

I-64 MAINLINE STAGE 5:
WORK PERFORMED CONCURRENTLY WITH I-55 STAGE 5 WORK

MAINTENANCE OF TRAFFIC

- EASTBOUND I-64 MAINLINE**
- REDUCE RAMP 55N64E AND EB I-64 TRAFFIC RIGHT FROM 3 LANES TO 2 LANES BEFORE THE METROLINK BRIDGE.
 - RAMP 55S64E IS OPEN TO TRAFFIC.
- WESTBOUND I-64 MAINLINE**
- ALL LANES OPEN TO TRAFFIC ON WB I-64 EXCEPT FOR 64W70W FLYOVER RAMP.
 - SHIFT EB I-64 TRAFFIC RIGHT FROM 3 LANES TO 2 LANES. CLOSE THE INSIDE LANE AND SHOULDER OF WB I-64 AND SHIFT THE OUTSIDE TWO LANES RIGHT.
 - MAINTAIN TWO LANES OF TRAFFIC ALONG WB I-64.
 - MERGE THE RIGHT TWO LANES OF WB I-64 TO ONE LANE APPROACHING RAMP 64W55S.
 - CLOSE THE INSIDE LANE OF RAMP 64W55S.
 - MAINTAIN ONE LANE OF TRAFFIC ALONG RAMP 64W55S.
 - MAINTAIN TRAFFIC TRAVELING FROM ENTRANCE RAMP E AND TO EXIT RAMP 64W55N.

WORK TO BE COMPLETED THIS STAGE

- EASTBOUND I-64 MAINLINE**
- MILL AND OVERLAY THE EXISTING INSIDE LANE AND SHOULDERS ALONG THE MEDIAN OF EB I-64 FROM APPROXIMATELY "EB I-64" STATION 44+62 TO 18TH STREET BRIDGE APPROACH.
- WESTBOUND I-64 MAINLINE**
- MILL AND OVERLAY THE EXISTING INSIDE LANE OF WB I-64/RAMP 64W55S FROM THE 18TH STREET BRIDGE APPROACH TO THE END TO I-55 BRIDGE APPROACH ON RAMP 64W55S.
 - MILL & OVERLAY / RECONSTRUCTION THE INSIDE SHOULDER OF WB I-64/RAMP 64W55S PER THE LIMITS AS SHOWN ON PLANS.

I-55 MAINLINE STAGE 5:
WORK PERFORMED CONCURRENTLY WITH I-64 STAGE 5 WORK

MAINTENANCE OF TRAFFIC

- SOUTHBOUND/NORTHBOUND I-55 MAINLINE**
- TEMPORARY LANE CLOSURES AS REQUIRED, MAINTAINING TWO LANES IN EACH DIRECTION FOR MAINLINE I-55 TRAFFIC

WORK TO BE COMPLETED THIS STAGE

- SOUTHBOUND/NORTHBOUND I-55 MAINLINE**
- PAVEMENT RESURFACING AND FINAL STRIPING.

I-64 MAINLINE STAGE 6:
WORK PERFORMED CONCURRENTLY WITH I-55 PAVEMENT RESURFACING AND FINAL STRIPING.

MAINTENANCE OF TRAFFIC I-64

- EASTBOUND**
- MERGE LEFT LANE OF RAMP 55N64E INTO INSIDE LANE OF EB I-64
 - SHIFT EB I-64 INSIDE LANE LEFT AND OUTSIDE LANE RIGHT.
 - MAINTAIN TWO LANES OF TRAFFIC ALONG EB I-64.
 - MAINTAIN ACCESS TO EXIT RAMP A.
 - TRAFFIC FROM WB I-64 TO RAMP 64W55N WILL BE DETOURED VIA EXIT RAMP D TO BAUGH AVENUE TO ENTRANCE RAMP E.
- WESTBOUND**
- CLOSE THE OUTSIDE LANE OF WB I-64 BY SHIFTING THE TWO OUTSIDE LANES LEFT: MERGING THE CENTER LANE INTO THE INSIDE LANE.
 - MERGE THE TWO LANES OF WB I-64 TO THE INSIDE LANE.
 - MAINTAIN TRAFFIC FROM ENTRANCE RAMP E TO RAMP 64W55N.
 - RAMP 64W70W TO REMAIN CLOSED TO TRAFFIC.
 - TRAFFIC FROM ENTRANCE RAMP E TO RAMP 64W55S WILL BE DETOURED VIA BAUGH AVENUE TO 15TH STREET TO ST. CLAIR AVENUE TO RAMP I.
 - MAINTAIN ONE LANE OF TRAFFIC ALONG WB I-64 AND RAMP 64W55S.

WORK TO BE COMPLETED THIS STAGE

- EASTBOUND**
- MILL AND OVERLAY THE EB I-64 CENTER LANE APPROXIMATELY FROM 64EB STA 42+13 TO THE 18TH STREET BRIDGE APPROACH.
- WESTBOUND**
- MILL AND OVERLAY THE OUTSIDE LANE AND DESIGNATED SHOULDERS ALONG WB I-64 FROM THE 18TH STREET BRIDGE APPROACH TO "WB I-64" STA 105+15
 - MILL AND OVERLAY CENTER LANE OF WB I-64 RIGHT / LANE OF 64W55S INCLUDING RIGHT SHOULDER APPROXIMATELY FROM STA 105+15 TO I-55 BRIDGE APPROACH ON RAMP 64W55S.

