

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

MUN 3035 (SECOND STREET) OVER ILLINOIS & MICHIGAN CANAL
SECTION: 13-00080-00-BR
PROJECT: BROS-4003(334)
STRUCTURE REPLACEMENT
CITY OF LOCKPORT
WILL COUNTY
JOB: C-91-314-14

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	1
BROS-4003(334)		ILLINOIS	CONTRACT NO. 61D71	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

TRAFFIC DATA

SECOND STREET
POSTED SPEED LIMIT - 25 MPH
DESIGN SPEED LIMIT - 30 MPH
2012 ADT = 900 VPD
2040 ADT = 1900 VPD

DESIGN DESIGNATION

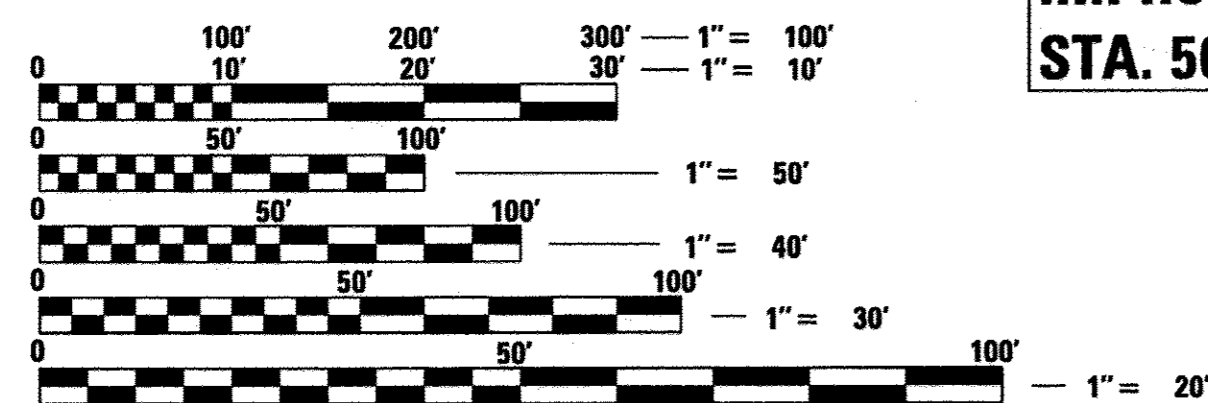
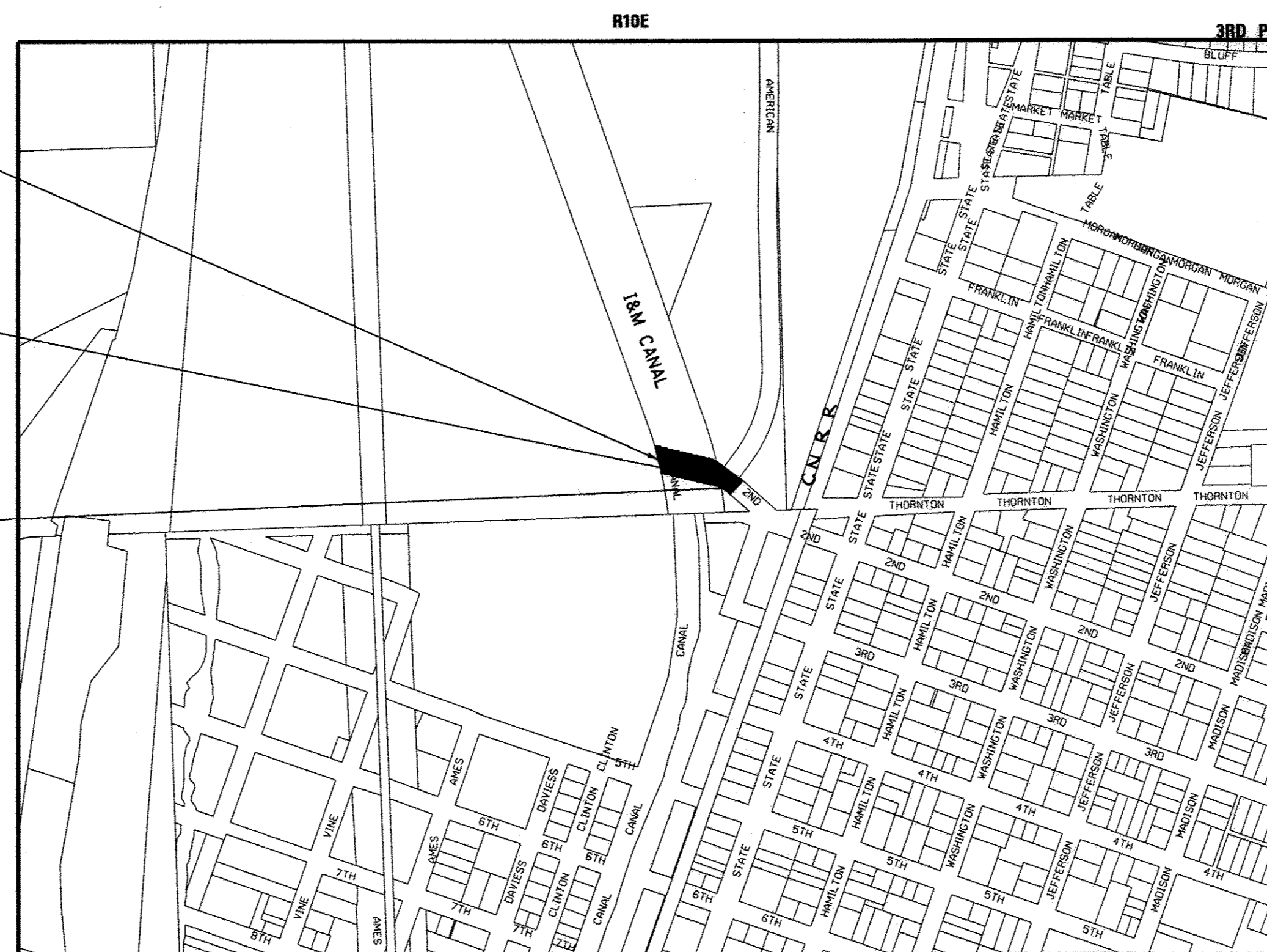
SECOND STREET: LOCAL ROAD



**SECOND STREET
IMPROVEMENT BEGINS
STA. 503 + 35**

**STRUCTURE REPLACEMENT
STA 504 + 23.22
EXISTING SN 099-6507
PROPOSED SN 099-6068**

**SECOND STREET
IMPROVEMENT ENDS
STA. 506 + 38**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. DESIGN STAGE REQUEST
DIG. No. X3210246



CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = WILL
CITY-TWNSHP. = LOCKPORT
SEC. & 1/4 SEC. NO. = 14 AND 23
48 HOURS (2 working days) BEFORE YOU DIG

CONTRACT NO. 61D71

GROSS LENGTH = 303 FT. = 0.057 MILE
NET LENGTH = 303 FT. = 0.057 MILE



PROJECT MANAGER: [Signature] "LICENSE EXPIRES 11-30-17"
 STRUCTURAL ENGINEER: [Signature] "LICENSE EXPIRES 11-30-18"
 ELECTRICAL ENGINEER: [Signature] "LICENSE EXPIRES 11-30-17"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: [Signature] 1-27-17
CITY OF LOCKPORT

PASSED: FEBRUARY 6, 2017
C. H. Holt
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID
BASED ON LIMITED REVIEW: February 7 2017
Regional Engineer

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FEDERAL AID PROGRAM ENGINEER: CHARLES F. RIDDLE, P.E., 847-703-4406, SCHRAMBURG, IL

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
515001-03	NAME PLATE FOR BRIDGES
601001-05	PIPE UNDERDRAINS
602001-02	CATCH BASIN TYPE A
602011-02	CATCH BASIN TYPE C
602401-03	MANHOLE TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
664001-02	CHAIN LINK FENCE
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS

DISTRICT 1 DETAILS

BD-22	DISTRICT 1 DETAIL - PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-32	DISTRICT 1 DETAIL - BUTT JOINT AND HMA TAPER DETAILS
TC-10	DISTRICT 1 DETAIL - TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT 1 DETAIL - DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-21	DISTRICT 1 DETAIL- DETOUR SIGNING FOR CLOSING STATE HIGHWAYS

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	DESIGNED - CAC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX TO SHEETS, HIGHWAY STANDARDS, DISTRICT ONE STANDARD DETAILS	MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - CJC	REVISED -			3035	13-00080-00-BR	WILL	42	2
	CHECKED - ADJ	REVISED -			CONTRACT NO. 61D71				
	DATE - 01-27-17	FILE - 131048SHT_GenNotes.dgn			SCALE:	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. THE LOCATIONS OF PUBLIC UTILITIES SHOWN ON THE PLANS REPRESENTS ONLY THE OPINION OF THE OWNER AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER AND THE ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND, SURFACE, AND OVERHEAD UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
3. THE CONTRACTOR SHALL NOTIFY THE CITY OF LOCKPORT PUBLIC WORKS DEPARTMENT AT 1-815-838-0549 AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN CITY UTILITY LOCATIONS.
4. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CITY RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF CITY WATER IF DEEMED NECESSARY. THE CONTRACTOR SHALL CONTACT CITY OF LOCKPORT PUBLIC WORKS AT 1-815-838-0549 REGARDING OBTAINING WATER.
5. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES STORM SEWER TRENCHES. REFER TO SIDEWALK DETAIL. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
6. THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL DELIVERIES.
7. ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS SI CONCRETE AND ALLOWED TO CURE FOR 72 HOURS PRIOR TO PLACEMENT OF SURFACE COURSE. CLASS PP CONCRETE SHALL BE USED IF PLACEMENT OF SURFACE COURSE IS PLANNED IN LESS THAN 72 HOURS. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY TO ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT.
8. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF AN ADA-COMPLIANT CAST-IN-PLACE CAST IRON 24"X48" MINIMUM NOMINAL SIZE PANEL AS MANUFACTURED BY E.J. NEENAH ENTERPRISES INC., OR TUFTILE. THE DOMES LOCATED ON THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
9. THE CONTRACTOR WILL BE REQUIRED TO USE A STEEL PLATE OR PLATES TO CLOSE ANY GAPS OCCURRING WHEN A FRAME IS OFFSET FROM THE STRUCTURE. THE STEEL PLATE SHALL BE 1/2-INCH THICK AND APPROXIMATELY 6-INCH WIDE BY 24-INCH LONG. SOME ADJUSTMENT IN SIZE MAY BE NECESSARY TO PREVENT THE STEEL PLATE FROM OVERHANGING THE OUTSIDE OF THE STRUCTURE WALL. THE STEEL PLATE SHALL BE BEDDED IN AND COVERED WITH MORTAR.
10. TRENCH BACKFILL FOR THIS PROJECT SHALL CONSIST OF CRUSHED CA-6 AND SHALL BE COMPACTED BY METHOD 1 ONLY.
11. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELOCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER.
12. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER.
13. ALL OPEN HOLE, BROKEN PAVEMENT AND TRENCHES RESULTING FROM STRUCTURE ADJUSTMENTS, OR CURB REPAIRS WORK SHALL BE BACKFILLED TO GRADE BY THE END OF THE DAY.
14. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETING EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
15. CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
16. ANY DAMAGE TO PAVEMENT, SIDEWALK, CURB OR ANY OTHER PORTION OF THE ROADWAY NOT SPECIFICALLY TO BE REMOVED AND REPLACED SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL CHARGE.
17. AN IEPA FORM LPC-663 HAS BEEN PROVIDED BY THE CITY AND IS INCLUDED IN THE SPECIAL PROVISIONS.
18. THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 72 HOURS IN ADVANCE OF BEGINNING WORK.
19. ALL STORM SEWER SHALL BE REINFORCED CONCRETE CULVERT, STORM DRAIN, AND SEWER PIPE.
20. THE ILLINOIS DEPARTMENT OF TRANSPORTATION IS NOT THE OWNER OF RECORD FOR THIS BRIDGE. THOSE SEEKING HISTORICAL AS-BUILT OR OTHER RECORD PLANS AND DOCUMENTS MUST CONTACT THE OWNER OF RECORD TO MAKE ARRANGEMENTS FOR ACCESS TO THIS INFORMATION. CONTACT CITY OF LOCKPORT PUBLIC WORKS DEPARTMENT AT 1-815-838-0549.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES

SCALE: _____ STA. _____ TO STA. _____

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	3
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61D71	

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNT	22	22
20101000	TEMPORARY FENCE	FOOT	24	24
20200100	EARTH EXCAVATION	CUYD	635	635
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CUYD	101	101
20300100	CHANNEL EXCAVATION	CUYD	28	28
20800150	TRENCH BACKFILL	CUYD	25	25
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	1,074	1,074
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CUYD	122	122
* 25000210	SEEDING, CLASS 2A	ACRE	0.15	0.15
* 25000312	SEEDING, CLASS 4A	ACRE	0.14	0.14
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	15	15
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	15	15
* 25100115	MULCH, METHOD 2	ACRE	0.75	0.75
* 25100630	EROSION CONTROL BLANKET	SQ YD	1,121	1,121
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	422	422

* INDICATES SPECIALTY ITEM
 SP INDICATES ITEMS COVERED BY SPECIAL PROVISIONS

CODE NO.	ITEM	UNT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
28000400	PERIMETER EROSION BARRIER	FOOT	594	594
28000510	INLET FILTERS	EACH	3	3
28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	1,200	1,200
28100105	STONE RIPRAP, CLASS A3	SQ YD	5	5
28200200	FILTER FABRIC	SQ YD	5	5
SP 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CUYD	88	88
SP 30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	926	926
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	327	327
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SQ YD	80	80
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1.1	1.1
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	48	48
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	117	117
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	278	278
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	165	165
40700100	BITUMINOUS MATERIALS (TACK COAT)	POUND	2,938	2,938

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: _____ STA. _____ TO STA. _____

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	4
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO. 61D71				

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
42001300	PROTECTIVE COAT	SQ YD	750	750
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2,359.0	2,359.0
42400800	DETECTABLE WARNINGS	SQ FT	86	86
44000100	PAVEMENT REMOVAL	SQ YD	881	881
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	748	748
44000600	SIDEWALK REMOVAL	SQ FT	475	475
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	8	8
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	20	20
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	40	40
44201723	CLASS D PATCHES, TYPE IV, 6 INCH	SQ YD	60	60
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	267	267
48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	5	5
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	3	3
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1	1
50200400	ROCK EXCAVATION FOR STRUCTURES	CUYD	50	50

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
50201101	COFFERDAM (TYPE 1) (LOCATION - 1)	EACH	1	1
50201102	COFFERDAM (TYPE 1) (LOCATION - 2)	EACH	1	1
50300225	CONCRETE STRUCTURES	CUYD	78.5	78.5
SP 50300285	FORM LINER TEXTURED SURFACE	SQ FT	2,400	2,400
50300300	PROTECTIVE COAT	SQ YD	163	163
50800105	REINFORCEMENT BARS	POUND	6,880	6,880
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	9,040	9,040
51500100	NAME PLATES	EACH	1	1
52200900	CONCRETE STRUCTURES (RETAINING WALL)	CUYD	51	51
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	65	65
55100200	STORM SEWER REMOVAL 6"	FOOT	23	23
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	75	75
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	28	28
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	2	2

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	5
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
60207905	CATCH BASINS, TYPE C, TYPE 11 FRAME AND GRATE	EACH	1	1
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	1	1
60500060	REMOVING INLETS	EACH	1	1
* 60265700	VALVE VAULTS TO BE ADJUSTED	EACH	3	3
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	485	485
* 63200310	GUARDRAIL REMOVAL	FOOT	143	143
* SP 66400305	CHAIN LINK FENCE, 6'	FOOT	17	17
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6
67100100	MOBILIZATION	L SUM	1	1
70300240	TEMPORARY PAVEMENT MARKING- LINE 6"	FOOT	1,056	1,056
70300260	TEMPORARY PAVEMENT MARKING- LINE 12"	FOOT	2,112	2,112
70300280	TEMPORARY PAVEMENT MARKING- LINE 24"	FOOT	144	144
72000100	SIGN PANEL - TYPE 1	SQ FT	44	44
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	9	9
72400710	RELOCATE SIGN PANEL - TYPE 1	SQ FT	3	3

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CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	13	13
73000100	WOOD SIGN SUPPORT	FOOT	117	117
73700200	REMOVE CONCRETE FOUNDATION - GROUND MOUNT	EACH	1	1
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	146	146
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	57	57
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	163	163
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	51	51
* SP 80400100	ELECTRIC SERVICE INSTALLATION	EACH	1	1
* SP 80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1	1
* SP 81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	165	165
* SP 81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	150	150
* SP 81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	720	720
* SP 81702140	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4	FOOT	315	315
* SP 82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1	1
SP Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1

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CHECKED - ADJ	REVISED -
DATE - 01-27-17	FILE - 131048SHT_500.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	6
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
SP Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	75	75
SP Z0038151	THREE-SIDED PRECAST CONCRETE STRUCTURES 32'X 8'	FOOT	60.5	60.5
* SP Z0055900	RUSTIC WOOD POST AND RAIL FENCE	FOOT	72	72
* A2002616	TREE, CARYA CORDIFORMIS (BITTERNUT HICKORY), 2" CALIPER, BALLED AND BURLAPPED	EACH	1	1
* A2002716	TREE, CARYA OVATA (SHAGBARK HICKORY), 2" CALIPER, BALLED AND BURLAPPED	EACH	1	1
* A2006716	TREE, QUERCUS MACROCARPA (BUR OAK), 2" CALIPER, BALLED AND BURLAPPED	EACH	2	2
* SP X0323697	RUSTIC RAIL FENCE REMOVAL	FOOT	20	20
SP X0326671	CONCRETE SURFACE COLOR TREATMENT	SQ FT	2,700	2,700
SP X2020110	GRADING AND SHAPING SHOULDERS	UNT	1	1
SP X4020900	AGGREGATE SURFACE COURSE, TYPE B SPECIAL	SQ YD	80	80
SP X4023000	TEMPORARY ACCESS (ROAD)	EACH	2	2
SP X5010205	REMOVAL OF EXISTING STRUCTURE, SPECIAL	EACH	1	1
* SP X5091755	PARAPET RAILING, SPECIAL	FOOT	77	77
* SP X6640300	CHAIN LINK FENCE REMOVAL	FOOT	86	86
SP X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1

* INDICATES SPECIALTY ITEM
 SP INDICATES ITEMS COVERED BY SPECIAL PROVISIONS

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	BRIDGE
				0011 URBAN
SP X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	733	733
* SP X8250500	LIGHTING UNIT COMPLETE, SPECIAL	EACH	4	4
* SP XX006490	LETTERING	L SUM	1	1
SP XX008257	STAMPED COLORED PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	569	569
SP XZ127900	RETAINING WALL REMOVAL	FOOT	80	80
SP XZ127902	RETAINING WALL, SPECIAL	SQ FT	220	220

* INDICATES SPECIALTY ITEM
 SP INDICATES ITEMS COVERED BY SPECIAL PROVISIONS

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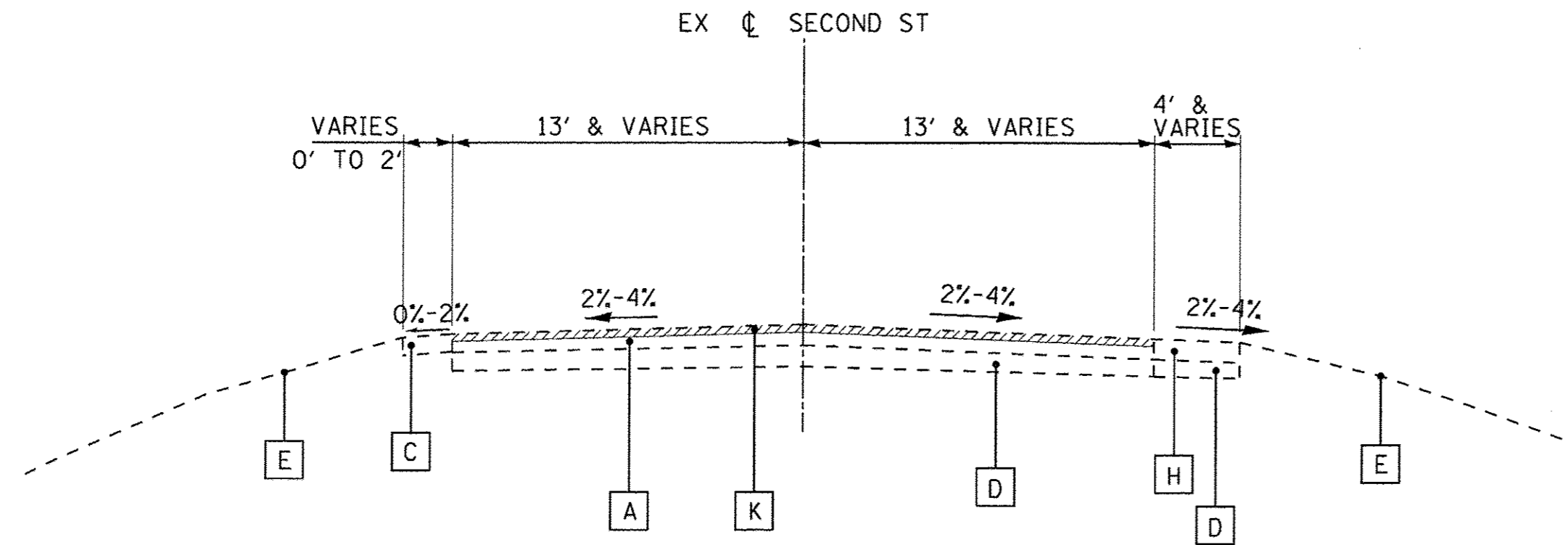
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	DATE - 01-27-17	FILE - 131048SHT_S00.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

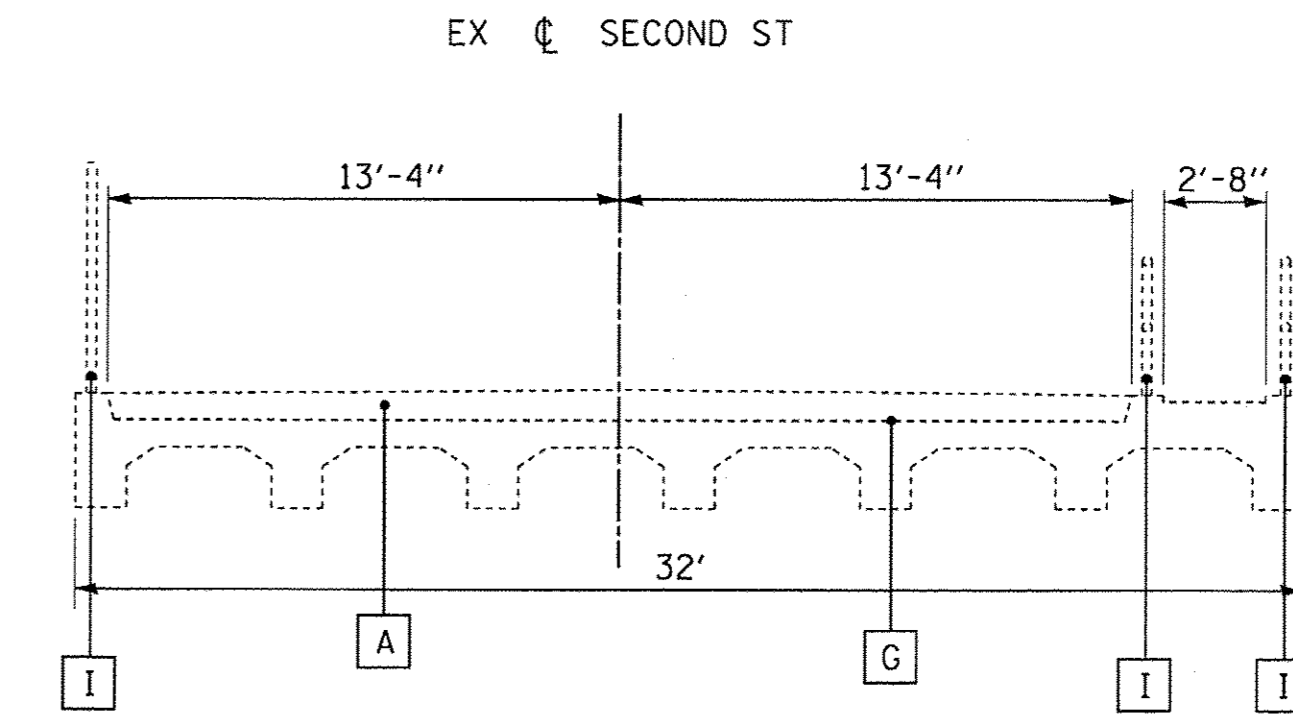
SUMMARY OF QUANTITIES

SCALE: STA. TO STA.

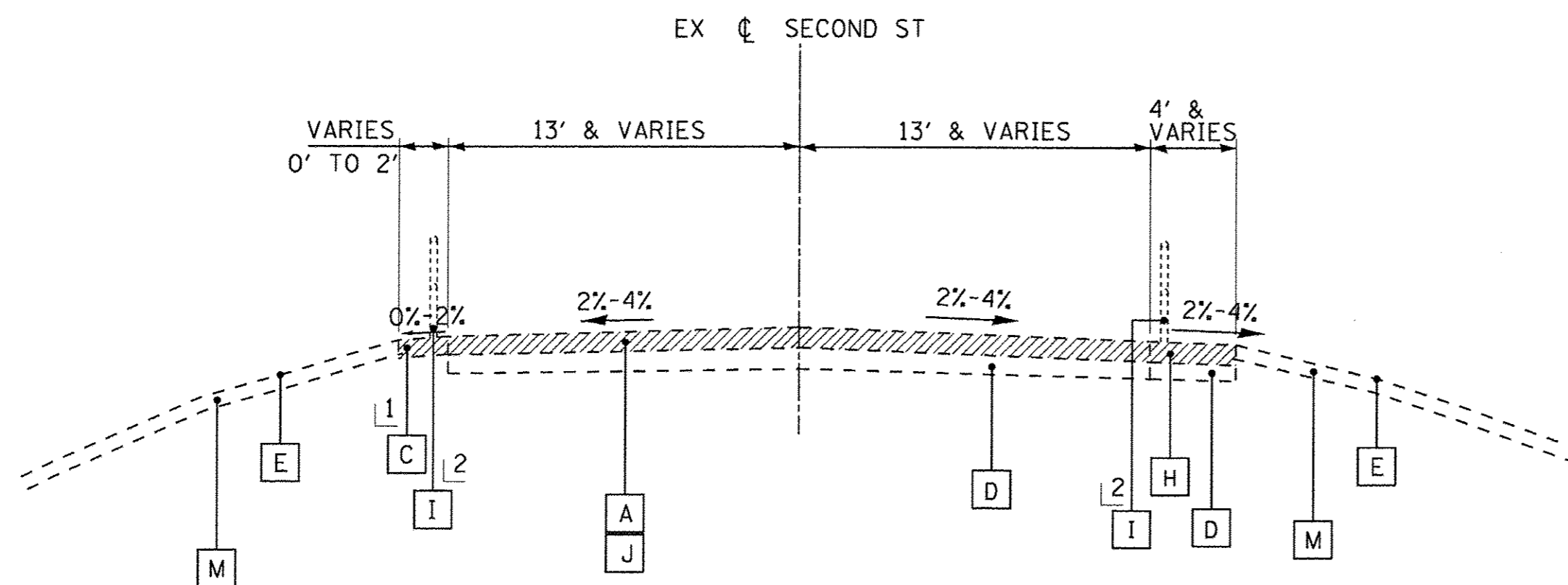
MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	7
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	



EXISTING TYPICAL SECTION
SECOND STREET
 STA 503+35 TO STA 503+78

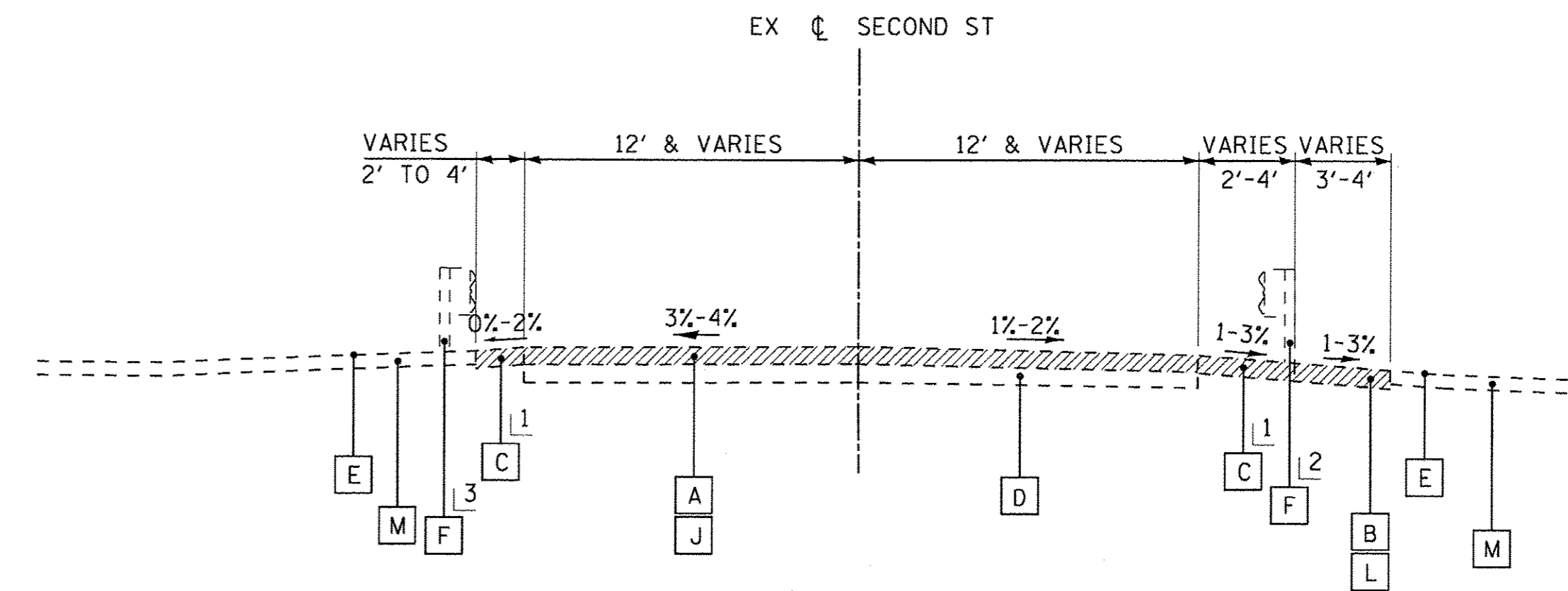


EXISTING TYPICAL SECTION
SECOND STREET
 STA 504+09.59 TO STA 504+36.89



EXISTING TYPICAL SECTION
SECOND STREET
 STA 503+78 TO STA 504+09.59

- 1 REMOVAL OF AGGREGATE SHOULDER PAID FOR AS EARTH EXCAVATION
- 2 PEDESTRIAN RAILING TO BE REMOVED STA 101+84 TO STA 102+56 INCLUDED IN REMOVAL OF EXISTING STRUCTURES



EXISTING TYPICAL SECTION
SECOND STREET
 STA 504+36.89 TO STA 506+38

- 1 REMOVAL OF AGGREGATE SHOULDER PAID FOR AS EARTH EXCAVATION
- 2 EXISTING GUARDRAIL TO BE REMOVED STA 102+50 TO STA 103+22
- 3 EXISTING GUARDRAIL TO BE REMOVED STA 102+56 TO STA 102+89

EXISTING LEGEND

- A HMA PAVEMENT - (VARIES 4 1/2" - 9 1/2")
- B SIDEWALK
- C AGGREGATE SHOULDER
- D AGGREGATE BASE COURSE- (VARIES 6" - 12")
- E GROUND
- F GUARDRAIL
- G BRIDGE SUPERSTRUCTURE
- H HMA SHOULDER
- I PEDESTRIAN RAILING
- J PAVEMENT REMOVAL
- K HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- L SIDEWALK REMOVAL
- M TOPSOIL REMOVAL (4")
- ITEM TO BE REMOVED

EXISTING TYPICAL SECTION NOTES:

- THE PAVEMENT AND BASE COURSE THICKNESS WERE TAKEN FROM THE "GEO TECHNICAL REPORT" PREPARED BY McCLEARY ENGINEERING DATED MAY 15, 2015.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE THICKNESSES OF THE EXISTING PAVEMENT TO BE REMOVED. NO ADDITIONAL COMPENSATION WILL BE ALLOWED BECAUSE OF VARIATIONS FROM THE ASSUMED THICKNESS SHOWN ON THE PLANS.

