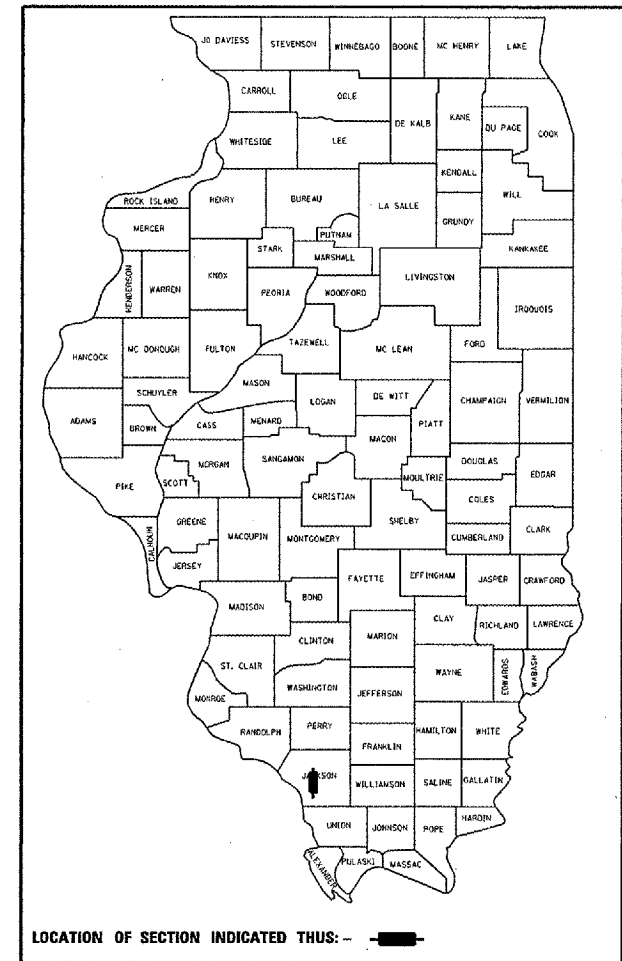


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	.	JACKSON	13	1

\*D9 SMART FY 05-1 C.N. 98844

D-99-002-04



LOCATION OF SECTION INDICATED THUS: - ■ -

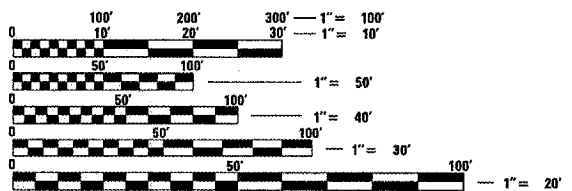
**PROPOSED  
HIGHWAY PLANS**

FAS ROUTE 1909 (ILL 127)  
SECTION D9 SMART FY 05-1  
PROJECT: MA-1909(110)  
JACKSON COUNTY  
C-99-007-04

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR INDEX OF SHEETS, SEE SHEET NO. 2

ADT: 4100 (2003)  
PV = 3750  
SU = 50  
MU = 300  
Trucks = 6.3%

TOWNSHIPS: MURPHYSBORO AND POMONA



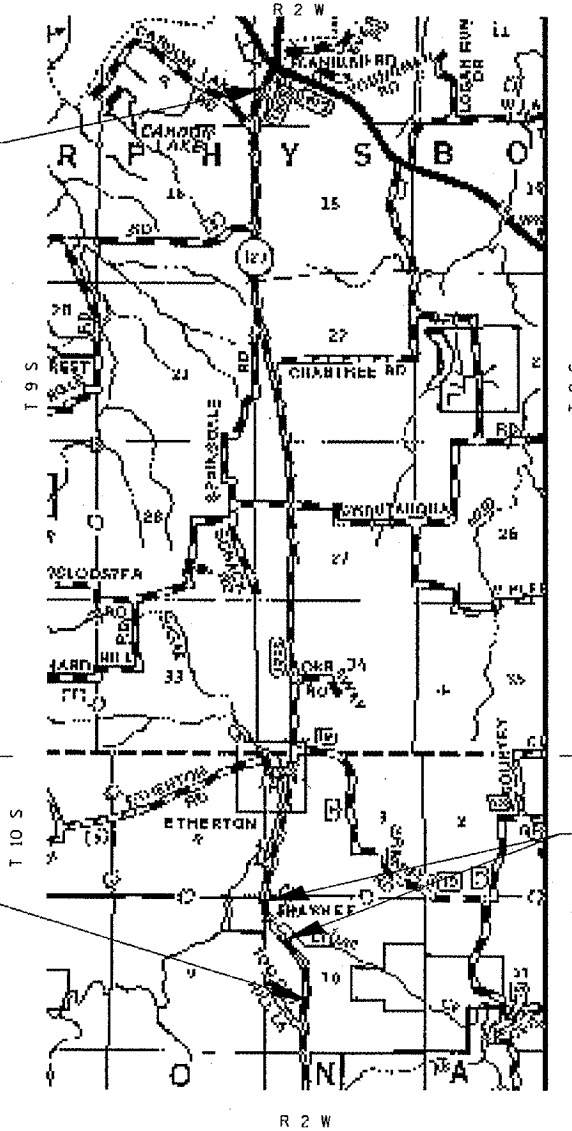
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

CONTRACT NO. 98844

PROPOSED IMPROVEMENT BEGINS  
STA. 0+82 (FAS 1909 - IL 127)

PROPOSED IMPROVEMENT ENDS  
STA. 335+53 (FAS 1909 - IL 127)



STR. NO. 039-0060 (CEDAR CREEK BRIDGE)  
OMIT STA. 300+09.66 TO STA. 301+16.66

PROJECT LENGTH:  
NET LENGTH = 33,364 FT. = 6.318 MILES  
GROSS LENGTH = 33,471 FT. = 6.339 MILES

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED *Feb 9, 2006*  
*Mark C. Lami*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

*March 24, 2006*  
*Mike Hine*  
ENGINEER OF DESIGN AND ENVIRONMENT

*March 24, 2006*  
*Milton L. Sees, P.E.*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS**

PROJECT ENGINEER: CHARLES STEIN (618)351-5210  
DESIGN TECHNICIAN: KATHI MCCORD (618)351-5216

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	SIGNATURE SHEET, INDEX OF SHEETS AND HIGHWAY STANDARDS ; PAVEMENT CORE SCHEDULE; STRUCTURE RATINGS
3	GENERAL NOTES; MIXTURE REQUIREMENTS
4	SUMMARY OF QUANTITIES
5 - 6	TYPICAL SECTIONS
7	PAVEMENT MARKING SCHEDULE
8	SCHEDULES: SIDE ROADS AND ENTRANCES, AND MAINLINE RESURFACING
9	LOCATION MAP FOR FAS 1909 (ILL. 127)
10	SIDE ROAD AND ENTRANCE DETAILS
11	BUTT JOINT DETAILS FOR BEGINNING AND END OF PROJECT
12	BUTT JOINT DETAILS FOR STR. NO. 039-0060
13	STANDARD DETAILS - BIT. CONCRETE TRANSITIONS; ROUGH GROOVED SURFACE SIGN; UNEVEN PAVEMENT SIGN

HIGHWAY STANDARDS

442201-01	701311-02
701011-01	701336-04
701301-02	702001-08
701306-01	780001-01
	781001-02

PAVEMENT CORE SCHEDULE  
(FOR INFORMATION ONLY)

STATION	EX. BIT. SURF. THICKNESS	EX PAVEMENT THICKNESS
FAS 1909 (ILL. 127) *		
LT. 3+93	3.5"	VAR. CR. ST.
LT. 56+75	4"	2" CR. ST. & 3" BIT.
LT. 109+53	4.5"	3" CR. ST. & 3" BIT.
LT. 162+33	5"	VAR. CR. ST.
LT. 215+13	3"	3" CR. ST. & 2" BIT.
LT. 267+93	3.5"	4" CR. ST. & 2.5" BIT.
LT. 320+73	3.5"	4" CR. ST. & 2.5" BIT.
LT. 334+75	3.5"	3" CR. ST. & 2" BIT.
RT. 3+93	5"	VAR. CR. ST.
RT. 56+75	3.75"	VAR. CR. ST.
RT. 109+53	4.5"	2" CR. ST. & 3" BIT.
RT. 162+33	4"	VAR. CR. ST.
RT. 215+13	3.5"	VAR. CR. ST.
RT. 267+93	5"	VAR. CR. ST.
RT. 320+73	4"	3" CR. ST. & 2" BIT.
RT. 334+75	3.5"	4" CR. ST. & 2" BIT.

