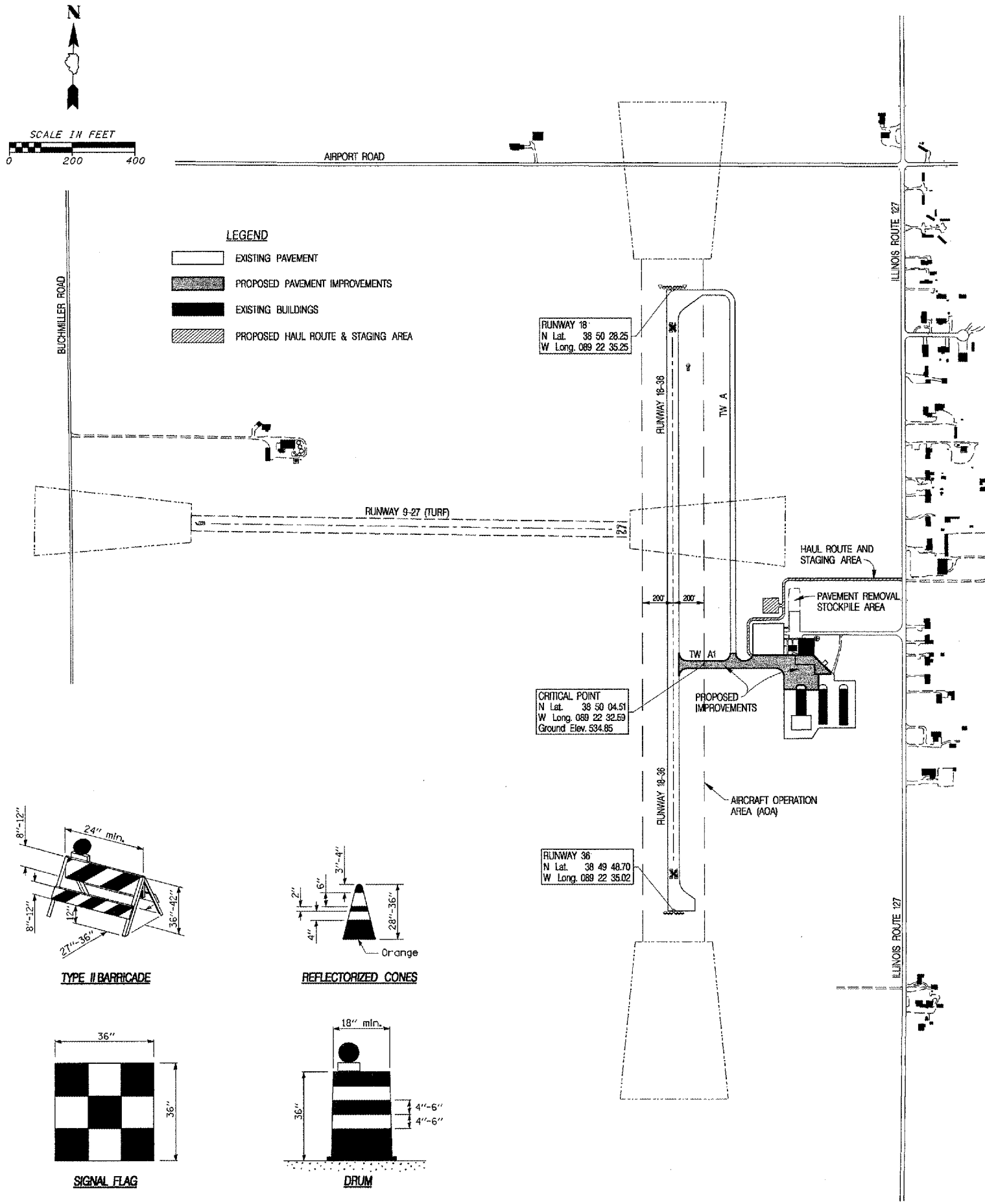


CONTRACT NUMBER	COUNTY	TOTAL SHEETS	SHEET NO.
GROO2	BOND	29	3



GENERAL NOTES

- The proposed project consists of taxiway and apron pavement rehabilitation and reconstruction, utility work, and minor earthwork at the Greenville Municipal Airport.
- This plan has been prepared to comply with the provisions of FAA Advisory Circular No. 150/5370-2E "Operational Safety on Airports During Construction". Aviation safety is the primary consideration at airports, especially during construction. Hazardous practices and marginal conditions created by construction activities can decrease or jeopardize operational safety on the airport. The purpose of this plan is to minimize disruption of normal aircraft operations and to avoid situations that compromise the airports operational safety. This plan in conjunction with the AC listed above sets forth guidelines for operational safety on the airport during the length of construction on this Project.
- The following plan was established and included in these plans to direct the Contractor in the placement of temporary traffic control systems and to provide a plan for compliance under FAA policies. The Contractor shall abide with all requirements in this plan as part of the Contract.
- Certain traffic maintenance items shall be placed as shown herein or as described in this plan. Other items shall be placed as directed by the Engineer based on situations resulting from type of activities, time of year, and weather conditions. All work associated with complying with the requirements of the safety plan, phasing requirements, maintenance of traffic and traffic control including, but not limited to, the moving and maintenance of barricades, temporary signing, temporary signing removal, Air Operations Area (AOA) lathe and ribbon, etc. shall be paid for under Item ARI50530 - Traffic Maintenance.
- The Contractor must stage construction around airport operations. The staging shown is suggested and is intended to provide the Contractor with major work areas while minimizing disruptions to airport operations. The Contractor shall submit a construction phasing plan to the Engineer and Airport Manager for approval. The Contractor may use alternate staging plans; however the alternate plans must maintain airport operations to the satisfaction of the Airport Manager and Resident Engineer and be approved by the Division of Aeronautics. The phasing plan must be approved before construction will be allowed to start.
