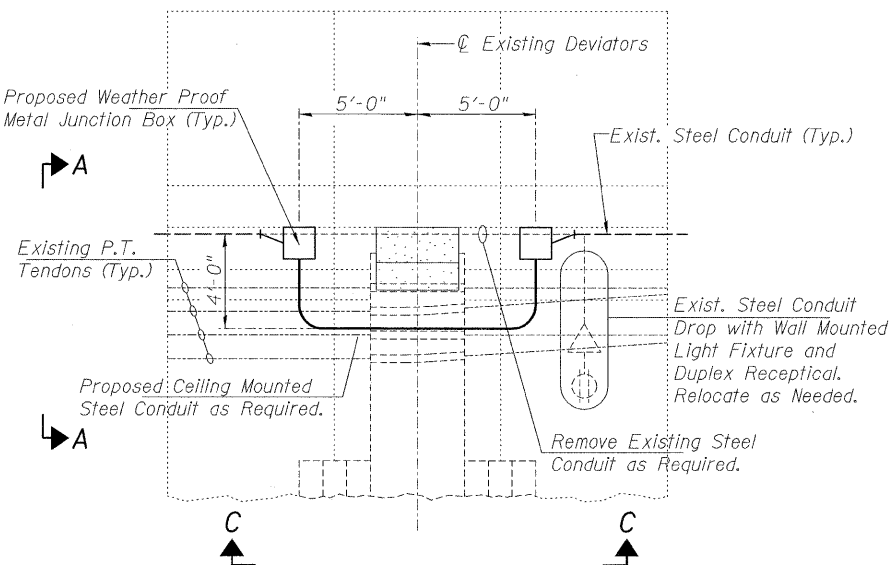
02/04/2012  
02/04/2012  
02/04/2012



**TRUNCATED HALF-PLAN VIEWS OF BOX GIRDERS**  
(Southbound Segments Upper Half and Northbound Segments Lower Half)



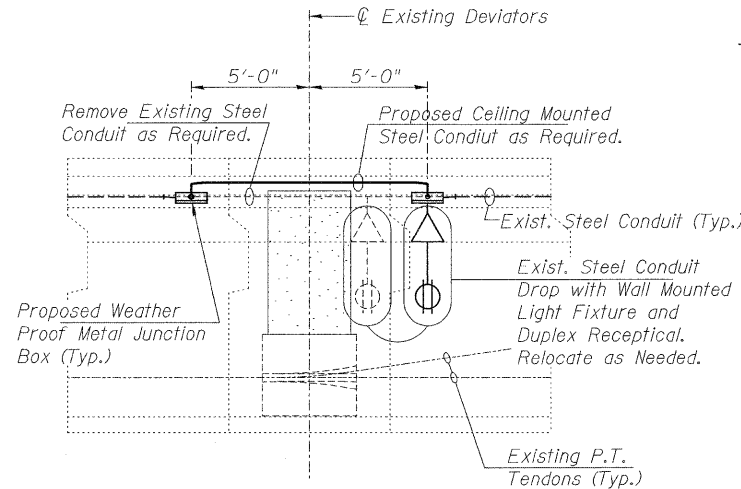
**PARTIAL BOX GIRDER SECTION SECTION A-A**  
(S.B.L. Segment SBI-57A Shown all other Similar)



**PARTIAL REFLECTED CEILING PLAN SECTION B-B**  
(Southbound Segment SBI-57A Shown all other Similar)

**BILL OF MATERIAL**

Item	Unit	S.B.	N.B.	Total Quantity
Relocate Existing Electrical System	L.S.	0.5	0.5	1.0



**PARTIAL BOX GIRDER WEB ELEVATION VIEW C-C**  
(Southbound Segment SBI-57A Shown all other Similar)

**NOTES:**

The Contractor shall complete all work affecting the ceiling and wall areas above and adjacent to the Existing P.T. Deviators within the Box Girders prior to construction of the Deviator Restraints.

The conduit rerouting plan shown is intended to provide general requirements for relocating the existing conduits to avoid work areas. The Contractor shall confirm the number and location of existing conduits in the field.

The Contractor shall get the approval of the Engineer prior to relocating conduit.

**LEGEND**

- Indicates a Proposed Ceiling Attached Steel Conduit Bypass for Existing Fillet/Wall Mounted Steel Conduit at Each Deviator Restraint, 11 Required S.B.L. Structure, and 10 Required N.B.L. Structure, a Total of 21.
- Indicates a Known Existing Steel Conduit Drop with Wall Mounted Light Fixture and Duplex Receptical - To Remain.
- Indicates a Known Existing Steel Conduit Drop to a Wall Mounted Switch - To Remain.
- Indicates Known Existing Fillet/Wall Mounted Steel Conduit Extending the Full Interior Length of Each Structure.
- Indicates Proposed Ceiling Mounted Steel Conduit for Each Bypass Loop.
- Indicates a Proposed Weather Proof Ceiling or Wall Mounted Steel Junction Box (Minimum of 2 Per Bypass Loop).
- Indicates a Known Existing Steel Conduit Drop with Wall Mounted Light Fixture and Duplex Receptical - To Be Removed and Relocated. Utilize the New Junction Box as a connection point for the relocated drop if possible 3 locations.

**NOTES (Cont'd.)**

All new conduit provided in order to complete the relocating operations shall be galvanized rigid steel conduit (RMC) of equivalent size to the conduit it is replacing.

The Contractor shall miss Post-Tensioning Bars and Reinforcement when attaching electrical system components to the Web and Top Flange of the Concrete Box Girder.

FILE NAME =	USER NAME =	DESIGNED - MNM	REVISOR	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INTERIOR ELECTRICAL LIGHTING SYSTEM MODIFICATIONS STRUCTURE NO. 101-0133 and 101-0134	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED - TEH	REVISOR			39	(201-1BIM-3)	WINNEBAGO	38	38	
PLOT SCALE =		DRAWN - Rod	REVISOR			CONTRACT NO. 64H72					
PLOT DATE =		CHECKED - MNM	REVISOR			ILLINOIS FED. AID PROJECT					