SOILS REPORT PAVEMENT CORE RESULTS:

(SB-1) STA 102+00, 5.4R LL, HMA PAVEMENT THICKNESS = 9.4 INCHES
 (SB-2) STA 102+41.4, 7.5R RL, HMA PAVEMENT THICKNESS = 6.6 INCHES
 (R-1) STA 101+85, 10.3R RL, HMA PAVEMENT THICKNESS = 4.8 INCHES
 (R-2) STA 102+75.4, 8.0R LL, HMA PAVEMENT THICKNESS = 4.6 INCHES

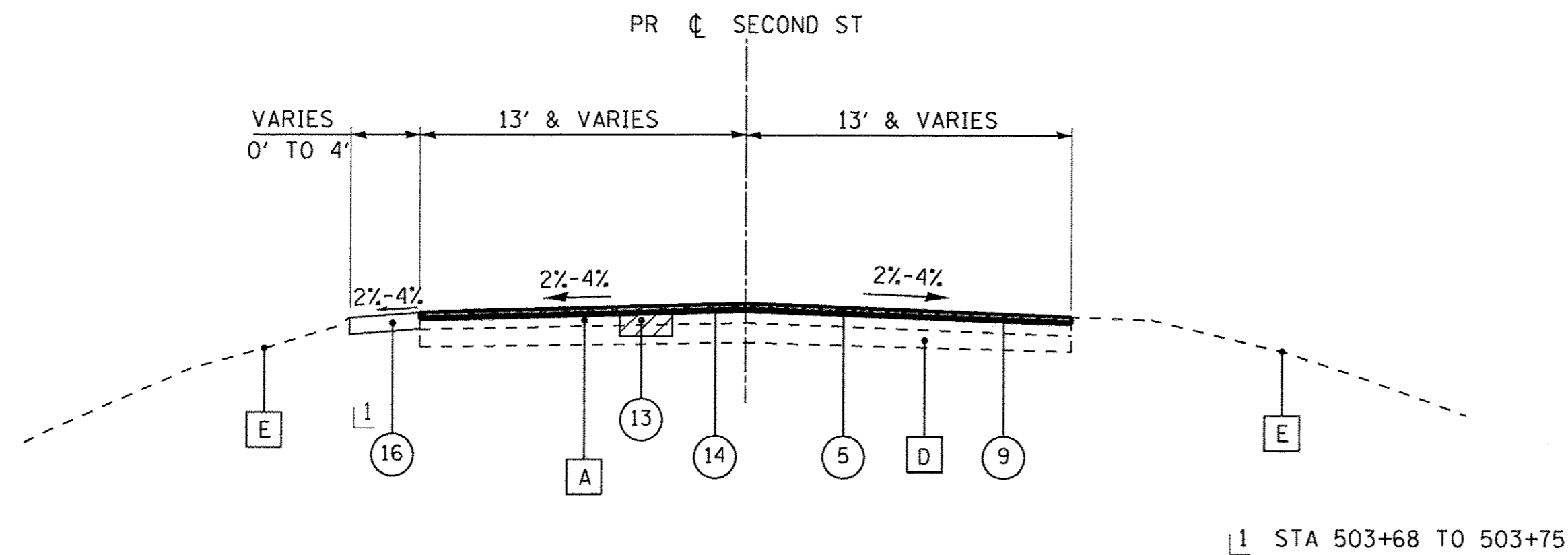
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	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_TypSec.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
SCALE:	STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	8
CONTRACT NO. 61D71				
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

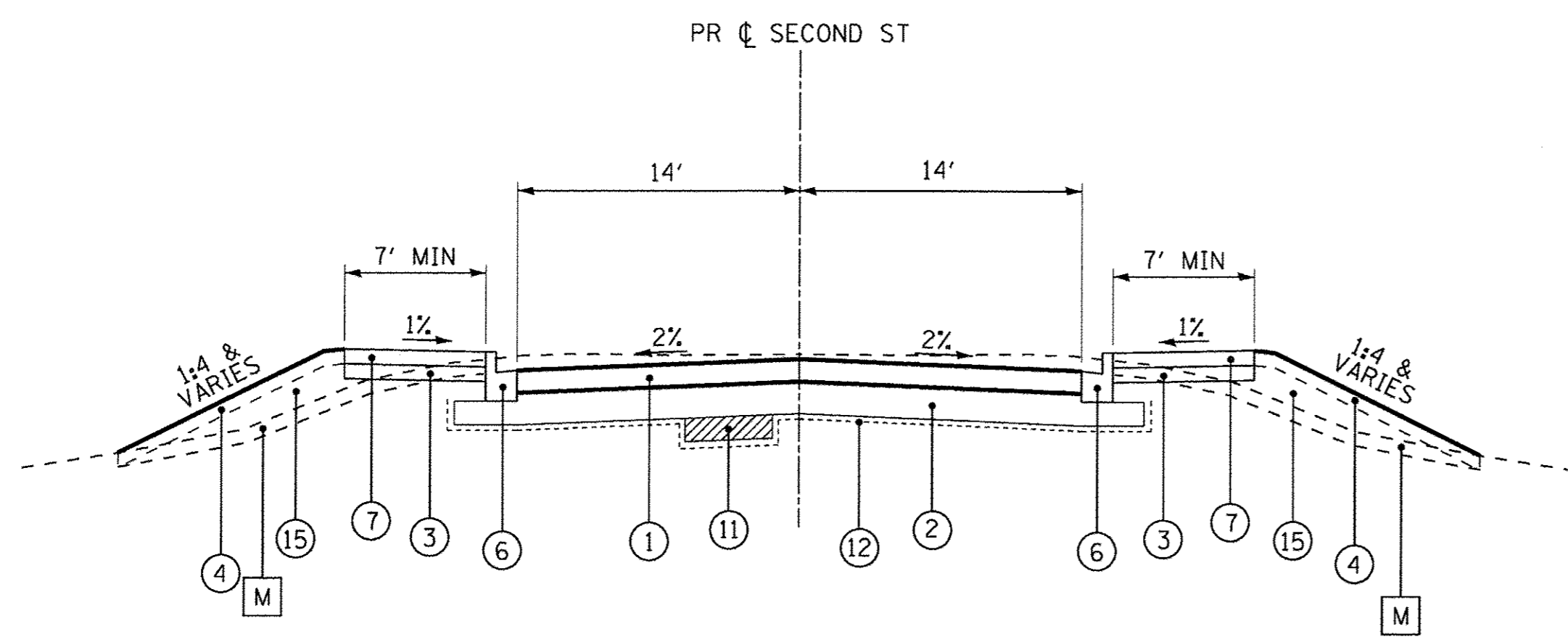


PROPOSED TYPICAL SECTION

SECOND STREET

STA 503+35 TO STA 503+71
STA 506+03 TO STA 506+38

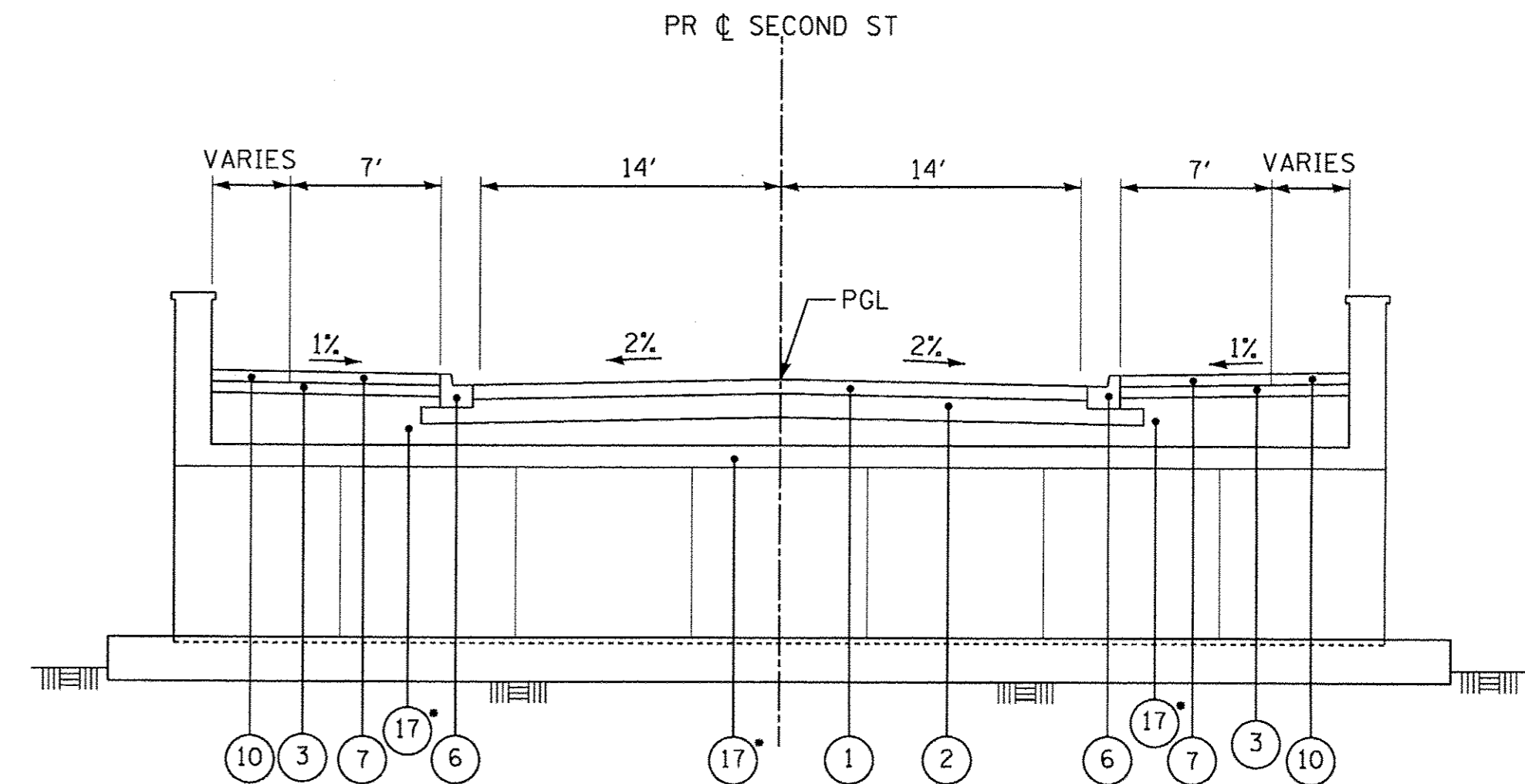
1 STA 503+68 TO 503+75



PROPOSED TYPICAL SECTION

SECOND STREET

STA 503+71 TO STA 504+05.46
STA 504+41.35 TO STA 506+03

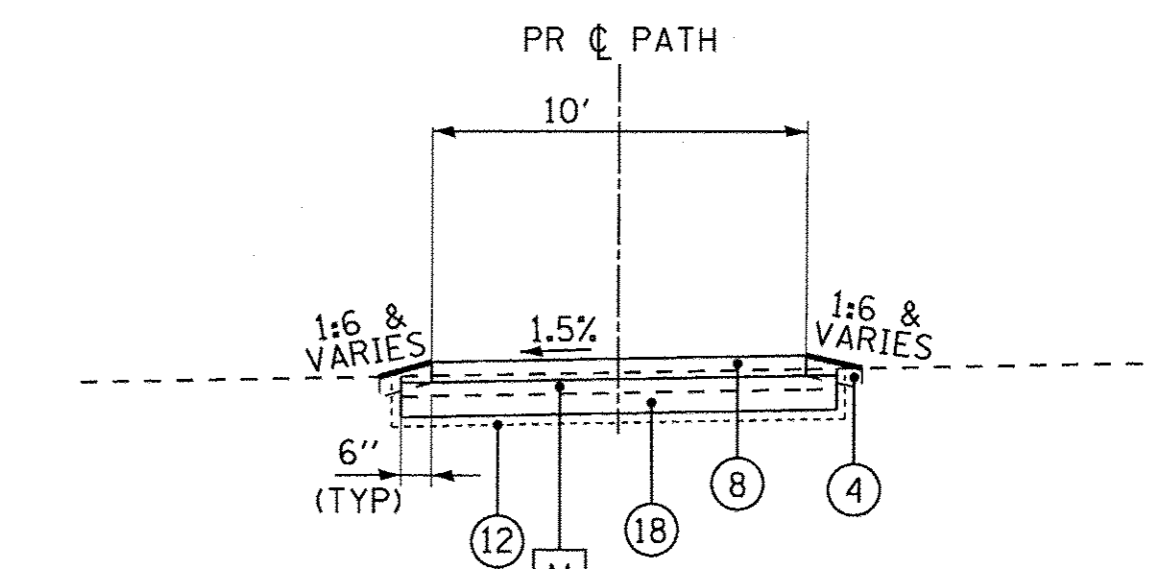


PROPOSED TYPICAL SECTION

SECOND STREET

STA 504+05.46 TO STA 504+41.35

• EXTENDS TO LIMITS OF EXCAVATION FOR THREE-SIDED PRECAST CONCRETE STRUCTURES



PROPOSED TYPICAL SECTION

TRAIL

STA 20+89.86 TO STA 21+60.24

PROPOSED LEGEND

- 1 PAVEMENT:
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 2"
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 5"
- 2 AGGREGATE SUBGRADE IMPROVEMENT, 12"
- 3 AGGREGATE BASE COURSE, TYPE B, 4"
- 4 TOPSOIL EXCAVATION AND PLACEMENT (4")
SEEDING, CLASS 2A OR SEEDING, CLASS 2A
- 5 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2"
- 6 COMBINATION CONCRETE CURB AND GUTTER, TYPE B.6-12
(SEE PLAN FOR LIMITS)
- 7 PCC SIDEWALK, 5-INCH
- 8 LIMESTONE SCREENINGS, 3"
PAID FOR AS AGGREGATE SURFACE COURSE, TYPE B SPECIAL
- 9 BITUMINOUS MATERIALS (TACK COAT)
- 10 STAMPED COLORED PCC SIDEWALK, 5 INCH
- 11 REMOVAL AND DISPOSAL OF UNSUITABLE MATERIALS
•• AGGREGATE SUBGRADE IMPROVEMENT (CU YD)
- 12 GEOTECHNICAL FABRIC FOR GROUND STABILIZATION
- 13 CLASS D PATCHES, TYPE 1-IV 6-INCH
- 14 LEVELING BINDER (MACHINE METHOD), N50 - 3/4"
- 15 EMBANKMENT
- 16 AGGREGATE SHOULDERS, TYPE B 6"
(SEE PLAN FOR LIMITS)
- 17 GRANULAR BACKFILL AS SPECIFIED BY THREE-SIDED PRECAST SUPPLIER
(INCLUDED IN COST OF THREE-SIDED PRECAST CONCRETE STRUCTURES)
- 18 AGGREGATE BASE COURSE, TYPE B, 6"

NOTES:

1. **AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOILS SHALL BE REMOVED AND REPLACED WITH ASI OR EMBANKMENT AS DETERMINED BY THE ENGINEER. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE BACK OF CURB AND GUTTER AND COME UP AT A 1:1 SLOPE TO AGGREGATE SUBGRADE IMPROVEMENT 12". THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL." ANY ASI AND REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR GROUND STABILIZATION NOT NEEDED AT THE TIME OF CONSTRUCTION SHALL BE DELETED FROM THE CONTRACT.
2. SEE PLAN AND PROFILE SHEETS FOR LIMITS OF PROPOSED RESURFACING.
3. PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE PLACED TO DRAIN THE LOW AREAS OF THE ROAD AND AT THE BASE OF UNDERCUTS AREAS REPAIRED BY AGGREGATE SUBGRADE IMPROVEMENT. TRANSVERSE UNDERDRAINS SHALL BE PLACED IN THE LOW POINTS WHERE THE ROAD WILL BE RECONSTRUCTED AND CONNECTED TO NEAREST DRAINAGE STRUCTURE. PIPE UNDERDRAINS, TYPE 2, 4" SHALL BE INSTALLED IN ACCORDANCE WITH THE LATEST STANDARD SPECIFICATIONS.

EXISTING LEGEND

- A HMA PAVEMENT - (VARIES 4 1/2" - 9 1/2")
- B SIDEWALK
- C AGGREGATE SHOULDER
- D AGGREGATE BASE COURSE- (VARIES 6" - 12")
- E GROUND
- F GUARDRAIL
- G BRIDGE SUPERSTRUCTURE
- H HMA SHOULDER
- I PEDESTRIAN RAILING
- J PAVEMENT REMOVAL
- K HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- L SIDEWALK REMOVAL
- M TOPSOIL REMOVAL (4")
- ITEM TO BE REMOVED

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
FULL DEPTH PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm), 2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 5" (IN TWO LIFTS)	4% @ 50 GYR
PAVEMENT RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, 1 1/2"	4% @ 50 GYR
LEVELING BINDER (MACHINE METHOD), N50 - 3/4"	4% @ 50 GYR
PATCHING	
CLASS D PATCH (HMA BINDER IL-19mm), 6"	4% @ 70 GYR

NOTE: THE CONTRACTOR SHALL MILL BEFORE PATCHING.
1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LB/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE 'SBS/SBR PG 76-22' AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS		MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE:	STA.	3035	13-00080-00-BR	WILL	42	9
	TO STA.	CONTRACT NO. 61D71		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT		

EARTHWORK SCHEDULE

LOCATION		EARTHWORK														
STA TO STA		1	2	3	4	5	6	7	8	9	10	11	12	13	14	15
		TOPSOIL EXCAVATION (CU YD)	TOPSOIL EXCAVATION FOR PLACEMENT (15% SHRINKAGE) (CU YD)	21101505 TOPSOIL EXCAVATION AND PLACEMENT (CU YD)	TOPSOIL BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)	30300001 AGGREGATE SUBGRADE IMPROVEMENT (CU YD)	UNSUITABLE EXCAVATION (TOPSOIL) (CU YD)	20201200 REMOVAL & DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	20200100 EARTH EXCAVATION (CU YD)	20300100 CHANNEL EXCAVATION (CU YD)	UTILITY EXCAVATION (CU YD)	EXCESS STRUCTURE EXCAVATION (CU YD)	TOTAL SUITABLE EXCAVATION (CU YD)	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE) (CU YD)	EMBANKMENT (CU YD)	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-) (CU YD)
SECOND STREET		70	61	64	(-3)	88	12	100	393	-	25	-	418	302	77	+241
503+71.00	503+80.00	2.2	1.9	1.5	+0.4	4.0	0.4	4.4	22.5	-	-	-	22.5	19.2	3.3	+16
503+80.00	504+00.00	7.2	6.1	2.0	+4.1	9.0	4.1	13.1	53.9	-	9.0	-	62.9	15.9	15.9	+6
504+00.00	504+05.00	2.4	2.1	0.3	+1.8	2.2	1.8	4.0	14.4	-	-	-	14.4	12.3	6.1	+2
504+05.00	504+41.00	0.0	-	0.0	-	-	-	-	-	-	-	-	-	-	-	+13
504+41.00	504+60.00	6.7	5.8	0.7	+5.1	8.5	5.1	13.6	22.1	-	-	-	22.1	18.8	16.4	+30
504+60.00	504+80.00	10.4	8.9	11.9	(-3.0)	9.0	-	9.0	23.8	-	1.0	-	24.8	21.1	8.4	+24
504+80.00	505+00.00	12.6	10.8	17.7	(-6.9)	9.0	-	9.0	29.5	-	15.0	-	44.5	37.9	8.2	+16
505+00.00	505+20.00	11.0	9.4	11.4	(-2.0)	9.0	-	9.0	34.6	-	-	-	34.6	29.4	5.6	+16
505+20.00	505+30.00	4.3	3.7	4.3	(-0.6)	4.5	-	4.5	20.0	-	-	-	20.0	17.1	1.0	+16
505+30.00	505+60.00	2.9	2.6	3.0	(-0.4)	4.5	-	4.5	18.5	-	-	-	18.5	15.8	0.1	+26
505+60.00	505+80.00	5.0	4.3	5.0	(-0.7)	9.0	-	9.0	34.1	-	-	-	34.1	29.0	2.8	+30
505+80.00	506+03.00	3.8	3.3	3.8	(-0.5)	9.0	-	9.0	41.5	-	-	-	41.5	35.3	5.6	+62
506+03.00		1.5	1.3	1.5	(-0.2)	10.3	-	10.3	77.5	-	-	-	77.5	65.9	3.5	
TOTALS		128	111	122	(-12)	88	13	101	635	28	25	-	660	508	77	+447

COLUMN 1 = TOPSOIL REMOVAL DEPTH 4"
 COLUMN 2 = [COLUMN 1] x 0.85
 COLUMN 3 = FROM CROSS SECTION END AREAS
 COLUMN 4 = [COLUMN 2] - [COLUMN 3]
 COLUMN 5 = ASSUMED TO BE 1' DEPTH UNDER APPROX 30% OF EXISTING PAVEMENT. ASSIGNED PROPORTIONALLY BY LENGTH
 COLUMN 6 = [COLUMN 4] (IF COLUMN 4 > 0)
 COLUMN 7 = [COLUMN 5] + [COLUMN 6]
 COLUMN 8 = FROM CROSS SECTION END AREAS
 COLUMN 9 = EXCAVATION WITHIN CHANNEL OUTSIDE OF STRUCTURE
 COLUMN 10 = STORM, SANITARY, WATER, UTILITY EXCAVATION
 COLUMN 11 = BALANCE STRUCTURE EXCAVATION AVAILABLE FOR REUSE
 COLUMN 12 = [COLUMN 8] + [COLUMN 10] + [COLUMN 11]
 COLUMN 13 = [COLUMN 12] x 0.85
 COLUMN 14 = FROM CROSS SECTION END AREAS
 COLUMN 15 = [COLUMN 13] - [COLUMN 14]

EARTHWORK PAY ITEM SUMMARY		TOTAL	
(20200100) EARTH EXCAVATION	635	CU YD	
(20201200) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	101	CU YD	
(20300100) CHANNEL EXCAVATION	28	CU YD	
(21101505) TOPSOIL EXCAVATION AND PLACEMENT	122	CU YD	
(30300001) AGGREGATE SUBGRADE IMPROVEMENT	88	CU YD	

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SCHEDULE OF QUANTITIES

ROADWAY/SHARED-USE PATH PAVEMENT SCHEDULE															
		44000100	44000158	40600982	40600625	40603080	40603335	40700100	30300112	21001000	35101800	48101500	48102100	X2020110	
		PAVEMENT REMOVAL	HOT-MIX ASPHALT SURFACE REMOVAL, 2-1/4"	HOT-MIX ASPHALT SURFACE REMOVAL BUTT JOINT	LEVELING BINDER (MACHINE METHOD), N50	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	BITUMINOUS MATERIALS (TACK COAT)	AGGREGATE SUBGRADE IMPROVEMENT 12"	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	AGGREGATE BASE COURSE, TYPE B 6"	*LIMESTONE SCREENINGS, 3"	AGGREGATE SHOULDERS, TYPE B 6"	AGGREGATE WEDGE SHOULDER, TYPE B TON	GRADING AND SHAPING SHOULDERS UNIT
FROM	TO	SQ YD	SQ YD	SQ YD	TON	TON	TON	POUND	SY YD	SQ YD	SQ YD	SQ YD	SQ YD	TON	UNIT
SECOND STREET (RESURFACING)															
503+35	503+43		274	51	26			24	185					3	1
506+03	506+38		474	66	22			40	320						
SECOND STREET (RECONSTRUCTION)															
503+43	506+03	841				278	101	2,433	790	790		5			
SHARED-USE PATH															
20+28	21+63	40								94	80	80			
TOTALS		881	748	117	48	278	165	2,938	790	884	80	80	5	3	1

*PAID FOR AS AGGREGATE SURFACE COURSE, TYPE B SPECIAL

COMBINATION CONCRETE CURB AND GUTTER SCHEDULE							
			30300112	21001000	60603800	42001300	
			AGGREGATE SUBGRADE IMPROVEMENT 12"	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 FOOT	PROTECTIVE COAT	
FROM	TO	SIDE	SQ YD	SQ YD	FOOT	SQ YD	
503+75	505+98	LT	72	100	256	118	
503+88	505+98	RT	64	90	229	106	
TOTALS			136	190	485	224	

PARKWAY RESTORATION SCHEDULE									
			PROPOSED RESTORATION AREA	21101505 TOPSOIL EXCAVATION AND PLACEMENT	25000210 SEEDING, CLASS 2A	25000312 SEEDING, CLASS 4A	25000400 NITROGEN FERTILIZER	25000600 POTASSIUM FERTILIZER	25100630 EROSION CONTROL BLANKET
FROM	TO	SIDE	SQ YD	CU YD	ACRE	ACRE	POUND	POUND	SQ YD
503+17	503+86	LT	380	43	0.01	0.07	1	1	380
503+91	504+04	RT	10	2		0.01			10
504+20	505+22	LT	416	44	0.06	0.05	6	6	416
504+69	505+30	RT	64	8	0.01	0.01	1	1	64
504+83	505+96	LT	178	20	0.05		5	5	178
505+50	505+97	RT	73	9	0.02		2	2	73
TOTALS			1,121	126	0.15	0.14	15	15	1,121

EROSION CONTROL SCHEDULE							
			28001100 TEMPORARY EROSION CONTROL BLANKET	28000250 TEMPORARY EROSION CONTROL SEEDING	25100115 MULCH, METHOD 2	28000400 PERIMETER EROSION BARRIER	28000510 INLET FILTERS
FROM	TO	SIDE	SQ YD	POUND	ACRE	FOOT	EACH
503+17	503+86	LT	380	133	0.16	128	
503+91	504+04	RT	10	4	0.02	21	1
504+20	505+22	LT	494	174	0.22	106	1
504+69	505+30	RT	64	22	0.04	76	1
504+83	505+96	LT	178	63	0.08	145	
505+50	505+97	RT	73	26	0.04	118	
TOTALS			1,199	422	0.75	594	3

PAVEMENT MARKING SCHEDULE											
				70300240 TEMPORARY PAVEMENT MARKING LINE 6"	70300260 TEMPORARY PAVEMENT MARKING LINE 12"	70300280 TEMPORARY PAVEMENT MARKING LINE 24"	78000200 THERMOPLASTIC PAVEMENT MARKING - LINE 4"	78000400 THERMOPLASTIC PAVEMENT MARKING - LINE 6"	78000600 THERMOPLASTIC PAVEMENT MARKING - LINE 12"	78000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24"	X7030005 TEMPORARY PAVEMENT MARKING REMOVAL
FROM	TO	SIDE	TYPE	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	SQ FT
503+77	503+88	RT	STOP BAR			48				12	24
504+51	506+02	RT	CURB LINE				146			30	
505+23	505+39	LT/RT	CROSSWALK					57		133	
505+76	506+58	LT	CROSSWALK	812	1,620						507
505+96	506+14	LT	STOP BAR			96				24	48
506+15	506+31	RT	STOP BAR							15	
508+92	509+05	LT/RT	CROSSWALK	244	492						154
TOTALS				1,056	2,112	144	146	57	163	51	733

SIGN REMOVAL SCHEDULE			72400100 REMOVE SIGN PANEL ASSEMBLY - TYPE A EACH
STATION	OFFSET		EACH
503+74	15' LT		1
503+95	36' RT		1
503+95	22' RT		1
504+52	23' LT		1
504+93	22' RT		1
504+98	20' LT		1
505+18	24' RT		1
505+20	21' LT		1
505+44	21' RT		1
TOTALS			9

72000100 SIGN PANEL - TYPE 1 SCHEDULE						
STATION	OFFSET	SIGN	QUANTITY	DIMENSION	AREA (SF)	
503+77	29' LT	W11-1	1	30x30	6.25	
503+77	29' LT	W16-5P	1	24x18	3.00	
503+92	32' RT	R1-1	1	30x30	6.25	
504+54	103' LT	W1-4L	1	18x18	2.25	
505+11	25' RT	D11-1	1	24x18	3.00	
505+11	25' RT	M6-4	1	12x9	0.75	
505+19	25' LT	R1-1	1	18x18	2.25	
505+26	26' RT	R5-3	1	24x24	4.00	
505+33	25' LT	R5-3	1	24x24	4.00	
505+45	24' RT	R1-1	1	18x18	2.25	
505+48	21' LT	M6-4	1	24x18	3.00	
505+48	21' LT	R1-1	1	12x9	0.75	
505+88	41' LT	R1-1	1	30x30	6.25	
TOTALS						44

SIDEWALK SCHEDULE								
			4400600 SIDEWALK REMOVAL	42400200 PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	42400800 DETECTABLE WARNINGS	42001300 PROTECTIVE COAT	35101600 AGGREGATE BASE COURSE, TYPE B 4"	XX008257 STAMPED COLORED PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH
FROM	TO	SIDE	SQ FT	SQ FT	SQ FT	SQ YD	SQ YD	SQ FT
503+83	505+37	LT		1063.0		238	119	
503+84	504+47	LT					27	238
503+84	503+89	LT			10			
503+91	503+98	RT			18			
503+93	505+94	RT		1296.0		288	144	
503+97	504+76	RT					37	331
504+41	504+77	RT	354					
505+24	505+34	LT			20			
505+28	505+38	RT			20			
505+45	505+79	RT	121					
505+80	505+90	RT			18			
TOTALS			475	2,359.0	86	526	327	569

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
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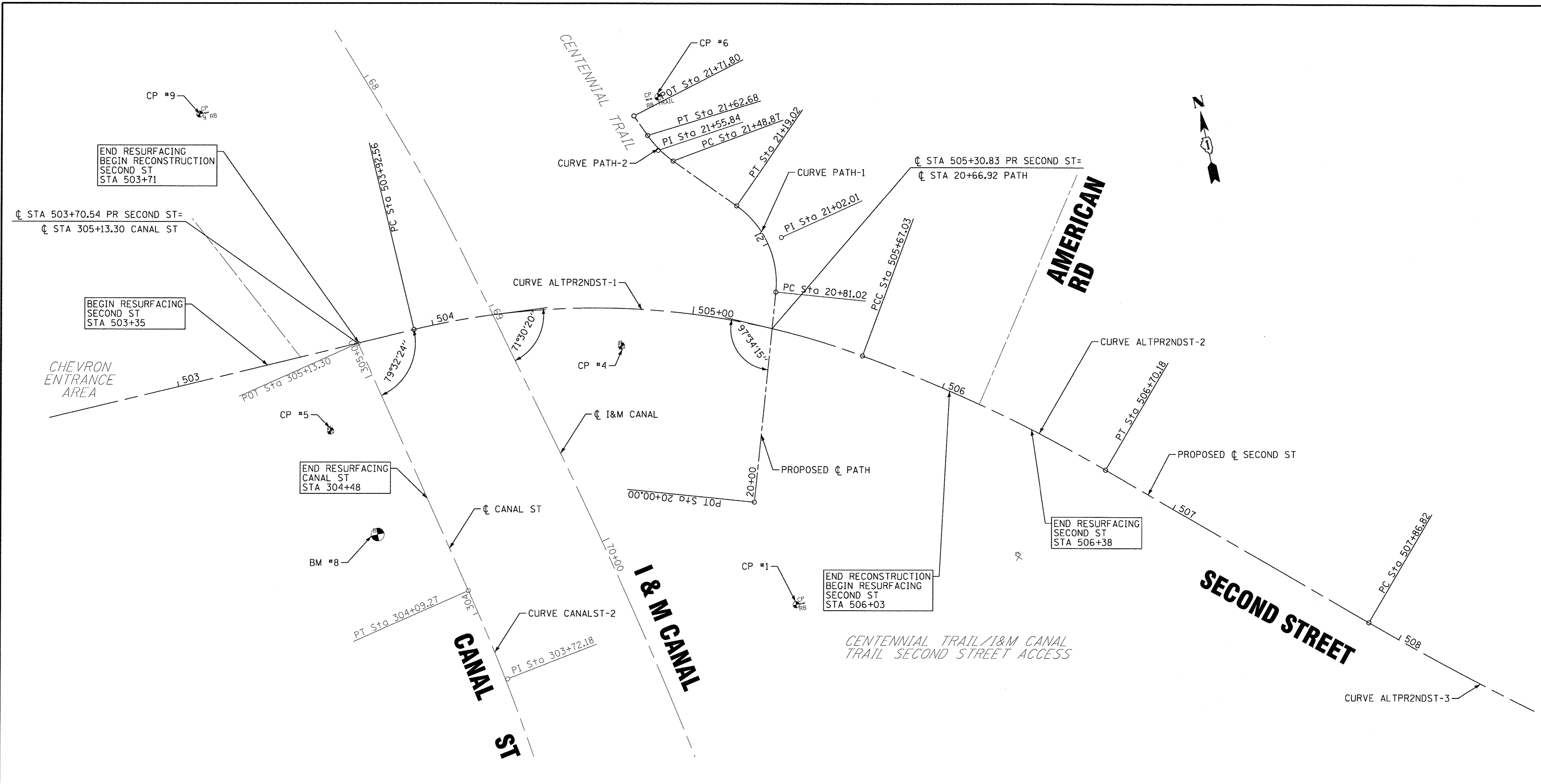
BAXTER & WOODMAN Consulting Engineers	DESIGNED - CAC	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_Schedules.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	11
CONTRACT NO. 61D71				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



BENCHMARK DATA

- BM #3 SOUTH BONNET BOLT AT FIRST FIRE HYDRANT
WEST OF TRACKS ON THE SOUTH SIDE OF SECOND ST
ELEV 587.92 (NAVD 88)
- BM #8 NORTHWEST BONNET BOLT AT FIRE HYDRANT
SOUTHWEST CORNER OF SECOND ST AND CANAL ST
ELEV 580.74 (NAVD 88)

CONTROL POINTS

CP #	N	E	ELEV	REMARKS
CP #1	N1795965.402969	E1060038.843816	ELEV 583.622846	REBAR
CP #4	N1796076.570896	E1059994.202453	ELEV 582.499446	PK NAIL
CP #5	N1796068.171217	E1059878.510720	ELEV 579.192303	PK NAIL
CP #6	N1796166.807994	E1060027.764027	ELEV 582.295038	REBAR
CP #9	N1796197.487012	E1059855.063505	ELEV 579.599045	REBAR

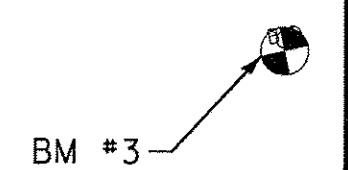
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	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_ALIGN1.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENTS, TIES, AND BENCHMARKS	
SCALE: 1" = 20'	STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	12
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	



PROPOSED ϕ SECOND ST

CHAIN ALTPR2NDST CONTAINS:
APS3 CUR ALTPR2NDST-1 CUR ALTPR2NDST-2 CUR ALTPR2NDST-3 APS4

BEGINNING CHAIN ALTPR2NDST DESCRIPTION

POINT APS3 N 1,796,089.73 E 1,059,525.46 STA 500+00.00
COURSE FROM APS3 TO PC ALTPR2NDST-1 N 88° 34' 51.58" E DIST 392.56

CURVE DATA

CURVE ALTPR2NDST-1
P.I. STATION = 504+82.43 N 1,796,101.68 E 1,060,007.75
DELTA = 33° 53' 04.63" (RT)
DEGREE = 19° 25' 20.27"
TANGENT = 89.87
LENGTH = 174.46
RADIUS = 295.00
EXTERNAL = 13.38
LONG CHORD = 171.93
MID. ORD. = 12.80
P.C. STATION = 503+92.56 N 1,796,099.45 E 1,059,917.91
P.T. STATION = 505+67.03 N 1,796,053.44 E 1,060,083.57
C.C. = N 1,795,804.54 E 1,059,925.21
BACK = N 88° 34' 51.58" E
AHEAD = S 57° 32' 03.80" E
CHORD BEAR = S 74° 28' 36.11" E

CURVE DATA

CURVE ALTPR2NDST-2
P.I. STATION = 506+18.73 N 1,796,025.69 E 1,060,127.19
DELTA = 9° 48' 29.43" (RT)
DEGREE = 9° 30' 29.66"
TANGENT = 51.70
LENGTH = 103.15
RADIUS = 602.59
EXTERNAL = 2.21
LONG CHORD = 103.03
MID. ORD. = 2.21
P.C. STATION = 505+67.03 N 1,796,053.44 E 1,060,083.57
P.T. STATION = 506+70.18 N 1,795,990.91 E 1,060,165.45
C.C. = N 1,795,545.03 E 1,059,760.10
BACK = S 57° 32' 03.80" E
AHEAD = S 47° 43' 34.36" E
CHORD BEAR = S 52° 37' 49.08" E

CURVE DATA

CURVE ALTPR2NDST-3
P.I. STATION = 508+25.12 N 1,795,886.68 E 1,060,280.09
DELTA = 4° 23' 11.25" (LT)
DEGREE = 5° 43' 46.48"
TANGENT = 38.30
LENGTH = 76.56
RADIUS = 1,000.00
EXTERNAL = 0.73
LONG CHORD = 76.54
MID. ORD. = 0.73
P.C. STATION = 507+86.82 N 1,795,912.44 E 1,060,251.75
P.T. STATION = 508+63.38 N 1,795,863.16 E 1,060,310.32
C.C. = N 1,796,652.38 E 1,060,924.43
BACK = S 47° 43' 34.36" E
AHEAD = S 52° 06' 45.61" E
CHORD BEAR = S 49° 55' 09.99" E

