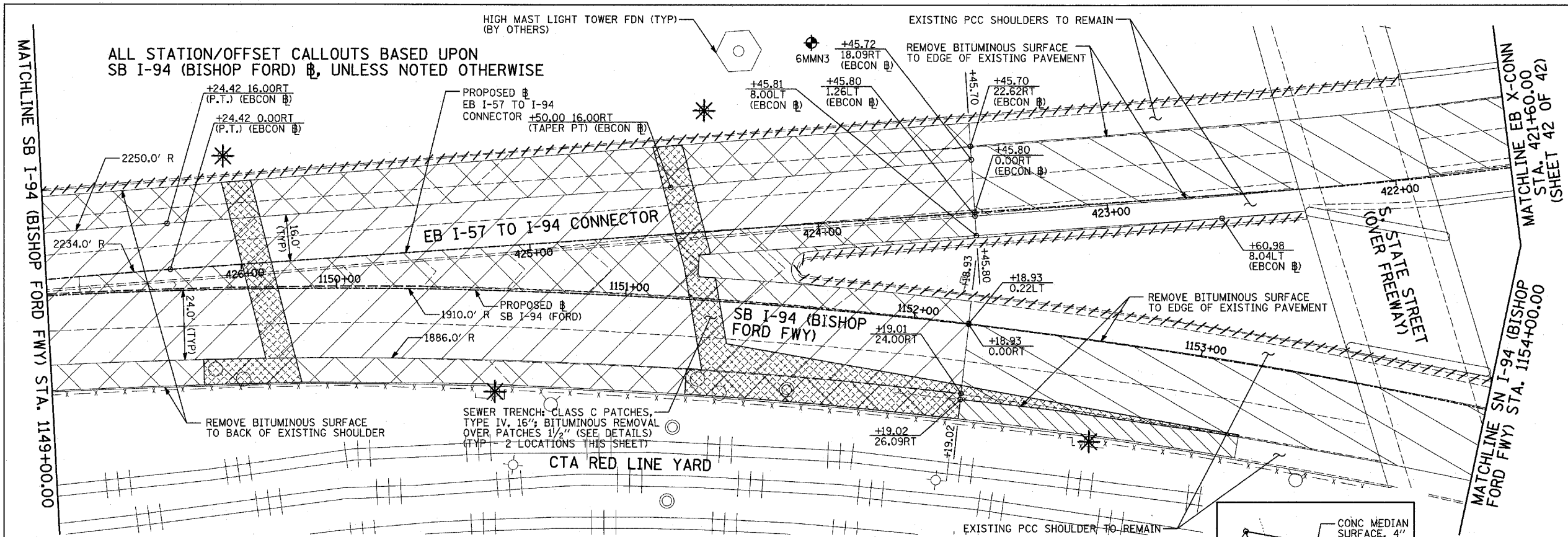
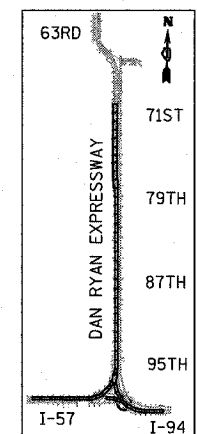
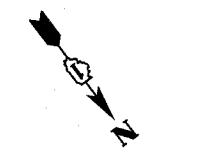
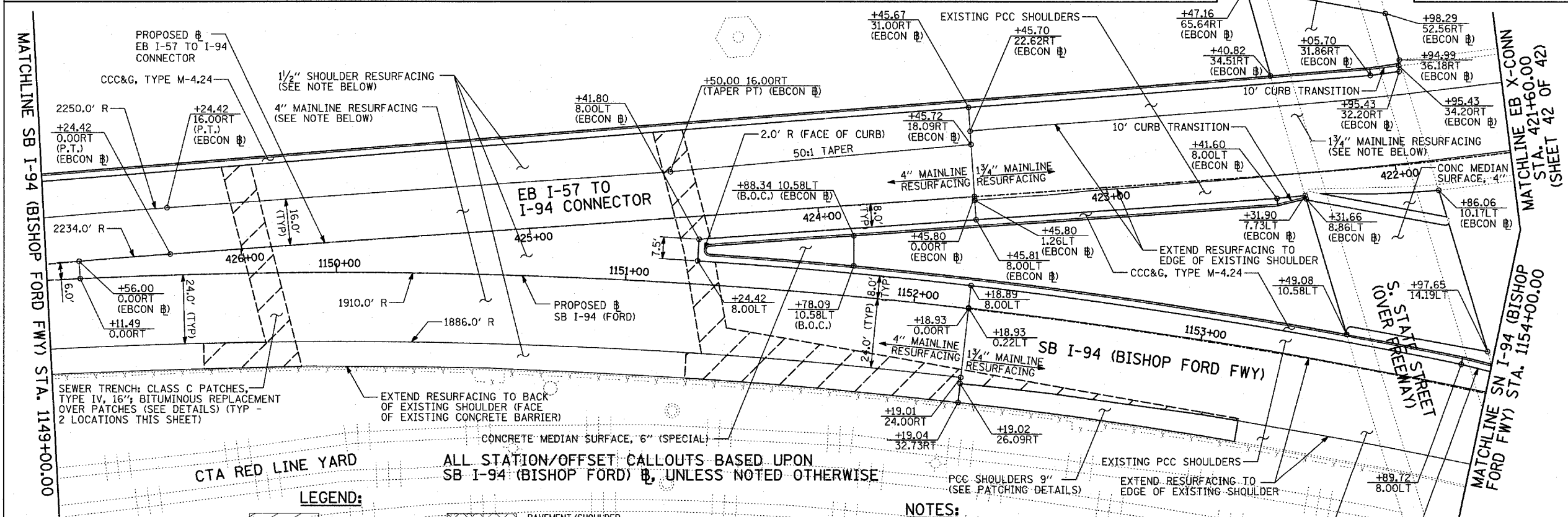


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	101
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				

62593



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

LEGEND:

- PAVEMENT REMOVAL
- PAVED SHOULDER REMOVAL
- BITUMINOUS SURFACE REMOVAL, 4"
- BITUMINOUS SURFACE REMOVAL, 1 1/2"
- BITUMINOUS SURFACE REMOVAL, 1 3/4"
- PAVEMENT/SHOULDER REMOVAL (SPECIAL) OVER EXISTING UTILITIES
- COMB CONC CURB & GUTTER REMOVAL
- CHAIN LINK FENCE REMOVAL
- CONCRETE BARRIER/GUARDRAIL REMOVAL
- SOIL BORING LOCATIONS
- LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)

TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)

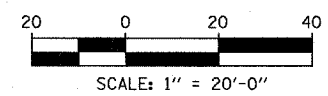
MAINLINE @
 STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

- 4" MAINLINE RESURFACING: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 1/4" POLY. BIT. CONC. BINDER COURSE, SUPERPAVE, IL-19, N105, 2 1/4"
- 1 3/4" MAINLINE RESURFACING: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- 1 1/2" SHOULDER RESURFACING: BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"



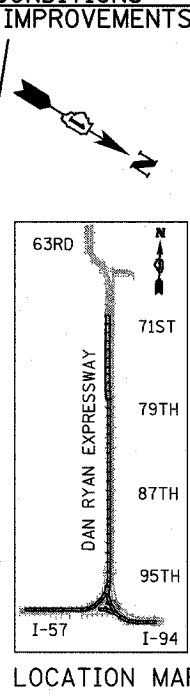
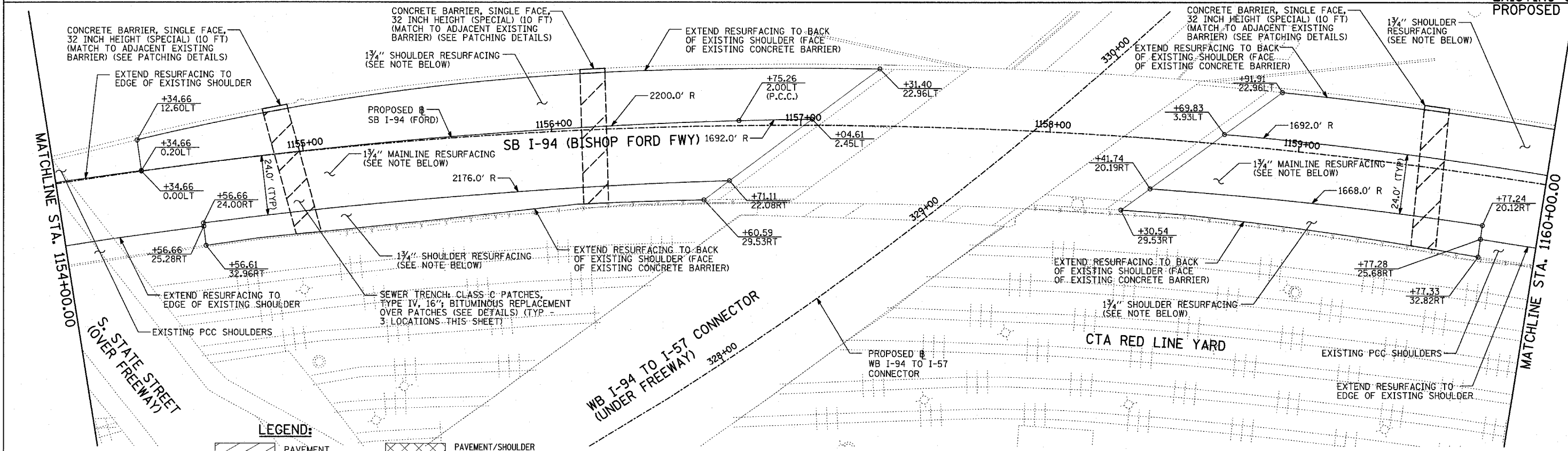
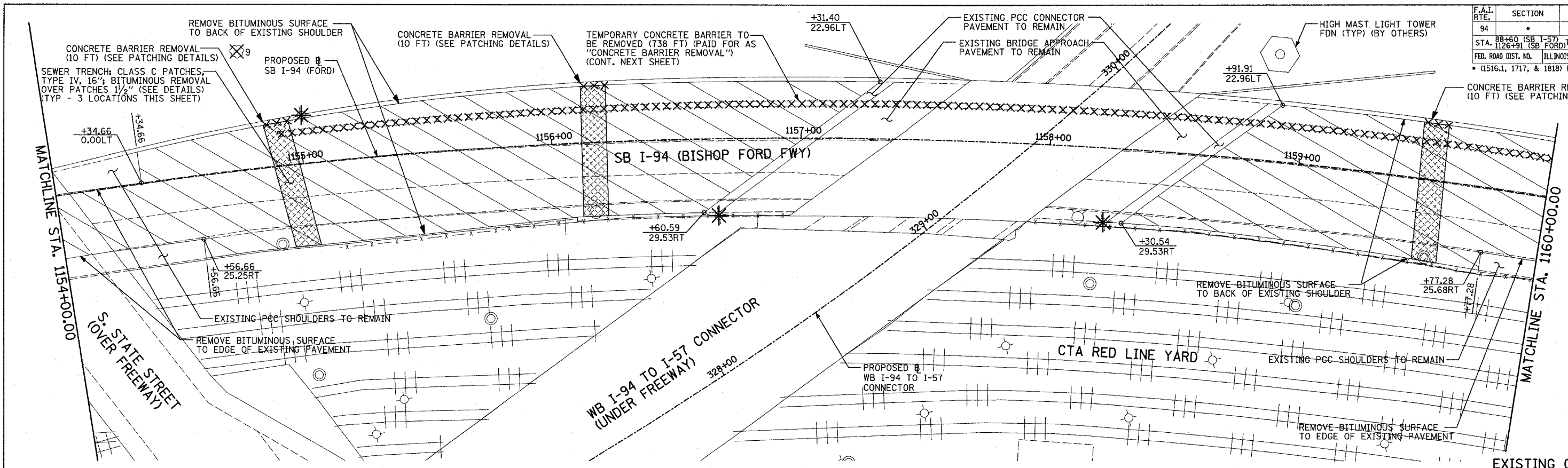
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (FORD) & EB I-57 TO I-94 CONN.
SB I-94 (FORD) STA. 1149+00.00 TO
STA. 1154+00.00 & EB CONN. STA. 421+60.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL



LEGEND:

- | | | | |
|--|------------------------------------|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

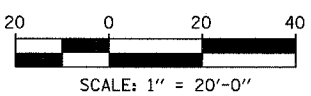
- PLAN NOTES:**
- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 - EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

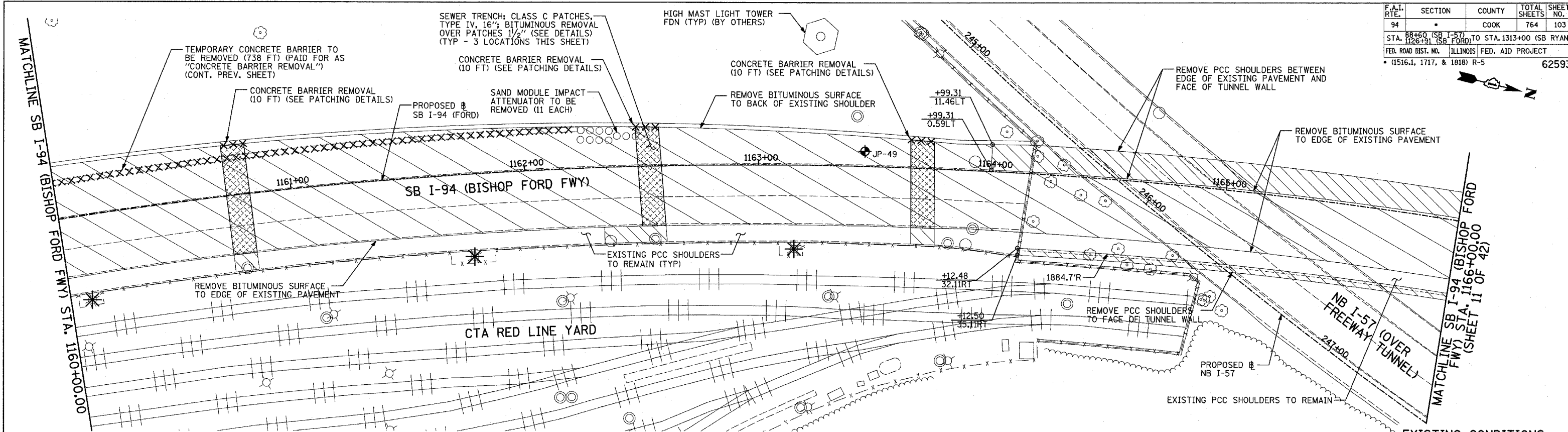
- 1 3/4" MAINLINE RESURFACING POLY, BIT, CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- 1 3/4" SHOULDER RESURFACING: BIT, CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N10, 1 1/2"

REVISIONS	
NAME	DATE

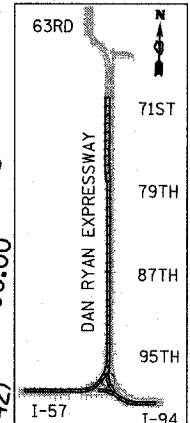
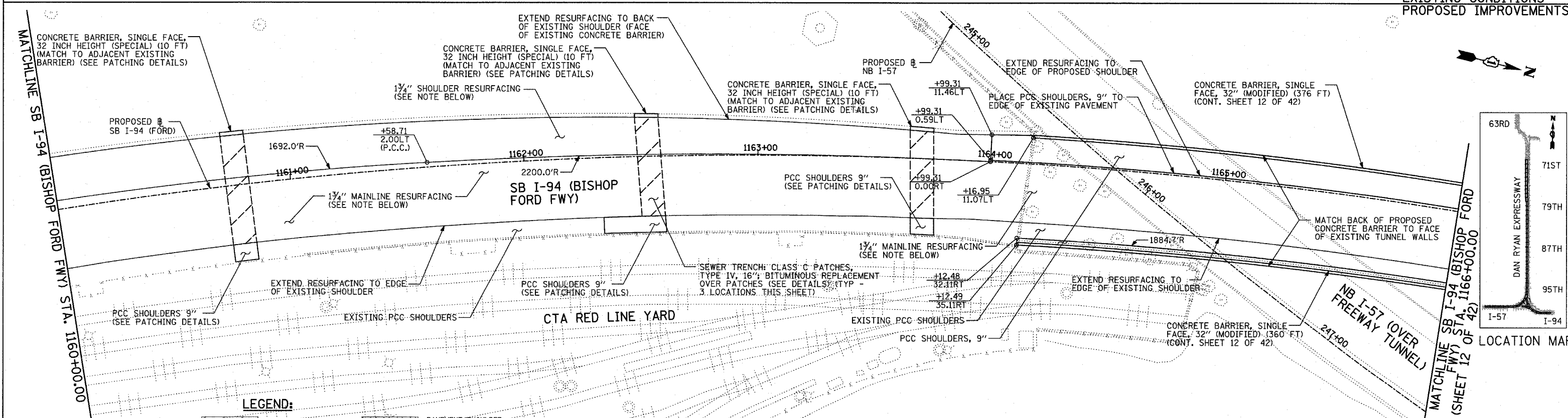
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PLAN
 SB I-94 (BISHOP FORD FREEWAY)
 BISHOP FORD STA. 1154+00.00 TO 1160+00.00



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	103
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				
				62593



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

LEGEND:

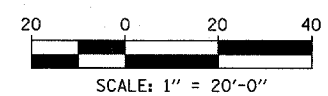
- | | | | | | |
|--|------------------------------------|--|---|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) | | SOIL BORING LOCATIONS |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL | | |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL | | |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | | | |
| | | | | | TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6) |
| | | | | | MAINLINE @ STATION/OFFSET |

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

- 1 3/4" MAINLINE RESURFACING: POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"
- 1 3/4" SHOULDER RESURFACING: BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "D", N70, 1 1/2"

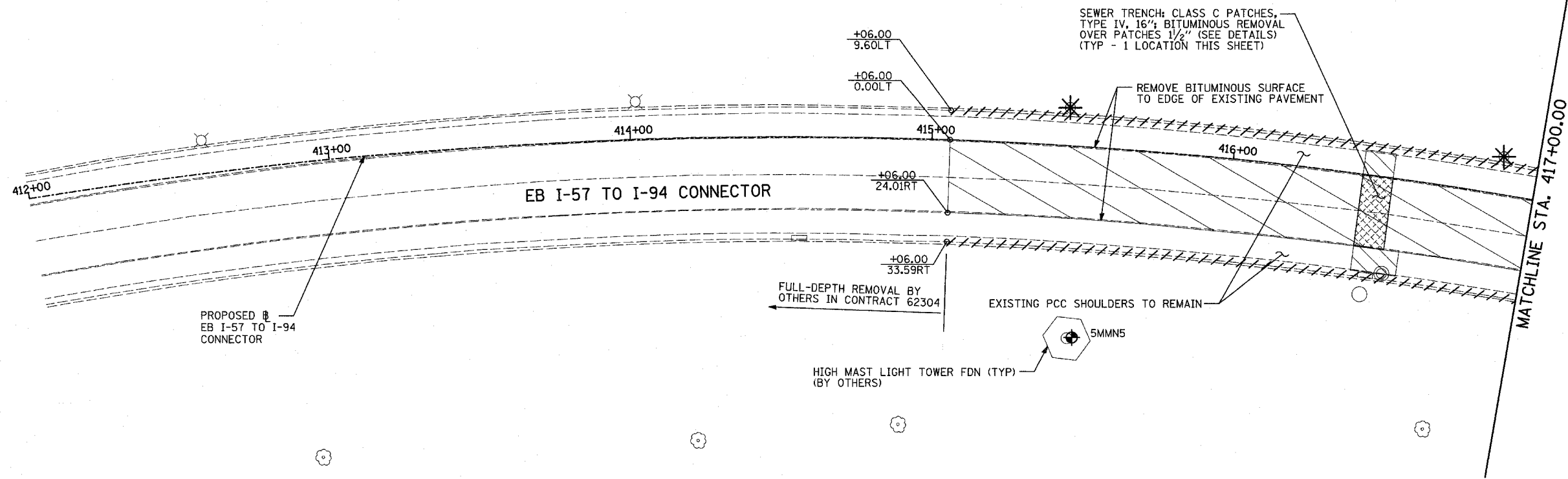


REVISIONS	
NAME	DATE

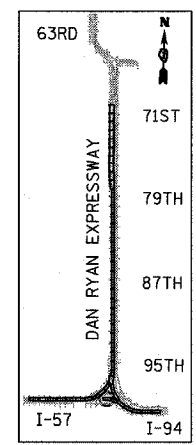
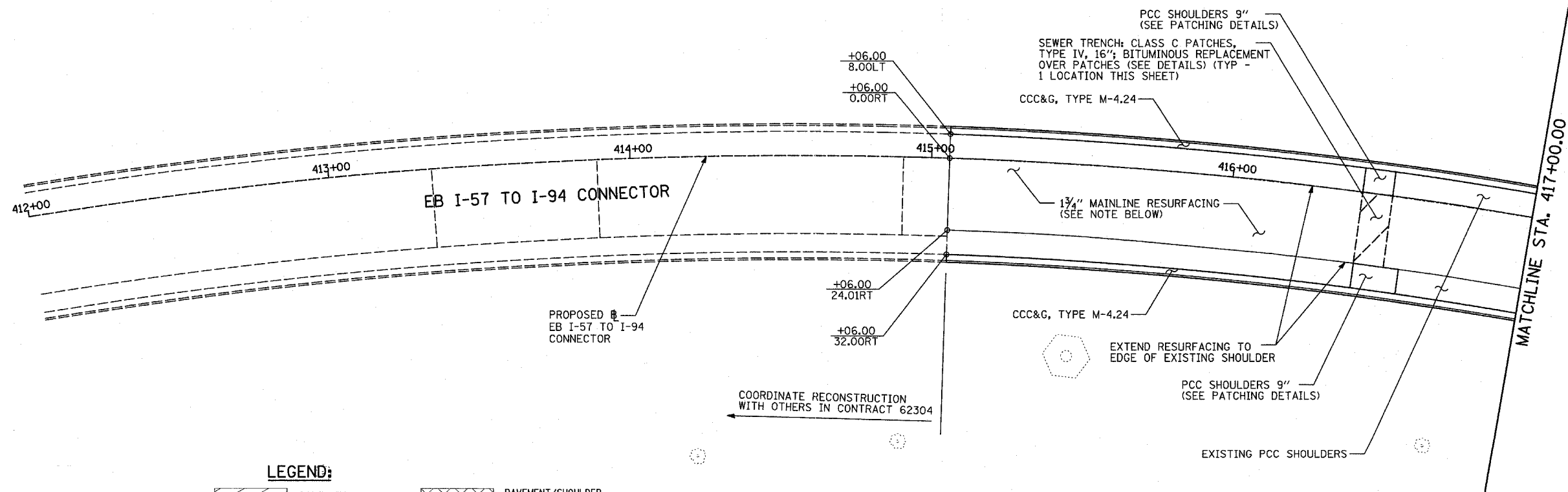
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
SB I-94 (BISHOP FORD FWY)
BISHOP FORD STA. 1160+00.00 TO 1166+00.00

SCALE: 1"=20'
DATE: MARCH 1, 2006
DRAWN BY: MPC
CHECKED BY: RTM

TYLIN INTERNATIONAL



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LOCATION MAP

LEGEND:

- | | | | |
|--|------------------------------------|--|---|
| | PAVEMENT REMOVAL | | PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES) |
| | PAVED SHOULDER REMOVAL | | COMB CONC CURB & GUTTER REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 4" | | CHAIN LINK FENCE REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 1/2" | | CONCRETE BARRIER/GUARDRAIL REMOVAL |
| | BITUMINOUS SURFACE REMOVAL, 1 3/4" | | SOIL BORING LOCATIONS |
| | | | LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS) |

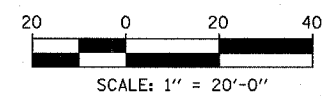
- 10 TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
- +XX.XX MAINLINE #
- XX.XXRT STATION/OFFSET

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

- 1 3/4" MAINLINE RESURFACING POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 3/4"



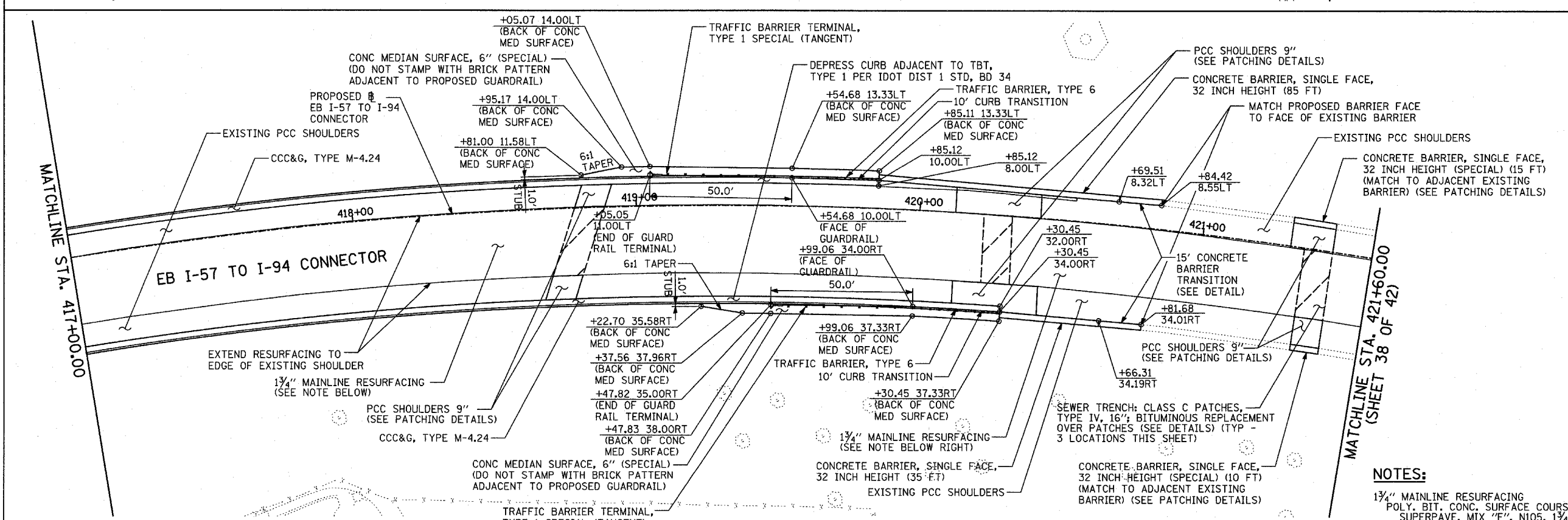
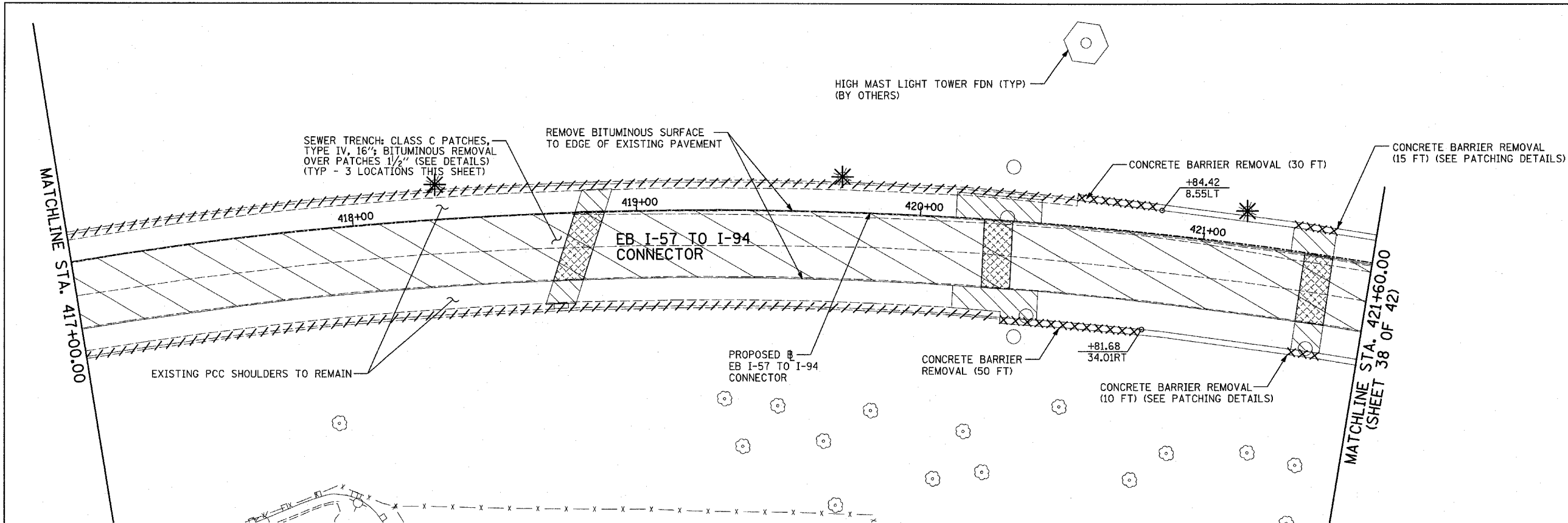
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
EB I-57 TO I-94 CONNECTOR
EB CONN. STA. 415+06.00 TO 417+00.00

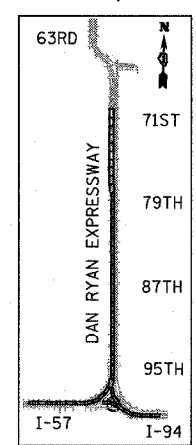
SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	105
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5 62593				



EXISTING CONDITIONS
PROPOSED IMPROVEMENTS



LEGEND:

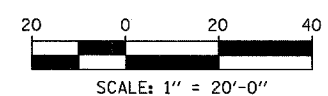
	PAVEMENT REMOVAL		BITUMINOUS SURFACE REMOVAL, 1 1/4"		SOIL BORING LOCATIONS
	PAVED SHOULDER REMOVAL		PAVEMENT/SHOULDER REMOVAL (SPECIAL) (OVER EXISTING UTILITIES)		LIGHT POLE FOUNDATION REMOVAL (SEE ROADWAY LIGHTING & SURVEILLANCE PLANS)
	BITUMINOUS SURFACE REMOVAL, 4"		COMB CONC CURB & GUTTER REMOVAL		TREE REMOVAL (UNITS) (NO UNIT SHOWN = <6)
	BITUMINOUS SURFACE REMOVAL, 1 1/2"		CHAIN LINK FENCE REMOVAL		MAINLINE STATION/OFFSET
			CONCRETE BARRIER/GUARDRAIL REMOVAL		

PLAN NOTES:

- ALL STATION/OFFSET/RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- EXACT LOCATIONS OF EXISTING PIERS/ABUTMENTS TO BE FIELD VERIFIED

NOTES:

1 1/4" MAINLINE RESURFACING POLY. BIT. CONC. SURFACE COURSE, SUPERPAVE, MIX "F", N105, 1 1/4"



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PLAN
EB I-57 TO I-94 CONNECTOR
EB CONN. STA. 417+00.00 TO 421+60.00

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: RTM

TYLIN INTERNATIONAL

CONTRACT 62593
WEST PROJECT LIMIT
SB I-57 STA 88+60

HALSTED ST.

RESURFACED
MATCH EXISTING
PROFILE

VPI STA. 89+45.00
ELEV. 4.93

STA. 89+75.00 ELEV. 4.10
15.09' MIN. CLEARANCE

$e_x = 0.72'$
 $K = 136$
280.00' V.C.

VPI STA. 91+15.00
ELEV. 2.10

EXISTING PROFILE
AT PROPOSED #

STA. 92+55.00 ELEV. 2.98

VPI STA. 93+60.00
ELEV. 3.64

+0.63%

+0.40%

VPI STA. 96+60.00
ELEV. 4.80

+0.40%

-0.52%

$e_x = -0.34'$
 $K = 327$
300.00' V.C.

PROPOSED PROFILE
SB I-57

STA. 95+00.00 ELEV. 4.20

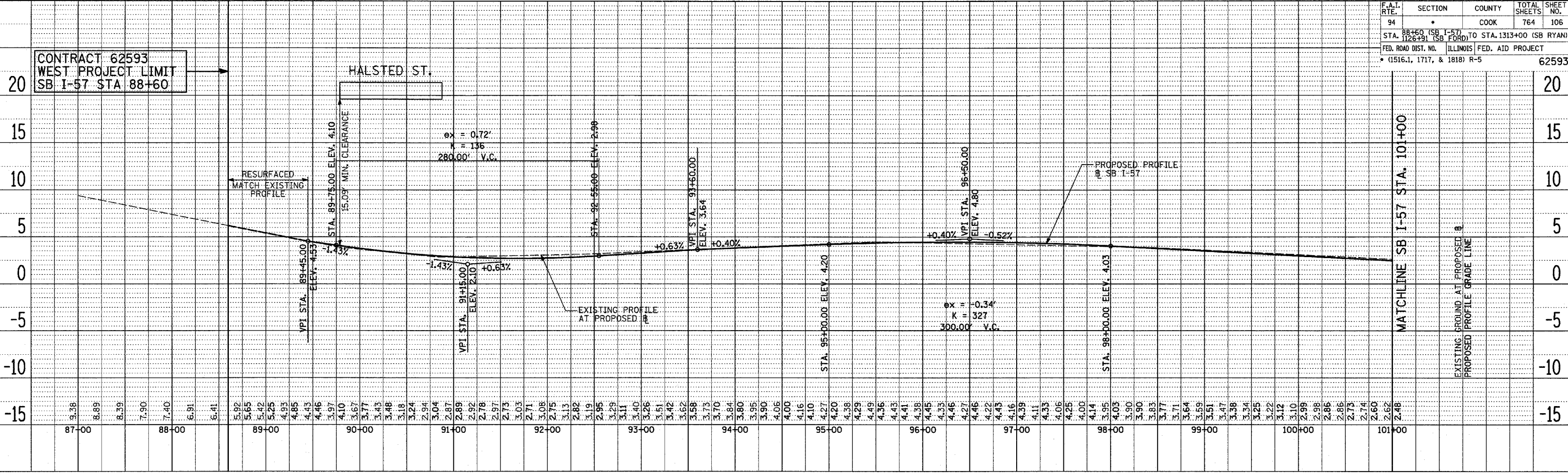
STA. 98+00.00 ELEV. 4.03

MATCHLINE SB I-57 STA. 101+00

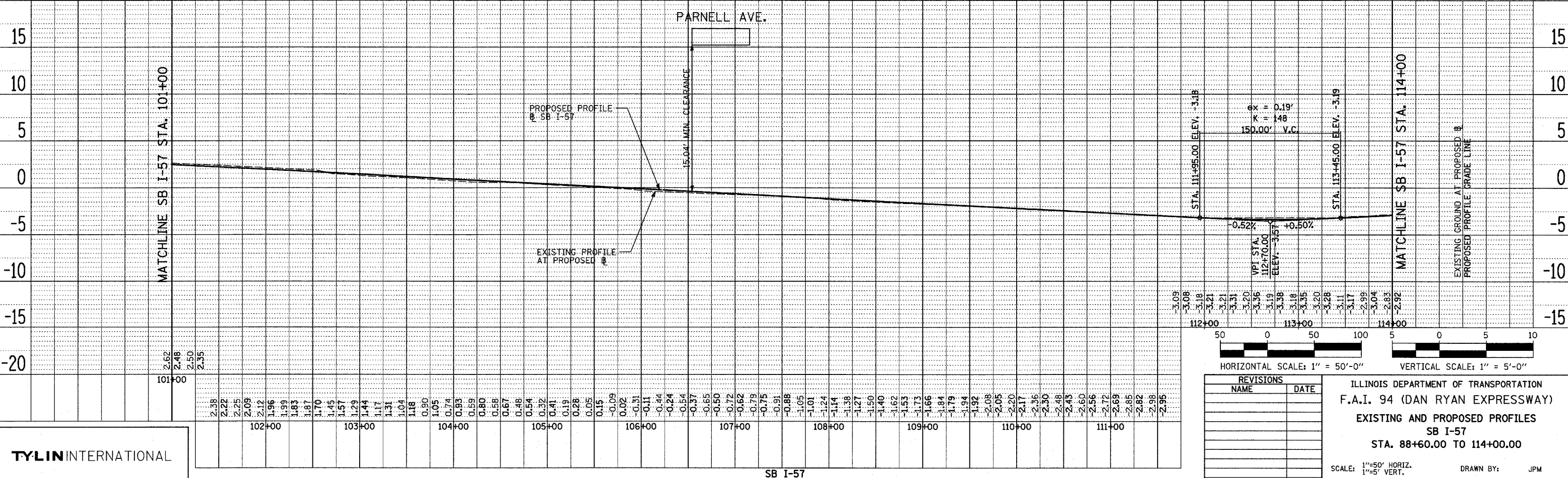
EXISTING GROUND AT PROPOSED #
PROPOSED PROFILE GRADE LINE

DATE	BY

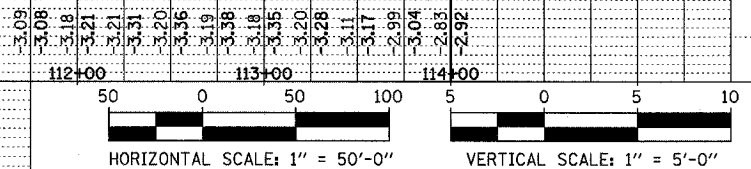
PLAN	REVISIONS	DATE



SB I-57



SB I-57



REVISIONS	
NAME	DATE

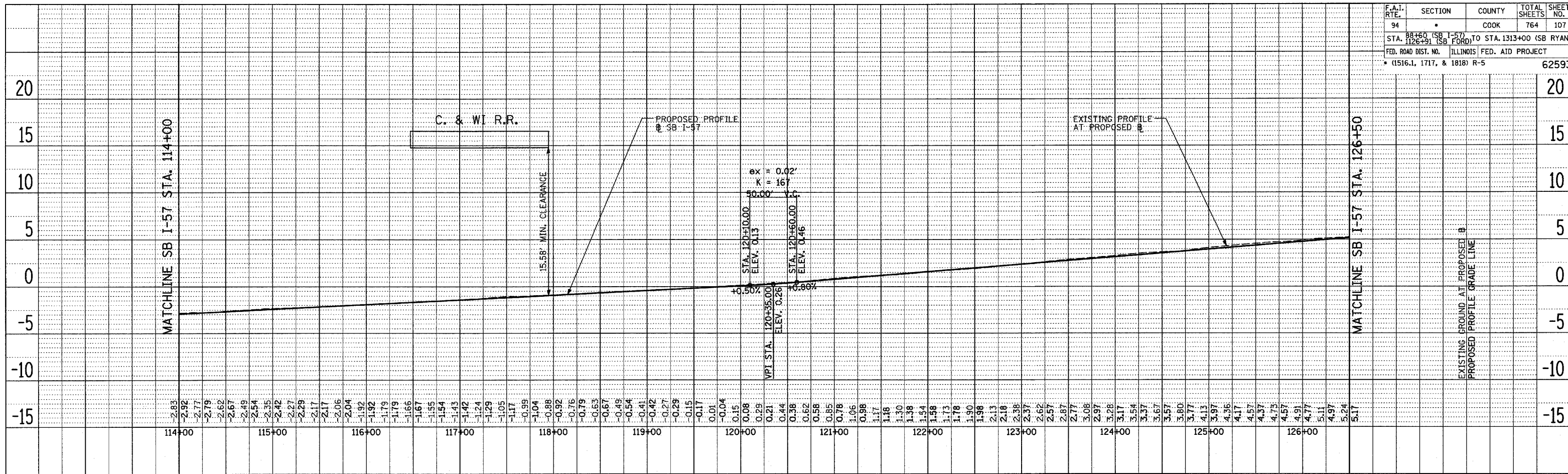
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
EXISTING AND PROPOSED PROFILES
SB I-57
STA. 88+60.00 TO 114+00.00

SCALE: 1"=50' HORIZ.
1"=5' VERT.
DATE: MARCH 1, 2006

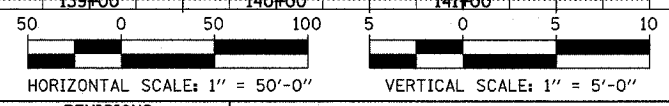
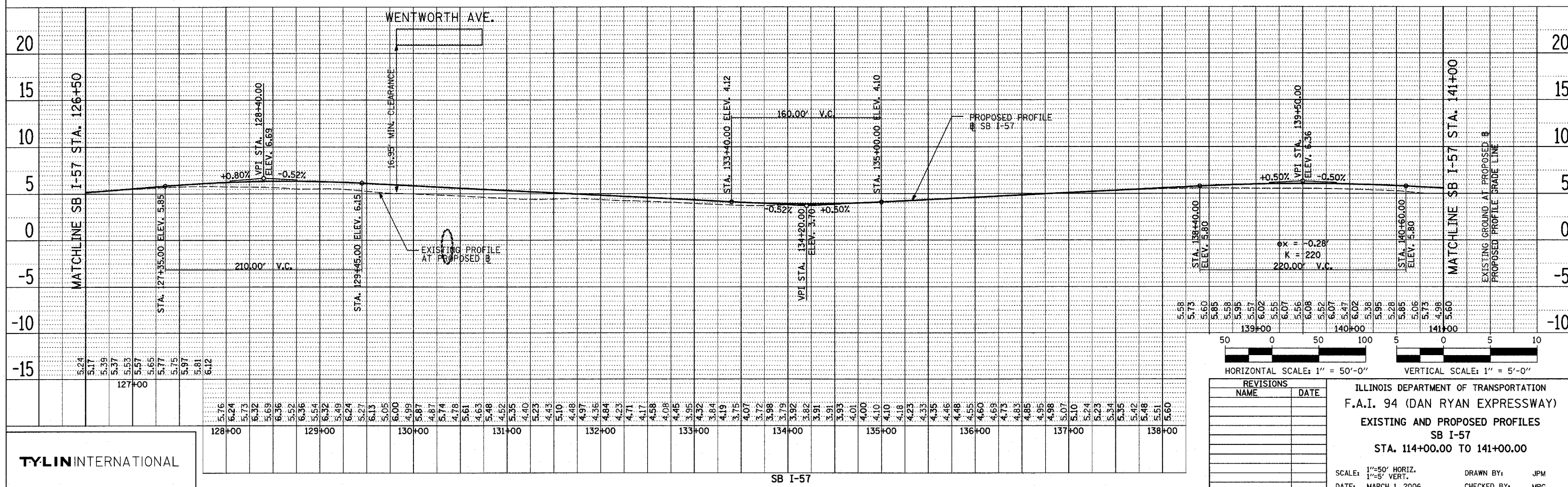
DRAWN BY: JPM
CHECKED BY: MPG

TYLIN INTERNATIONAL

PLAN	REVISIONS	BY	DATE
NO.	NO.		
	NO.		
	NO.		



PLAN	REVISIONS	BY	DATE
NO.	NO.		
	NO.		
	NO.		



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 SB I-57
 STA. 114+00.00 TO 141+00.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.

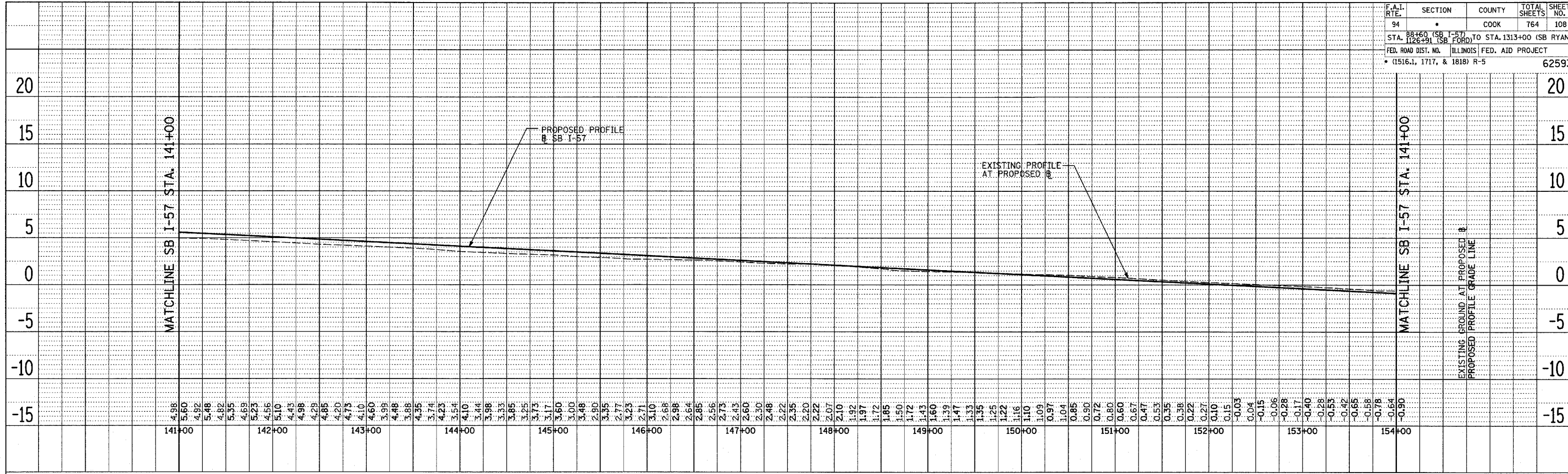
DATE: MARCH 1, 2006

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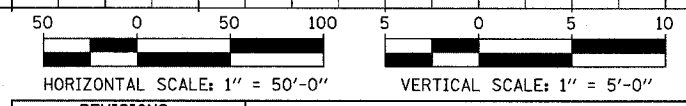
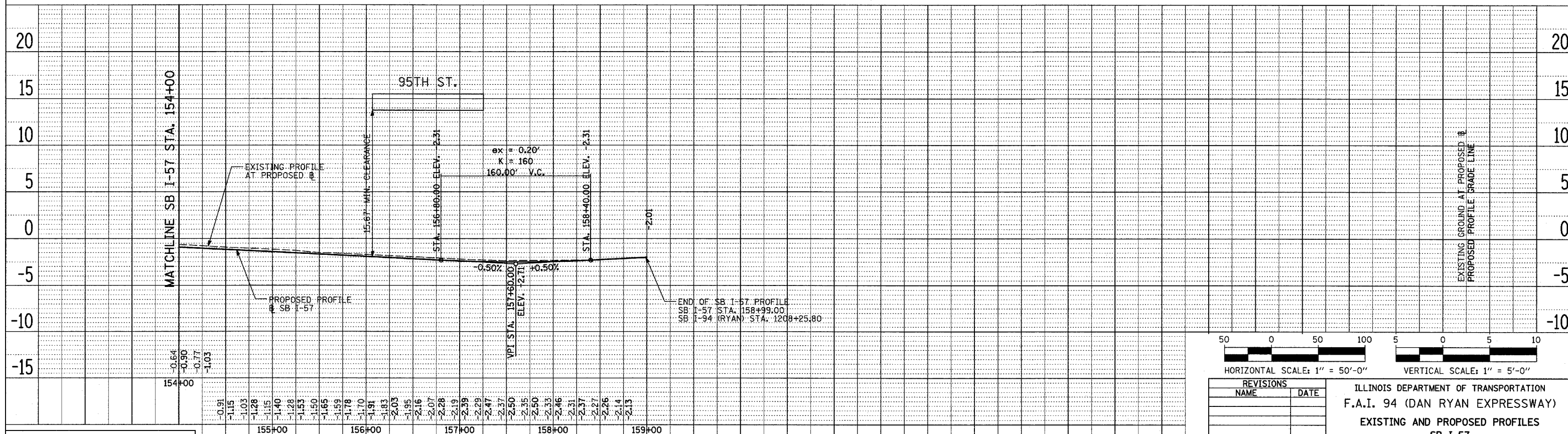
TYLIN INTERNATIONAL

SB I-57

PLAN	BY	DATE
REVISIONS		
NOTED		
ALIGNED		
CHECKED		
PAID FILE NAME		
NO.		



PLAN	BY	DATE
REVISIONS		
NOTED		
ALIGNED		
CHECKED		
PAID FILE NAME		
NO.		



REVISIONS	
NAME	DATE

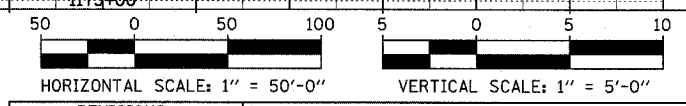
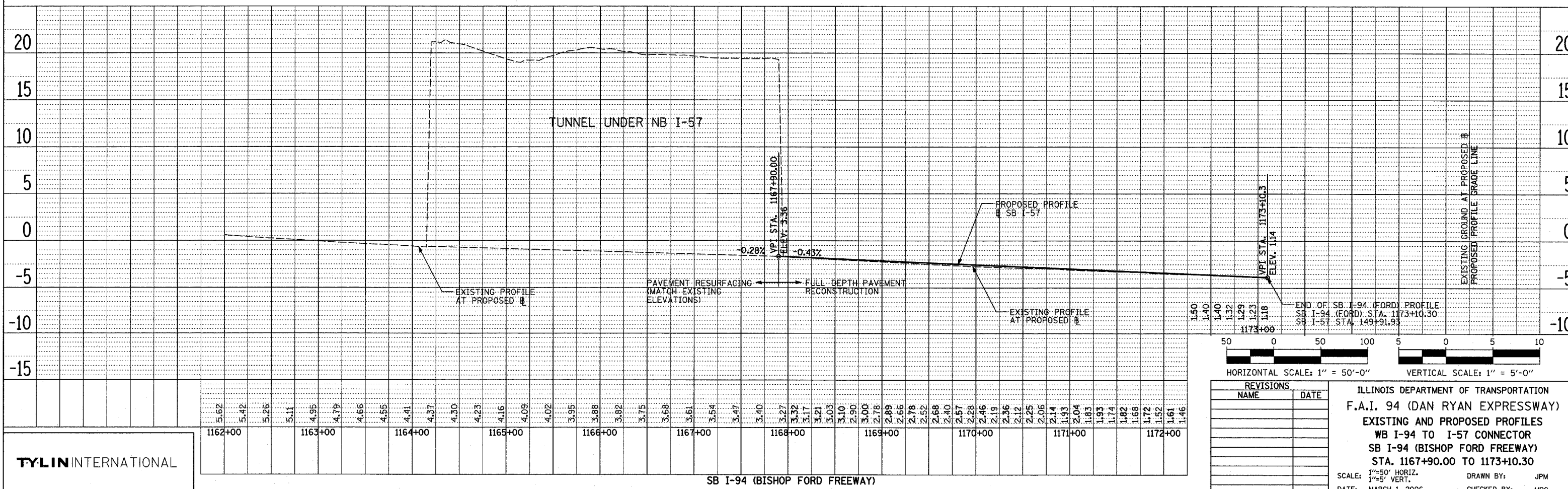
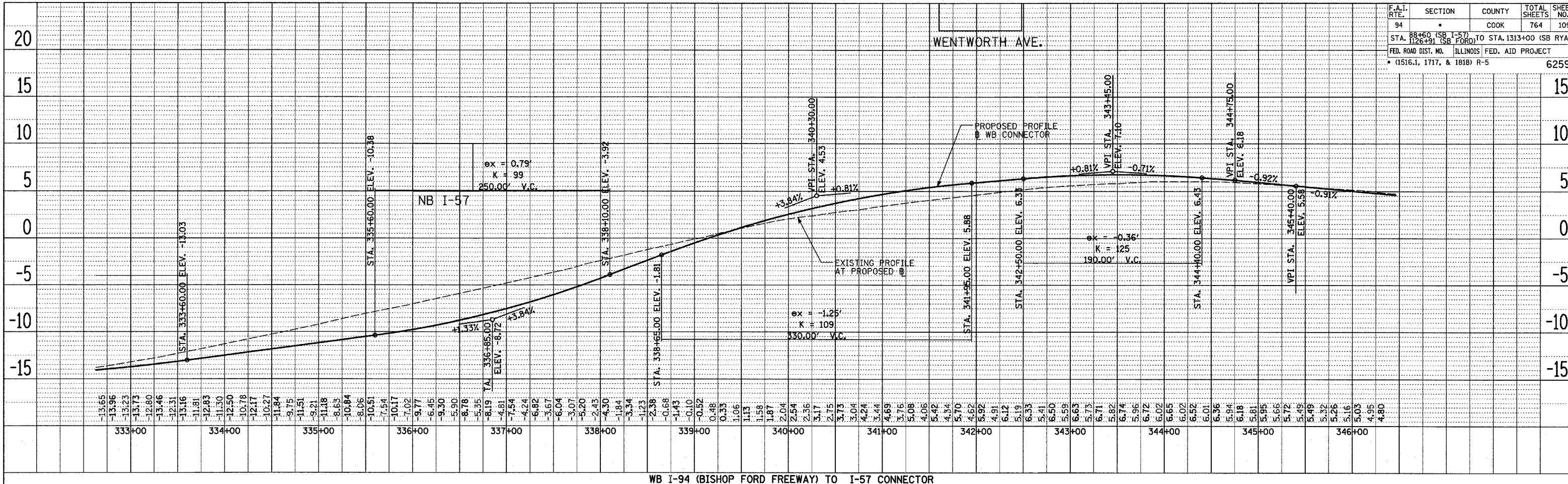
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 SB I-57
 STA. 141+00 TO 158+99

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 1, 2006

DRAWN BY: JPM
 CHECKED BY: MPG

TYLIN INTERNATIONAL

SB I-57



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING AND PROPOSED PROFILES

WB I-94 TO I-57 CONNECTOR

SB I-94 (BISHOP FORD FREEWAY)

STA. 1167+90.00 TO 1173+10.30

SCALE: 1"=50' HORIZ. DRAWN BY: JPM

1"=5' VERT. CHECKED BY: MPG

DATE: MARCH 1, 2006

TYLIN INTERNATIONAL

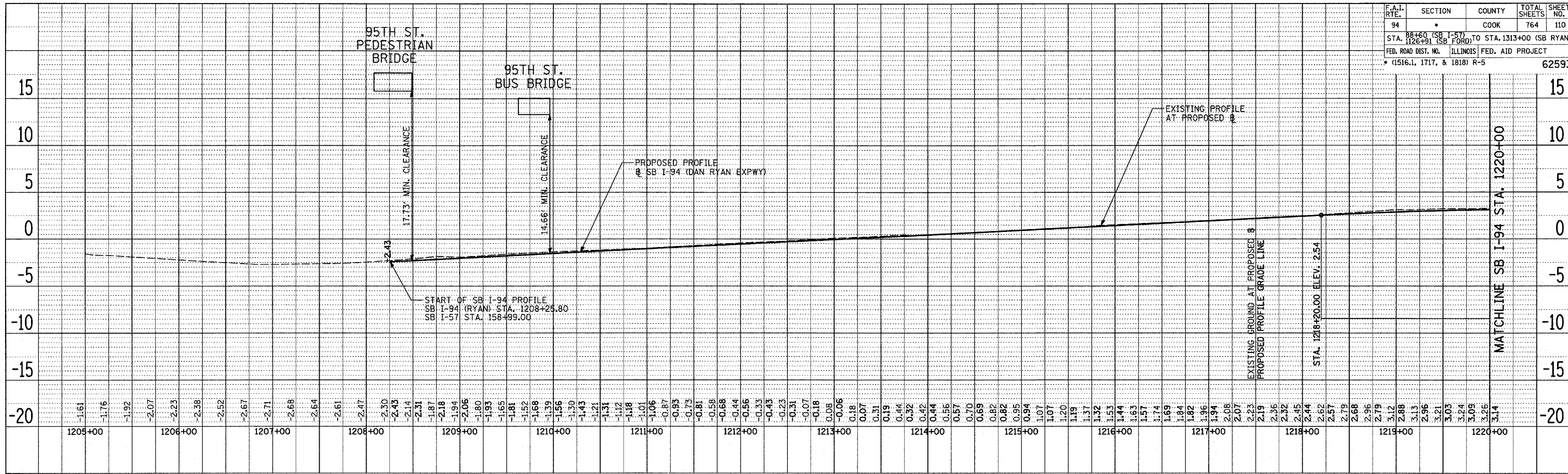
PLAN SURVEYED PLOTTED ALIGNED CHECKED DATE

NOTE BOOK NO. CAD FILE NAME

PLAN SURVEYED PLOTTED ALIGNED CHECKED DATE

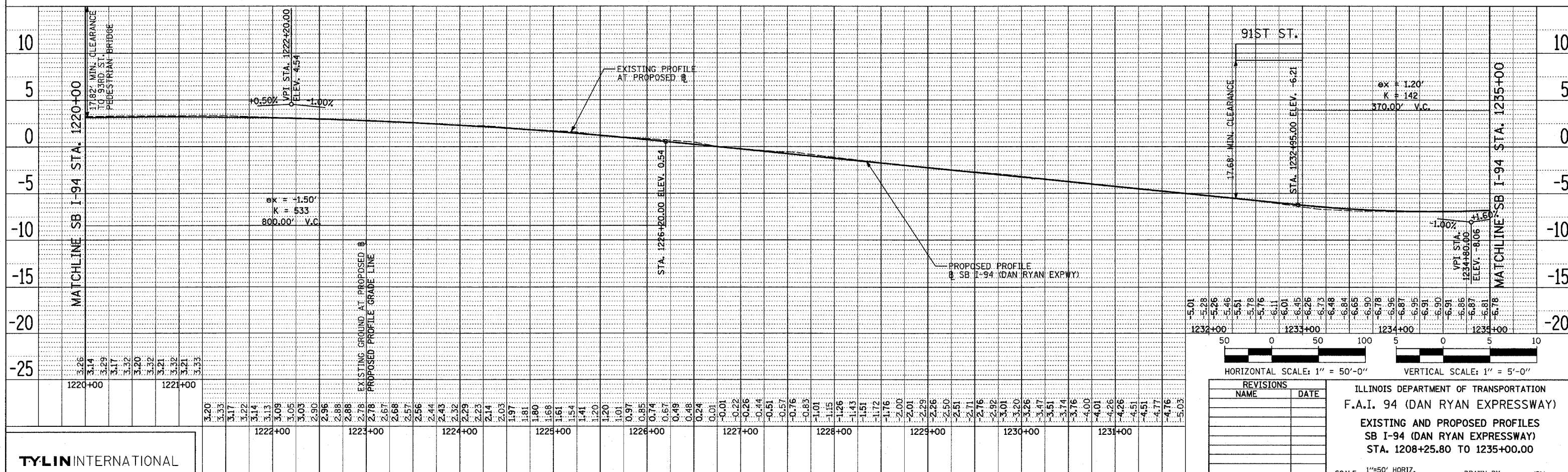
NOTE BOOK NO. CAD FILE NAME

PLAN	DATE
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	



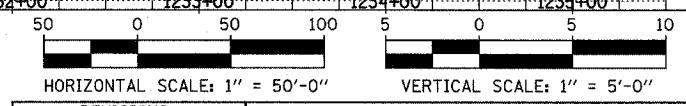
SB I-94 (DAN RYAN EXPRESSWAY)

PLAN	DATE
BY	
REVISIONS	
NO.	
DATE	
BY	
REVISIONS	
NO.	
DATE	



SB I-94 (DAN RYAN EXPRESSWAY)

TYLIN INTERNATIONAL



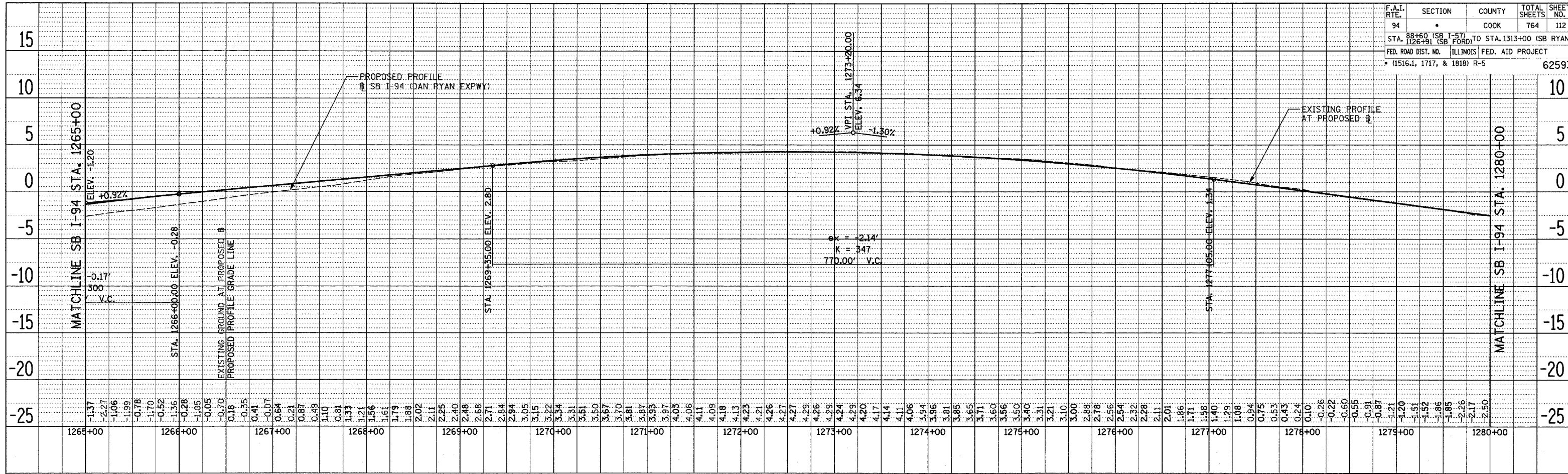
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1208+25.80 TO 1235+00.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 1, 2006

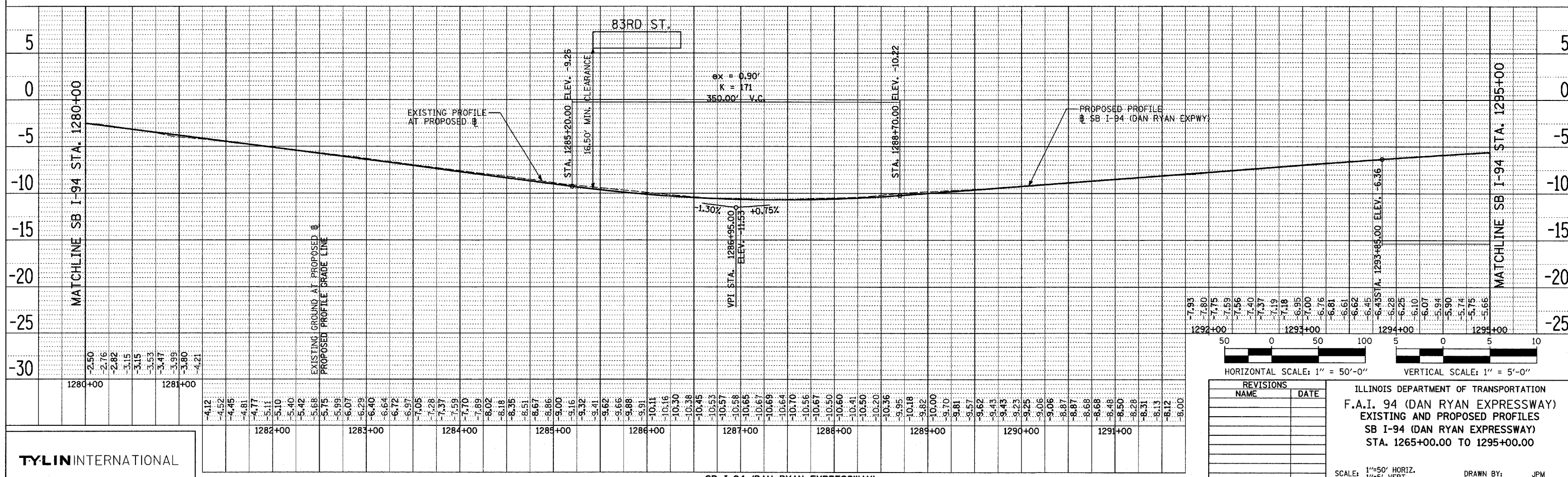
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 CHECKED BY: MPG

DATE: _____
 BY: _____
 PLAN: _____
 REVISIONS: _____
 CHECKED: _____
 NO. _____

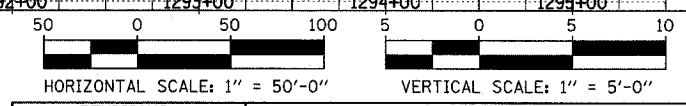


SB I-94 (DAN RYAN EXPRESSWAY)

DATE: _____
 BY: _____
 PLAN: _____
 REVISIONS: _____
 CHECKED: _____
 NO. _____



SB I-94 (DAN RYAN EXPRESSWAY)



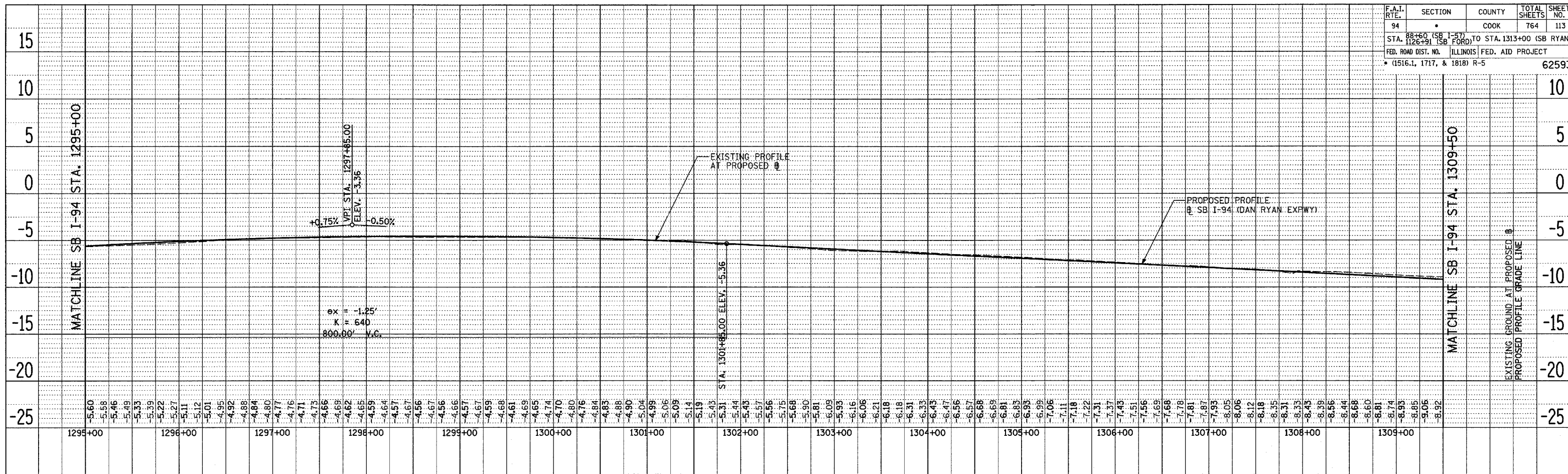
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1265+00.00 TO 1295+00.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 1, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

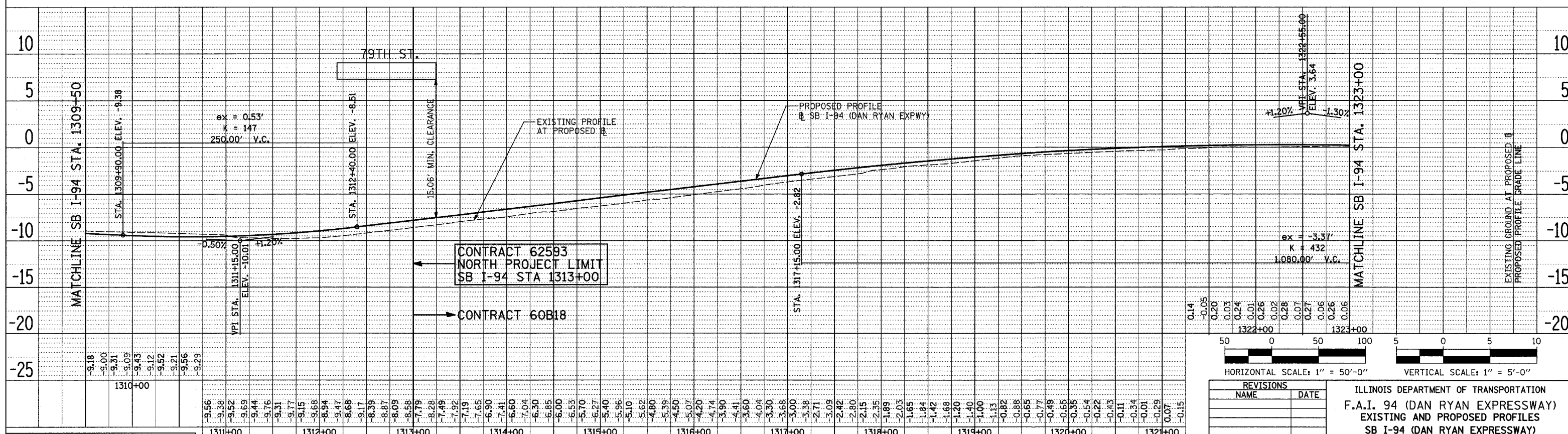
TYLIN INTERNATIONAL

PLAN	DATE
NO.	
BY	
CHECKED	
DATE	

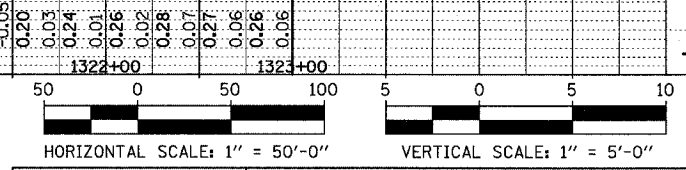


SB I-94 (DAN RYAN EXPRESSWAY)

PLAN	DATE
NO.	
BY	
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DATE	



SB I-94 (DAN RYAN EXPRESSWAY)

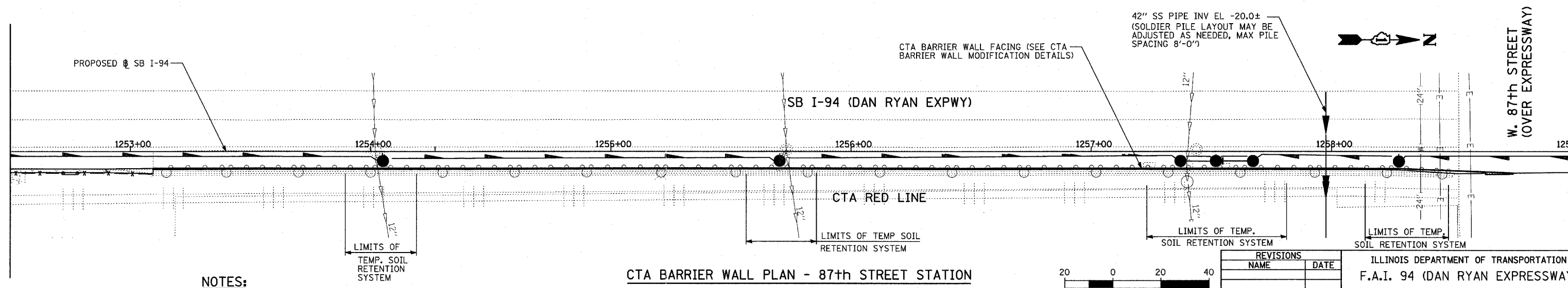
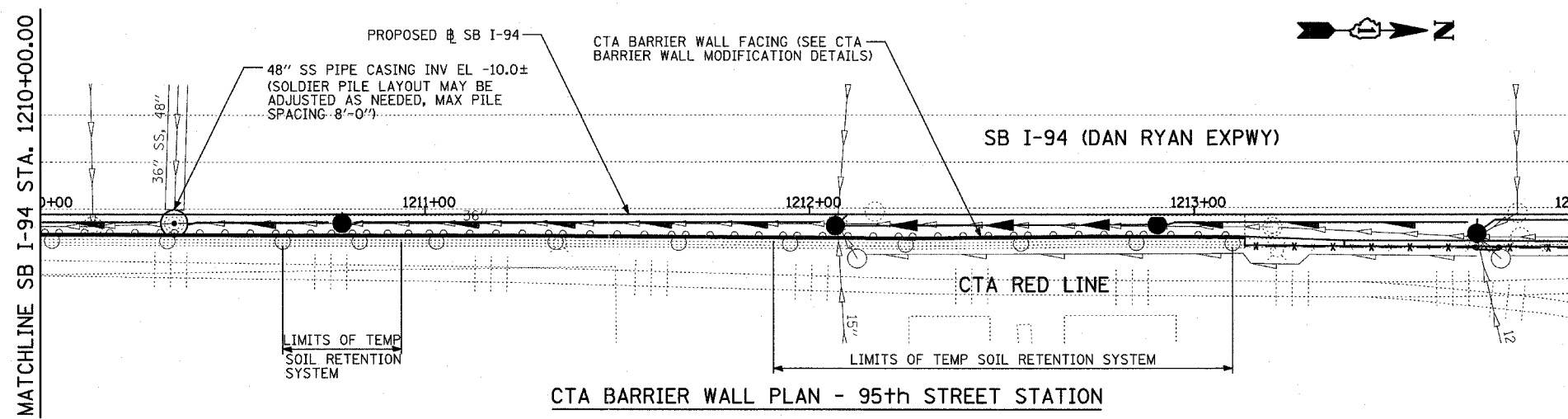
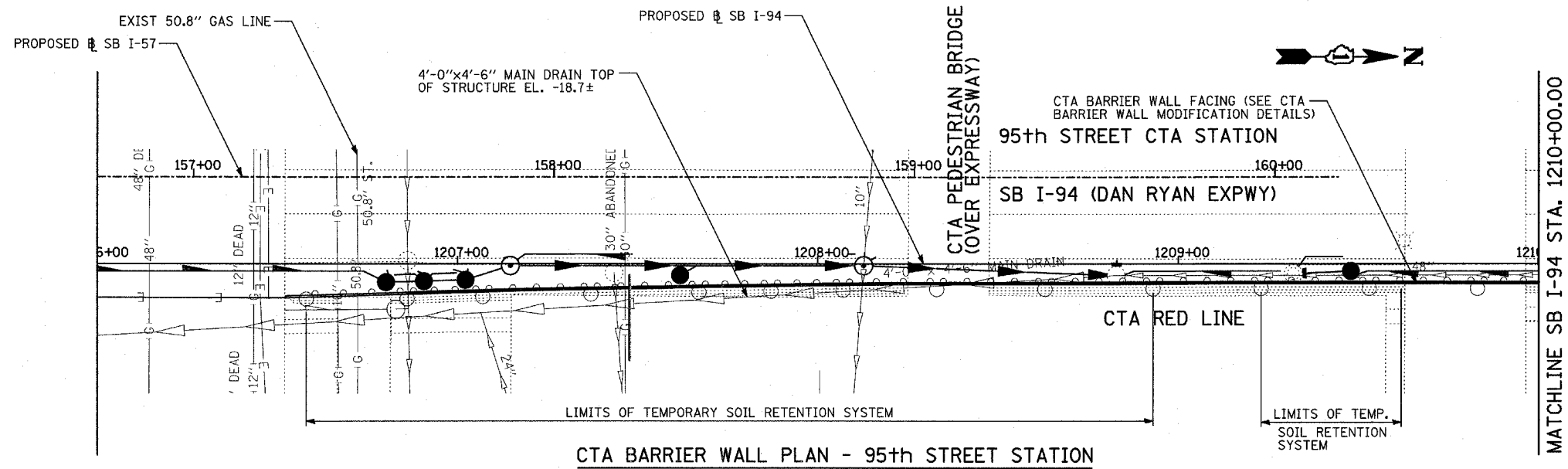


TYLIN INTERNATIONAL

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING AND PROPOSED PROFILES
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1295+00.00 TO 1323+00.00

SCALE: 1"=50' HORIZ.
 1"=5' VERT.
 DATE: MARCH 1, 2006
 DRAWN BY: JPM
 CHECKED BY: MPG

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	114
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				
				62593



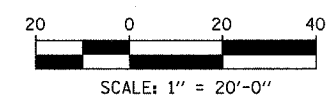
NOTES:

1. THE LIMITS SHOWN FOR THE TEMPORARY SOIL RETENTION SYSTEM FOR DRAINAGE IMPROVEMENTS IS APPROXIMATE. THE ACTUAL LIMITS SHALL BE AS DESIGNED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER & THE CTA.
2. THE AREA OF THE TEMPORARY SOIL RETENTION SYSTEM FOR THE PROPOSED DRAINAGE STRUCTURE(S) IS AS INDICATED ON THE SCHEDULE GIVEN ON SHEET 151.

3. THE PROPOSED SOLDIER PILE WALL & DETAILS INDICATED ON THIS SHEET & SHEETS B1-B3 ARE DESIGNED TO RETAIN THE CTA BALLAST DURING EXCAVATION FOR THE PROPOSED PAVEMENT SECTION ONLY.
4. WORK THIS SHEET WITH SHEETS B1 TO B3.
5. EXISTING SUB-PIER LOCATIONS ARE TAKEN FROM EXISTING PLANS, SEE SHEETS 124-129.

LEGEND:

- - EXISTING SUB-PIER
- - PROPOSED SOLDIER PILE
- - PROPOSED CATCH BASIN

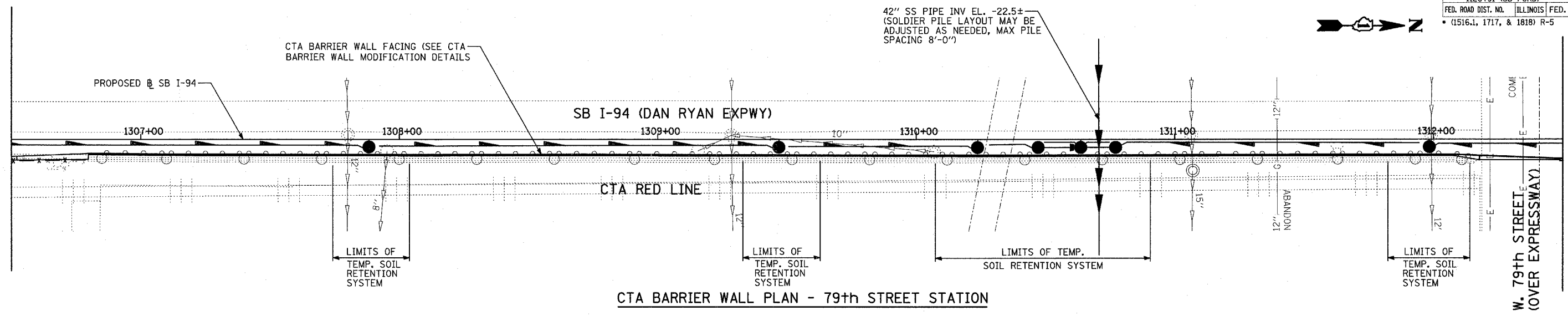


REVISIONS	
NAME	DATE

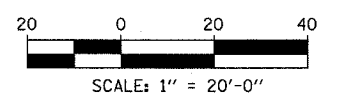
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH, 87TH & 95TH STREET STATIONS:
 LOCATION PLAN 95TH & 87TH STREET

SCALE: DATE: MARCH 1, 2006

DRAWN BY: PL
 CHECKED BY: SP



CTA BARRIER WALL PLAN - 79th STREET STATION



NOTES:

- THE LIMITS SHOWN FOR THE TEMPORARY SOIL RETENTION SYSTEM FOR DRAINAGE IMPROVEMENTS IS APPROXIMATE. THE ACTUAL LIMITS SHALL BE AS DESIGNED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER & THE CTA.
- THE AREA OF THE TEMPORARY SOIL RETENTION SYSTEM FOR THE PROPOSED DRAINAGE STRUCTURE(S) IS AS INDICATED ON THE SCHEDULE GIVEN ON SHEET 151.
- THE PROPOSED SOLDIER PILE WALL & DETAILS INDICATED ON THIS SHEET & SHEETS B1-B3 ARE DESIGNED TO RETAIN THE CTA BALLAST DURING EXCAVATION FOR THE PROPOSED PAVEMENT SECTION ONLY.
- WORK THIS SHEET WITH SHEETS B1 TO B3.
- EXISTING SUB-PIER LOCATIONS ARE TAKEN FROM EXISTING PLANS, SEE SHEETS 124-129.