- When conflicts arise between construction activities and aircraft operations and safety, aircraft operations and safety shall take precedence and shall govern.
- Before beginning any construction activity, the Contractor must, through the airport operator, give notice, using the Notice to Airmen (NOTAM) System, of proposed location, time, and date of commencement of construction. Upon completion of work and return of all such areas to standard conditions, the Contractor must, through the airport operator, verify the cancellation of all notices issued via the NOTAM System.
- All airport runways, taxiways and aprons shall be kept open to aircraft traffic to the maximum extent possible during construction except as noted herein.
- Runway 18-36 will be closed any time construction activities are within 200' of the runway centerline. Construction activity within the safety area of a runway, taxiway or apron performed under normal operational conditions must be performed when the runway, taxiway, or apron is closed or use-restricted.
- Vehicles and equipment shall not be allowed within areas 40' from the centerline of active taxiways or 200' from the centerline of active runways.
- The Contractor, through training identified in his/her Compliance Plan, shall ensure that all construction personnel are familiar with safety procedures and regulations on the airport.
- A weekly progress and safety meeting shall be held throughout the duration of the project to discuss and coordinate construction activities and operational safety issues.
- The Contractor shall provide a point of contact who will coordinate an immediate response to correct any construction related activity that may adversely affect the operational safety of the airport.
- The Contractor and airport operator must perform onsite inspections during the project, with immediate remedy of any deficiencies, whether caused by negligence, oversight, or project scope change.