COURSE FROM PT ALTPR2NDST-2 TO PC ALTPR2NDST-3 S 47° 43' 34.36" E DIST 116.64

POINT APS4 N 1,795,824.02 E 1,060,360.62 STA 509+27.12

ENDING CHAIN ALTPR2NDST DESCRIPTION

PROPOSED ϕ PATH

CHAIN PATH CONTAINS:
PATH1 CUR PATH-1 CUR PATH-2 PATH2

BEGINNING CHAIN PATH DESCRIPTION

POINT PATH1 N 1,796,007.27 E 1,060,031.38 STA 20+00.00
COURSE FROM PATH1 TO PC PATH-1 N 17° 54' 13.89" E DIST 80.00

CURVE DATA

CURVE PATH-1
P.I. STATION = 21+04.84 N 1,796,107.03 E 1,060,063.61
DELTA = 63° 40' 23.12" (LT)
DEGREE = 143° 14' 22.02"
TANGENT = 24.84
LENGTH = 44.45
RADIUS = 40.00
EXTERNAL = 7.08
LONG CHORD = 42.20
MID. ORD. = 6.02
P.C. STATION = 20+80.00 N 1,796,083.40 E 1,060,055.97
P.T. STATION = 21+24.45 N 1,796,124.35 E 1,060,045.81
C.C. = N 1,796,095.69 E 1,060,017.91
BACK = N 17° 54' 13.89" E
AHEAD = N 45° 46' 09.23" W
CHORD BEAR = N 13° 55' 57.67" W

COURSE FROM PT PATH-1 TO PC PATH-2 N 45° 46' 09.23" W DIST 22.00

CURVE DATA

CURVE PATH-2
P.I. STATION = 21+54.38 N 1,796,145.23 E 1,060,024.36
DELTA = 22° 09' 02.86" (RT)
DEGREE = 141° 28' 15.82"
TANGENT = 7.93
LENGTH = 15.66
RADIUS = 40.50
EXTERNAL = 0.77
LONG CHORD = 15.56
MID. ORD. = 0.75
P.C. STATION = 21+46.45 N 1,796,139.70 E 1,060,030.05
P.T. STATION = 21+62.11 N 1,796,152.50 E 1,060,021.19
C.C. = N 1,796,168.72 E 1,060,058.30
BACK = N 45° 46' 09.23" W
AHEAD = N 23° 37' 06.37" W
CHORD BEAR = N 34° 41' 37.80" W

COURSE FROM PT PATH-2 TO PATH2 N 23° 17' 25.29" W DIST 20.00

POINT PATH2 N 1,796,170.87 E 1,060,013.28 STA 21+82.11

ENDING CHAIN PATH DESCRIPTION

ϕ CANAL ST

CHAIN CANALST CONTAINS:
CS1 CUR CANALST-1 CUR CANALST-2 CS2

BEGINNING CHAIN CANALST DESCRIPTION

POINT CS1 N 1,795,590.36 E 1,059,961.01 STA 300+00.00
COURSE FROM CS1 TO PC CANALST-1 N 4° 56' 26.59" W DIST 218.51

CURVE DATA

CURVE CANALST-1
P.I. STATION = 302+29.83 N 1,795,819.34 E 1,059,941.22
DELTA = 1° 37' 20.01" (LT)
DEGREE = 7° 09' 43.10"
TANGENT = 11.33
LENGTH = 22.65
RADIUS = 800.00
EXTERNAL = 0.08
LONG CHORD = 22.65
MID. ORD. = 0.08
P.C. STATION = 302+18.51 N 1,795,808.05 E 1,059,942.19
P.T. STATION = 302+41.16 N 1,795,830.59 E 1,059,939.92
C.C. = N 1,795,739.15 E 1,059,145.17
BACK = N 4° 56' 26.59" W
AHEAD = N 6° 33' 46.60" W
CHORD BEAR = N 5° 45' 06.59" W

COURSE FROM PT CANALST-1 TO PC CANALST-2 N 6° 33' 46.60" W DIST 93.88

CURVE DATA

CURVE CANALST-2
P.I. STATION = 303+72.18 N 1,795,960.75 E 1,059,924.95
DELTA = 5° 18' 57.99" (LT)
DEGREE = 7° 09' 43.10"
TANGENT = 37.14
LENGTH = 74.23
RADIUS = 800.00
EXTERNAL = 0.86
LONG CHORD = 74.20
MID. ORD. = 0.86
P.C. STATION = 303+35.04 N 1,795,923.86 E 1,059,929.19
P.T. STATION = 304+09.27 N 1,795,997.10 E 1,059,917.30
C.C. = N 1,795,832.42 E 1,059,134.44
BACK = N 6° 33' 46.60" W
AHEAD = N 11° 52' 44.59" W
CHORD BEAR = N 9° 13' 15.59" W

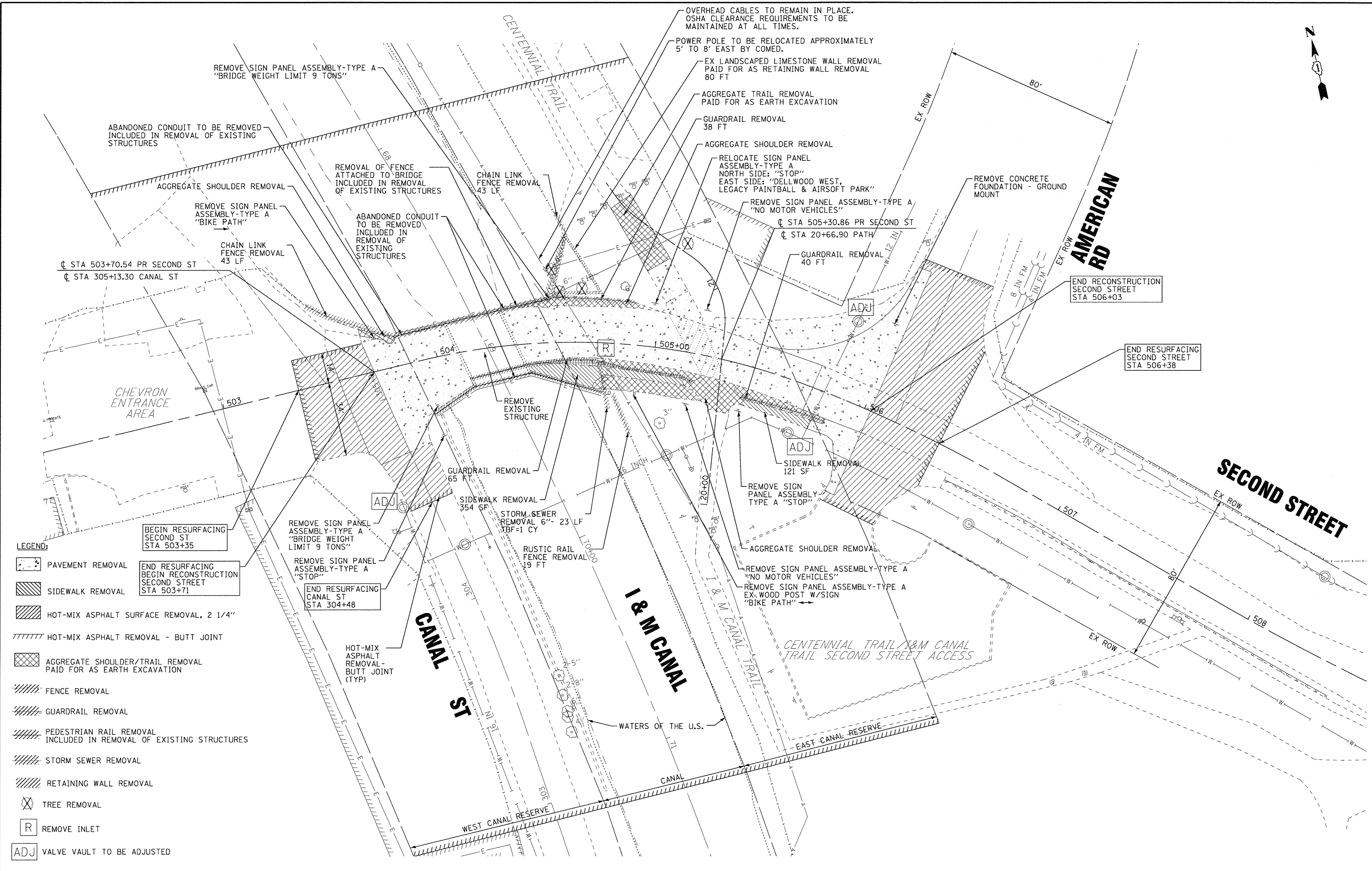
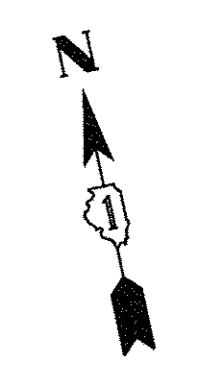
COURSE FROM PT CANALST-2 TO CS2 N 11° 52' 44.59" W DIST 104.04

POINT CS2 N 1,796,098.91 E 1,059,895.89 STA 305+13.30

ENDING CHAIN CANALST DESCRIPTION

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 STATE OF ILLINOIS PROFESSIONAL ENGINEERING EXAMINER
 LICENSE NO. - BR-00127017-APR-16
 PROJECT NO. - 131048SHT-Second St. Bridge Rehab. CADDA\Sheet's-Ph2\131048SHT_ALIGN.dgn
 DATE: 2/3/2017 10:49:45 AM

	DESIGNED - CAC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ALIGNMENTS, TIES, AND BENCHMARKS				MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - CJC	REVISED -		3035	13-00080-00-BR	WILL	42	13				
	CHECKED - ADJ	REVISED -		CONTRACT NO. 61D71								
	DATE - 01-27-17	FILE - 131048SHT_ALIGN.dgn		SCALE: NONE				STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	



LEGEND:

- PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- HOT-MIX ASPHALT REMOVAL - BUTT JOINT
- AGGREGATE SHOULDER/TRAIL REMOVAL PAID FOR AS EARTH EXCAVATION
- FENCE REMOVAL
- GUARDRAIL REMOVAL
- PEDESTRIAN RAIL REMOVAL INCLUDED IN REMOVAL OF EXISTING STRUCTURES
- STORM SEWER REMOVAL
- RETAINING WALL REMOVAL
- TREE REMOVAL
- REMOVE INLET
- VALVE VAULT TO BE ADJUSTED

BEGIN RESURFACING SECOND ST STA 503+35

END RESURFACING BEGIN RECONSTRUCTION SECOND STREET STA 503+71

REMOVE SIGN PANEL ASSEMBLY-TYPE A "STOP"

END RESURFACING CANAL ST STA 304+48

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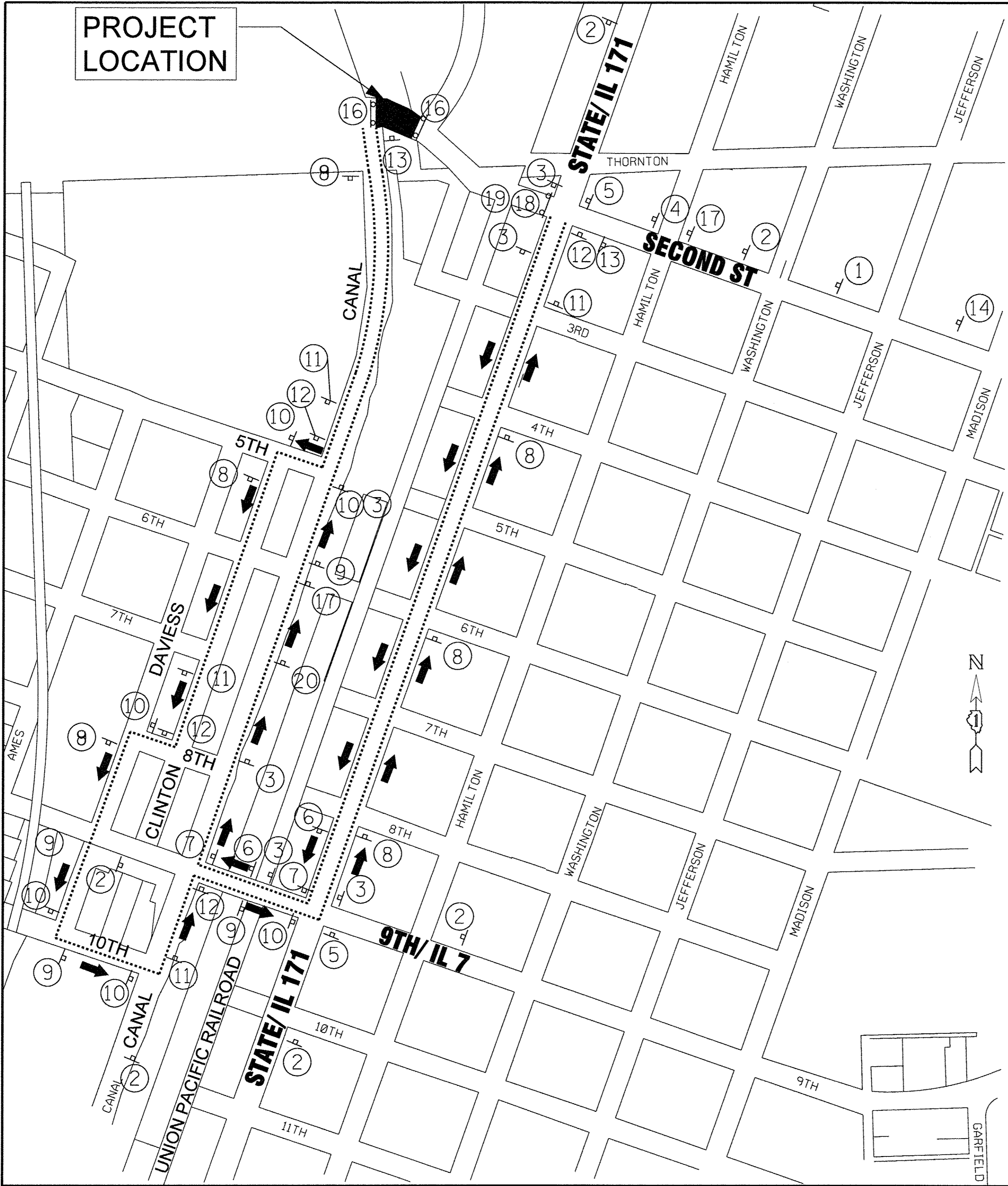
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DRAWN - CJC	REVISED -
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DATE - 01-27-17	FILE - 131048SHT_REMI.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SECOND STREET OVER ILLINOIS & MICHIGAN CANAL
EXISTING CONDITIONS AND REMOVAL PLAN**

SCALE: 1" = 20' STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	14
CONTRACT NO. 61D71				
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

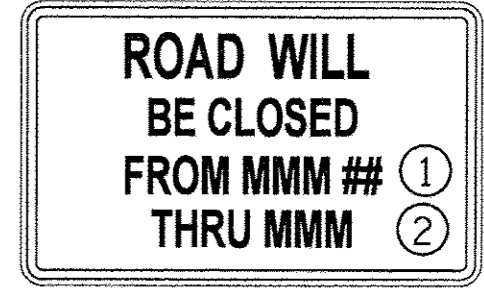


DETOUR MAP
NO SCALE

MAINTENANCE OF TRAFFIC NOTES:

- REFER TO DISTRICT DETAIL TC-21 FOR SIGN SPACING, EXCEPT AS INDICATED ON THE SUGGESTED MAINTENANCE OF TRAFFIC PLAN.
- THE CONTRACTOR SHALL NOTIFY IDOT AND THE CITY OF LOCKPORT (815-838-0549 OPT 8) 10 DAYS IN ADVANCE OF OPERATION OF THE DETOUR ROUTE.
- THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL). ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- EROSION AND SEDIMENT CONTROLS SHALL BE INSTALLED PRIOR TO START OF CONSTRUCTION WITH CONTINUED MAINTENANCE FOR THE DURATION OF CONSTRUCTION. SEE EROSION CONTROL SHEETS FOR ADDITIONAL INFORMATION.
- FULL ROADWAY CLOSURE AT THE BRIDGE IS PERMITTED DURING CONSTRUCTION. THE CONTRACTOR SHALL MAINTAIN EMERGENCY VEHICLE AND LOCAL TRAFFIC DURING THE CONSTRUCTION. THE COST FOR MAINTAINING ACCESS ON SECOND ST SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- THE CONTRACTOR SHALL PROVIDE TWO-WAY TRAFFIC ON SECOND STREET EAST OF AMERICAN ROAD. ACCESS TO AMERICAN ROAD AND THE CENTENNIAL TRAIL/I&M CANAL TRAIL SECOND STREET ACCESS PARKING LOT FROM IL 171 SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
- DETOUR SIGN ASSEMBLIES SHALL BE M4-9 SERIES.
- ALL SIGNS, BARRICADES, SIGN'S LIGHTS & FLAGS SHALL BE IN ACCORDANCE WITH THOSE INDICATED IN THE TRAFFIC CONTROL STANDARDS OF THE MUTCD & THE STANDARD SPECIFICATIONS.

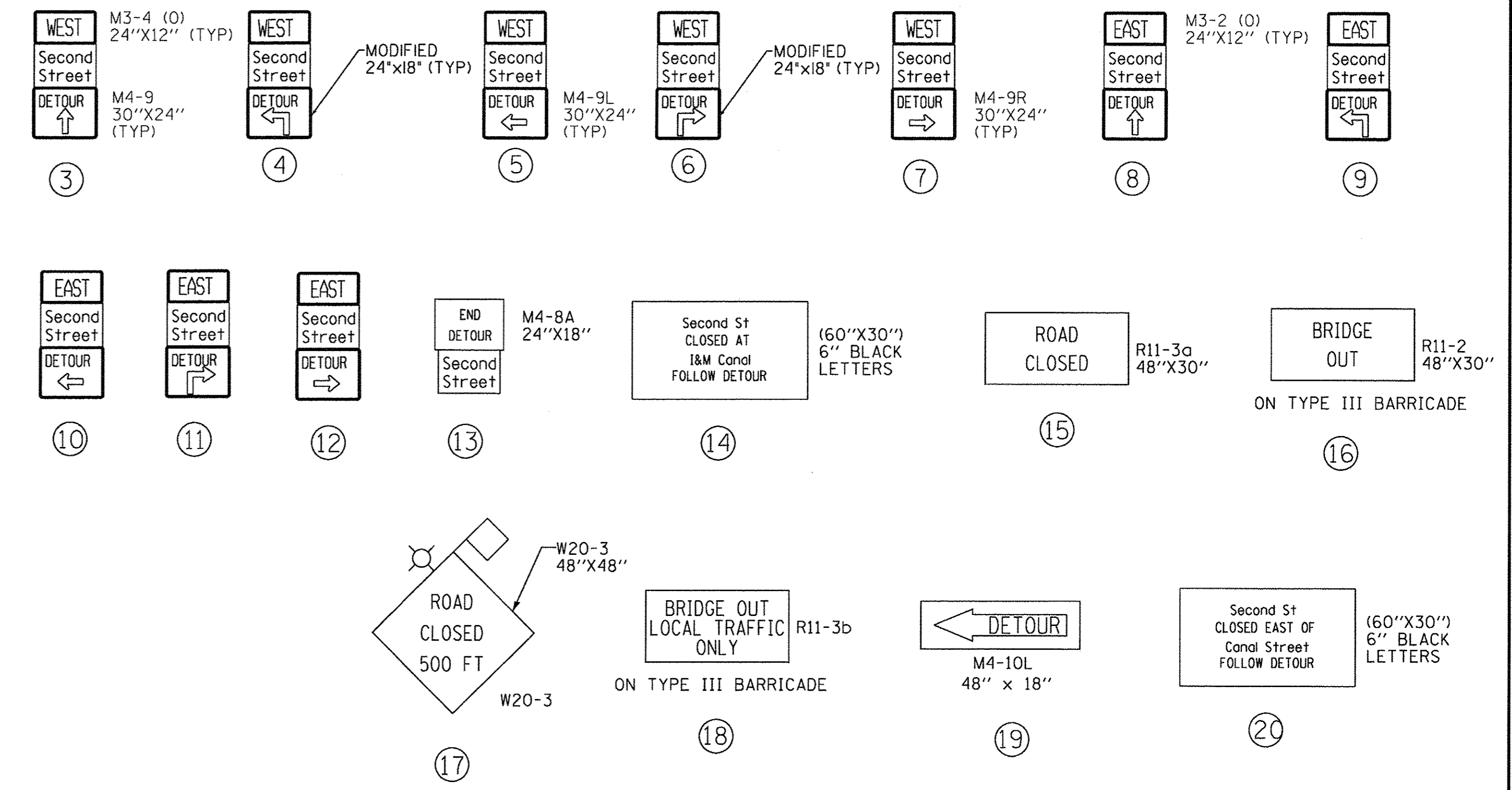
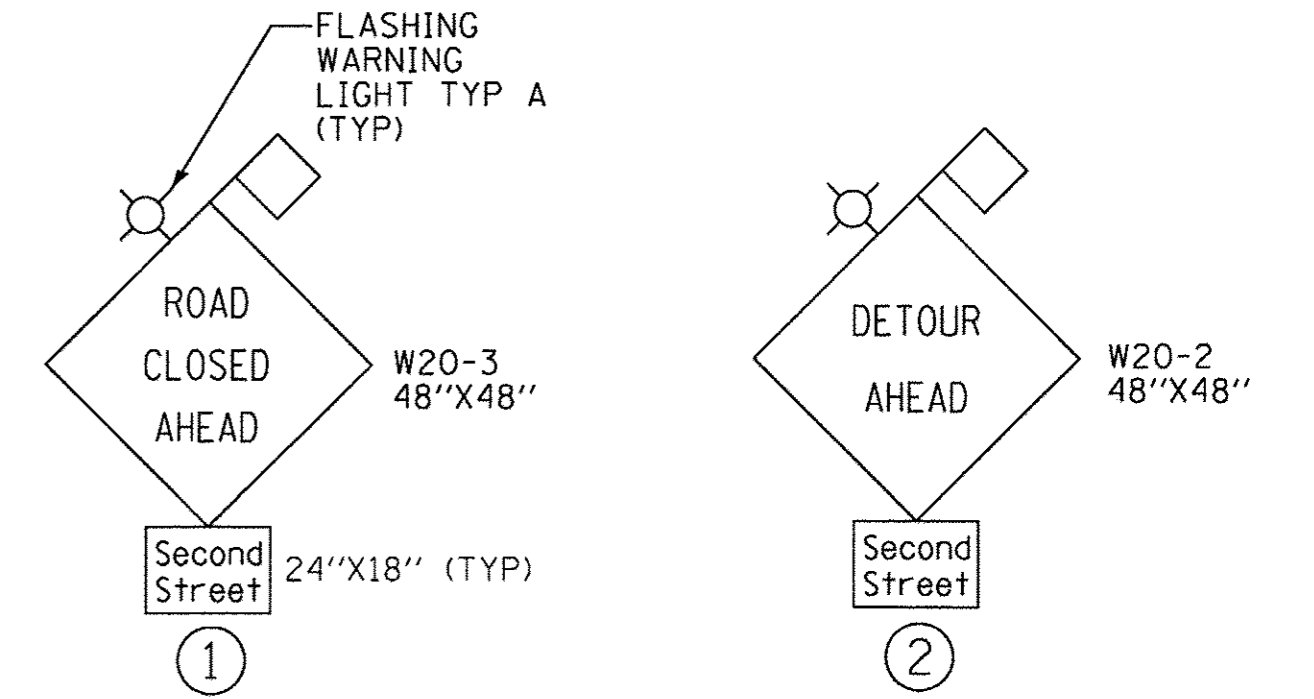
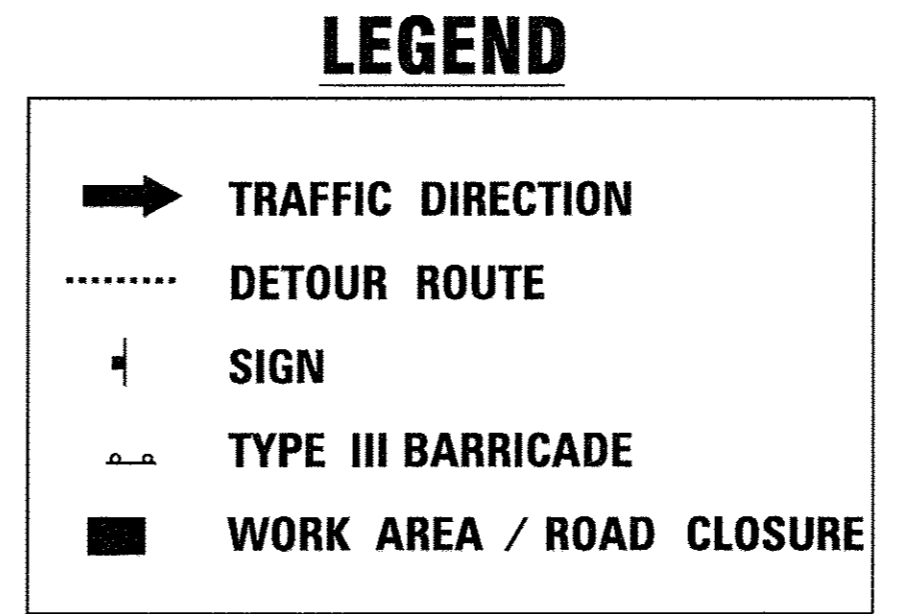
ARTERIAL ROAD INFORMATION SIGN FOR ROADS TO BE FULLY CLOSED AND DETOURED
SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING. ONE SIGN ASSEMBLY EQUALS 37.3 SQ. FT.



- OVERLAY PANEL ① TO CONTAIN STARTING DATE OF FULL CLOSURE AND DETOUR (i.e. "FROM APR 2")
- OVERLAY PANEL ② TO CONTAIN ENDING MONTH OF FULL CLOSURE & DETOUR (i.e. "THRU JULY") OMIT THE DATE ON PANEL ②; MONTH ONLY.

ERECT SIGN ASSEMBLY (POST-MOUNTED WITH PANELS ① AND ②) IN PLACE ON ROAD TO BE CLOSED IN EACH DIRECTION NEAR POINT OF CLOSURE OR WITHIN SECTION TO BE FULLY CLOSED TWO (2) WEEKS PRIOR TO START DATE OF FULL CLOSURE. REMOVE ASSEMBLY AFTER CLOSURE.

TEMPORARY INFORMATION SIGN DETAIL



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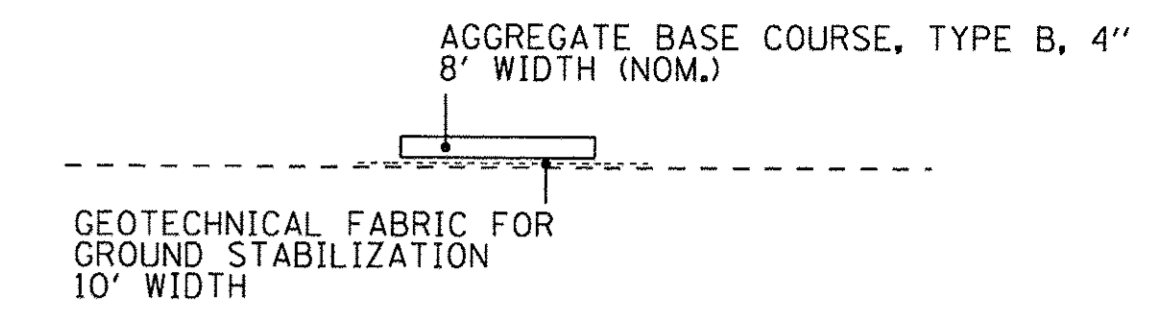
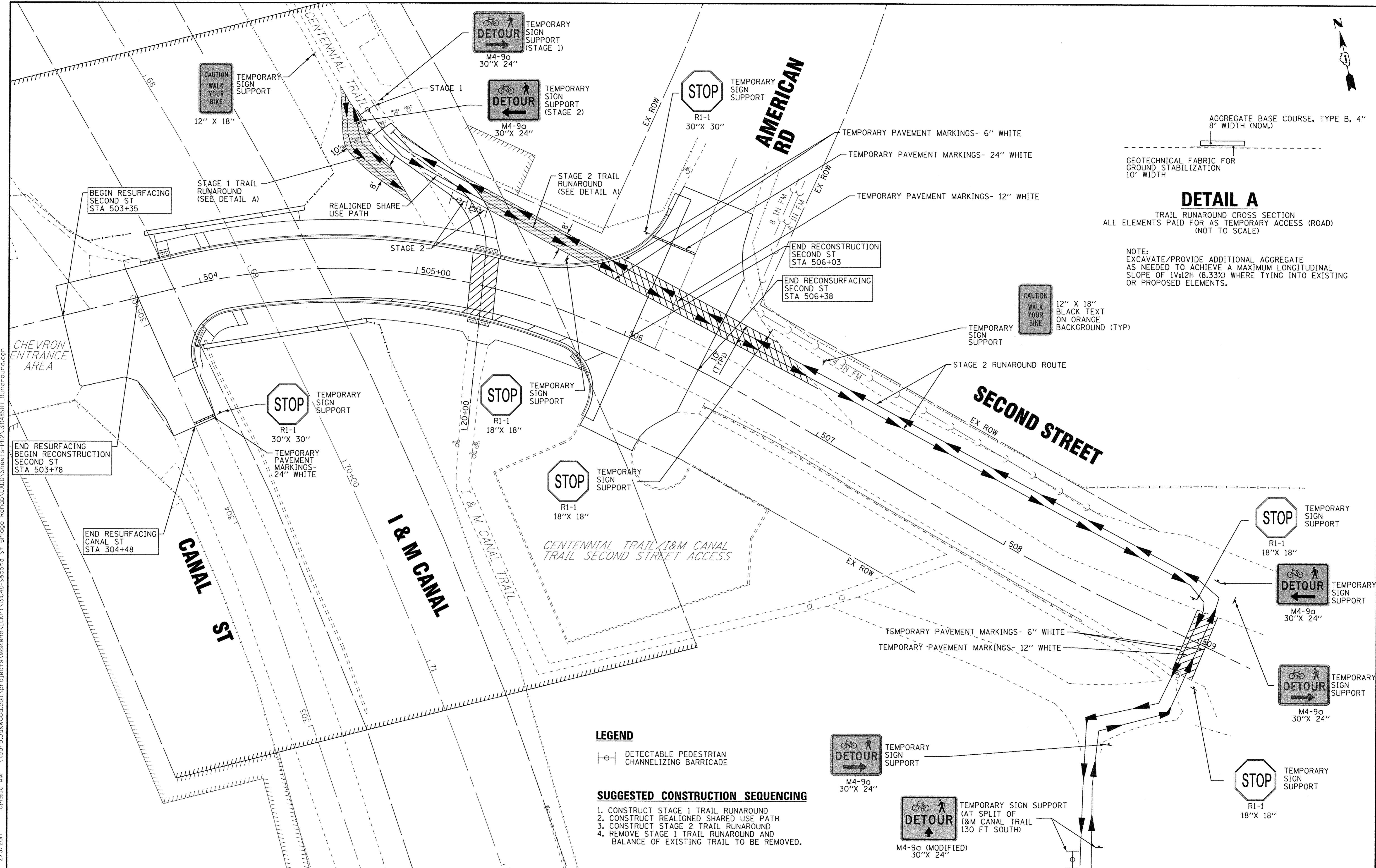
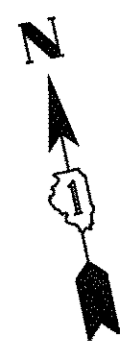
BAXTER & WOODMAN Consulting Engineers	DESIGNED - CAC	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_MOT-DeTour.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED MAINTENANCE OF TRAFFIC - DETOUR

SCALE: NONE	STA. TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT BR05-4003334
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PLAN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	16
				CONTRACT NO. 61D71

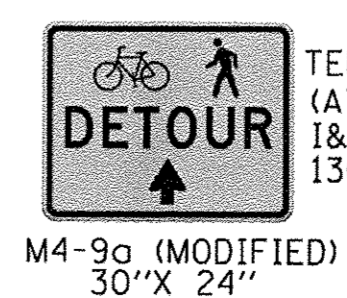
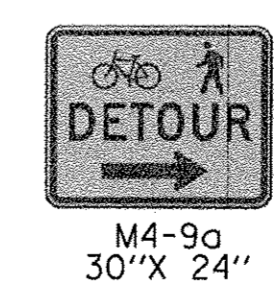


LEGEND

DETECTABLE PEDESTRIAN CHANNELIZING BARRICADE

SUGGESTED CONSTRUCTION SEQUENCING

1. CONSTRUCT STAGE 1 TRAIL RUNAROUND
2. CONSTRUCT REALIGNED SHARED USE PATH
3. CONSTRUCT STAGE 2 TRAIL RUNAROUND
4. REMOVE STAGE 1 TRAIL RUNAROUND AND BALANCE OF EXISTING TRAIL TO BE REMOVED.