LEGEND:

- - EXISTING SUB-PIER
- - PROPOSED SOLDIER PILE
- - PROPOSED CATCH BASIN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH, 87TH & 95TH STREET STATIONS:
 LOCATION PLAN 79TH STREET

SCALE: DATE: MARCH 1, 2006
 DRAWN BY: PL
 CHECKED BY: SP



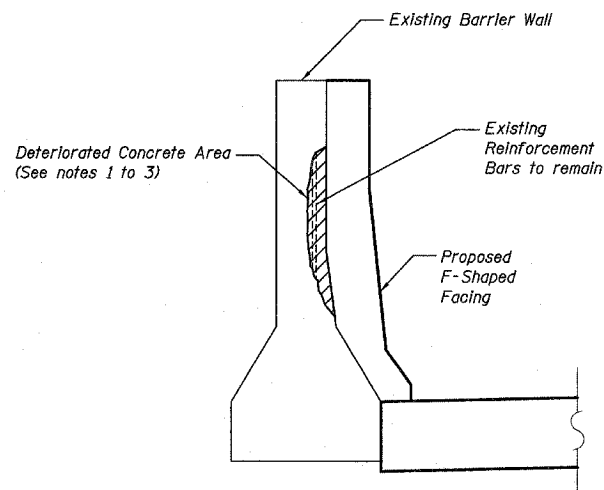
CTA BARRIER WALL LOCATION AND MODIFICATION TABLE

STATION	LOCATION		MODIFICATION
	STATIONS		
79th ST.	STA. 1306+80 TO 1312+08		REFACING
87th ST.	STA. 1253+09 TO 1258+27		REFACING
95th ST.	SB I-57 STA 157+60 TO 1213+13		REFACING

STATIONS ARE RELATIVE TO SB I-94 UNLESS NOTED

DESIGN STRESSES:

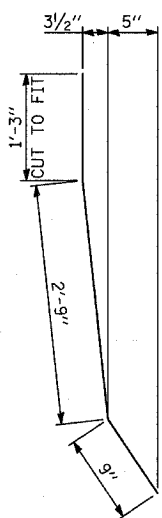
FIELD UNITS
 $f'_c = 3,500$ PSI
 $f_y = 60,000$ PSI (REINF.)



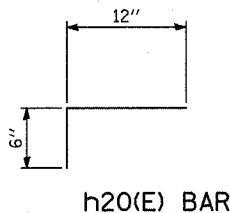
CONCRETE REPAIR DETAIL

NOTES ON CONCRETE REPAIR DETAIL

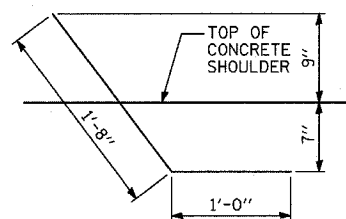
- REMOVE DETERIORATED CONCRETE TO SOUND CONCRETE IN ACCORDANCE WITH SECTION 501.03 OF THE STANDARD SPECIFICATIONS. UNDERCUT EXPOSED REINFORCEMENT BARS BY $\frac{3}{4}$ " TO PROVIDE CLEARANCE BETWEEN EXISTING REINFORCEMENT BARS AND THE REMAINING CONCRETE.
- EXPOSED EXISTING REINFORCEMENT BARS SHALL REMAIN IN PLACE AND BE BLAST CLEANED TO GRAY METAL. REINFORCEMENT BARS THAT HAVE BEEN CUT OR HAVE LOST 25% OR MORE OF THEIR ORIGINAL CROSS SECTIONAL AREA SHALL BE SUPPLEMENTED BY NEW EPOXY COATED REINFORCEMENT BARS.
- THE COST OF REMOVING DETERIORATED CONCRETE AND SUPPLEMENTING AND CLEANING EXISTING REINFORCEMENT BARS SHALL BE INCLUDED IN THE COST PER CUBIC YARD OF "CONCRETE STRUCTURES".



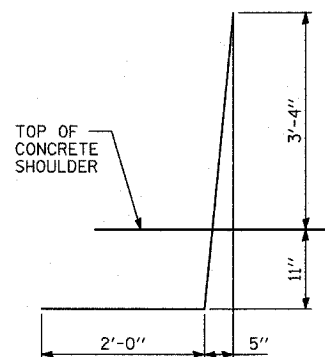
d3(E) BAR



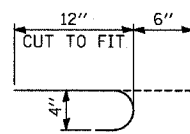
h20(E) BAR



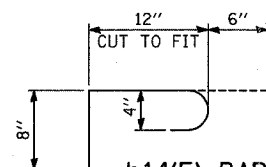
d4(E) BAR



d7(E) BAR



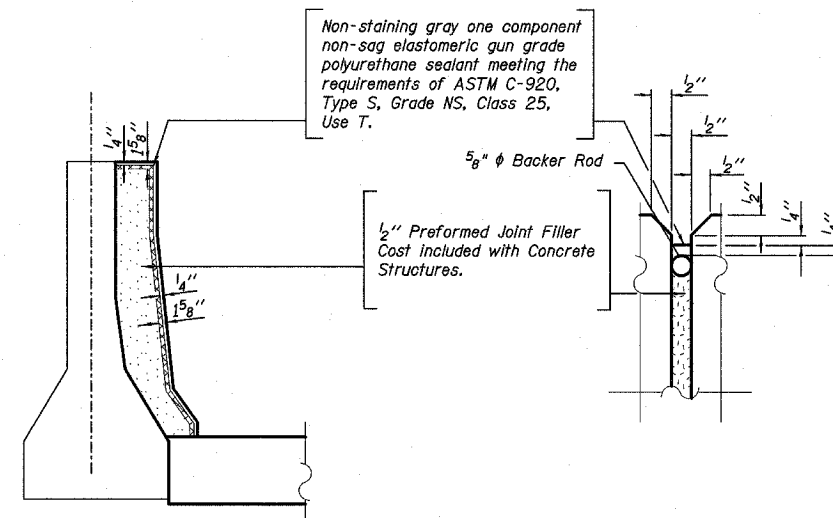
h3(E) BAR



h14(E) BAR

GENERAL NOTES:

- IN LOCATIONS WHERE THE MAIN SEWERS, DRAINAGE STRUCTURES, PUBLIC AND PRIVATE DRAIN CONNECTIONS ARE DISTURBED OR DAMAGED DURING CONSTRUCTION BY THE CONTRACTOR, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO RESTORE AND REPLACE THE DAMAGED FACILITIES AT HIS/HER OWN EXPENSE TO THE SATISFACTION OF THE DEPARTMENT OF WATER MANAGEMENT. THE SEWER FLOWS MUST BE MAINTAINED AT ALL TIMES.
- IN LOCATIONS WHERE PILING/DRILLED SHAFTS ARE BEING PROPOSED IN THE VICINITY OF THE EXISTING SEWER FACILITIES, THE CONTRACTOR MUST PROVIDE A PLAN OF CONSTRUCTION METHODS WITH CALCULATIONS SEALED BY A STRUCTURAL ENGINEER LICENSED BY THE STATE OF ILLINOIS PROVING NO DAMAGE OR HARM TO THE STRUCTURAL INTEGRITY OF THE CITY'S SEWER SYSTEM. THE PLAN MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE CHICAGO DEPARTMENT OF WATER MANAGEMENT PRIOR TO CONSTRUCTION.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT NO GAP REMAINS BETWEEN PROPOSED CTA FENCING OR WHERE PROPOSED CTA FENCING TERMINATES AND EXISTING CTA FENCE REMAINS IN PLACE.
- PLAN DIMENSIONS AND DETAILS RELATING TO EXISTING STRUCTURES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION OR A CHANGE IN THE SCOPE OF WORK. THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE FOR THE WORK.
- ALL CONSTRUCTION JOINTS SHALL BE BONDED.
- ALL EXPOSED CONCRETE EDGES SHALL HAVE A $\frac{3}{4}$ " BY 45° CHAMFER EXCEPT WHERE SHOWN OTHERWISE. CHAMFER ON VERTICAL EDGES SHALL BE CONTINUED A MINIMUM OF 1 FOOT BELOW THE FINISHED GROUND LINE.
- A PROTECTIVE COAT SHALL BE APPLIED WITHIN THE LIMITS SHOWN ON THE PLANS ACCORDING TO SECTION 503.19 OF THE STANDARD SPECIFICATIONS.
- REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF AASHTO M31 OR M322 GRADE 60.
- REINFORCEMENT BARS DESIGNATED "E" SHALL BE EPOXY COATED.
- THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND PERFORMANCE OF THE LAGGING USING NO LESS THAN 3" NOMINAL ROUGH-SAWN THICKNESS AND THE MINIMUM TABULATED UNIT STRESS IN BENDING (f_b), USED IN THE DESIGN OF THE TIMBER LAGGING SHALL BE 1000 psi.
- HAMMER DRIVING OF PILES WILL NOT BE ALLOWED.



BARRIER JOINT DETAILS

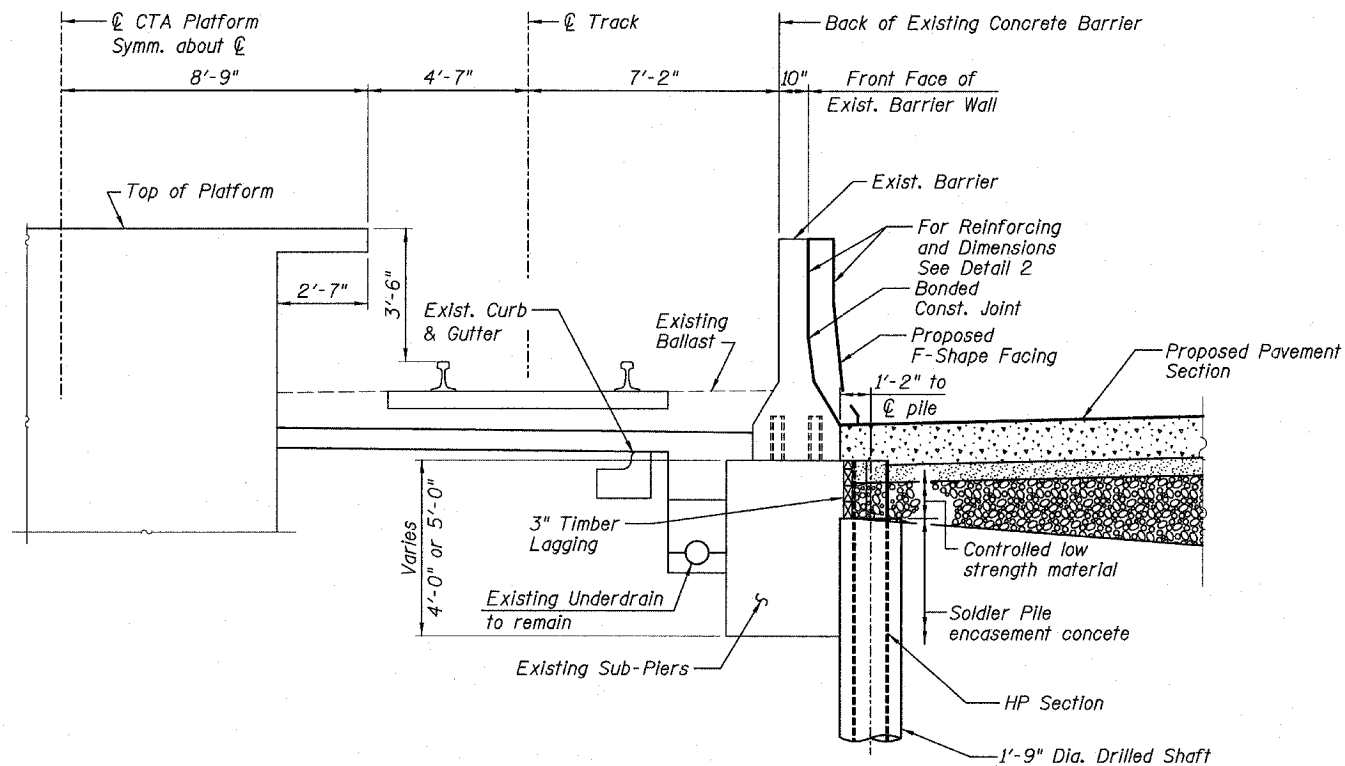
(AT EXISTING BARRIER JOINT LOCATIONS)

SHEET B1 OF 8

REVISIONS	
NAME	DATE

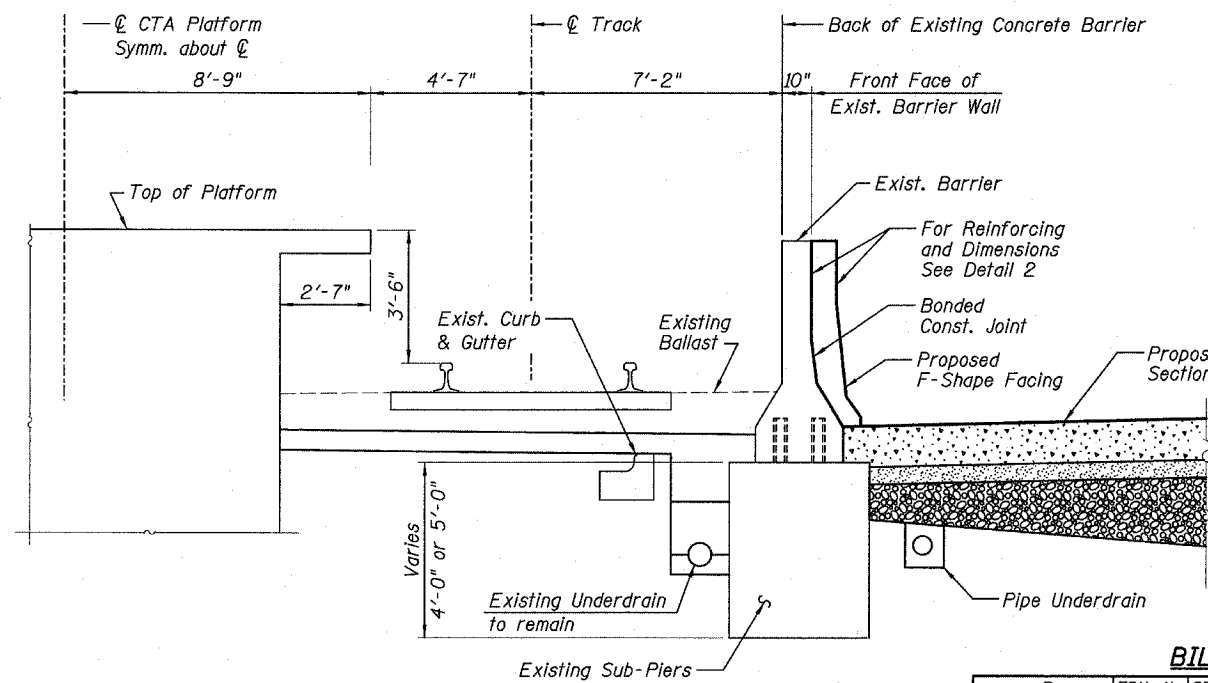
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 LOCATION AND MODIFICATION TABLE,
 GENERAL NOTES AND BAR DETAILS

SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: MPG, SP
 CHECKED BY: PF



**TYPICAL SECTION
BETWEEN SUB-PIERS**
(Pipe Underdrain not shown)

Contractor to cut and dispose of soldier pile tops as required for paving and as directed by the Engineer. Cost to be included in "Furnishing Soldier Piles (HP Section)".



**TYPICAL SECTION
AT SUB-PIER**

LEGEND:

	Concrete Removal
	PCC Pavement
	BAM
	Sub Base

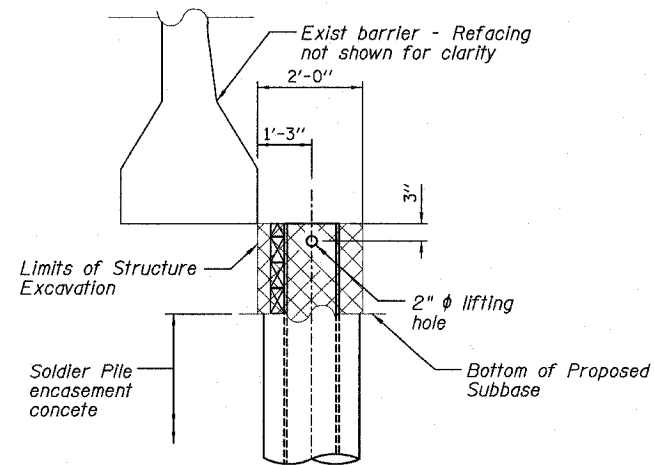
BILL OF MATERIAL

Bar	79th No.	87th No.	95th No.	Size	Length	Shape
d3(E)	528	518	627	#4	4'-9"	
d4(E)	528	518	627	#4	2'-8"	
d12(E)	4	4	2	#4	3'-4"	
h1(E)	108	108	174	#4	29'-7"	
h3(E)	792	777	941	#4	1'-6"	
h12(E)	6	6	3	#4	1'-4"	
h14(E)	8	8	4	#4	2'-2"	
Item	Unit	79th Qty	87th Qty	95th Qty		
Furnishing Soldier Piles (HP Section)	Foot	1056	1068	1332		
Drilling and Setting Soldier Piles (in Soil)	Cu. Ft.	2753	2759	3516		
Untreated Timber Lagging	Sq. Ft.	778	872	1111		
Reinforcement Bars, Epoxy Coated	Pound	5570	5510	7510		
* Concrete Structures	Cu. Yd.	63	64	78		
Protective Coat	Sq. Yd.	370	374	453		
Structure Excavation	Cu. Yd.	97	100	139		

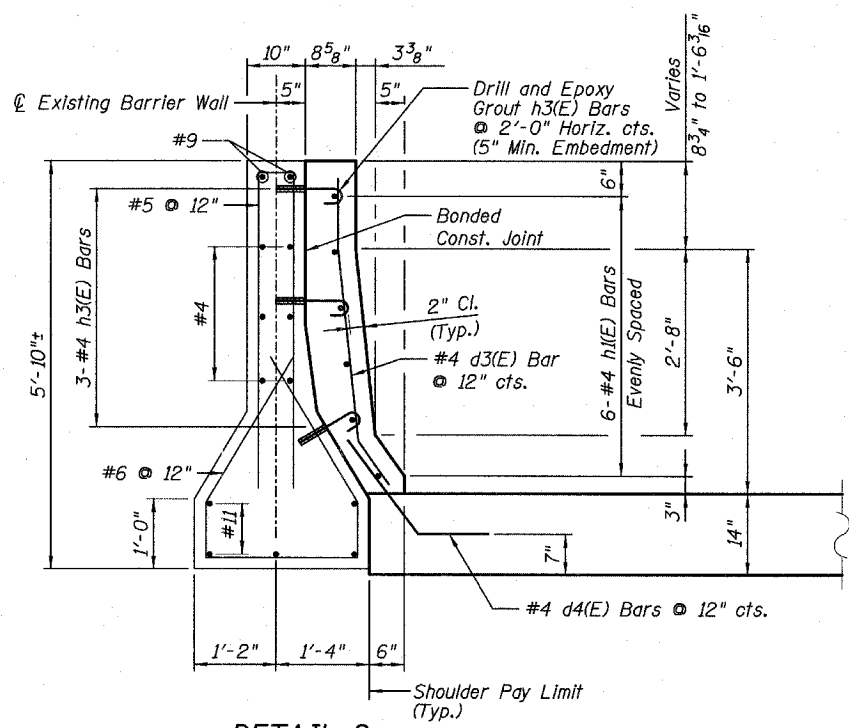
* Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

NOTES:

1. Drilling and grouting of #4 h3(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. #4 d4(E) bars shall be seated in their final position prior to the placement of the concrete shoulder. Bars shall not be mucked into place.
3. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h3(E) bars shall be included in the cost for Concrete Structures.
4. Provide joints in facing to match those in the existing barrier, see Sht B1 for details.
5. h1(E) bars shall be cut as needed for fit.
6. See Sht. B1 for bar detailing.

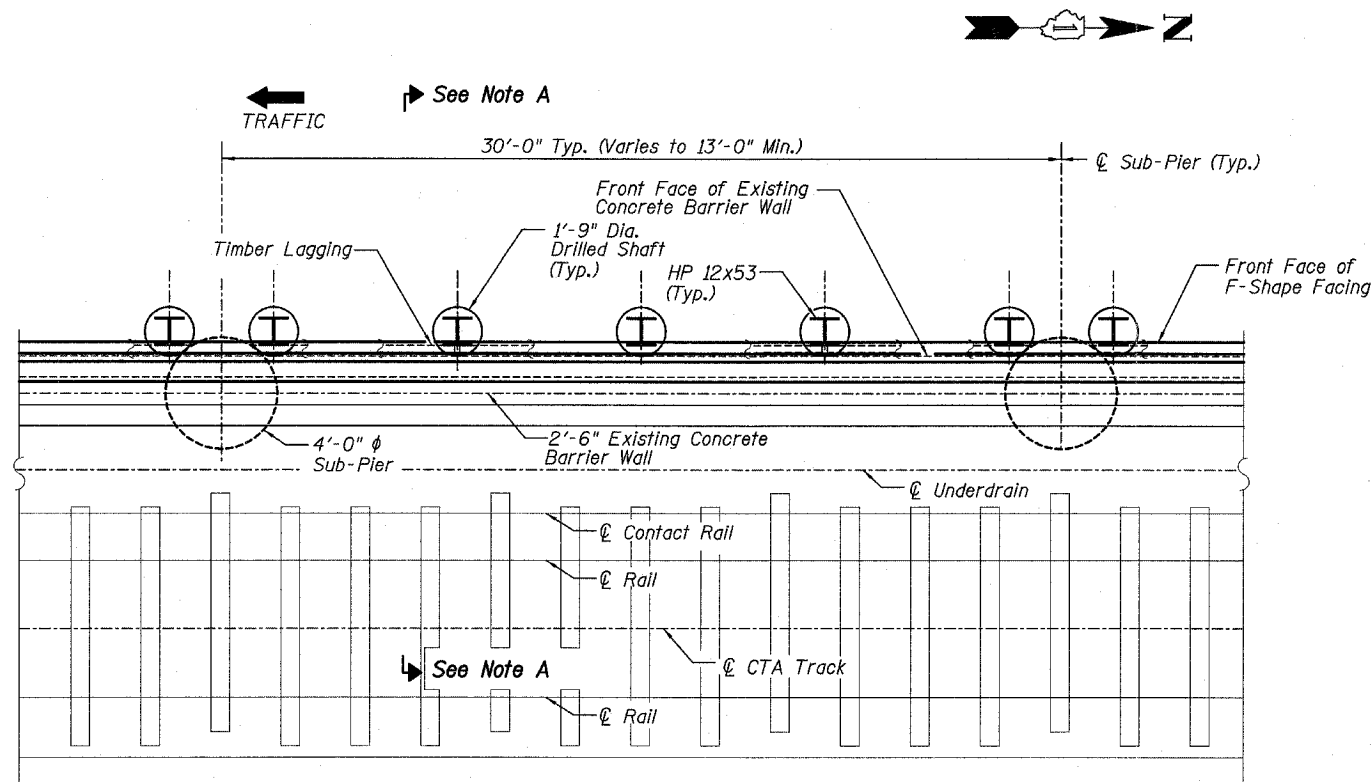


MISCELLANEOUS DETAILS



DETAIL 2

REVISIONS	
NAME	DATE

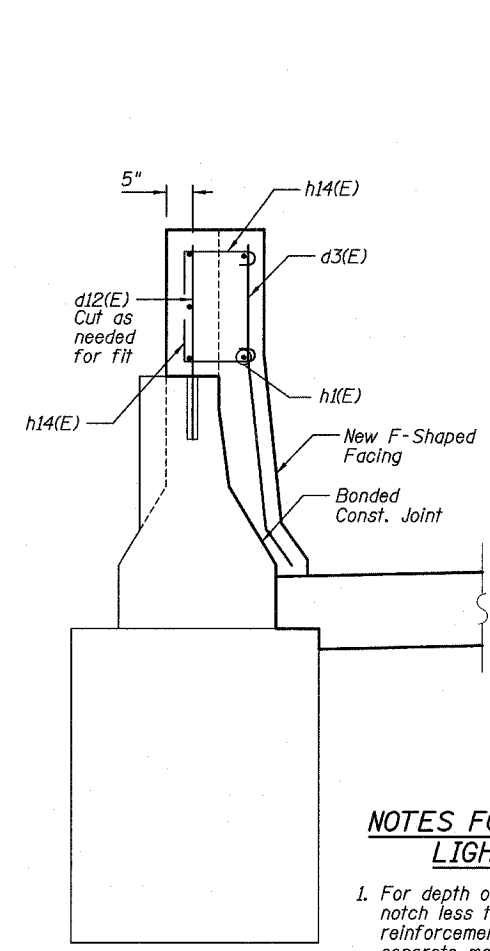


Note A:
See Sheet B2, "Typical Section between Sub-piers"

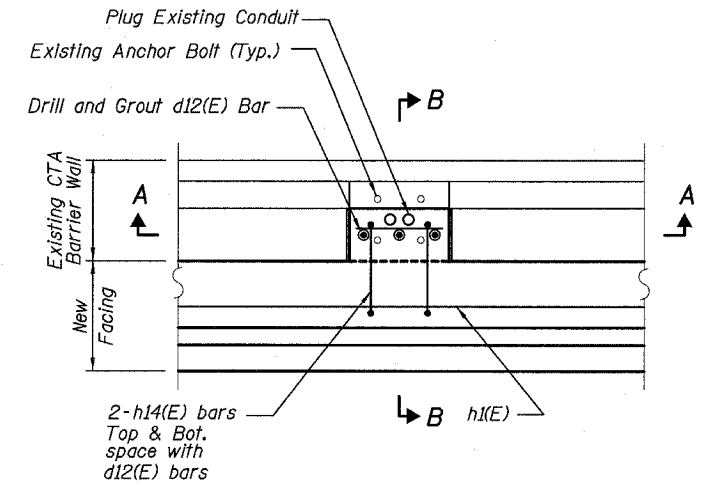
TYPICAL PLAN

DETAIL A NOTES

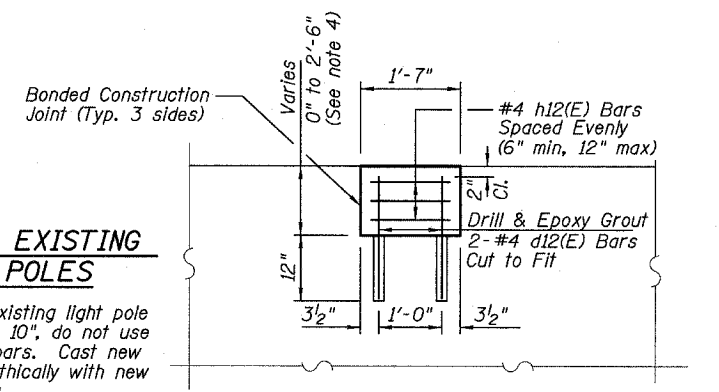
Soldier Piles are to be HP 12x53, 12' Long (288 required.) Fy min = 36,000 psi (M270 Grade 36)
Pile locations can be adjusted to miss proposed drainage structures. Maximum pile spacing 8'-0".



SECTION B-B



PLAN - DETAILS AT EXISTING LIGHT POLES

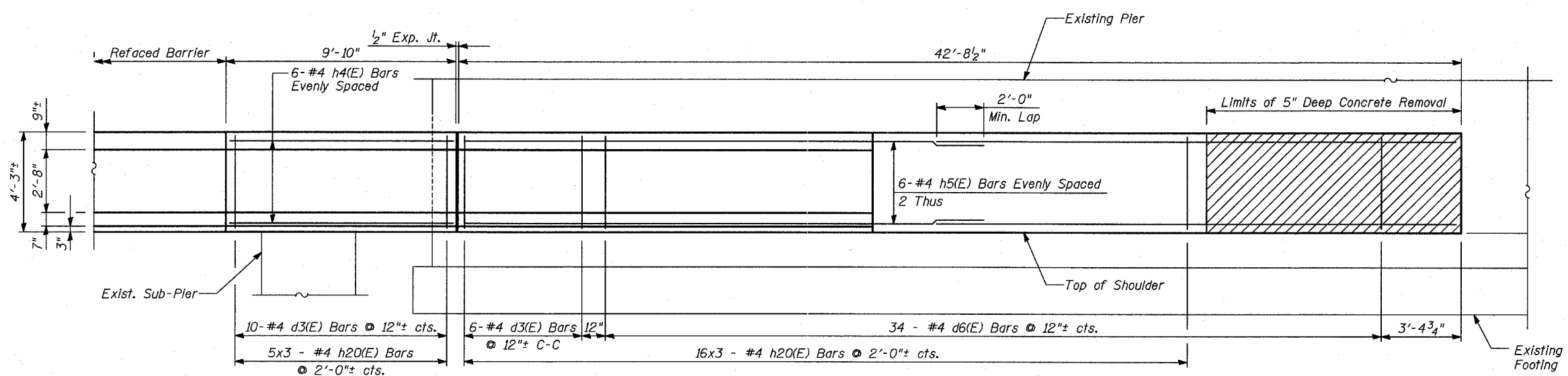
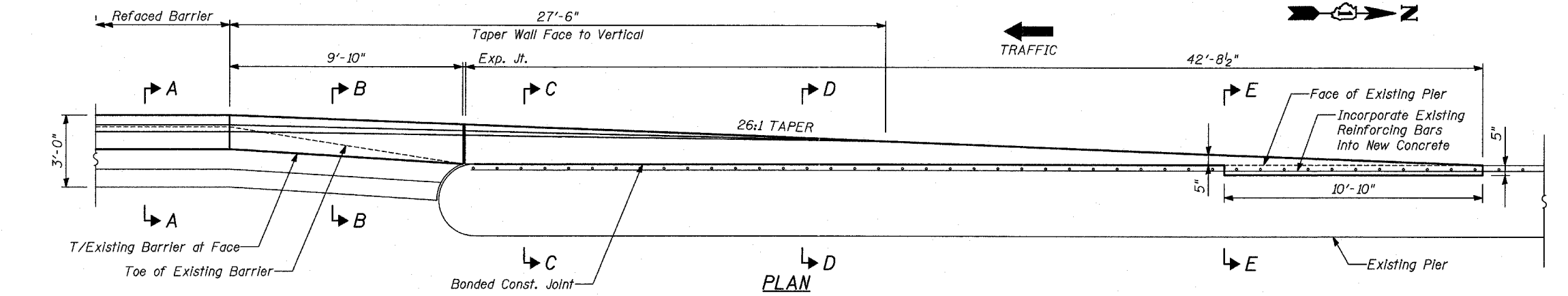


SECTION A-A

NOTES FOR EXISTING LIGHT POLES

1. For depth of existing light pole notch less than 10", do not use reinforcement bars. Cast new concrete monolithically with new F-Shape facing.
2. Additional reinforcing details for light pole base details shown, for F-Shape Facing details and reinforcement, see Detail 2, Sht B2.

REVISIONS	
NAME	DATE



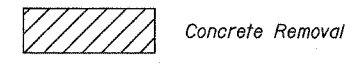
ELEVATION

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)	16	#4	4'-9"	
d6(E)	34	#4	3'-11"	
h4(E)	6	#4	9'-6"	
h5(E)	12	#4	22'-1"	
h20(E)	63	#4	1'-6"	

Item	Unit	Quantity
Reinforcement Bars, Epoxy Coated	Pound	420
Concrete Structures	Cu. Yd.	8
Protective Coat	Sq. Yd.	31
Concrete Removal	Cu. Yd.	1

LEGEND:



NOTES:

1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see Sheet B7.
6. For locations of Transition, see Roadway Plans.

The pay items for the soldier pile wall are included in the quantities on Sheet B2.

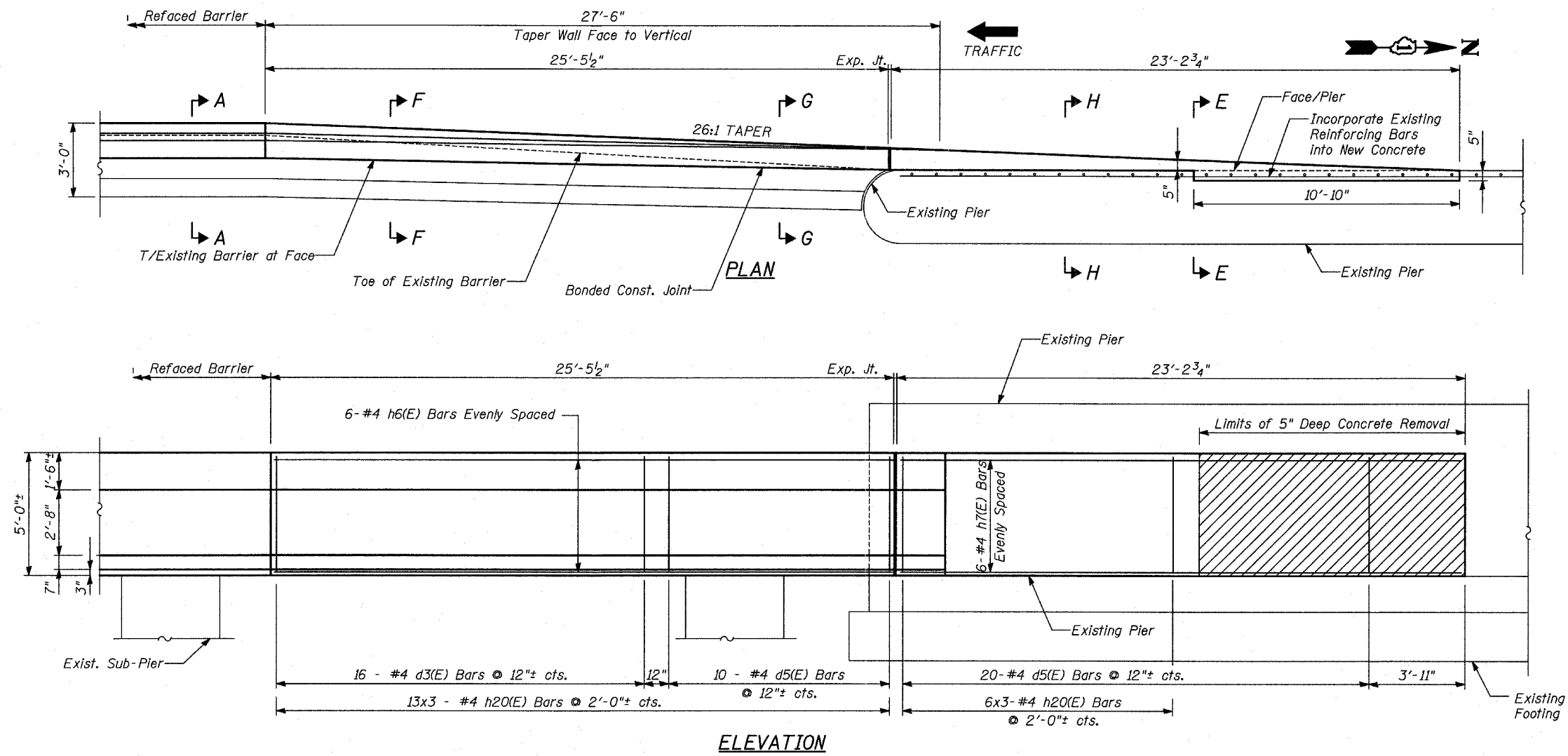
* Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

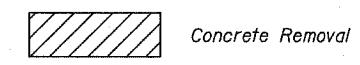
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 79TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN AND ELEVATION

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: SP



ELEVATION

LEGEND:



NOTES:

1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see Sheet B7.
6. For locations of Transition, see Roadway Plans.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d3(E)	16	#4	4'-9"	
d5(E)	30	#4	4'-8"	
h6(E)	6	#4	25'-0"	
h7(E)	6	#4	22'-8"	
h20(E)	57	#4	1'-6"	
Item	Unit	Quantity		
Reinforcement Bars, Epoxy Coated	Pound	400		
* Concrete Structures	Cu. Yd.	6		
Protective Coat	Sq. Yd.	32		
Concrete Removal	Cu. Yd.	1		

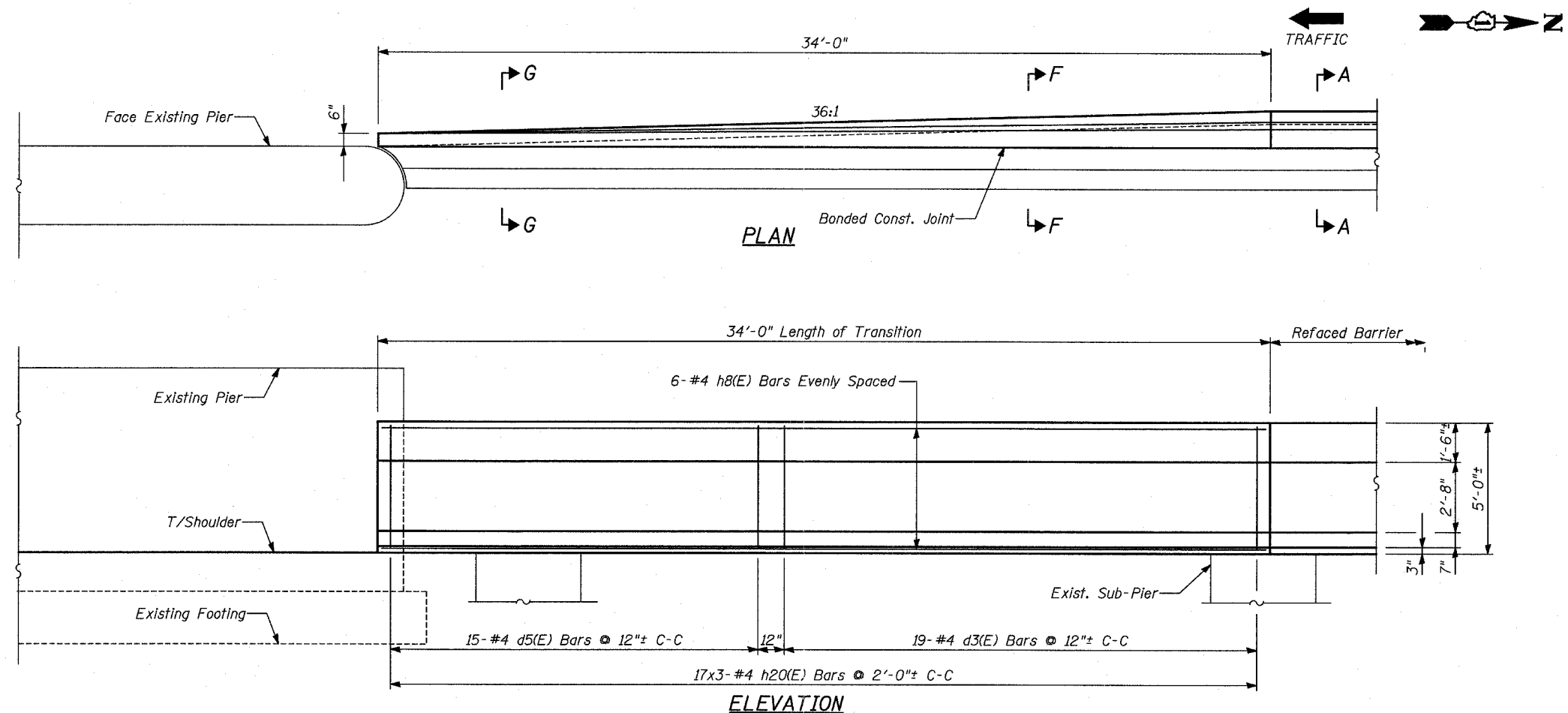
The pay items for the soldier pile wall are included in the quantities on Sheet B2.
 * Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

SHEET B5 OF 8

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 87TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN AND ELEVATION

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: SP



BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
h8(E)	6	#4	33'-8"	—	
h20(E)	51	#4	1'-6"	└	
d3(E)	19	#4	4'-9"	—	
d5(E)	15	#4	4'-8"	—	
Item				Unit	Quantity
Reinforcement Bars, Epoxy Coated				Pound	300
* Concrete Structures				Cu. Yd.	4
Protective Coat				Sq. Yd.	24

NOTES:

1. Drilling and grouting of #4 h20(E) bars shall be in accordance with Section 584 of the Standard Specifications.
2. The cost of drilling holes, furnishing and placing Epoxy Grout and setting #4 h20(E) bars shall be included in the cost for Concrete Structures.
3. Provide joints in facing to match those in the existing barrier. See Sht B1 for joint details.
4. For Refaced Barrier details, see Sheet B2.
5. For Sections A-A thru H-H see sheet B7.
6. For locations of Transition, see Roadway Plans.

The pay items for the soldier pile wall are included in the quantities on Sheet B2.

* Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

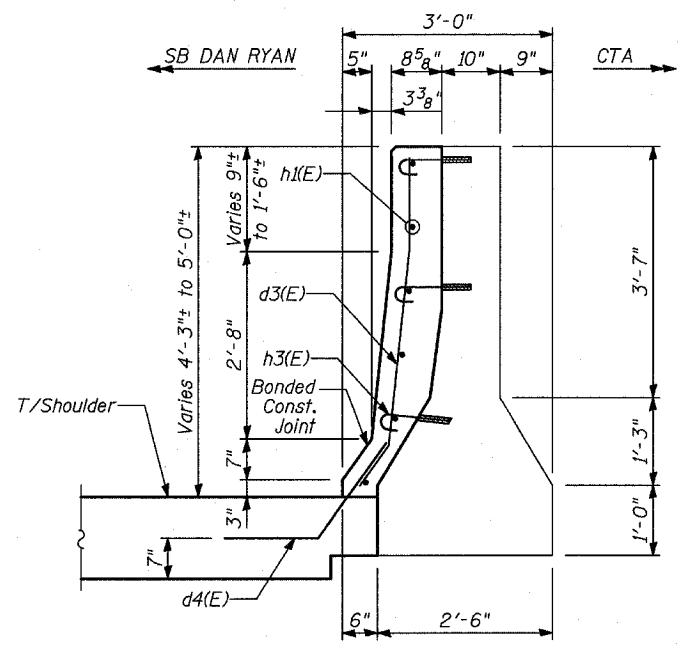
SHEET B6 OF 8

TYLIN INTERNATIONAL

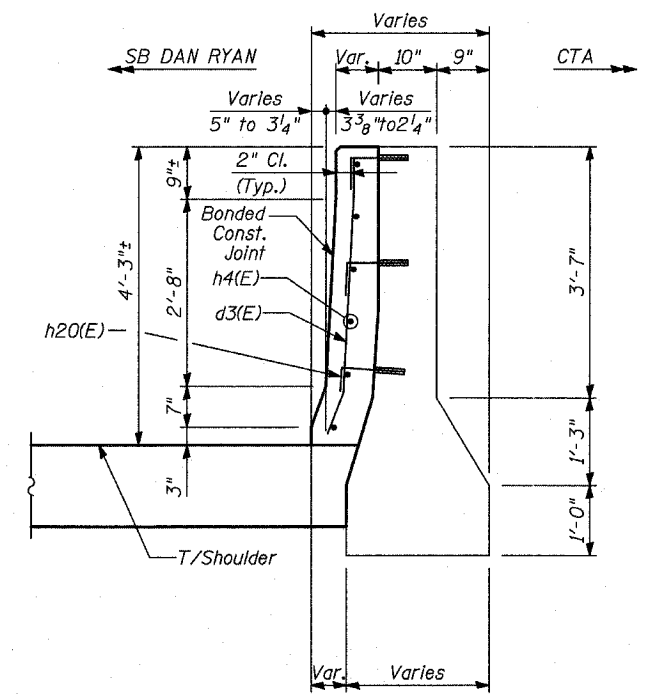
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 CTA STATION BARRIER WALL MODIFICATIONS
 95TH STREET STATION
 REFACED BARRIER TO PIER TRANSITION
 DETAILS: PLAN AND ELEVATION

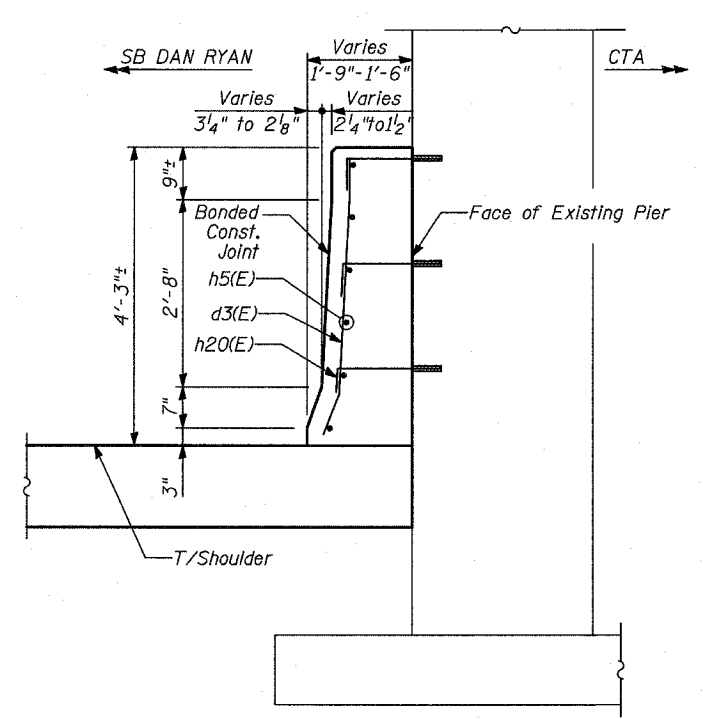
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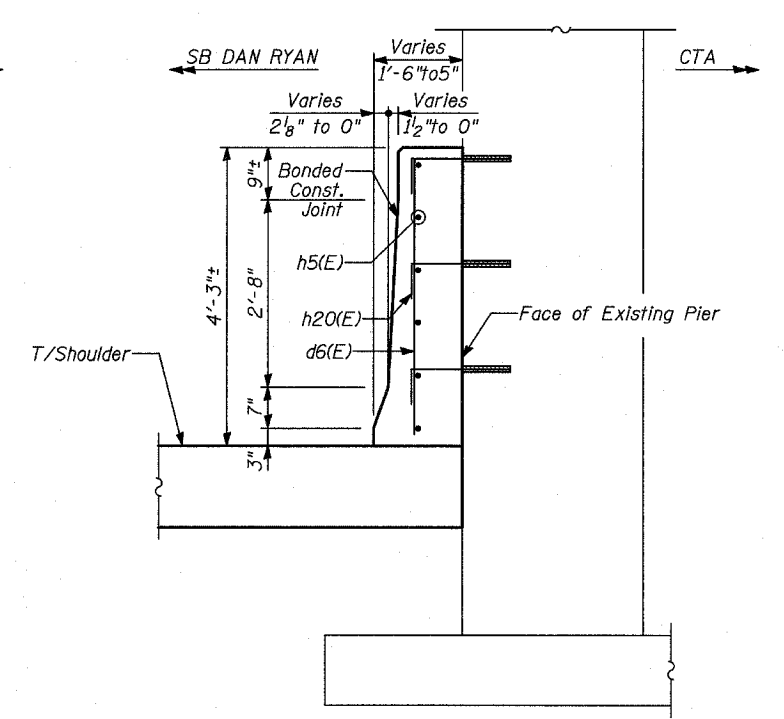
SECTION A-A



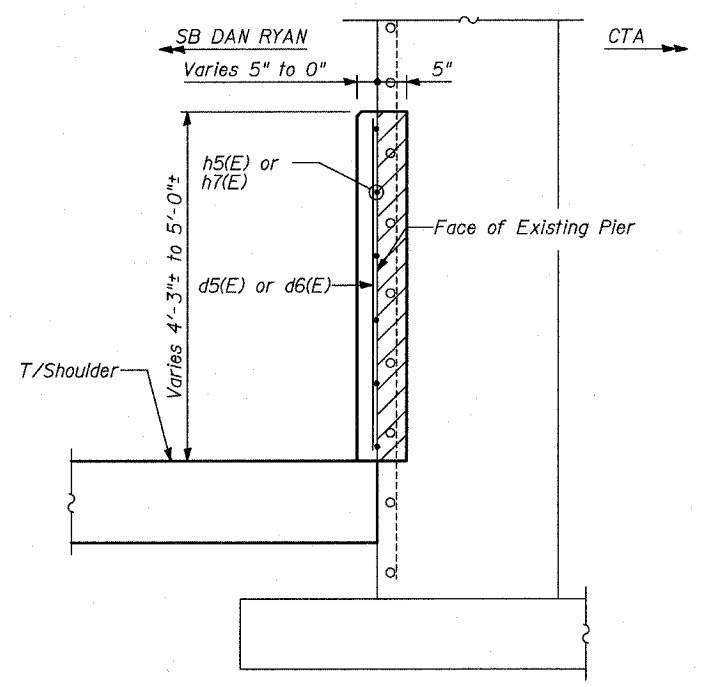
SECTION B-B



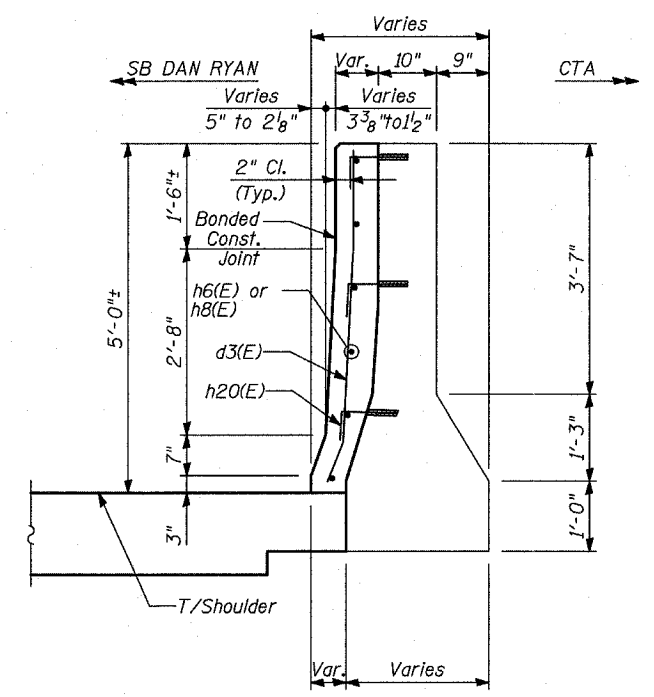
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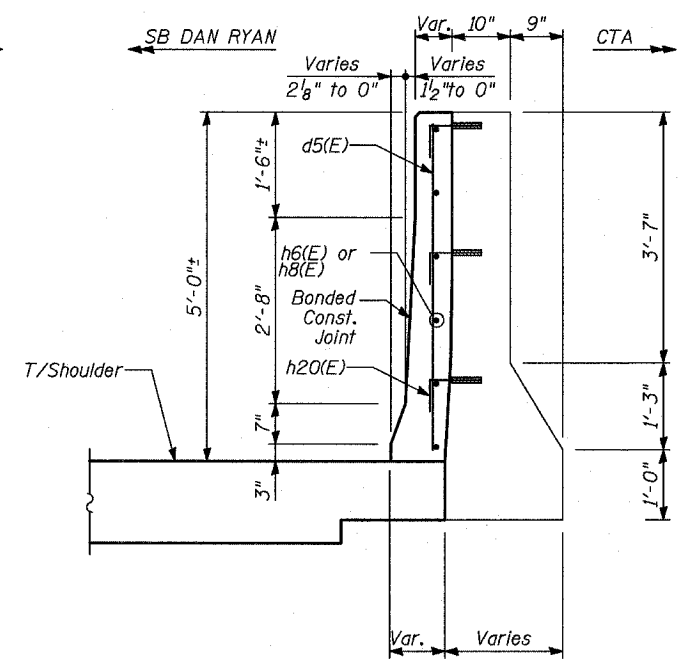
SECTION D-D



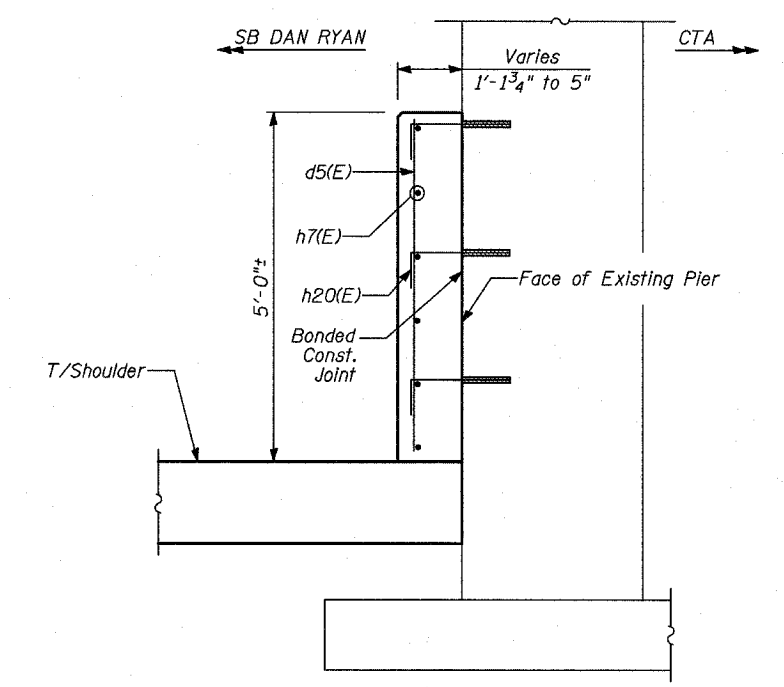
SECTION E-E



SECTION F-F



SECTION G-G



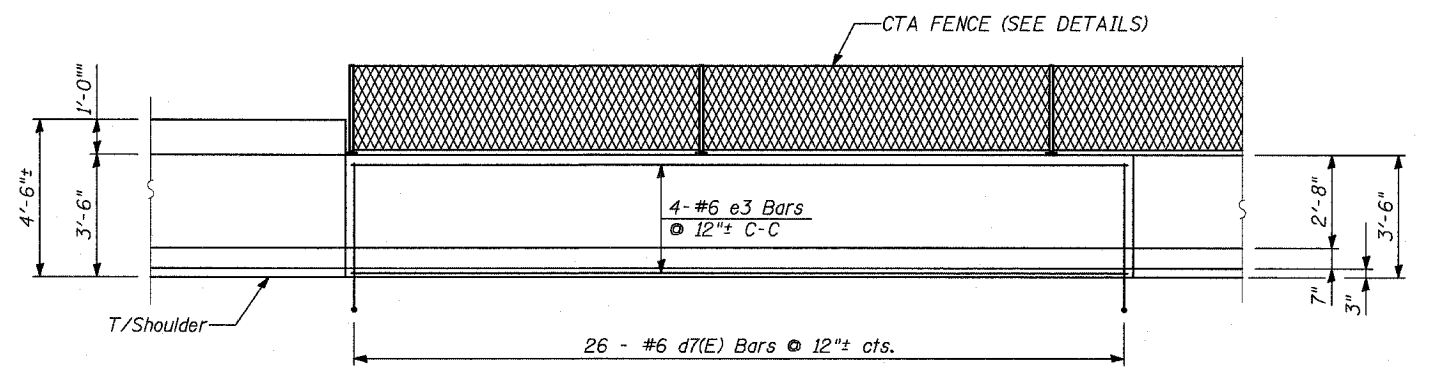
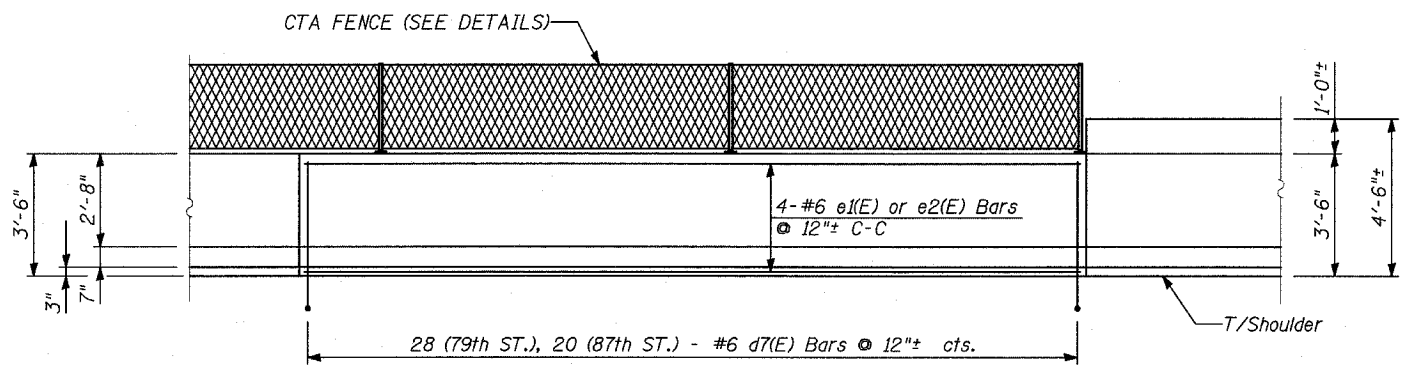
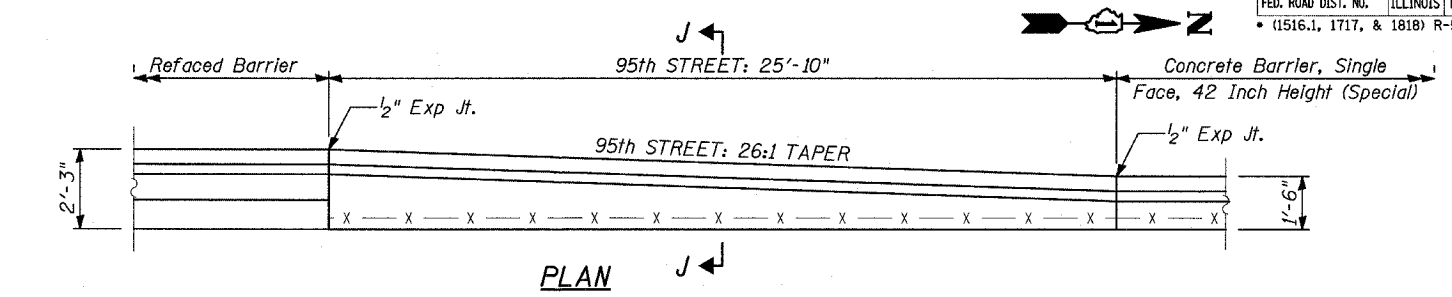
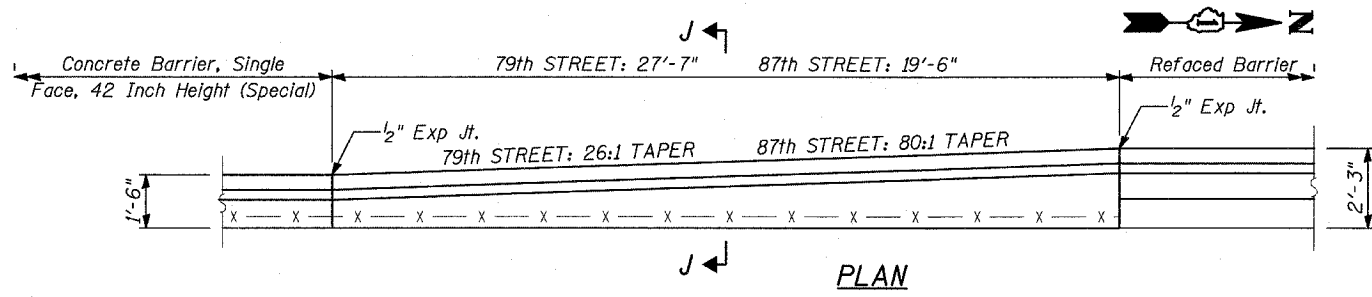
SECTION H-H

NOTE
Drill and Epoxy Grout h3(E) and h20(E) Bars
(5" Min. Embedment)

TYLIN INTERNATIONAL

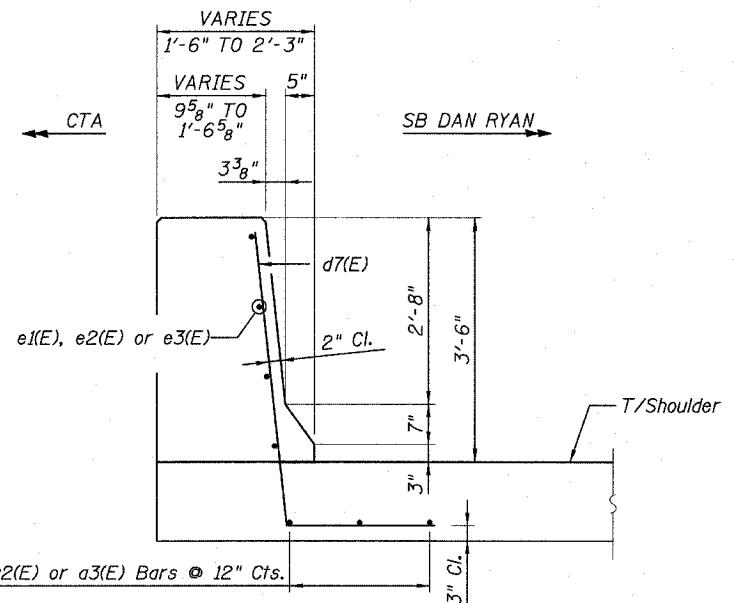
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
CTA STATION BARRIER WALL MODIFICATIONS
79TH, 87TH AND 95TH STREET STATIONS
REFACED BARRIER TO PIER TRANSITION
DETAILS: TYPICAL SECTIONS
SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: SP



ELEVATION

ELEVATION



SECTION J-J

BILL OF MATERIAL

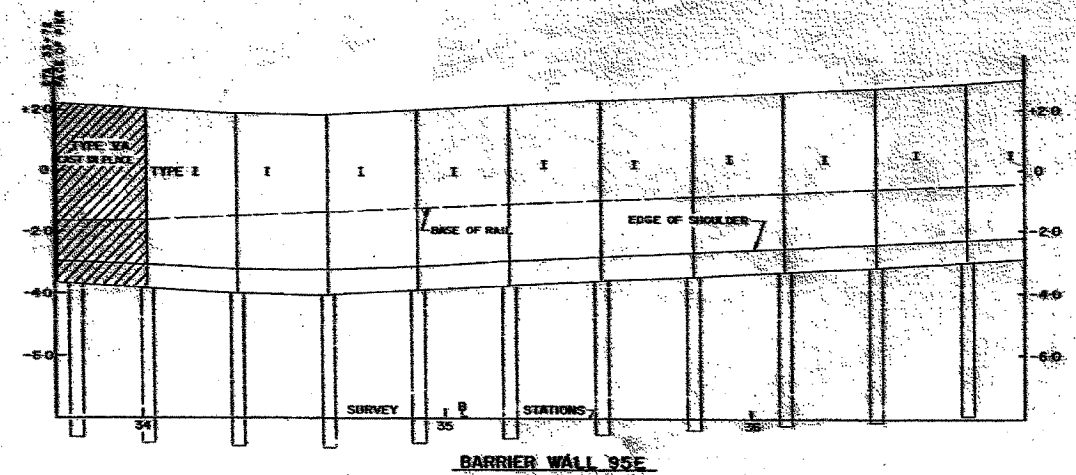
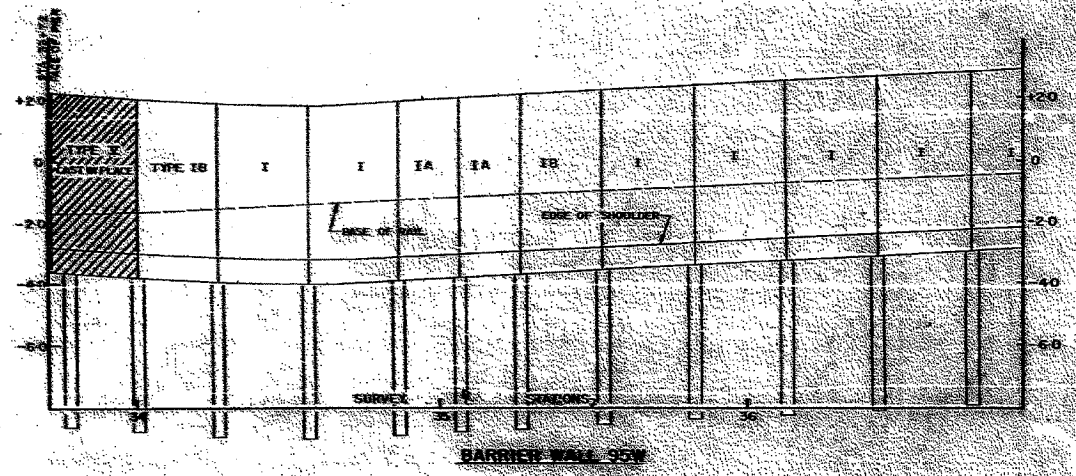
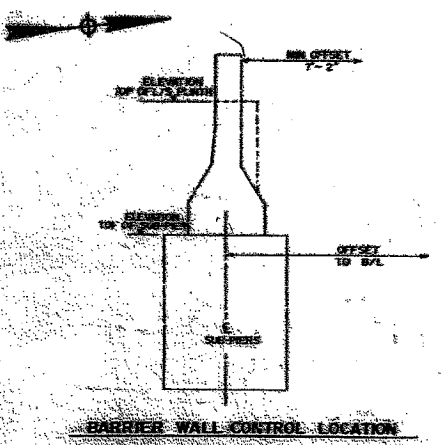
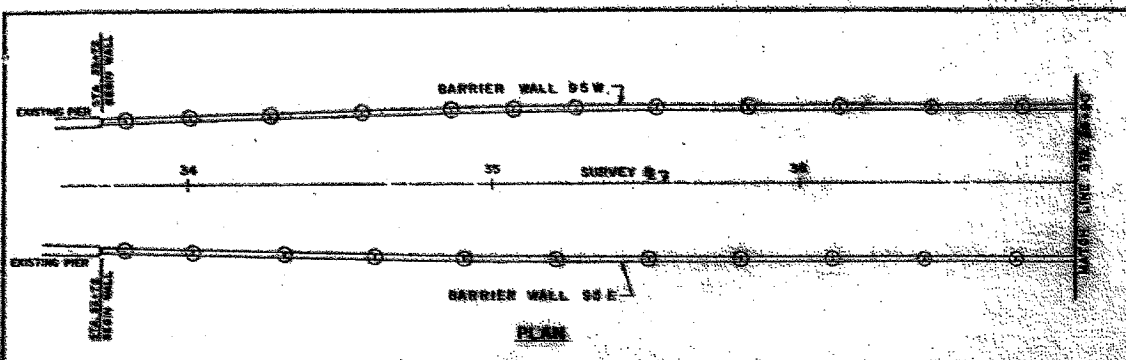
Bar	79th No.	87th No.	95th No.	Size	Length	Shape
a1(E)	3	-	-	#5	27'-3"	—
a2(E)	-	3	-	#5	19'-2"	—
a3(E)	-	-	3	#5	25'-6"	—
d7(E)	28	20	26	#6	6'-3"	┌
e1(E)	4	-	-	#6	27'-3"	—
e2(E)	-	4	-	#6	19'-2"	—
e3(E)	-	-	4	#6	25'-6"	—
Item Unit 79th Qty 87th Qty 95th Qty						
Reinforcement Bars, Epoxy Coated	Pound.	520	370	480		
* Concrete Structures	Cu. Yd.	5	4	5		
Protective Coat	Sq. Yd.	16	11	15		

NOTES:

1. Provide joints in facing to match those in the Existing Barrier. See Sht B1 for joint details.
2. For Refaced Barrier details, see Sheet B2.
3. For Concrete Barrier, Single Face, 42" (Special), see Typical Section details.
5. For locations of Transition, see Roadway Plans.

* Coarse Aggregate shall meet the requirements of Article 1004.01-1004.02 of the Standard Specifications except the gradation will be limited to CA-11 or CA-14.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	F.A.I. 94 (DAN RYAN EXPRESSWAY)	
		CTA STATION BARRIER WALL MODIFICATIONS	
		79TH, 87TH AND 95TH STREET STATIONS	
		REFACED BARRIER TO NEW 42 INCH BARRIER	
		DETAILS: PLAN, ELEVATION & TYPICAL SECTION	
		SCALE: NONE	DRAWN BY: MPG
		DATE: MARCH 1, 2006	CHECKED BY: SP



BARRIER WALL 95W

STATION	TOP OF WALL	TOP OF CURB	ELEVATION	FINISH
34+00	28.50	28.50		
34+10	28.50	28.50		
34+20	28.50	28.50		
34+30	28.50	28.50		
34+40	28.50	28.50		
34+50	28.50	28.50		
34+60	28.50	28.50		
34+70	28.50	28.50		
34+80	28.50	28.50		
34+90	28.50	28.50		
35+00	28.50	28.50		
35+10	28.50	28.50		
35+20	28.50	28.50		
35+30	28.50	28.50		
35+40	28.50	28.50		
35+50	28.50	28.50		
35+60	28.50	28.50		
35+70	28.50	28.50		
35+80	28.50	28.50		
35+90	28.50	28.50		
36+00	28.50	28.50		

BARRIER WALL 95E

STATION	TOP OF WALL	TOP OF CURB	ELEVATION	FINISH
34+00	28.50	28.50		
34+10	28.50	28.50		
34+20	28.50	28.50		
34+30	28.50	28.50		
34+40	28.50	28.50		
34+50	28.50	28.50		
34+60	28.50	28.50		
34+70	28.50	28.50		
34+80	28.50	28.50		
34+90	28.50	28.50		
35+00	28.50	28.50		
35+10	28.50	28.50		
35+20	28.50	28.50		
35+30	28.50	28.50		
35+40	28.50	28.50		
35+50	28.50	28.50		
35+60	28.50	28.50		
35+70	28.50	28.50		
35+80	28.50	28.50		
35+90	28.50	28.50		
36+00	28.50	28.50		

NOTES:
 ALL STRUCTURES IN THE MARGINE
 HEIGHT OF BARRIER WALLS ON
 SHOULDER TO BE MARKED IN THE FIELD.
 FOR DETAILS OF LIGHTING STD.
 ANCHORAGE SEE SHEET S-22

NOTE:
 ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

NOTE:
 ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

In Charge: C.A.M.
 Designed by: A.T.
 Drawn by: D.S.
 Checked by: S.B.
 Approved by: R.T.B.

APPROVED AS WORKING PLAN
 MAY 27 1968
 REVISIONS OF WORKING PLAN
 K. SCHAEFER
 K. SCHAEFER

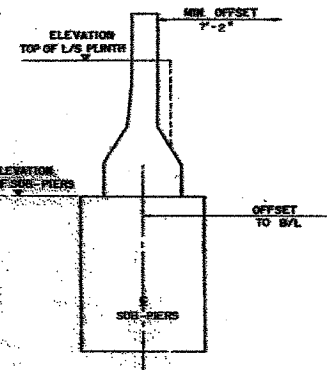
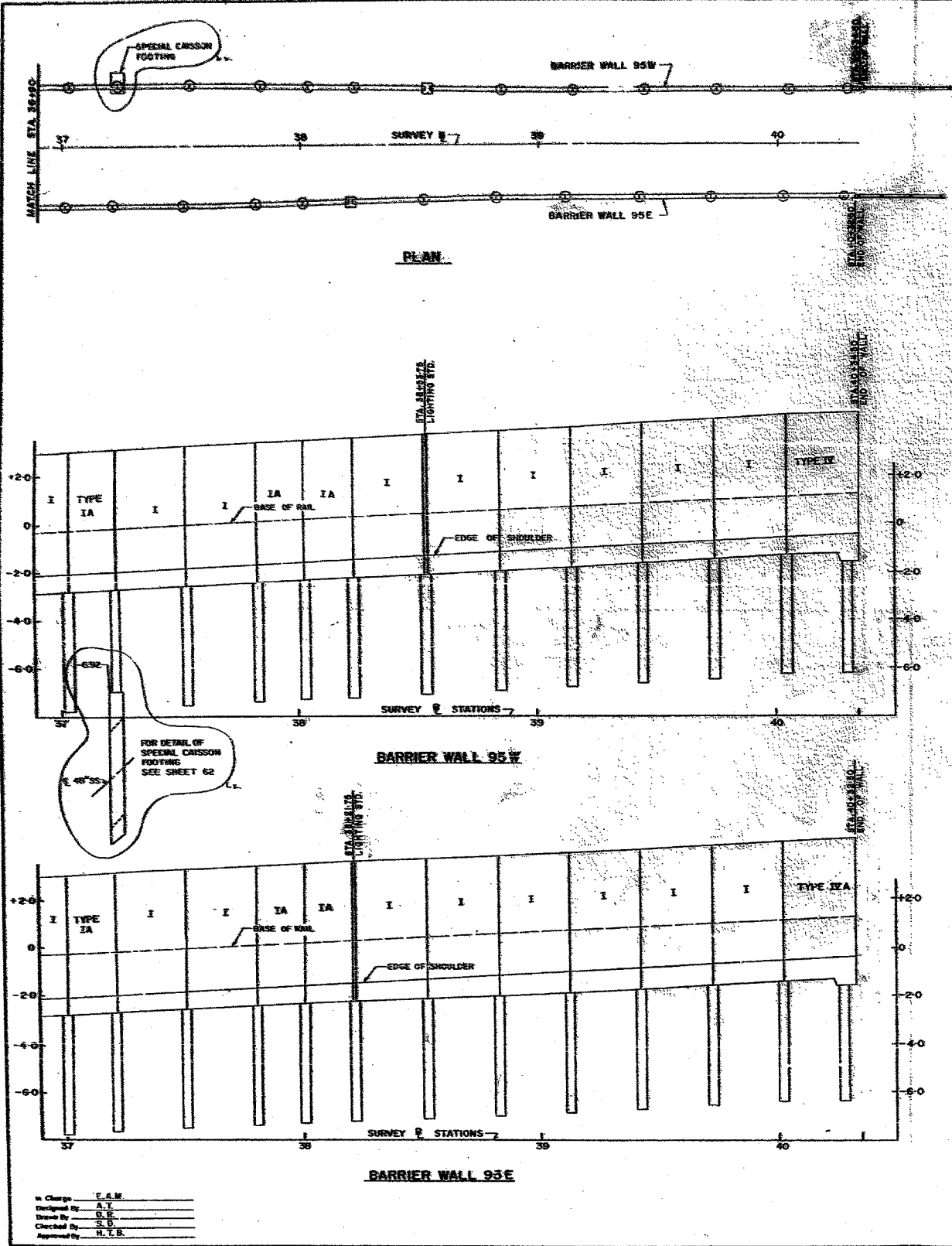
CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA 33 + 72 TO STA 36+90
 HORIZ. SCALE: 1" = 20'
 VERT. SCALE: 1" = 2'
 SHEET NO. 58 OF 211 SHEETS
 APRIL, 1968

TYLIN INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 95TH STREET (1 OF 2)
 SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: TGB



NOTES:
 1. ALL STRUCTURES IN THE IMMEDIATE VICINITY OF THE BARRIER WALLS OR SIDEPIERS TO BE VERIFIED IN THE FIELD.
 2. SEE DETAILS OF EXISTING STD. ANCHORAGE SEE SHEET S-22.

BARRIER WALL 95W

STATION	DEPTH OF SUBPIERS	ELEVATION TOP OF SUBPIERS	ELEVATION TOP OF SUBPIERS	ELEVATION TOP OF SUBPIERS	REMARKS
37+00	26.30	-2.30			
37+10	26.30	-2.30			
37+20	26.30	-2.30			
37+30	26.30	-2.30			
37+40	26.30	-2.30			
37+50	26.30	-2.30			
37+60	26.30	-2.30			
37+70	26.30	-2.30			
37+80	26.30	-2.30			
37+90	26.30	-2.30			
38+00	26.30	-2.30			
38+10	26.30	-2.30			
38+20	26.30	-2.30			
38+30	26.30	-2.30			
38+40	26.30	-2.30			
38+50	26.30	-2.30			
38+60	26.30	-2.30			
38+70	26.30	-2.30			
38+80	26.30	-2.30			
38+90	26.30	-2.30			
39+00	26.30	-2.30			
39+10	26.30	-2.30			
39+20	26.30	-2.30			
39+30	26.30	-2.30			
39+40	26.30	-2.30			
39+50	26.30	-2.30			
39+60	26.30	-2.30			
39+70	26.30	-2.30			
39+80	26.30	-2.30			
39+90	26.30	-2.30			
40+00	26.30	-2.30			

BARRIER WALL 95E

STATION	DEPTH OF SUBPIERS	ELEVATION TOP OF SUBPIERS	ELEVATION TOP OF SUBPIERS	ELEVATION TOP OF SUBPIERS	REMARKS
37+00	26.30	-2.30			
37+10	26.30	-2.30			
37+20	26.30	-2.30			
37+30	26.30	-2.30			
37+40	26.30	-2.30			
37+50	26.30	-2.30			
37+60	26.30	-2.30			
37+70	26.30	-2.30			
37+80	26.30	-2.30			
37+90	26.30	-2.30			
38+00	26.30	-2.30			
38+10	26.30	-2.30			
38+20	26.30	-2.30			
38+30	26.30	-2.30			
38+40	26.30	-2.30			
38+50	26.30	-2.30			
38+60	26.30	-2.30			
38+70	26.30	-2.30			
38+80	26.30	-2.30			
38+90	26.30	-2.30			
39+00	26.30	-2.30			
39+10	26.30	-2.30			
39+20	26.30	-2.30			
39+30	26.30	-2.30			
39+40	26.30	-2.30			
39+50	26.30	-2.30			
39+60	26.30	-2.30			
39+70	26.30	-2.30			
39+80	26.30	-2.30			
39+90	26.30	-2.30			
40+00	26.30	-2.30			

CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA. 36+90 TO STA 40 + 34.50
 HORZ 1"= 20'
 SCALE: VERT. 1"= 2'
 SHEET NO. S 9 OF 21 SHEETS
 APRIL, 1968

APPROVED AS WORKING PLAN
 REVIEWED AS WORKING PLAN

In Charge: F.A.M.
 Designed by: A.T.
 Drawn by: D.R.
 Checked by: S.D.
 Approved by: H.T.B.