\*LIMITS OF CORE SURVEY BEGINS AT INTERSECTION OF ILL. 13/127 AND ENDS NORTH OF TOM CAT HILL LANE.

STRUCTURE RATINGS

STRUCTURE NO.	INV. RATING	OPER. RATING
FAS 1909 (ILL. 127)		
039-0060	33.3	55.0

Prepared By: *Joe Zlaniewicz*  
DISTRICT STUDIES & PLANS ENGINEER

Examined By: *James Travis Emory*  
DISTRICT LAND ACQUISITION ENGINEER

Examined By: *Carrie Nelson*  
DISTRICT PROGRAM DEVELOPMENT ENGINEER

Examined By: *Jim Smothers*  
DISTRICT OPERATIONS ENGINEER

Examined By: *Joseph Linn*  
DISTRICT CONSTRUCTION ENGINEER

Examined By: *Bruce W. Peoples*  
DISTRICT MATERIALS ENGINEER

Examined By: *Dan L. Carter*  
DISTRICT PROJECT IMPLEMENTATION ENGINEER

Examined By: *Dan L. Carter*  
ASSISTANT REGIONAL ENGINEER

Approved By: *Mary L. Lane*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

DATE *Feb 9 2004*

### GENERAL NOTES

THE THICKNESS OF BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED.

FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES EXCEPT FOR QC/QA OF BITUMINOUS MIXTURES:

ALL BITUMINOUS CONCRETE.....2.016 TONS/CU.YD.  
 ALL AGGREGATE.....2.05 TONS/CU.YD.  
 BITUMINOUS MATERIALS (PRIME COAT)  
     ON PAVEMENT.....0.09 GALS./SQ. YD.  
     ON AGG. SURFACE.....0.32 GALS./SQ. YD.  
 AGGREGATE (PRIME COAT).....0.0015 TONS/SQ. YD.

THE QUANTITY OF SHORT TERM PAVEMENT MARKING SHOWN IN THE PLANS WERE BASED ON ONE APPLICATION EACH FOR THE PRIME COAT AND SURFACE COURSE.

PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS, THE RESIDENT ENGINEER SHOULD CONTACT THE BUREAU OF OPERATIONS AND ARRANGE FOR INSPECTION AND APPROVAL OF THE PAVEMENT MARKING LAYOUT.

THE CONTRACTOR SHALL STAMP STATIONING IN THE PROPOSED BITUMINOUS MATS AT 300 FT. INTERVALS ON ALTERNATING SIDES OF THE PAVEMENT AND AS DIRECTED BY THE ENGINEER. THE STATION SYMBOL STAMPS USED SHALL BE FURNISHED BY THE CONTRACTOR. THEY SHALL BE 5/2" TALL OF A DESIGN APPROVED BY THE ENGINEER AND SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, BITUMINOUS RESURFACING SHALL BE PLACED IN A SEQUENCE THAT WILL MINIMIZE THE TIME THE CENTER-LINE EDGE IS EXPOSED TO TRAFFIC.

QUANTITIES SHOWN IN THE PLANS FOR PATCHING ARE ESTIMATES. THE ACTUAL AMOUNT OF PATCHING REQUIRED SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

RECLAIMED ASPHALT PAVEMENT (RAP) WILL NOT BE ALLOWED FOR USE AS AGGREGATE IN AGGREGATE SHOULDERS, TYPE B.

PROPOSED AGGREGATE SHOULDERS, TYPE B SHALL BUTT INTO EXISTING BITUMINOUS SHOULDERS AT EXISTING STRUCTURES WHERE BITUMINOUS SHOULDERS ARE PRESENT.

ANY EXISTING SIGNS WHICH INTERFERE WITH CONSTRUCTION SHALL BE REMOVED, STORED AND RE-ERECTED AS DIRECTED BY THE ENGINEER IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL COMPLETE ALL PATCHING PRIOR TO THE BITUMINOUS SURFACE REMOVAL.

#### COMMITMENTS:

A COMMITMENT HAS BEEN MADE THAT THE RESIDENT ENGINEER WILL REPORT THE QUANTITIES AND COSTS OF THE PATCHING LOCATED AT RT. STA. 238+50, NEAR GRAMMER RD., TO THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DISTRICT 9, SAFETY AND CLAIMS MANAGER. THIS COMMITMENT IS AS OF JULY 27, 2005.

BITUMINOUS MIXTURE DESIGNS SHALL BE PREPARED AS DIRECTED BELOW:

#### SURFACE COURSE AND LEVEL BINDER REQUIREMENTS:

Mixture Use(s):	Bituminous Concrete Surface Course (Superpave), Mix. C, N90
AC/PG:	PG64-22
RAP % (Max.):	10
Design Air Voids:	4.0%, 90 Gyration Superpave Design
Mixture Composition: (Gradation Mixture)	IL-9.5mm or IL-12.5 mm
Friction Aggregate:	Mixture C

#### PATCHING REQUIREMENTS:

Mixture Use(s):	Bituminous Concrete Surface Course (Superpave), Mix. C, N90
AC/PG:	PG64-22
RAP % (Max.):	10
Design Air Voids:	4.0%, 90 Gyration Superpave Design
Mixture Composition: (Gradation Mixture)	IL-19.0mm
Friction Aggregate:	None

#### SIDE ROADS AND ENTRANCES REQUIREMENT:

Pay Item:	Incidental Bituminous Surface
Mixture Use(s):	Bituminous Concrete Surface Course (Superpave), Mix. C, N90
AC/PG:	PG64-22
RAP % (Max.):	10
Design Air Voids:	4.0%, 90 Gyration Superpave Design
Mixture Composition: (Gradation Mixture)	IL-9.5mm or IL-12.5 mm
Friction Aggregate:	Mixture C

#### BITUMINOUS SHOULDERS REQUIREMENT:

Pay Item:	Bituminous Shoulders
Mixture Use(s):	Bituminous Concrete Surface Course (Superpave), Mix. C, N90
AC/PG:	PG64-22
RAP % (Max.):	10
Design Air Voids:	4.0%, 90 Gyration Superpave Design
Mixture Composition: (Gradation Mixture)	IL-9.5mm or IL-12.5 mm
Friction Aggregate:	Mixture C