RAMP 64W55S WORK

THE RESURFACING SLOPE CORRECTION OF RAMP 64W55S WILL REQUIRE A FULL CLOSURE OF THE RAMP AND SHALL TAKE PLACE DURING THE INTERSTATE WEEKEND CLOSURE, SPECIAL, FOR I-64.

I-64 MAINLINE STAGE 7:
WORK PERFORMED CONCURRENTLY WITH I-55 PAVEMENT RESURFACING AND FINAL STRIPING.

MAINTENANCE OF TRAFFIC

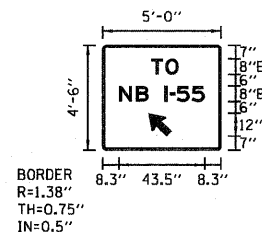
- EASTBOUND**
- MERGE RIGHT LANE OF EB I-64 INTO THE CENTER LANE.
 - MAINTAIN ACCESS TO EXIT RAMP A.
 - MAINTAIN TWO TRAVEL LANES ALONG EB I-64.
 - SHIFT EB I-64 REMAINING TWO LANES LEFT.
 - PARTIALLY CLOSE ENTRANCE RAMP B TO MAINTAIN ACCESS TO EB I-64.
- WESTBOUND**
- MERGE WB I-64 INSIDE LANE INTO CENTER LANE.
 - MAINTAIN TWO TRAVEL LANES ALONG WB I-64.
 - WB I-64 TRAFFIC TO RAMP 64W55N RIGHT LANE EXIT ONLY.
 - SHIFT LEFT LANE OF WB I-64 LEFT.
 - RAMP 64W70W TO REMAIN CLOSED TO TRAFFIC.
 - MAINTAIN WB I-64 ACCESS FROM RAMP E.

WORK TO BE COMPLETED THIS STAGE

- EASTBOUND**
- MILL AND OVERLAY THE EB I-64 RIGHT LANE APPROXIMATELY FROM "EB I-64" STA 42+13 TO THE 18TH STREET BRIDGE APPROACH.
 - MILL AND OVERLAY THE RAMP B TIE-IN TO EB I-64 APPROXIMATELY FROM "EB I-64" STA 57+50 TO THE 18TH STREET BRIDGE APPROACH.
- WESTBOUND**
- MILL AND OVERLAY THE OUTSIDE LANE AND DESIGNATED SHOULDERS ALONG WB I-64 FROM THE 18TH STREET BRIDGE APPROACH TO "WB I-64" STA 105+15.
 - MILL AND OVERLAY THE WB I-64 CENTER LANE FROM THE 18TH STREET BRIDGE APPROACH TO 64WB STA 105+15

GENERAL NOTES:

- DUE TO THE SKEWED NATURE OF THE ROADWAYS IN THIS PROJECT, THE FOLLOWING ARE THE COMPASS DESIGNATIONS TO THE ROADWAYS:
I-55 (NORTH-SOUTH) I-70 (EAST-WEST) I-64 (EAST-WEST)
- TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- TYPE III BARRICADES AND ROAD CLOSURE SIGNS SHALL BE POSITIONED AS SHOWN, ACCORDING TO HIGHWAY STANDARD 701901 AND AS DIRECTED BY THE ENGINEER.
- TYPE A LOW INTENSITY FLASHING WARNING LIGHTS SHALL BE USED ON EACH SIGN IN ADVANCE OF THE WORK DURING HOURS OF DARKNESS.
- ALL WARNING SIGNS SHALL BE A MINIMUM OF 48" x 48" AND HAVE A BLACK LEGEND AND BORDER ON A FLUORESCENT ORANGE REFLECTORIZED BACKGROUND.
- ALL TYPE III BARRICADES UTILIZED FOR ROAD CLOSURES SHALL HAVE TWO LOW INTENSITY FLASHING LIGHTS MOUNTED ON TOP OF EACH BARRICADE.
- EXACT LOCATION OF ALL WARNING SIGNS AND BARRICADES SHALL BE STAKED IN THE FIELD FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION.
- PRIOR TO START OF CONSTRUCTION, ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
- IN AREAS WHERE A MINIMUM OFFSET OF 2 FEET FROM THE TRAVEL LANE TO THE FACE OF TEMPORARY CONCRETE BARRIER CANNOT BE ACHIEVED, THE BARRIER SHALL BE ANCHORED TO THE PAVEMENT. ANCHORS SHALL ALSO BE PROVIDED WHERE CONSTRUCTION ACTIVITY PLACES A TRAFFIC HAZARD WITHIN 3.5 FEET OF THE TEMPORARY CONCRETE BARRIER. NO ADDITIONAL PAYMENT SHALL BE MADE FOR ANCHORING OF TEMPORARY CONCRETE BARRIER.
- A MINIMUM 11' LANE WIDTH SHALL BE MAINTAINED ON ALL INTERSTATE LANES OPEN TO TRAFFIC DURING CONSTRUCTION.
- COOPERATION BETWEEN CONTRACTORS MAY BE REQUIRED
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER 28 DAYS PRIOR TO ANY ANTICIPATED CLOSURES.
- ALL MILL AND OVERLAY OPERATIONS ARE TO BE PERFORMED DURING OFF-PEAK HOURS.
- LANE WIDTHS IN THESE PLANS ARE ASSUMED TO BE 12 FT UNLESS DESIGNATED OTHERWISE.
- OFFSETS BETWEEN THE EDGE OF TRAVEL LANE AND THE FACE OF TEMPORARY CONCRETE BARRIER ARE ASSUMED TO BE 2 FT UNLESS DESIGNATED OTHERWISE.
- WET REFLECTIVE TEMPORARY TAPE SHALL BE USED ON ALL FINAL SURFACES AND SURFACES OUTSIDE OF CONSTRUCTION LIMITS.
- THE CONTRACTOR SHALL BE ADVISED THAT THE WORK DURING PEAK HOURS SPECIFICATION TAKE PRECEDENCE OVER THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL.
- BLACK WET REFLECTIVE TEMPORARY TAPE SHALL BE USED TO COVER EXISTING PAVEMENT MARKINGS INSTEAD OF USING PAVEMENT MARKING REMOVAL AT LOCATIONS OUTSIDE OF THE PROJECT LIMITS AND THE FINAL STRIPING PLANS.