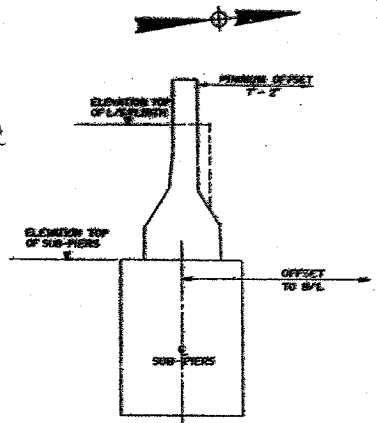
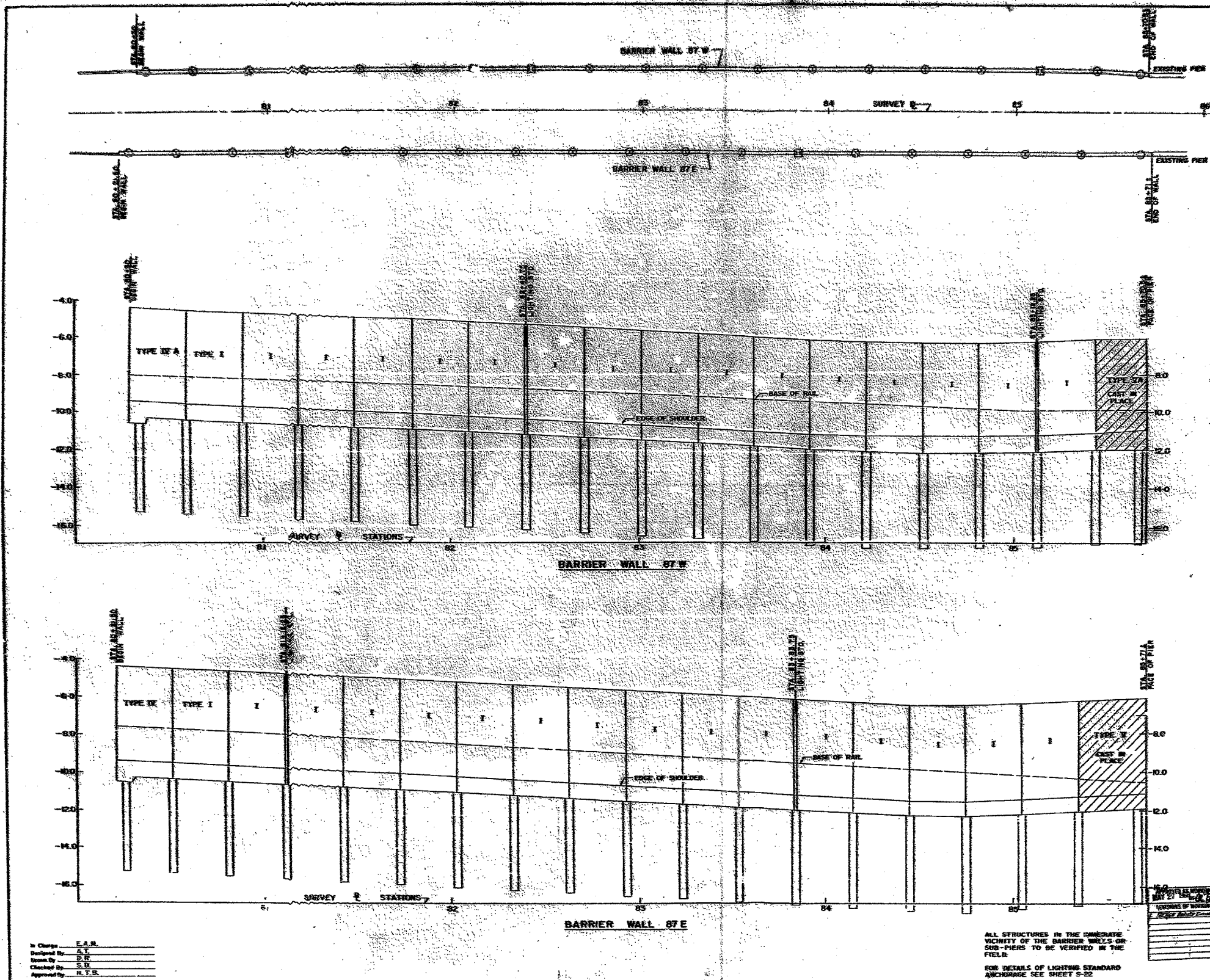
FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 95TH STREET (2 OF 2)

SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 1, 2006 CHECKED BY: TGB



BARRIER WALL CONTROL LOCATION

BARRIER WALL 87W

STATION	TYPE	HEIGHT	REMARKS
81+00	TYPE IA	11.50	
81+10	TYPE I	11.50	
81+20	I	11.50	
81+30	I	11.50	
81+40	I	11.50	
81+50	I	11.50	
81+60	I	11.50	
81+70	I	11.50	
81+80	I	11.50	
81+90	I	11.50	
82+00	I	11.50	
82+10	I	11.50	
82+20	I	11.50	
82+30	I	11.50	
82+40	I	11.50	
82+50	I	11.50	
82+60	I	11.50	
82+70	I	11.50	
82+80	I	11.50	
82+90	I	11.50	
83+00	I	11.50	
83+10	I	11.50	
83+20	I	11.50	
83+30	I	11.50	
83+40	I	11.50	
83+50	I	11.50	
83+60	I	11.50	
83+70	I	11.50	
83+80	I	11.50	
83+90	I	11.50	
84+00	I	11.50	
84+10	I	11.50	
84+20	I	11.50	
84+30	I	11.50	
84+40	I	11.50	
84+50	I	11.50	
84+60	I	11.50	
84+70	I	11.50	
84+80	I	11.50	
84+90	I	11.50	
85+00	I	11.50	
85+10	I	11.50	
85+20	I	11.50	
85+30	I	11.50	
85+40	I	11.50	
85+50	I	11.50	
85+60	I	11.50	
85+70	I	11.50	
85+80	I	11.50	
85+90	I	11.50	
86+00	I	11.50	

NOTE: ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

BARRIER WALL 87E

STATION	TYPE	HEIGHT	REMARKS
81+00	TYPE IA	11.50	
81+10	TYPE I	11.50	
81+20	I	11.50	
81+30	I	11.50	
81+40	I	11.50	
81+50	I	11.50	
81+60	I	11.50	
81+70	I	11.50	
81+80	I	11.50	
81+90	I	11.50	
82+00	I	11.50	
82+10	I	11.50	
82+20	I	11.50	
82+30	I	11.50	
82+40	I	11.50	
82+50	I	11.50	
82+60	I	11.50	
82+70	I	11.50	
82+80	I	11.50	
82+90	I	11.50	
83+00	I	11.50	
83+10	I	11.50	
83+20	I	11.50	
83+30	I	11.50	
83+40	I	11.50	
83+50	I	11.50	
83+60	I	11.50	
83+70	I	11.50	
83+80	I	11.50	
83+90	I	11.50	
84+00	I	11.50	
84+10	I	11.50	
84+20	I	11.50	
84+30	I	11.50	
84+40	I	11.50	
84+50	I	11.50	
84+60	I	11.50	
84+70	I	11.50	
84+80	I	11.50	
84+90	I	11.50	
85+00	I	11.50	
85+10	I	11.50	
85+20	I	11.50	
85+30	I	11.50	
85+40	I	11.50	
85+50	I	11.50	
85+60	I	11.50	
85+70	I	11.50	
85+80	I	11.50	
85+90	I	11.50	
86+00	I	11.50	

NOTE: ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

CITY OF CHICAGO
DEPARTMENT OF PUBLIC WORKS
BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
MEDIAN PREPARATION & FENCING
CONTRACT DR-8
BARRIER WALL PLAN & PROFILES
STA. 80+21 TO STA. 85+71
87E & 87W

SCALE: HORIZ. 1"=20'
VERT. 1"=2'
SHEET NO. 510 OF 211 SHEETS

In Charge: F.A.M.
Designed By: A.T.
Drawn By: D.R.
Checked By: S.B.
Approved By: H.T.R.

ALL STRUCTURES IN THE IMMEDIATE VICINITY OF THE BARRIER WALLS OR SUB-PIERS TO BE VERIFIED IN THE FIELD.
FOR DETAILS OF LIGHTING STANDARD ANCHORAGE SEE SHEET 5-22

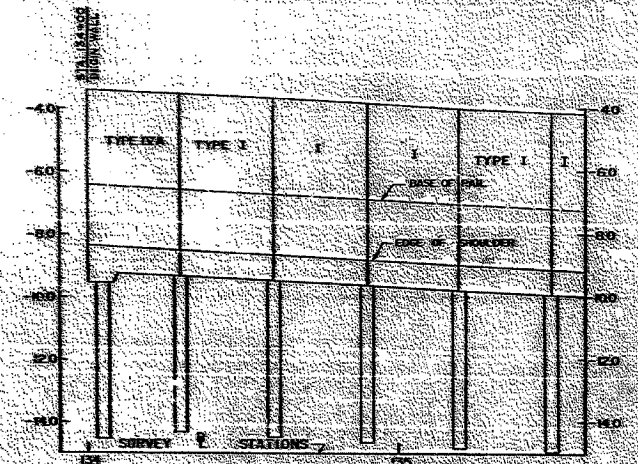
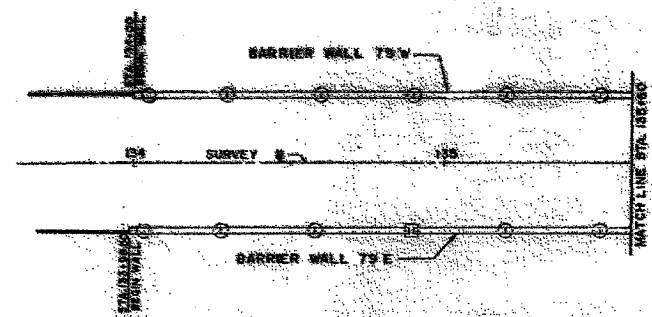
TYLIN INTERNATIONAL

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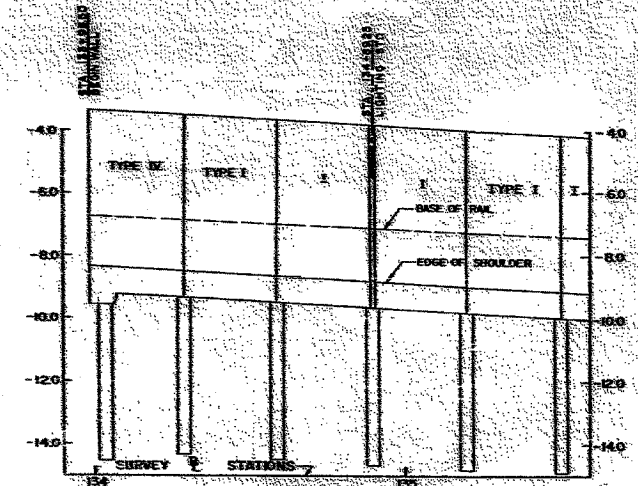
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

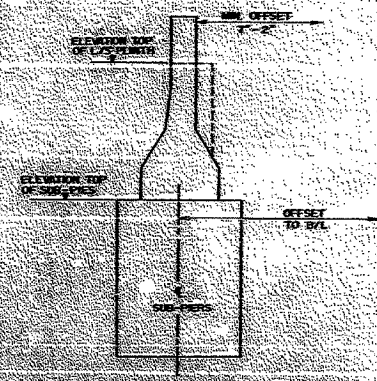
EXISTING CONCRETE BARRIER AT
CTA STATIONS:
85TH STREET (1 OF 1)
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: JUS
CHECKED BY: TGB



BARRIER WALL 79W



BARRIER WALL 79E



BARRIER WALL CROSS-SECTION LOCATION

NOTES:
 ALL DIMENSIONS IN THE UNCONCRETE PORTION OF THE BARRIER WALLS OR WALLS SHOULD BE MEASURED IN THE FIELD. FOR DETAILS OF EXISTING STD. AND GRADE SEE SHEET S-22.

BARRIER WALL 79W

STATION	DATE	BY	REVISION
134+00	2/28/68	J.S.	1
134+25	2/28/68	J.S.	2
134+50	2/28/68	J.S.	3
134+75	2/28/68	J.S.	4
135+00	2/28/68	J.S.	5

NOTE:
 ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

BARRIER WALL 79E

STATION	DATE	BY	REVISION
134+00	2/28/68	J.S.	1
134+25	2/28/68	J.S.	2
134+50	2/28/68	J.S.	3
134+75	2/28/68	J.S.	4
135+00	2/28/68	J.S.	5

NOTE:
 ALL WALLS ARE TYPE I UNLESS OTHERWISE NOTED.

APPROVED REVISIONS PLAN
REVISED BY: J.S.
DATE: 2/28/68
REVISIONS OF WORKING PLAN
DATE: 2/28/68
BY: J.S.

CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA. 133+98.50 TO STA. 135+60
 HORZ 1" = 20' 79E & 79W
 SCALE: VERT 1" = 2' APRIL, 1968
 SHEET NO. 5 OF 21 SHEETS

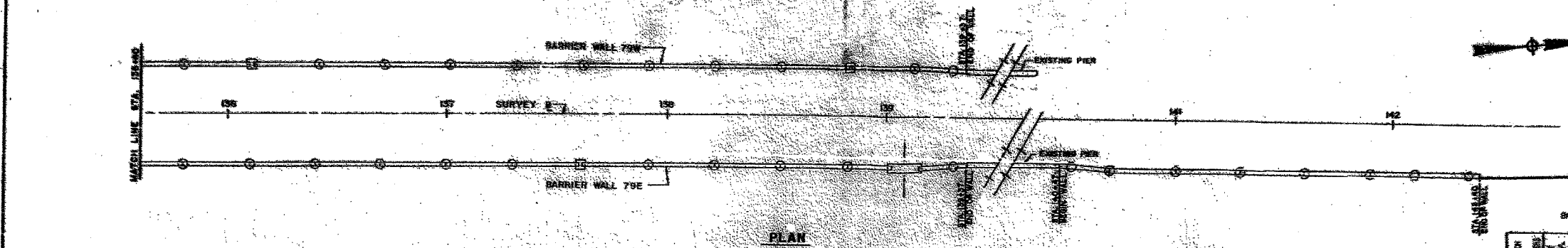
In Charge: E.A.M.
 Designed By: A.T.
 Drawn By: D.B.
 Checked By: C.B.
 Approved By: H.T.B.

TYLIN INTERNATIONAL

FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 79TH STREET (1 OF 2)
 SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 1, 2006 CHECKED BY: TGB



PLAN

BARRIER WALL 79W

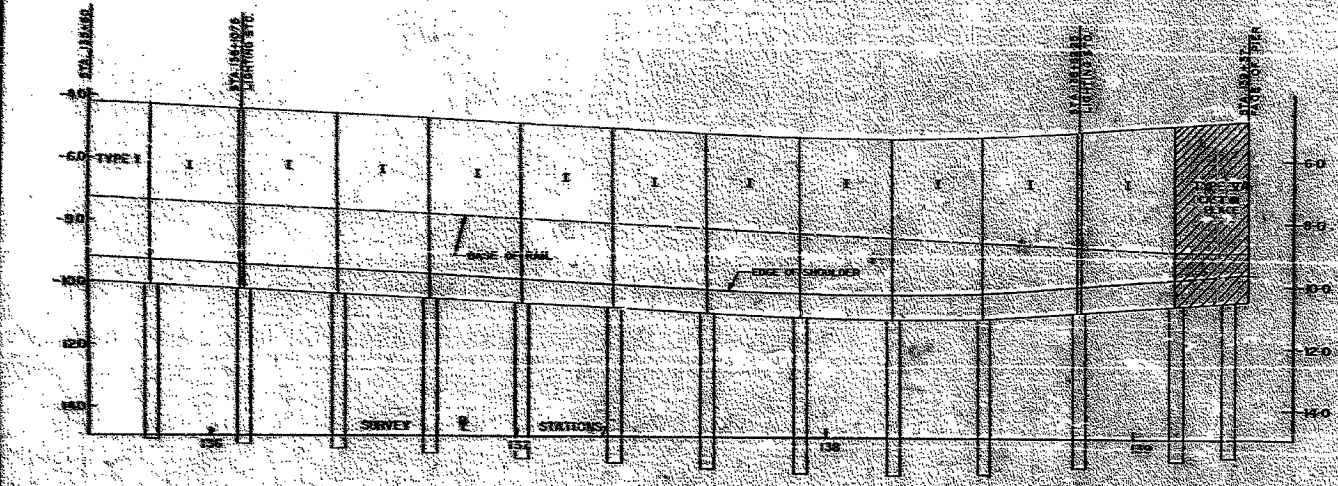
STATION	ELEVATION	HEIGHT	OFFSET	REMARKS
136+00	21.85	41.00		
136+05	21.85	41.00	4.00	EXIST. STR.
136+10	21.85	41.00		
136+15	21.85	41.00		
136+20	21.85	41.00		
136+25	21.85	41.00		
136+30	21.85	41.00		
136+35	21.85	41.00		
136+40	21.85	41.00		
136+45	21.85	41.00		
136+50	21.85	41.00		
136+55	21.85	41.00		
137+00	21.85	41.00	7.00	EXIST. STR.
137+05	21.85	41.00		
137+10	21.85	41.00		
137+15	21.85	41.00		
137+20	21.85	41.00		
137+25	21.85	41.00		
137+30	21.85	41.00		
137+35	21.85	41.00		
137+40	21.85	41.00		
137+45	21.85	41.00		
137+50	21.85	41.00		

NOTE:
 ALL DIMS ARE TYPE 1 UNLESS OTHERWISE NOTED.

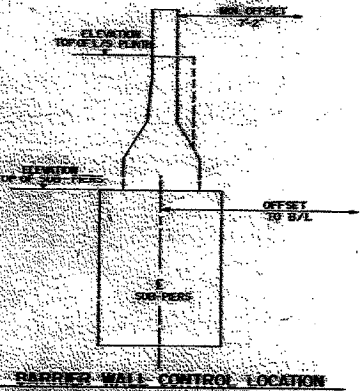
BARRIER WALL 79E

STATION	ELEVATION	HEIGHT	OFFSET	REMARKS
138+00	21.85	41.00		
138+05	21.85	41.00		
138+10	21.85	41.00		
138+15	21.85	41.00		
138+20	21.85	41.00		
138+25	21.85	41.00		
138+30	21.85	41.00		
138+35	21.85	41.00		
138+40	21.85	41.00		
138+45	21.85	41.00		
138+50	21.85	41.00		
138+55	21.85	41.00		
139+00	21.85	41.00	7.50	EXIST. STR.
139+05	21.85	41.00		
139+10	21.85	41.00		
139+15	21.85	41.00		
139+20	21.85	41.00		
139+25	21.85	41.00		
139+30	21.85	41.00		
139+35	21.85	41.00		
139+40	21.85	41.00		
139+45	21.85	41.00		
139+50	21.85	41.00		
139+55	21.85	41.00		
140+00	21.85	41.00		
140+05	21.85	41.00		
140+10	21.85	41.00		
140+15	21.85	41.00		
140+20	21.85	41.00		
140+25	21.85	41.00		
140+30	21.85	41.00		
140+35	21.85	41.00		
140+40	21.85	41.00		
140+45	21.85	41.00		
140+50	21.85	41.00		

NOTE:
 ALL DIMS ARE TYPE 1 UNLESS OTHERWISE NOTED.

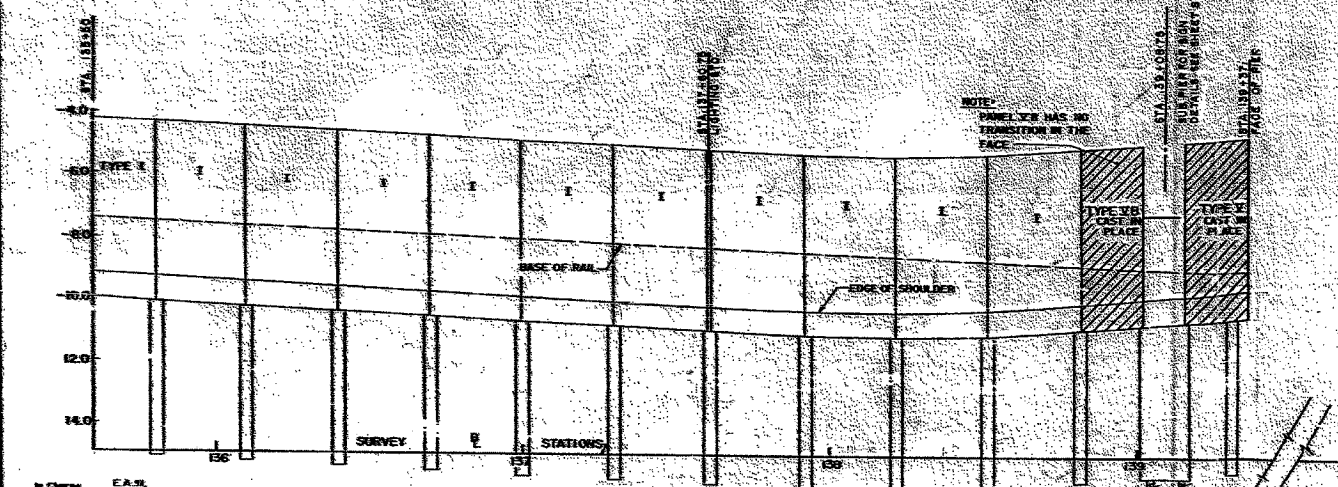


BARRIER WALL 79W



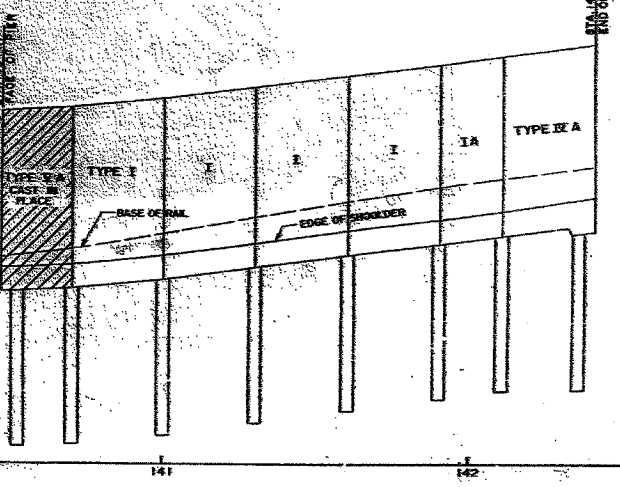
BARRIER WALL CONTROL LOCATION

NOTES:
 ALL CONCRETE SHALL BE SPREAD
 IN PLACE OR IN BARRELS OR
 FORMS TO BE SET IN THE FIELD.
 PERFORMING OF LIGHTING SETS ANCHORAGE
 SEE SHEET 197.



BARRIER WALL 79E

NOTE:
 PANEL 28 HAS NO
 TRANSITION IN THE
 FACE.



BARRIER WALL 79E

In Charge: E.A.M.
 Designed by: A.E.
 Drawn by: B.S.
 Checked by: S.D.
 Approved by: H.T.R.

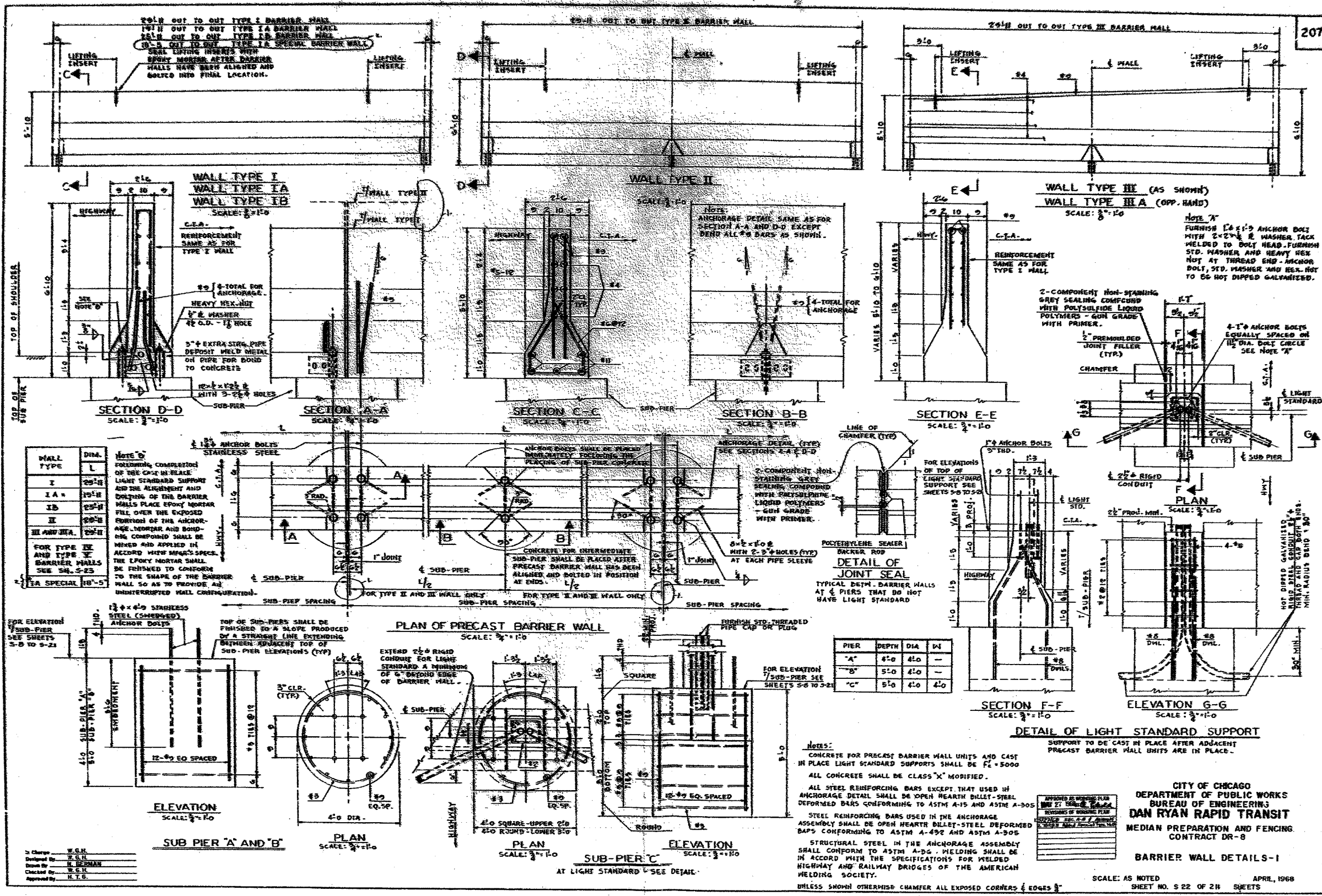
CITY OF CHICAGO
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF ENGINEERING
 DAN RYAN RAPID TRANSIT
 MEDIAN PREPARATION & FENCING
 CONTRACT DR-8
 BARRIER WALL PLAN & PROFILES
 STA. 135+60 TO STA. 142+50
 79E & 79W
 HORZ. 1" = 20'
 SCALE: VERT. 1" = 2'
 SHEET NO. 511A OF 211 SHEETS
 APRIL, 1969

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 EXISTING CONCRETE BARRIER AT
 CTA STATIONS:
 79TH STREET (2 OF 2)
 SCALE: NONE DRAWN BY: JUS
 DATE: MARCH 1, 2006 CHECKED BY: TGB

TYLIN INTERNATIONAL

FOR INFORMATION ONLY



207

TYLIN INTERNATIONAL

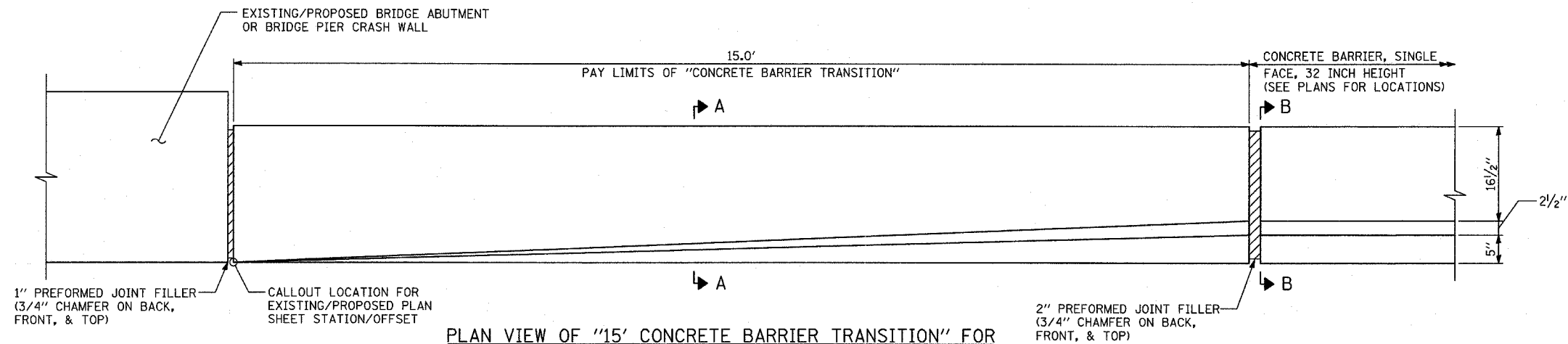
FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

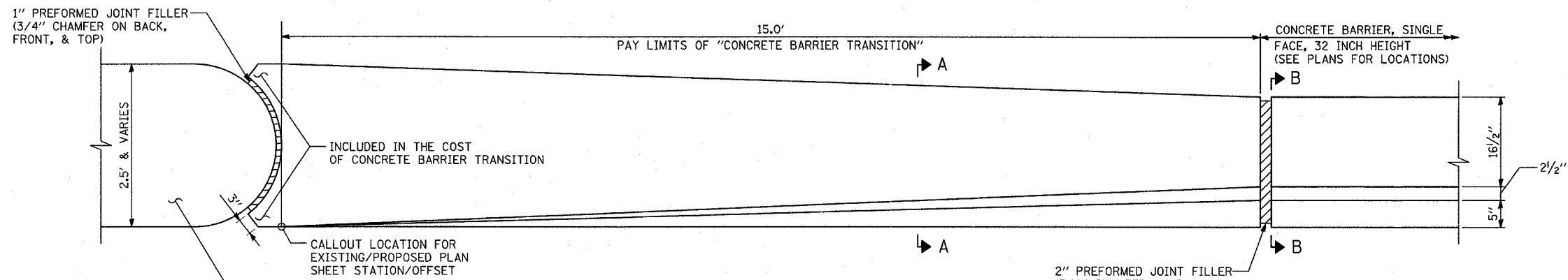
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

EXISTING CONCRETE BARRIER AT
 CTA STATIONS
 BARRIER DETAILS

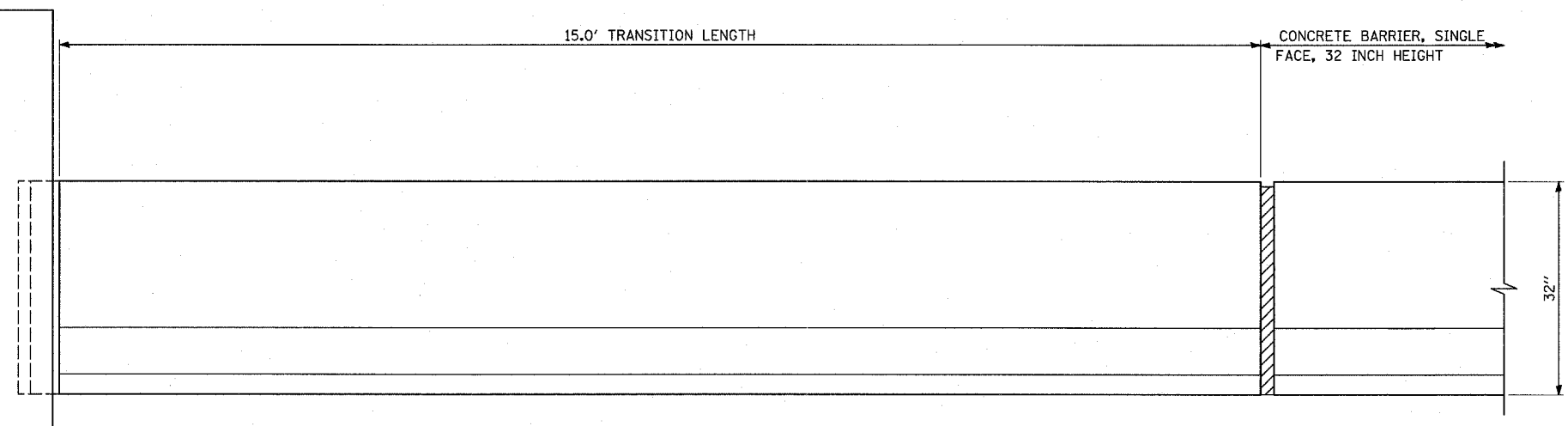
SCALE: NONE DRAWN BY: JJS
 DATE: MARCH 1, 2006 CHECKED BY: TGB



PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
ADJACENT TO SQUARE PIER/ABUTMENT



PLAN VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
ADJACENT TO ROUND PIER/ABUTMENT



ELEVATION VIEW OF "15' CONCRETE BARRIER TRANSITION" FOR
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT

NOTE:
ALL PREFORMED JOINT FILLER INCLUDED
IN THE COST OF "CONCRETE BARRIER
TRANSITION"

TYLIN INTERNATIONAL

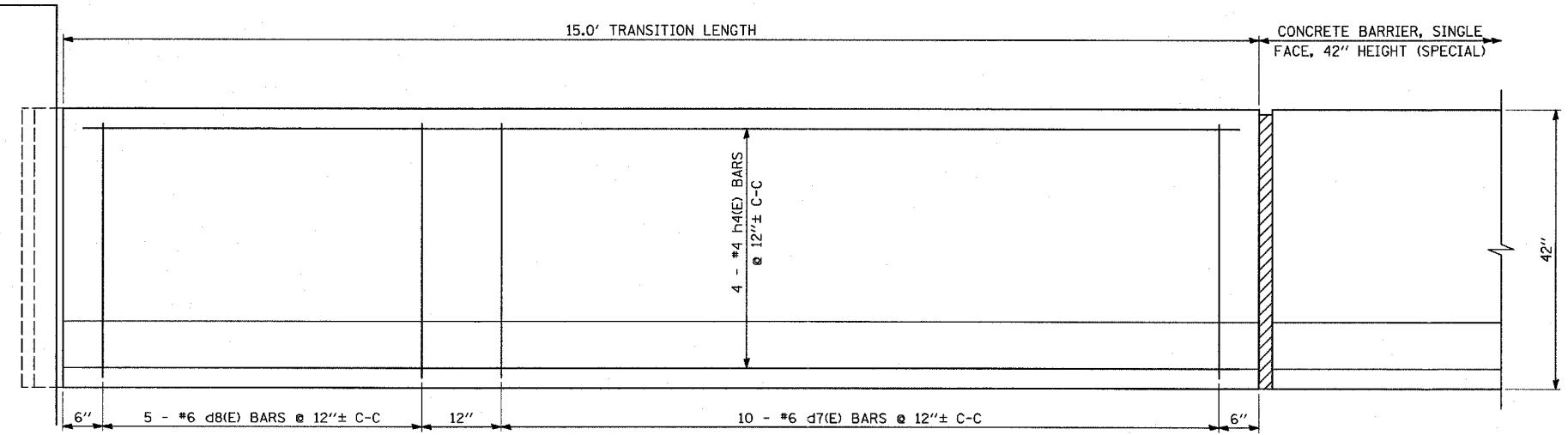
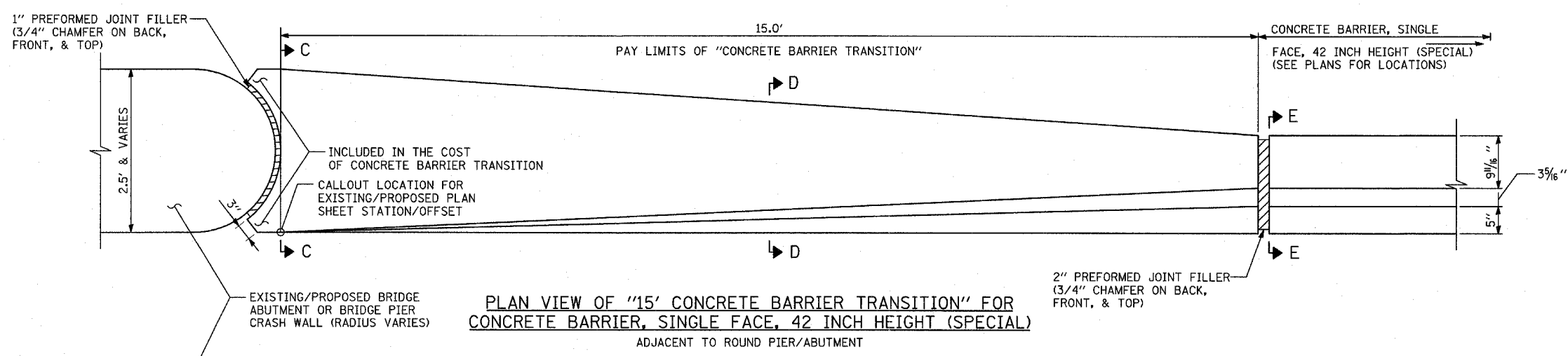
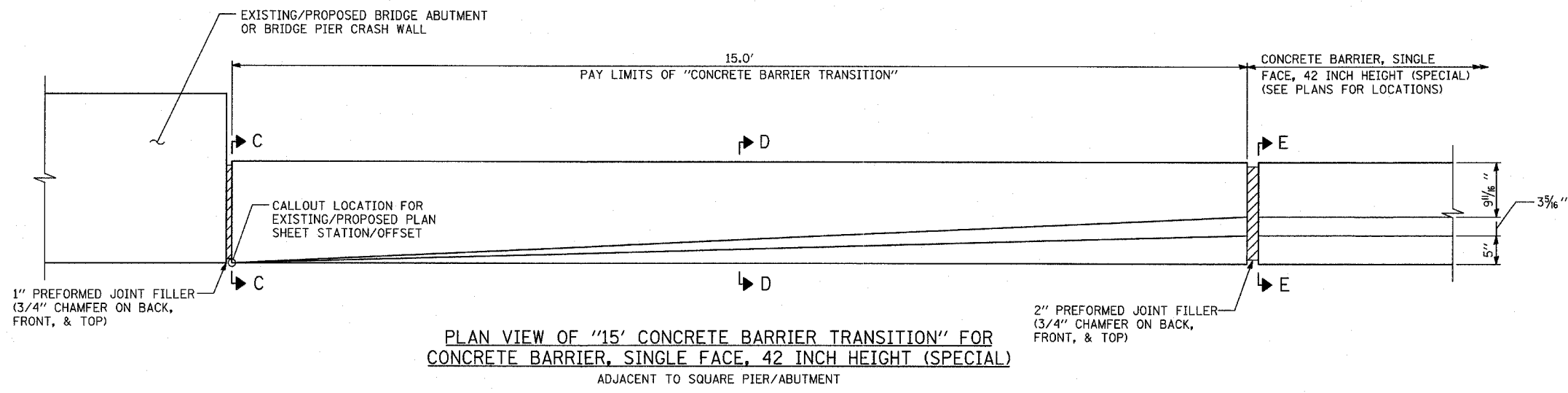
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
CONCRETE BARRIER TRANSITION
FOR CONCRETE BARRIER, 32 INCH HEIGHT

SCALE: NONE
DATE: MARCH 1, 2006

DRAWN BY: MPG
CHECKED BY: TGB



NOTE:
ALL PREFORMED JOINT FILLER INCLUDED IN THE COST OF "CONCRETE BARRIER TRANSITION"

TYLIN INTERNATIONAL

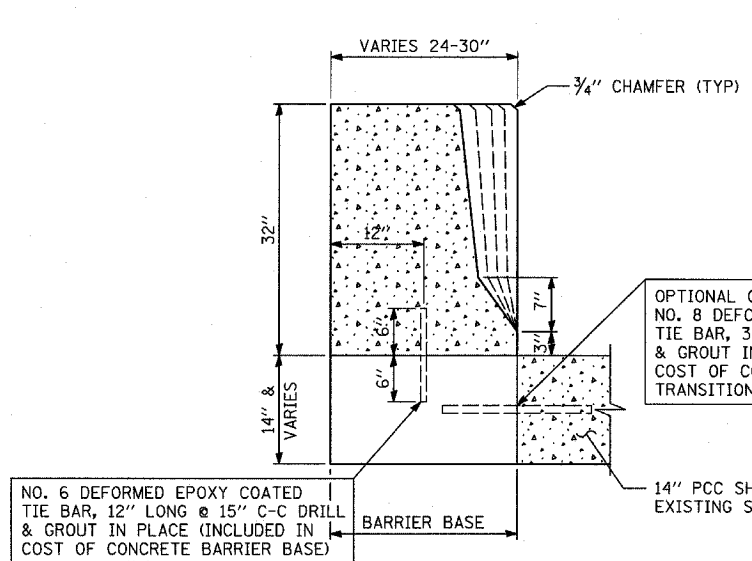
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

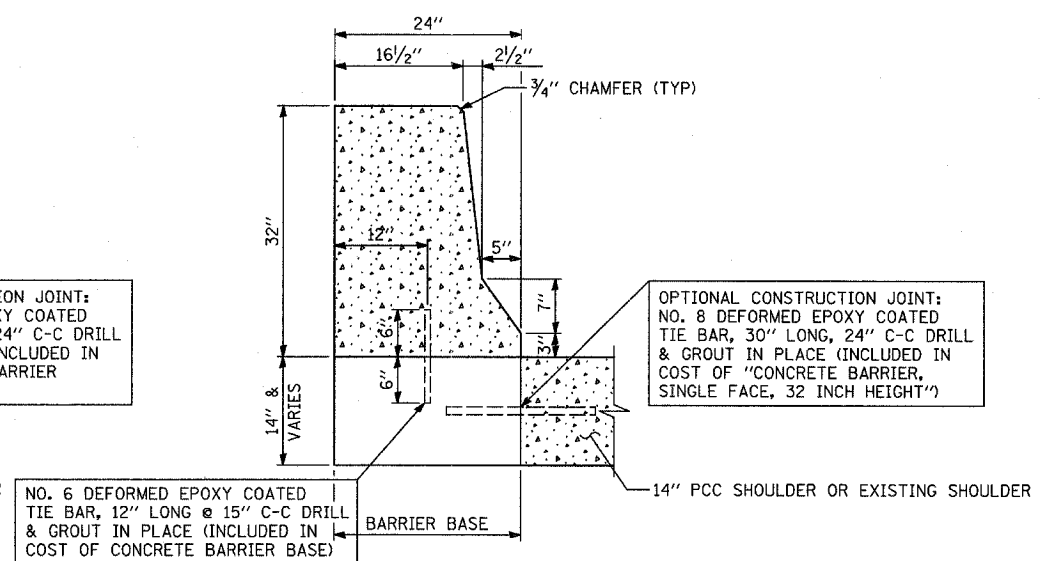
MISCELLANEOUS DETAILS
CONCRETE BARRIER TRANSITION FOR
CONCRETE BARRIER, 42 INCH HEIGHT (SPECIAL)

SCALE: NONE
DATE: MARCH 1, 2006

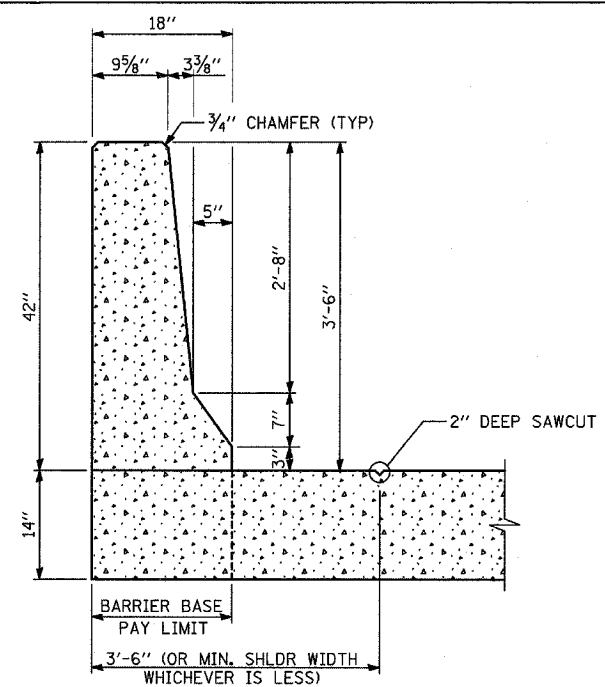
DRAWN BY: MPG
CHECKED BY: TGB



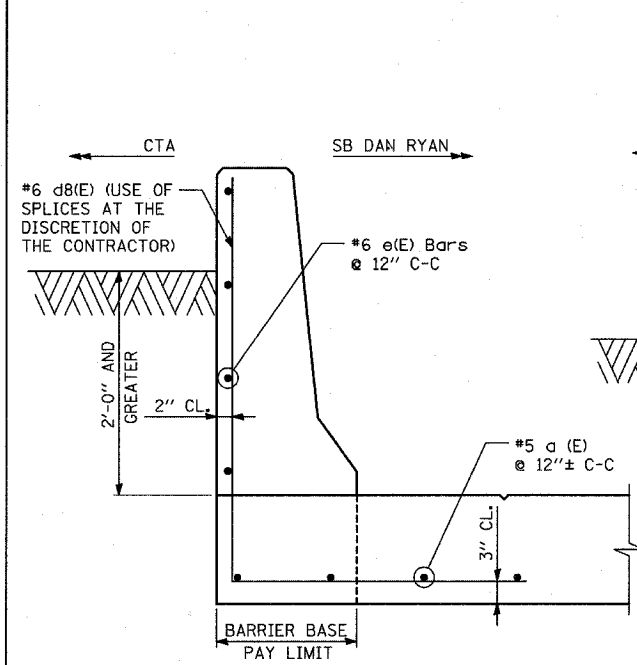
SECTION A-A
15' CONCRETE BARRIER TRANSITION
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT



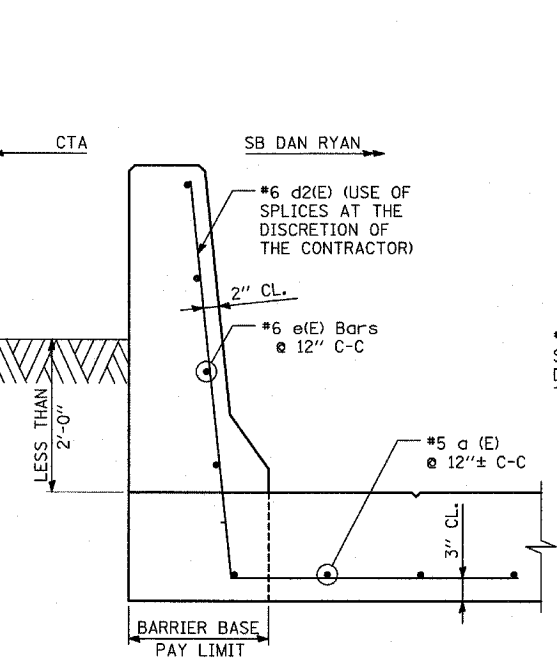
SECTION B-B
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT
& CONCRETE BARRIER BASE



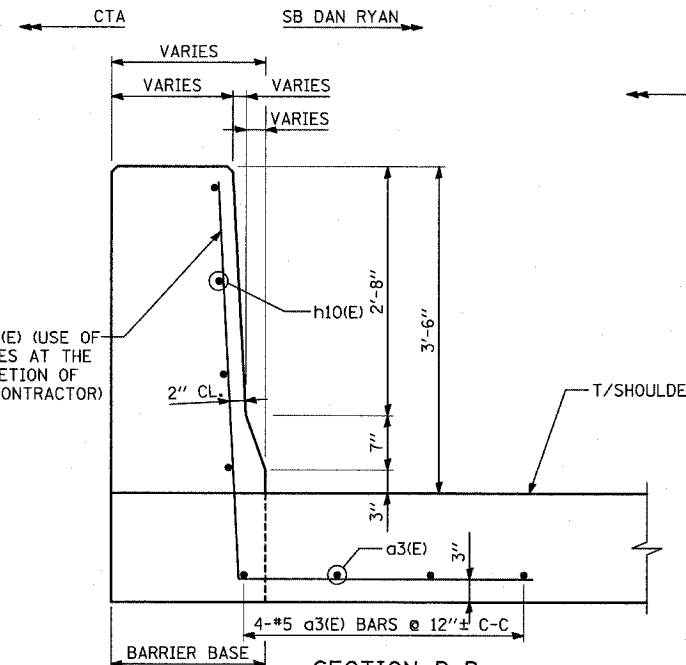
SECTION E-E
CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT
(SPECIAL) DIMENSIONS
SEE BELOW FOR REINFORCEMENT OPTIONS



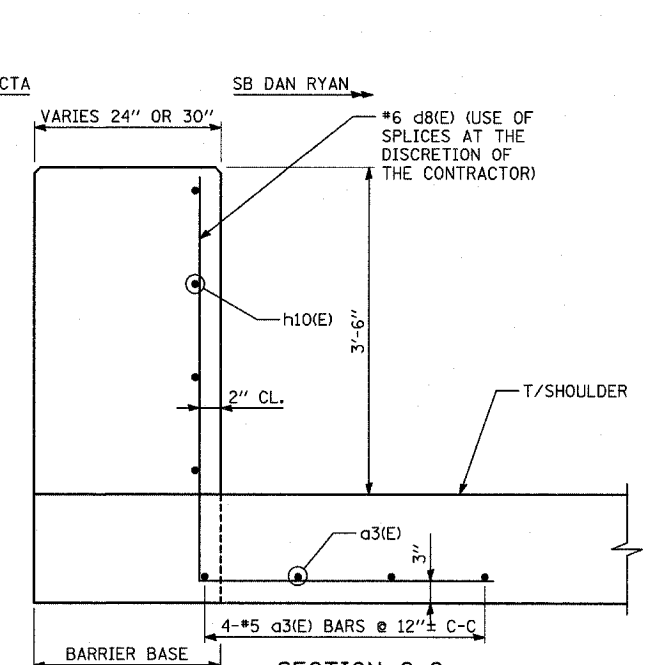
REINFORCEMENT #1
RETAINED HEIGHT GREATER THAN 2'-0"
SECTION E-E: CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



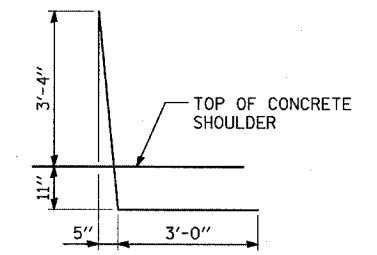
REINFORCEMENT #2
RETAINED HEIGHT LESS THAN 2'-0"



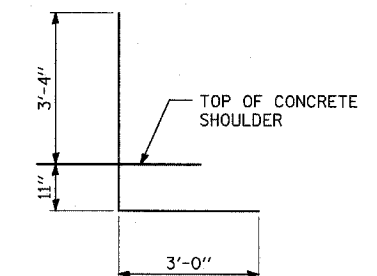
SECTION D-D
15' CONCRETE BARRIER TRANSITION
CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



SECTION C-C
15' CONCRETE BARRIER TRANSITION
CONCRETE BARRIER, SINGLE FACE, 42 INCH HEIGHT (SPECIAL)



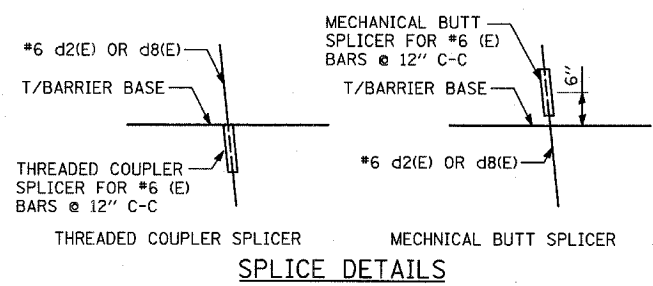
d2(E) BAR



d8(E) BAR

NOTES:

1. BAR SPLICERS SHALL BE CAPABLE OF DEVELOPING A MINIMUM OF 125% OF THE YIELD STRENGTH OF A #6 BAR.
2. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE SEATED IN THE FINAL POSITION PRIOR TO THE CONCRETE OPERATIONS. BARS CANNOT BE MUCKED INTO PLACE.
3. MAINTAIN SLOPE OF FACE AS SHOWN ON DETAILS.
4. ALL REINFORCING BARS, BAR SPLICERS, TIE BARS AND DOWEL BARS SHALL BE INCLUDED IN THE COST OF "CONCRETE BARRIER, SINGLE FACE" OF THE TYPE SPECIFIED

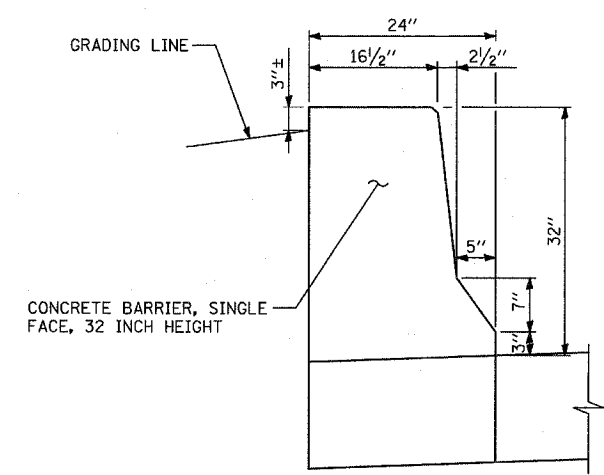
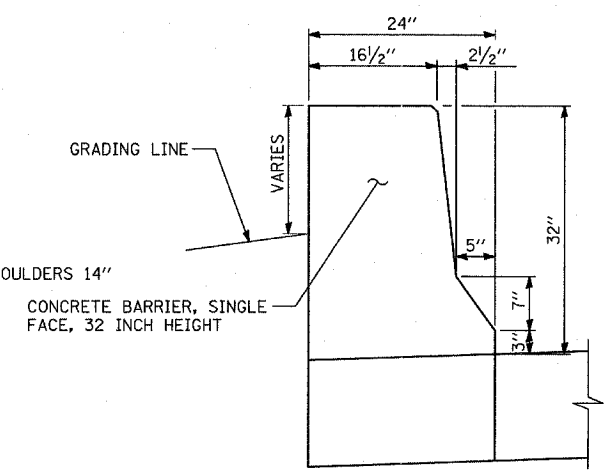
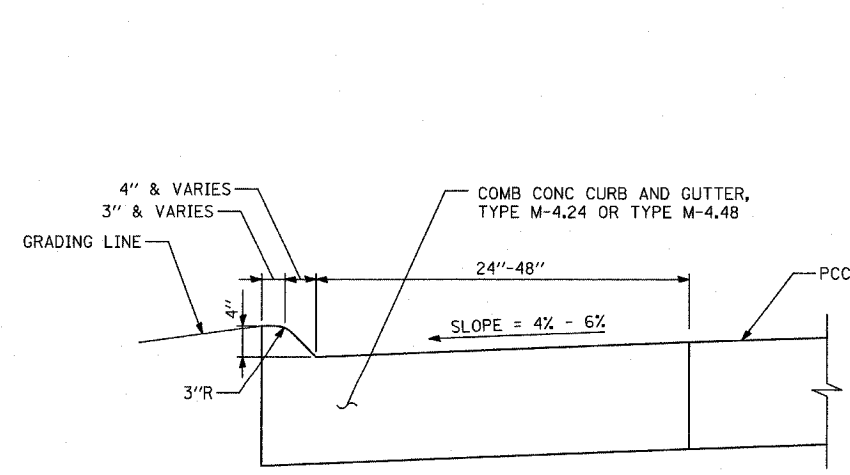
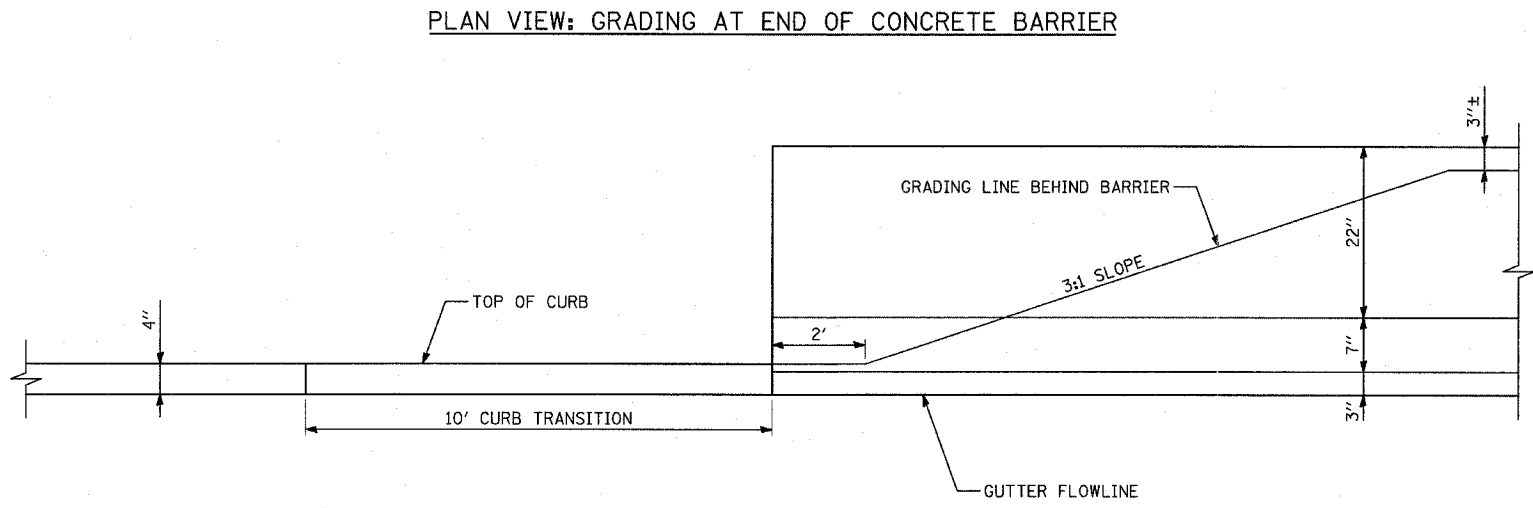
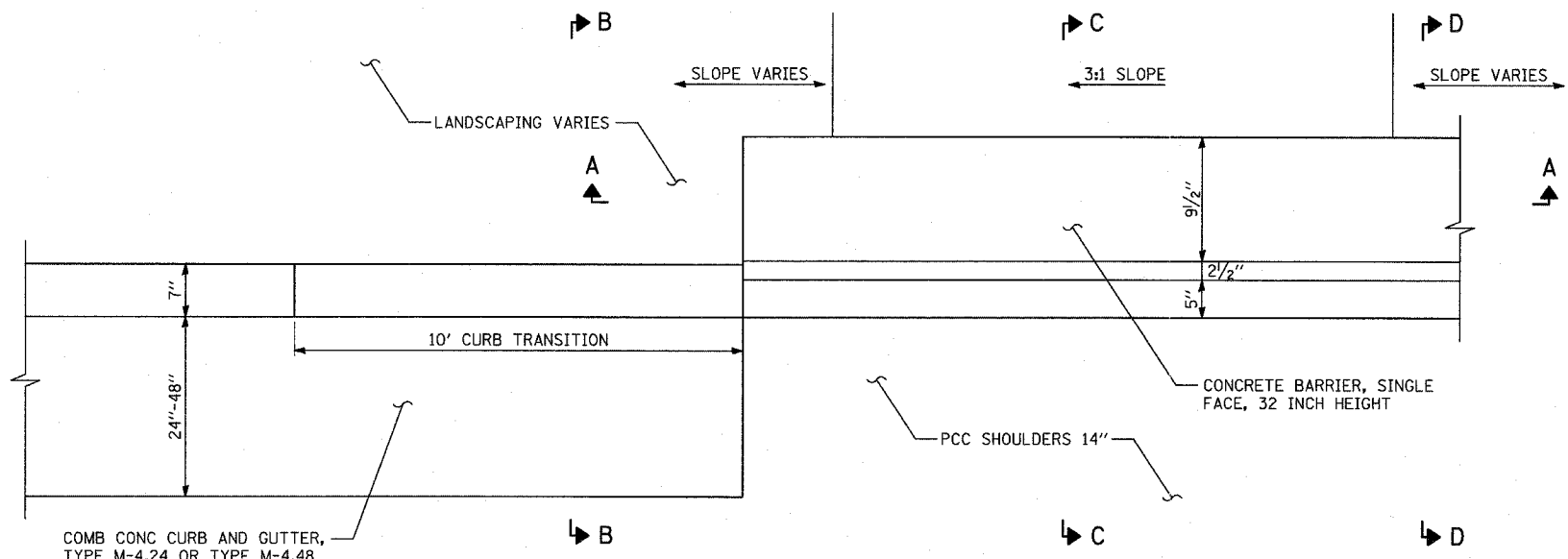


SPlice DETAILS

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MISCELLANEOUS DETAILS
CONCRETE BARRIER TRANSITION
TYPICAL SECTIONS
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB



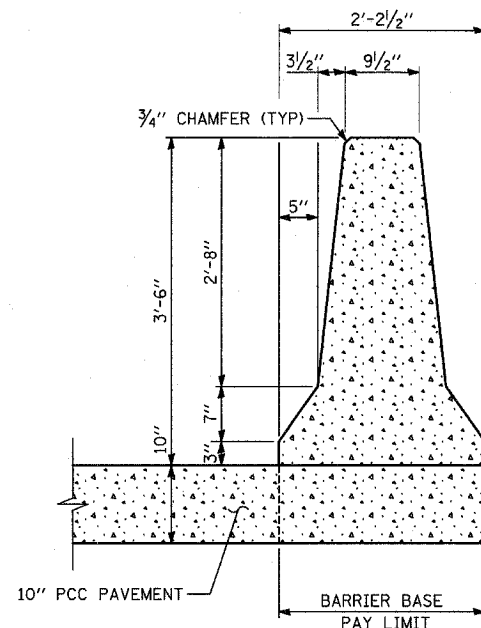
- NOTES:
- SEE DRAINAGE AND UTILITY PLANS, CROSS-SECTIONS, AND LANDSCAPING PLANS FOR GRADING LIMITS AND DETAILS.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
 GRADING DETAIL AT END
 OF CONCRETE BARRIER

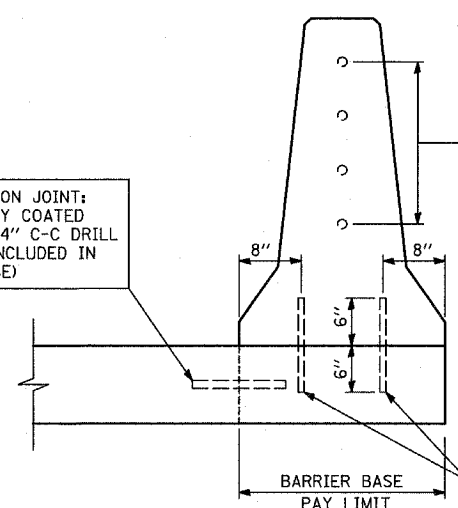
SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



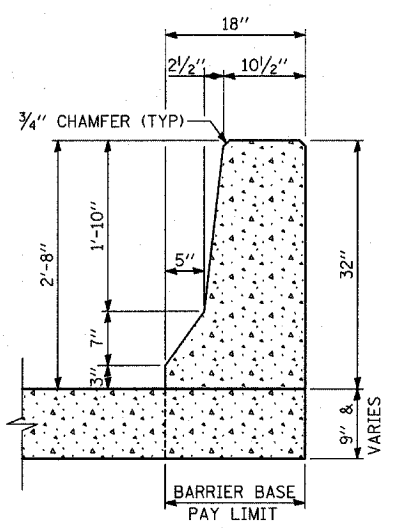
DIMENSIONS

ALL DIMENSIONS ARE APPROXIMATE BASED ON EXISTING PLANS AND MAY VARY - MATCH EXISTING BARRIER

CONCRETE BARRIER, DOUBLE FACE, 42 INCH HEIGHT
ALONG SB I-57



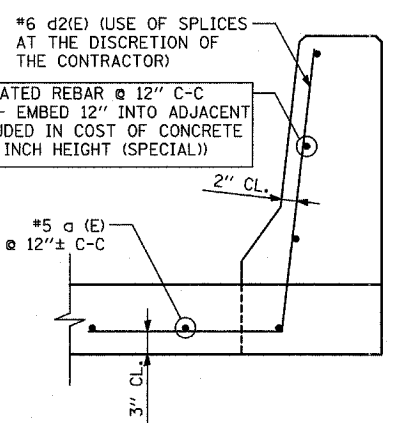
REINFORCEMENT



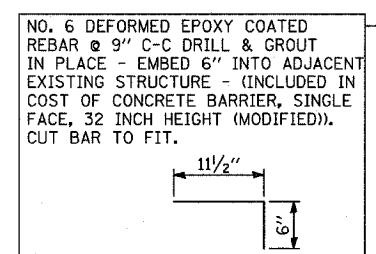
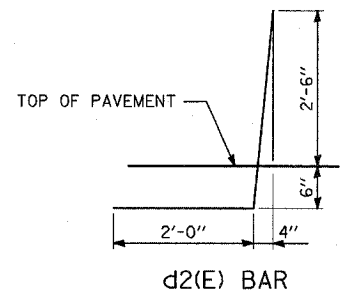
DIMENSIONS

ALL DIMENSIONS ARE APPROXIMATE BASED ON EXISTING PLANS AND MAY VARY - MATCH EXISTING BARRIER

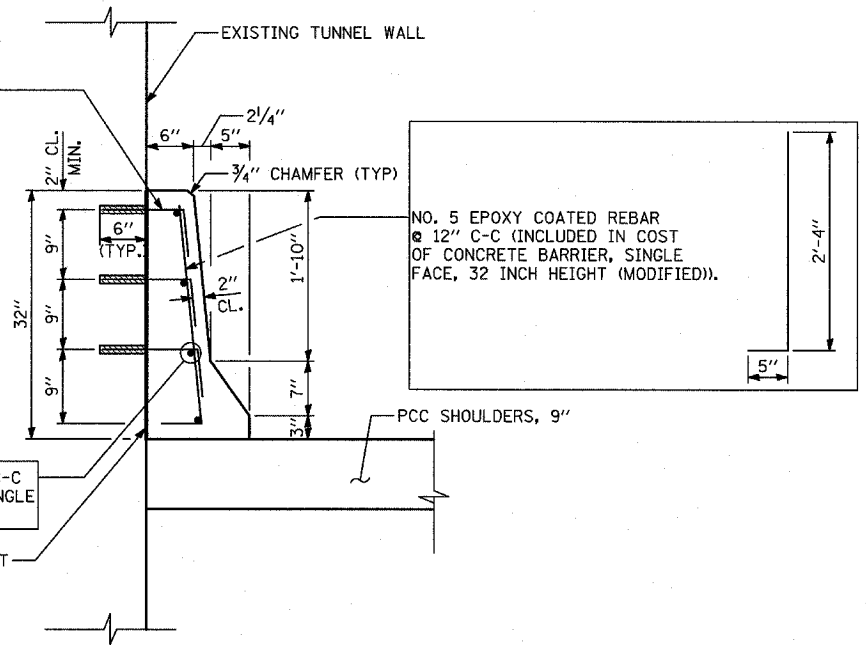
CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (SPECIAL)
ALONG SB I-94 (BISHOP FORD FREEWAY) AND EB I-57 TO I-94 CONNECTOR



REINFORCEMENT



NO. 5 DEFORMED EPOXY COATED REBAR @ 9" C-C (INCLUDED IN COST OF CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)).



CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)
SCALE: 1"=1'-0"

PROVIDE JOINTS IN FACING TO MATCH THOSE IN THE EXISTING STRUCTURES. (INCLUDED IN COST OF CONCRETE BARRIER, SINGLE FACE, 32 INCH HEIGHT (MODIFIED)).

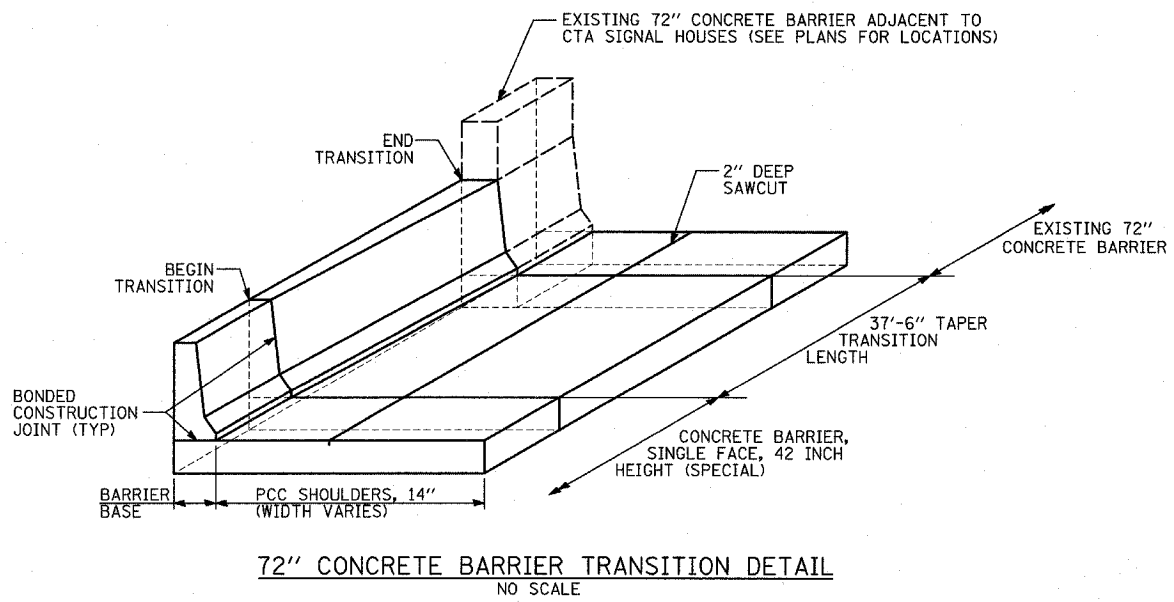
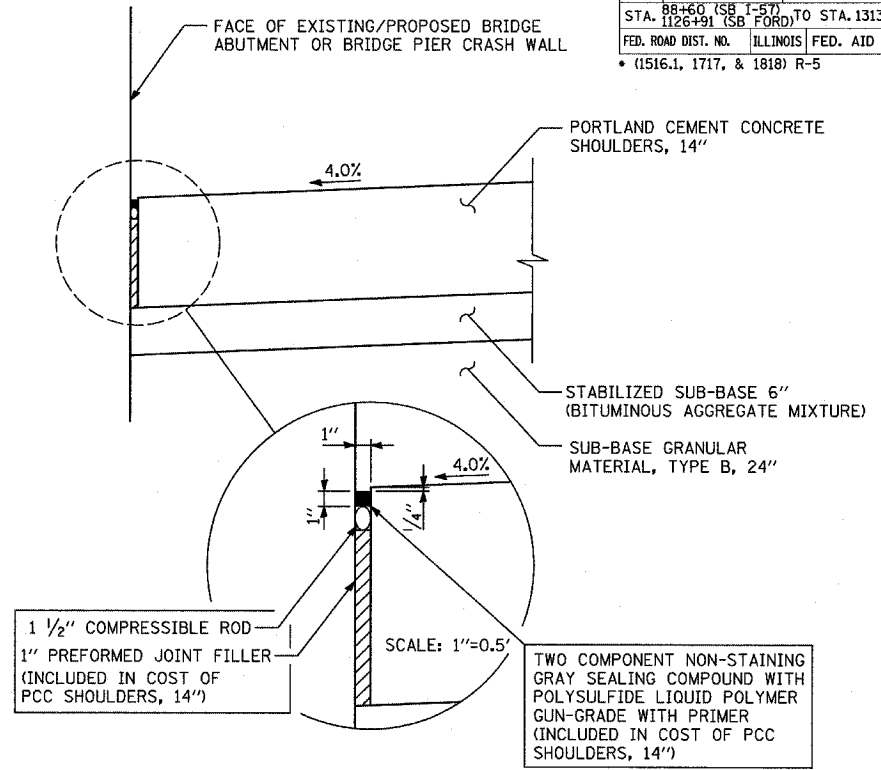
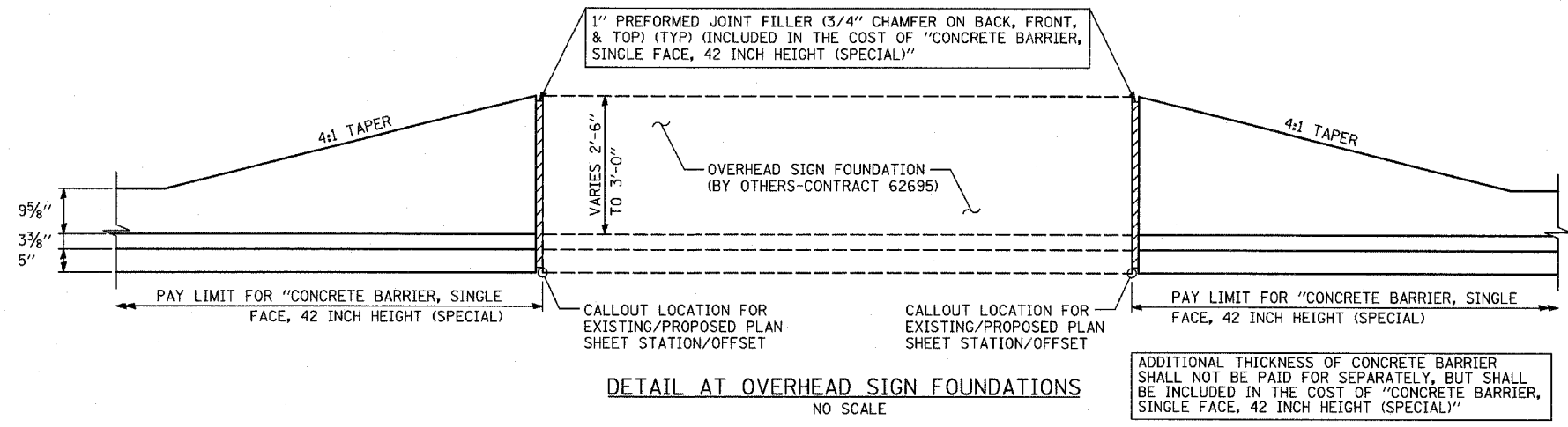


REVISIONS	
NAME	DATE

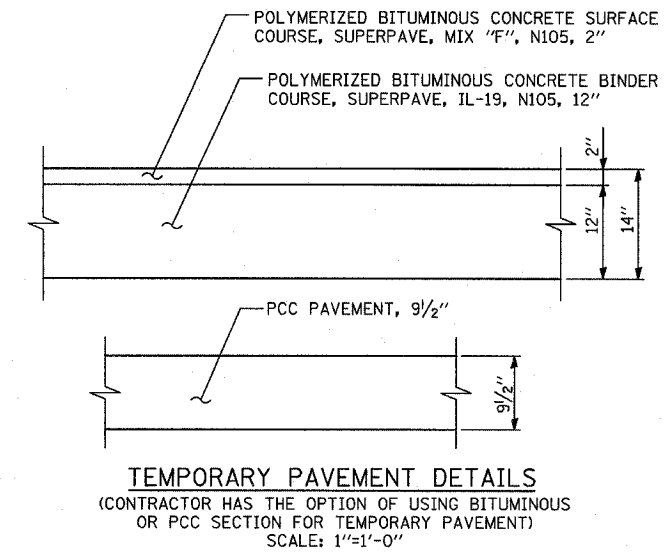
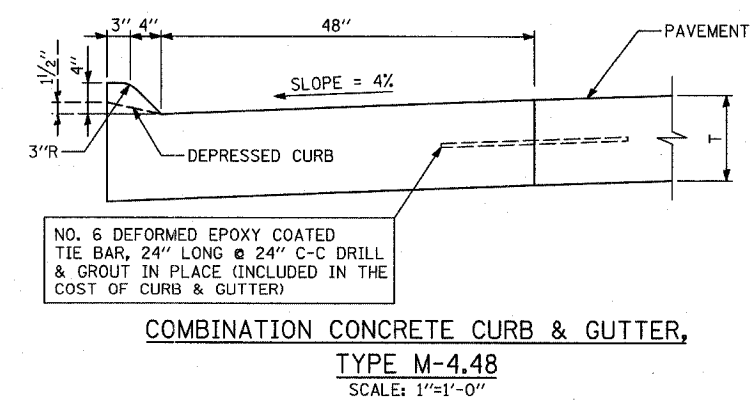
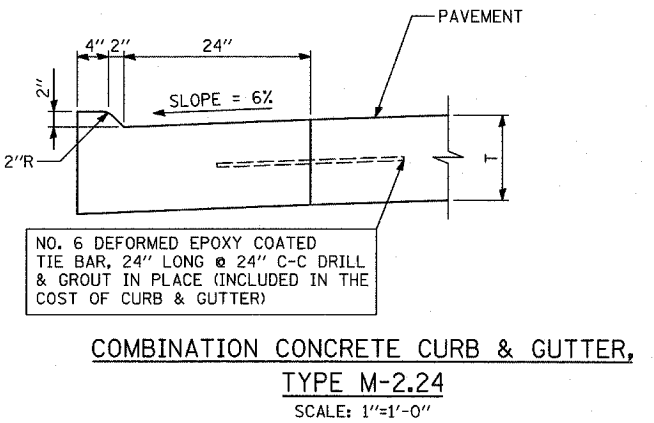
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
PLAN AND TYPICAL SECTION DETAILS
SHEET 1 OF 2

SCALE: AS SHOWN DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: TGB



PCC SHOULDERS ADJACENT TO BRIDGE ABUTMENT/CRASH WALL
SCALE: 1"=1'-0"

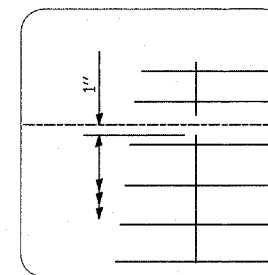
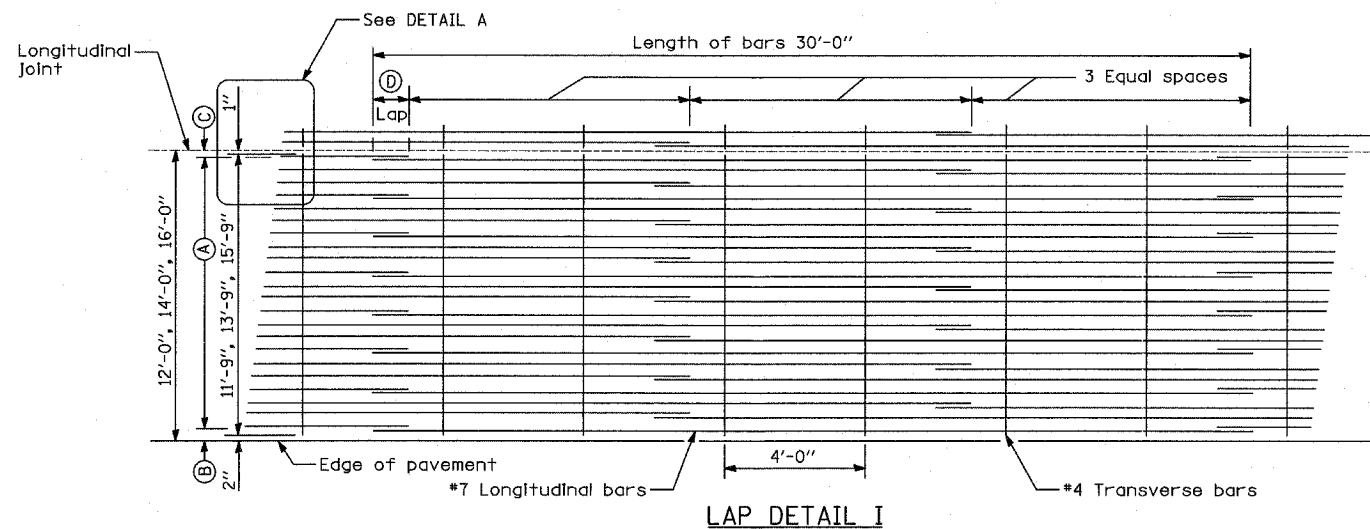


REVISIONS	
NAME	DATE

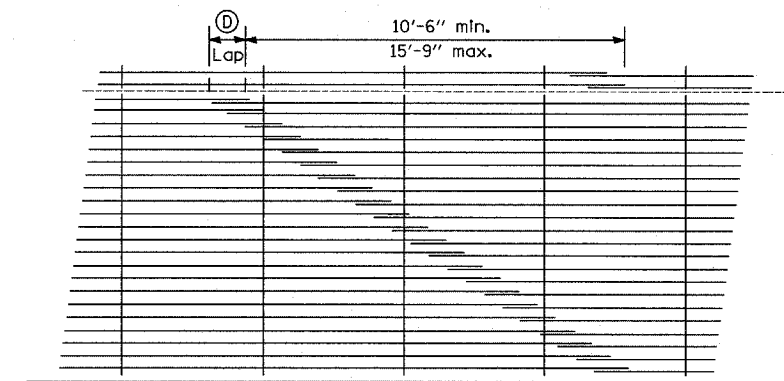
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
PLAN AND TYPICAL SECTION DETAILS
SHEET 2 OF 2

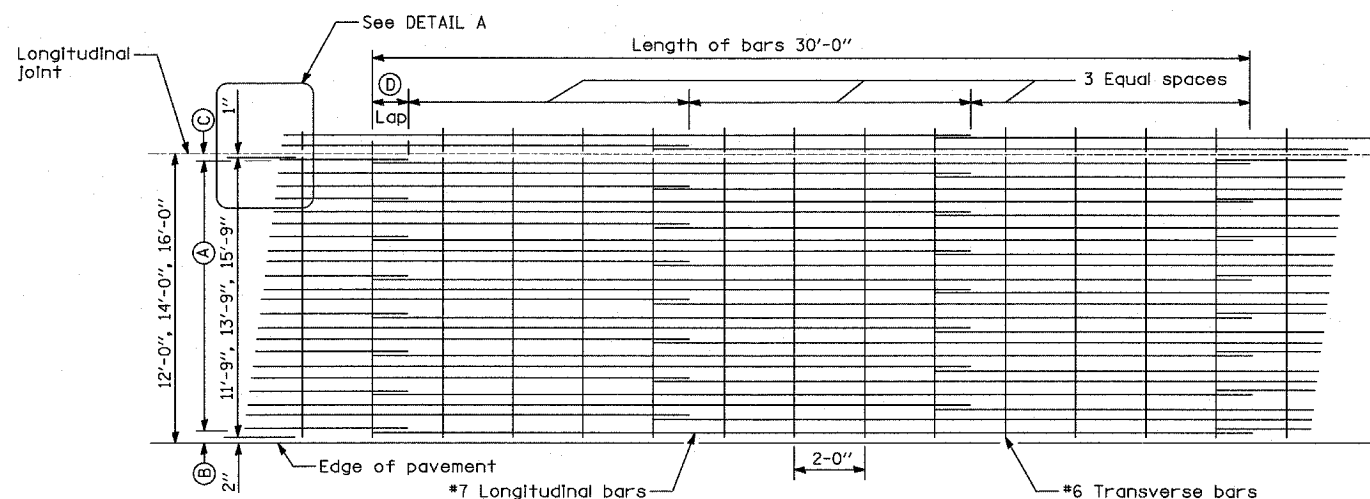
SCALE: AS SHOWN
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB



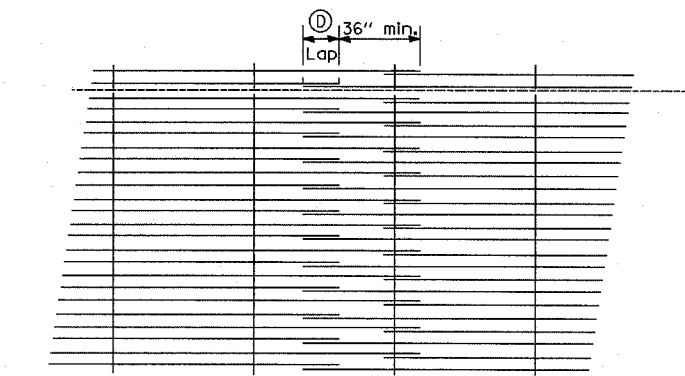
DETAIL A



LAP DETAIL II



ADDITIONAL REINFORCEMENT IN GORE AREAS - SEE PAVEMENT JOINTING AND ELEVATION PLANS FOR LOCATIONS



LAP DETAIL III

GENERAL NOTES

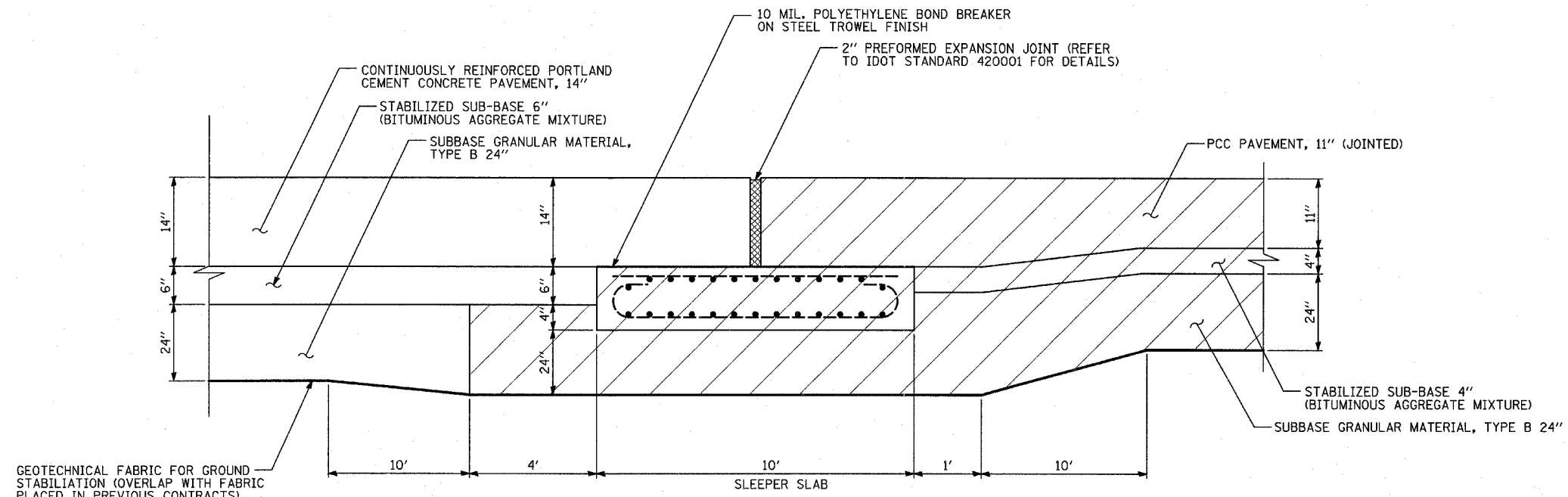
1. THE PAVEMENT REINFORCEMENT SHALL BE 4.5" FROM THE TOP OF PAVEMENT.
2. EXCEPT AS NOTED OR SHOWN, THE DIMENSIONS AND NOTES SPECIFIED FOR LAP DETAIL I ARE TYPICAL FOR LAP DETAIL II AND III.
3. THE (B) DIMENSION AND THE DISTANCE FROM THE END OF THE TRANSVERSE BAR TO THE EDGE OF PAVEMENT MAY BE INCREASED BY 1" FOR SLIP FORM PAVING.