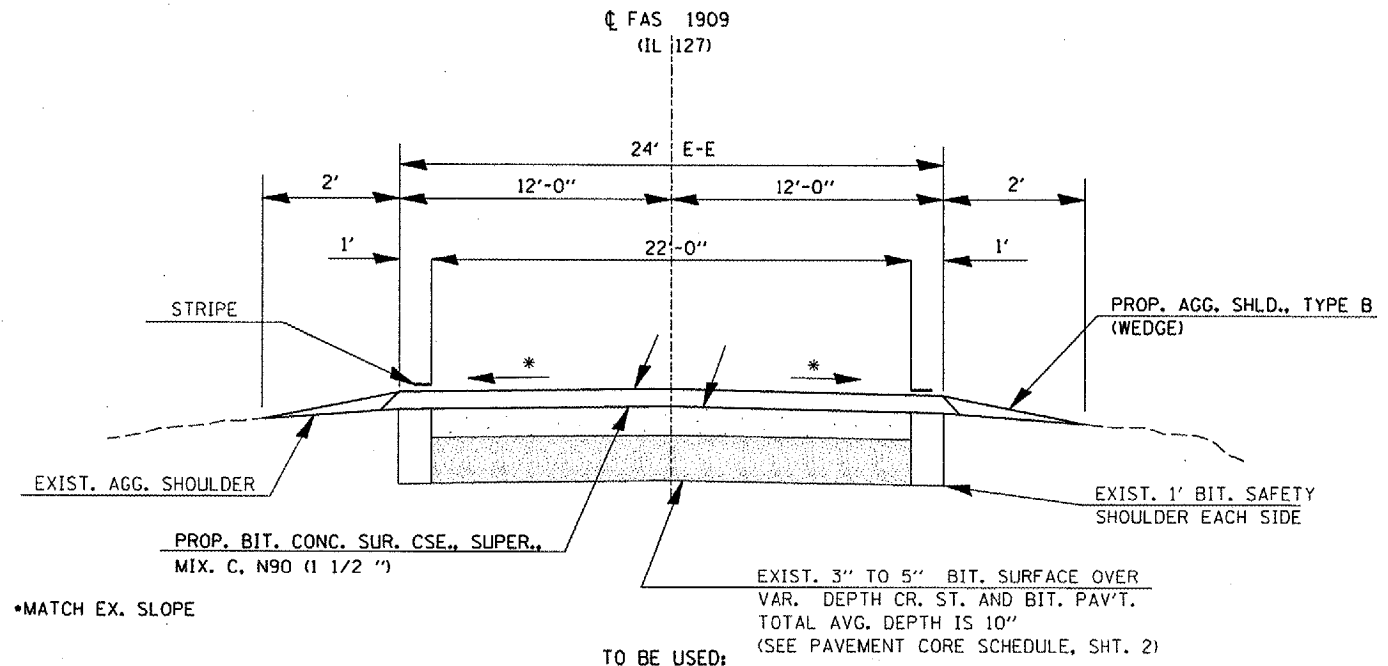
## SUMMARY OF QUANTITIES

CODE NUMBER	ITEM DESCRIPTION CONSTRUCTION TYPE CODE	UNIT	FAS 1909
			STP-RURAL-STATE 80% FED / 20% STATE JACKSON COUNTY QUANTITY .1000
X0322729	MATERIAL TRANSFER DEVICE	TON	7,660.0
X4066418	BITUMINOUS CONCRETE SURFACE COURSE, SUPERPAVE, MIX C, N90	TON	7,513.0
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	8,423.0
40600300	AGGREGATE (PRIME COAT)	TON	137.0
40600570	LEVELING BINDER (MACHINE METHOD), MIXTURE C, TYPE 2	TON	44.0
40600895	CONSTRUCTING TEST STRIP	EACH	1.0
40600980	BITUMINOUS SURFACE REMOVAL - BUTT JOINT	SO YD	985.0
40600990	TEMPORARY RAMP	SO YD	65.0
40800040	INCIDENTAL BITUMINOUS SURFACE	TON	160.0
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	15.0
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	75.0
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	37.0
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	630.0
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1,508.0
48201000	BITUMINOUS SHOULDERS	TON	46.0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	2.0
67100100	MOBILIZATION	L SUM	1.0
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1.0
70100600	TRAFFIC CONTROL AND PROTECTION, STANDARD 701336	L SUM	1.0
70300100	SHORT- TERM PAVEMENT MARKING	FOOT	6,674.0
70300220	TEMPORARY PAVEMENT MARKING, LINE 4"	FOOT	114,074.0
70300280	TEMPORARY PAVEMENT MARKING, LINE 24"	FOOT	16.0
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	39,848.0
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	114,074.0
* 78001180	PAINT PAVEMENT MARKING - LINE 24"	FOOT	16.0
* 78100100	RAISED REFLECTIVE PAVEMENT MARKERS	EACH	418.0
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	418.0

\* SPECIALTY ITEMS

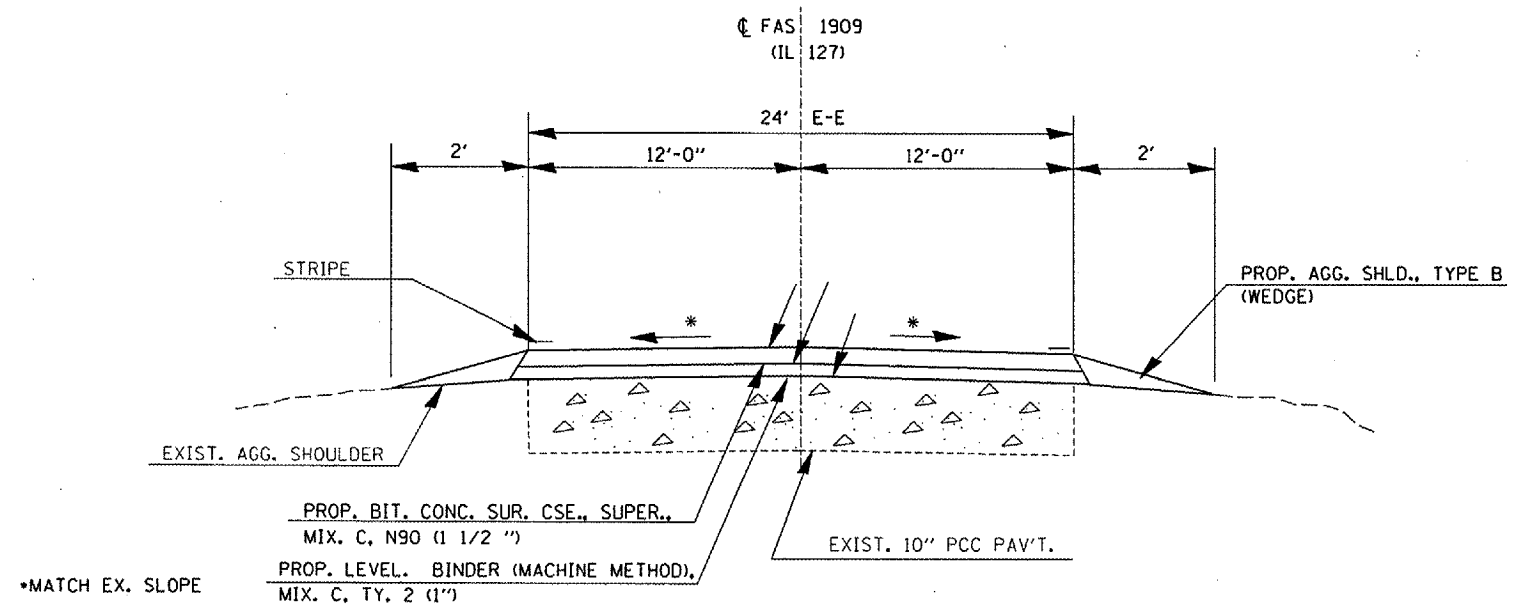
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909		JACKSON	12	5
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*09 SMART FY 05-1				C.N. 98844

