

INDEX OF SHEETS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY OF CHICAGO

DAN RYAN EXPRESSWAY:	ADT (2010)	DESIGN SPEED	POSTED SPEED
SB I-94 (DAN RYAN)	140,500	60 MPH	55 MPH
SB I-94 (BISHOP FORD)	87,600	60 MPH	55 MPH
SB I-57	65,800	60 MPH	55 MPH
SB 79TH STREET C-D ENTRANCE RAMP	15,300	45 MPH	40 MPH
SB 83RD STREET C-D EXIT RAMP	6,800	45 MPH	40 MPH
SB 87TH STREET EXIT RAMP	12,600	45 MPH	40 MPH
SB 87TH STREET ENTRANCE RAMP	13,400	45 MPH	40 MPH
SB 95TH STREET EXIT RAMP	11,900	45 MPH	40 MPH
SB WENTWORTH AVE. EXIT RAMP	4,800	45 MPH	40 MPH
SB HALSTED STREET EXIT RAMP	21,300	45 MPH	40 MPH
EB I-57 TO I-94 CROSS-CONNECTOR	22,800	50 MPH	50 MPH

PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 94 (DAN RYAN EXPRESSWAY)
SECTION (1516.1, 1717, & 1818) R-5
PROJECT NO.: *IM-094-3(401)061*
I-57 TO 79TH STREET

SB LANES 1 - 5, SHOULDERS & CTA BARRIER WALL
COOK COUNTY
C-91-095-03

VOL. 1 OF 2

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
D-91-421-01	(1516.1, 1717, & 1818) R-5		62593	



PROJECT DESCRIPTION

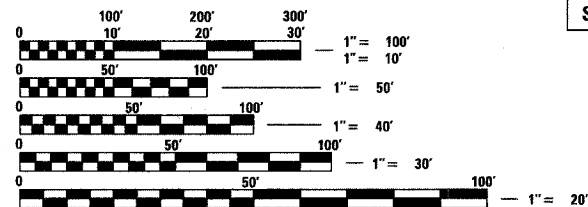
THE PROPOSED IMPROVEMENT CONSISTS OF PAVEMENT AND CTA BARRIER WALL RECONSTRUCTION, WIDENING, RAMP TERMINALS, SHOULDERS, AND DRAINAGE MODIFICATIONS ALONG SB I-94 (DAN RYAN & BISHOP FORD) AND SB I-57; AND PAVEMENT RESURFACING ALONG SB I-94 (BISHOP FORD) AND EB I-57 TO I-94 CONNECTOR.

INTERIOR WALL AND CEILING REPAIRS TO:

S.N. 016-0073: SOUTHBOUND BISHOP FORD TUNNEL AND ADJACENT CTA TUNNEL UNDER NORTHBOUND I-57.

SUBSTRUCTURE AND ABUTMENT REPAIRS TO:

S.N. 016-0143: BELT RAILWAY & NORFOLK SOUTHERN BRIDGE OVER SOUTHBOUND AND NORTHBOUND I-94 (DAN RYAN EXPRESSWAY).
S.N. 016-0144 & S.N. 016-0145: CHICAGO RAIL LINK BRIDGES OVER SOUTHBOUND AND NORTHBOUND I-94 (DAN RYAN EXPRESSWAY).



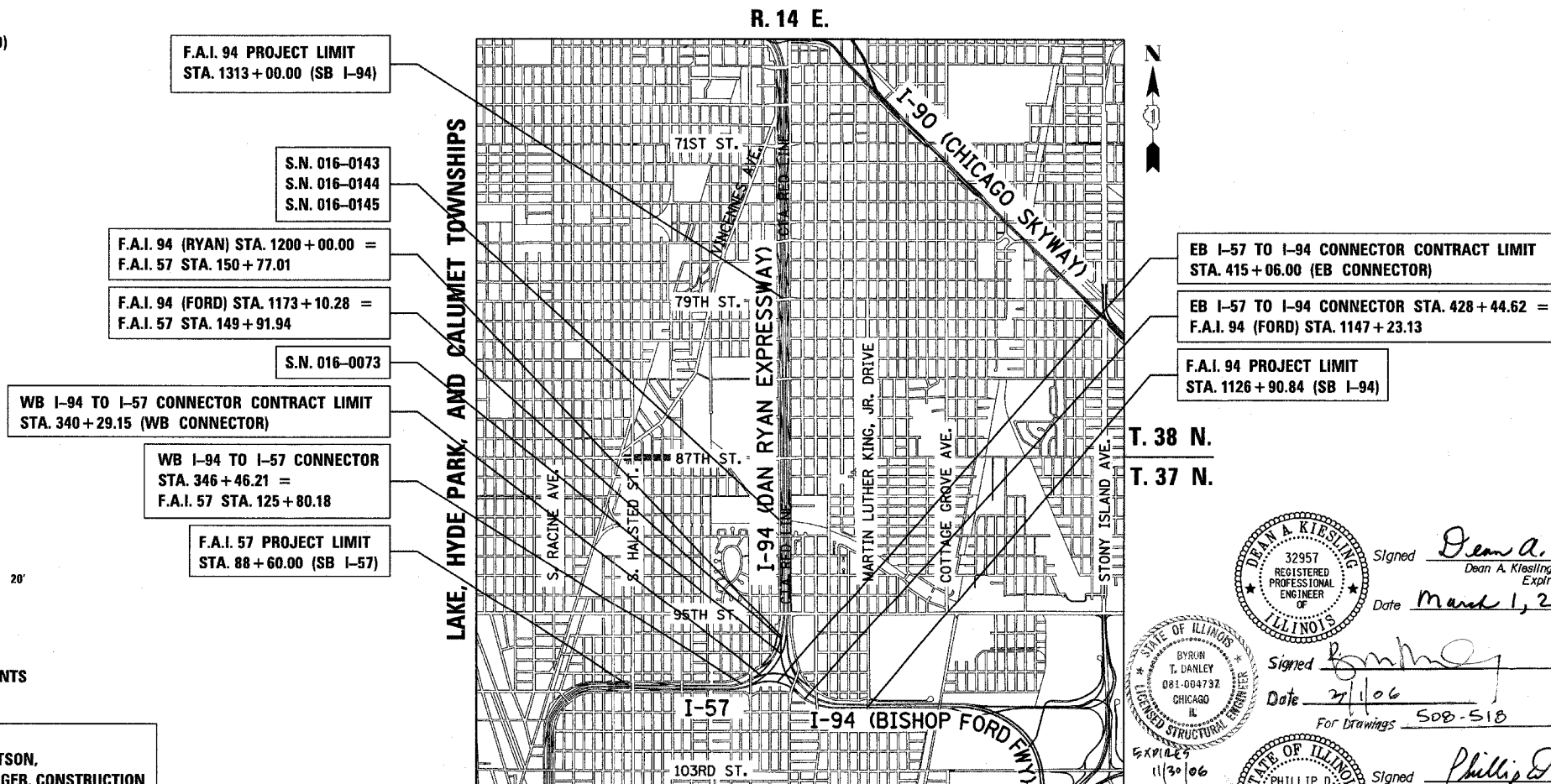
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES; REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

DIGGER:
CHICAGO UTILITY ALERT NETWORK
(312) 744-7000

CTA CONTACT:
MARVIN A. WATSON,
GENERAL MANAGER, CONSTRUCTION
(312) 681-3860

CONTRACT NO. 62593

DISTRICT ONE DESIGN / PROJECT MANAGER:
BRIAN KUTTAB (847) 705 - 4431



F.A.I. 94 PROJECT LIMIT
STA. 1313 + 00.00 (SB I-94)

S.N. 016-0143
S.N. 016-0144
S.N. 016-0145

F.A.I. 94 (RYAN) STA. 1200 + 00.00 =
F.A.I. 57 STA. 150 + 77.01

F.A.I. 94 (FORD) STA. 1173 + 10.28 =
F.A.I. 57 STA. 149 + 91.94

S.N. 016-0073

WB I-94 TO I-57 CONNECTOR CONTRACT LIMIT
STA. 340 + 29.15 (WB CONNECTOR)

WB I-94 TO I-57 CONNECTOR
STA. 346 + 46.21 =
F.A.I. 57 STA. 125 + 80.18

F.A.I. 57 PROJECT LIMIT
STA. 88 + 60.00 (SB I-57)

EB I-57 TO I-94 CONNECTOR CONTRACT LIMIT
STA. 415 + 06.00 (EB CONNECTOR)

EB I-57 TO I-94 CONNECTOR STA. 428 + 44.62 =
F.A.I. 94 (FORD) STA. 1147 + 23.13

F.A.I. 94 PROJECT LIMIT
STA. 1126 + 90.84 (SB I-94)

LOCATION MAP

GROSS LENGTH OF PROJECT = 24088.3 FT. = 4.562 MI.
NET LENGTH OF PROJECT = 24088.3 FT. = 4.562 MI.
MAP SCALE: 1" = 1/2 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

CONTRACT 23

SUBMITTED *March 2* 20 *06*

Diane M. O'Keefe DISTRICT ENGINEER

March 24, 2006
Mike Nunez ENGINEER OF DESIGN AND ENVIRONMENT

March 24, 2006
Milton R. Seas, P.E. DIRECTOR, DIVISION OF HIGHWAYS

Dean A. Kiesel Signed *Dean A. Kiesel, P.E.* 32957 REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS Expires 11-30-2007. Date *March 1, 2006*

Thomas Mikolajewski Signed *Thomas Mikolajewski, P.E.* 062-056151 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS Expires 11-30-2007. Date *March 1, 2006* For Electrical Drawings 441-485

Phillip D. Frey Signed *Phillip D. Frey, S.E.* 081-004826 CHICAGO ILLINOIS Expires 11-30-2006. Date *3/1/06* For Structural Drawings 342-347 AND 519-534

BYRON T. DANLEY 081-004737 CHICAGO ILL. REGISTERED STRUCTURAL ENGINEER Date *2/1/06* For Drawings 508-518

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GENERAL NOTES:

- UTILITY LOCATIONS SHOWN ON THESE PLANS MAY NOT BE CORRECT OR COMPLETE. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE RESPECTIVE UTILITIES OF THE CITY OF CHICAGO. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL THE CHICAGO UTILITY ALERT NETWORK AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE, AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED). CONTACT THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PERMIT SECTION AT (312) 747-7893 FOR WATER AND SEWER LOCATIONS.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKS AND MONUMENTS. THE ENGINEER OR AN AUTHORIZED SURVEYOR AGENT WILL WITNESS OR OTHERWISE REFERENCE AND RESET MONUMENTS AS NECESSARY. ALL PROPERTY CORNERS EXCEPT THOSE WITHIN AREAS WHERE THE SCHEDULE, IF PROVIDED, SHOWS PLACEMENT OF R.O.W. MARKERS SHALL REMAIN UNDISTURBED.
- THE CONTRACTOR SHALL NOT SET UP A YARD OR FIELD OFFICE ON IDOT PROPERTY WITHOUT WRITTEN PERMISSION FROM IDOT.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY SAFETY PRECAUTIONS TO PROTECT AND PROVIDE ACCESS TO ADJUTING PROPERTY, UTILITIES, PEDESTRIANS, AND VEHICULAR TRAFFIC.
- NIGHT OPERATIONS: WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTION IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AS WELL AS THE ADJOINING RESIDENTIAL AREAS.
- ALL ELEVATIONS SHOWN ARE BASED ON THE CHICAGO CITY DATUM OF 0.00, WHICH IS 579.19 FEET ABOVE MEAN TIDE NEW YORK. (NAVD 88)
- THE CONTRACTOR SHALL MAINTAIN THE SURFACE DRAINAGE OF ALL ROADWAYS DURING CONSTRUCTION OF THIS PROJECT. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, INLETS AND CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE SAME. HE SHALL PROVIDE AND MAINTAIN A TEMPORARY OUTLET, AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL INSTALLATION IS COMPLETE INCLUDING PAVEMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT. COORDINATION WITH IDOT CONTRACT 60304 REQUIRED.
- ON STATE STANDARD 483001, SUB-BASE GRANULAR MATERIAL, TYPE B 24" SHALL BE USED AS THE IMPROVED SUBGRADE.
- ALL STORM SEWER CONNECTIONS, WITH PIPES 27 INCH DIAMETER AND SMALLER SHALL BE MADE WITH PRECAST "TEE" OR "WYE" PIPES. FOR PROPOSED STORM SEWER PIPES LARGER THAN 27 INCH DIAMETER, OPENINGS OF THE SPECIFIED DIAMETER SHALL BE MADE IN THE PIPE AT THE TIME IT IS MANUFACTURED. PRECAST "TEE" AND "WYE" PIPE CONNECTIONS FOR STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST FOR THE STORM SEWERS.
- 10' TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTER AND MEDIUM ITEMS TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR "COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24" UNLESS OTHERWISE SHOWN.
- NO PAYMENT WILL BE MADE FOR RESTORATION BEYOND THE LIMITS SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 V:H.
- SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 AND SPECIAL PROVISIONS ENTITLED "RAILROAD PROTECTIVE LIABILITY INSURANCE" AND "CTA COORDINATION" REGARDING FLAGGERS AND WORK PERFORMED ADJACENT TO THE CTA PASSENGER TRAIN RAIL FACILITIES.
- A PAINT STRIPE SHALL BE APPLIED TO THE FACE OF BARRIER WALL ADJACENT TO ALL DRAINAGE STRUCTURES. THE STRIPE SHALL BE 4" WIDE BY 12" LONG, AND EXTEND VERTICALLY AT 90 DEGREES FROM THE TOP EDGE OF THE BARRIER. THE PAINT WILL BE SPECIFIED AS ORANGE PAINT AND SHALL BE ZINC OXIDE. COST OF THE STRIPE IS TO BE INCLUDED IN THE COST OF THE CONCRETE BARRIER.
- STORM SEWERS TO BE REMOVED SHALL NOT BE SALVAGED.
- THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD/IN.
- ANY WASTE GENERATED AS A SPECIAL WASTE OR A WASTE NOT CERTIFIED AS A NON-SPECIAL WASTE FROM THIS PROJECT SHOULD BE MANIFESTED OFF-SITE USING THE GENERATOR NUMBER ASSOCIATED WITH COOK COUNTY, WHICH IS 0318995023.

GENERAL NOTES (CONT.):

- THE CONTRACTOR SHALL BE AWARE THAT MANY CITY OF CHICAGO SEWERS ARE LOCATED IN THE PROPOSED SUB-BASE OR A SHORT DISTANCE BELOW THE SUBGRADE. THE CONTRACTOR SHALL PROTECT THESE FACILITIES FROM DAMAGE DURING CONSTRUCTION OPERATIONS AND SHALL BE RESPONSIBLE FOR ANY DAMAGE AND REPAIR DURING CONSTRUCTION. GAS, ELECTRIC AND TELEPHONE FACILITIES ARE ALSO LOCATED BELOW THE AREA OF PROPOSED CONSTRUCTION. DURING CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH THE PRIVATE UTILITIES (GAS, ELECTRIC AND TELEPHONE) SO THAT THESE UTILITIES MAY PROVIDE APPROPRIATE PROTECTION FOR THEIR FACILITIES. ANY DAMAGE DONE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE.
- THE LOCATIONS OF VARIOUS ITEMS SUCH AS PAVEMENT, BARRIER WALLS AND DRAINAGE STRUCTURES BUILT IN CONTRACTS UNDER CONSTRUCTION DURING THE PREPARATION OF THESE PLANS ARE BASED ON THE PUBLISHED CONTRACT PLAN DRAWINGS AVAILABLE DURING DESIGN. THE CONTRACTOR MUST FIELD VERIFY LIMITS, LOCATIONS AND ELEVATIONS OF THESE PREVIOUSLY CONSTRUCTED ITEMS.
- LOCATIONS OF ACCESS CONTROL FENCING AS SHOWN ON THE PLANS MAY BE ADJUSTED AS DIRECTED BY THE ENGINEER TO BETTER FIT FIELD CONDITIONS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED FENCING OR WHERE PROPOSED FENCING TERMINATES AND EXISTING FENCE REMAINS IN PLACE.
- TEMPORARY CONCRETE BARRIER: THE BARRIER UNIT AT EACH END OF THE INSTALLATIONS SHALL BE SECURED TO THE PAVEMENT OR SHOULDER USING THREE (3) ANCHORING PINS FOR F SHAPE OR THREE (3) DOWEL BARS FOR NEW JERSEY SHAPE.
- HAMMER DRIVING OF PILES WILL NOT BE ALLOWED.
- CRUSHING PLANT AND CONCRETE PLANT LOCATIONS REQUIRE CITY OF CHICAGO APPROVAL.
- ALL PROPOSED HIGH MAST LIGHT TOWER FOUNDATIONS CONSTRUCTED BY OTHERS ARE SHOWN AS EXISTING IN THIS CONTRACT. HOWEVER, DUE TO THE VARYING PROJECT SCHEDULES ALL HIGH MAST LIGHT TOWER FOUNDATIONS MAY NOT BE CONSTRUCTED AT THE START OF THIS CONTRACT.
- ALL PAVEMENT SHALL BE CONSTRUCTED USING THE SPECIAL PROVISION "EXTENDED LIFE CONCRETE PAVEMENT (30 YEAR)".

GENERAL NOTES - LANDSCAPE REQUIREMENTS:

- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD.
- AREAS TO BE SEEDED BETWEEN NOVEMBER 1 AND APRIL 1 SHALL REQUIRE DORMANT SEEDING, WHICH SHALL BE INCLUDED IN THE COST OF SEEDING, CLASS 2A.

GENERAL NOTES - SEDIMENT AND EROSION CONTROL REQUIREMENTS:

- EROSION CONTROL ITEMS ARE CONSIDERED TO BE HIGH PRIORITY ITEMS ON THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF THE SPECIFICATION NECESSARY TO ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY WAY ALL EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES, WHICH WILL POTENTIALLY CREATE ERODABLE CONDITIONS.
- THE EROSION CONTROL MEASURE SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURE. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
- THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURE PRIOR TO STRIPPING EXISTING VEGETATION.

PROJECT COMMITMENTS:

IDOT HAS MADE THE FOLLOWING COMMITMENTS FOR THE PROJECT:

IDOT HAS COMMITTED TO ADDRESS CONSTRUCTION RELATED AIR QUALITY CONCERNS. THESE STRATEGIES INCLUDE REQUIRING DETAILED DUST CONTROL PLANS, REQUIRING THE USE OF CLEANER BURNING DIESEL FUELS ON CERTAIN DIESEL POWERED CONSTRUCTION EQUIPMENT AND/OR THE INSTALLATION OF EXHAUST EMISSION SCRUBBERS, AND THE REDUCTION OF CONSTRUCTION EQUIPMENT IDLING TIMES. THESE STRATEGIES ARE ADDRESSED IN SPECIAL PROVISIONS INCLUDED IN THIS CONTRACT AND DEVELOPED FOR THE DAN RYAN RECONSTRUCTION PROJECT.

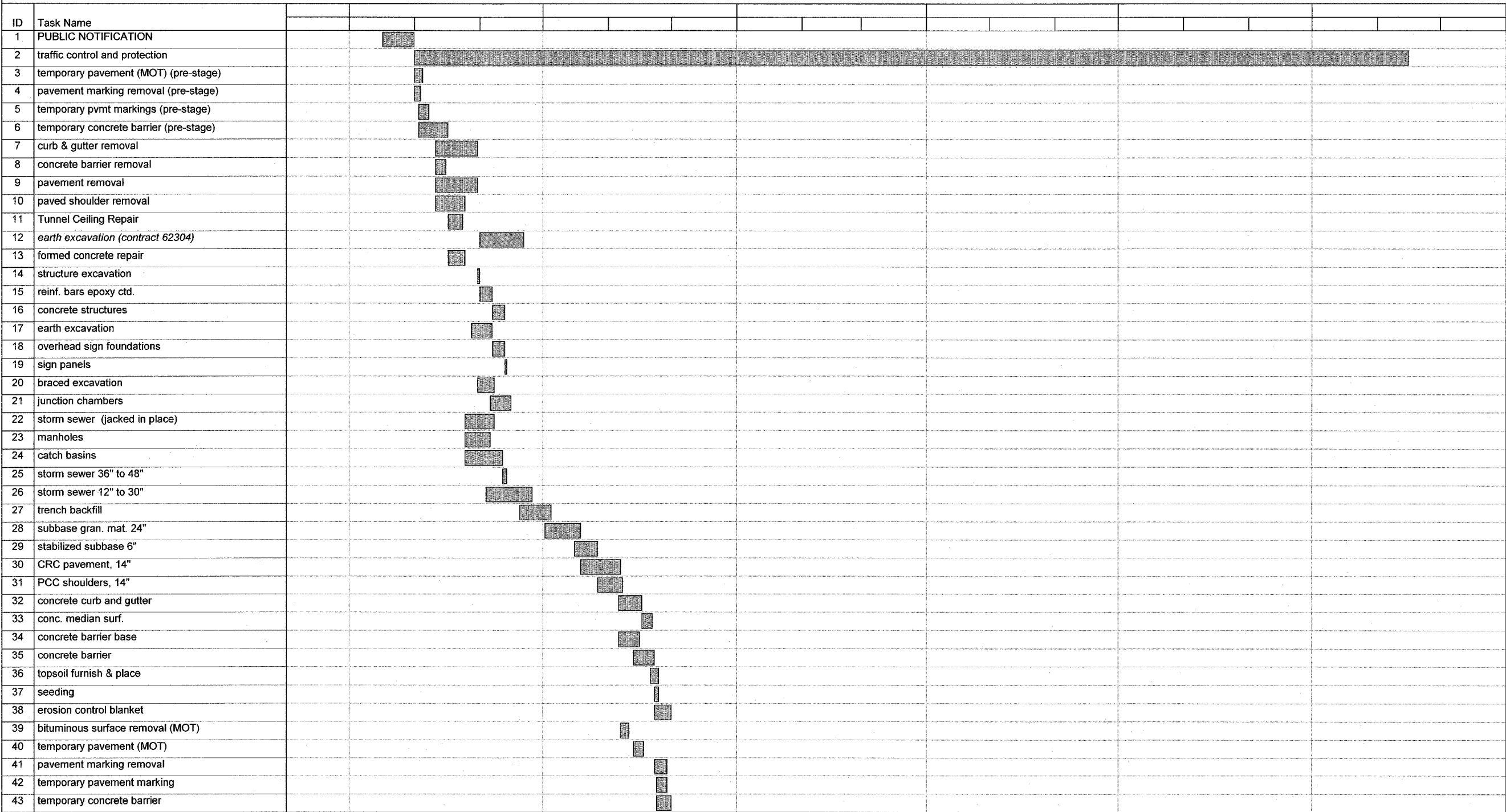
IDOT HAS COMMITTED TO REDUCE TIRE-PAVEMENT HIGHWAY TRAFFIC NOISE FOR THE FINISHED PROJECT BY INCLUDING A SPECIAL PROVISION TO INCORPORATE VARIABLE WIDTH AND SKEWED "TINING" OF THE NEW CONCRETE PAVEMENT.

GENERAL NOTES - CITY OF CHICAGO:

- ALL CATCH BASINS IN THE CITY OF CHICAGO MUST MEET THE DEPARTMENT OF WATER MANAGEMENT'S STANDARDS.
- SEWER SIZES 21" DIAMETER OR SMALLER MUST BE EXTRA STRENGTH VITRIFIED CLAY PIPE C-700 OR DUCTILE IRON PIPE WITH PUSH-ON OR MECHANICAL JOINTS. SEWER SIZES 24" DIAMETER OR LARGER MUST BE REINFORCED CONCRETE PIPE TYPE C-76, CLASS III, WALL "B" WITH "O-RING" JOINTS.
- PERMITS FROM THE DEPARTMENT OF WATER MANAGEMENT ARE REQUIRED FOR ALL UNDERGROUND STORM, SANITARY AND COMBINED SEWER SYSTEM CONSTRUCTION, AND FOR ALL WORK INVOLVING ADJUSTMENT OF SEWER STRUCTURES. THE DEPARTMENT OF WATER MANAGEMENT'S PERMIT MUST BE OBTAINED BY A LICENSED SEWER DRAIN LAYER PRIOR TO START OF CONSTRUCTION. THE LICENSED SEWER CONTRACTOR/ SUBCONTRACTOR MUST SUBMIT TWO SETS OF PLANS APPROVED BY THE DEPARTMENT OF WATER MANAGEMENT FOR THE ISSUANCE OF THE SEWER PERMIT TO BUREAU OF ENGINEERING SERVICES-SEWER SECTION, JARDINE PURIFICATION PLANT, EL+51, ROOM 313, 1000 E. OHIO ST., CHICAGO, IL 60611. INSPECTION WILL BE PROVIDED BY THE DEPARTMENT OF WATER MANAGEMENT.
- IF THE SEWER PIPE COVER IS REDUCED TO LESS THAN 3 FT., CONCRETE ENCASEMENT OF THE SEWER OR REPLACEMENT OF THE SEWER WITH CLASS 52 DUCTILE IRON PIPE WILL BE REQUIRED.
- IN CASE OF DAMAGE TO CITY OF CHICAGO SEWERS, PRIVATE AND PUBLIC DRAINS, SEWER STRUCTURES AND/OR BENCH MONUMENTS, THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE DEPARTMENT OF WATER MANAGEMENT AT (312) 744-0409 OR (312) 744-0408.
- PERFORATED LIDS SHALL BE PLACED ON ALL SEWER MANHOLES AND CATCH BASINS.
- SIDEWALK ACCESSIBILITY RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
- CITY OF CHICAGO WATER VALVE VAULTS AND SEWER STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION WITHOUT WRITTEN PERMISSION FROM THE CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT.
- CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 3".
- BACKFILL MATERIAL UNDER SIDEWALKS SHALL BE FA-2.
- PAVEMENT REPLACEMENT AROUND FRAMES AND GRATES OR LIDS WHERE DRAINAGE, WATER MAIN OR ELECTRICAL STRUCTURES ARE ADJUSTED OR RECONSTRUCTED, SHALL BE WITH CLASS SI CONCRETE.
- ALL PAVEMENT PATCHING ALONG FRONTAGE ROADS SHALL BE CLASS C.
- PRE-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ISSUANCE OF SEWER PERMIT. POST-CONSTRUCTION VIDEO TAPED INSPECTION REQUIRED PRIOR TO ACCEPTANCE OF SEWER BY THE DEPARTMENT OF WATER MANAGEMENT.
- THE CONTRACTOR IS RESPONSIBLE FOR THE ADEQUATE PROTECTION OF THE EXISTING SEWERS, DRAIN CONNECTIONS, SEWER STRUCTURES AND BENCH MONUMENTS DURING CONSTRUCTION OPERATIONS AND USE OF HEAVY EQUIPMENT IN THE LIMITS OF THE PROJECT.
- THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT MUST BE NOTIFIED AT LEAST 48 HOURS IN ADVANCE OF CONSTRUCTION, WITH THE NAME AND TELEPHONE NUMBER OF THE RESIDENT ENGINEER WHO COULD BE CONTACTED FOR ANY SEWER EMERGENCY.
- MANHOLES, CATCH BASINS AND INLETS MUST BE PROTECTED FROM THE ENTRY OF ASPHALT/DEBRIS INTO THE SEWER SYSTEM DURING CONSTRUCTION. THE CONTRACTOR MUST MARK LOCATIONS OF ALL SEWER STRUCTURES ON THE SIDEWALK BEFORE STARTING PAVEMENT REMOVAL/REPLACEMENT. ADJUSTMENT OF FRAMES AND LIDS OF SEWER STRUCTURES MUST BE COMPLETED PRIOR TO STREET RESURFACING.
- THE CONTRACTOR MUST LOCATE AND PROMPTLY CONNECT TO THE NEW SEWERS ALL LIVE HOUSE DRAINS, CATCH BASIN DRAINS AND OTHER EXISTING LATERALS, DRAINS AND SEWERS, OF WHATEVER NATURE, WHICH ARE CONNECTED TO THE EXISTING SEWERS BEING REPAIRED OR REPLACED.
- EXISTING CATCH BASIN LATERALS TO BE REUSED MUST BE RODDED AND FLUSHED IN THE PRESENCE OF THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT INSPECTOR. A NEW CONNECTION TO THE MAIN SEWER IS REQUIRED IF THE EXISTING CATCH BASIN LATERAL IS NOT APPROVED BY THE SEWER INSPECTOR.
- THE FRAMES AND LIDS OF SEWER STRUCTURES TO BE ABANDONED, REMOVED, OR FILLED MUST BE SALVAGED AND THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT NOTIFIED FOR PICKUP.
- WHEN A SEWER STRUCTURE IS ABANDONED, ALL PIPE OPENINGS MUST BE PLUGGED, STRUCTURES FILLED WITH TRENCH BACKFILL, LIDS AND FRAMES REMOVED AND SURFACE RESTORED AS PER THE SEWER UNIT OF THE DEPARTMENT OF WATER MANAGEMENT STANDARDS AND SPECIFICATIONS.

REVISIONS	
NAME	DATE

CONTRACT 62593



Suggested Sequence of Work
Date: Sat 2/25/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
STAGE 1

SCALE: NONE
DATE: MARCH 1, 2006

DRAWN BY: JJS
CHECKED BY: TGB

ID	Task Name																			
1	PUBLIC NOTIFICATION																			
2	traffic control and protection																			
3	pavement marking removal (pre-stage)																			
4	temporary pvmt markings (pre-stage)																			
5	temporary concrete barrier (pre-stage)																			
6	curb & gutter removal																			
7	pavement removal																			
8	earth excavation																			
9	catch basins																			
10	storm sewer 12" to 30"																			
11	trench backfill																			
12	subbase gran. mat. 24"																			
13	stabilized subbase 6"																			
14	CRC pavement, 14"																			
15	concrete barrier base																			
16	concrete barrier																			

Suggested Sequence of Work Date: Fri 2/24/06

Task: Milestone: Rolled Up Task: Rolled Up Progress: External Tasks: Group By Summary: Progress: Summary: Rolled Up Milestone: Split: Project Summary:



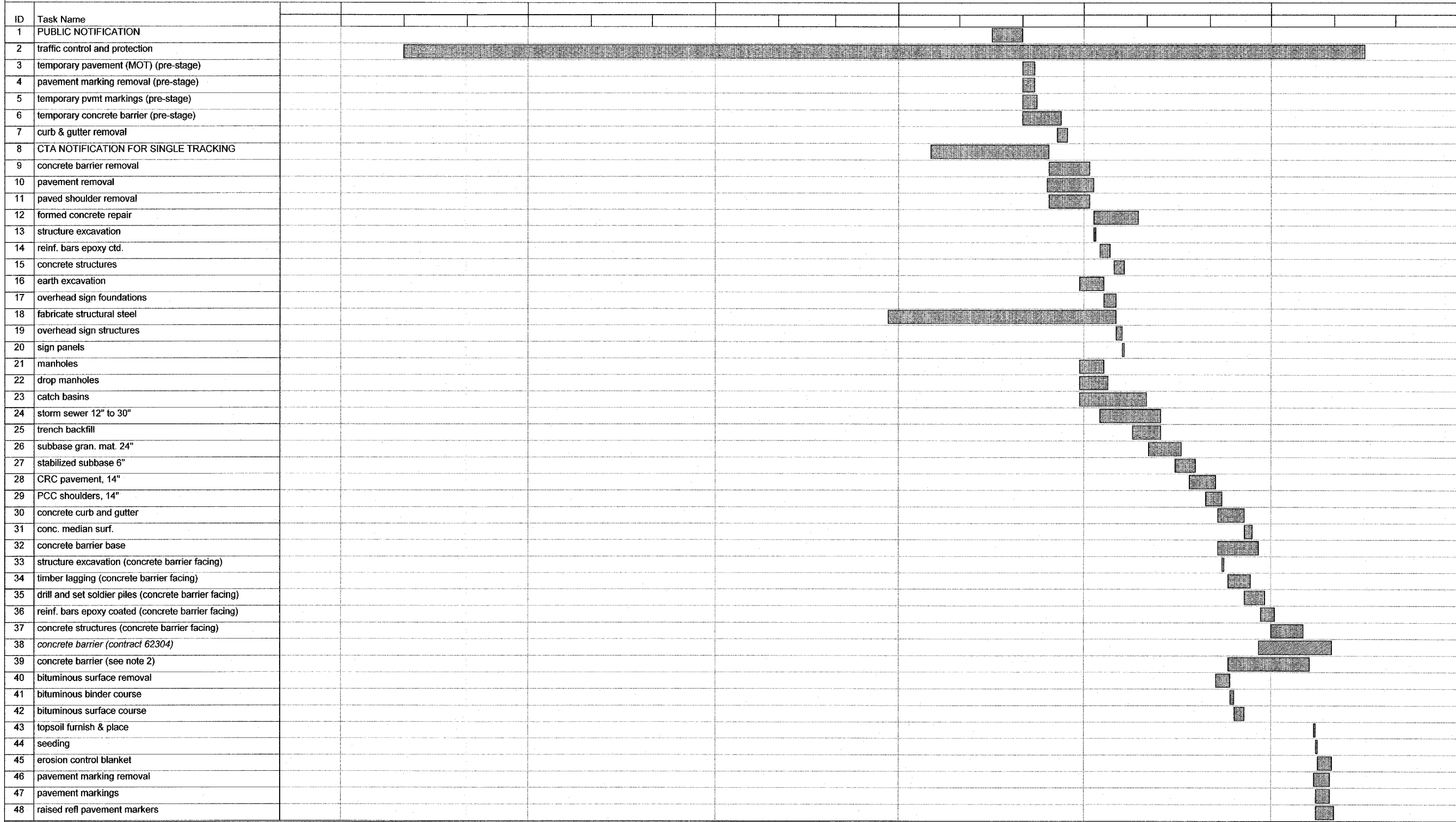
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
 STAGE 2

SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 1, 2006 CHECKED BY: TGB

CONTRACT 62593



Suggested Sequence of Work
Date: Sat 2/25/06

Task		Milestone		Rolled Up Task		Rolled Up Progress		External Tasks		Group By Summary	
Progress		Summary		Rolled Up Milestone		Split		Project Summary			



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUGGESTED SEQUENCE OF WORK
 STAGE 3

SCALE: NONE
 DATE: MARCH 1, 2006

DRAWN BY: JJS
 CHECKED BY: TCB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE					
				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY-2A SNO16-0143 0144, 0145 SNO16-0073
20100110	TREE REMOVAL (6-15 UNITS DIAMETER)	UNIT	784	775	9				
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	116	116					
20101000	TEMPORARY FENCE	FOOT	5961	5601	360				
20200100	EARTH EXCAVATION	CU YD	136021	136021					
20200410	EARTH EXCAVATION (SPECIAL)	CU YD	115	115					
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	10875	10875					
20700220	POROUS GRANULAR EMBANKMENT	CU YD	678	526	152				
20700420	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	4401	4401					
20800150	TRENCH BACKFILL	CU YD	6039	4921	1118				
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	153634	153634					
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	47619	35729	11890				
* 21101630	TOPSOIL FURNISH AND PLACE, 8"	SQ YD	8007	8007					
* 21101645	TOPSOIL FURNISH AND PLACE, 12"	SQ YD	19496	16229	3267				
* 21101825	COMPOST FURNISH AND PLACE, 6"	SQ YD	19149	17951	1198				
* 25000210	SEEDING, CLASS 2A	ACRE	11.75	9.00	2.75				
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1146	898	248				
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	1141	893	248				
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1138	890	248				
* 25000750	MOWING	ACRE	29.25	22.00	7.25				
* 25001800	SEEDING, CLASS 4 (MODIFIED)	ACRE	3.00	3.00					
* 25001820	SEEDING, CLASS 5 (MODIFIED)	ACRE	1.00	0.50	0.50				
* 25002014	SEEDING, CLASS 4A (MODIFIED)	ACRE	1.00	0.50	0.50				
* 25100630	EROSION CONTROL BLANKET	SQ YD	74385	59228	15157				
* 25200200	SUPPLEMENTAL WATERING	UNIT	3910	3128	782				
* 28000200	EARTH EXCAVATION FOR EROSION CONTROL	CU YD	90	60	30				
* 28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	1412	1098	314				
* 28000300	TEMPORARY DITCH CHECKS	EACH	77	61	16				
* 28000510	INLET FILTERS	EACH	21	21					
28001000	AGGREGATE (EROSION CONTROL)	TON	5	2	3				
31101860	SUB-BASE GRANULAR MATERIAL, TYPE B 24"	SQ YD	157503	157503					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	14.1	0.1	14.0				
40600300	AGGREGATE (PRIME COAT)	TON	71	1	70				
40600895	CONSTRUCTING TEST STRIP	EACH	2	2					

- * - SPECIALTY ITEM
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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 1 OF 8

SCALE: NONE DRAWN BY: MPC
DATE: MARCH 1, 2006 CHECKED BY: TGB

Rev.

03/01/2006 12:31:26 PM

03/10/06 12:31:27 PM
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE						
				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY-2A SNO16-0143 0144, 0145	SNO16-0073
40601000	BITUMINOUS REPLACEMENT OVER PATCHES	TON	184		184					
42001300	PROTECTIVE COAT	SQ YD	163773	160475	3298					
42100380	CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"	SQ YD	113317	113317						
42101426	LUG SYSTEM COMPLETE 26'	EACH	1	1						
42101436	LUG SYSTEM COMPLETE 36'	EACH	1	1						
44000004	BITUMINOUS SURFACE REMOVAL 1"	SQ YD	21837	21837						
44000006	BITUMINOUS SURFACE REMOVAL 1 1/2"	SQ YD	5465	290	5175					
44000011	BITUMINOUS SURFACE REMOVAL 4"	SQ YD	10570		10570					
44000100	PAVEMENT REMOVAL	SQ YD	109404	109404						
44000106	BITUMINOUS REMOVAL OVER PATCHES 1 1/2"	SQ YD	2194		2194					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	18173	14289	3884					
44001980	CONCRETE BARRIER REMOVAL	FOOT	6524	5273	1251					
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SQ FT	2940	2940						
44004250	PAVED SHOULDER REMOVAL	SQ YD	29961	29024	937					
44004260	PAVED SHOULDER REMOVAL (SPECIAL)	SQ YD	746	746						
44004400	PAVEMENT REMOVAL (SPECIAL)	SQ YD	2338	2338						
44201427	CLASS C PATCHES, TYPE II, 16 INCH	SQ YD	100		100					
44201433	CLASS C PATCHES, TYPE IV, 16 INCH	SQ YD	2033		2033					
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	17527	17527						
48101200	AGGREGATE SHOULDERS, TYPE B	TON	406	406						
48300400	PORTLAND CEMENT CONCRETE SHOULDERS 9"	SQ YD	794		794					
50102400	CONCRETE REMOVAL	CU YD	2	2						
50200100	STRUCTURE EXCAVATION	CU YD	748	748						
* 50200400	ROCK EXCAVATION FOR STRUCTURES	CU YD	28	28						
50300225	CONCRETE STRUCTURES	CU YD	403	403						
50300300	PROTECTIVE COAT	SQ YD	1326	1326						
50301245	FORMED CONCRETE REPAIR (DEPTH EQUAL TO OR LESS THAN 5")	SQ FT	1850					1569	281	
* 50700209	UNTREATED TIMBER LAGGING	SQ FT	2761	2761						
50700211	FURNISHING SOLDIER PILES (HP SECTION)	FOOT	3456	3456						
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	57950	57950						
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	8416	7038	1378					
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	3081	2369	712					
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	2404	1576	828					

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**SUMMARY OF QUANTITIES
SHEET 2 OF 8**

SCALE: NONE DRAWN BY: MPG
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE						
				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY-2A SNO16-0143, 0144, 0145	SNO16-0073
550A0400	STORM SEWERS, CLASS A, TYPE 2 21"	FOOT	323	307	16					
550A0410	STORM SEWERS, CLASS A, TYPE 2 24"	FOOT	1385	489	896					
550A0430	STORM SEWERS, CLASS A, TYPE 2 30"	FOOT	511	225	286					
550A0450	STORM SEWERS, CLASS A, TYPE 2 36"	FOOT	33	33						
550A0480	STORM SEWERS, CLASS A, TYPE 2 48"	FOOT	28	28						
550A0660	STORM SEWERS, CLASS A, TYPE 3 15"	FOOT	565	141	424					
550A0680	STORM SEWERS, CLASS A, TYPE 3 18"	FOOT	274	193	81					
550A0710	STORM SEWERS, CLASS A, TYPE 3 24"	FOOT	570	570						
550A0720	STORM SEWERS, CLASS A, TYPE 3 27"	FOOT	243	243						
550A0730	STORM SEWERS, CLASS A, TYPE 3 30"	FOOT	621	450	171					
550A0750	STORM SEWERS, CLASS A, TYPE 3 36"	FOOT	170	125	45					
550A1030	STORM SEWERS, CLASS A, TYPE 4 30"	FOOT	184		184					
550A1070	STORM SEWERS, CLASS A, TYPE 4 42"	FOOT	13	13						
550A1280	STORM SEWERS, CLASS A, TYPE 5 24"	FOOT	144	144						
550A1340	STORM SEWERS, CLASS A, TYPE 5 42"	FOOT	15	15						
550A1640	STORM SEWERS, CLASS A, TYPE 6 42"	FOOT	6	6						
55100300	STORM SEWER REMOVAL 8"	FOOT	92	92						
55100400	STORM SEWER REMOVAL 10"	FOOT	3187	3097	90					
55100500	STORM SEWER REMOVAL 12"	FOOT	5759	5176	583					
55100700	STORM SEWER REMOVAL 15"	FOOT	253	253						
55100900	STORM SEWER REMOVAL 18"	FOOT	156	156						
55101200	STORM SEWER REMOVAL 24"	FOOT	327	327						
55101400	STORM SEWER REMOVAL 30"	FOOT	161	161						
55101600	STORM SEWER REMOVAL 36"	FOOT	385	385						
552A0900	STORM SEWERS JACKED IN PLACE, CLASS A 24"	FOOT	49	49						
552A1100	STORM SEWERS JACKED IN PLACE, CLASS A 30"	FOOT	265	265						
552A1300	STORM SEWERS JACKED IN PLACE, CLASS A 36"	FOOT	115	115						
59000100	EPOXY CRACK SEALING	FOOT	372					331	41	
60107700	PIPE UNDERDRAINS 6"	FOOT	34761	34761						
60108200	PIPE UNDERDRAINS 6" (SPECIAL)	FOOT	912	912						
60201310	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	277	219	58					
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	44	35	9					

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ILLINOIS DEPARTMENT OF TRANSPORTATION
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SUMMARY OF QUANTITIES
SHEET 3 OF 8

SCALE: NONE DRAWN BY: MPG
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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE						
				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY 2A SNO16-0143, 0145	SNO16-0073
60208210	CATCH BASINS, TYPE C, TYPE 20 FRAME AND GRATE	EACH	2	2						
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	21	9	12					
60221100	MANHOLES, TYPE A, 5'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	37	30	7					
60223800	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	7	4	3					
60237420	INLETS, TYPE A, TYPE 20 FRAME AND GRATE	EACH	6	6						
60250200	CATCH BASINS TO BE ADJUSTED	EACH	50	49	1					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1						
60255500	MANHOLES TO BE ADJUSTED	EACH	72	64	8					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	13	13						
60500040	REMOVING MANHOLES	EACH	40	32	8					
60500050	REMOVING CATCH BASINS	EACH	179	151	28					
60500060	REMOVING INLETS	EACH	68	63	5					
60500105	FILLING MANHOLES	EACH	2	2						
60500205	FILLING CATCH BASINS	EACH	9	8	1					
60608521	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24	FOOT	337.5	337.5						
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	4606	462	4144					
60618324	CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)	SQ FT	7123	5584	1539					
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	16	12	4					
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)	EACH	16	12	4					
63200310	GUARDRAIL REMOVAL	FOOT	286	286						
63700805	CONCRETE BARRIER TRANSITION	FOOT	631	556	75					
64200105	SHOULDER RUMBLE STRIP	FOOT	40962	30924	10038					
* 66400560	CHAIN LINK FENCE, 6' (SPECIAL)	FOOT	10031	10031						
* 66402900	CHAIN LINK GATE, 6' X 6' SINGLE	EACH	9	9						
66410300	CHAIN LINK FENCE REMOVAL	FOOT	330	330						
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	38080	38080						
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1						
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2						
67000600	ENGINEER'S FIELD LABORATORY	CAL MO	15	15						
67100100	MOBILIZATION	L SUM	1	0.7	0.3					
70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	1					1		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	61030	48120	12910					

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**SUMMARY OF QUANTITIES
SHEET 4 OF 8**

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SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE						
				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY-3N SNO16-0143 0144, 0145	SNO16-0073
70300520	PAVEMENT MARKING TAPE, TYPE III, 4"	FOOT	55468	55468						
70300530	PAVEMENT MARKING TAPE, TYPE III, 5"	FOOT	8077	8077						
70300550	PAVEMENT MARKING TAPE, TYPE III, 8"	FOOT	14621	14621						
70300560	PAVEMENT MARKING TAPE, TYPE III, 12"	FOOT	2150	2150						
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	98606	82978	15628					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	28130	23070	5060					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	50840	42990	7850					
* 72000100	SIGN PANEL - TYPE 1	SQ FT	55					55		
* 72000200	SIGN PANEL - TYPE 2	SQ FT	100					100		
* 72000300	SIGN PANEL - TYPE 3	SQ FT	475					475		
* 72400310	REMOVE SIGN PANEL - TYPE 1	SQ FT	18					18		
* 72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	48					48		
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	85					85		
* 73000100	WOOD SIGN SUPPORT	FOOT	395					395		
* 73300100	OVERHEAD SIGN SUPPORT STRUCTURE - SPAN, TYPE I-A (4'-0" X 4'-6")	FOOT	80					80		
* 73305000	OVERHEAD SIGN STRUCTURE WALKWAY	FOOT	44					44		
* 73400200	DRILLED SHAFT CONCRETE FOUNDATIONS	CU YD	16					16		
* 73600100	REMOVE OVERHEAD SIGN STRUCTURE - SPAN	EACH	1					1		
* 73700300	REMOVE CONCRETE FOUNDATION - OVERHEAD	EACH	5					5		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11734	1170	10564					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	806		806					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	503	29	474					
* 78003120	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 5"	FOOT	2126	147	1979					
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	107783	73089	34694					
* 78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	15458	12369	3089					
* 78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	25687	22618	3069					
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	5360	4630	730					
* 78008200	POLYUREA PAVEMENT MARKING TYPE I - LETTERS AND SYMBOLS	SQ FT	110	110						
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	29479	29479						
* 78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	14771	14771						
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	11817	11817						
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	2448	2448						

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SUMMARY OF QUANTITIES
SHEET 5 OF 8

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				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL SFTY-3N SNO16-0143 0144, 0145
* 78100100	RAISED REFLECTIVE PAVEMENT MARKERS	EACH	1830	1629	201				
* 78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	3852	2816	1036				
* 78200410	GUARDRAIL MARKERS, TYPE A	EACH	64	48	16				
* 78200530	BARRIER WALL MARKERS, TYPE C	EACH	235	200	35				
* 78201000	TERMINAL MARKERS, DIRECT APPLIED	EACH	16	12	4				
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	16172	10833	5339				
* 80700140	GROUND ROD, 5/8" DIA. X 10 FT.	EACH	11			11			
* 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	699			699			
* 81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	794			794			
* 81023750	CONDUIT ENCASED IN CONCRETE, 3" DIA., PVC	FOOT	583				583		
* 81400200	HEAVY DUTY HANDHOLE	EACH	22				22		
* 81500200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	3047			3047			
* 83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	99			99			
* 84200705	LIGHTING FOUNDATION REMOVAL, PARTIAL	EACH	115			115			
* 87900200	DRILL EXISTING HANDHOLE	EACH	11			11			
89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	3				3		
X0325291	CONCRETE SEALING	SQ YD	2130						2130
X0325290	CORED DRAIN HOLES	EACH	64						64
X0325288	CTA FENCE (SPECIAL)	FOOT	1133	1133					
X0325286	JUNCTION CHAMBER NO. 63	EACH	1	1					
X0325287	JUNCTION CHAMBER NO. 73	EACH	1	1					
* X0325293	PROTECT AND MAINTAIN EXISTING CTA TUNNEL LIGHTING SYSTEM	L SUM	1						1
* X0325292	PROTECT AND MAINTAIN EXISTING IDOT TUNNEL LIGHTING SYSTEM	L SUM	1						1
* X0325289	SEEDING, CLASS 5A (MODIFIED)	ACRE	2.75	2.75					
* C2C05818	SHRUB, RHUS AROMATICA GRO-LOW (GROW-LOW FRAGRANT SUMAC), 18" WIDTH, CONTAINER	EACH	560	560					
* C3005924	SHRUB, RHUS GLABRA (SMOOTH SUMAC), 2' HEIGHT, BARE ROOT	EACH	365	365					
* C3006024	SHRUB, RHUS TYPHINA (STAGHORN SUMAC), 2' HEIGHT, BARE ROOT	EACH	430	430					
* E20200G1	VINE-PARTHENOCISSUS QUINQUEFOLIA (VIRGINIA CREEPER), 1-GALLON POT	EACH	634	634					
* E20220G1	VINE-PARTHENOCISSUS TRICUSPIDATA (BOSTON IVY), 1-GALLON POT	EACH	469	469					
* K0030400	PERENNIAL PLANTS, DAYLILIES	UNIT	7	7					
X0300057	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	10	2	8				
⊙ X0320333	ROADWAY CLEANING (SPECIAL)	EACH	28	28					

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SUMMARY OF QUANTITIES
SHEET 6 OF 8

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SUMMARY OF QUANTITIES

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				PCC PAVEMENT J000-2A	BITUMINOUS PAVEMENT I000-2A	LIGHTING Y030-1E	I.T.S. Y032-1F	SIGNING Y002-1C	STRUCTURAL-SFTY-2A SNO16-0143, 0144, 0145	SNO16-0073
X0320870	BRACED EXCAVATION	CU YD	954	751	203					
X0321866	REMOVE, STORE AND RE-ERECT SIGN PANEL	SQ FT	525					525		
X0322194	POLYMER MODIFIED PORTLAND CEMENT MORTAR	SQ FT	27						27	
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	1509	128				1381		
X0322671	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	3360	3360						
* X0322859	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	16	16						
* X0323426	SEDIMENT CONTROL, DRAINAGE STRUCTURE INLET FILTER CLEANING	EACH	42	42						
* X0323973	SEDIMENT CONTROL, SILT FENCE	FOOT	16599	11737	4862					
* X0323974	SEDIMENT CONTROL, SILT FENCE MAINTENANCE	FOOT	4150	2934	1216					
X0323988	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	11932	11932						
X0324112	BARRIER BASE	FOOT	17253	15660	1593					
* X0324455	DRILLING AND SETTING SOLDIER PILES (IN SOIL)	CU FT	9028	9028						
* X0324469	CONDUIT ENCASED, REINFORCED CONCRETE, 2 - 4" DIA., CNC	FOOT	78			78				
* X0324471	CONDUIT ENCASED, REINFORCED CONCRETE, 4 - 4" DIA., CNC	FOOT	243			243				
* X0324646	CONDUIT ENCASED, REINFORCED CONCRETE, 6 - 4" DIA., CNC	FOOT	1605			1605				
X0324697	SOIL STABILIZERS	POUND	221000	221000						
X0324698	APPLYING DUST SUPPRESSION AGENT	UNIT	107	107						
* X0324980	CONDUIT ENCASED, REINFORCED CONCRETE, 3 - 4" DIA., CNC AND 1-2" DIA., CNC 2 WIDE X 2 HIGH	FOOT	97			97				
X0325082	CTA BARRIER REMOVAL	FOOT	9199	9199						
X0325083	CTA FENCE	FOOT	8280	8280						
X0325084	CTA GATES	EACH	9	9						
* X0325130	TUBULAR TRAFFIC SIGN POST	EACH	1					1		
X0712400	TEMPORARY PAVEMENT	SQ YD	11173	9719	1454					
X4066426	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	TON	652	25	627					
X4066550	POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105	TON	1690	43	1647					
X4066660	POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105	TON	1332		1332					
X4409400	BITUMINOUS SURFACE REMOVAL 1 3/4"	SQ YD	8621	436	8185					
X4834090	PORTLAND CEMENT CONCRETE SHOULDERS 14"	SQ YD	29133	29133						
X6022120	MANHOLES, DROP TYPE, 7'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	2	1					
X6022130	MANHOLES, DROP TYPE, 8'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	1	1					
X6061001	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48	FOOT	1833.0	1833.0						
X6063600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24	FOOT	13251.5	9612.5	3639.0					

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ILLINOIS DEPARTMENT OF TRANSPORTATION
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SUMMARY OF QUANTITIES
SHEET 7 OF 8

SCALE: NONE
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: TGB

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION	UNIT	URBAN TOTAL	URBAN - 90% FEDERAL, 10% STATE						
				PCC PAVEMENT	BITUMINOUS PAVEMENT	LIGHTING	I.T.S.	SIGNING	STRUCTURAL	
				J000-2A	I000-2A	Y030-1E	Y032-1F	Y002-1C	SNO16-0143, 0144, 0145	
X6370910	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT	FOOT	6543	5971	572					
X6370912	CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT, SPECIAL	FOOT	210		210					
X6370925	CONCRETE BARRIER, SINGLE FACE, 42" (SPECIAL)	FOOT	9123	9123						
X6370935	CONCRETE BARRIER, SINGLE FACE, 32" (MODIFIED)	FOOT	736		736					
X6370940	CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT	FOOT	10	10						
X6640210	TEMPORARY CHAIN LINK FENCE (PORTABLE)	FOOT	1575	1575						
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	24	24						
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1						
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	457	457						
X7015000	CHANGEABLE MESSAGE SIGN	CAL MO	160	160						
XX001854	STABILIZED SUB-BASE, 6"	SQ YD	153261	153261						
XX004200	PORTLAND CEMENT CONCRETE PAVEMENT 14" (JOINTED)	SQ YD	1083	1083						
XX004201	PAVEMENT REINFORCEMENT 14"	SQ YD	113317	113317						
XX004812	VIDEO TAPING OF SEWERS	FOOT	5619	5619						
XX005489	STEEL CASINGS 48"	FOOT	380	380						
Z0002400	BALLAST	TON	2155	2155						
⊙ Z0010605	CLEANING DRAINAGE SYSTEM	L SUM	1						1	
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.7	0.3					
Z0013825	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	344	283	61					
□ Z0030250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	9	9						
□ Z0030280	IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, NARROW), TEST LEVEL 3	EACH	1	1						
□ Z0030350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	24	21	3					
□ Z0030360	IMPACT ATTENUATORS, RELOCATE (SEVERE USE, NARROW), TEST LEVEL 3	EACH	1	1						
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	0.5	0.1			0.3	0.1	
□ Z0056220	SAND MODULE IMPACT ATTENUATOR TO BE REMOVED	EACH	22	11	11					
Z0068400	STEEL CASINGS 42"	FOOT	49	49						

- * - SPECIALTY ITEM
- - IDOT PAY CODE SFTY-3N
- △ - IDOT PAY CODE Y080.
- ⊙ - NON-PARTICIPATING

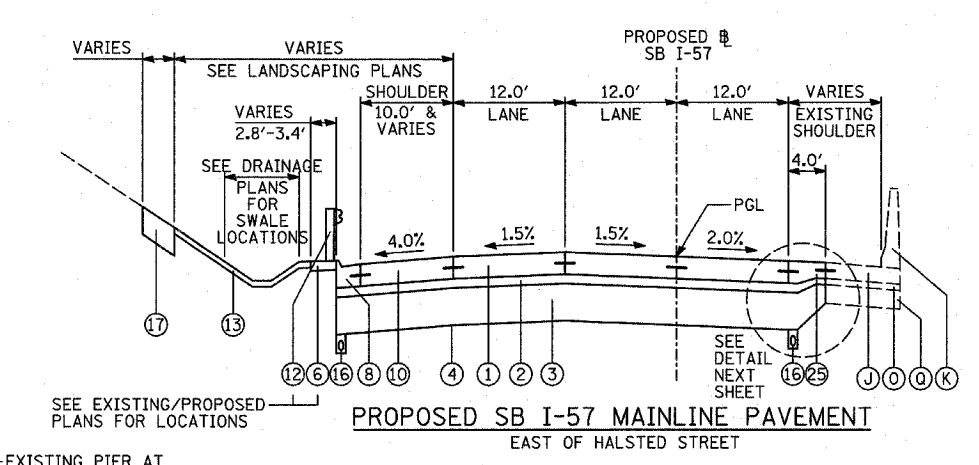
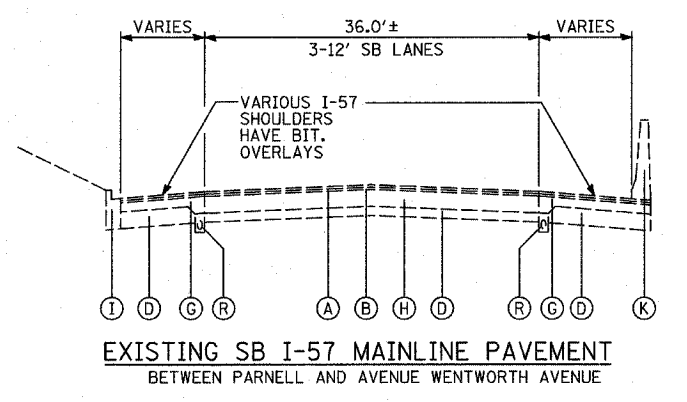
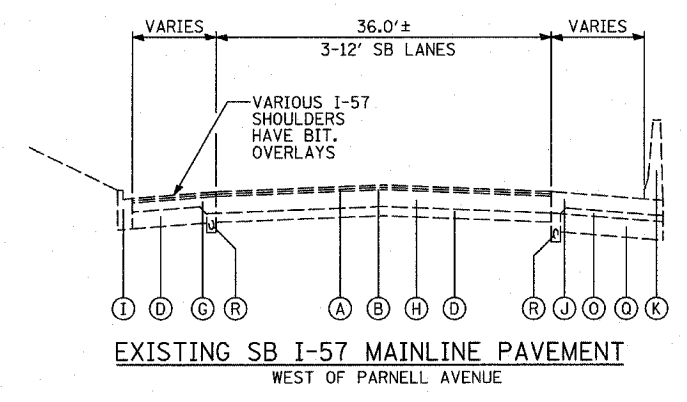
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REVISIONS	
NAME	DATE

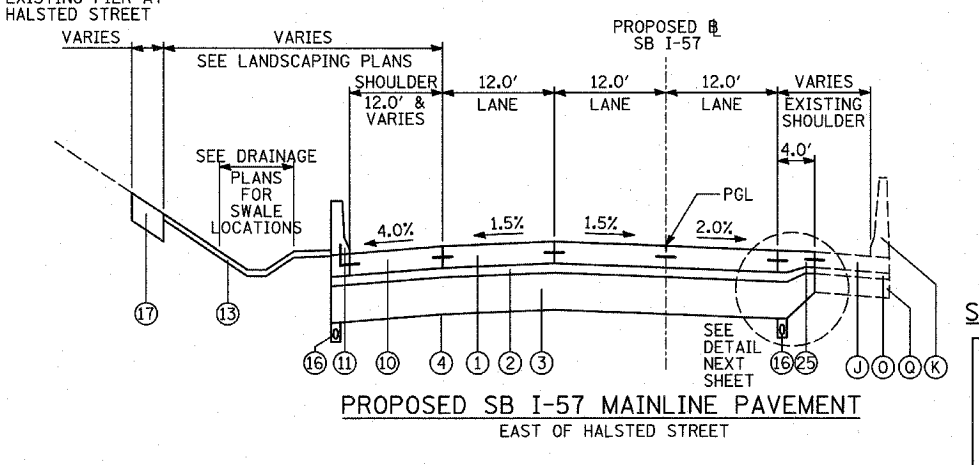
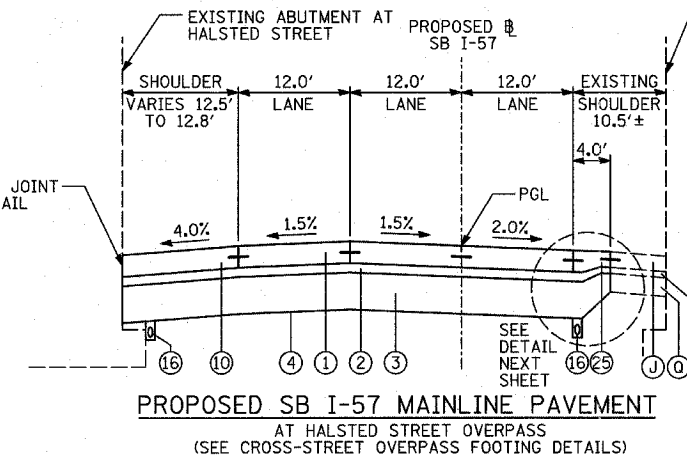
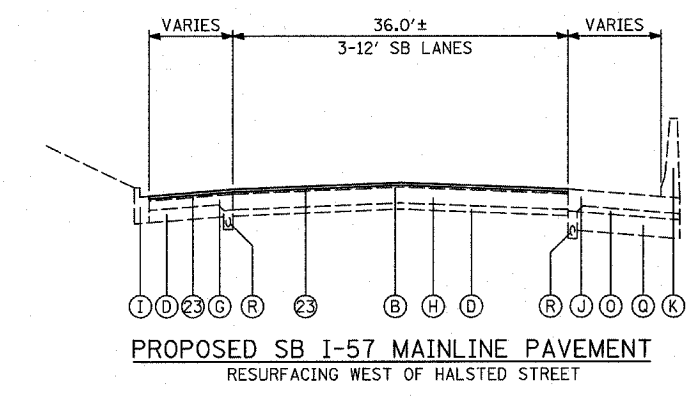
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SUMMARY OF QUANTITIES
SHEET 8 OF 8

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: TGB

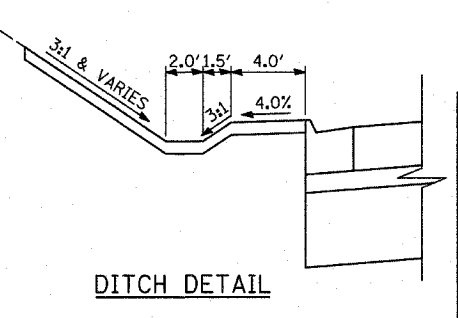
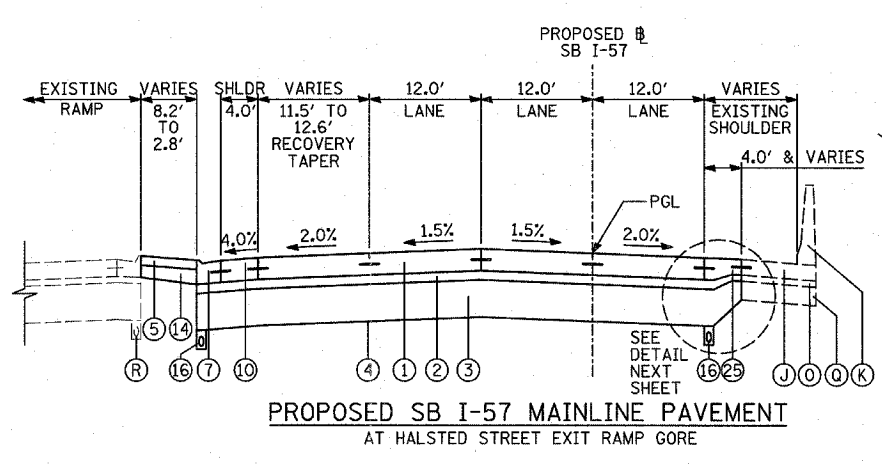
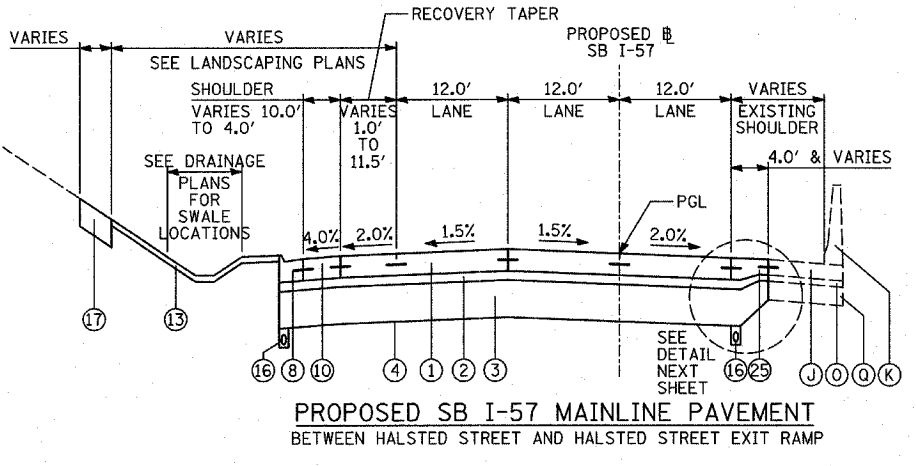


- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- (A) BIT CONC SURFACE COURSE, 1 1/2"±
 - (B) BIT CONC BINDER COURSE, 1 1/2"±
 - (C) BIT CONC BINDER COURSE, 4 3/4"±
 - (D) SUB-BASE GRANULAR MATERIAL, 4"±
 - (E) SUB-BASE GRANULAR MATERIAL, 6"±
 - (F) CRUSHED STONE, 5"±
 - (G) PCC SHOULDERS, 9"±
 - (H) PCC BASE COURSE, 9"±
 - (I) COMB CONC CURB & GUTTER
 - (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - (K) CONCRETE BARRIER WALL
 - (L) PCC SHOULDERS, 11"±
 - (M) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - (N) JOINTED PCC PAVEMENT, 11"±
 - (O) STABILIZED SUB-BASE, 4"±
 - (P) SUB-BASE GRANULAR MATERIAL, 24"±
 - (Q) SUB-BASE GRANULAR MATERIAL, 12"±
 - (R) PIPE UNDERDRAIN



STRUCTURAL PAVEMENT DESIGN INFORMATION

STRUCTURAL DESIGN TRAFFIC:	YEAR = 2020
PV = 322,920	SU = 7,020 MU = 21,060
ROAD/STREET CLASSIFICATION:	CLASS 1
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:	
P = 8%	S = 37% M = 37%
TRAFFIC FACTOR:	ACTUAL TF = 116.08 AC TYPE = N/A
	MINIMUM TF = 8.26
AC GRADE:	BINDER = - SURFACE = -
SUBGRADE SUPPORT RATING:	SSR = 2.00
ILLINOIS BEARING RATIO:	IBR = 3.00



BITUMINOUS MIXTURE REQUIREMENT

ITEM	AC TYPE	VOIDS	RAP %
SHOULDER RESURFACING 1 1/2" & 1 3/4" DEPTH: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70	PG 64-22	4%±70 Gyr.	10
MAINLINE RESURFACING 1 3/4" DEPTH: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105	SBS/SBR PG 76-22	4%±105 Gyr.	0
4" DEPTH: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105 POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105	SBS/SBR PG 76-22 SBS/SBR PG 76-22	4%±105 Gyr 4%±105 Gyr	0
TEMPORARY PAVEMENT POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105 POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105	SBS/SBR PG 76-22 SBS/SBR PG 76-22	4%±105 Gyr 4%±105 Gyr	0
STABILIZED SUBBASE, 6"	PG 58-22	3%±50 Gyr	25
BITUMINOUS REPLACEMENT OVER PATCHES	PG 64-22	4%±70 Gyr	15

THE UNIT WEIGHT USED TO CALCULATE ALL BITUMINOUS SURFACE MIXTURES IS 112 LB/SQ YD./IN.

POTENTIAL UNDERCUT

"POROUS GRANULAR EMBANKMENT, SUBGRADE" (PGES) HAS BEEN RECOMMENDED FOR USE AT LOCATIONS WITH SOILS THAT TEND TO BE UNSTABLE OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 303.13 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIALS ARE ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH PGES AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE THE CONTRACTOR.