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BAXTER & WOODMAN Consulting Engineers	DESIGNED - CAC	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_Runaround.dgn

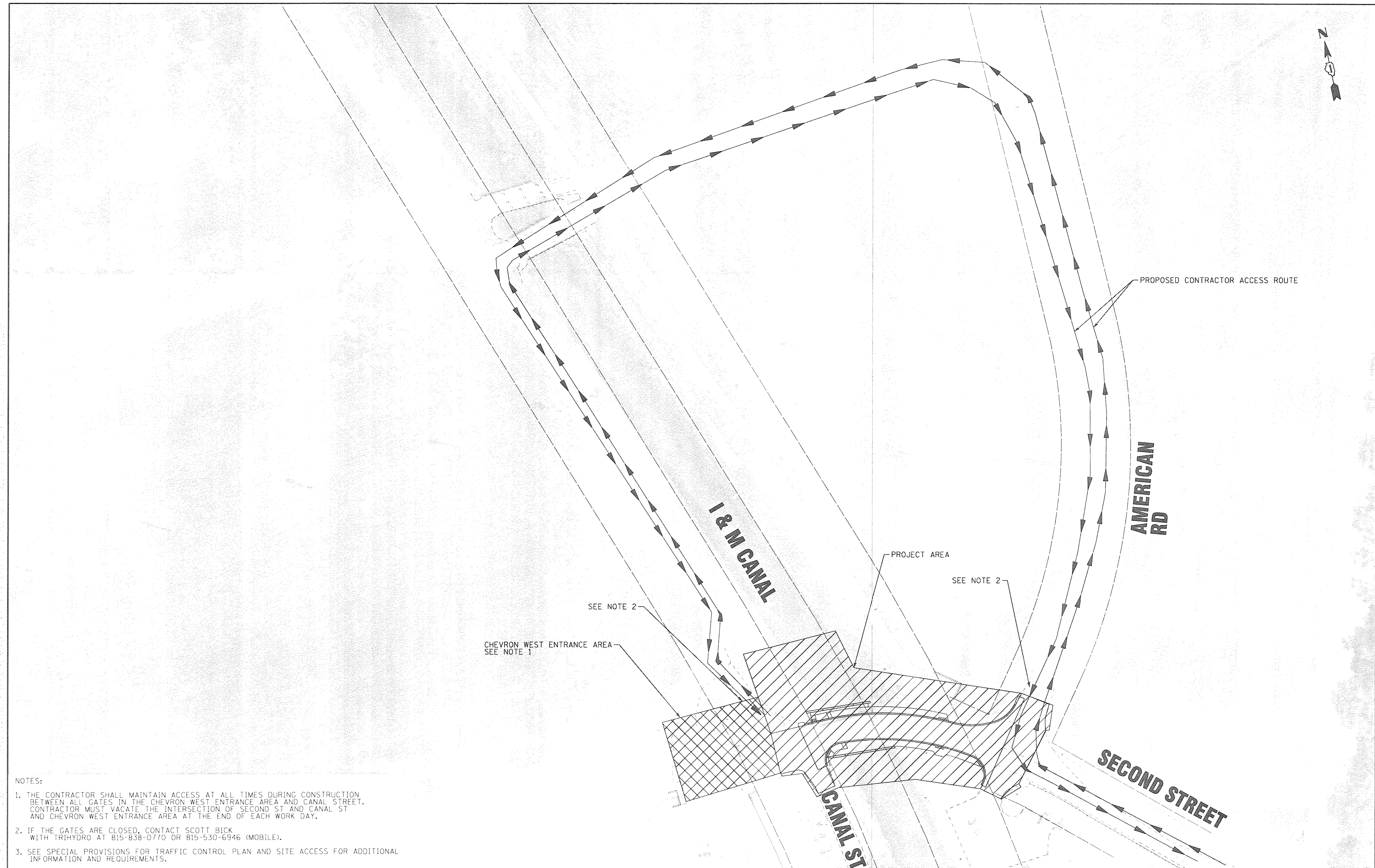
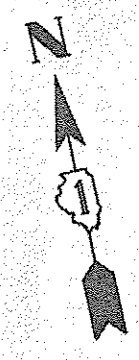
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC -
TRAIL RUNAROUND

SCALE: 1" = 20'

STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	17
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	



NOTES:

1. THE CONTRACTOR SHALL MAINTAIN ACCESS AT ALL TIMES DURING CONSTRUCTION BETWEEN ALL GATES IN THE CHEVRON WEST ENTRANCE AREA AND CANAL STREET. CONTRACTOR MUST VACATE THE INTERSECTION OF SECOND ST AND CANAL ST AND CHEVRON WEST ENTRANCE AREA AT THE END OF EACH WORK DAY.
2. IF THE GATES ARE CLOSED, CONTACT SCOTT BICK WITH TRIHYDRO AT 815-838-0770 OR 815-530-6946 (MOBILE).
3. SEE SPECIAL PROVISIONS FOR TRAFFIC CONTROL PLAN AND SITE ACCESS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.

BAXTER WOODMAN	DESIGNED	CAC	REVISED	
	DRAWN	CJC	REVISED	
	CHECKED	ADJ	REVISED	
	DATE	01-27-17	FILE	131048SHT_Contractor Access.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED CONTRACTOR ACCESS ROUTE

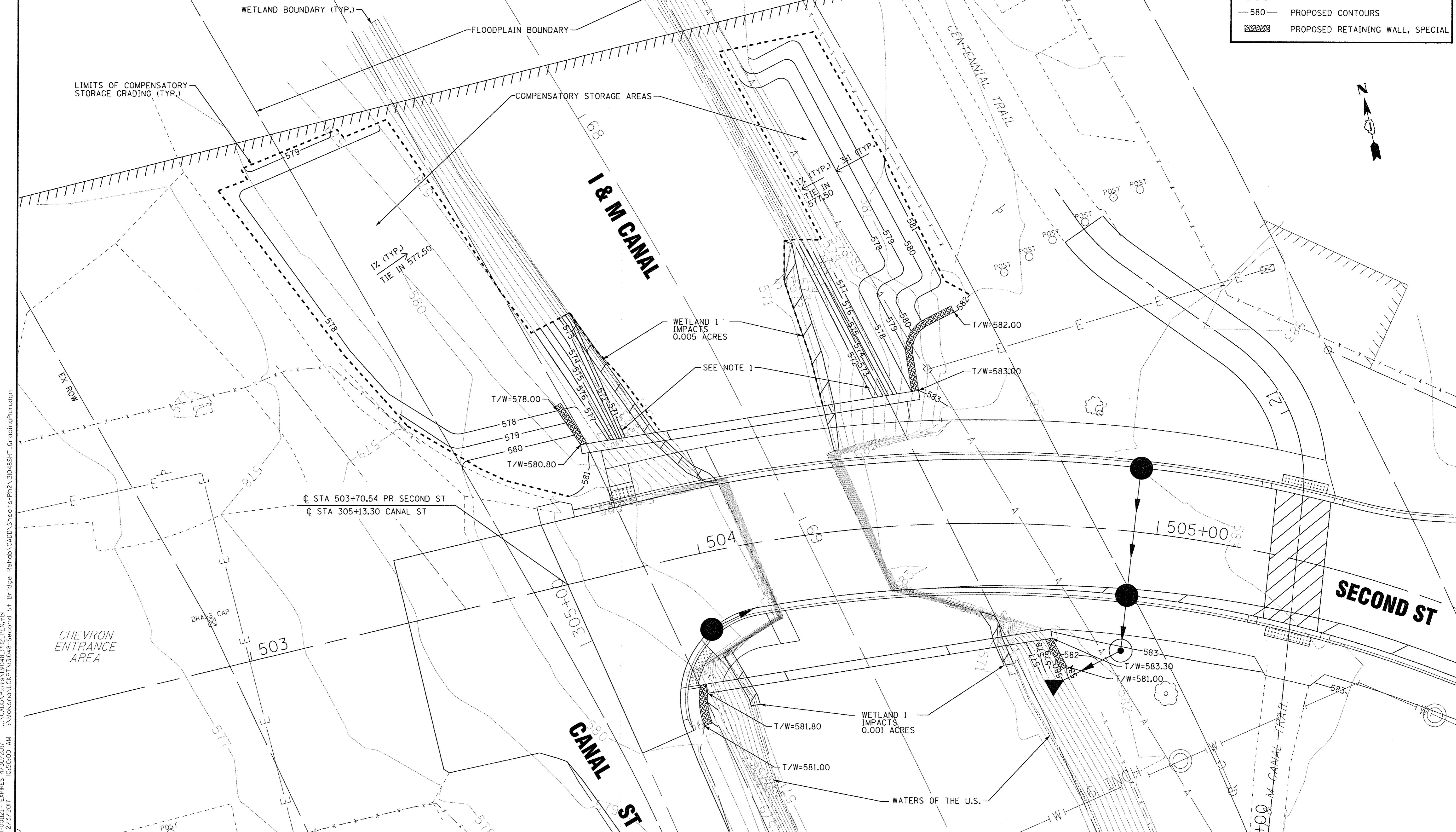
SCALE: 1"=50' STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5055	13 00080-00-BR	WILL	42	18
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	

NOTE

1. PROPOSED CONTOURS IN THIS AREA REPRESENT ASSUMED ROCK FACE/CANAL BANK
2. SEE SHEET 31 FOR RETAINING WALL DETAILS

LEGEND	
-----	ORDINARY HIGH WATER MARK
-----	WATERS EDGE
-580-	EXISTING CONTOURS
-580-	PROPOSED CONTOURS
	PROPOSED RETAINING WALL, SPECIAL



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 LICENSE NO. - BR-00127 - EXPIRES 6/30/2017
 FILE NO. - 131048SHT_131048 Second St Bridge Rehab VADef-BW.Dwg
 DATE - 2/27/2017 10:50:00 AM

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	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_GradingPlan.dgn

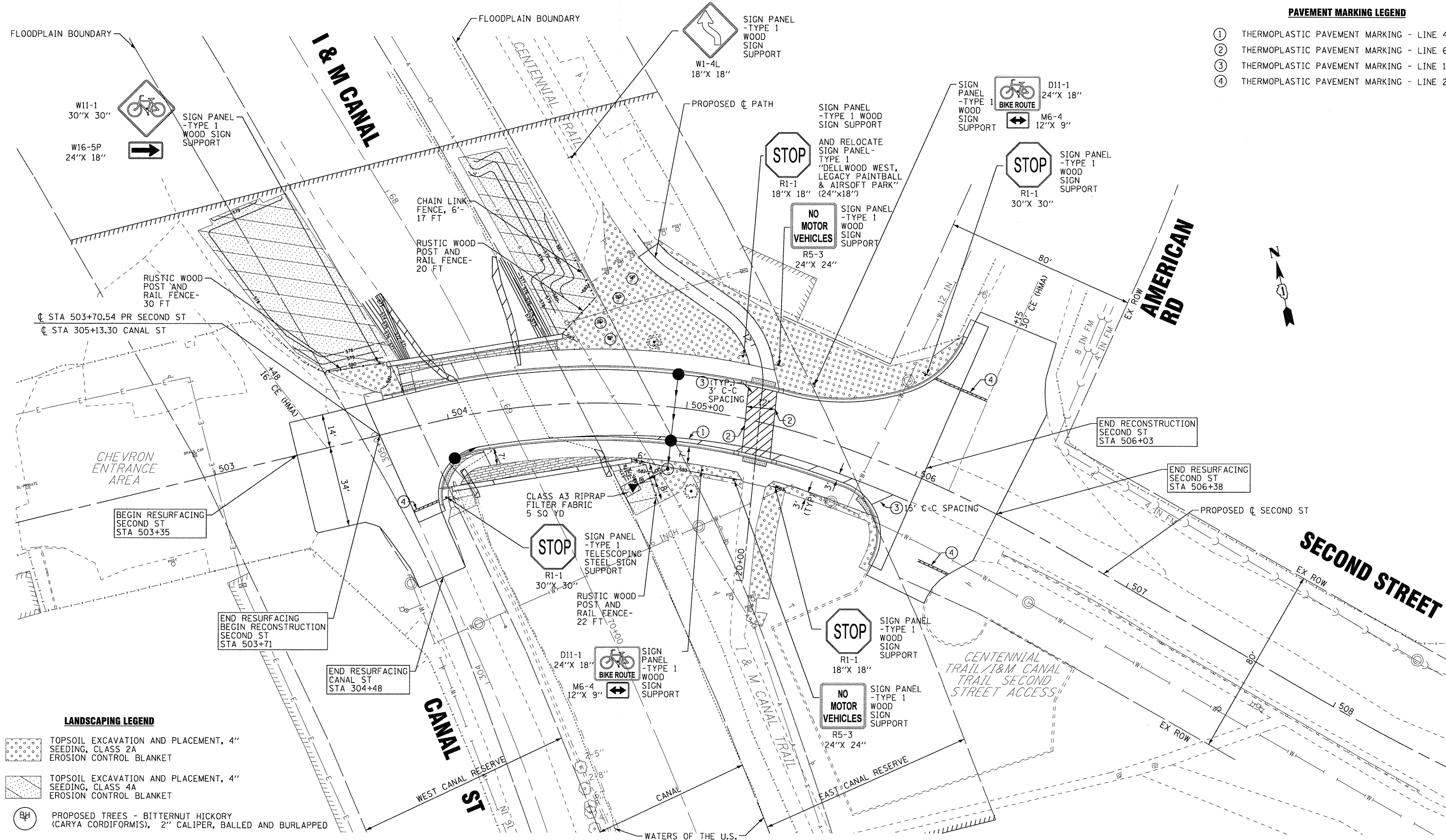
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SECOND STREET OVER ILLINOIS & MICHIGAN CANAL
GRADING PLAN

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	21
CONTRACT NO. 61D71				
SCALE: 1"=10'		STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

PAVEMENT MARKING LEGEND

- ① THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE)
- ② THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE)
- ③ THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE)
- ④ THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE)



LANDSCAPING LEGEND

-
-
-
-
-
-

NOTE:
THE LOCATION OF THE PROPOSED TREES ARE APPROXIMATE. FINAL TREE LOCATION SHALL BE DETERMINED AND APPROVED IN THE FIELD BY THE CITY FORESTER AND THE ENGINEER.

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 LICENSE NO. - 084-00121 - EXPIRES 4/30/2017
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	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT.PML.dgn

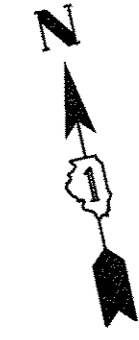
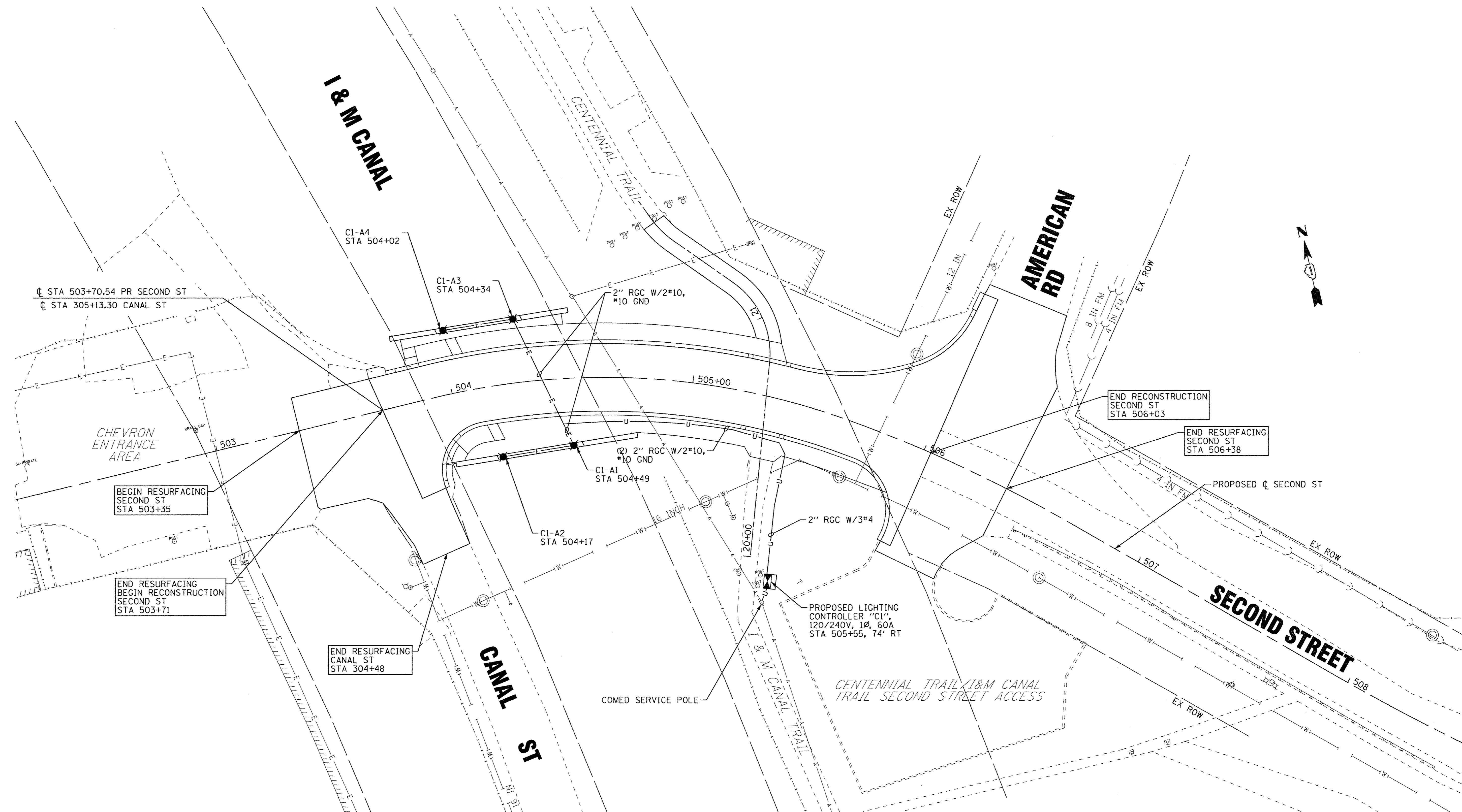
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING, SIGNING AND LANDSCAPING PLAN

SCALE: 1" = 20'

STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	22
CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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BAXTER & WOODMAN <small>Consulting Engineers</small>	DESIGNED - MWH	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_Lighting1.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

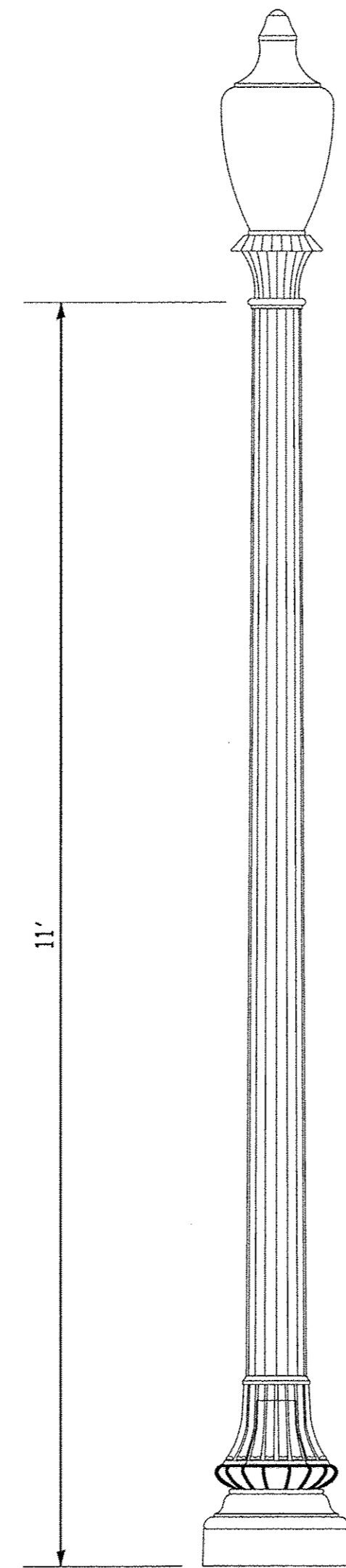
STREET LIGHTING PLAN

SCALE: 1" = 20'

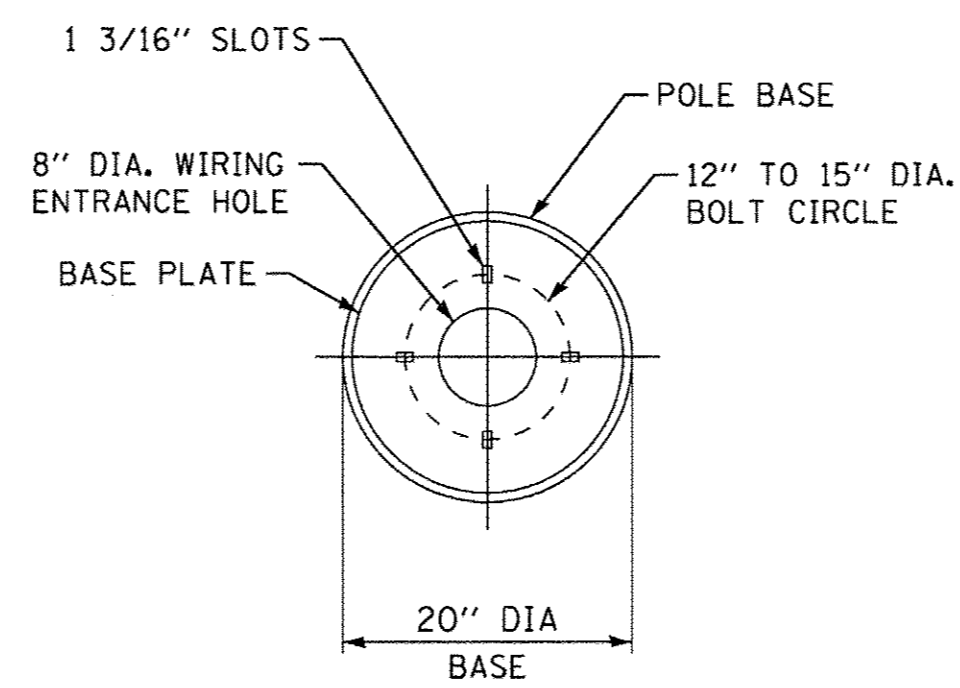
STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	23
CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

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 LICENSE NO. - 384-00121 - EXPIRES 4/30/2017
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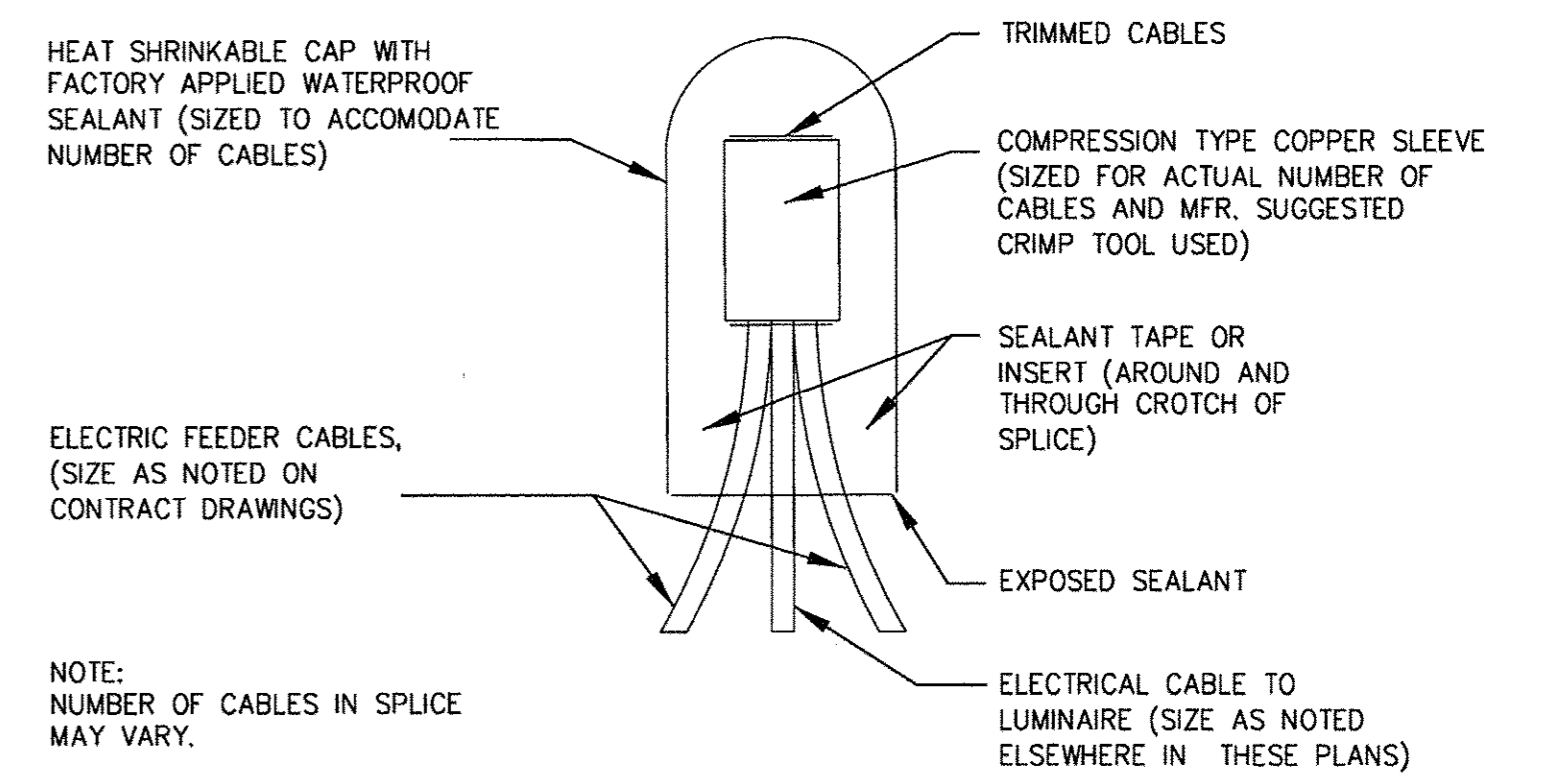


LIGHTING UNIT, BRIDGE MOUNTED
NO SCALE



ANCHOR BOLT DETAIL
NO SCALE

SUMMARY OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1
80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	165
81200230	CONDUIT EMBEDDED IN STRUCTURE, 2" DIA., PVC	FOOT	150
81702110	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 10	FOOT	720
81702140	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 4	FOOT	315
82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1
X8250500	LIGHTING UNIT COMPLETE, SPECIAL	EACH	4



SPLICING ELECTRICAL CABLES BASIC MATERIALS AND METHODS
NO SCALE

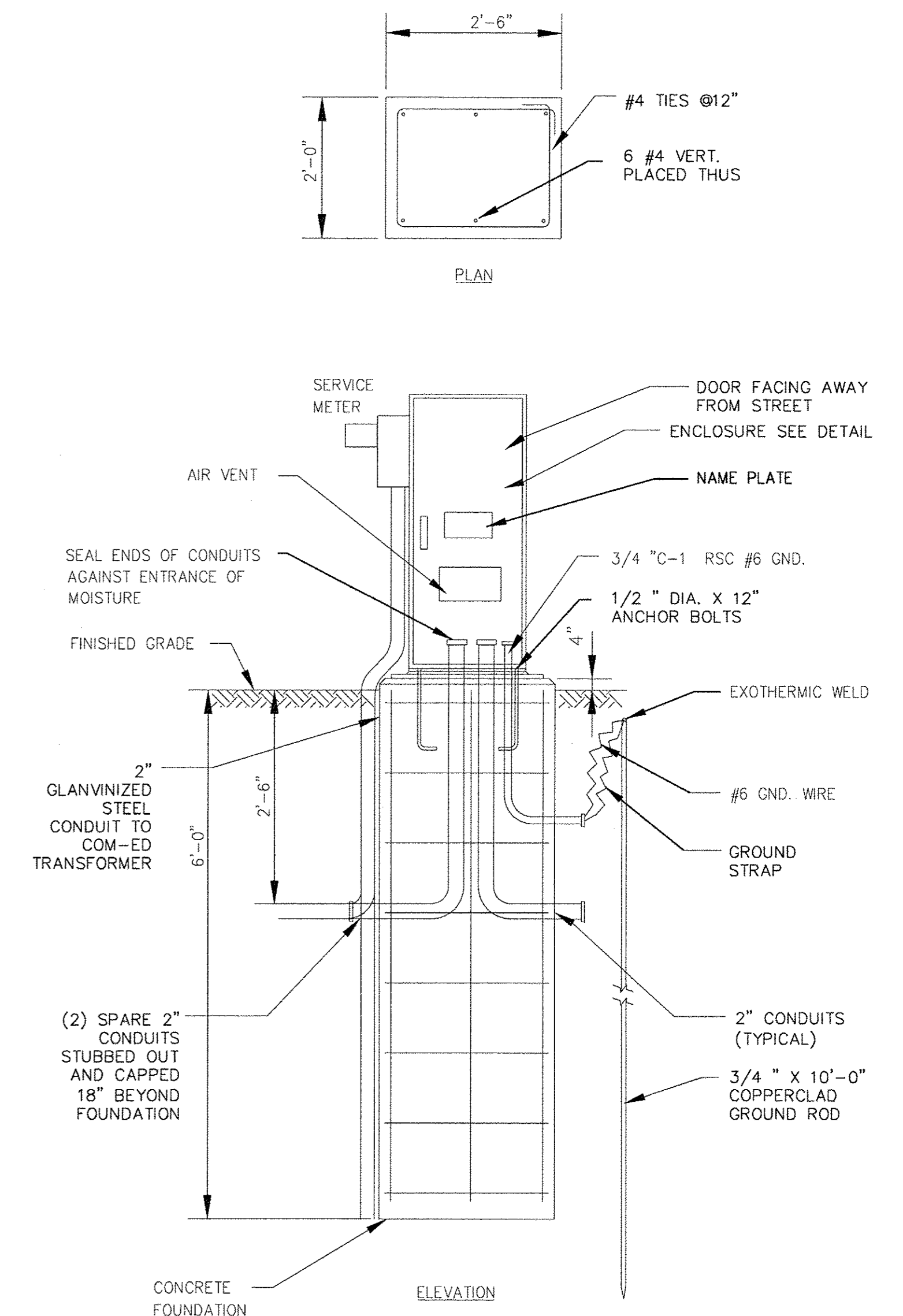
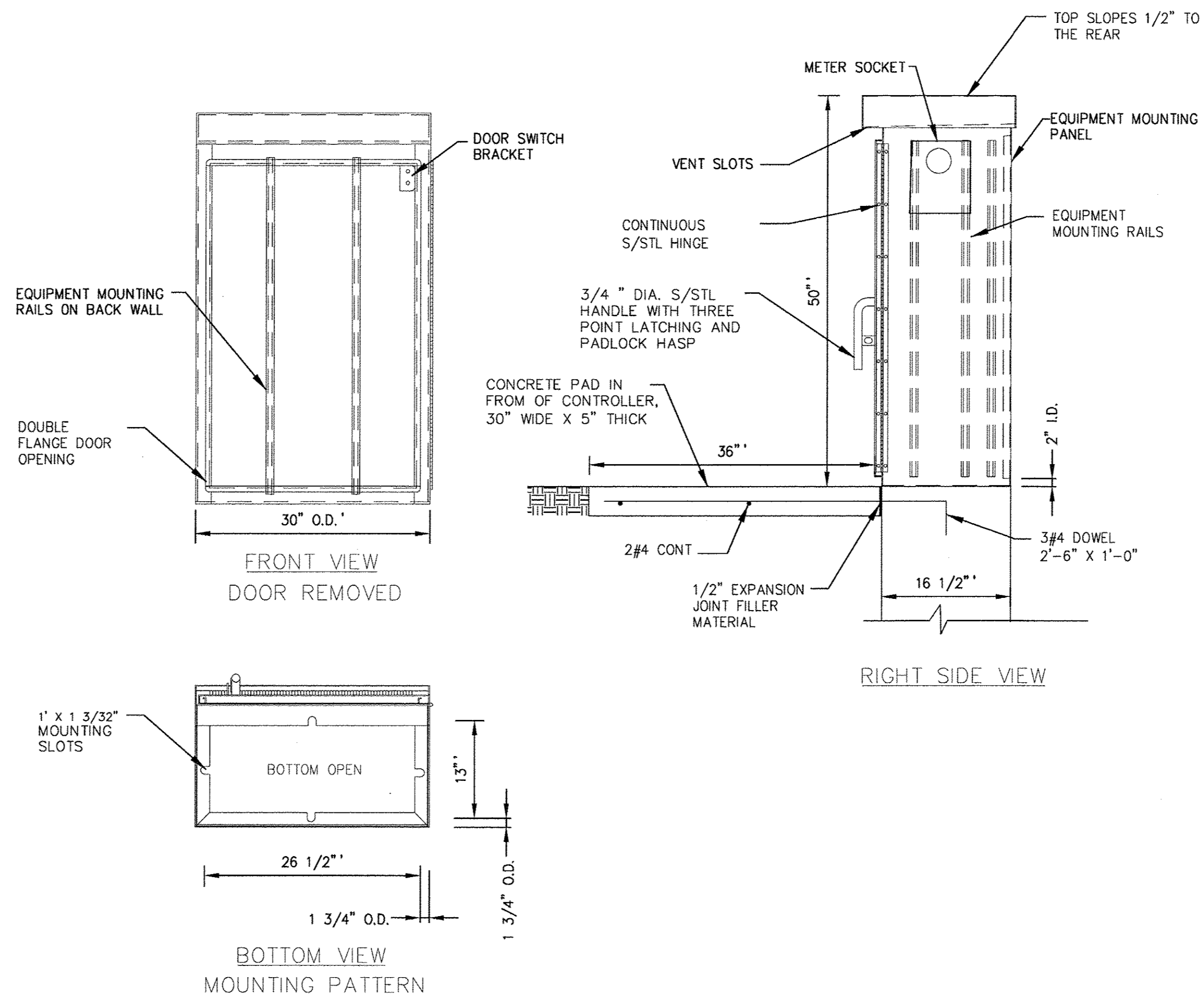
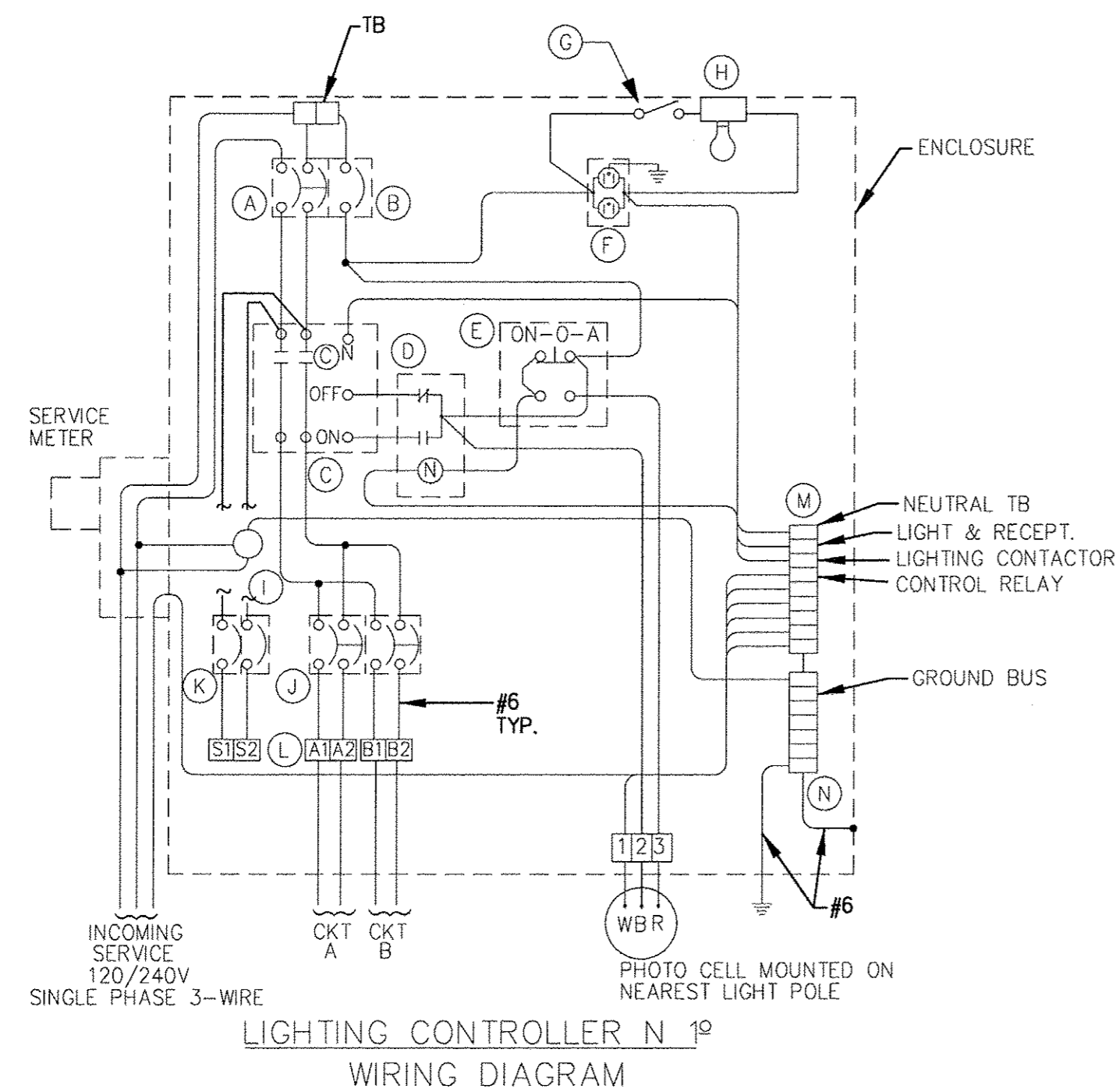


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DRAWN - CJC	REVISED -
CHECKED - ADJ	REVISED -
DATE - 01-27-17	FILE - 131048SHT_Lighting-Detail.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE: NONE		STA.	TO STA.
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MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	24
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	



LIGHTING CONTROLLER AND FOUNDATION DETAIL
NO SCALE

NOTE:
INSTALL (2) SPARE 2" CONDUITS STUBBED OUT AND CAPPED 18" BEYOND FOUNDATION.