Pavement Width	Bar Size	Pavement Thickness	(A) (Approx. Spacing)	(B)	(C)	(D)
12 feet	#7	14"	23 spaces (24 bars) @ 6"	3 1/2"	3"	26"
14 feet	#7	14"	27 spaces (28 bars) @ 6"	3 1/2"	3"	26"
16 feet	#7	14"	31 spaces (32 bars) @ 6"	3 1/2"	3"	26"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MISCELLANEOUS DETAILS
EXTENDED LANE REINFORCEMENT FOR
CONTINUOUSLY REINFORCED PCC PAVEMENT

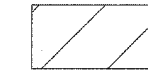
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: CTE, MPG
CHECKED BY: TGB



RAMP TERMINAL DETAILS AT EXISTING SLEEPER SLAB

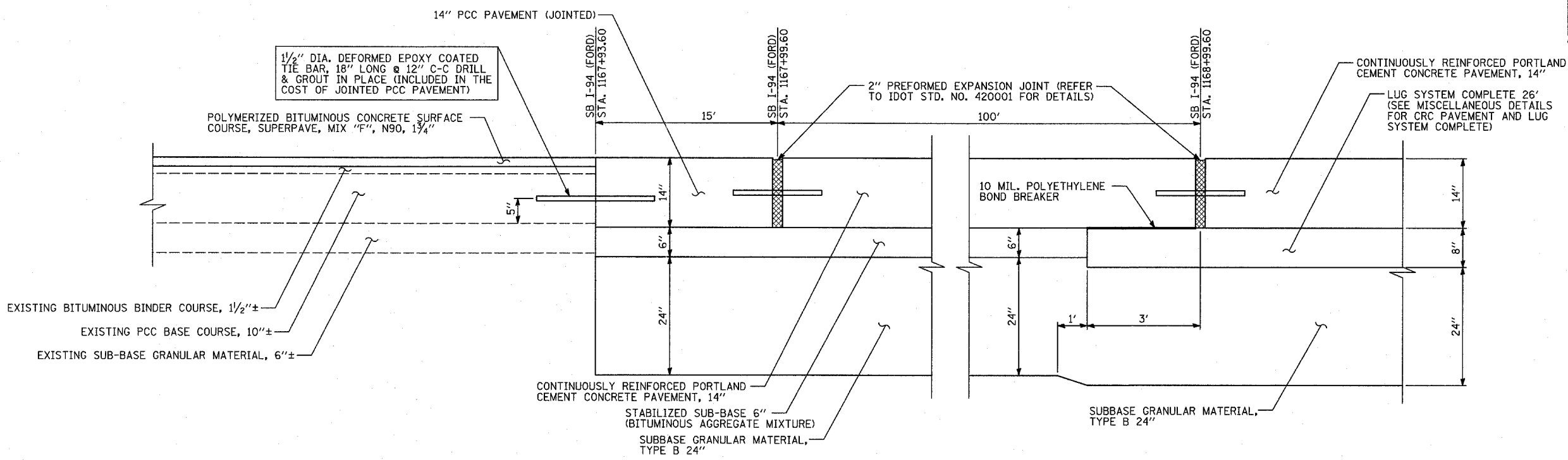
NOTES:

1. THE THICKENED EDGE OF THE SUB-BASE SHALL BE INCLUDED IN THE COST OF "SUB-BASE GRANULAR MATERIAL, TYPE B 24"

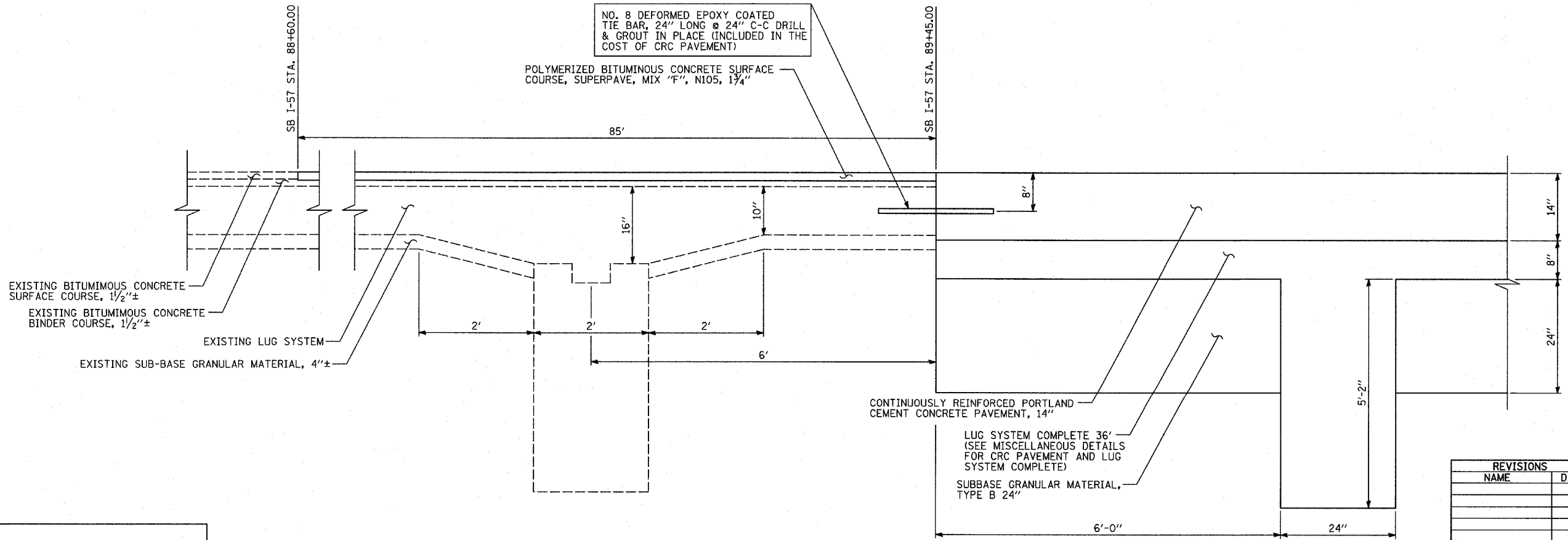


EXISTING PAVEMENT ITEMS CONSTRUCTED IN PREVIOUS CONTRACTS.

REVISIONS	
NAME	DATE



PROPOSED/EXISTING PAVEMENT TRANSITION NORTH OF TUNNEL ALONG SB I-94 (BISHOP FORD FREEWAY)



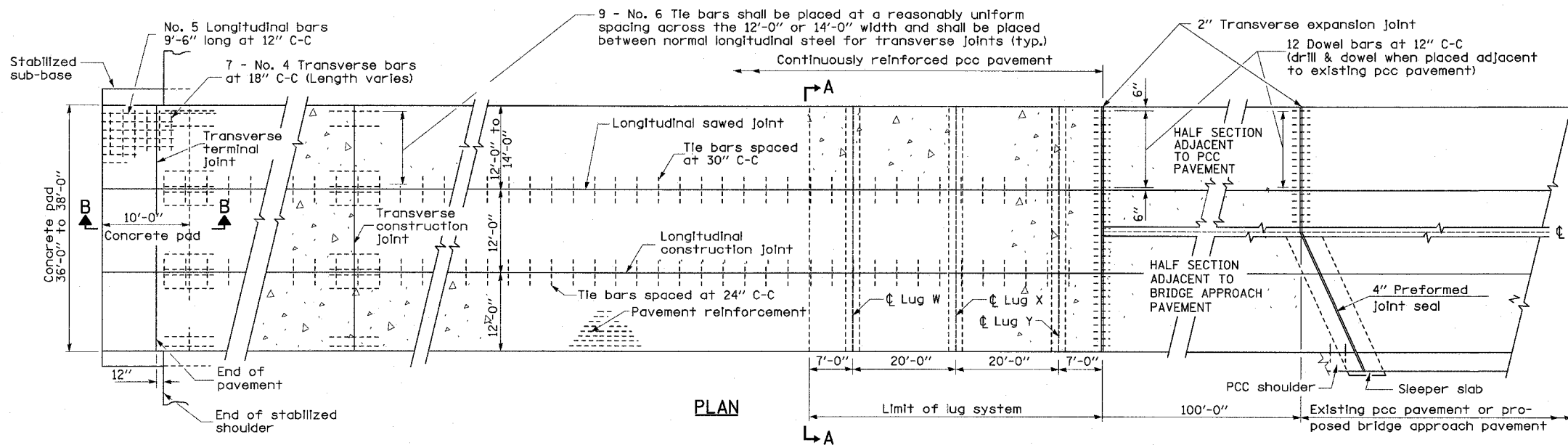
PROPOSED/EXISTING PAVEMENT TRANSITION AT WEST PROJECT LIMIT ALONG I-57

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 MISCELLANEOUS PAVEMENT ELEVATION
 AND JOINTING DETAILS
 SHEET 2 OF 2

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: TGB



MATERIALS REQUIRED FOR (1) ONE LUG SYSTEM COMPLETE, 26'
(Excluding CRC Concrete, Pavement Reinforcement, Stabilized Sub-base and Sub-base Granular Material)

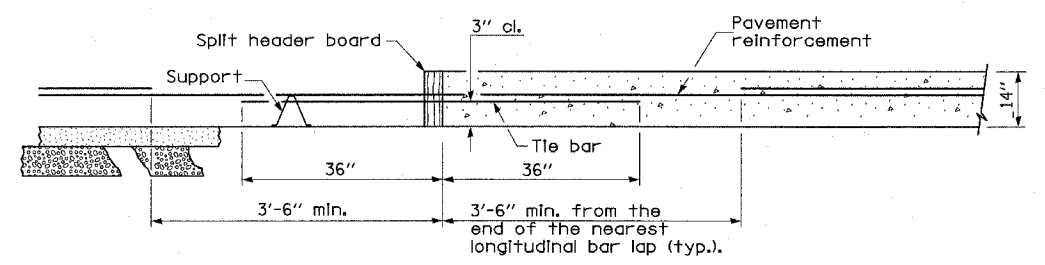
Bar No.	Size	Length	Shape
a	144 No. 8	16'-8"	U
b	24 No. 5	27'-0"	U
c	144 No. 5	20'-0"	U
d	28 No. 4	11'-9" or 13'-9"	U

Concrete (cu. yds.) 29.85
Reinforcing Bars (lbs.) 10330
Concrete Pad (sq. yds.) 139

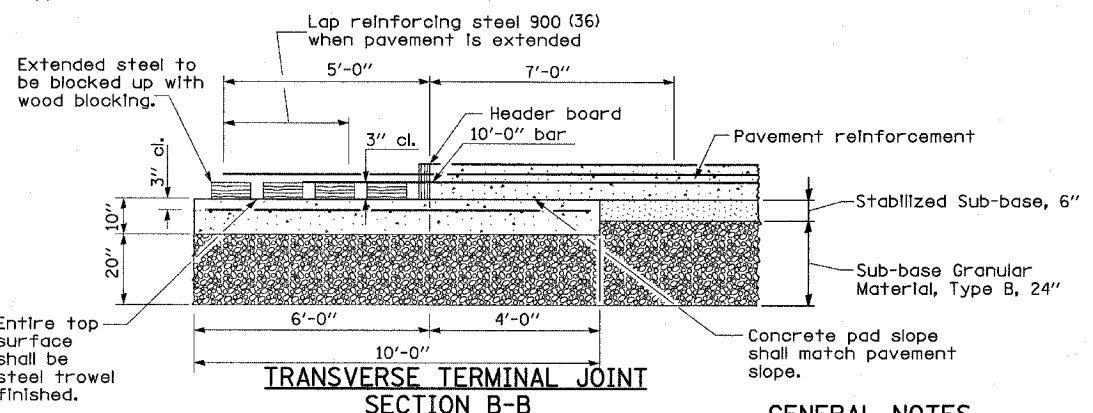
MATERIALS REQUIRED FOR (1) ONE LUG SYSTEM COMPLETE, 36'
(Excluding CRC Concrete, Pavement Reinforcement, Stabilized Sub-base and Sub-base Granular Material)

Bar No.	Size	Length	Shape
a	198 No. 8	16'-8"	U
b	24 No. 5	37'-0"	U
c	198 No. 5	20'-0"	U
d	42 No. 4	11'-9"	U

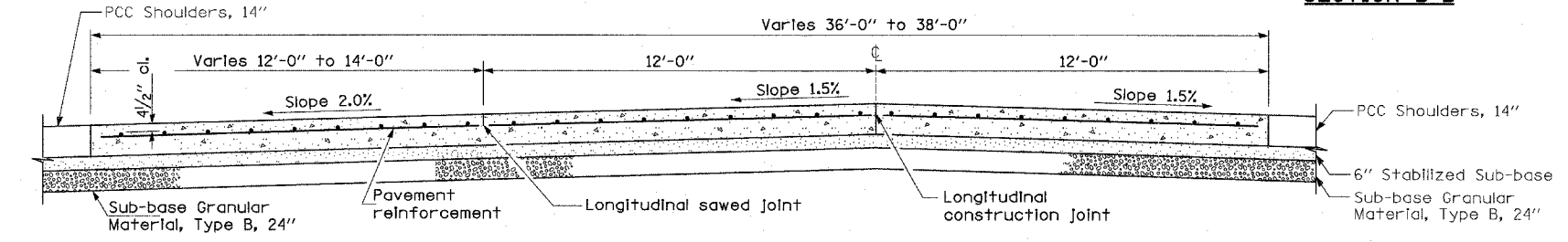
Concrete (cu. yds.) 41.33
Reinforcing Bars (lbs.) 14200
Concrete Pad (sq. yds.) 192



TRANSVERSE CONSTRUCTION JOINT



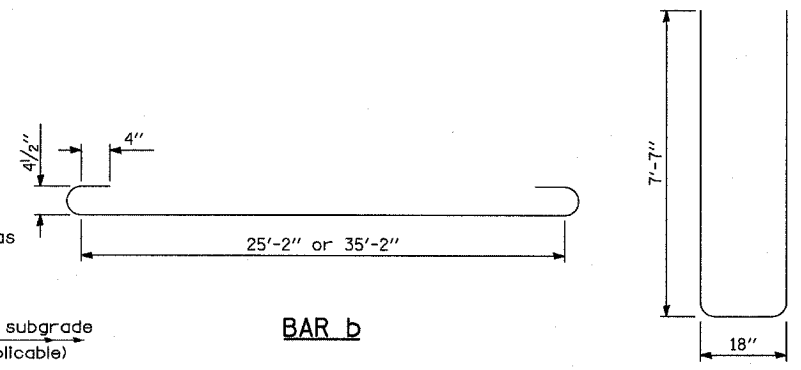
TRANSVERSE TERMINAL JOINT SECTION B-B



SECTION A-A (TYPICAL 3-LANE, 1-WAY WITH SHOULDERS)

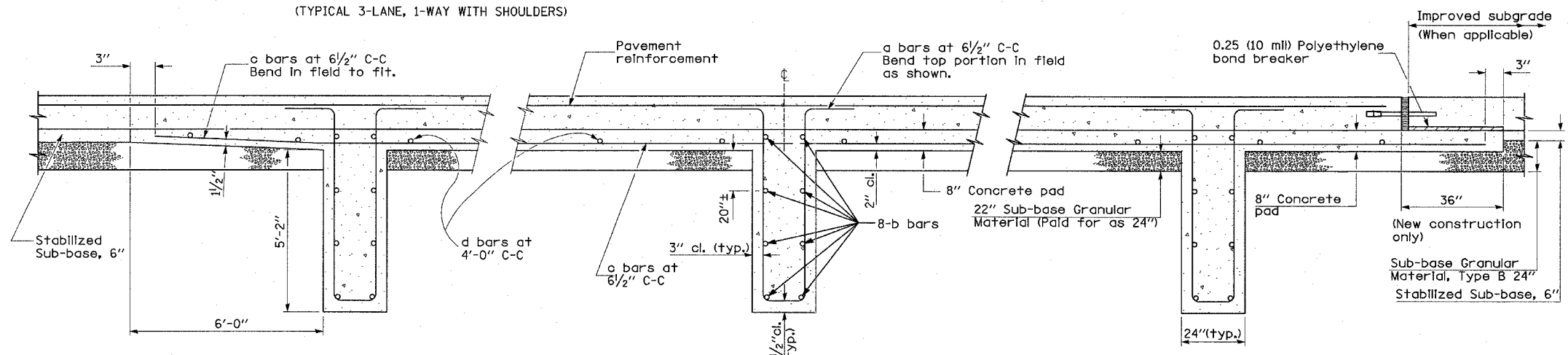
GENERAL NOTES

See Pavement Reinforcement details for additional information.
See Standards 420001 and 420401 for joint details not shown.
All dimensions are in inches unless otherwise shown.
Sub-base granular material that varies in thickness below the lug-system or the transverse terminal joint will be paid for as "Sub-base Granular Material, Type B, 24"



BAR b

BAR a



SECTION AT LUG W

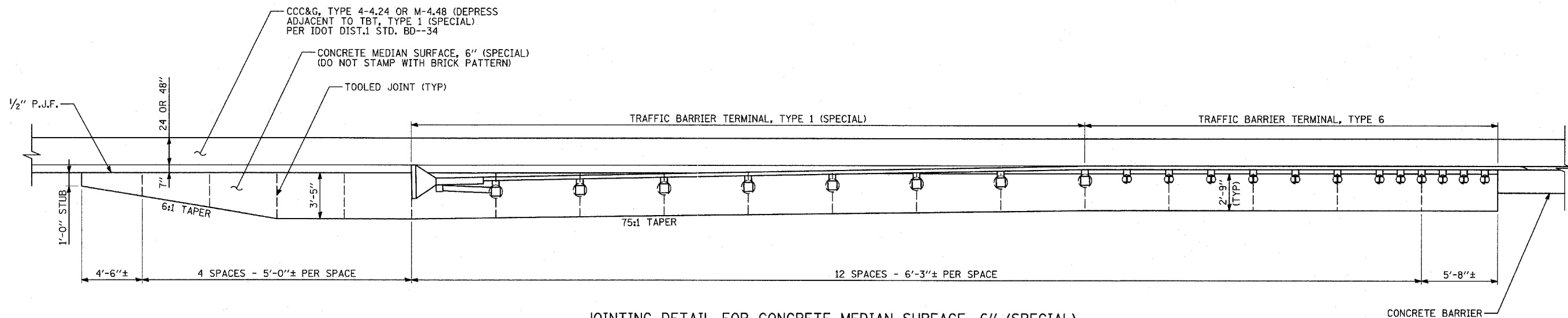
SECTION AT LUG X

SECTION AT LUG Y

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MISCELLANEOUS DETAILS
CONTINUOUSLY REINFORCED PORTLAND CEMENT CONCRETE PAVEMENT AND LUG SYSTEM COMPLETE, 26' & 36'
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB



**JOINTING DETAIL FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)
AT TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) AND TYPE 6**

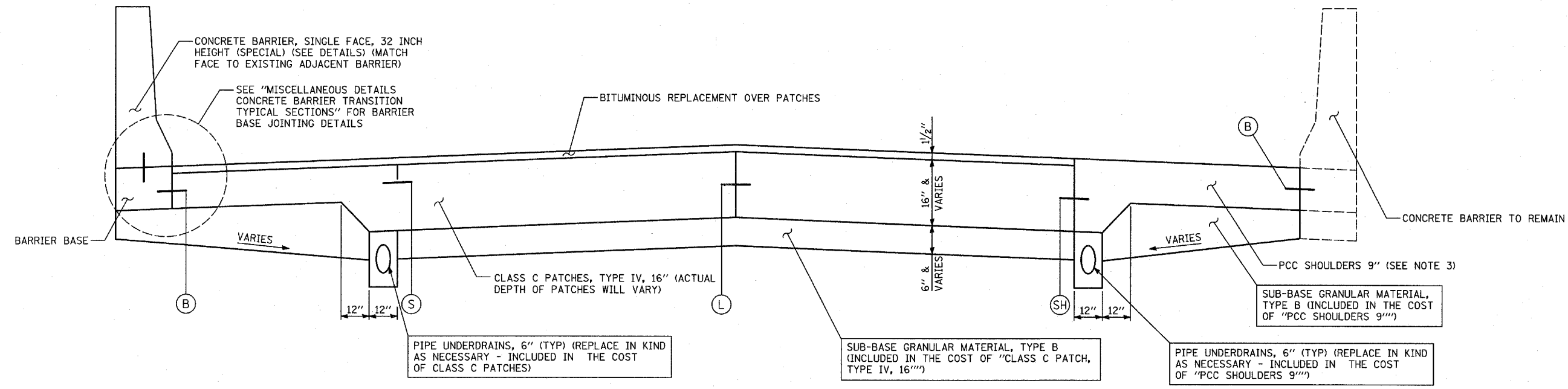
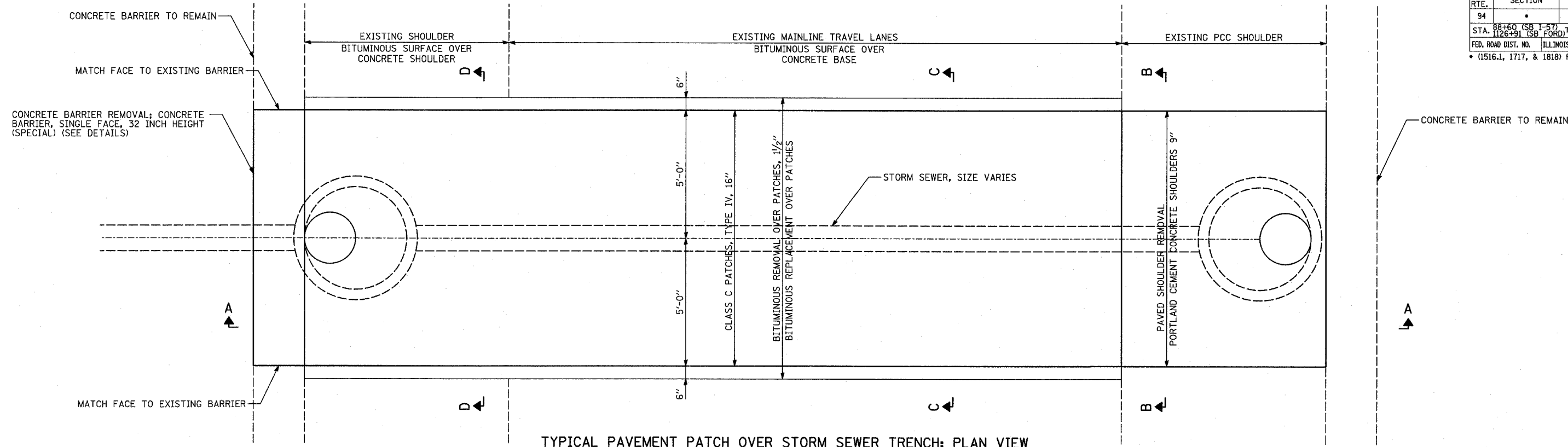
TYLININTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
 PAVEMENT JOINTING DETAILS
 FOR CONCRETE MEDIAN SURFACE, 6" (SPECIAL)
 AT TRAFFIC BARRIER TERMINALS

SCALE: NONE DRAWN BY: MPG
 DATE: MARCH 1, 2006 CHECKED BY: TGB



SECTION A-A

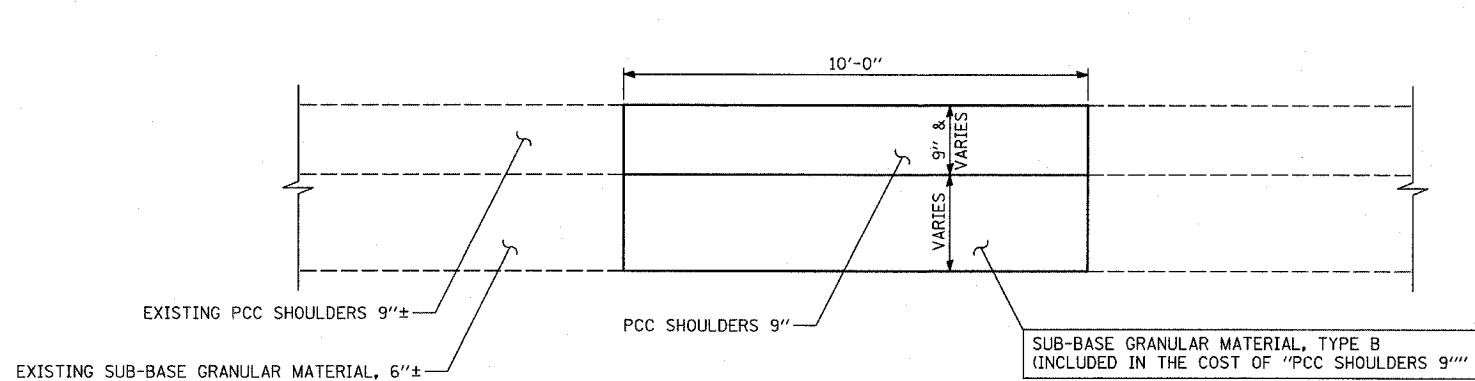
NOTES:

- TRENCH WIDTH MUST BE 5'-0" EITHER SIDE FROM SEWER OR DRAINAGE STRUCTURE CENTER LINE.
- LOCATIONS OF CONCRETE BARRIER TO REMAIN, CONCRETE BARRIER TO BE REPLACED, PCC SHOULDERS, AND BITUMINOUS SURFACED SHOULDERS VARY.
- ADDITIONAL THICKNESS OF "PCC SHOULDERS 9" NEED TO MATCH THE DEPTH OF CLASS C PATCHES SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS 9".
- SEE PAVEMENT JOINTING AND ELEVATION PLANS LEGEND FOR DESCRIPTIONS OF LONGITUDINAL BARRIER CONSTRUCTION JOINTS, LONGITUDINAL CONSTRUCTION JOINTS, LONGITUDINAL SHOULDER CONSTRUCTION JOINTS, AND SAWED LONGITUDINAL JOINTS. ALL JOINTS SHALL BE INCLUDED IN THE COST OF "CLASS C PATCHES, TYPE IV, 16", OR PCC SHOULDERS, 9"
- CONNECT UNDERDRAIN TO CATCH BASIN IF UNDERDRAIN IS DRAINING TOWARD PATCH.

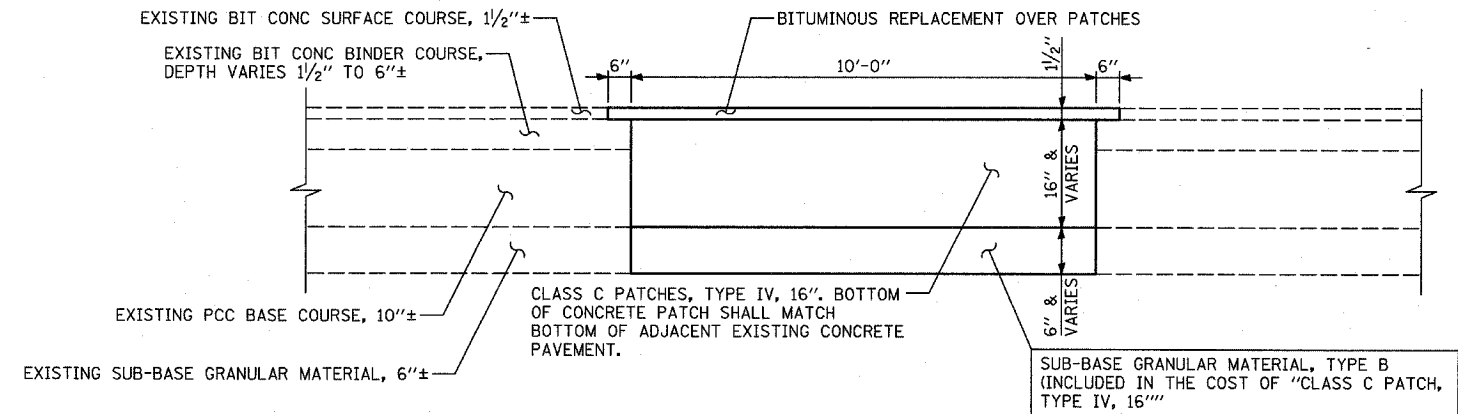
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS:
 PAVEMENT PATCHING OVER SEWER
 TRENCHES: SHEET 1 OF 2
 PLAN AND ELEVATION

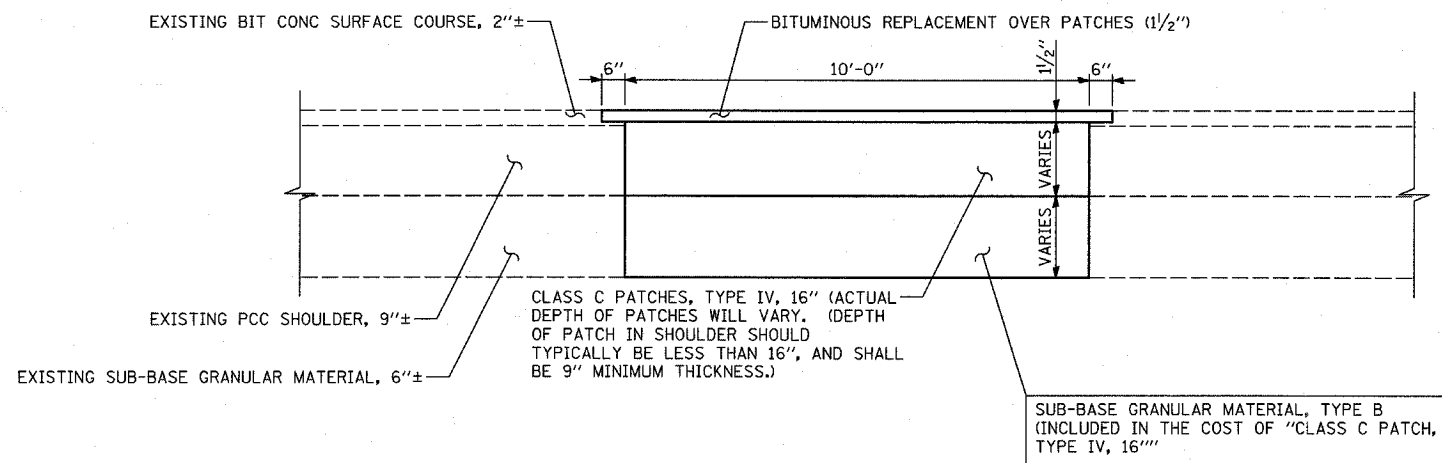
SCALE: NONE
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



SECTION B-B
PCC SHOULDER WITH NO OVERLAY



SECTION C-C
BITUMINOUS SURFACE OVER CONCRETE BASE



SECTION D-D
BITUMINOUS SURFACE OVER CONCRETE SHOULDER

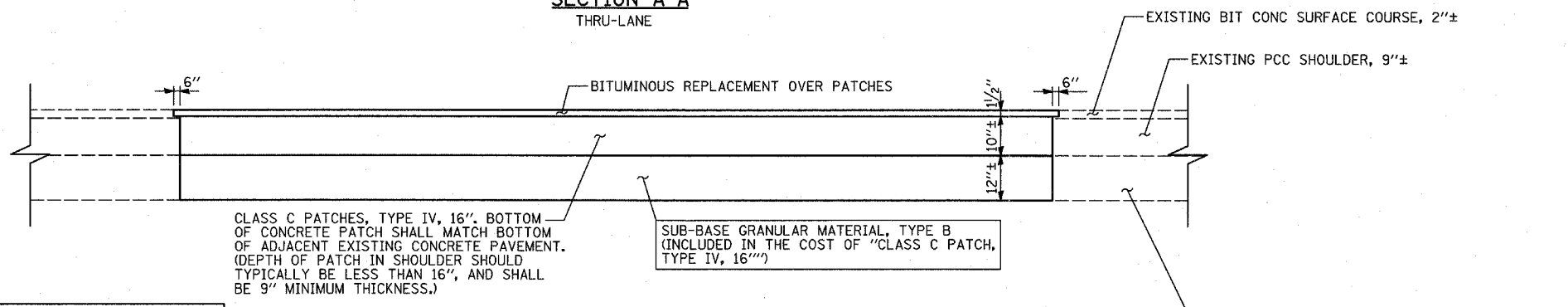
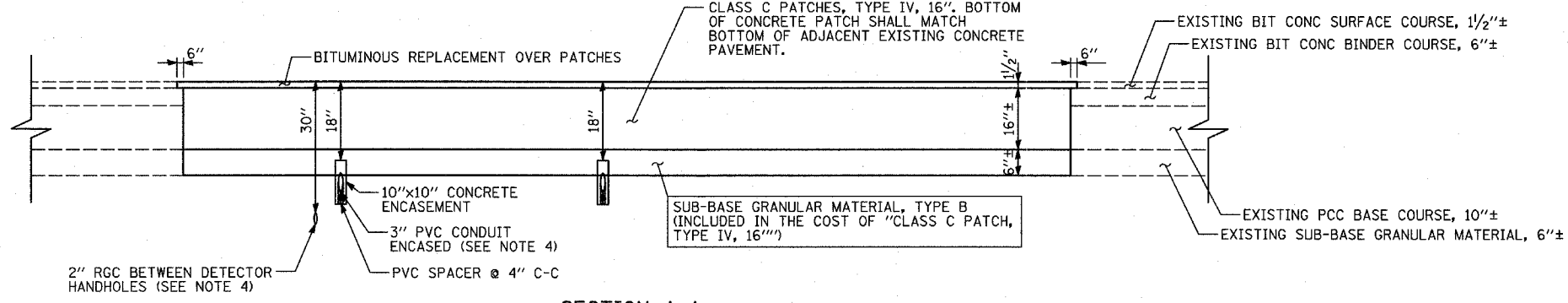
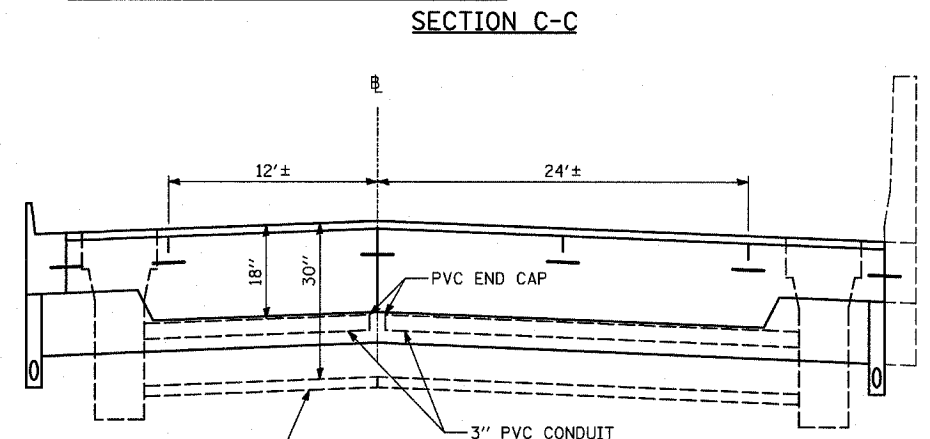
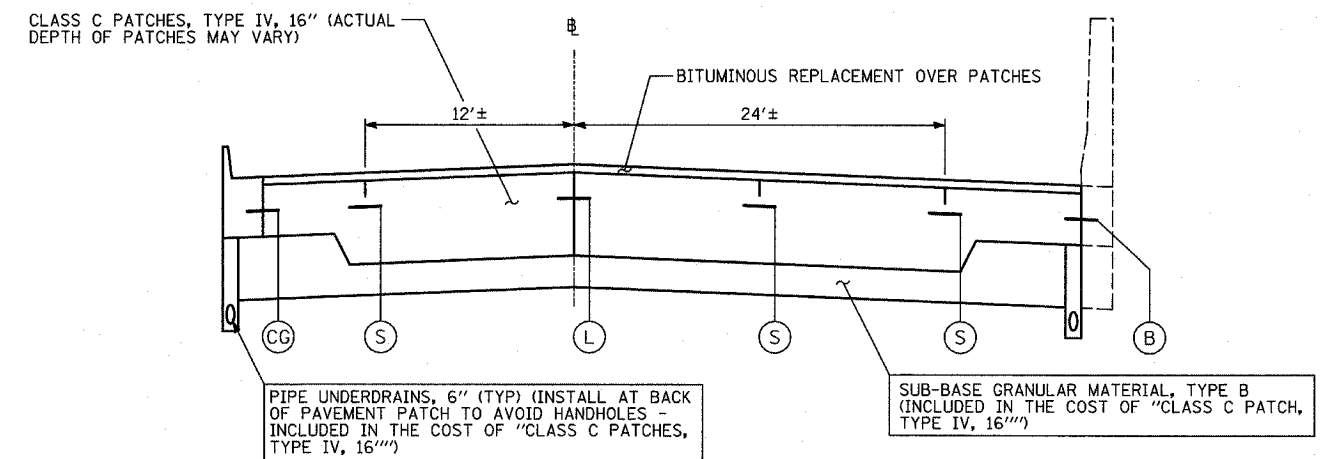
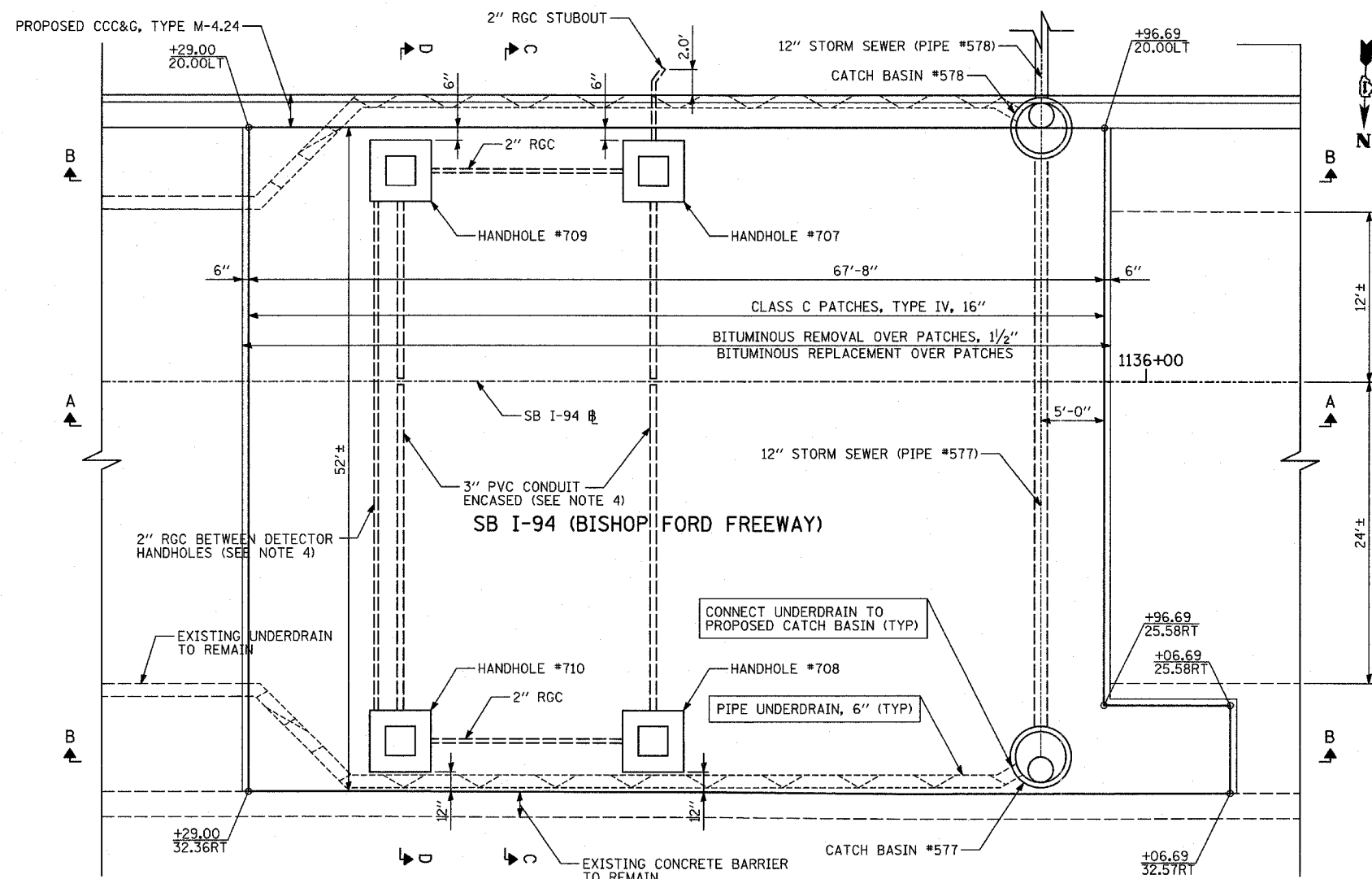
NOTES:

- TRENCH WIDTH MUST BE 5'-0" EITHER SIDE FROM SEWER OR DRAINAGE STRUCTURE CENTER LINE.
- LOCATIONS OF CONCRETE BARRIER TO REMAIN, CONCRETE BARRIER TO BE REPLACED, PCC SHOULDERS, AND BITUMINOUS SURFACED SHOULDERS VARY.
- ADDITIONAL THICKNESS OF "PCC SHOULDERS 9" NEEDED TO MATCH THE DEPTH OF CLASS C PATCHES SHALL BE INCLUDED IN THE COST OF PCC SHOULDERS 9".

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MISCELLANEOUS DETAILS:
PAVEMENT PATCHING OVER SEWER
TRENCHES: SHEET 2 OF 2
TYPICAL SECTIONS

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB



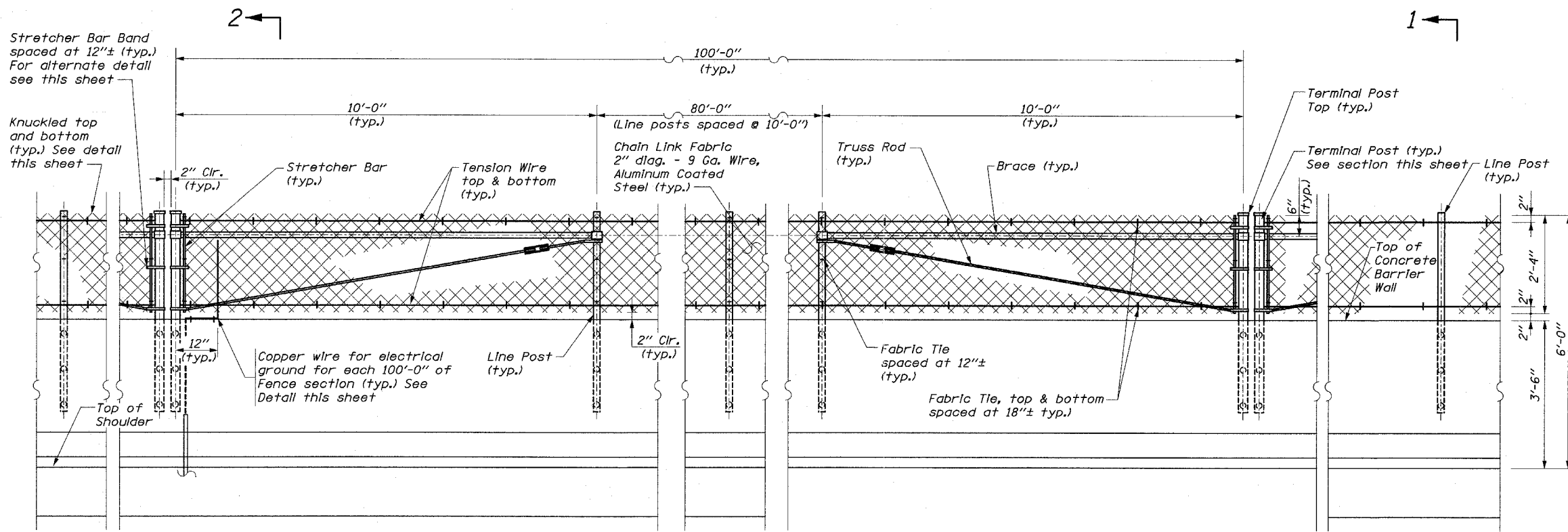
- NOTES:**
- REFER TO ELECTRICAL INFRASTRUCTURE SHEETS FOR HANDHOLE AND CONDUIT DETAILS.
 - SEE PAVEMENT JOINTING AND ELEVATION PLANS LEGEND FOR DESCRIPTIONS OF CONSTRUCTION JOINTS, LONGITUDINAL CONSTRUCTION JOINTS, LONGITUDINAL BARRIER CONSTRUCTION JOINTS, AND SAWED LONGITUDINAL JOINTS. ALL JOINTS SHALL BE INCLUDED IN THE COST OF "CLASS C PATCHES, TYPE IV, 16", OR PCC SHOULDERS, 9"
 - PIPE UNDERDRAIN SHALL BE CONNECTED TO EXISTING UNDERDRAIN AT STA. 1135+29 AND CONNECTED TO PROPOSED CATCH BASIN AS SHOWN. THIS WORK SHALL BE INCLUDED IN THE COST OF "CLASS C PATCHES, TYPE IV, 16"
 - DETECTOR CONDUIT TO BE INSTALLED UNDER TRAVEL LANES BY CONSTRUCTION STAGE. CONTRACTOR MUST VERIFY THE DETECTOR CONDUIT STRAIGHTNESS ACCORDING TO THE CONTRACT SPECIFICATIONS BEFORE THE PAVEMENT PATCH IS INSTALLED.

REVISIONS	
NAME	DATE

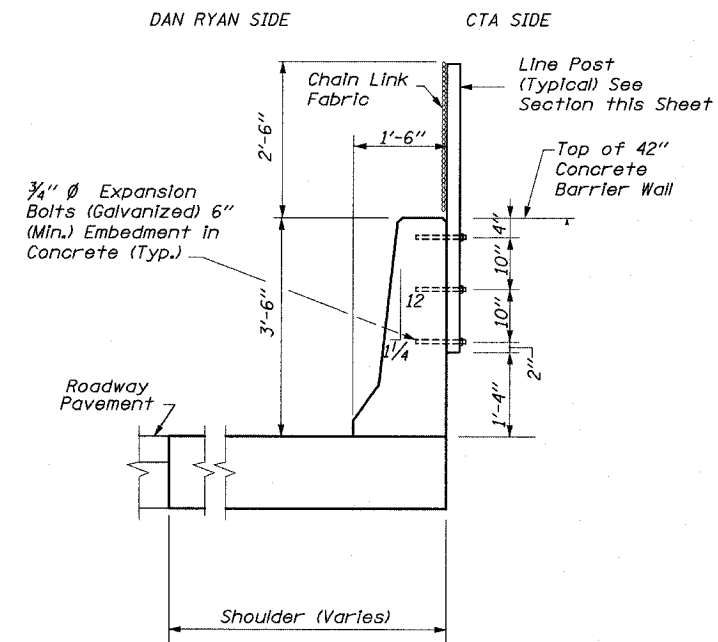
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MISCELLANEOUS DETAILS:
PAVEMENT PATCHING OVER UTILITY TRENCH
SB I-94 (BISHOP FORD FREEWAY)
STA. 1135+29.00 TO 1135+96.69

SCALE: NONE DRAWN BY: MPG
DATE: MARCH 1, 2006 CHECKED BY: TGB

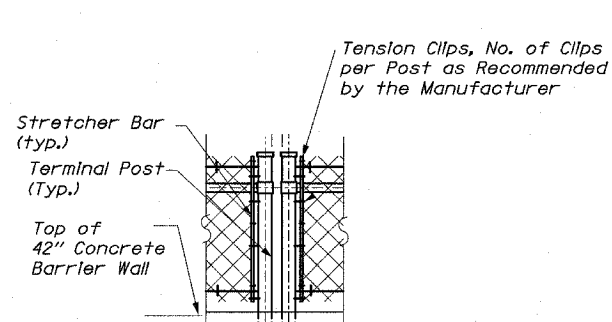
TYLIN INTERNATIONAL



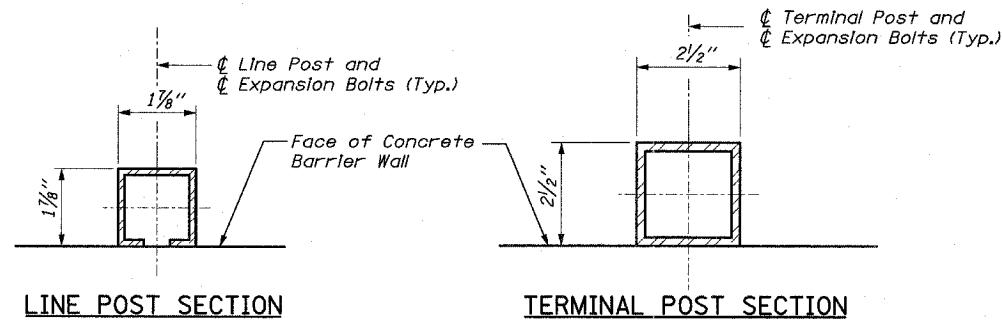
ELEVATION
CTA FENCE ON CONCRETE BARRIER WALL



SECTION 1-1

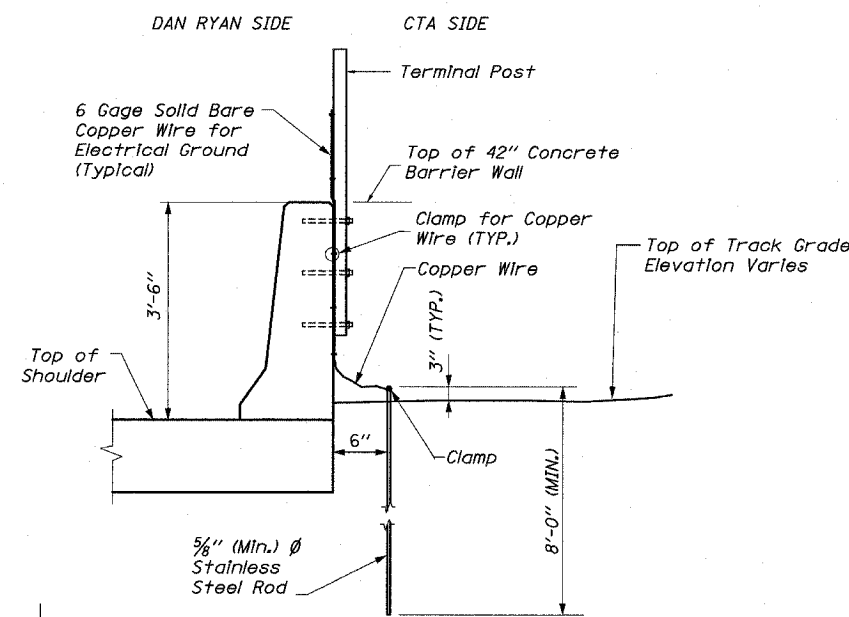


ALTERNATE DETAIL FOR
STRETCHER BAR BAND

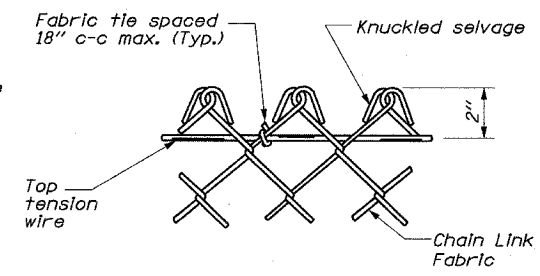


NOTE:
The Contractor May Use Another C Section as per the Standards of the Illinois Department of Transportation.

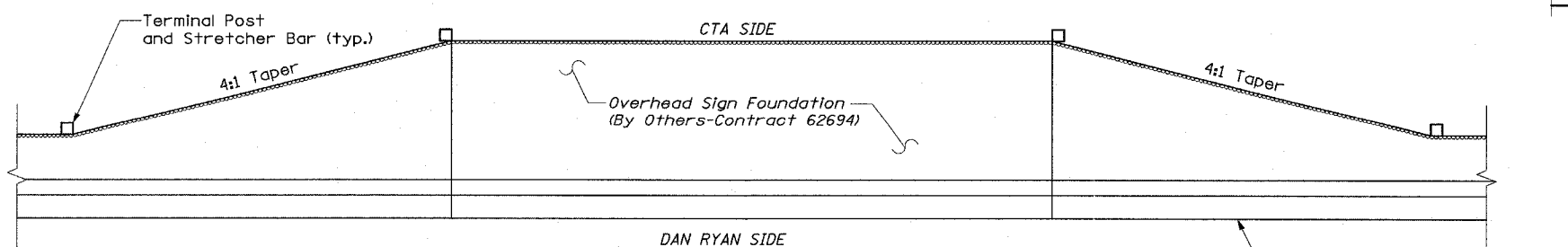
NOTE:
Where the fence meets bridge piers and other vertical structures taller than the fence a maximum gap of 2" clear between the terminal post and the structure shall be provided.



SECTION 2-2
ELECTRICAL GROUND DETAIL



KNUCKLED DETAIL



DETAIL AT OVERHEAD SIGN FOUNDATIONS

Fence, hardware and posts around the overhead sign foundation are paid for as CTA Fence.

Edwards and Kelcey
ONE NORTH FRANKLIN
CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM

TYLIN INTERNATIONAL

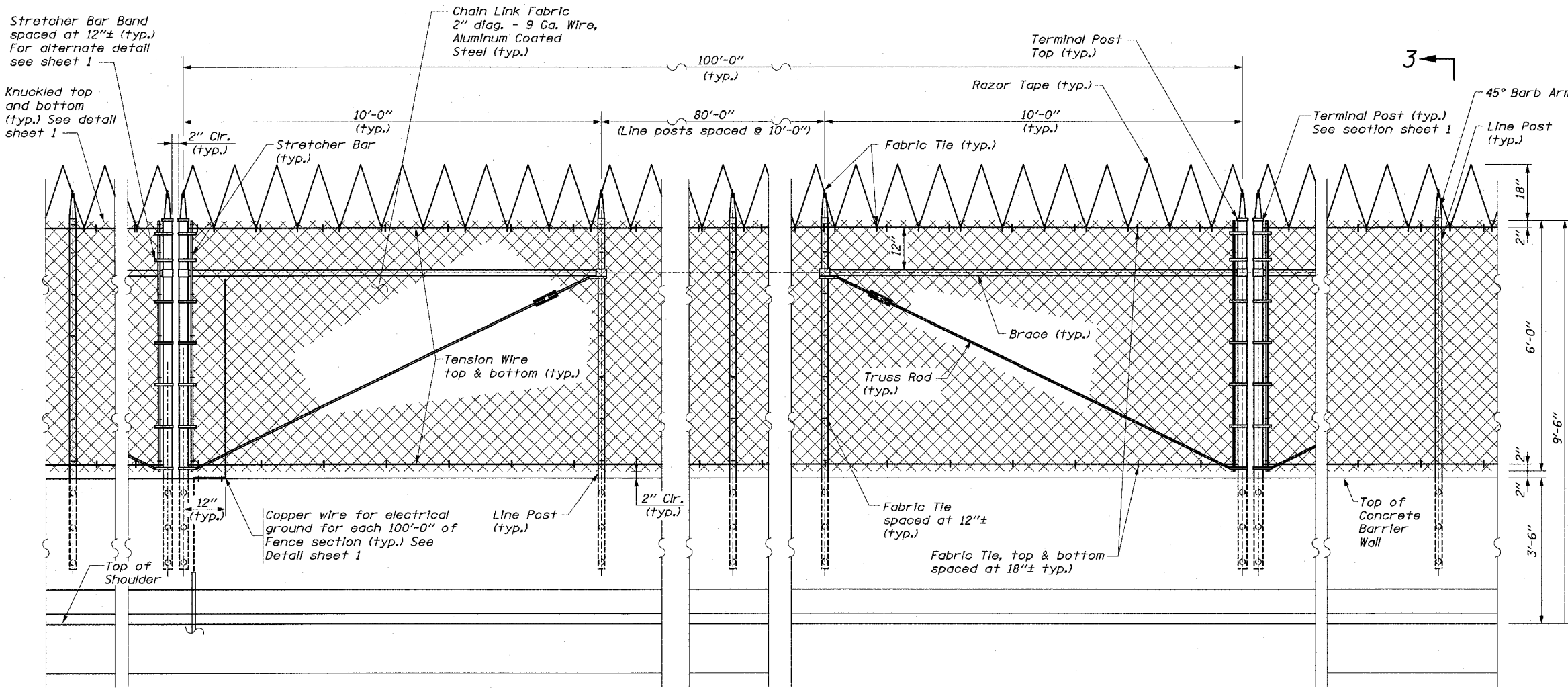
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

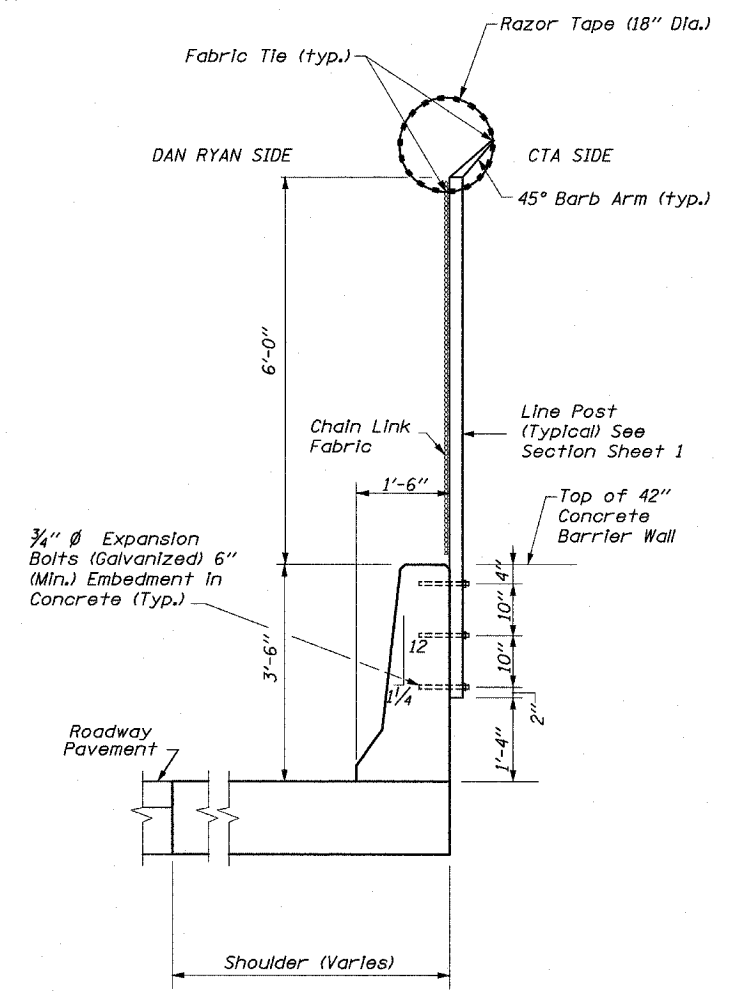
CTA FENCE
ELEVATIONS AND DETAILS
SHEET 1 OF 4

SCALE: NONE
DATE: MARCH 1, 2006

DRAWN BY: RLK
CHECKED BY: EL



ELEVATION
CTA FENCE (SPECIAL) ON CONCRETE BARRIER WALL



SECTION 3-3

NOTE:
Where the fence meets bridge piers and other vertical structures taller than the fence a maximum gap of 2" clear between the terminal post and the structure shall be provided.

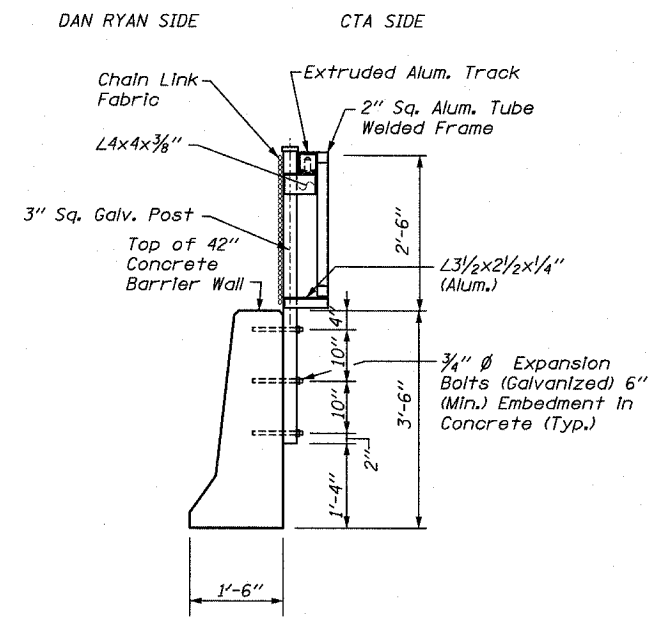
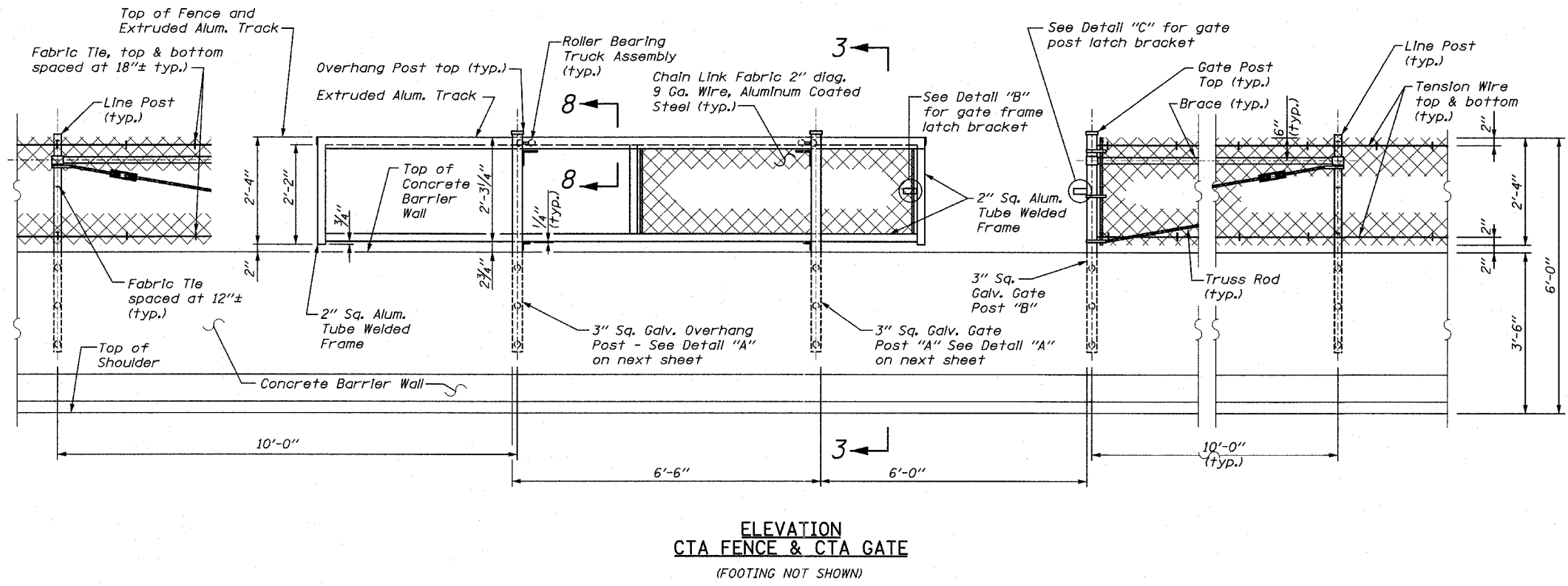
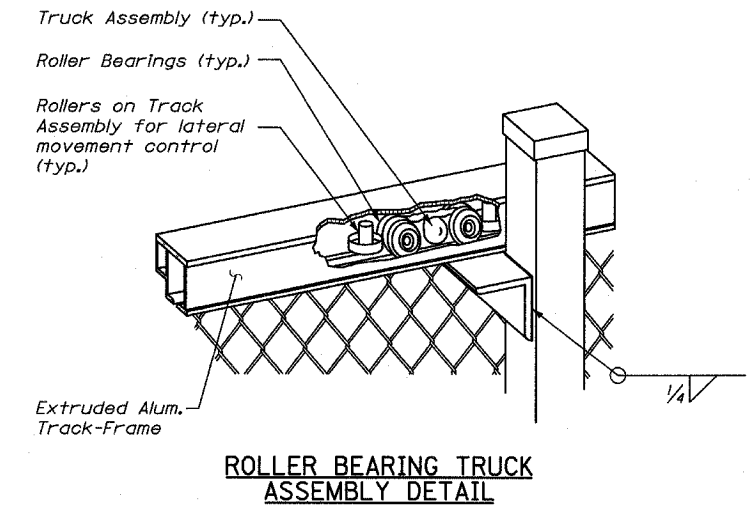
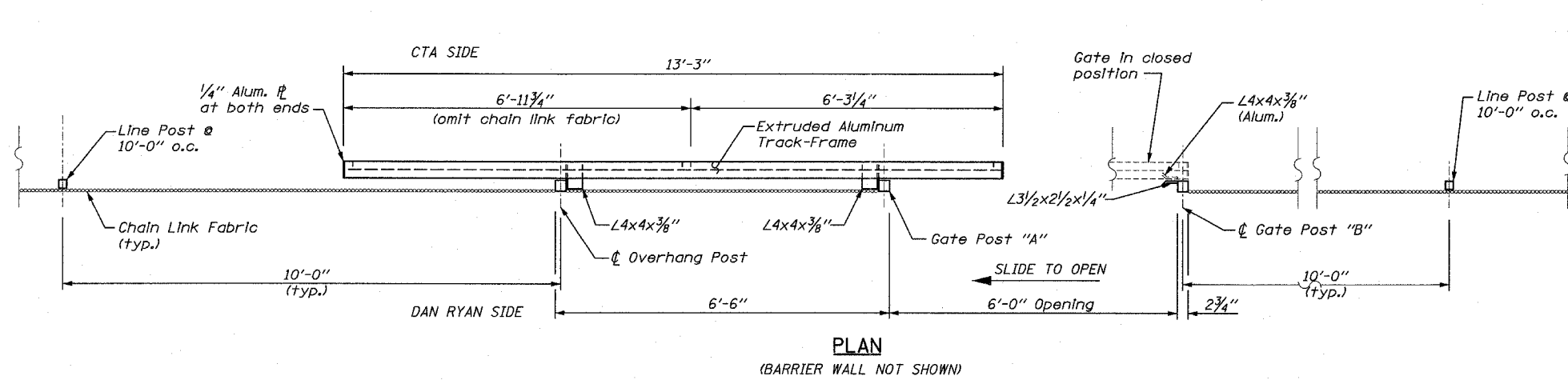
TYLIN INTERNATIONAL

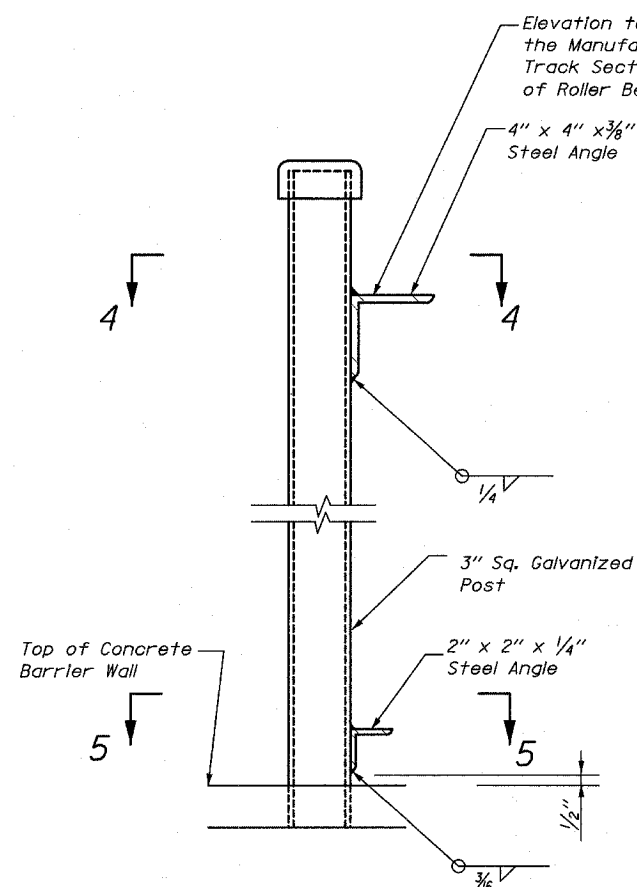
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

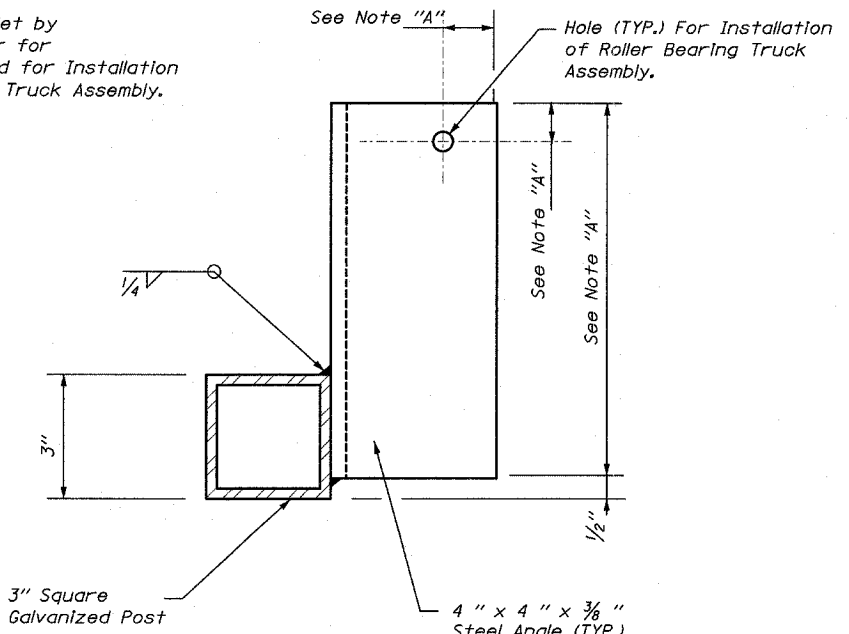
CTA FENCE (SPECIAL)
ELEVATIONS AND DETAILS
SHEET 2 OF 4

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: MPG
CHECKED BY: TGB

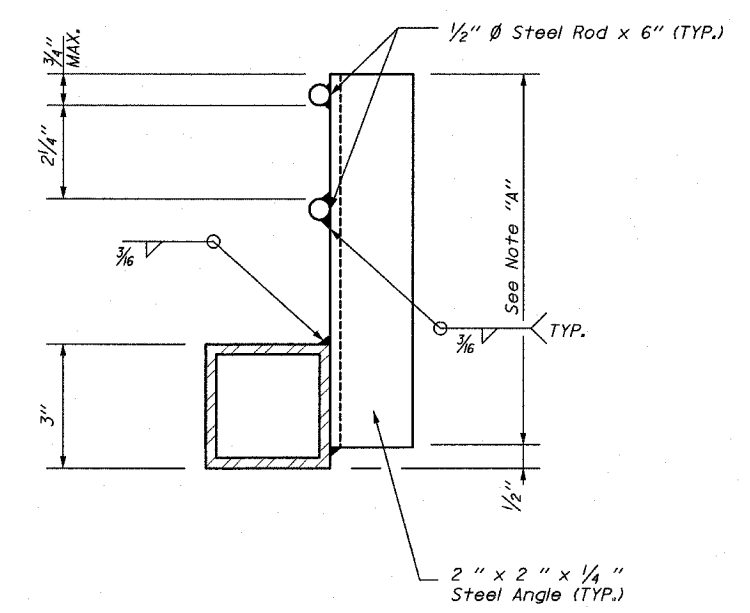




ELEVATION



SECTION 4-4



SECTION 5-5

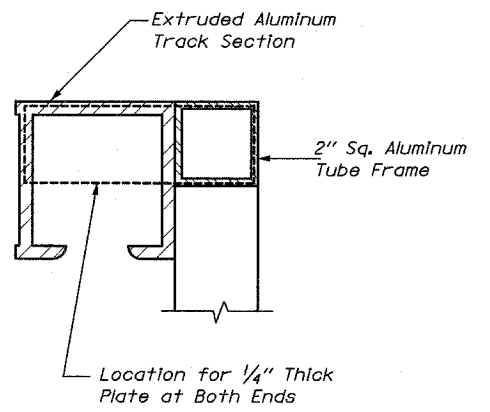
DETAIL A

For Overhang Post - As Shown
For Gate Post "A" - Opposite Hand

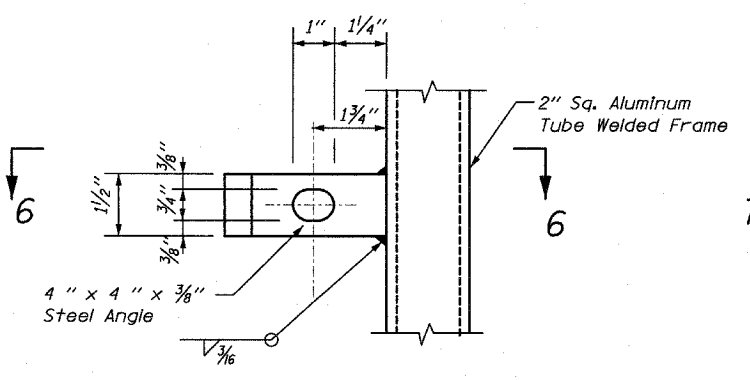
NOTE "A": The Manufacturer of the Sliding Gate Shall Determine the Length of Angles and Location of Hole for Installation of Roller Bearing Truck Assembly.