### TYPICAL SECTION



TO BE USED:  
 STA. 3+73 TO 146+57  
 STA. 159+91 TO STA. 300+09 (NORTH BUTT JT. S.N. 039-0060)  
 STA. 301+16 (SOUTH BUTT JT. S.N. 039-0060) TO STA. 335+53

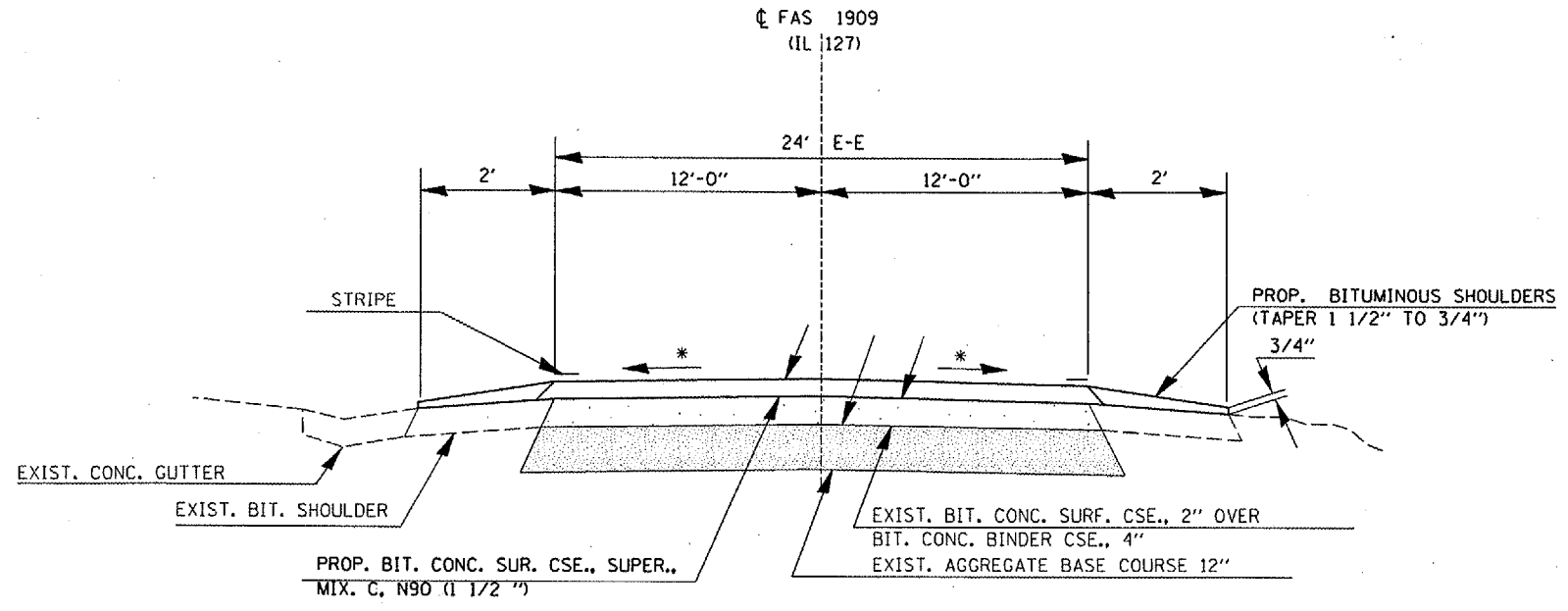
### TYPICAL SECTION



TO BE USED:  
 STA. 0+82 TO STA. 3+73 (END OF PCC PAV'T.)

F. & S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	*	JACKSON	13	6
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

\*D9 SMART FY 05-1 C.N. 98844



•MATCH EX. SLOPE

TO BE USED:  
 STA. 146+57 TO STA. 159+91  
 AT CHAUTAUQUA RD./IL. 127 INTERSECTION

### PAVEMENT MARKING SCHEDULE

STATION TO STATION				PAINT PAVEMENT MARKING				TEMPORARY		NOTES
				4 INCH WHITE	4 INCH YELLOW	4 INCH SKIP-DASH	24 INCH WHITE	4 INCH	24 INCH	
				FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT	
FAS 1909 (IL 127)										
	0+82						16.0		16.0	Stop Bar
	+82.0	TO	300+09.7	58,975.0				58,975.0		Edge line
	+82.0	TO	49+72.0			1,225.0		1,225.0		
RIGHT	49+72.0	TO	58+75.0		903.0	226.0		1,129.0		NP ZONE
	58+75.0	TO	61+55.0			70.0		70.0		
LEFT	61+55.0	TO	67+67.0		612.0			612.0		NP ZONE
	61+55.0	TO	66+09.0			114.0		114.0		
RIGHT	66+09.0	TO	86+84.0		2,075.0			2,075.0		NP ZONE
	67+67.0		75+59.0			198.0		198.0		
LEFT	75+59.0	TO	98+14.0		2,255.0			2,255.0		NP ZONE
	301+16.7	TO	335+53.0	6,873.0				6873.0		Edge line
	86+84	TO	96+29			236.0		236.0		
RIGHT	96+29.0	TO	164+03.0		6,774.0			6,774.0		NP ZONE
	98+14.0	TO	106+27.0			203.0		203.0		
LEFT	106+27.0	TO	182+51.0		7,624.0			7,624.0		NP ZONE
RIGHT	171+69.0	TO	180+88.0		919.0			919.0		NP ZONE
	164+03.0	TO	189+22.0			630.0		630.0		
LEFT	182+51.0	TO	191+91.0		940.0			940.0		NP ZONE
RIGHT	189+22.0	TO	201+78.0		1,256.0			1,256.0		NP ZONE
LEFT	200+25.0	TO	213+61.0		1,336.0			1,336.0		NP ZONE
	191+91.0		200+25.0			209.0		209.0		
	201+78.0		224+65.0			572.0		572.0		
RIGHT	214+67.0		227+76.0		1,309.0			1,309.0		NP ZONE
LEFT	224+65.0		240+70.0		1,605.0			1,605.0		NP ZONE
	227+76.0		289+27.0			1,538.0		1,538.0		
RIGHT	227+82.0	TO	335+53.0		10,771.0			10,771.0		NP ZONE
LEFT	289+27.0		335+53.0		4,626.0			4,626.0		NP ZONE
SUB-TOTALS				65,848.0	43,005.0	5,221.0	16.0	114,074.0	16.0	
TOTALS					114,074		16	114,074	16	

### SIDEROAD SCHEDULE

STATION	SIDE ROAD NAME	SURFACE TYPE	WIDTH AT EDGE OF PAVEMENT	DISTANCE TO END OF SURFACING	WIDTH AT END OF SURFACING	SURFACE AREA	BITUMINOUS SURFACE REMOVAL-BUTT JOINT	BITUMINOUS MATERIAL PRIME COAT	AGGREGATE PRIME COAT	INCIDENTAL BIT. SURFACE
			(FT)	(FT)	(FT)	(SQ YD)	(SQ YD)	(GAL)	(TON)	(TON)
LT 59+14	JONESBORO ROAD	OIL & CHIP	53	15	20	60.83	0.0	91.5	0.024	10.0
RT 59+14	JONESBORO ROAD	OIL & CHIP	58	15	21	65.83	0.0	21.1	0.026	11.0
LT 140+45	CRABTREE ROAD	BIT.	54	15	23	64.17	64.2	5.8	0.026	6.0
LT 153+73	CHAUTAUQUA ROAD	BIT.	82	15	78	101.60	101.6	9.2	0.131	9.0
LT 249+21	CARAWAY ROAD	AGG.	80	15	37	97.50	0.0	31.2	0.039	16.0
LT 276+14	GRAMMER ROAD	BIT.	110	15	55	137.50	137.5	12.4	0.055	12.0
RT 59+66	CARBON LAKE ROAD	OIL & CHIP	68	15	30	81.67	0.0	26.1	0.033	14.0
RT 95+57	HOFFMAN ROAD	BIT.	127	15	92	182.50	182.5	16.4	0.073	15.0
RT 126+19	SPRINGDALE ROAD	BIT.	62	15	28	75.00	75.0	6.8	0.030	6.0
RT 153+79	ORCHARD HILL ROAD	BIT.	94	15	40	97.50	97.5	8.8	0.168	8.0
RT 281+95	ETHERTON ROAD	BIT.	92	15	36	106.67	106.7	9.7	0.043	9.0
<b>TOTALS:</b>							<b>765</b>	<b>239</b>	<b>0.65</b>	<b>116</b>