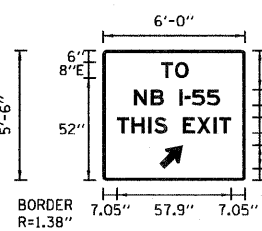


SIGN NUMBER	TC SGN 1
WIDTH x HGHT.	5'-0" x 4'-6"
BORDER WIDTH	0.75"
CORNER RADIUS	1.38"
MOUNTING	Ground Mount
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black/Black

SYMBOL	ROT	X	Y	WID	HT
AR_Type D	225	23.2	7.1	10	15

Letters locations are panel edge to lower left corner

LETTER POSITIONS (X)											LENGTH	SERIES/SIZE	
T	O												E 2000
24.7	31.9											13.8	8
N	B		I	-	5	5							E 2000
8.3	16.9	23.3	31.3	34.1	37.7	45.3						43.5	8

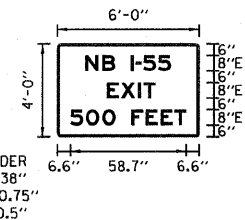


SIGN NUMBER	TC SGN 2
WIDTH x HGHT.	6'-0" x 5'-6"
BORDER WIDTH	0.75"
CORNER RADIUS	1.38"
MOUNTING	Ground Mount
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black/Black

SYMBOL	ROT	X	Y	WID	HT
AR_Type D	315	29.5	6	10	15

Letters locations are panel edge to lower left corner

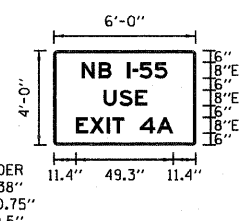
LETTER POSITIONS (X)											LENGTH	SERIES/SIZE	
T	O												E 2000
29.4	36.5											13.8	8
N	B		I	-	5	5							E 2000
15.7	24.3	30.7	38.7	41.5	45.1	52.7						43.4	8
T	H	I	S		E	X	I	T					E 2000
7	14.4	23	26	32.5	40.5	47.6	56.2	59				57.9	8



SIGN NUMBER	TC SGN 3
WIDTH x HGHT.	6'-0" x 4'-0"
BORDER WIDTH	0.75"
CORNER RADIUS	1.38"
MOUNTING	Ground Mount
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black/Black

LETTER POSITIONS (X)											LENGTH	SERIES/SIZE	
N	B		I	-	5	5							E 2000
14.2	22.8	29.3	37.3	40	43.6	51.2						43.4	8
E	X	I	T										E 2000
25.3	32.4	41	43.8									24.5	8
5	0	0		F	E	E	T						E 2000
6.6	14.5	22.8	29.5	37.5	44.9	52.5	59.4					58.7	8

Letters locations are panel edge to lower left corner



SIGN NUMBER	TC SGN 4
WIDTH x HGHT.	6'-0" x 4'-0"
BORDER WIDTH	0.75"
CORNER RADIUS	1.38"
MOUNTING	Ground Mount
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black/Black

LETTER POSITIONS (X)											LENGTH	SERIES/SIZE	
N	B		I	-	5	5							E 2000
14.2	22.8	29.3	37.3	40	43.6	51.2						43.4	8
U	S	E											E 2000
25.3	33.4	41.5										22.2	8
E	X	I	T		4	A							E 2000
11.4	18.5	27	29.8	35.8	43.8	52.5						49.3	8

Letters locations are panel edge to lower left corner