NOTE: The Slide Gates Shall be of the Cantilevered Type with no Track or Support Across the Opening.

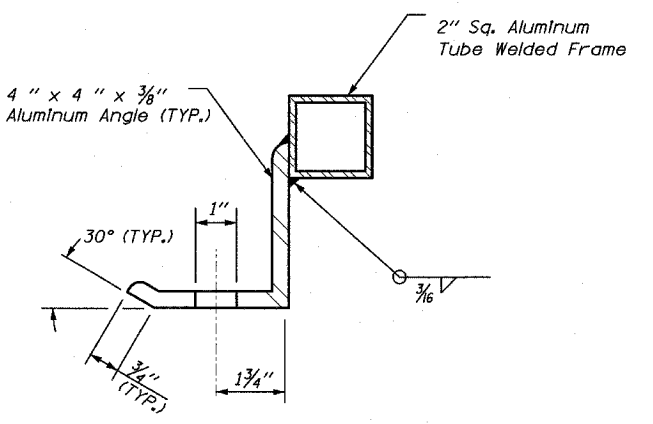
Edwards AND Kelcey
ONE NORTH FRANKLIN
CHICAGO, IL 60606
PHONE: (312) 251-3000
FAX: (312) 251-3015
WEB: WWW.EKCORP.COM



SECTION 8-8



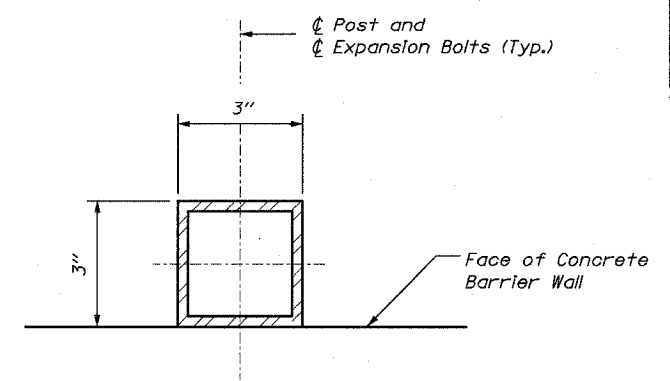
ELEVATION



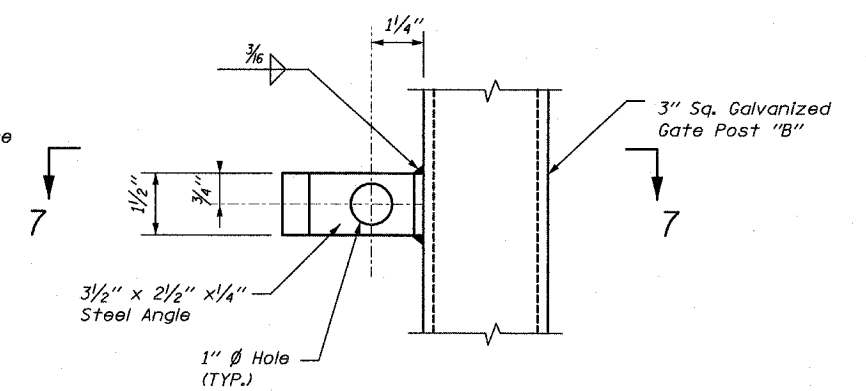
SECTION 6-6

DETAIL B

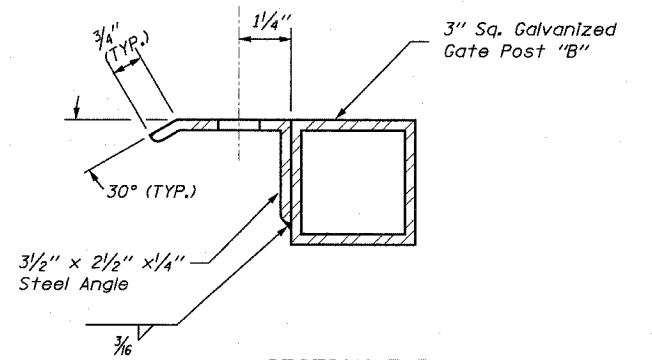
NOTE "B": The Manufacturer of the Sliding Gate Shall Set the Angle Based on the Size of the Track Frame



GATE AND OVERHANG POST SECTION



ELEVATION



SECTION 7-7

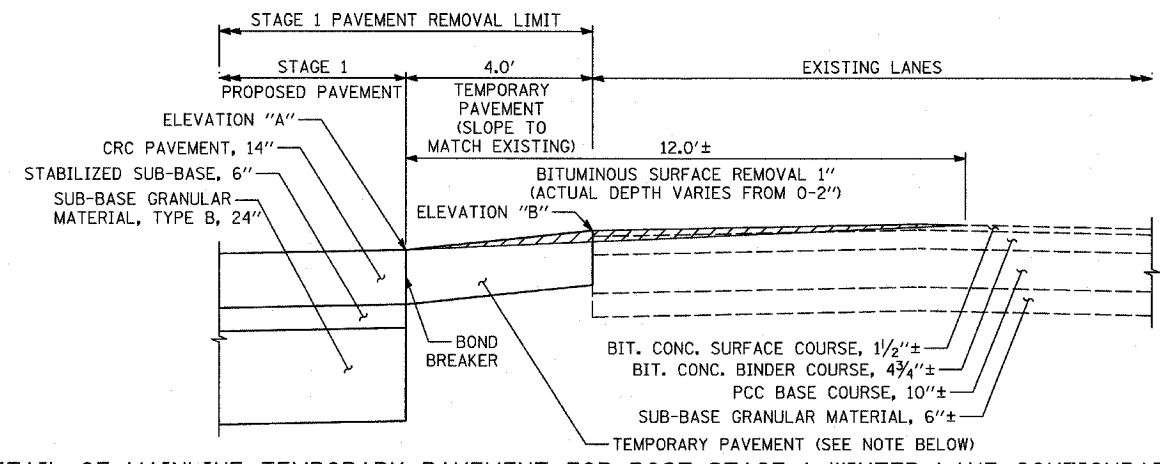
DETAIL C

REVISIONS	
NAME	DATE

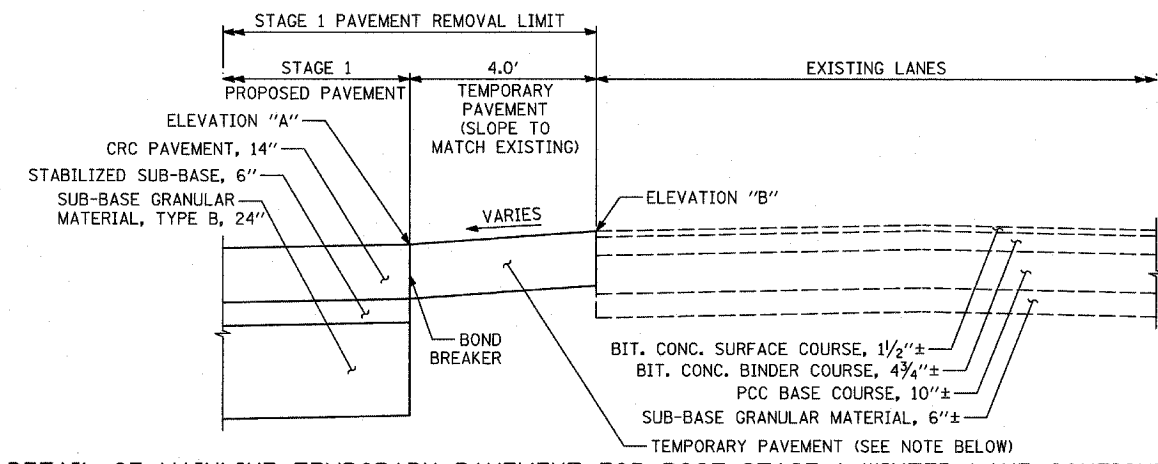
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

CTA FENCE
ELEVATIONS AND DETAILS
SHEET 4 OF 4

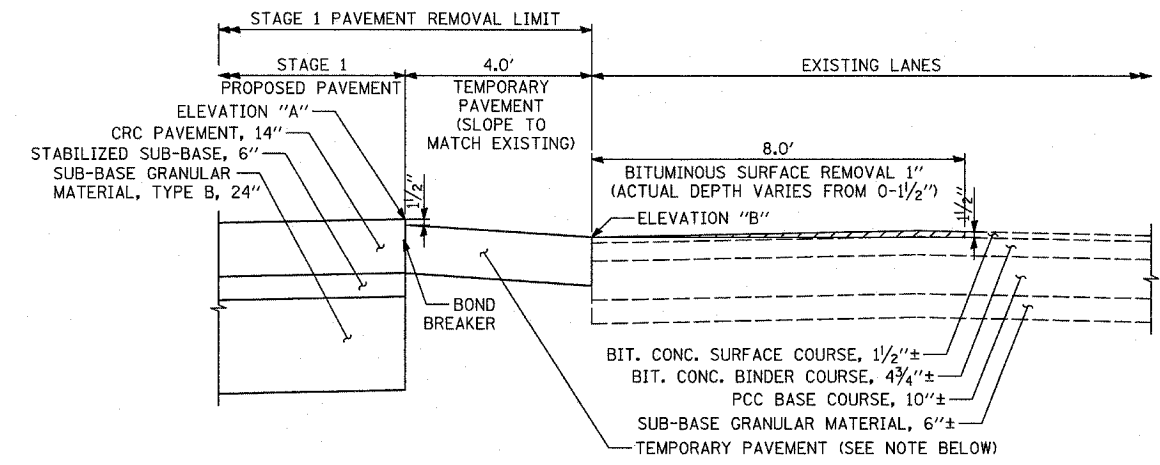
SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: RLK
CHECKED BY: EL



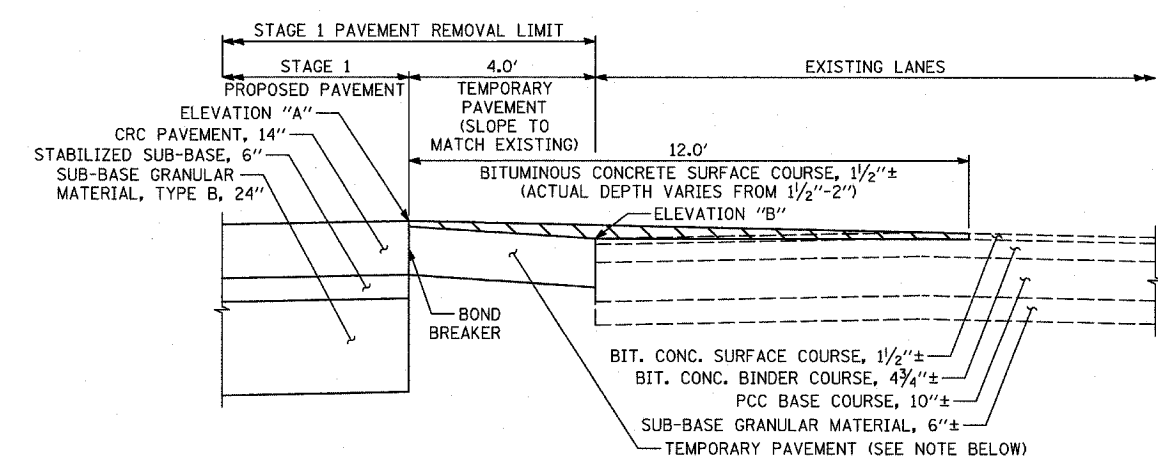
DETAIL OF MAINLINE TEMPORARY PAVEMENT FOR POST-STAGE 1 WINTER LANE CONFIGURATION
 USE WHERE ELEVATION "A" IS MORE THAN 2" BELOW ELEVATION "B"
 SEE MAINTENANCE OF TRAFFIC PLANS FOR LOCATIONS



DETAIL OF MAINLINE TEMPORARY PAVEMENT FOR POST-STAGE 1 WINTER LANE CONFIGURATION
 USE WHERE ELEVATION "A" IS 0-2" BELOW ELEVATION "B"
 SEE MAINTENANCE OF TRAFFIC PLANS FOR LOCATIONS



STEP 1
 BITUMINOUS SURFACE REMOVAL

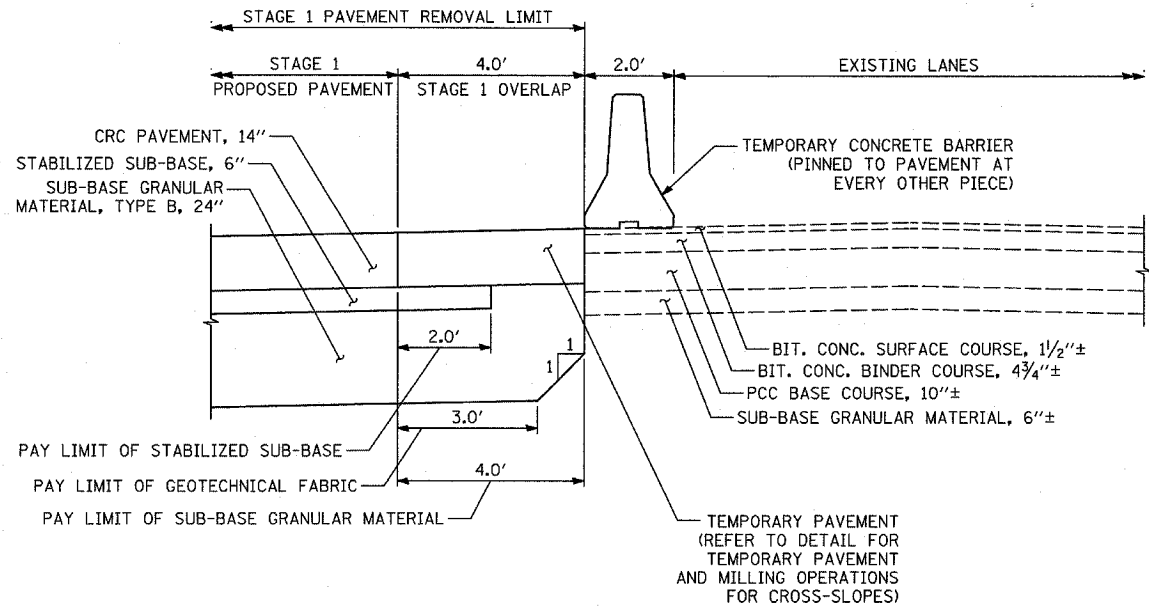


STEP 2
 PLACEMENT OF BITUMINOUS SURFACE COURSE

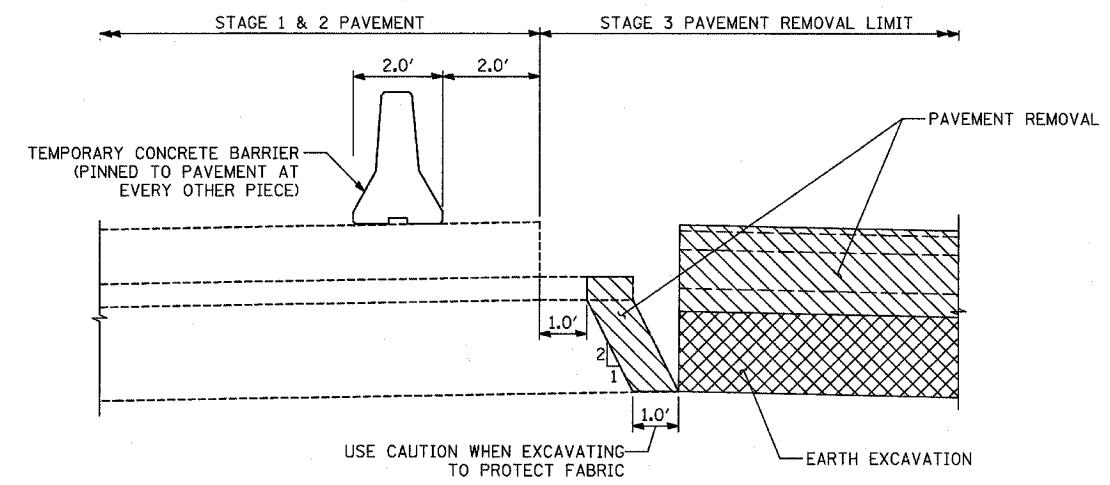
NOTE:
 CONTRACTOR MUST USE A 14" THICK CONCRETE TEMPORARY PAVEMENT OPTION. ADDITIONAL THICKNESS OF CONCRETE TEMPORARY PAVEMENT WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN THE COST OF THE TEMPORARY PAVEMENT.

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NAME	DATE

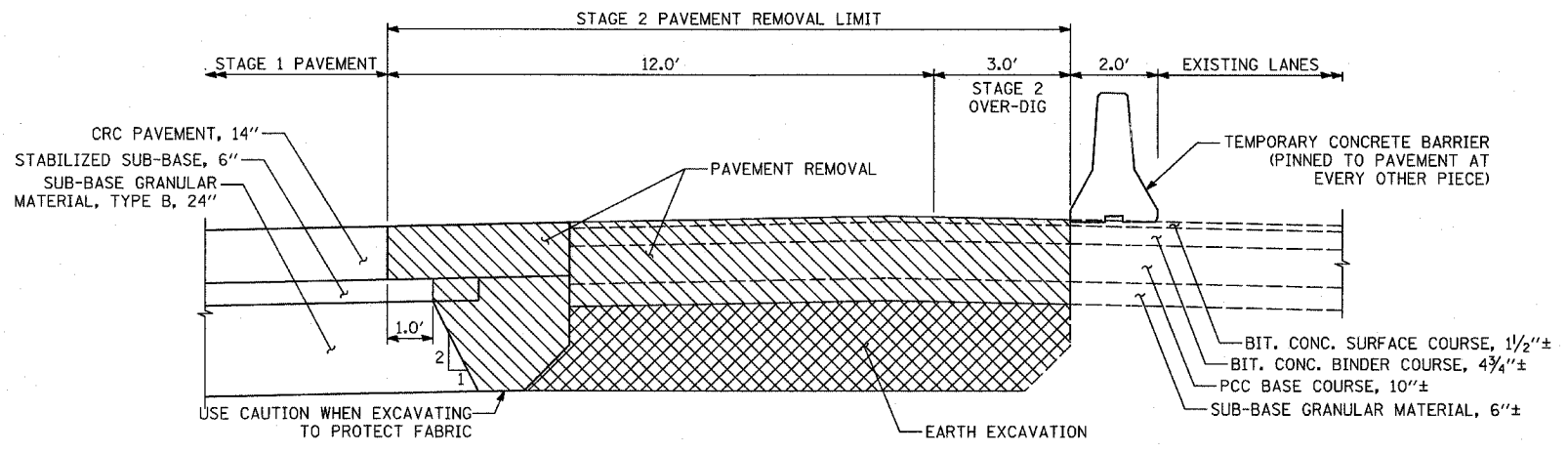
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MISCELLANEOUS DETAILS
 TEMPORARY PAVEMENT & MILLING OPERATIONS
 SCALE: AS SHOWN
 DATE: MARCH 1, 2006
 DRAWN BY: MPG
 CHECKED BY: TGB



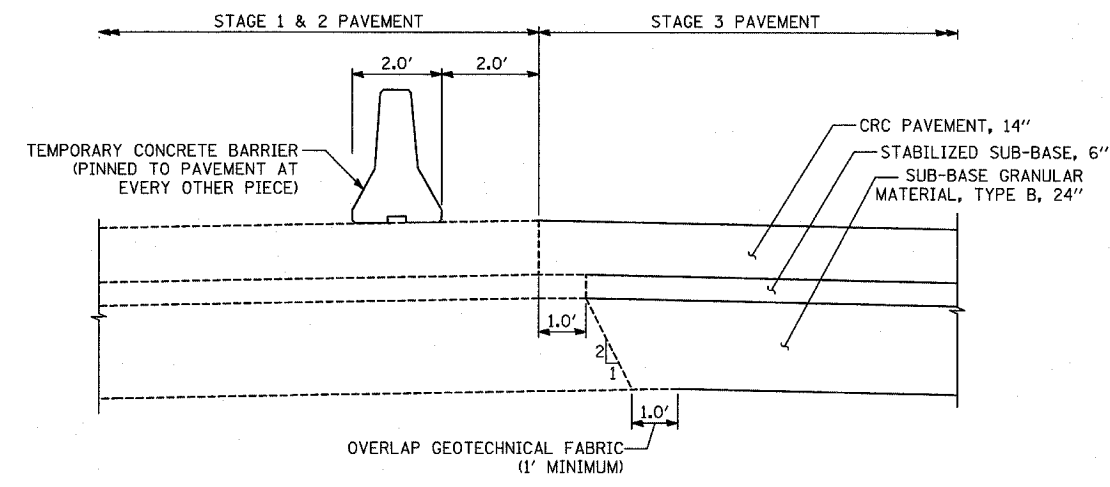
STAGE 1 PAVEMENT



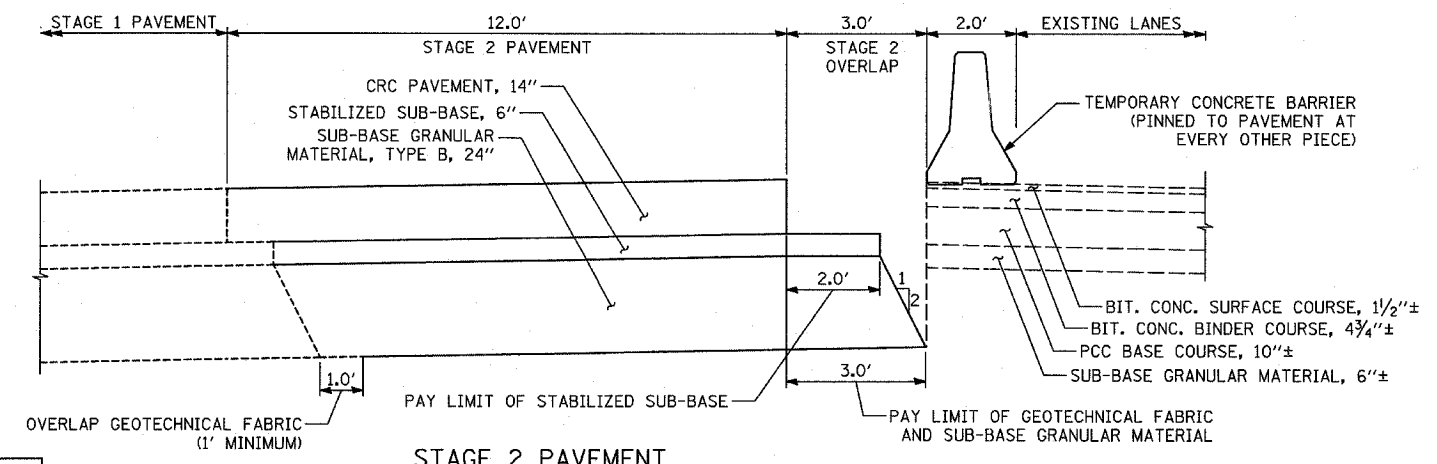
STAGE 3 REMOVAL



STAGE 2 REMOVAL & OVER-DIG



STAGE 3 PAVEMENT



STAGE 2 PAVEMENT
SEE NOTE #2

NOTES:

1. THESE SECTIONS DEPICT MAINLINE DAN RYAN. SIMILAR SITUATIONS OCCUR ALONG I-57 WITH A LESSER NUMBER OF LANES, BUT SAME CONSTRUCTION REQUIREMENTS.
2. STAGE 2A WILL REQUIRE A 3.0' WIDE STRIP OF TEMPORARY PAVEMENT BETWEEN SB I-94 STA. 1260+00 AND STA. 1270+00. ADDITIONAL THICKNESS OF CONCRETE WILL BE INCLUDED IN THE COST OF THE TEMPORARY PAVEMENT.

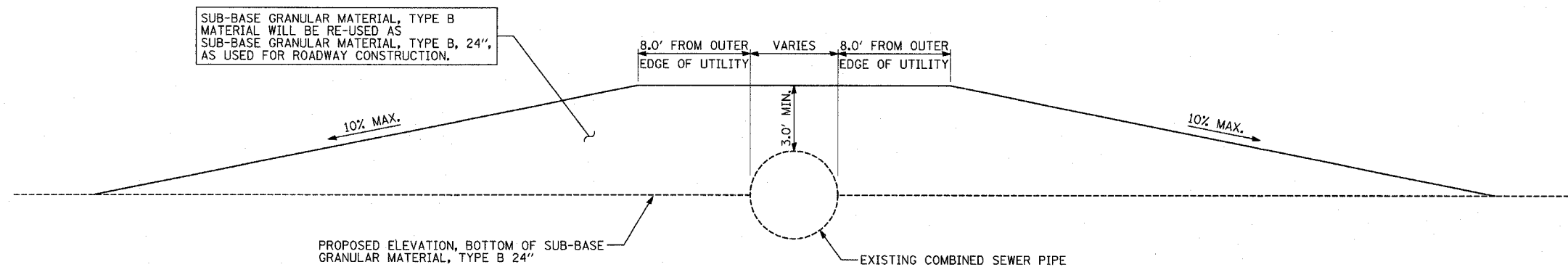
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS:
OVER-DIG AREAS

SCALE: AS SHOWN
DATE: MARCH 1, 2006

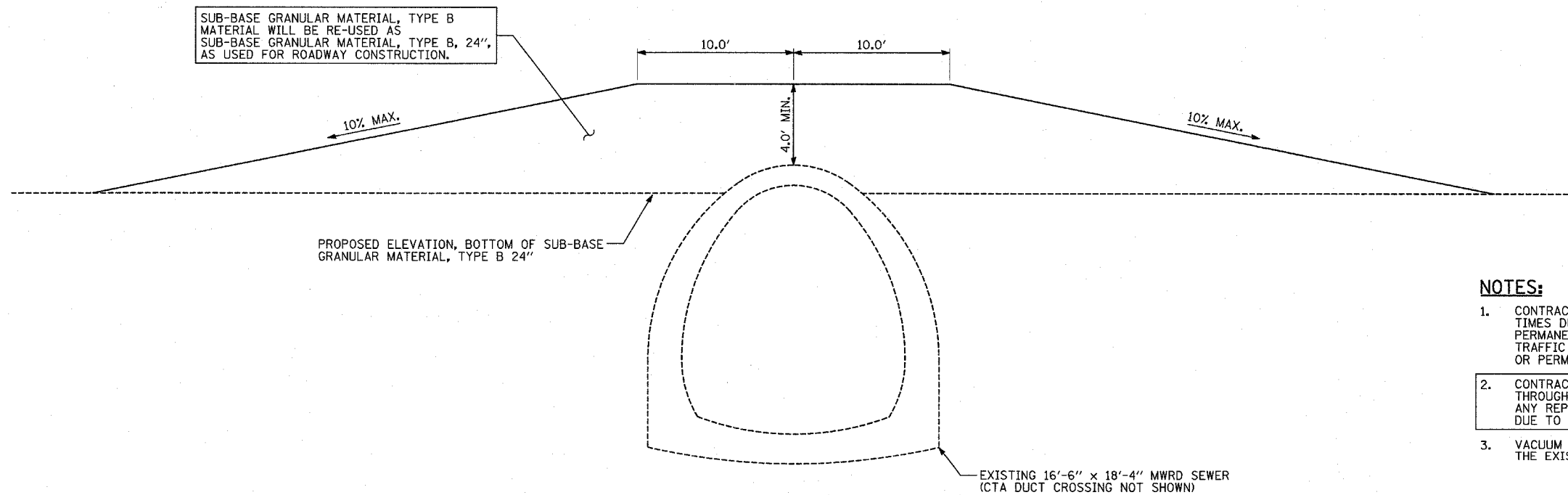
DRAWN BY: RTM
CHECKED BY: TGB



COMBINED SEWER CONSTRUCTION PROTECTION
(SEE COMBINED SEWER CROSSING STATIONS)

COMBINED SEWER CROSSING STATIONS:

1. SB I-57 STA. 90+32.99 (60")
2. SB I-57 STA. 130+21.70/
WB I-94 TO I-57 CONN. STA. 342+09.91 (5'-8" x 3'-7" BOX)
3. SB I-57 STA. 143+31.38 (7'-6" x 6'-0" DOUBLE BOX)
4. SB I-94 STA. 1245+59.70 (48")
5. SB I-94 STA. 1265+92.81 (DOUBLE 48")
6. SB I-94 STA. 1279+30.52 (TRIPLE 48")
7. SB I-94 STA. 1325+70.55 (10'-0" x 8'-0" BOX)
8. SB I-94 STA. 1345+70.10 (60")

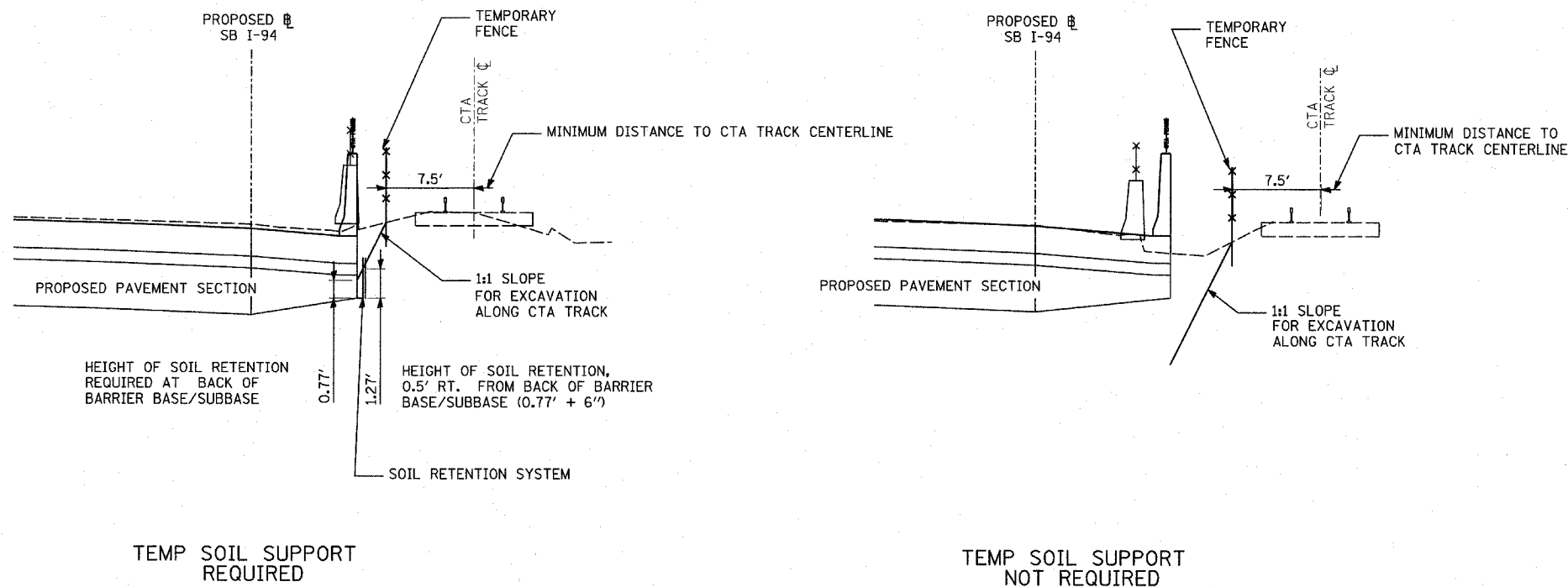


16'-6" X 18'-4" MWRD SEWER - SB I-94 STA. 1358+93.46
CTA DUCT - SB I-94 STA. 1314+13.59
CTA DUCT - SB I-57 STA. 151+94.38
CONSTRUCTION PROTECTION

NOTES:

1. CONTRACTOR WILL PROVIDE A MINIMUM COVER, AS SHOWN ABOVE AT ALL TIMES DURING CONSTRUCTION AND PRIOR TO THE INSTALLATION OF THE PERMANENT PROTECTION SLAB AND/OR ROADWAY. CONSTRUCTION TRAFFIC SHALL NOT CROSS THE UTILITIES WITHOUT TEMPORARY COVER OR PERMANENT SLAB IN PLACE.
2. CONTRACTOR IS RESPONSIBLE FOR THE INTEGRITY OF SEWER PIPE THROUGH THE DURATION OF THE PROJECT, AND WILL NOT BE PAID FOR ANY REPAIRS REQUIRED, AS DETERMINED BY THE ENGINEER, TO THE PIPE, DUE TO CONSTRUCTION ACTIVITIES.
3. VACUUM EXCAVATION OR HAND DIGGING MUST BE USED WITHIN 2'-0" OF THE EXISTING 16'-6" x 18'-4" MWRD SEWER AND CTA DUCTS ONLY.

REVISIONS	
NAME	DATE



SOIL RETENTION SCHEDULE

NBDR STATION	HEIGHT OF RETENTION FT (SET 6" BEHIND BARRIER BASE)	SOIL RETENTION AREA SF
1213+81	0.00	0.0
1214+31	0.98	49.0
1214+81	1.39	69.5
1215+31	1.59	79.5
1215+81	1.90	95.0
1216+31	1.98	99.0
1216+81	1.78	89.0
1217+31	1.43	71.5
1217+81	1.49	74.5
1218+31	1.43	71.5
1218+81	1.27	63.5
1219+31	1.17	58.5
1219+81	1.33	66.5
1220+31	1.30	65.0
1220+81	1.14	57.0
1221+31	1.12	56.0
1221+81	1.12	56.0
1222+31	1.10	55.0
1222+81	0.63	31.5
1223+31	0.00	0.0
1259+31	0.00	0.0
1259+81	0.85	42.5
1260+31	0.00	0.0
1312+81	0.00	0.0
1313+31	1.34	67.0
1313+81	0.00	0.0
* NUMBER OF DRAINAGE STRUCTURES		109
** 1206+58 - 1208+93		1255.0
** 1211+90 - 1213+10		640.0
TOTAL		11932.0

CALCULATION OF HEIGHT OF TEMP SOIL SUPPORT

- * 80 SF OF EARTH RETENTION IS REQUIRED FOR EACH DRAINAGE STRUCTURE. SEE DRAINAGE SCHEDULES FOR DRAINAGE STRUCTURES REQUIRING EARTH RETENTION
- ** AREA REFLECTS EARTH RETENTION SYSTEM REQUIRED IN ADDITION TO THAT REQUIRED FOR THE PROPOSED DRAINAGE STRUCTURES, SEE SHEET 114.

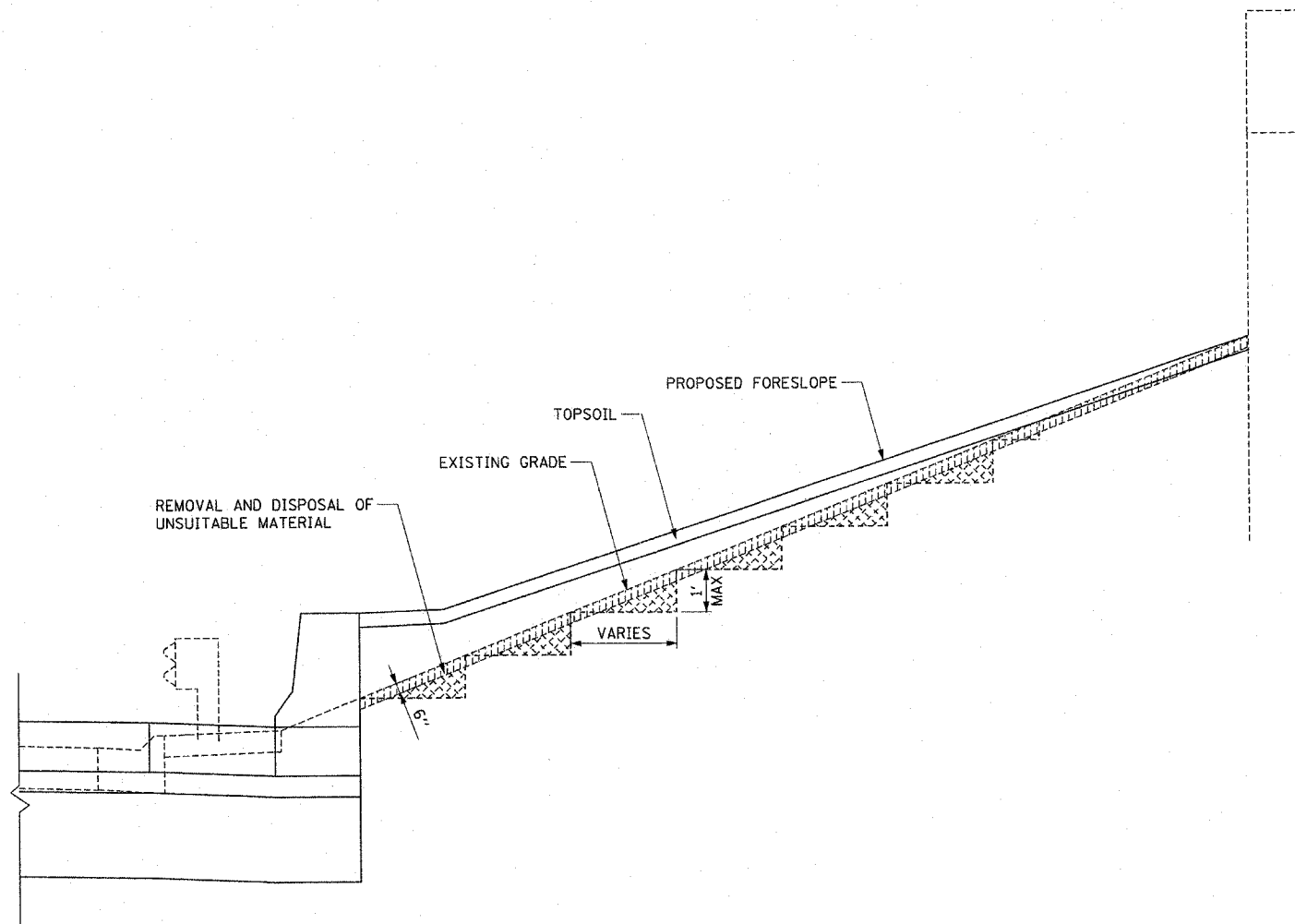
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)

MISCELLANEOUS DETAILS
TEMPORARY SOIL RETENTION
SECTIONS AND SCHEDULE

SCALE: NONE
DATE: MARCH 1, 2006
DRAWN BY: JJS
CHECKED BY: JPM/SP

BENCHING DETAIL FOR EMBANKMENT N.T.S.



LEGEND:

EARTH EXCAVATION (SPECIAL)

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

- NOTES:**
1. SEE LANDSCAPING PLANS AND DETAILS FOR TOPSOIL AND SEEDING TYPES.
 2. THE EXCAVATION OF THE TOP 6" OF EXISTING SOIL SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL."
 3. CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
 4. EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.04 OF THE STANDARD SPECIFICATIONS.
 5. BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
 6. EQUAL 6-INCH LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.06 OF THE STANDARD SPECIFICATIONS.
 7. EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC YARD FOR "EARTH EXCAVATION (SPECIAL)". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
 8. SEE CROSS SECTIONS SHEETS FOR EMBANKMENT AREAS THAT NEED TO BE BENCHED.

EARTH EXCAVATION (SPECIAL) SCHEDULE

ALIGNMENT	STATION		EARTH EXCAVATION (SPECIAL) (CU YD)
	FROM	TO	
SB I-57	144+00	145+80	50
SB I-94	1265+50	1269+00	65
TOTAL			115

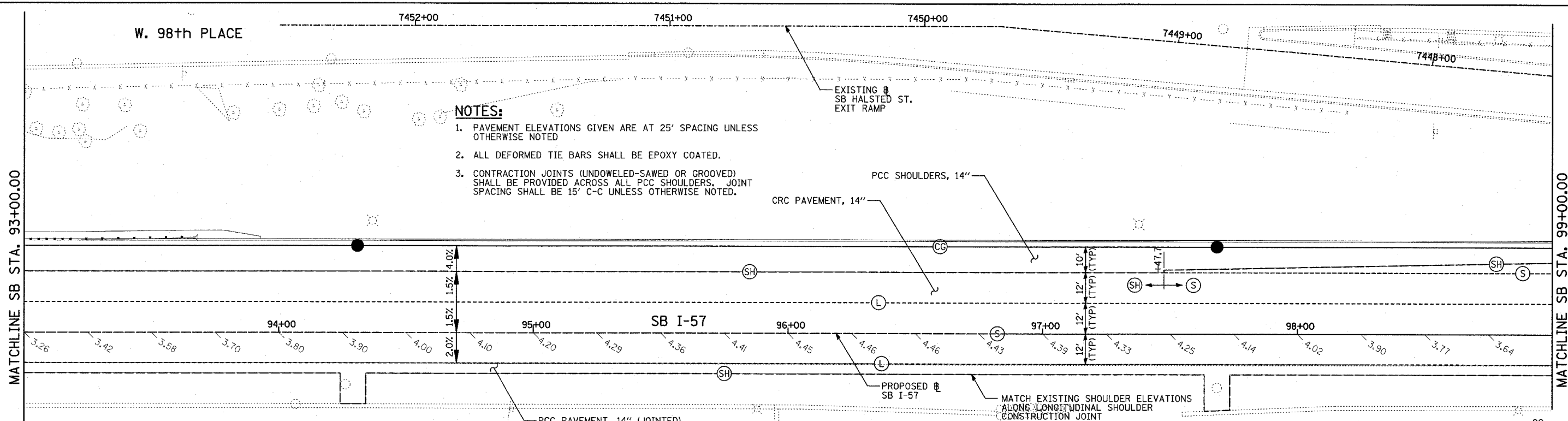
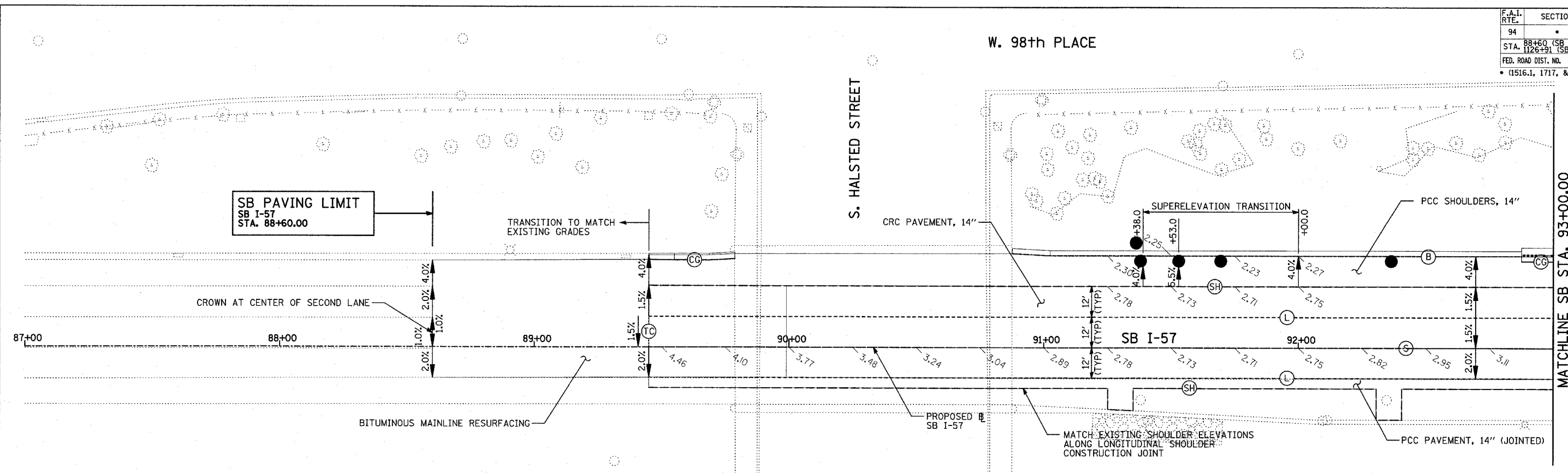
CTE | AECOM

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION F.A.I. 90/94 (DAN RYAN EXPRESSWAY) 63RD STREET TO 55TH STREET (NB LOCAL LANES)
NAME	DATE	
		ROADWAY DETAILS BENCHING DETAIL FOR EMBANKMENT

SCALE: NONE DRAWN BY: NJH
DATE: April 7, 2006 CHECKED BY: XXX

RDT-12

P:\P40466\CM\IN8-Cntfrs 18 & 19\SYM\CNTT 18A\Roadway Details\MK1810122.dwg 11:38:38 AM 2/20/2006



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

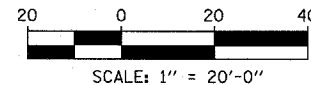
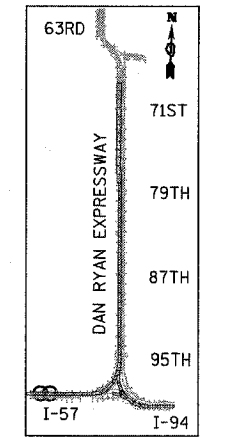
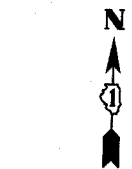
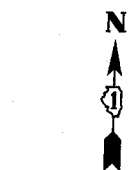
- | | | |
|--|---|--|
| <p>(B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.</p> <p>(CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)</p> <p>(S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)</p> | <p>(SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)</p> <p>(L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> | <p>(E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).</p> <p>(EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)</p> |
|--|---|--|

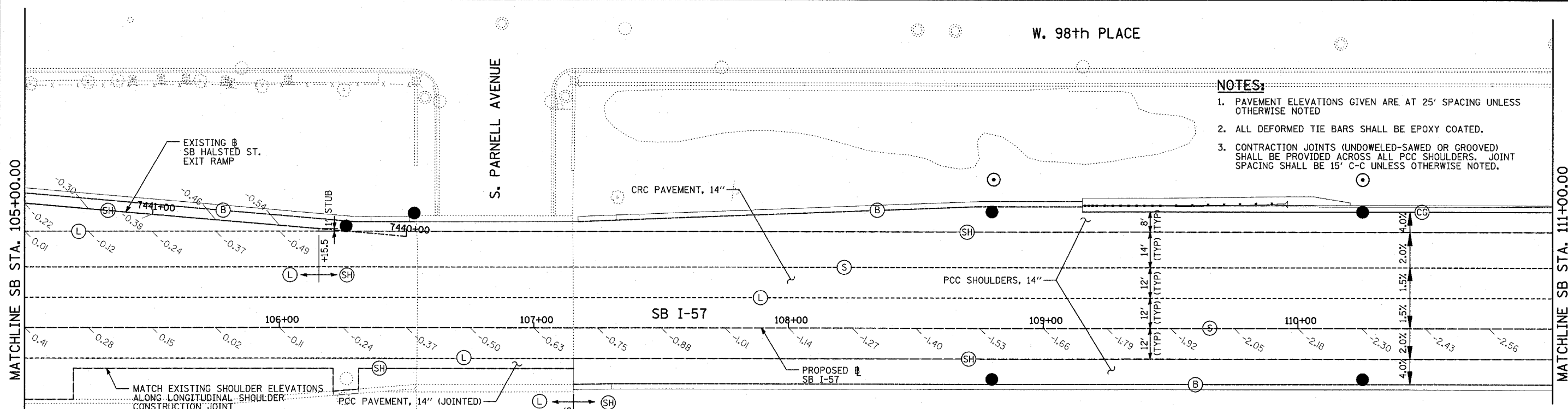
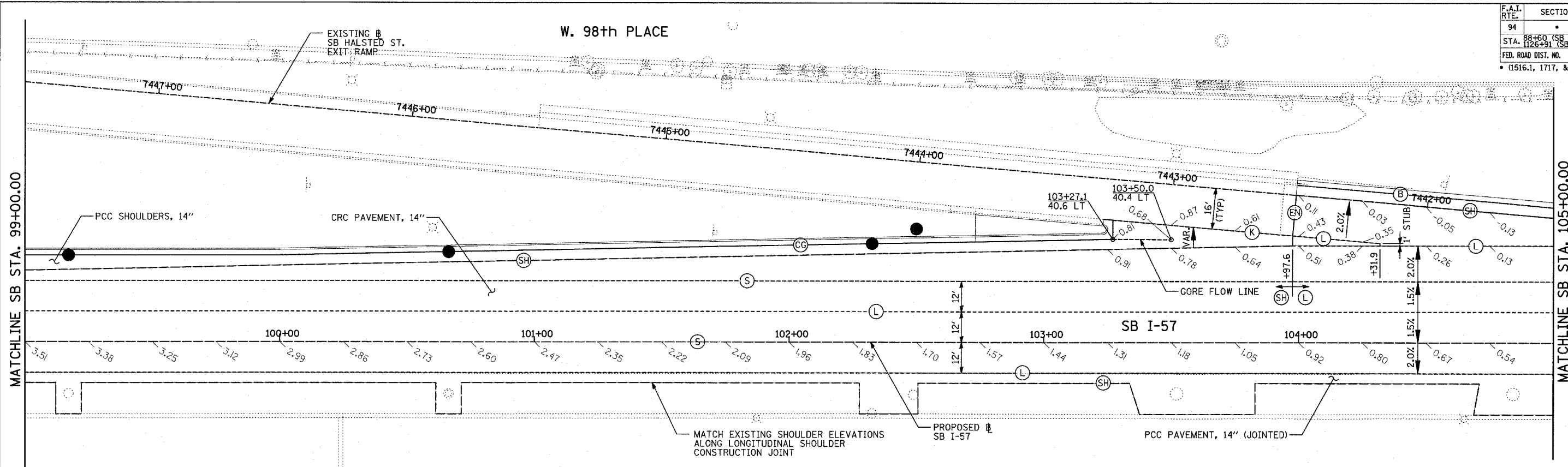
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-57
STA. 88+60.00 TO 99+00.00
SHEET 1 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006
DRAWN BY: JJS
CHECKED BY: MPG





NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

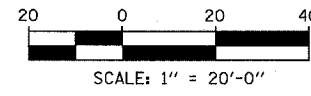
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-57
STA. 99+00.00 TO 111+00.00
SHEET 2 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006
DRAWN BY: JJS
CHECKED BY: MPG



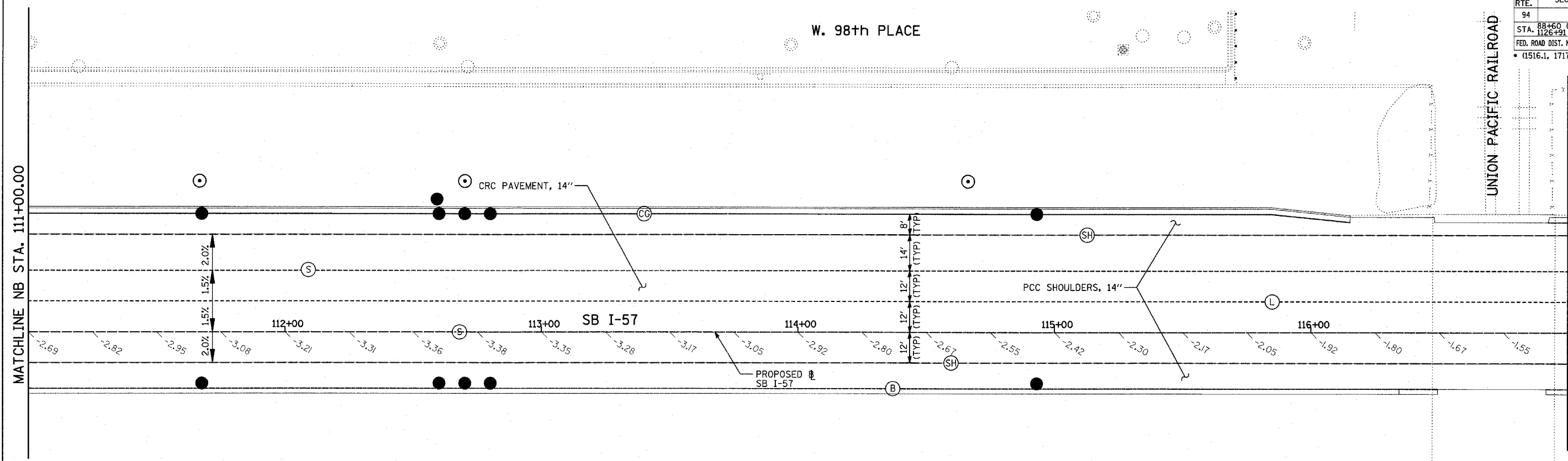
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	154
STA. 112+00 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5 62593				

W. 98th PLACE

MATCHLINE NB STA. 111+00.00

MATCHLINE SB STA. 117+00.00

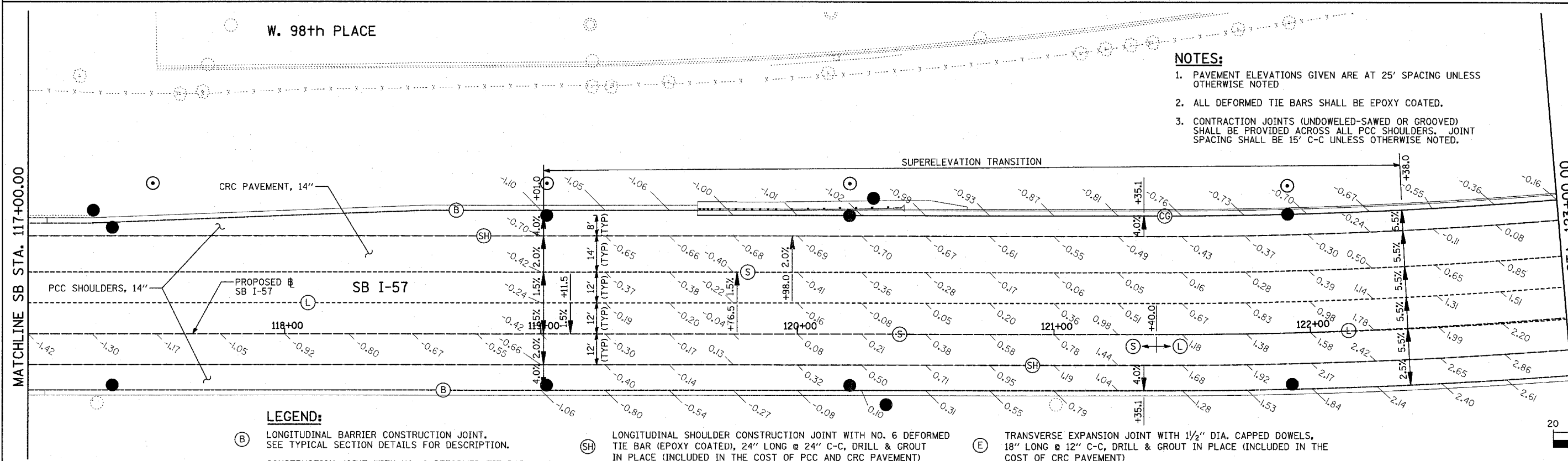
UNION PACIFIC RAILROAD



MATCHLINE SB STA. 117+00.00

MATCHLINE SB STA. 123+00.00

W. 98th PLACE



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
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- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

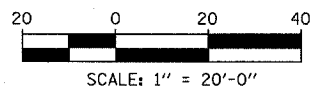
TYLIN INTERNATIONAL

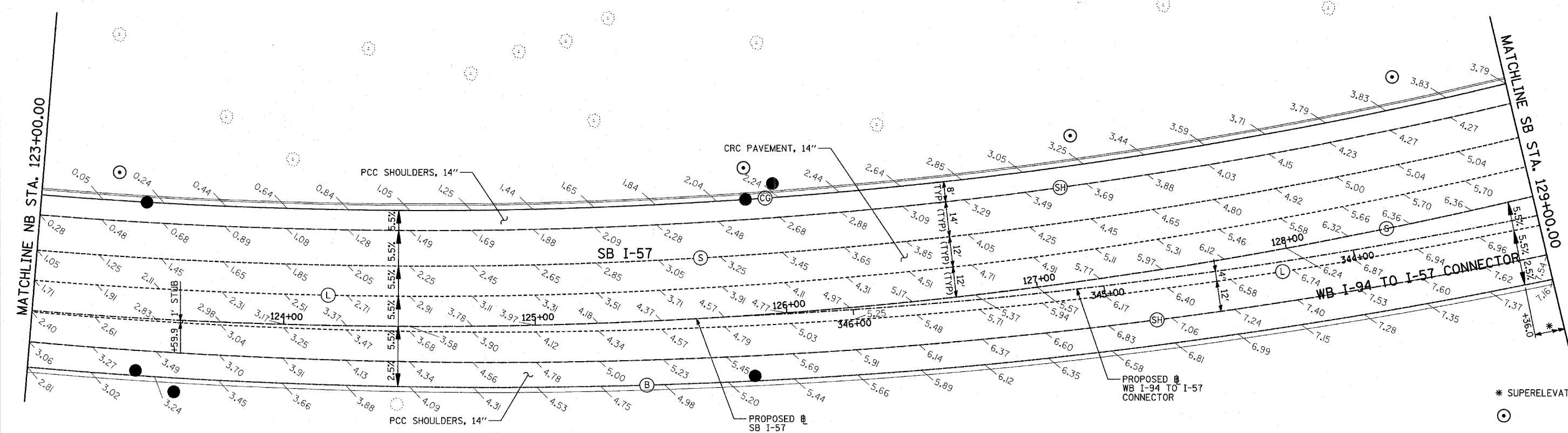
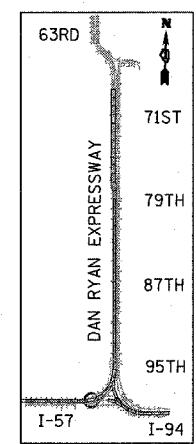
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-57
STA. 111+00.00 TO 123+00.00
SHEET 3 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006

DRAWN BY: JJS
CHECKED BY: MPG



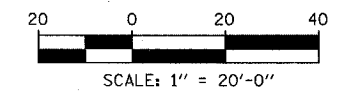


NOTES:

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LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

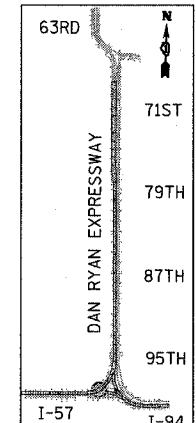
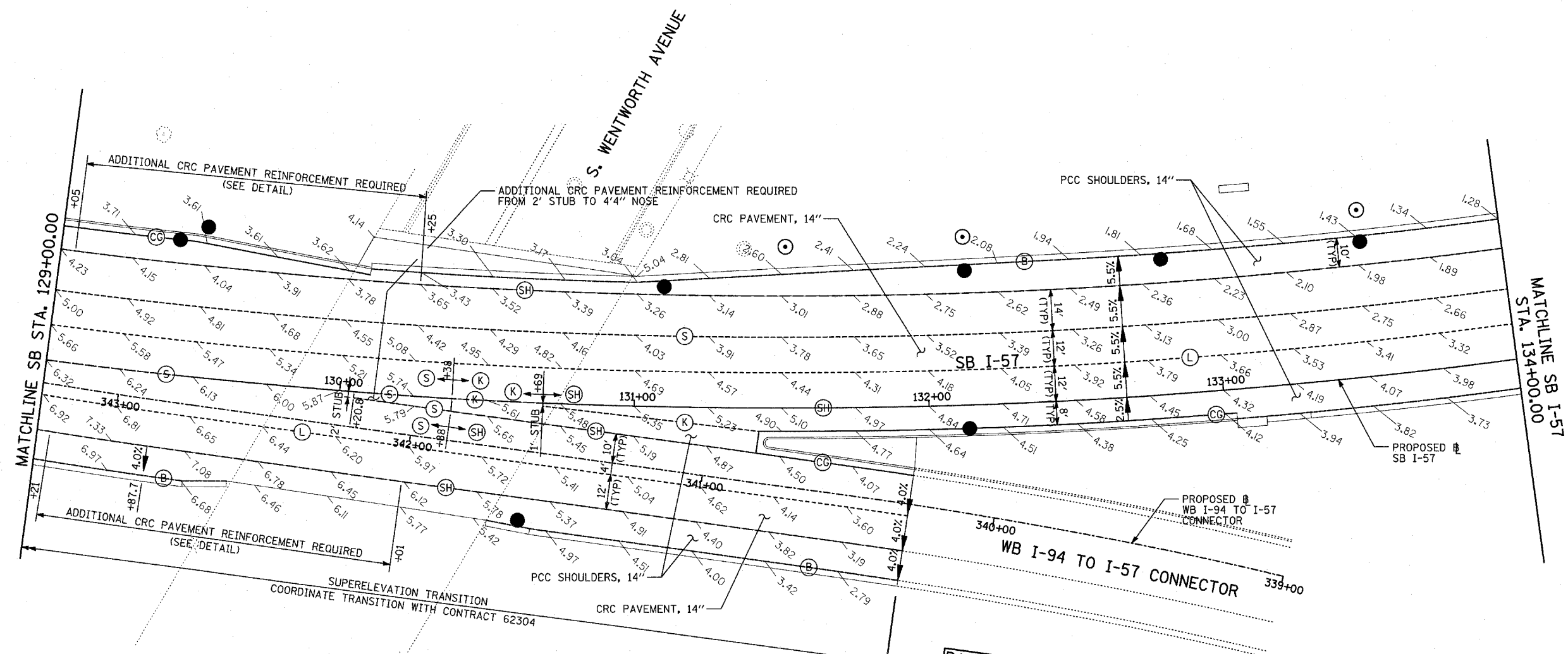
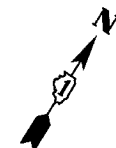


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-57 & WB I-94 TO I-57 CONNECTOR
 SB I-57 STA. 123+00.00 TO 129+00.00
 SHEET 4 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



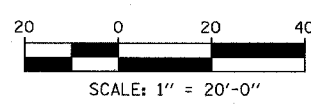
LOCATION MAP

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

NOTES:

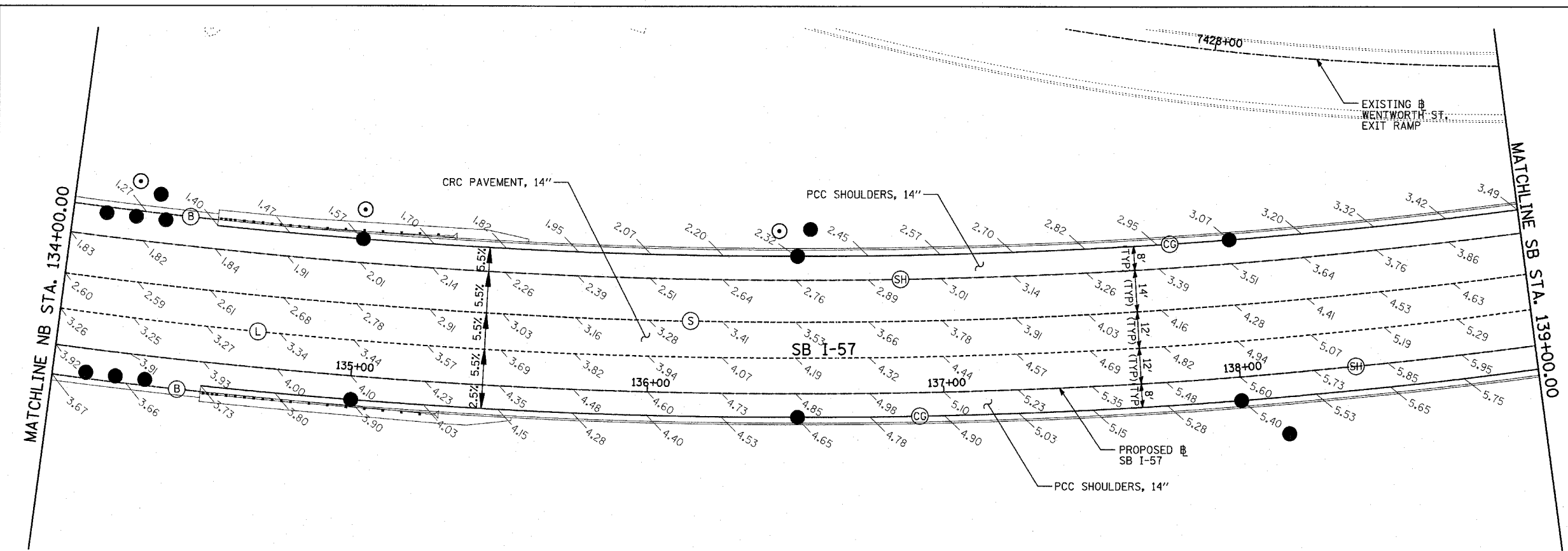
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



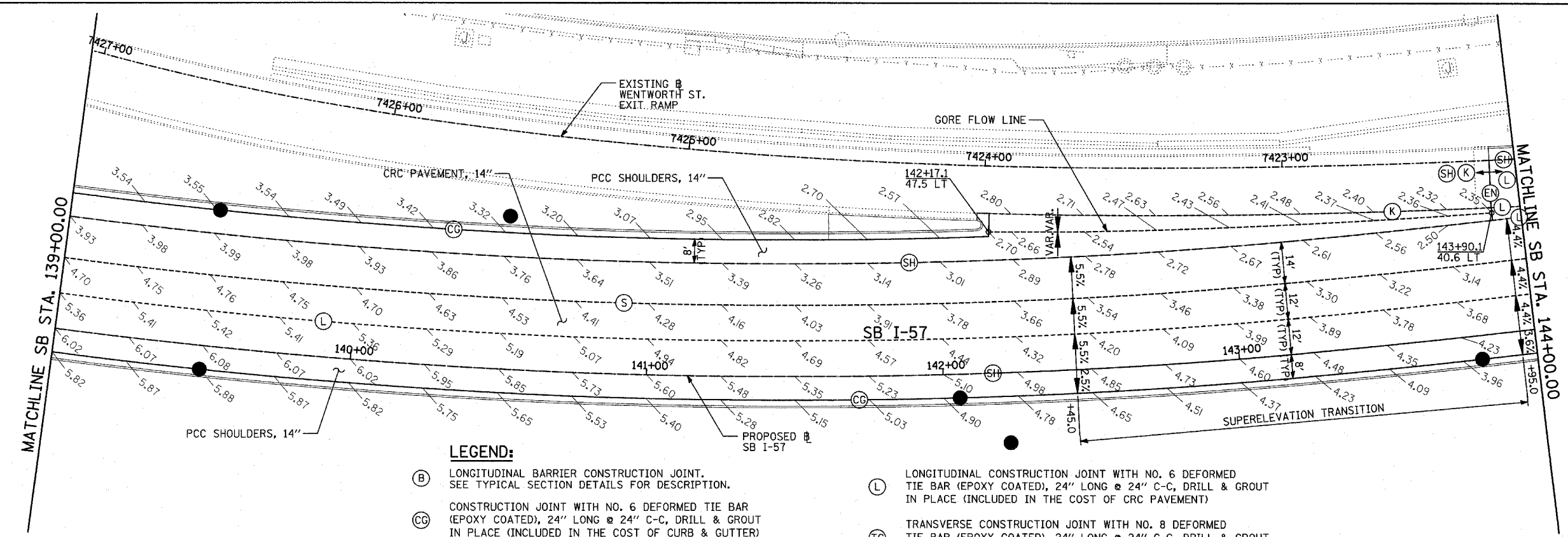
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-57 & WB I-94 TO I-57 CONNECTOR
 SB I-57 STA. 129+00.00 TO STA. 134+00.00
 SHEET 5 OF 23

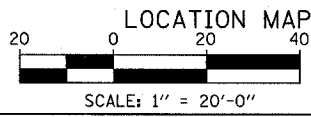
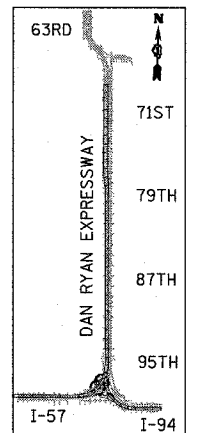
SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
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- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
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 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

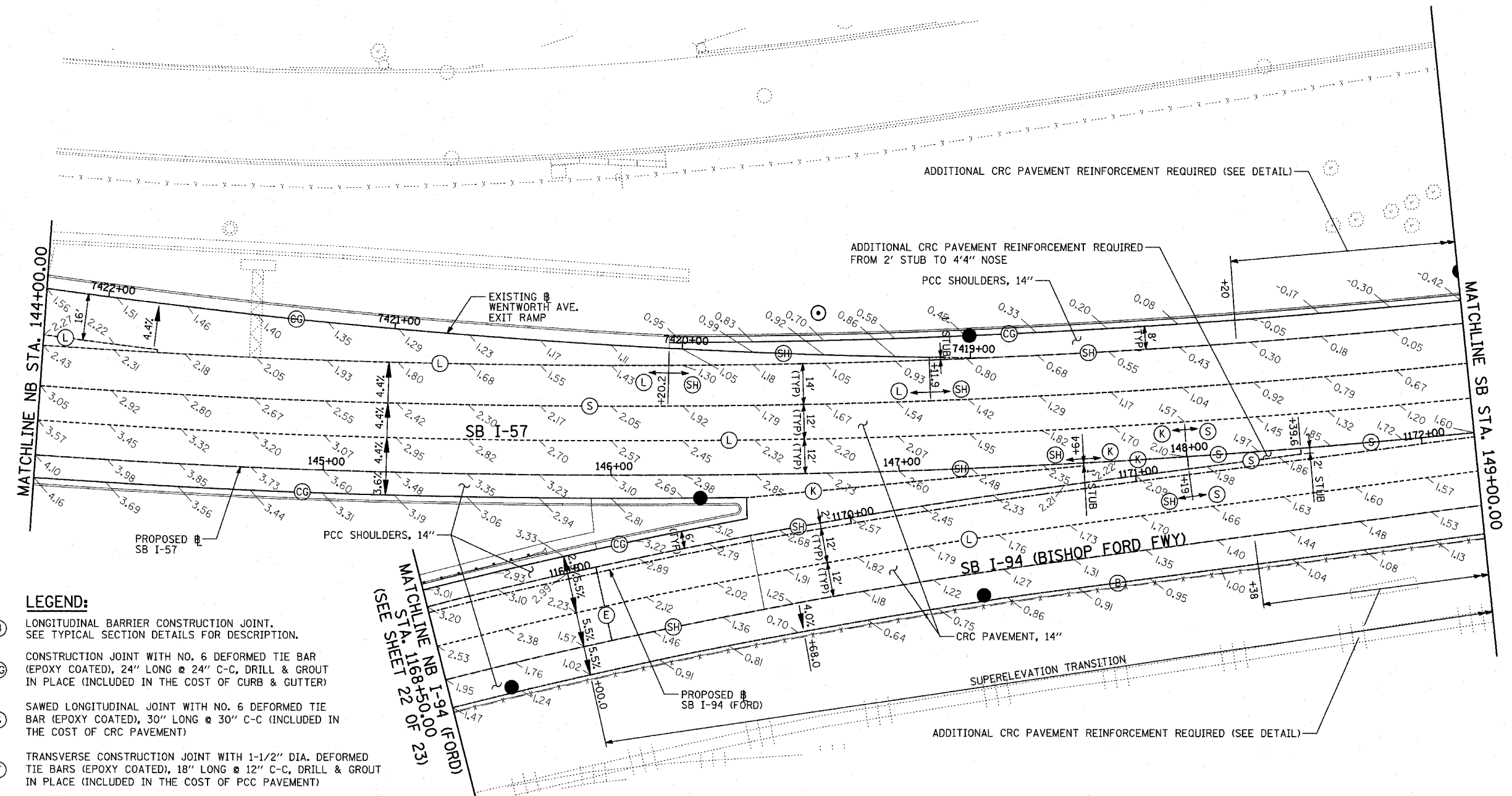


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-57
 STA. 134+00.00 TO 144+00.00
 SHEET 6 OF 23**

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: JPM

TYLIN INTERNATIONAL

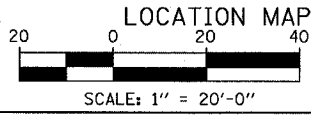
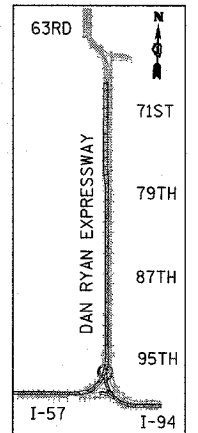


LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
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- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
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NOTES:

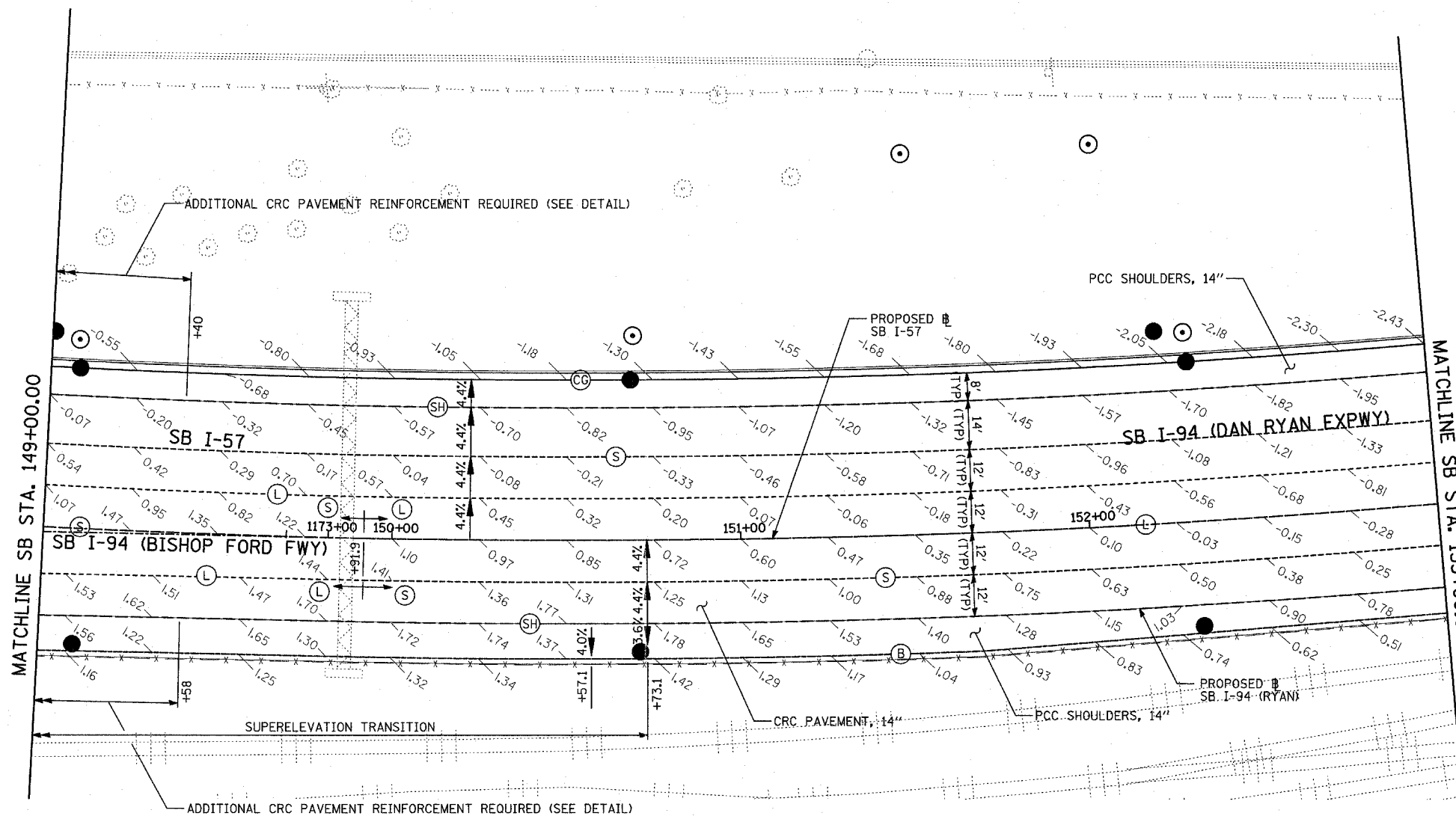
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-57 & SB I-94 (BISHOP FORD FWY)
 SB I-57 STA. 144+00.00 TO 148+00.00
 SHEET 7 OF 23**