### ENTRANCE SCHEDULE

NUMBER	ENTRANCE TYPE	SURFACE TYPE	WIDTH AT EDGE OF PAVEMENT	DISTANCE TO END OF SURFACING	WIDTH AT END OF SURFACING	SURFACE AREA	BITUMINOUS SURFACE REMOVAL-BUTT JOINT	BITUMINOUS MATERIAL PRIME COAT	AGGREGATE PRIME COAT	INCIDENTAL BIT. SURFACE	AGGREGATE WEDGE
			(FT)	(FT)	(FT)	(SQ YD)	(SQ YD)	(GAL)	(TON)	(TON)	(TON)
89	PRIVATE ENTRANCE	AGGREGATE	12	4	12	475		152.0	0.7	40.00	7
2	COMERCIAL ENTRANCE	AGGREGATE	35	4	35	31		10.0	0.10	2.00	0
2	COMERCIAL ENTRANCE	BITUMINOUS	35	4	35	31	31	3.0	0.10	2.00	1
10	FIELD ENTRANCE	SOD									
<b>TOTALS:</b>							<b>31</b>	<b>165</b>	<b>0.9</b>	<b>44</b>	<b>8</b>

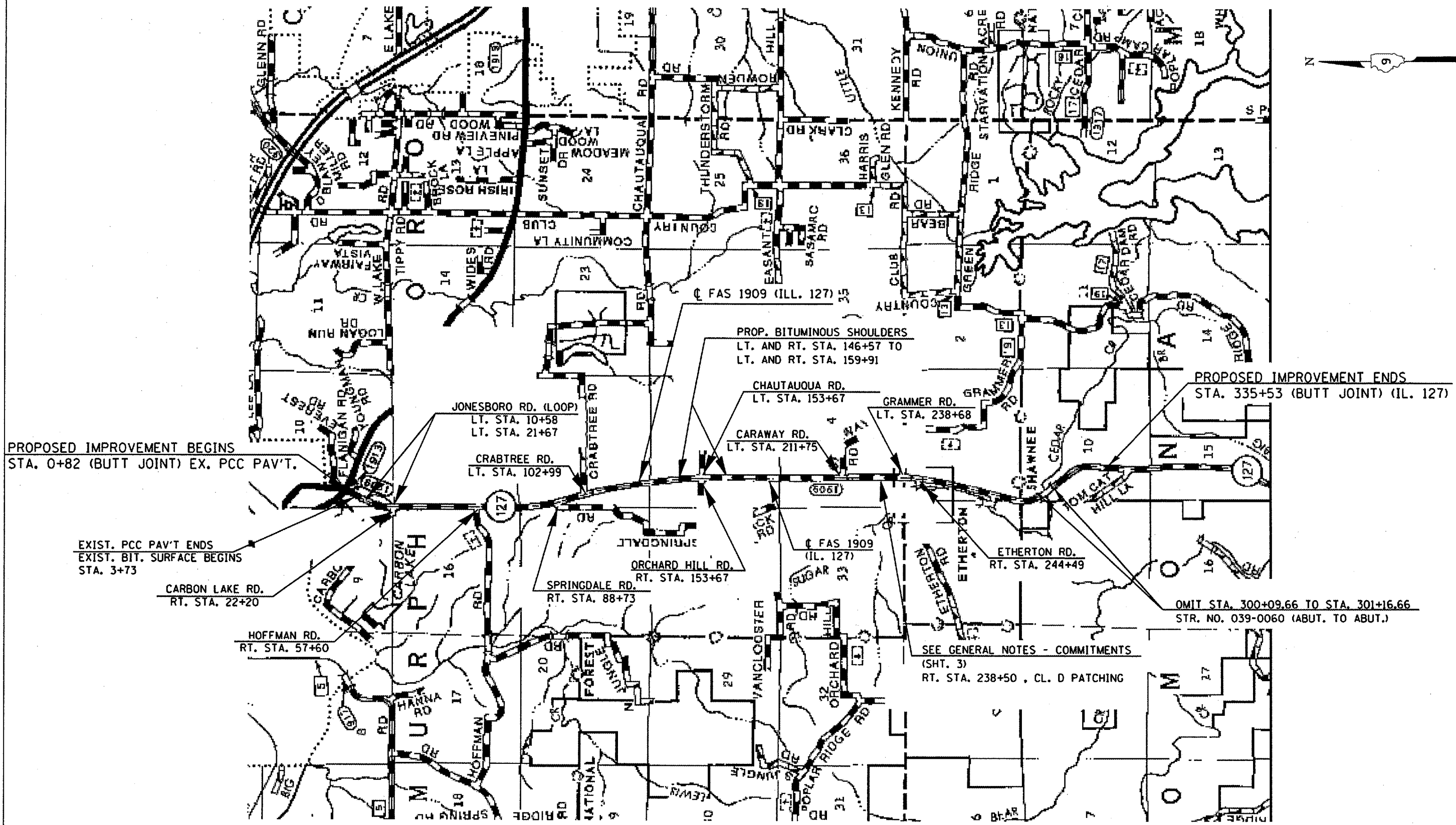
\* AGGREGATE WEDGE QUANTITIY TO BE INCLUDED IN COST OF AGGREGATE SHOULDER, TY. B.

### MAINLINE RESURFACING SCHEDULE

STATION TO STATION		LENGTH	ROADWAY WIDTH	ROADWAY AREA	BIT. SURF. REM. BUTT JOINT	TEMPORARY RAMP	BIT. SURF. CSE. SUPERPAVE. MIX C. N 90	BIT. MAT. PRIME COAT	AGG. PRIME COAT	AGG. SHOULDER WIDTH	AGG. SHOULDER TYPE B	BITUMINOUS SHOULDERS	LEVEL. BINDER (MM), MIX. C. T2
		FOR INFORMATIONAL PURPOSE ONLY											
		(FT)	(FT)	(SQ YD)	(SQ YD)	(SQ YD)	(TON)	(GAL)	(TON)	(FT)	(TON)	(TON)	(TON)
<b>FAS 1909 (IL 127)</b>													
LT & RT	0+82 TO 3+73	291	24	776	27	23	66	71	1	2	10	0	44
LT & RT	3+73 TO 146+57	14,284	24	38,091			3,216	3,464	58	2	640	0	0
LT & RT	146+57 TO 159+91	1,334	24	3,557			300	323	5	2		46	0
LT & RT	159+91 TO 300+09	14,018	24	37,381	54	14	3,156	3,399	57	2	650	0	0
LT & RT	300+09 TO 301+16												
LT & RT	301+16 TO 335+53	3,437	24	9,166	108	28	774	834	14	2	200	0	0
<b>CEDAR CREEK BRIDGE</b>													
<b>SUBTOTAL - FAS 1909 SECTION</b>		<b>33,364</b>		<b>88,972</b>	<b>189.0</b>	<b>65.0</b>	<b>7513</b>	<b>8091</b>	<b>135</b>		<b>1500</b>	<b>46</b>	<b>44</b>