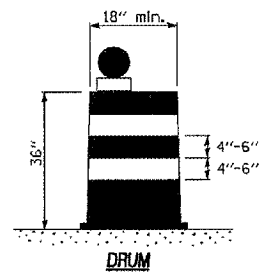
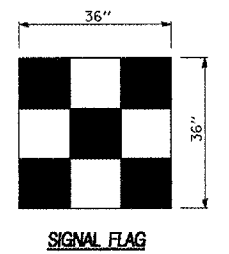
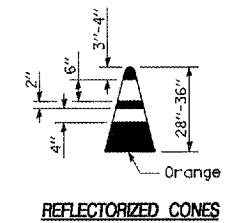
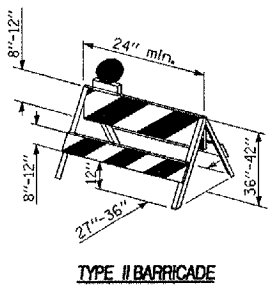
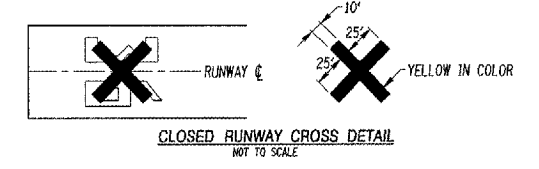
- The maximum anticipated height of any piece of construction equipment will be 25 feet. The tallest piece of equipment is expected to be a long-bed dump trailer.
- The Contractor will be required to be in two-way radio contact (123.050 MHz) with the Airport UNICOM. The Contractor shall have communication equipment capable of changing frequencies to contact each other and the Airport UNICOM or have multiple radios. This will keep the Contractor in constant contact with the Greenville Airport Authority and enable the Airport to immediately contact the Contractor in case of an aeronautical emergency that would require action by the Contractor and/or his personnel. The Contractor shall provide 2 extra sets of radios to the Resident Engineer and Airport Manager for their use during the project.
- The Contractor will be required to put airport signal flags and have beacon (flashing yellow) lights on all vehicles and equipment anytime they are on airport property.
- The Contractor, his employees and equipment shall be restricted to the project work area. The Contractor shall also provide all workers with some type of tag or garment to identify the person as being part of the construction crew.

RUNWAY AND TAXIWAY AIR OPERATIONS AREA (AOA)

- A runway safety area or air operations area is the defined surface surrounding the runway suitable for reducing the risk of damage to airplanes in the event of an undershoot, overshoot, or excursion from the runway. Construction activities within the AOA are subject to the following conditions:
 - No construction may occur closer than 200 feet from the runway centerline unless the runway is closed.
 - A taxiway AOA is a defined surface alongside the taxiway suitable for reducing the risk of damage to an airplane unintentionally departing the taxiway.
 - No construction may occur closer than 40 feet from the taxiway centerline unless the taxiway is closed. Construction activity may occur closer to a taxiway if adequate wingtip clearance exists between the aircraft and equipment/materials and;
 - Appropriate NOTAM's are issued and;
 - Marking and lighting meeting the provisions of "Hazard Marking and Lighting" detailed herein are implemented.
- Work within the runway and/or taxiway AOA shall be limited to 15 daily closures. No overnight runway or taxiway closures will be permitted. Any dropoff shall be adequately lighted, signed and barricaded.
- The Resident Engineer will establish the Aircraft Operational Area (AOA) for the runway and taxiway by placing a row of lathe to delineate the distance from centerline of the AOA. No personnel or equipment will cross the lathe row without closing the respective runway or taxiway.

TEMPORARY RUNWAY CLOSURE

Any time the Contractor is working within 200 feet of the runway centerline, the runway will be closed. After appropriate NOTAM's are issued, the Contractor will place runway closure crosses and lighted barricades as required. Runway closure crosses shall be yellow in color and shall be made of a suitable material as approved by the Resident Engineer. The crosses will be placed over the numerals as shown below and secured in a manner approved by the Resident Engineer. The crosses and barricades will be placed each day the runway is closed and removed when the runway is re-opened.



DATE	DESCRIPTION OF THE REVISIONS	BY

GREENVILLE AIRPORT AUTHORITY

HENRY, MEISENHEIMER & GEMDE, INC.
ENGINEERS
CARLYLE, ILLINOIS 62231
www.hmgengr.com
(618) 594-3711 Fax (618) 594-8217

CONSTRUCTION SAFETY PLAN
AIRPORT LAYOUT

IL PROJ. NO. GRE-3215
A.I.P. PROJ. NO. 3-17-0049-B4
FILE NAME GA-SAFETY01
DATE 03-24-2006