(SEE SCHEDULE OF QUANTITIES FOR LOCATIONS)

NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

PROPOSED LEGEND

- (1) CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
- (2) STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
- (3) SUB-BASE GRANULAR MATERIAL, TYPE B 24"
- (4) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- (5) CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
- (6) CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
- (7) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
- (8) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
- (9) COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
- (10) PORTLAND CEMENT CONCRETE SHOULDERS 14"

- (11) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- (12) TRAFFIC BARRIER TERMINAL, TYPE VARIES
- (13) TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
- (14) AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
- (15) CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
- (16) PIPE UNDERDRAIN, 6" (SEE DETAILS)
- (17) TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
- (18) ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
- (19) CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)

- (20) PORTLAND CEMENT CONCRETE SHOULDERS 9"
- (21) 1 1/2" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"
- (22) 1 3/4" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
- (23) 1 3/4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- (24) 4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- (25) PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

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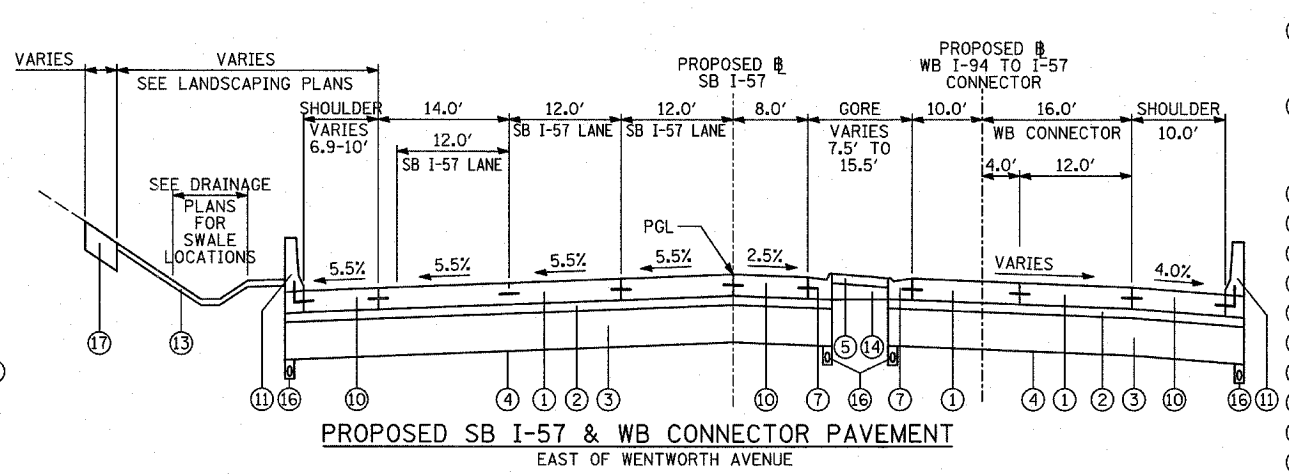
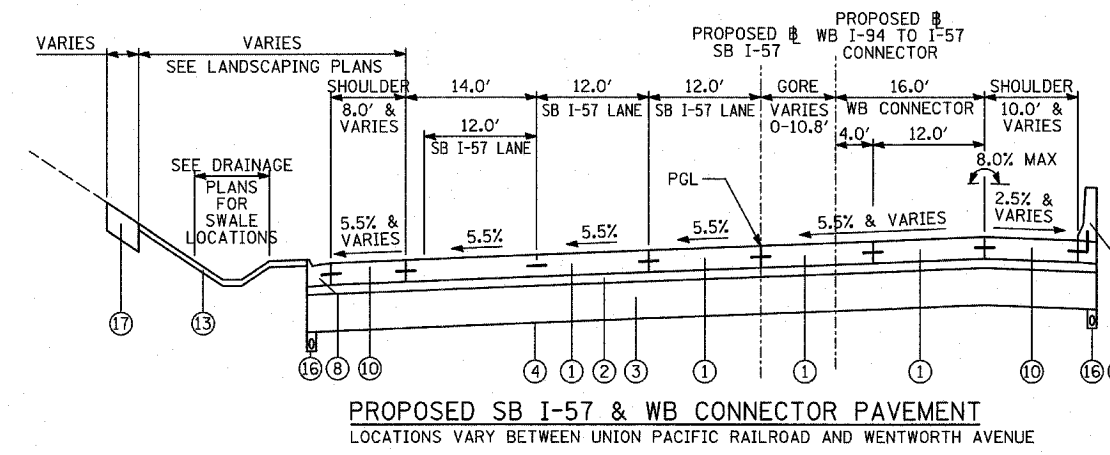
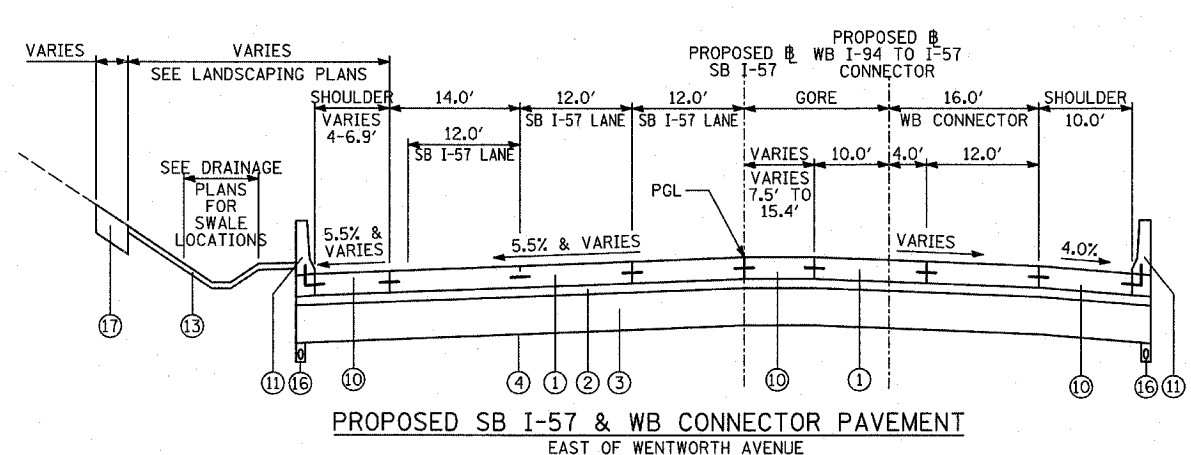
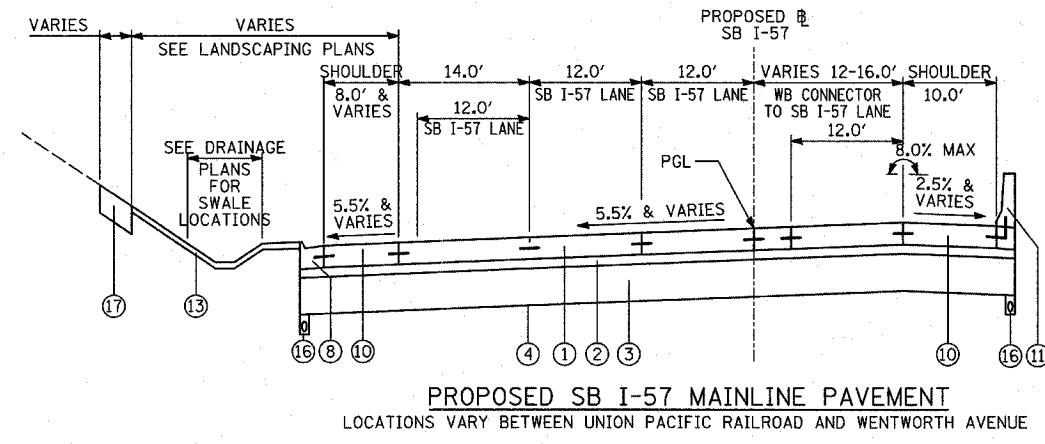
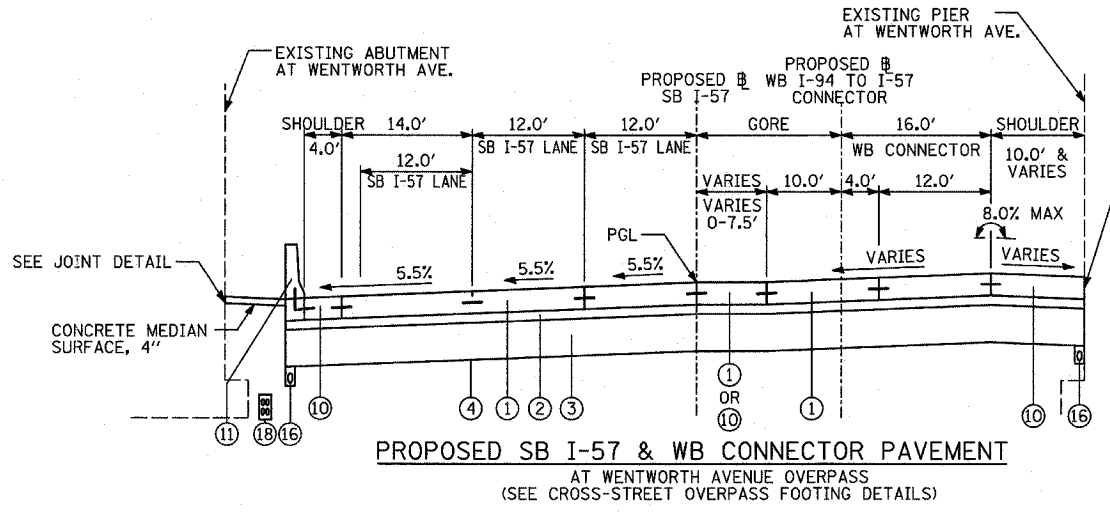
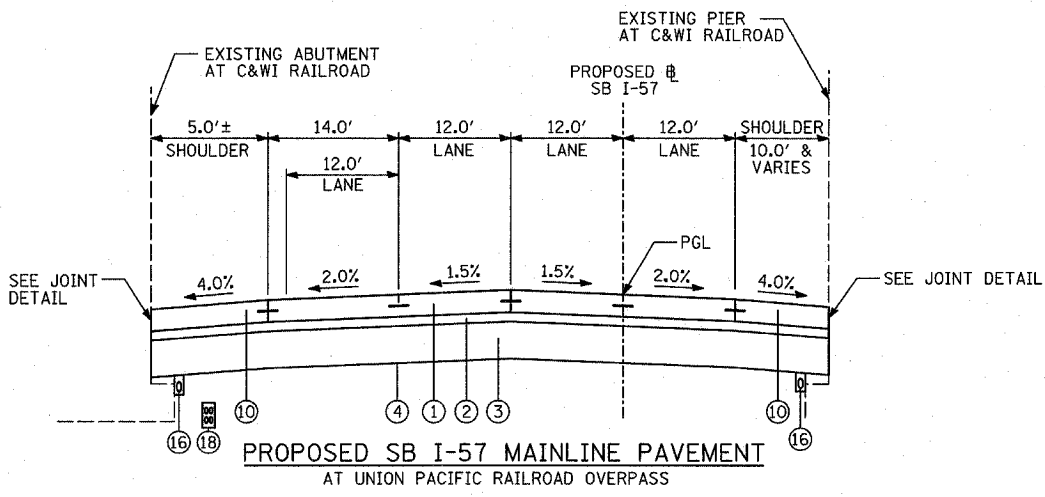
REVISIONS

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
**EXISTING & PROPOSED TYPICAL SECTIONS
SB I-57
(SHEET 1 OF 8)**

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	17
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				
62593				



- PROPOSED LEGEND**
- ① CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
 - ② STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - ③ SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - ④ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ⑤ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - ⑥ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - ⑧ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - ⑨ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - ⑩ PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - ⑪ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑫ TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - ⑬ TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - ⑭ AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - ⑮ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑯ PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - ⑰ TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - ⑱ ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - ⑲ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑳ PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ㉑ 1/2" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
 - ㉒ 1 3/4" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
 - ㉓ 1 3/4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
 - ㉔ 4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - ㉕ PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)
- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- (A) BIT CONC SURFACE COURSE, 1 1/2"±
 - (B) BIT CONC BINDER COURSE, 1 1/2"±
 - (C) BIT CONC BINDER COURSE, 4 3/4"±
 - (D) SUB-BASE GRANULAR MATERIAL, 4"±
 - (E) SUB-BASE GRANULAR MATERIAL, 6"±
 - (F) CRUSHED STONE, 5"±
 - (G) PCC SHOULDERS, 9"±
 - (H) PCC BASE COURSE, 9"±
 - (I) COMB CONC CURB & GUTTER
 - (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - (K) CONCRETE BARRIER WALL
 - (L) PCC SHOULDERS, 11"±
 - (M) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - (N) JOINTED PCC PAVEMENT, 11"±
 - (O) STABILIZED SUB-BASE, 4"±
 - (P) SUB-BASE GRANULAR MATERIAL, 24"±
 - (Q) SUB-BASE GRANULAR MATERIAL, 12"±
 - (R) PIPE UNDERDRAIN

NOTES:
1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

REVISIONS	
NAME	DATE

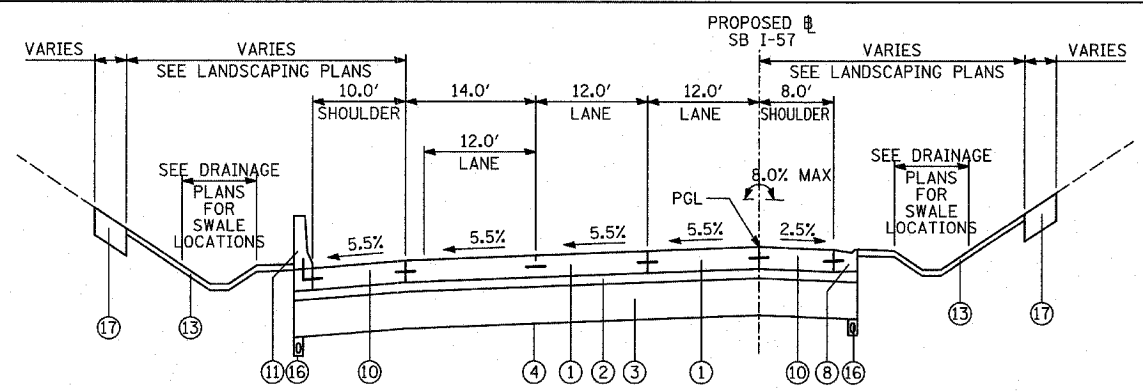
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

PROPOSED TYPICAL SECTIONS
SB I-57 & WB I-94 TO I-57 CONNECTOR
(SHEET 3 OF 8)

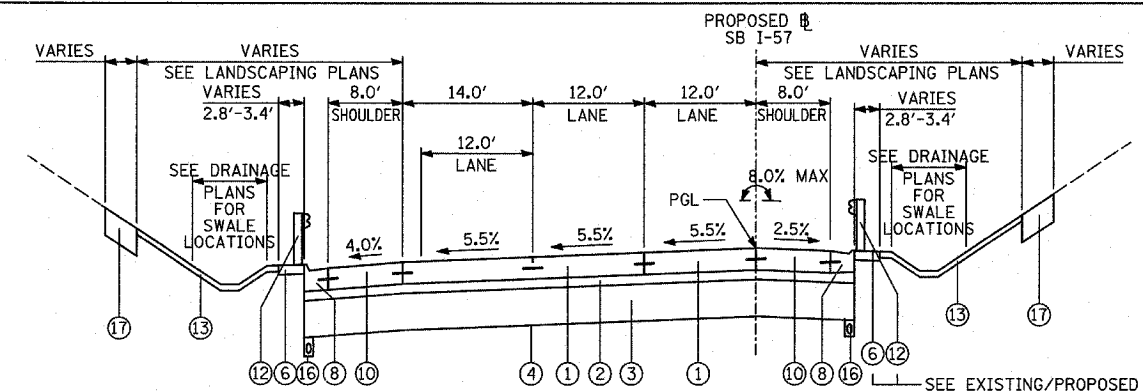
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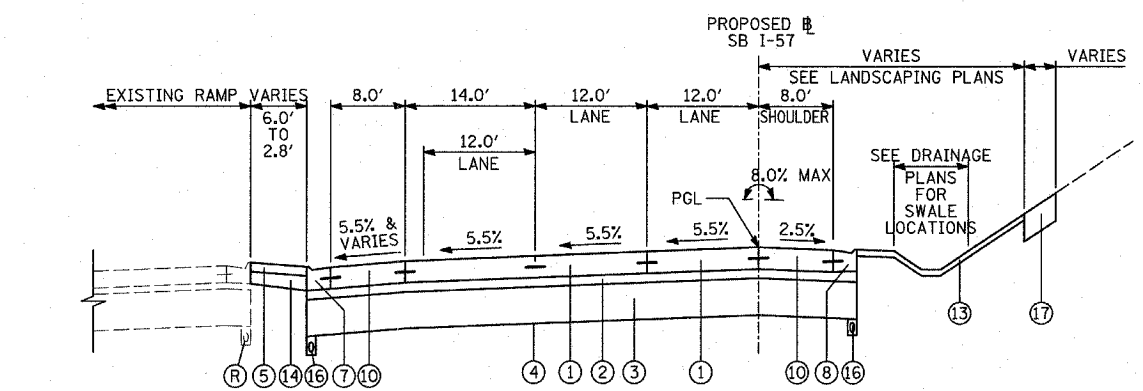
PROPOSED SB I-57 MAINLINE PAVEMENT
 LOCATIONS VARY BETWEEN I-57/WB CONNECTOR MERGE AND WENTWORTH AVENUE EXIT RAMP



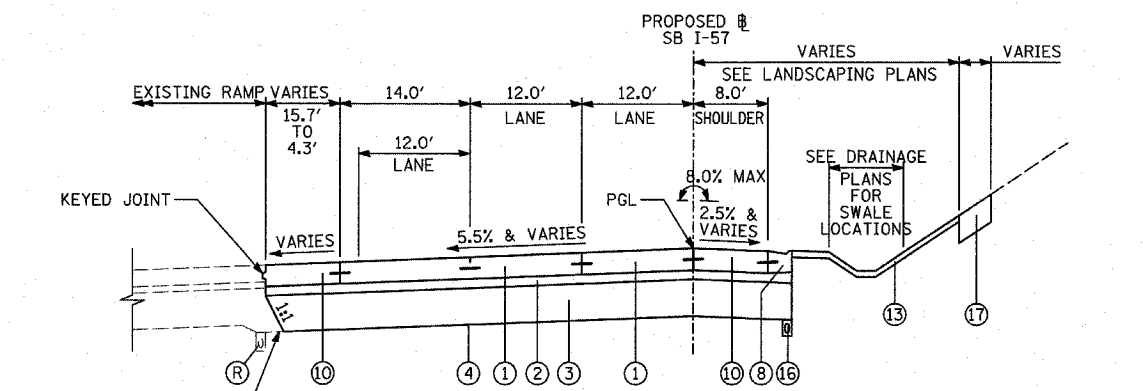
PROPOSED SB I-57 MAINLINE PAVEMENT
 LOCATIONS VARY BETWEEN I-57/WB CONNECTOR MERGE AND WENTWORTH AVENUE EXIT RAMP

- PROPOSED LEGEND**
- 1 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
 - 2 STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - 3 SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - 4 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - 5 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - 6 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - 7 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - 8 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - 9 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - 10 PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - 11 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 12 TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - 13 TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - 14 AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)"; (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - 15 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 16 PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - 17 TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - 18 ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - 19 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 1/2" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"
 - 22 1 3/4" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
 - 23 1 3/4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
 - 24 4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - 25 PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

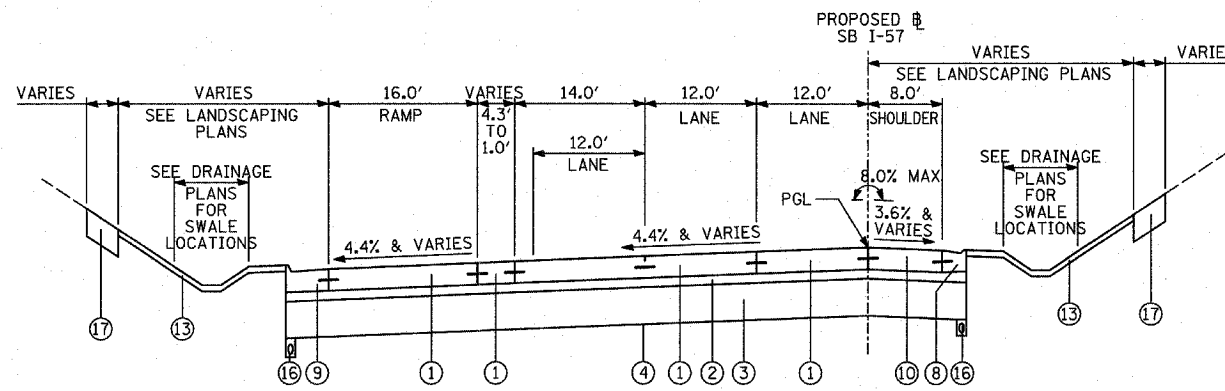
- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- A BIT CONC SURFACE COURSE, 1 1/2"±
 - B BIT CONC BINDER COURSE, 1 1/2"±
 - C BIT CONC BINDER COURSE, 3 3/4"±
 - D SUB-BASE GRANULAR MATERIAL, 4"±
 - E SUB-BASE GRANULAR MATERIAL, 6"±
 - F CRUSHED STONE, 5"±
 - G PCC SHOULDERS, 9"±
 - H PCC BASE COURSE, 9"±
 - I COMB CONC CURB & GUTTER
 - J PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - K CONCRETE BARRIER WALL
 - L PCC SHOULDERS, 11"±
 - M CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)";)
 - N JOINTED PCC PAVEMENT, 11"±
 - O STABILIZED SUB-BASE, 4"±
 - P SUB-BASE GRANULAR MATERIAL, 24"±
 - Q SUB-BASE GRANULAR MATERIAL, 12"±



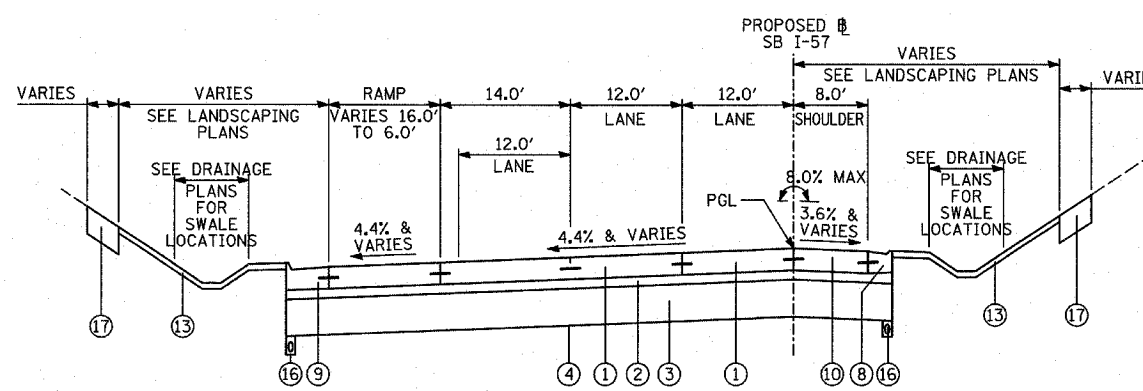
PROPOSED SB I-57 MAINLINE PAVEMENT
 WENTWORTH AVENUE EXIT RAMP GORE



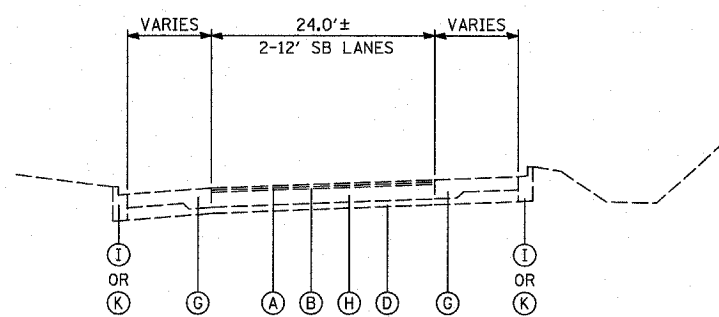
PROPOSED SB I-57 MAINLINE PAVEMENT
 WENTWORTH AVENUE EXIT RAMP GORE



PROPOSED SB I-57 MAINLINE PAVEMENT
 WENTWORTH AVENUE EXIT RAMP GORE



PROPOSED SB I-57 MAINLINE PAVEMENT
 WENTWORTH AVENUE EXIT RAMP



EXISTING SB I-57 MAINLINE PAVEMENT
 BETWEEN SB I-57/SB I-94 SPLIT AND I-57/WB CONNECTOR MERGE

NOTES:

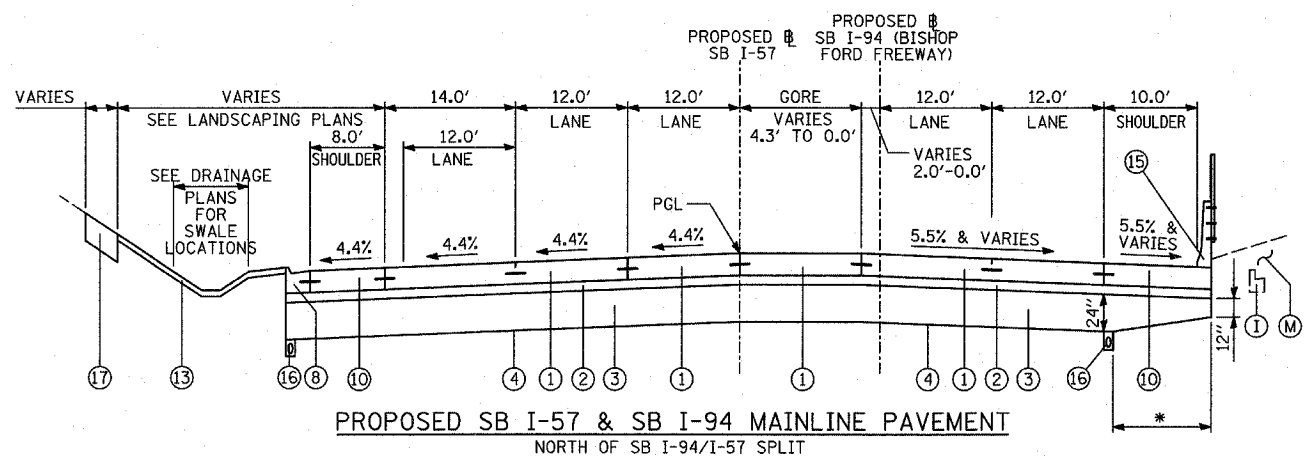
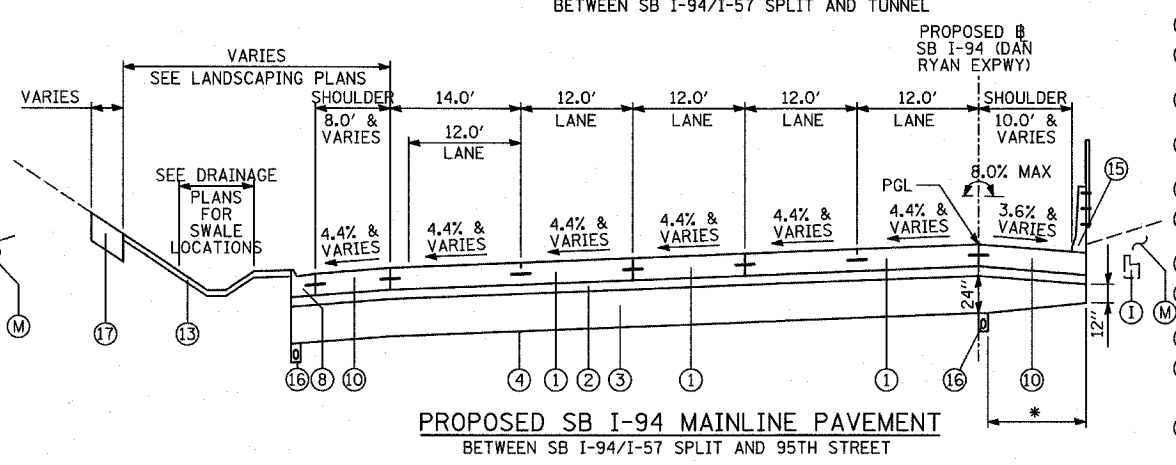
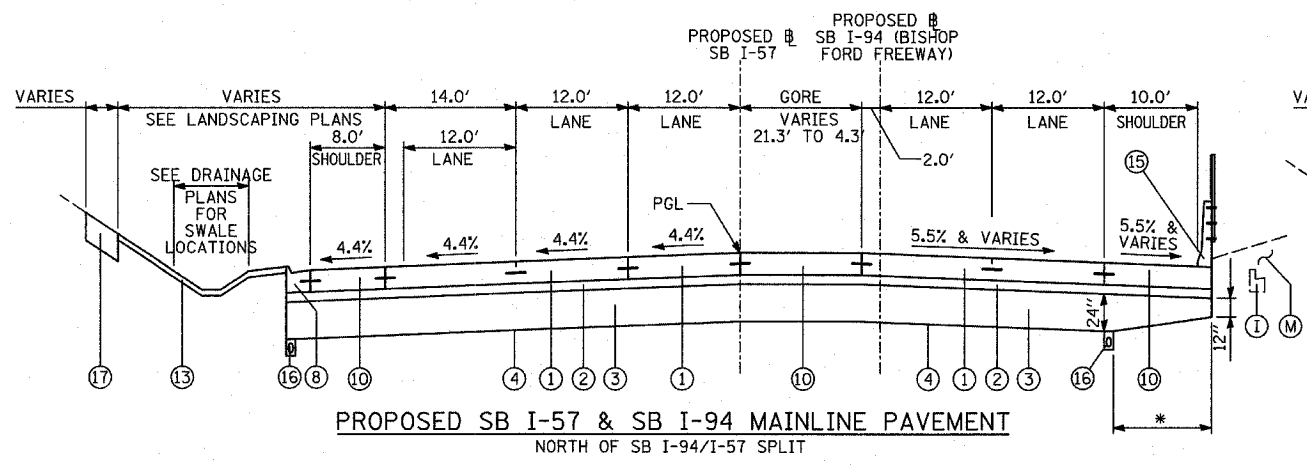
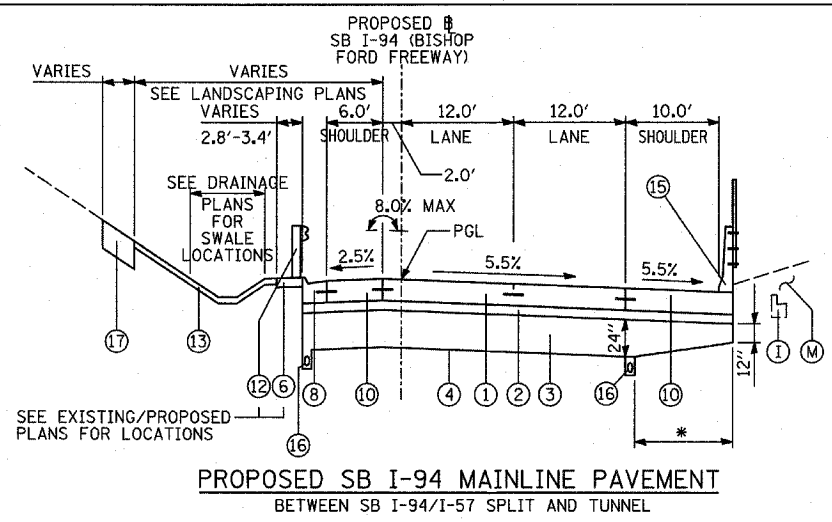
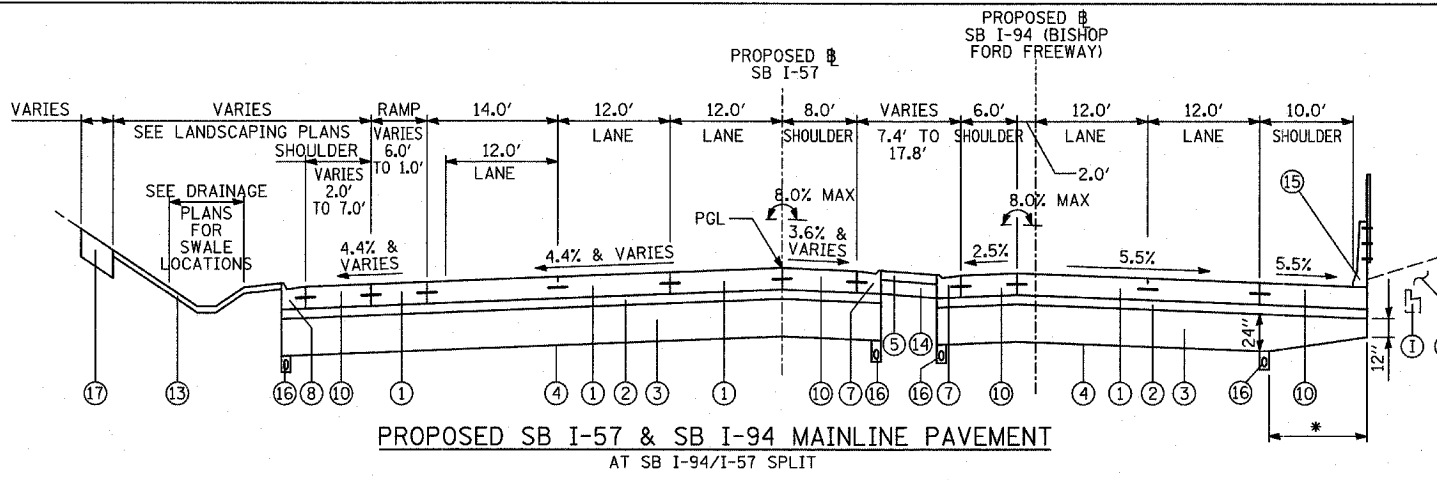
1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
2. ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS
 SB I-57
 (SHEET 4 OF 8)

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	19
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				
62593				



- PROPOSED LEGEND**
- ① CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
 - ② STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - ③ SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - ④ GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - ⑤ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - ⑥ CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - ⑦ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - ⑧ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - ⑨ COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - ⑩ PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - ⑪ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑫ TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - ⑬ TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - ⑭ AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - ⑮ CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑯ PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - ⑰ TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - ⑱ ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - ⑲ CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - ⑳ PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - ㉑ 1/2" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
 - ㉒ 1 3/4" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
 - ㉓ 1 3/4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
 - ㉔ 4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - ㉕ PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- (A) BIT CONC SURFACE COURSE, 1 1/2"±
 - (B) BIT CONC BINDER COURSE, 1 1/2"±
 - (C) BIT CONC BINDER COURSE, 4 3/4"±
 - (D) SUB-BASE GRANULAR MATERIAL, 4"±
 - (E) SUB-BASE GRANULAR MATERIAL, 6"±
 - (F) CRUSHED STONE, 5"±
 - (G) PCC SHOULDERS, 9"±
 - (H) PCC BASE COURSE, 9"±
 - (I) COMB CONC CURB & GUTTER
 - (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - (K) CONCRETE BARRIER WALL
 - (L) PCC SHOULDERS, 11"±
 - (M) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - (N) JOINTED PCC PAVEMENT, 11"±
 - (O) STABILIZED SUB-BASE, 4"±
 - (P) SUB-BASE GRANULAR MATERIAL, 24"±
 - (Q) SUB-BASE GRANULAR MATERIAL, 12"±
 - (R) PIPE UNDERDRAIN

NOTES:

1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
- * - VARYING DEPTH PAID FOR AS "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

PROPOSED TYPICAL SECTIONS
SB I-57 & SB I-94 (BISHOP FORD FREEWAY AND DAN RYAN EXPRESSWAY)
(SHEET 5 OF 8)

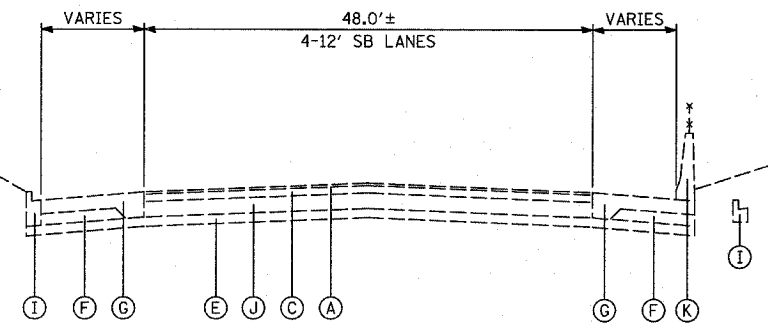
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DATE: MARCH 1, 2006

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CHECKED BY: TGB

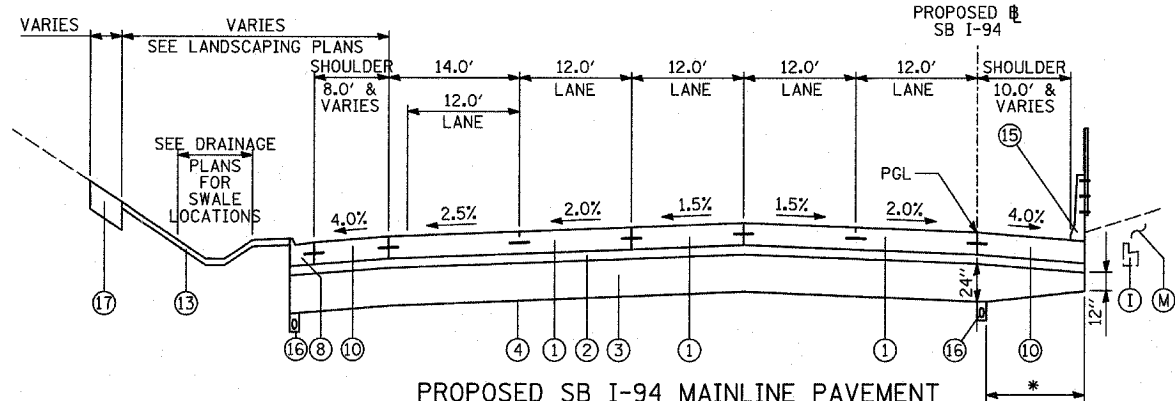
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	20
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				

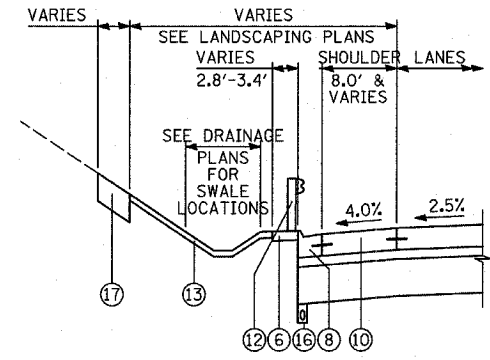
62593



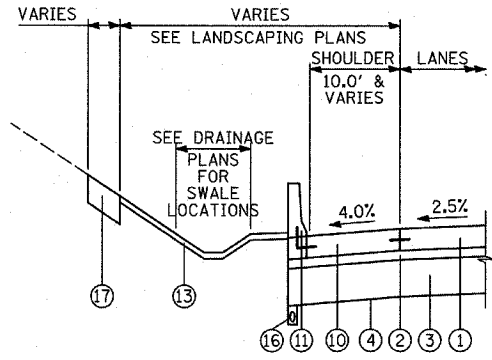
EXISTING SB I-94 MAINLINE PAVEMENT



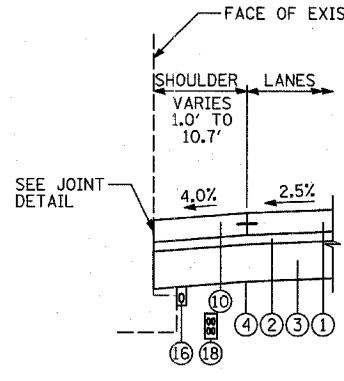
PROPOSED SB I-94 MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS



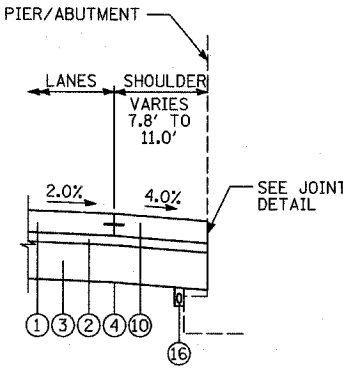
PROPOSED SB I-94 (DAN RYAN EXPWY)
ALTERNATE WEST PAVEMENT EDGE
TREATMENTS: GUARDRAIL
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



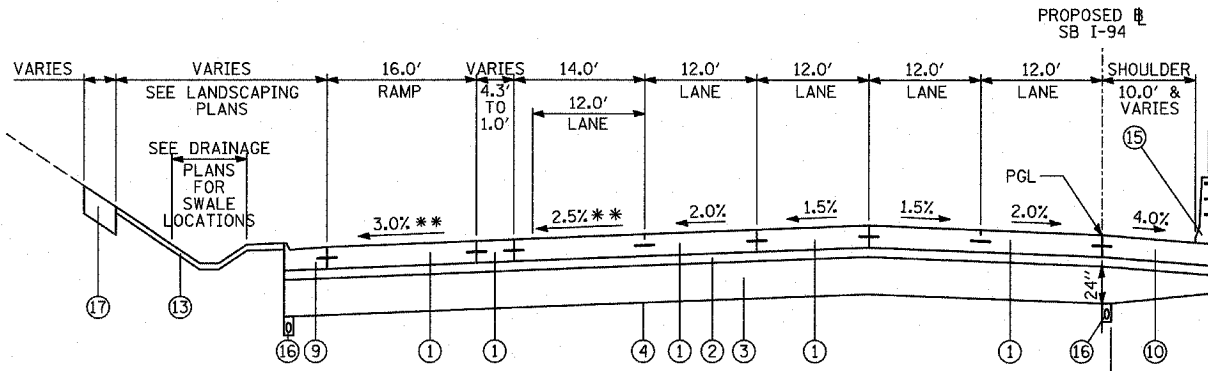
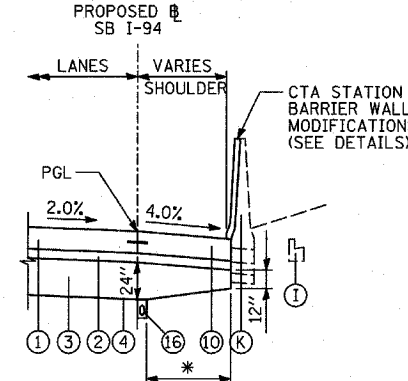
PROPOSED SB I-94 (DAN RYAN EXPWY)
ALTERNATE WEST PAVEMENT EDGE
TREATMENTS: CONCRETE BARRIER
LOCATIONS VARY - SEE EXISTING/PROPOSED PLANS



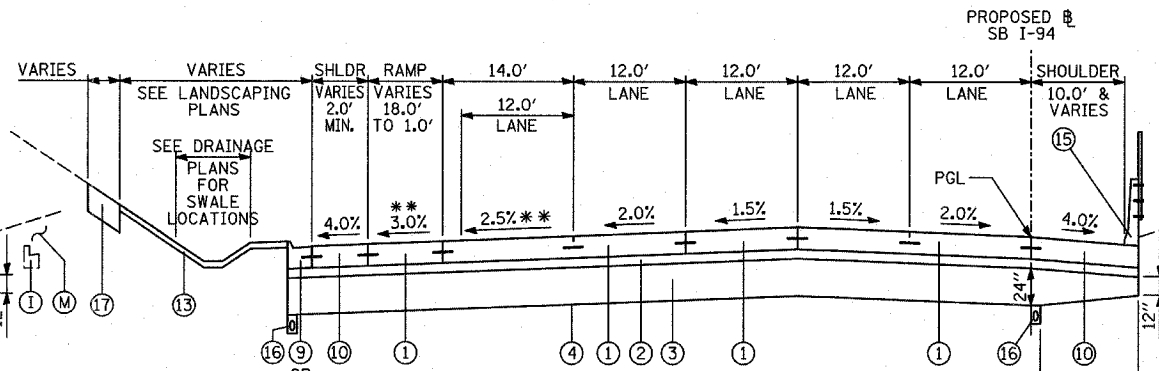
PROPOSED SB I-94 (DAN RYAN EXPWY)
ALTERNATE PAVEMENT EDGE
TREATMENTS: EXISTING PIER/ABUTMENT
LOCATIONS VARY AT BRIDGES - SEE EXISTING/PROPOSED PLANS FOR LOCATIONS AND CROSS-STREET OVERPASS FOOTING DETAILS FOR ADDITIONAL DETAIL



PROPOSED SB I-94 (DAN RYAN EXPWY)
ALTERNATE WEST PAVEMENT EDGE
TREATMENTS: CONCRETE BARRIER
ADJACENT TO CTA STATIONS - SEE EXISTING/PROPOSED PLANS



PROPOSED SB I-94 MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS



PROPOSED SB I-94 MAINLINE PAVEMENT
LOCATIONS VARY BETWEEN 95TH STREET AND 71st TO 75th C-D RAMPS

- PROPOSED LEGEND**
- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14" & PAVEMENT REINFORCEMENT, 14"
 - STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL) (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 1/2" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
 - 3/4" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 3/4"
 - 1 1/4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 1/4"
 - 4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 1/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 1 1/2"±
 - BIT CONC BINDER COURSE, 4 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - PCC SHOULDERS, 11"±
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADE IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL) (SPECIAL) (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - JOINTED PCC PAVEMENT, 11"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 24"±
 - SUB-BASE GRANULAR MATERIAL, 12"±

NOTES:

- REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

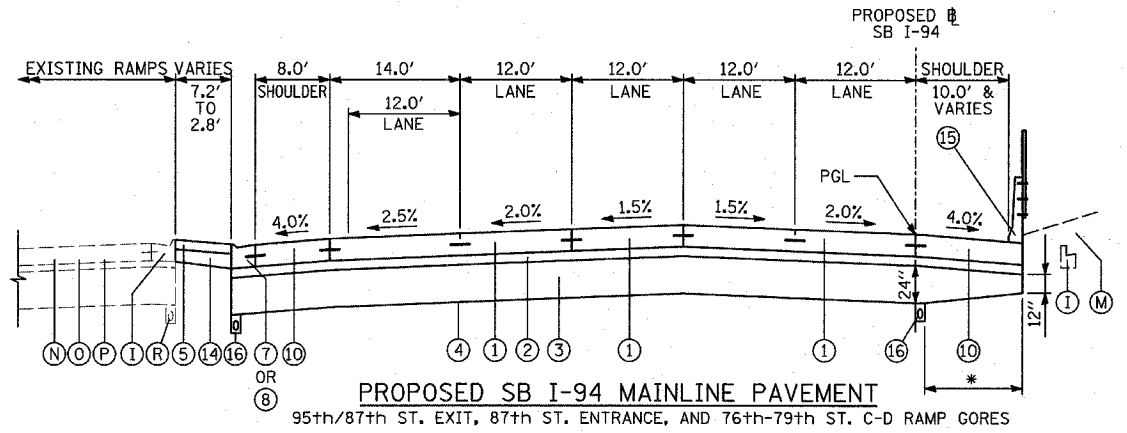
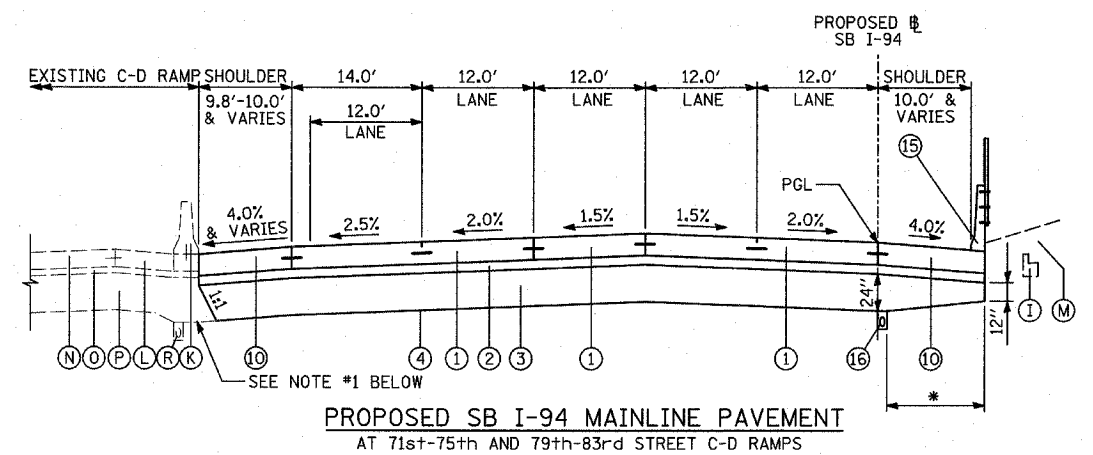
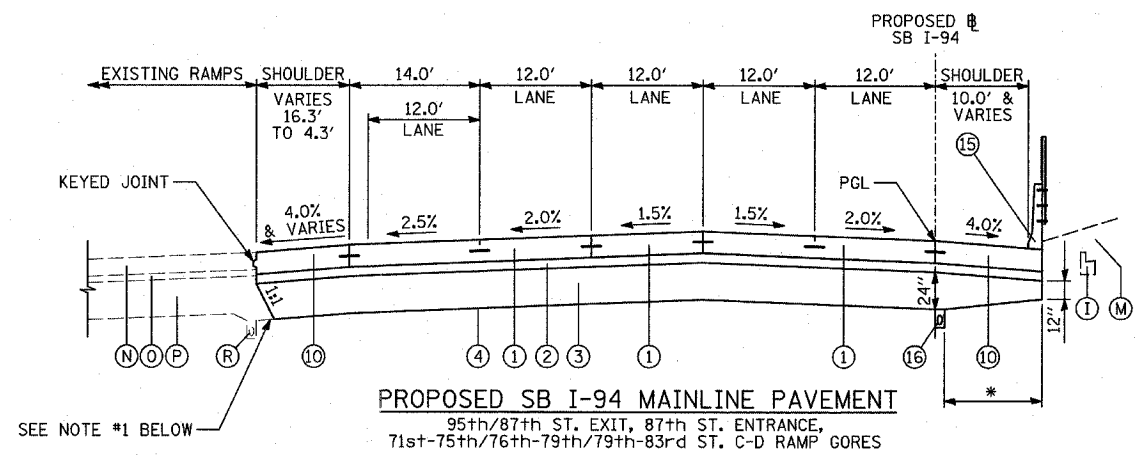
* - VARYING DEPTH PAID FOR AS "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

** - CROSS-SLOPES VARY AT 87TH STREET ENTRANCE RAMP

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. 94 (DAN RYAN EXPRESSWAY)
NAME	DATE	
		EXISTING & PROPOSED TYPICAL SECTIONS SB I-94 (DAN RYAN EXPRESSWAY) (SHEET 6 OF 8)

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: TGB

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- PROPOSED LEGEND**
- 1 CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
 - 2 STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - 3 SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - 4 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - 5 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - 6 CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - 7 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - 8 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - 9 COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - 10 PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - 11 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 12 TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - 13 TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - 14 AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - 15 CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 16 PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - 17 TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - 18 ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - 19 CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - 20 PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 21 1/2" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
 - 22 1 3/4" SHOULDER RESURFACING; BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
 - 23 1 3/4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
 - 24 4" MAINLINE RESURFACING; POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - 25 PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- (A) BIT CONC SURFACE COURSE, 1 1/2"±
 - (B) BIT CONC BINDER COURSE, 1 1/2"±
 - (C) BIT CONC BINDER COURSE, 3 3/4"±
 - (D) SUB-BASE GRANULAR MATERIAL, 4"±
 - (E) SUB-BASE GRANULAR MATERIAL, 6"±
 - (F) CRUSHED STONE, 5"±
 - (G) PCC SHOULDERS, 9"±
 - (H) PCC BASE COURSE, 9"±
 - (I) COMB CONC CURB & GUTTER
 - (J) PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - (K) CONCRETE BARRIER WALL
 - (L) PCC SHOULDERS, 11"±
 - (M) CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - (N) JOINTED PCC PAVEMENT, 11"±
 - (O) STABILIZED SUB-BASE, 4"±
 - (P) SUB-BASE GRANULAR MATERIAL, 24"±
 - (Q) SUB-BASE GRANULAR MATERIAL, 12"±

NOTES:

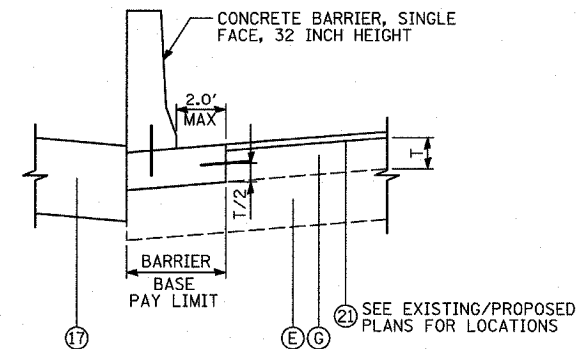
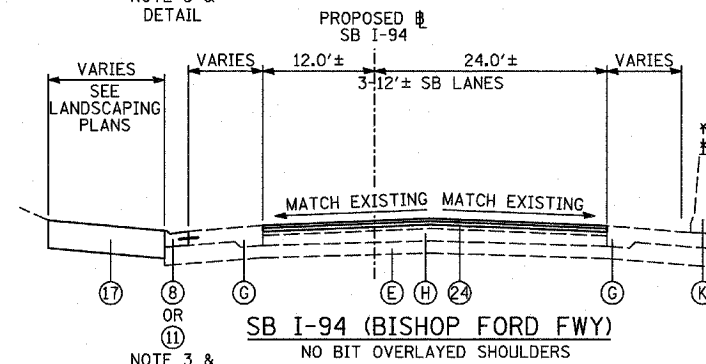
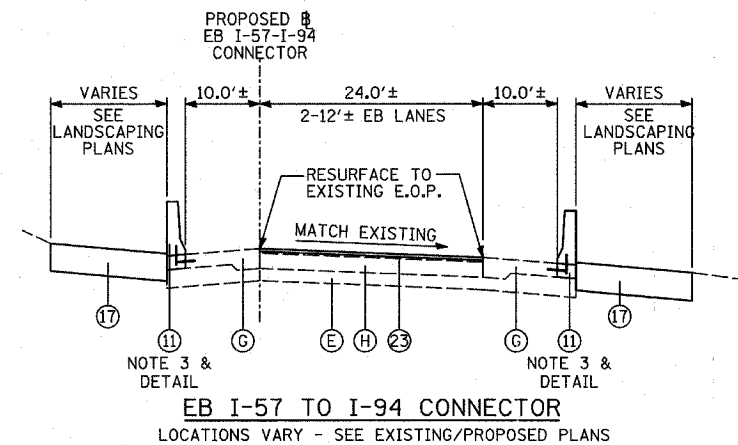
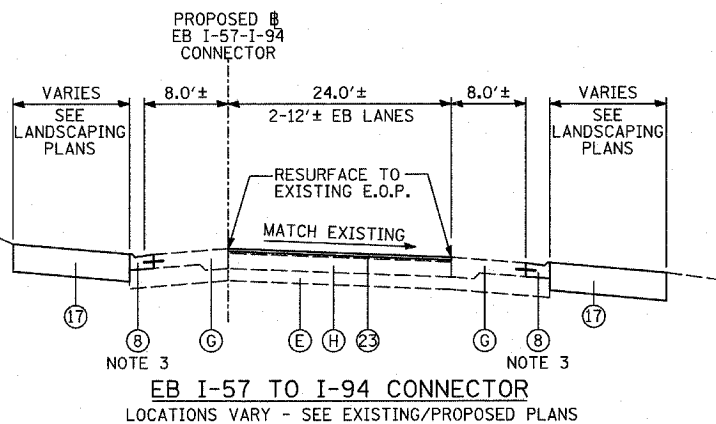
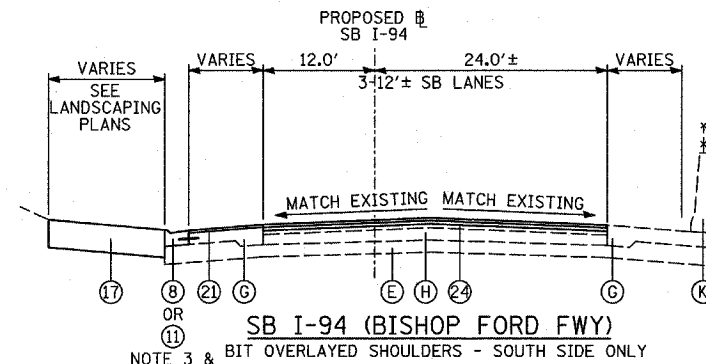
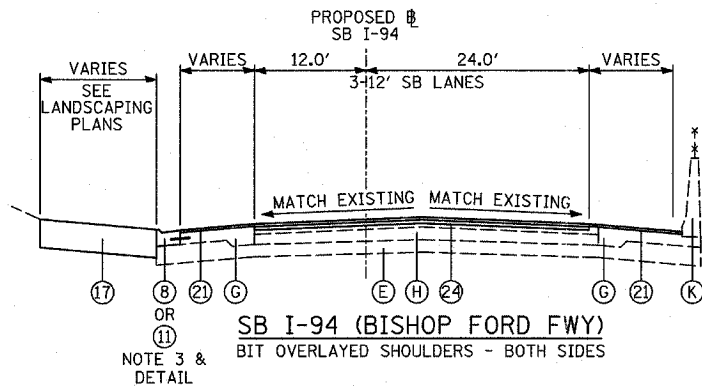
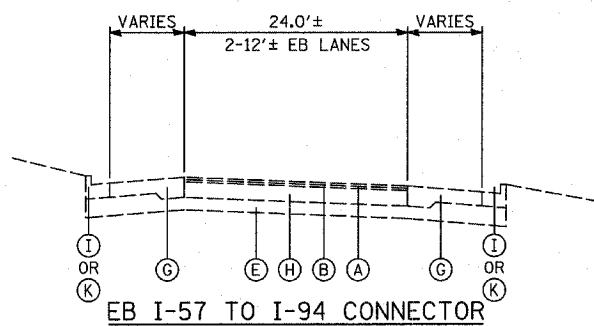
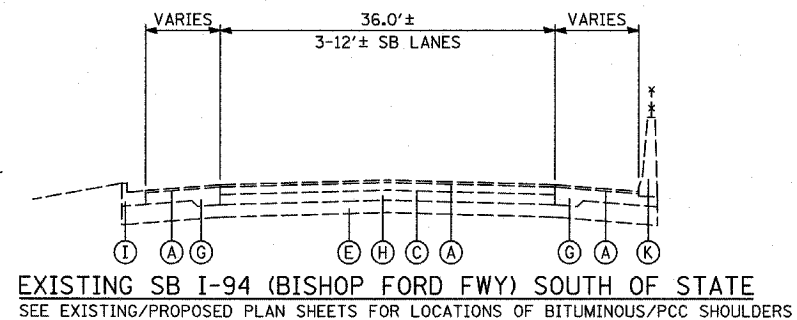
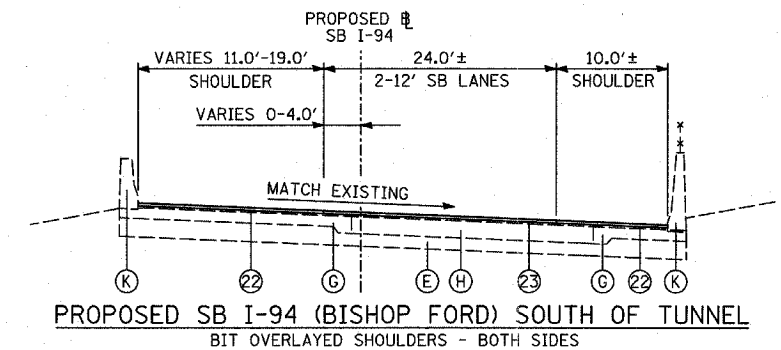
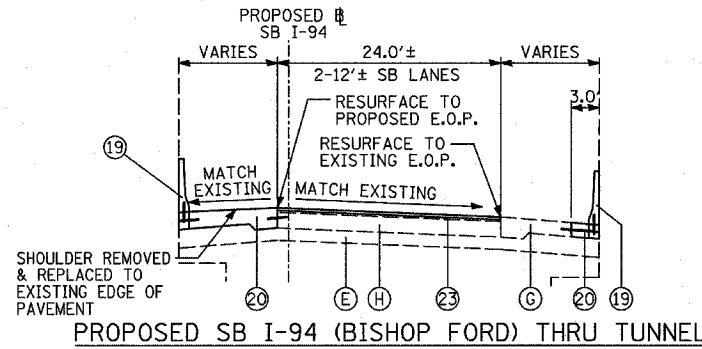
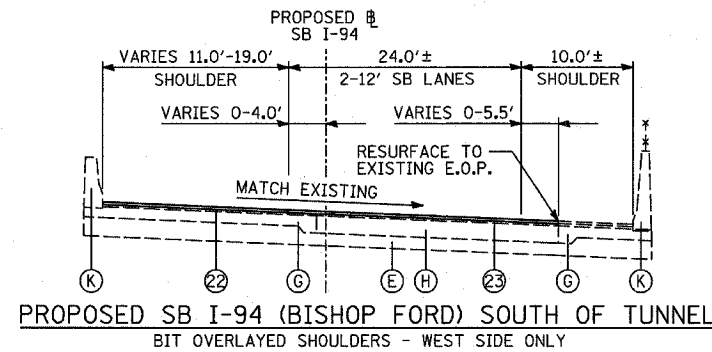
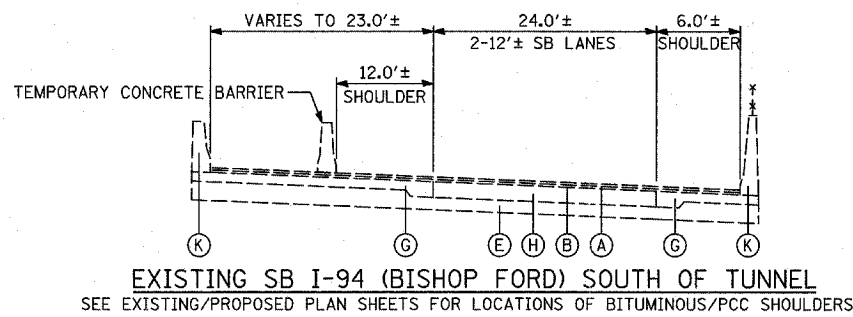
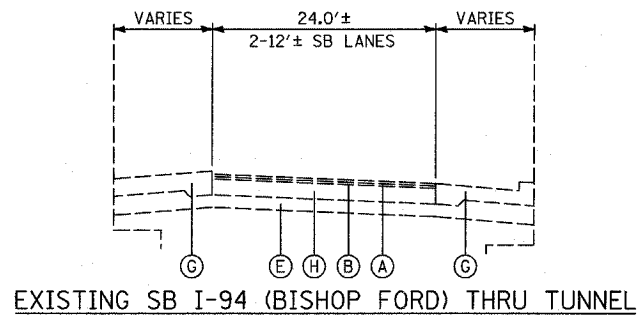
1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.
2. ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

* - VARYING DEPTH PAID FOR AS "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. 94 (DAN RYAN EXPRESSWAY)
NAME	DATE	
		PROPOSED TYPICAL SECTIONS SB I-94 (DAN RYAN EXPRESSWAY) (SHEET 7 OF 8)
SCALE: NONE	DRAWN BY: MPG	CHECKED BY: TGB
DATE: MARCH 1, 2006		

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	22
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				
				62593



- PROPOSED LEGEND**
- CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT 14"; & PAVEMENT REINFORCEMENT, 14"
 - STABILIZED SUB-BASE, 6" (BITUMINOUS AGGREGATE MIXTURE)
 - SUB-BASE GRANULAR MATERIAL, TYPE B 24"
 - GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL)
 - CONCRETE MEDIAN SURFACE, 6 INCH (SPECIAL) (WITHOUT STAMPED PATTERN)
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24
 - COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48
 - PORTLAND CEMENT CONCRETE SHOULDERS 14"
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT; BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - TRAFFIC BARRIER TERMINAL, TYPE VARIES
 - TOPSOIL FURNISH AND PLACE, 4"; SEEDING, CLASS 2A; EROSION CONTROL BLANKET
 - AGGREGATE FILL (INCLUDED IN THE COST OF "CONCRETE MEDIAN SURFACE, 6" (SPECIAL)") (MATCH DEPTH TO ADJACENT CURB & GUTTER)
 - CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL); BARRIER BASE; CTA FENCE (SEE DETAILS); BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PIPE UNDERDRAIN, 6" (SEE DETAILS)
 - TOPSOIL FURNISH AND PLACE, 12"; COMPOST FURNISH AND PLACE, 6"; EROSION CONTROL BLANKET; SEEDING (SEE PLAN FOR CLASS)
 - ELECTRICAL DUCTBANK (SEE ELECTRICAL INFRASTRUCTURE PLANS)
 - CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED); BARRIER BASE; BARRIER WALL MARKERS (PER IDOT STD. 635001 & 635006)
 - PORTLAND CEMENT CONCRETE SHOULDERS 9"
 - 1/2" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1/2"
 - 1 3/4" SHOULDER RESURFACING: BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 3/4"
 - 1 3/4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
 - 4" MAINLINE RESURFACING: POLYMERIZED BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "E", N105, 1 3/4"; POLYMERIZED BITUMINOUS CONCRETE BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
 - PORTLAND CEMENT CONCRETE PAVEMENT, 14" (JOINTED)

- EXISTING LEGEND** ALL EXISTING PAVEMENT DEPTHS ARE FROM AS-BUILT PLANS AND ARE SUBJECT TO CHANGE
- BIT CONC SURFACE COURSE, 1/2"±
 - BIT CONC BINDER COURSE, 1/2"±
 - BIT CONC BINDER COURSE, 3/4"±
 - SUB-BASE GRANULAR MATERIAL, 4"±
 - SUB-BASE GRANULAR MATERIAL, 6"±
 - CRUSHED STONE, 5"±
 - PCC SHOULDERS, 9"±
 - PCC BASE COURSE, 9"±
 - COMB CONC CURB & GUTTER
 - PCC PAVEMENT, 10"± (W/ PAVEMENT FABRIC, 80 LBS±/100 SF)
 - CONCRETE BARRIER WALL
 - PCC SHOULDERS, 11"±
 - CTA BALLAST STONE; REGRADE AS NECESSARY (INCLUDE REGRADING IN THE COST OF "CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)")
 - JOINTED PCC PAVEMENT, 11"±
 - STABILIZED SUB-BASE, 4"±
 - SUB-BASE GRANULAR MATERIAL, 24"±
 - SUB-BASE GRANULAR MATERIAL, 12"±

NOTES:

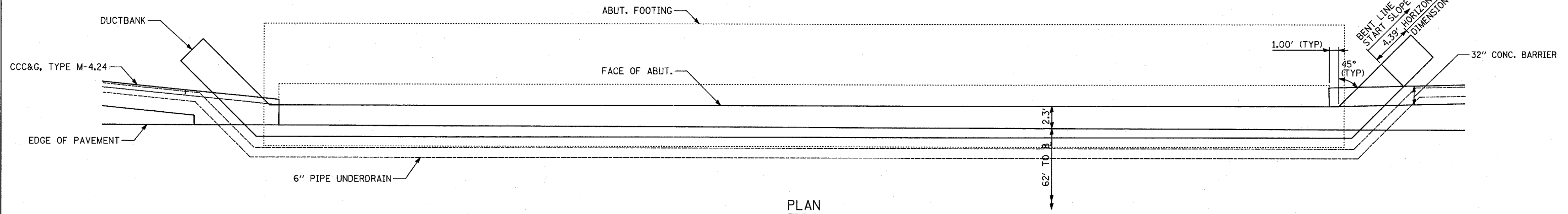
1. REFER TO PAVEMENT JOINTING AND ELEVATION PLANS FOR DESCRIPTIONS AND DETAILS OF PAVEMENT JOINTS.

2. ANY REQUIRED REGRADING OF EXISTING ADJACENT SUB-BASE GRANULAR MATERIAL SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

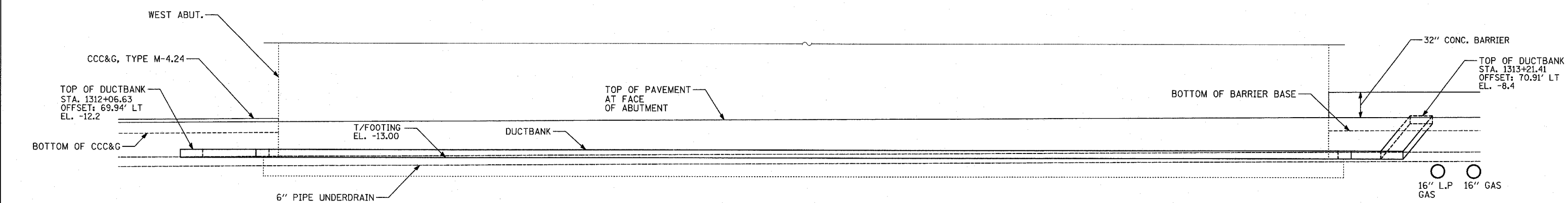
3. PROPOSED CURB OR BARRIER BASE SHALL BE PLACED ON EXISTING SUBGRADE WITH THICKNESS EQUAL TO EXISTING ADJACENT PCC SHOULDERS. RECOMPACTING OF EXISTING SUBGRADE OR PLACEMENT OF ADDITIONAL AGGREGATE SHALL BE INCLUDED IN THE COST OF "COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24, OR "CONCRETE BARRIER, SINGLE FACE", OF THE TYPE SPECIFIED.

REVISIONS	
NAME	DATE

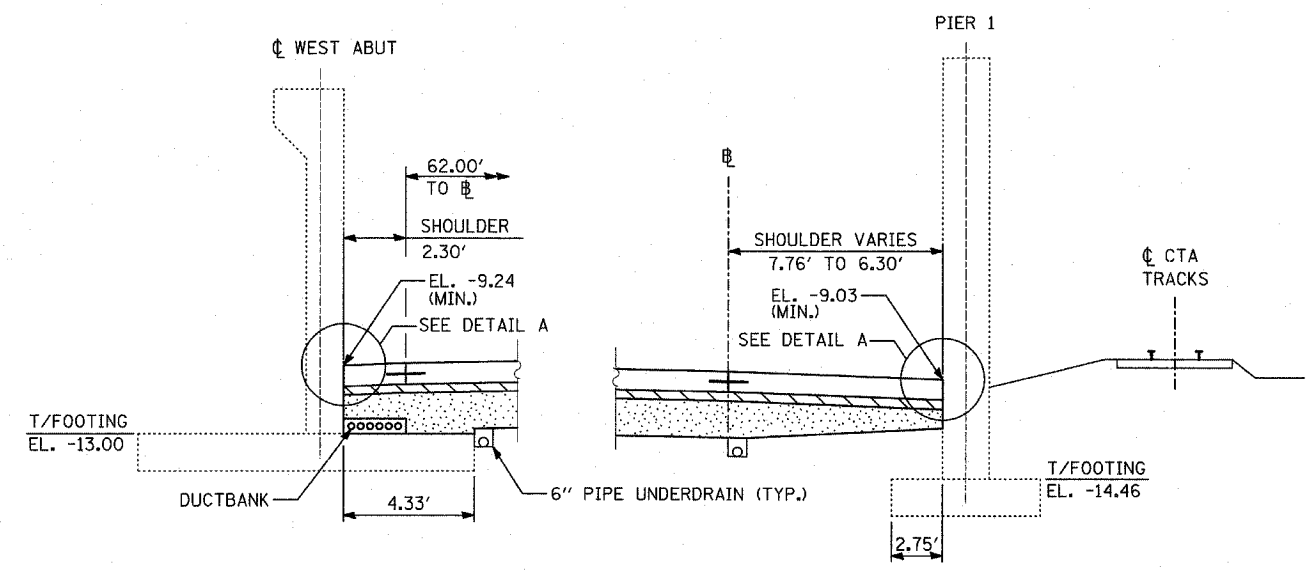
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING & PROPOSED TYPICAL SECTIONS INTERCHANGE AREA (SHEET 8 OF 8)
 SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: TGB



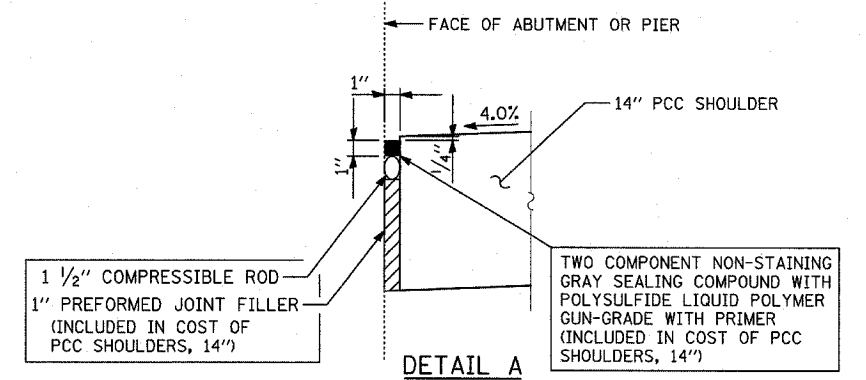
PLAN



ELEVATION



SHOULDER DETAIL AT 79th STREET



DETAIL A

SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

NOTES

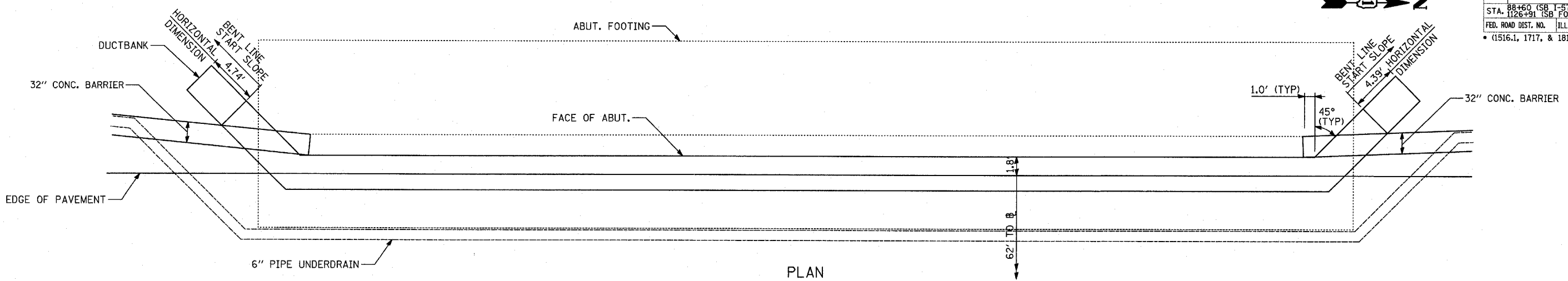
- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

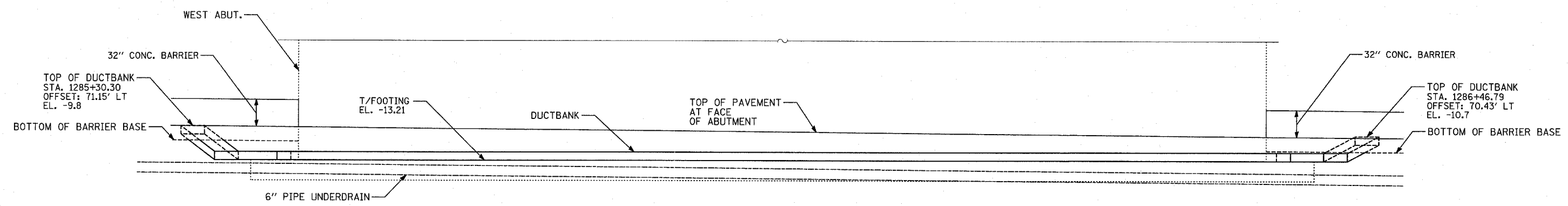
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 79TH. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB

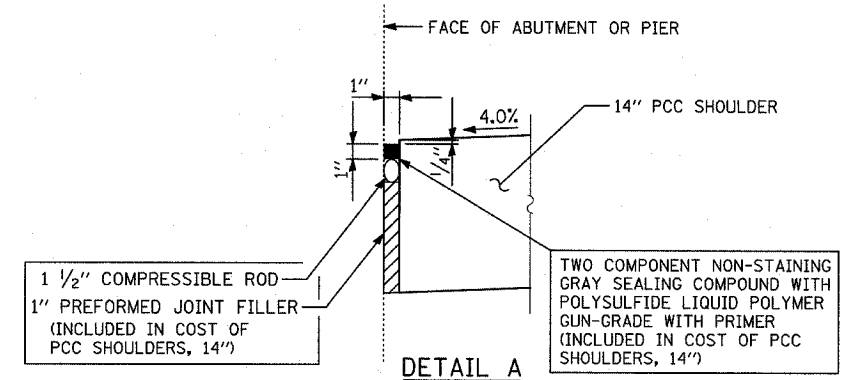
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	23
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				



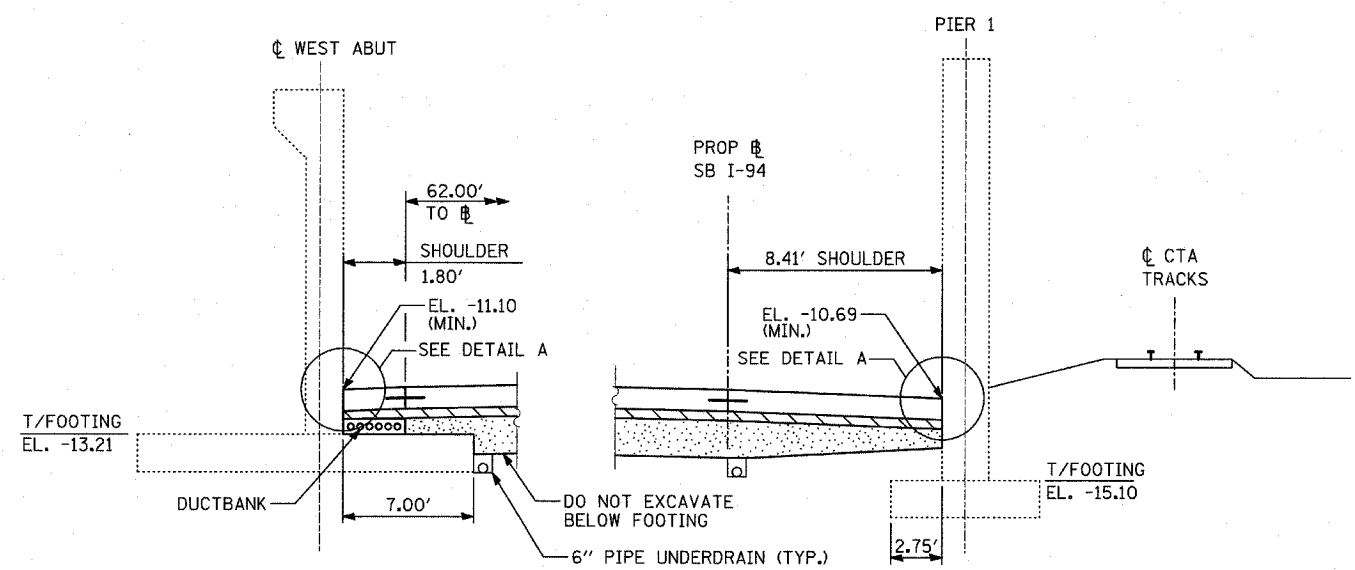
PLAN



ELEVATION



DETAIL A



SHOULDER DETAIL AT 83rd STREET

NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

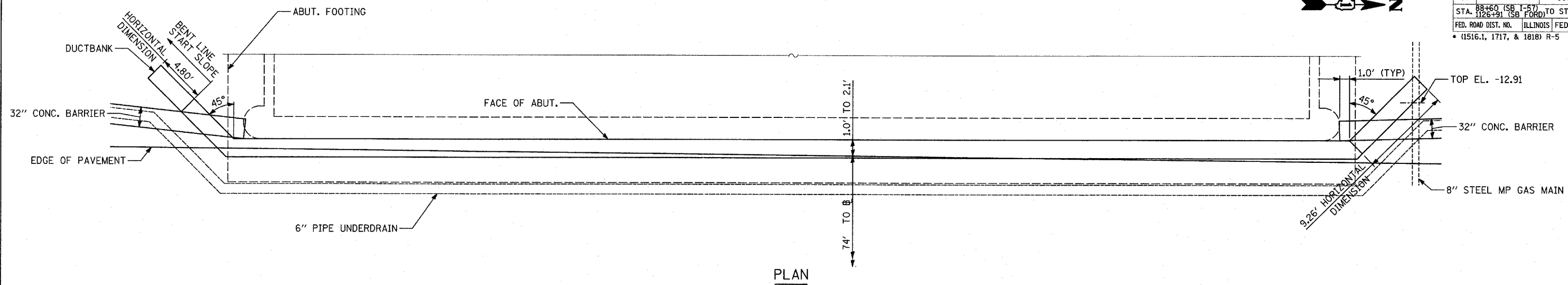
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 83RD. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

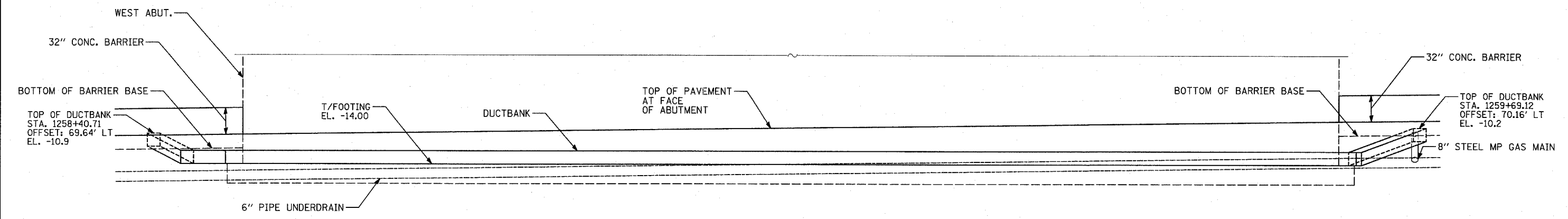
SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB

TYLIN INTERNATIONAL

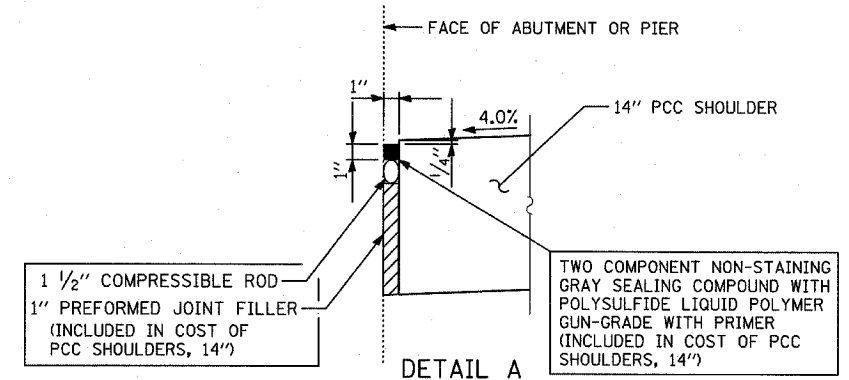
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	24
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				



PLAN

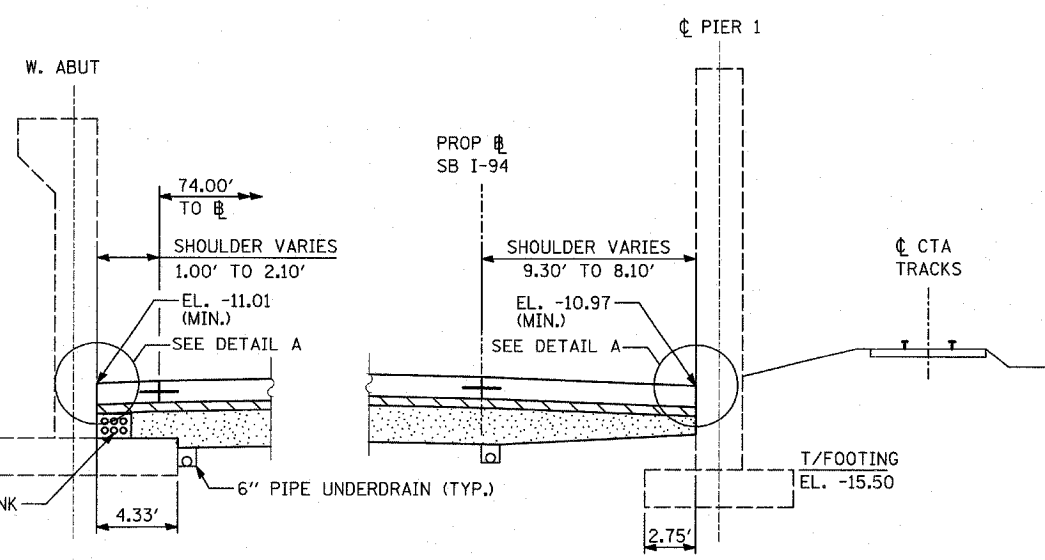


ELEVATION



DETAIL A

SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"



SHOULDER DETAIL AT 87th STREET

NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

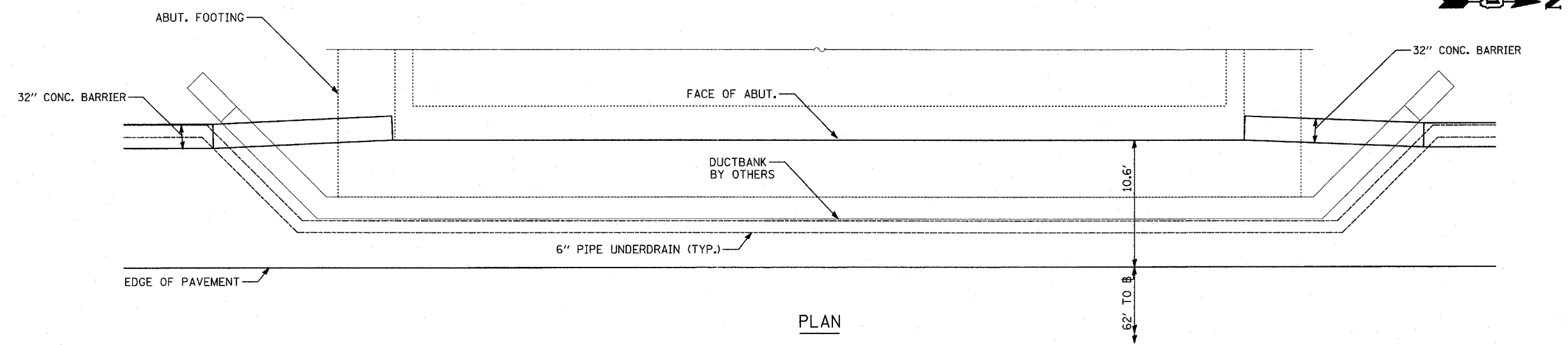
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 87TH STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