NOTES:

- CABINET SHALL BE FABRICATED FROM 0.125-INCH SHEET ALUMINUM #3003H14, FORMED AND ARC WELDED ASSEMBLY WITH NEMA 3R RATING.
- ALL SCREWS AND HARDWARE SHALL BE PLATED, GALVANIZED, OR MADE OF BRASS, ALUMINUM OR STAINLESS STEEL.
- NAME PLATE SHALL HAVE ENGRAVED 0.75-INCH HIGH LETTERS FILLED IN BLACK: "STREET LIGHTING".
- CABINET SHALL BE PRIMED AND PAINTED AS SPECIFIED.
- ELECTRIC UTILITY METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET AS SHOWN ON THE PANEL LAYOUT DIAGRAM.
- THE COMPLETED CONTROLLER SHALL BE U.L. LISTED AS AN INDUSTRIAL CONTROL PANEL UNDER UL508.
- METAL MOUNTING PANEL SHALL BE #10 GAUGE GALVANIZED SHEET STEEL FLANGED BACK 0.75-INCHES I.D. ON 4 SIDES.
- CIRCUIT BREAKERS AND CONTACTORS AND OTHER COMPONENTS SHALL BE MOUNTED ON 0.125-INCH THICK GLASTIC INSULATION BACK PANEL.
- ALL DEVICES SHALL BE FRONT REMOVABLE.
- BUS BAR SHALL HAVE 22 LUG TERMINALS SIZED TO ACCOMMODATE REQUIRED WIRE SIZES. NEUTRAL BUS SHALL BE PAINTED WHITE. GROUND BUS SHALL BE PAINTED GREEN.
- ALL LUGS SHALL BE COPPER SCREWS AND CONNECTORS, SPRING HELD.
- ALL WIRING TERMINATIONS SHALL BE RATED NOT LESS THAN 75 DEGREE CENTIGRADE.
- ALL CONTROL WIRING SHALL BE 600V MACHINE TOOL WIRE TYPE MTW.
- ALL POWER WIRING SHALL BE 600V TYPE RHH/RHW.
- A LAMINATED COPY OF THE CIRCUIT SCHEMATIC DIAGRAM SHALL BE ATTACHED TO THE INSIDE OF THE CONTROLLER.
- ALL 120 VOLT SYSTEM AND ALL CONTROL WIRING SHALL BE #12 AWG STRANDED UNLESS OTHERWISE INDICATED.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

ITEM	QTY.	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, 240 V. SINGLE-PHASE, 60 A., BOLT-ON TYPE, TRIP INTERRUPTING RATING OF 22,000 RMS SYMMETRICAL AMPERES AT 240 V.
B	1	CONTROL CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, SINGLE-POLE, 120 V. SINGLE PHASE 20 A. BOLT-ON TYPE, TRIP INTERRUPTING RATING OF 14,000 RMS SYMMETRICAL AMPERES AT 120 V.
C	1	LIGHTING CONTACTOR MECHANICALLY HELD, 100 A. 2-POLE, 600 V. WITH 120 COIL
D	1	CONTROL RELAY RATED 12 A. AT 120 VAC
E	1	ON-OFF-AUTO 3-POSITION SELECTOR SWITCH HEAVY DUTY SWITCH, RATED FOR 10 A. AT 600 VAC.
F	1	GFCI RECEPTACLE, 120 V., 20 A. SPEC. GRADE, NEMA CONFIG. 5-20R
G	1	SPDT MOMENTARY NORMALLY OPEN, NORMALLY CLOSED PUSHBUTTON RATED 15 A. AT 120 V.
H	1	60 WATT LIGHT FIXTURE, VAPOR TIGHT, WITH GLOBE, GUARD AND MOUNTING BOX.
I	1	SECONDARY SURGE ARRESTER, 175 VAC PHASE-TO-GROUND MAXIMUM, 10,000 AMPS
J	4	BRANCH CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 2-POLE, 240 V. SINGLE-PHASE, 30A. TRIP INTERRUPTING RATING 10,000 RMS SYMMETRICAL AMPERES AT 240 V.
K	2	BRANCH CIRCUIT BREAKER, MOLDED CASE, THERMAL MAGNETIC, 1-POLE, 120 V. SINGLE-PHASE, 20A. TRIP INTERRUPTING RATING 10,000 RMS SYMMETRICAL AMPERES.
L	1	TERMINAL BLOCK RATED 600 V., 85 A.
M	1	COPPER NEUTRAL BUS
N	1	COPPER GROUND BUS

BAXTER & WOODMAN
Consulting Engineers

DESIGNED - CAC	REVISED -
DRAWN - CJC	REVISED -
CHECKED - ADJ	REVISED -
DATE - 01-27-17	FILE - 131048SHT_Lighting-Details.dgn

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHTING DETAILS

SCALE: NONE

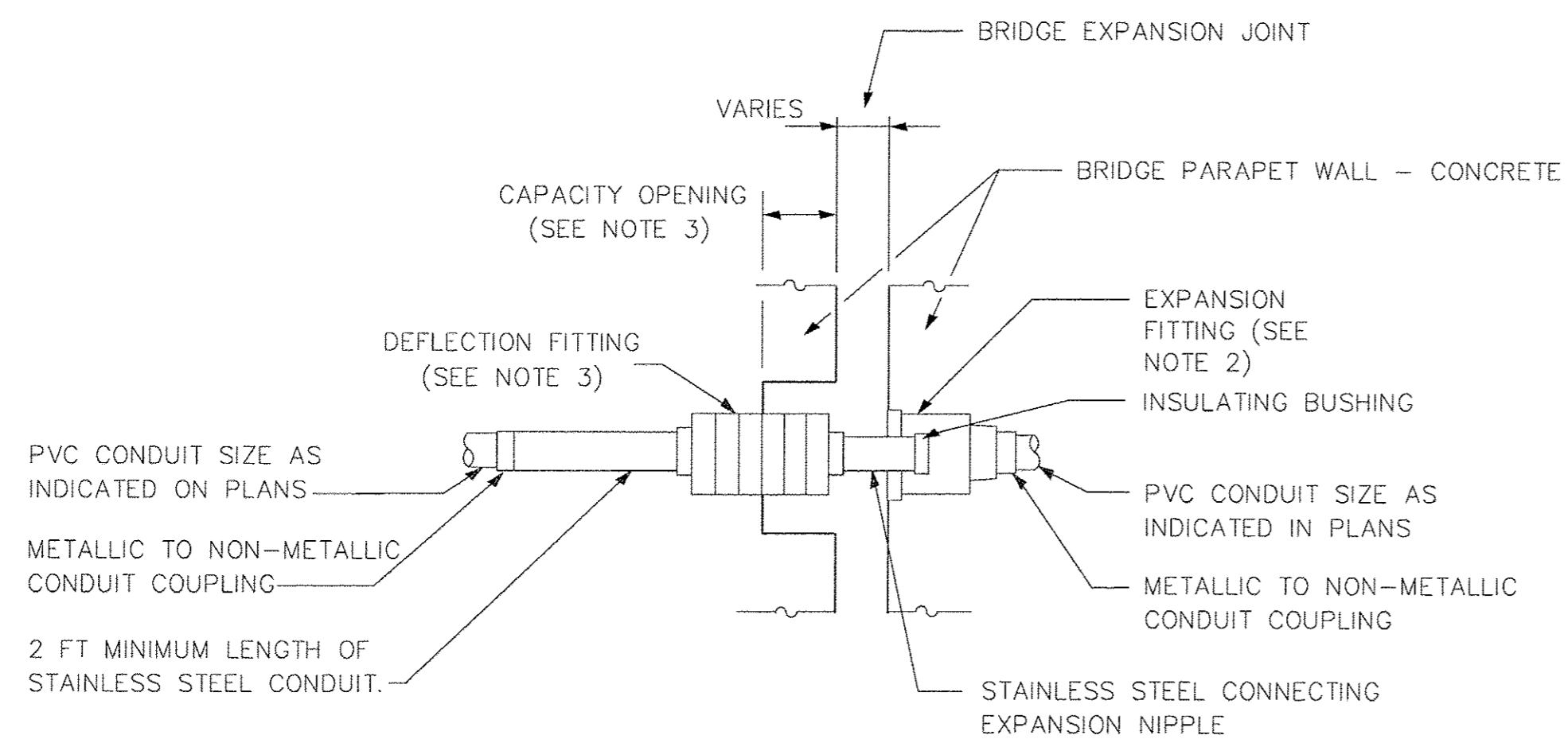
STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS/FED. AID PROJECT				

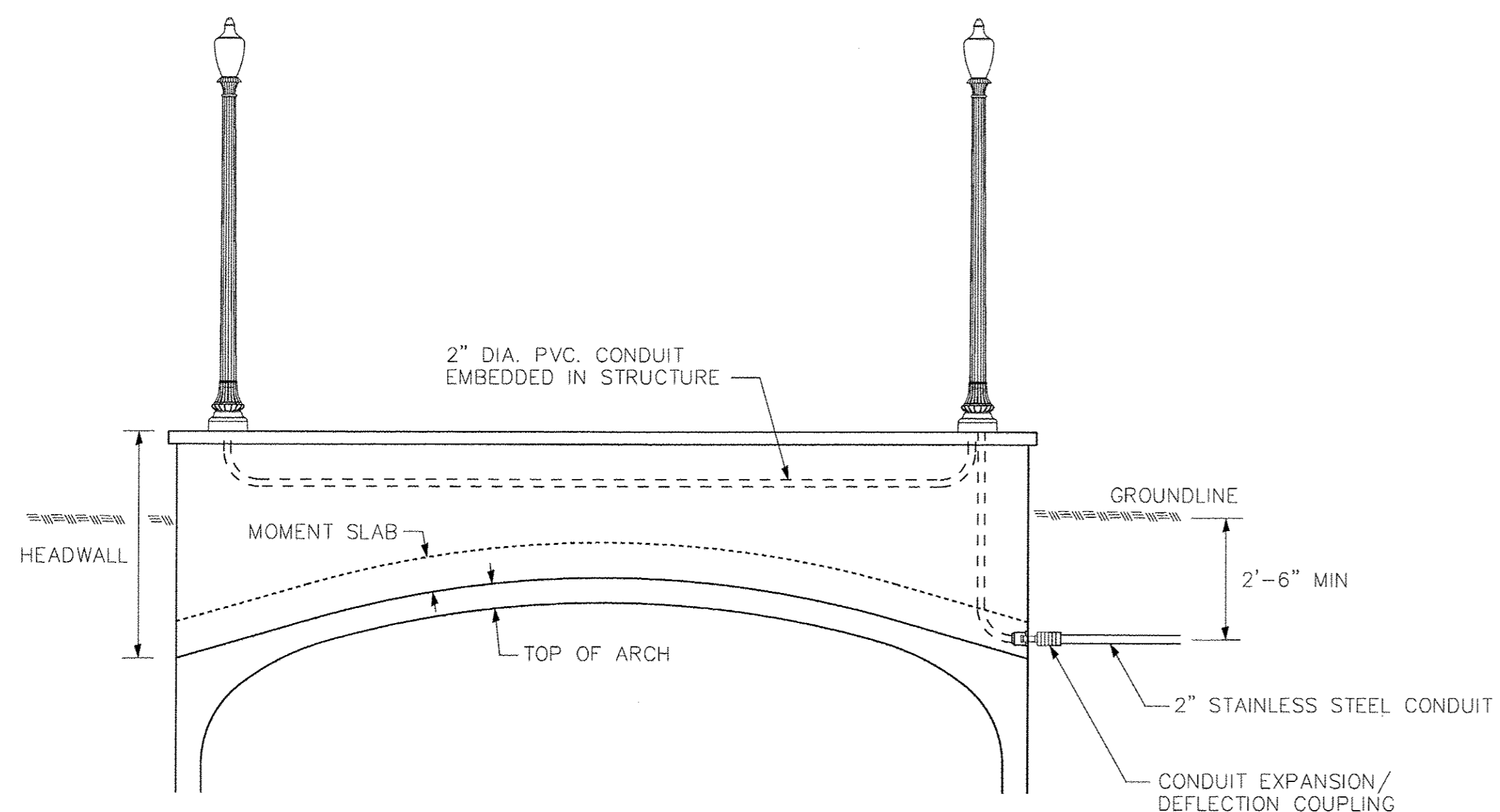
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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 184-00121 - EXPIRES 4/30/2017
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CONDUIT EXPANSION DEFLECTION COUPLING NOTES

1. THE CONTRACTOR SHALL INSTALL A CONDUIT EXPANSION/DEFLECTION COUPLING AT THE JOINTS IN THE CONCRETE PARAPET ON THE BRIDGE CAPABLE OF ACCEPTING THE LONGITUDINAL MOVEMENT. ALL METALIC PARTS OF THE COUPLING SHALL BE MADE OF STAINLESS STEEL OR AS APPROVED BY THE ENGINEER. ANY NON-STAINLESS METAL SHALL BE HOT DIP GALVANIZED AND COATED TO PREVENT REACTION WITH THE CONCRETE. THE COST OF THE COUPLING SHALL BE PART OF AND INCIDENTAL TO THE CONDUIT SYSTEM.
2. THE BARREL IN THE EXPANSION FITTING SHALL BE FULLY EMBEDDED IN THE CONCRETE ON ONE SIDE OF THE EXPANSION JOINT. ONE HALF THE LENGTH OF THE DEFLECTION FITTING SHALL BE EMBEDDED IN THE CONCRETE ON THE OTHER SIDE OF THE COUPLING.
3. A CAVITY OPENING 3" LARGER IN DIAMETER THAN THE DEFLECTION FITTING SHALL BE PROVIDED IN THE CONCRETE TO ENSURE PROPER PERFORMANCE OF THE COUPLING.
4. CAREFUL ATTENTION TO JOINT MOVEMENT OVER A RANGE OF TEMPERATURES SHALL BE COORDINATED WITH THE SELECTION AND INSTALLATION OF THE COUPLING TO ENSURE THE RANGE OF MOVEMENT OF THE COUPLING IS NOT EXCEEDED AT TEMPERATURE EXTREMES.
5. ALL MANUFACTURER'S INSTALLATION INSTRUCTIONS SHALL BE CAREFULLY FOLLOWED TO ENSURE OPTIMUM PERFORMANCE OF THE EXPANSION/DEFLECTION COUPLING.
6. THE CONTRACTOR SHALL INSTALL COUPLINGS AT ALL BRIDGE EXPANSION JOINTS AND SHALL BE RESPONSIBLE TO DETERMINE THE PROPER NUMBER OF COUPLINGS REQUIRED.

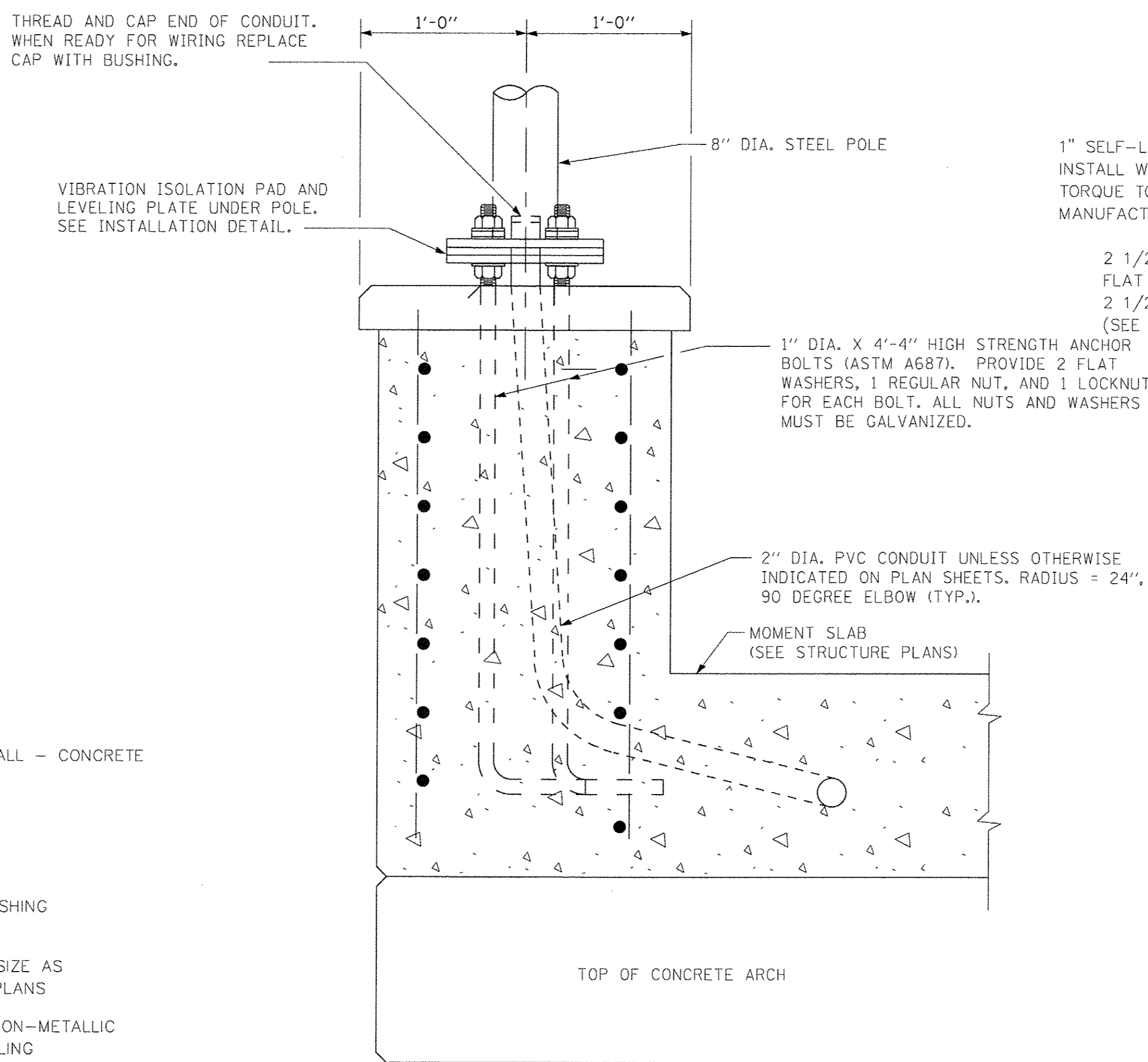


CONDUIT EXPANSION/DEFLECTION COUPLING DETAIL



CONDUIT AT HEADWALL NOTES

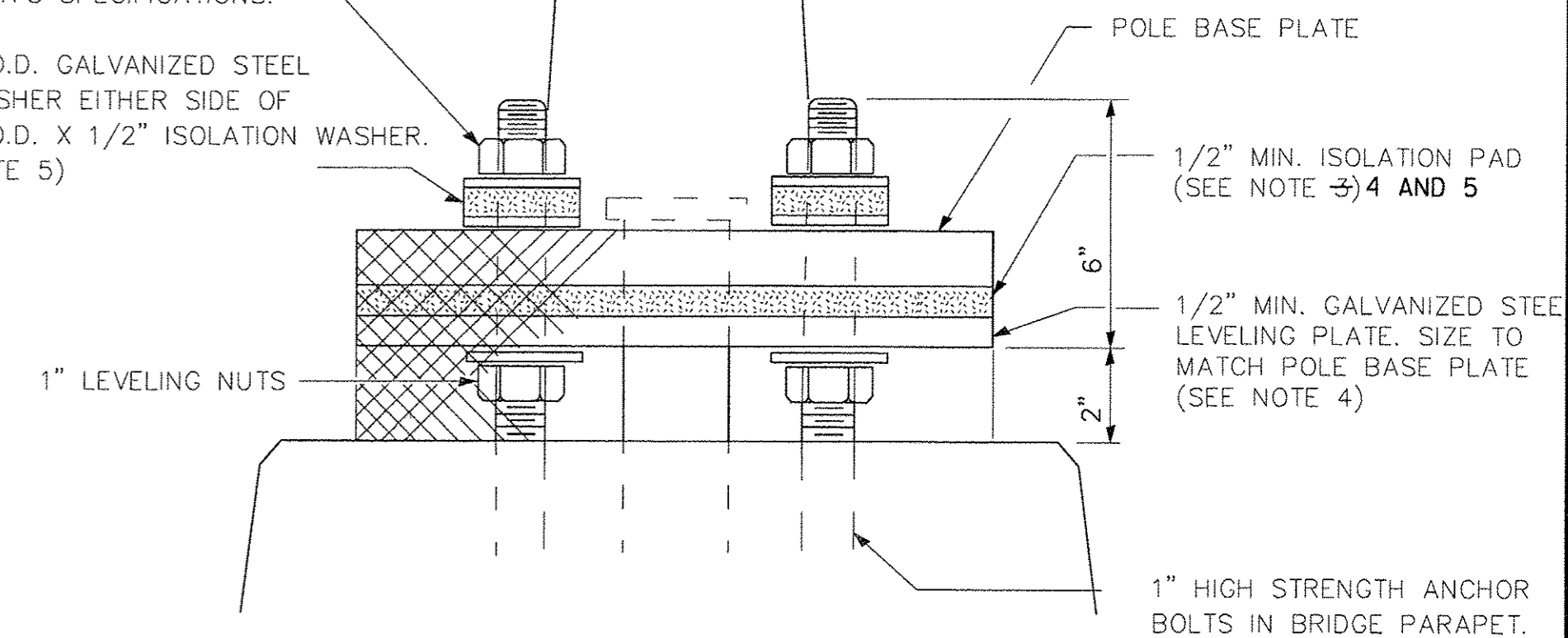
1. STAINLESS STEEL CONDUIT, COUPLINGS, AND ELBOWS SHALL BE ACCORDING TO SECTION 810 OF THE STANDARD SPECIFICATIONS, AS APPLICABLE, SHALL BE TYPE 304 OR TYPE 316, AND SHALL BE MANUFACTURED ACCORDING TO UL STANDARD 6A AND ANSI STANDARD C 80.1.
2. CONDUIT FITTINGS SHALL BE THE THREADED TYPE, SHALL BE TYPE 304 OR TYPE 316 STAINLESS STEEL, AND SHALL BE MANUFACTURED ACCORDING TO UL STANDARD 514B.



SECTION A-A

1" SELF-LOCKING NUT OR DOUBLE NUT. INSTALL WITH TORQUE WRENCH AND TORQUE TO THE ISOLATION PAD MANUFACTURER'S SPECIFICATIONS.

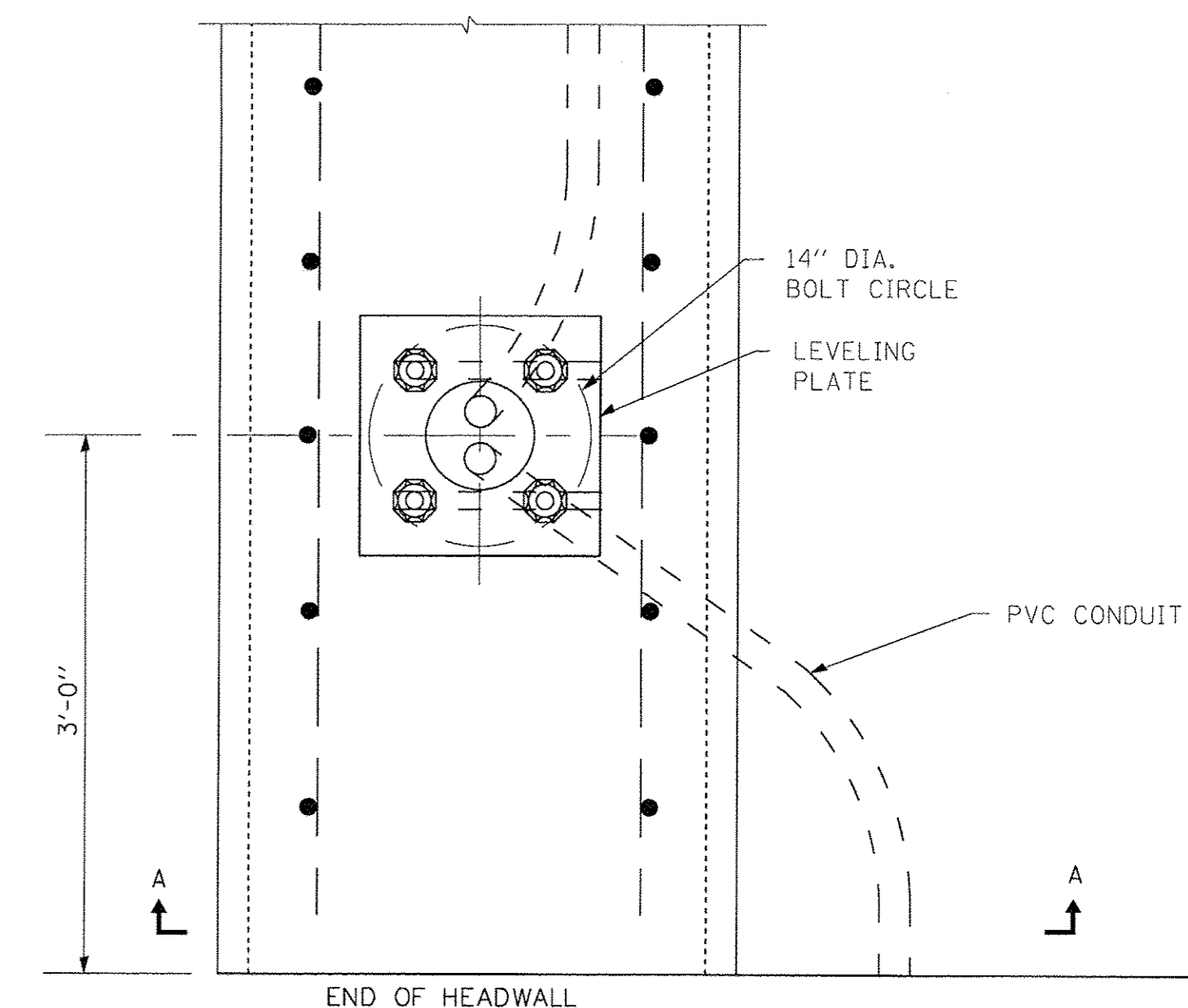
2 1/2" O.D. GALVANIZED STEEL FLAT WASHER EITHER SIDE OF 2 1/2" O.D. X 1/2" ISOLATION WASHER. (SEE NOTE 5)



POLE MOUNTED ON BRIDGE PARAPET (INSTALLATION DETAIL)

PARAPET MOUNTED LIGHT POLE NOTES

1. THE OUTLINE OF THE POLE LEVELING PLATE AND VIBRATION ISOLATION PAD SHALL MATCH THAT OF THE POLE BASE PLATE.
2. THE COST OF ANCHOR BOLTS, AND FOUNDATION IS INCLUDED WITH CONCRETE SUPERSTRUCTURE. SEE BRIDGE PLANS.
3. ANCHOR BOLTS SHALL BE THREADED A MINIMUM OF 6 INCHES WITH A MINIMUM OF 3" OF THREADED ANCHOR BOLTS EMBEDDED IN THE FOUNDATION AND PROJECT A MINIMUM OF 2 3/4" ABOVE THE TOP OF THE FOUNDATION.
4. THE VIBRATION ISOLATION PAD AND LEVELING PLATE SHALL MATCH THE FOOTPRINT OF THE POLE BASE PLATE.
5. THICKNESS OF ISOLATION PAD AND WASHERS SHALL BE ACCORDING TO THE ISOLATION PAD MANUFACTURER'S RECOMMENDATIONS BASED UPON POLE HEIGHT AND LOADING.



PLAN LIGHT POLE MOUNTED ON CONCRETE HEADWALL

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	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_Lighting-Details.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: NONE

LIGHTING DETAILS

STA. TO STA.

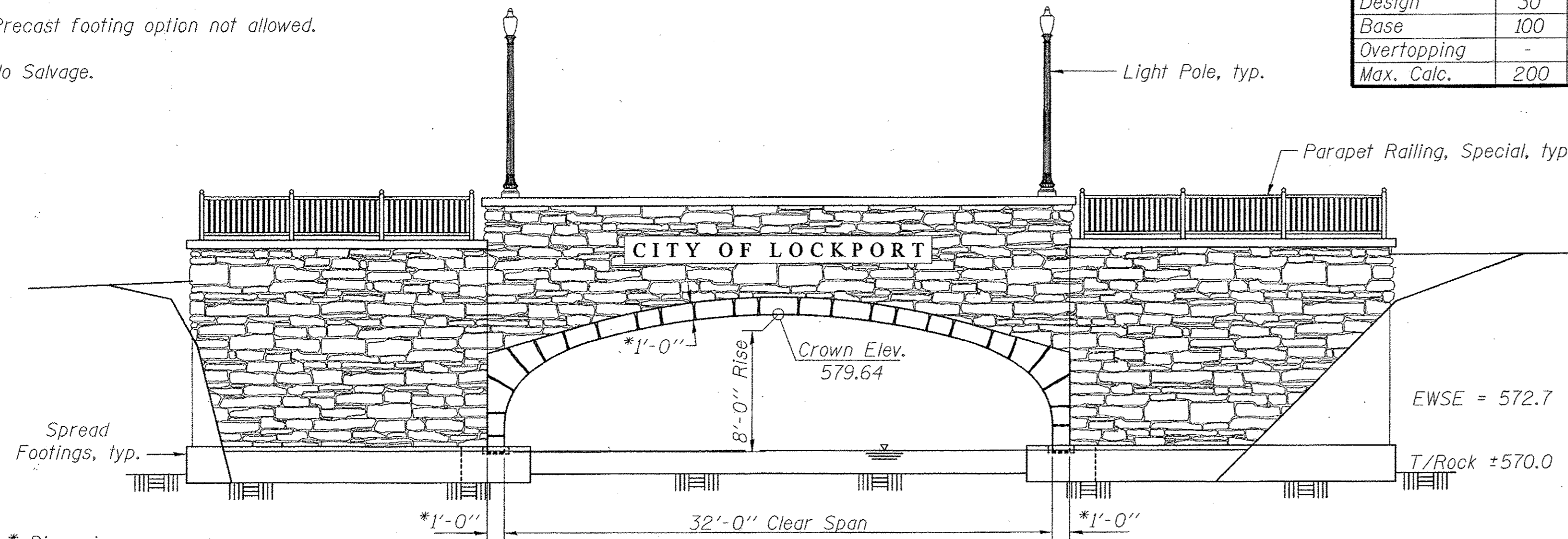
MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	26
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	

Benchmark: BM #8 Northwest bolt on fire hydrant, southwest corner intersection of 2nd Street and Canal Street. Elev. 580.74

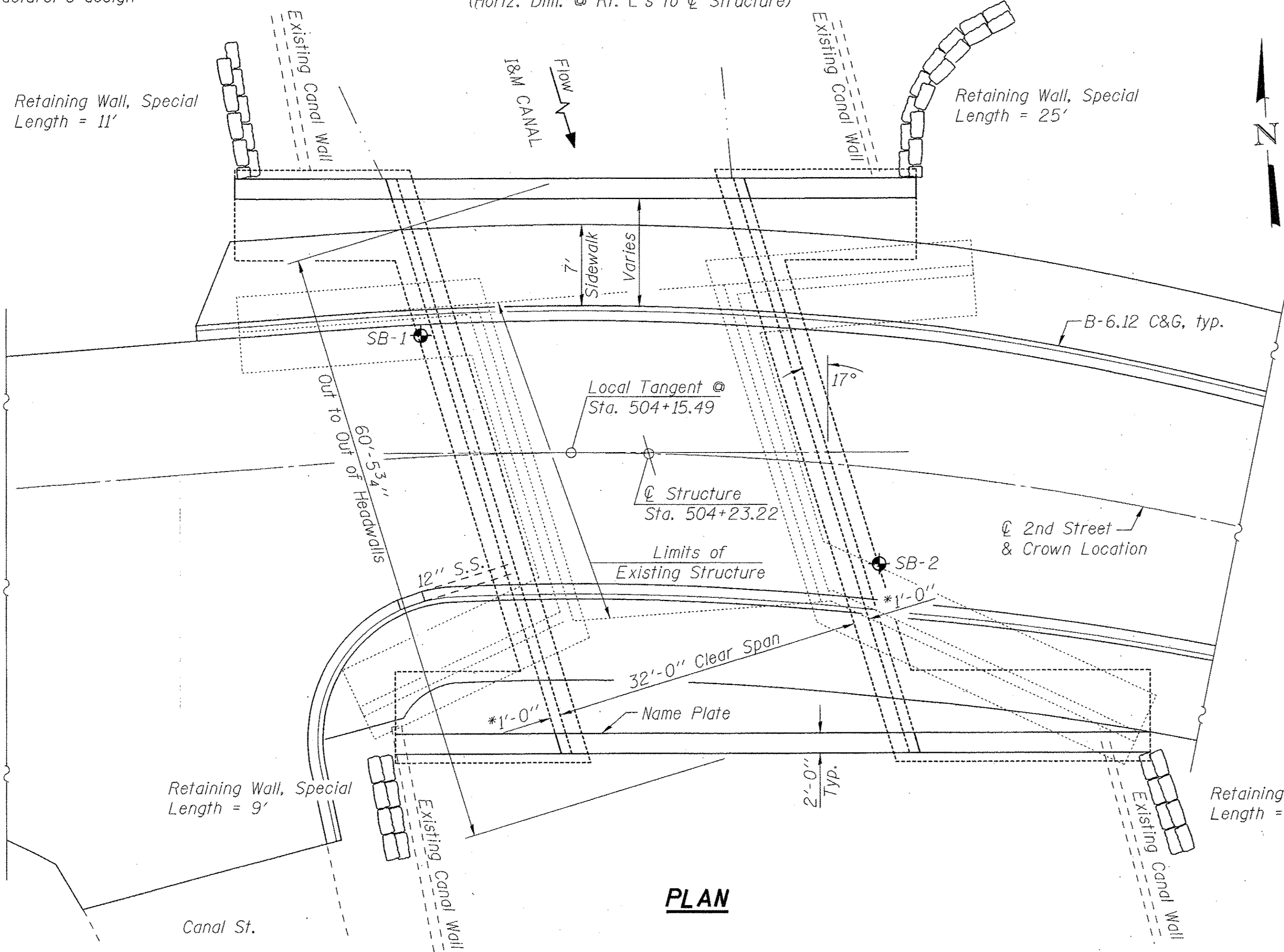
Existing Structure: SN 099-6507, built in 1939, is a single-span cast in place concrete tee-beam superstructure, supported by closed concrete abutments on spread footings keyed into rock. 28'-0" back to back of abutments and 32'-0" out to out width. Structure to be completely removed and replaced with a three-sided precast concrete arch structure on spread footings keyed into rock. Road to be closed and traffic detoured during construction.