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

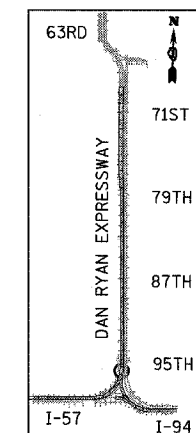


NOTES:

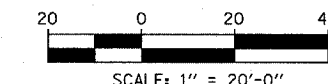
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

- | | |
|---|---|
| <p>(B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.</p> <p>(CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)</p> <p>(S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)</p> <p>(SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)</p> | <p>(L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)</p> <p>(K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).</p> <p>(EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)</p> |
|---|---|



LOCATION MAP



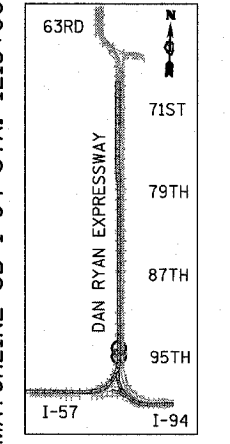
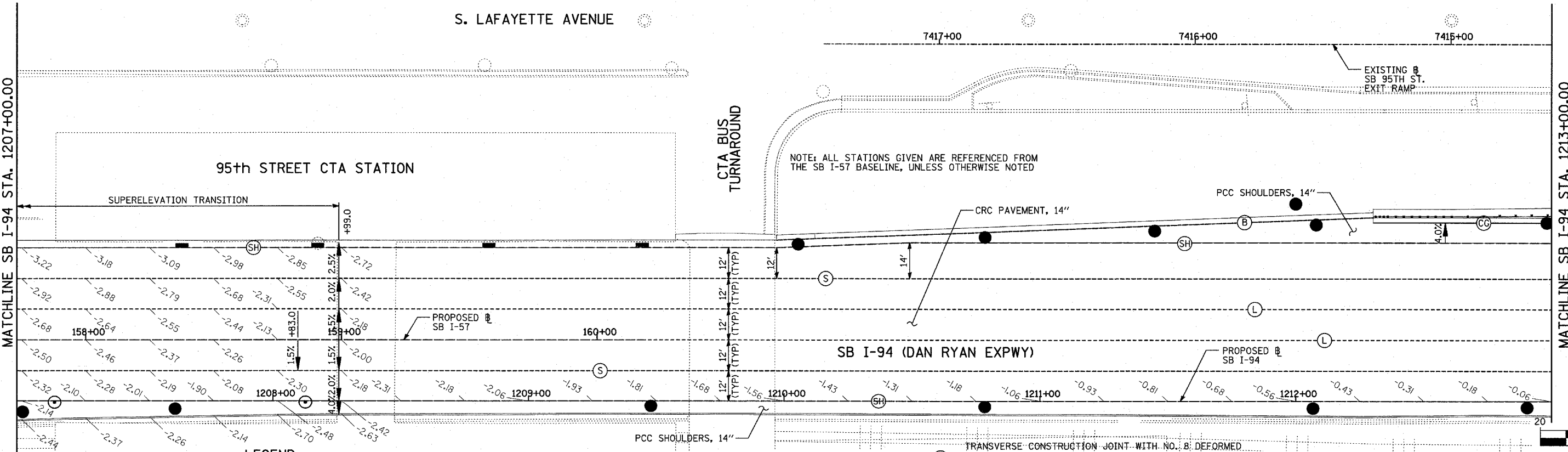
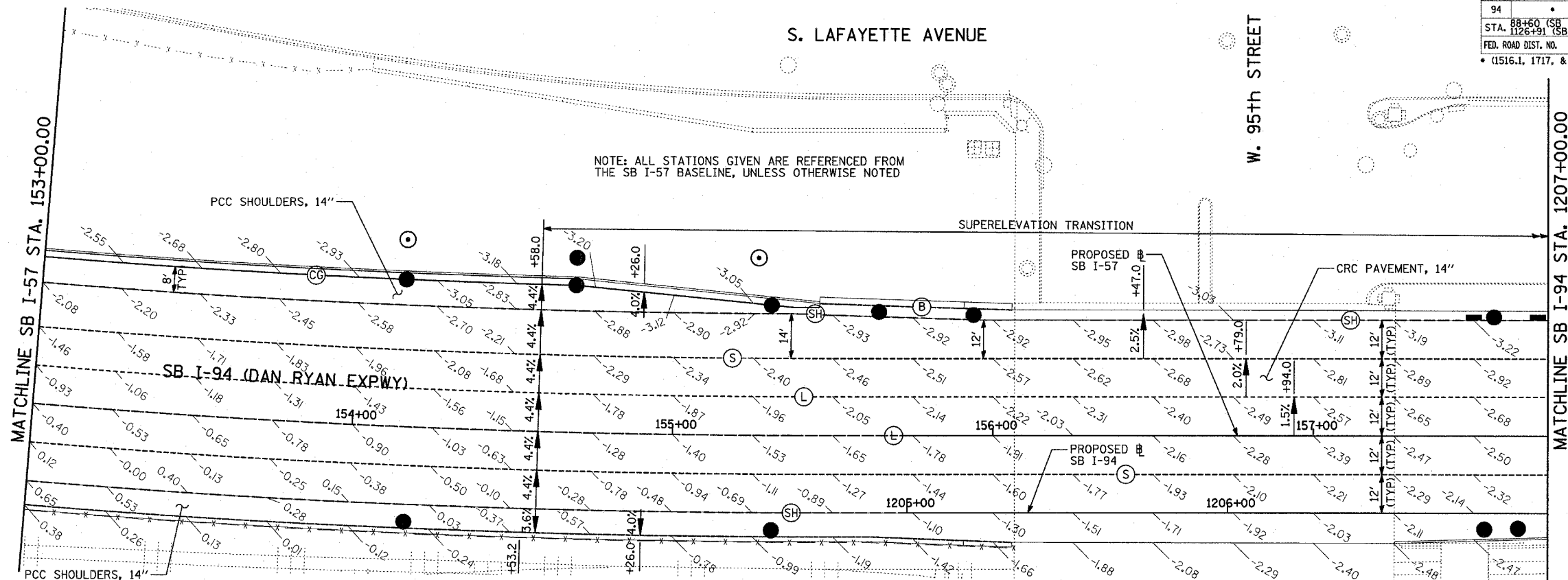
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-57
 STA. 148+00.00 TO 153+00.00
 SHEET 8 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JUS
 CHECKED BY: MPG

TYLIN INTERNATIONAL

- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LEGEND:

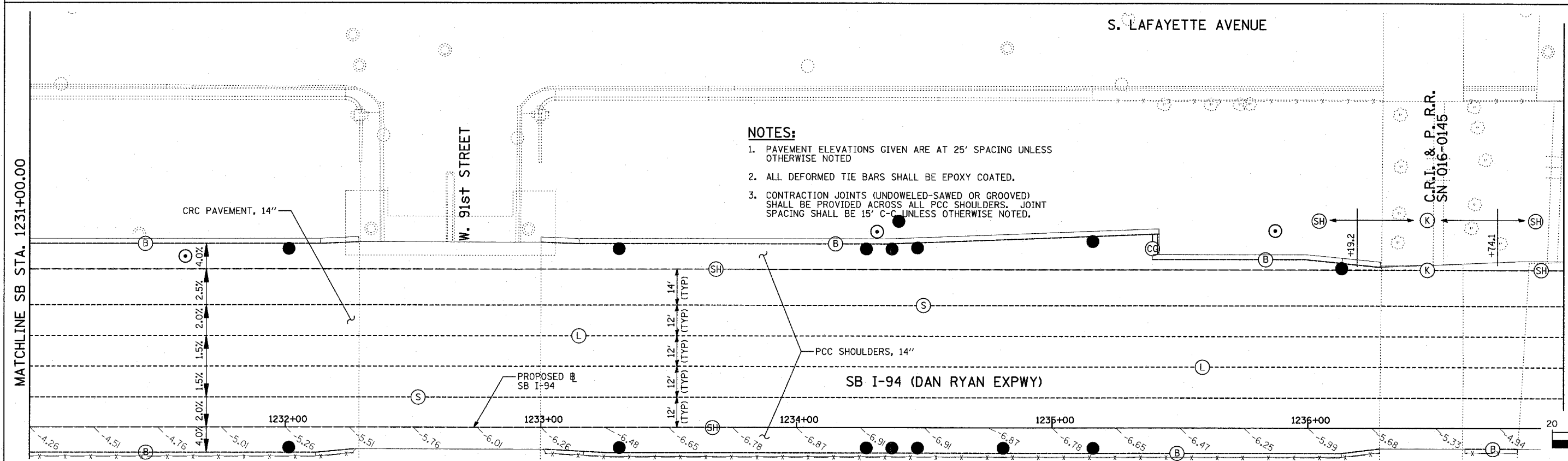
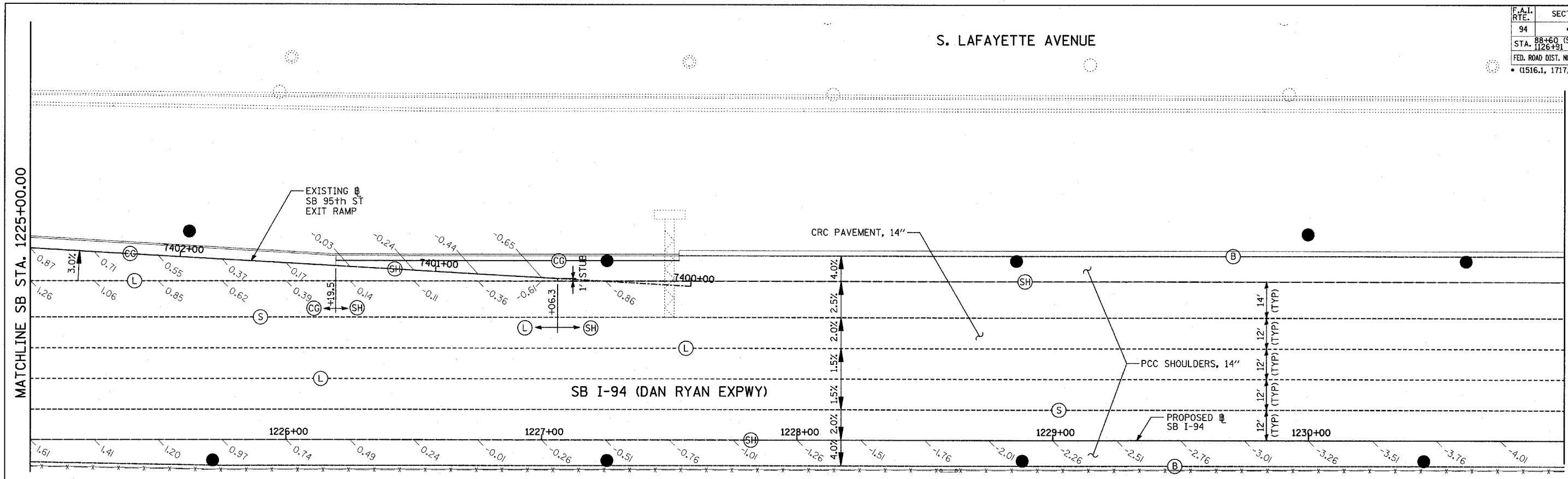
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

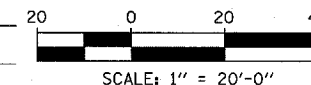
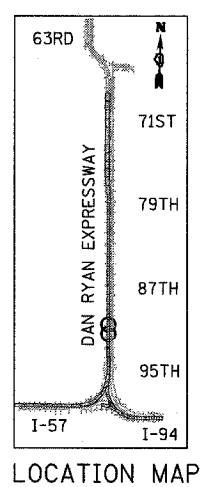
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-57 & SB I-94 - SB I-57 STA. 153+00.00
TO SB I-94 STA. 1213+00.00
SHEET 9 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006
DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

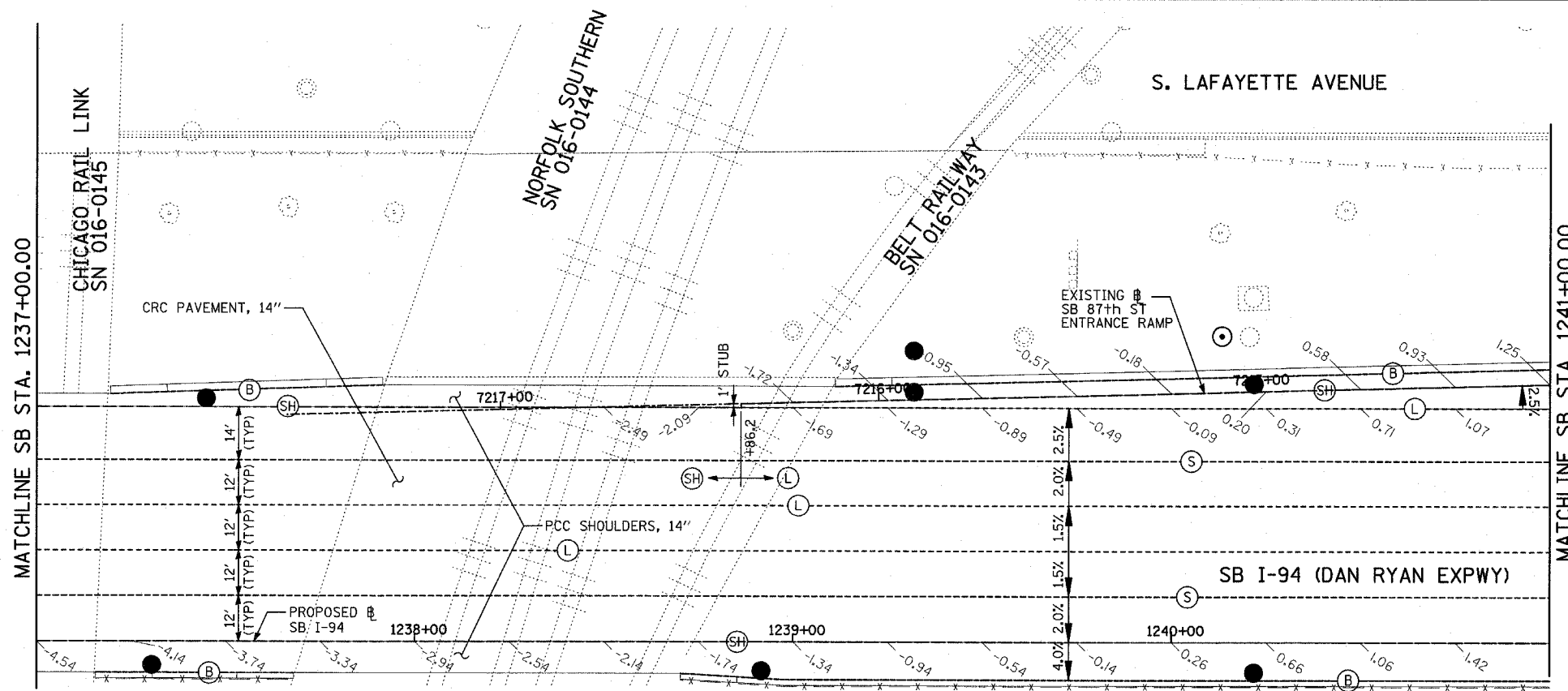
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
STA. 1225+00.00 TO 1237+00.00
SHEET 11 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006

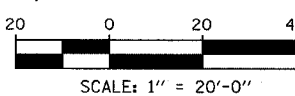
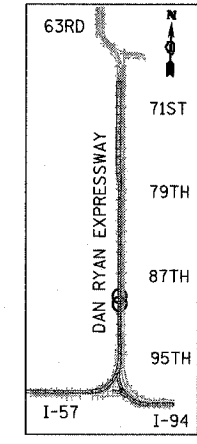
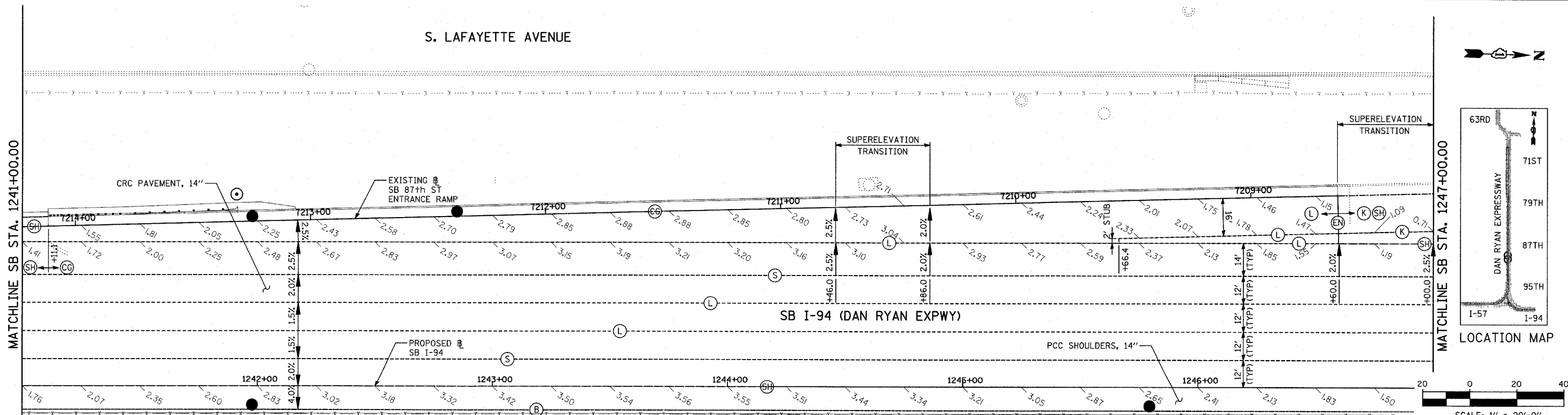
DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL



NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



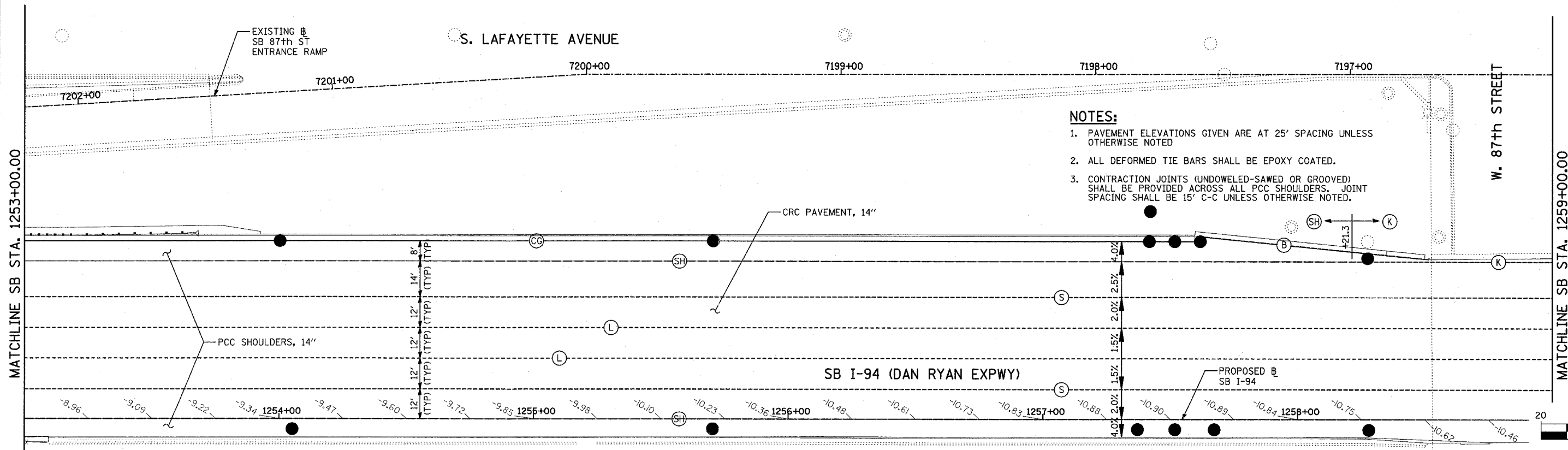
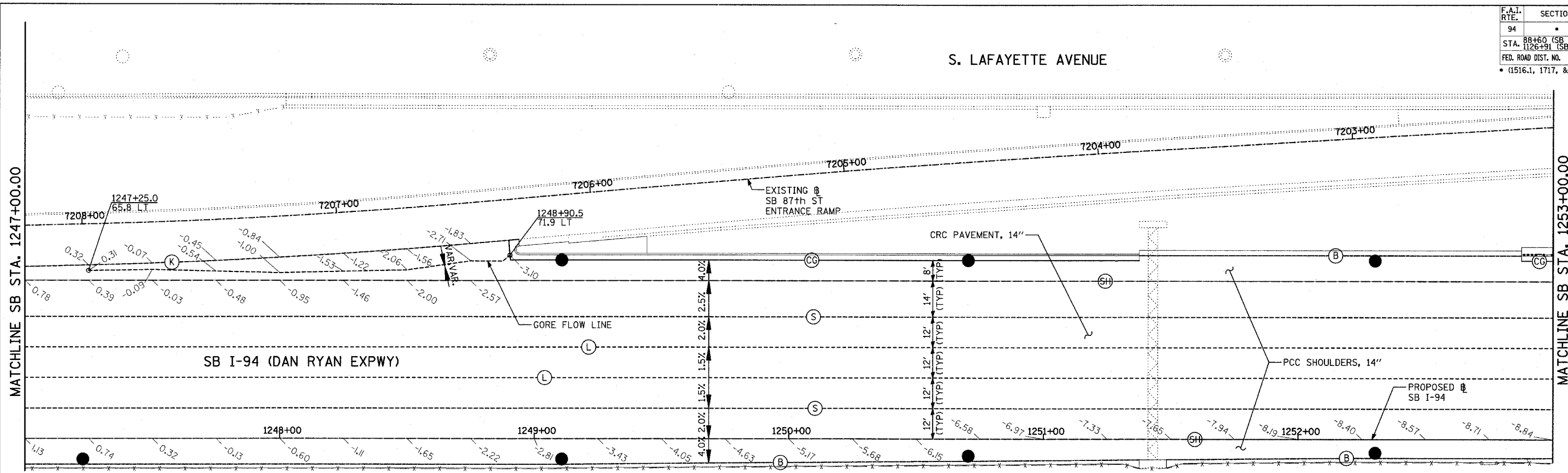
LEGEND:

- | | | |
|--|---|--|
| (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. | (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) | (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) | (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) | (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). |
| | | (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |

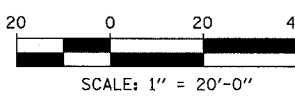
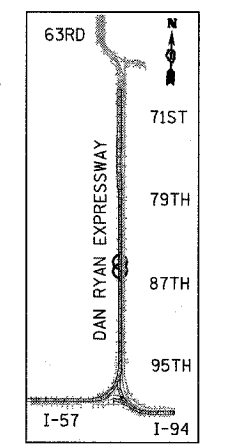
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1237+00.00 TO 1247+00.00
 SHEET 12 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

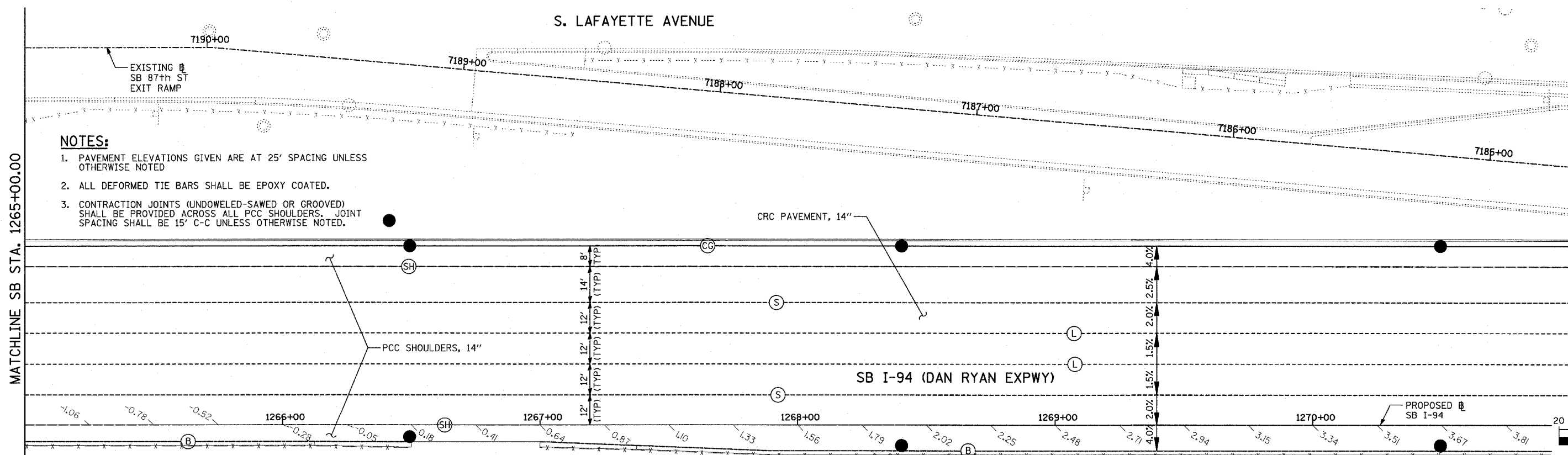
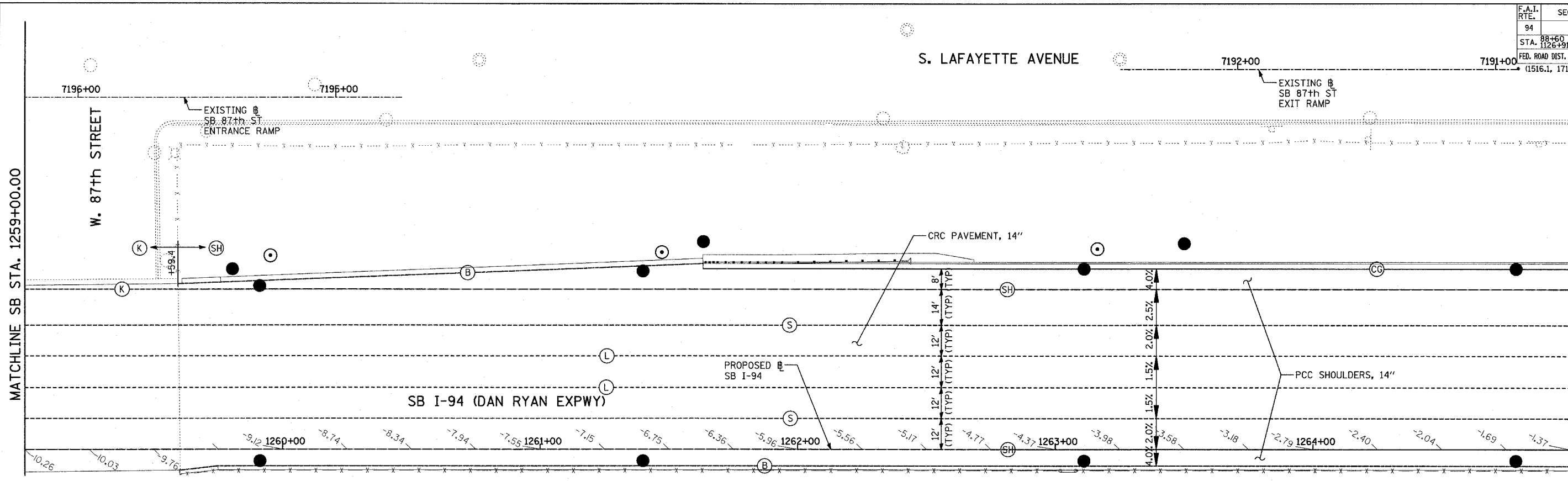
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1247+00.00 TO 1259+00.00
 SHEET 13 OF 23**

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: JUS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)

- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

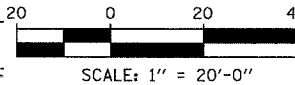
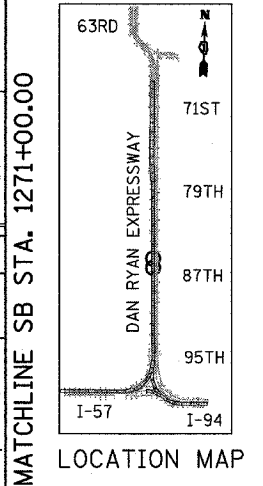
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

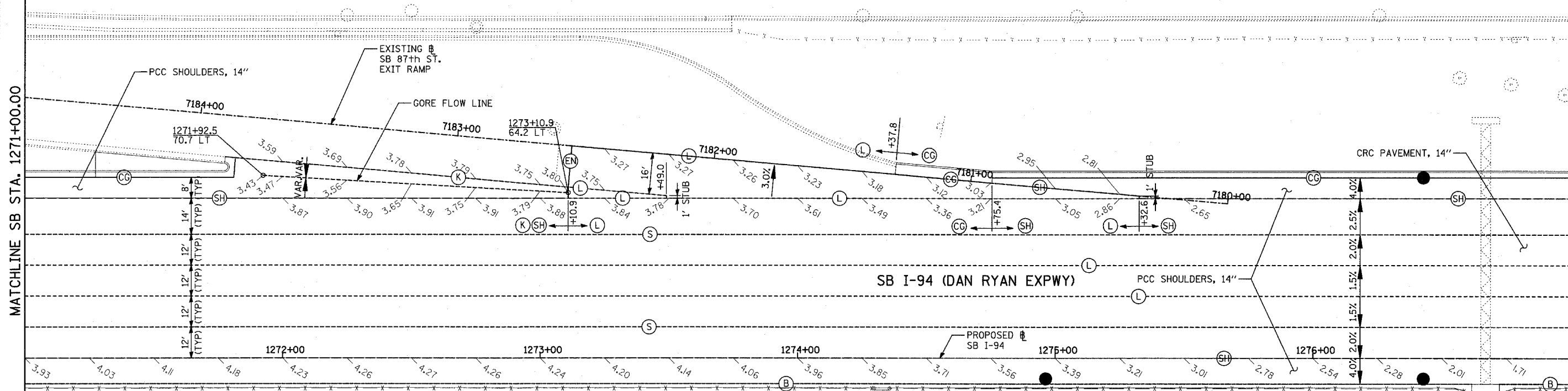
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1259+00.00 TO 1271+00.00
 SHEET 14 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

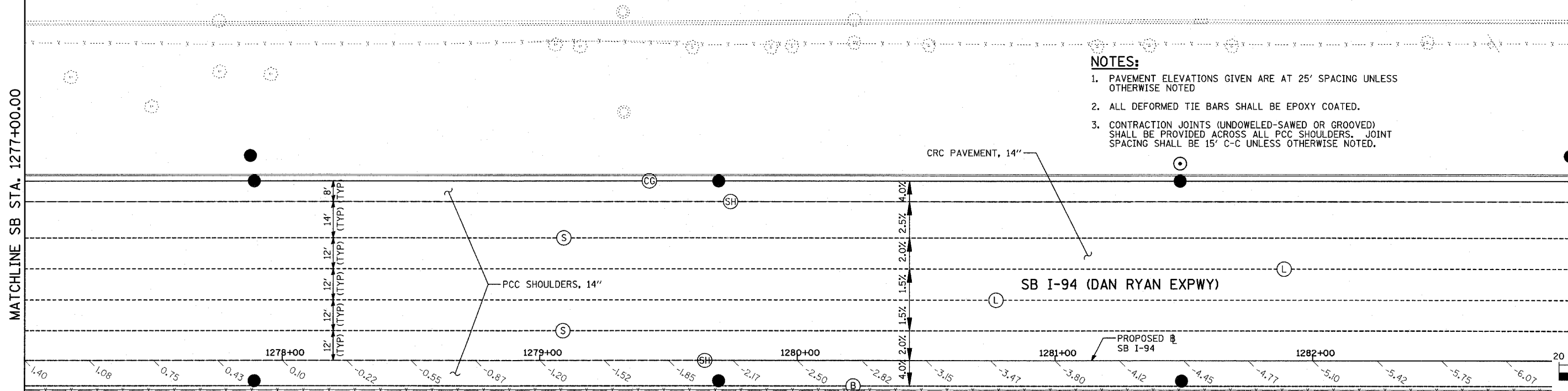
TYLIN INTERNATIONAL



S. LAFAYETTE AVENUE

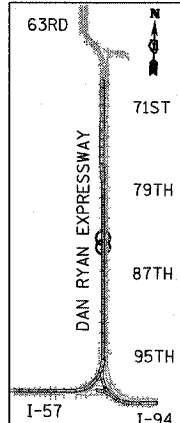


S. LAFAYETTE AVENUE

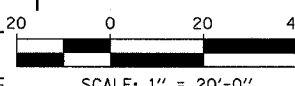


NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP



LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

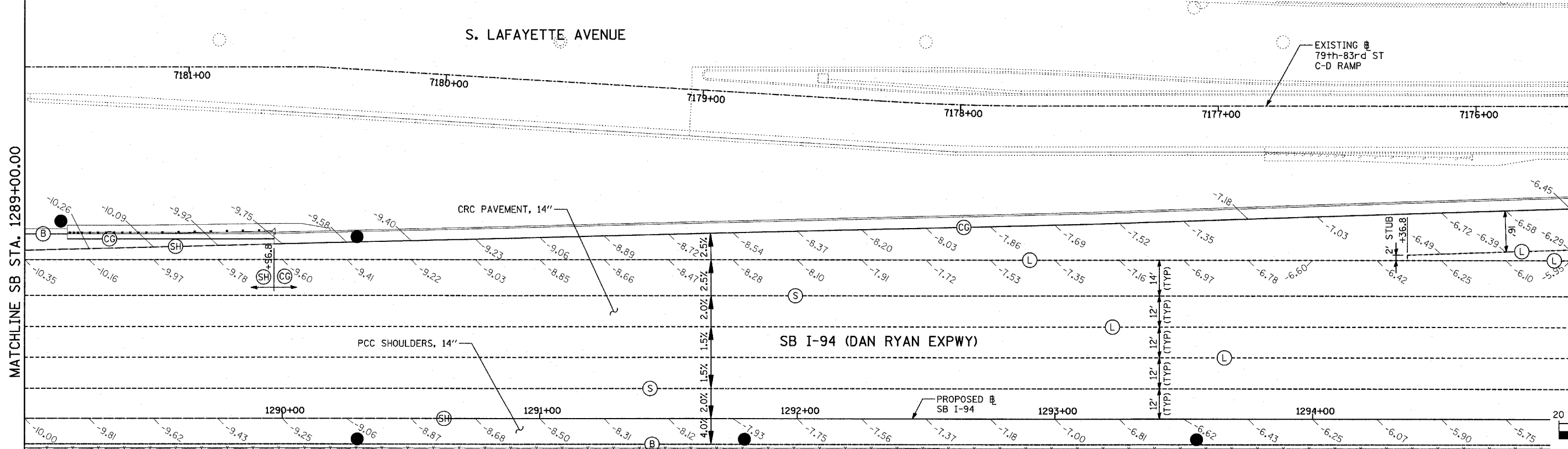
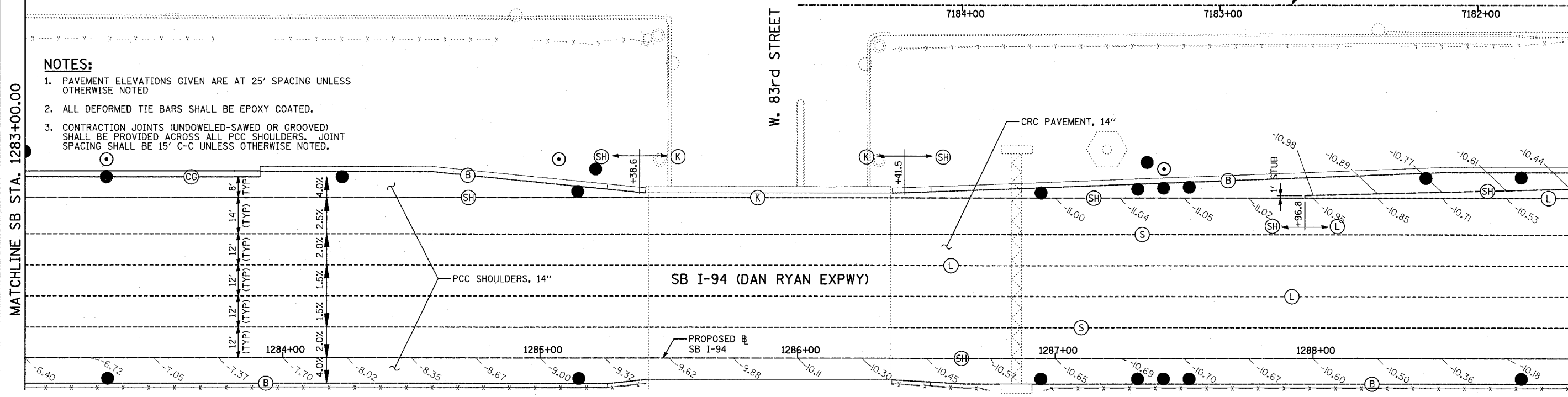
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION PLAN
SB I-94 (DAN RYAN EXPRESSWAY)
STA. 1271+00.00 TO 1283+00.00
SHEET 15 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

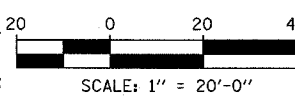
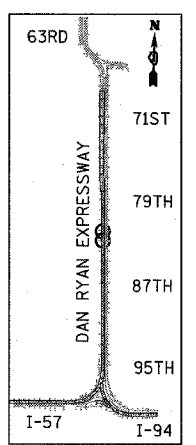
NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



MATCHLINE SB STA. 1289+00.00

MATCHLINE SB STA. 1295+00.00



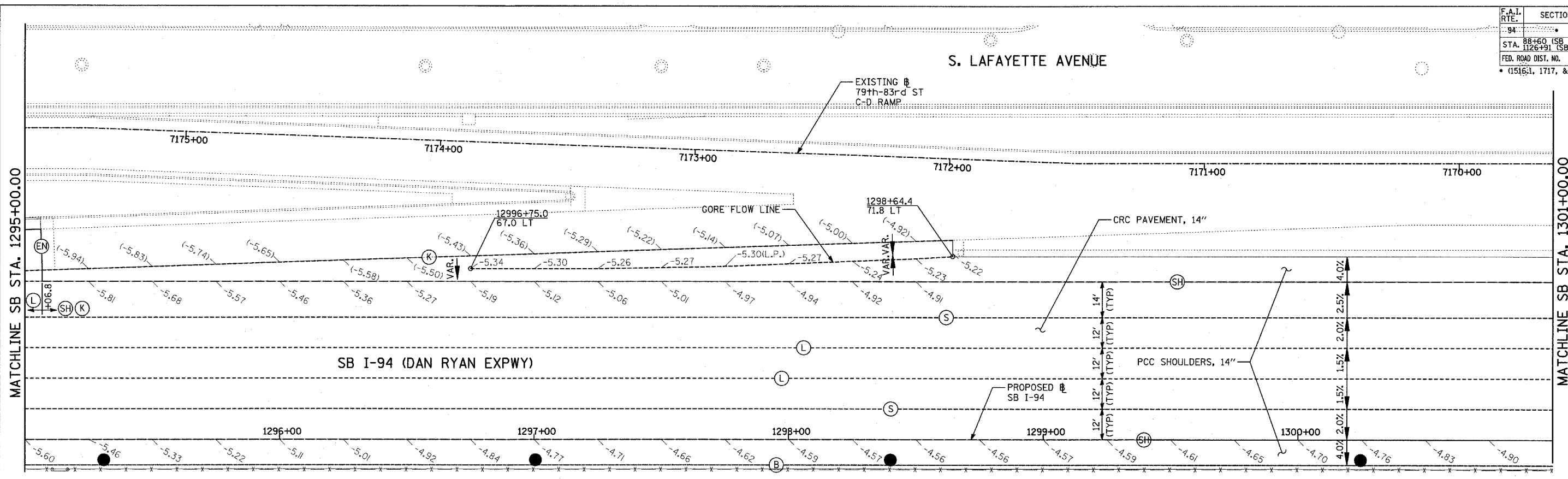
- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
 - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
 - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1283+00.00 TO 1295+00.00
 SHEET 16 OF 23**

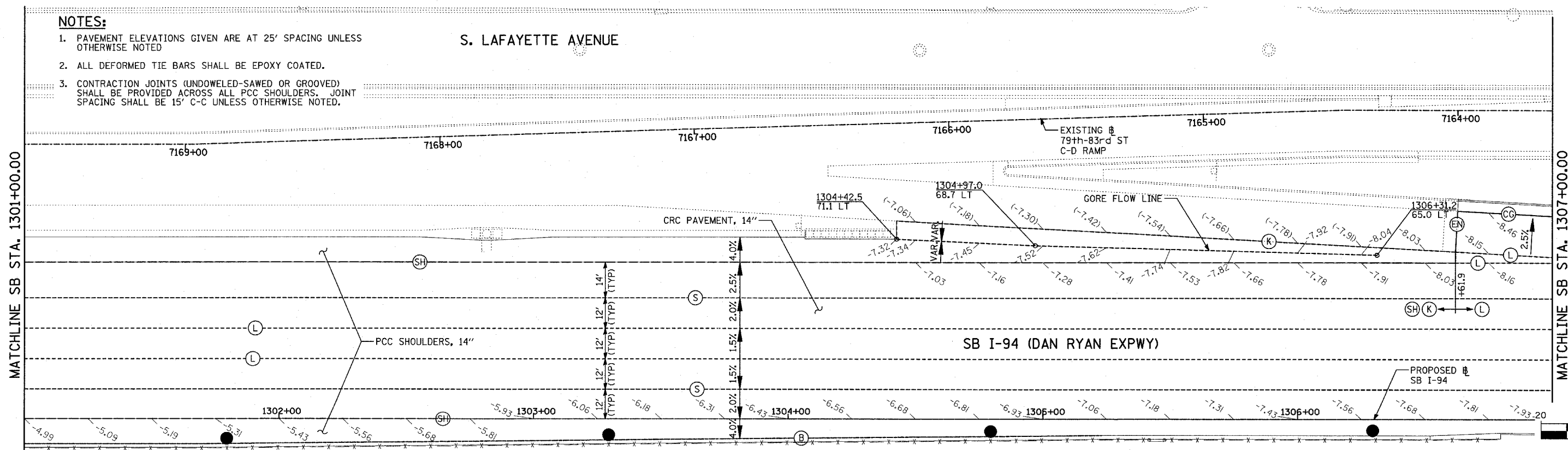
SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



NOTES:

- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
- ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
- CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



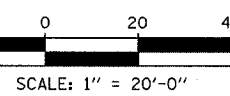
LEGEND:

- | | | |
|--|---|--|
| (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. | (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) | (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) | (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) | (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). |
| | | (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |

MATCHLINE SB STA. 1301+00.00

MATCHLINE SB STA. 1307+00.00

LOCATION MAP



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
**F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1295+00.00 TO 1307+00.00
 SHEET 17 OF 23**

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

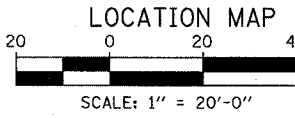
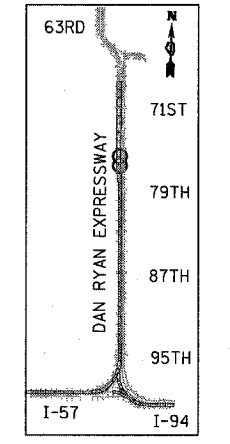
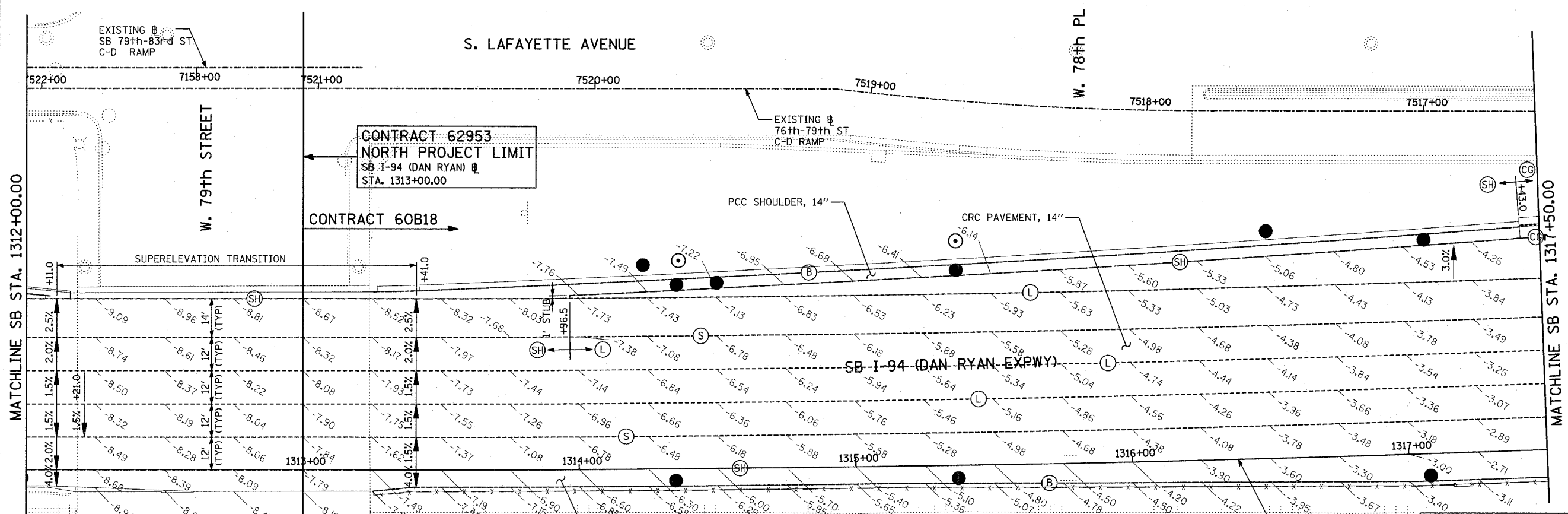
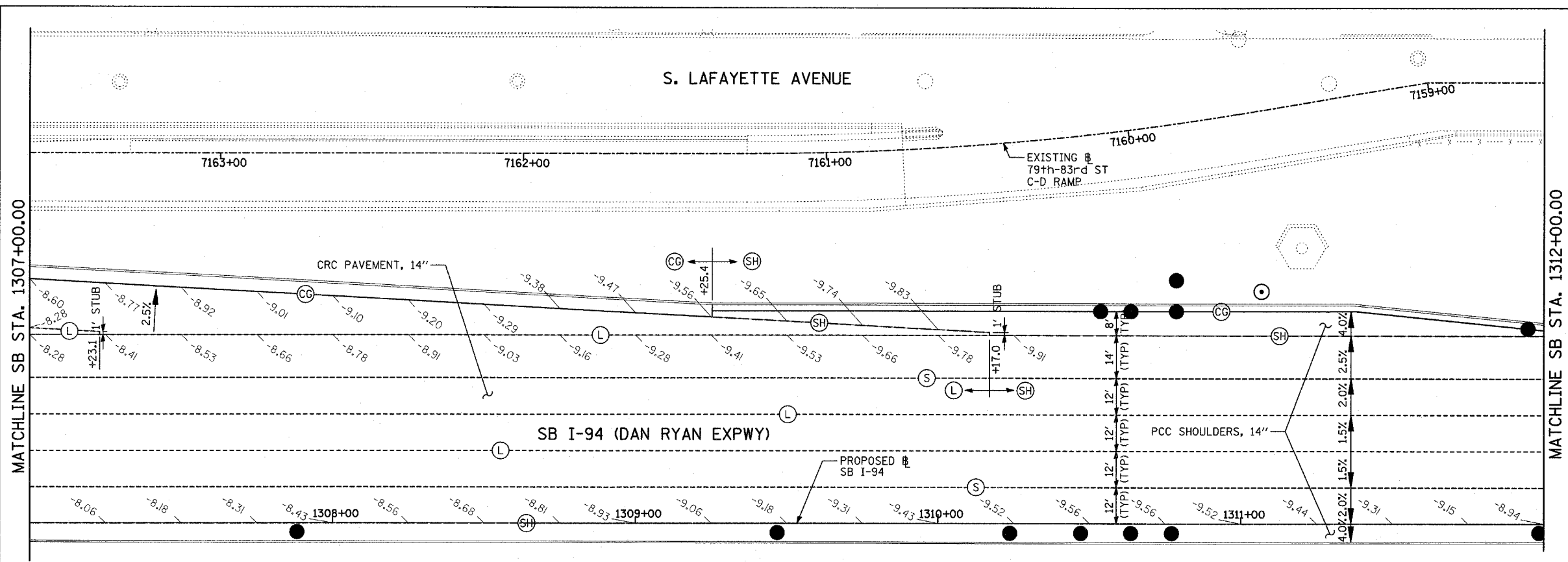
TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	169
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
• (1516.1, 1717, & 1818) R-5 62593				



LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



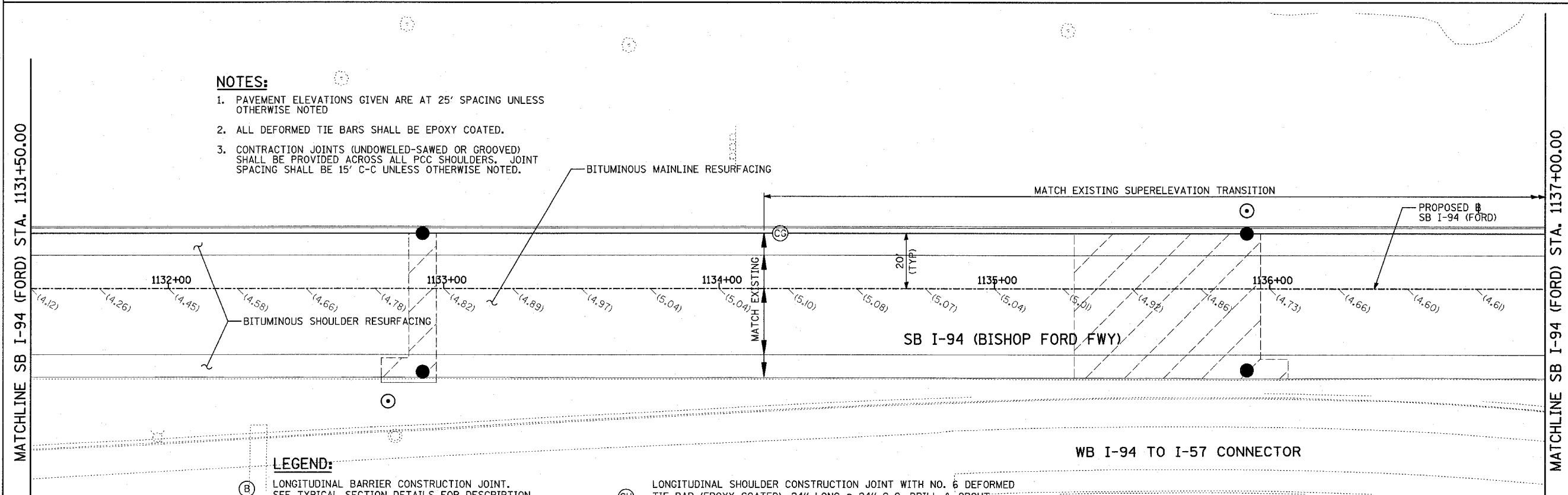
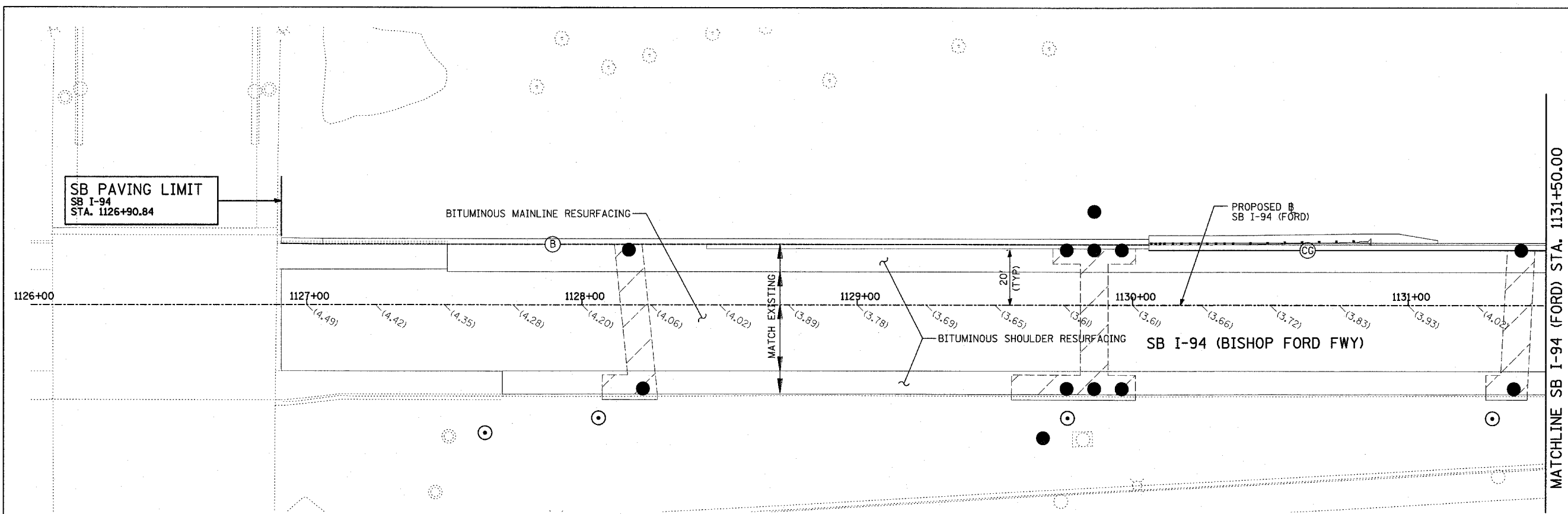
- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

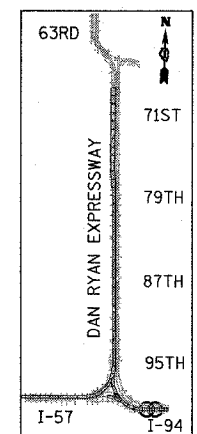
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (DAN RYAN EXPRESSWAY)
 STA. 1307+00.00 TO 1317+50.00
 SHEET 18 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

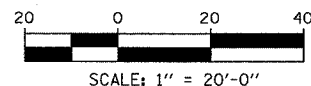
TYLIN INTERNATIONAL



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LOCATION MAP



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)

- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)

- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

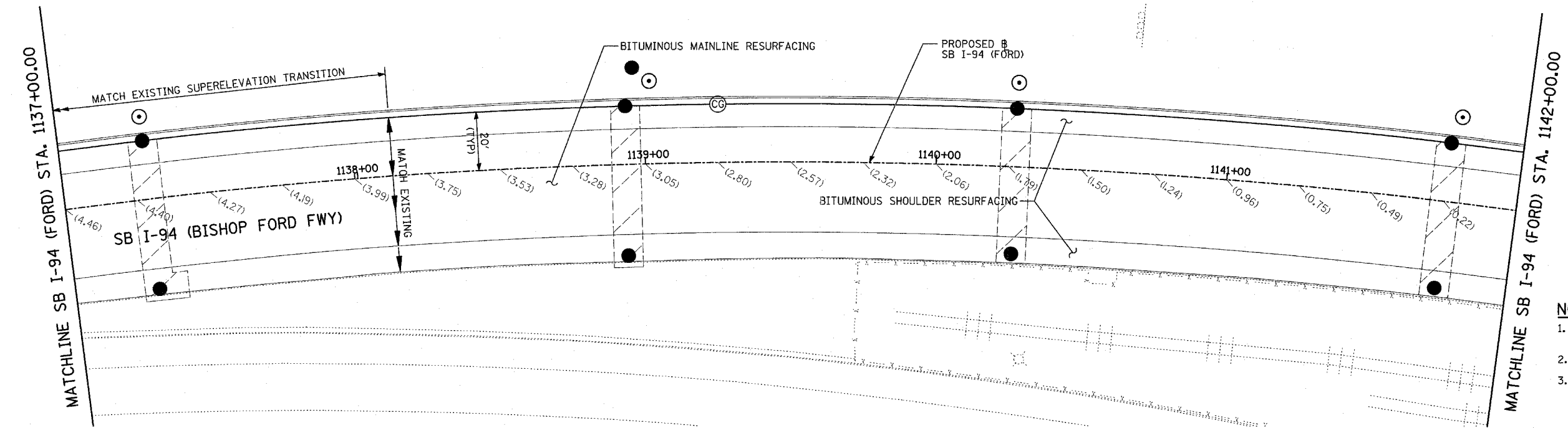
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
PAVEMENT JOINTING AND ELEVATION
SB I-94 (BISHOP FORD FWY)
STA. 1126+90.84 TO 1137+00.00
SHEET 19 OF 23

SCALE: 1"=20'
DATE: MARCH 1, 2006

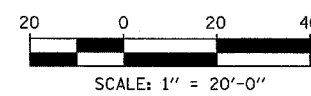
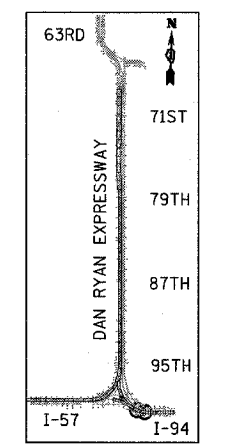
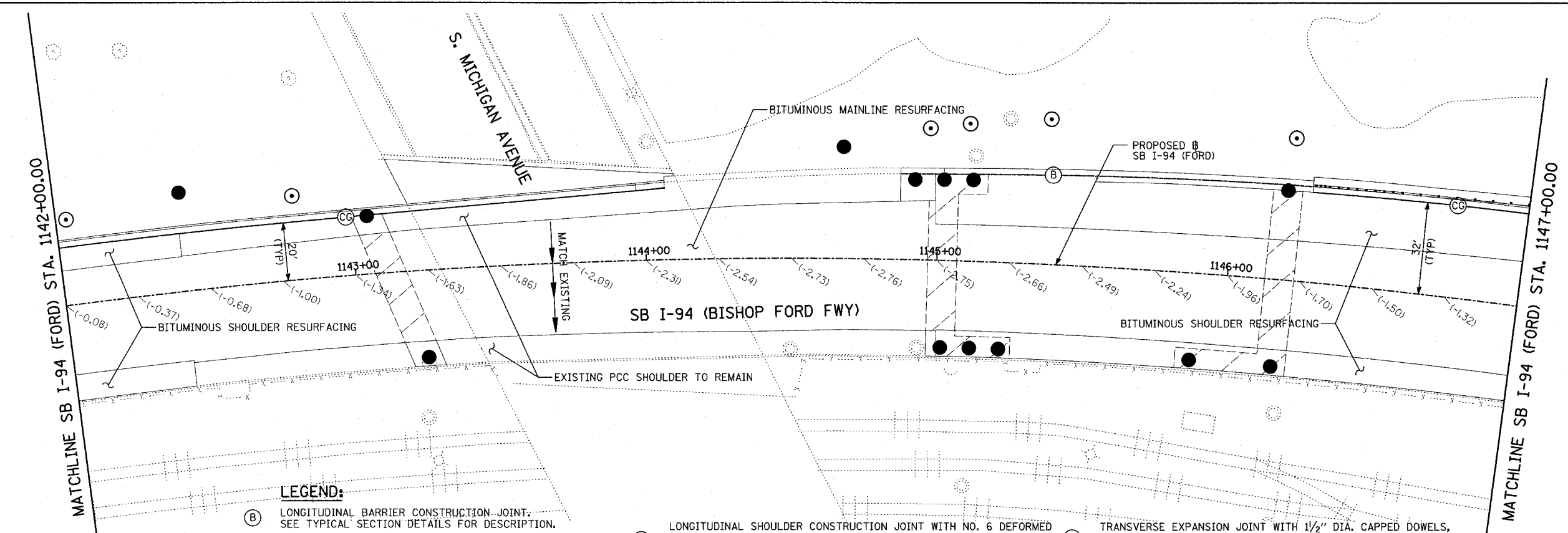
DRAWN BY: JJS
CHECKED BY: MPG

TYLIN INTERNATIONAL

WB I-94 TO I-57 CONNECTOR



- NOTES:**
- PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 - ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 - CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



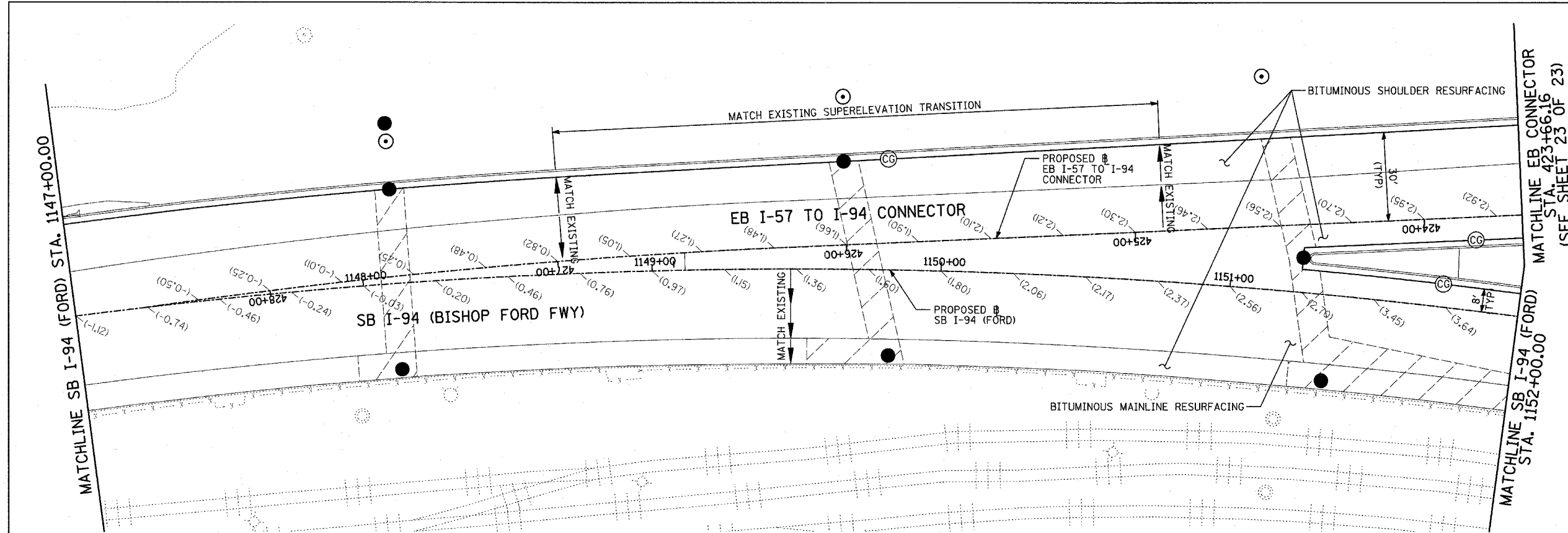
- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
 - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
 - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION
 SB I-94 (BISHOP FORD FWY)
 STA. 1137+00.00 TO 1147+00.00
 SHEET 20 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JJS
 CHECKED BY: MPG

TYLIN INTERNATIONAL



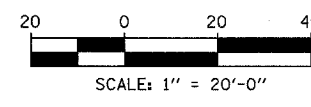
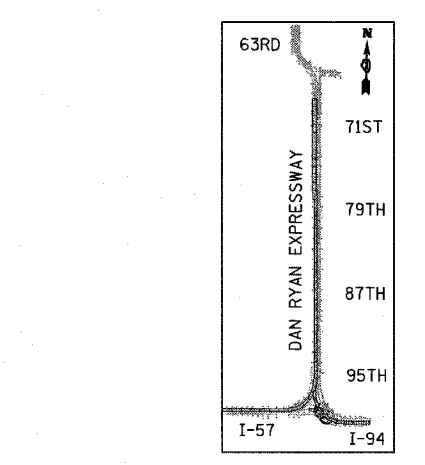
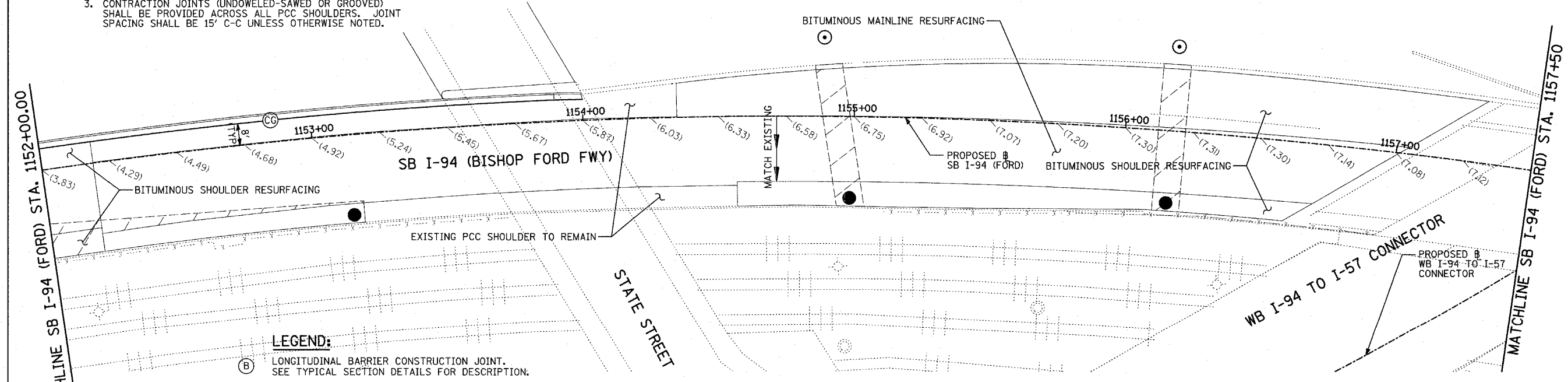
MATCHLINE SB I-94 (FORD) STA. 1147+00.00

MATCHLINE EB CONNECTOR STA. 423+66.16 (SEE SHEET 23 OF 23)

MATCHLINE SB I-94 (FORD) STA. 1152+00.00

NOTES:

1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



LEGEND:

- | | | | | | |
|------|--|------|--|------|--|
| (B) | LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION. | (SH) | LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT) | (E) | TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (CG) | CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER) | (L) | LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT) | (K) | LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS). |
| (S) | SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT) | (TC) | TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | (EN) | TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT) |
| (T) | TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT) | | | | |

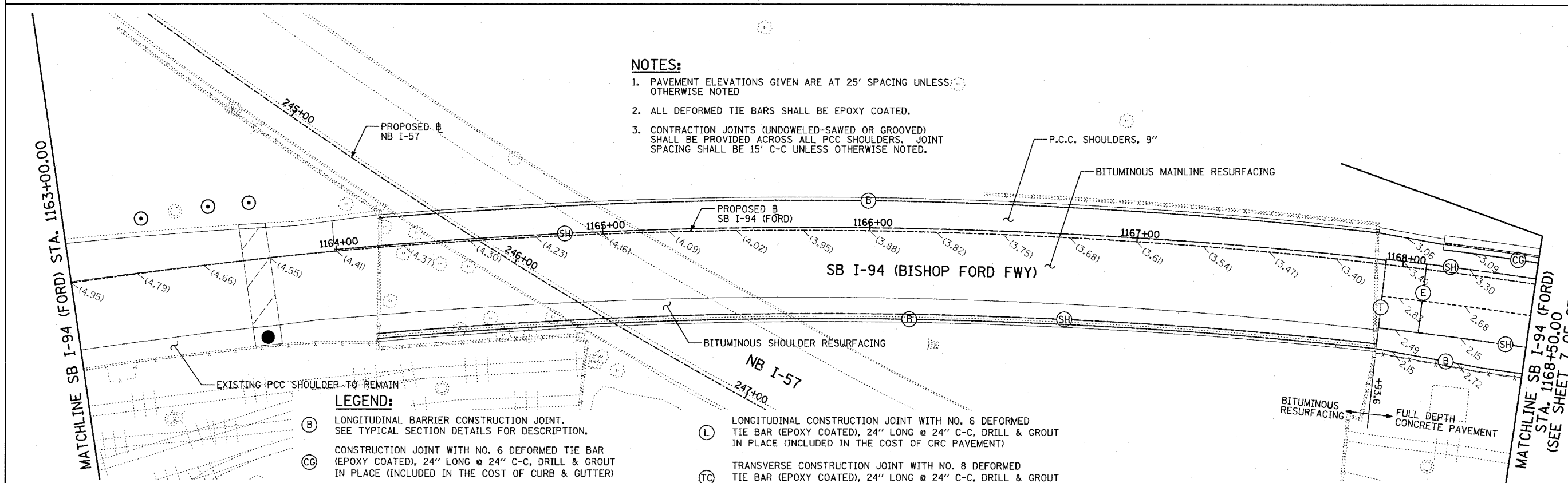
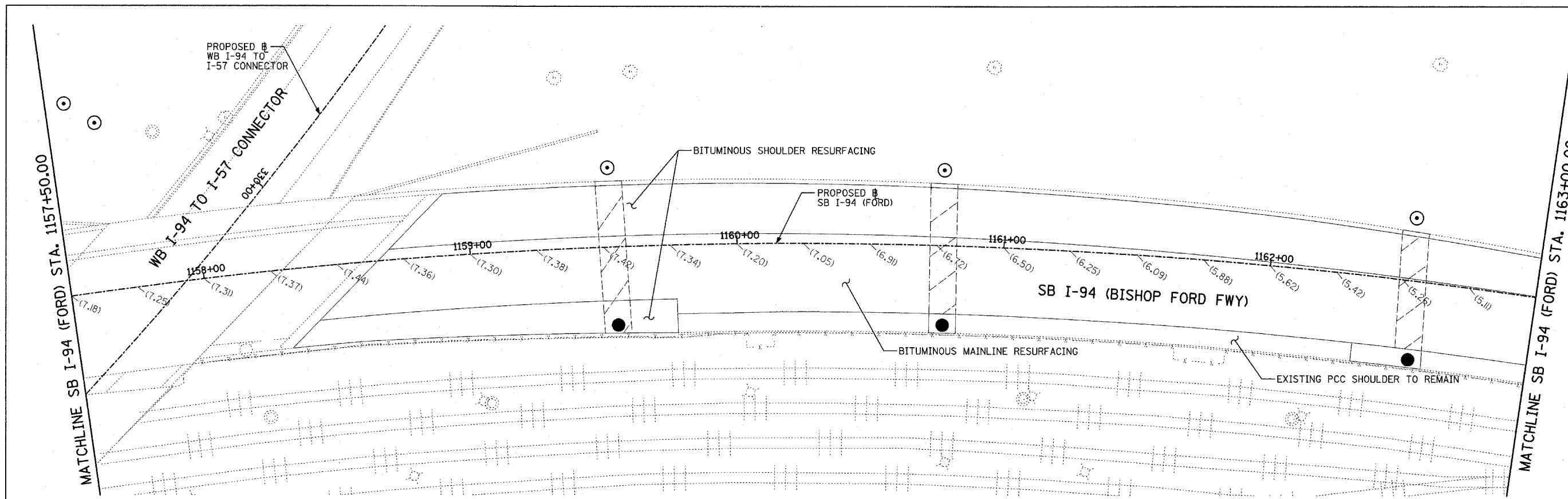
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION
 SB I-94 (BISHOP FORD Fwy)
 STA. 1147+00.00 TO 1157+50.00
 SHEET 21 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JUS
 CHECKED BY: MPG

TYLIN INTERNATIONAL

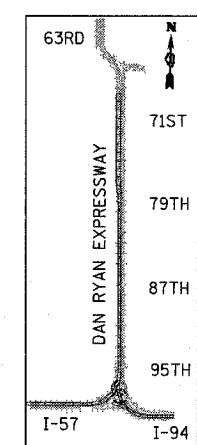
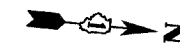
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	173
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5 62593				



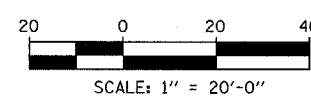
- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.

LEGEND:

- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
- (CG) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
- (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
- (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
- (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
- (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
- (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
- (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



LOCATION MAP

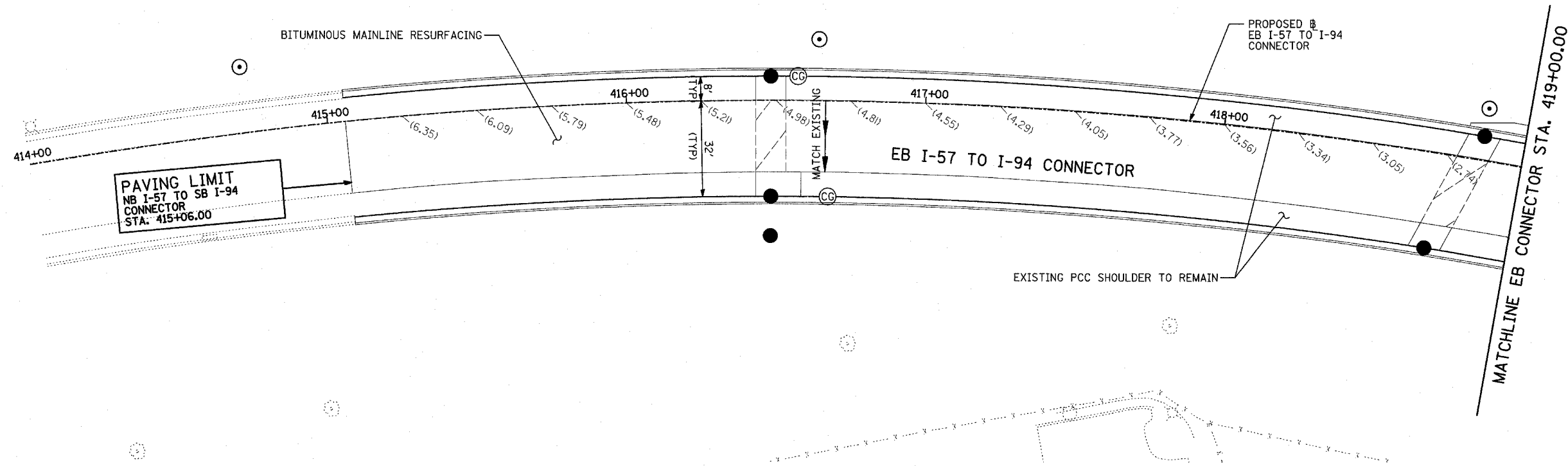


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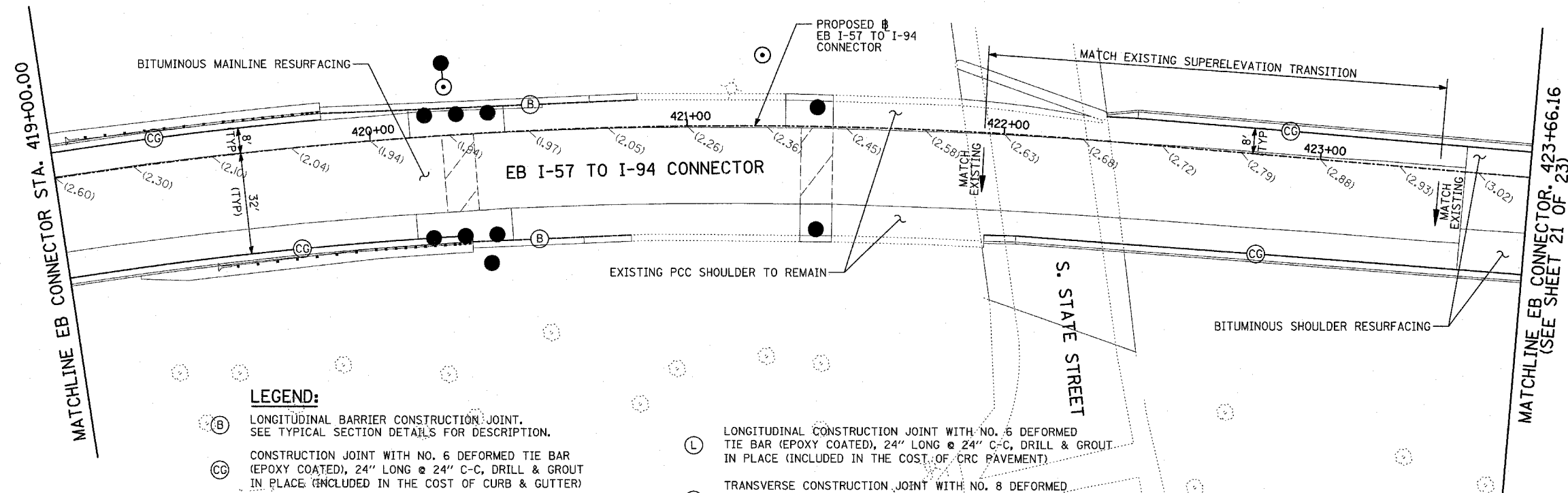
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION PLAN
 SB I-94 (BISHOP FORD FWY)
 STA. 1157+50.00 TO 1168+50.00
 SHEET 22 OF 23

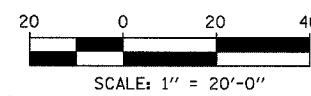
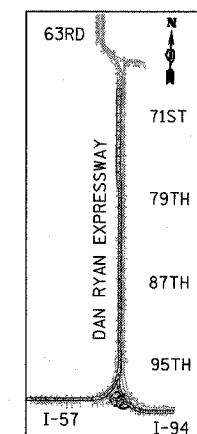
SCALE: 1"=20'
 DATE: MARCH 1, 2006
 DRAWN BY: JUS
 CHECKED BY: MPG



- NOTES:**
1. PAVEMENT ELEVATIONS GIVEN ARE AT 25' SPACING UNLESS OTHERWISE NOTED
 2. ALL DEFORMED TIE BARS SHALL BE EPOXY COATED.
 3. CONTRACTION JOINTS (UNDOWELED-SAWED OR GROOVED) SHALL BE PROVIDED ACROSS ALL PCC SHOULDERS. JOINT SPACING SHALL BE 15' C-C UNLESS OTHERWISE NOTED.