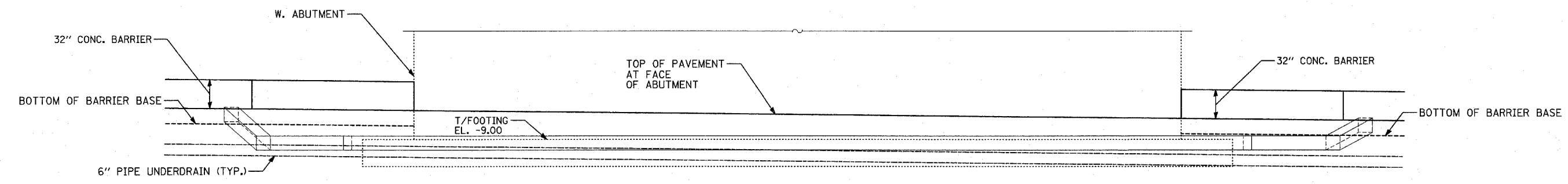
SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB

TYLIN INTERNATIONAL

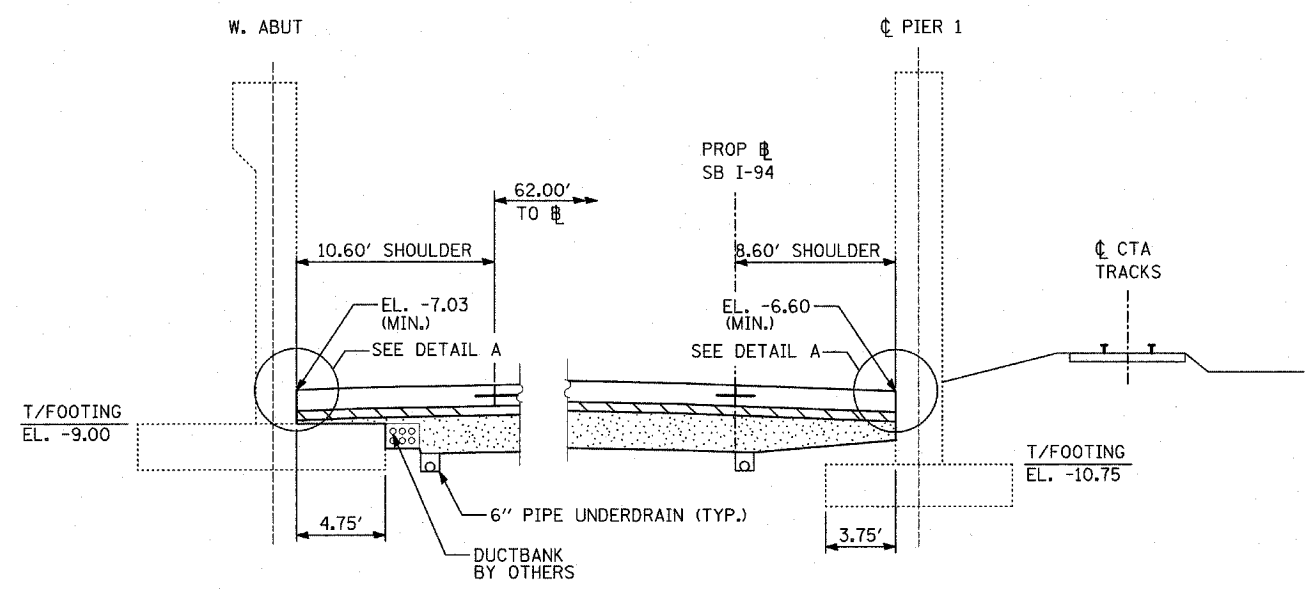
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	25
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				



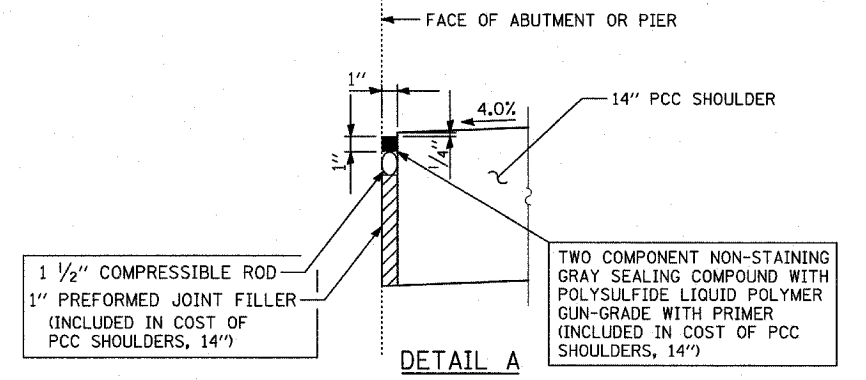
PLAN



ELEVATION



SHOULDER DETAIL AT 91st STREET



SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

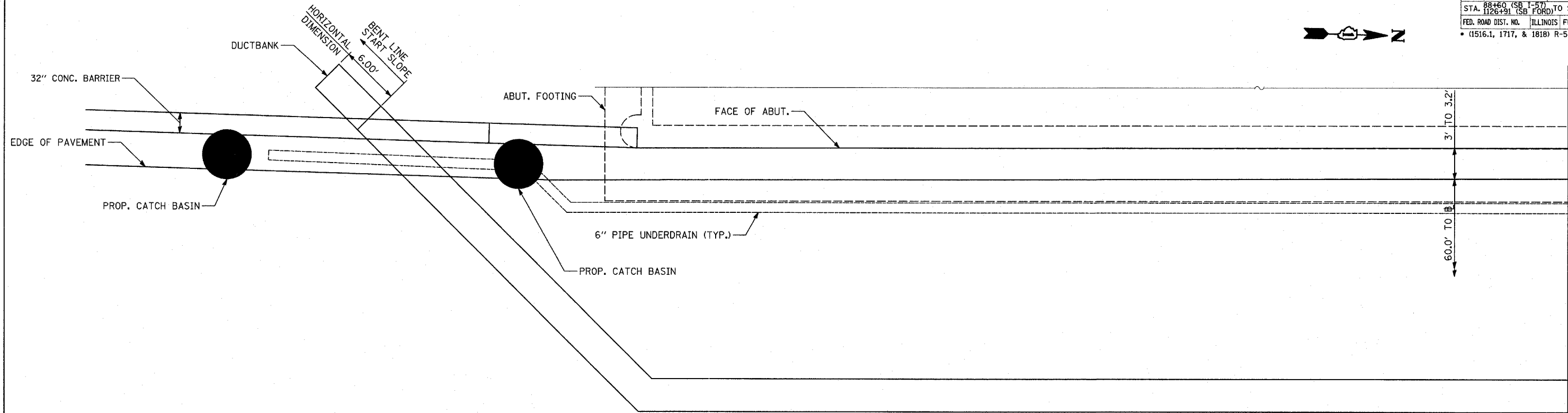
NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE Poured DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

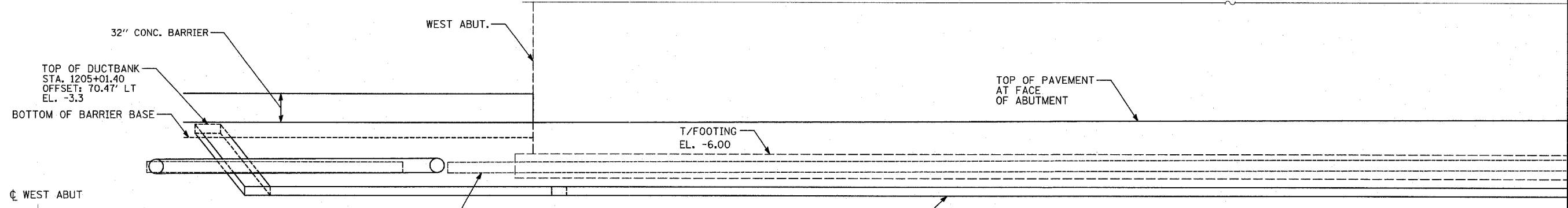
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 91ST. STREET OVERPASS FOOTINGS &
 UNDERDRAIN DETAILS

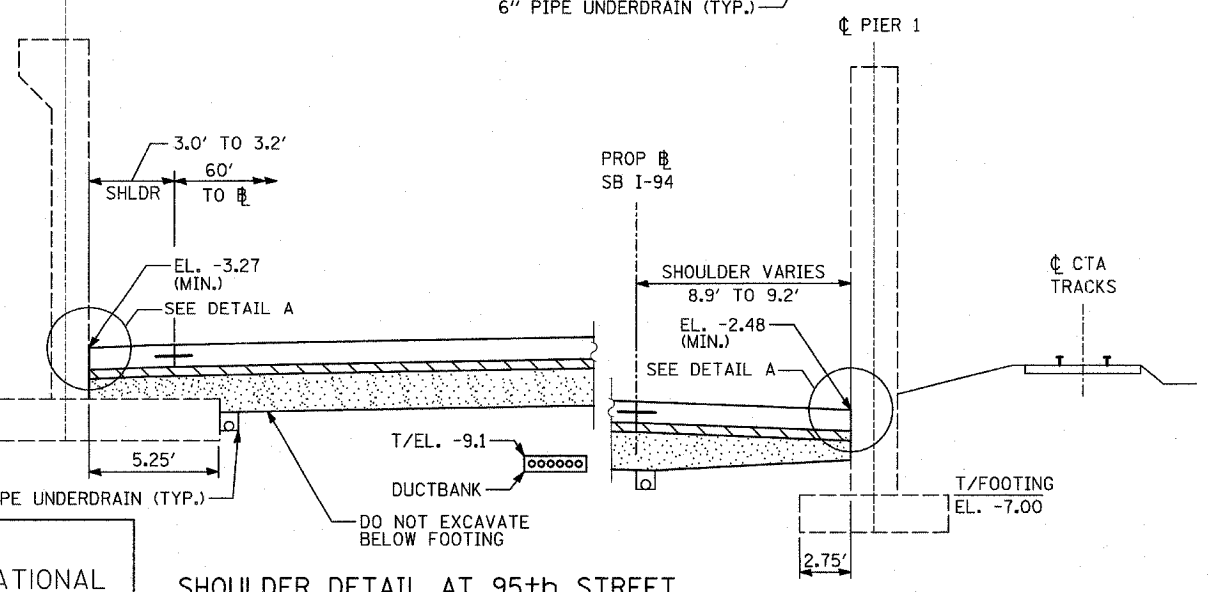
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 CHECKED BY: TGB



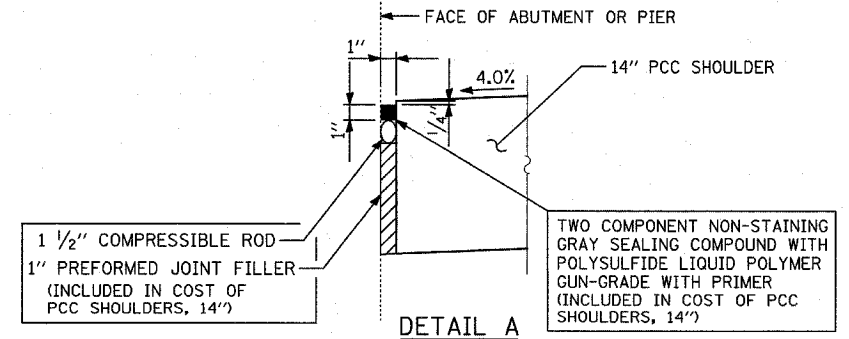
PLAN



ELEVATION



SHOULDER DETAIL AT 95th STREET



DETAIL A

NOTES

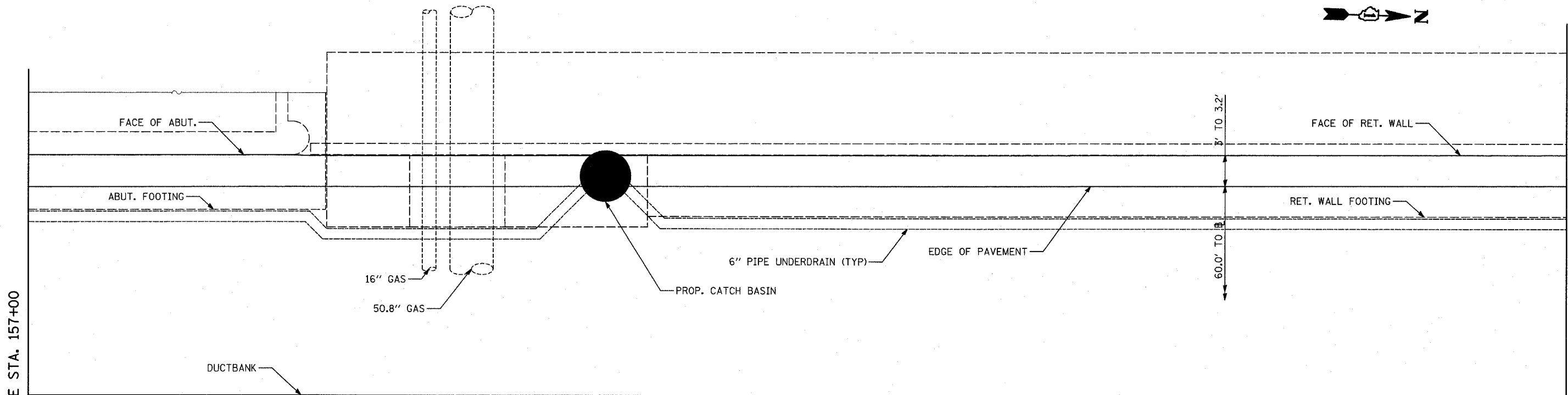
1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14\"/>
- 6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- 7. MAINTAIN 3\"/>

REVISIONS	
NAME	DATE

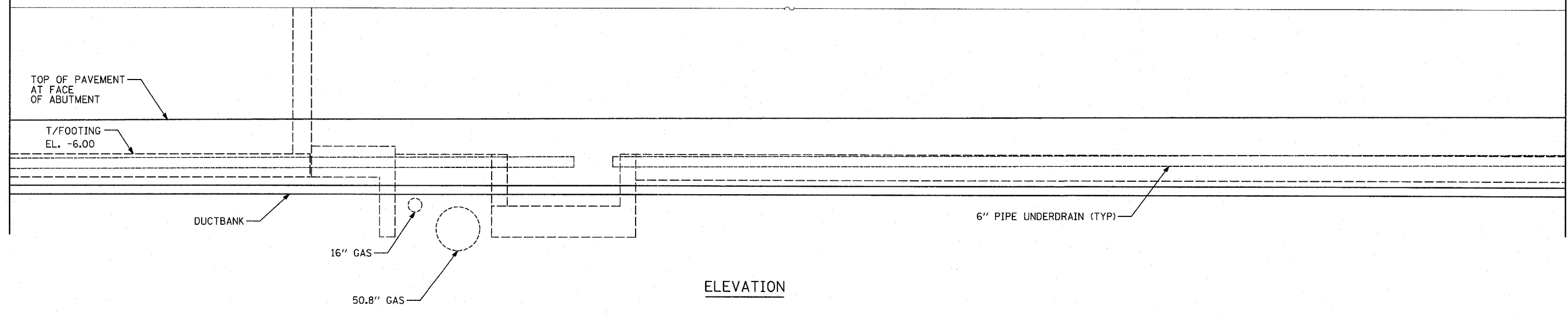
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 1 OF 4

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB

MATCHLINE STA. 157+00



PLAN



ELEVATION

NOTE:

THE GAS MAIN ELEVATIONS ARE TAKEN FROM SUE DATA. SEE SUE SHEETS FOR MORE INFORMATION.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 2 OF 4

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB

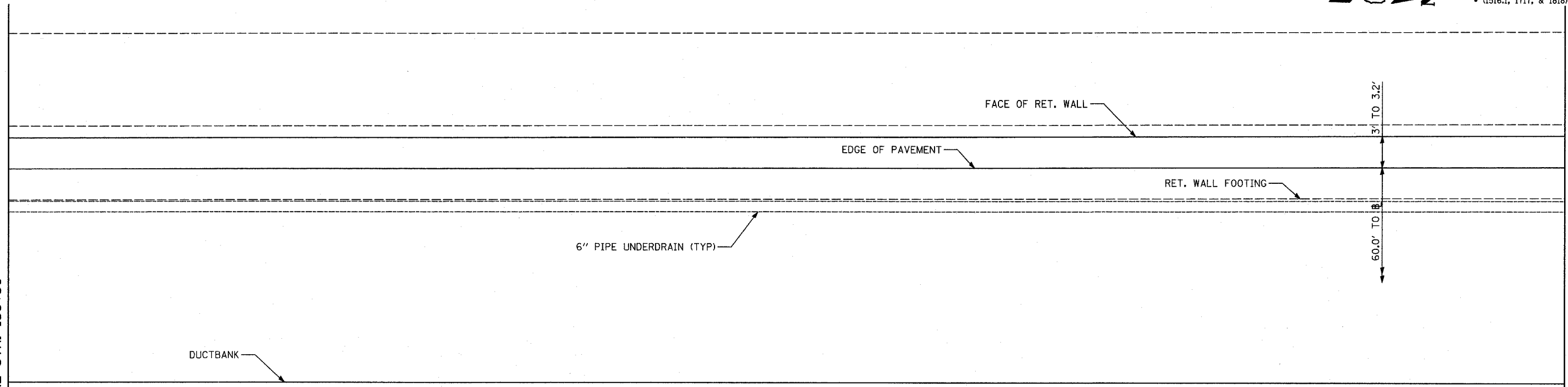


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	28
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				



MATCHLINE STA. 158+50

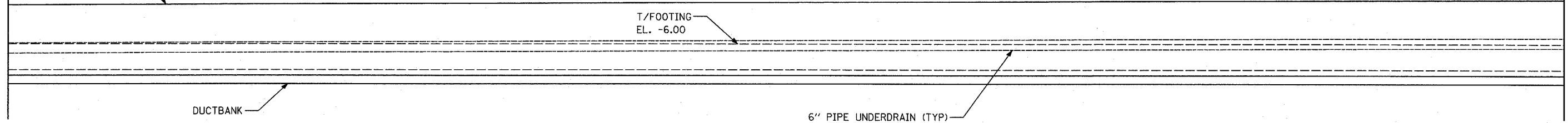
MATCHLINE STA. 160+00



PLAN

TOP OF PAVEMENT
AT FACE OF
RET. WALL

T/FOOTING
EL. -6.00



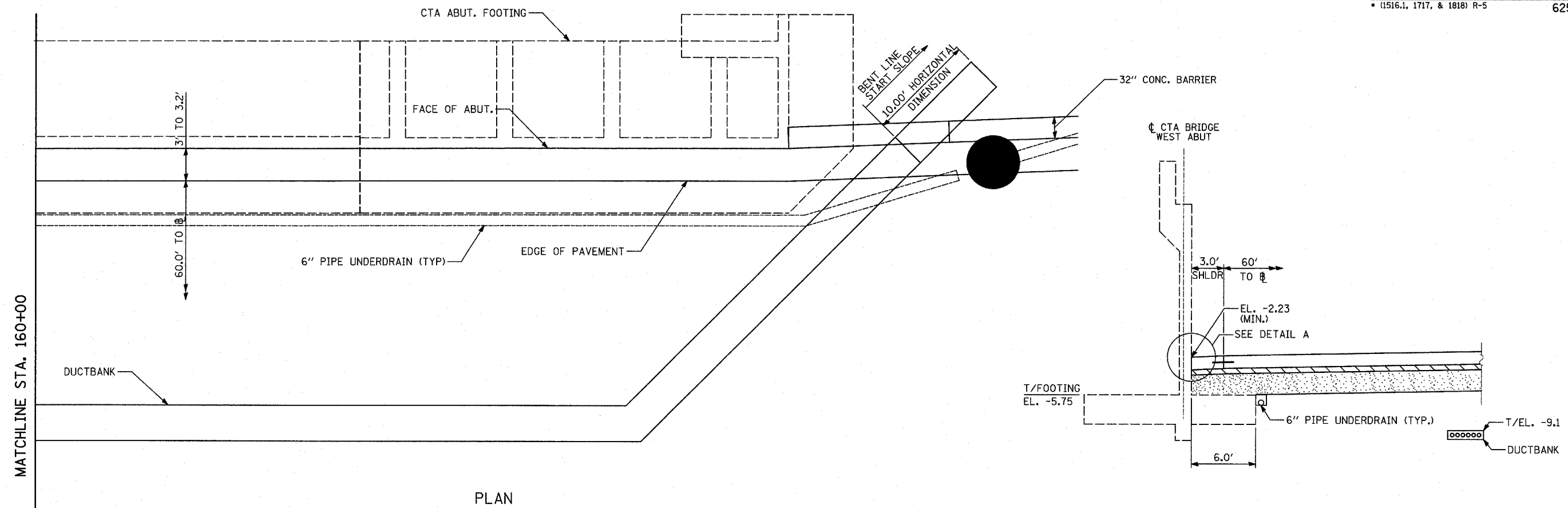
ELEVATION

TYLIN INTERNATIONAL

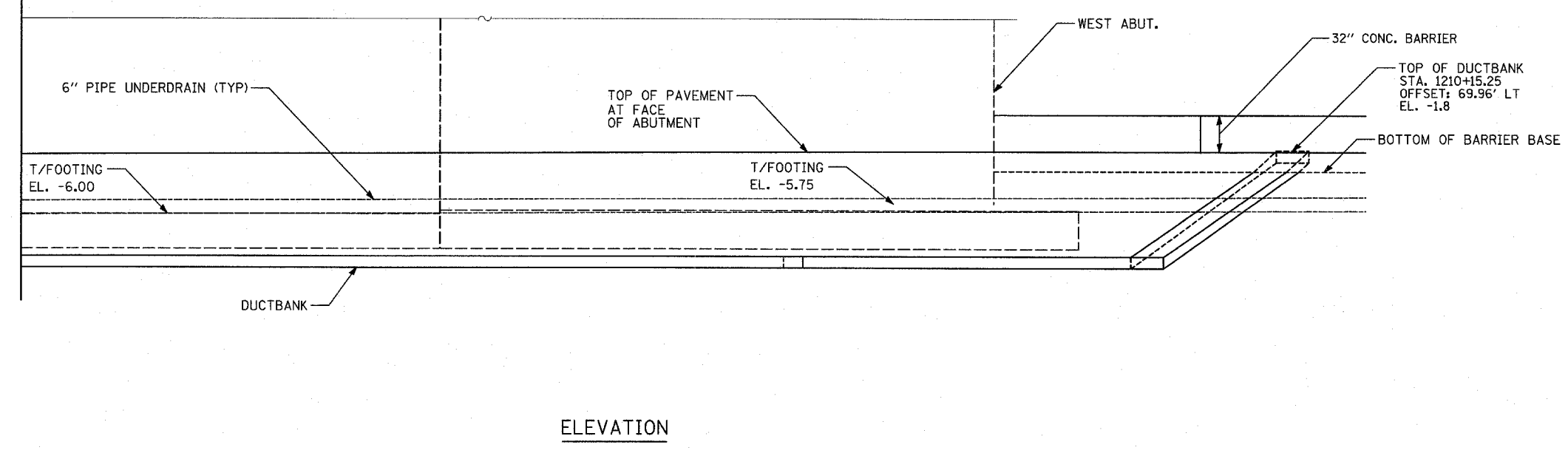
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 3 OF 4

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB



SHOULDER DETAIL AT CTA BRIDGE



ELEVATION

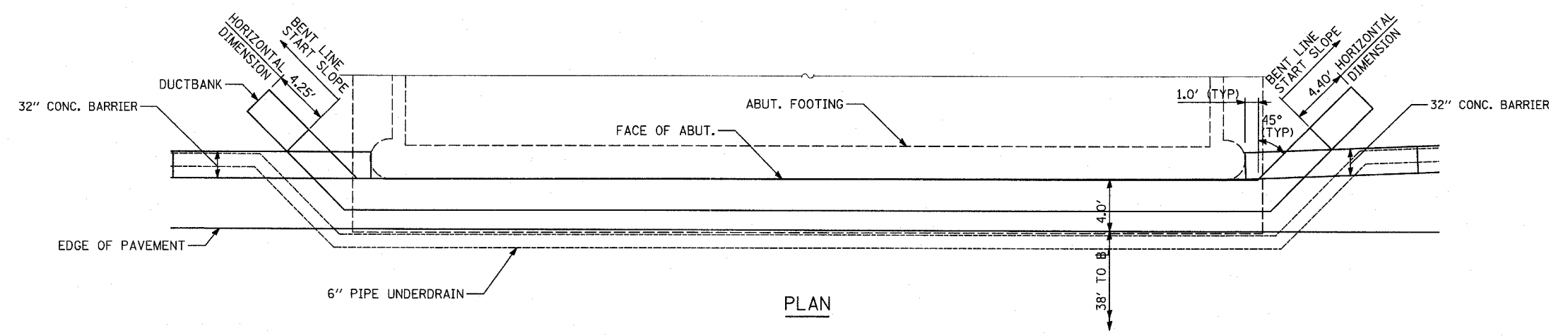
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

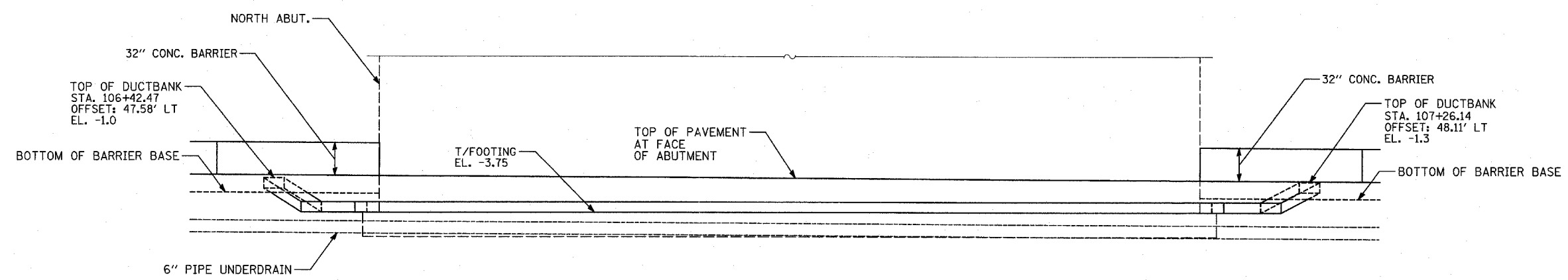
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 95TH. STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS
 SHEET 4 OF 4

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB, PL
 CHECKED BY: TGB

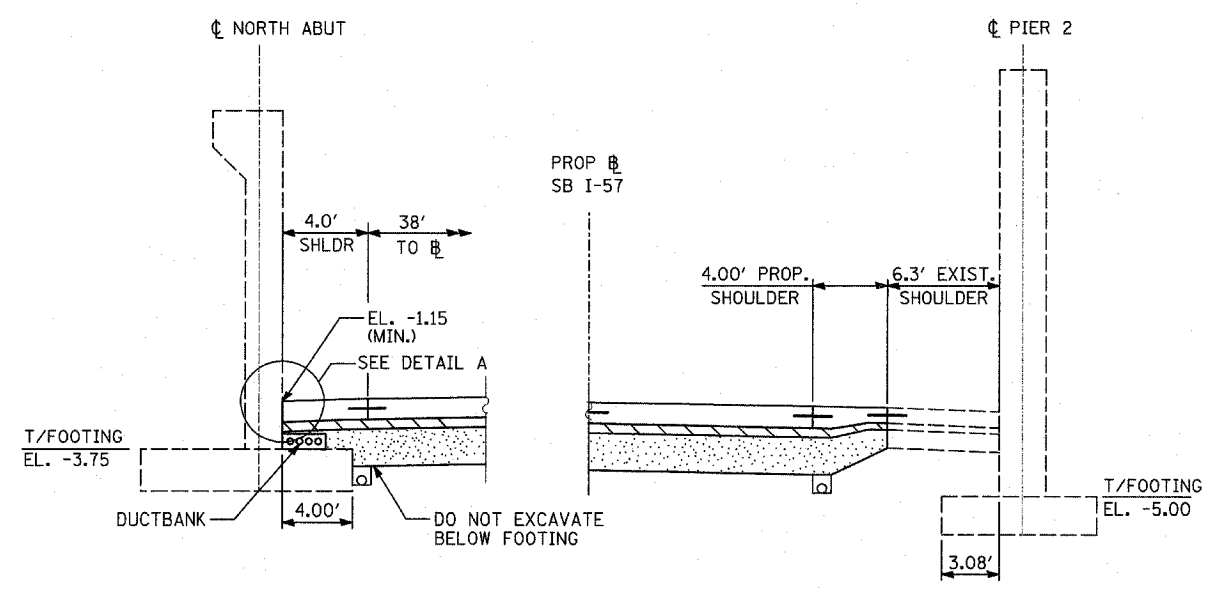
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	30
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5 62593				



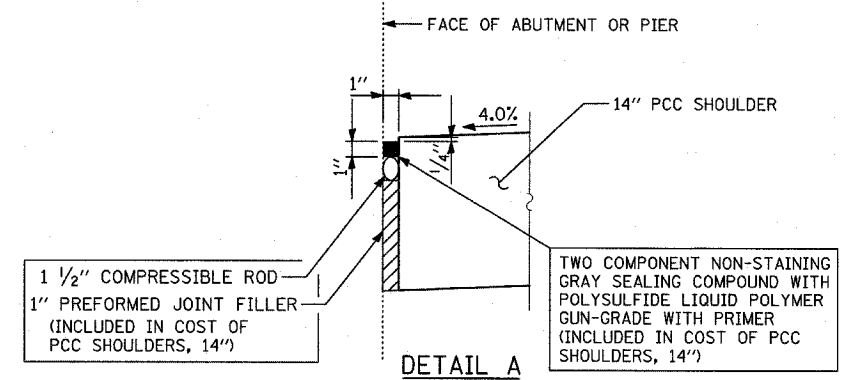
PLAN



ELEVATION



SHOULDER DETAIL AT S. PARNELL AVENUE



DETAIL A

SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

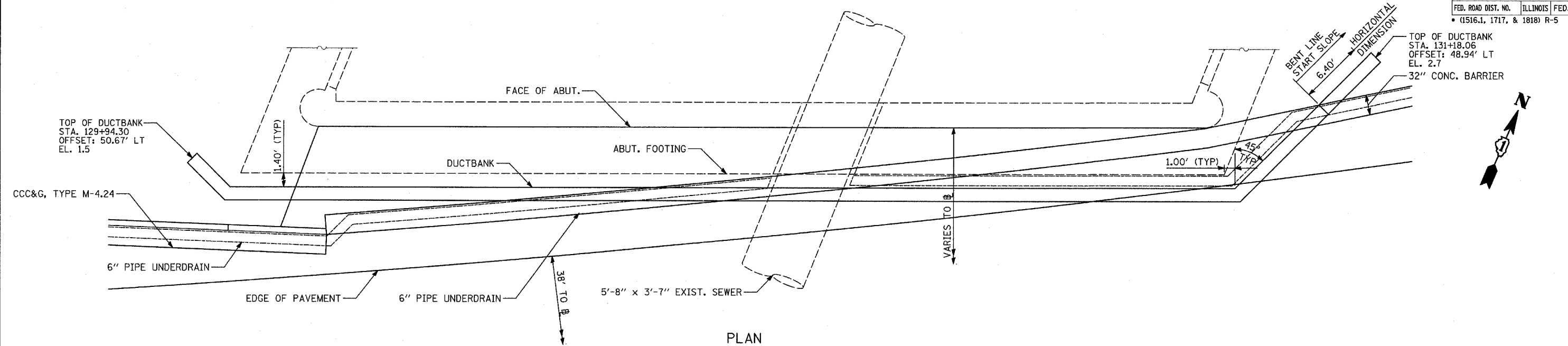
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 PARNELL AVE. OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB

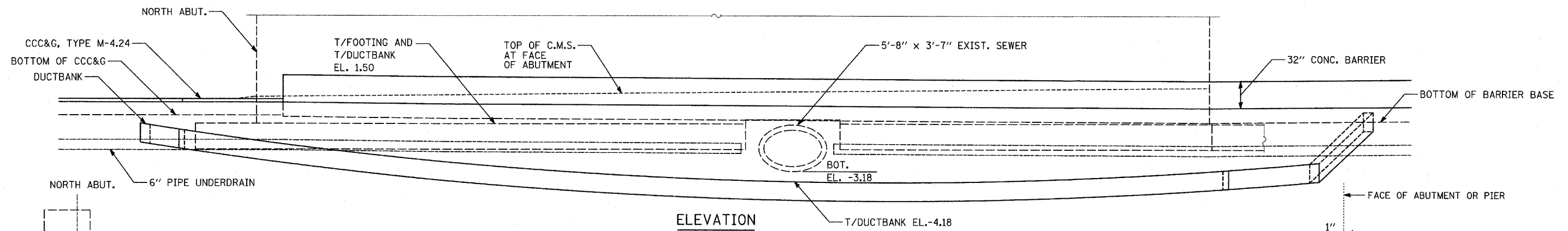


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	31
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				

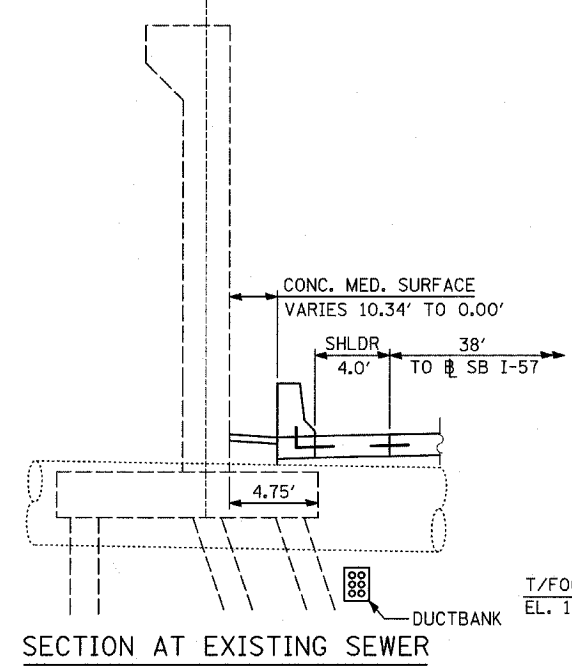
62593



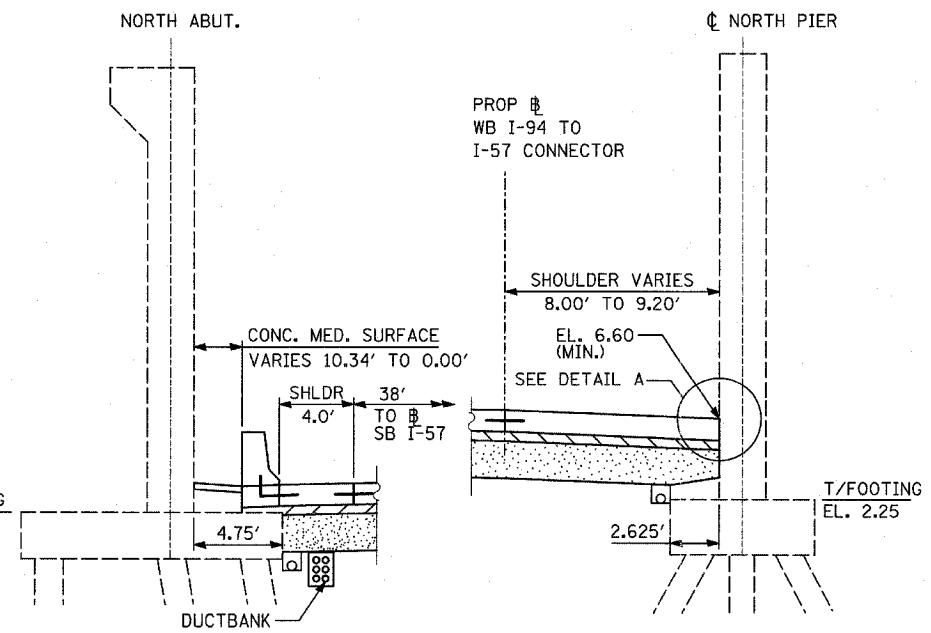
PLAN



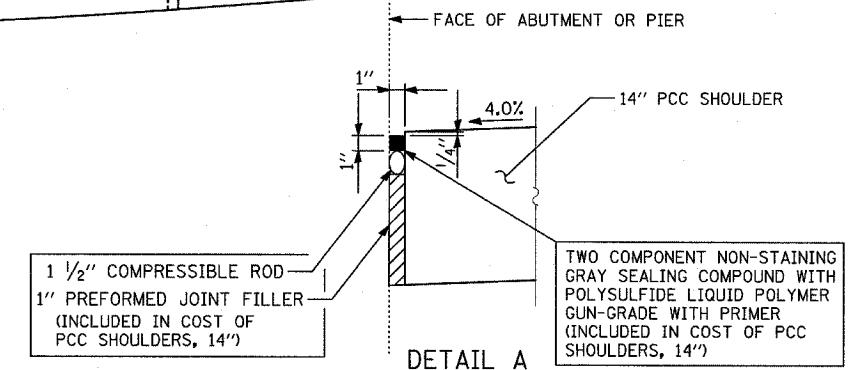
ELEVATION



SECTION AT EXISTING SEWER



SHOULDER DETAIL AT WENTWORTH AVENUE



DETAIL A

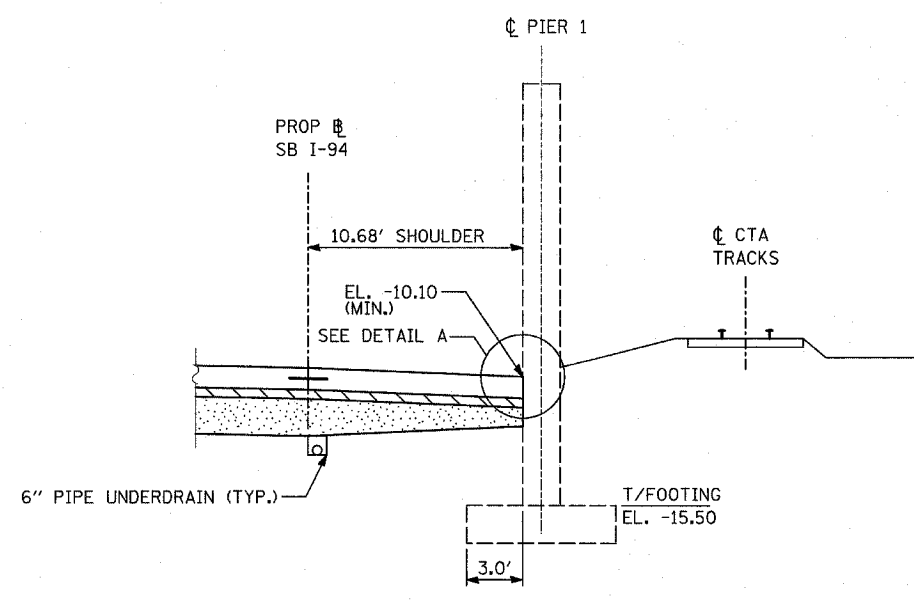
SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

NOTES

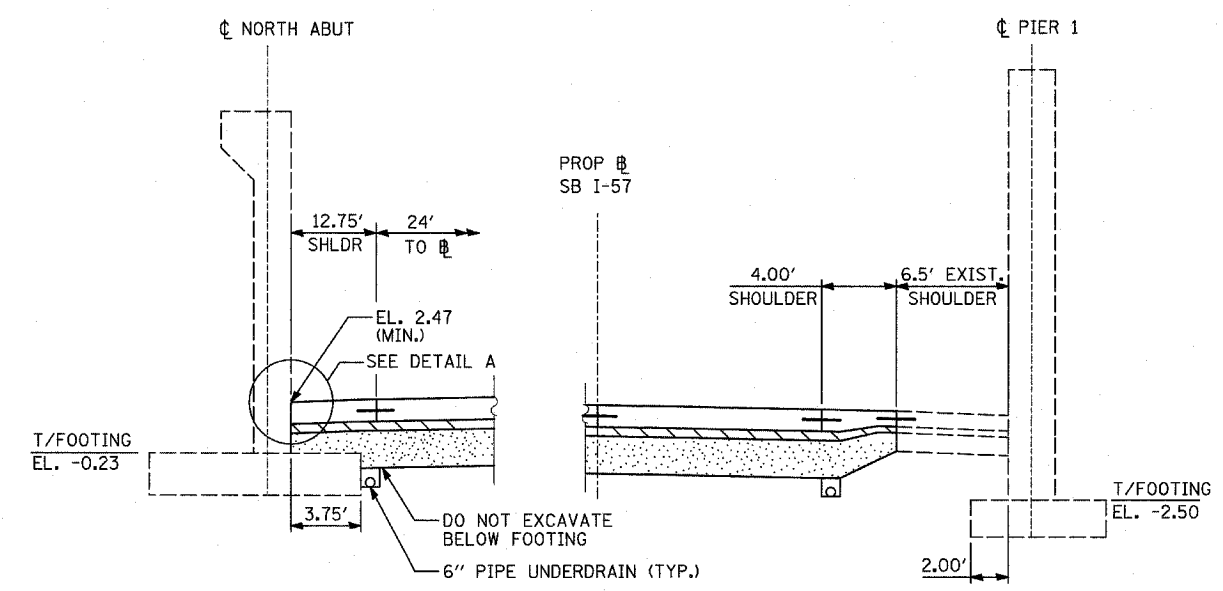
1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. CONTRACTOR SHALL SUPPORT EXISTING SEWER WHEN EXCAVATING FOR DUCTBANK.
8. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

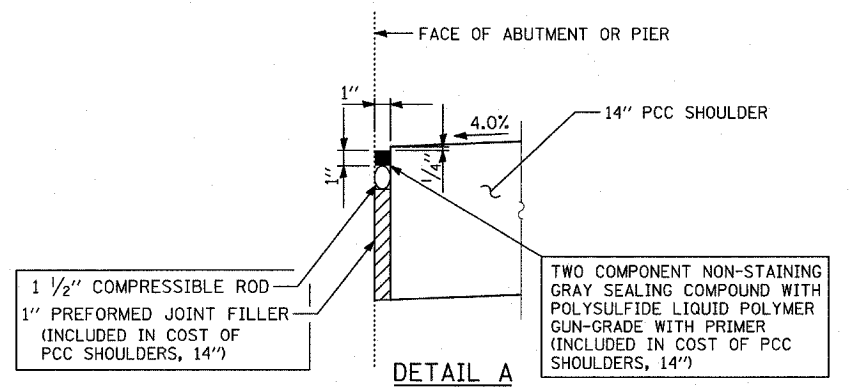
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS:
WENTWORTH AVE. OVERPASS FOOTINGS,
DUCTBANK & UNDERDRAIN DETAILS
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: TB
CHECKED BY: TGB



SHOULDER DETAIL AT 76TH STREET



SHOULDER DETAIL AT S. HALSTED STREET



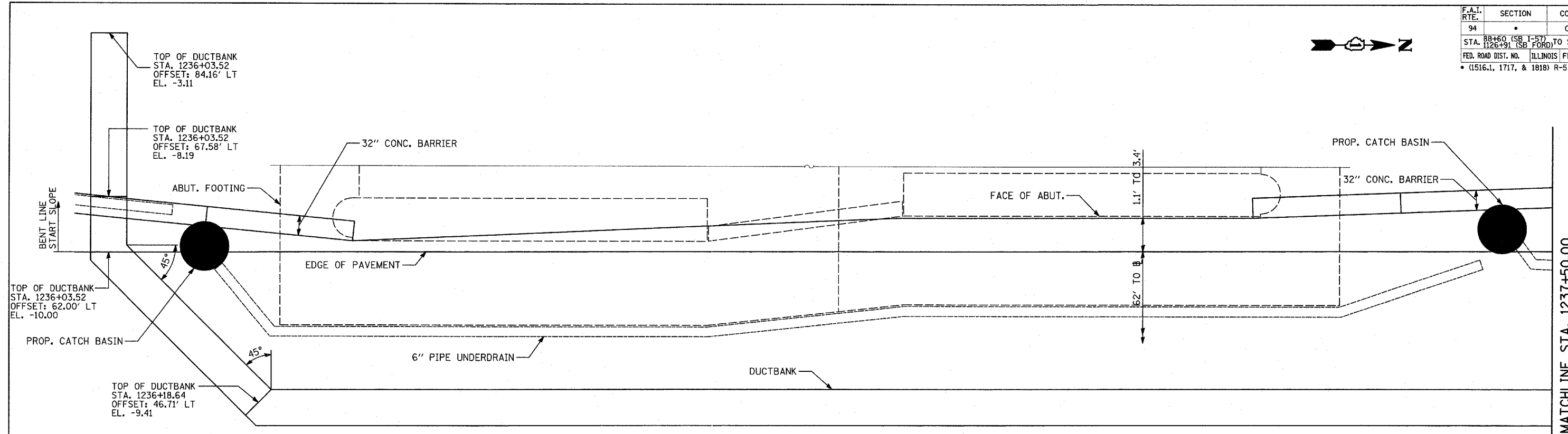
SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS: PLAN AND TYPICAL SECTION DETAILS"

NOTES

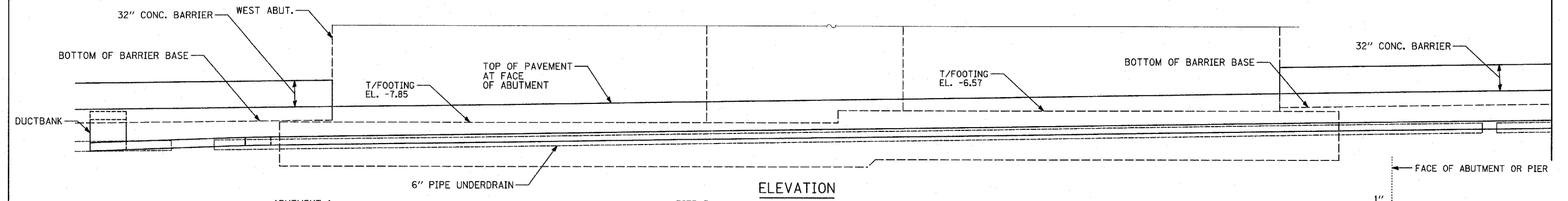
- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

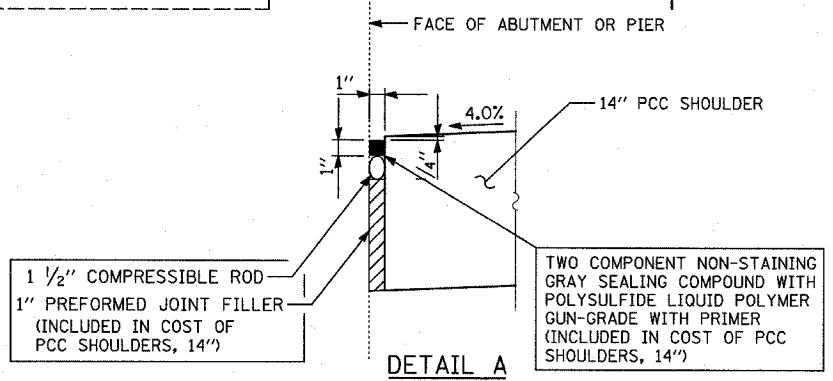
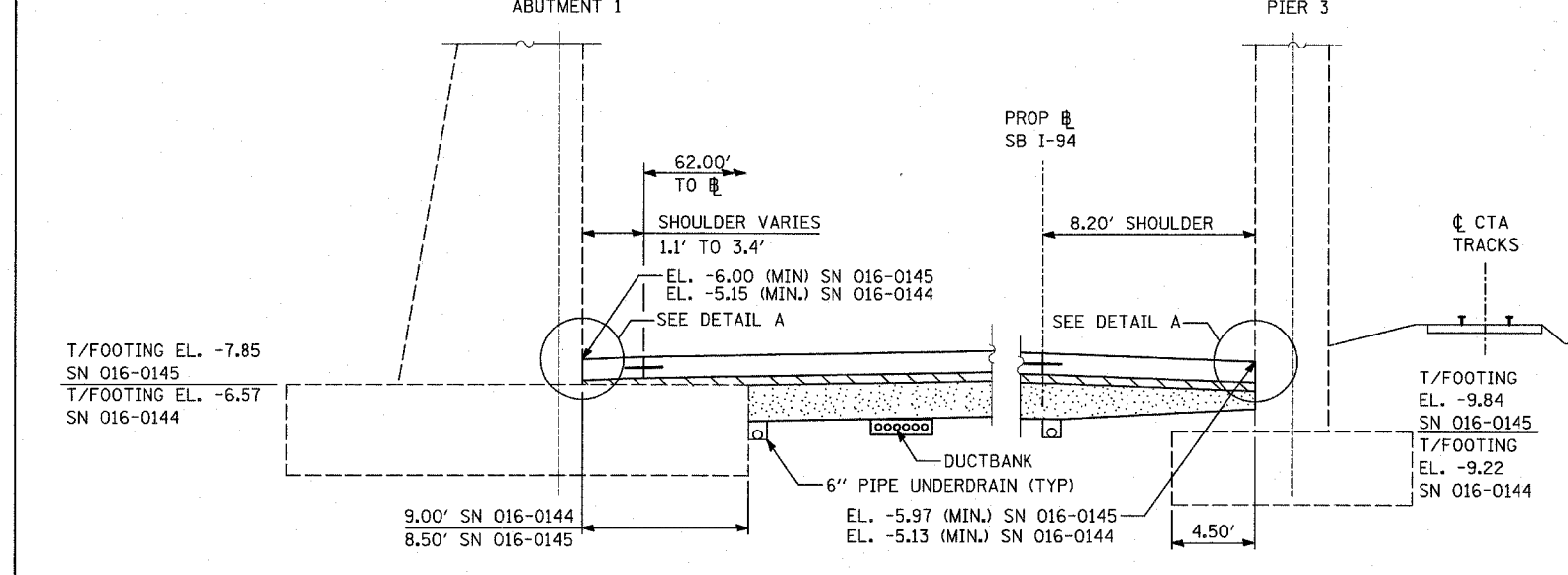
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 76TH STREET AND HALSTED STREET
 SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB, KA
 CHECKED BY: TGB



PLAN



ELEVATION



DETAIL A

NOTES

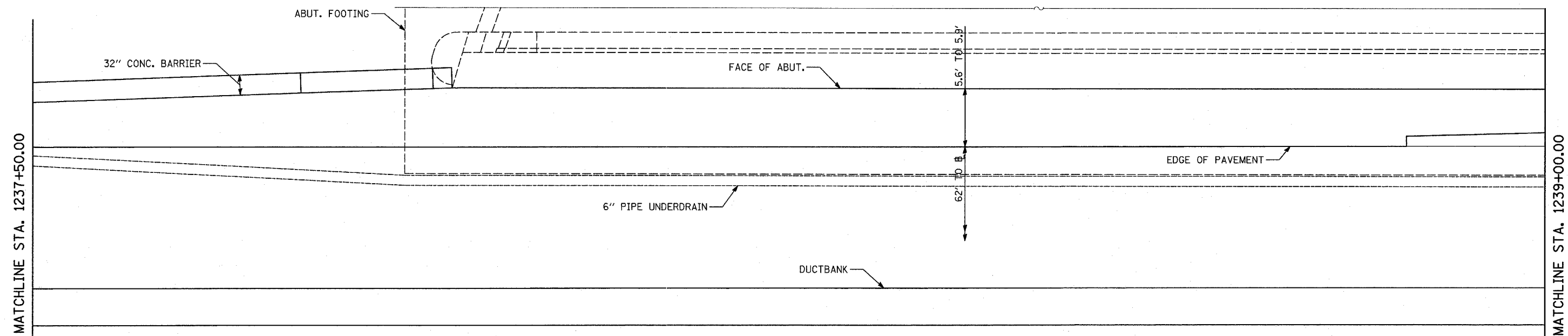
1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

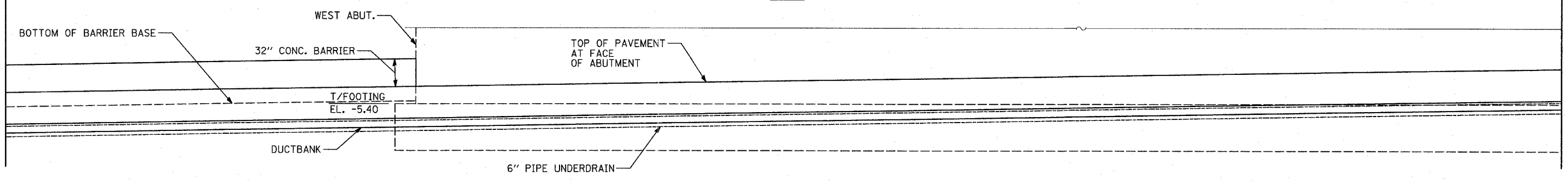
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS:
CHICAGO RAIL LINK, NORFOLK SOUTHERN AND BELT RAILWAY OVERPASS FOOTINGS, DUCTBANK & UNDERDRAIN DETAILS SHEET 1 OF 3

SCALE: NONE
 DATE: MARCH 1, 2006

DRAWN BY: TB, KA
 CHECKED BY: TCB



PLAN



ELEVATION

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

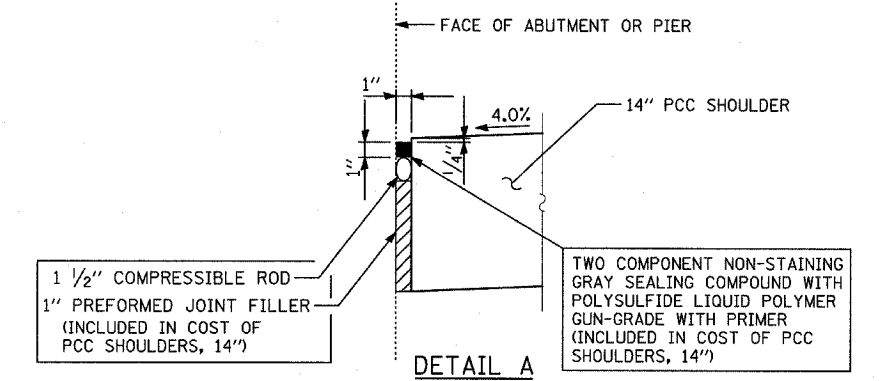
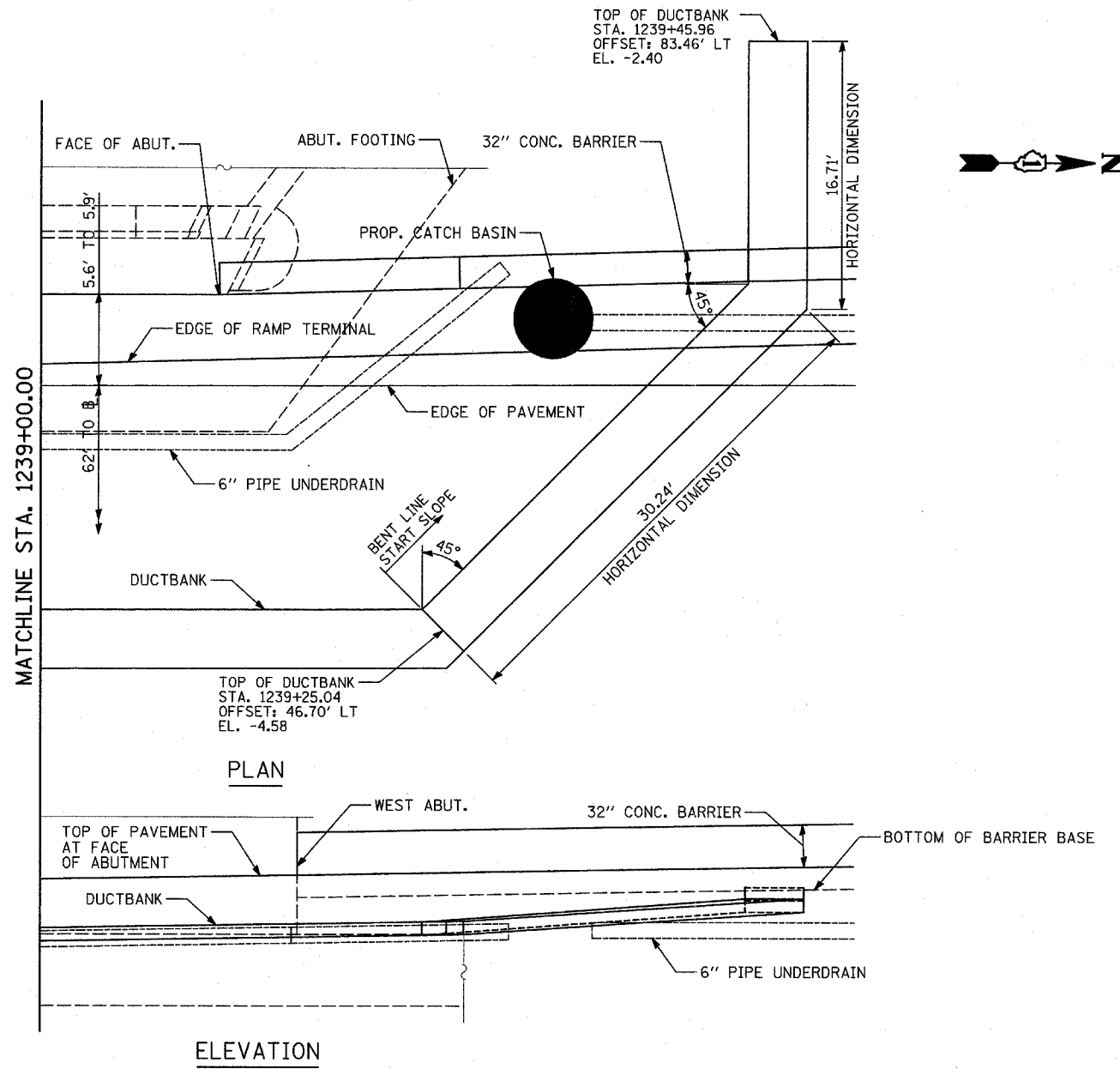
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 CHICAGO RAIL LINK, NORFOLK SOUTHERN AND
 BELT RAILWAY OVERPASS FOOTINGS, DUCTBANK
 & UNDERDRAIN DETAILS SHEET 2 OF 3

SCALE: NONE
 DATE: MARCH 1, 2006

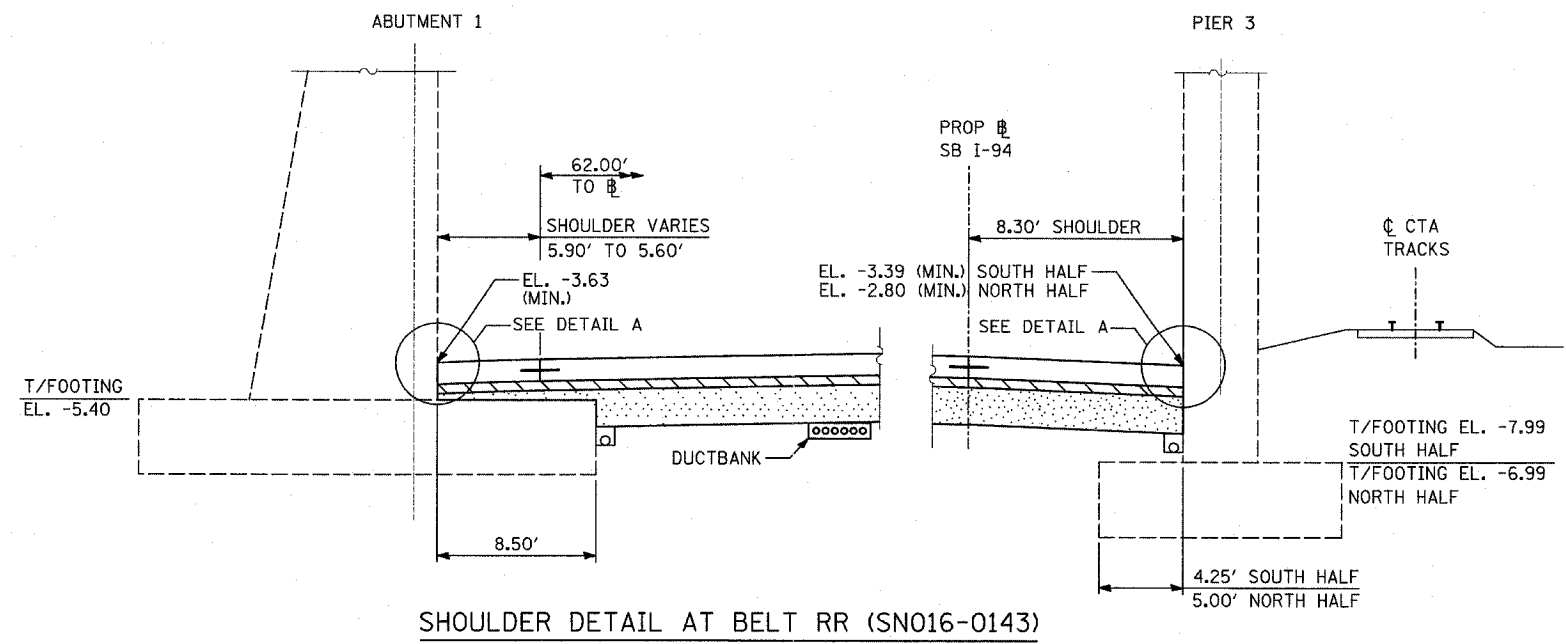
DRAWN BY: TB, KA
 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	35
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				

62593



SAME DETAIL APPEARS ON "MISCELLANEOUS DETAILS; PLAN AND TYPICAL SECTION DETAILS"



SHOULDER DETAIL AT BELT RR (SN016-0143)

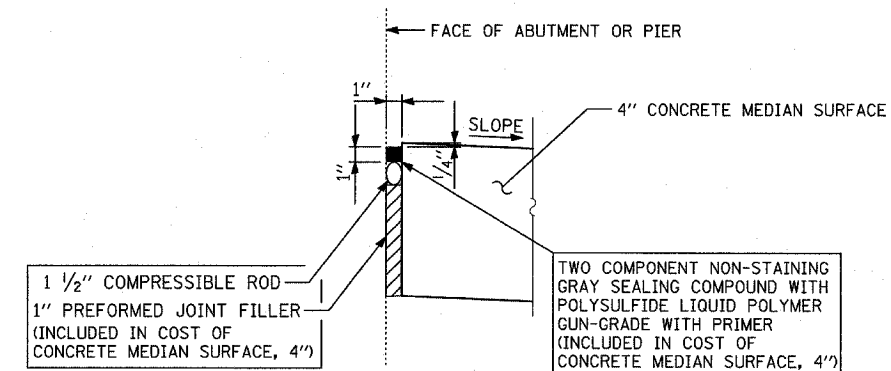
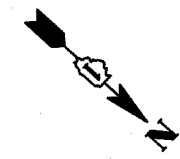
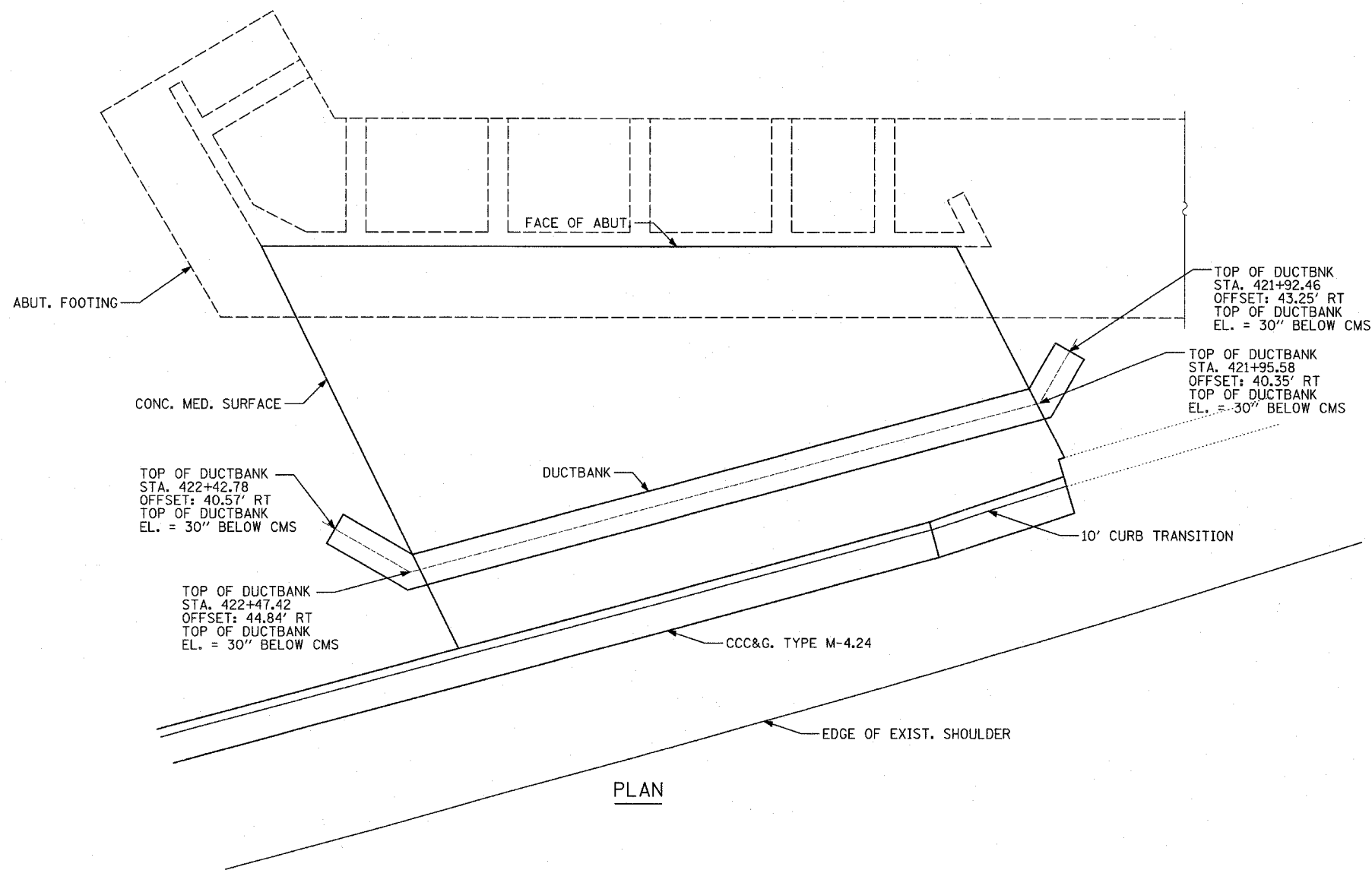
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

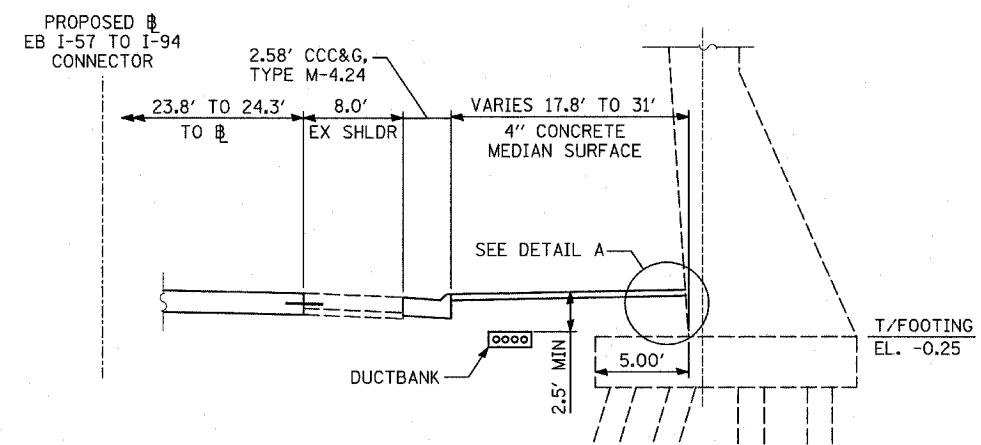
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
TYPICAL SECTIONS AND DETAILS;
CHICAGO RAIL LINK, NORFOLK SOUTHERN AND
BELT RAILWAY OVERPASS FOOTINGS, DUCTBANK
& UNDERDRAIN DETAILS SHEET 3 OF 3

SCALE: NONE
DATE: MARCH 1, 2006

DRAWN BY: TB, KA
CHECKED BY: TGB



DETAIL A



SHOULDER DETAIL AT STATE STREET

NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.