Precast footing option not allowed.

No Salvage.



* Dimensions may vary according to three-sided manufacturer's design



PLAN

WATERWAY INFORMATION

Drainage Area = 37 Sq. Mi. Low Grade Elev. 581.00 @ Sta. 503+74										
Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Nat. H.W.E.		Head - Ft.		Headwater El.	
			Exist.	Prop.	Exist.	Prop.	Exist.	Prop.	Exist.	Prop.
Design	10	841	166	211	577.25	0.33	0.15	577.58	577.40	
Base	30	1049	190	235	578.26	0.39	0.19	578.65	578.45	
Overtopping	100	1215	212	250	579.14	0.42	0.24	579.56	579.38	
Max. Calc.	200	1552	238	254	580.25	0.91	0.78	581.16	581.03	

2 yr. flow rate = 331 ft³/s

DESIGN SCOUR ELEVATION TABLE

Event/Limit State	Design Scour Elevations (ft.)	Item 113
Footings		
Q100	569.80	5
Q200	569.80	
Design	569.80	
Check	569.80	

INDEX OF SHEETS

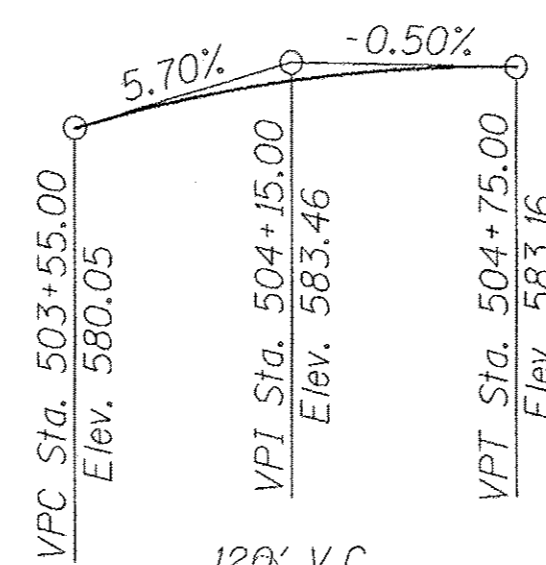
- General Plan
- Footing Plan
- Retaining Wall Details
- Three-Sided Structure Details
- Architectural Details
- Boring Logs

GENERAL NOTES

Removal of the existing bridge railings, chain link fence, and abandoned conduits attached to existing structure shall be included with the cost of Removal of Existing Structures. The finished three-sided structure shall conform to the geometry shown on the plans, including the arch shape shown and the overall geometry shown, and the arch shape shall be carried through the entire interior of the structure. If the skew shown cannot be accomplished using exclusively precast units, a cast-in-place substitution will be allowed for the outer portions of the structure. Any portions of the structure using the cast in place substitution shall match the shape of the interior precast arch units. The precast supplier shall be responsible for all design and detailing of cast in place arch sections, including their connections to precast units. No additional compensation for this substitution will be allowed. Precast footing, wingwall and headwall options are not allowed. Maximum applied service bearing pressure, Qmax. = 16,000 PSF. The foundation design is based on the following maximum unfactored reactions applied at the top of the footing: Vmax = 18.6 k/ft.; Hmax = 8.9 k/ft. The Contractor shall verify that the selected structure meets these design parameters. If the design parameters are exceeded, a complete foundation design with calculations, details, and the required seals shall be submitted for review and approval. Reinforcement bars designated (E) shall be epoxy coated. The Contractor shall obtain a construction permit from the Illinois Department of Natural Resources (IDNR), Office of Water Resources for any temporary construction activity placed in the water. This shall include the placement of material for run-arounds, causeways, cofferdams, etc. Any permit application by the Contractor shall refer to the IDNR 3704 Floodway Construction permit number allowing permanent construction as shown in the contract plans.

ILLINOIS & MICHIGAN CANAL
BUILT 20__ BY
CITY OF LOCKPORT
WILL COUNTY
SEC. 13-00080-00-BR
STA. 504+23.22
STR. NO. 099-6068 LOADING HL-93

NAME PLATE
See Std. 515001



PROFILE GRADE

LOADING HL-93
Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS
AASHTO LRFD Bridge Design Specifications
7th Edition with Interims

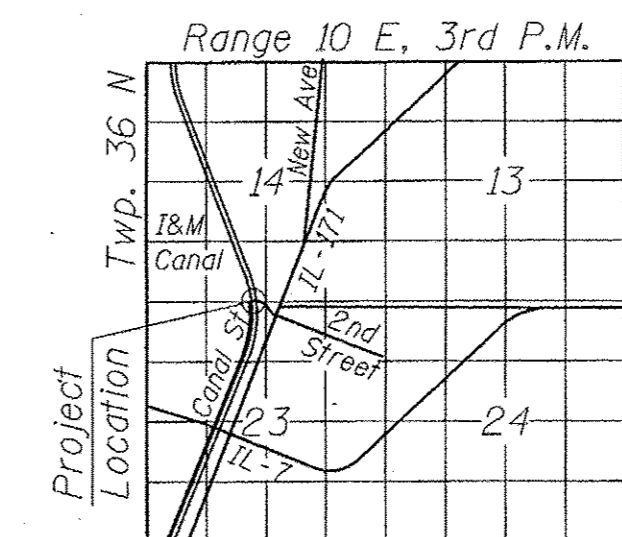
DESIGN STRESSES

FIELD UNITS
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

PRECAST UNITS
f'c = 4,500 psi
fy = 65,000 psi (Welded Wire Fabric)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.077g
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.127g
Soil Site Class = C



LOCATION SKETCH



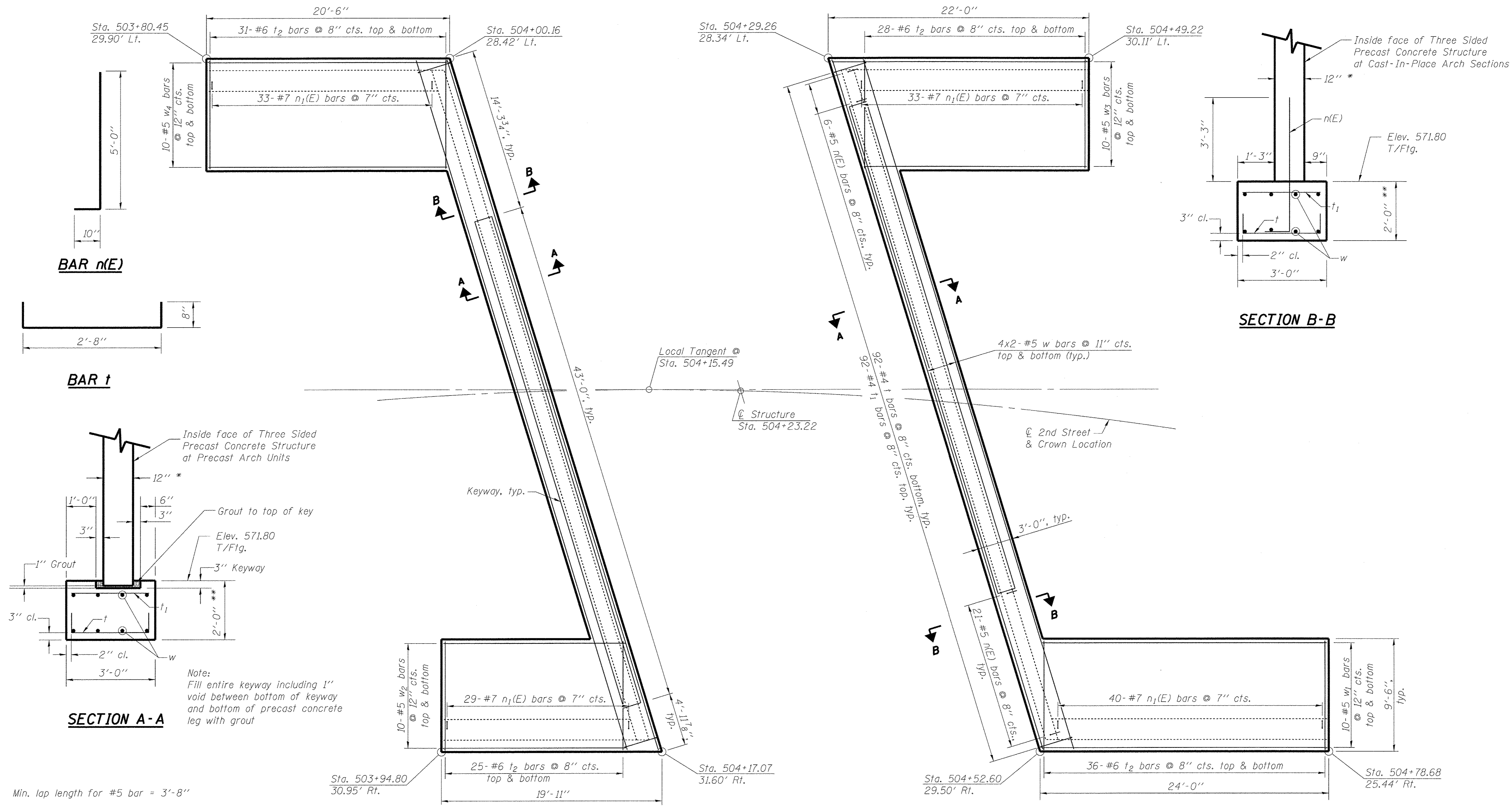
DATE: 1/24/17
LICENSE EXPIRES 11/30/18

I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current AASHTO LRFD Bridge Design Specifications.

GENERAL PLAN
SECOND STREET OVER I&M CANAL
PUBLIC WATERS
SEC. 13-00080-00-BR
WILL COUNTY
STATION 504+23.22
STRUCTURE NO. 099-6068

	USER NAME =	DESIGNED - BLB	REVISIONS -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL PLAN STRUCTURE NO. 099-6068	MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED - AS	REVISIONS -			3035	13-00080-00-BR	WILL	42	27
	PLOT DATE =	DRAWN - BLB	REVISIONS -	SHEET NO. 1 OF 6 SHEETS		CONTRACT NO. 61D71				
		CHECKED - AS	REVISIONS -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BRCS-4003(334)				

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00121 - EXPIRES 4/30/2017
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 \\corfp-boxwood.com\Projects\DeKalb\Lockport\13048-Second St Bridge Rehab\40-Design\Bridges\plans.dgn



Min. lap length for #5 bar = 3'-8"

Bars indicated thus 4 x 2 etc. indicates four lines of bars with two lengths per line.

* Precast slab and wall thickness may vary according to manufacturer's design.

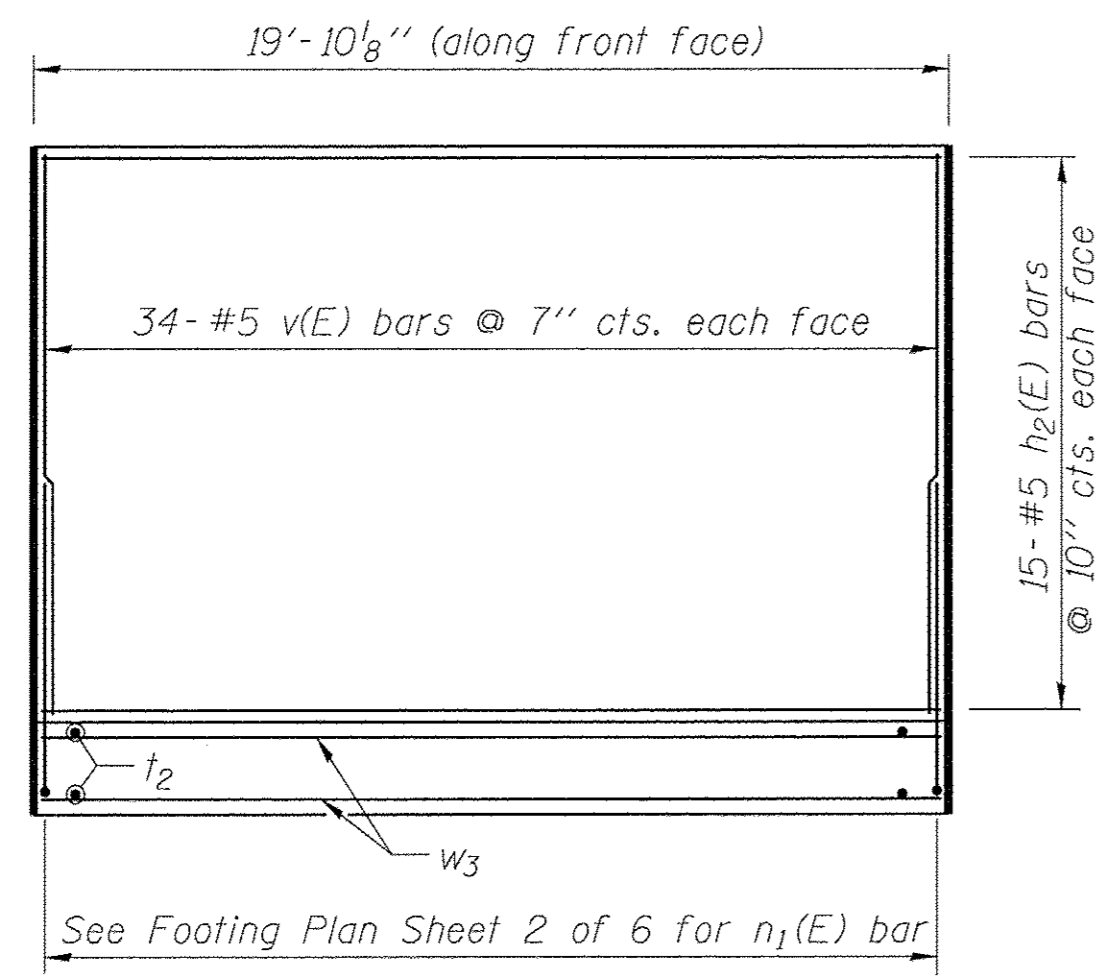
** The bottom of footing elevation shall be adjusted to ensure a minimum embedment of 3 inches in non-weathered rock. The rock excavation shall be made with near-vertical sides at the plan dimensions to allow the sides and base of the embedded portion of the footing to be cast against undisturbed rock surfaces.

FOOTING PLAN

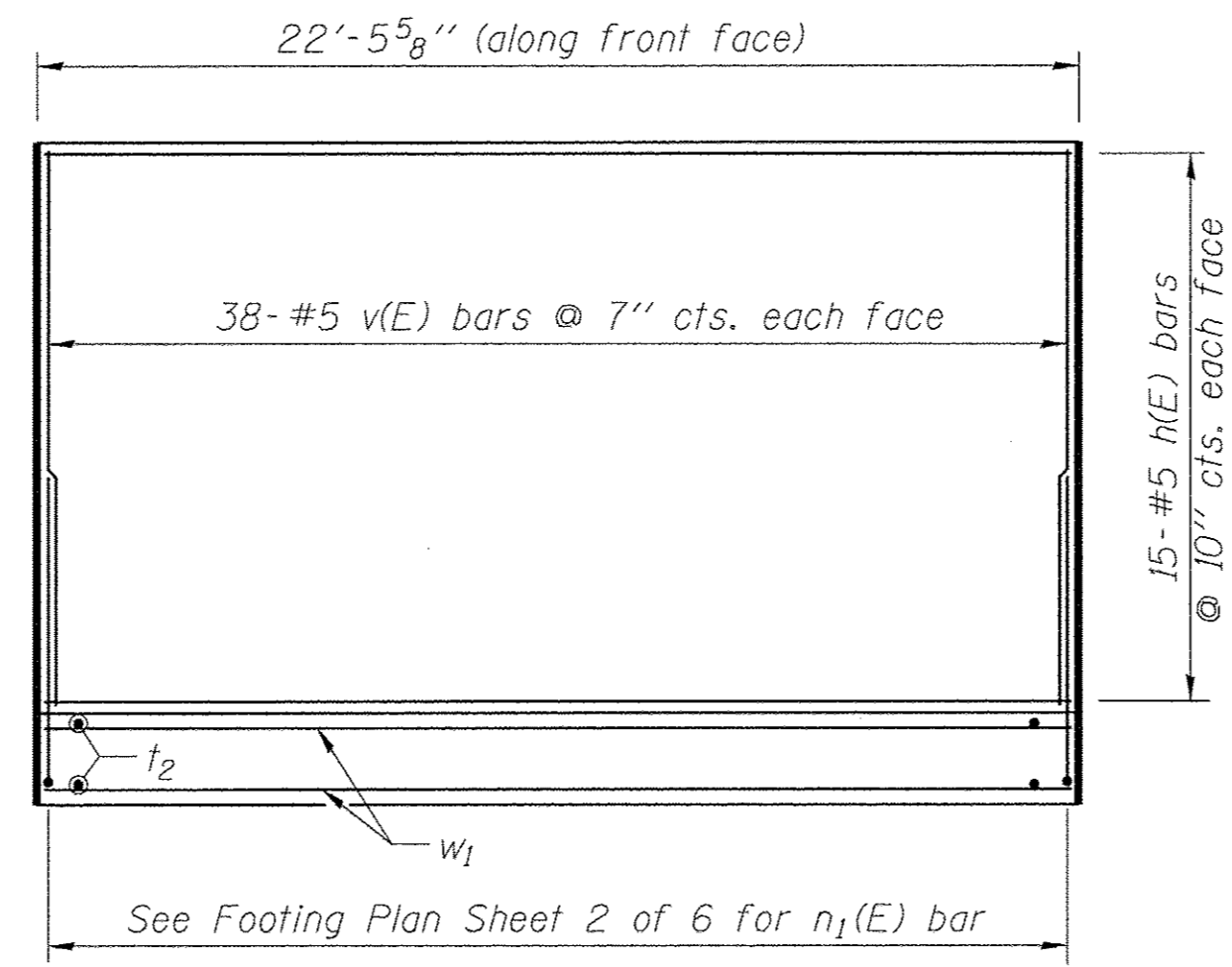
Notes:
 Footing Stations & Offsets are given relative to the centerline of 2nd Street. See Roadway Plan sheets.
 Keyway length shown is based on an assumed 42'-0" total lay length of precast arch units. The contractor shall make any necessary adjustments if a different length of precast portion is proposed.

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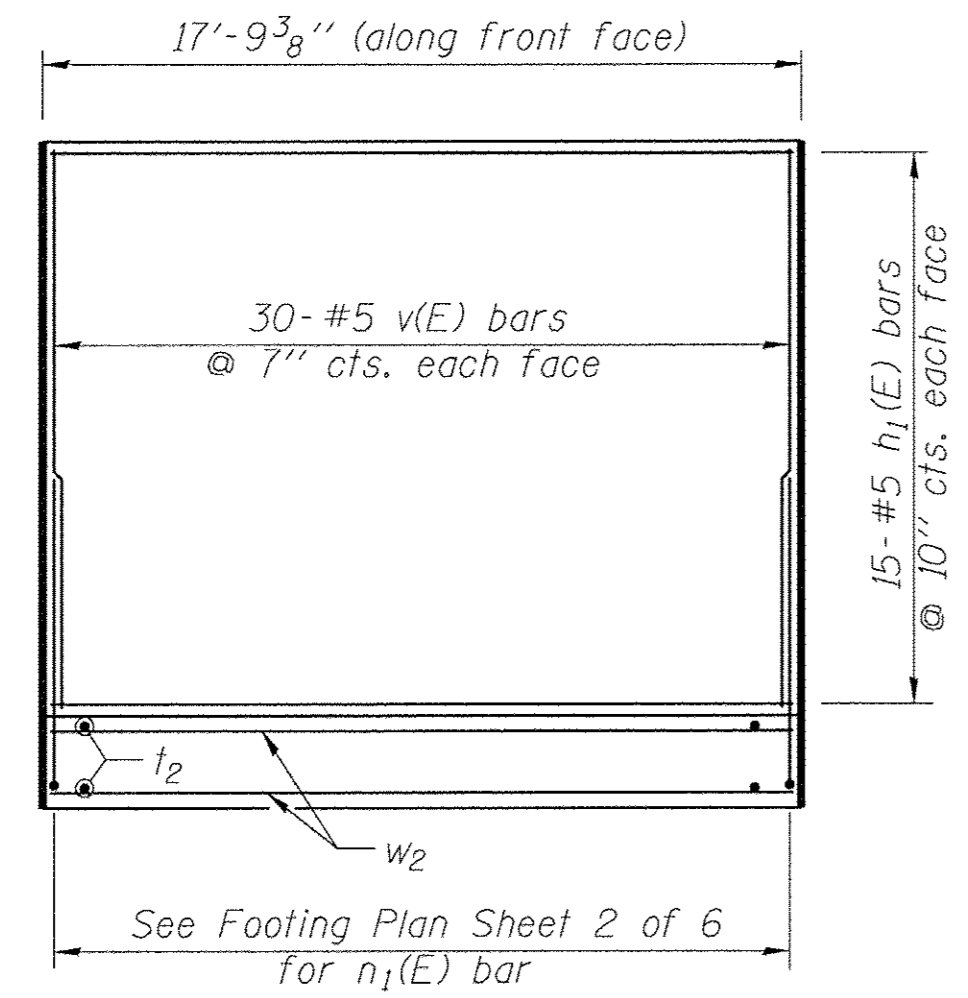
BAXTER & WOODMAN Consulting Engineers	USER NAME =	DESIGNED - AS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FOOTING DETAILS STRUCTURE NO. 099-6068	MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED - BLB	REVISED -			3035	13-00080-00-BR	WILL	42	28
	PLOT DATE =	DRAWN - AS	REVISED -			CONTRACT NO. 61D71				
		CHECKED - BLB	REVISED -			SHEET NO. 2 OF 6 SHEETS				
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BROS-40031334				



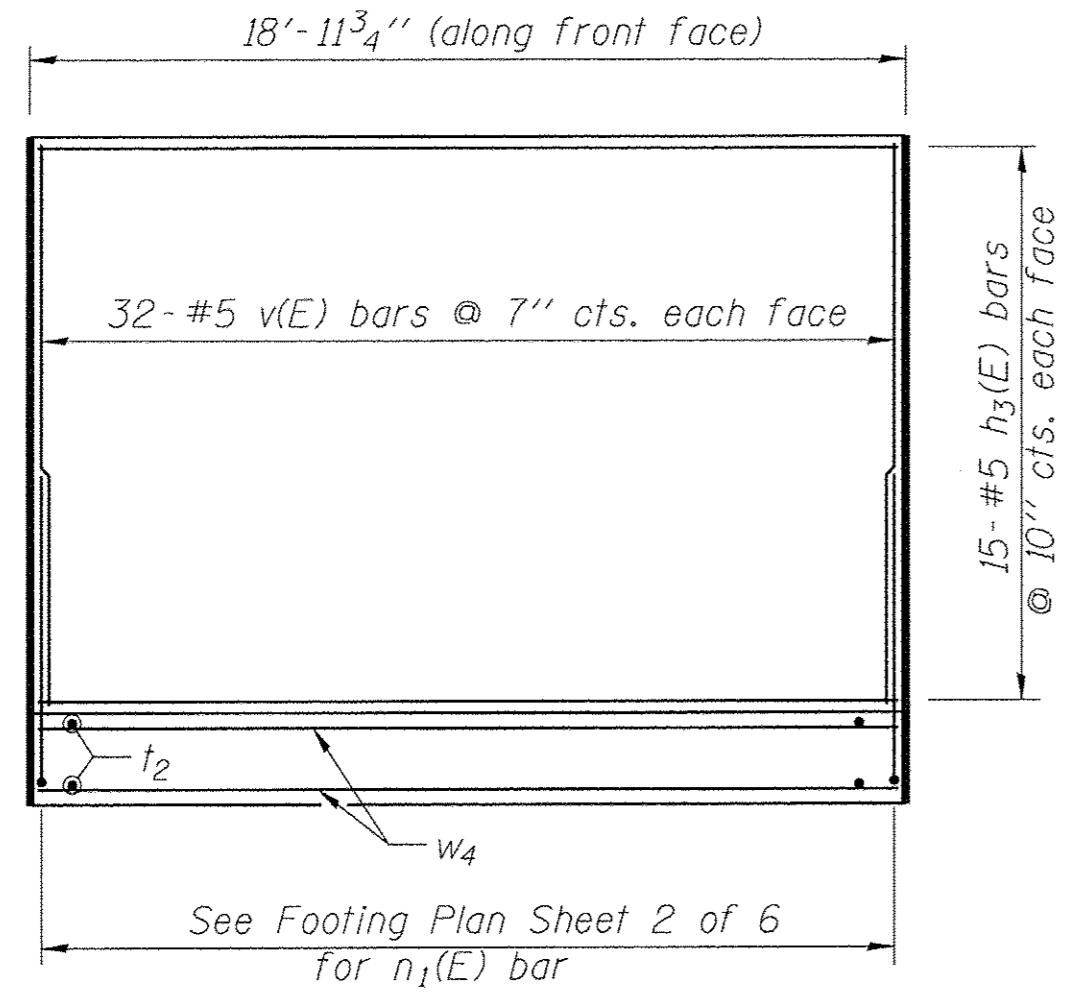
NE WINGWALL ELEVATION



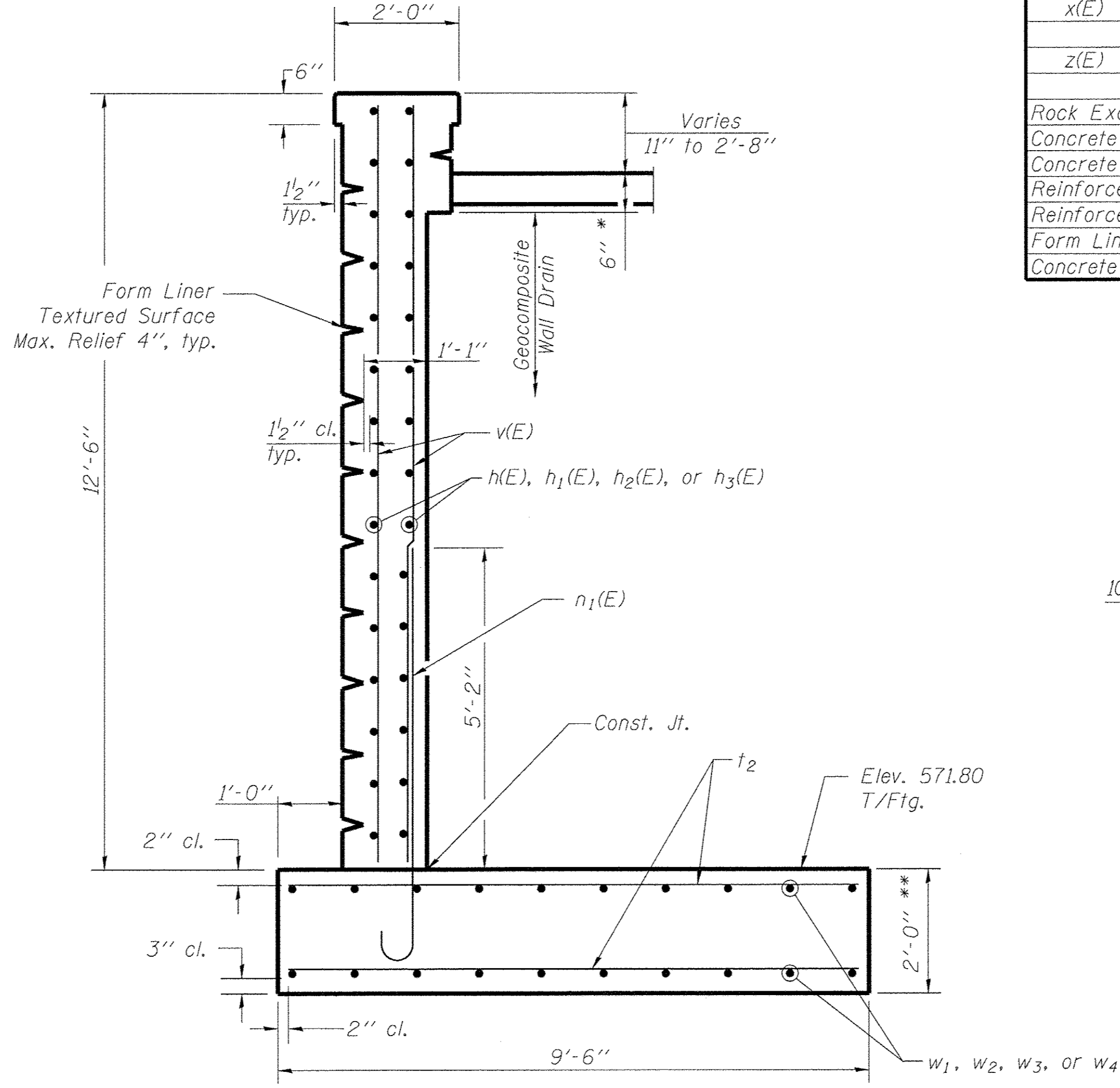
SE WINGWALL ELEVATION



SW WINGWALL ELEVATION



NW WINGWALL ELEVATION

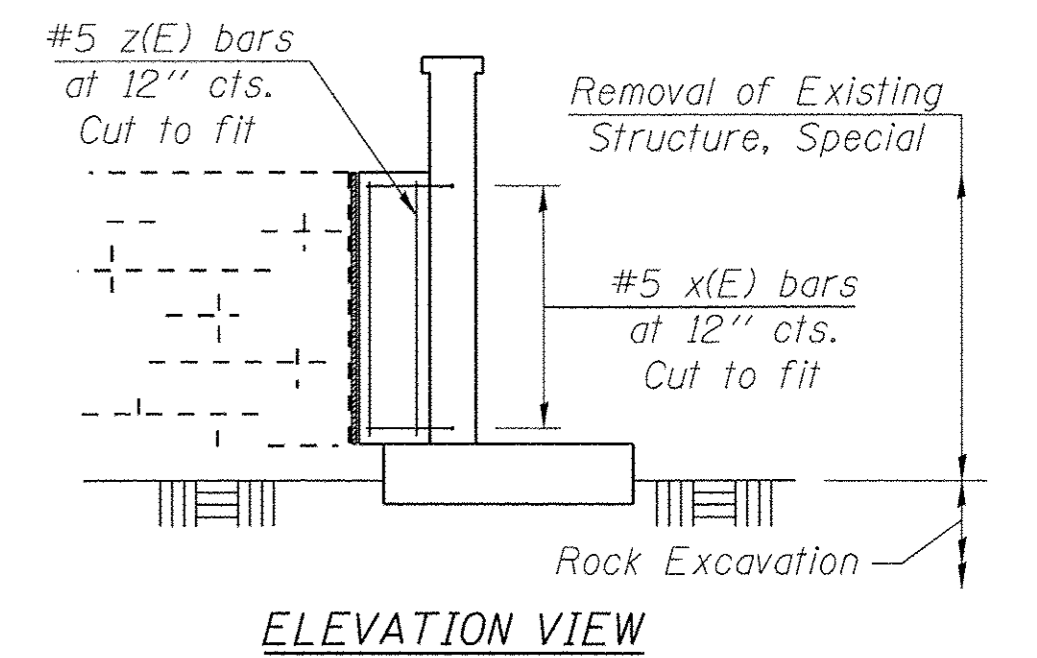
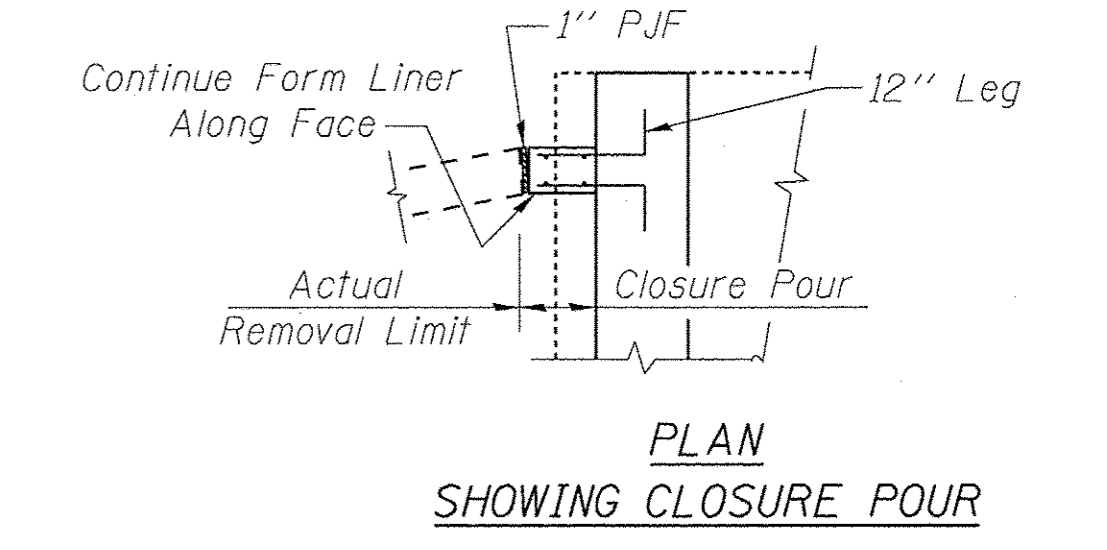
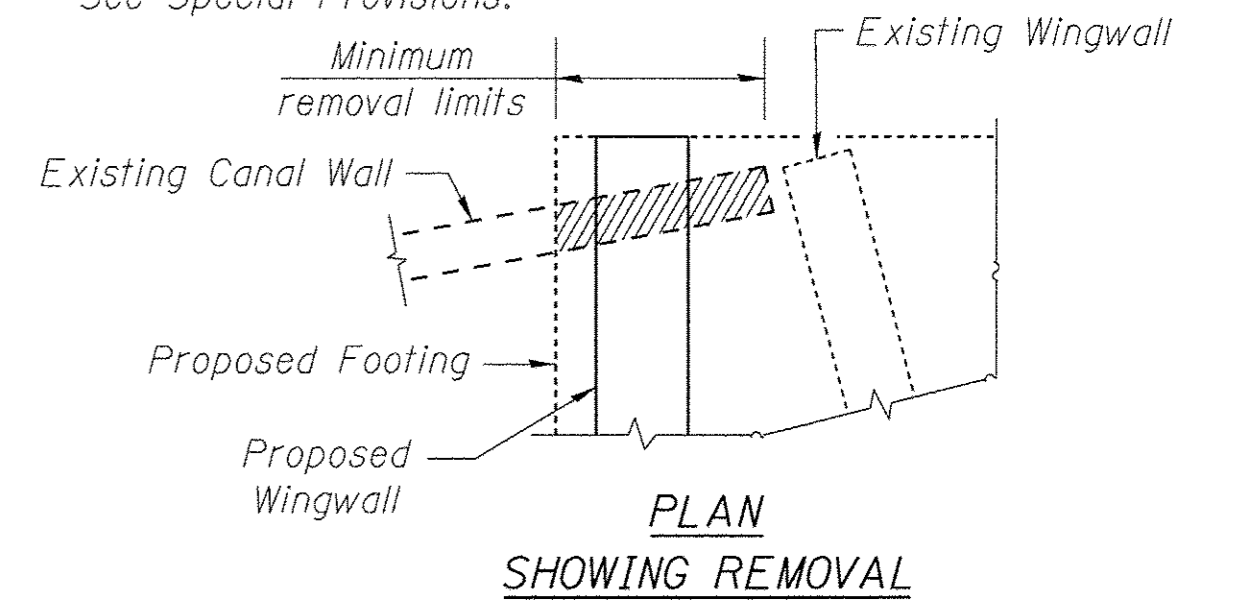


TYPICAL WINGWALL SECTION

**FOOTINGS AND WINGWALLS
BILL OF MATERIAL**

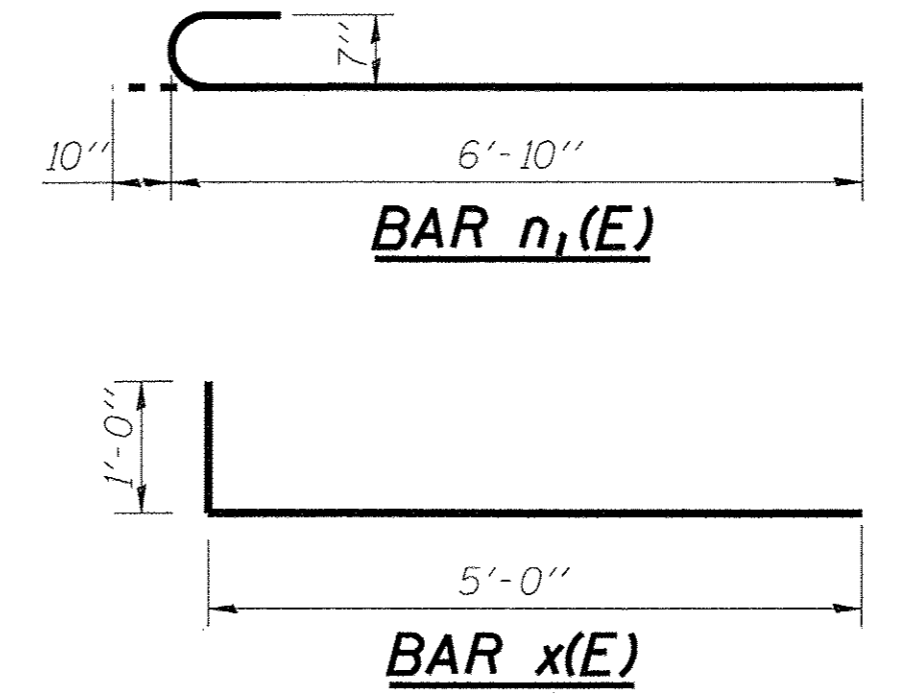
Bar	No.	Size	Length	Shape	
h(E)	30	#5	22'-2"	—	
h ₁ (E)	30	#5	17'-2"	—	
h ₂ (E)	30	#5	19'-0"	—	
h ₃ (E)	30	#5	18'-7"	—	
n(E)	54	#5	5'-10"	┘	
n ₁ (E)	135	#7	7'-8"	┘	
t	184	#4	4'-0"	┘	
t ₁	184	#4	2'-8"	—	
t ₂	240	#6	9'-2"	—	
v(E)	268	#5	12'-4"	—	
w	32	#5	32'-10"	—	
w ₁	20	#5	23'-8"	—	
w ₂	20	#5	16'-8"	—	
w ₃	20	#5	18'-9"	—	
w ₄	20	#5	20'-2"	—	
x(E)	64	#5	6'-0"	┘	
z(E)	40	#5	8'-0"	—	
Rock Excavation for Structures				Cu Yd	36
Concrete Structures				Cu Yd	78.5
Concrete Structures (Retaining Wall)				Cu Yd	51.0
Reinforcement Bars				Pound	6,880
Reinforcement Bars, Epoxy Coated				Pound	9,040
Form Liner Textured Surface				Sq Ft	2,141
Concrete Surface Color Treatment				Sq Ft	2,441

Limit of removal to be kept to a minimum. See Special Provisions.