- LEGEND:**
- (B) LONGITUDINAL BARRIER CONSTRUCTION JOINT. SEE TYPICAL SECTION DETAILS FOR DESCRIPTION.
 - (CC) CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CURB & GUTTER)
 - (S) SAWED LONGITUDINAL JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 30" LONG @ 30" C-C (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (T) TRANSVERSE CONSTRUCTION JOINT WITH 1-1/2" DIA. DEFORMED TIE BARS (EPOXY COATED), 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC PAVEMENT)
 - (SH) LONGITUDINAL SHOULDER CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF PCC AND CRC PAVEMENT)
 - (L) LONGITUDINAL CONSTRUCTION JOINT WITH NO. 6 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (TC) TRANSVERSE CONSTRUCTION JOINT WITH NO. 8 DEFORMED TIE BAR (EPOXY COATED), 24" LONG @ 24" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (E) TRANSVERSE EXPANSION JOINT WITH 1/2" DIA. CAPPED DOWELS, 18" LONG @ 12" C-C, DRILL & GROUT IN PLACE (INCLUDED IN THE COST OF CRC PAVEMENT)
 - (K) LONGITUDINAL KEYED JOINT WITHOUT TIE BARS (INCLUDED IN THE COST OF CRC PAVEMENT OR PCC SHOULDERS).
 - (EN) TRANSVERSE EXPANSION JOINT (NO DOWELS INCLUDED) (INCLUDED IN THE COST OF CRC PAVEMENT)



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 PAVEMENT JOINTING AND ELEVATION
 EB I-57 TO I-94 CONNECTOR
 STA. 415+06.00 TO 423+66.16
 SHEET 23 OF 23

SCALE: 1"=20'
 DATE: MARCH 1, 2006

DRAWN BY: JJS
 CHECKED BY: MPG

GENERAL NOTES:

SEE SPECIAL PROVISION TITLED TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)
 A WORK ZONE SPEED LIMIT OF 45 M.P.H. MUST BE MAINTAINED AT ALL TIMES.
 QUANTITY FOR TEMPORARY PAVEMENT MARKING - LINE 6" WAS ASSUMED EQUAL TO THE LENGTH OF TEMPORARY CONCRETE BARRIER. UTILIZED WHEN THE BARRIER IS LOCATED ONE FOOT OR LESS FROM THE EDGE OF TRAVELED WAY.
 ALL TEMPORARY PAVEMENT MARKINGS PLACED DURING THE WINTER LANE CONFIGURATION OR ON EXISTING PAVEMENT SHALL BE PAID FOR AS EPOXY PAVEMENT MARKING OF THE LINE TYPE AND WIDTH SPECIFIED.
 ALL TEMPORARY PAVEMENT MARKINGS PLACED ON NEW PAVEMENT SHALL BE PAID FOR AS PAVEMENT MARKING TAPE TYPE III OF THE LINE TYPE AND WIDTH SPECIFIED EXCEPT WHEN DIRECTED OTHERWISE.
 A TOTAL OF 10 CHANGEABLE MESSAGE SIGNS SHALL BE LOCATED ALONG THE MAINLINE AND NEAR RAMP CLOSURES FOR THE DURATION OF THIS CONTRACT. EXACT PLACEMENT OF THE SIGNS SHALL BE DETERMINED BY THE RESIDENT ENGINEER.
 ANY RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH THE TEMPORARY TRAFFIC LANES MUST HAVE THE REFLECTIVE LENSES REMOVED AS DIRECTED BY THE RESIDENT ENGINEER. MEASURE OF PAYMENT FOR REMOVAL OF REFLECTORS IS INCLUDED IN THE CONTRACT UNIT PRICE FOR TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).

FOR ACCESS CONTROL, THE CONTRACTOR IS REQUIRED TO PLACE CHAIN LINK FENCE 6' (SPECIAL), 7"-2" CLEAR FROM THE CENTERLINE OF CTA TRACK, PRIOR TO REMOVING EXISTING CTA CHAIN LINK FENCE AND BARRIER WALL. THE CONTRACTOR MUST ALSO PLACE CHAIN LINK GATES, 6' X 6' SINGLE AT ALL LOCATIONS WHERE THERE ARE EXISTING CTA FENCE GATES. THE CHAIN LINK FENCE 6' (SPECIAL) MUST REMAIN IN PLACE UNTIL THE NEW PERMANENT CTA FENCE IS COMPLETED.
 SEE MISCELLANEOUS DETAILS FOR MAINLINE TEMPORARY PAVEMENT AND MILLING OPERATIONS FOR THE POST-STAGE 1 WINTER LANE CONFIGURATION.

THE RESIDENT ENGINEER SHALL ASSESS THE EXISTING CONDITION OF THE PAVEMENT LOCATED ALONG THE INSIDE SHOULDER. DAMAGED AREAS WILL REQUIRE SHOULDER REHABILITATION INCLUDING MILLING AND RESURFACING. ADDITIONAL QUANTITIES OF 325.0 SQ YD FOR BITUMINOUS SURFACE REMOVAL 1/2" AND 28.0 TONS FOR BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX "D", N70 HAVE BEEN INCLUDED IN THE PLANS.

CONSTRUCTION STAGING NOTES

PRE STAGE 1 & 1A - NIGHT TIME OPERATIONS

INSTALL RAMP CLOSURE ADVANCE INFORMATION SIGNS ON STAGE 1 RAMP CLOSURES A MINIMUM OF 2 WEEKS IN ADVANCE OF THE CLOSURE. SEE STAGE 1 RAMP CLOSURE GUIDELINES IN THE SPECIAL PROVISIONS.

COORDINATE 3 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #60B18 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

PERFORM MEDIAN REMOVAL PARTIAL DEPTH OPERATIONS FROM SB I-57 STA. 129+25 TO STA. 131+77.

UTILIZE DISTRICT 1 DETAIL TC-17 TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES TO INSTALL TEMPORARY PAVEMENT WIDENING AND AGGREGATE SHOULDER FROM SB I-57 STA. 131+78 TO STA. 146+45.

COMPLETE SHOULDER REHABILITATION FOR AREAS OF DAMAGED PAVEMENT PRIOR TO SHIFTING TRAFFIC.

REMOVE EXISTING PAVEMENT MARKINGS AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

REMOVE LENSES FROM RAISED REFLECTIVE PAVEMENT MARKERS.

INSTALL TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGE 1 & 1A CONSTRUCTION.

COVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

STAGE 1 & 1A CONSTRUCTION (AUGUST 1ST, 2006 - NOVEMBER 30, 2006)

STAGE 1 CONSTRUCTS LANES 4, 5, & THE OUTSIDE SHOULDER ALONG THE MAINLINE SB I-94 (DAN RYAN), LANES 3, 4, & THE OUTSIDE SHOULDER ALONG SB I-57, AND RAMP TERMINALS ALONG BOTH SB I-94 AND SB I-57 INCLUDING ALL ADDITIONAL WORK WITHIN THE PROPOSED LIMITS.

STAGE 1 ALSO COMPLETES ALL JACKED IN PLACE STORM SEWERS. ADDITIONAL STAGING COORDINATION WITH CONTRACT #62304 MAY BE REQUIRED.

ALL REPAIR WORK IN THE SB I-94 (BISHOP FORD) AND CTA TUNNELS MUST BE COMPLETED AT THE BEGINNING OF STAGE 1. SEE SPECIAL PROVISIONS FOR THE INTERIM COMPLETION DATE.

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 3-11' LANES UNTIL TRAFFIC IS SPLIT AT THE SB I-57 & SB I-94 (BISHOP FORD) JUNCTION. THE CENTER LANE WILL SPLIT INTO TWO LANES ALLOWING 2-11' LANES OF TRAFFIC TO BOTH SB I-57 & SB I-94 (BISHOP FORD). THE WB I-94 TO I-57 CONNECTOR WILL MERGE INTO THE SB I-57 TRAFFIC.

DURING STAGE 1, CONSTRUCTION WORK SHALL BE PROHIBITED BETWEEN SB I-57 STA. 119+18 AND STA. 141+46. THIS AREA IS REQUIRED TO CREATE A SAFER MERGE BETWEEN SB I-57 & THE WB I-94 TO SB I-57 CONNECTOR DUE TO THE TRAFFIC CONFIGURATION IN CONTRACT #62304.

STAGE 1A IS REQUIRED TO CONSTRUCT LANES 3, 4, & THE OUTSIDE SHOULDER FROM SB I-57 STA. 119+18 TO STA. 141+46. THIS STAGE WILL COINCIDE WITH STAGE 1 WHILE THE REMAINING STAGE 1 WORK IS COMPLETED AND MUST BE COORDINATED WITH CONTRACT #62304.

PRE STAGE WINTER LANE CONFIGURATION - NIGHT TIME OPERATIONS

PERFORM TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS ALONG THE SB I-94 & SB I-57 (DAN RYAN) PRIOR TO THE START OF THE WINTER LANE CONFIGURATION.

COORDINATE 4 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #60B18 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGES 1 & 1A AND REPLACE WITH EPOXY PAVEMENT MARKINGS.

RELOCATE TEMPORARY CONCRETE BARRIER WALL TO CREATE ADEQUATE SHOULDERS FOR SNOW STORAGE AND RELOCATE REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR THE WINTER LANE CONFIGURATION.

INSTALL IMPACT ATTENUATOR, TEMPORARY (SEVERE USE, NARROW) AND REQUIRED TEMPORARY CONCRETE BARRIER TO PROTECT BLUNT END OF PERMANENT BARRIER WALL AT SB I-57 STA. 92+88.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

WINTER LANE CONFIGURATION (DECEMBER 1, 2006 - APRIL 1, 2007)

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED LEFT TO THE INSIDE SHOULDER IN 4-11' LANES UNTIL TRAFFIC IS SPLIT AT THE SB I-57 & SB I-94 (BISHOP FORD) JUNCTION. THE RIGHT 2-11' LANES WILL WEAVE RIGHT TO SB I-57 WITH THE LEFT 2-11' LANES HEADING TO SB I-94 (BISHOP FORD). THE WB I-94 TO I-57 CONNECTOR WILL ADD A THIRD 11' LANE ALONG SB I-57.

PRE STAGE 2 & 2A - NIGHT TIME OPERATIONS

COORDINATE 3 LANES OF SPLIT TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #60B18 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT EB I-57 TO I-94 CONNECTOR STA. 415+06 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM THE WINTER LANE CONFIGURATION AND REPLACE WITH EPOXY OR PAVEMENT MARKING TAPE DEPENDING IF THE PAVEMENT IS EXISTING, NEWLY BUILT OR UNLESS DIRECTED OTHERWISE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGES 2 & 2A.

PERFORM TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS PRIOR TO THE START OF STAGE 2A FROM DAN RYAN STA. 1260+00 TO 1270+00.

IMPLEMENT 87TH STREET ENTRANCE RAMP TEMPORARY DETOUR FOR TRAFFIC REQUIRING ACCESS TO SB I-94 (BISHOP FORD).

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

STAGE 2 & 2A CONSTRUCTION (APRIL 2, 2007 - MAY 31, 2007)

STAGE 2 CONSTRUCTS LANE 3 ALONG THE MAINLINE SB I-94 (DAN RYAN) FROM STA. 1216+60 TO STA. 1313+00, PROPOSED STORM SEWER, AND CURB & GUTTER WITHIN THE SOUTH PORTION OF SB I-94 (BISHOP FORD) STA. 1143+00 & STA. 1151+00.

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SPLIT INTO 2-11' EXPRESS LANES SHIFTED LEFT TO THE INSIDE SHOULDER AND 1-11' LOCAL LANE SHIFTED RIGHT TO THE OUTSIDE SHOULDER UNTIL STA. 1216+60. A SLIP RAMP WILL BE PLACED BETWEEN STA. 1270+00 AND STA. 1280+00 TO ALLOW TRAFFIC FROM THE LOCAL LANES ACCESS TO THE EXPRESS LANES AND SB I-94 (BISHOP FORD). AN EXIT RAMP FROM THE EXPRESS LANES TO SB I-57 WILL ADD A SECOND LOCAL LANE AT SB I-57 STA. 157+33. THE WB I-94 TO I-57 CONNECTOR WILL ADD A THIRD LANE ALONG SB I-57. THE EB I-57 TO I-94 CONNECTOR WILL ADD AN ADDITIONAL LANE ALONG THE SB I-94 (BISHOP FORD) TRAFFIC.

THE CONTRACTOR MUST FIRST COMPLETE LANE 3 BETWEEN SB I-94 (DAN RYAN) STA. 1260+00 AND STA. 1270+00 IN ORDER TO IMPLEMENT STAGE 2A.

STAGE 2A IS REQUIRED TO CONSTRUCT LANE 3 BETWEEN SB I-94 (DAN RYAN) STA. 1270+00 AND STA. 1280+00. THE SLIP RAMP FROM THE LOCAL LANE TO EXPRESS LANES WILL BE MOVED BETWEEN SB I-94 (DAN RYAN) STA. 1260+00 AND STA. 1270+00. THIS STAGE WILL COINCIDE WITH STAGE 2 WHILE THE REMAINING STAGE 2 WORK IS COMPLETED.

PRE STAGE 3, 3A, 3B, 3C, & 3D - NIGHT TIME OPERATIONS

COORDINATE 3 LANES OF TRAFFIC AT SB I-94 (DAN RYAN) STA. 1313+00 WITH CONTRACT #60B18 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT WB I-94 TO I-57 CONNECTOR STA. 340+29 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

COORDINATE SINGLE LANE OF TRAFFIC AT EB I-57 TO I-94 CONNECTOR STA. 415+06 WITH CONTRACT #62304 IN ORDER TO MAINTAIN TRAFFIC FLOW.

PERFORM TEMPORARY PAVEMENT AND COLD MILLING OPERATIONS FROM DAN RYAN STA. 1216+60 TO STA. 1221+60.

REMOVE CONFLICTING TEMPORARY PAVEMENT MARKINGS FROM STAGE 2 AND REPLACE WITH EPOXY OR PAVEMENT MARKING TAPE DEPENDING IF THE PAVEMENT IS EXISTING, NEWLY CONSTRUCTED, OR UNLESS DIRECTED OTHERWISE.

RELOCATE TEMPORARY CONCRETE BARRIER WALL AND REMAINING TEMPORARY TRAFFIC CONTROL DEVICES FOR STAGES 3, 3A, 3B, 3C, & 3D.

COVER & UNCOVER PERMANENT AND TEMPORARY SIGNS AS NEEDED.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	175
STA. 88+60 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516, 1717, & 1818) R-5				

62593

STAGE 3 CONSTRUCTION (JUNE 1, 2007 - OCTOBER 31, 2007)

(NORTH OF SB I-94 (DAN RYAN) STA. 1221+60)

STAGE 3 CONSTRUCTS LANES 1, 2, & THE INSIDE SHOULDER ALONG THE MAINLINE SB I-94 (DAN RYAN) FROM STA. 1221+60 TO STA. 1313+00.

MAINLINE SB I-94 (DAN RYAN) TRAFFIC WILL BE SHIFTED RIGHT TO THE OUTSIDE SHOULDER IN 3-11' LANES UNTIL TRAFFIC IS SPLIT AT THE SB I-57 & SB I-94 (BISHOP FORD) JUNCTION. THE RIGHT 2-11' LANES WILL CONTINUE ALONG SB I-57 WITH THE LEFT 1-11' LANE CONTINUING ALONG THE SB I-94 (BISHOP FORD).

STAGE 3A, 3B, 3C, & 3D CONSTRUCTION (JUNE 1, 2007 - OCTOBER 31, 2007)

(SOUTH OF SB I-94 (DAN RYAN) STA. 1221+60)

STAGES 3A, 3B, 3C, & 3D CONSTRUCT LANES 1, 2, & THE INSIDE SHOULDER ALONG SB I-57, LANE 3 TO SB I-94 (DAN RYAN) STA. 1216+60, AND THE REMAINING PROPOSED STORM SEWER, CURB & GUTTER, SHOULDERS, TUNNEL WORK, & RESURFACING OPERATIONS ALONG SB I-94 (BISHOP FORD) AND THE EB I-57 TO I-94 CONNECTOR.

IN STAGE 3A, MAINLINE I-94 (DAN RYAN) TRAFFIC WILL BE SPLIT FOR THE SB I-57 & SB I-94 (BISHOP FORD) JUNCTION AT STA. 1221+60. THE WB I-94 TO I-57 CONNECTOR WILL ADD A THIRD LANE ALONG SB I-57, BUT WILL BE PROHIBITED FROM EXITING AT HALSTED STREET. THE EB I-57 TO I-94 CONNECTOR WILL ADD AN ADDITIONAL LANE ALONG THE SB I-94 (BISHOP FORD) TRAFFIC. THIS STAGE WILL COINCIDE WITH STAGE 3 WHILE THE REMAINING WORK IS COMPLETED AND MUST BE COORDINATED WITH CONTRACT #62304.

THE CONTRACTOR MUST FIRST COMPLETE THE REMAINING WORK FROM LANE 2 AT SB I-57 STA. 121+40 EXTENDING TO LANE 3 AT SB I-94 (DAN RYAN) STA. 1216+60, PORTIONS OF STORM SEWER, CURB AND GUTTER, AND SHOULDER ALONG THE INSIDE OF THE EB I-57 TO I-94 CONNECTOR, TEMPORARY PAVEMENT WIDENING AND PORTIONS OF STORM SEWER FROM SB I-94 (BISHOP FORD) STA. 1126+89 TO STA. 1143+16, AND THE TUNNEL WORK ALONG THE OUTSIDE OF SB I-94 (BISHOP FORD) IN ORDER TO IMPLEMENT STAGE 3B.

STAGE 3B IS REQUIRED TO CONSTRUCT LANES 1, 2, AND THE INSIDE SHOULDER WITHIN SB I-57 STA. 89+45 AND WB I-94 TO I-57 CONNECTOR STA. 340+29, ALL REMAINING STORM SEWER, CURB & GUTTER, AND SHOULDER ITEMS ALONG SB I-94 (BISHOP FORD) AND THE EB I-57 TO I-94 CONNECTOR, AND THE TUNNEL WORK ALONG THE INSIDE OF SB I-94 (BISHOP FORD). THIS STAGE WILL COINCIDE WITH STAGE 3 WHILE THE REMAINING WORK IS COMPLETED AND MUST BE COORDINATED WITH CONTRACT #62304.

IN STAGE 3B, MAINLINE I-94 (DAN RYAN) TRAFFIC WILL BE SPLIT FOR THE SB I-57 & SB I-94 (BISHOP FORD) JUNCTION AT STA. 152+92. THE WB I-94 TO I-57 CONNECTOR WILL MERGE INTO THE SB I-57 TRAFFIC. THE EB I-57 TO I-94 CONNECTOR WILL ADD AN ADDITIONAL LANE ALONG THE SB I-94 (BISHOP FORD) TRAFFIC. THE SB I-57 WILL BE IN 2-11' LANES BEGINNING AT STA. 149+91.

THE CONTRACTOR MUST COMPLETE ALL CONSTRUCTION FROM SB I-57 STA. 89+45 TO STA. 107+16 EXCEPT WORK CONSTRAINED BY THE LIMITED WORKING SPACE IN ORDER TO IMPLEMENT STAGE 3C.

THE CONTRACTOR MUST ALSO COMPLETE ALL DRAINAGE WORK ON THE SB I-94 (BISHOP FORD) AND COMPLETE RECONSTRUCTION WORK ON THE DAN RYAN FROM STA. 1205+00 TO 1221+60 PRIOR TO IMPLEMENTING STAGE 3C.

STAGE 3C IS REQUIRED TO TO GIVE THE CONTRACTOR ENOUGH WORKING SPACE TO POUR LANES 1, 2, & THE INSIDE SHOULDER BETWEEN SB I-57 STA. 89+45 AND STA. 107+16. THIS STAGE WILL COINCIDE WITH STAGES 3 & 3B WHILE THE REMAINING WORK IS COMPLETED. AT SB I-57 STA. 108+58 THE RIGHT LANE OF TRAFFIC WILL BE FORCED ONTO THE HALSTED ST. EXIT RAMP AND ONLY A SINGLE 11' LANE OF TRAFFIC WILL BE USED ALONG SB I-57.

IN STAGE 3C, MAINLINE TRAFFIC WILL BE EXPANDED TO 4-11' LANES AT SB I-94 (DAN RYAN) STA. 1215+53. THE TRAFFIC WILL SPLIT AT SB I-57 STA. 152+92. THE SB I-94 (BISHOP FORD) TRAFFIC WILL BE WIDENED TO 2-12' LANES. THE SB I-57 TRAFFIC WILL BE IN THE SAME LANE CONFIGURATION AS STAGE 3B THROUGH STA. 108+58 WHEN TRAFFIC IS REDUCED TO ONE LANE. THE EB I-57 TO I-94 TRAFFIC WILL BE WIDENED TO 1-12' LANE WHERE IT WILL ADD AN ADDITIONAL LANE TO THE SB I-94 (BISHOP FORD).

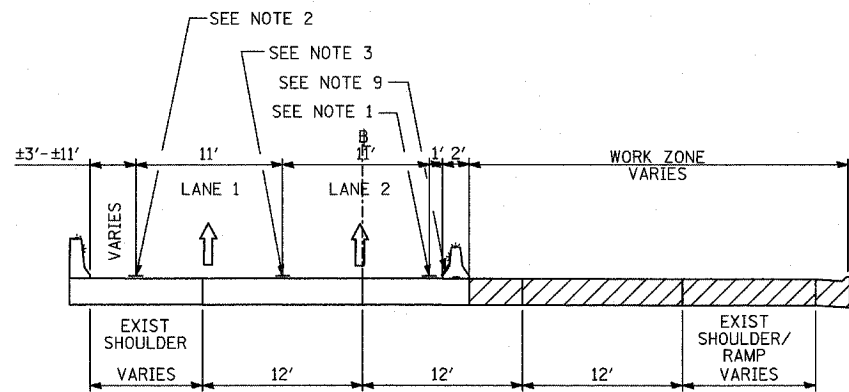
THE CONTRACTOR MUST FIRST COMPLETE LANES 1, 2, & THE INSIDE SHOULDER IN STAGE 3C FROM SB I-57 STA. 89+45 TO STA. 107+16 IN ORDER TO IMPLEMENT STAGE 3D. A 2-WEEK TIME LIMIT WILL BE APPLIED TO THIS SECTION DUE TO SEVERE TRAFFIC CONSTRAINTS.

STAGE 3D IS REQUIRED TO INCREASE SB I-57 FROM 1-11' LANE TO 2-11' LANES BETWEEN STA. 89+45 AND STA. 107+16. THIS STAGE WILL COINCIDE WITH STAGES 3, 3B, & 3C WHILE THE REMAINING WORK IS COMPLETED.

REVISIONS	
NAME	DATE

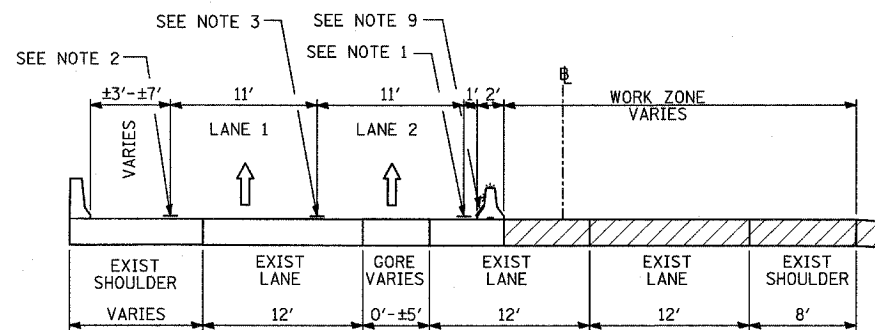
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 1 OF 12

SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 1, 2006 CHECKED BY: TGB



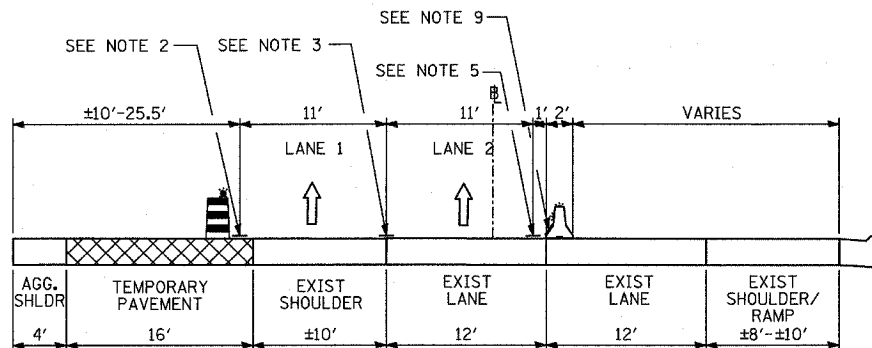
A-A

STAGE 1: SB I-57; STA. 88+55 TO STA. 119+18
 STAGE 1A: SB I-57; STA. 88+55 TO STA. 119+18



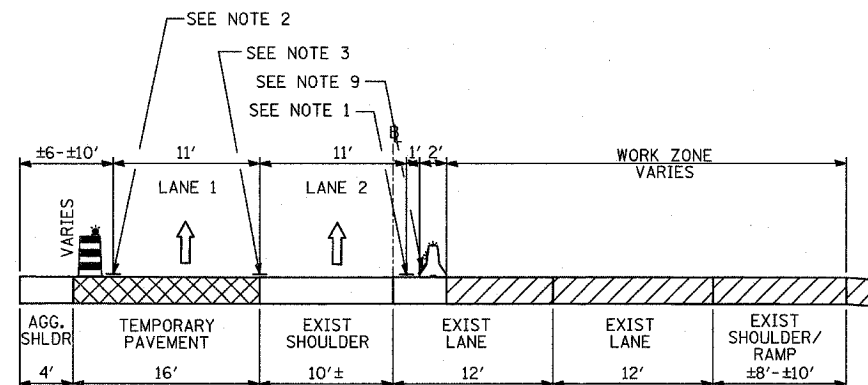
D-D

STAGE 1A: SB I-57; STA. 119+18 TO STA. 124+50



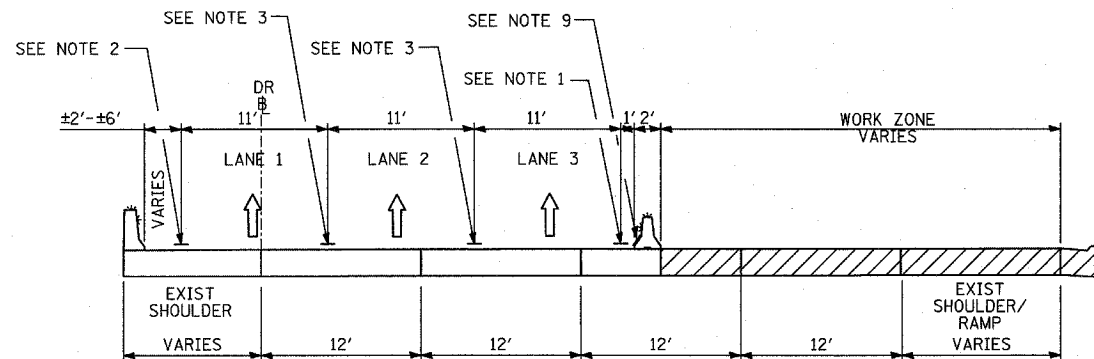
B-B

STAGE 1: SB I-57; STA. 132+04 TO STA. 141+46



E-E

STAGE 1A: SB I-57; STA. 132+04 TO STA. 141+46



C-C

STAGE 1: SB I-94 DAN RYAN; STA. 1205+33 TO STA. 1313+00

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

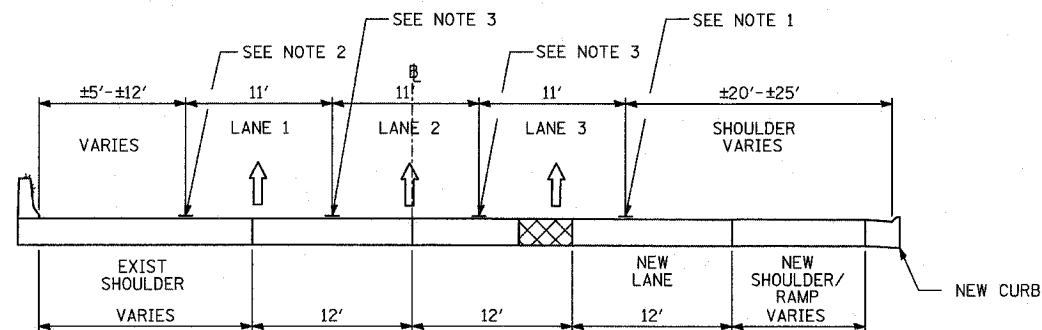
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

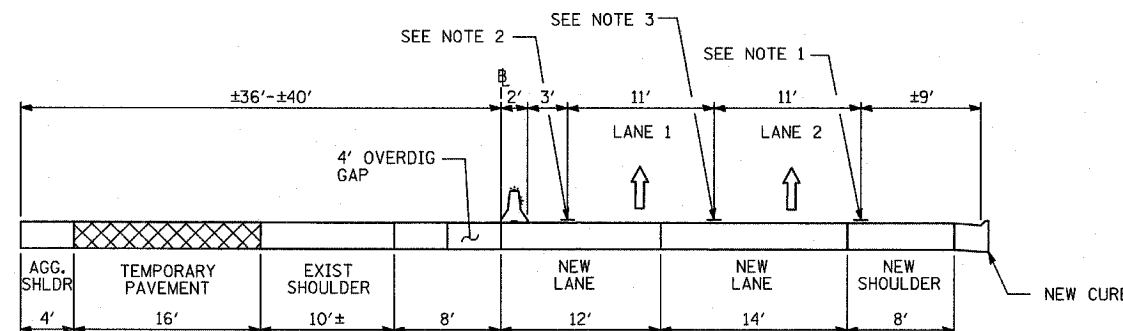
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 2 OF 12

SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 1, 2006 CHECKED BY: TGB



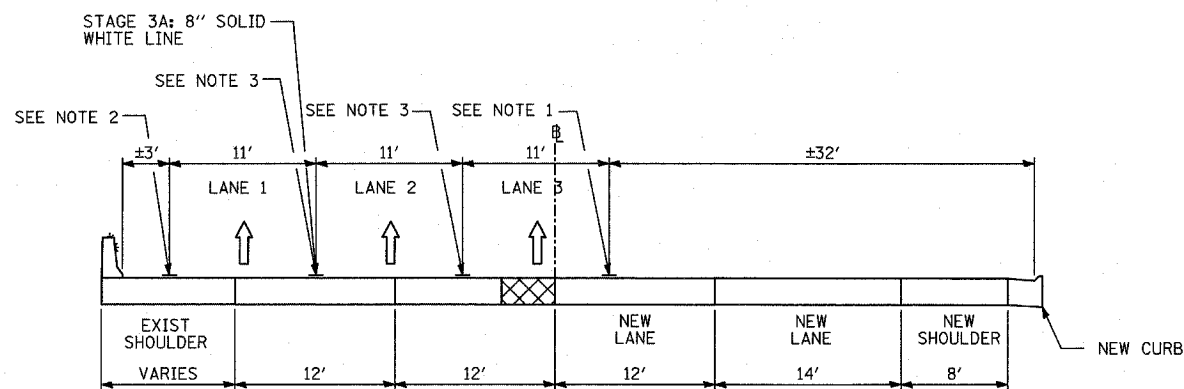
F-F

WINTER LANE CONFIGURATION: SB I-57; STA. 88+60 TO STA. 103+83
 STAGE 2: SB I-57; STA. 88+60 TO STA. 103+83
 STAGE 3A: SB I-57; STA. 88+60 TO 103+83



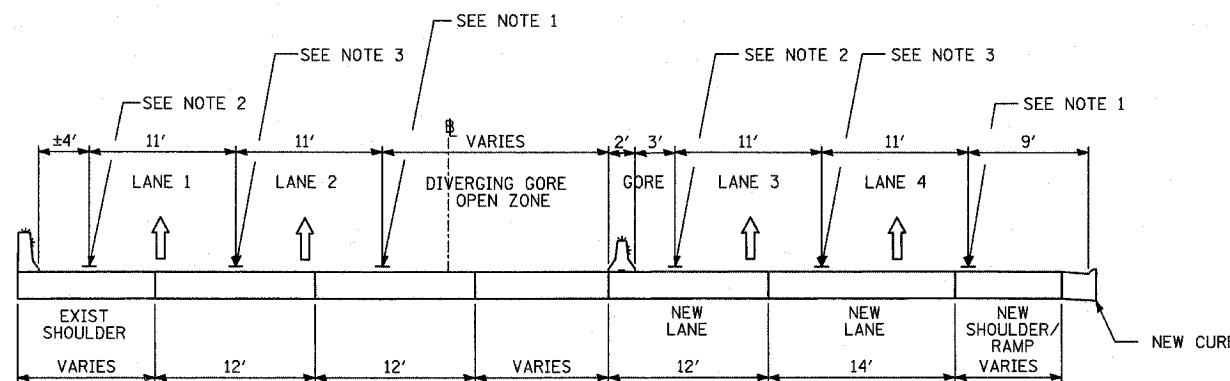
I-I

WINTER LANE CONFIGURATION: SB I-57; STA. 132+60 TO STA. 143+89
 STAGE 2: SB I-57; STA. 132+60 TO STA. 143+89



G-G

WINTER LANE CONFIGURATION: SB I-57; STA. 108+28 TO STA. 118+28
 STAGE 2: SB I-57; STA. 108+28 TO STA. 118+28
 STAGE 3A: SB I-57; STA. 108+28 TO STA. 113+35



J-J

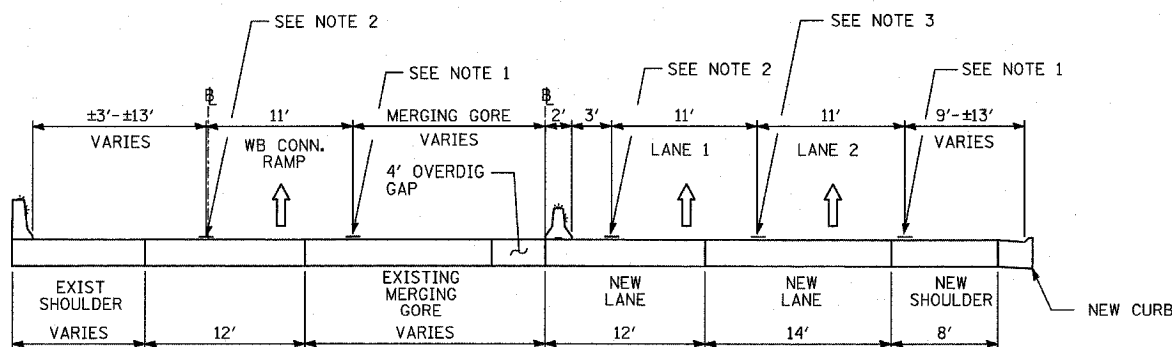
WINTER LANE CONFIGURATION: SB I-57; STA. 149+92 TO STA. 154+40

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

• ALL MOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC



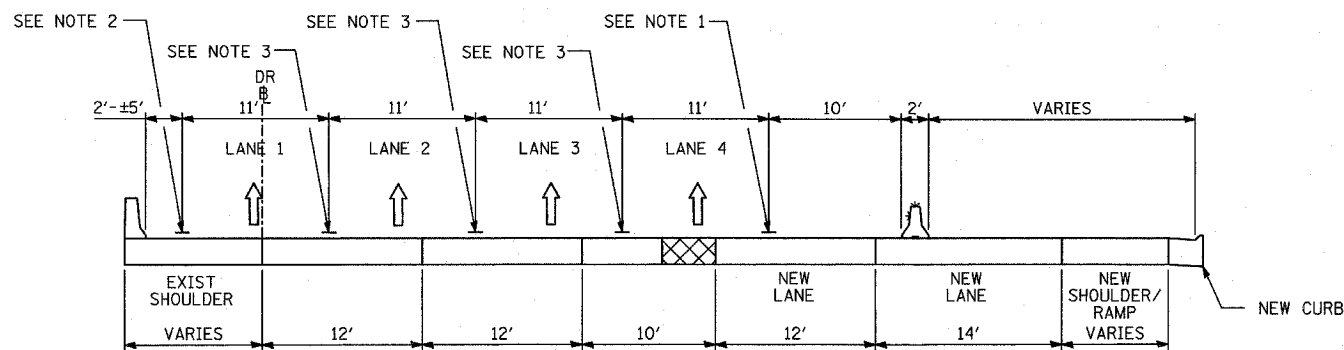
H-H

WINTER LANE CONFIGURATION: SB I-57; STA. 123+00 TO STA. 129+00
 STAGE 2: SB I-57; STA. 122+28 TO STA. 129+00

REVISIONS	
NAME	DATE

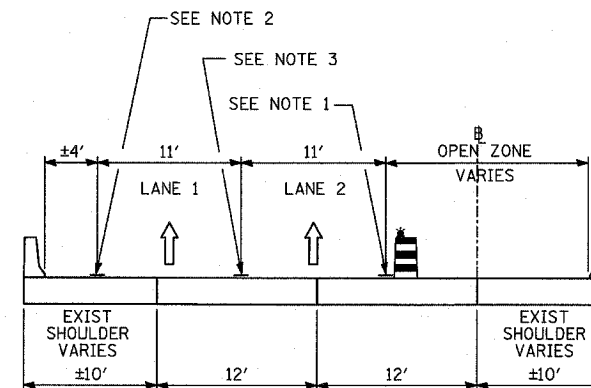
ILLINOIS DEPARTMENT OF TRANSPORTATION
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 GENERAL NOTES
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 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 3 OF 12

SCALE: NO SCALE
 DATE: MARCH 1, 2006
 DRAWN BY: RTM
 CHECKED BY: TGB



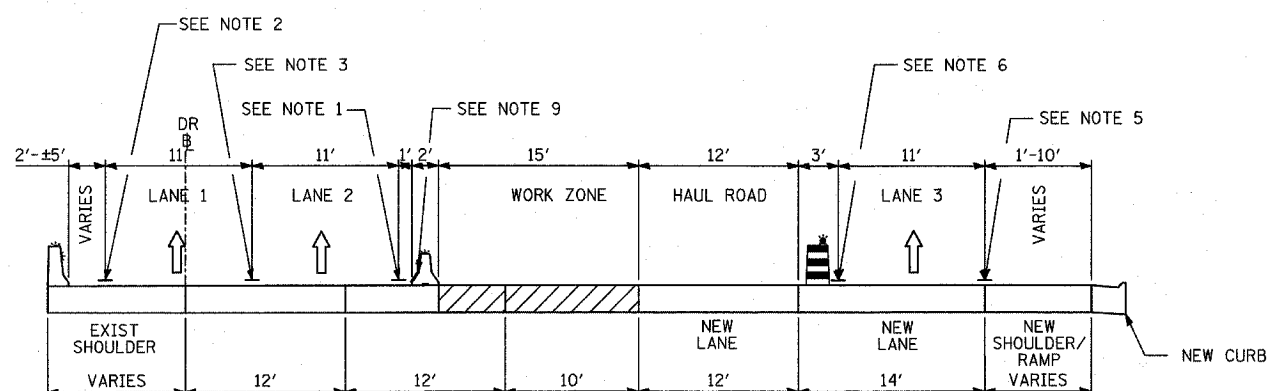
K-K

WINTER LANE CONFIGURATION: SB I-94 DAN RYAN; STA. 1217+27 TO 1313+00



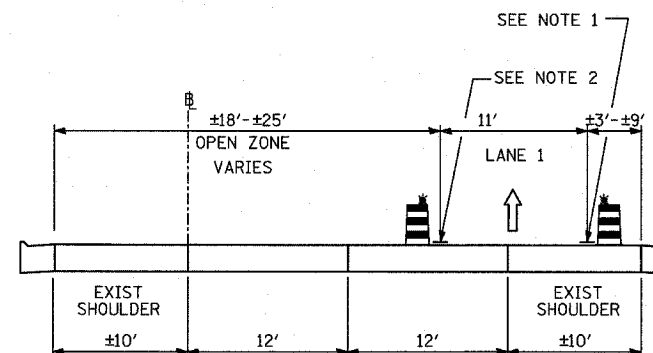
N-N

STAGE 2: SB I-94 BISHOP FORD; STA. 1151+57 TO STA. 1164+00



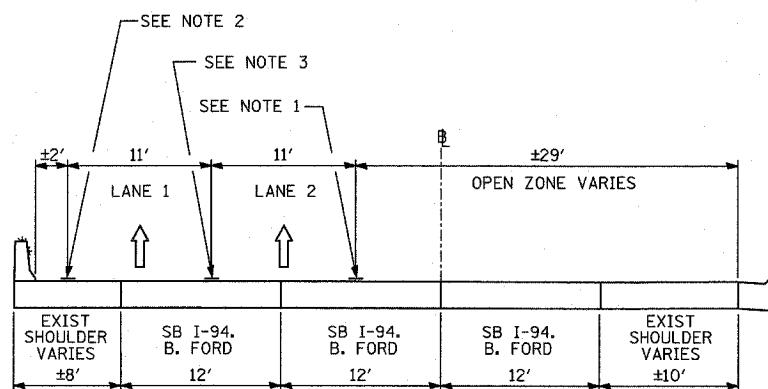
L-L

STAGE 2: SB I-94 DAN RYAN; STA. 1216+60 TO STA. 1270+00
 STAGE 2: SB I-94 DAN RYAN; STA. 1282+51 TO STA. 1313+00
 STAGE 2A: SB I-94 DAN RYAN; STA. 1216+60 TO STA. 1260+00
 STAGE 2A: SB I-94 DAN RYAN; STA. 1272+51 TO STA. 1313+00



O-O

STAGE 2: EB CROSS CONNECTOR; STA. 415+06 TO STA. 421+00



M-M

STAGE 2: SB I-94 BISHOP FORD; STA. 1137+07 TO STA. 1142+61

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

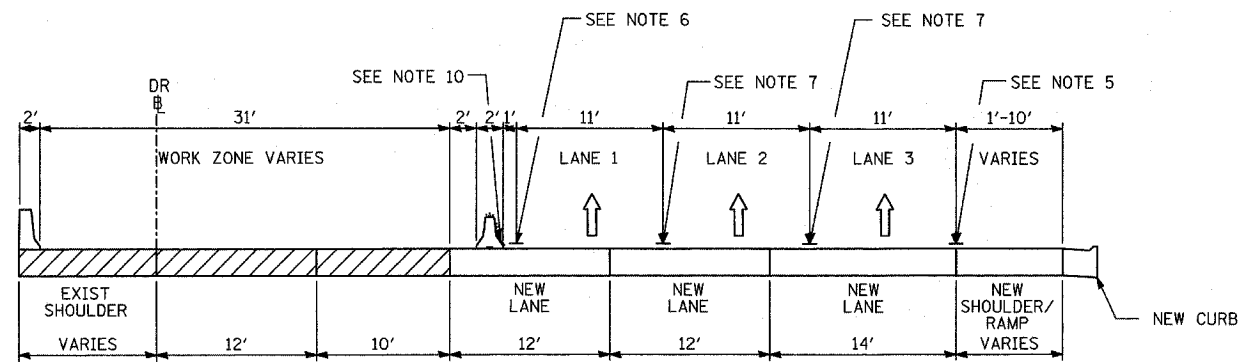
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

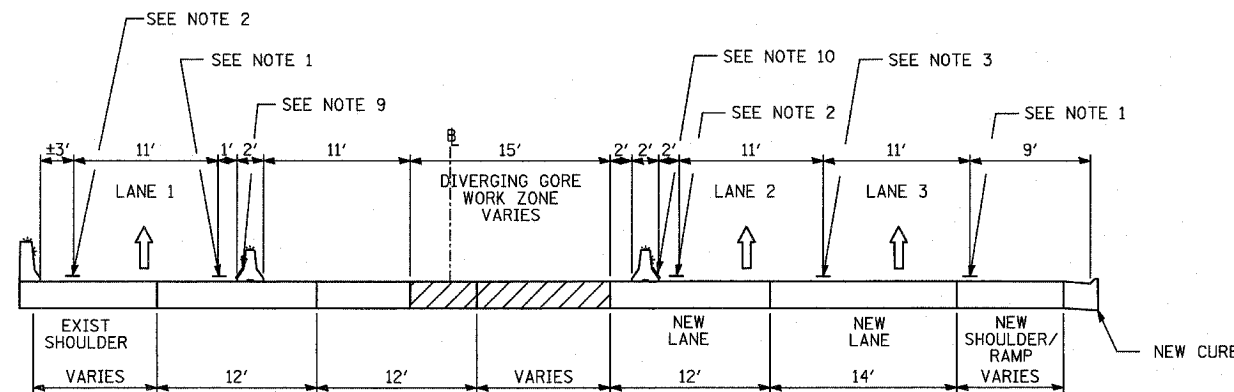
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
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 SHEET 4 OF 12

SCALE: NO SCALE DRAWN BY: RTM
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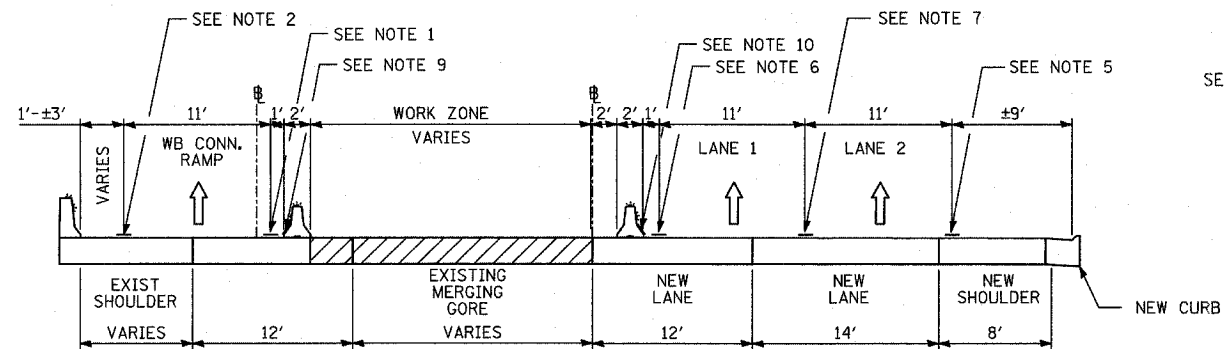
P-P

STAGE 3: SB I-94 DAN RYAN; STA. 1221+60 TO STA. 1313+00
 STAGE 3B: SB I-94 DAN RYAN; STA. 1205+00 TO STA. 1221+60



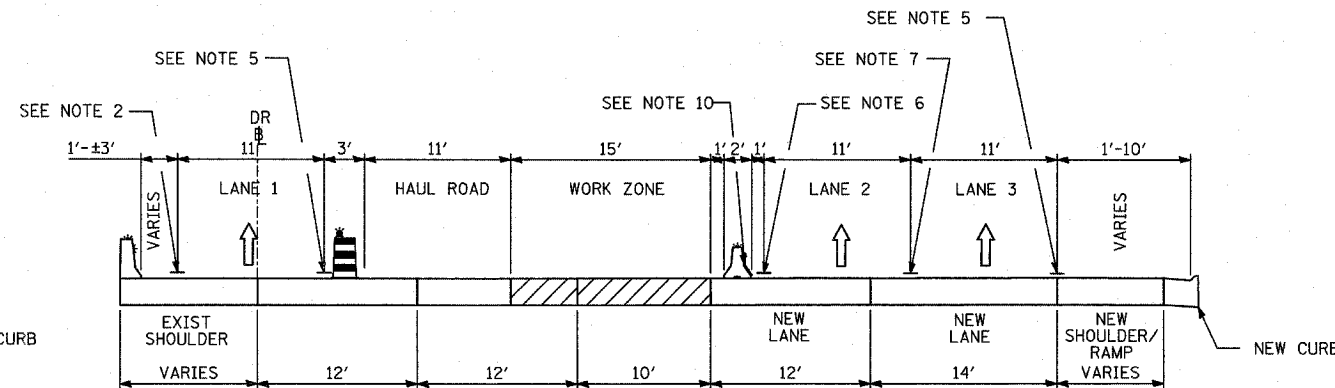
S-S

STAGE 3A: SB I-57; STA. 149+92 TO STA. 155+37



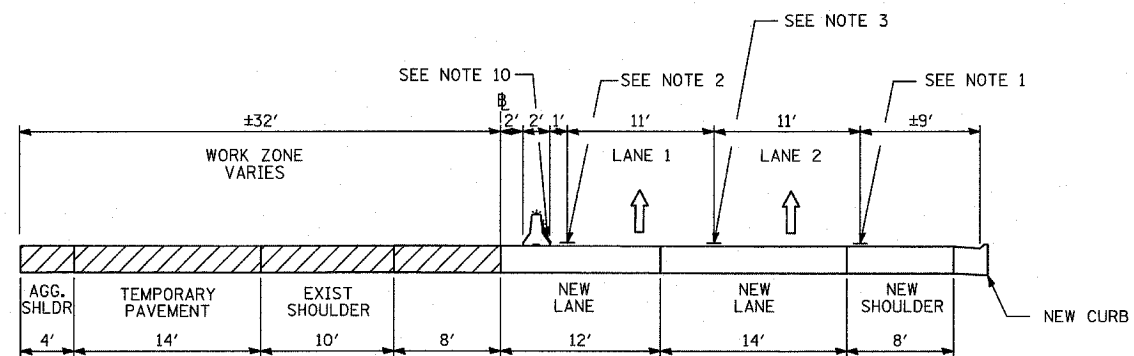
Q-Q

STAGE 3A: SB I-57; STA. 120+40 TO STA. 126+00



T-T

STAGE 3A: SB I-94 DAN RYAN; STA. 1205+00 TO STA. 1216+60



R-R

STAGE 3A: SB I-57; STA. 132+05 TO STA. 143+90

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

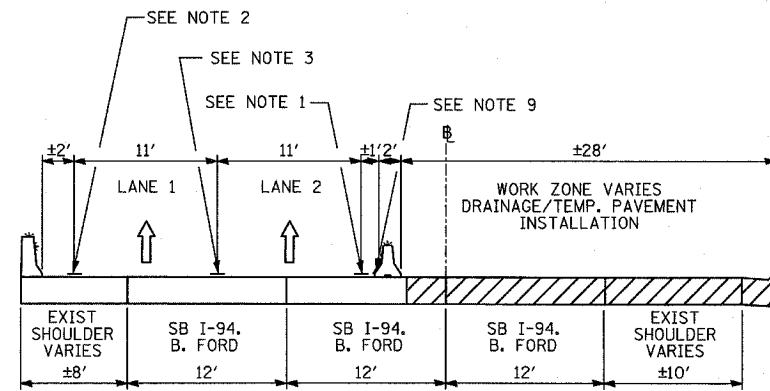
• ALL MOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
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 SHEET 5 OF 12

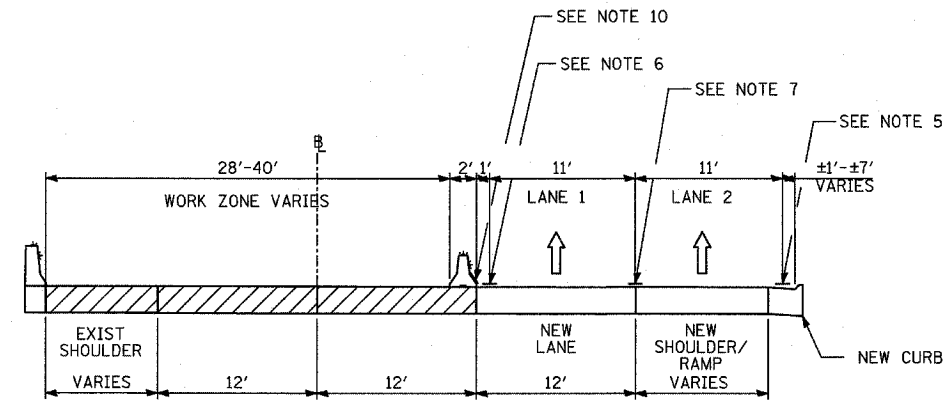
SCALE: NO SCALE DRAWN BY: RTM
 DATE: MARCH 1, 2006 CHECKED BY: TGB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	*	COOK	764	180
88+60 (SB I-57) TO STA. 1126+91 (SB FORD)				
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				
				62593



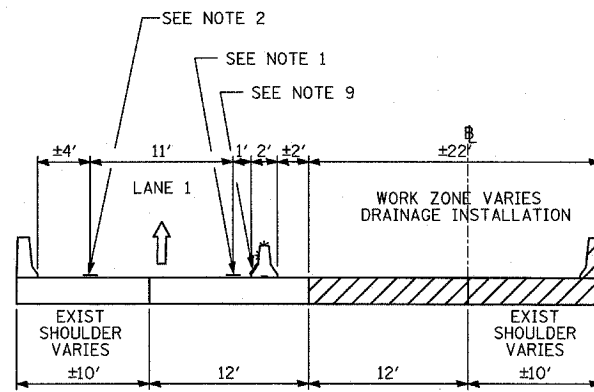
U-U

STAGE 3A: SB I-94 BISHOP FORD; STA. 1133+92 TO STA. 1143+07



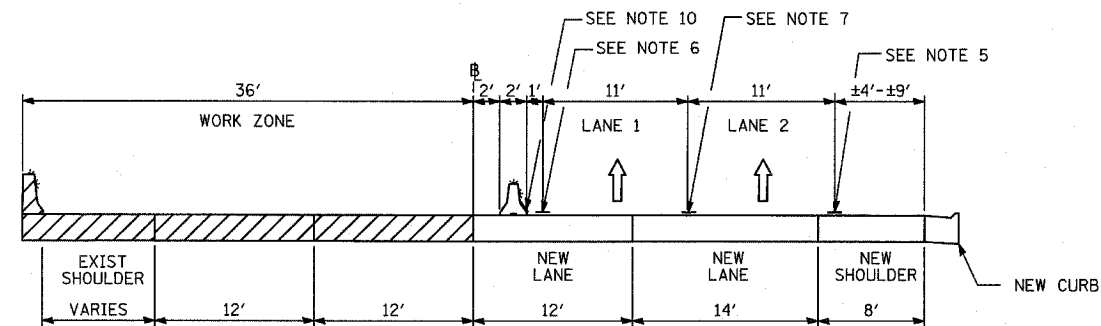
X-X

STAGE 3B: SB I-57; STA. 89+60 TO STA. 103+98



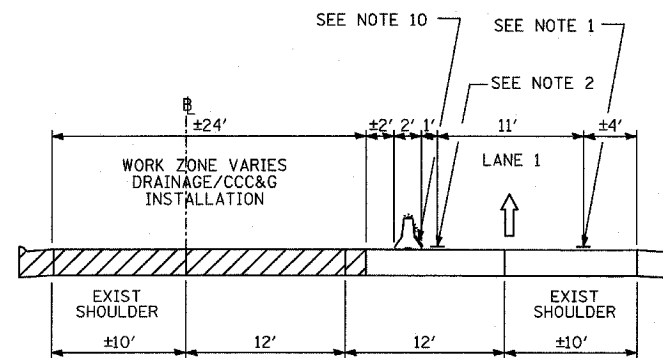
V-V

STAGE 3A: SB I-94 BISHOP FORD; STA. 1151+57 TO STA. 1164+00



Y-Y

STAGE 3B: SB I-57; STA. 107+16 TO STA. 121+36
 STAGE 3C: SB I-57; STA. 107+16 TO STA. 121+36
 STAGE 3D: SB I-57; STA. 107+16 TO STA. 121+36



W-W

STAGE 3A: EB CROSS CONNECTOR; STA. 415+06 TO STA. 421+81

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

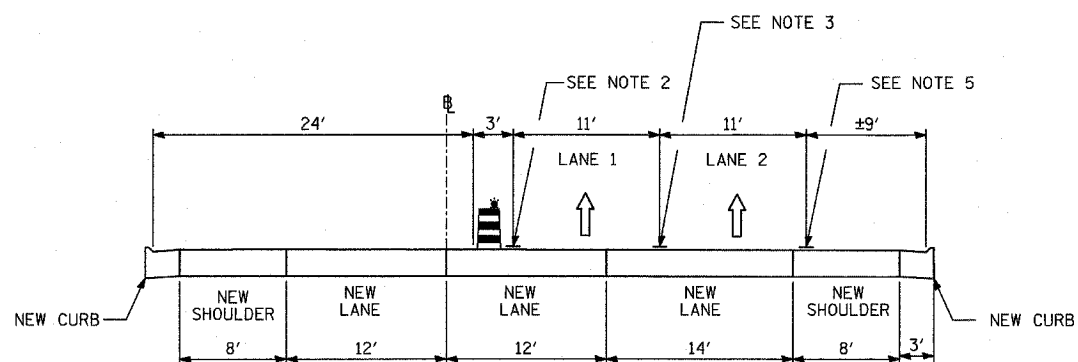
* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

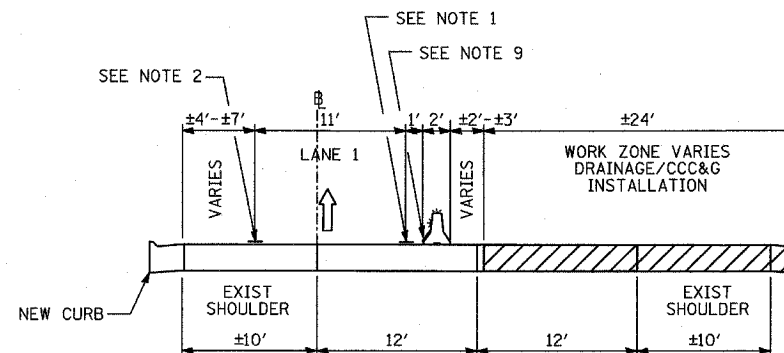
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
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 SHEET 6 OF 12

SCALE: NO SCALE DRAWN BY: RTM
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Z-Z

STAGE 3B: SB I-57; STA. 132+05 TO STA. 143+90
 STAGE 3C: SB I-57; STA. 132+05 TO STA. 143+90
 STAGE 3D: SB I-57; STA. 132+05 TO STA. 143+90



CC-CC

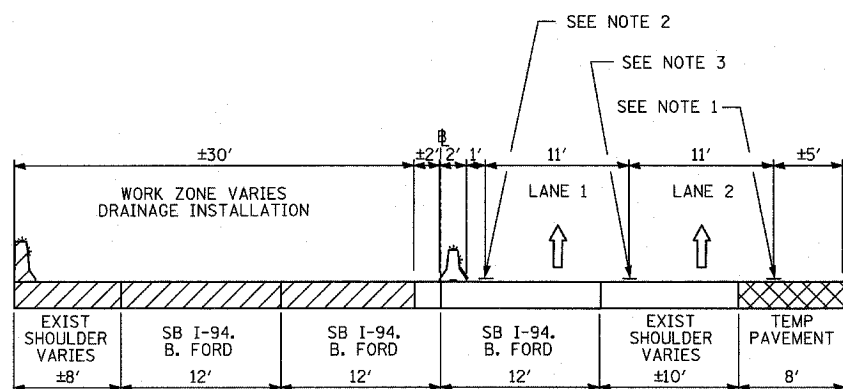
STAGE 3B: EB CROSS CONNECTOR; STA. 415+06 TO STA. 421+81

- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

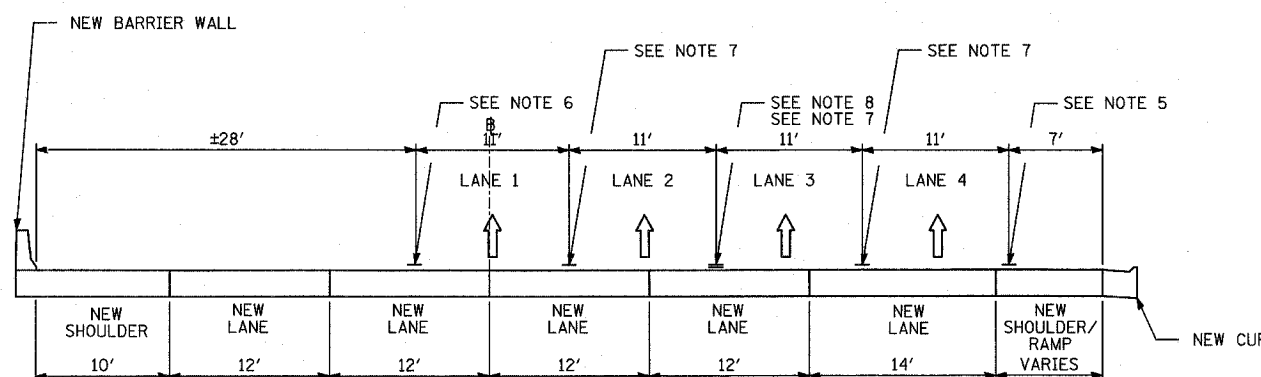
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

• ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC



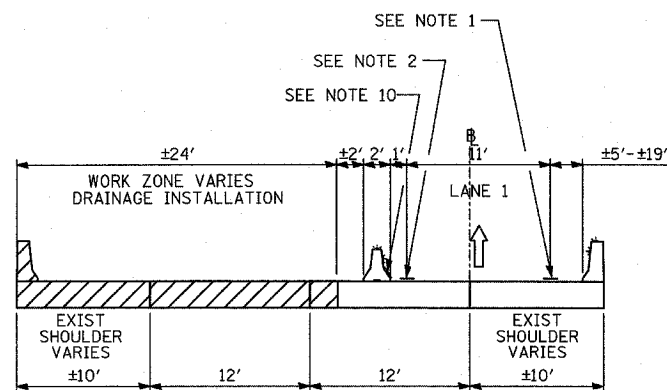
AA-AA

STAGE 3B: SB I-94 BISHOP FORD; STA. 1128+03 TO STA. 1143+00



DD-DD

STAGE 3C: SB I-57; STA. 150+58 TO STA. 154+68



BB-BB

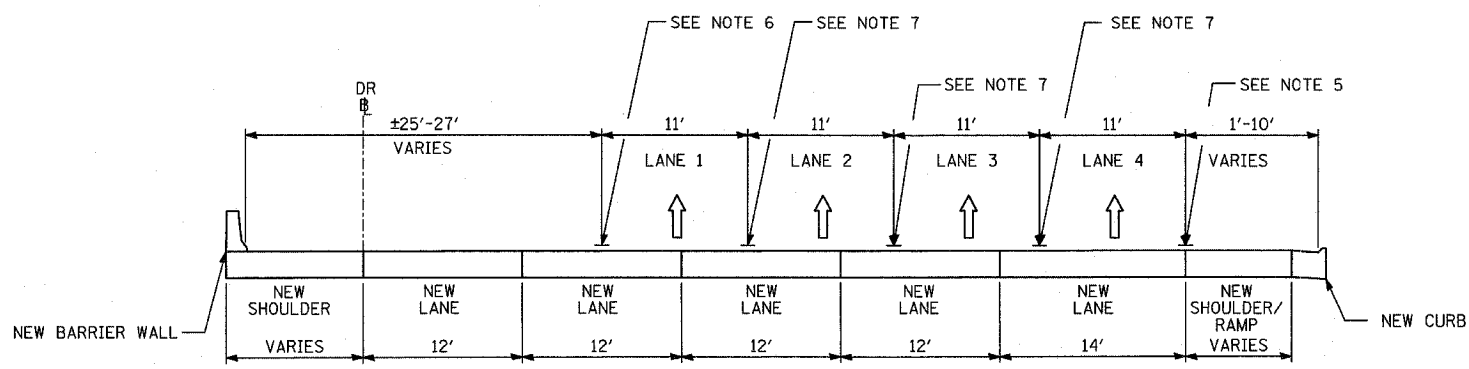
STAGE 3B: SB I-94 BISHOP FORD; STA. 1152+00 TO STA. 1167+00

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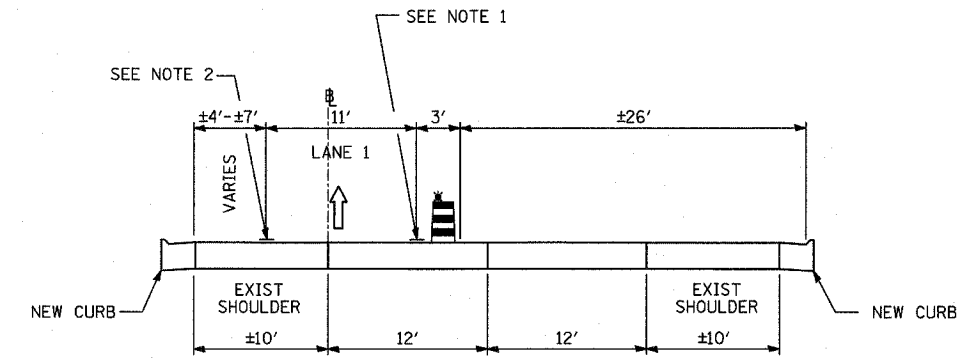
ILLINOIS DEPARTMENT OF TRANSPORTATION
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 SHEET 7 OF 12

SCALE: NO SCALE
 DATE: MARCH 1, 2006
 DRAWN BY: RTM
 CHECKED BY: TGB

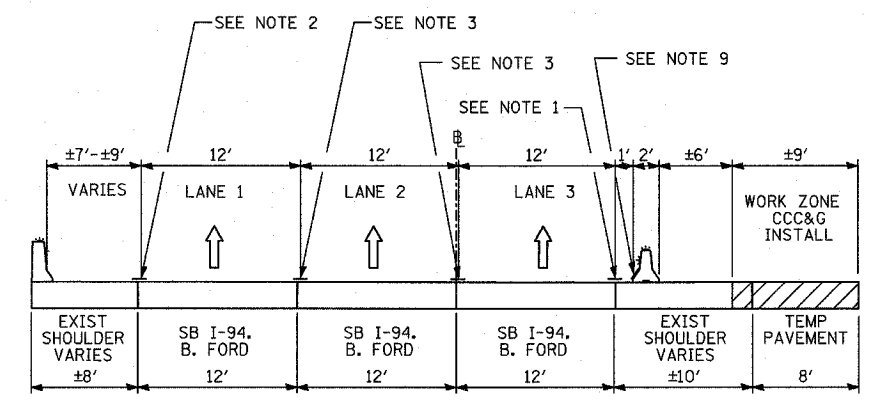




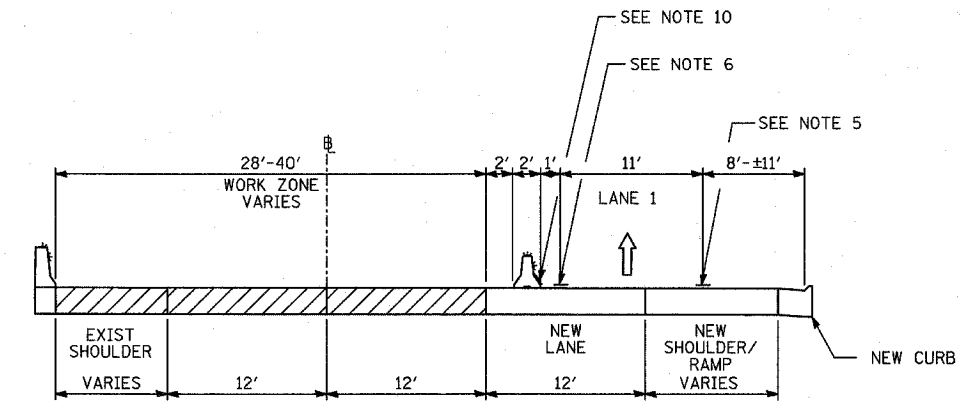
EE-EE
 STAGE 3C: SB I-94 DAN RYAN; STA. 1205+00 TO STA. 1215+53



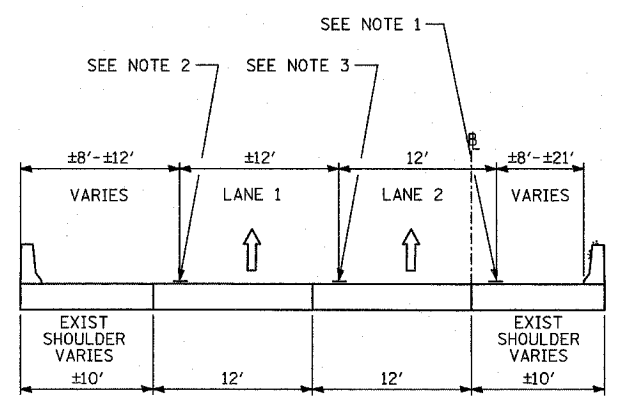
HH-HH
 STAGE 3C: EB CROSS CONNECTOR; STA. 415+06 TO STA. 421+81



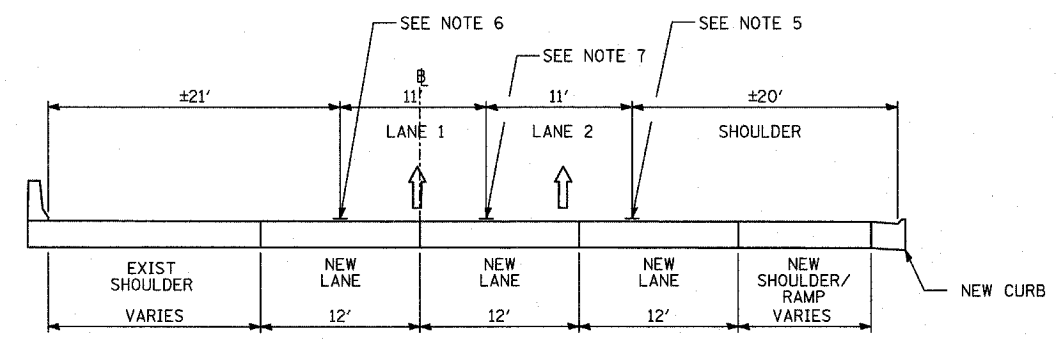
FF-FF
 STAGE 3C: SB I-94 BISHOP FORD; STA. 1127+00 TO STA. 1143+04



II-II
 STAGE 3C: SB I-57; STA. 88+60 TO STA. 103+98



GG-GG
 STAGE 3C: SB I-94 BISHOP FORD; STA. 1152+00 TO STA. 1167+00



JJ-JJ
 STAGE 3D: SB I-57; STA. 87+48 TO 97+48

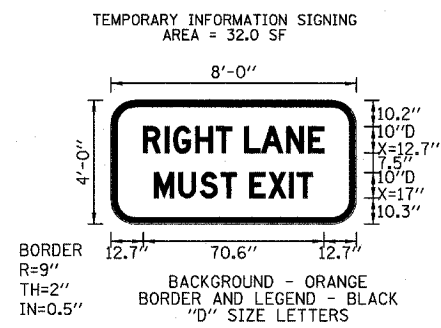
- NOTE 1 - EPOXY PAVEMENT MARKING-LINE 4" (WHITE)
- NOTE 2 - EPOXY PAVEMENT MARKING-LINE 4" (YELLOW)
- NOTE 3 - EPOXY PAVEMENT MARKING-SKIP-DASH 5" (WHITE), 10' LINE WITH 30' SPACE
- NOTE 4 - EPOXY PAVEMENT MARKING-LINE 8" (WHITE)
- NOTE 5 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (WHITE)
- NOTE 6 - PAVEMENT MARKING TAPE, TYPE III 4" LINE (YELLOW)
- NOTE 7 - PAVEMENT MARKING TAPE, TYPE III 5" SKIP-DASH (WHITE), 10' LINE WITH 30' SPACE
- NOTE 8 - PAVEMENT MARKING TAPE, TYPE III 8" LINE (WHITE)
- NOTE 9 - TEMPORARY PAVEMENT MARKING-LINE 6" (WHITE)
- NOTE 10 - TEMPORARY PAVEMENT MARKING-LINE 6" (YELLOW)

LEGEND:

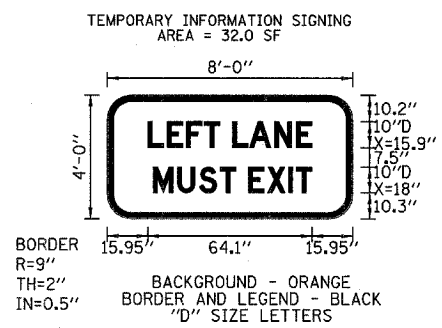
- TEMPORARY CONCRETE BARRIER (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- TRAFFIC MOVEMENT
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- SINGLE-FACE CONCRETE BARRIER, 32"
- SINGLE-FACE CONCRETE BARRIER, 32" (WITH REFLECTORS ON TOP AND SIDE FACING TRAFFIC)
- PAVEMENT REMOVAL AREA
- TEMPORARY PAVEMENT

* ALL NOT TYPICAL SECTIONS ARE DRAWN IN THE DIRECTION OF TRAFFIC

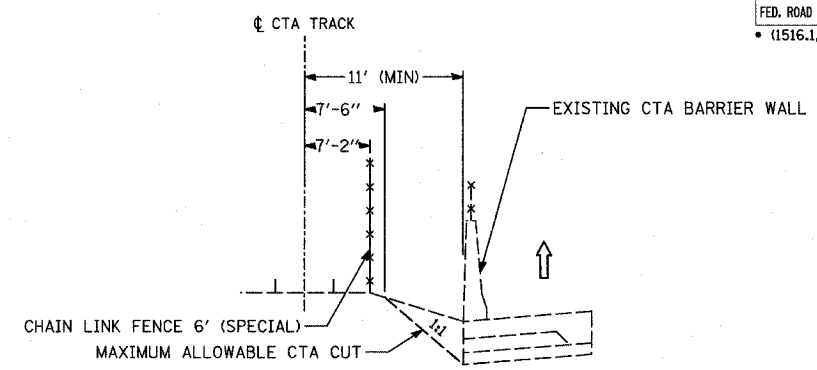
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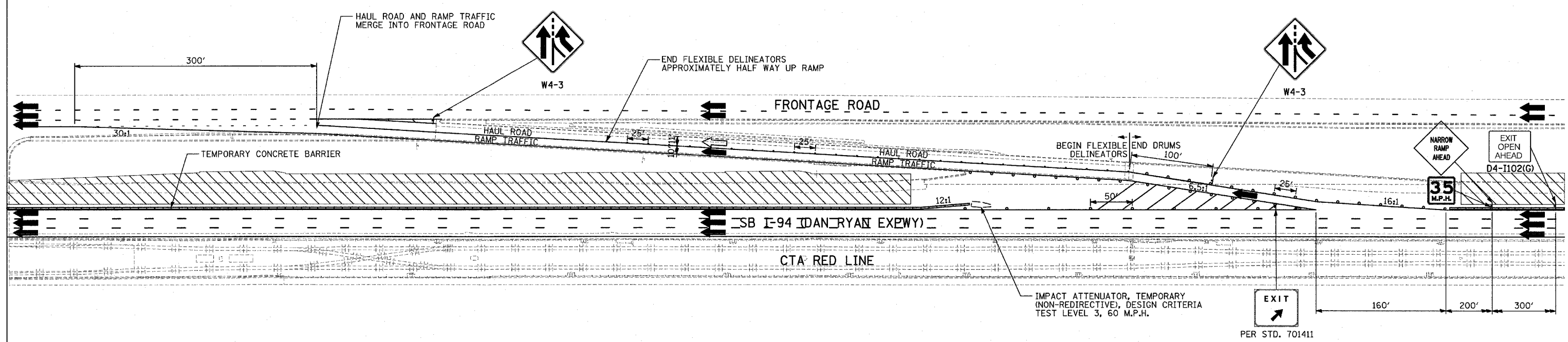
"RIGHT LANE MUST EXIT" SIGN DETAIL



"LEFT LANE MUST EXIT" SIGN DETAIL



LOCATION OF CHAIN LINK FENCE 6' (SPECIAL)



SUGGESTED MAINTENANCE OF TRAFFIC STAGE 1 WORK ZONE EGRESS DETAIL

(AT HALSTED ST., WENTWORTH AVE., & 95TH ST. RAMPS)

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
 - TRAFFIC LANE
 - CONSTRUCTION TRAFFIC
 - WORK ZONE
 - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
 - DELINEATORS

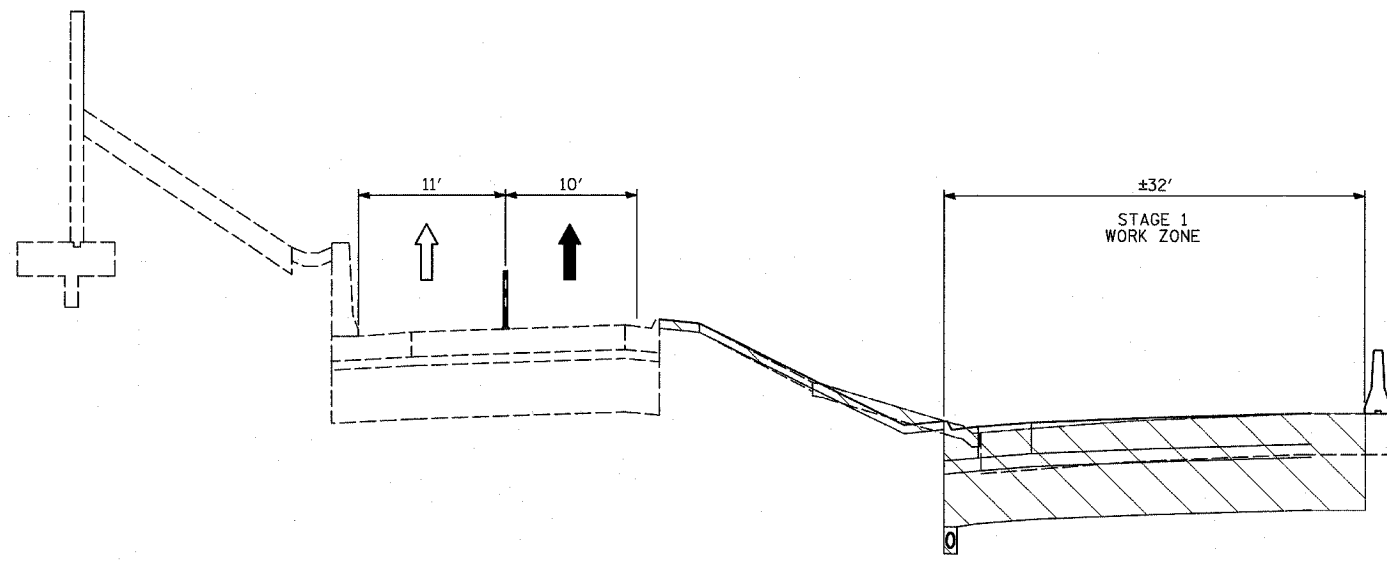
- NOTES:**
1. ALL CONSTRUCTION VEHICLES MUST STAY WITHIN THE HAUL ROAD. CROSSING EXIT RAMP TRAFFIC IS PROHIBITED.
 2. THE CONTRACTOR SHALL BE RESPONSIBLE TO DIFFERENTIATE BETWEEN THE RAMP EXIT AND HAUL ROAD ENTRANCE.
 3. THE CONTRACTOR SHALL UTILIZE EITHER THE A.I.S. OR C-D ROADWAY TO MERGE HAUL ROAD AND RAMP TRAFFIC AT ALL OTHER EXIT RAMP LOCATIONS. A STOP SIGN SHALL BE PLACED AT THE UPSTREAM END OF THE C-D ROADWAY TO STOP TRAFFIC ON THE RAMP FROM THE FRONTAGE ROAD.
 4. ALL SIGNING FROM DISTRICT 1 DETAIL TC-18 SHALL BE APPLIED TO THE ABOVE DETAIL AND ANY OTHER WORK ZONE ACCESS OPTIONS WHICH MAY BE USED BY THE CONTRACTOR.
 5. SEE FOLLOWING SHEET FOR TYPICAL CROSS-SECTIONS AT EACH LOCATION.