REVISIONS	
NAME	DATE

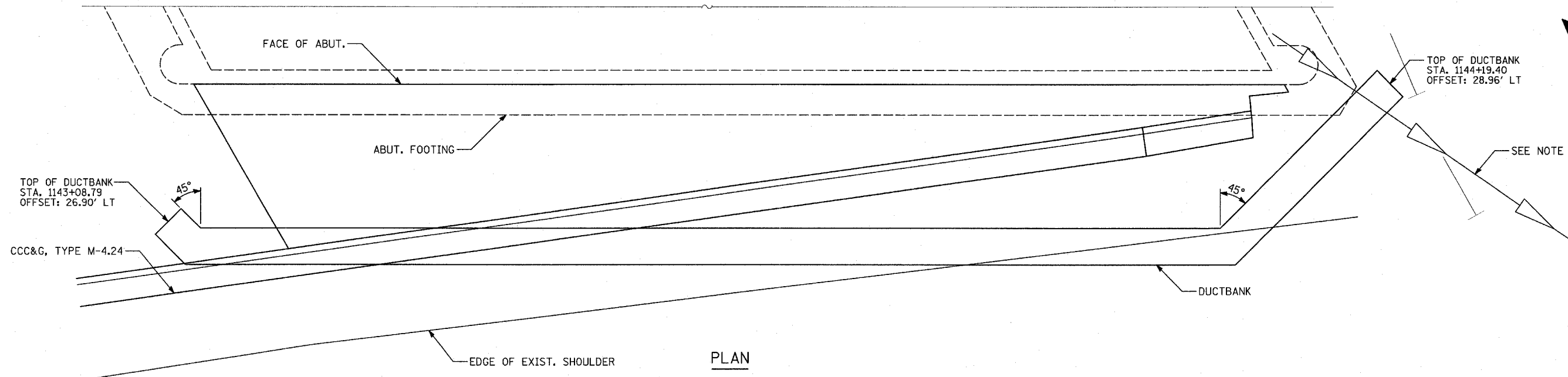
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

TYPICAL SECTIONS AND DETAILS:
 STATE STREET OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

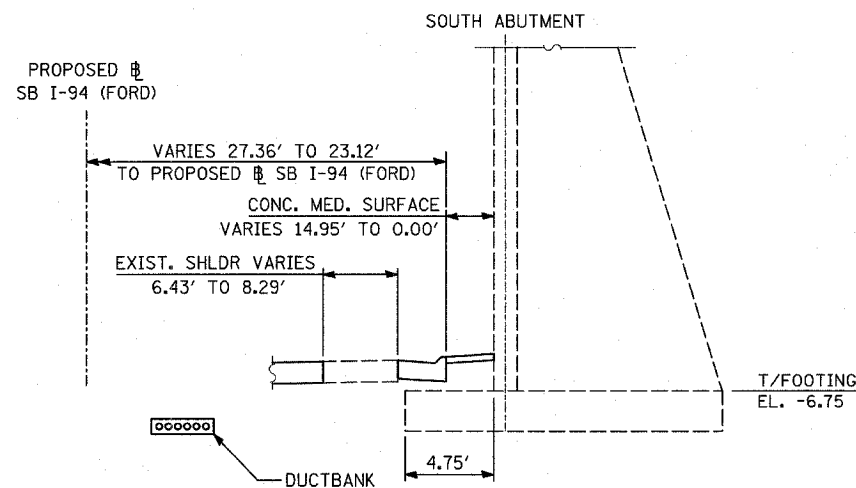
SCALE: NONE
 DATE: MARCH 1, 2006

DRAWN BY: TB, PL
 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	37
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				
				62593



PLAN



SHOULDER DETAIL AT CRI&P RR AND C&WI RR

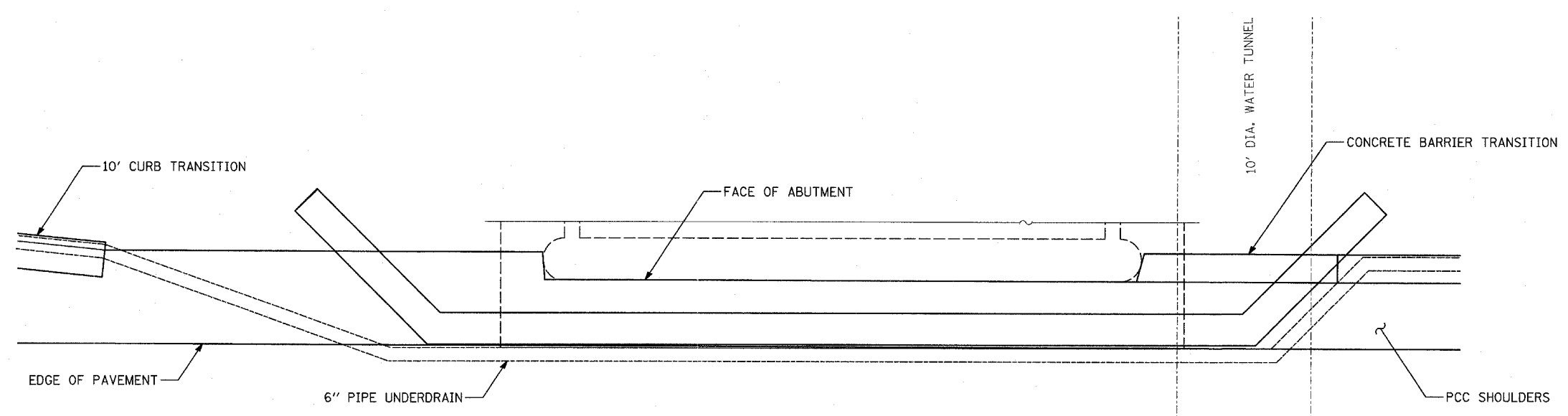
NOTES

- SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
- ALL VIEWS LOOKING UPSTATION.
- CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
- ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
- THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
- REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
- ELEVATIONS FOR STORM SEWER ARE UNKNOWN. CONTRACTOR TO HAND EXCAVATE TO AVOID DAMAGING SEWER PIPE. CONTRACTOR TO ADJUST DUCTBANK ELEVATIONS TO AVOID CONFLICT WITH SEWER.
- MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 SOUTH MICHIGAN AVE. OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB



PLAN

NOTES

1. SEE TYPICAL SECTIONS FOR COMPOSITION OF EXISTING AND PROPOSED PAVEMENT.
2. ALL VIEWS LOOKING UPSTATION.
3. CONTRACTOR SHALL USE CAUTION DURING PAVEMENT REMOVAL AND EXCAVATION OPERATIONS AROUND EXISTING BRIDGE FOUNDATIONS.
4. ALL DIMENSIONS AT RIGHT ANGLES TO EXPRESSWAY.
5. THE TOP OF FOOTING ELEVATIONS ARE TAKEN FROM EXISTING PLANS. IF THE TOP OF FOOTING ELEVATION VARIES SUCH THAT THE PROPOSED PCC PAVEMENT WOULD NEED TO BE POURED DIRECTLY ONTO THE TOP OF FOOTING, THE CONTRACTOR SHALL PROVIDE A 10 MIL POLYETHYLENE BOND BREAKER ON TOP OF THE EXISTING FOOTING PRIOR TO PLACING THE PAVEMENT. THIS WORK SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS, 14"
6. REFER TO PROPOSED ELECTRICAL SHEETS FOR DUCTBANK PAY ITEM.
7. NO INFORMATION WAS AVAILABLE FOR THE EXISTING BRIDGE, THEREFORE FOOTING SIZE AND ELEVATION ARE UNKNOWN. CONTRACTOR SHALL ADJUST DUCTBANK LOCATION AND ELEVATION AS NECESSARY TO AVOID CONFLICT WITH FOOTING. CONTRACTOR SHALL ADJUST UNDERDRAIN LOCATION AND ELEVATION AS NECESSARY.
8. MAINTAIN 3" MIN. STABILIZED SUBBASE (BAM) OVER DUCTBANK. CONTRACTOR SHALL ADJUST DUCTBANK AS NECESSARY TO MEET THIS REQUIREMENT.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 TYPICAL SECTIONS AND DETAILS:
 UNION PACIFIC R.R. OVERPASS FOOTINGS,
 DUCTBANK & UNDERDRAIN DETAILS

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: TB
 CHECKED BY: TGB



PAVEMENT REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM		TO		PAVEMENT REM (SQ YD)	PAVT REMOVAL SPL (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-57 MAINLINE	SB I-57	89+45.0	SB I-57	90+89.3	641	
SB I-57 MAINLINE	SB I-57	90+89.1	SB I-57	91+19.3		134
SB I-57 MAINLINE	SB I-57	91+19.1	SB I-57	100+00.0	3975	
SB I-57 MAINLINE	SB I-57	100+00.0	SB I-57	110+00.0	5343	
SB I-57 MAINLINE	SB I-57	110+00.0	SB I-57	120+00.0	4023	
SB I-57 MAINLINE	SB I-57	120+00.0	SB I-57	130+33.6	4781	
SB I-57 MAINLINE	SB I-57	130+21.5	SB I-57	130+68.2		88
WB CONNECTOR MAINLINE	WB CON	340+29.2	WB CON	341+85.9	405	
WB CONNECTOR MAINLINE	WB CON	341+75.6	WB CON	342+18.3		87
SB I-57 MAINLINE	SB I-57	130+55.4	SB I-57	140+00.0	2492	
SB I-57 MAINLINE	SB I-57	140+00.0	SB I-57	143+04.0	1297	
SB I-57 MAINLINE	SB I-57	142+80.3	SB I-57	143+59.3		300
SB I-57 MAINLINE	SB I-57	143+38.9	SB I-57	150+00.0	3011	
SB I-57 MAINLINE	SB I-57	150+00.0	SB I-57	151+55.4	848	
SB I-57 MAINLINE	SB I-57	151+49.9	SB I-57	152+22.2		374
SB I-57 MAINLINE	SB I-57	152+17.8	SB I-57	157+17.9	2874	
SB I-57 MAINLINE	SB I-57	157+17.9	SB I-57	157+55.2		245
SB I-94 (BISHOP FORD) MAINLINE	SB I-94(FORD)	1167+93.6	SB I-94(FORD)	1171+75.2	1103	
SB I-94 (DAN RYAN) MAINLINE	SB I-57	157+55.2	SB I-94(RYAN)	1210+00.0	1988	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1210+00.0	SB I-94(RYAN)	1220+00.0	5761	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1220+00.0	SB I-94(RYAN)	1226+65.4	5126	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1226+65.2	SB I-94(RYAN)	1226+95.4		171
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1226+65.4	SB I-94(RYAN)	1226+95.4		25
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1226+95.4	SB I-94(RYAN)	1227+77.8	39	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1226+95.2	SB I-94(RYAN)	1230+00.0	1691	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1230+00.0	SB I-94(RYAN)	1240+00.0	6035	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1240+00.0	SB I-94(RYAN)	1245+44.7	4110	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1245+44.7	SB I-94(RYAN)	1245+74.7		180
87TH STREET ENTRANCE RAMP	SB I-94(RYAN)	1245+44.7	SB I-94(RYAN)	1245+74.7		67
87TH STREET ENTRANCE RAMP	SB I-94(RYAN)	1245+74.7	SB I-94(RYAN)	1246+59.7	189	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1245+74.7	SB I-94(RYAN)	1250+00.0	2514	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1250+00.0	SB I-94(RYAN)	1259+61.2	5788	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1259+60.0	SB I-94(RYAN)	1259+91.2		166
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1259+90.0	SB I-94(RYAN)	1265+71.8	3220	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1265+71.8	SB I-94(RYAN)	1266+13.8		232
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1266+13.8	SB I-94(RYAN)	1270+00.0	2134	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1270+00.0	SB I-94(RYAN)	1279+05.5	6102	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1279+05.5	SB I-94(RYAN)	1279+55.5		269
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1279+55.5	SB I-94(RYAN)	1290+00.0	5683	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1290+00.0	SB I-94(RYAN)	1300+00.0	8018	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1300+00.0	SB I-94(RYAN)	1310+00.0	7409	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1310+00.0	SB I-94(RYAN)	1310+90.0	55	
SB I-94 (DAN RYAN) MAINLINE	SB I-94(RYAN)	1310+00.0	SB I-94(RYAN)	1313+00.0	1579	
REMOVAL OF TEMPORARY PAVEMENT	-	-	-	-	11170	
				I000-2A TOTAL		
				J000-2A TOTAL	109404	2338
				TOTAL	109404	2338

COMBINATION CONCRETE CURB AND GUTTER REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			COMB CURB GUTTER REM (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-57	SB I-57	89+45.0	35.9 LT	SB I-57	103+23.2	46.5 LT	1385
SB I-57	SB I-57	103+99.4	61.9 LT	SB I-57	107+15.5	33.0 LT	318
SB I-57	SB I-57	109+05.3	33.9 LT	SB I-57	116+95.0	33.3 LT	790
SB I-57	SB I-57	118+16.2	33.9 LT	SB I-57	130+95.2	37.6 LT	1261
WB CONNECTOR	WB CON	340+29.5	16.7 RT	WB CON	340+45.7	20.0 RT	19
SB I-57	SB I-57	131+79.5	10.9 RT	SB I-57	133+58.3	6.0 RT	183
SB I-57	SB I-57	134+97.5	5.9 RT	SB I-57	145+65.8	10.8 RT	1073
SB I-57	SB I-57	135+12.6	37.7 LT	SB I-57	142+13.2	52.0 LT	694
SB I-57	SB I-57	143+91.5	62.3 LT	SB I-57	147+84.1	34.0 LT	387
SB I-57/SB I-94 (DAN RYAN)	SB I-57	149+64.8	33.9 LT	SB I-57	156+06.8	39.7 LT	636
EB CONNECTOR	EB CON	415+06.0	9.6 LT	EB CON	420+54.7	9.2 LT	553
EB CONNECTOR	EB CON	415+06.0	33.6 RT	EB CON	420+30.5	34.3 RT	513
EB CONNECTOR	EB CON	422+31.7	9.7 LT	EB CON	424+08.3	14.9 LT	179
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1128+45.3	22.4 LT	SB I-94(FORD)	1144+06.7	26.9 LT	1570
SB FORD/EB CONNECTOR	SB I-94(FORD)	1145+53.3	31.3 LT	EB CON	421+95.4	34.2 RT	824
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1151+58.3	13.5 LT	SB I-94(FORD)	1153+99.7	9.6 LT	245
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1211+52.4	61.7 LT	SB I-94(RYAN)	1217+45.6	61.3 LT	594
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1218+47.5	60.8 LT	SB I-94(RYAN)	1221+67.9	76.0 LT	328
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1223+71.5	86.3 LT	SB I-94(RYAN)	1232+29.1	63.1 LT	859
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1234+28.1	61.1 LT	SB I-94(RYAN)	1236+28.8	63.0 LT	201
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1235+78.4	54.2 LT	SB I-94(RYAN)	1246+59.2	86.3 LT	1082
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1244+95.9	56.9 LT	SB I-94(RYAN)	1258+52.6	63.0 LT	1360
87TH STREET ENTRANCE RAMP	SB I-94(RYAN)	1244+95.9	56.9 LT	SB I-94(RYAN)	1248+94.1	76.0 LT	400
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1263+72.2	62.0 LT	SB I-94(RYAN)	1265+01.2	68.0 LT	131
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1265+94.5	68.0 LT	SB I-94(RYAN)	1271+77.6	76.0 LT	614
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1274+38.2	75.3 LT	SB I-94(RYAN)	1277+56.0	62.2 LT	319
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1279+18.4	62.2 LT	SB I-94(RYAN)	1285+42.0	63.8 LT	624
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1290+34.4	74.1 LT	SB I-94(RYAN)	1295+06.2	86.3 LT	473
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1306+63.0	86.3 LT	SB I-94(RYAN)	1312+18.5	63.7 LT	558
				I000-2A TOTAL			3884
				J000-2A TOTAL			14289
				TOTAL			18173

TYLININTERNATIONAL

NOTES:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

**SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 1**

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

PAVED SHOULDER REMOVAL

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		PAVED SHLD REMOVAL (SQ YD)	PAVED SHLD REM SPL (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-57 SHOULDER	LT	SB I-57	89+45.0	SB I-57	90+89.4	165	
SB I-57 SHOULDER	LT	SB I-57	90+89.3	SB I-57	91+19.4		35
SB I-57 SHOULDER	LT	SB I-57	91+19.3	SB I-57	102+52.2	1291	
HALSTED EXIT RAMP GORE	-	SB I-57	103+01.1	SB I-57	103+23.2	12	
HALSTED ST. EXIT RAMP SHOULDER	LT	SB I-57	103+99.4	SB I-57	105+07.5	38	
SB I-57 SHOULDER	LT	SB I-57	104+86.4	SB I-57	130+37.9	2620	
SB I-57 SHOULDER	RT	SB I-57	107+15.6	SB I-57	130+05.8	2201	
SB I-57 SHOULDER	LT	SB I-57	116+12.0	SB I-57	117+22.1	99	
SB I-57/WB CONNECTOR GORE	-	SB I-57	129+24.6	SB I-57	130+21.5	42	
SB I-57/WB CONNECTOR GORE	-	SB I-57	130+17.6	SB I-57	130+55.4		35
SB I-57/WB CONNECTOR GORE	-	SB I-57	130+49.8	SB I-57	131+95.0	273	
SB I-57 SHOULDER	LT	SB I-57	130+33.6	SB I-57	130+72.6		31
SB I-57 SHOULDER	LT	SB I-57	130+68.2	SB I-57	140+70.9	980	
SB I-57 SHOULDER	RT	SB I-57	131+79.6	SB I-57	143+08.4	1215	
SB I-57/WB CONNECTOR GORE	-	SB I-57	131+80.1	SB I-57	132+46.8	98	
WENTWORTH AVE. EXIT RAMP GORE	-	SB I-57	141+89.1	SB I-57	142+13.2	12	
WB CONNECTOR SHOULDER	LT	WB CON	340+29.2	WB CON	341+89.8	156	
WB CONNECTOR SHOULDER	LT	WB CON	341+85.9	WB CON	342+22.3		34
SB I-57 SHOULDER	RT	SB I-57	143+04.0	SB I-57	143+63.4		60
SB I-57 SHOULDER	RT	SB I-57	143+59.3	SB I-57	146+42.9	303	
SB I-57 SHOULDER	LT	SB I-57	143+60.5	SB I-57	151+49.9	730	
SB I-57/I-94 (BISHOP FORD) GORE	-	SB I-57	145+65.8	SB I-57	146+42.9	72	
SB I-57/I-94 (BISHOP FORD) GORE	-	SB I-57	146+42.9	SB I-57	148+56.9	227	
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1163+99.3	SB I-94(FORD)	1167+93.6	494	
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1164+12.5	SB I-94(FORD)	1167+93.6	125	
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1167+93.6	SB I-94(FORD)	1169+61.3	217	
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1167+93.6	SB I-94(FORD)	1171+75.2	286	
SB I-57 SHOULDER	RT	SB I-57	148+58.1	SB I-57	151+56.2	247	
SB I-57 SHOULDER	LT	SB I-57	151+48.8	SB I-57	152+17.8		74
SB I-57 SHOULDER	RT	SB I-57	151+55.4	SB I-57	152+22.8		51
SB I-57/I-94 (DAN RYAN) SHOULDER	LT	SB I-57	152+16.9	SB I-57	157+17.9	602	
SB I-57/I-94 (DAN RYAN) SHOULDER	RT	SB I-57	152+22.2	SB I-57	156+06.7	296	
SB I-57/I-94 (DAN RYAN) SHOULDER	LT	SB I-57	157+17.9	SB I-57	157+55.2		54
I-94 (DAN RYAN) SHOULDER	LT	SB I-57	157+55.2	SB I-94(RYAN)	1221+22.5	1869	
SB FORD/EB CONNECTOR GORE	-	EB CON	422+31.9	EB CON	424+42.8	49	
95TH STREET EXIT RAMP GORE	LT	SB I-94(RYAN)	1221+51.0	SB I-94(RYAN)	1221+68.0	9	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1225+42.0	SB I-94(RYAN)	1226+65.4	61	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1226+65.4	SB I-94(RYAN)	1226+95.4		31
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1226+95.4	SB I-94(RYAN)	1241+67.8	1600	
87TH STREET ENTRANCE RAMP GORE	LT	SB I-94(RYAN)	1244+97.8	SB I-94(RYAN)	1245+44.7	23	
87TH STREET ENTRANCE RAMP GORE	LT	SB I-94(RYAN)	1245+44.7	SB I-94(RYAN)	1245+73.2		18
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1249+18.9	SB I-94(RYAN)	1259+60.0	1410	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1259+59.7	SB I-94(RYAN)	1259+90.0		47
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1259+89.7	SB I-94(RYAN)	1265+71.8	673	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1264+84.8	SB I-94(RYAN)	1265+71.8	53	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1265+71.8	SB I-94(RYAN)	1266+12.4		20
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1265+71.8	SB I-94(RYAN)	1266+13.8		48
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1266+13.8	SB I-94(RYAN)	1272+41.6	617	
87TH STREET EXIT RAMP GORE	LT	SB I-94(RYAN)	1271+77.4	SB I-94(RYAN)	1271+88.9	7	

(CONT. ABOVE)

PAVED SHOULDER REMOVAL (CONT. FROM BELOW)

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		PAVED SHLD REMOVAL (SQ YD)	PAVED SHLD REM SPL (SQ YD)	
		ALIGNMENT	STATION	ALIGNMENT	STATION			
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1276+28.2	SB I-94(RYAN)	1279+05.5	315		
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1279+05.5	SB I-94(RYAN)	1279+55.5		58	
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1279+55.5	SB I-94(RYAN)	1290+71.0	1328		
SB I-94 (DAN RYAN) SHOULDER	LT	SB I-94(RYAN)	1305+36.7	SB I-94(RYAN)	1313+00.0	862		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1213+13.2	SB I-94(RYAN)	1226+65.2	1033		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1226+65.1	SB I-94(RYAN)	1226+95.2		24	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1226+95.1	SB I-94(RYAN)	1233+64.9	549		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1233+77.9	SB I-94(RYAN)	1245+44.7	919		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1245+44.7	SB I-94(RYAN)	1245+74.7		22	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1245+74.7	SB I-94(RYAN)	1253+09.5	577		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1258+53.0	SB I-94(RYAN)	1259+61.4	106		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1259+61.2	SB I-94(RYAN)	1259+91.4		24	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1259+91.2	SB I-94(RYAN)	1265+71.8	438		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1265+71.8	SB I-94(RYAN)	1266+13.8		33	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1266+13.8	SB I-94(RYAN)	1279+05.5	1095		
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1279+05.5	SB I-94(RYAN)	1279+55.5		47	
SB I-94 (DAN RYAN) SHOULDER	RT	SB I-94(RYAN)	1279+55.5	SB I-94(RYAN)	1313+00.0	3298		
SEWER TRENCH PATCHES	-	-	-	-	-	269		
						I000-2A TOTAL	937	
						J000-2A TOTAL	29024	746
						TOTAL	29961	746

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	40
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				

62593

TYLIN INTERNATIONAL

NOTES:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 2

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

BITUMINOUS SURFACE REMOVAL

DESCRIPTION OF EXISTING LOCATION	OFFSET DIRECTION	FROM		TO		BIT SURF REM 1 1/2 (SQ YD)	BIT SURF REM 1 3/4 (SQ YD)	BIT SURF REM 4 (SQ YD)	
		ALIGNMENT	STATION	ALIGNMENT	STATION				
SB I-57 MAINLINE	-	SB I-57	88+60.0	SB I-57	89+45.0		436		
EB CONNECTOR MAINLINE	-	EB CON	415+06.0	EB CON	423+45.8		2224		
EB CONNECTOR SHOULDER	LT	EB CON	423+45.8	EB CON	424+41.8	86			
EB CONN./SB I-94 (FORD) SHOULDER	RT	EB CON	423+45.7	SB I-94(FORD)	1144+98.8	1184			
EB CONNECTOR MAINLINE	-	EB CON	423+45.7	EB CON	426+56.0			564	
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1126+90.8	SB I-94(FORD)	1135+00.0			3244	
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1135+00.0	SB I-94(FORD)	1145+00.0			4117	
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1145+00.0	SB I-94(FORD)	1152+19.0			2645	
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1127+51.1	SB I-94(FORD)	1141+41.2	1388			
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1127+71.1	SB I-94(FORD)	1142+41.7	1401			
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1145+00.3	SB I-94(FORD)	1152+19.1	691			
EB CONN./SB I-94 (FORD) GORE	LT	SB I-94(FORD)	1149+11.4	SB I-94(FORD)	1151+24.4	305			
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1151+24.4	SB I-94(FORD)	1152+18.9	85			
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1152+18.9	SB I-94(FORD)	1157+31.4		2090		
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1158+30.5	SB I-94(FORD)	1167+93.6		3871		
INSIDE SHOULDER PATCHES - MAINLINE	-	-	-	-	-	290			
INSIDE SHOULDER PATCHES - INTERCHANGE	-	-	-	-	-	35			
						1000-2A TOTAL	5175	8185	10570
						J000-2A TOTAL	290	436	
						TOTAL	5465	8621	10570

SAND MODULE IMPACT ATTENUATOR TO BE REMOVED

DESCRIPTION OF EXISTING LOCATION	ALIGNMENT	FROM		SAND MOD IMP ATT REM (EACH)	
		STATION	OFFSET		
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1162+47.8	12.8 LT	11	
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1168+35.8	14.0 LT	11	
				1000-2A TOTAL	11
				J000-2A TOTAL	11
				TOTAL	22

CONCRETE BARRIER REMOVAL

DESCRIPTION OF EXISTING LOCATION	BARRIER TYPE	FROM			TO			CONC BARRIER REMOV (FOOT)
		ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-57	PERMANENT	SB I-57	107+15.5	33.6 LT	SB I-57	109+05.3	34.5 LT	190
SB I-57	PERMANENT	SB I-57	107+15.6	21.8 RT	SB I-57	116+47.4	21.3 RT	932
SB I-57	PERMANENT	SB I-57	116+94.9	21.0 RT	SB I-57	129+66.5	33.8 RT	1283
SB I-57	PERMANENT	SB I-57	116+95.0	34.1 LT	SB I-57	118+16.2	34.4 LT	122
SB I-57	PERMANENT	SB I-57	130+95.2	38.6 LT	SB I-57	135+12.7	39.5 LT	410
WB CONNECTOR	PERMANENT	WB CON	340+29.2	26.1 LT	WB CON	341+70.4	25.1 LT	141
SB I-57	PERMANENT	SB I-57	133+58.4	6.6 RT	SB I-57	134+97.5	6.5 RT	140
SB I-57	PERMANENT	SB I-57	147+84.0	35.4 LT	SB I-57	149+64.8	35.9 LT	179
SB I-57/I-94 (DAN RYAN)	TEMPORARY	SB I-57	149+56.2	34.3 RT	SB I-57	150+05.6	33.8 RT	50
EB CONNECTOR	PERMANENT	EB CON	420+30.4	35.0 RT	EB CON	420+81.7	35.0 RT	50
EB CONNECTOR	PERMANENT	EB CON	420+54.7	10.2 LT	EB CON	420+84.4	9.6 LT	30
SB I-94 (BISHOP FORD FREEWAY)	PERMANENT	SB I-94(FORD)	1126+90.8	23.0 LT	SB I-94(FORD)	1128+45.3	23.2 LT	155
SB I-94 (BISHOP FORD FREEWAY)	PERMANENT	SB I-94(FORD)	1144+87.0	30.5 LT	SB I-94(FORD)	1145+53.2	32.3 LT	68
SB I-94 (BISHOP FORD FREEWAY)	TEMPORARY	SB I-94(FORD)	1154+91.2	12.8 LT	SB I-94(FORD)	1162+23.2	16.6 LT	738
SB I-94 (BISHOP FORD FREEWAY)	PERMANENT	SB I-94(FORD)	1167+89.9	14.0 LT	SB I-94(FORD)	1168+09.2	16.7 LT	24
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1209+97.5	63.8 LT	SB I-94(RYAN)	1211+52.4	62.2 LT	155
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1217+45.6	61.3 LT	SB I-94(RYAN)	1218+47.4	61.7 LT	102
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1223+64.6	7.7 RT	SB I-94(RYAN)	1223+94.6	8.5 RT	30
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1224+05.4	8.5 RT	SB I-94(RYAN)	1224+25.4	7.9 RT	20
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1233+00.0	65.0 LT	SB I-94(RYAN)	1234+28.1	61.8 LT	129
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1237+19.1	66.1 LT	SB I-94(RYAN)	1237+92.5	69.5 LT	74
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1239+14.8	69.6 LT	SB I-94(RYAN)	1240+70.5	73.7 LT	156
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1251+07.6	7.8 RT	SB I-94(RYAN)	1251+37.6	9.0 RT	30
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1251+48.4	9.2 RT	SB I-94(RYAN)	1251+68.4	7.9 RT	20
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1259+59.5	64.7 LT	SB I-94(RYAN)	1263+72.2	62.7 LT	413
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1276+37.0	7.4 RT	SB I-94(RYAN)	1276+87.0	7.4 RT	50
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1277+56.0	61.0 LT	SB I-94(RYAN)	1279+18.4	62.8 LT	163
SB I-94 (DAN RYAN EXPRESSWAY)	PERMANENT	SB I-94(RYAN)	1286+36.0	66.6 LT	SB I-94(RYAN)	1290+34.3	75.0 LT	400
SB I-94 (DAN RYAN EXPRESSWAY)	TEMPORARY	SB I-94(RYAN)	1286+55.0	8.7 RT	SB I-94(RYAN)	1287+05.0	7.5 RT	50
SB I-57 SEWER TRENCHES	PERMANENT	-	-	-	-	-	-	10
INTERCHANGE SEWER TRENCHES	PERMANENT	-	-	-	-	-	-	210
							1000-2A TOTAL	1251
							J000-2A TOTAL	5273
							TOTAL	6524

CTA BARRIER REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			CTA BARRIER REMOV (FOOT)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1167+93.7	33.7 RT	SB I-94(FORD)	1172+74.2	34.3 RT	476	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-57	150+05.6	33.8 RT	SB I-94(RYAN)	1205+32.4	9.6 RT	606	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1213+13.2	8.3 RT	SB I-94(RYAN)	1223+64.6	7.7 RT	1052	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1224+25.4	8.8 RT	SB I-94(RYAN)	1232+26.5	10.5 RT	802	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1232+99.9	10.8 RT	SB I-94(RYAN)	1236+28.0	9.5 RT	329	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1236+60.4	9.6 RT	SB I-94(RYAN)	1236+82.0	9.6 RT	22	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1237+15.2	9.6 RT	SB I-94(RYAN)	1237+67.9	9.5 RT	53	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1238+68.9	9.0 RT	SB I-94(RYAN)	1251+07.6	7.5 RT	1239	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1251+68.4	7.4 RT	SB I-94(RYAN)	1253+09.5	9.6 RT	142	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1259+60.3	8.3 RT	SB I-94(RYAN)	1266+50.0	7.1 RT	690	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1267+00.0	7.2 RT	SB I-94(RYAN)	1276+37.0	7.1 RT	937	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1276+87.0	7.2 RT	SB I-94(RYAN)	1285+42.1	8.0 RT	856	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1286+36.0	8.6 RT	SB I-94(RYAN)	1286+55.0	8.7 RT	20	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1287+05.0	7.2 RT	SB I-94(RYAN)	1306+79.7	7.9 RT	1975	
							1000-2A TOTAL	
							J000-2A TOTAL	9199
							TOTAL	9199

NOTES:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 3

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: RTM

TREE REMOVAL (6 TO 15 UNITS)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
SB I-57	91+05.2	52.4 LT	1839469.4	1172789.1	6
SB I-57	91+25.1	59.2 LT	1839476.7	1172808.9	10
SB I-57	91+69.7	69.8 LT	1839488.4	1172853.2	6
SB I-57	107+32.9	51.6 LT	1839508.5	1174416.4	13
SB I-57	117+05.2	81.0 LT	1839561.8	1175387.7	12
SB I-57	117+34.7	89.5 LT	1839571.0	1175417.0	8
SB I-57	117+60.2	83.9 LT	1839566.1	1175442.6	14
SB I-57	118+54.2	61.1 LT	1839545.6	1175537.1	10
SB I-57	118+84.4	90.1 LT	1839575.3	1175566.6	8
SB I-57	119+07.4	82.2 LT	1839568.0	1175589.8	9
SB I-57	119+27.1	70.7 LT	1839556.9	1175609.7	10
SB I-57	119+29.4	80.4 LT	1839566.7	1175611.9	8
SB I-57	119+57.8	69.3 LT	1839556.3	1175640.4	6
SB I-57	119+62.5	93.5 LT	1839580.6	1175644.6	12
SB I-57	119+82.1	96.9 LT	1839584.5	1175664.1	14
SB I-57	120+23.1	84.2 LT	1839572.8	1175705.4	6
SB I-57	120+30.2	89.0 LT	1839577.8	1175712.3	12
SB I-57	120+77.5	56.4 LT	1839546.4	1175760.5	12
SB I-57	120+77.5	56.4 LT	1839546.4	1175760.5	6
SB I-57	120+77.5	56.4 LT	1839546.4	1175760.5	8
SB I-57	121+14.6	77.8 LT	1839568.7	1175797.0	9
SB I-57	124+01.9	65.6 LT	1839580.8	1176074.2	11
SB I-57	129+72.9	66.8 LT	1839751.8	1176595.7	13
SB I-57	129+97.7	90.2 LT	1839783.5	1176606.3	14
SB I-57	130+15.8	75.5 LT	1839778.4	1176628.4	7
SB I-57	130+19.8	80.6 LT	1839781.8	1176629.4	9
SB I-57	133+51.5	40.0 LT	1839926.3	1176920.0	6
SB I-57	133+81.9	40.2 LT	1839945.0	1176943.1	8
SB I-57	133+91.8	40.1 LT	1839951.1	1176950.7	7
SB I-57	151+48.5	84.5 LT	1841468.2	1177641.3	10
SB I-57	153+20.1	97.8 LT	1841634.8	1177640.7	10
SB I-57	153+20.2	108.3 LT	1841635.4	1177630.2	10
SB I-57	153+20.9	106.1 LT	1841636.0	1177632.4	10
SB I-57	153+26.7	106.7 LT	1841641.6	1177632.1	10
SB I-57	153+53.1	91.7 LT	1841666.5	1177648.1	10
SB I-57	153+55.9	95.2 LT	1841669.3	1177644.6	10
SB I-57	153+60.2	96.6 LT	1841673.5	1177643.4	10
SB I-57	153+63.0	92.8 LT	1841676.1	1177647.3	10
SB I-94(FORD)	1154+78.6	47.7 LT	1839541.7	1178020.6	9
SB I-94(RYAN)	1237+36.8	81.1 LT	1845117.2	1177589.5	14
SB I-94(RYAN)	1237+49.9	77.2 LT	1845130.4	1177593.1	10
SB I-94(RYAN)	1259+71.6	99.8 LT	1847350.6	1177506.8	7
SB I-94(RYAN)	1259+72.2	81.2 LT	1847351.7	1177525.4	14
SB I-94(RYAN)	1259+90.6	104.7 LT	1847369.4	1177501.5	13
SB I-94(RYAN)	1261+00.6	103.4 LT	1847479.4	1177499.5	8
SB I-94(RYAN)	1261+07.5	111.5 LT	1847486.1	1177491.3	10
SB I-94(RYAN)	1261+10.7	105.7 LT	1847489.4	1177497.0	10
SB I-94(RYAN)	1261+17.9	111.8 LT	1847496.4	1177490.7	8
SB I-94(RYAN)	1261+20.4	103.9 LT	1847499.2	1177498.5	7
SB I-94(RYAN)	1261+22.7	111.6 LT	1847501.2	1177490.7	12
SB I-94(RYAN)	1261+27.7	110.8 LT	1847506.2	1177491.4	8
SB I-94(RYAN)	1261+30.8	107.2 LT	1847509.5	1177494.9	6
SB I-94(RYAN)	1261+37.9	110.6 LT	1847516.5	1177491.3	7

(CONT. ABOVE)

TREE REMOVAL (6 TO 15 UNITS) (CONT. FROM BELOW)

ALIGNMENT	LOCATION				TREE REMOV 6-15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
SB I-94(RYAN)	1261+48.2	103.9 LT	1847527.0	1177497.7	8
SB I-94(RYAN)	1261+59.8	111.6 LT	1847538.4	1177489.7	6
SB I-94(RYAN)	1261+60.4	97.0 LT	1847539.3	1177504.3	12
SB I-94(RYAN)	1261+71.9	95.7 LT	1847550.8	1177505.2	12
SB I-94(RYAN)	1283+74.7	113.6 LT	1849752.3	1177424.3	6
SB I-94(RYAN)	1283+82.6	123.4 LT	1849759.9	1177414.3	10
SB I-94(RYAN)	1283+90.7	121.3 LT	1849768.1	1177416.1	9
SB I-94(RYAN)	1283+94.6	115.9 LT	1849772.0	1177421.4	7
SB I-94(RYAN)	1283+97.9	105.0 LT	1849775.7	1177432.2	15
SB I-94(RYAN)	1284+05.9	111.6 LT	1849783.5	1177425.4	6
SB I-94(RYAN)	1284+06.3	97.6 LT	1849784.3	1177439.3	10
SB I-94(RYAN)	1284+35.6	115.0 LT	1849813.1	1177421.2	12
SB I-94(RYAN)	1284+35.6	115.0 LT	1849813.1	1177421.2	9
SB I-94(RYAN)	1284+76.5	92.8 LT	1849854.6	1177442.2	12
SB I-94(RYAN)	1284+86.5	122.3 LT	1849863.7	1177412.4	6
SB I-94(RYAN)	1284+94.4	122.0 LT	1849871.6	1177412.5	10
SB I-94(RYAN)	1285+09.9	87.3 LT	1849888.2	1177446.7	14
SB I-94(RYAN)	1285+22.5	83.0 LT	1849900.9	1177450.7	6
SB I-94(RYAN)	1285+30.6	112.8 LT	1849908.1	1177420.6	6
SB I-94(RYAN)	1285+30.6	81.2 LT	1849909.0	1177452.2	14
SB I-94(RYAN)	1285+35.4	111.3 LT	1849913.0	1177422.0	14
SB I-94(RYAN)	1286+52.6	103.7 LT	1850030.3	1177426.2	6
SB I-94(RYAN)	1286+61.1	104.8 LT	1850038.8	1177424.9	6
SB I-94(RYAN)	1286+69.7	95.3 LT	1850047.7	1177434.1	14
SB I-94(RYAN)	1286+71.5	108.0 LT	1850049.0	1177421.4	7
SB I-94(RYAN)	1287+44.5	110.0 LT	1850122.0	1177417.3	8
SB I-94(RYAN)	1288+18.1	118.1 LT	1850195.3	1177407.1	14
SB I-94(RYAN)	1288+29.2	118.8 LT	1850206.5	1177406.1	6
SB I-94(RYAN)	1288+31.7	118.7 LT	1850209.0	1177406.2	6
TOTAL (6-15 UNITS)					784

TREE REMOVAL (OVER 15 UNITS)

ALIGNMENT	LOCATION				TREE REMOV OVER 15 (UNIT)
	STATION	OFFSET	NORTHING	EASTING	
SB I-57	119+23.9	89.1 LT	1839575.3	1175606.1	18
SB I-57	120+31.7	81.9 LT	1839570.7	1175714.0	18
SB I-57	130+26.2	95.2 LT	1839800.6	1176627.8	26
SB I-94(RYAN)	1259+82.7	89.2 LT	1847362.0	1177517.1	16
SB I-94(RYAN)	1284+52.6	109.7 LT	1849830.3	1177426.0	22
SB I-94(RYAN)	1288+25.7	105.1 LT	1850203.4	1177419.9	16
TOTAL (>15 UNITS)					116

CHAIN LINK FENCE REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			CH LK FENCE REMOV (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-57	149+51.3	34.8 RT	SB I-57	150+10.6	34.3 RT	63
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1223+60.0	8.2 RT	SB I-94(RYAN)	1224+30.0	8.6 RT	71
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1251+03.0	8.3 RT	SB I-94(RYAN)	1251+73.1	8.4 RT	71
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1276+32.0	7.9 RT	SB I-94(RYAN)	1276+92.0	7.9 RT	63
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1286+50.0	9.2 RT	SB I-94(RYAN)	1287+10.0	8.0 RT	62
1000-2A TOTAL							
J000-2A TOTAL							330
TOTAL							330

GUARDRAIL REMOVAL

DESCRIPTION OF EXISTING LOCATION	FROM			TO			GUARDRAIL REMOV (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-57	SB I-57	90+87.6	37.3 LT	SB I-57	92+91.7	39.1 LT	205
HALSTED STREET EXIT RAMP	SB I-57	103+99.4	61.9 LT	SB I-57	104+79.7	54.2 LT	81
1000-2A TOTAL							
J000-2A TOTAL							286
TOTAL							286

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NOTES:

"ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
REMOVAL QUANTITIES - SHEET 4

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

PROPOSED RECONSTRUCTED MAINLINE PAVEMENT

LOCATION DESCRIPTION	FROM		TO		PCC PVT 14 JOINTD (SQ YD)	CONT REINF PCC PVT 14 (SQ YD)	PAVT REINFORCMENT 14 (SQ YD)	SUB GRAN MAT B 24 (SQ YD)	GEOTECH FAB F/GR STAB (SQ YD)	STAB SUB-BASE 6	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION							
SB I-57 (RIGHT SHOULDER)	SB I-57	89+45.0	SB I-57	107+15.6	1039			1039	1039	1039	1039
SB I-57	SB I-57	89+45.0	SB I-57	100+00.0		4319	4319	5729	5729	5513	4319
SB I-57	SB I-57	100+00.0	SB I-57	120+00.0		11190	11190	14821	14821	14821	11190
SB I-57	SB I-57	120+00.0	SB I-57	140+00.0		10313	10313	15033	15033	15033	10313
WB CONNECTOR	WB CON	340+29.2	WB CON	241+87.7		458	458	725	725	725	458
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1167+93.6	SB I-94(FORD)	1168+08.8	44			79	79	79	44
SB I-94 (BISHOP FORD FREEWAY)	SB I-94(FORD)	1168+18.6	SB I-94(FORD)	1171+18.6		891	891	1519	1519	1362	891
SB I-57/SB I-94 (DAN RYAN)	SB I-57	140+00.0	SB I-94(RYAN)	1210+00.0		12401	12401	16333	16333	16333	12401
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1210+00.0	SB I-94(RYAN)	1230+00.0		14171	14171	18334	18334	18334	14171
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1230+00.0	SB I-94(RYAN)	1250+00.0		14695	14695	18815	18815	18815	14695
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1250+00.0	SB I-94(RYAN)	1270+00.0		13778	13778	17860	17860	17860	13778
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1270+00.0	SB I-94(RYAN)	1290+00.0		14120	14120	18548	18548	18548	14120
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1290+00.0	SB I-94(RYAN)	1310+00.0		14911	14911	18885	18885	18885	14911
SB I-94 (DAN RYAN EXPRESSWAY)	SB I-94(RYAN)	1310+00.0	SB I-94(RYAN)	1313+00.0		2070	2070	2539	2539	2539	2070
ADDITIONAL QUANTITY FOR STAGING OVERDIG	-	-	-	-				7244	3375	3375	
I000-2A TOTAL											
J000-2A TOTAL					1083	113317	113317	157503	153634	153261	114400
TOTAL					1083	113317	113317	157503	153634	153261	114400

PROPOSED RESURFACED MAINLINE PAVEMENT

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		P BCSC SUPER "F" N105 1 3/4" (TON)	P BCSC SUP IL19 N105 2 1/4" (TON)	BC SC SUPER "D" N70 1 1/2" (TON)	BC SC SUPER "D" N70 1 3/4" (TON)
		ALIGNMENT	STATION	ALIGNMENT	STATION				
SB I-57 MAINLINE	-	SB I-57	88+60.0	SB I-57	89+45.0	43			
EB CONNECTOR MAINLINE	-	EB CON	415+06.0	EB CON	423+45.8	218			
EB CONNECTOR MAINLINE	-	EB CON	423+45.7	EB CON	426+56.0	55	71		
EB CONNECTOR SHOULDER	LT	EB CON	423+45.8	EB CON	424+41.8			7	
EB CONN./SB I-94 (FORD) SHOULDER	RT	EB CON	423+45.7	SB I-94(FORD)	1144+98.8			101	
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1126+90.8	SB I-94(FORD)	1152+19.0	980	1261		
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1152+18.9	SB I-94(FORD)	1157+04.6	126			
SB I-94 (BISHOP FORD) MAINLINE	-	SB I-94(FORD)	1158+41.7	SB I-94(FORD)	1167+93.6	268			
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1127+51.1	SB I-94(FORD)	1142+41.2			116	
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1127+71.1	SB I-94(FORD)	1142+41.7			118	
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1145+00.2	SB I-94(FORD)	1152+19.1			58	
SB FORD/EB CONNECTOR GORE	-	SB I-94(FORD)	1149+11.4	SB I-94(FORD)	1151+24.4			26	
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1151+24.4	SB I-94(FORD)	1152+18.9			7	
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1154+34.7	SB I-94(FORD)	1157+31.4				61
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1154+56.6	SB I-94(FORD)	1156+71.1				18
SB I-94 (BISHOP FORD) SHOULDER	RT	SB I-94(FORD)	1158+30.5	SB I-94(FORD)	1159+77.3				17
SB I-94 (BISHOP FORD) SHOULDER	LT	SB I-94(FORD)	1158+69.8	SB I-94(FORD)	1163+99.3				95
INSIDE SHOULDER PATCHES - MAINLINE	-	-	-	-	-			25	
INSIDE SHOULDER PATCHES - INTERCHANGE	-	-	-	-	-			3	
I000-2A TOTAL						1647	1332	436	191
J000-2A TOTAL						43		25	
TOTAL						1690	1332	461	191

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 1

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: RTM

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM2.24 (FOOT)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-57	SB I-57	102+73.1	41.5 LT	SB I-57	103+23.2	46.5 LT	56.5	16
SB I-57	SB I-57	141+61.8	48.0 LT	SB I-57	142+13.2	52.0 LT	56.5	16
SB I-57	SB I-57	145+93.1	10.0 RT	SB I-57	146+45.0	11.8 RT	53.5	15
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1169+11.6	10.0 LT	SB I-94(FORD)	1169+63.4	11.8 LT	53.0	15
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1221+12.9	72.0 LT	SB I-94(RYAN)	1221+67.9	76.0 LT	61.5	18
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1271+27.4	72.0 LT	SB I-94(RYAN)	1271+77.6	76.0 LT	56.5	16
I000-2A TOTAL								
J000-2A TOTAL							337.5	96
TOTAL							337.5	96

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.48

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.48 (FOOT)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-57	SB I-57	143+91.5	62.3 LT	SB I-57	146+20.6	48.0 LT	225.5	118
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1223+71.5	86.3 LT	SB I-94(RYAN)	1226+19.6	72.0 LT	248.5	130
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1241+11.0	72.6 LT	SB I-94(RYAN)	1246+59.2	86.3 LT	548.5	288
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1274+38.2	75.3 LT	SB I-94(RYAN)	1274+75.6	72.0 LT	37.5	20
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1289+96.8	72.0 LT	SB I-94(RYAN)	1295+06.2	86.3 LT	510.0	268
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1306+63.0	86.3 LT	SB I-94(RYAN)	1309+25.4	72.0 LT	263.0	138
I000-2A TOTAL								
J000-2A TOTAL							1833.0	962
TOTAL							1833.0	962

COMBINATION CONCRETE CURB AND GUTTER, TYPE M-4.24

LOCATION DESCRIPTION	FROM			TO			COMB CC&G TM4.24 (FOOT)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET		
SB I-57	SB I-57	89+45.0	36.0 LT	SB I-57	89+78.7	36.5 LT	34.0	10
SB I-57	SB I-57	92+87.5	36.0 LT	SB I-57	102+73.1	41.5 LT	986.0	299
SB I-57	SB I-57	109+15.4	48.0 LT	SB I-57	116+15.3	44.9 LT	700.5	212
SB I-57	SB I-57	119+61.0	48.0 LT	SB I-57	130+07.7	42.0 LT	1025.5	310
SB I-57	SB I-57	131+43.9	11.9 RT	SB I-57	133+04.0	10.0 RT	162.0	49
SB I-57	SB I-57	134+50.0	10.0 RT	SB I-57	145+93.1	10.0 RT	1149.0	348
SB I-57	SB I-57	134+50.0	48.0 LT	SB I-57	141+61.8	48.0 LT	694.0	210
SB I-57	SB I-57	146+20.6	48.0 LT	SB I-57	155+46.0	41.2 LT	911.0	276
WB CONNECTOR	WB CON	340+29.2	12.0 RT	WB CON	340+81.0	13.9 RT	53.0	16
EB CONNECTOR	EB CON	415+06.0	10.0 LT	EB CON	419+85.1	10.0 LT	482.5	146
EB CONNECTOR	EB CON	415+06.0	34.0 RT	EB CON	420+30.5	34.0 RT	512.0	155
EB CON - SB I-94 (FORD)	EB CON	421+95.4	34.2 RT	SB I-94(FORD)	1146+26.7	33.1 LT	749.0	227
EB CONNECTOR	EB CON	422+31.9	8.3 LT	EB CON	424+40.3	11.9 LT	210.0	64
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1130+05.8	22.0 LT	SB I-94(FORD)	1144+06.8	26.7 LT	1409.5	427
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1151+26.4	11.9 LT	SB I-94(FORD)	1153+99.7	9.7 LT	276.0	84
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1168+14.3	10.0 LT	SB I-94(FORD)	1169+11.6	10.0 LT	98.0	30
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1212+30.3	72.0 LT	SB I-94(RYAN)	1216+75.0	72.0 LT	445.0	135
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1218+75.0	72.0 LT	SB I-94(RYAN)	1221+12.9	72.0 LT	238.0	72
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1226+19.6	72.0 LT	SB I-94(RYAN)	1227+53.8	72.0 LT	134.5	41
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1235+41.2	68.0 LT	SB I-94(RYAN)	1235+41.2	76.1 LT	8.5	3
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1248+94.1	76.0 LT	SB I-94(RYAN)	1251+37.6	72.0 LT	249.5	76
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1252+87.6	72.0 LT	SB I-94(RYAN)	1257+59.7	72.0 LT	472.0	143
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1261+63.3	72.0 LT	SB I-94(RYAN)	1271+27.4	72.0 LT	964.5	292
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1274+75.6	72.0 LT	SB I-94(RYAN)	1283+91.3	72.0 LT	916.0	277
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1289+16.6	72.0 LT	SB I-94(RYAN)	1289+96.8	72.0 LT	80.5	24
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1309+25.4	72.0 LT	SB I-94(RYAN)	1312+15.9	64.1 LT	291.0	88
I000-2A TOTAL							3639.0	1102
J000-2A TOTAL							9612.5	2910
TOTAL							13251.5	4012

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 2

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

CONCRETE MEDIAN SURFACE, 6" (SPECIAL)

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		CONC MEDIAN SURF 6 SP (SQ FT)
		ALIGNMENT	STATION	ALIGNMENT	STATION	
SB I-57 - GUARDRAIL	LT	SB I-57	92+87.5	SB I-57	93+92.7	305
SB I-57 - HALSTED EXIT GORE	LT	SB I-57	102+73.1	SB I-57	103+23.2	279
SB I-57 - GUARDRAIL	LT	SB I-57	109+15.4	SB I-57	110+20.5	305
SB I-57 - GUARDRAIL	LT	SB I-57	119+61.0	SB I-57	120+66.2	305
SB I-57 - WB CONN GORE	RT	SB I-57	131+45.7	SB I-57	131+95.6	325
SB I-57 - GUARDRAIL	RT	SB I-57	134+50.0	SB I-57	135+54.6	300
SB I-57 - GUARDRAIL	LT	SB I-57	134+50.0	SB I-57	135+57.6	310
SB I-57 - WENTWORTH EXIT GORE	LT	SB I-57	141+61.8	SB I-57	142+13.2	225
EB CONNECTOR - GUARDRAIL	LT	EB CON	418+81.0	SB I-94(RYAN)	419+85.1	297
EB CONNECTOR - GUARDRAIL	RT	EB CON	419+22.7	SB I-94(RYAN)	420+30.5	312
SB I-94 (FORD) - GUARDRAIL	LT	SB I-94(FORD)	1130+05.8	SB I-94(FORD)	1131+10.8	305
SB I-94 (FORD) - GUARDRAIL	LT	SB I-94(FORD)	1146+26.6	SB I-94(FORD)	1147+29.7	298
SB I-94 (FORD) - EB CONN GORE	LT	SB I-94(FORD)	1151+28.2	SB I-94(FORD)	1151+78.1	327
SB I-94 (FORD) - SB I-57 GORE	LT	SB I-94(FORD)	1168+14.3	SB I-94(FORD)	1169+61.6	673
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1212+30.3	SB I-94(RYAN)	1213+35.5	305
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1218+75.0	SB I-94(RYAN)	1219+80.2	305
SB I-94 (RYAN) - 95TH EXIT GORE	LT	SB I-94(RYAN)	1221+12.9	SB I-94(RYAN)	1221+67.9	246
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1241+10.9	SB I-94(RYAN)	1242+16.1	305
SB I-94 (RYAN) - 87TH ENTRANCE GORE	LT	SB I-94(RYAN)	1248+94.2	SB I-94(RYAN)	1249+44.3	236
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1252+87.6	SB I-94(RYAN)	1253+92.8	305
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1261+63.2	SB I-94(RYAN)	1262+68.4	305
SB I-94 (RYAN) - 87TH EXIT GORE	LT	SB I-94(RYAN)	1271+27.4	SB I-94(RYAN)	1271+77.5	255
SB I-94 (RYAN) - GUARDRAIL	LT	SB I-94(RYAN)	1289+16.6	SB I-94(RYAN)	1290+18.5	295
I000-2A TOTAL						1539
J000-2A TOTAL						5584
TOTAL						7123

PORTLAND CEMENT CONCRETE SHOULDERS 9"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		PCC SHOULDERS 9 (SQ YD)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-94 (FORD) - INSIDE TUNNEL	LT	SB I-94(FORD)	1163+99.3	SB I-94(FORD)	1167+93.6	446	446
SB I-94 (FORD) - INSIDE TUNNEL	RT	SB I-94(FORD)	1164+12.5	SB I-94(FORD)	1167+93.6	79	79
SEWER TRENCH PATCHES	-	-	-	-	-	269	269
I000-2A TOTAL						794	794
J000-2A TOTAL							
TOTAL						794	794

PORTLAND CEMENT CONCRETE SHOULDERS 14"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		PCC SHOULDERS 14 (SQ YD)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-57 - WEST OF HALSTED RAMP	LT	SB I-57	89+45.0	SB I-57	103+97.6	1358	1358
SB I-57 - HALSTED RAMP TO WENTWORTH RAMP	LT	SB I-57	103+99.0	SB I-57	143+90.1	3360	3360
SB I-57 - PARNELL AV TO WB CONNECTOR	RT	SB I-57	107+15.6	WB CON	340+29.2	2750	2750
SB I-57 - WB CONNECTOR TO SB FORD	RT	SB I-57	130+38.0	SB I-57	148+00.4	1677	1677
SB I-94 (FORD) - NORTH OF TUNNEL	LT	SB I-94(FORD)	1167+93.6	SB I-94(FORD)	1169+64.7	121	121
SB I-57 - WENTWORTH RAMP TO 95TH ST	LT	SB I-57	146+20.5	SB I-57	156+05.9	773	773
SB I-94 (RYAN) - 95TH ST TO 95TH RAMP	LT	SB I-57	156+05.9	SB I-94(RYAN)	1223+70.4	1408	1408
SB I-94 (RYAN) - 95TH RAMP TO 87TH ENT	LT	SB I-94(RYAN)	1226+19.5	SB I-94(RYAN)	1236+28.2	1023	1023
SB I-94 (RYAN) - 95TH RAMP TO 87TH ENT	LT	SB I-94(RYAN)	1236+28.2	SB I-94(RYAN)	1241+11.1	226	226
SB I-94 (RYAN) - 87TH ENT TO 87TH ST	LT	SB I-94(RYAN)	1246+59.7	SB I-94(RYAN)	1273+10.9	2286	2286
SB I-94 (RYAN) - 87TH EXIT TO 79TH ENT	LT	SB I-94(RYAN)	1274+75.4	SB I-94(RYAN)	1289+96.8	1148	1148
SB I-94 (RYAN) - 79TH ENT TO 83RD EXIT	LT	SB I-94(RYAN)	1295+06.8	SB I-94(RYAN)	1306+61.9	1284	1284
SB I-94 (RYAN) - 83RD EXIT TO 79TH ST	LT	SB I-94(RYAN)	1309+25.3	SB I-94(RYAN)	1312+06.9	188	188
SB I-94 (RYAN) - 79TH ST TO 75TH ENT	LT	SB I-94(RYAN)	1312+15.9	SB I-94(RYAN)	1313+00.0	21	21
SB I-94 (FORD-RYAN) MEDIAN	RT	SB I-94(FORD)	1167+93.6	SB I-94(RYAN)	1210+00.0	1517	1517
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1210+00.0	SB I-94(RYAN)	1230+00.0	1747	1747
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1230+00.0	SB I-94(RYAN)	1250+00.0	2154	2154
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1250+00.0	SB I-94(RYAN)	1270+00.0	1689	1689
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1270+00.0	SB I-94(RYAN)	1290+00.0	2202	2202
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1290+00.0	SB I-94(RYAN)	1310+00.0	1969	1969
SB I-94 (DAN RYAN) MEDIAN	RT	SB I-94(RYAN)	1310+00.0	SB I-94(RYAN)	1313+00.0	232	232
I000-2A TOTAL							
J000-2A TOTAL						29133	29133
TOTAL						29133	29133

CONCRETE MEDIAN SURFACE, 4"

LOCATION DESCRIPTION	OFFSET DIRECTION	FROM		TO		CONC MEDIAN SURF 4 (SQ FT)	PROTECTIVE COAT (SQ YD)
		ALIGNMENT	STATION	ALIGNMENT	STATION		
SB I-57 - INDER WENTWORTH AV	LT	SB I-57	130+03.0	SB I-57	130+99.8	462	51
EB CONNECTOR - UNDER STATE ST	LT	EB CON	421+84.3	EB CON	722+26.7	174	19
EB CONNECTOR - UNDER STATE ST	RT	EB CON	421+95.0	EB CON	422+47.2	1102	122
SB I-94 (FORD)/EB CONN - UNDER STATE ST	LT	SB I-94(FORD)	1153+27.4	SB I-94(FORD)	1153+97.7	2034	226
SB I-94 (FORD) - UNDER MICHIGAN AV	LT	SB I-94(FORD)	1143+12.6	SB I-94(FORD)	1144+10.3	834	93
I000-2A TOTAL						4144	460
J000-2A TOTAL						462	51
TOTAL						4606	512

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 3

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

CONCRETE BARRIER

LOCATION DESCRIPTION	FROM			TO			CONC BAR SIN FACE 32 (FOOT)	CONC BAR SIN FACE 32 SPL (FOOT)	CONC BAR SIN FACE 32 MOD (FOOT)	CONC BAR SIN FACE 42 (FOOT)	CONC BAR SIN FACE 42 SPL (FOOT)	CONC BAR TRANS (FOOT)	BARRIER BASE (FOOT)	BAR WALL MKR TYPE C (EACH)	PROTECTIVE COAT (SQ YD)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET									
SB I-57	SB I-57	90+87.6	36.8 LT	SB I-57	91+02.6	36.0 LT						15	15		12
SB I-57	SB I-57	91+02.6	36.0 LT	SB I-57	92+87.5	36.0 LT	185						185	5	141
SB I-57	SB I-57	103+99.4	61.9 LT	SB I-57	106+37.9	41.8 LT	240						240	5	183
SB I-57	SB I-57	106+37.9	41.8 LT	SB I-57	106+52.9	41.8 LT						15	15		12
SB I-57	SB I-57	107+15.5	41.9 LT	SB I-57	107+30.5	42.5 LT						15	15		12
SB I-57	SB I-57	107+30.5	42.5 LT	SB I-57	109+15.4	48.0 LT	185						185	5	141
SB I-57	SB I-57	116+92.4	43.0 LT	SB I-57	117+07.4	43.0 LT						15	15		12
SB I-57	SB I-57	117+07.4	43.0 LT	SB I-57	119+61.0	48.0 LT	254						254	5	193
SB I-57	SB I-57	130+07.7	42.0 LT	SB I-57	134+50.0	48.0 LT	432						432	6	329
SB I-57	SB I-57	107+15.6	22.2 RT	SB I-57	107+30.6	22.0 RT						15	15		12
SB I-57	SB I-57	107+30.6	22.0 RT	SB I-57	116+34.3	22.0 RT	904						904	7	688
SB I-57	SB I-57	116+34.3	22.0 RT	SB I-57	116+49.3	22.0 RT						15	15		12
SB I-57	SB I-57	116+91.6	22.0 RT	SB I-57	117+06.6	22.0 RT						15	15		12
SB I-57	SB I-57	117+06.6	22.0 RT	SB I-57	129+50.1	35.5 RT	1255						1255	8	955
SB I-57	SB I-57	129+50.1	35.5 RT	SB I-57	129+64.7	34.0 RT						15	15		12
SB I-57	SB I-57	133+13.9	10.0 RT	SB I-57	133+28.9	10.0 RT						15	15		12
SB I-57	SB I-57	133+28.9	10.0 RT	SB I-57	134+50.0	10.0 RT	122						122	5	93
WB CONNECTOR	WB CON	340+29.2	26.0 LT	WB CON	341+55.5	26.0 LT	127						127	5	97
WB CONNECTOR	WB CON	341+55.5	26.0 LT	WB CON	341+70.5	25.1 LT						15	15		12
EB CONNECTOR	EB CON	419+85.1	10.0 LT	EB CON	420+69.5	8.3 LT	85						85	5	65
EB CONNECTOR	EB CON	420+69.5	8.3 LT	EB CON	420+84.4	8.6 LT						15	15		12
EB CONNECTOR	EB CON	420+30.5	34.0 RT	EB CON	420+66.3	34.2 RT	35						35	5	27
EB CONNECTOR	EB CON	420+66.3	34.2 RT	EB CON	420+81.7	34.0 RT						15	15		12
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1126+90.8	22.2 LT	SB I-94(FORD)	1127+05.8	22.0 LT						15	15		12
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1127+05.8	22.0 LT	SB I-94(FORD)	1130+05.8	22.0 LT	300						300	5	228
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1144+87.0	29.5 LT	SB I-94(FORD)	1145+01.8	29.8 LT						15	15		12
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1145+01.8	29.8 LT	SB I-94(FORD)	1146+26.7	33.1 LT	127						127	5	97
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1164+17.0	11.1 LT	SB I-94(FORD)	1167+89.8	11.4 LT			376				376	5	131
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1167+89.8	11.4 LT	SB I-94(FORD)	1168+14.3	10.0 LT	25						25	5	19
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1164+12.5	34.0 RT	SB I-94(FORD)	1167+78.3	32.0 RT			360				360	5	126
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1167+78.3	32.0 RT	SB I-94(FORD)	1167+93.6	31.9 RT						15	15		12
SB I-94 (DAN RYAN)	SB I-57	155+46.0	41.2 LT	SB I-57	155+90.9	39.7 LT	45						45	5	34
SB I-94 (DAN RYAN)	SB I-57	155+90.9	39.7 LT	SB I-57	156+05.9	39.2 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1209+97.1	63.0 LT	SB I-94(RYAN)	1210+12.1	63.6 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1210+12.1	63.6 LT	SB I-94(RYAN)	1212+30.3	72.0 LT	219						219	5	167
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1216+75.0	72.0 LT	SB I-94(RYAN)	1218+75.0	72.0 LT	200						200	5	152
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1227+53.8	72.0 LT	SB I-94(RYAN)	1232+13.8	72.0 LT	460						460	6	350
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1232+13.8	72.0 LT	SB I-94(RYAN)	1232+28.8	72.7 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1233+00.0	72.6 LT	SB I-94(RYAN)	1233+15.0	72.0 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1233+15.0	72.0 LT	SB I-94(RYAN)	1235+41.8	76.1 LT	227						227	5	173
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1235+39.2	66.0 LT	SB I-94(RYAN)	1236+13.3	64.6 LT	75						75	5	57
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1236+13.3	64.6 LT	SB I-94(RYAN)	1236+28.2	63.1 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+19.6	65.4 LT	SB I-94(RYAN)	1237+34.6	65.9 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+34.6	65.9 LT	SB I-94(RYAN)	1237+76.5	67.4 LT	42						42	5	32
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+76.5	67.4 LT	SB I-94(RYAN)	1237+91.5	67.9 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1239+11.1	67.6 LT	SB I-94(RYAN)	1239+26.1	68.0 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1239+26.1	68.0 LT	SB I-94(RYAN)	1241+11.0	72.6 LT	185						185	5	141
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1251+37.6	72.0 LT	SB I-94(RYAN)	1252+87.6	72.0 LT	150						150	5	114
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1257+59.7	72.0 LT	SB I-94(RYAN)	1258+34.9	64.5 LT	76						76	5	58
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1258+34.9	64.5 LT	SB I-94(RYAN)	1258+49.8	63.0 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1259+61.0	64.2 LT	SB I-94(RYAN)	1259+76.0	64.8 LT						15	15		12
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1259+76.0	64.8 LT	SB I-94(RYAN)	1261+63.3	72.0 LT	188						188	5	143
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1283+91.3	72.0 LT	SB I-94(RYAN)	1285+25.9	65.3 LT	135						135	5	103
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1285+25.9	65.3 LT	SB I-94(RYAN)	1285+40.8	63.8 LT						15	15		12

(CONT. NEXT SHEET)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	46
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				
				62593

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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 4

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

CONCRETE BARRIER (CONT. FROM PREVIOUS SHEET)

LOCATION DESCRIPTION	FROM			TO			CONC BAR SIN FACE 32 (FOOT)	CONC BAR SIN FACE 32 SPL (FOOT)	CONC BAR SIN FACE 32 MOD (FOOT)	CONC BAR SIN FACE 42 (FOOT)	CONC BAR SIN FACE 42 SPL (FOOT)	CONC BAR TRANS (FOOT)	BARRIER BASE (FOOT)	BAR WALL MKR TYPE C (EACH)	PROTECTIVE COAT (SQ YD)	
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET										
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1286+36.8	63.8 LT	SB I-94(RYAN)	1286+51.8	64.4 LT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1286+51.8	64.4 LT	SB I-94(RYAN)	1289+16.6	72.0 LT	265						265	5	202	
SB I-94 (DAN RYAN)	SB I-94(FORD)	1167+93.6	31.9 RT	SB I-57	155+90.9	32.6 RT				1117			1117	7	971	
SB I-94 (DAN RYAN)	SB I-57	155+90.9	32.6 RT	SB I-57	156+05.9	33.2 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1213+39.0	6.8 RT	SB I-94(RYAN)	1223+94.6	7.5 RT				1056			1056	7	918	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1224+05.4	7.5 RT	SB I-94(RYAN)	1232+11.8	10.0 RT				807			807	7	702	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1232+11.8	10.0 RT	SB I-94(RYAN)	1232+26.8	8.6 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1233+01.8	8.7 RT	SB I-94(RYAN)	1233+16.7	9.5 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1233+16.7	9.5 RT	SB I-94(RYAN)	1236+13.1	10.0 RT				297			297	5	258	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1236+13.1	10.0 RT	SB I-94(RYAN)	1236+28.0	8.2 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1236+61.4	8.2 RT	SB I-94(RYAN)	1236+81.9	8.2 RT				21			21	5	18	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+15.7	8.3 RT	SB I-94(RYAN)	1237+30.7	8.3 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+30.7	8.3 RT	SB I-94(RYAN)	1237+53.0	8.3 RT				23			23	5	20	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1237+53.0	8.3 RT	SB I-94(RYAN)	1237+68.0	8.4 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1238+70.3	8.5 RT	SB I-94(RYAN)	1238+85.3	9.3 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1238+85.3	9.3 RT	SB I-94(RYAN)	1251+37.6	8.0 RT				1253			1253	8	1089	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1251+48.4	7.9 RT	SB I-94(RYAN)	1252+90.0	7.3 RT				142			142	5	123	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1259+60.0	8.1 RT	SB I-94(RYAN)	1259+74.9	6.5 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1259+74.9	6.5 RT	SB I-94(RYAN)	1266+12.5	6.5 RT				638			638	6	555	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1266+12.5	6.5 RT	SB I-94(RYAN)	1266+50.0	6.6 RT						38	38		30	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1267+00.0	6.6 RT	SB I-94(RYAN)	1267+37.5	8.0 RT						38	38		30	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1267+37.5	8.0 RT	SB I-94(RYAN)	1276+61.6	10.0 RT				925			925	7	804	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1276+72.4	10.0 RT	SB I-94(RYAN)	1285+26.1	10.0 RT				854			854	7	743	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1285+26.1	10.0 RT	SB I-94(RYAN)	1285+41.0	8.4 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1286+36.6	8.4 RT	SB I-94(RYAN)	1286+51.6	9.2 RT						15	15		12	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1286+51.6	9.2 RT	SB I-94(RYAN)	1286+79.0	10.0 RT				28			28	5	24	
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1286+91.0	10.0 RT	SB I-94(RYAN)	1306+52.1	6.5 RT				1962			1962	9	1706	
SB I-57 SEWER TRENCHES	-	-	-	-	-	-				10			10		9	
INTERCHANGE SEWER TRENCHES	-	-	-	-	-	-							210		191	
							1000-2A TOTAL	572	210	736			75	1593	35	942
							J000-2A TOTAL	5971			10	9123	556	15660	200	12922
							TOTAL	6543	210	736	10	9123	631	17253	235	13864

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 5

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

CTA FENCE

LOCATION DESCRIPTION	FROM			TO			CTA FENCE (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1213+13.2	8.3 RT	SB I-94(RYAN)	1213+73.2	8.6 RT	60
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1213+79.2	8.7 RT	SB I-94(RYAN)	1215+66.4	9.2 RT	188
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1215+72.4	9.2 RT	SB I-94(RYAN)	1228+56.3	11.8 RT	1285
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1228+62.3	11.8 RT	SB I-94(RYAN)	1232+26.8	10.3 RT	365
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1233+01.8	10.4 RT	SB I-94(RYAN)	1236+28.2	9.9 RT	327
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1236+61.4	9.9 RT	SB I-94(RYAN)	1236+81.9	10.0 RT	21
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1237+15.7	10.0 RT	SB I-94(RYAN)	1237+68.0	10.1 RT	53
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1238+70.2	10.2 RT	SB I-94(RYAN)	1252+72.0	9.0 RT	1403
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1252+78.0	9.0 RT	SB I-94(RYAN)	1253+09.5	9.6 RT	32
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1259+60.1	9.8 RT	SB I-94(RYAN)	1263+02.0	8.2 RT	342
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1263+08.0	8.2 RT	SB I-94(RYAN)	1266+50.0	8.9 RT	342
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1267+00.0	8.8 RT	SB I-94(RYAN)	1268+30.0	11.8 RT	130
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1268+36.0	11.8 RT	SB I-94(RYAN)	1274+29.8	11.8 RT	594
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1274+35.8	11.8 RT	SB I-94(RYAN)	1285+41.2	10.2 RT	1106
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1286+36.5	10.2 RT	SB I-94(RYAN)	1295+10.9	11.8 RT	875
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1295+16.9	11.8 RT	SB I-94(RYAN)	1305+41.8	8.2 RT	1025
SB I-94 (DAN RYAN) - MEDIAN	SB I-94(RYAN)	1305+47.8	8.2 RT	SB I-94(RYAN)	1306+79.7	7.9 RT	132
I000-2A TOTAL							
J000-2A TOTAL							8280
TOTAL							8280

CTA FENCE (SPECIAL)

LOCATION DESCRIPTION	FROM			TO			CTA FENCE SPL (FOOT)
	ALIGNMENT	STATION	OFFSET	ALIGNMENT	STATION	OFFSET	
SB I-94 (FORD/RYAN) - MEDIAN	SB I-94(FORD)	1167+93.7	33.6 RT	SB I-57	156+06.7	35.0 RT	1133
I000-2A TOTAL							
J000-2A TOTAL							1133
TOTAL							1133

PROPOSED GUARDRAIL

LOCATION DESCRIPTION	ALIGNMENT	END OF TYPE 6 TERMINAL		JOINT BETWEEN TERMINALS		END OF TYPE 1 TERMINAL		TR BAR TRM T1 SPL TAN (EACH)	TRAF BAR TERM T6 (EACH)	GUARDRAIL MKR TYPE A (EACH)	TERMINAL MARKER - DA (EACH)
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET				
SB I-57	SB I-57	92+87.5	36.0 LT	93+18.2	36.0 LT	93+68.2	37.0 LT	1	1	4	1
SB I-57	SB I-57	109+15.4	48.0 LT	109+46.0	48.0 LT	109+96.0	49.0 LT	1	1	4	1
SB I-57	SB I-57	119+61.0	48.0 LT	119+91.7	48.0 LT	120+41.6	49.0 LT	1	1	4	1
SB I-57	SB I-57	134+50.0	10.0 RT	134+80.5	10.0 RT	135+30.2	11.0 RT	1	1	4	1
SB I-57	SB I-57	134+50.0	48.0 LT	134+81.5	48.0 LT	135+32.8	49.0 LT	1	1	4	1
EB CONNECTOR	EB CON	419+85.1	10.0 LT	419+54.7	10.0 LT	419+05.1	11.0 LT	1	1	4	1
EB CONNECTOR	EB CON	420+30.5	34.0 RT	419+99.1	34.0 RT	419+47.8	35.0 RT	1	1	4	1
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1130+05.8	22.0 LT	1130+36.5	22.0 LT	1130+86.5	23.0 LT	1	1	4	1
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1146+26.7	33.1 LT	1146+56.8	33.6 LT	1147+05.9	35.0 LT	1	1	4	1
SB I-94 (BISHOP FORD)	SB I-94(FORD)	1168+14.3	10.0 LT	1168+44.8	10.0 LT	1168+94.5	11.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1212+30.3	72.0 LT	1212+61.0	72.0 LT	1213+11.0	73.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1218+75.0	72.0 LT	1219+05.7	72.0 LT	1219+55.6	73.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1241+11.0	72.6 LT	1241+41.6	73.4 LT	1241+91.6	75.6 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1252+87.6	72.0 LT	1253+18.3	72.0 LT	1253+68.3	73.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1261+63.3	72.0 LT	1261+93.9	72.0 LT	1262+43.9	73.0 LT	1	1	4	1
SB I-94 (DAN RYAN)	SB I-94(RYAN)	1289+16.6	72.0 LT	1289+47.3	72.0 LT	1289+97.2	73.0 LT	1	1	4	1
I000-2A TOTAL								4	4	16	4
J000-2A TOTAL								12	12	48	12
TOTAL								16	16	64	16

CTA GATES

LOCATION DESCRIPTION	LOCATION			CTA GATES (EACH)
	ALIGNMENT	STATION	OFFSET	
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1213+73.2	8.6 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1215+66.4	9.2 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1228+56.3	11.8 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1252+72.0	9.0 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1263+02.0	8.2 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1268+30.0	11.8 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1274+29.8	11.8 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1295+10.9	11.8 RT	1
SB I-94 (DAN RYAN) MEDIAN	SB I-94(RYAN)	1305+41.8	8.2 RT	1
I000-2A TOTAL				
J000-2A TOTAL				9
TOTAL				9

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

SCHEDULE OF QUANTITIES
PROPOSED QUANTITIES - SHEET 6

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

TEMPORARY IMPACT ATTENUATORS

LOCATION		IMPACT ATTENUATORS REQUIRED (EACH)	STAGE *	REMAINS THROUGH STAGE *	TEMPORARY IMPACT ATTENUATOR (EACH)†	RELOCATE IMPACT ATTENUATOR (EACH)†	ATTENUATOR REMAIN FROM STAGE-STAGE (EACH)	ATTENUATOR RELOCATED @ END STAGE (EACH)	ATTENUATOR REMOVED @ END STAGE (EACH)
ALIGNMENT	STATION								
SB I-57	103+60	1	1	1A	1		1		
SB I-57	142+96	1	1	1A	1		1		
SB I-94	1222+01	1	1	1A	1		1		
SB I-94	1234+50	1	1	1A	1		1		
SB I-94	1272+67	1	1	1A	1		1		
SB I-94	1296+11	1	1	1A	1		1		
SB I-94	1304+38	1	1	1A	1		1		
SB I-57	103+60	1	1A	1A					1
SB I-57	142+96	1	1A	1A				1	
SB I-94	1222+01	1	1A	1A					1
SB I-94	1234+50	1	1A	1A				1	
SB I-94	1272+67	1	1A	1A				1	
SB I-94	1296+11	1	1A	1A				1	
SB I-94	1304+38	1	1A	1A				1	
SB I-57*	93+38	1	W	3D	1		1		
SB I-57	158+25	1	W	W		1		1	
SB I-94	1234+50	1	W	W		1		1	
SB I-94	1272+66	1	W	W		1		1	
SB I-94	1287+09	1	W	W		1		1	
SB I-94	1304+37	1	W	W		1		1	
SB I-57*	93+38	1	2	3D			1		
SB I-57	155+65	1	2	2A		1	1		
SB I-94	1215+28	1	2	2A		1	1		
SB I-94	1276+87	1	2	2		1		1	
SB I-94	1283+38	1	2	2		1		1	
EB CONN	420+97	1	2	2A		1	1		
STAGE 2 WORK ZONES		2	2	2A	2		2		
SB I-57*	93+38	1	2A	3D			1		
SB I-57	155+65	1	2A	2A				1	
SB I-94	1215+28	1	2A	2A				1	
SB I-94	1266+82	1	2A	2A		1		1	
SB I-94	1273+38	1	2A	2A		1		1	
EB CONN	420+97	1	2A	2A				1	
STAGE 2 WORK ZONES		2	2A	2A				2	
STAGE 3 WORK ZONES		2	3	3		2			2
SB I-57*	93+38	1	3A	3D			1		
SB I-57	155+82	1	3A	3A		1		1	
SB I-94	1219+02	1	3A	3A		1		1	
SB BF	1144+47	1	3A	3A		1		1	
STAGE 3A WORK ZONES		2	3A	3A		2		2	
SB I-57*	93+38	1	3B	3D			1		
SB BF	1127+17	1	3B	3B		1			1
STAGE 3B WORK ZONES		4	3B	3B		4			4
SB I-57*	93+38	1	3C	3D				1	
SB I-57*	93+38	1	3D	3D		1			1
STAGE 1 TOTALS:		7			7		7		
STAGE 1A TOTALS:		7					5		2
WINTER TOTALS:		6			1	5	1	5	
STAGE 2 TOTALS:		8			2	5	6	2	
STAGE 2A TOTALS:		8				2	1	7	
STAGE 3 TOTALS:		2				2			2
STAGE 3A TOTALS:		6				5	1	5	
STAGE 3B TOTALS:		6				5	1		5
STAGE 3C TOTALS:		1						1	
STAGE 3D TOTALS:		1				1			1
FINAL TOTALS:		52			10	25	17	25	10

+ - DENOTES IDOT PAY ITEMS
 • - DENOTES IMPACT ATTENUATORS, TEMPORARY (SEVERE USE, NARROW), TEST LEVEL 3

NOTES:

- "ALIGNMENT" REFERS TO PROPOSED ALIGNMENTS.
- STAGES 3 AND 3A BEGIN AT THE SAME TIME.