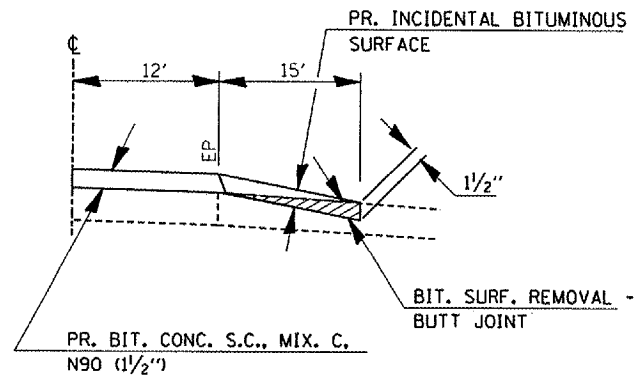
F.A.S. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	9	JACKSON	13	9
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*09 SMART FY 05-1				C.N. 98844



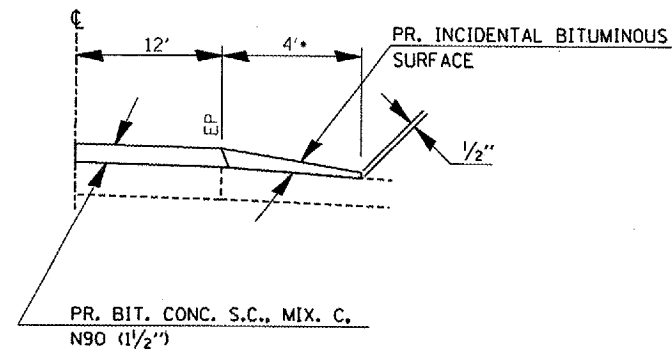
LOCATION MAP - FAS 1909 (IL. 127 ) STA. 0+82 TO STA. 335+53

# SIDEROAD AND ENTRANCE DETAILS

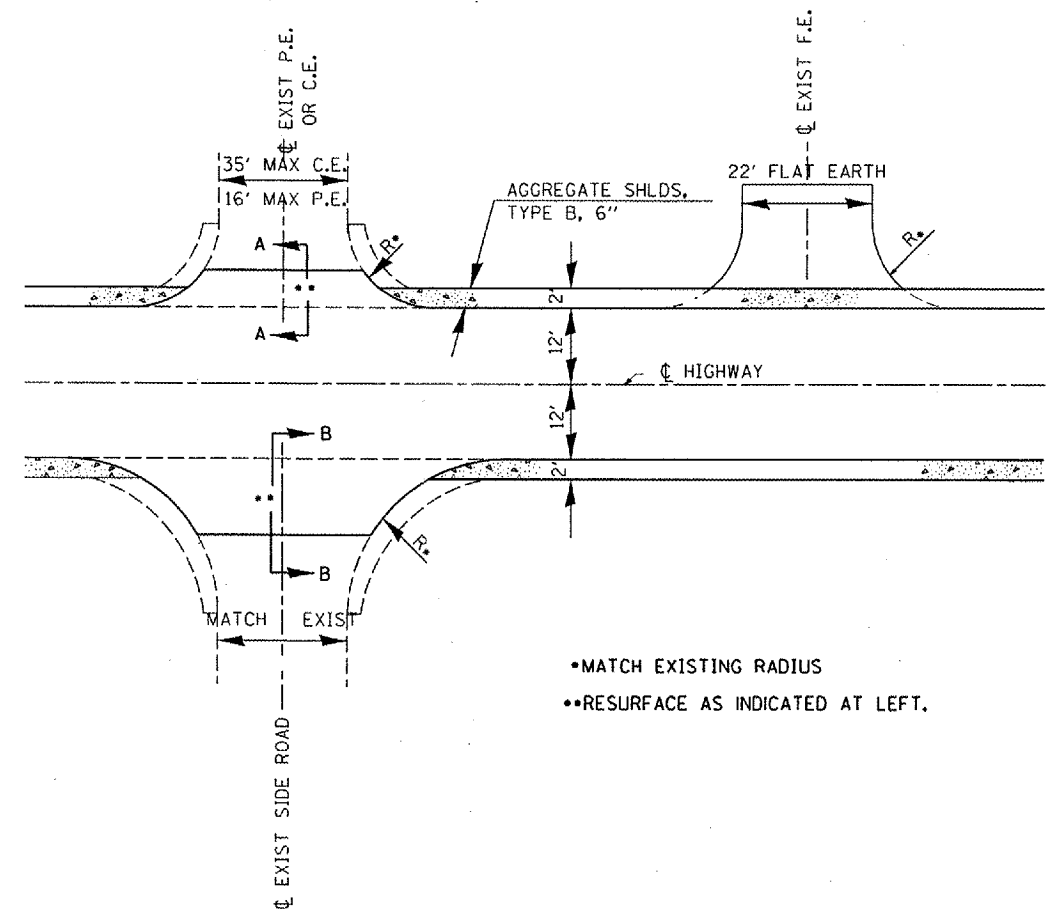
F. A. S. A/E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	*	JACKSON	13	10
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*09 SMART FY 05-1				C.N. 98844



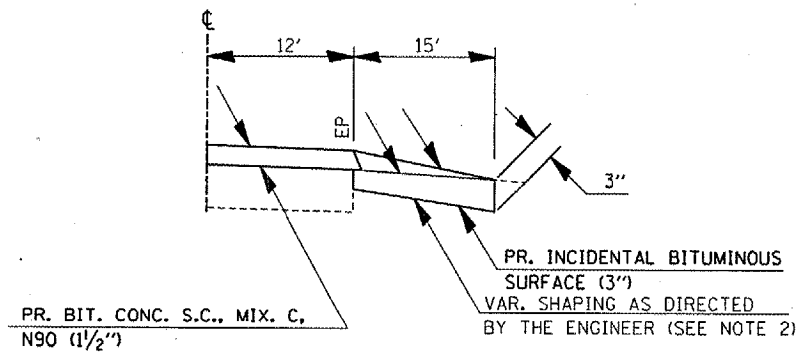
SIDE ROAD SEC. B-B  
EX. BIT. OR CONC. SURFACE



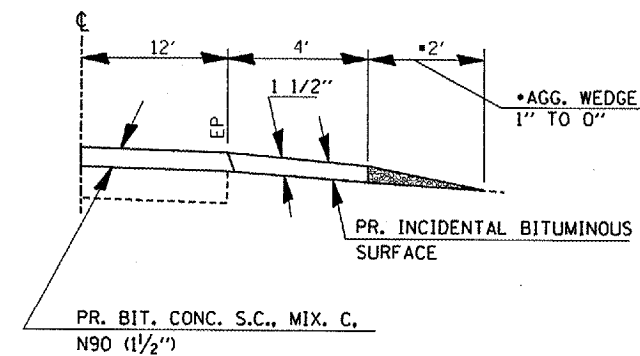
PE OR CE SEC. A-A  
EX. BIT. OR CONC. SURFACE  
•OR END OF BIT. APRON  
2 PE'S & 4 CE'S



- MATCH EXISTING RADIUS
- RESURFACE AS INDICATED AT LEFT.