TIE-IN TO EXISTING CANAL WALLS

Nominal quantity provided for Reinforcement Bars x(E) and z(E). Reinforcement for payment to be determined by actual removal limits. Closure pour shall be incorporated into wingwall pour if possible.



* Form Liner placed beyond dimension shown will not be measured for payment.

** The bottom of footing elevation shall be adjusted to ensure a minimum embedment of 3 inches in non-weathered rock. The rock excavation shall be made with near-vertical sides at the plan dimensions to allow the sides and base of the embedded portion of the footing to be cast against undisturbed rock surfaces.

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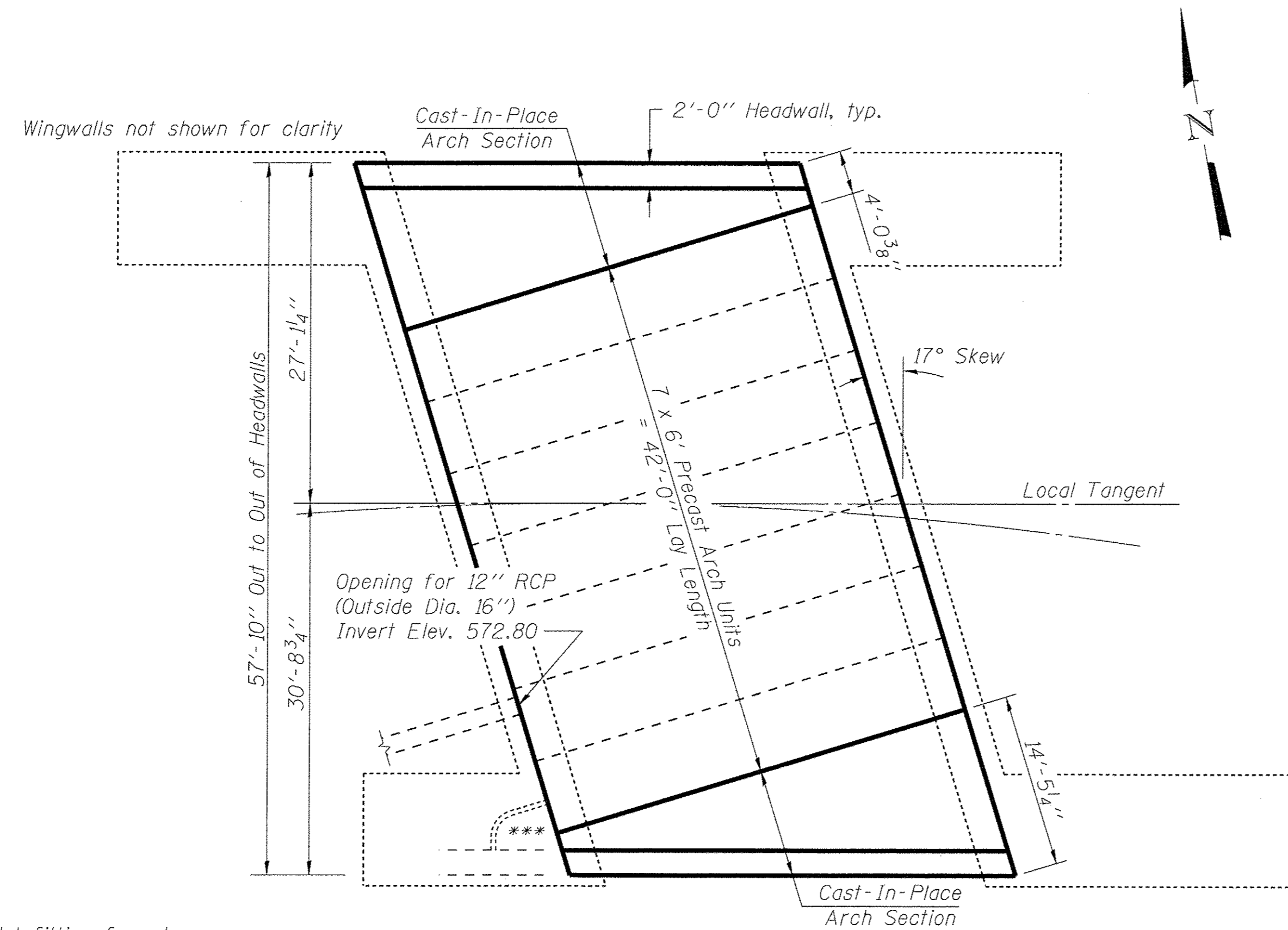
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CHECKED - BLB	CHECKED - BLB	REVISD -
PLOT SCALE =	DRAWN - AS	REVISD -
PLOT DATE =	CHECKED - BLB	REVISD -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**RETAINING WALL DETAILS
STRUCTURE NO. 099-6068**

SHEET NO. 3 OF 6 SHEETS

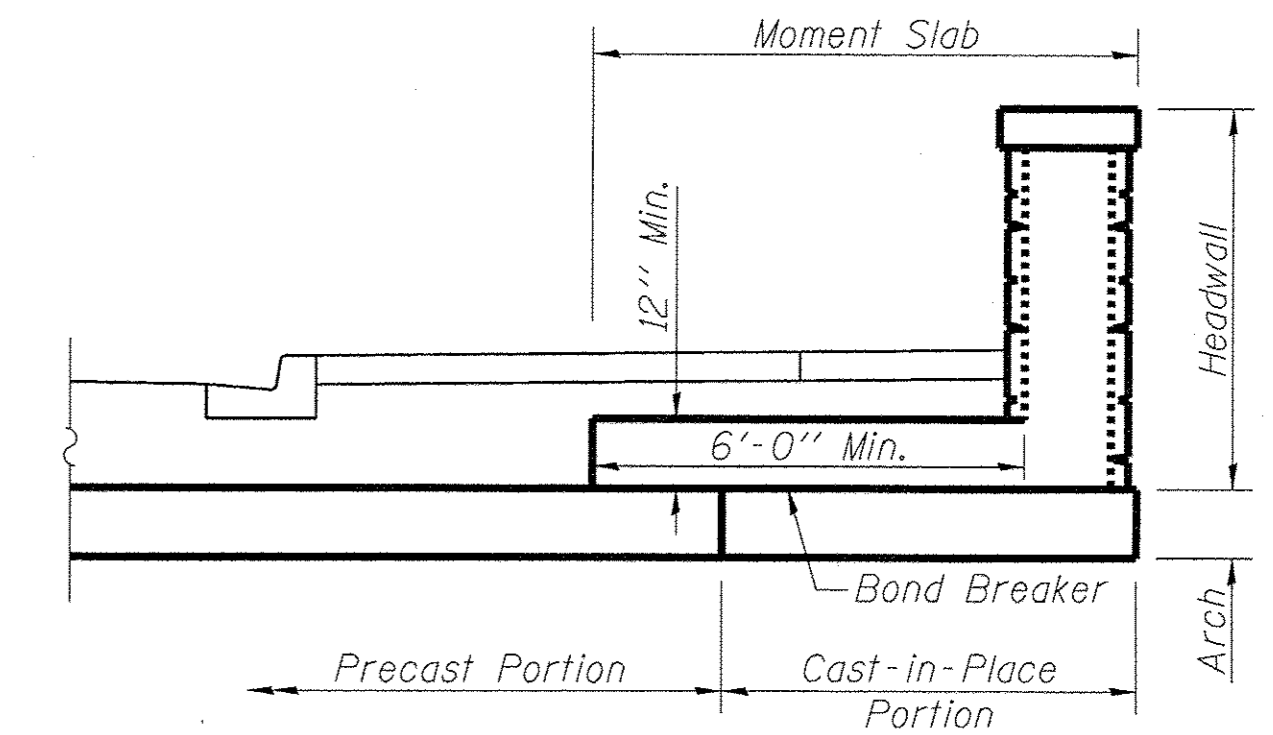
MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	29
CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BROS-4003(334)				



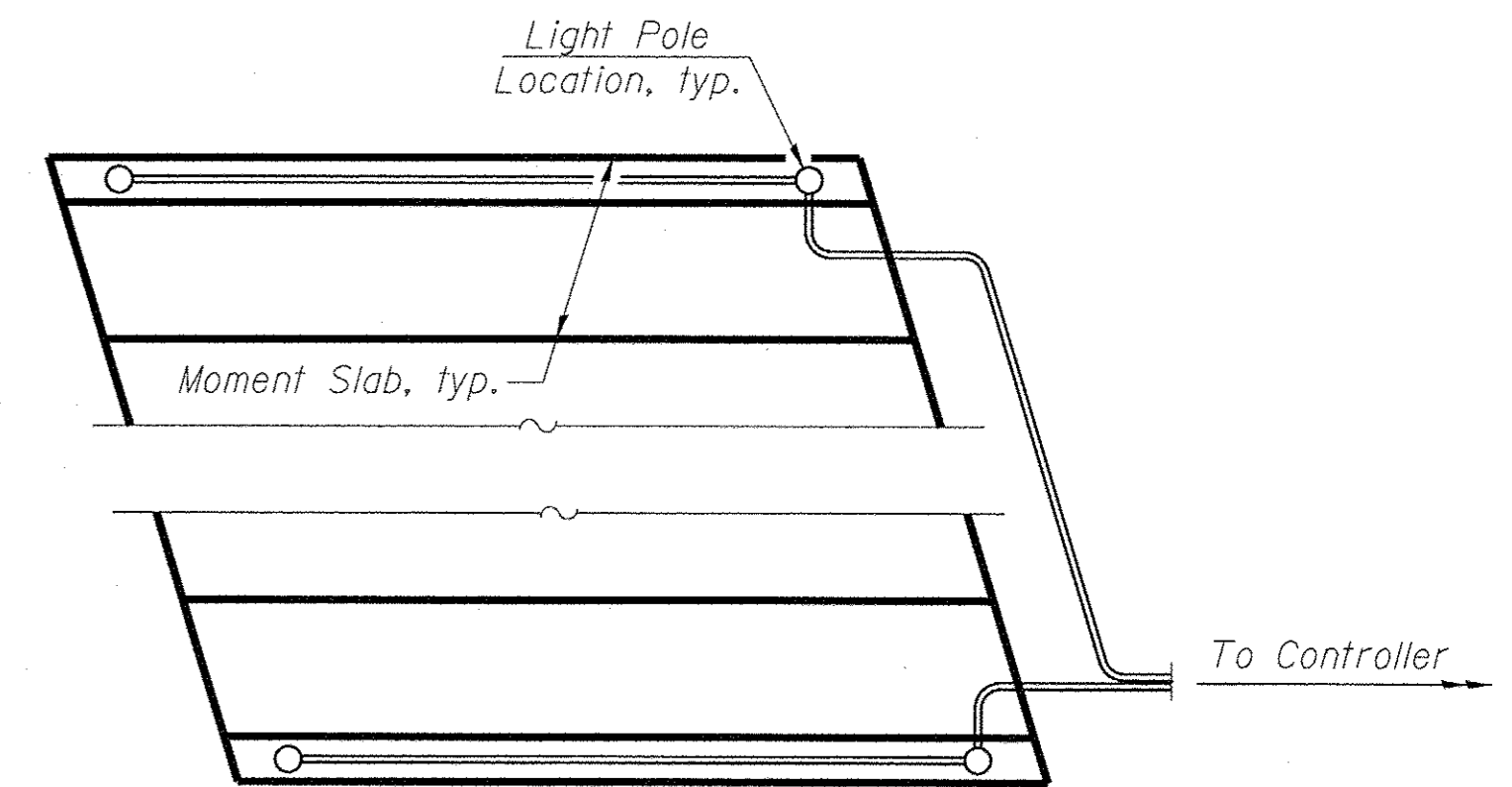
*** Route outlet fitting from base of Geocomposite Wall Drain to nearest weep hole in arch leg, typ. 4 wingwalls. Cost included with Geocomposite Wall Drain

THREE-SIDED STRUCTURE LAYOUT

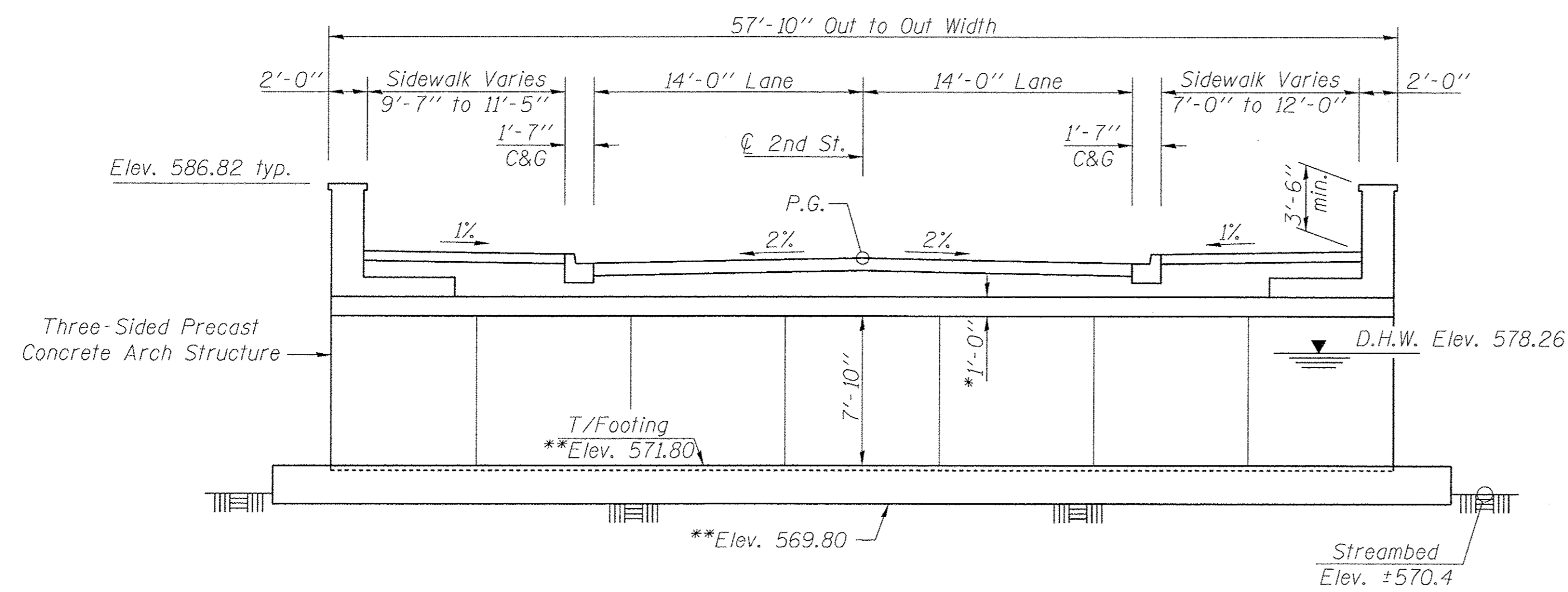
Suggested layout shown. Dimensions of precast and cast-in-place portions may be adjusted based on requirements of Three-Sided structure supplier. Overall out-to-out dimensions may not be altered.



SECTION THRU HEADWALL



SUGGESTED CONDUIT ROUTING



LONGITUDINAL SECTION

(Horizontal dimensions vary along curve)

NOTES

Cast-in-place headwalls and moment slab shall be designed by the precast supplier to withstand a crash loading equivalent to a Test-Level 2 impact loading as defined by the AASHTO LRFD Bridge Design Specifications. If an alternate headwall connection to the arch is proposed, the shop drawings shall clearly show the method proposed and shall provide supporting calculations. Conduits shall be routed through the moment slab to light pole locations on the headwalls. Precast arch supplier shall account for these conduits in calculations if necessary. A suggested routing for conduits is shown. See Lighting Details for additional information. Inside faces of precast units shall be free of any permanent marking, such as labeling, stenciling, unit numbering, etc. Any marking necessary for installation shall be placed on the outside of the precast unit. Opening for 12" storm sewer outlet in southwest portion of structure to be detailed by three-sided structure manufacturer and shown on shop drawings.

- * Precast slab and wall thickness may vary according to manufacturer's design.
- ** The bottom of footing elevation shall be adjusted to ensure a minimum embedment of 3 inches in non-weathered rock. The rock excavation shall be made with near-vertical sides at the plan dimensions to allow the sides and base of the embedded portion of the footing to be cast against undisturbed rock surfaces.

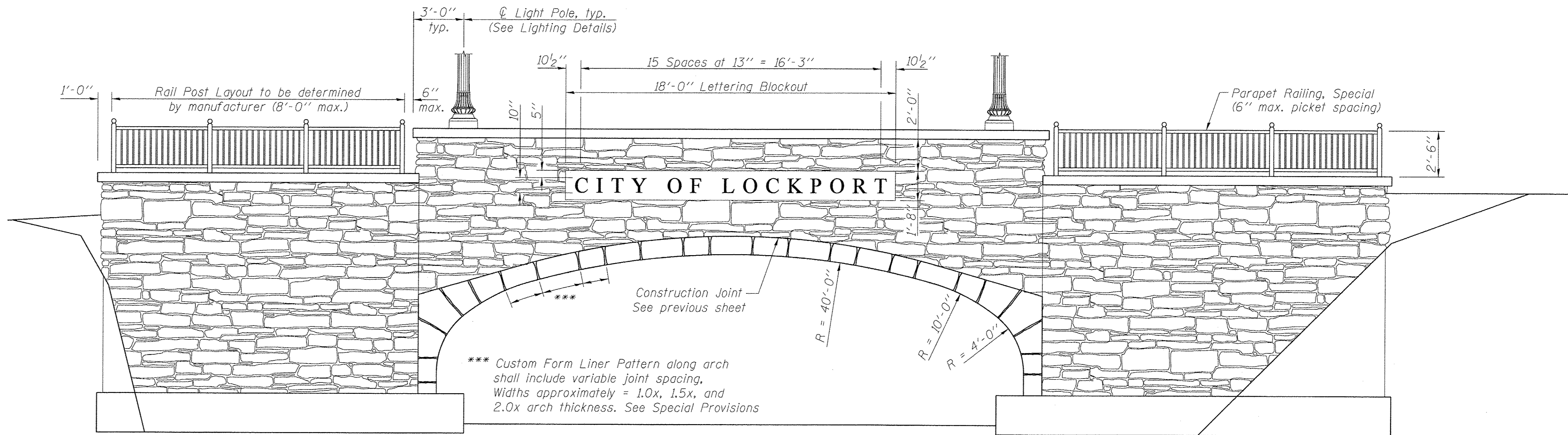
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BAXTER & WOODMAN Consulting Engineers	USER NAME =	DESIGNED - BLB	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	THREE-SIDED STRUCTURE DETAILS STRUCTURE NO. 099-6068	MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE =	CHECKED - AS	REVISED -			3035	13-00080-00-BR	WILL	42	30
	PLOT DATE =	DRAWN - BLB	REVISED -			CONTRACT NO. 61D71				
		CHECKED - AS	REVISED -			SHEET NO. 4 OF 6 SHEETS				
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BROS-4003334				

NOTES

Form liner pattern, texture and color treatment shall be continuous around the perimeter of headwalls and wingwalls. Form liner pattern shall continue across vertical construction joints uninterrupted and with no disruption in pattern.

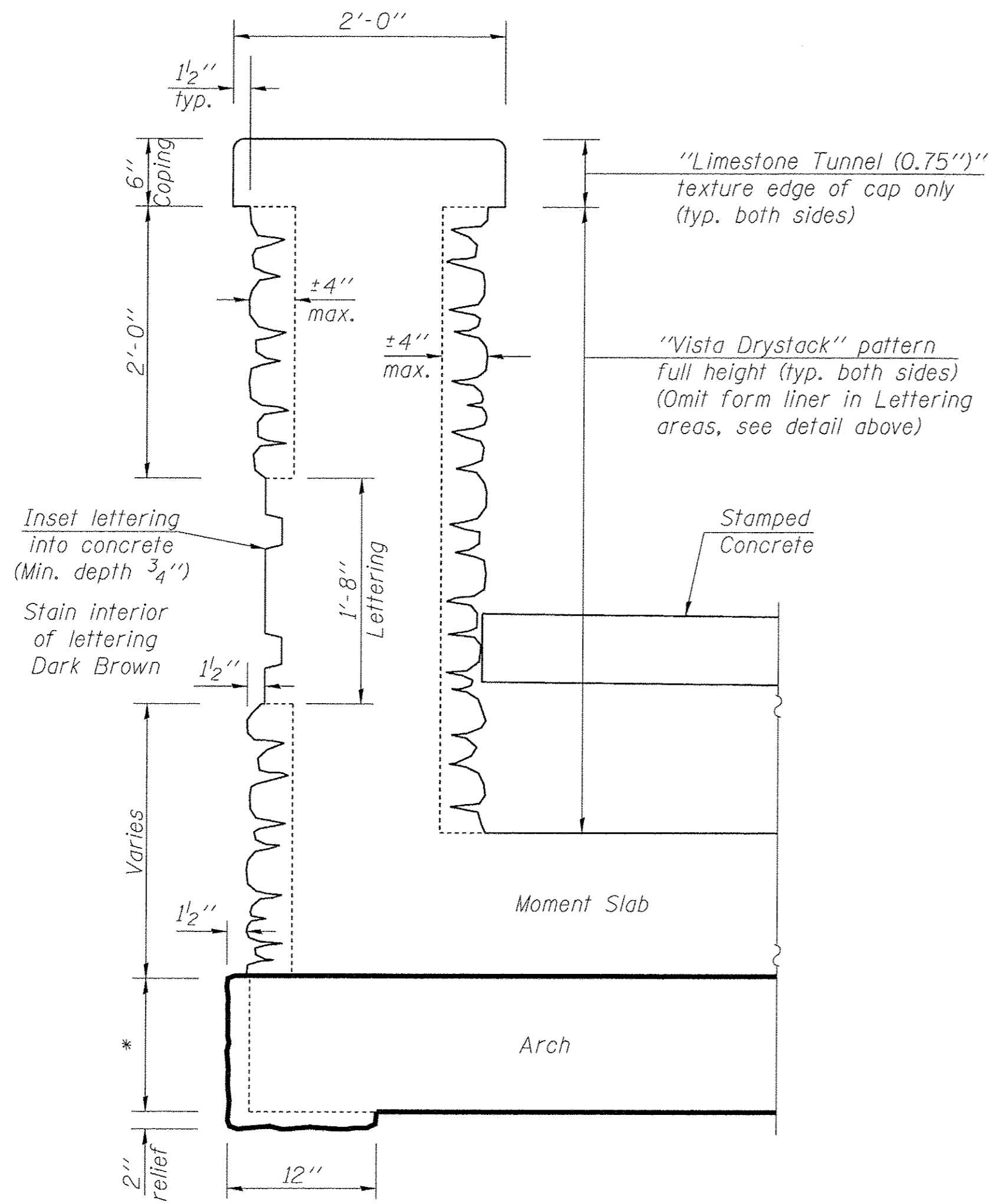
A full-height construction joint shall be placed between wingwalls and headwall/arch structure. A 1/2" PJF or other suitable joint material shall be placed along the full height of the joint, and recessed or cut back to minimize visibility. Cost included with Concrete Structures, Retaining Wall.



*** Custom Form Liner Pattern along arch shall include variable joint spacing. Widths approximately = 1.0x, 1.5x, and 2.0x arch thickness. See Special Provisions

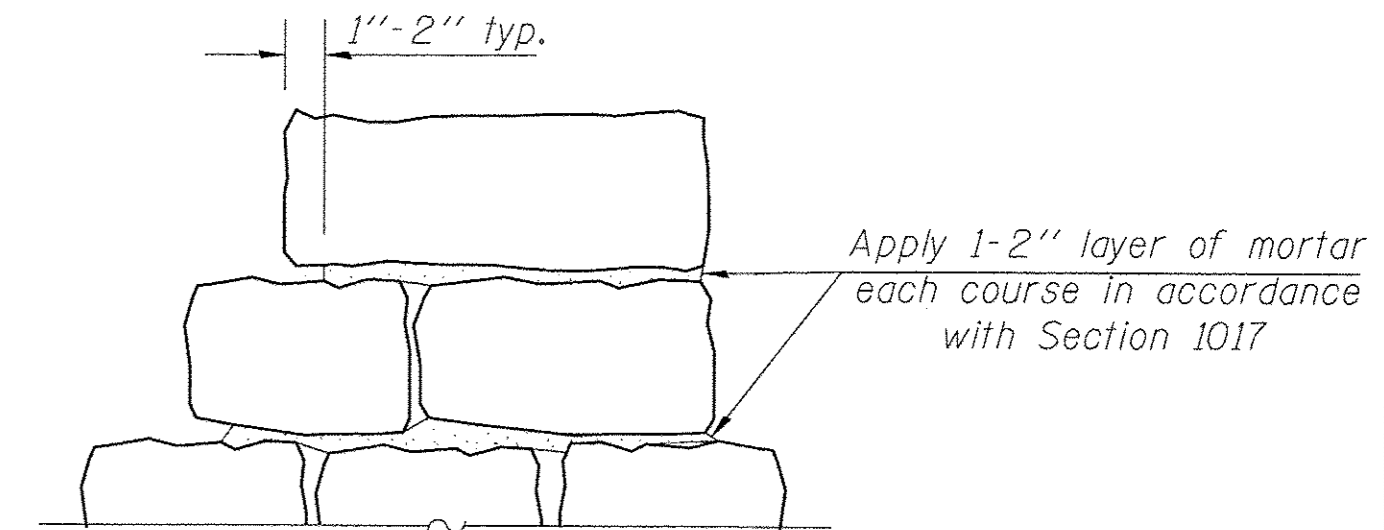
**FORM LINER AND HEADWALL LETTERING DETAILS
OUTSIDE ELEVATION VIEW**

Arch geometry may vary according to manufacturer's design

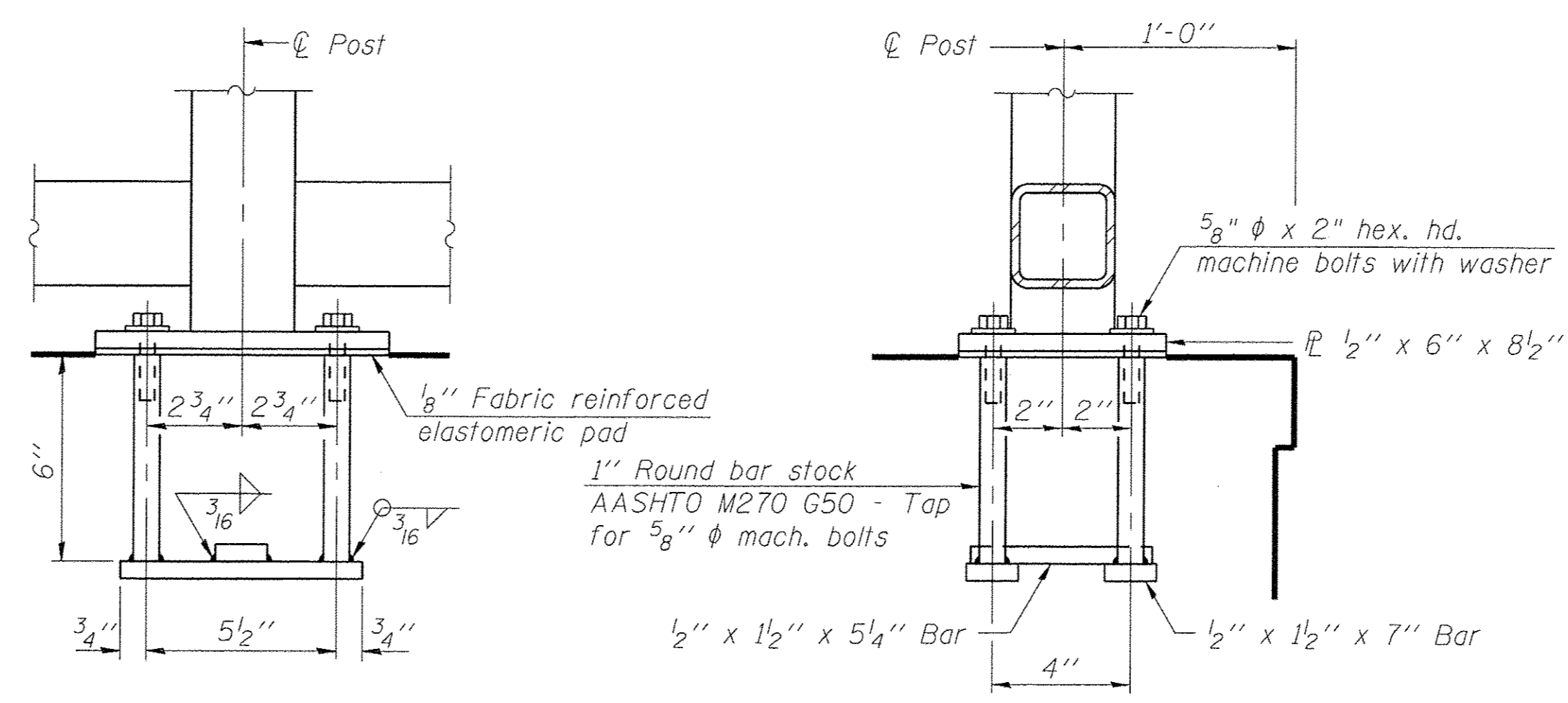


FORM LINER DETAILS

* Arch thickness to be determined by three-sided supplier. Contractor shall adjust interior arch formwork for cast-in-place portion to match precast portion, accounting for additional form liner relief as shown.