REVISIONS	
NAME	DATE

EARTHWORK SCHEDULE

LOCATION DESCRIPTION	FROM		TO		EARTH EXCAVATION (CU YD)	EMBANKMENT (CU YD)	REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU YD)
	ALIGNMENT	STATION	ALIGNMENT	STATION			
HALSTED TO PARNELL	SB I-57	89+50.0	SB I-57	106+50.0	7715	10	745
PARNELL TO RAILROAD BRIDGE	SB I-57	107+00.0	SB I-57	116+50.0	12950	5	935
RAILROAD BRIDGE TO WENTWORTH	SB I-57	117+00.0	SB I-57	130+50.0	9785	60	1060
WENTWORTH TO 95TH	SB I-57	131+00.0	SB I-57	158+50.0	18825	180	1895
TEMP. PAVEMENT WIDENING (MOT ST. 1)	SB I-57	132+00.0	SB I-57	146+00.0	520	25	335
I-57 NB to I-94 SB CONNECTOR	EBXCON	415+06.0	EBXCON	424+23.0	35	5	525
MLK DRIVE TO I-57 SB	SB I-94	1127+00.0	SB I-94	1169+00.0	575	25	970
95TH TO BELT RAILWAY	SB I-94	1208+00.0	SB I-94	1238+00.0	23510	45	840
BELT RAILWAY TO 87TH	SB I-94	1238+50.0	SB I-94	1259+00.0	17450	30	1055
87TH TO 83RD	SB I-94	1259+50.0	SB I-94	1286+00.0	21515	140	1690
83RD TO 79TH	SB I-94	1286+50.0	SB I-94	1313+00.0	18740	5	825
12" PGES - SEE SEPARATE SCHEDULE					4401		
TOTAL					136021	530	10875

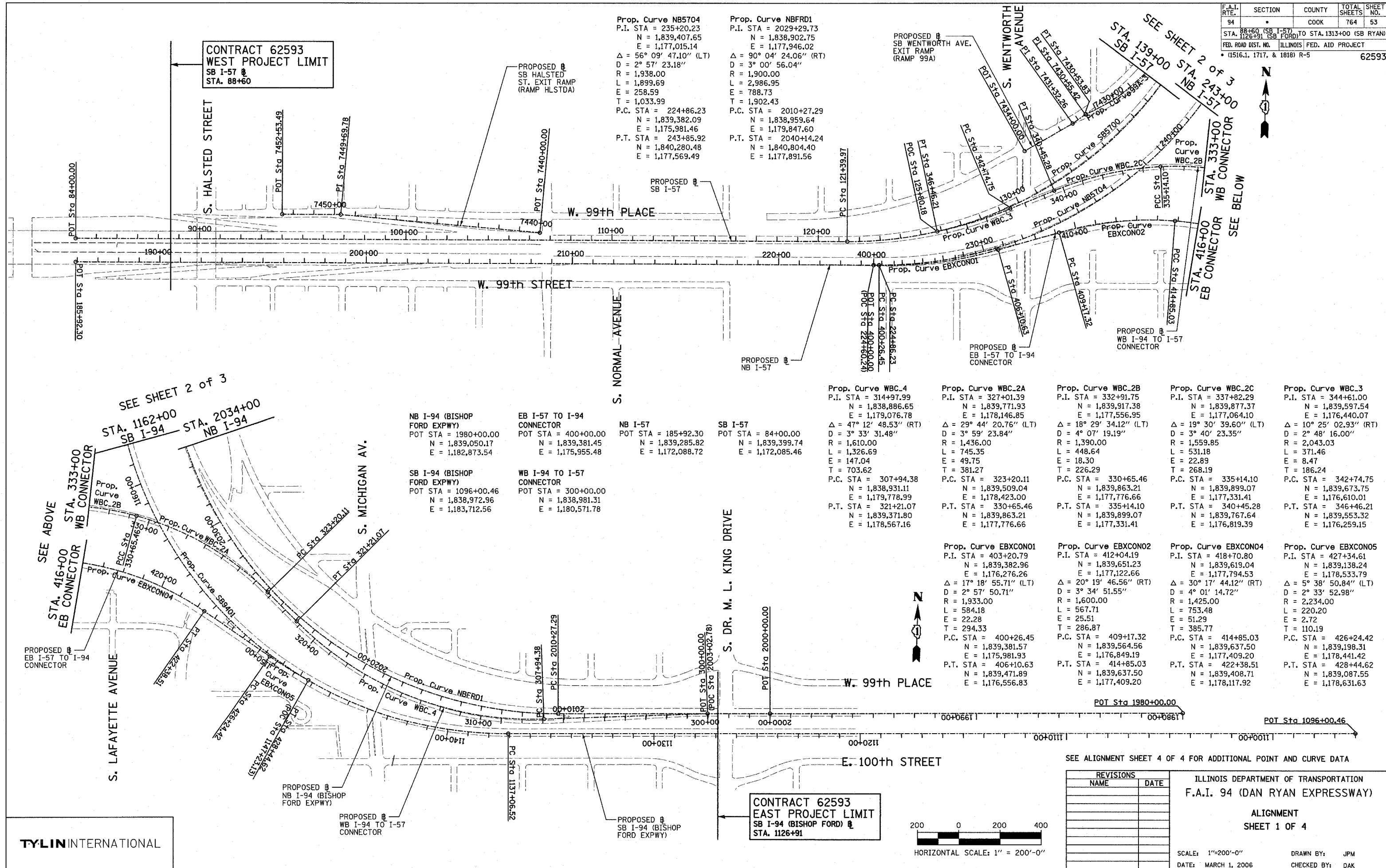
POROUS GRANULAR EMBANKMENT, SUBGRADE

ALIGNMENT	DESCRIPTION	STATION		LENGTH (FOOT)	WIDTH (FOOT)	DEPTH (INCH)	VOLUME (CU YD)
		FROM	TO				
SB I-94	MAINLINE DAN RYAN	1224+65.0	1228+70.0	405	86	12	1290
SB I-94	MAINLINE DAN RYAN	1270+00.0	1280+00.0	1000	84	12	3111
TOTAL (CU YD)							4401

REVISIONS	
NAME	DATE

Prop. Curve NB5704
 P.I. STA = 235+20.23
 N = 1,839,407.65
 E = 1,177,015.14
 $\Delta = 56^\circ 09' 47.10''$ (LT)
 D = 2° 57' 23.18"
 R = 1,938.00
 L = 1,899.69
 E = 258.59
 T = 1,033.99
 P.C. STA = 224+86.23
 N = 1,839,382.09
 E = 1,175,981.46
 P.T. STA = 243+85.92
 N = 1,840,280.48
 E = 1,177,569.49

Prop. Curve NBFRD1
 P.I. STA = 2029+29.73
 N = 1,838,902.75
 E = 1,177,946.02
 $\Delta = 90^\circ 04' 24.06''$ (RT)
 D = 3° 00' 56.04"
 R = 1,900.00
 L = 2,986.95
 E = 788.73
 T = 1,902.43
 P.C. STA = 2010+27.29
 N = 1,838,959.64
 E = 1,179,847.60
 P.T. STA = 2040+14.24
 N = 1,840,804.40
 E = 1,177,891.56



NB I-94 (BISHOP FORD EXPWY)
 POT STA = 1980+00.00
 N = 1,839,050.17
 E = 1,182,873.54

EB I-57 TO I-94 CONNECTOR
 POT STA = 400+00.00
 N = 1,839,381.45
 E = 1,175,955.48

NB I-57
 POT STA = 185+92.30
 N = 1,839,285.82
 E = 1,172,088.72

SB I-57
 POT STA = 84+00.00
 N = 1,839,399.74
 E = 1,172,085.46

SB I-94 (BISHOP FORD EXPWY)
 POT STA = 1096+00.46
 N = 1,838,972.96
 E = 1,183,712.56

WB I-94 TO I-57 CONNECTOR
 POT STA = 300+00.00
 N = 1,838,981.31
 E = 1,180,571.78

Prop. Curve WBC-4
 P.I. STA = 314+97.99
 N = 1,838,886.65
 E = 1,179,076.78
 $\Delta = 47^\circ 12' 48.53''$ (RT)
 D = 3° 33' 31.48"
 R = 1,610.00
 L = 1,326.69
 E = 147.04
 T = 703.62
 P.C. STA = 307+94.38
 N = 1,838,931.11
 E = 1,179,778.99
 P.T. STA = 321+21.07
 N = 1,839,371.80
 E = 1,178,567.16

Prop. Curve WBC-2A
 P.I. STA = 327+01.39
 N = 1,839,771.93
 E = 1,178,146.85
 $\Delta = 29^\circ 44' 20.76''$ (LT)
 D = 3° 59' 23.84"
 R = 1,436.00
 L = 745.35
 E = 49.75
 T = 381.27
 P.C. STA = 323+20.11
 N = 1,839,509.04
 E = 1,177,776.66
 P.T. STA = 330+65.46
 N = 1,839,863.21
 E = 1,177,776.66

Prop. Curve WBC-2B
 P.I. STA = 332+82.29
 N = 1,839,917.38
 E = 1,177,556.95
 $\Delta = 18^\circ 29' 34.12''$ (LT)
 D = 4° 07' 19.19"
 R = 1,390.00
 L = 448.64
 E = 18.30
 T = 226.29
 P.C. STA = 330+65.46
 N = 1,839,863.21
 E = 1,177,776.66
 P.T. STA = 335+14.10
 N = 1,839,899.07
 E = 1,177,331.41

Prop. Curve WBC-2C
 P.I. STA = 337+82.29
 N = 1,839,877.37
 E = 1,177,064.10
 $\Delta = 19^\circ 30' 39.60''$ (LT)
 D = 3° 40' 23.35"
 R = 1,559.85
 L = 531.18
 E = 22.89
 T = 268.19
 P.C. STA = 335+14.10
 N = 1,839,899.07
 E = 1,177,331.41
 P.T. STA = 340+45.28
 N = 1,839,767.64
 E = 1,176,819.39

Prop. Curve WBC-3
 P.I. STA = 344+61.00
 N = 1,839,597.54
 E = 1,176,440.07
 $\Delta = 10^\circ 25' 02.93''$ (RT)
 D = 2° 48' 16.00"
 R = 2,043.03
 L = 371.46
 E = 8.47
 T = 186.24
 P.C. STA = 342+74.75
 N = 1,839,673.75
 E = 1,176,610.01
 P.T. STA = 346+46.21
 N = 1,839,553.32
 E = 1,176,259.15

Prop. Curve EBXCNO1
 P.I. STA = 403+20.79
 N = 1,839,382.96
 E = 1,176,276.26
 $\Delta = 17^\circ 18' 55.71''$ (LT)
 D = 2° 57' 50.71"
 R = 1,933.00
 L = 584.18
 E = 22.28
 T = 294.33
 P.C. STA = 400+26.45
 N = 1,839,381.57
 E = 1,175,981.93
 P.T. STA = 406+10.63
 N = 1,839,471.89
 E = 1,176,556.83

Prop. Curve EBXCNO2
 P.I. STA = 418+04.19
 N = 1,839,651.23
 E = 1,177,122.66
 $\Delta = 20^\circ 19' 46.56''$ (RT)
 D = 3° 34' 51.55"
 R = 1,600.00
 L = 567.71
 E = 25.51
 T = 385.87
 P.C. STA = 409+17.32
 N = 1,839,564.56
 E = 1,176,849.19
 P.T. STA = 414+85.03
 N = 1,839,637.50
 E = 1,177,409.20

Prop. Curve EBXCNO4
 P.I. STA = 418+70.80
 N = 1,839,619.04
 E = 1,177,794.53
 $\Delta = 30^\circ 17' 44.12''$ (RT)
 D = 4° 01' 14.72"
 R = 1,425.00
 L = 753.48
 E = 51.29
 T = 385.77
 P.C. STA = 414+85.03
 N = 1,839,637.50
 E = 1,177,409.20
 P.T. STA = 422+38.51
 N = 1,839,408.71
 E = 1,178,117.92

Prop. Curve EBXCNO5
 P.I. STA = 427+34.61
 N = 1,839,138.24
 E = 1,178,533.79
 $\Delta = 5^\circ 38' 50.84''$ (LT)
 D = 2° 33' 52.98"
 R = 2,234.00
 L = 220.20
 E = 2.72
 T = 110.19
 P.C. STA = 426+24.42
 N = 1,839,198.31
 E = 1,178,441.42
 P.T. STA = 428+44.62
 N = 1,839,087.55
 E = 1,178,631.63

SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

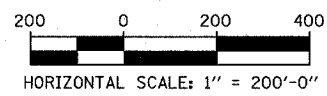
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

ALIGNMENT
 SHEET 1 OF 4

SCALE: 1"=200'-0"
 DATE: MARCH 1, 2006

DRAWN BY: JPM
 CHECKED BY: DAK



TYLIN INTERNATIONAL

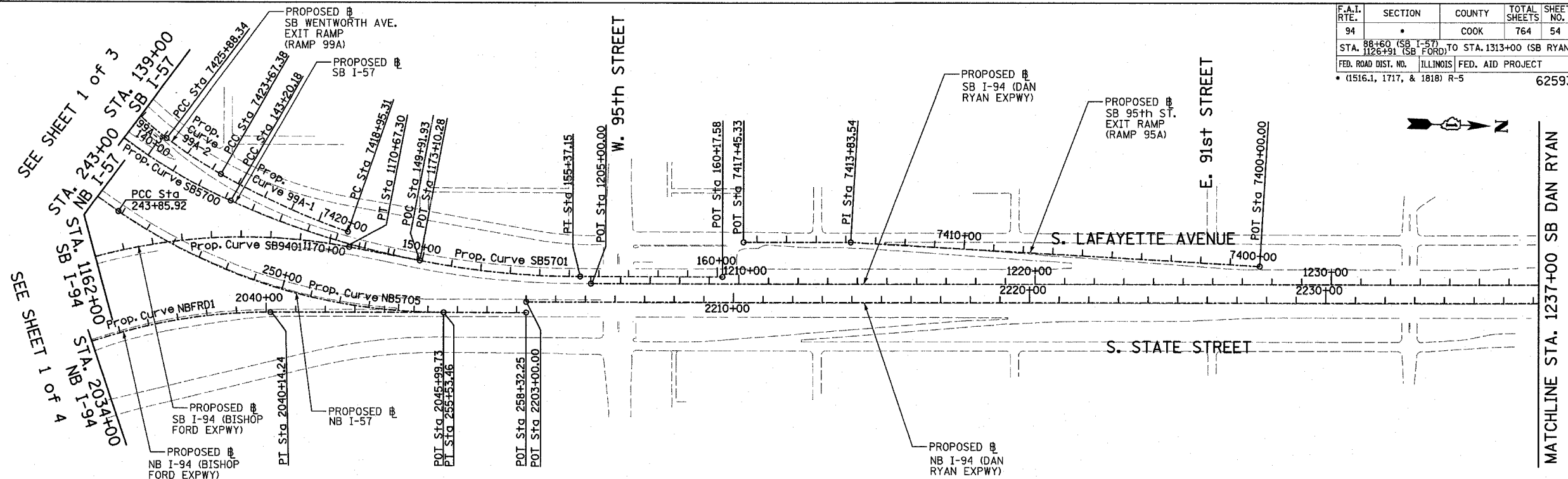
CONTRACT 62593
 EAST PROJECT LIMIT
 SB I-94 (BISHOP FORD) @
 STA. 1126+91

CONTRACT 62593
 WEST PROJECT LIMIT
 SB I-57 @
 STA. 88+60

SEE SHEET 2 OF 3
 STA. 139+00 SB I-57
 STA. 243+00 NB I-57
 STA. 333+00 WB CONNECTOR
 STA. 416+00 EB CONNECTOR
 SEE BELOW

SEE SHEET 2 OF 3
 STA. 1162+00 SB I-94
 STA. 2034+00 NB I-94

SEE ABOVE
 STA. 416+00 EB CONNECTOR
 STA. 333+00 WB CONNECTOR



Prop. Curve SB5700
 P.I. STA = 133+70.06
 N = 1,839,521.70
 E = 1,177,054.03
 $\Delta = 66^\circ 11' 55.15''$ (LT)
 $D = 3^\circ 02' 10.83''$
 R = 1,887.00
 L = 2,180.21
 E = 365.53
 T = 1,230.09
 P.C. STA = 121+39.97
 N = 1,839,491.52
 E = 1,175,824.31
 P.T. STA = 143+20.18
 N = 1,840,659.02
 E = 1,177,522.68

Prop. Curve SB5701
 P.I. STA = 149+37.75
 N = 1,841,230.00
 E = 1,177,757.97
 $\Delta = 24^\circ 02' 07.49''$ (LT)
 $D = 1^\circ 58' 30.13''$
 R = 2,901.00
 L = 1,216.96
 E = 65.01
 T = 617.56
 P.C. STA = 143+20.18
 N = 1,840,659.02
 E = 1,177,522.68
 P.T. STA = 155+37.15
 N = 1,841,847.31
 E = 1,177,740.29

Prop. Curve NB5705
 P.I. STA = 249+87.51
 N = 1,840,788.31
 E = 1,177,892.02
 $\Delta = 34^\circ 03' 37.98''$ (LT)
 $D = 2^\circ 55' 02.28''$
 R = 1,964.00
 L = 1,167.54
 E = 90.07
 T = 601.59
 P.C. STA = 243+85.92
 N = 1,840,280.48
 E = 1,177,569.49
 P.T. STA = 255+53.46
 N = 1,841,389.65
 E = 1,177,874.80

Prop. Curve SB9401
 P.I. STA = 1160+15.97
 N = 1,838,783.95
 E = 1,177,299.83
 $\Delta = 100^\circ 48' 58.07''$ (RT)
 $D = 2^\circ 59' 59.20''$
 R = 1,910.00
 L = 3,360.79
 E = 1,086.95
 T = 2,309.46
 P.C. STA = 1137+06.52
 N = 1,838,851.99
 E = 1,179,608.28
 P.T. STA = 1170+67.30
 N = 1,841,064.16
 E = 1,177,666.20

SB I-94 (DAN RYAN EXPWY)
 POT STA = 1205+00.00
 N = 1,841,884.04
 E = 1,177,763.25

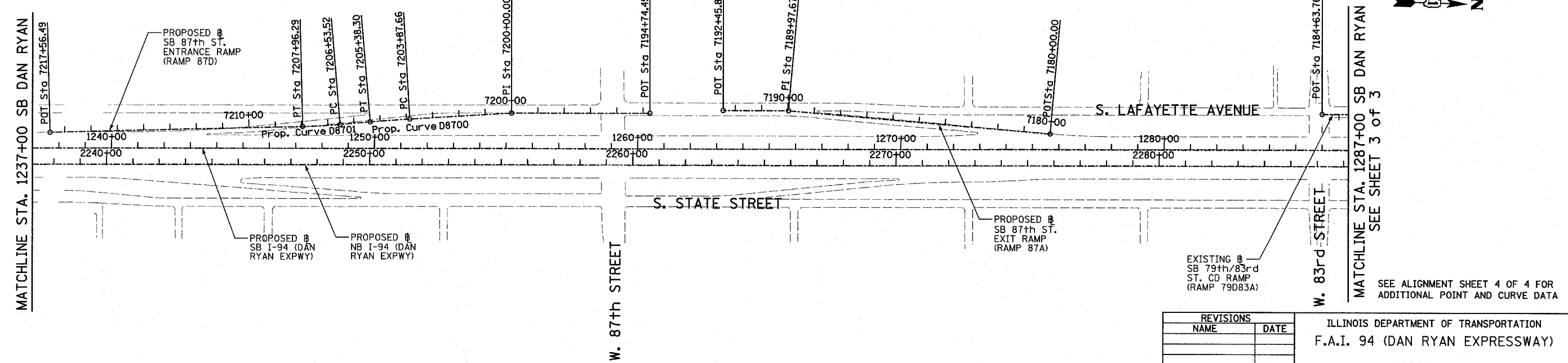
SB I-57
 POT STA = 160+17.58
 N = 1,842,327.55
 E = 1,177,726.54

NB I-57
 POT STA = 258+32.25
 N = 1,841,668.33
 E = 1,177,866.82

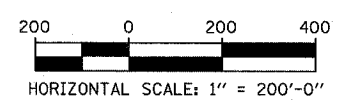
NB I-94 (DAN RYAN EXPWY)
 POT STA = 2203+00.00
 N = 1,841,667.31
 E = 1,177,830.83

SB I-94 (BISHOP FORD EXPWY)
 POT STA = 1173+10.28
 N = 1,841,304.06
 E = 1,177,704.74

NB I-94 (BISHOP FORD EXPWY)
 POT STA = 2045+99.73
 N = 1,841,389.65
 E = 1,177,874.80



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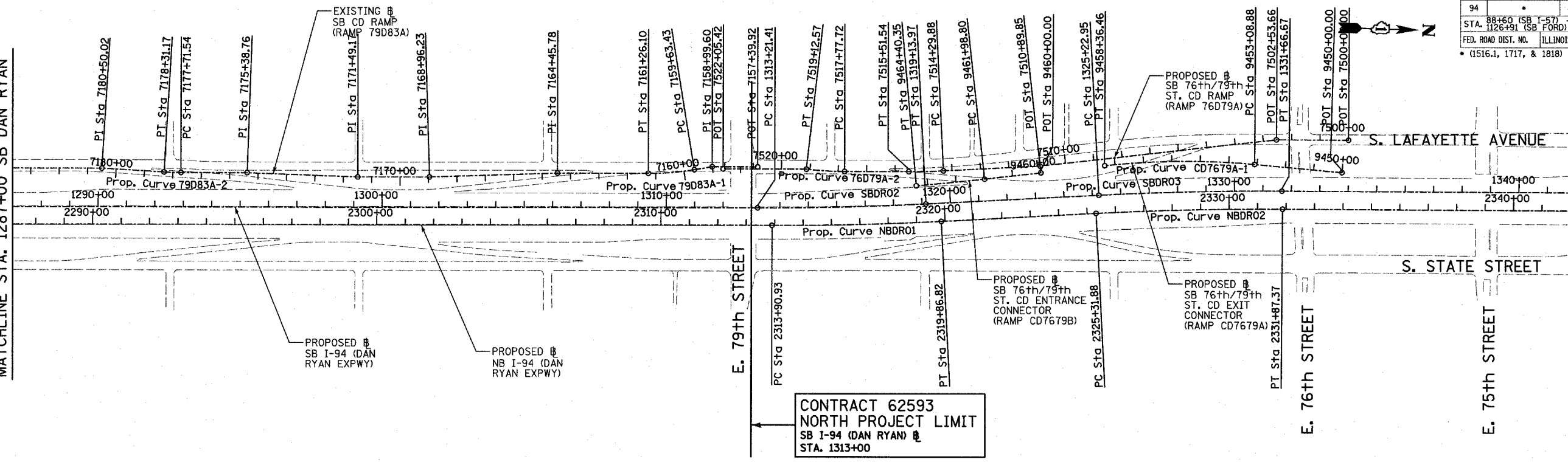


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
ALIGNMENT
SHEET 2 OF 4
 SCALE: 1"=200'
 DATE: MARCH 1, 2006
 DRAWN BY: JPM
 CHECKED BY: DAK

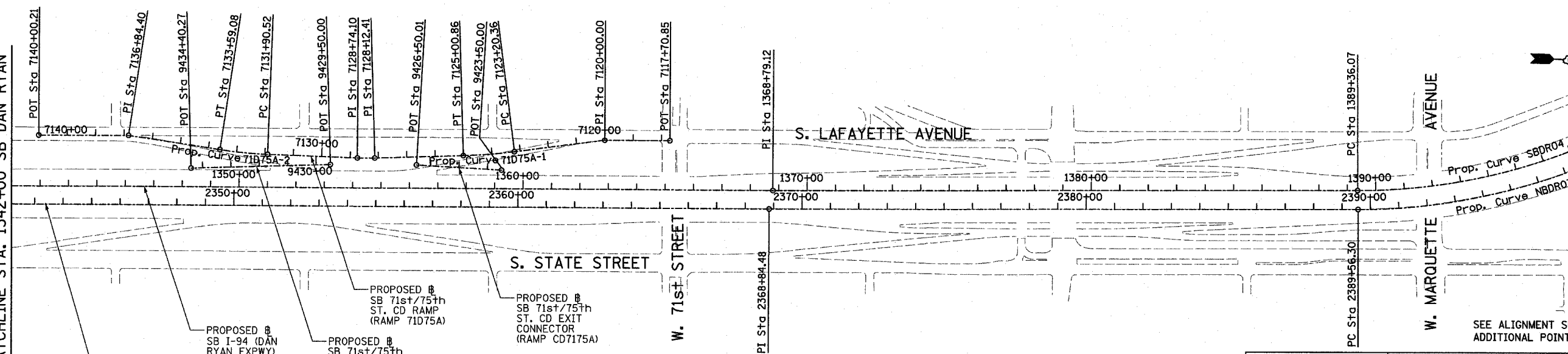
SEE SHEET 2 OF 3

MATCHLINE STA. 1287+00 SB DAN RYAN

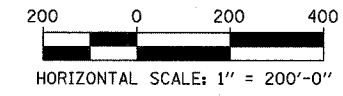


Prop. Curve NBDRO1	Prop. Curve NBDRO2	Prop. Curve NBDRO3	Prop. Curve SBDRO2	Prop. Curve SBDRO3	Prop. Curve SBDRO4	NB I-94 (DAN RYAN EXPRESSWAY)	SB I-94 (DAN RYAN EXPRESSWAY)
P.I. STA = 2316+88.94 N = 1,853,051.58 E = 1,177,504.84 Δ = 2° 58' 38.60" (LT) D = 0° 29' 58.77" R = 11,467.00 L = 595.89 E = 3.87 T = 298.01 P.C. STA = 2313+90.93 N = 1,852,753.69 E = 1,177,513.37 P.T. STA = 2319+86.82 N = 1,853,348.62 E = 1,177,480.85	P.I. STA = 2328+59.71 N = 1,854,218.69 E = 1,177,410.58 Δ = 3° 16' 48.34" (RT) D = 0° 30' 01.44" R = 11,450.00 L = 655.49 E = 4.69 T = 327.84 P.C. STA = 2325+31.88 N = 1,853,891.91 E = 1,177,436.97 P.T. STA = 2331+87.37 N = 1,854,546.43 E = 1,177,402.92	P.I. STA = 2398+13.58 N = 1,861,170.38 E = 1,177,231.19 Δ = 44° 36' 17.12" (LT) D = 2° 44' 29.13" R = 2,090.00 L = 1,627.06 E = 168.99 T = 857.27 P.C. STA = 2389+56.30 N = 1,860,313.48 E = 1,177,256.19 P.T. STA = 2405+83.37 N = 1,861,762.92 E = 1,176,611.65	P.I. STA = 1316+17.76 N = 1,852,997.25 E = 1,177,445.02 Δ = 2° 58' 38.60" (LT) D = 0° 30' 08.86" R = 11,403.00 L = 592.56 E = 3.85 T = 296.35 P.C. STA = 1313+21.41 N = 1,852,701.02 E = 1,177,453.50 P.T. STA = 1319+13.97 N = 1,853,292.63 E = 1,177,421.16	P.I. STA = 1328+44.90 N = 1,854,220.54 E = 1,177,346.22 Δ = 3° 16' 11.04" (RT) D = 0° 30' 28.59" R = 11,280.00 L = 643.72 E = 4.59 T = 321.95 P.C. STA = 1325+22.95 N = 1,853,899.63 E = 1,177,372.14 P.T. STA = 1331+66.67 N = 1,854,542.40 E = 1,177,338.64	P.I. STA = 1397+75.78 N = 1,861,149.16 E = 1,177,163.97 Δ = 44° 32' 58.56" (LT) D = 2° 47' 41.70" R = 2,050.00 L = 1,593.95 E = 165.31 T = 839.71 P.C. STA = 1389+36.07 N = 1,860,309.83 E = 1,177,189.29 P.T. STA = 1405+30.02 N = 1,861,729.54 E = 1,176,557.11	P.I. STA = 2368+84.48 N = 1,858,242.53 E = 1,177,316.62	P.I. STA = 1368+79.12 N = 1,858,253.81 E = 1,177,251.32

MATCHLINE STA. 1342+00 SB DAN RYAN



TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 ALIGNMENT
 SHEET 3 OF 4
 SCALE: 1"=200'
 DATE: MARCH 1, 2006
 DRAWN BY: JPM
 CHECKED BY: DAK

SEE ALIGNMENT SHEET 4 OF 4 FOR ADDITIONAL POINT AND CURVE DATA

SB HALSTED ST.
EXIT RAMP
(RAMP HLSTDA)
POT STA = 7440+00.00
N = 1,839,490.94
E = 1,174,333.70
P.I. STA = 7449+69.78
N = 1,839,551.73
E = 1,173,365.83
POT STA = 7452+53.49
N = 1,839,544.76
E = 1,173,082.20

Prop. Curve 79D83A-1
P.I. STA = 7160+44.94
N = 1,852,394.55
E = 1,177,340.32
 $\Delta = 9^\circ 19' 12.90''$ (RT)
D = 5° 43' 46.48"
R = 1,000.00
L = 162.67
E = 3.32
T = 81.51
P.C. STA = 7159+63.43
N = 1,852,474.58
E = 1,177,324.82
P.T. STA = 7161+26.10
N = 1,852,313.07
E = 1,177,342.65

Prop. Curve 79D83A-2
P.I. STA = 7178+01.36
N = 1,850,638.94
E = 1,177,390.59
 $\Delta = 3^\circ 24' 57.69''$ (RT)
D = 5° 43' 46.48"
R = 1,000.00
L = 59.62
E = 0.44
T = 29.82
P.C. STA = 7177+71.54
N = 1,850,668.74
E = 1,177,389.73
P.T. STA = 7178+31.17
N = 1,850,609.13
E = 1,177,389.66

SB C-D RAMP
(RAMP 79D83A)
POT STA = 7157+39.92
N = 1,852,696.86
E = 1,177,308.11
P.I. STA = 7158+99.60
N = 1,852,537.24
E = 1,177,312.68
P.I. STA = 7164+45.78
N = 1,851,993.52
E = 1,177,351.80
P.I. STA = 7168+96.23
N = 1,851,543.85
E = 1,177,378.34
P.I. STA = 7171+49.17
N = 1,851,291.02
E = 1,177,385.58
P.I. STA = 7175+38.76
N = 1,850,901.43
E = 1,177,383.07
P.I. STA = 7180+50.02
N = 1,850,390.38
E = 1,177,382.88
POT STA = 7184+63.76
N = 1,849,976.81
E = 1,177,394.72

SB WENTWORTH AVE.
EXIT RAMP
(RAMP 99A)
POT STA = 7430+55.42
N = 1,840,141.17
E = 1,176,961.02
POT STA = 7431+32.26
N = 1,840,094.48
E = 1,176,899.99
POT STA = 7434+00.00
N = 1,839,955.89
E = 1,176,670.92

Prop. Curve 99A-1
P.I. STA = 7421+32.05
N = 1,840,832.05
E = 1,177,544.75
 $\Delta = 10^\circ 49' 08.53''$ (RT)
D = 2° 17' 30.59"
R = 2,500.00
L = 472.07
E = 11.18
T = 236.74
P.C. STA = 7418+95.31
N = 1,841,058.12
E = 1,177,615.03
P.T. STA = 7423+67.38
N = 1,840,623.20
E = 1,177,433.29

Prop. Curve 99A-2
P.I. STA = 7424+78.02
N = 1,840,525.59
E = 1,177,381.20
 $\Delta = 7^\circ 26' 49.68''$ (RT)
D = 3° 22' 13.22"
R = 1,700.00
L = 220.96
E = 3.60
T = 110.64
P.C. STA = 7423+67.38
N = 1,840,623.20
E = 1,177,433.29
P.T. STA = 7425+88.34
N = 1,840,435.56
E = 1,177,316.90

Prop. Curve 99A-3
P.I. STA = 7428+26.42
N = 1,840,241.82
E = 1,177,178.53
 $\Delta = 29^\circ 38' 02.27''$ (RT)
D = 6° 21' 58.31"
R = 900.00
L = 465.49
E = 30.96
T = 238.08
P.C. STA = 7425+88.34
N = 1,840,435.56
E = 1,177,316.90
P.T. STA = 7430+53.83
N = 1,840,141.84
E = 1,176,962.46

SB 76th/79th ST.
CD RAMP
(RAMP 76D79A)
POT STA = 7500+00.00
N = 1,854,771.42
E = 1,177,153.21
P.I. STA = 7502+53.66
N = 1,854,517.84
E = 1,177,159.17
P.I. STA = 7510+89.85
N = 1,853,689.73
E = 1,177,275.17
POT STA = 7522+05.42
N = 1,852,575.71
E = 1,177,319.03

Prop. Curve 76D79A-1
P.I. STA = 7514+90.72
N = 1,853,290.06
E = 1,177,306.25
 $\Delta = 2^\circ 48' 22.72''$ (RT)
D = 2° 18' 23.74"
R = 2,484.00
L = 121.66
E = 0.75
T = 60.84
P.C. STA = 7514+29.88
N = 1,853,350.72
E = 1,177,301.54
P.T. STA = 7515+51.54
N = 1,853,229.24
E = 1,177,307.99

Prop. Curve 76D79A-2
P.I. STA = 7518+45.22
N = 1,852,935.69
E = 1,177,316.40
 $\Delta = 6^\circ 31' 33.29''$ (RT)
D = 4° 50' 21.01"
R = 1,184.00
L = 134.86
E = 1.92
T = 67.50
P.C. STA = 7517+77.72
N = 1,853,003.16
E = 1,177,314.47
P.T. STA = 7519+12.57
N = 1,852,868.44
E = 1,177,310.65

SB 76th/79th ST.
CD EXIT CONNECTOR
(CD7679A)
POT STA = 9450+00.00
N = 1,854,751.70
E = 1,177,267.37
Prop. Curve CD7679A-1
P.I. STA = 9455+73.58
N = 1,854,179.28
E = 1,177,230.83
 $\Delta = 11^\circ 37' 34.33''$ (LT)
D = 2° 12' 13.26"
R = 2,600.00
L = 527.58
E = 13.44
T = 264.70
P.C. STA = 9453+08.88
N = 1,854,443.44
E = 1,177,247.69
P.T. STA = 9458+36.46
N = 1,853,917.14
E = 1,177,267.55

SB 76th/79th ST.
CD ENTRANCE CONNECTOR
(CD7679B)
POT STA = 9460+00.00
N = 1,853,693.06
E = 1,177,298.94
Prop. Curve CD7679B-1
P.I. STA = 9463+19.58
N = 1,853,376.57
E = 1,177,343.27
 $\Delta = 1^\circ 13' 13.54''$ (RT)
D = 0° 30' 18.91"
R = 11,340.00
L = 241.55
E = 0.64
T = 120.78
P.C. STA = 9461+98.80
N = 1,853,496.18
E = 1,177,326.52
P.T. STA = 9464+40.35
N = 1,853,256.63
E = 1,177,357.48

SB 95th ST. EXIT RAMP
(RAMP 95A)
POT STA = 7400+00.00
N = 1,844,139.73
E = 1,177,638.63
P.I. STA = 7413+83.54
N = 1,842,756.77
E = 1,177,598.60
POT STA = 7417+45.33
N = 1,842,395.13
E = 1,177,608.95

Prop. Curve D8700
P.I. STA = 7204+62.99
N = 1,846,437.54
E = 1,177,524.69
 $\Delta = 1^\circ 30' 03.72''$ (LT)
D = 0° 59' 47.21"
R = 5,750.00
L = 150.64
E = 0.49
T = 75.32
P.C. STA = 7203+87.66
N = 1,846,512.58
E = 1,177,518.09
P.T. STA = 7205+38.30
N = 1,846,362.71
E = 1,177,533.26

Prop. Curve D8701
P.I. STA = 7207+24.93
N = 1,846,177.29
E = 1,177,554.49
 $\Delta = 3^\circ 27' 36.43''$ (RT)
D = 2° 25' 25.25"
R = 2,364.00
L = 142.76
E = 1.08
T = 71.40
P.C. STA = 7206+53.52
N = 1,846,248.23
E = 1,177,546.37
P.T. STA = 7207+96.29
N = 1,846,105.99
E = 1,177,558.32

SB 71st/75th ST.
CD RAMP
(RAMP 71D75A)
POT STA = 7117+70.85
N = 1,857,886.40
E = 1,177,086.26
P.I. STA = 7120+00.00
N = 1,857,657.31
E = 1,177,091.65
P.I. STA = 7128+12.41
N = 1,856,850.29
E = 1,177,177.64
P.I. STA = 7128+74.10
N = 1,856,788.62
E = 1,177,179.10
P.I. STA = 7136+84.40
N = 1,855,981.27
E = 1,177,124.75
POT = 7140+00.21
N = 1,855,665.56
E = 1,177,132.18

Prop. Curve 71D75A-1
P.I. STA = 7124+10.68
N = 1,857,251.52
E = 1,177,154.81
 $\Delta = 5^\circ 35' 23.99''$ (RT)
D = 3° 05' 49.45"
R = 1,850.00
L = 180.49
E = 2.20
T = 90.32
P.C. STA = 7123+20.36
N = 1,857,340.76
E = 1,177,140.92
P.T. STA = 7125+00.86
N = 1,857,161.34
E = 1,177,159.94

Prop. Curve 71D75A-2
P.I. STA = 7132+74.89
N = 1,856,387.85
E = 1,177,175.17
 $\Delta = 6^\circ 30' 28.18''$ (RT)
D = 3° 51' 39.25"
R = 1,484.00
L = 168.56
E = 2.40
T = 84.37
P.C. STA = 7131+90.52
N = 1,856,472.22
E = 1,177,176.00
P.T. STA = 7133+59.08
N = 1,856,304.13
E = 1,177,164.79

SB 71st/75th ST.
CD EXIT CONNECTOR
(RAMP CD7175A)
POT STA = 9423+50.00
N = 1,857,297.2866
E = 1,177,207.4713
POT STA = 9426+50.01
N = 1,856,997.4225
E = 1,177,198.1891
CD ENTRANCE CONNECTOR
(RAMP CD7175B)
POT STA = 9429+50.00
N = 1,856,694.2083
E = 1,177,205.3235
POT STA = 9434+40.27
N = 1,856,204.7281
E = 1,177,233.1786

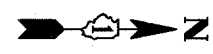
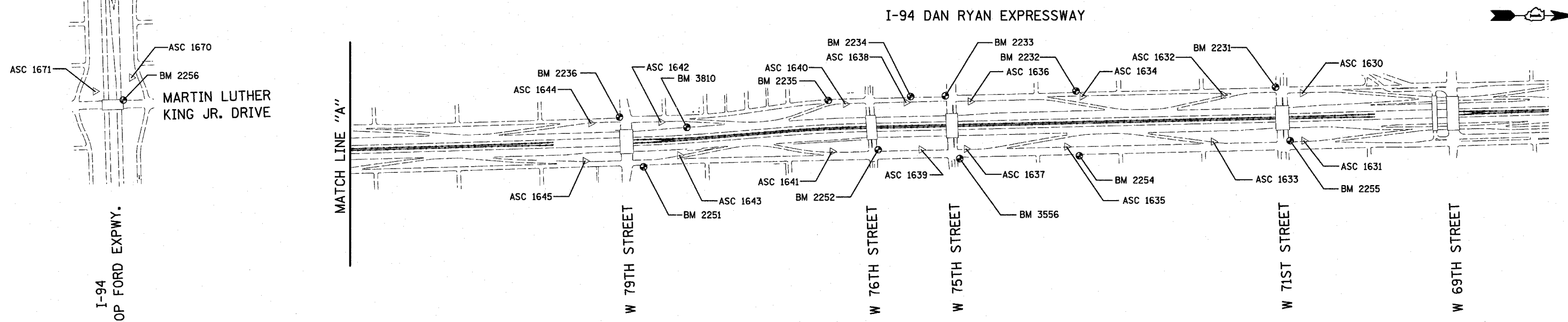
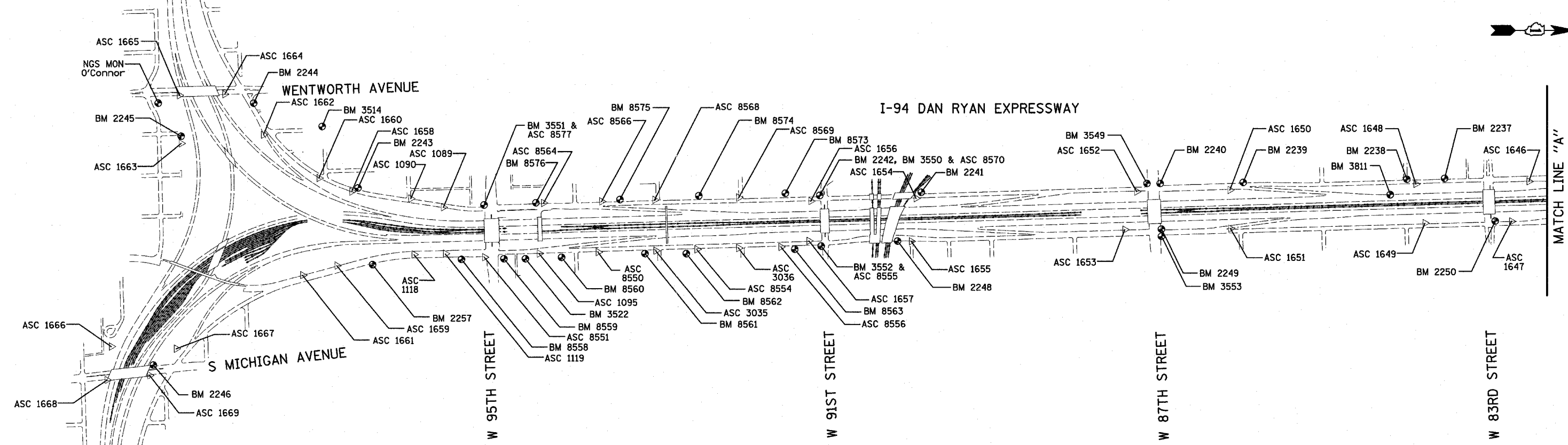
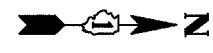
SB 87th ST.
ENTRANCE RAMP
(RAMP 87D)
POT STA = 7194+74.45
N = 1,847,424.06
E = 1,177,468.58
P.I. STA = 7200+00.00
N = 1,846,898.74
E = 1,177,484.09
POT STA = 7217+56.49
N = 1,845,147.17
E = 1,177,609.78

SB 87th ST. EXIT RAMP
(RAMP 87A)
POT STA = 7180+00.00
N = 1,848,946.30
E = 1,177,501.00
P.I. STA = 7189+97.67
N = 1,847,950.35
E = 1,177,442.53
POT STA = 7192+45.83
N = 1,847,702.29
E = 1,177,449.63

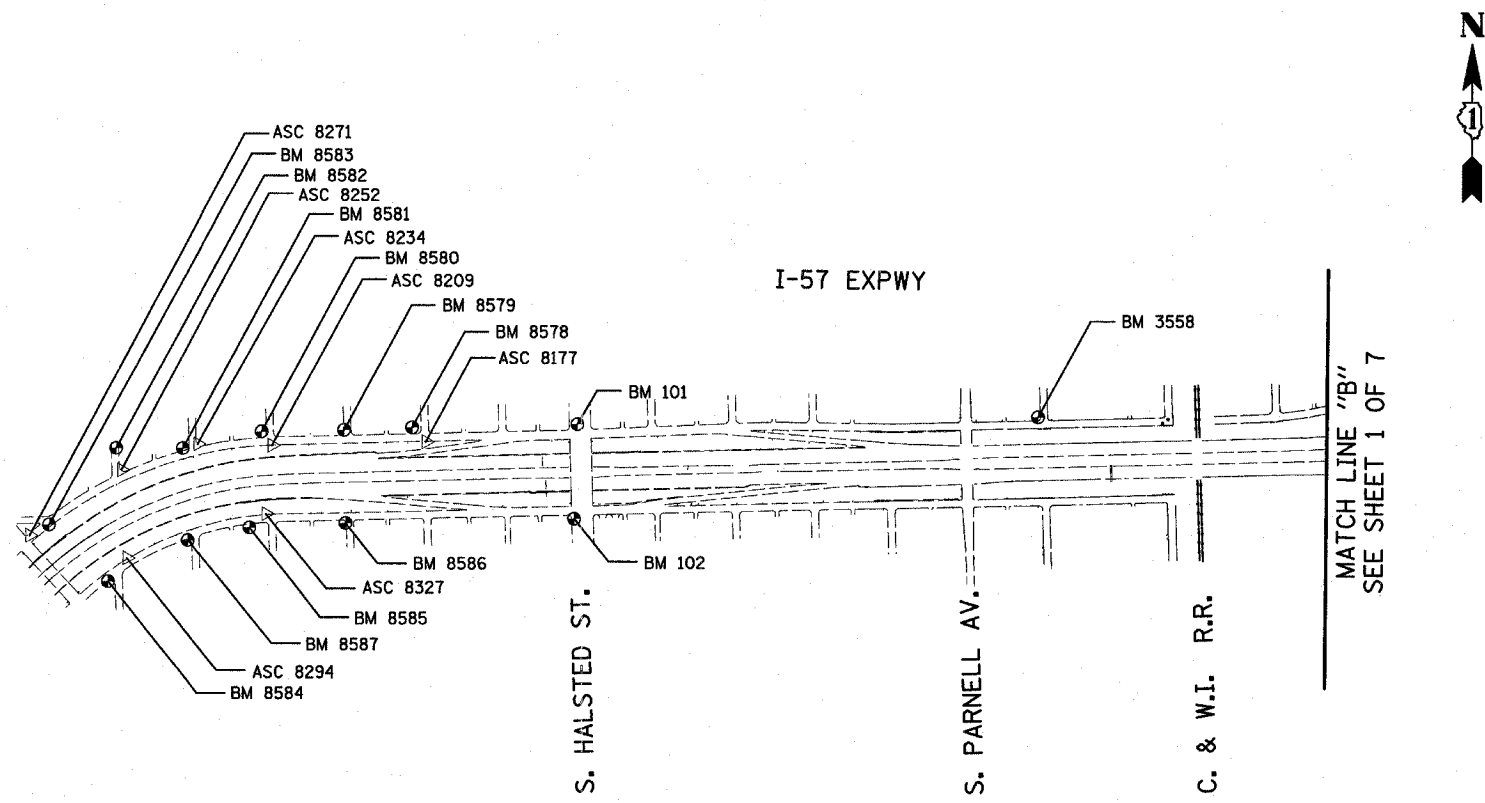
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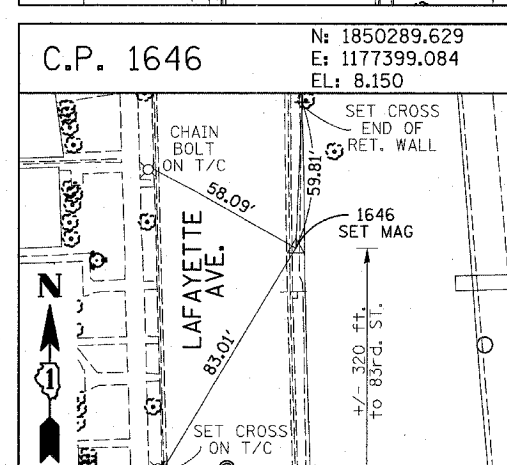
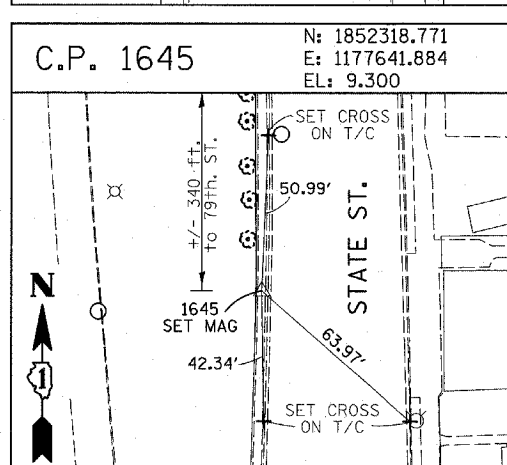
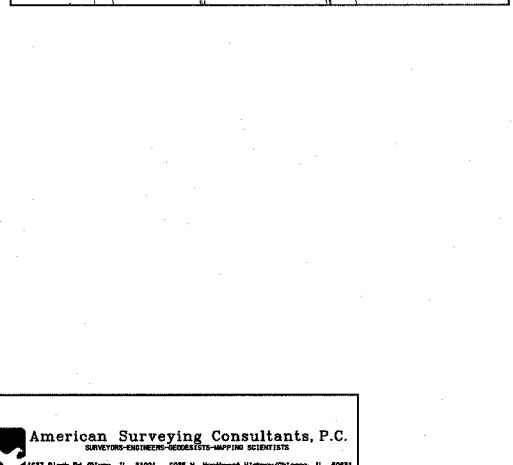
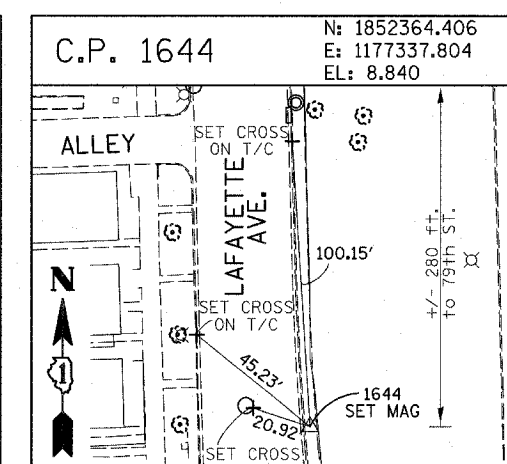
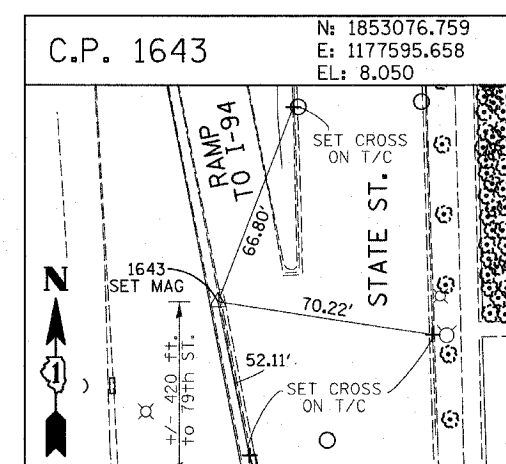
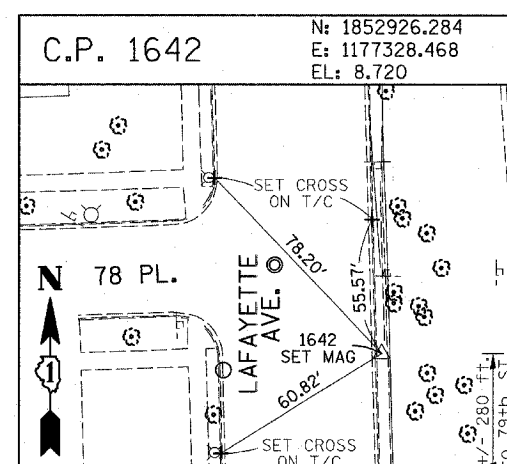
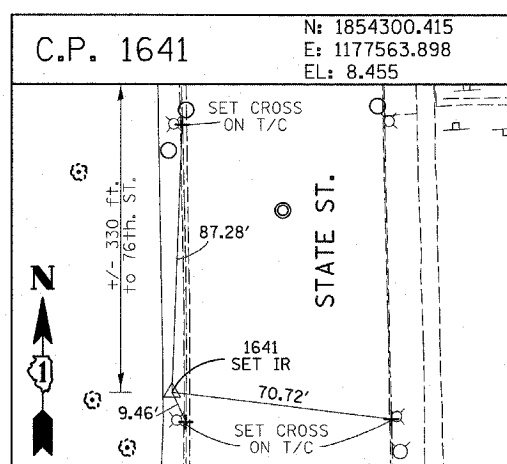
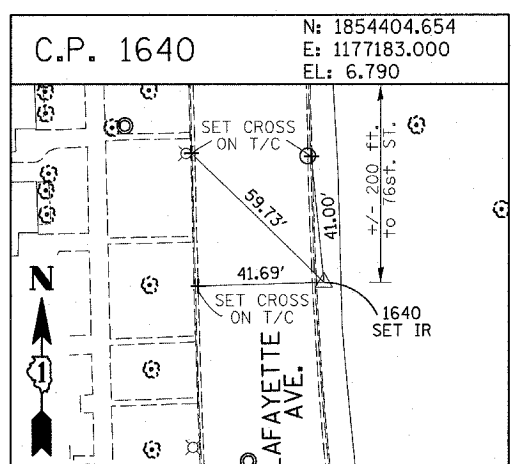
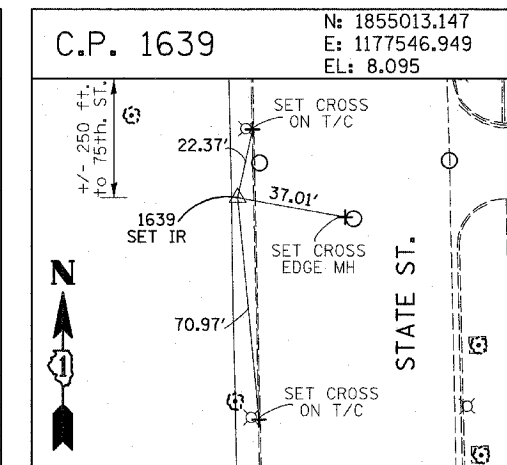
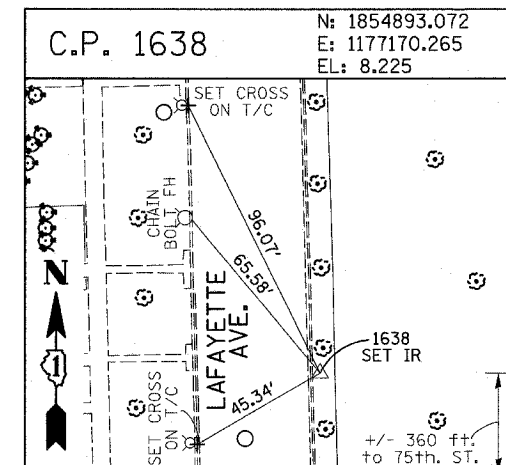
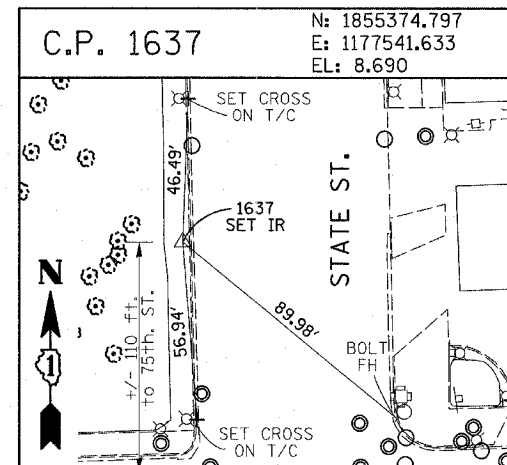
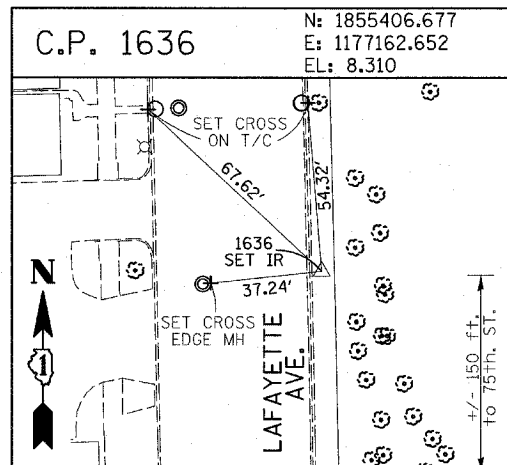
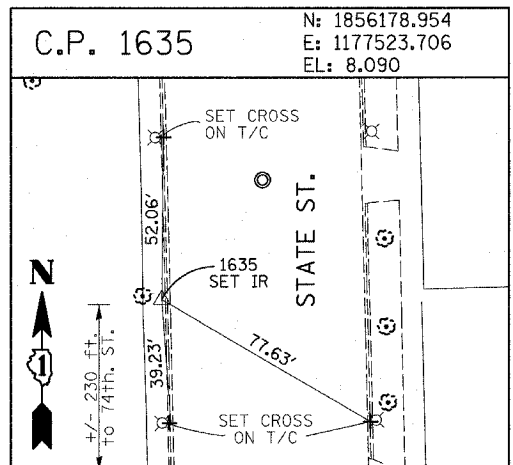
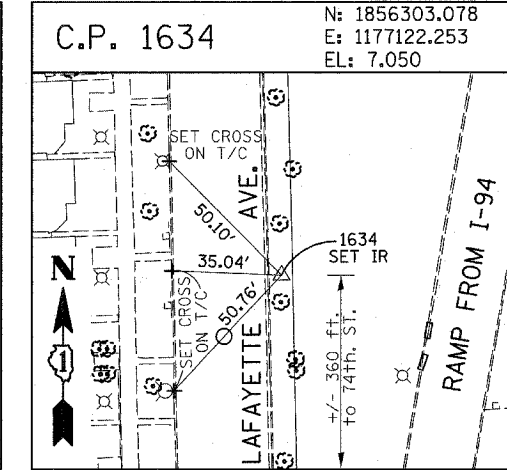
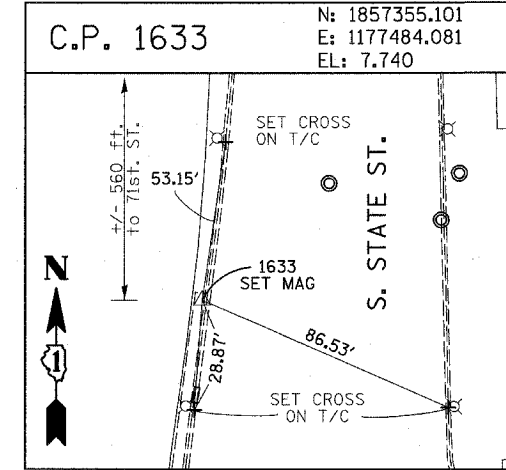
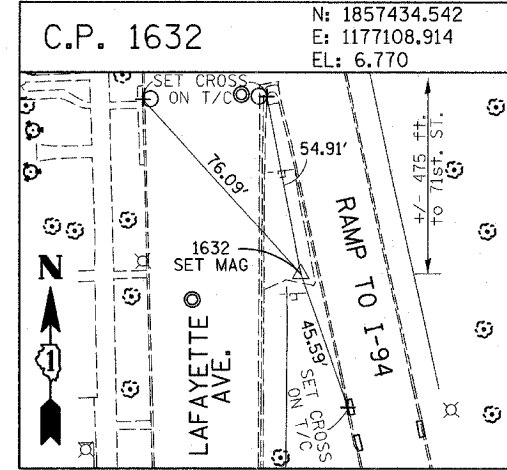
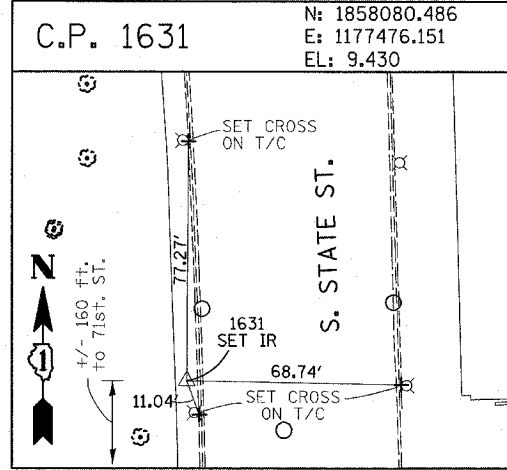
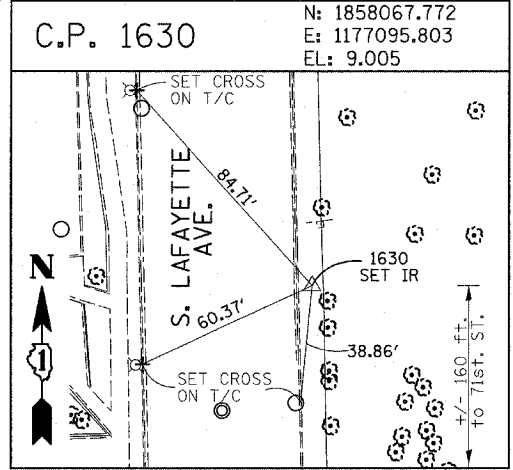
I-57 EXPWY
SEE SHEET 2 OF 7
MATCH LINE "B"



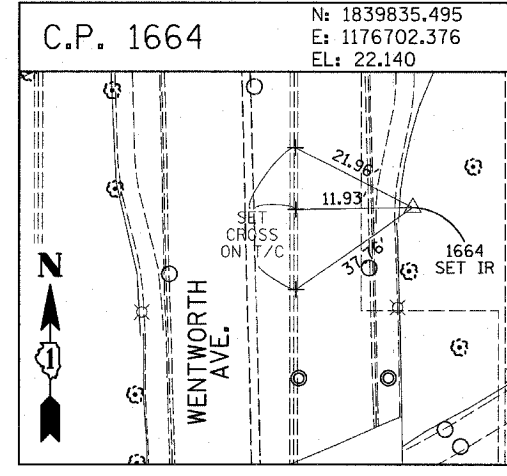
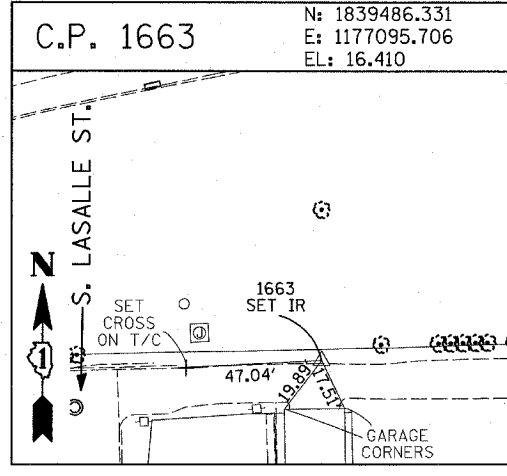
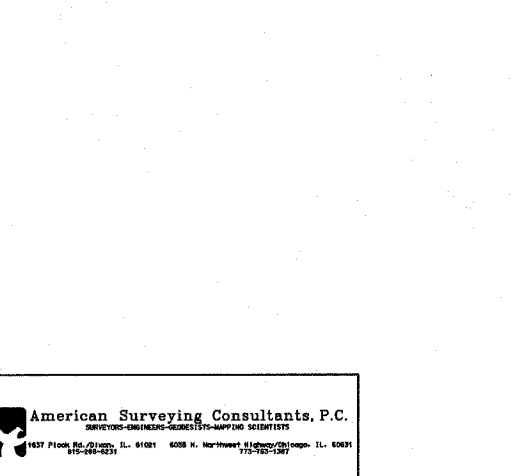
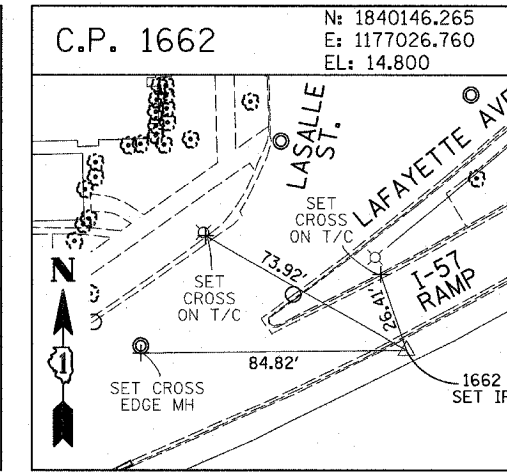
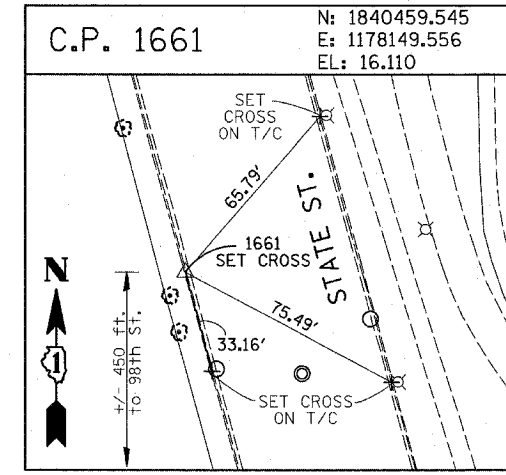
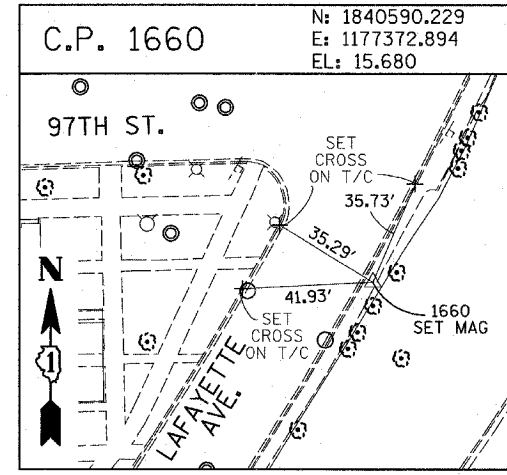
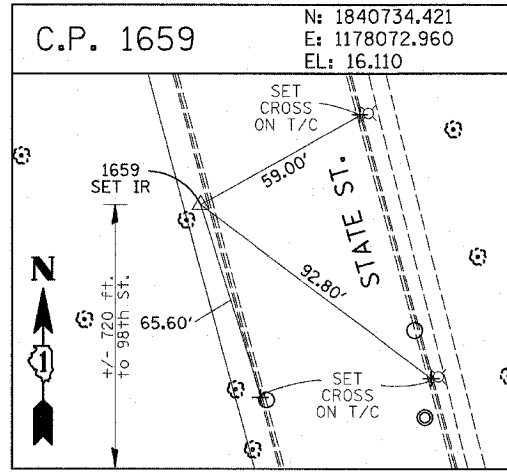
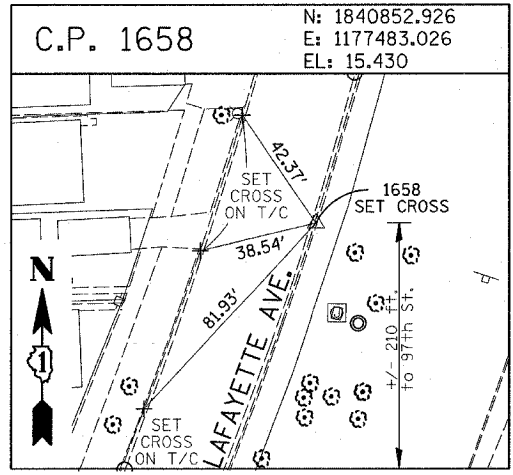
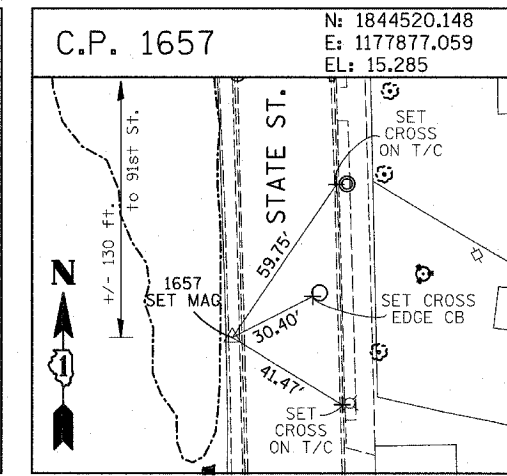
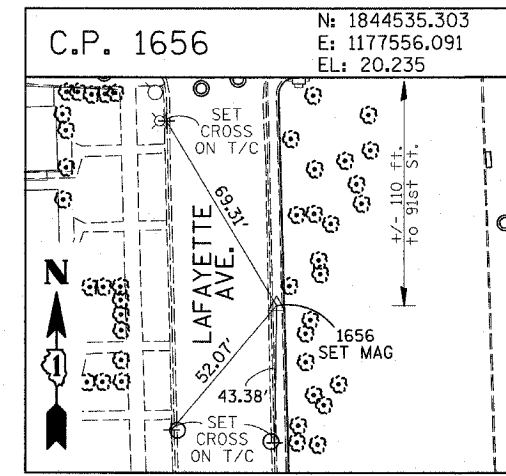
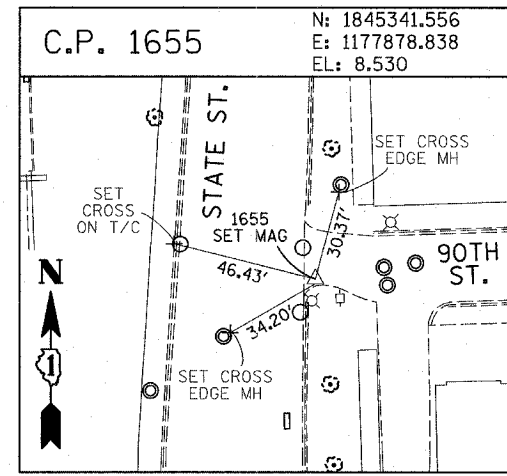
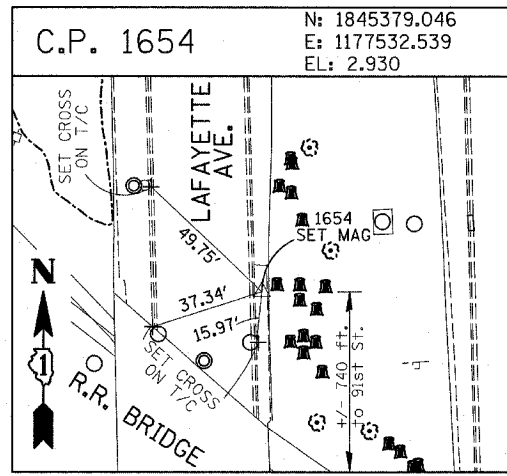
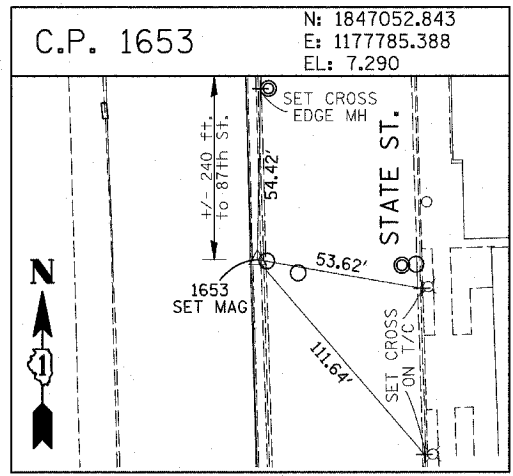
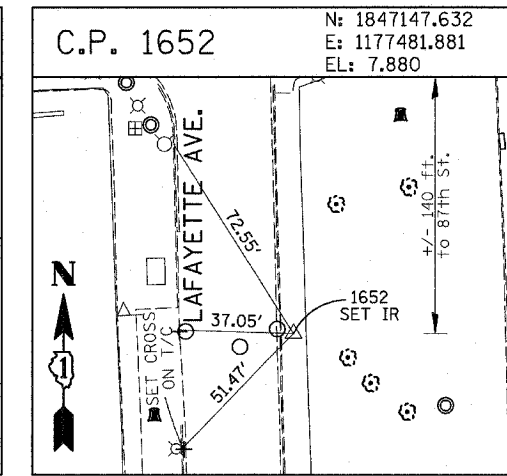
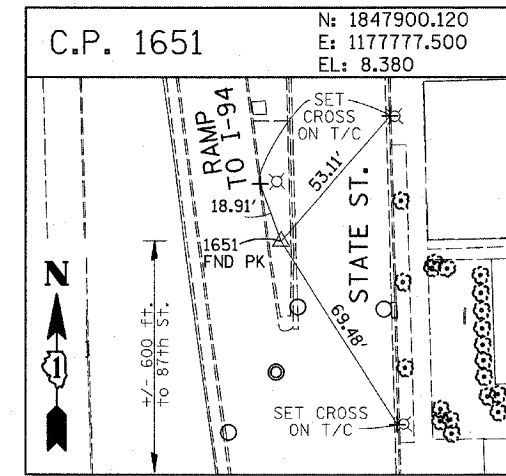
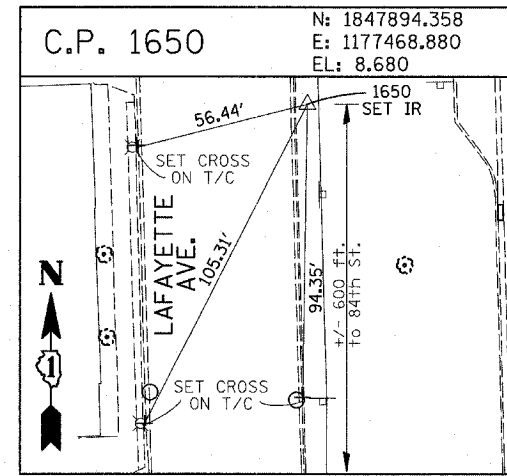
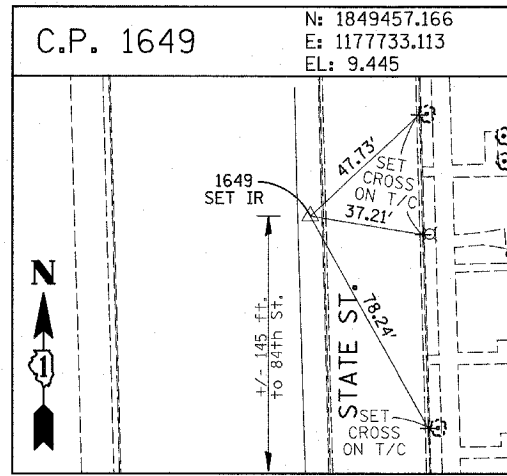
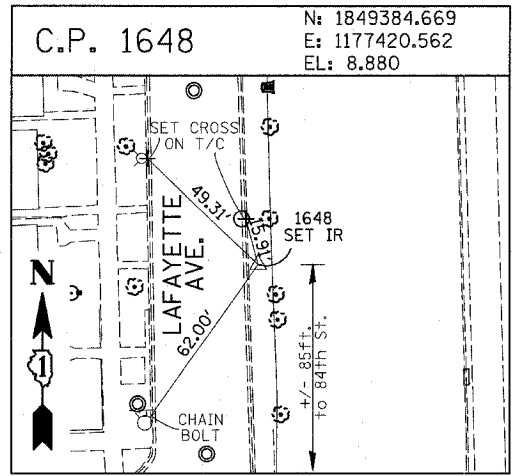
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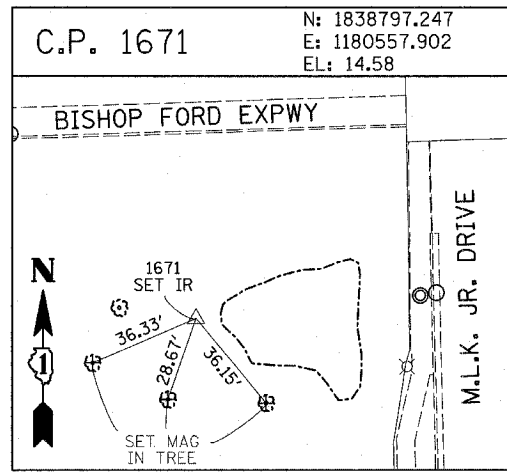
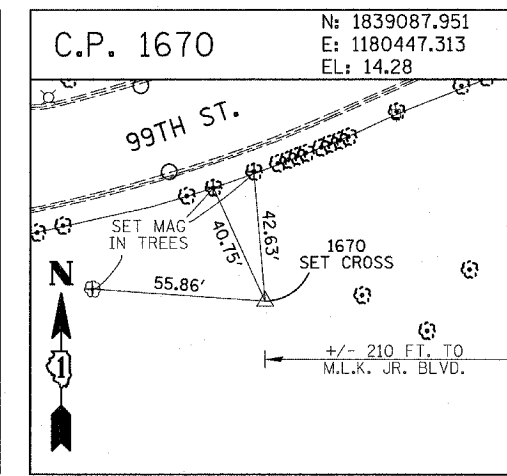
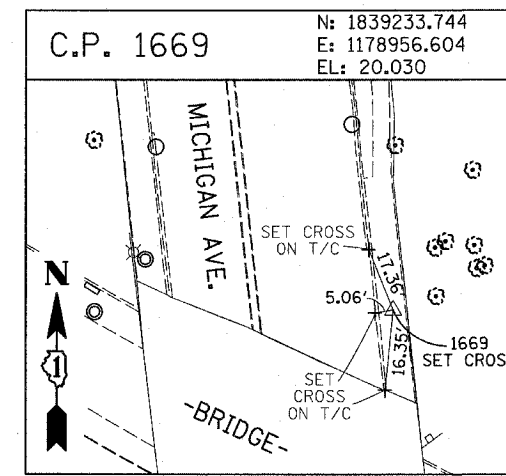
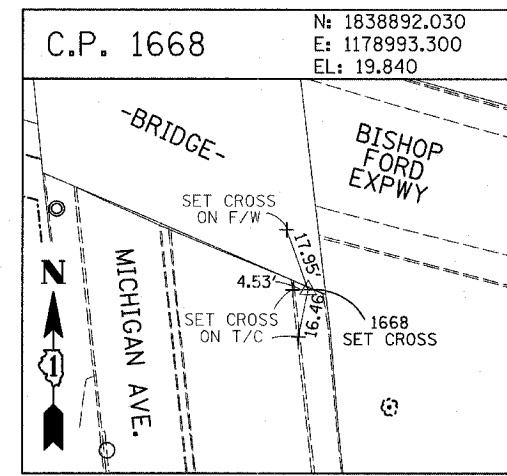
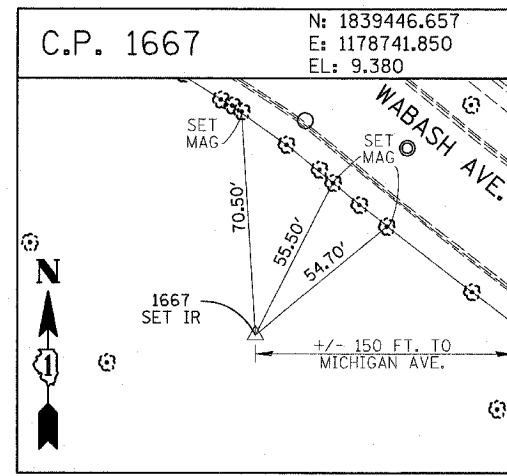
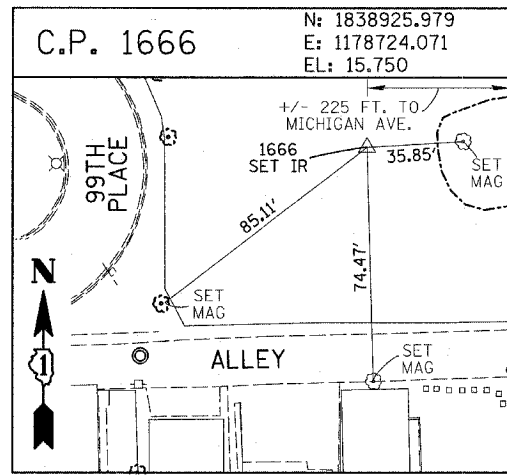


American Surveying Consultants, P.C.
1157 Flood Rd., Chicago, IL 60611

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
SURVEY TIES FOR CONTROL POINTS
SHEET 4 OF 7
SCALE: 1"=30'
DATE: MARCH 1, 2006
DRAWN BY: GSP
CHECKED BY: MMW

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	61
STA. 1126+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				
				62593