SIDE ROAD SEC. B-B  
EX. AGGREGATE SURFACE  
(SEE NOTE 1)



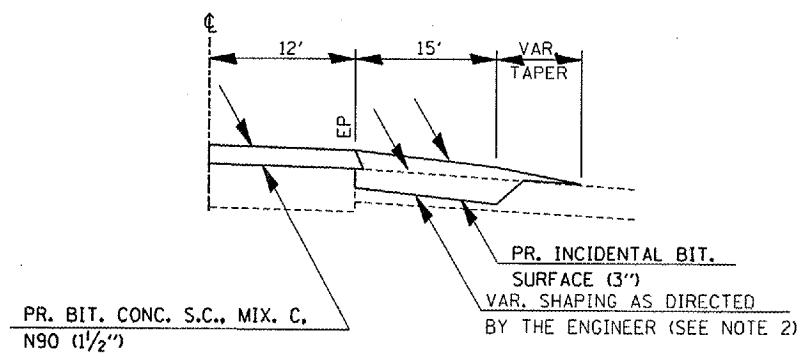
PE OR CE SEC. A-A  
EX. AGGREGATE SURFACE  
•SEE ENTRANCE SCHEDULE  
FOR QUANTITY.

### NOTES

1. IF EXISTING SUBBASE IS INADEQUATE, AS DETERMINED BY THE ENGINEER, THE SIDE ROADS SHALL BE CORED OUT AND AGGREGATE SUBBASE, TYPE B SHALL BE PLACED FOR BASE. THE COST OF CORING OUT THE SIDE ROAD AND ANY AGGREGATE BASE COURSE SHALL BE PAID FOR AS SPECIFIED IN ART. 109.04 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. IF EXISTING SUBBASE IS DETERMINED TO BE ADEQUATE, THE PREPARATION OF THE BASE SHALL BE CONSTRUCTED ACCORDING TO ARTICLE 406.19.
2. VARIABLE SHAPING IS INCLUDED IN THE COST OF INCIDENTAL BITUMINOUS SURFACE.

### ENTRANCE SCHEDULE

TYPE	NUMBER OF ENTRANCES
P.E.	89
C.E.	4
S.R.	11



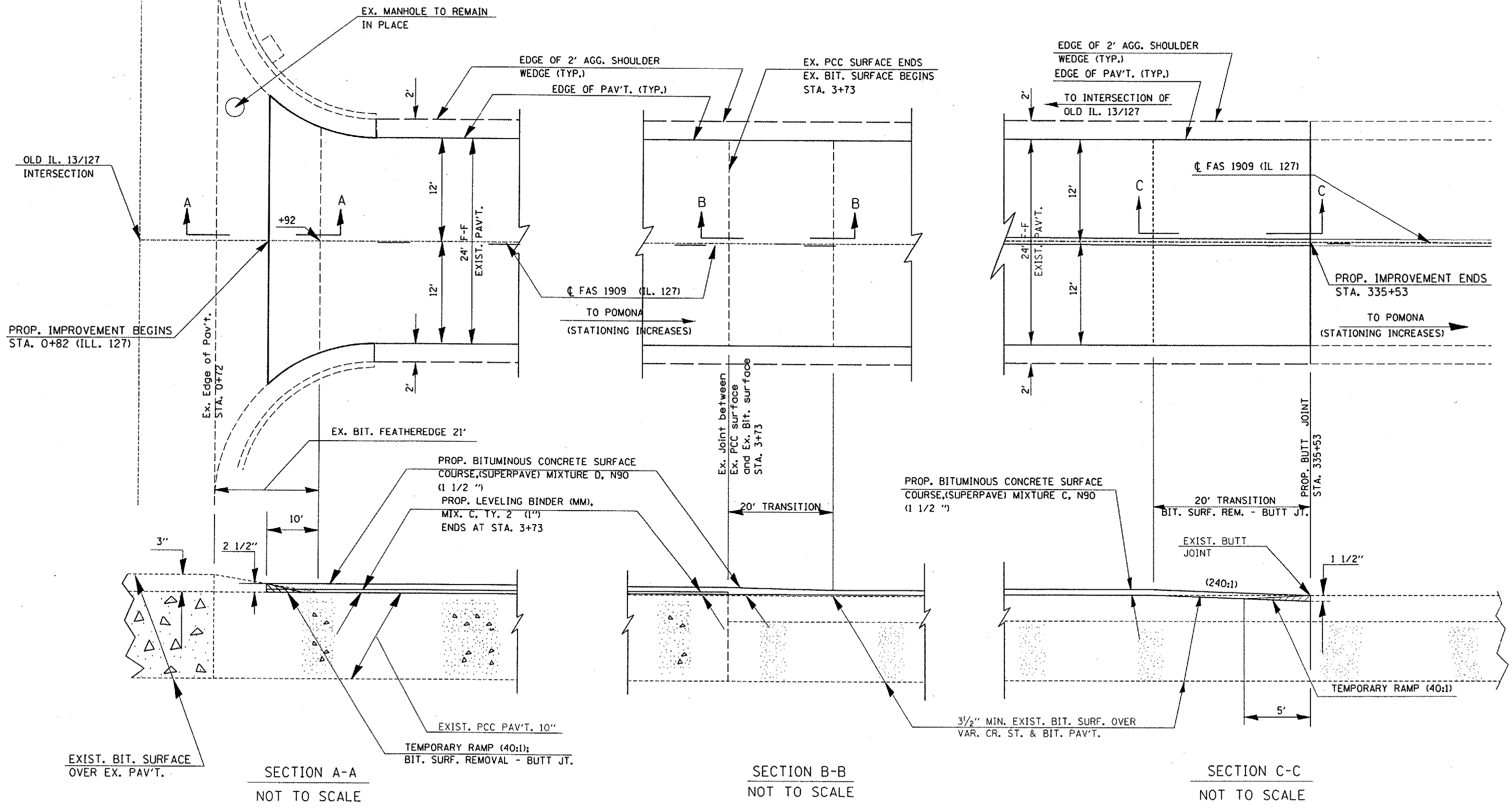
SIDE ROAD SEC. B-B  
EX. OIL & CHIP SURFACE  
(SEE NOTE 1)

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	*	JACKSON	13	11
STA.		TO STA.		
FED. AID DIST. NO.		ILLINOIS	FED. AID PROJECT	
*D9 SMART FY 05-1				C.N. 98844