REVISIONS	
NAME	DATE

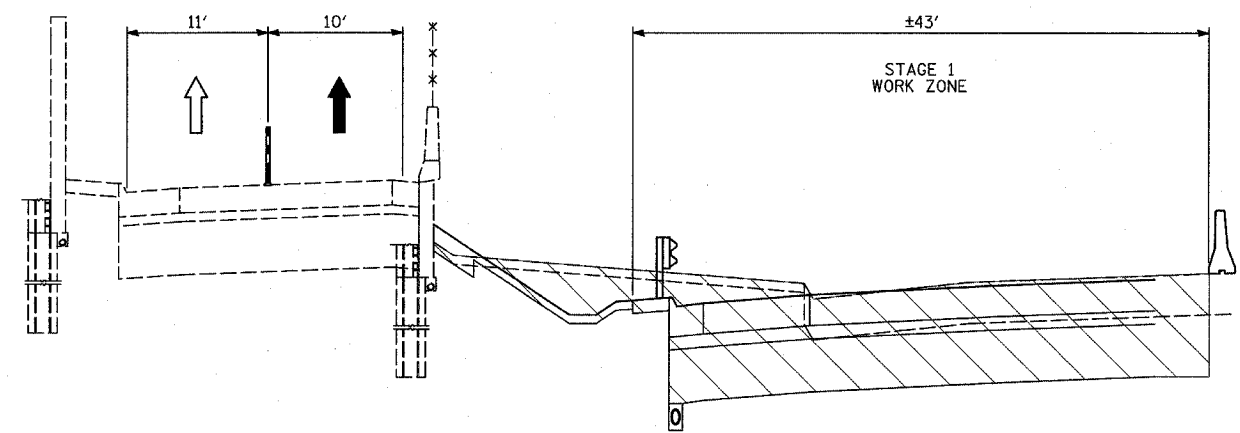
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
GENERAL NOTES
CONSTRUCTION STAGING NOTES
MAINTENANCE OF TRAFFIC DETAILS
SHEET 10 OF 12

SCALE: NO SCALE
DATE: MARCH 1, 2006

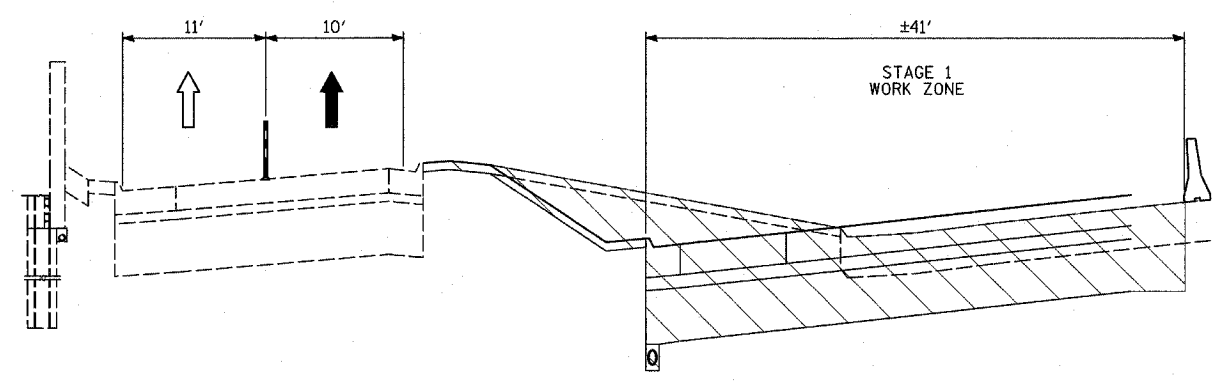
DRAWN BY: RTM
CHECKED BY: TGB








TYPICAL CROSS SECTION OF WORK ZONE EGRESS AT HALSTED ST. EXIT



TYPICAL CROSS SECTION OF WORK ZONE EGRESS AT 95TH ST. EXIT



TYPICAL CROSS SECTION OF WORK ZONE EGRESS AT WENTWORTH AVE. EXIT

- LEGEND:**
-  TEMPORARY CONCRETE BARRIER
 -  TRAFFIC LANE
 -  CONSTRUCTION TRAFFIC
 -  WORK ZONE
 -  FLEXIBLE DELINEATORS

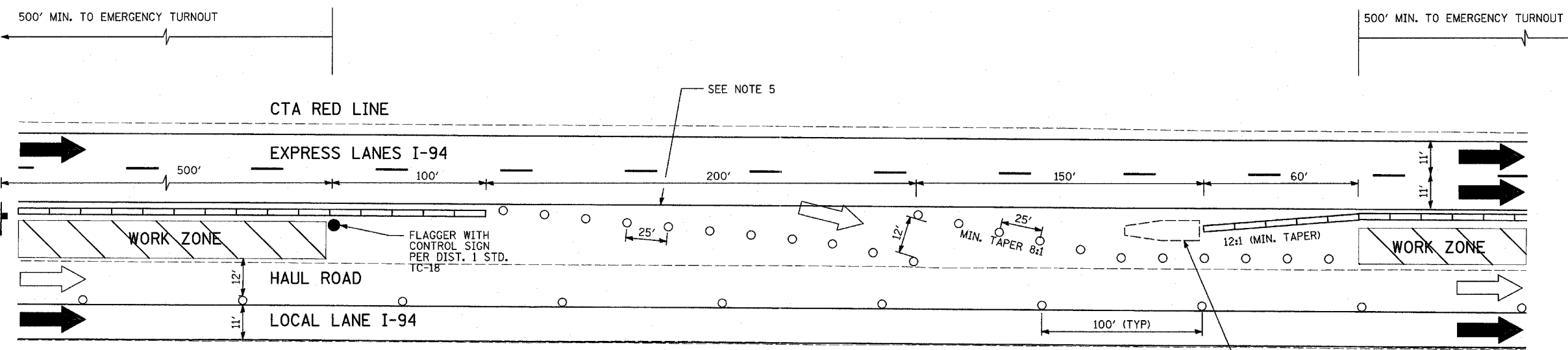
TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

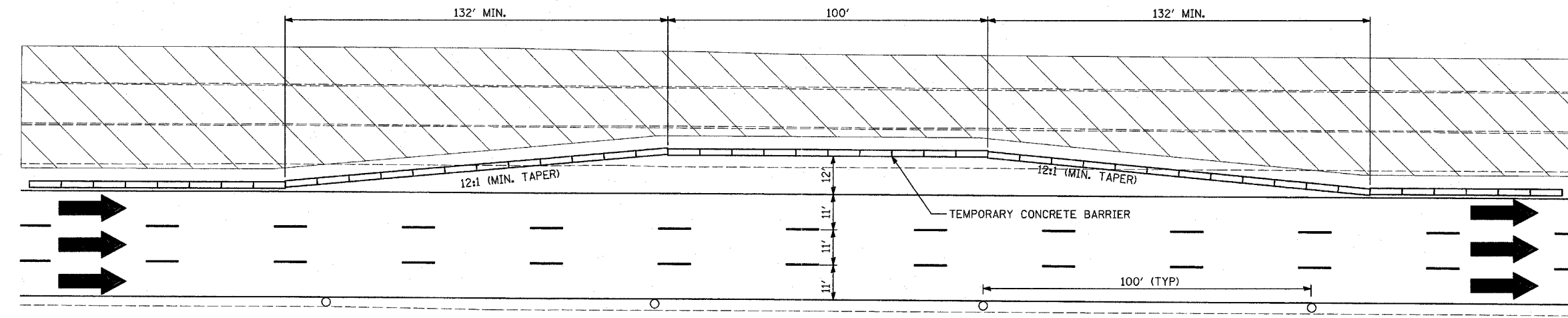
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 GENERAL NOTES
 CONSTRUCTION STAGING NOTES
 MAINTENANCE OF TRAFFIC DETAILS
 SHEET 11 OF 12

SCALE: NO SCALE
 DATE: MARCH 1, 2006

DRAWN BY: RTM
 CHECKED BY: TGB



SUGGESTED ACCESS FROM EXPRESS LANES



MAINTENANCE OF TRAFFIC EMERGENCY TURNOUT DETAIL
EMERGENCY TURNOUTS WILL BE LOCATED APPROXIMATELY EVERY HALF MILE.

- LEGEND:**
- TEMPORARY CONCRETE BARRIER
 - TRAFFIC LANE
 - CONSTRUCTION TRAFFIC
 - WORK ZONE
 - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
 - ARROW BOARD

NOTES:

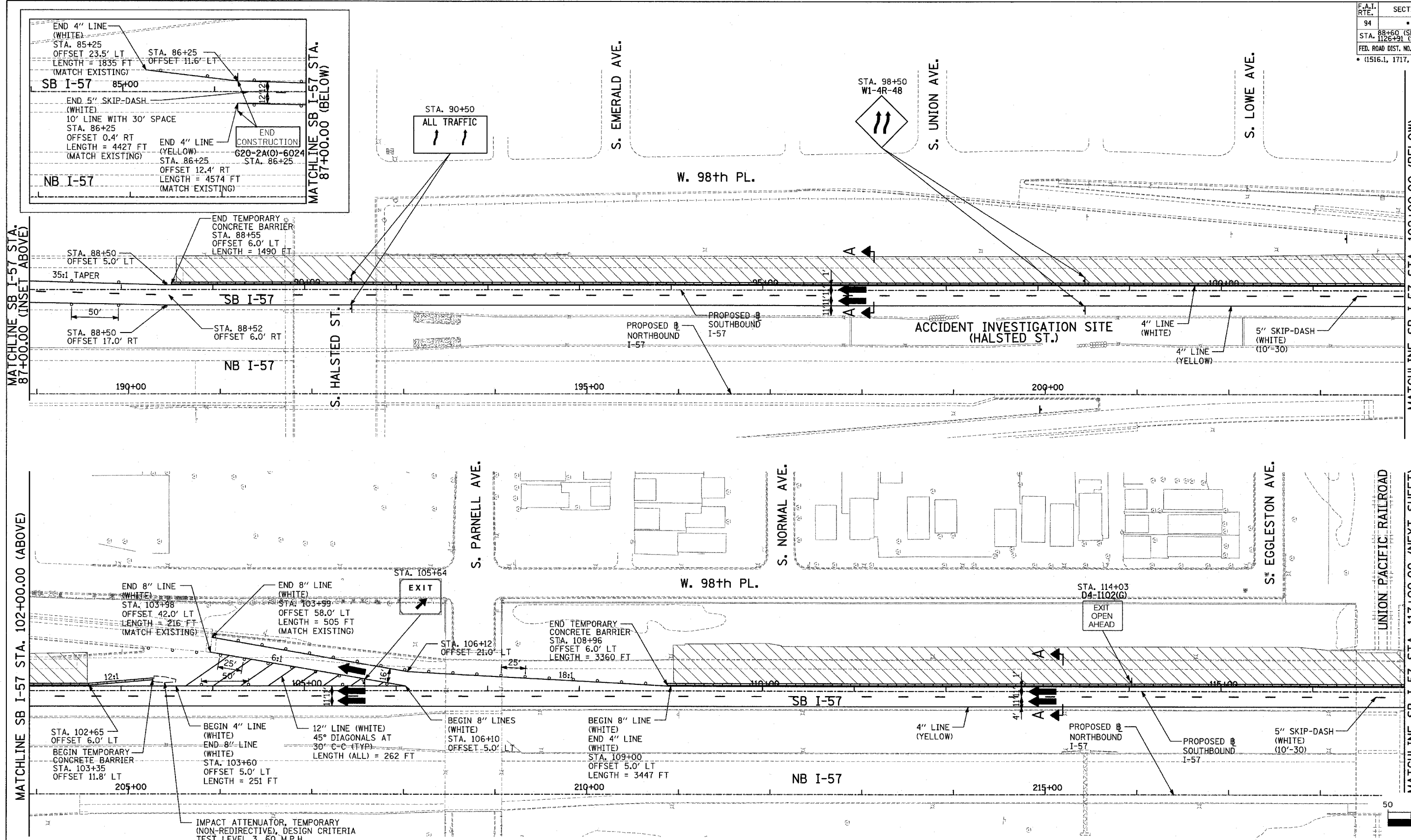
1. THERE CAN BE NO MORE THAN TWO (2) WORK ZONE ACCESS/EGRESS COMBINATIONS AND THE CONTRACTOR MUST MAINTAIN AT LEAST ONE (1) EXPRESS LANE ACCESS DURING STAGE 2 CONSTRUCTION.
2. THE CONTRACTOR SHALL NOT ENTER OR EXIT THE HAUL ROAD WITHIN THE 100' BARREL-SPACING AREA. WORK ZONE ACCESS AND EGRESS WILL ONLY BE PERMITTED AT THE DESIGNATED LOCATIONS.
3. FOR EXPRESS LANE WORK ZONE EGRESS, TAPER LENGTHS SHALL FOLLOW DISTRICT 1 STANDARD TC-18.
4. THE CONTRACTOR SHALL CLOSE OPENINGS WITH BARRELS WHEN NOT BEING USED FOR ACCESS.
5. TEMPORARY PAVEMENT MARKINGS SHALL BE CARRIED THRU THE OPENING.

TYLIN INTERNATIONAL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
GENERAL NOTES
CONSTRUCTION STAGING NOTES
MAINTENANCE OF TRAFFIC DETAILS
SHEET 12 OF 12

SCALE: NO SCALE DRAWN BY: RTM
DATE: MARCH 1, 2006 CHECKED BY: TGB

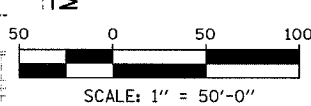
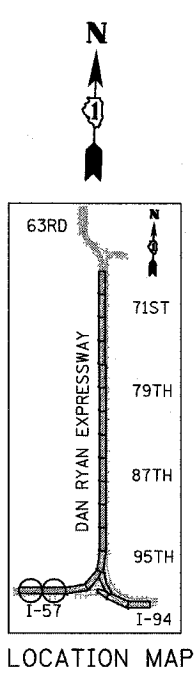


MATCHLINE SB I-57 STA. 87+00.00 (INSET ABOVE)

MATCHLINE SB I-57 STA. 102+00.00 (BELOW)

MATCHLINE SB I-57 STA. 102+00.00 (ABOVE)

MATCHLINE SB I-57 STA. 117+00.00 (NEXT SHEET)



TYLIN INTERNATIONAL

NOTE:

ON ALL THE STAGE 1 PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

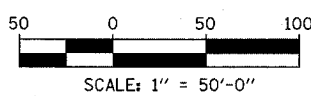
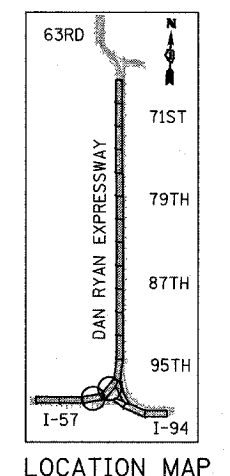
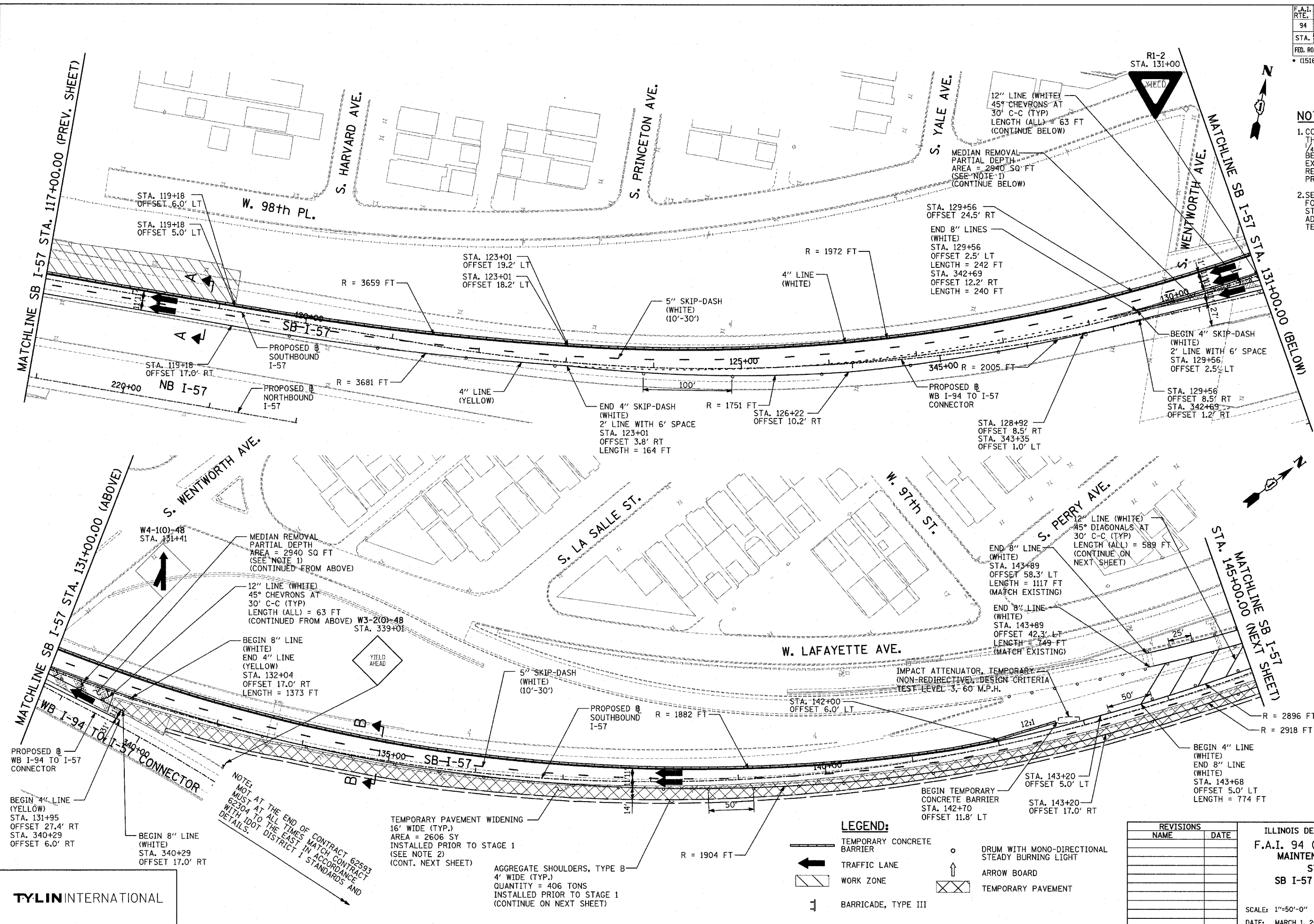
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-57 STA. 84+00 TO 117+00
 SHEET 1 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

NOTE:

- CORRUGATED MEDIAN REMOVAL THICKNESS RANGES FROM 1/4" TO 3/4". MEDIAN MUST BE FLUSH WITH THE ADJACENT EXISTING PAVEMENT. MEDIAN REMOVAL MUST BE PERFORMED PRIOR TO THE START OF STAGE 1.
- SEE DRAINAGE AND UTILITY PLANS FOR LOCATIONS OF DRAINAGE STRUCTURES THAT WILL REQUIRE ADJUSTMENT IN THE AREA OF THE TEMPORARY PAVEMENT WIDENING.



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-57 STA. 117+00 TO 145+00
 SHEET 2 OF 7

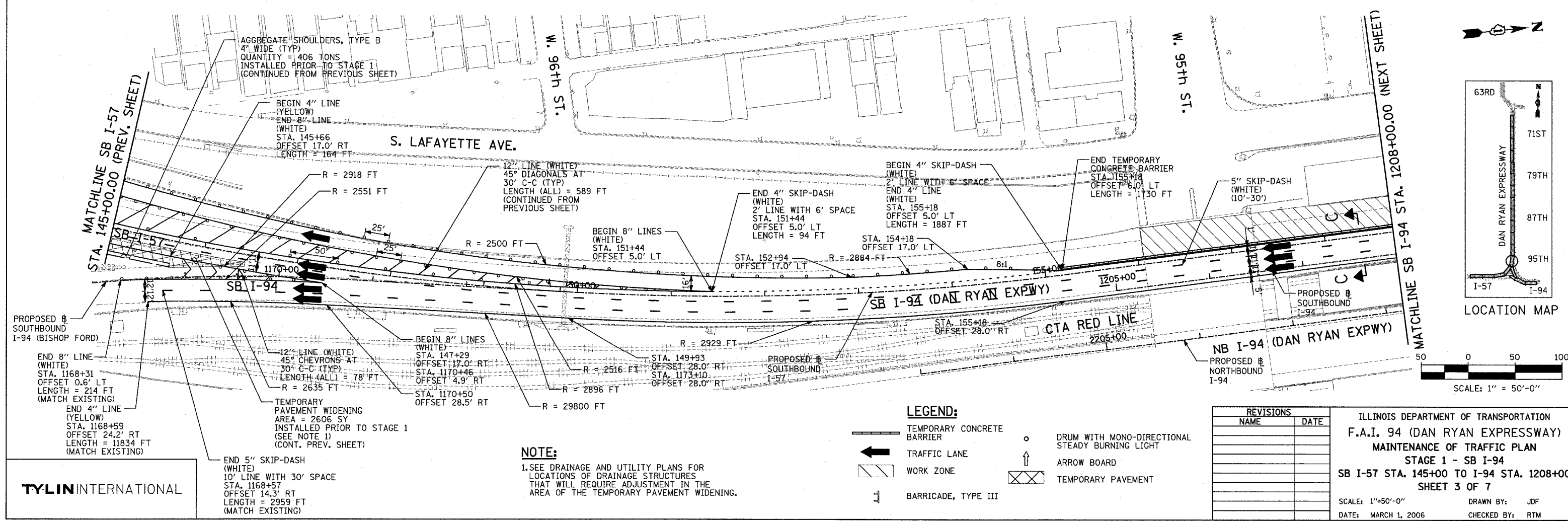
SCALE: 1"=50'-0" DRAWN BY: JDF
 DATE: MARCH 1, 2006 CHECKED BY: RTM

TYLIN INTERNATIONAL

NOTE: NOT AT THE END OF CONTRACT 62593 MUST AT ALL TIMES MATCH CONTRACT 62304 TO THE EAST IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

TEMPORARY PAVEMENT WIDENING
 16' WIDE (TYP.)
 AREA = 2606 SY
 INSTALLED PRIOR TO STAGE 1
 (SEE NOTE 2)
 (CONT. NEXT SHEET)

AGGREGATE SHOULDERS, TYPE B
 4' WIDE (TYP.)
 QUANTITY = 406 TONS
 INSTALLED PRIOR TO STAGE 1
 (CONTINUE ON NEXT SHEET)



NOTE:
 1. SEE DRAINAGE AND UTILITY PLANS FOR LOCATIONS OF DRAINAGE STRUCTURES THAT WILL REQUIRE ADJUSTMENT IN THE AREA OF THE TEMPORARY PAVEMENT WIDENING.

LEGEND:

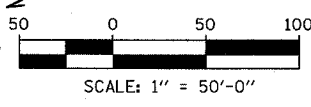
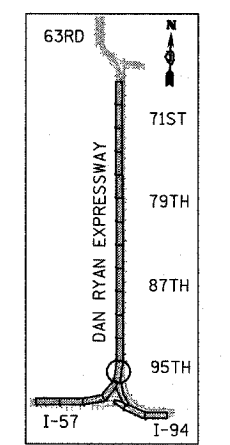
	TEMPORARY CONCRETE BARRIER		DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
	TRAFFIC LANE		ARROW BOARD
	WORK ZONE		TEMPORARY PAVEMENT
	BARRICADE, TYPE III		

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-57 STA. 145+00 TO I-94 STA. 1208+00
 SHEET 3 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

TYLIN INTERNATIONAL

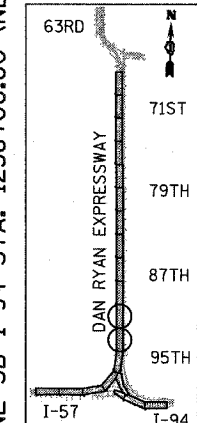
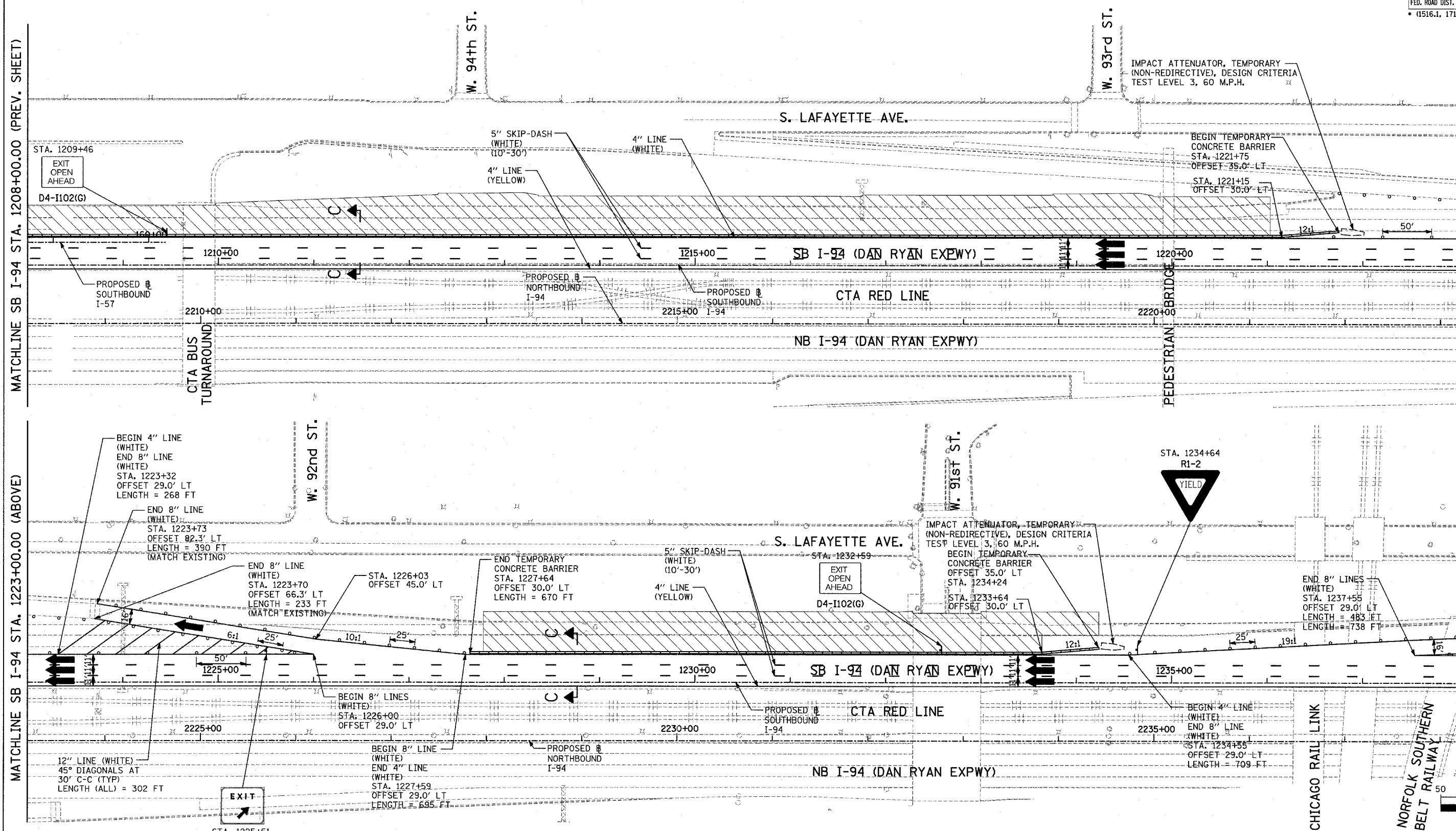


MATCHLINE SB I-94 STA. 1208+00.00 (PREV. SHEET)

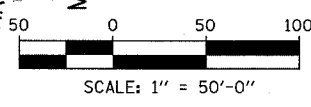
MATCHLINE SB I-94 STA. 1223+00.00 (BELOW)

MATCHLINE SB I-94 STA. 1223+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1238+00.00 (NEXT SHEET)



LOCATION MAP



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

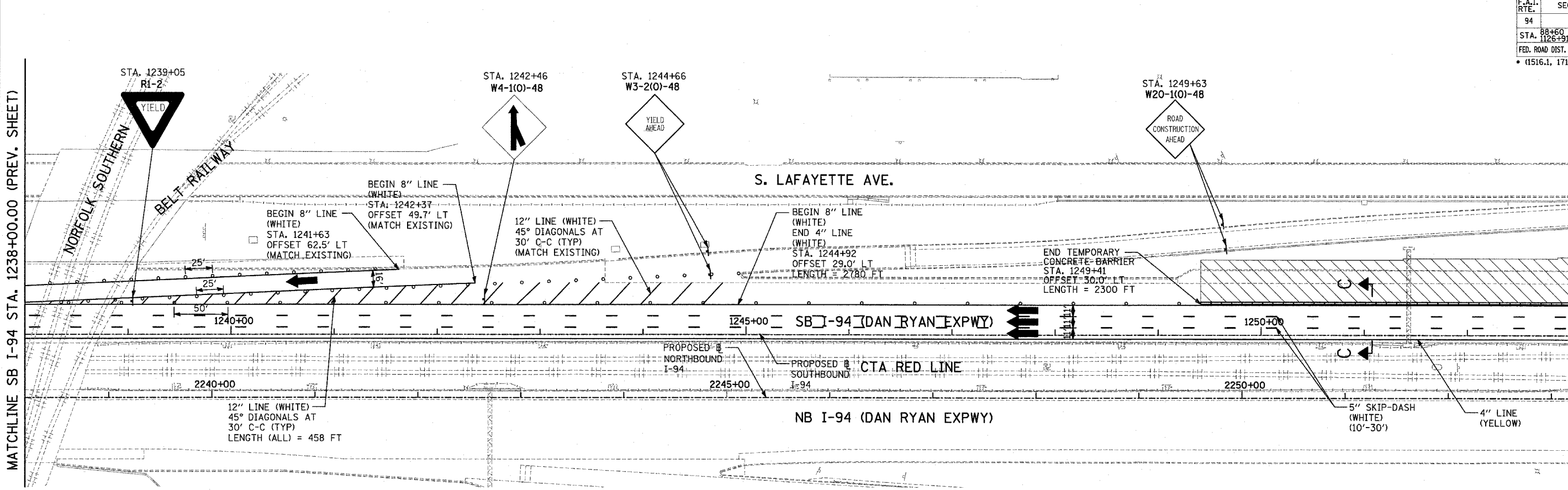
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-94 STA. 1208+00 TO 1238+00
 SHEET 4 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	191
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				

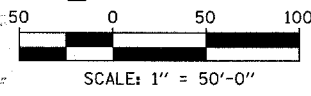
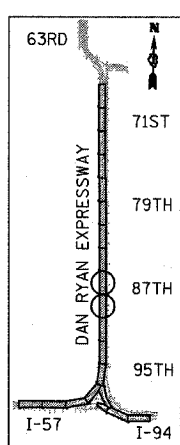
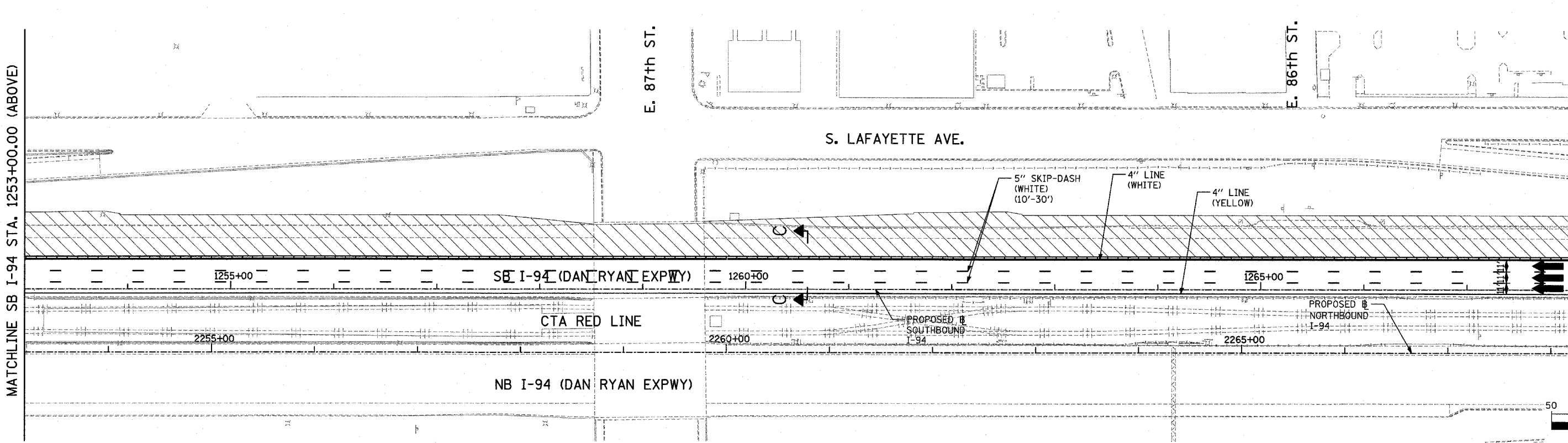
MATCHLINE SB I-94 STA. 1238+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1253+00.00 (BELOW)



MATCHLINE SB I-94 STA. 1253+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1268+00.00 (NEXT SHEET)



TYLIN INTERNATIONAL

LEGEND:

	TEMPORARY CONCRETE BARRIER		DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
	TRAFFIC LANE		ARROW BOARD
	WORK ZONE		TEMPORARY PAVEMENT
	BARRICADE, TYPE III		

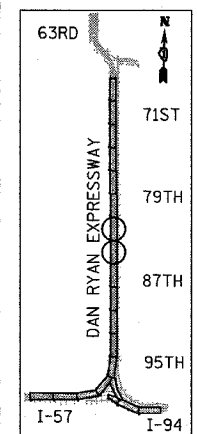
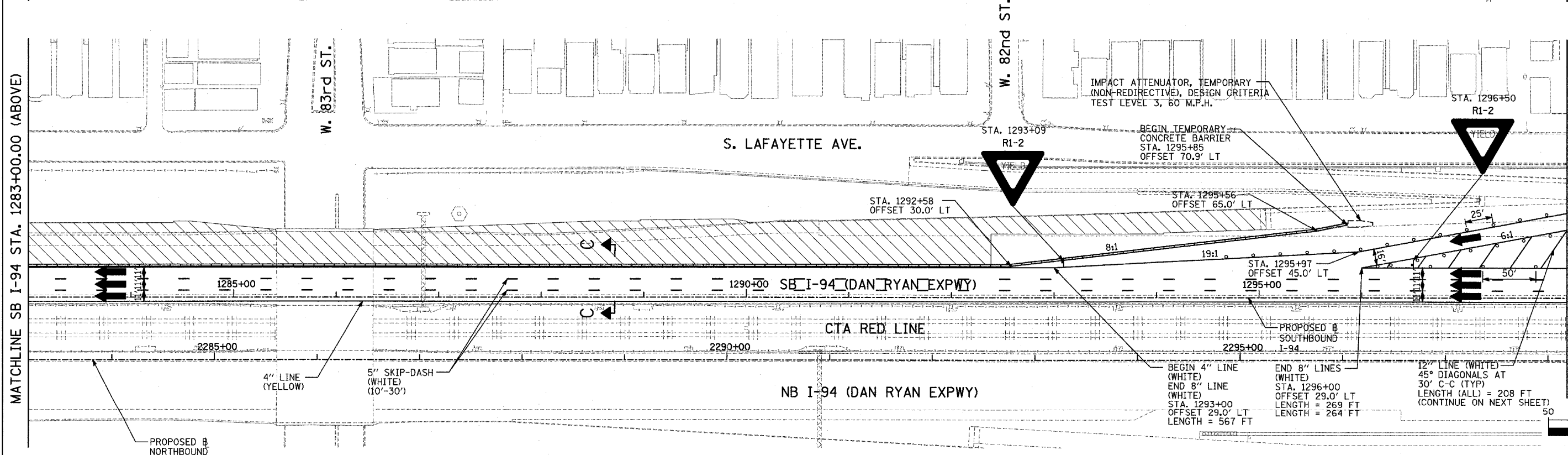
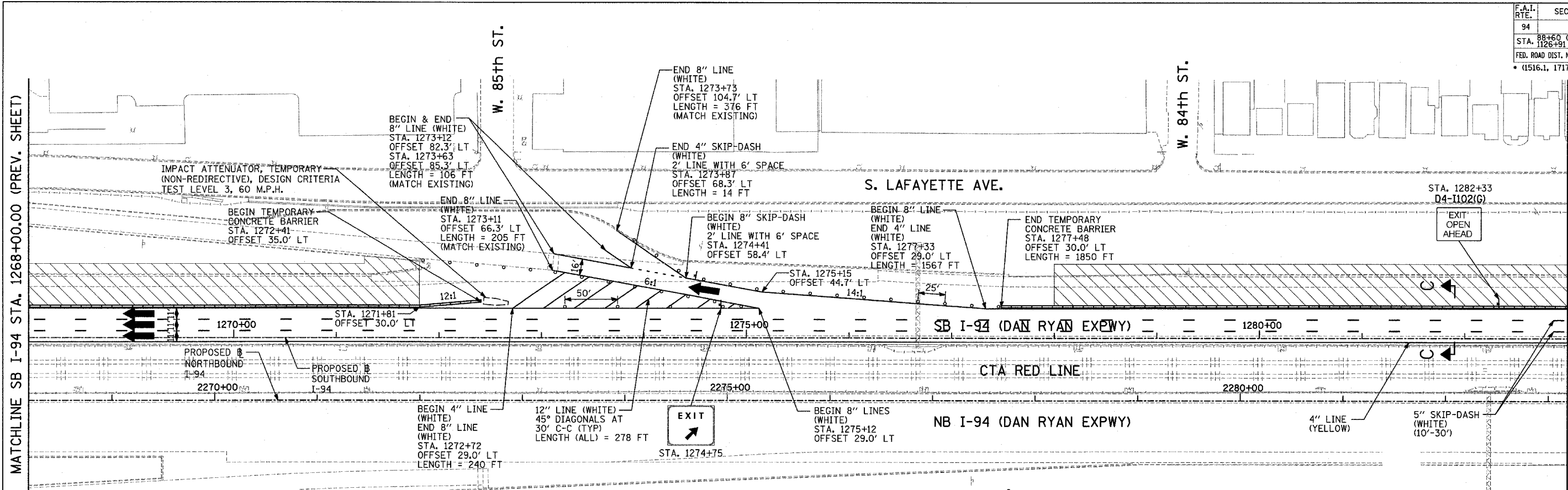
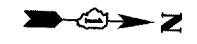
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-94 STA. 1238+00 TO 1268+00
 SHEET 5 OF 7

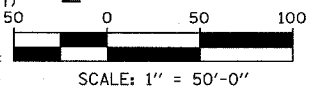
SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	192
SB I-94 (SB I-57) TO STA. 1313+00 (SB RYAN)				
STA. 1126+91 (SB FORD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516.1, 1717, & 1818) R-5				

62593



LOCATION MAP



TYLIN INTERNATIONAL

LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

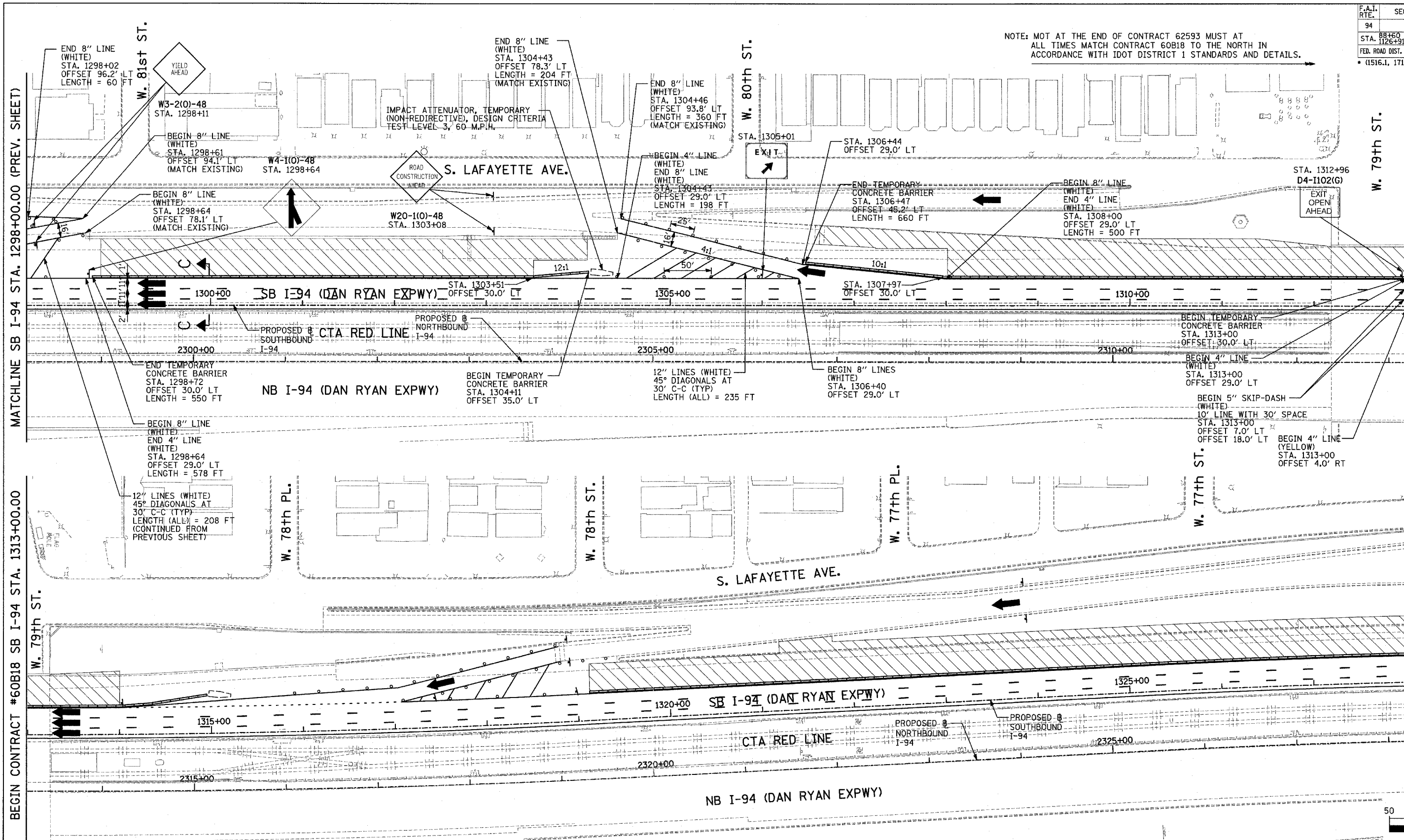
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MAINTENANCE OF TRAFFIC PLAN
STAGE 1 - SB I-94
SB I-94 STA. 1268+00 TO 1298+00
SHEET 6 OF 7

SCALE: 1"=50'-0"
DATE: MARCH 1, 2006

DRAWN BY: JDF
CHECKED BY: RTM

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	193
88+60 (SB I-57) TO STA. 1126+91 (SB FORD)				
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
(1516J, 1717, & 1818) R-5 62593				

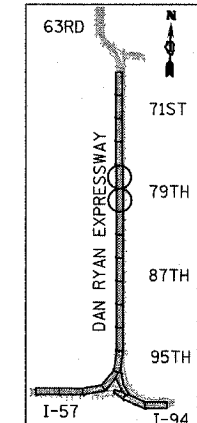
NOTE: MOT AT THE END OF CONTRACT 62593 MUST AT ALL TIMES MATCH CONTRACT 60B18 TO THE NORTH IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.



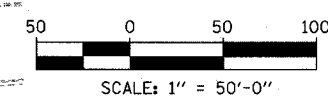
MATCHLINE SB I-94 STA. 1298+00.00 (PREV. SHEET)

BEGIN CONTRACT #60B18 SB I-94 STA. 1313+00.00

END CONTRACT #62593 SB I-94 STA. 1313+00.00



LOCATION MAP



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

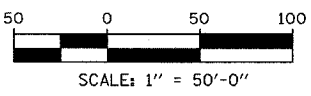
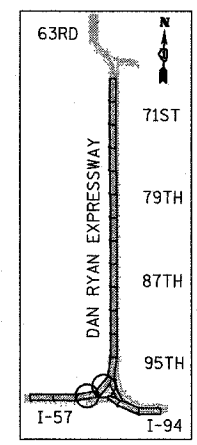
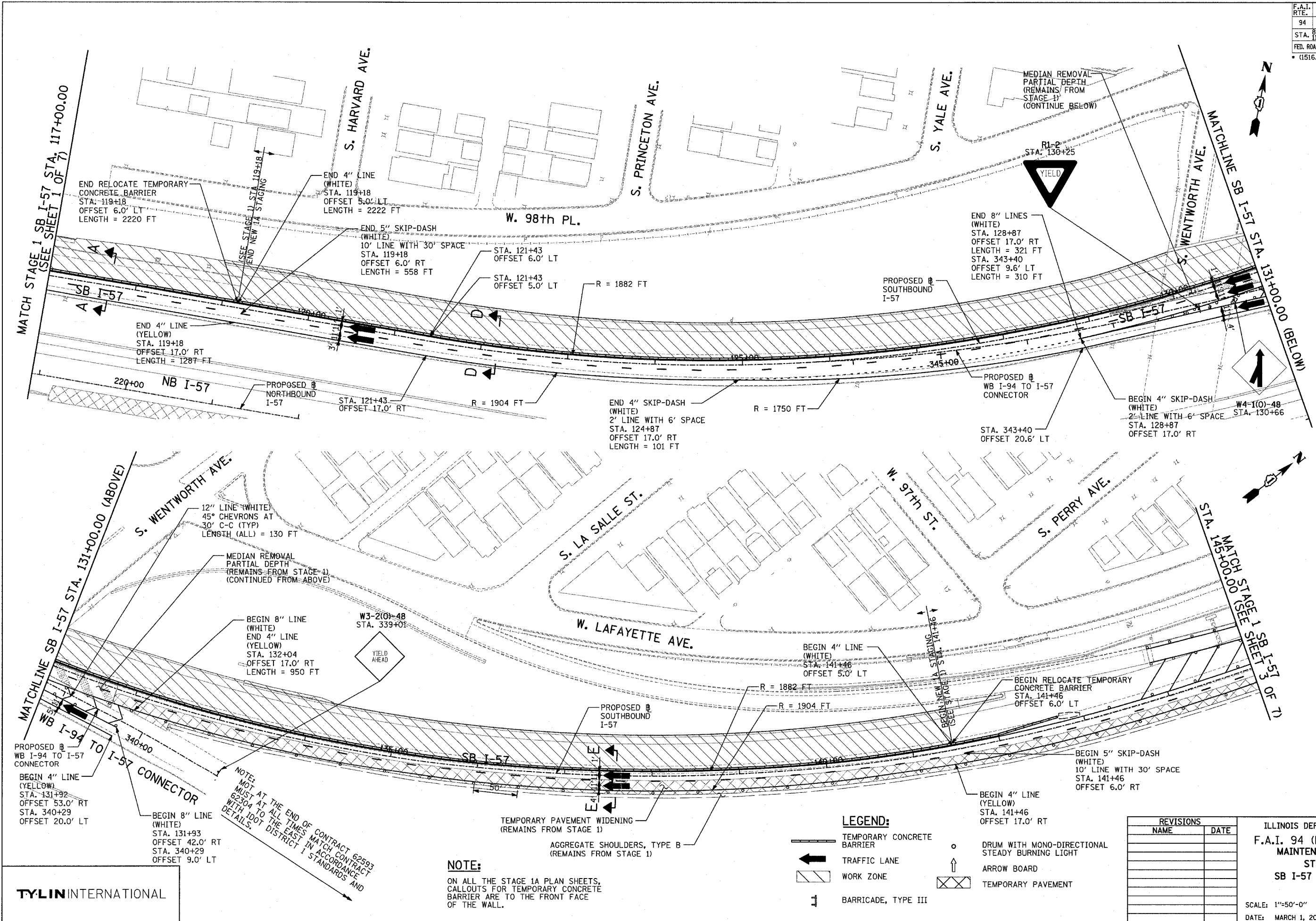
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MAINTENANCE OF TRAFFIC PLAN
 STAGE 1 - SB I-94
 SB I-94 STA. 1298+00 TO 1328+00
 SHEET 7 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

TYLIN INTERNATIONAL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	764	194
STA. 1126+91 (SB FORD) TO STA. 1313+00 (SB RYAN)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
* (1516.1, 1717, & 1818) R-5				
				62593



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

NOTE:
ON ALL THE STAGE 1A PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

NOTE:
NOT AT THE END OF CONTRACT 62593 MUST AT ALL TIMES MATCH CONTRACT 62304 TO THE EAST IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

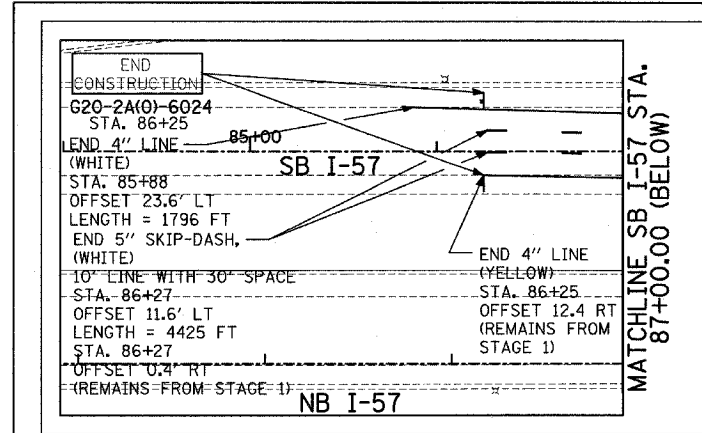
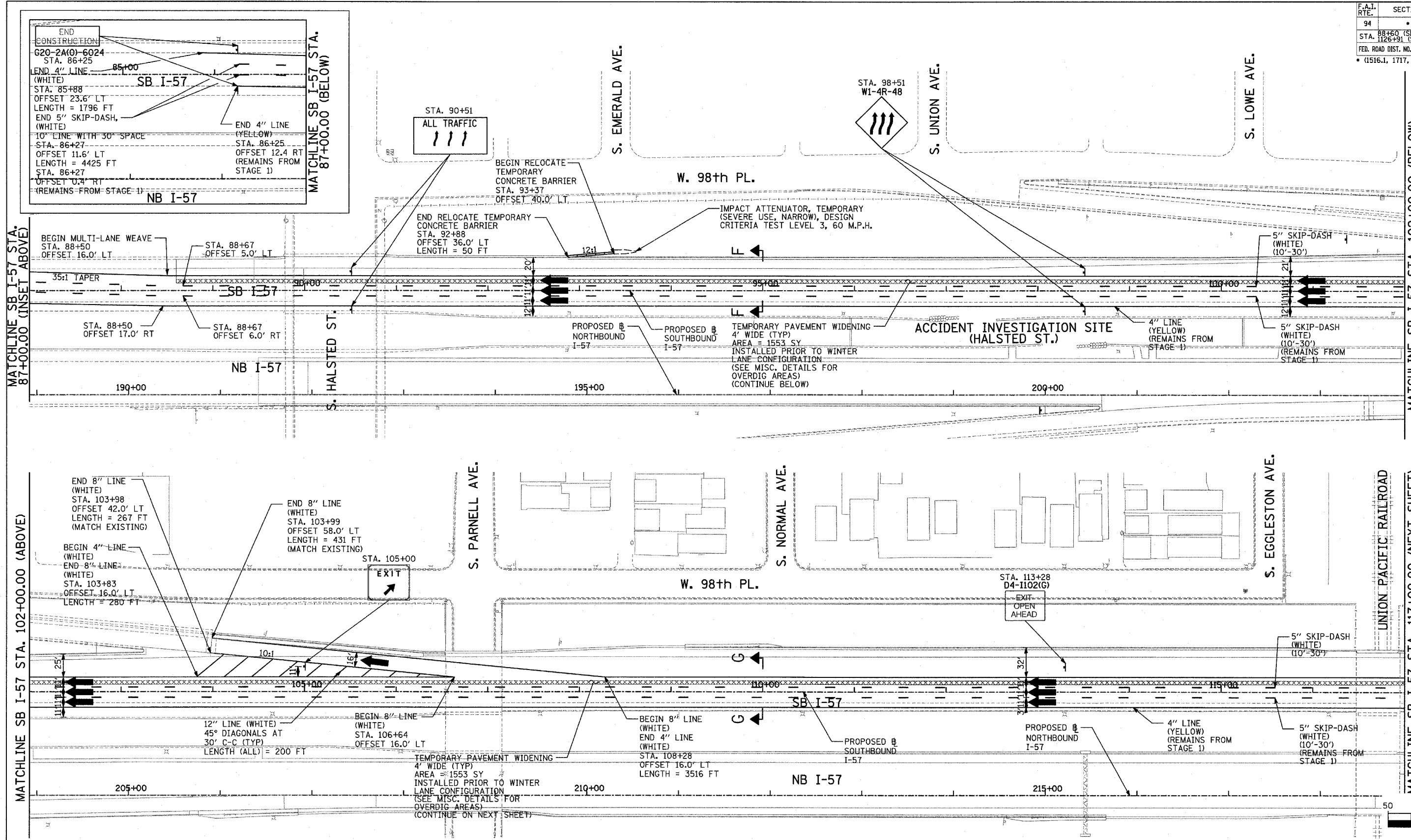
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MAINTENANCE OF TRAFFIC PLAN
STAGE 1A - SB I-94
SB I-57 STA. 117+00 TO 145+00
SHEET 1 OF 1

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006

DRAWN BY: JDF
 CHECKED BY: RTM

TYLIN INTERNATIONAL

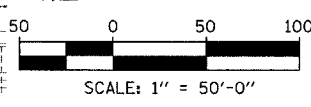
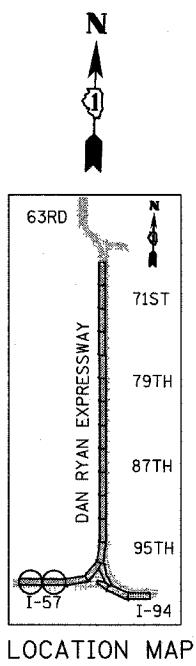


MATCHLINE SB I-57 STA. 87+00.00 (INSET ABOVE)

MATCHLINE SB I-57 STA. 102+00.00 (BELOW)

MATCHLINE SB I-57 STA. 102+00.00 (ABOVE)

MATCHLINE SB I-57 STA. 117+00.00 (NEXT SHEET)



NOTE:
ON ALL THE WINTER LANE CONFIGURATION PLAN SHEETS, CALLOUTS FOR TEMPORARY CONCRETE BARRIER ARE TO THE FRONT FACE OF THE WALL.

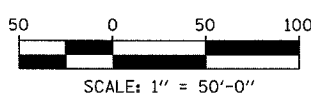
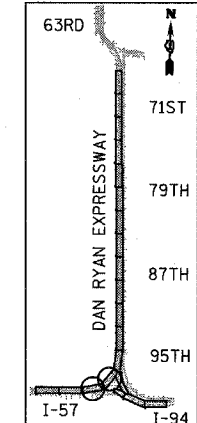
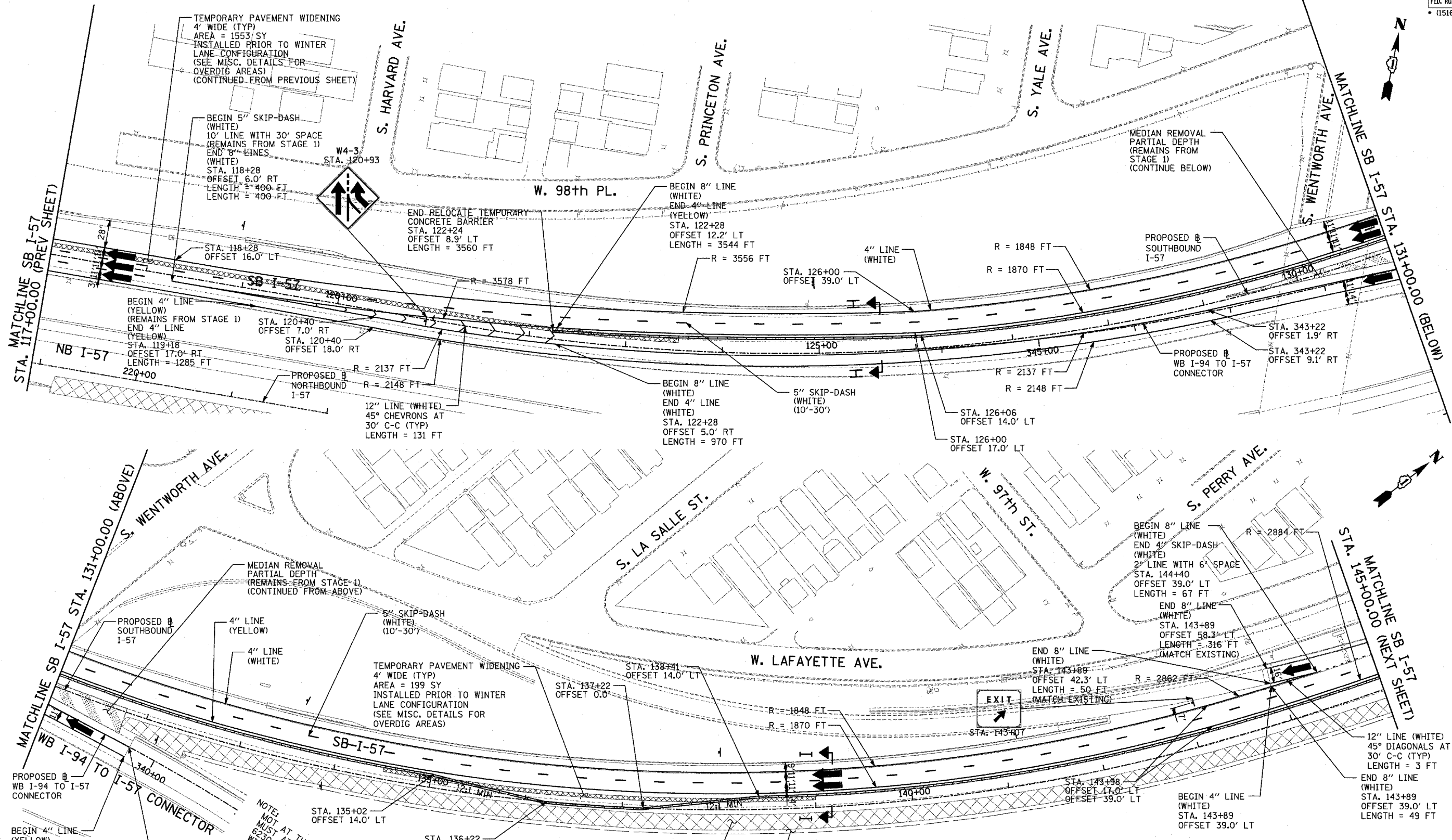
- LEGEND:**
- TEMPORARY CONCRETE BARRIER
 - TRAFFIC LANE
 - WORK ZONE
 - BARRICADE, TYPE III
 - DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
 - ARROW BOARD
 - TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MAINTENANCE OF TRAFFIC PLAN
WINTER LANE CONFIGURATION - SB I-94
SB I-57 STA. 87+00 TO 117+00
SHEET 1 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

TYLIN INTERNATIONAL



NOTE: MOI AT THE END OF CONTRACT 62593 MUST AT ALL TIMES MATCH CONTRACT 62304 TO THE EAST IN ACCORDANCE WITH IDOT DISTRICT 1 STANDARDS AND DETAILS.

LEGEND:

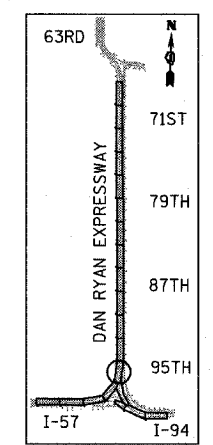
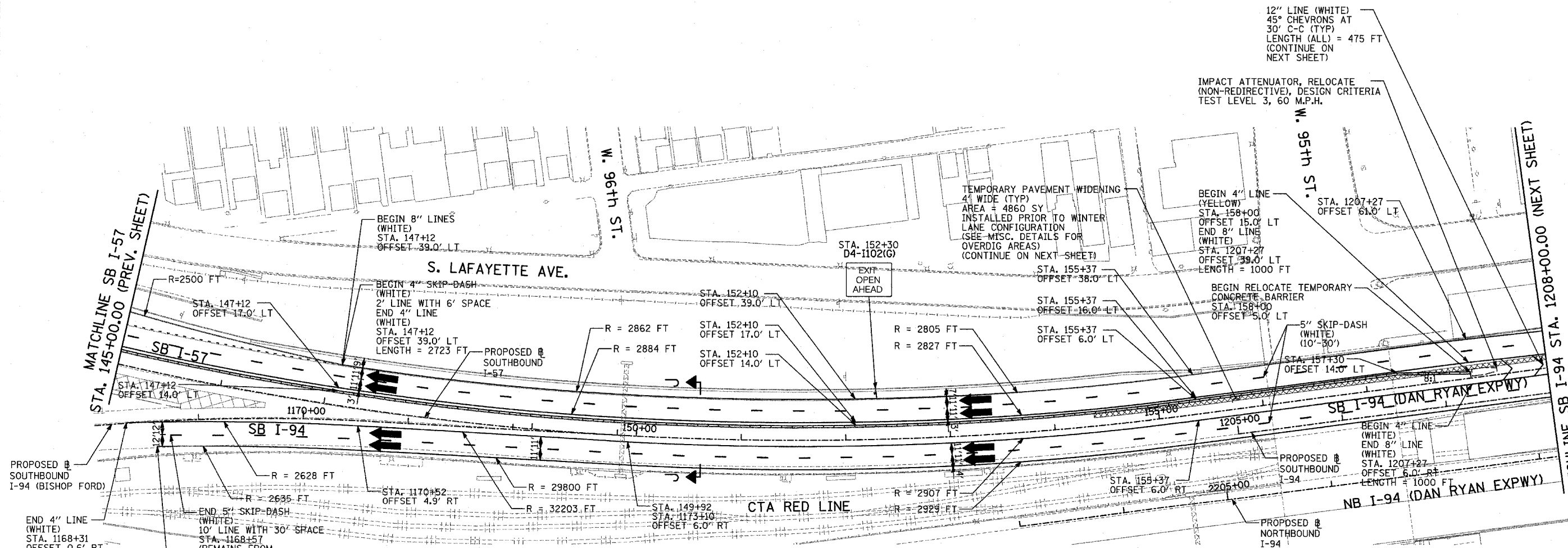
- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

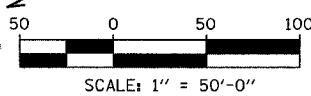
ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 WINTER LANE CONFIGURATION - SB I-57
 SB I-57 STA. 117+00 TO 145+00
 SHEET 2 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

TYLIN INTERNATIONAL



LOCATION MAP



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. 94 (DAN RYAN EXPRESSWAY)
 MAINTENANCE OF TRAFFIC PLAN
 WINTER LANE CONFIGURATION - SB I-94
 SB I-57 STA. 145+00 TO I-94 STA. 1208+00
 SHEET 3 OF 7

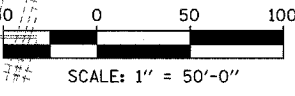
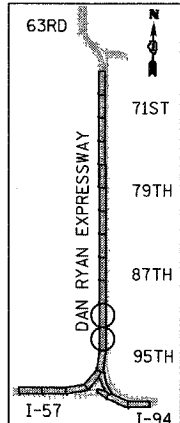
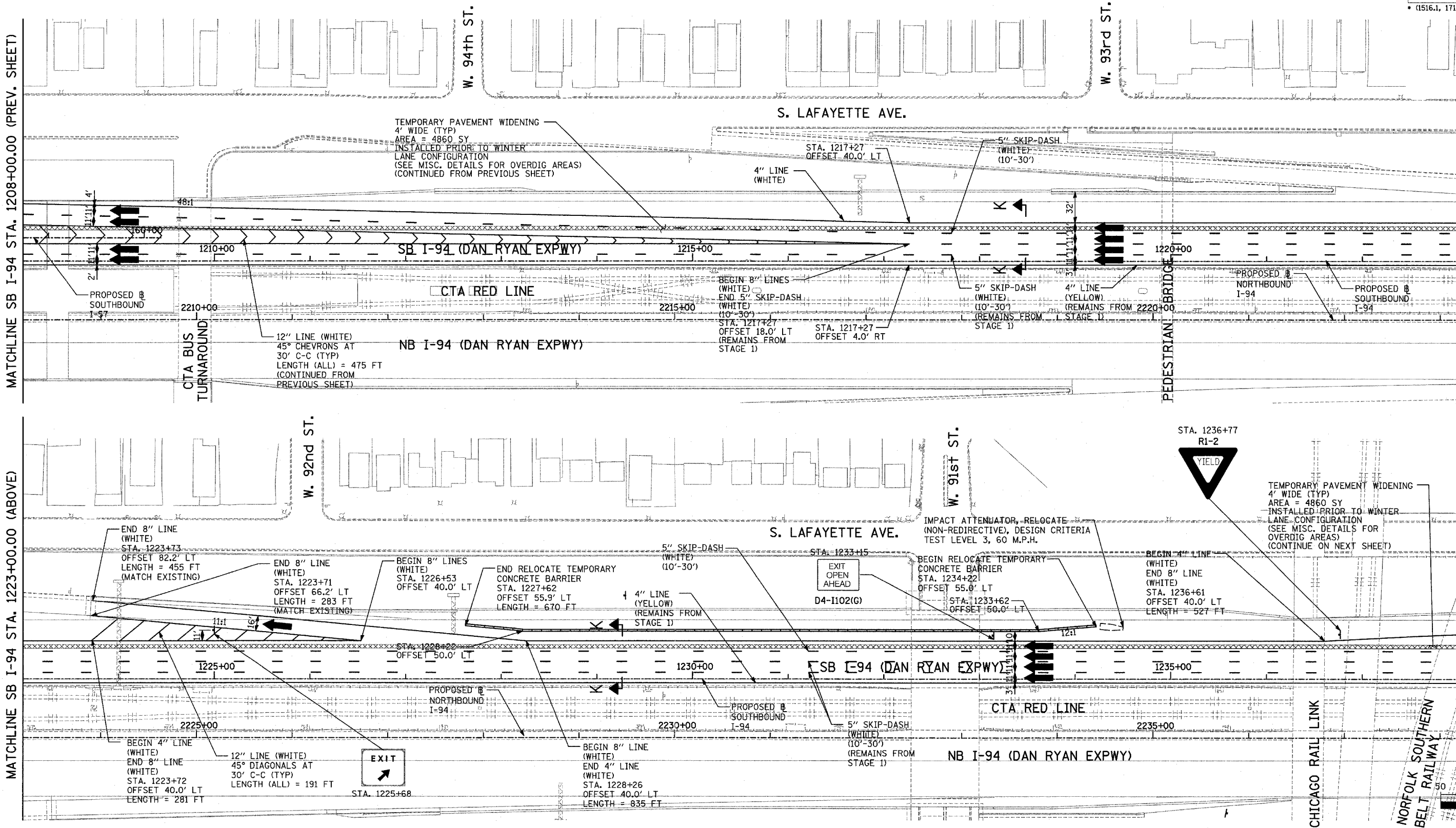
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 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

MATCHLINE SB I-94 STA. 1208+00.00 (PREV. SHEET)

MATCHLINE SB I-94 STA. 1223+00.00 (BELOW)

MATCHLINE SB I-94 STA. 1223+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1238+00.00 (NEXT SHEET)



TYLIN INTERNATIONAL

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ILLINOIS DEPARTMENT OF TRANSPORTATION

F.A.I. 94 (DAN RYAN EXPRESSWAY)

MAINTENANCE OF TRAFFIC PLAN

WINTER LANE CONFIGURATION - SB I-94

SB I-94 STA. 1208+00 TO 1238+00

SHEET 4 OF 7

SCALE: 1"=50'-0"

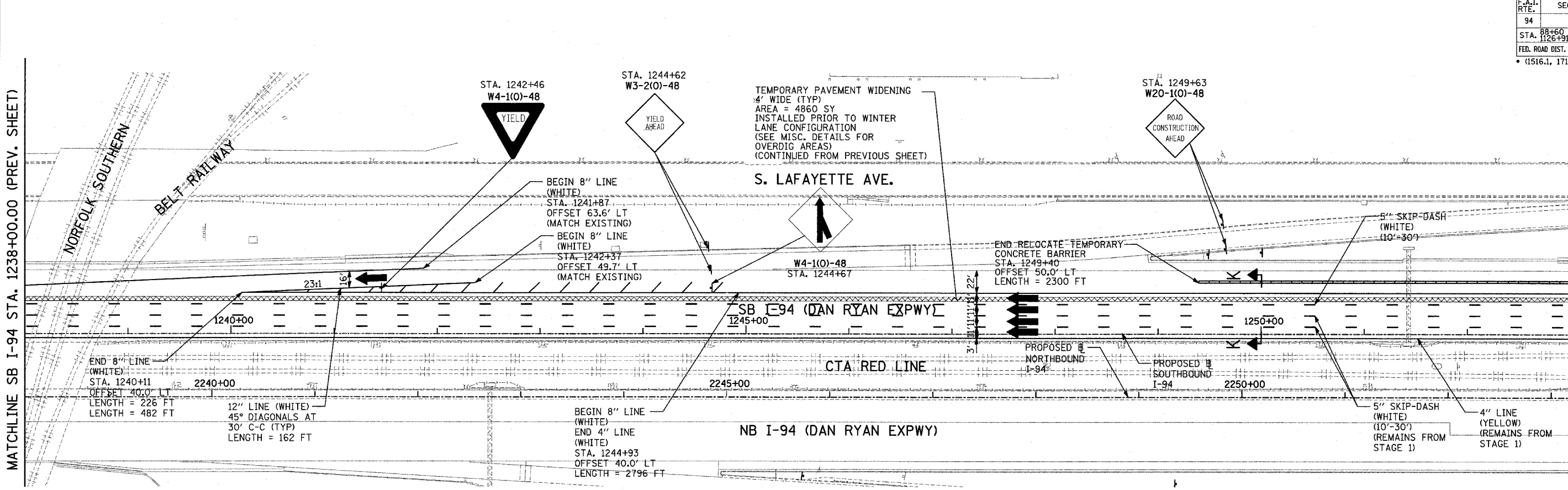
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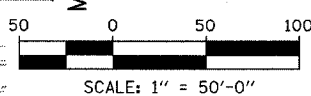
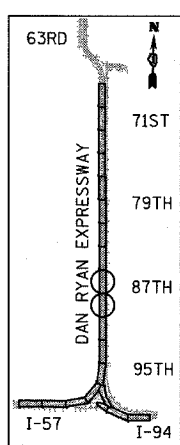
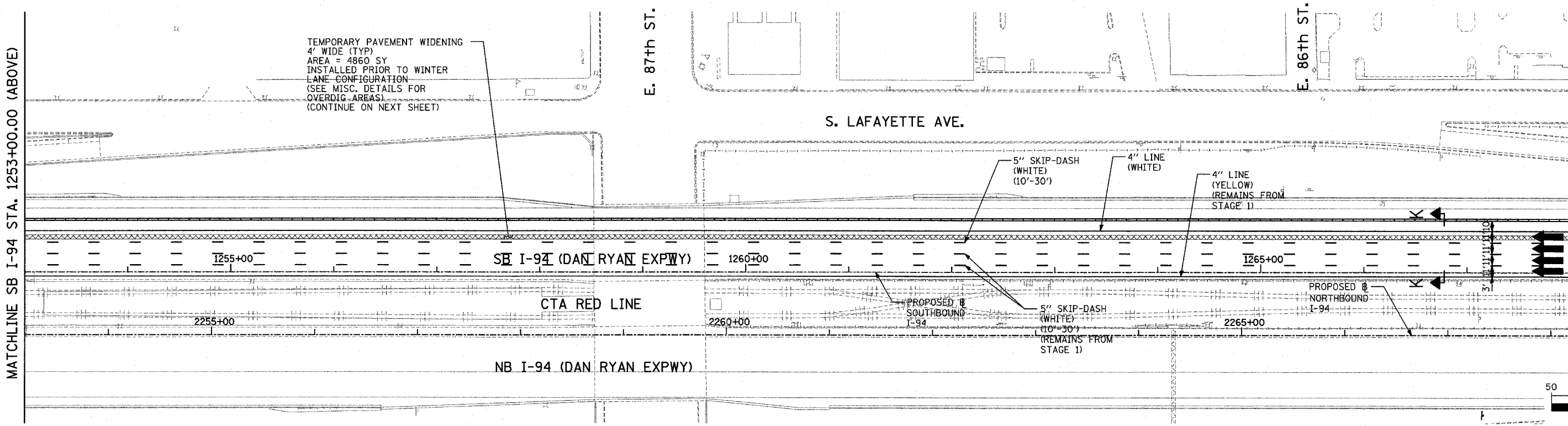
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MATCHLINE SB I-94 STA. 1253+00.00 (BELOW)



MATCHLINE SB I-94 STA. 1253+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1268+00.00 (NEXT SHEET)



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

REVISIONS	
NAME	DATE

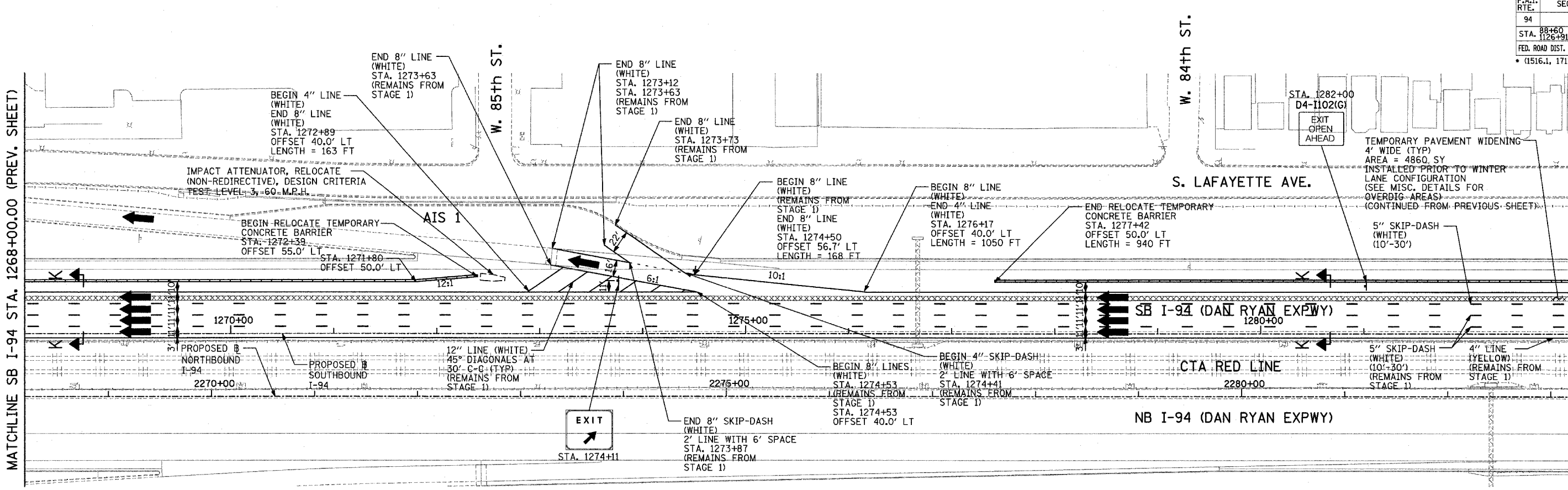
ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. 94 (DAN RYAN EXPRESSWAY)
MAINTENANCE OF TRAFFIC PLAN
WINTER LANE CONFIGURATION - SB I-94
SB I-94 STA. 1238+00 TO 1268+00
SHEET 5 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

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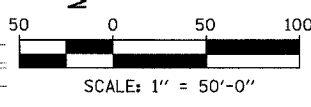
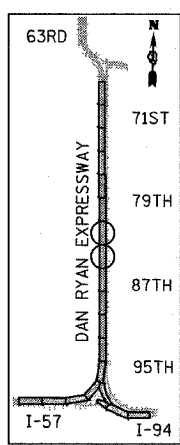
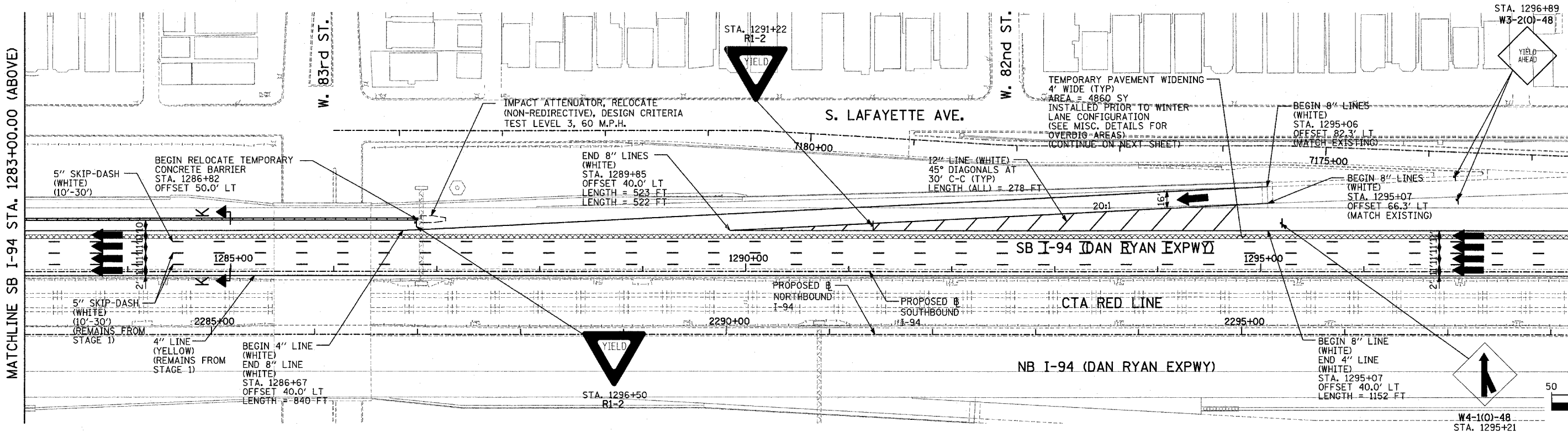
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MATCHLINE SB I-94 STA. 1283+00.00 (BELOW)



MATCHLINE SB I-94 STA. 1283+00.00 (ABOVE)

MATCHLINE SB I-94 STA. 1298+00.00 (NEXT SHEET)



LEGEND:

- TEMPORARY CONCRETE BARRIER
- TRAFFIC LANE
- WORK ZONE
- BARRICADE, TYPE III
- DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- ARROW BOARD
- TEMPORARY PAVEMENT

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 MAINTENANCE OF TRAFFIC PLAN
 WINTER LANE CONFIGURATION - SB I-94
 SB I-94 STA. 1268+00 TO 1298+00
 SHEET 6 OF 7

SCALE: 1"=50'-0"
 DATE: MARCH 1, 2006
 DRAWN BY: JDF
 CHECKED BY: RTM

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