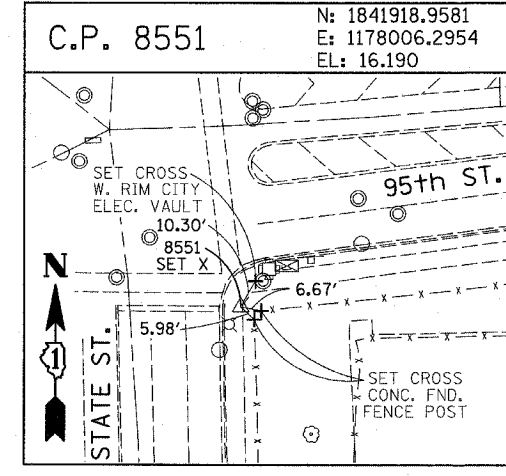
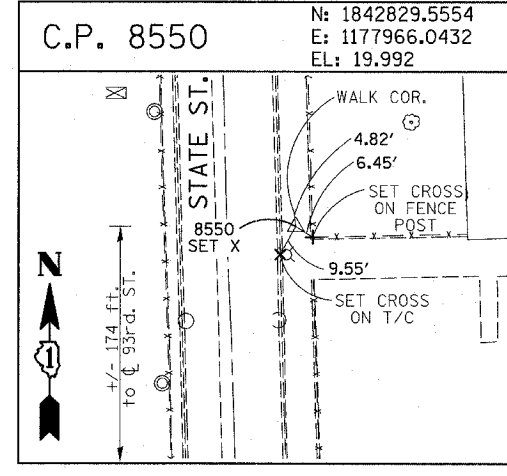
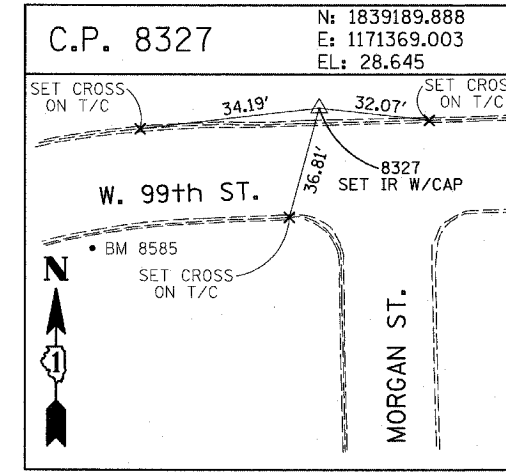
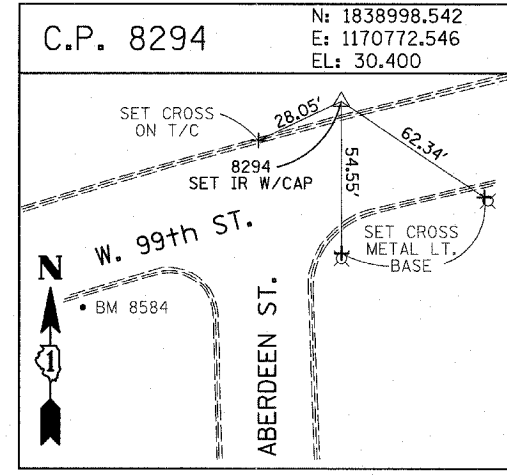
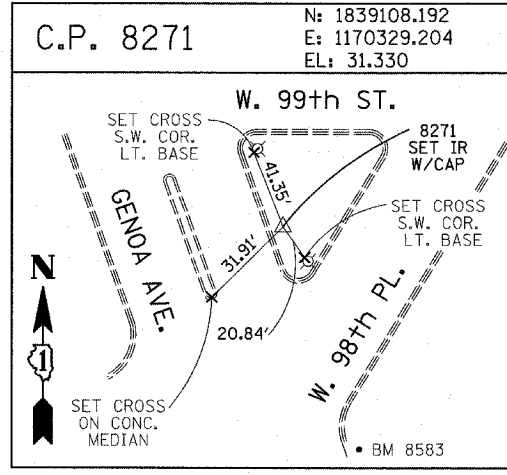
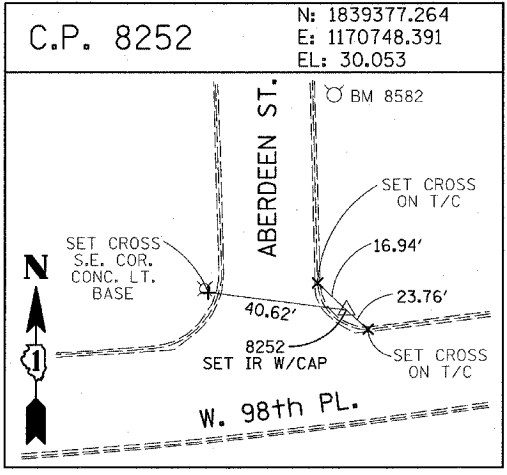
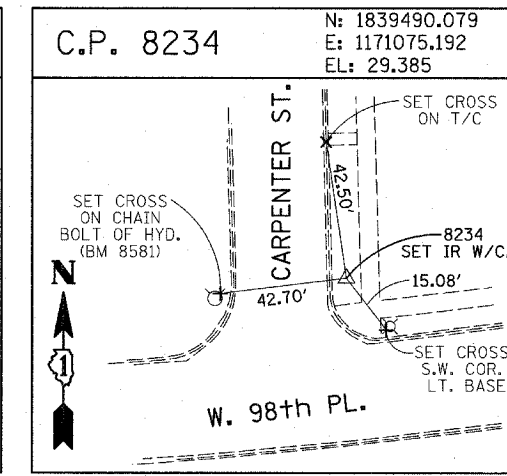
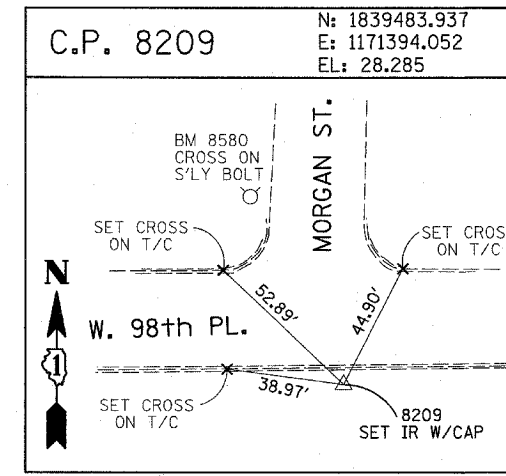
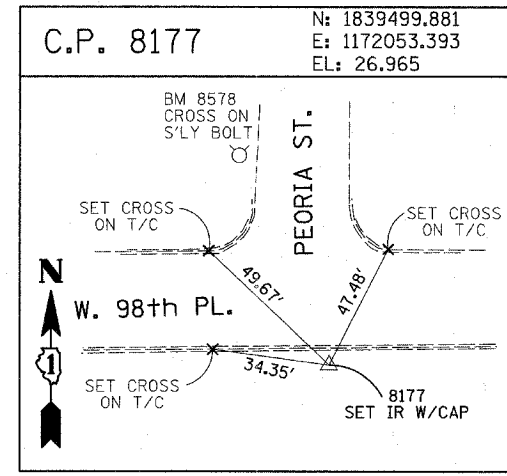
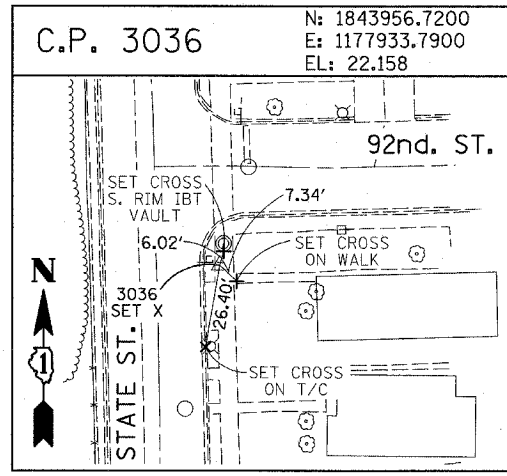
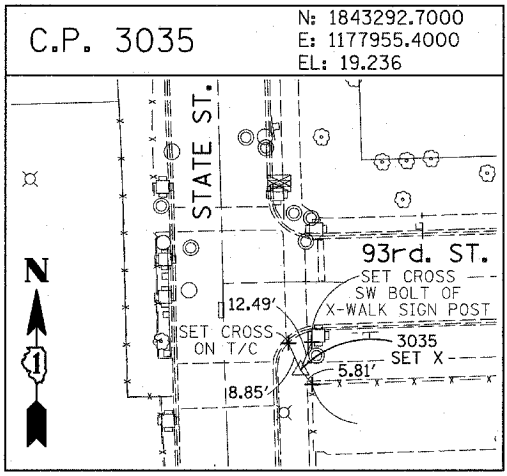
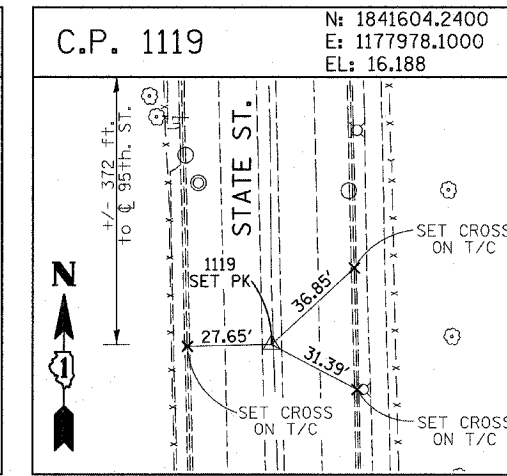
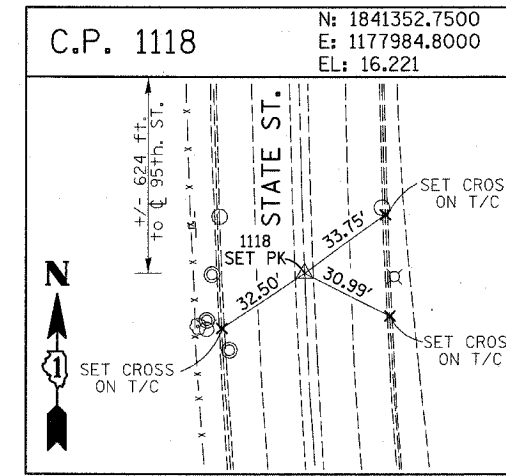
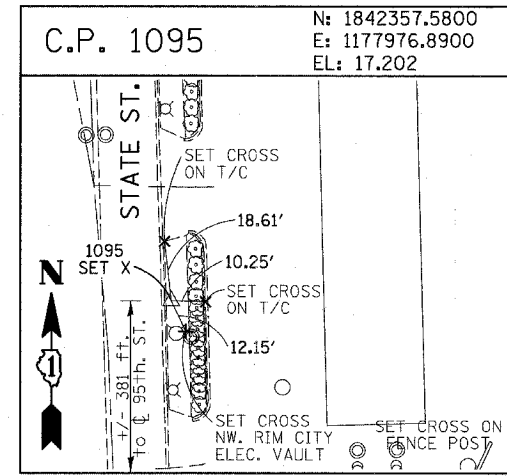
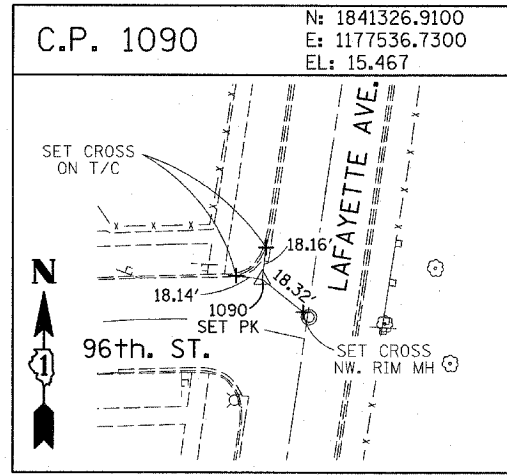
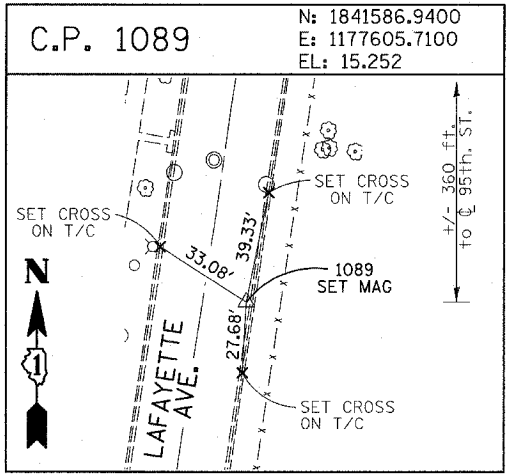


PROJECT BENCHMARKS

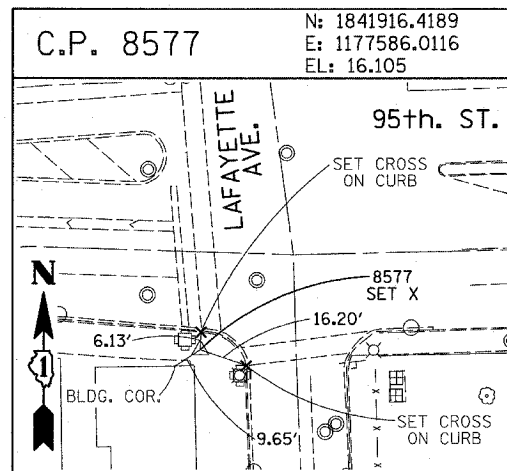
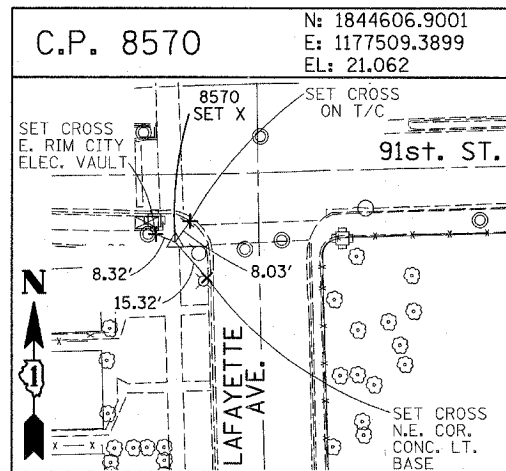
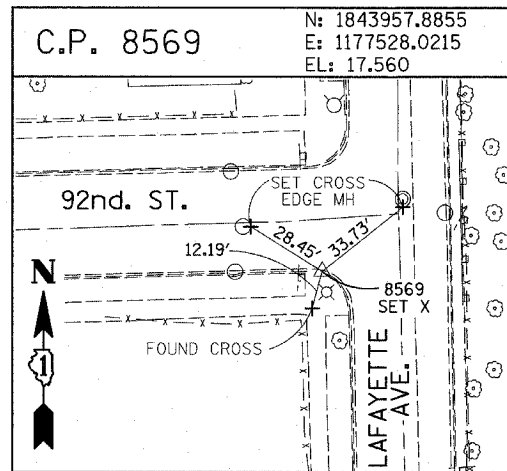
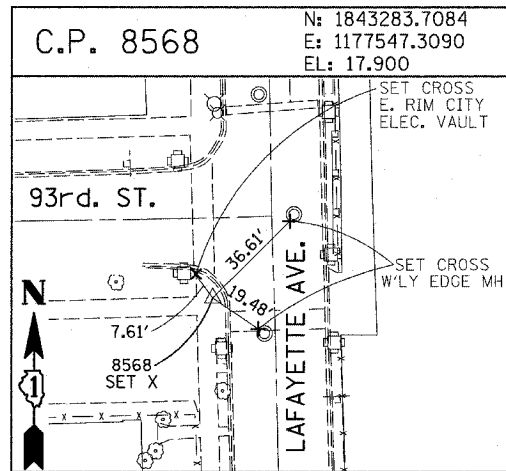
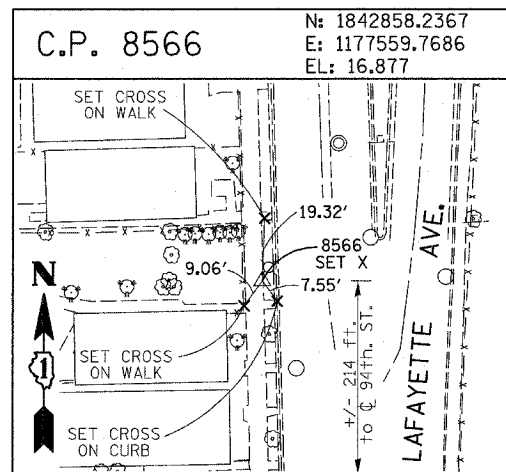
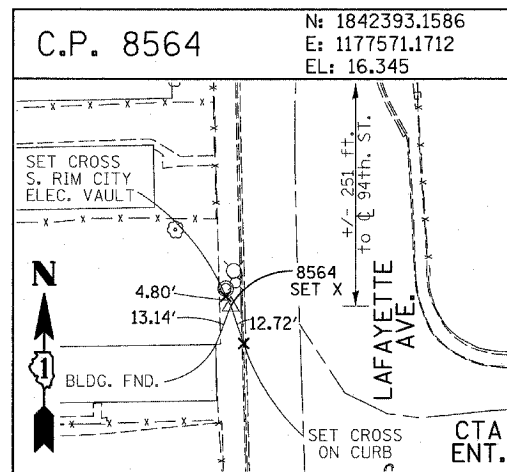
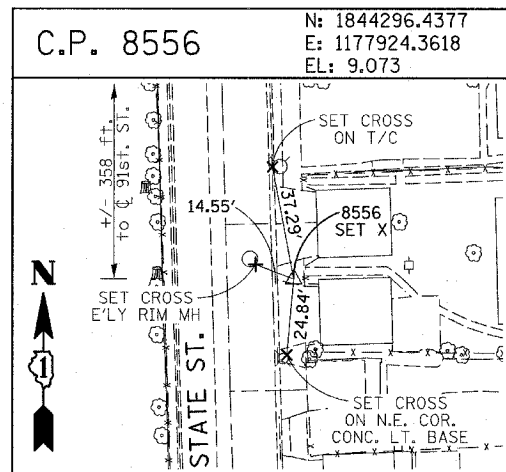
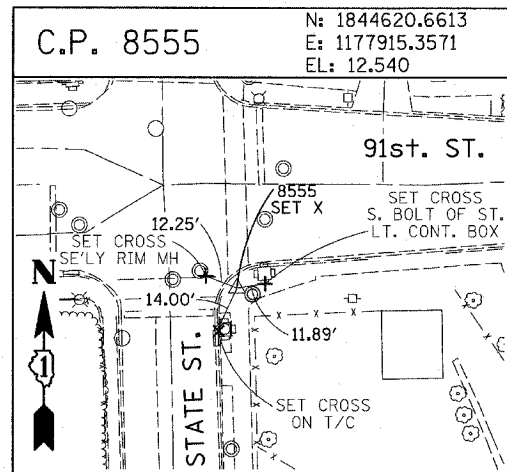
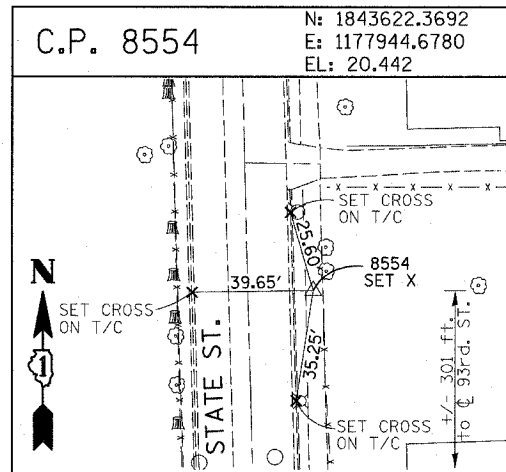
- BM 2231 ELEVATION= 10.24 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.
- BM 2232 ELEVATION= 8.46 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2233 ELEVATION= 9.90 FEET
SET CROSS ON NORTHWEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 75TH STREET AND LAFAYETTE AVENUE.
- BM 2234 ELEVATION= 9.56 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 75TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 320 FEET SOUTH OF 75TH STREET.
- BM 2235 ELEVATION= 7.50 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 76TH STREET ON THE WEST SIDE OF LAFAYETTE. APPROXIMATELY 330 FEET SOUTH OF 76TH STREET.
- BM 2236 ELEVATION= 10.41 FEET
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 79TH STREET AND LAFAYETTE AVENUE.
- BM 2237 ELEVATION= 9.20 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF 83RD STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 350 FEET NORTH OF 83RD STREET.
- BM 2238 ELEVATION= 10.04 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT OF THE NORTHWEST CORNER OF 4TH STREET AND LAFAYETTE AVENUE.
- BM 2239 ELEVATION= 9.59 FEET
SET CROSS ON CHAIN BOLT OF SECOND FIRE HYDRANT NORTH OF 87TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 720 FEET NORTH OF 87TH STREET.
- BM 2240 ELEVATION= 9.49 FEET
SET CROSS ON THE NORTHWEST BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 78TH STREET AND LAFAYETTE AVENUE.
- BM 2241 ELEVATION= 3.63 FEET
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTHWEST CORNER OF LAFAYETTE AVENUE AND RAILROAD BRIDGE. APPROXIMATELY 770 FEET NORTH OF 91ST STREET.
- BM 2242 ELEVATION= 22.82 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE SOUTHWEST CORNER OF 91ST STREET AND LAFAYETTE AVENUE.
- BM 2243 ELEVATION= 15.68 FEET
SET SQUARE CUT ON THE TOP OF CURB NEXT TO THIRD LIGHT POLE NORTH OF 97TH STREET ON THE WEST SIDE OF LAFAYETTE AVENUE. APPROXIMATELY 260 FEET NORTH OF 97TH STREET.
- BM 2244 ELEVATION= 15.94 FEET
SET SQUARE CUT ON TOP OF CURB NEXT TO LIGHT POLE ON THE NORTH SIDE OF 98TH STREET. APPROXIMATELY 120 FEET EAST OF WENTWORTH AVENUE.
- BM 2245 ELEVATION= 16.35 FEET
SET SQUARE CUT ON THE EAST END OF CURB AT THE NORTHEAST CORNER OF FIRST ALLEY NORTH OF 99TH STREET. APPROXIMATELY 35 FEET EAST OF LASALLE STREET.
- BM 2246 ELEVATION= 20.00 FEET
SET SQUARE CUT WITH CROSS ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF STATE STREET BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 20 FEET NORTH OF THE NORTH EDGE OF THE BRIDGE.
- BM 2248 ELEVATION= 9.77 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF RAILROAD BRIDGE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 110 FEET SOUTH OF 90TH STREET.
- BM 2249 ELEVATION= 9.67 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 87TH AND STATE STREET.
- BM 2250 ELEVATION= 10.03 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 83RD ST. AND STATE STREET.
- BM 2251 ELEVATION= 10.95 FEET
FOUND CROSS ON EAST BOLT OF FIRE HYDRANT ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 135 FEET NORTH OF 79TH STREET.
- BM 2252 ELEVATION= 10.02 FEET
SET CROSS ON WEST BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF STATE STREET AND 76TH STREET.
- BM 2254 ELEVATION= 9.17 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT SOUTH OF 73RD STREET ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 330 FEET SOUTH OF 73RD STREET.
- BM 2255 ELEVATION= 11.25 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF 71ST STREET AND STATE STREET.
- BM 2256 ELEVATION= 16.36 FEET
SET SQUARE CUT ON TOP OF ABUTMENT WALL ON THE NORTHWEST CORNER OF MARTIN LUTHER KING DRIVE BRIDGE OVER THE BISHOP FORD EXPRESSWAY. APPROXIMATELY 1 FOOT NORTH OF THE NORTH END OF THE BRIDGE.
- BM 2257 ELEVATION= 18.25 FEET
SET CROSS ON NORTHEAST BOLT OF LIGHT POLE BASE ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 970 FEET SOUTH OF 95TH STREET.
- BM 2258 ELEVATION= 10.53 FEET
SET CROSS ON NORTHWEST BOLT OF LIGHT BASE ON NORTHWEST CORNER OF 71ST STREET AND LAFAYETTE AVENUE.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. 94 (DAN RYAN EXPRESSWAY)
NAME	DATE	
		SURVEY TIES FOR CONTROL POINTS SHEET 5 OF 7

SCALE: 1"=30'
 DATE: MARCH 1, 2006
 DRAWN BY: GSP
 CHECKED BY: MMW



REVISIONS	
NAME	DATE



PROJECT BENCHMARKS

- BM 101 ELEVATION= 24.89 FEET
SET SQUARE CUT ON SOUTHERLY CURB OF WEST 98TH PLACE. APPROXIMATELY 70 FEET EAST OF THE CENTERLINE OF GREEN STREET.
- BM 102 ELEVATION= 27.36 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF WEST 99TH STREET AND GREEN STREET.
- BM 8558 ELEVATION= 17.73 FEET
SET SQUARE CUT ON SOUTHEAST CORNER OF FOURTH LIGHT BASE SOUTH OF 95TH STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8559 ELEVATION= 17.10 FEET
SET SQUARE CUT ON SOUTHWEST CORNER OF CONCRETE SIGN BASE AT THE NORTHWEST CORNER OF THE MOBIL GAS STATION PROPERTY AT THE NORTHEAST CORNER OF 95TH STREET AND STATE STREET.
- BM 8560 ELEVATION= 17.50 FEET
SET SQUARE CUT ON SOUTHEAST CORNER OF CONCRETE BUS SLAB ON THE EAST SIDE OF STATE STREET. APPROXIMATELY 50 FEET SOUTH OF THE CENTERLINE OF 94TH STREET.
- BM 8561 ELEVATION= 19.77 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8562 ELEVATION= 22.29 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT NORTH OF 93RD STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8563 ELEVATION= 20.47 FEET
SET CROSS ON SOUTHEASTERLY FLANGE BOLT OF FIRST FIRE HYDRANT SOUTH OF 91ST STREET, ON THE EAST SIDE OF STATE STREET.
- BM 8573 ELEVATION= 19.89 FEET
SET SQUARE CUT ON EAST SIDE OF SIDEWALK OPPOSITE TO THE SECOND FIRE HYDRANT SOUTH OF 91ST STREET, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8574 ELEVATION= 18.90 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9224 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8575 ELEVATION= 17.77 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT APPROXIMATELY AT 9326 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.

- BM 8576 ELEVATION= 17.43 FEET
SET CROSS ON THE WESTERLY FLANGE BOLT OF FIRE HYDRANT AT THE VACANT LOT BETWEEN 9416 AND 9422 S. LAFAYETTE AVENUE, ON THE WEST SIDE OF LAFAYETTE AVENUE.
- BM 8578 ELEVATION= 27.92 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND PEORIA STREET.
- BM 8579 ELEVATION= 28.59 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND SANGAMON STREET.
- BM 8580 ELEVATION= 29.24 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE NORTHWEST CORNER OF WEST 98TH PLACE AND MORGAN STREET.
- BM 8581 ELEVATION= 30.82 FEET
SET CROSS ON CHAIN BOLT OF FIRE HYDRANT ON THE NORTHWEST CORNER OF CARPENTER STREET AND WEST 98TH PLACE.
- BM 8582 ELEVATION= 30.69 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT NORTH OF WEST 98TH PLACE, ON THE EAST SIDE OF ABERDEEN STREET.
- BM 8583 ELEVATION= 33.14 FEET
SET SQUARE CUT ON CONCRETE BRIDGE WALL AT THE SOUTHEASTERLY CORNER OF GENOA AVENUE AND 99TH STREET.
- BM 8584 ELEVATION= 32.36 FEET
SET CROSS ON CHAIN BOLT OF FIRST FIRE HYDRANT WEST OF ABERDEEN STREET, ON THE SOUTHERLY SIDE OF 99TH STREET.
- BM 8585 ELEVATION= 30.28 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRST FIRE HYDRANT WEST OF MORGAN STREET, ON THE SOUTH SIDE OF 99TH STREET.
- BM 8586 ELEVATION= 28.75 FEET
SET CROSS ON NORTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND SANGAMON STREET.
- BM 8587 ELEVATION= 31.19 FEET
SET CROSS ON SOUTHERLY FLANGE BOLT OF FIRE HYDRANT AT THE SOUTHWEST CORNER OF 99TH STREET AND CARPENTER STREET.

BM 3514 ELEVATION= 17.56 FEET
SET CROSS ON CHAIN BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF LASALLE ST. AND 97TH ST.

BM 3522 ELEVATION= 18.06 FEET
SET CROSS ON SOUTH SOUTHEAST FLANGE BOLT OF HYDRANT ± HALFWAY BETWEEN NORTH AND SOUTH ENTRANCE TO CITGO, EAST SIDE OF STATE ST. AND NORTH OF 95TH ST.

BM 3549 ELEVATION= 9.49 FEET
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 87TH ST. AND LAFAYETTE AVE.

BM 3550 ELEVATION= 22.88 FEET
SET CROSS WESTERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 91ST ST. AND LAFAYETTE AVE.

BM 3551 ELEVATION= 17.50 FEET
SET CROSS NORTHERLY FLANGE BOLT OF HYDRANT AT SOUTHWEST INTERSECTION OF 95TH ST. AND LAFAYETTE AVE.

BM 3552 ELEVATION= 14.81 FEET
SET CROSS NORTHEAST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT SOUTHEAST INTERSECTION OF 91ST ST. AND STATE ST.

BM 3553 ELEVATION= 8.47 FEET
SET CROSS NORTHWEST BOLT OF TRAFFIC SIGNAL/LIGHT BASE AT NORTHEAST INTERSECTION OF 87TH ST. AND STATE ST.

BM 3556 ELEVATION= 9.06 FEET
SET CROSS EASTERLY FLANGE BOLT OF HYDRANT AT NORTHEAST CORNER OF 75TH ST. AND STATE ST.

BM 3558 ELEVATION= 18.15 FEET
SET CROSS SOUTHWESTERLY FLANGE BOLT OF HYDRANT AT NORTHWEST INTERSECTION OF NORMAL ST. AND 98TH PLACE.

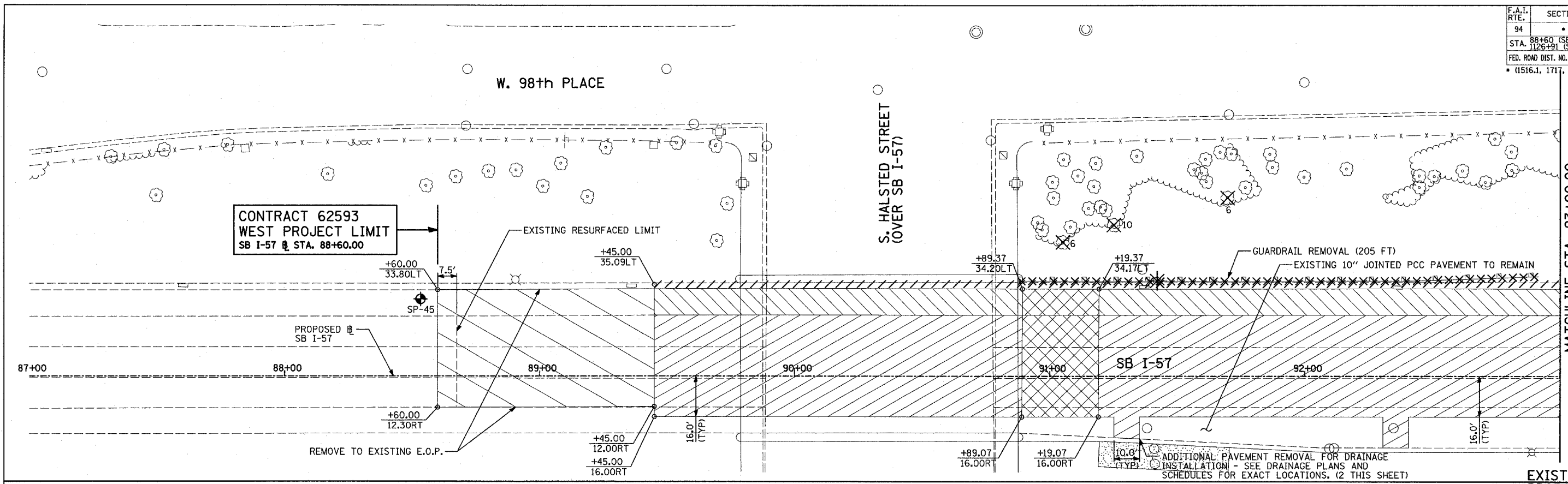
BM 3810 ELEVATION= 0.29 FEET
CUT SQUARE ON SOUTHERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN, OPPOSITE OF WEST 77TH PLACE, APPROXIMATELY 1000 FEET NORTH OF 79TH STREET BRIDGE.

BM 3811 ELEVATION= 1.95 FEET
CUT SQUARE ON SOUTHEASTERLY CORNER OF METAL BASE FOR OVERHEAD SIGN, ON THE WESTERLY SIDE OF SOUTHBOUND DAN RYAN APPROXIMATELY 840 FEET SOUTHERLY OF 83RD STREET.

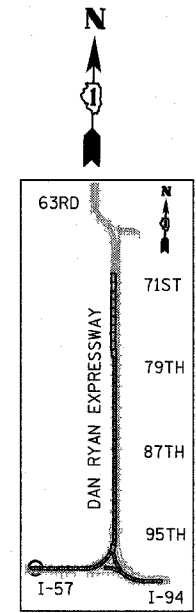
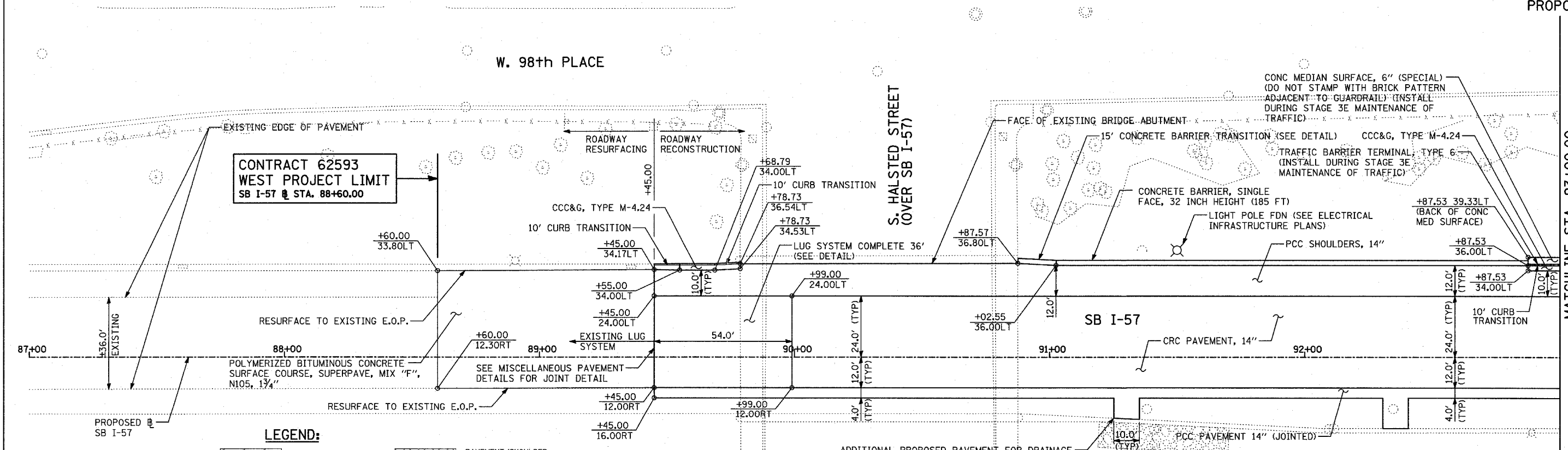
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 SURVEY TIES FOR CONTROL POINTS
 SHEET 7 OF 7

SCALE: 1"=30'
 DATE: MARCH 1, 2006
 DRAWN BY: GSP
 CHECKED BY: MMW



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS

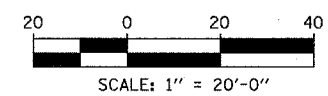


LOCATION MAP

LEGEND:

- | | | | | | |
|--|---------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | +XX.XX MAINLINE @ |
| | BITUMINOUS SURFACE REMOVAL, 4\"/> | | CHAIN LINK FENCE REMOVAL | | XX.XXRT STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2\"/> | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | PLAN NOTES: |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4\"/> | | SOIL BORING LOCATIONS | | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED |

ADDITIONAL PROPOSED PAVEMENT FOR DRAINAGE INSTALLATION - SEE DRAINAGE PLANS AND SCHEDULES FOR EXACT LOCATIONS. (2 THIS SHEET)



REVISIONS	
NAME	DATE

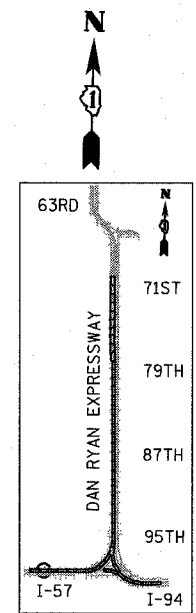
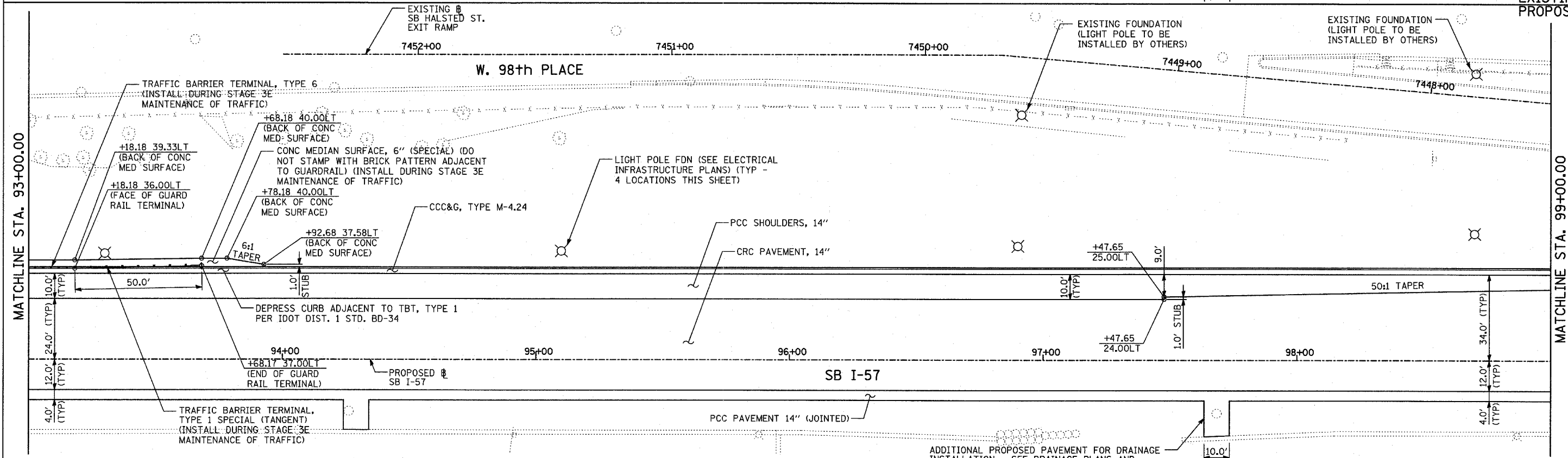
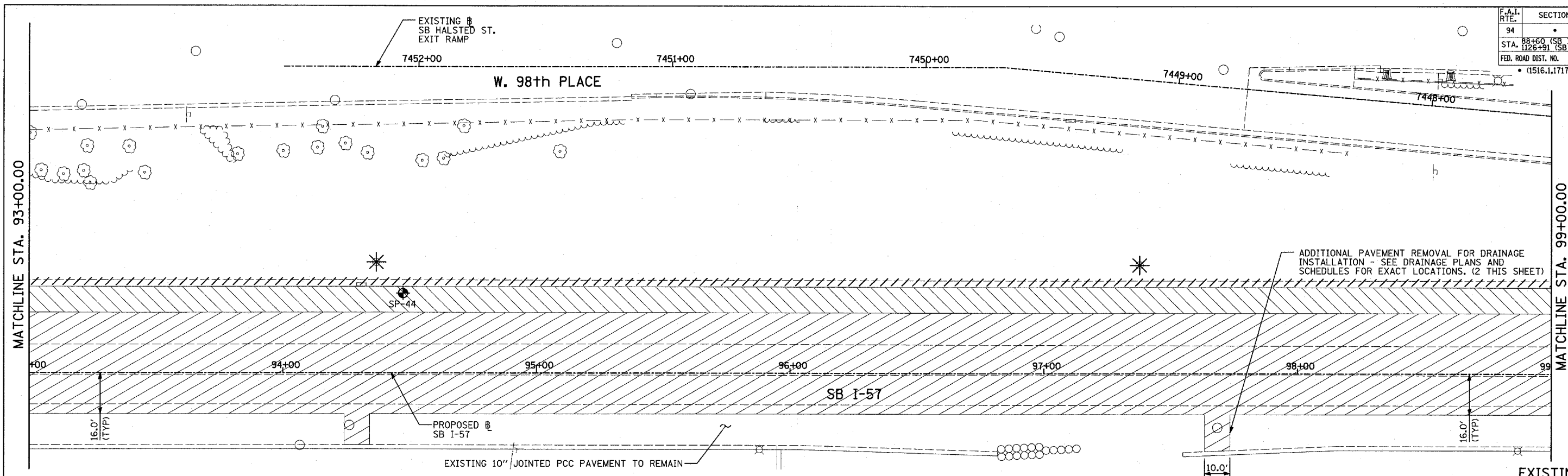
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-57
SB I-57 STA. 88+60.00 TO 93+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	65
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717 ETC, 1919.15 A) R-5 62593				

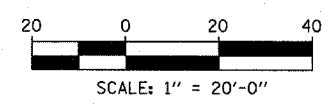


LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



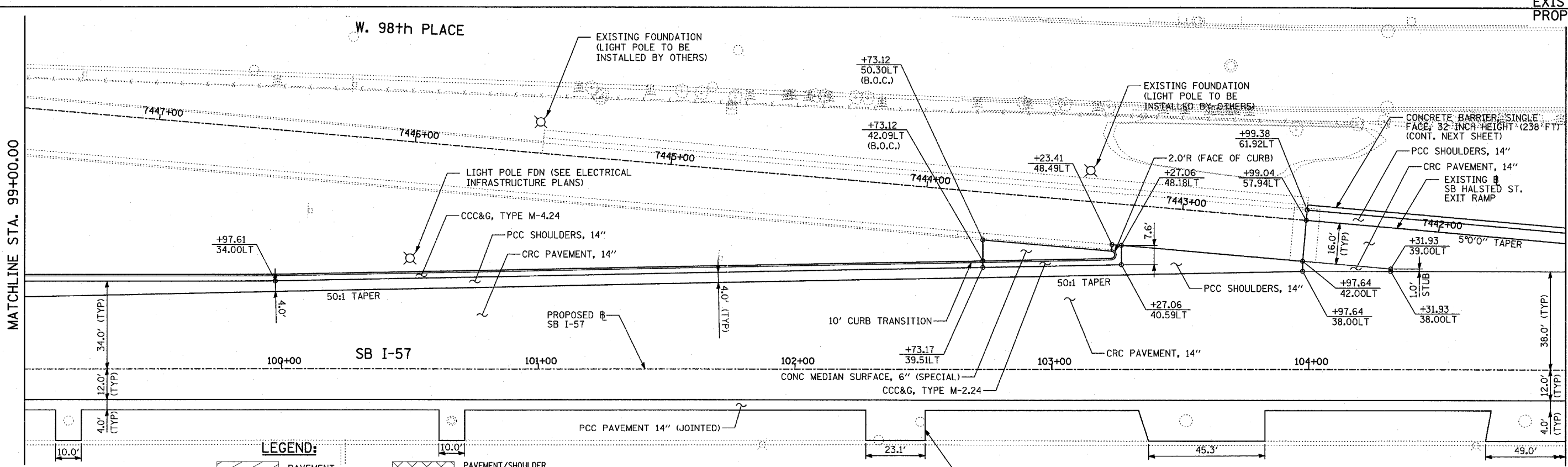
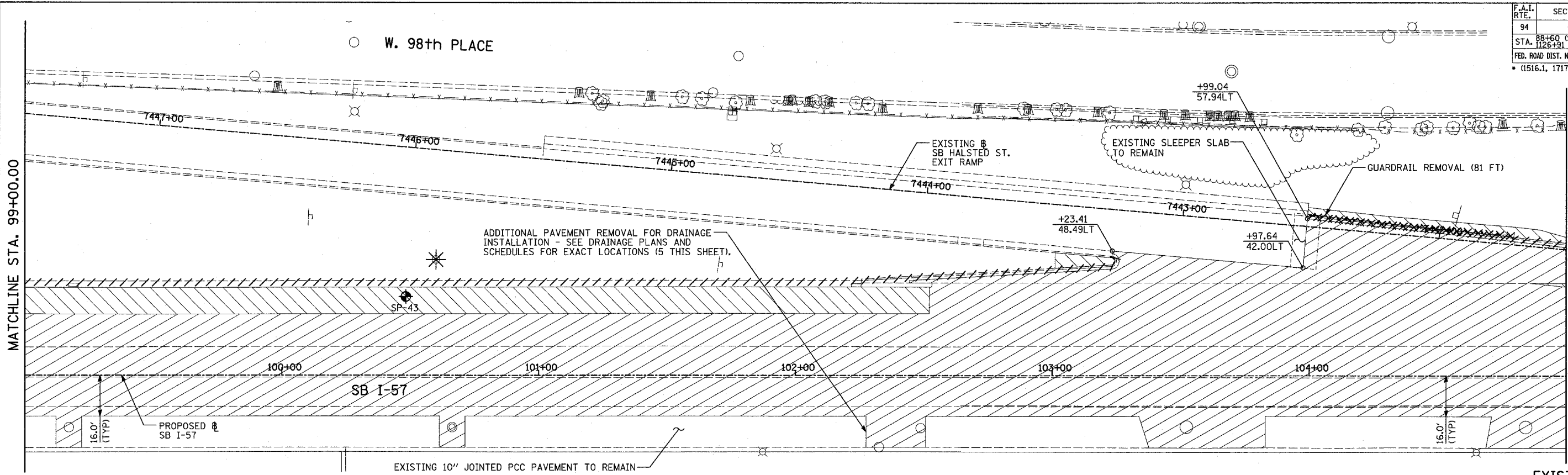
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
 SB I-57
 SB I-57 STA. 93+00.00 TO 99+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM

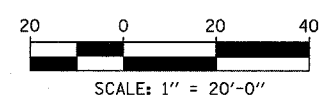
TYLIN INTERNATIONAL



LEGEND:

	PAVEMENT REMOVAL		PAVEMENT (SHOULDER) REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL		MAINLINE STATION/OFFSET
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL		PLAN NOTES:
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL		- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS		- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)		

ADDITIONAL PROPOSED PAVEMENT FOR DRAINAGE INSTALLATION - SEE DRAINAGE PLANS AND SCHEDULES FOR EXACT LOCATIONS. (5 THIS SHEET)



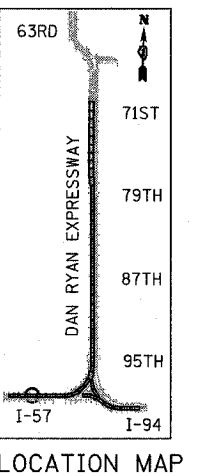
REVISIONS	
NAME	DATE

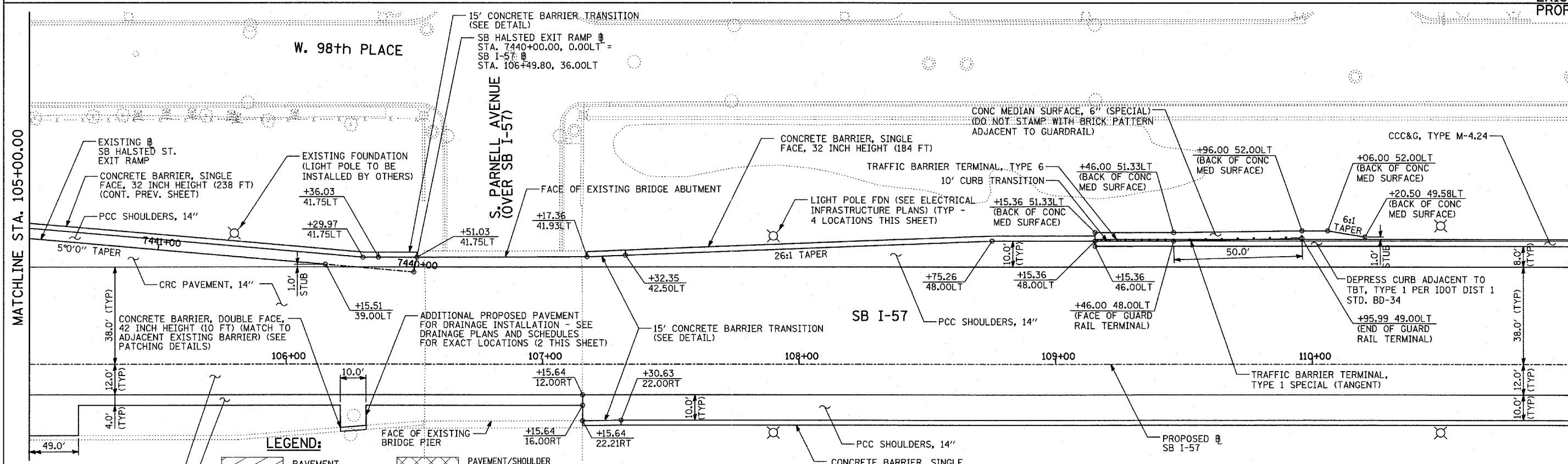
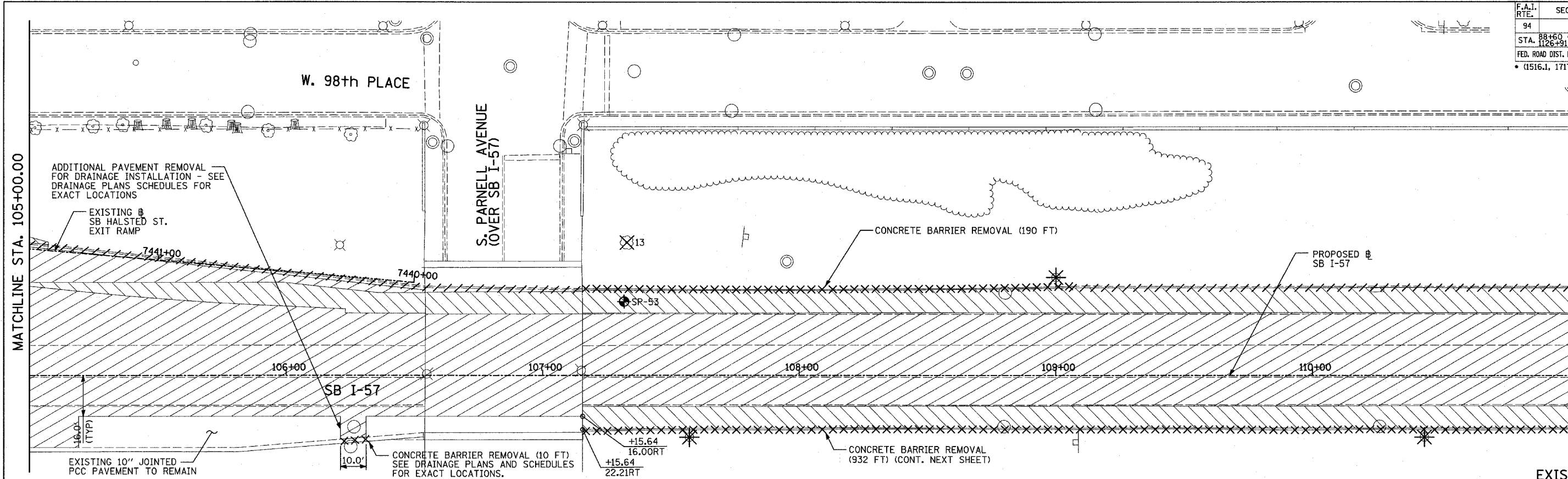
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-57
 SB I-57 STA. 99+00.00 TO 105+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM

TYLIN INTERNATIONAL





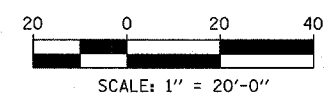
TYLIN INTERNATIONAL

LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

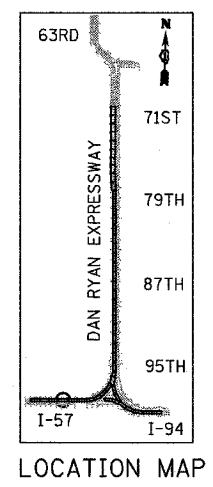


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
 SB I-57
 SB I-57 STA. 105+00.00 TO 111+00.00

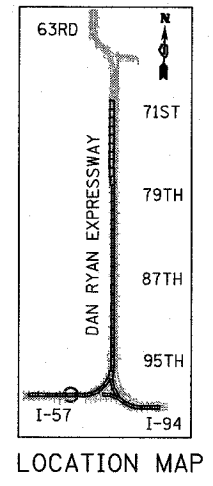
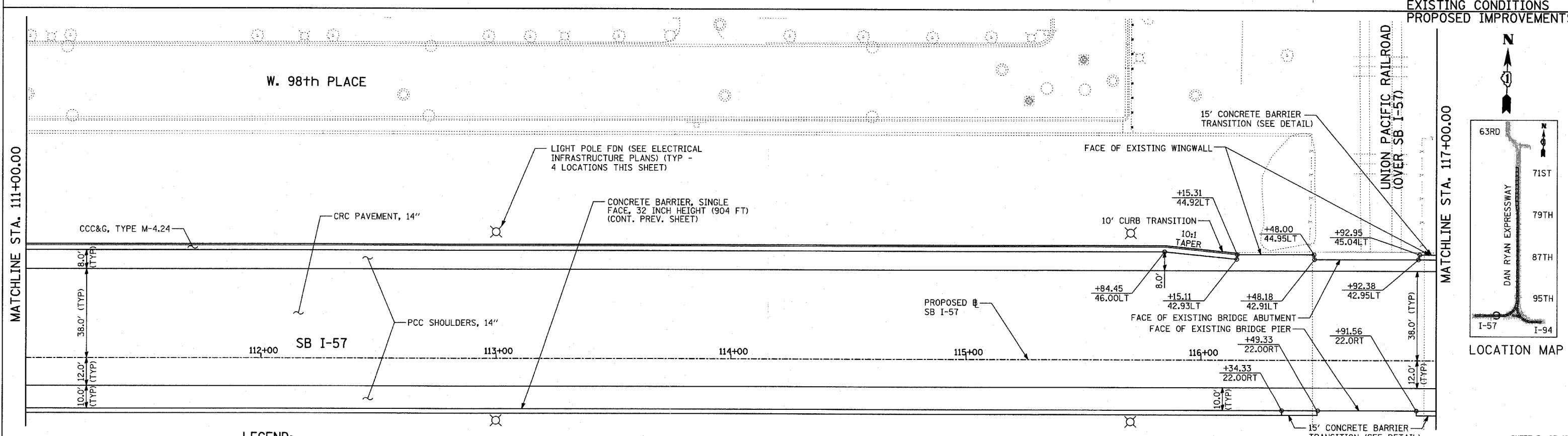
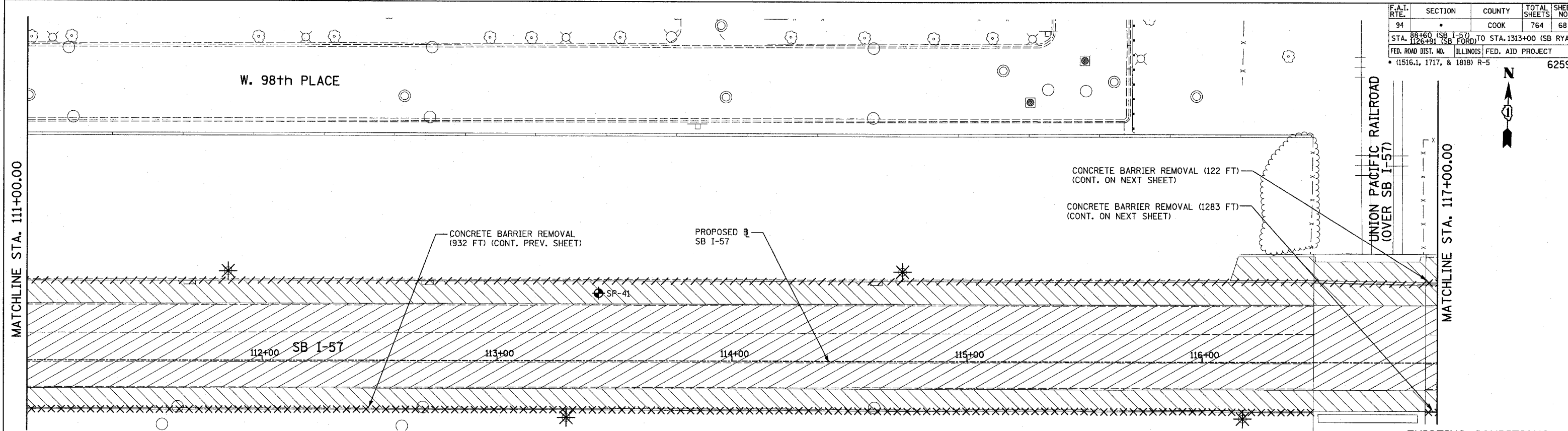
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 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	68
STA. 1125+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				

62593

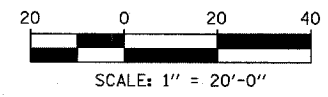


LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



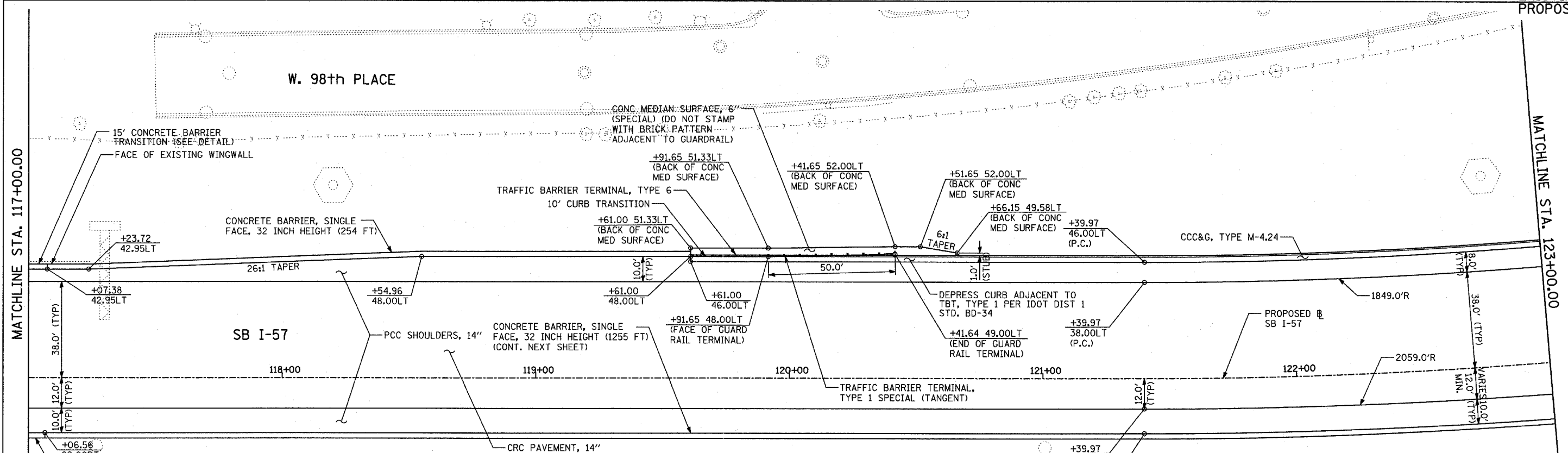
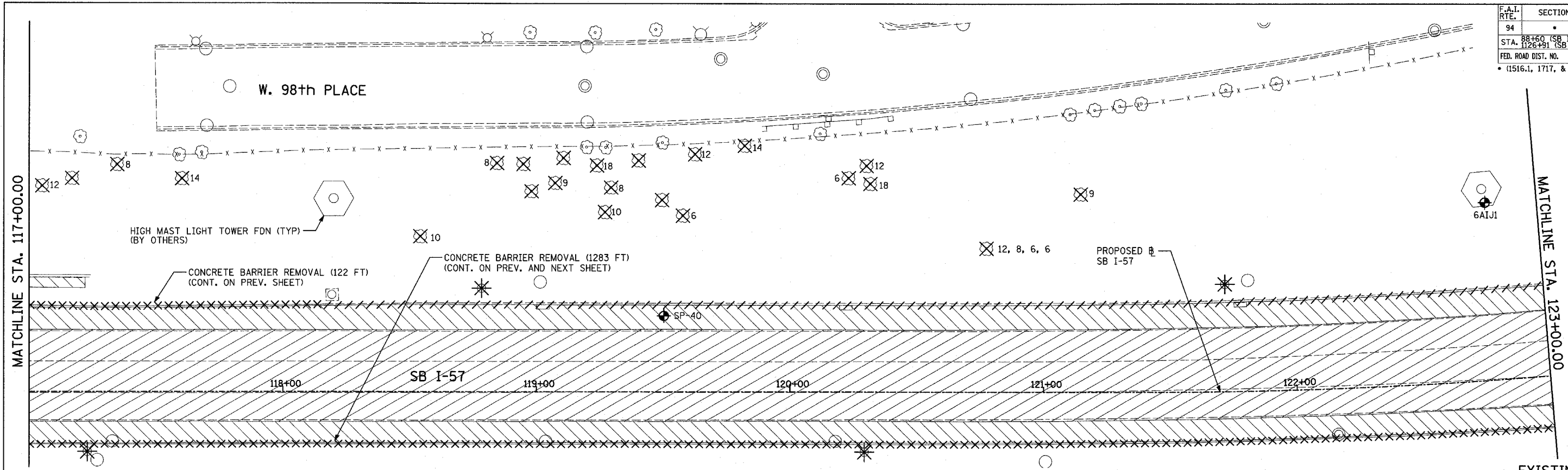
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-57
SB I-57 STA. 111+00.00 TO 117+00.00

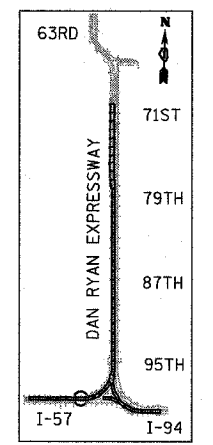
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CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	69
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

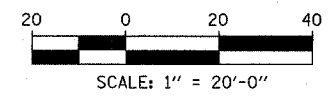
TYLIN INTERNATIONAL

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE @ STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



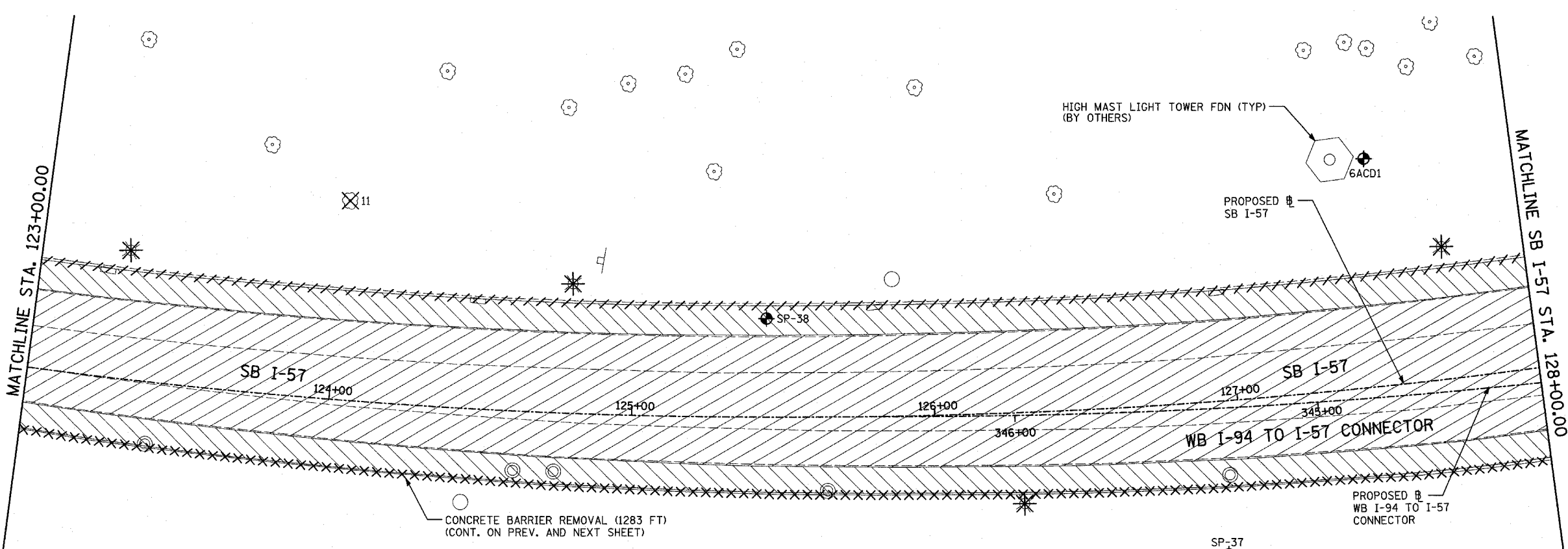
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
I-57
SB I-57 STA. 117+00.00 TO 123+00.00

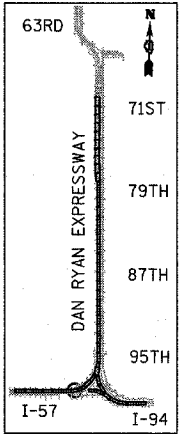
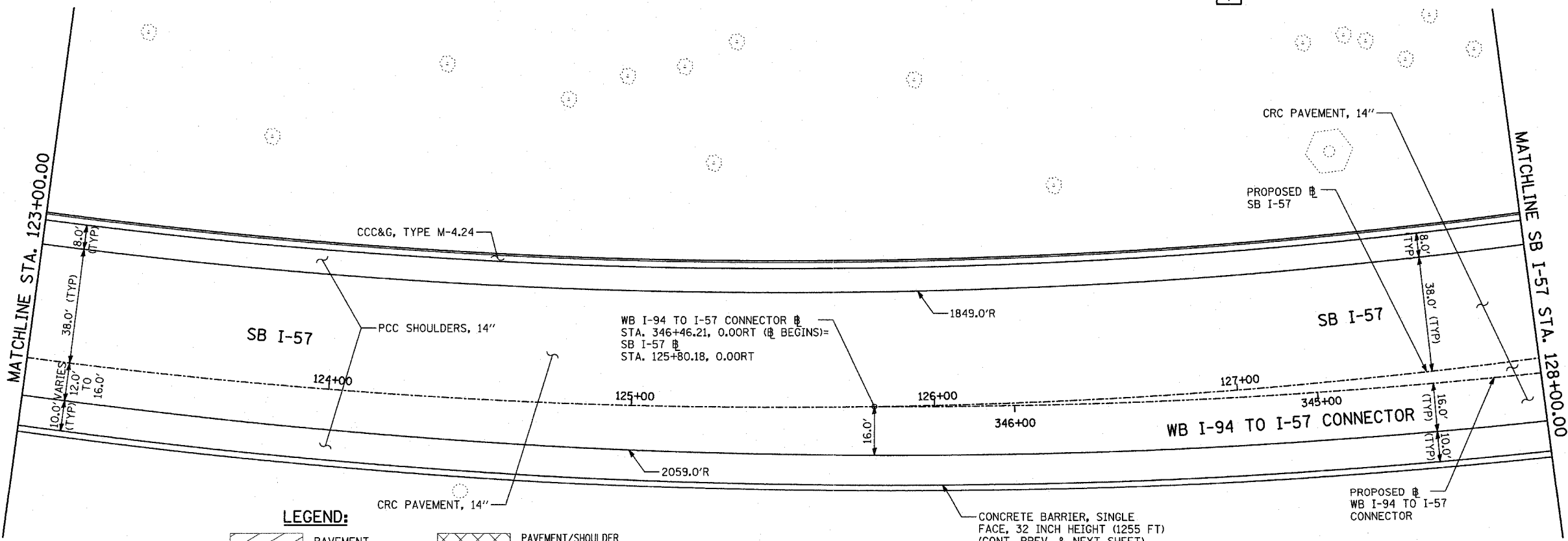
SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	70
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS

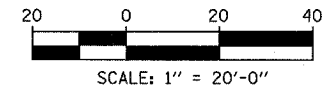


LOCATION MAP

LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | +XX.XX MAINLINE # |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | XX.XXRT STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | PLAN NOTES: |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS | | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED |

TYLIN INTERNATIONAL

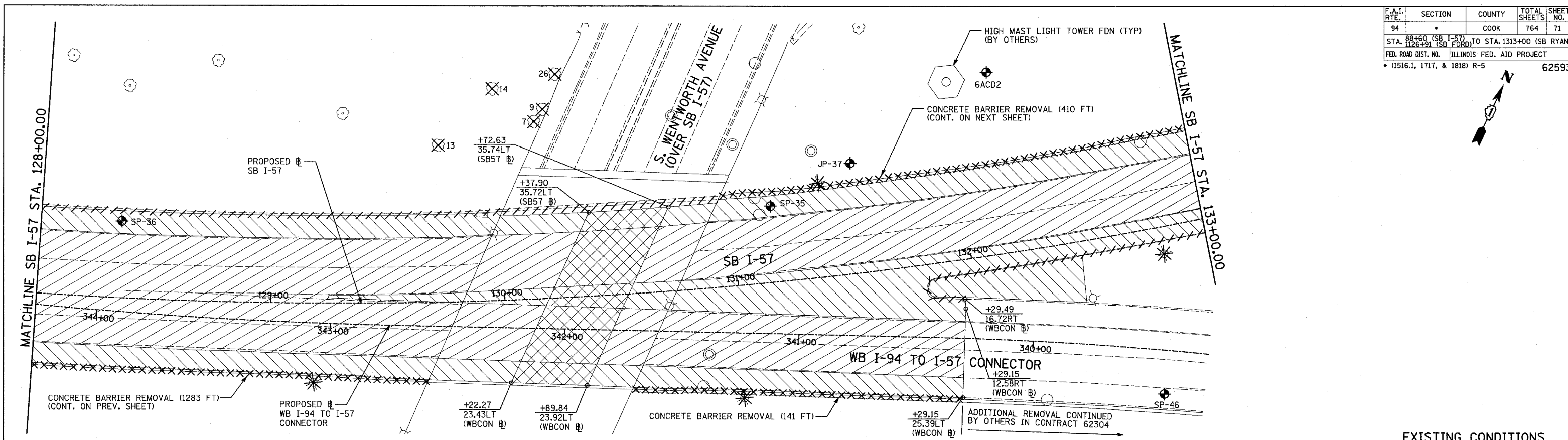


REVISIONS	
NAME	DATE

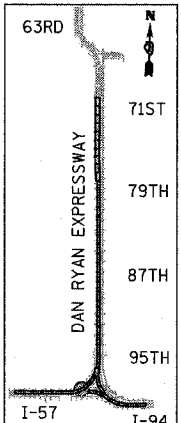
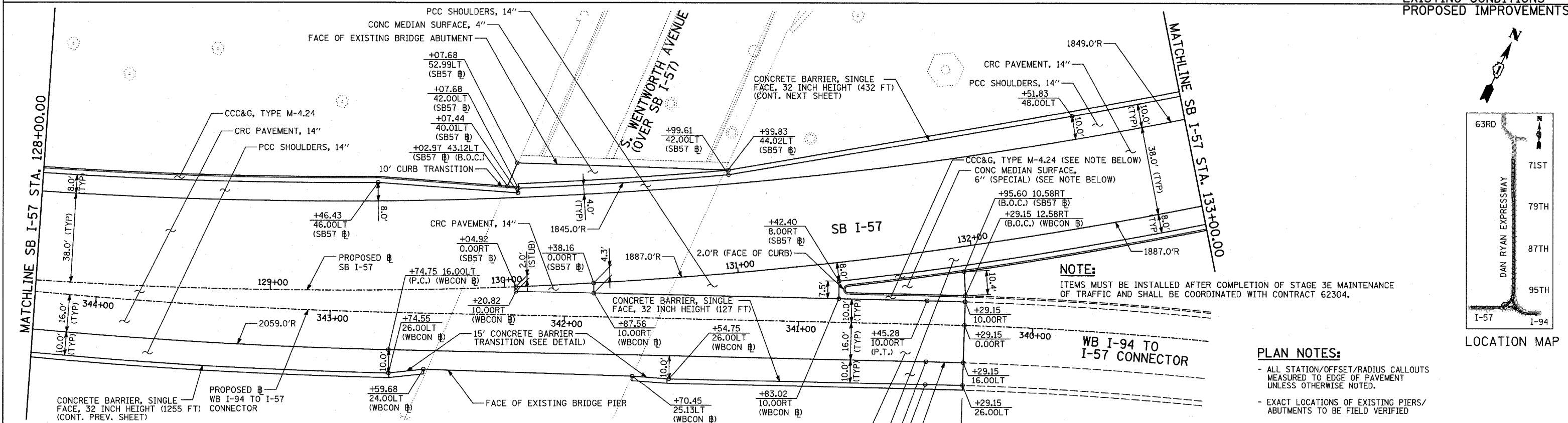
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-57
SB I-57 STA. 123+00.00 TO 128+00.00

S.N. DESIGNED BY:
SCALE: 1"=20' DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	71
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

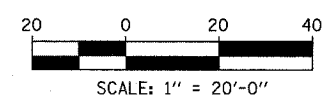
NOTE:
ITEMS MUST BE INSTALLED AFTER COMPLETION OF STAGE 3E MAINTENANCE OF TRAFFIC AND SHALL BE COORDINATED WITH CONTRACT 62304.