**BUTT JOINT DETAIL**  
STA. 0+82 (BEGINNING OF PROJECT)

**TRANSITION DETAIL**  
STA. 3+73 TO STA. 3+93

**BUTT JOINT DETAIL**  
STA. 335+53 (END OF PROJECT)



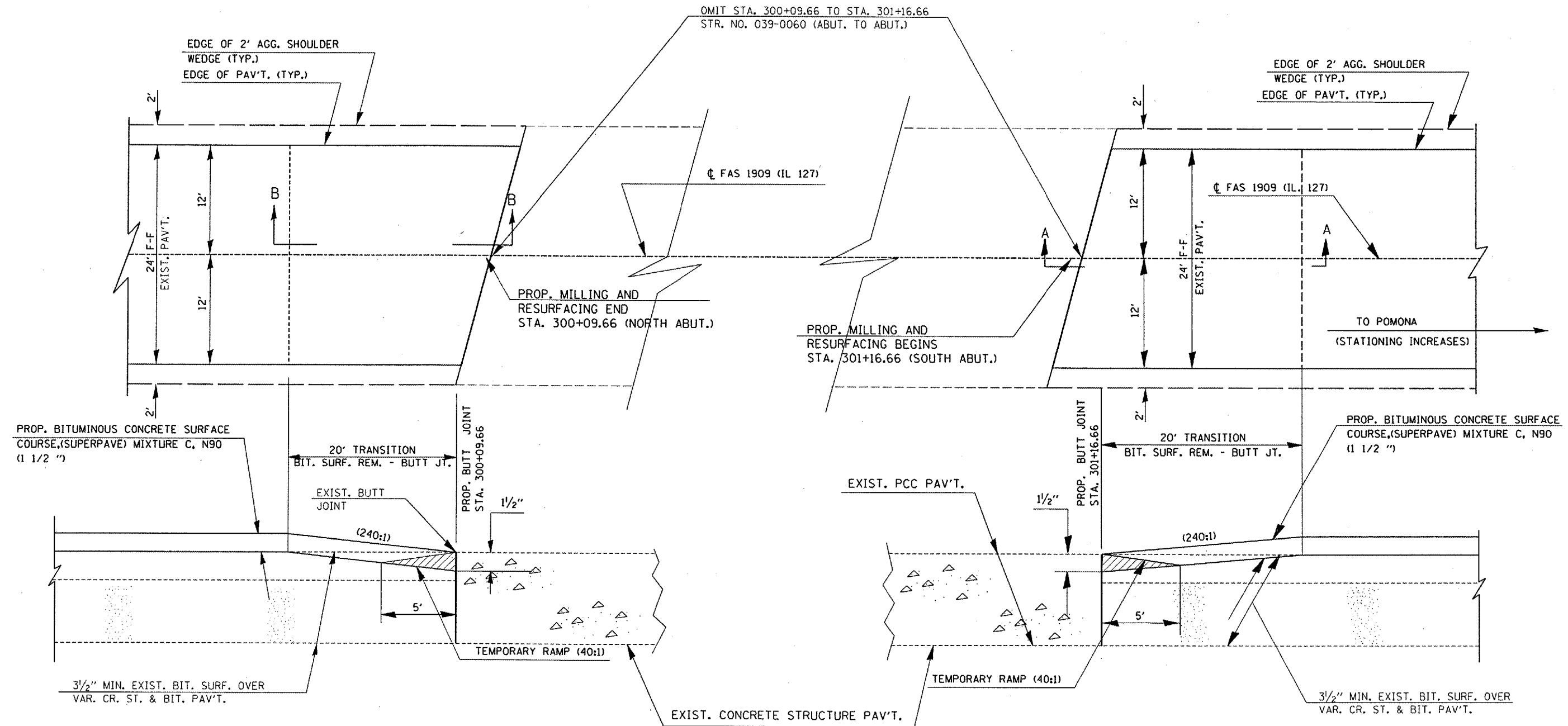
**BUTT JOINT DETAILS - STA. 0+82 AND STA. 335+53;  
TRANSITION DETAIL - STA. 3+73 TO STA. 3+93**

P. A. S. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909	*	JACKSON	13	12
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
*D9 SMART FY 05-1				C.N. 98844

**BUTT JOINT DETAIL**  
STA. 300+09.66

**BUTT JOINT DETAIL**  
STA. 301+16.66

OMIT STA. 300+09.66 TO STA. 301+16.66  
STR. NO. 039-0060 (ABUT. TO ABUT.)

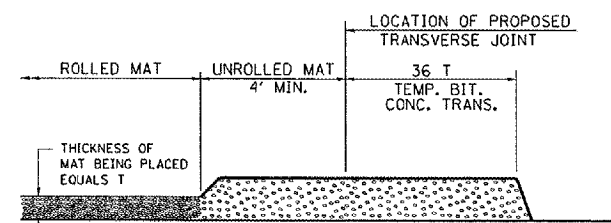


SECTION B-B  
NOT TO SCALE

SECTION A-A  
NOT TO SCALE

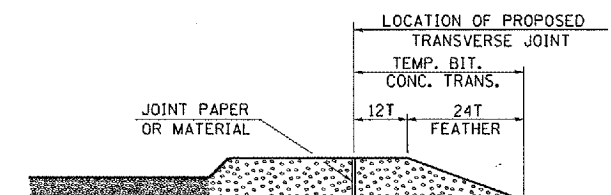
F. A. S. No.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1909		JACKSON	13	13
STA.	TO STA.			
FED. ROAD DIST. NO.	MILEAGE	FED. AID PROJECT		
*D9 SMART FY 05-1			C.N. 98844	

### TEMPORARY BITUMINOUS CONCRETE TRANSITIONS



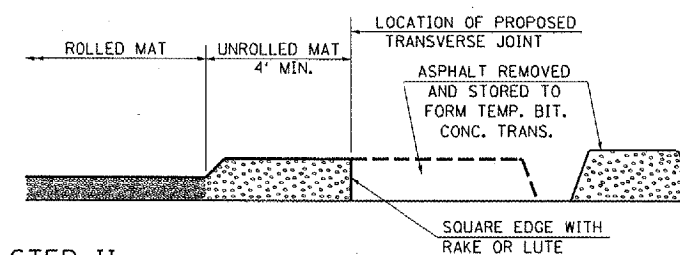
#### STEP I

1. PLACE BITUMINOUS MAT, LENGTH 36 TIMES THE THICKNESS OF THE MAT BEING PLACED PAST THE PROPOSED TRANSVERSE JOINT LOCATION USING NORMAL OPERATING PROCEDURES.
2. EXTREME CARE SHOULD BE TAKEN TO MAINTAIN ENOUGH MATERIAL IN FRONT OF THE SCREED TO MAINTAIN REQUIRED PAVING DEPTH.



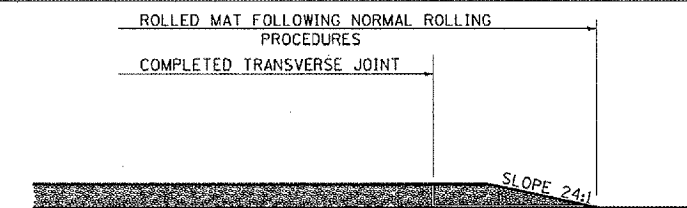
#### STEP III

1. JOINT PAPER OR OTHER PRESELECTED JOINT MATERIAL IS THEN PLACED IN THE CLEARED AREA AND THE EXCESS ASPHALT USED TO HAND FORM A TRANSITION TO THE DIMENSIONS SHOWN ABOVE.
2. NOTE THAT IN CONSTRUCTING THE TRANSITION, THE MAT DEPTH IS CONTINUED AS PART OF THE TRANSITION BEFORE FORMING THE FEATHER.



#### STEP II

1. MOVE THE PAVER OUT OF THE WAY AND REMOVE THE ASPHALT FROM THE AREA OF THE PROPOSED TEMPORARY BITUMINOUS CONCRETE TRANSITION.
2. SQUARE UP THE END OF THE MAT WITH A RAKE OR LUTE.
3. NOTE THAT THE MAT WITHIN 4' OF THE END OF JOINT IS NOT TO BE ROLLED AT THIS TIME.



#### STEP IV

1. COMPLETE TEMPORARY TRANSITION BY ROLLING.
2. TO RESUME PAVING, AT THE JOINT, REMOVE TEMPORARY TRANSITION AND DISPOSE OF THE MATERIAL ACCORDING TO ART. 202.03 OF THE STD. SPECS. (COST INCLUDED IN THE CONTRACT).
3. CONSTRUCTING THE TEMPORARY TRANSITIONS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE BITUMINOUS MATERIAL BEING PLACED.

REVISIONS
REVISION 2-15-89
REVISION 8-16-94
REVISION
REVISION

STD. 9-26

### DETAIL - BIT. CONCRETE TRANSITIONS