PLAN NOTES:
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

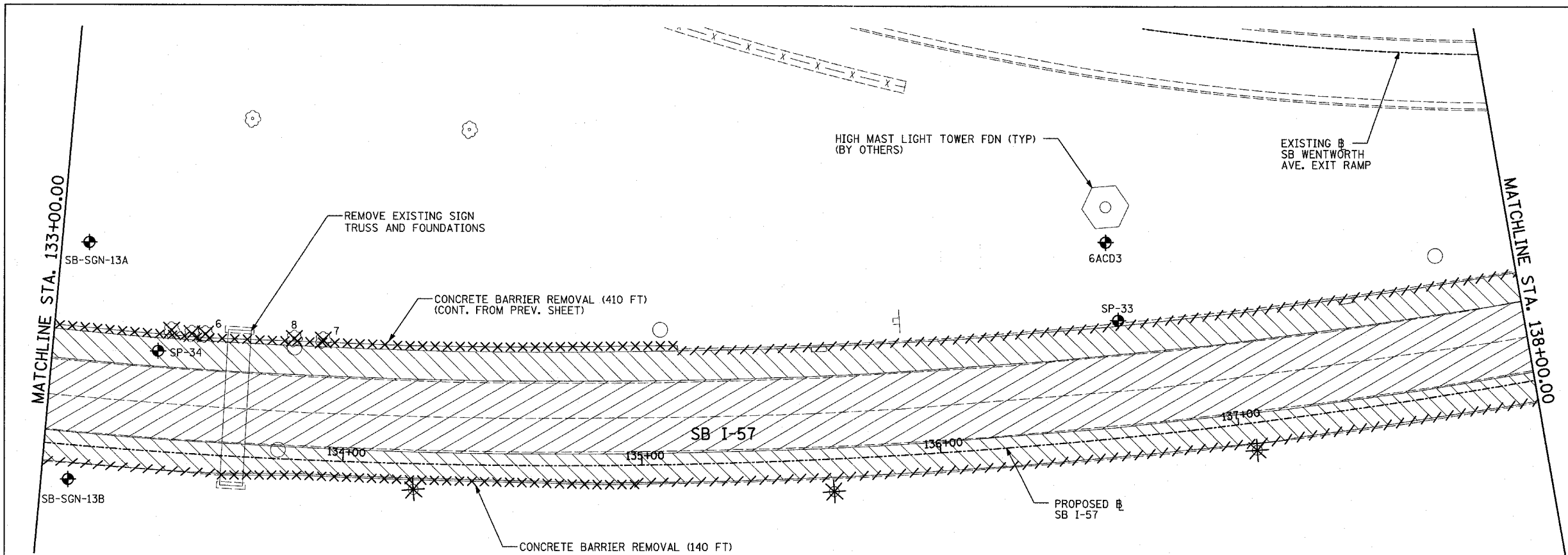
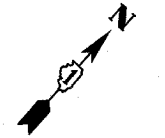
TYLIN INTERNATIONAL



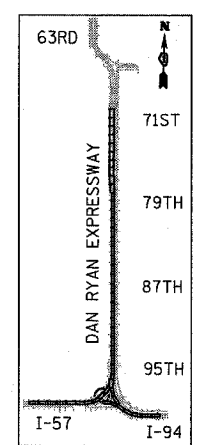
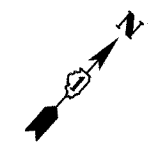
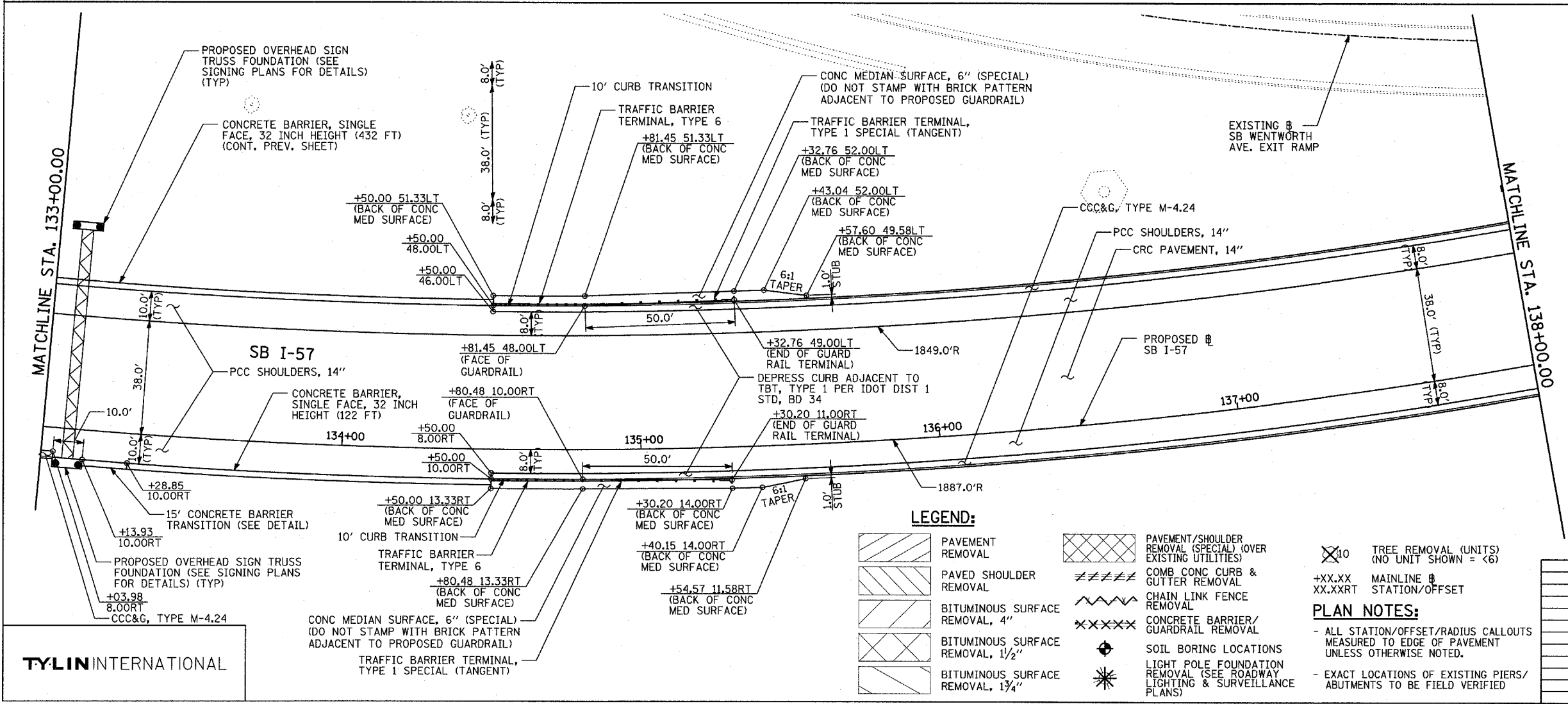
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
I-57
SB I-57 STA. 128+00.00 TO 133+00.00

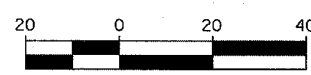
SCALE: 1"=20'
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: RTM



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



LEGEND:

- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- BITUMINOUS SURFACE REMOVAL, 4"
- BITUMINOUS SURFACE REMOVAL, 1 1/2"
- BITUMINOUS SURFACE REMOVAL, 1 3/4"
- PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
- COMB CONC CURB & GUTTER REMOVAL
- CHAIN LINK FENCE REMOVAL
- CONCRETE BARRIER/GUARDRAIL REMOVAL
- SOIL BORING LOCATIONS
- LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
- TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- MAINLINE
- STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

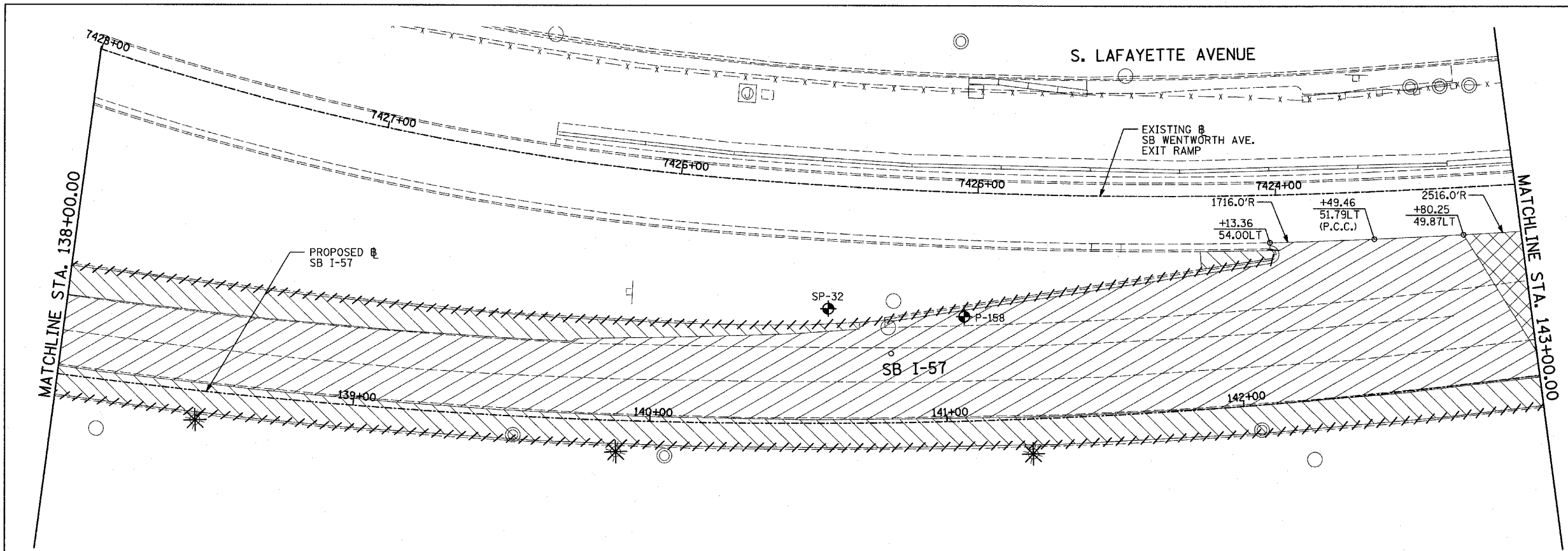
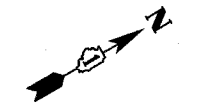
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
I-57
SB I-57 STA. 133+00.00 TO 138+00.00

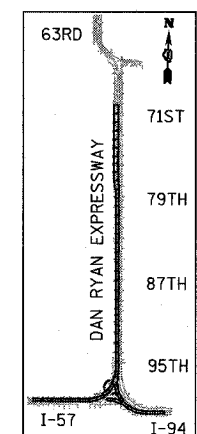
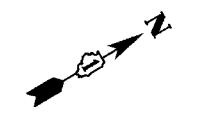
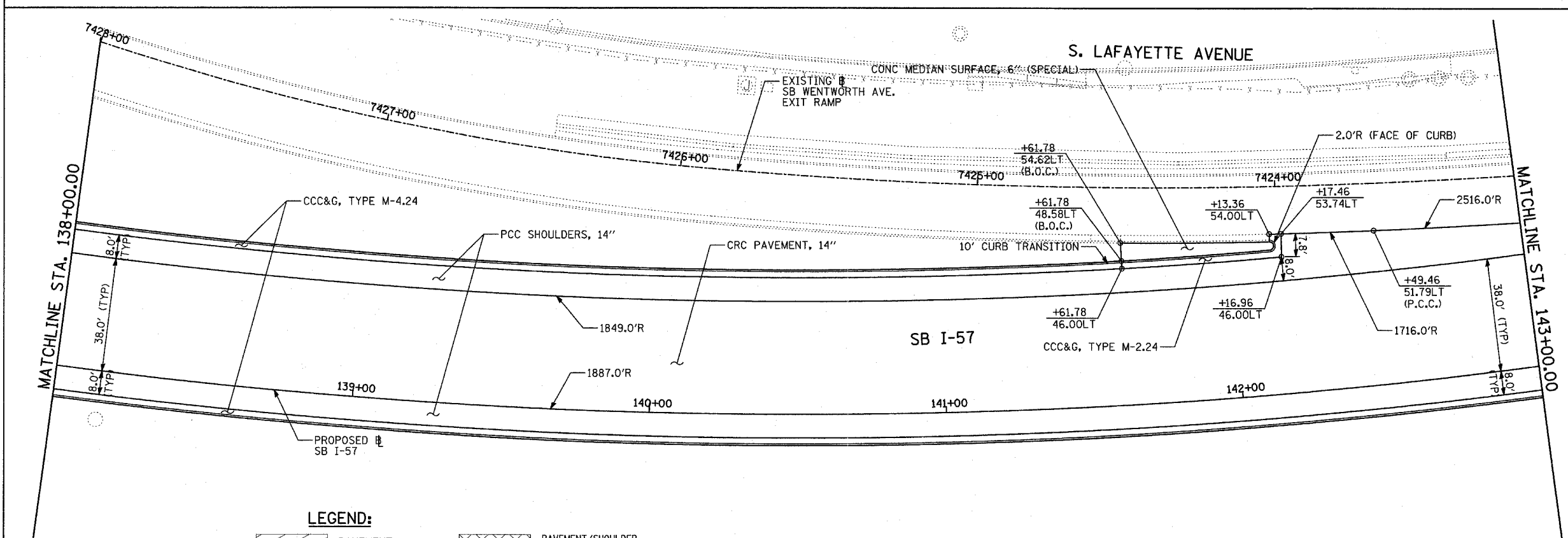
SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

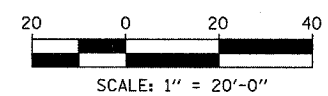
LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | SOIL BORING LOCATIONS | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | | | |

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL

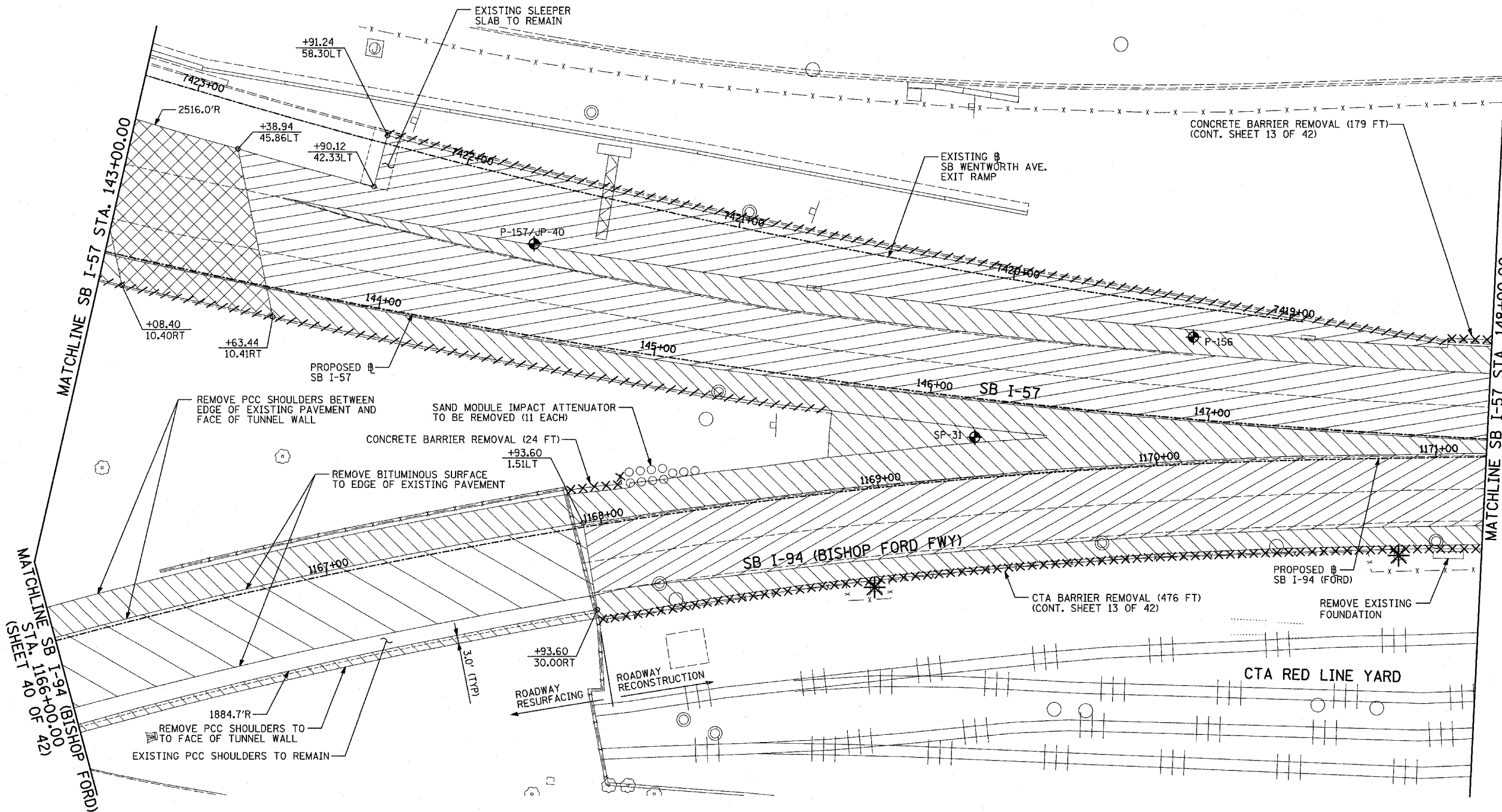


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-57
STA. 138+00.00 TO STA. 143+00.00

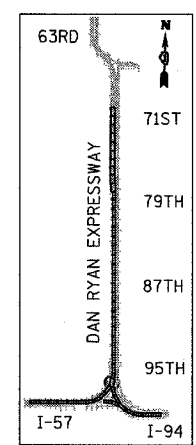
SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM



MATCHLINE SB I-94 (BISHOP FORD)
STA. 1166+00.00
(SHEET 40 OF 42)

MATCHLINE SB I-57 STA. 148+00.00



LOCATION MAP

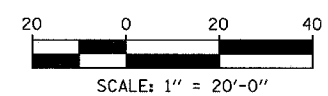
LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | SOIL BORING LOCATIONS | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | | | |

PLAN NOTES:

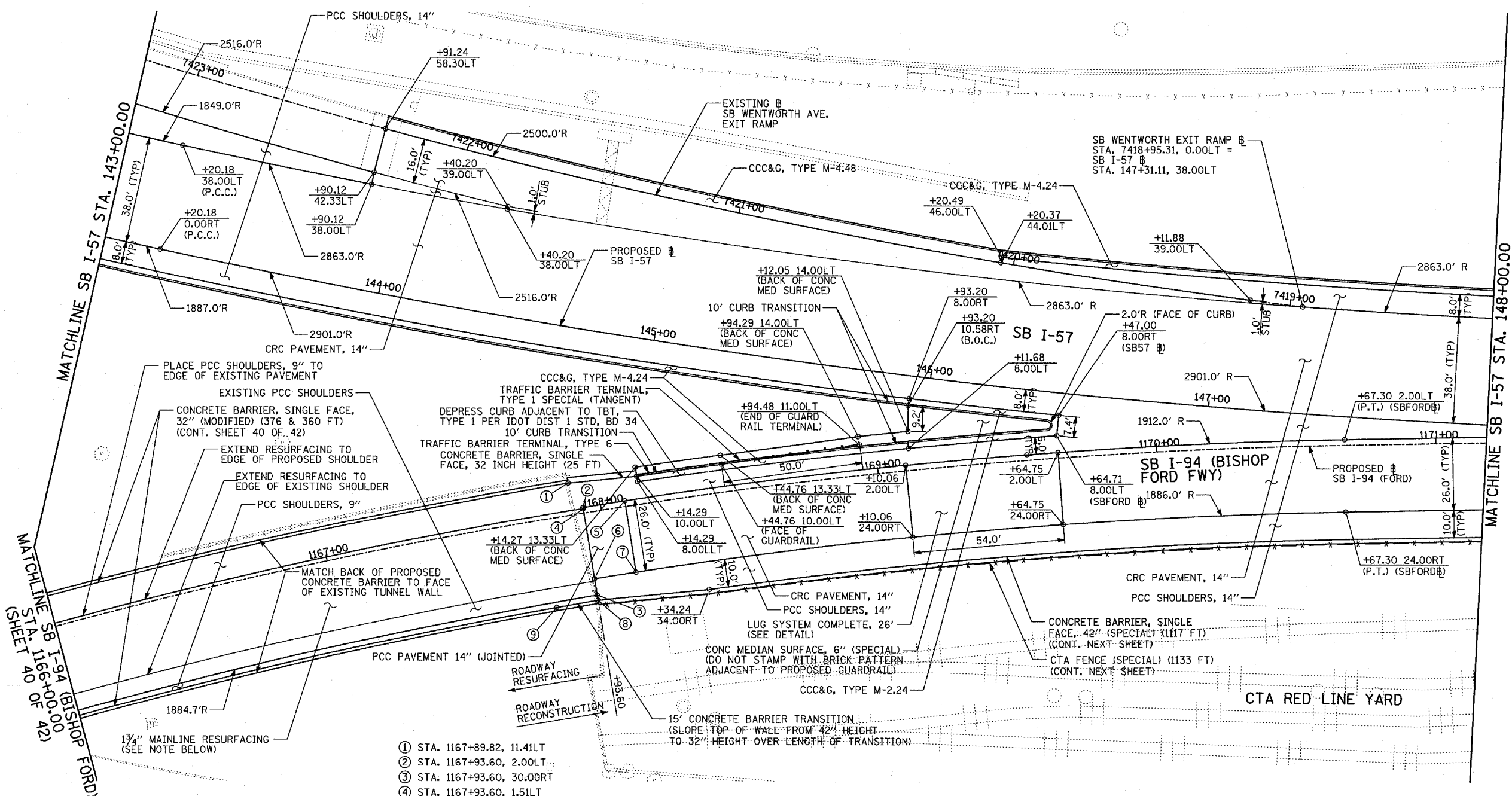
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING PLAN
SB I-57 & SB I-94 (BISHOP FORD FWY)
SB I-57 STA. 143+00.00 & SB I-94 (FORD)
STA. 1166+00.00 TO SB I-57 STA. 148+00.00
SCALE: 1"=20' DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: RTM



NOTES:
 1 3/4" MAINLINE RESURFACING (SEE NOTE BELOW)
 POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"

- ① STA. 1167+89.82, 11.41LT
- ② STA. 1167+93.60, 2.00LT
- ③ STA. 1167+93.60, 30.00RT
- ④ STA. 1167+93.60, 1.51LT
- ⑤ STA. 1168+08.79, 2.00LT
- ⑥ STA. 1167+93.60, 24.00RT
- ⑦ STA. 1168+08.79, 24.00RT
- ⑧ STA. 1167+93.60, 31.87RT
- ⑨ STA. 1167+78.34, 31.96RT

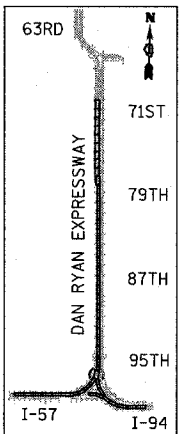
LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6>)
 +XX.XX MAINLINE #
 XX.XXRT STATION/OFFSET

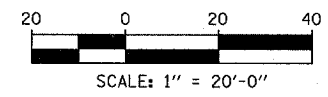
PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



LOCATION MAP

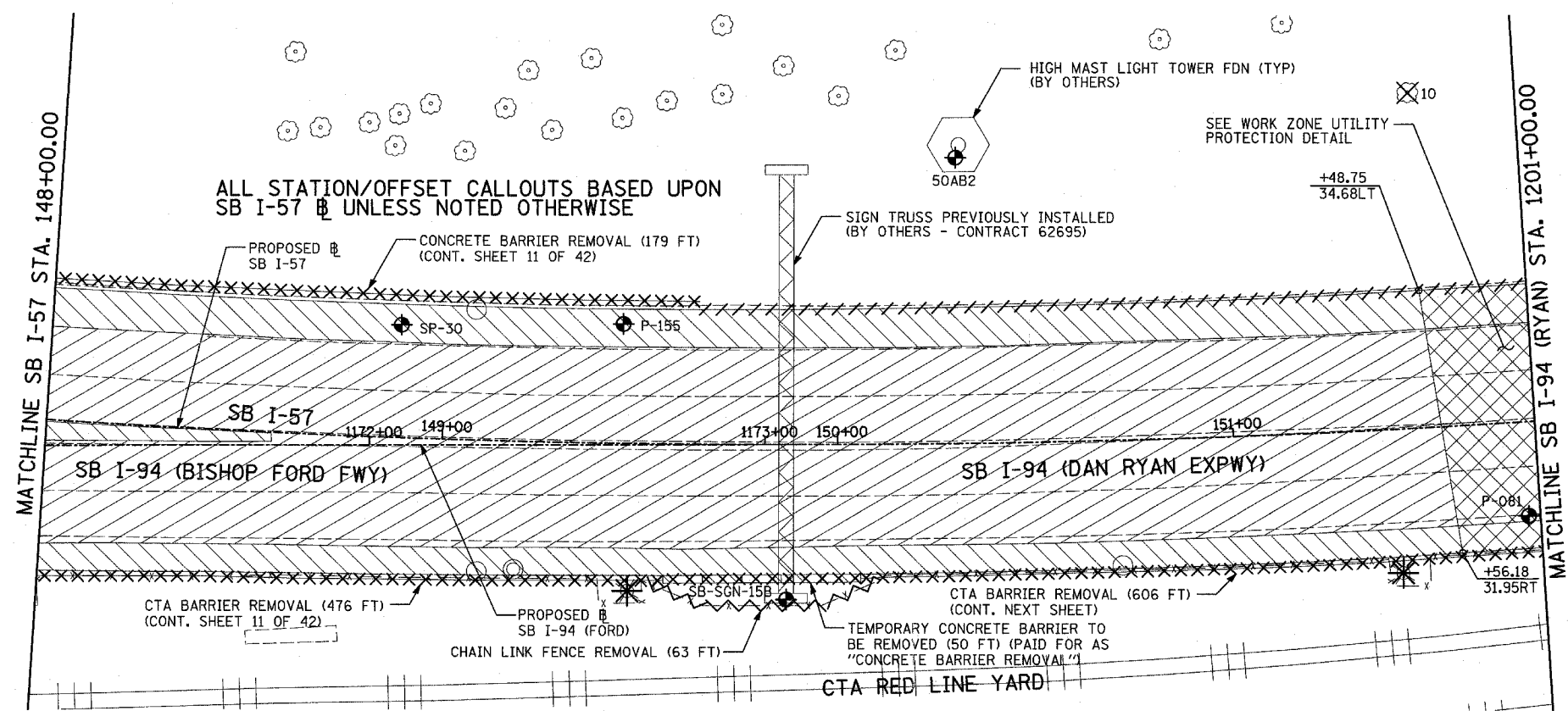
TYLIN INTERNATIONAL



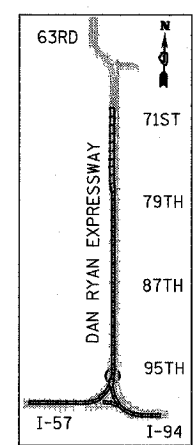
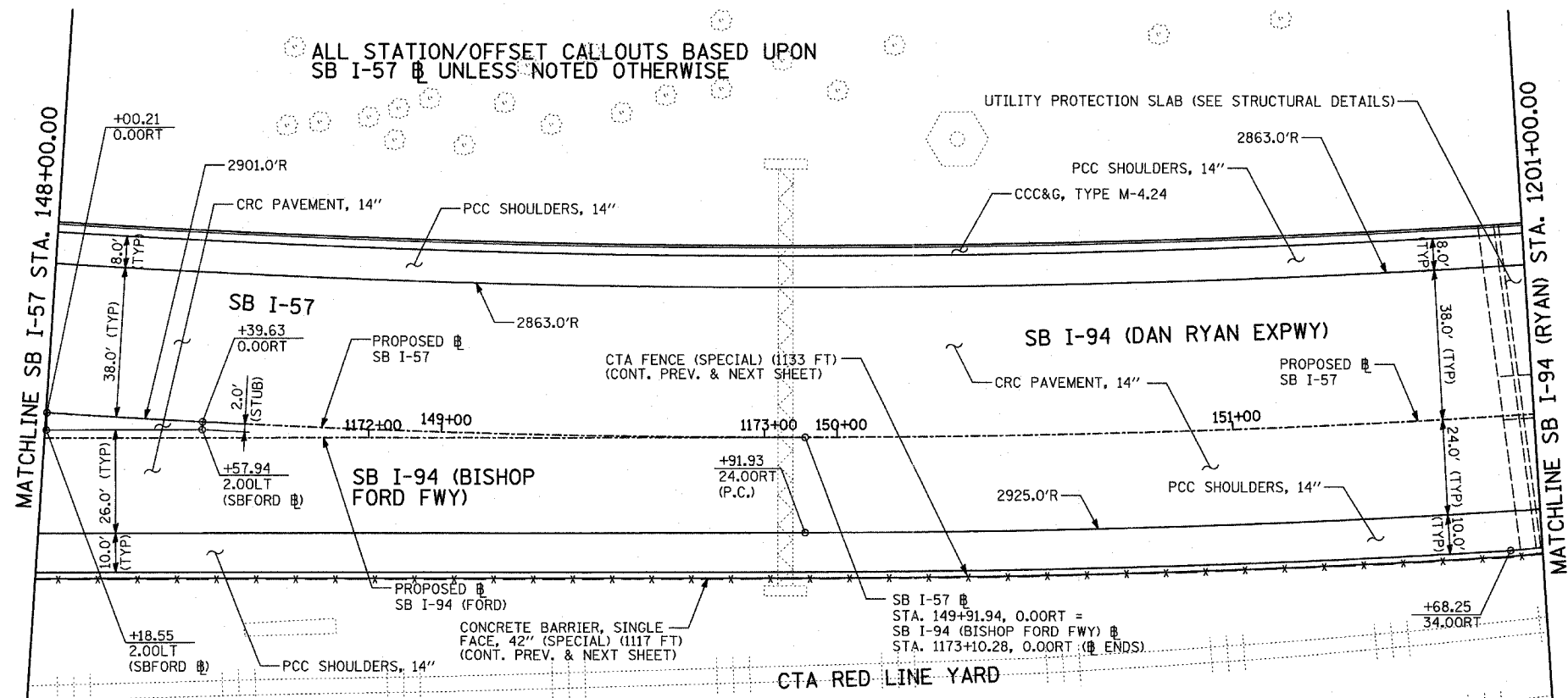
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
PROPOSED PLAN
 SB I-57 & SB I-94 (BISHOP FORD FWY)
 SB I-57 STA. 143+00.00 & SB I-94 (FORD)
 STA. 1166+00.00 TO SB I-57 STA. 148+00.00
 SCALE: 1"=20' DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	76
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



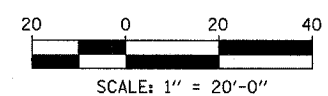
LOCATION MAP

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



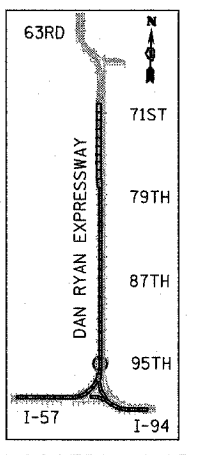
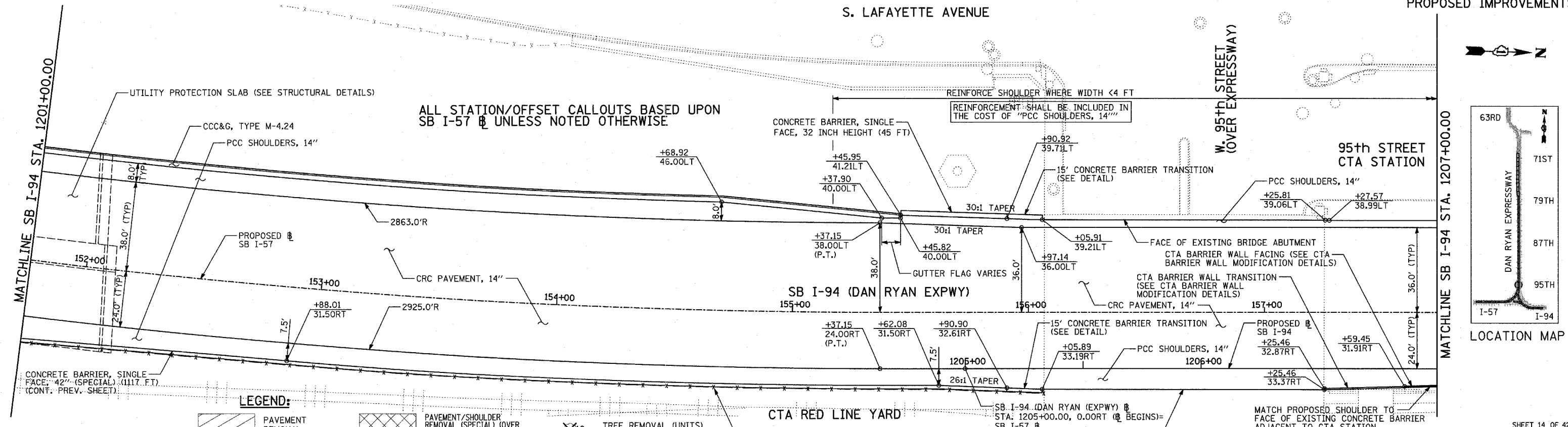
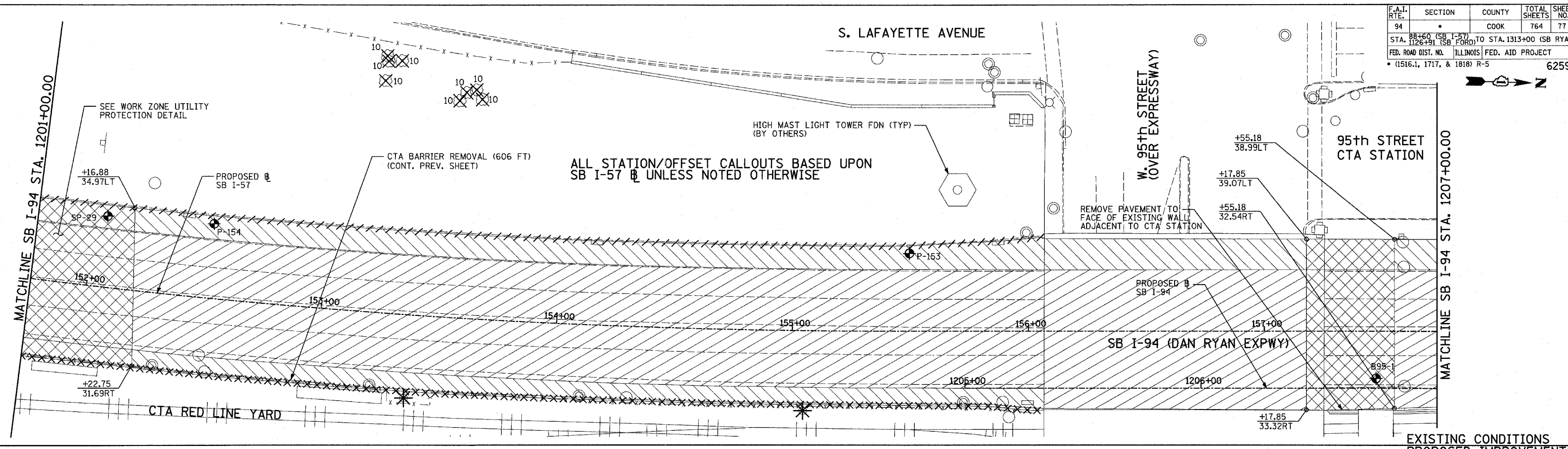
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (RYAN/FORD) & SB I-57
SB I-57 STA. 148+00.00 TO
SB I-94 (RYAN) STA. 1201+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL



LEGEND:

- | | | | |
|--|------------------------------------|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- +XX.XX MAINLINE # STATION/OFFSET
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

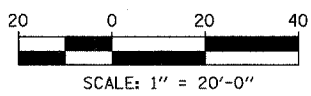
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1201+00.00 TO 1207+00.00

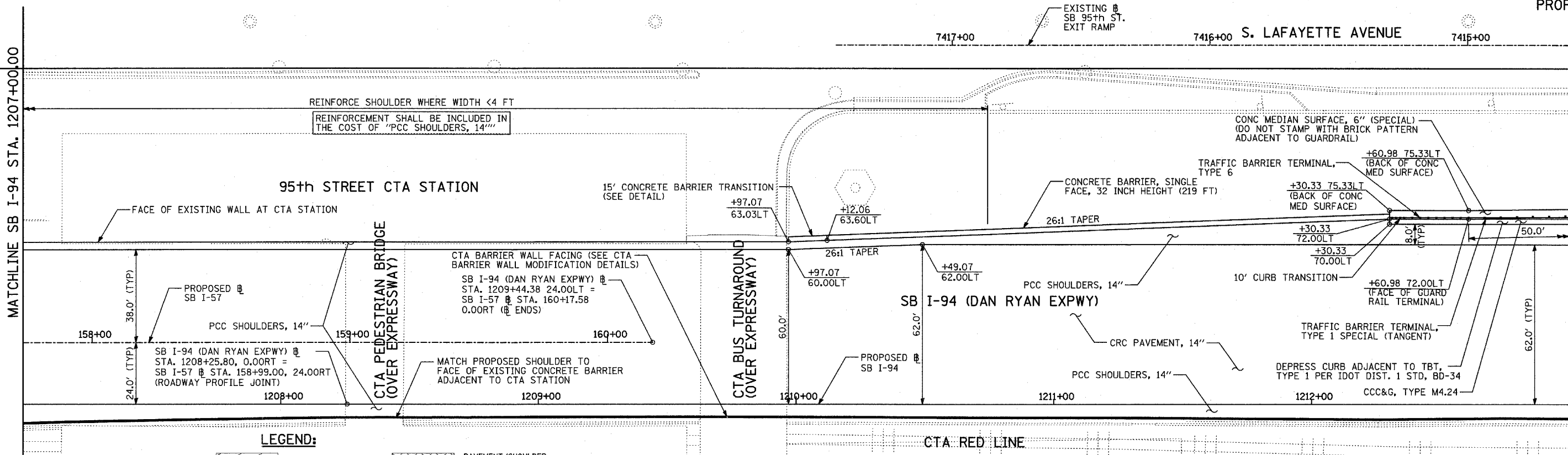
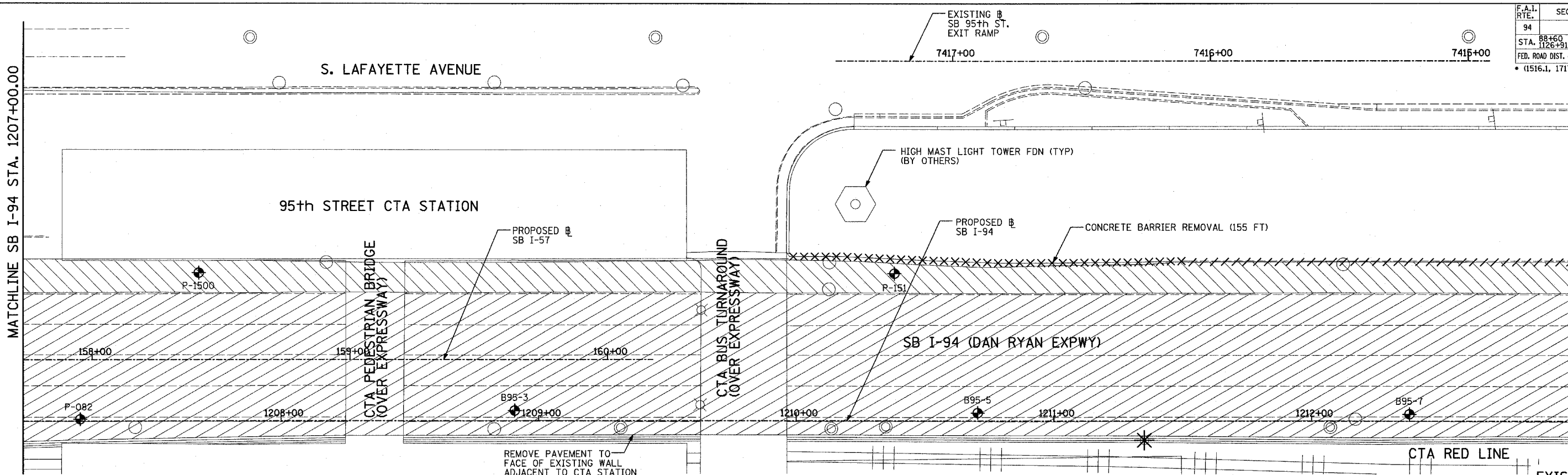
SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM

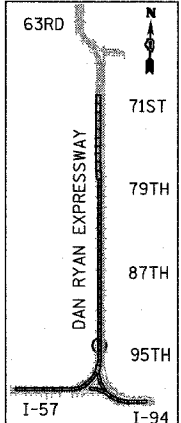
TYLIN INTERNATIONAL



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	78
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS

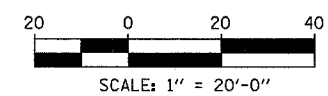


LOCATION MAP

LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE & STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | PLAN NOTES: |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS | | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | |

TYLIN INTERNATIONAL

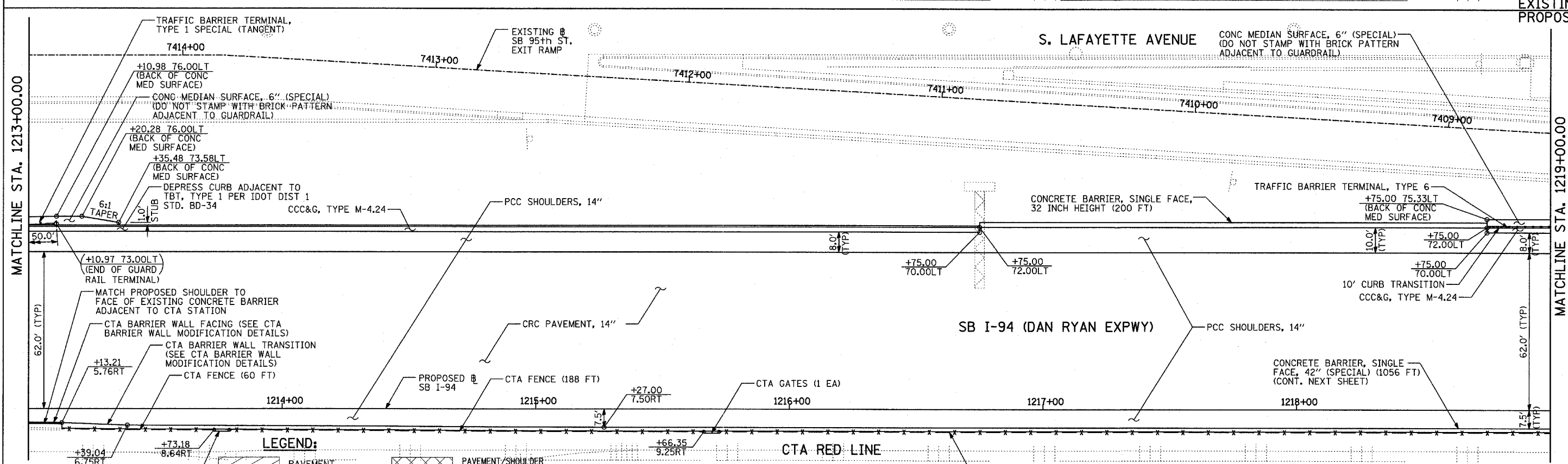
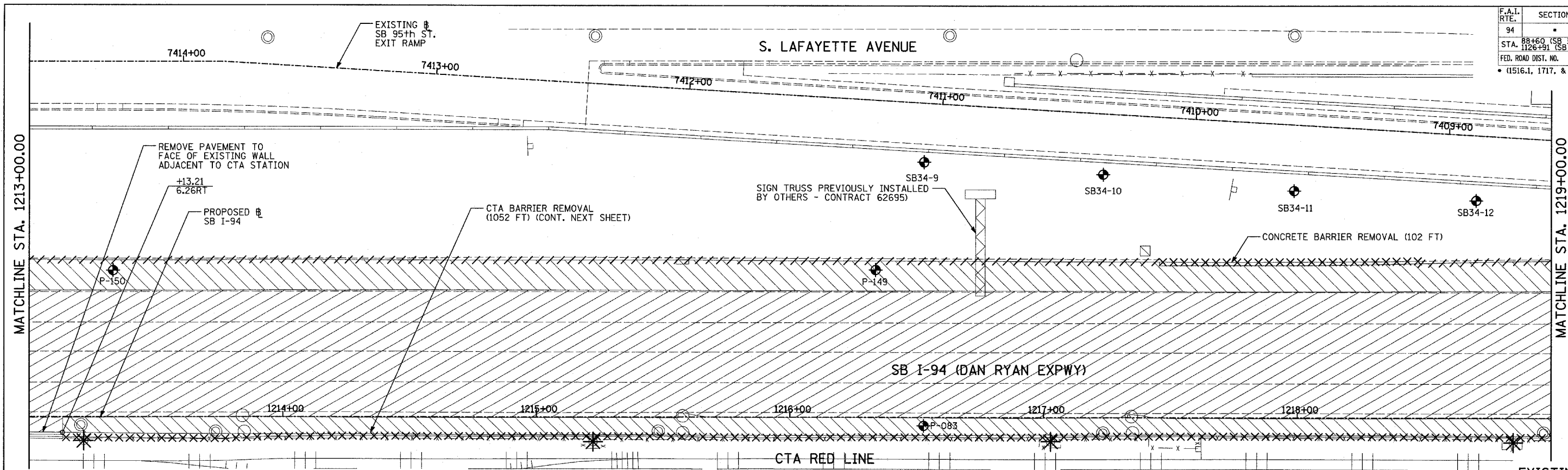


REVISIONS	
NAME	DATE

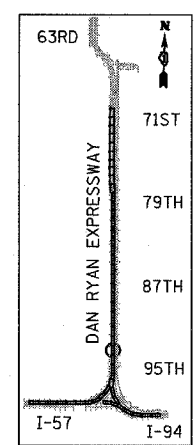
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1207+00.00 TO 1213+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM



EXISTING CONDITIONS
 PROPOSED IMPROVEMENTS

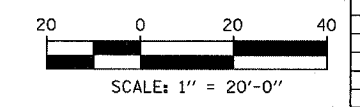


LOCATION MAP

LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE # |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | PLAN NOTES: |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS | | - ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED |

TYLININTERNATIONAL

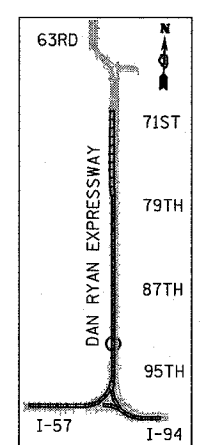
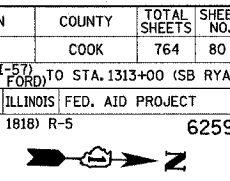
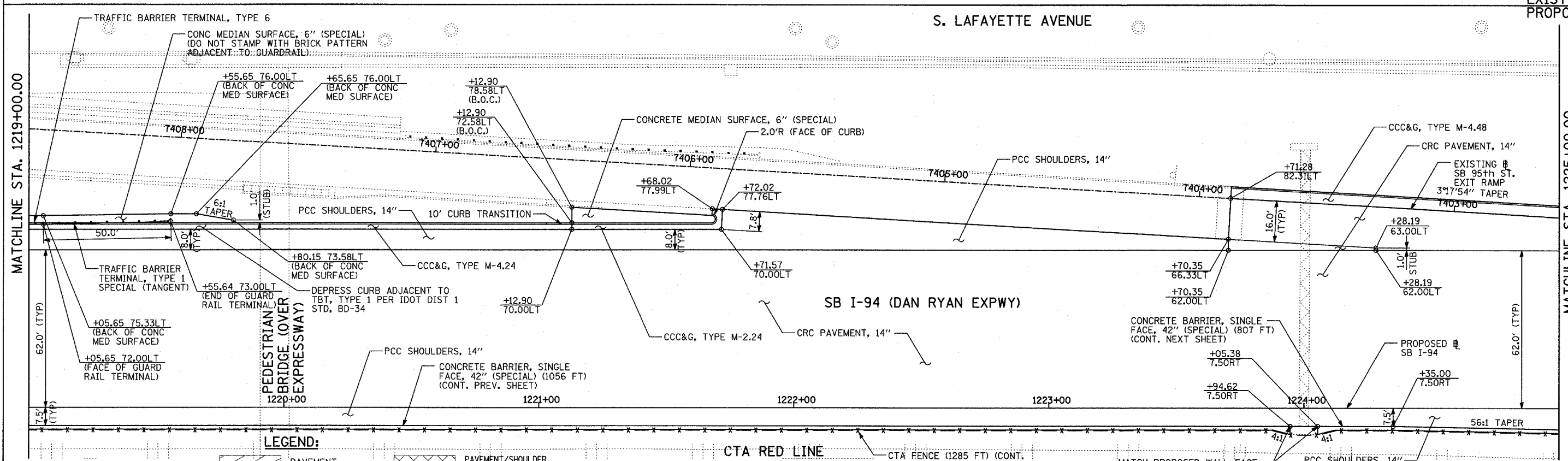
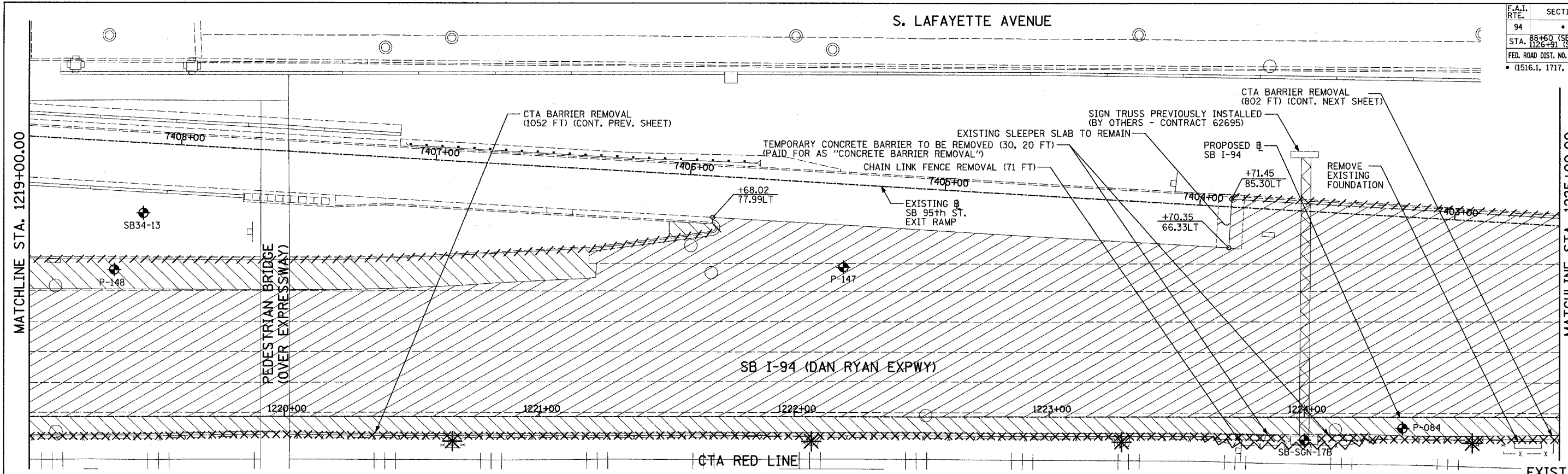


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1213+00.00 TO 1219+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM



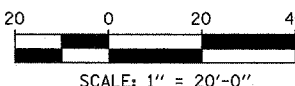
LEGEND:

- | | | | |
|--|------------------------------------|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- MAINLINE #
- STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1219+00.00 TO 1225+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM

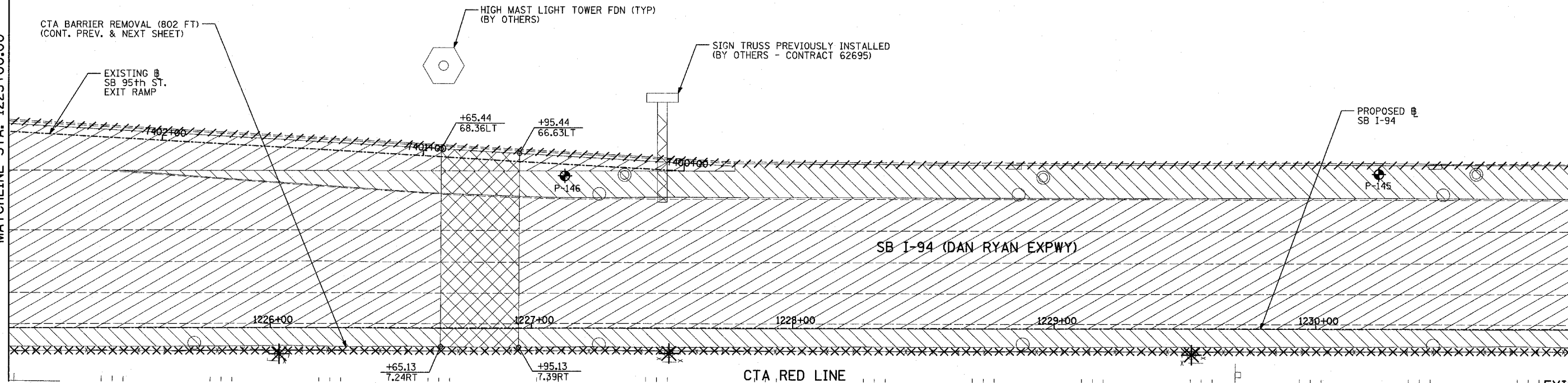
TYLIN INTERNATIONAL

S. LAFAYETTE AVENUE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	81
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				

MATCHLINE STA. 1225+00.00

MATCHLINE STA. 1231+00.00

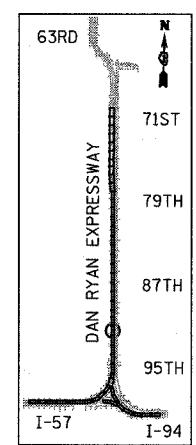
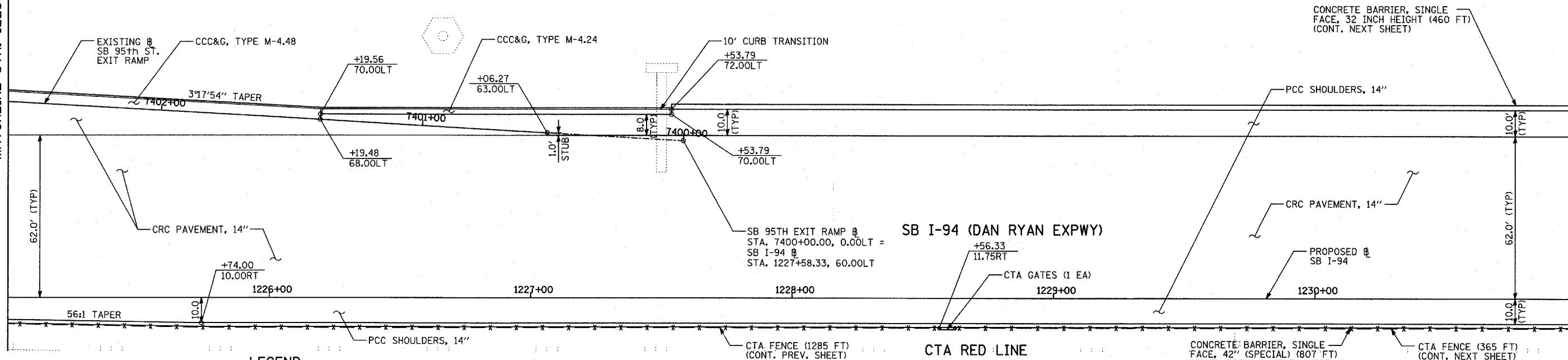


EXISTING CONDITIONS
PROPOSED IMPROVEMENTS

S. LAFAYETTE AVENUE

MATCHLINE STA. 1225+00.00

MATCHLINE STA. 1231+00.00



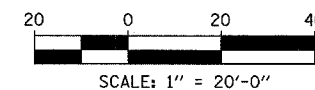
LOCATION MAP

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE # STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

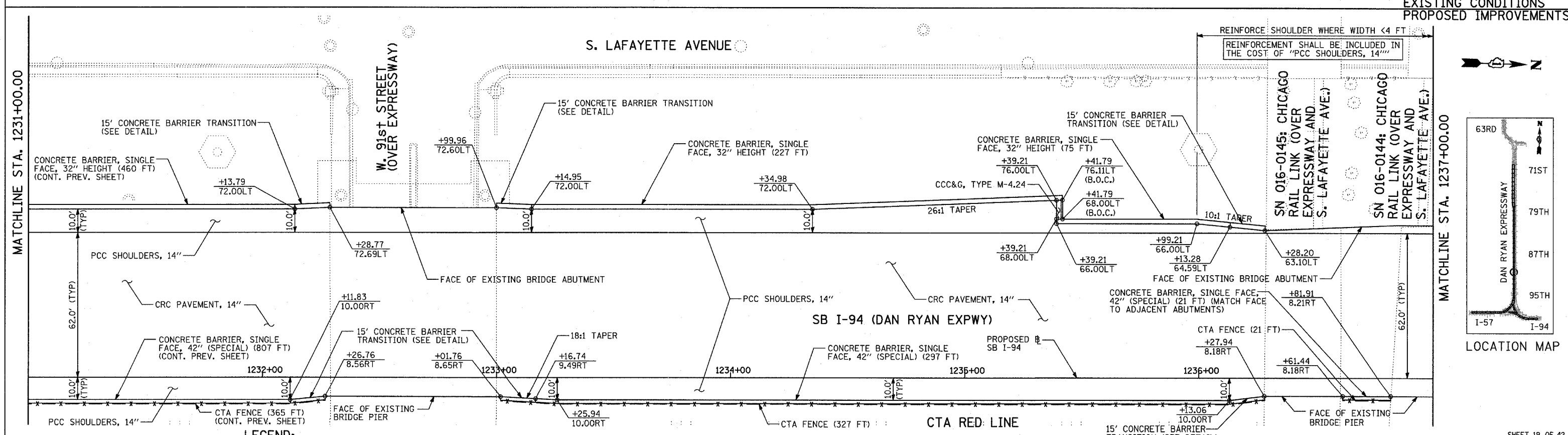
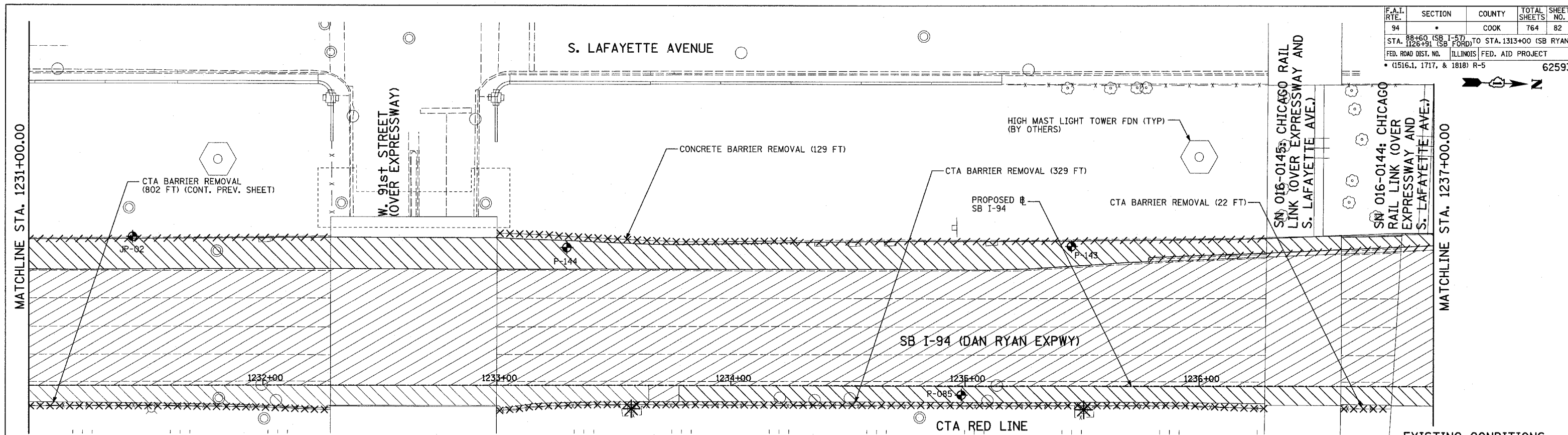
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1225+00.00 TO 1231+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	82
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



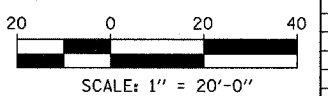
LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

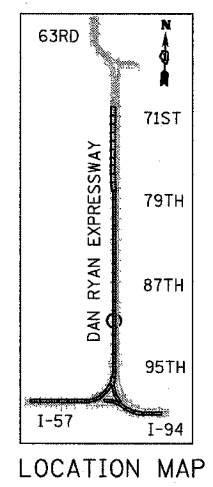


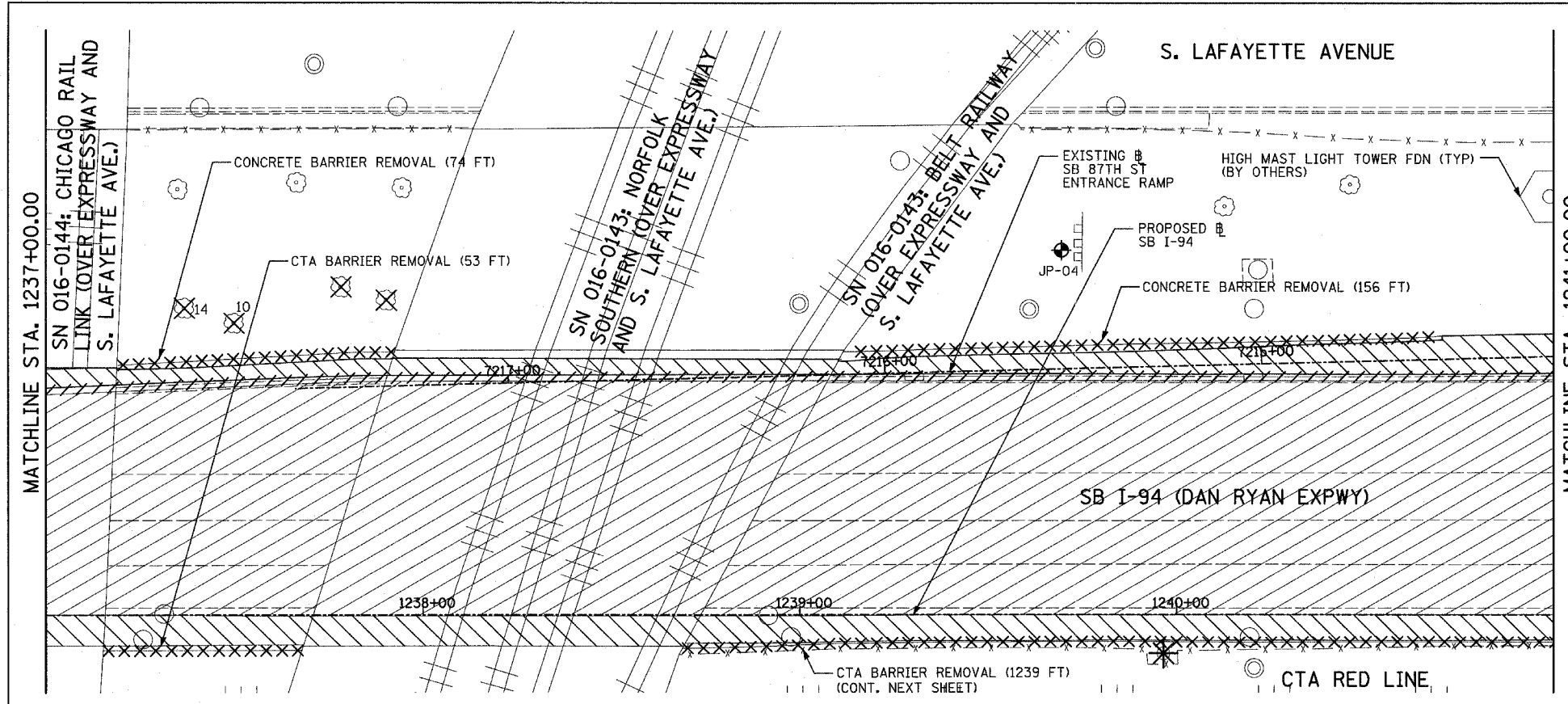
TYLIN INTERNATIONAL

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1231+00.00 TO 1237+00.00

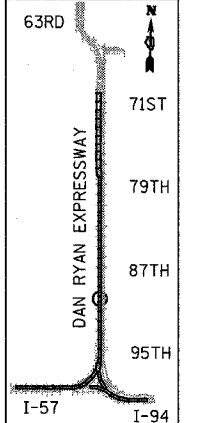
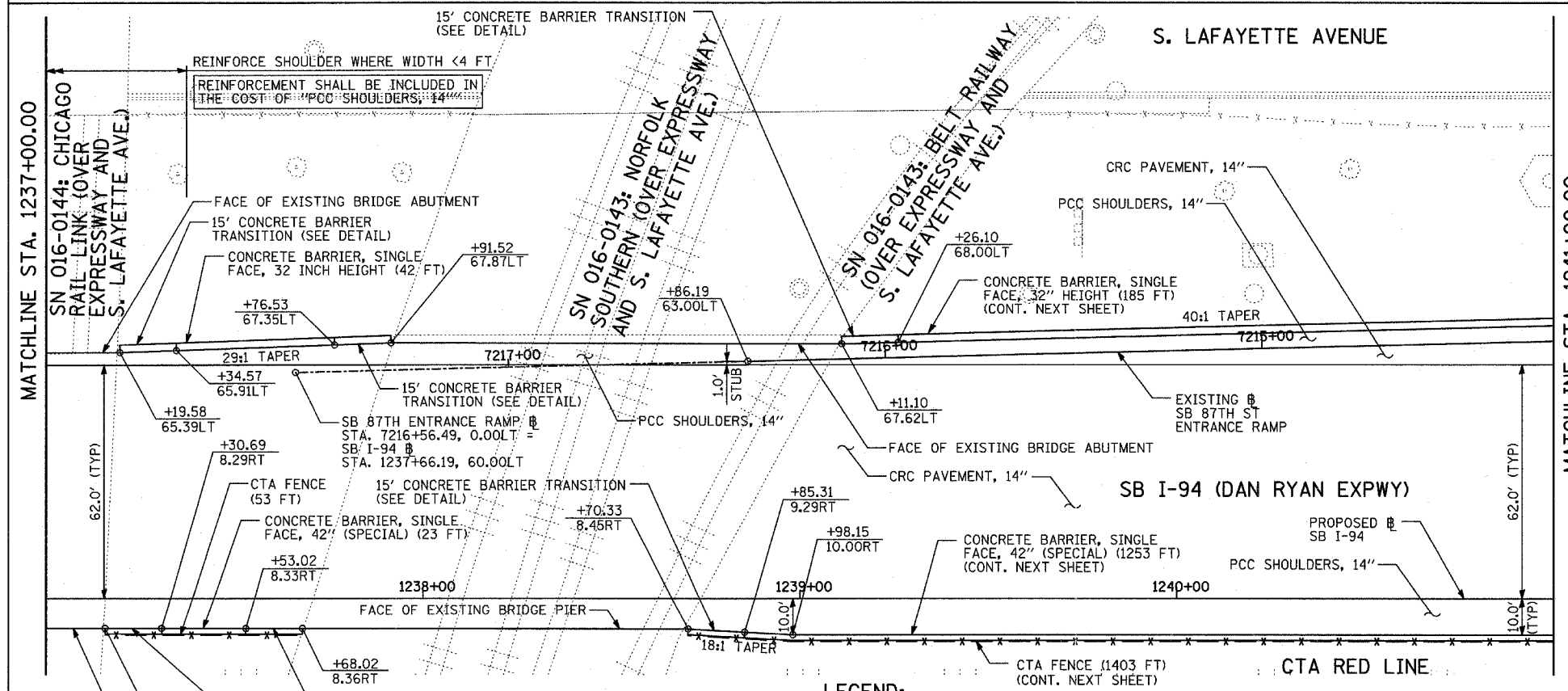
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DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM





EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



SCALE: 1" = 20'-0"
SHEET 20 OF 42

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
	CONCRETE BARRIER/GUARDRAIL REMOVAL				

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

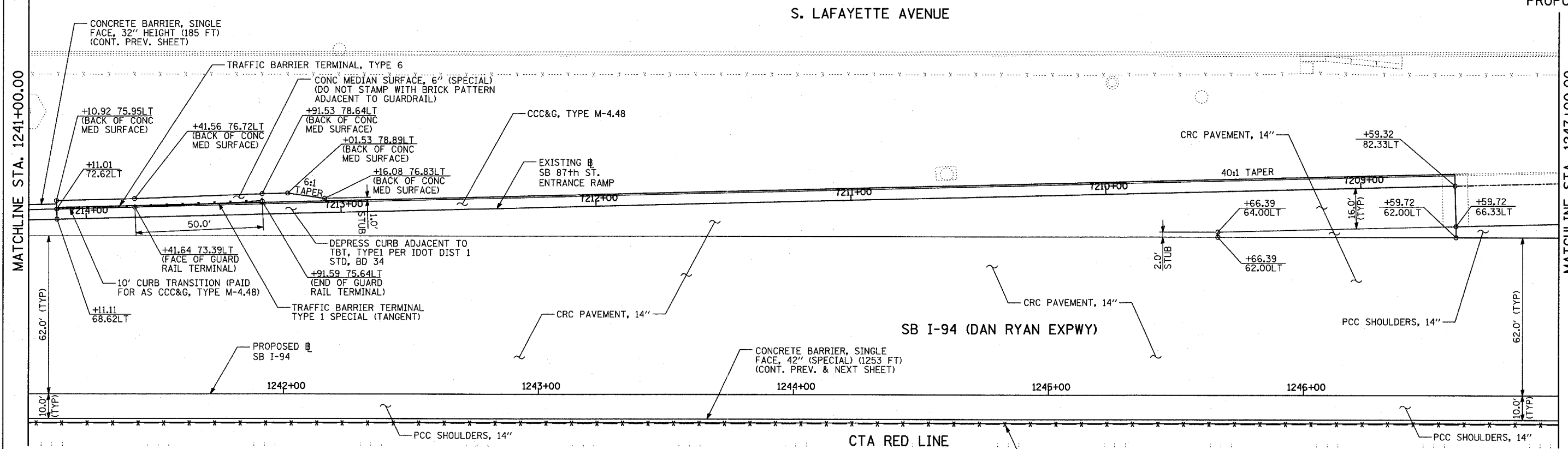
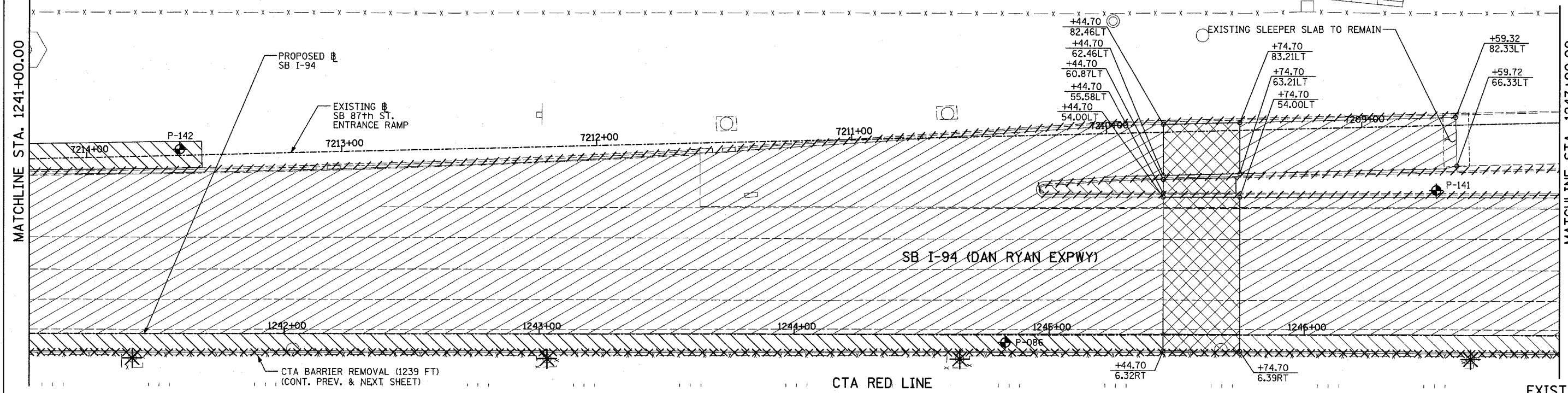
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1237+00.00 TO 1241+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

S. LAFAYETTE AVENUE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	84
STA. 88+60 (SB I-57) TO STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				

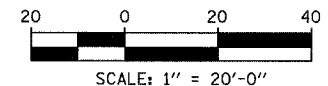


LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



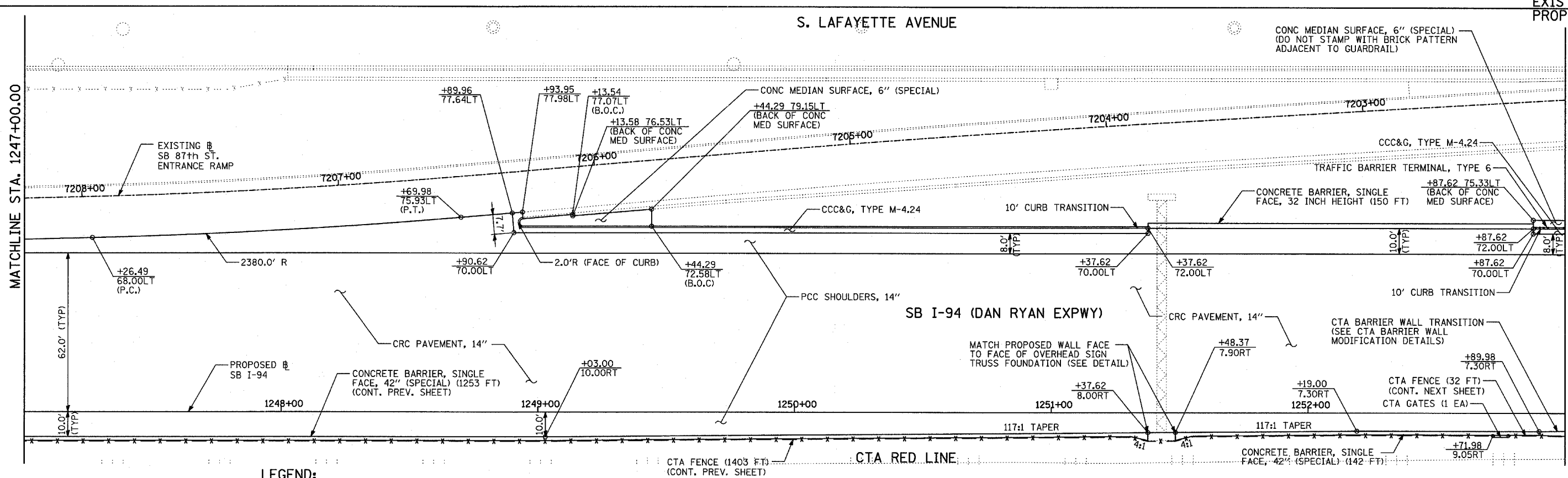
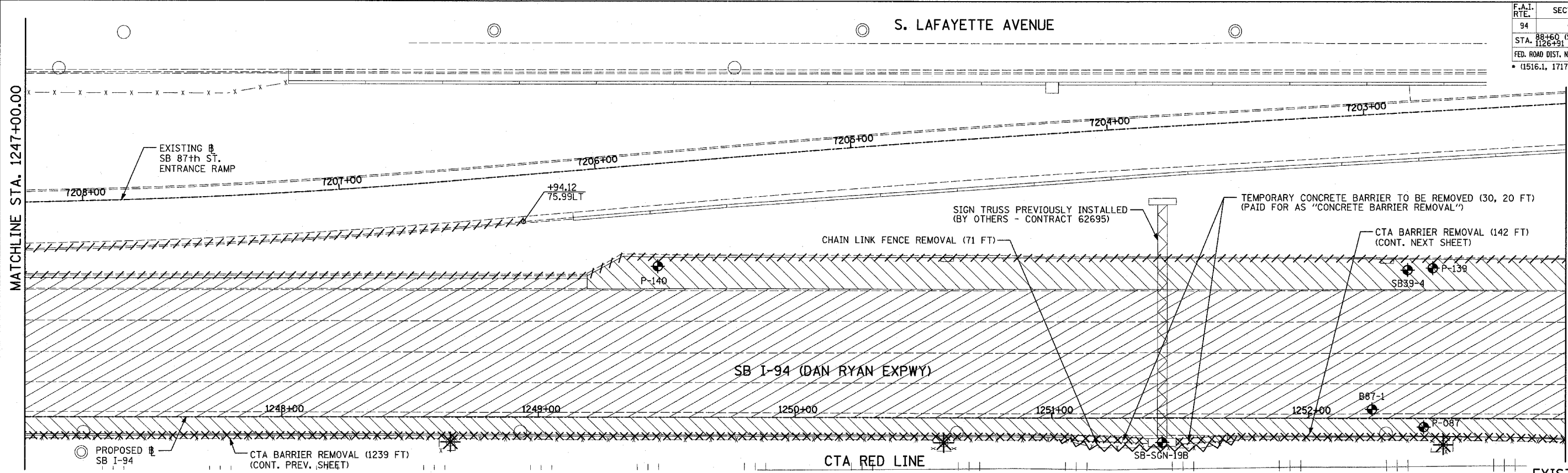
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1241+00.00 TO 1247+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	85
STA. 1126+00 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				

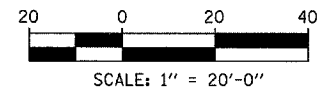


LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE & STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

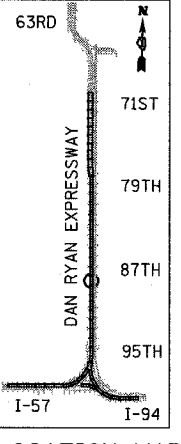


REVISIONS	
NAME	DATE

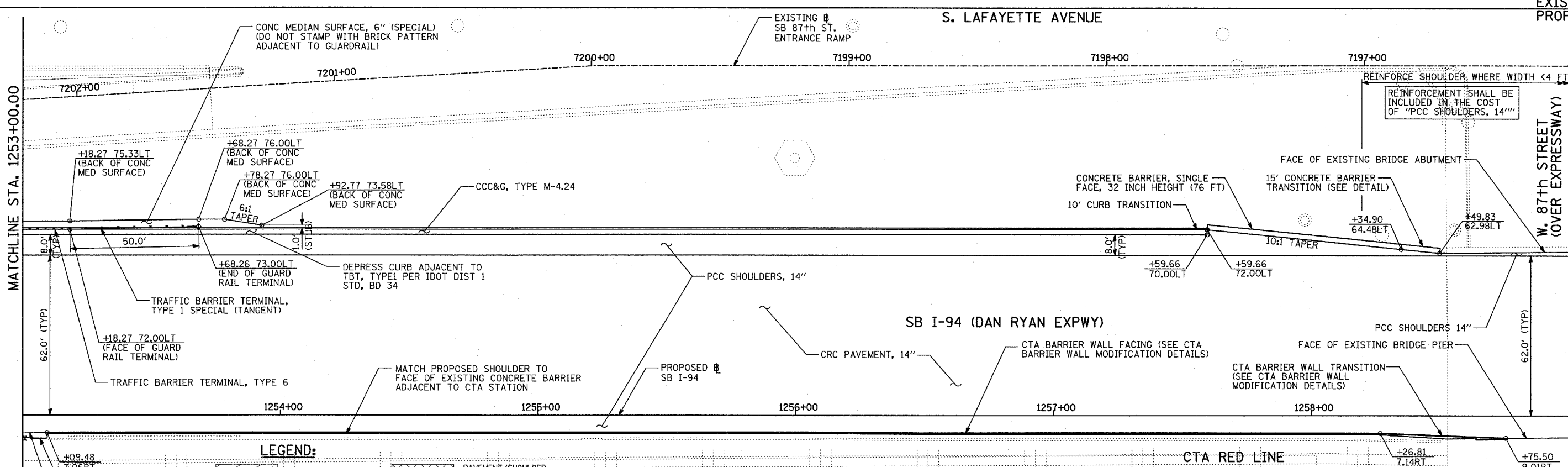
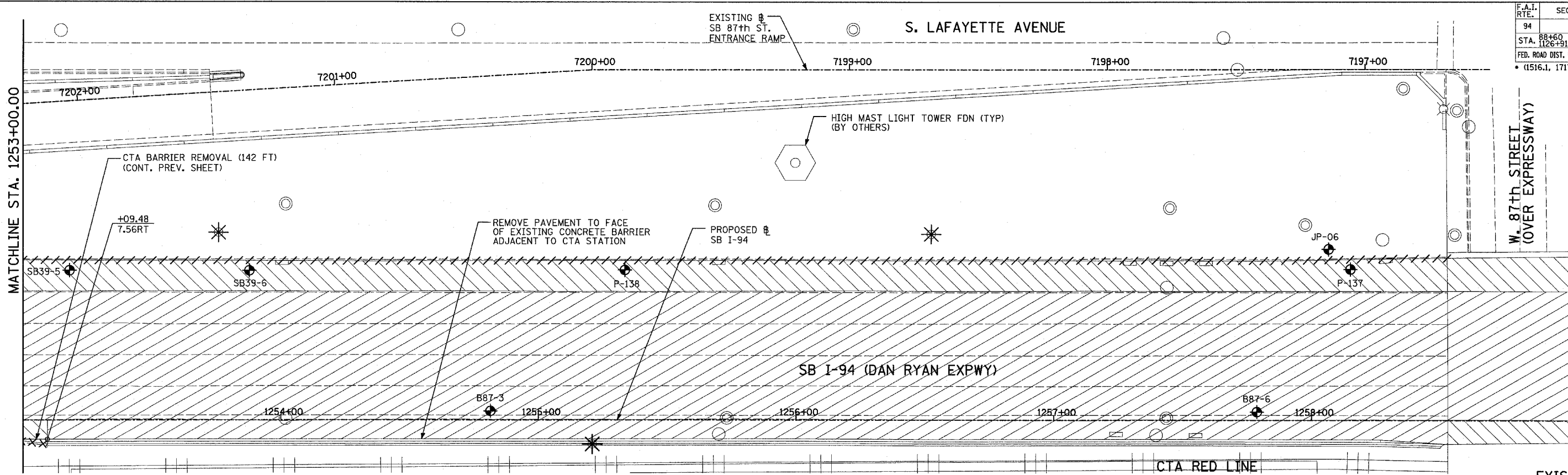
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1247+00.00 TO 1253+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM



TYLIN INTERNATIONAL



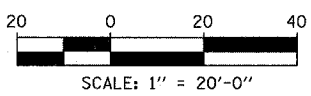
LEGEND:

- | | | | |
|--|------------------------------------|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

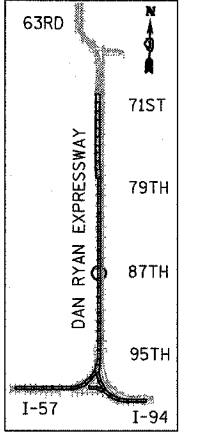


REVISIONS	
NAME	DATE

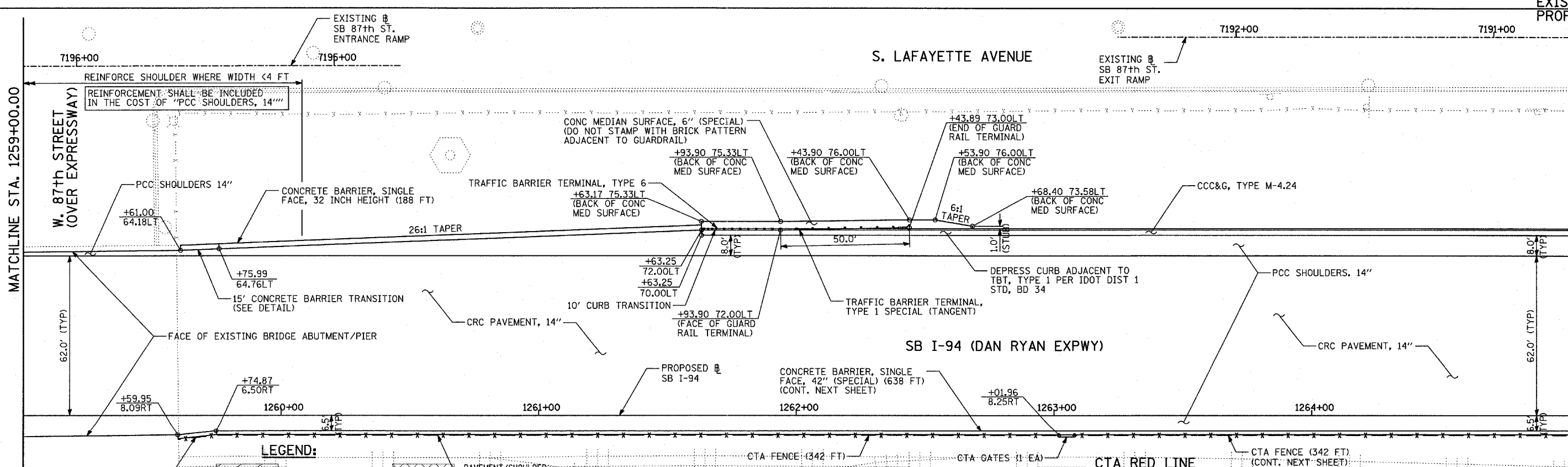
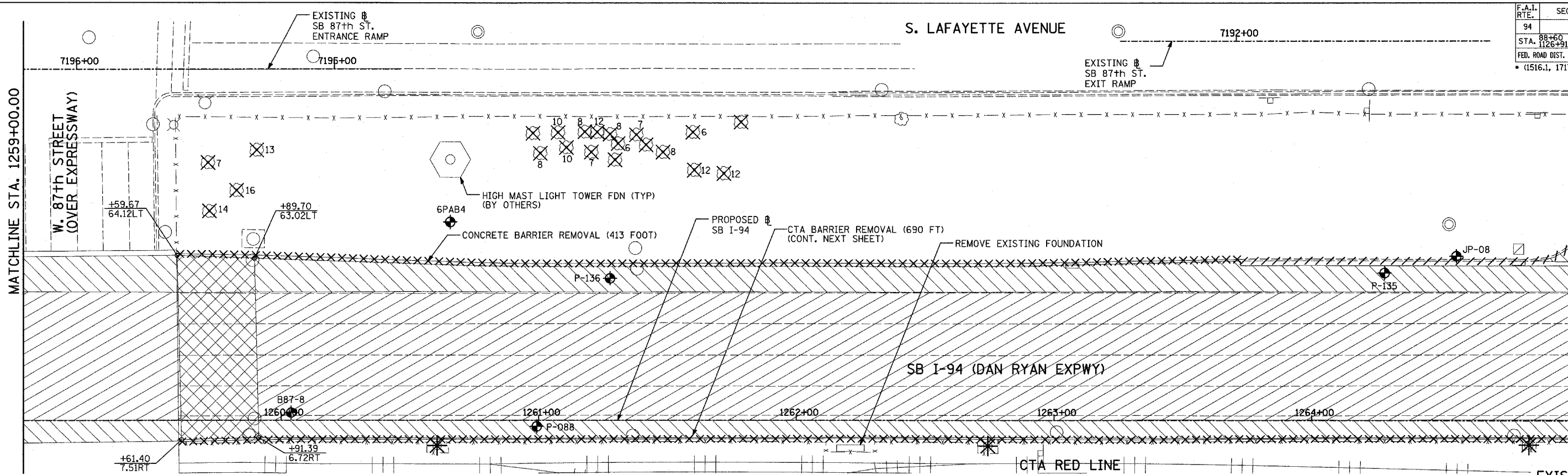
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1253+00.00 TO 1259+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM



TYLIN INTERNATIONAL

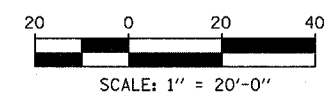


LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL		MAINLINE #
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL		STATION/OFFSET
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL		SOIL BORING LOCATIONS
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

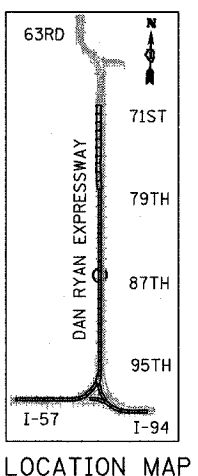


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1259+00.00 TO 1265+00.00

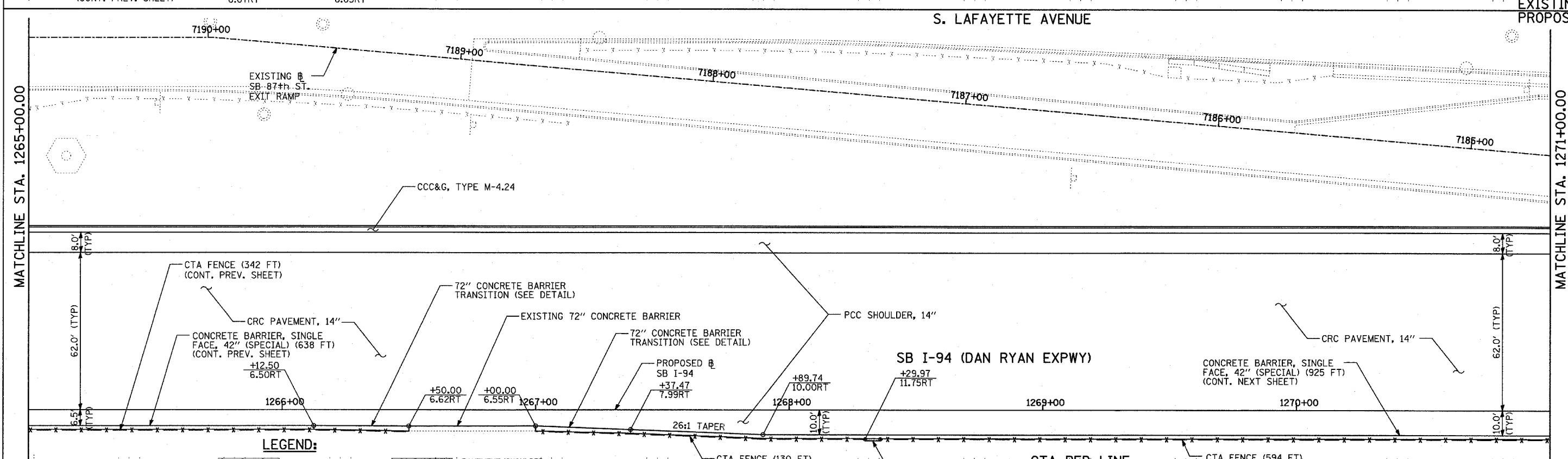
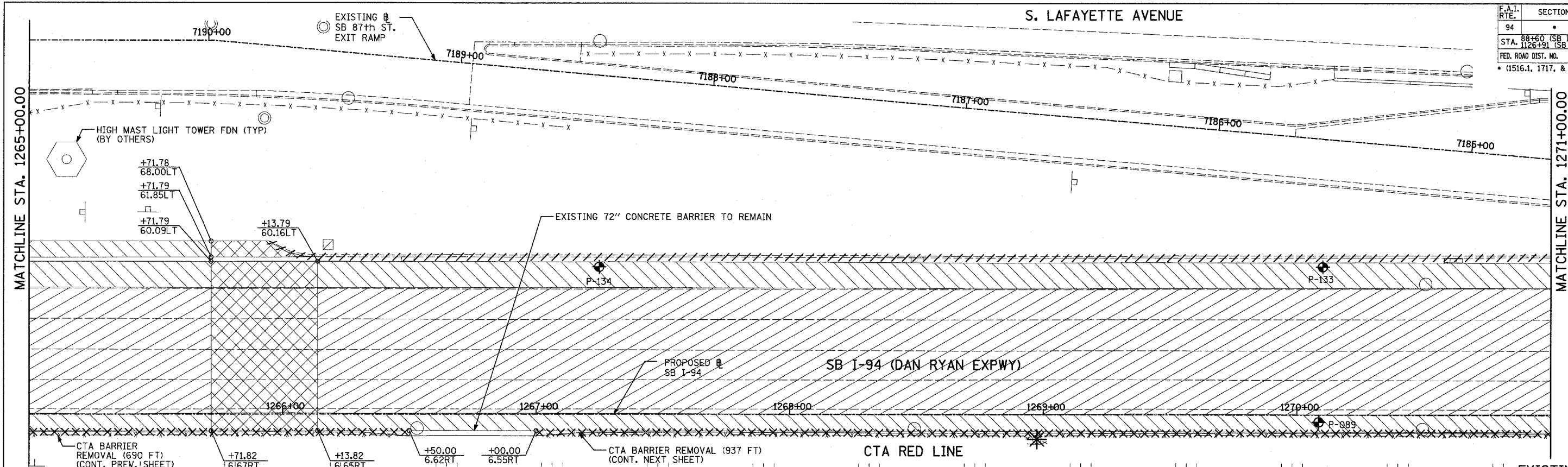
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 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM



TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	88
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				
				62593



LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

PLAN NOTES:

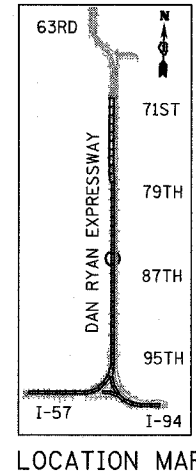
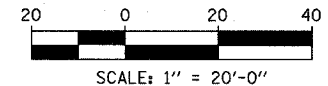
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

REVISIONS	
NAME	DATE

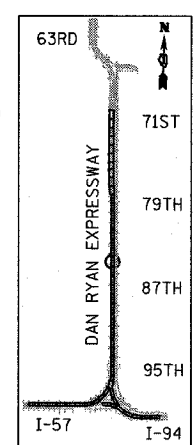
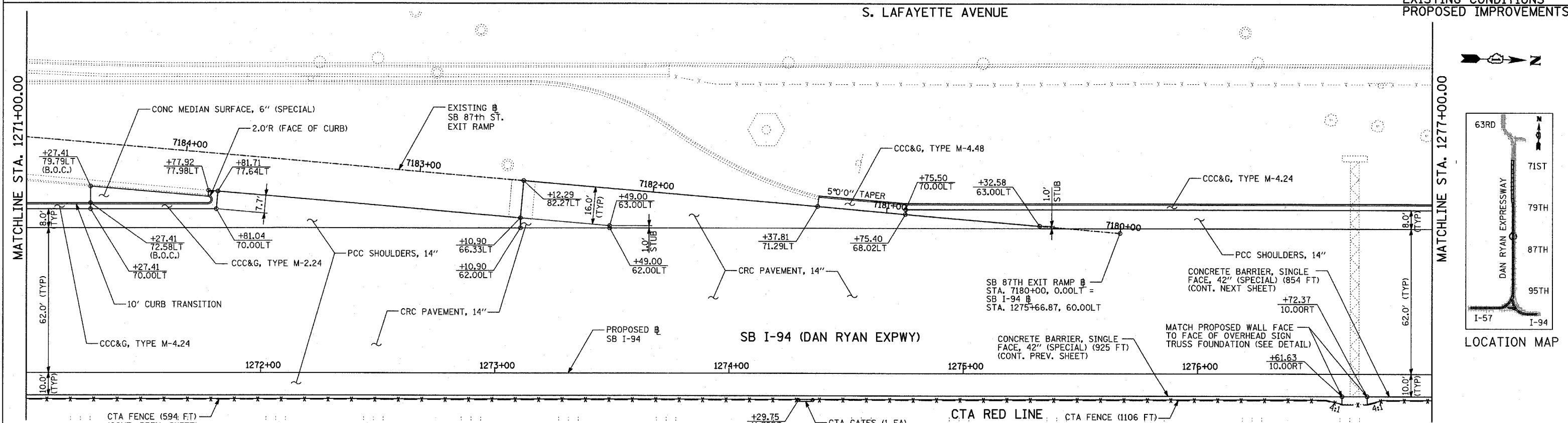
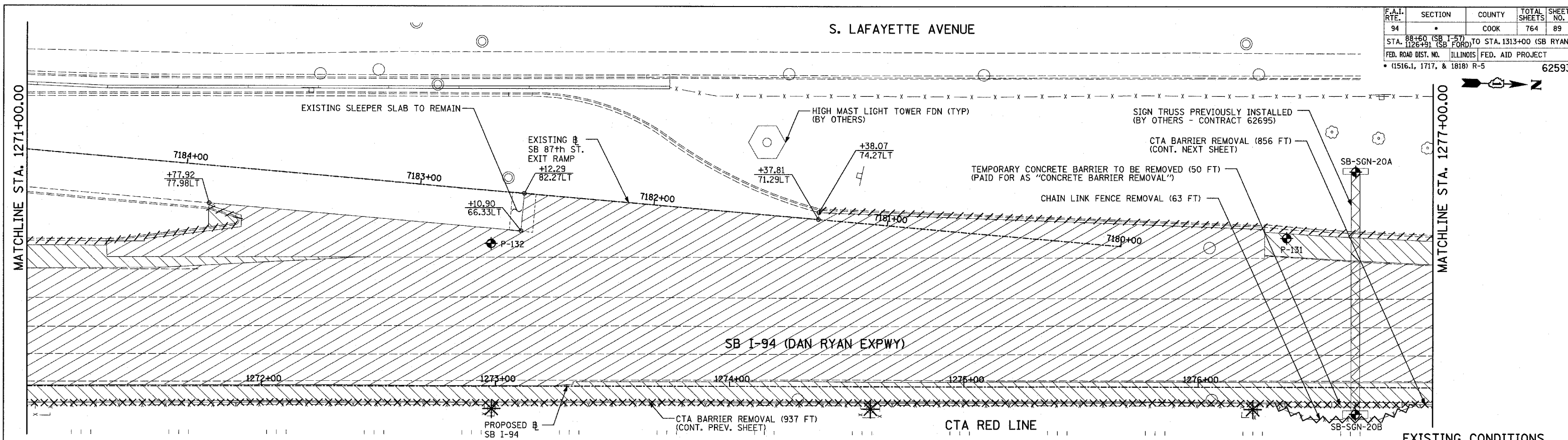
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1265+00.00 TO 1271+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: RTM

TYLIN INTERNATIONAL



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	89
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				



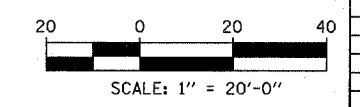
LOCATION MAP

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE # STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 SB I-94 STA. 1271+00.00 TO 1277+00.00

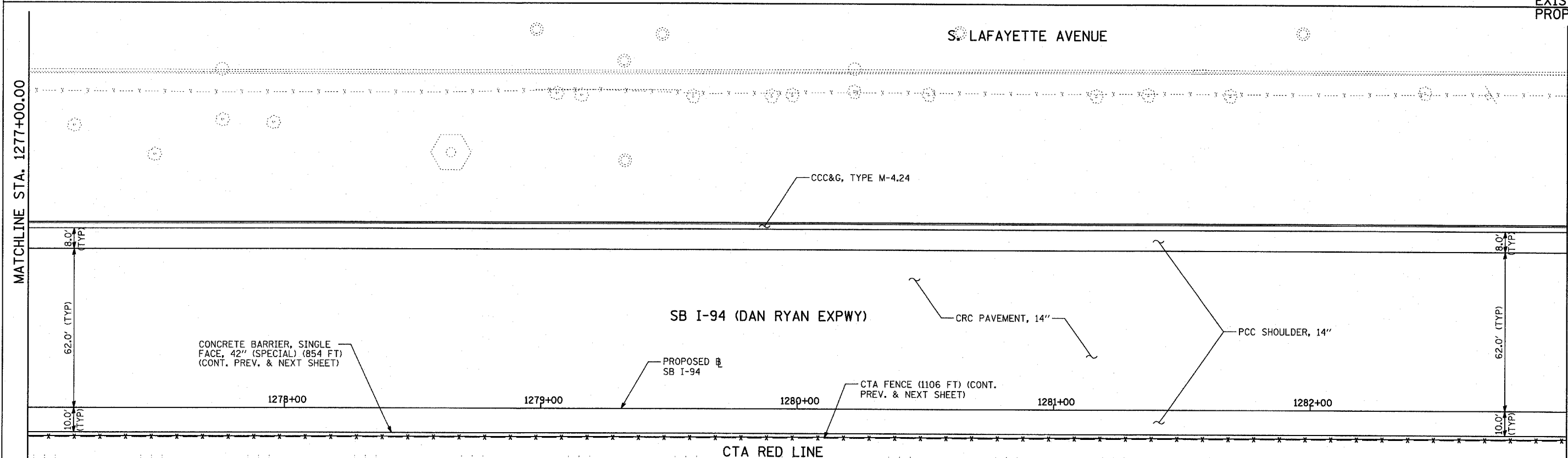
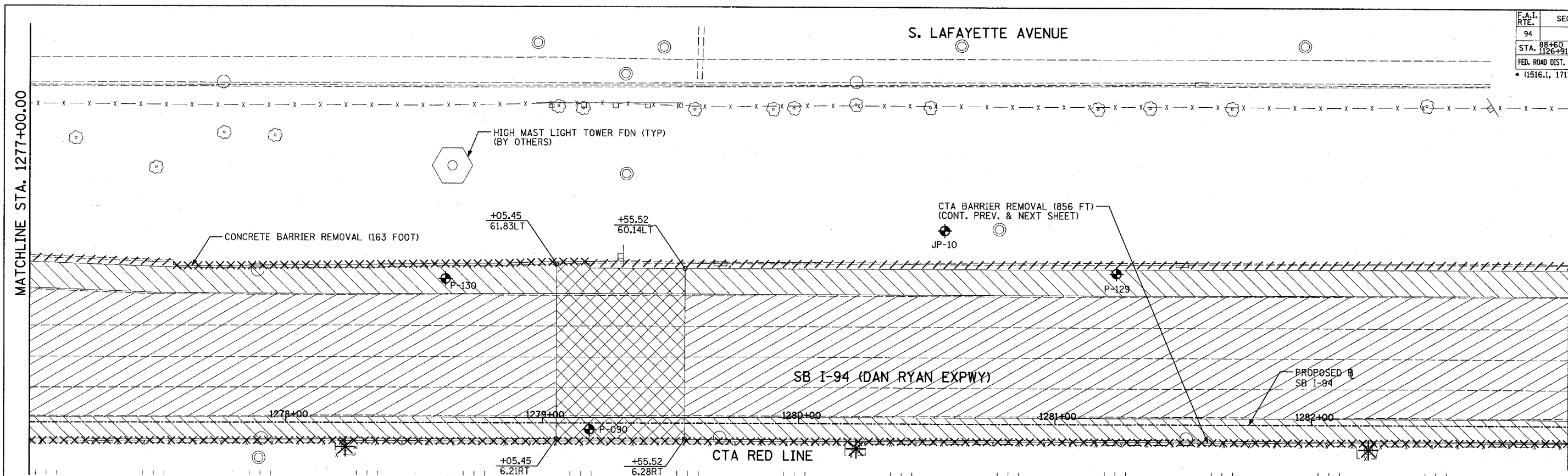
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 DATE: MARCH 1, 2006

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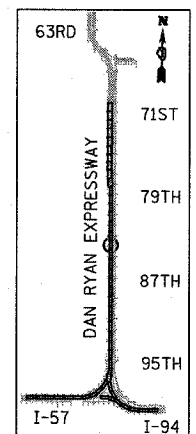
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	90
STA. 126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				

62593



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



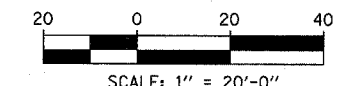
LOCATION MAP

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE B STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

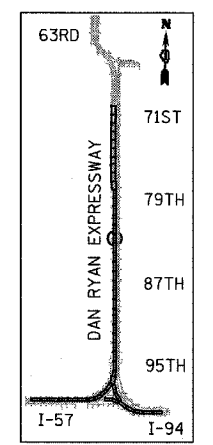
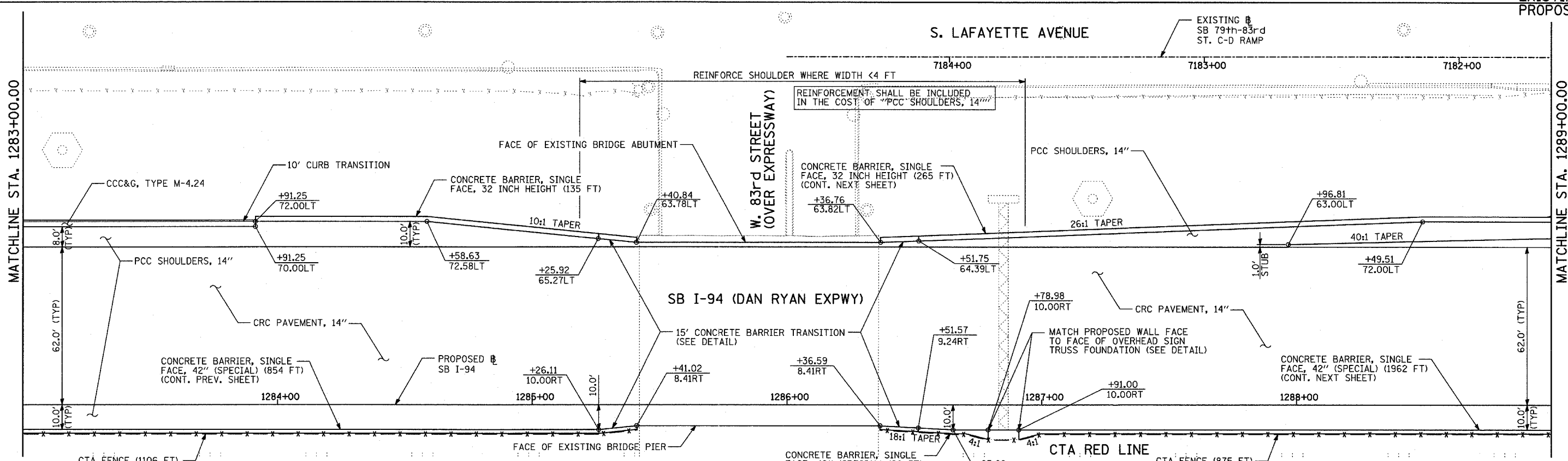
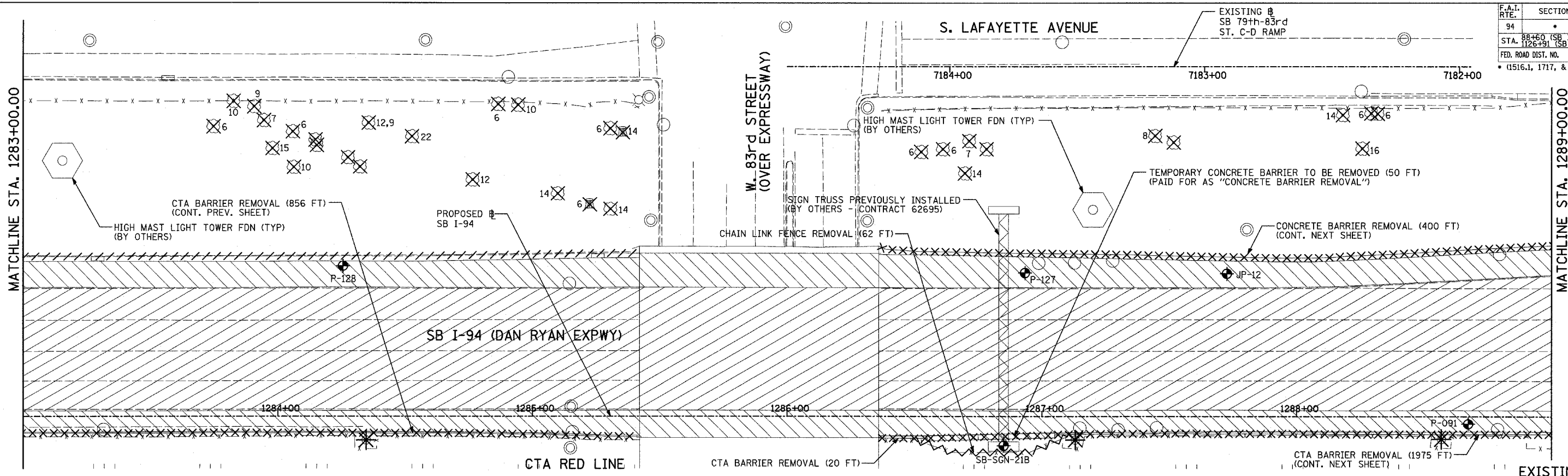
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1277+00.00 TO 1283+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: MPG
 CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	91
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5 62593				

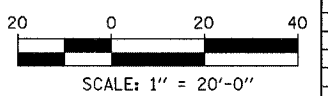


LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE # STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

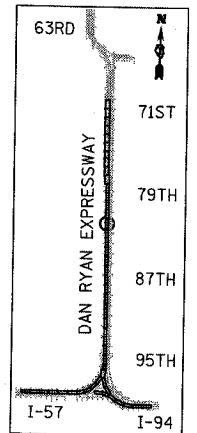
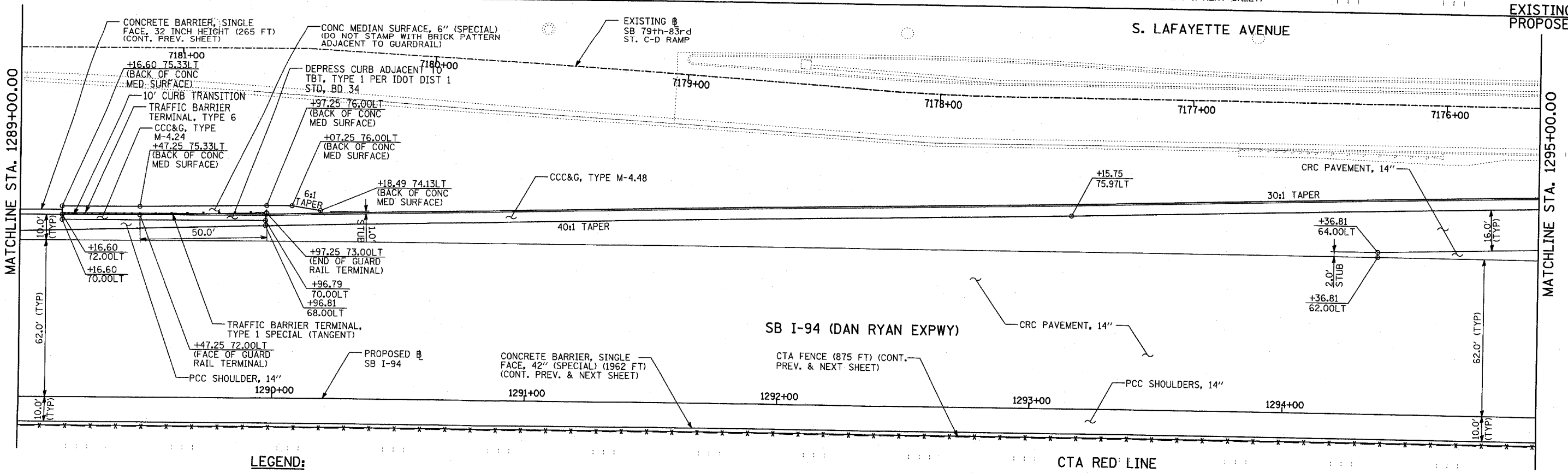
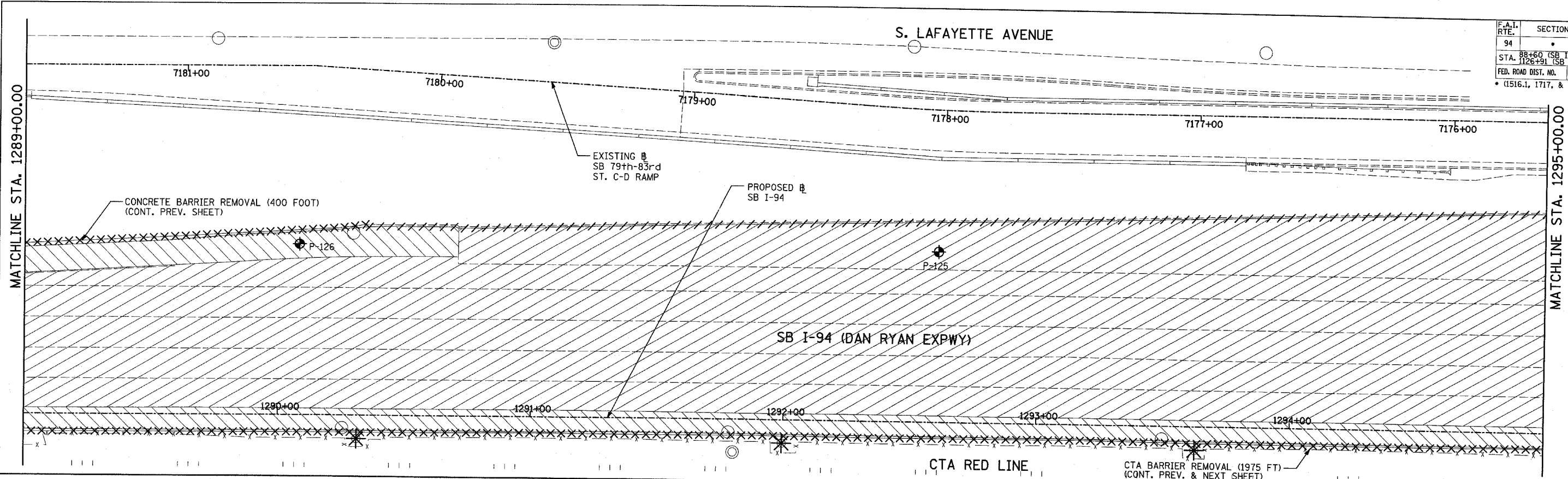
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1283+00.00 TO 1289+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	92
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				
				62593

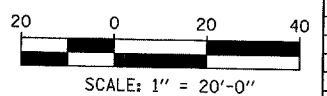


LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | PAVED SHOULDER REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
| | BITUMINOUS SURFACE REMOVAL, 4" | | COMB CONC CURB & GUTTER REMOVAL | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CHAIN LINK FENCE REMOVAL | | MAINLINE STATION/OFFSET |
| | | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | |

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1289+00.00 TO 1295+00.00

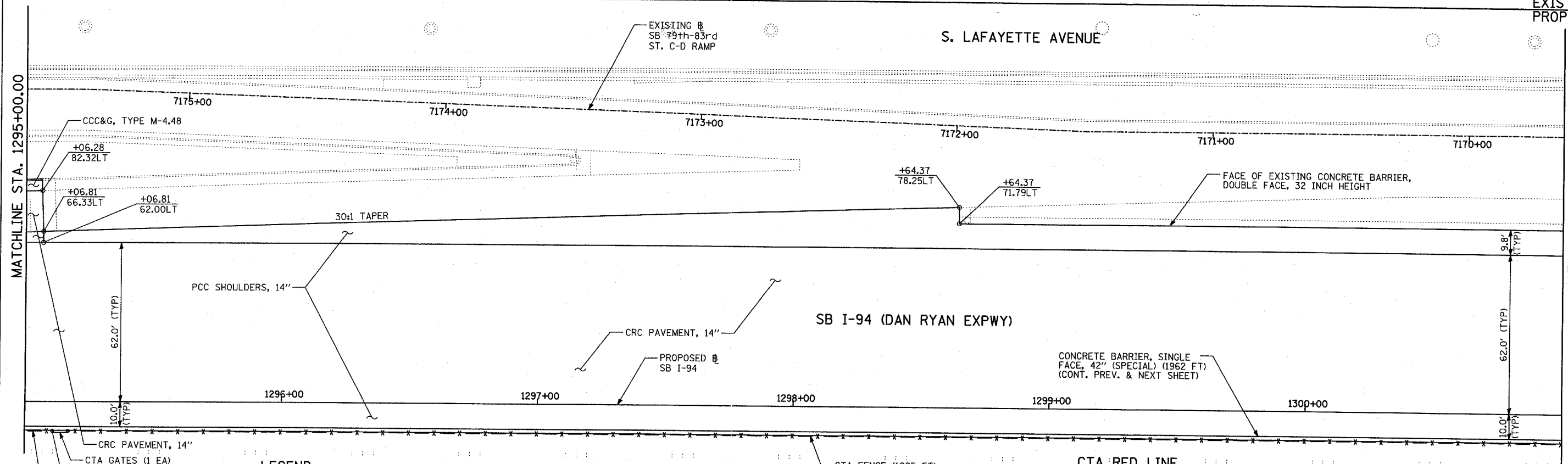
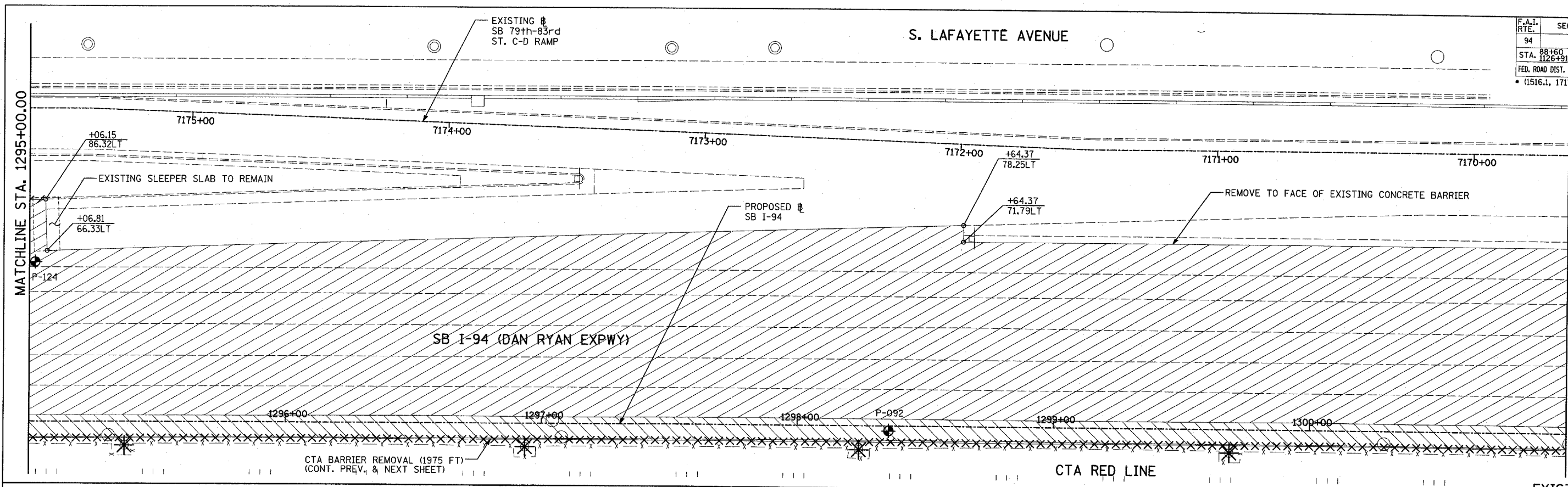
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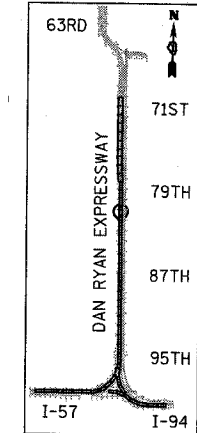
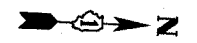
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	93
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				

62593



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

TYLIN INTERNATIONAL

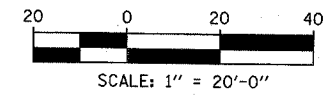
LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/2"
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL
			CONCRETE BARRIER/GUARDRAIL REMOVAL

	SOIL BORING LOCATIONS
	LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = 6)
	MAINLINE STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



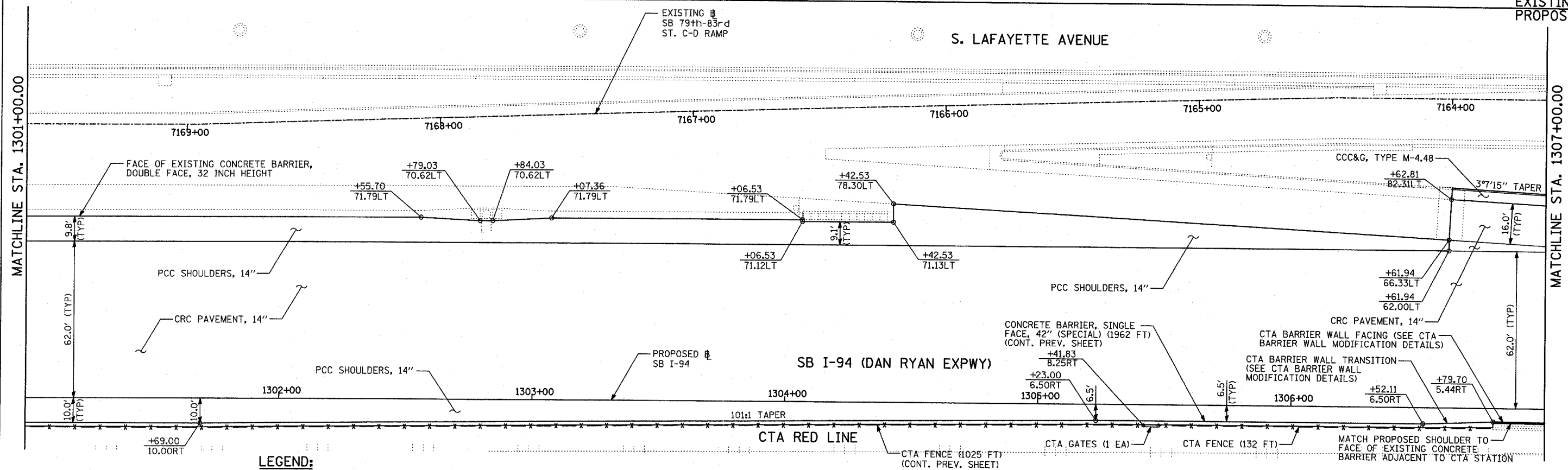
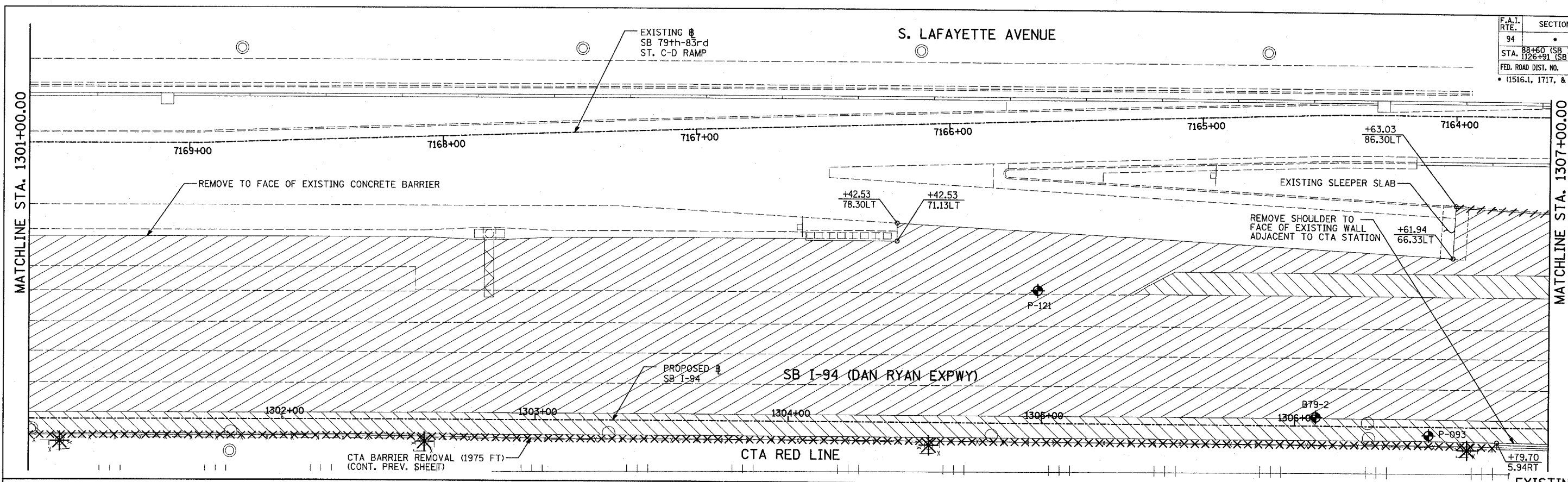
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1295+00.00 TO 1301+00.00

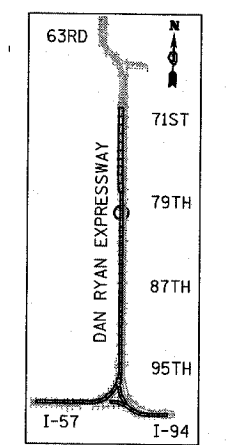
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DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	94
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



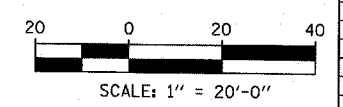
LOCATION MAP

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

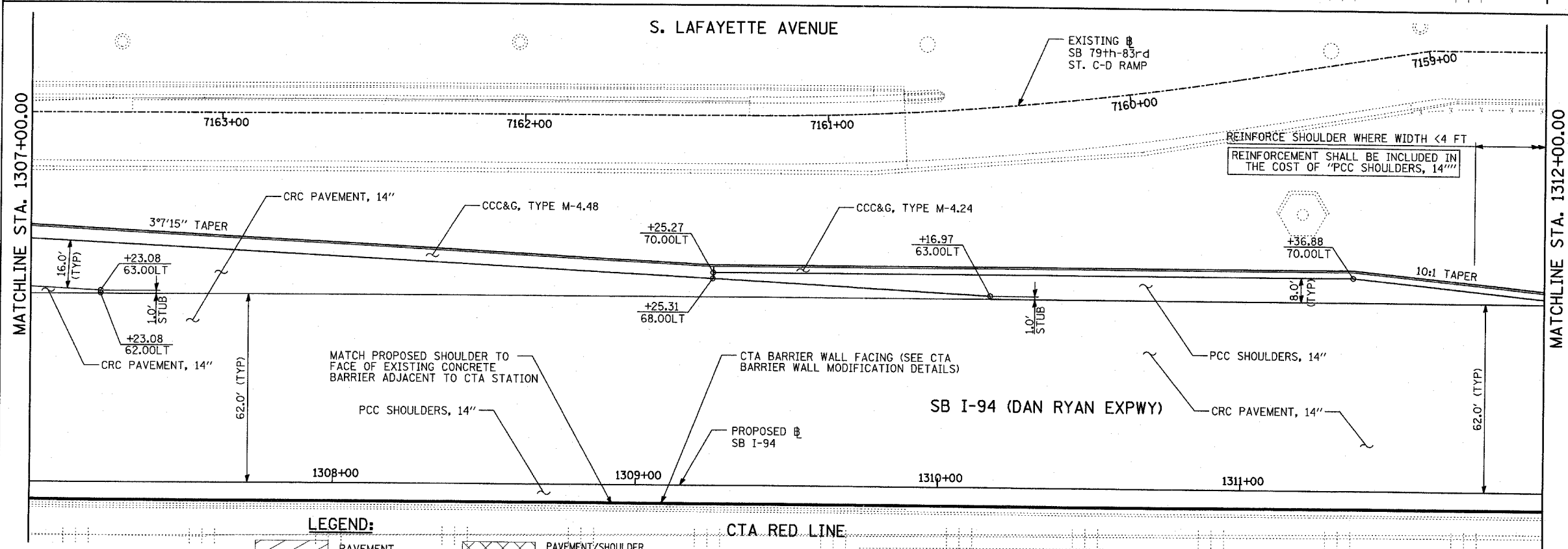
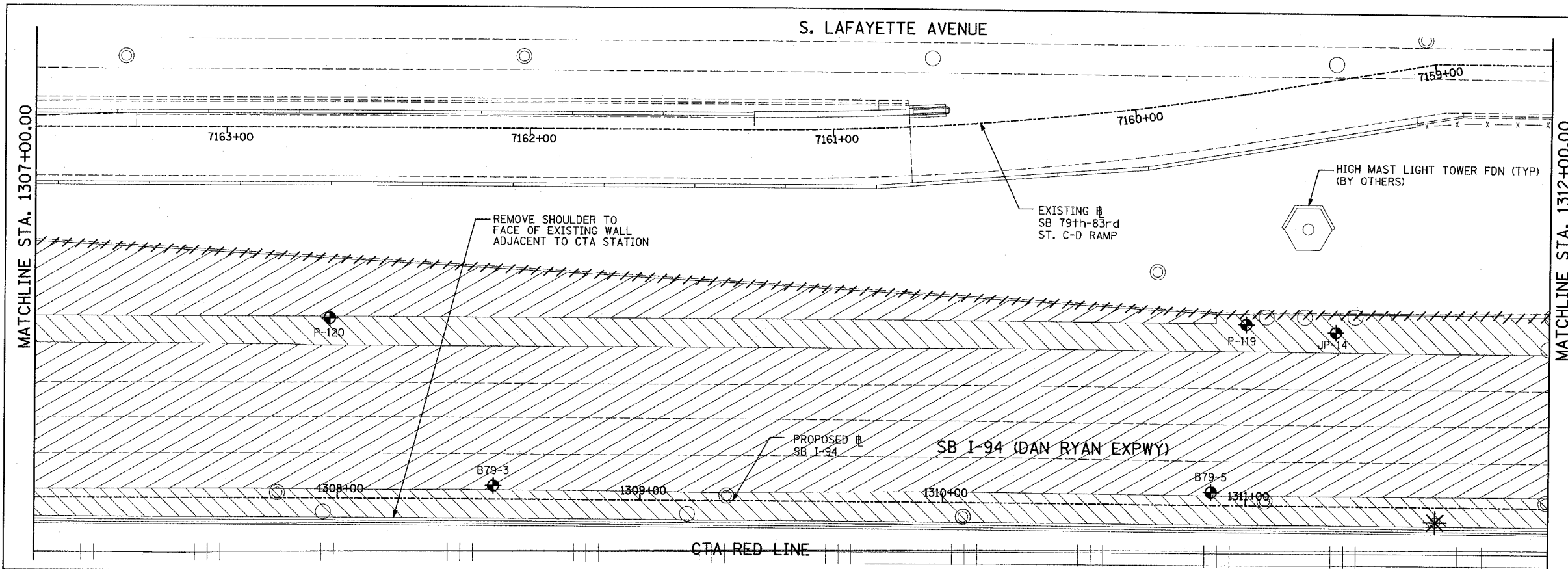
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1301+00.00 TO 1307+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

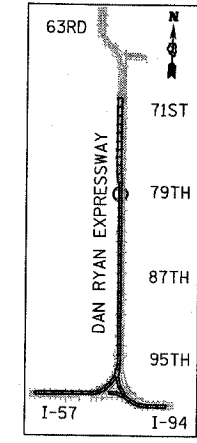
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TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	95
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1125+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

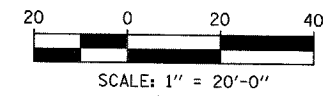
LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) OVER EXISTING UTILITIES
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

CTA RED LINE

- TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
 - +XX.XX MAINLINE #
 - XX.XXRT STATION/OFFSET
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

TYLIN INTERNATIONAL



REVISIONS	
NAME	DATE

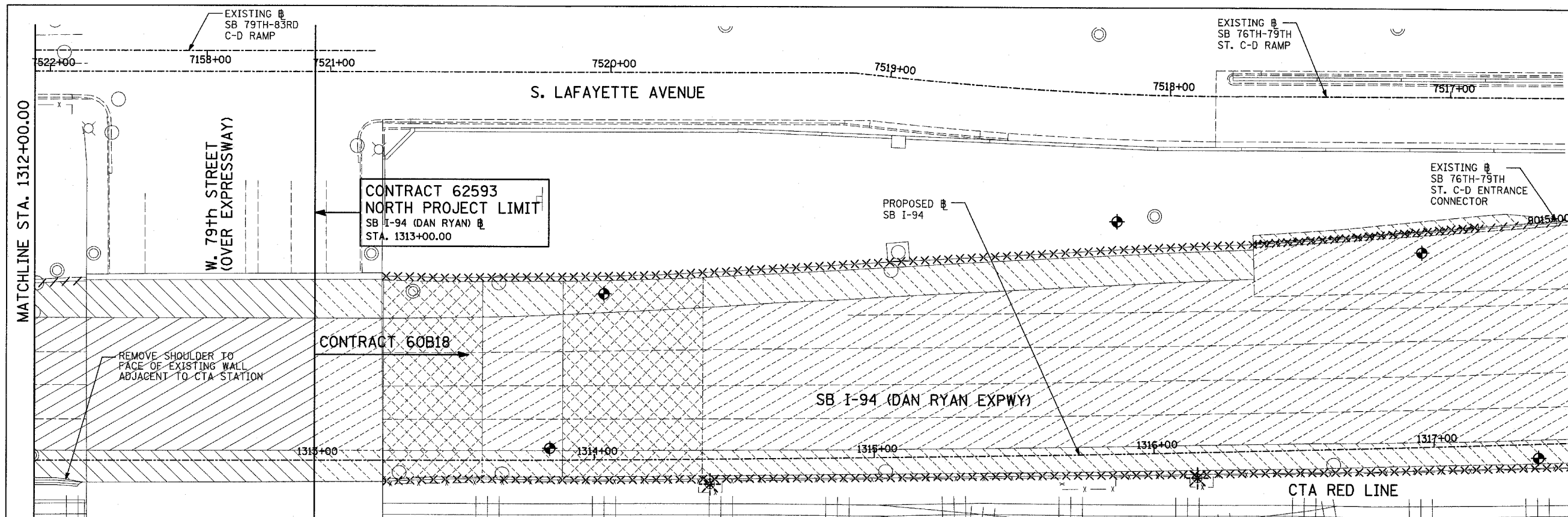
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1307+00.00 TO 1312+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

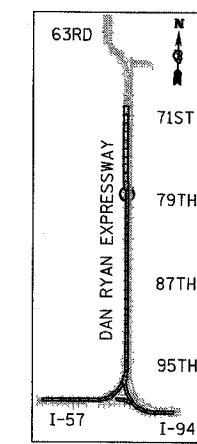
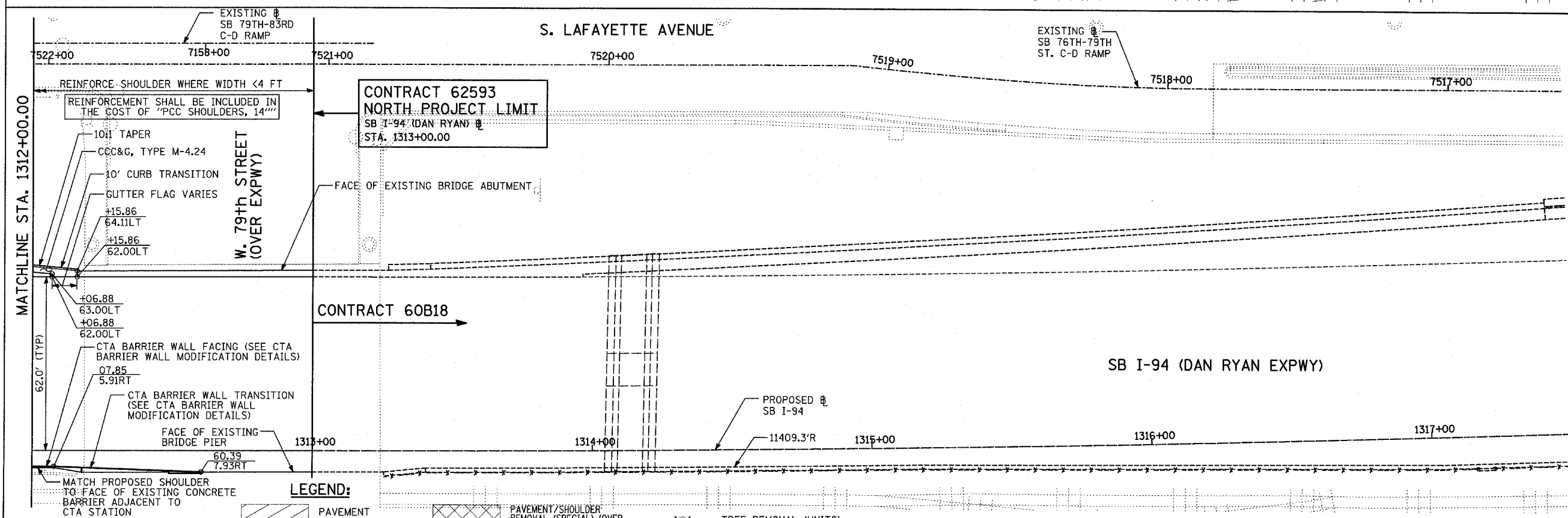
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	96
STA. 88+60 (SB I-57) TO STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				

62593



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



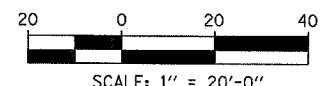
LOCATION MAP

LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SOULDER REMOVAL (SPECIAL) OVER EXISTING UTILITIES | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE # |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS | | |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | |

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED



REVISIONS	
NAME	DATE

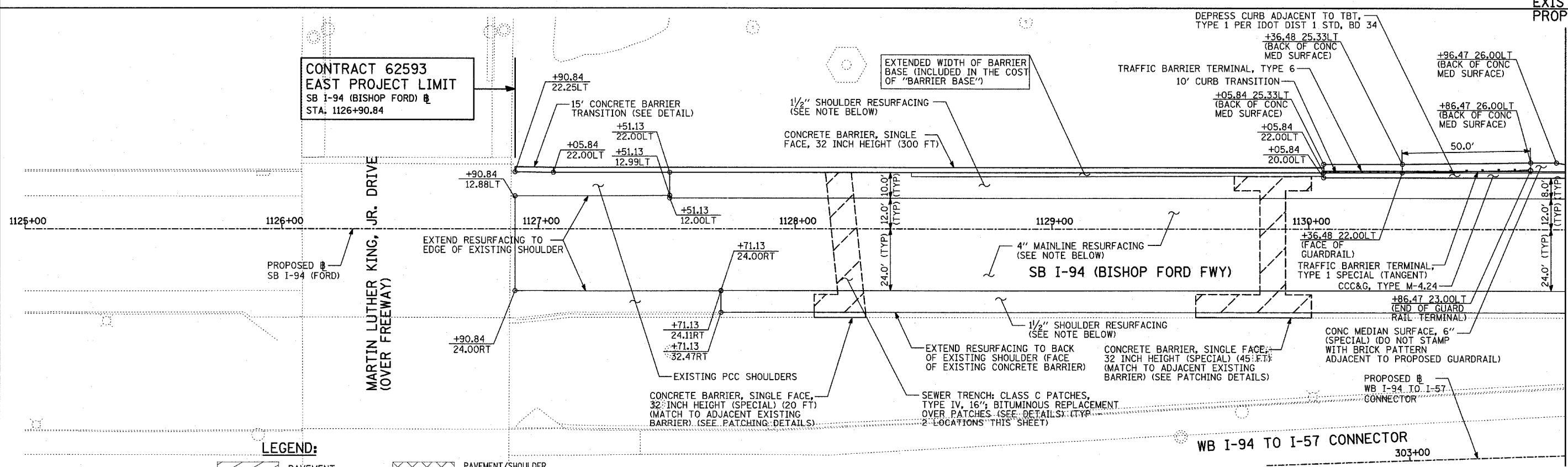
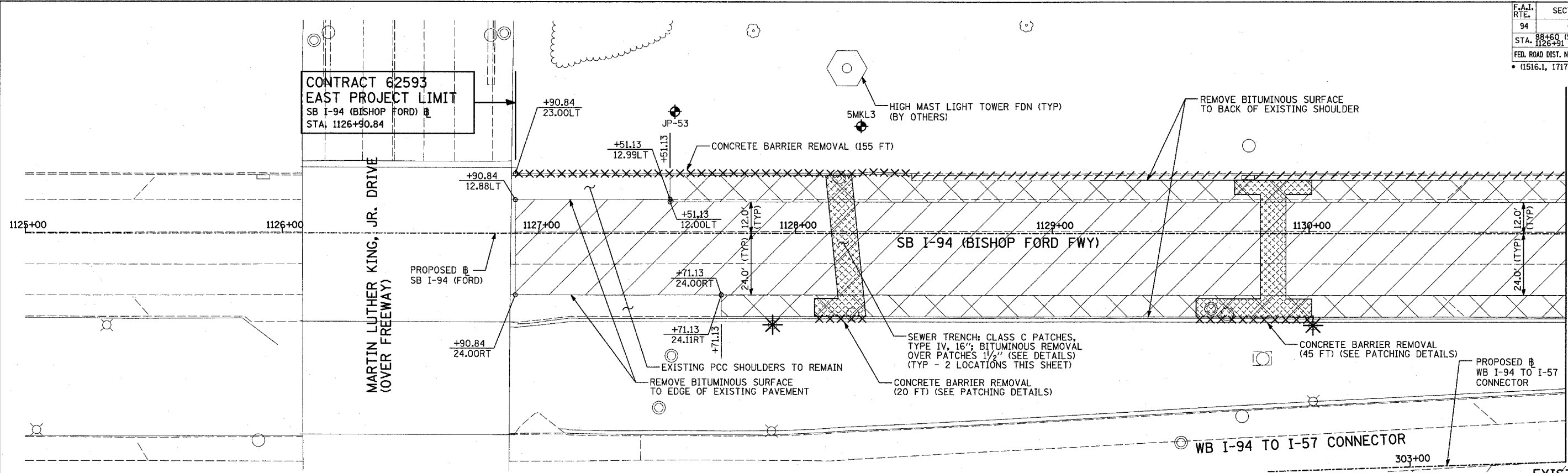
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
SB I-94 STA. 1312+00.00 TO 1313+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

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CHECKED BY: RTM

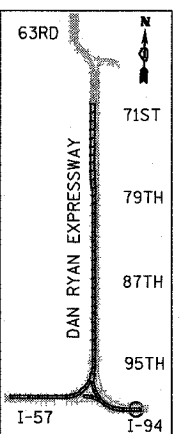
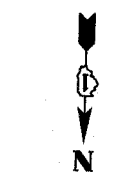
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	97
STA. 88+60 (SB I-57) TO STA. 1126+91 (SB FORD) TO STA. 1131+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5				



MATCHLINE STA. 1131+00.00

MATCHLINE STA. 1131+00.00



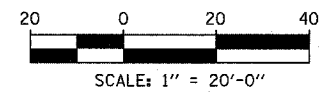
SHEET 34 OF 42

LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL		MAINLINE STATION/OFFSET
	BITUMINOUS SURFACE REMOVAL, 4"		CHAIN LINK FENCE REMOVAL		PLAN NOTES:
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CONCRETE BARRIER/GUARDRAIL REMOVAL		- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
	BITUMINOUS SURFACE REMOVAL, 1 3/4"		SOIL BORING LOCATIONS		- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)		

NOTES:

- 4" MAINLINE RESURFACING: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 1/4" POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- 1 1/2" SHOULDER RESURFACING: BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"

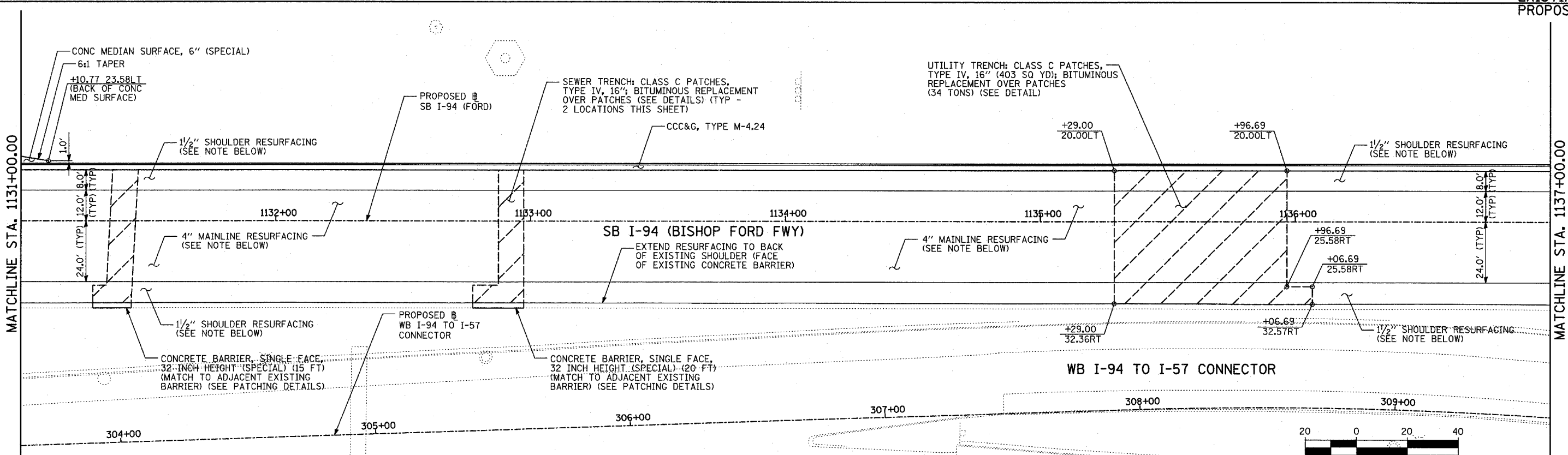
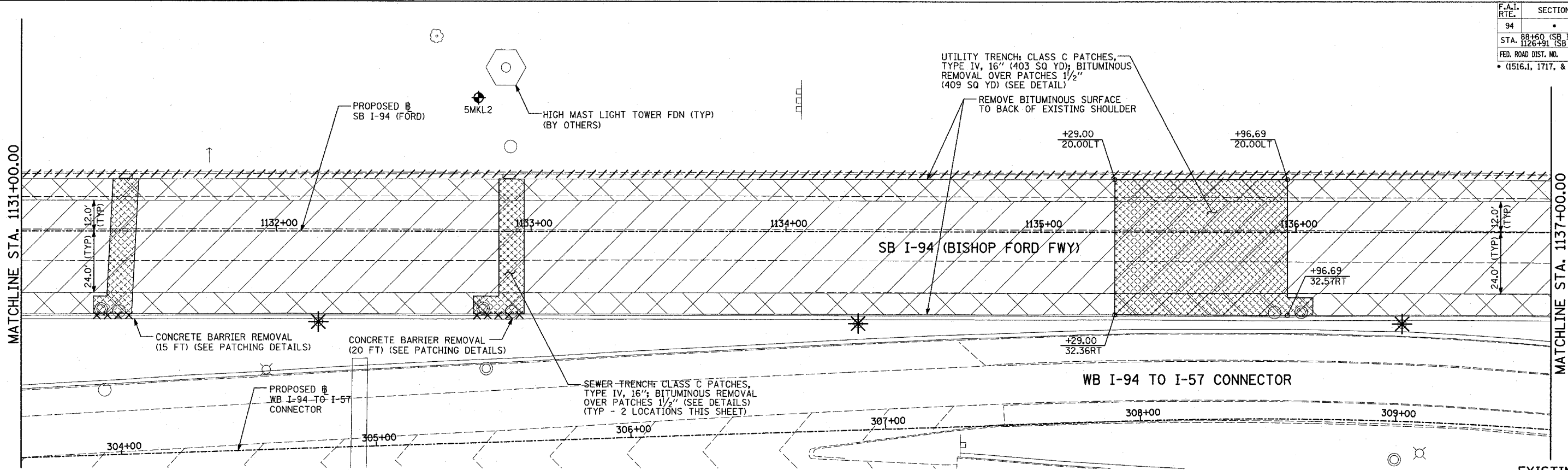


REVISIONS	
NAME	DATE

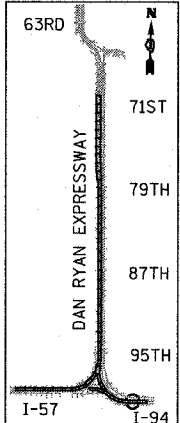
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (BISHOP FORD FREEWAY)
 BISHOP FORD STA. 1126+90.84 TO 1131+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: RTM

TYLIN INTERNATIONAL



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP



TYLIN INTERNATIONAL

LEGEND:

	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 3/4"
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL
			CONCRETE BARRIER/GUARDRAIL REMOVAL

	SOIL BORING LOCATIONS
	LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	MAINLINE # STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

4" MAINLINE RESURFACING:
POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"

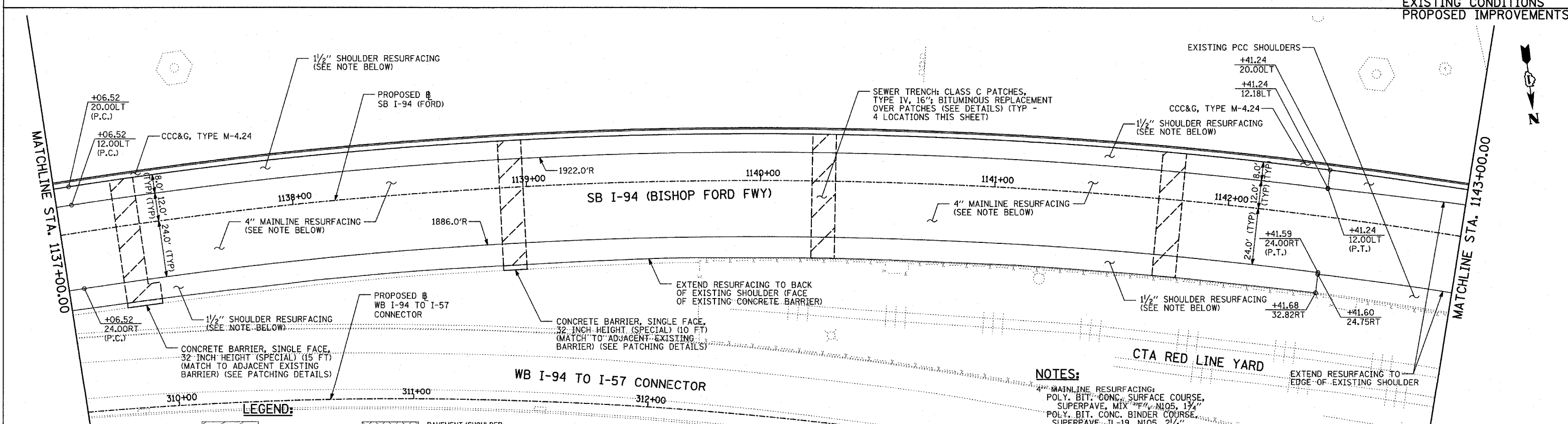
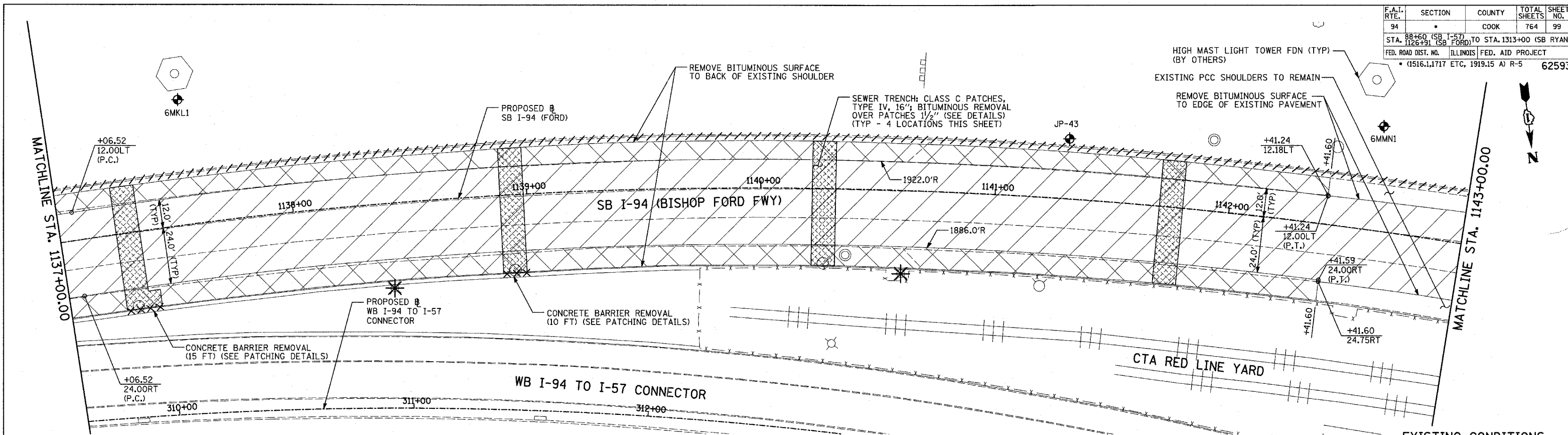
1/2" SHOULDER RESURFACING:
BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N105, 1 1/2"

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
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SB I-94 (BISHOP FORD FREEWAY)
BISHOP FORD STA. 1131+00.00 TO 1137+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

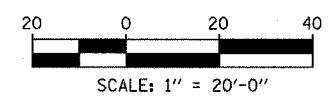
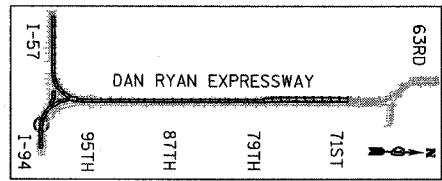


LEGEND:

- | | | | | | |
|--|------------------------------------|--|---|--|--|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | MAINLINE STATION/OFFSET |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | PLAN NOTES: |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | - ALL STATION/OFFSET/RADIUS CALLO MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED. |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS | | - EXACT LOCATIONS OF EXISTING PIEF ABUTMENTS TO BE FIELD VERIFIED |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) | | |

NOTES:

- 4" MAINLINE RESURFACING: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N105, 1 1/2"
- 1/2" SHOULDER RESURFACING: BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"

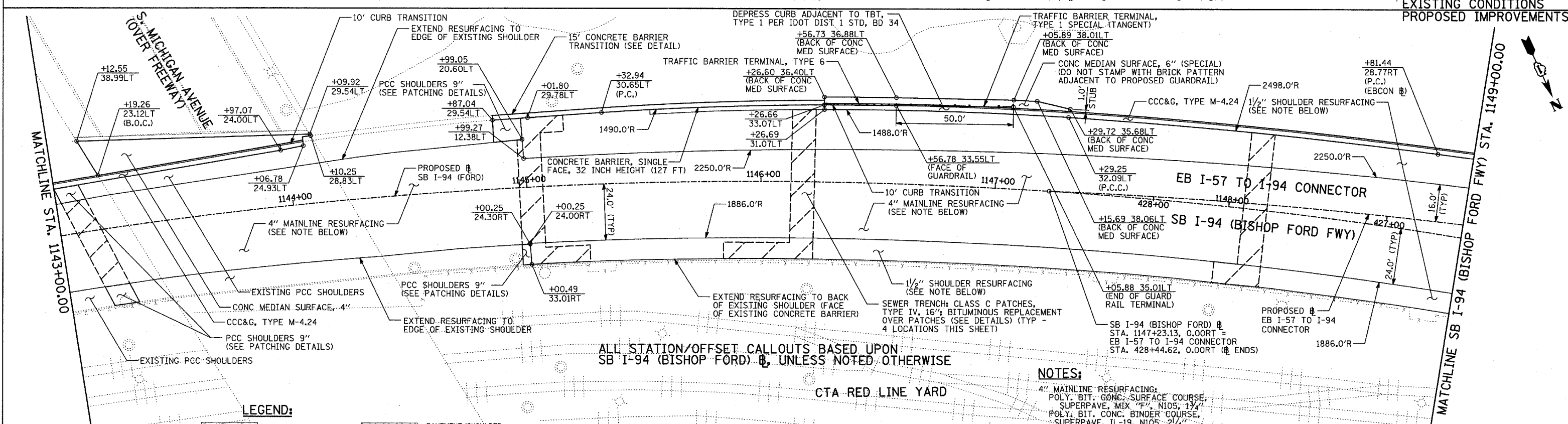
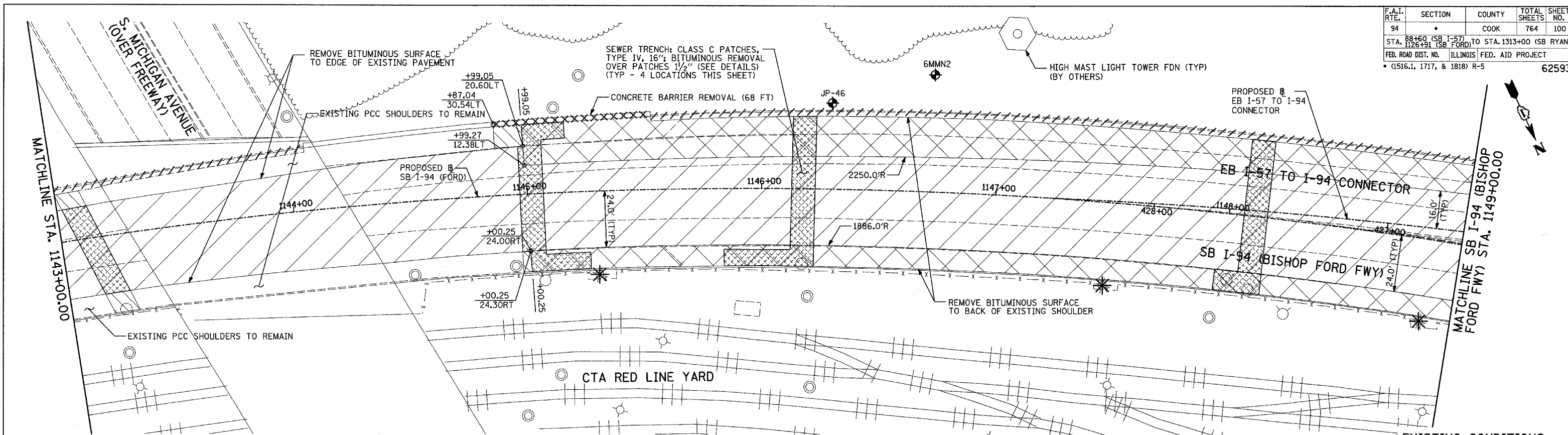


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NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (BISHOP FORD FREEWAY)
BISHOP FORD STA. 1137+00.00 TO 1143+00.00

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	100
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5 62593				



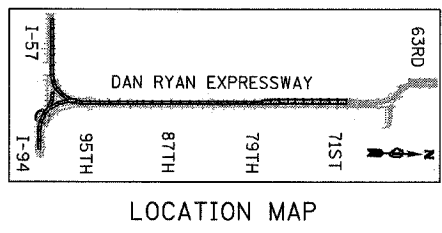
TYLIN INTERNATIONAL

LEGEND:

	PAVEMENT REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) OVER EXISTING UTILITIES
	PAVED SHOULDER REMOVAL		COMB CONC CURB & GUTTER REMOVAL
	BITUMINOUS SURFACE REMOVAL, 4\"/>		CHAIN LINK FENCE REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 1/2\"/>		CONCRETE BARRIER/GUARDRAIL REMOVAL
	BITUMINOUS SURFACE REMOVAL, 1 3/4\"/>		SOIL BORING LOCATIONS
			LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
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NOTES:

4\"/>

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ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (BISHOP FORD FREEWAY)
BISHOP FORD STA. 1143+00.00 TO 1149+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006
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CHECKED BY: RTM