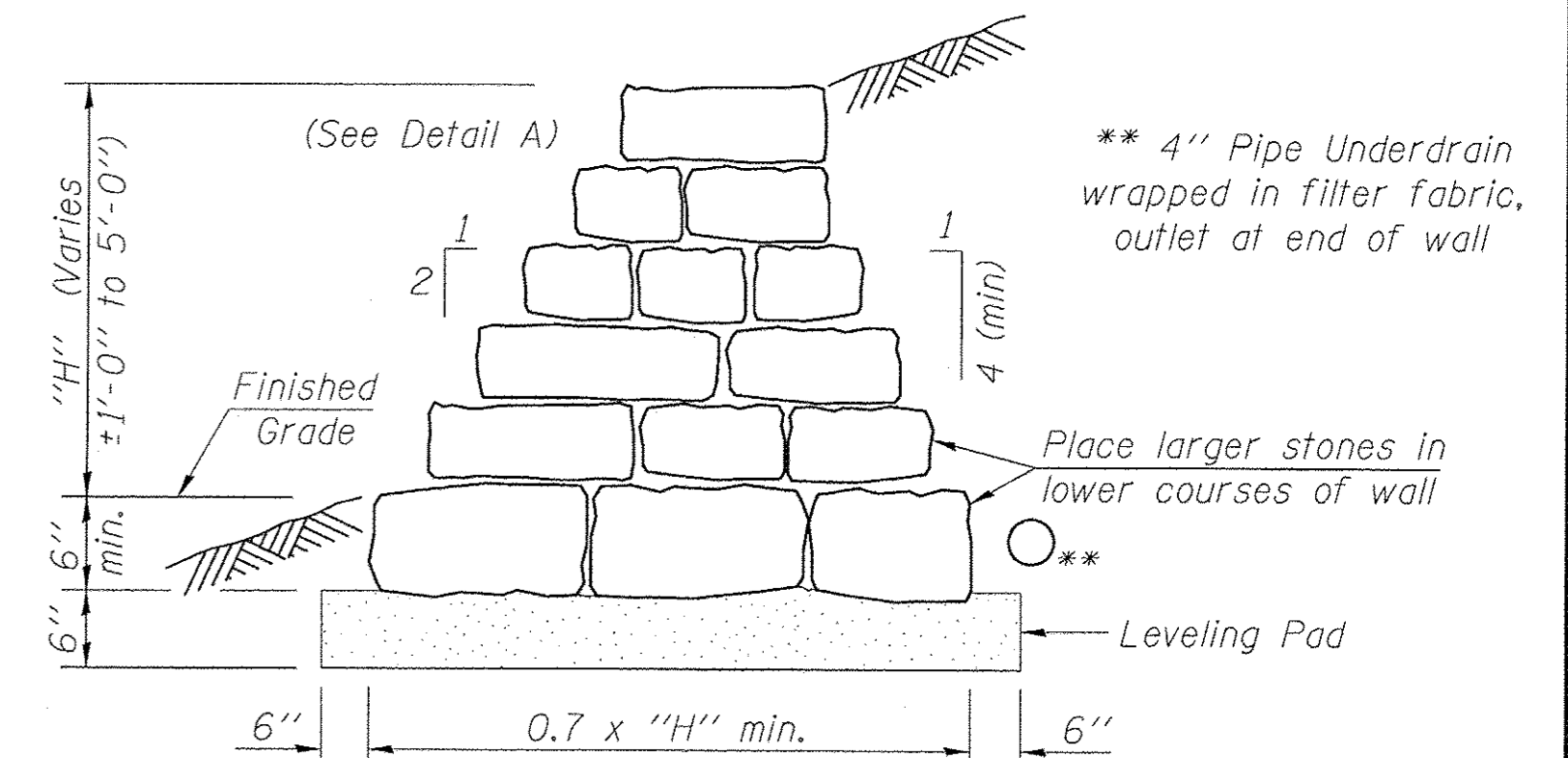


DETAIL A



ANCHOR BOLT DETAILS

In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting 5/8" anchor rods according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



**RETAINING WALL, SPECIAL
TYPICAL CROSS SECTION**

Notes:
See Grading Plan for wall layout and finished grade elevations.

Cost of excavation, leveling pad, pipe underdrain, mortar, and backfill included with the cost of Retaining Wall, Special.

See Special Provision for additional information.

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 PROJECT NO. - 13-00080-00-BR
 SHEET NO. - 5 OF 6 SHEETS



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PLOT DATE =	DRAWN - BLB	REVISED -
	CHECKED - AS	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARCHITECTURAL DETAILS
STRUCTURE NO. 099-6068**

SHEET NO. 5 OF 6 SHEETS

MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	31
CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BR05-4003(334)				

SOIL BORING LOG

Solutions You Can Build On
 ROUTE 2nd St. & I & M Canal DESCRIPTION 4' west of west abutment 5.4' north of 2nd st. centerline LOGGED BY TLM
 SECTION 099-6507 LOCATION SW 1/4, SEC. 14, TWP. 36N, RNG. 10E, 3rd PM.
 COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO.	DEPTH	D	B	U	M	Surface Water Elev.	Stream Bed Elev.
099-6507		(ft)	(/ft)	(tsf)	(%)		
Station	102+00					571.72 ft	570.50 ft
Offset	5.4 ft Lt.						
Ground Surface Elev.	582.82 ft						
9.4" of HMA over brown aggregate							
	581.82	12					
Loose brown Sand & Gravel fill, sand fractions fine to medium & gravel is small to medium							
		7		4			
		6					
		2					
		2		5			
		3					
		5					
		4		6			
		4					
Brown clayey Sand & Gravel							
	574.82	4					
		4		7			
		10					
		4					
	570.82	9		7			
		12					
White weathered Dolomite							
	569.12	50/2"		12			
Borehole continued with rock coring.							
		15					
		20					

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, form 137 (Rev. 8-99)

SOIL BORING LOG

Solutions You Can Build On
 ROUTE 2nd St. & I & M Canal DESCRIPTION 9' east of east abutment 7.5' south of 2nd st centerline LOGGED BY TLM
 SECTION 099-6507 LOCATION SW 1/4, SEC. 14, TWP. 36N, RNG. 10E, 3rd PM.
 COUNTY Will DRILLING METHOD Hollow Stem Auger HAMMER TYPE CME Automatic

STRUCT. NO.	DEPTH	D	B	U	M	Surface Water Elev.	Stream Bed Elev.
099-6507		(ft)	(/ft)	(tsf)	(%)		
Station	102+41.4					571.72 ft	570.50 ft
Offset	7.5 ft Rt.						
Ground Surface Elev.	582.86 ft						
6.6" of HMA over +12" of aggregate							
	581.36	7					
Medium dense brown gravelly Sand							
		7		5			
	579.86						
Loose brown gravelly Sand							
		6					
		5		5			
		4					
	577.35						
Loose brown gravelly Sand, some clay							
		5					
		4		7			
		4					
Begin Rock Core at 12 ft.							
		5					
		5		8			
		10					
		3					
	570.86	50/3		14			
Borehole continued with rock coring.							
		15					
		20					

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, form 137 (Rev. 8-99)

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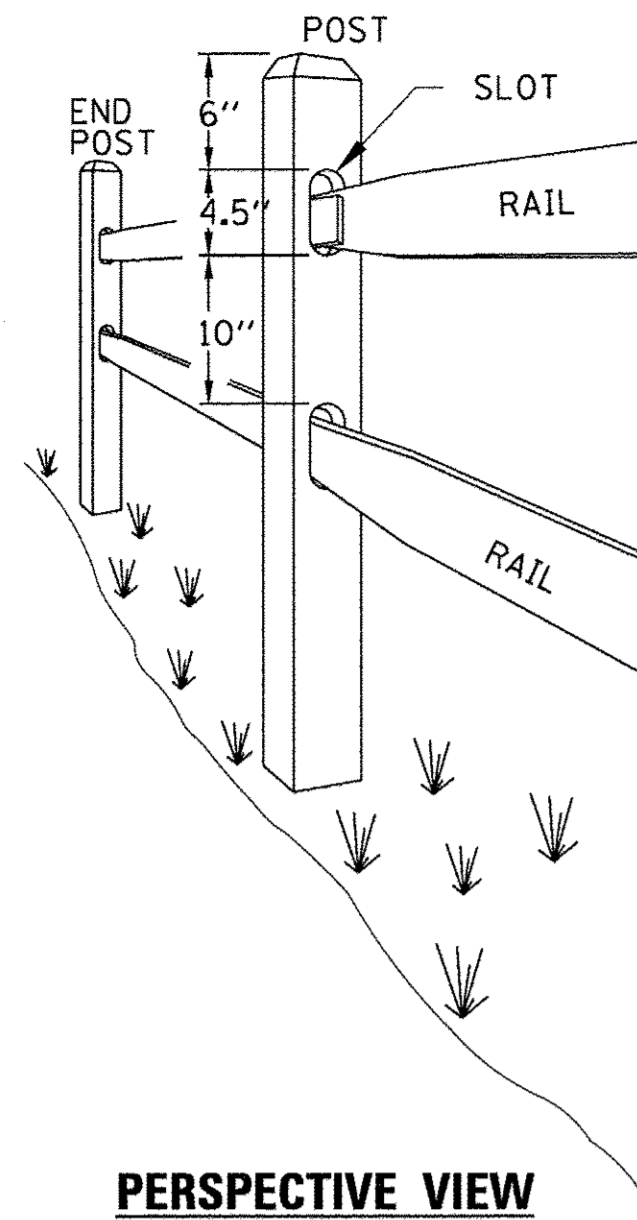
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	CHECKED - AS	REVISED -
PLOT SCALE =	DRAWN - BLB	REVISED -
PLOT DATE =	CHECKED - AS	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BORING LOGS
 STRUCTURE NO. 099-6068

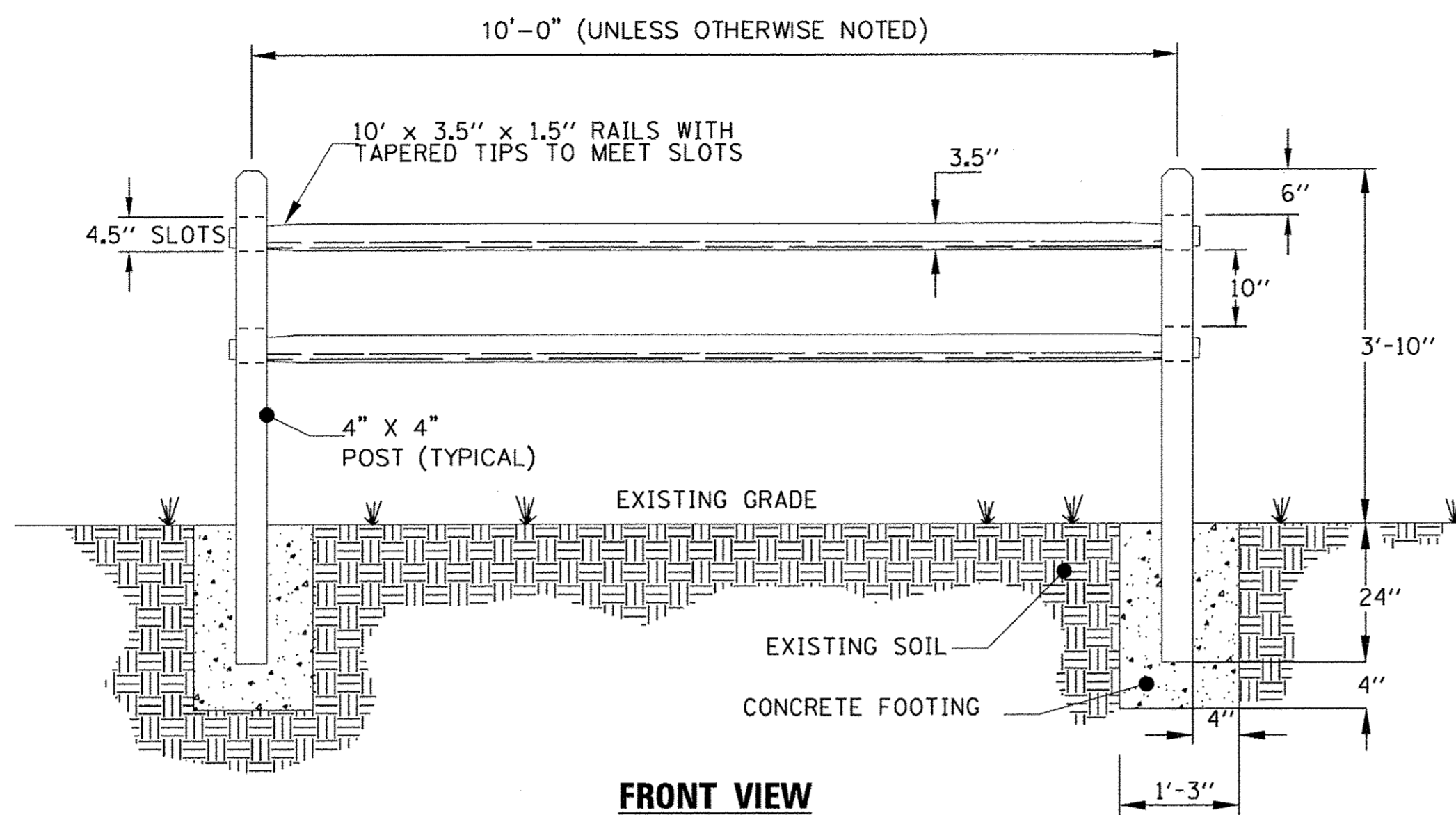
SHEET NO. 6 OF 6 SHEETS

MUN ST	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	32
CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT BROS-4003(334)				



PERSPECTIVE VIEW

NOTES:
 1) ALL WOOD FOR SPLIT RAIL FENCE TO BE PRESSURE TREATED.
 2) END POST SHALL BE TERMINAL POST.



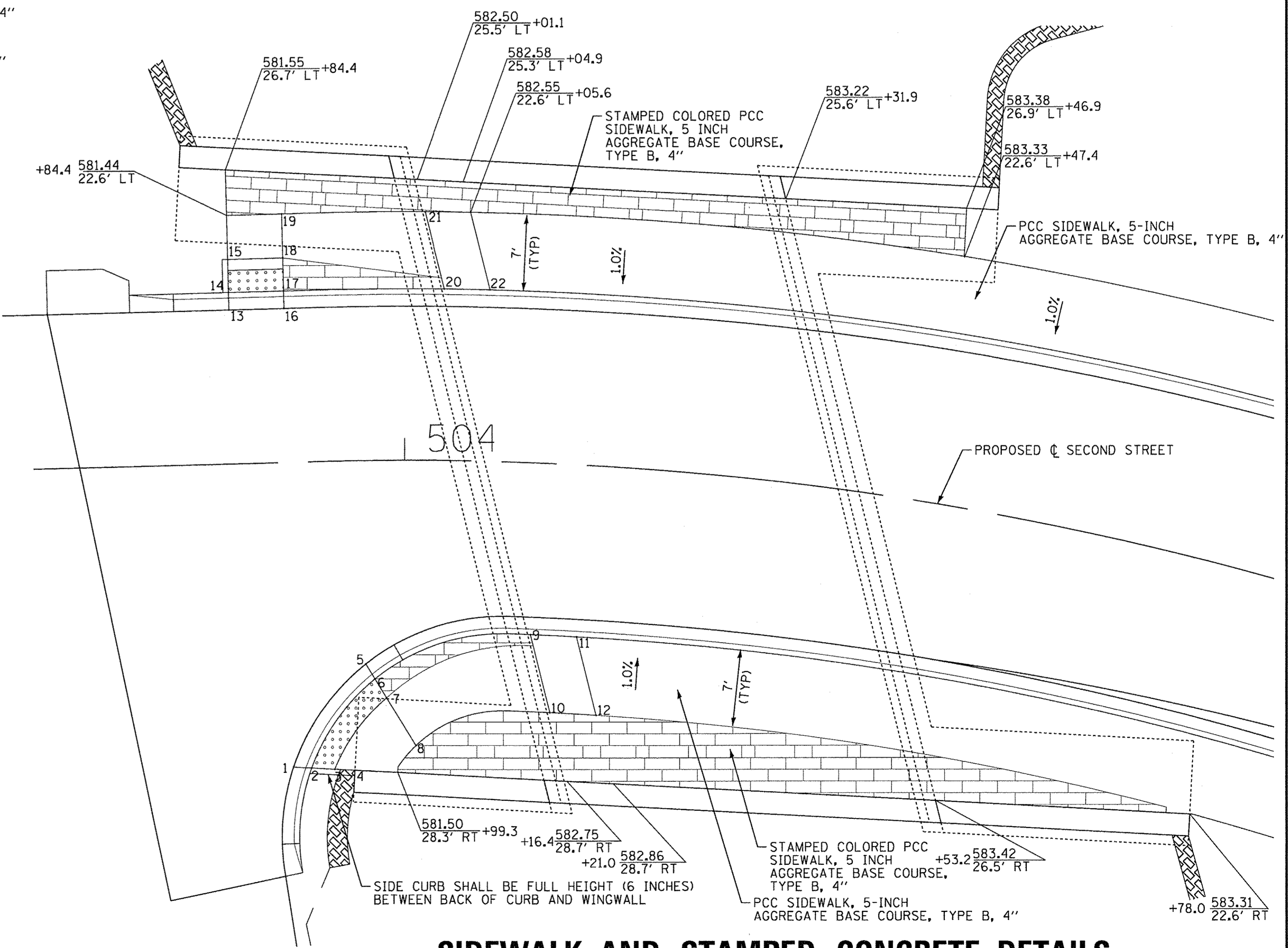
FRONT VIEW

RUSTIC WOOD POST AND RAIL FENCE

(STANDARD SPLIT-RAIL FENCE)

SIDEWALK GRADES AND ELEVATIONS

PT	STA	ELEV	OFFSET
1	503+89.2	581.28	27.7' RT
2	503+90.9	581.34	27.8' RT
3	503+93.0	581.38	28.0' RT
4	503+95.0	581.42	28.1' RT
5	503+96.3	581.50	18.5' RT
6	503+97.1	581.56	19.8' RT
7	503+98.3	581.56	21.6' RT
8	504+01.1	581.56	25.6' RT
9	504+12.1	582.65	15.6' RT
10	504+14.3	582.65	22.6' RT
11	504+16.5	582.73	15.6' RT
12	504+18.8	582.73	22.6' RT
13	503+84.4	581.24	14.0' LT
14	503+84.4	581.30	15.6' LT
15	503+84.4	581.36	18.6' LT
16	503+89.4	581.39	14.0' LT
17	503+89.4	581.45	15.6' LT
18	503+89.4	581.46	18.6' LT
19	503+89.4	581.54	22.6' LT
20	504+03.4	582.40	15.6' LT
21	504+01.7	582.47	22.6' LT
22	504+07.4	582.48	15.6' LT



SIDEWALK AND STAMPED CONCRETE DETAILS

(SECOND STREET)

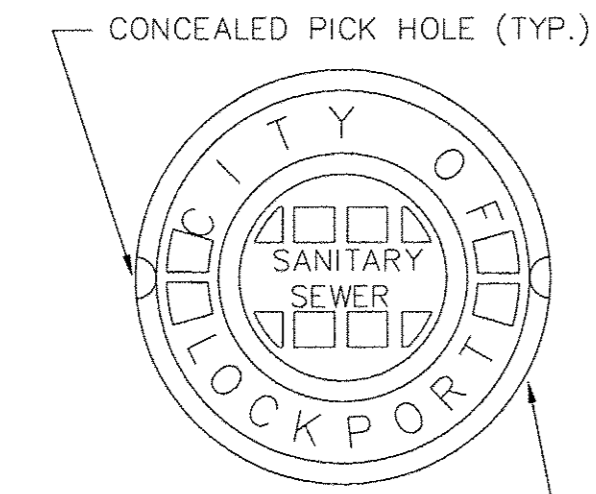
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BAXTER & WOODMAN Consulting Engineers	DESIGNED - CAC	REVISED -
	DRAWN - CJC	REVISED -
	CHECKED - ADJ	REVISED -
	DATE - 01-27-17	FILE - 131048SHT_Misc-Details.dgn

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY DETAILS	
SCALE:	STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	33
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61D71	



SANITARY SEWER
 NEENAH R-1710 W/WATERTIGHT LID
 EAST JORDAN 1050Z1 W/WATERTIGHT LID

WATERMAIN
 NEENAH R-1710 W/WATERTIGHT LID
 EAST JORDAN 1050Z1 W/WATERTIGHT LID

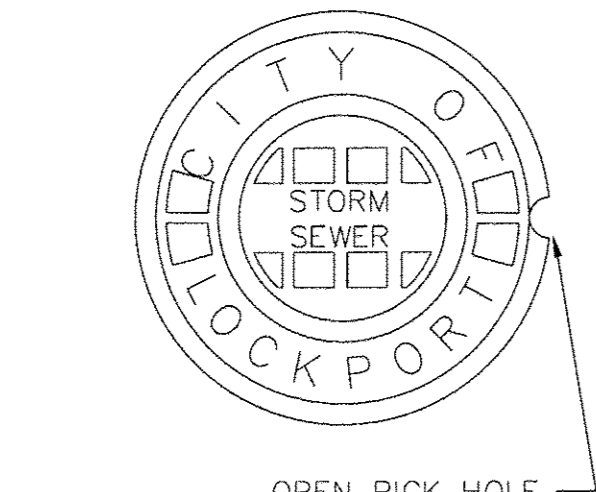
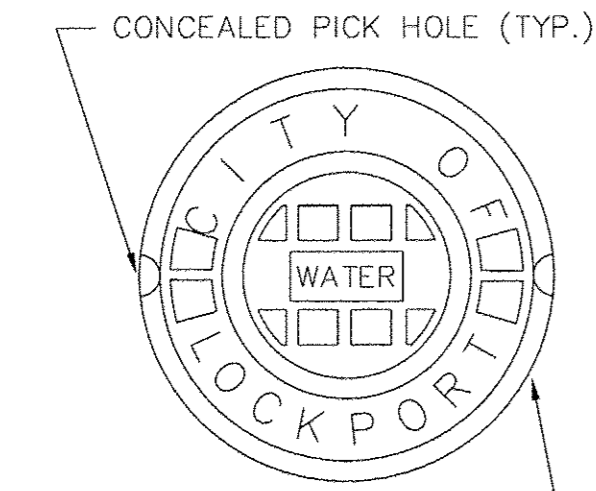
STORM SEWER
 PAVEMENT - OPEN LID
 NEENAH R-1710 W/TYPE D GRATE
 EAST JORDAN 1050Z1 W/TYPE M1 GRATE
 GRASSED AREA - OPEN LID
 NEENAH R-1710 W/TYPE B GRATE
 NEENAH R-4340-B
 EAST JORDAN 1050Z1 W/TYPE N GRATE
 EAST JORDAN 6527

PAVEMENT - CLOSED LID
 GRASSED AREA - CLOSED LID
 NEENAH R-1710 W/SOLID LID
 EAST JORDAN 1050Z1 W/TYPE A LID

CURB & GUTTER - TYPE B6.12
 NEENAH R-3278-A
 EAST JORDAN 7221 W/T1 BACK & M1 GRATE

CURB & GUTTER - TYPE M3.12
 NEENAH R-3501-E2
 EAST JORDAN 7250

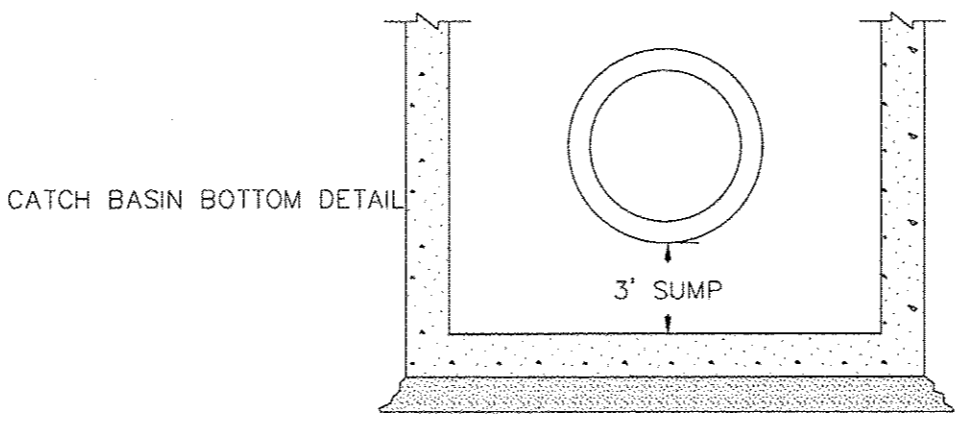
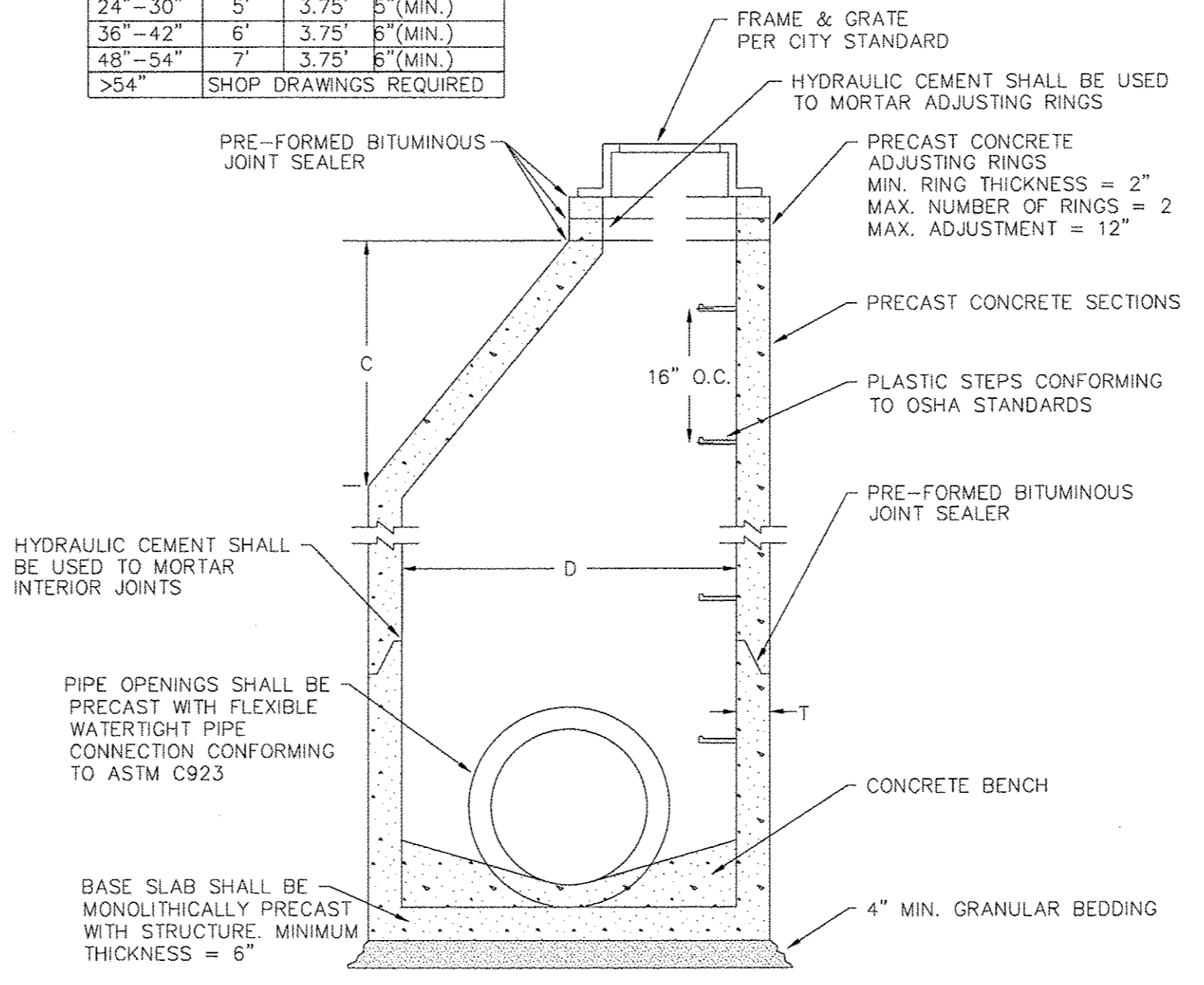
CURB & GUTTER - DEPRESSED
 NEENAH R-3525-L
 NEENAH R-3205
 EAST JORDAN 7210 W/TYPE M3 GRATE
 EAST JORDAN 7464



**EXHIBIT #1001
 CITY OF LOCKPORT
 FRAME & LID
 DETAIL**

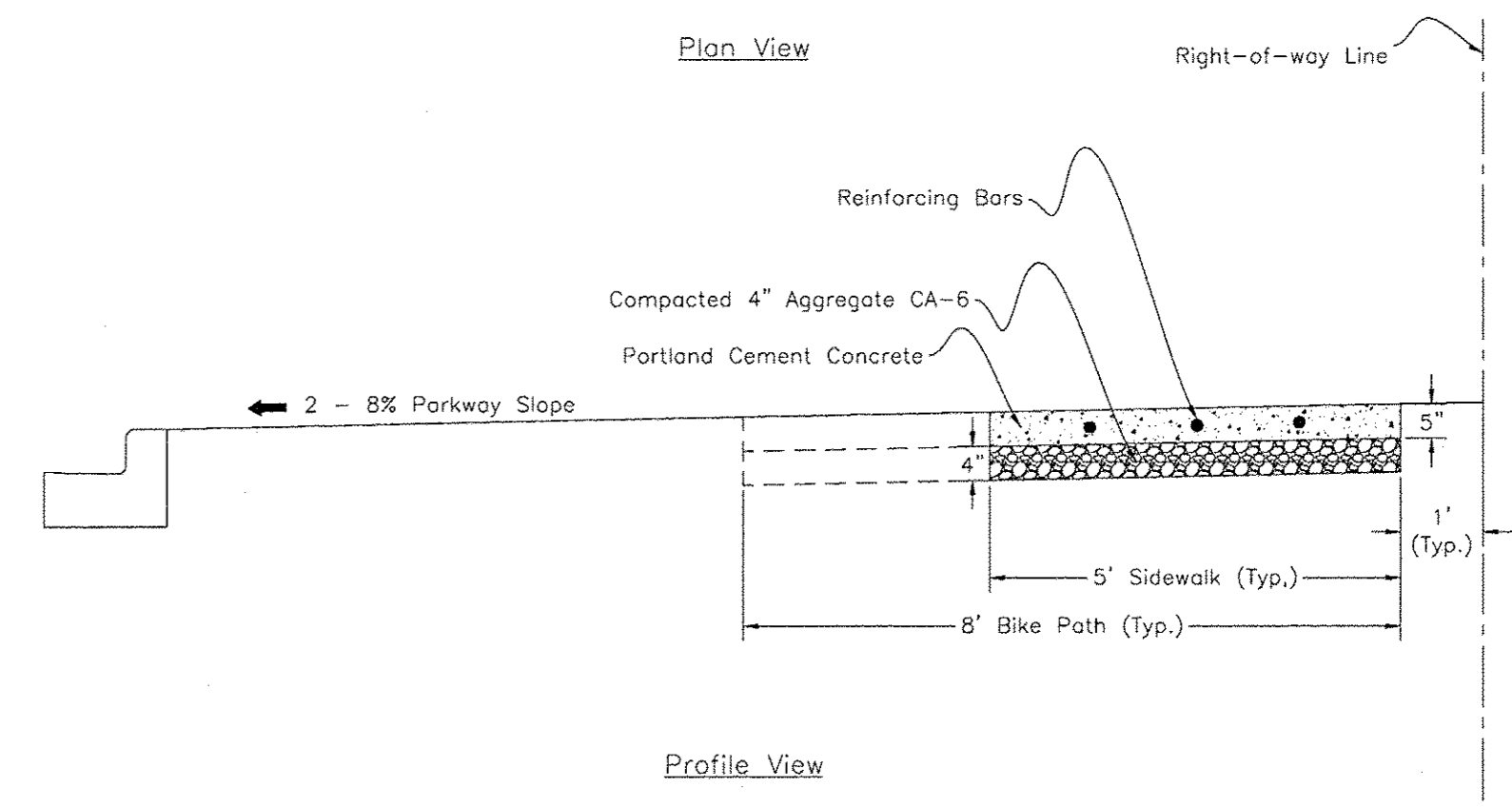
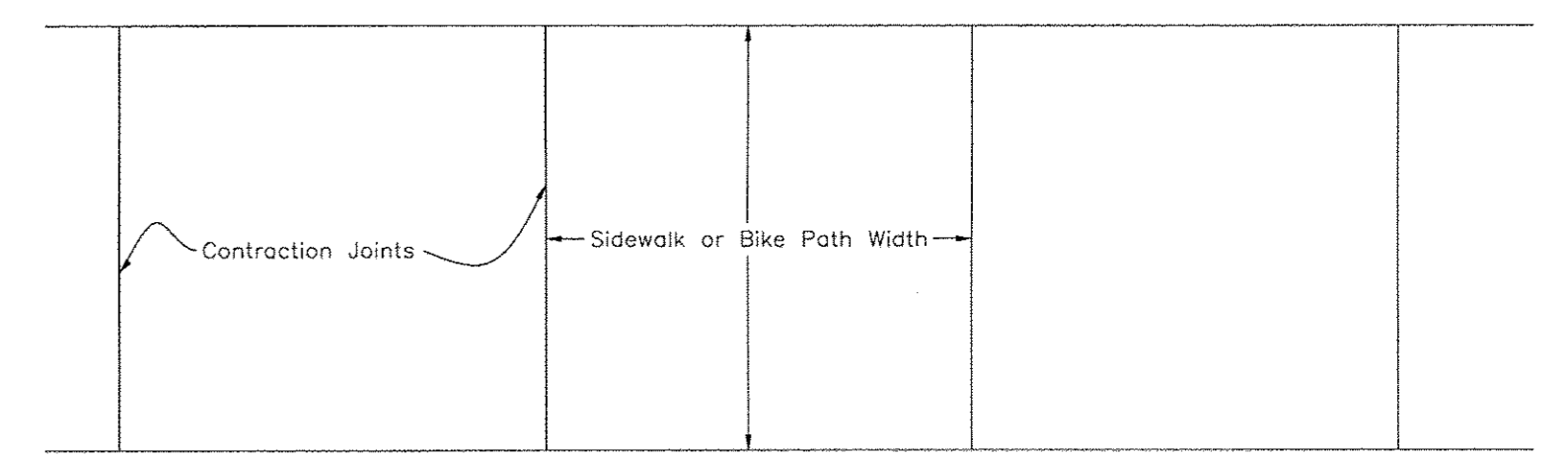
ADOPTED:	FEBRUARY 2002
REVISIONS	

PIPE DIA.	D	C	T
10"-21"	4'	2.5'	5"(MIN.)
24"-30"	5'	3.75'	6"(MIN.)
36"-42"	6'	3.75'	6"(MIN.)
48"-54"	7'	3.75'	6"(MIN.)
>54"	SHOP DRAWINGS REQUIRED		



**EXHIBIT #
 CITY OF LOCKPORT
 MANHOLE/CATCH BASIN
 DETAIL**

ADOPTED:	NOV. 20, 2000
REVISIONS	



- Notes:**
- At all trench crossings, 3 - 10' x 1/2" reinforcing bars shall be placed, equally spaced and centered over the trench.
 - Concrete thickness shall be 6" at all residential driveways and trench crossings, and 8" at all commercial driveway locations.
 - 1/2" expansion joints shall be constructed at a maximum spacing of 50'.
 - DOT Class 'SI' 6.1 bag mix. No fly ash.
 - Curb Ramps and Detectable Warnings shall be per ADAAG, Illinois Accessibility Code, latest revision.

**EXHIBIT #502
 CITY OF LOCKPORT
 SIDEWALK DETAIL**

ADOPTED:	FEB 02, 2007
REVISIONS	

REINFORCEMENT BARS AND ADDITIONAL CONCRETE THICKNESS INCLUDED IN COST OF PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
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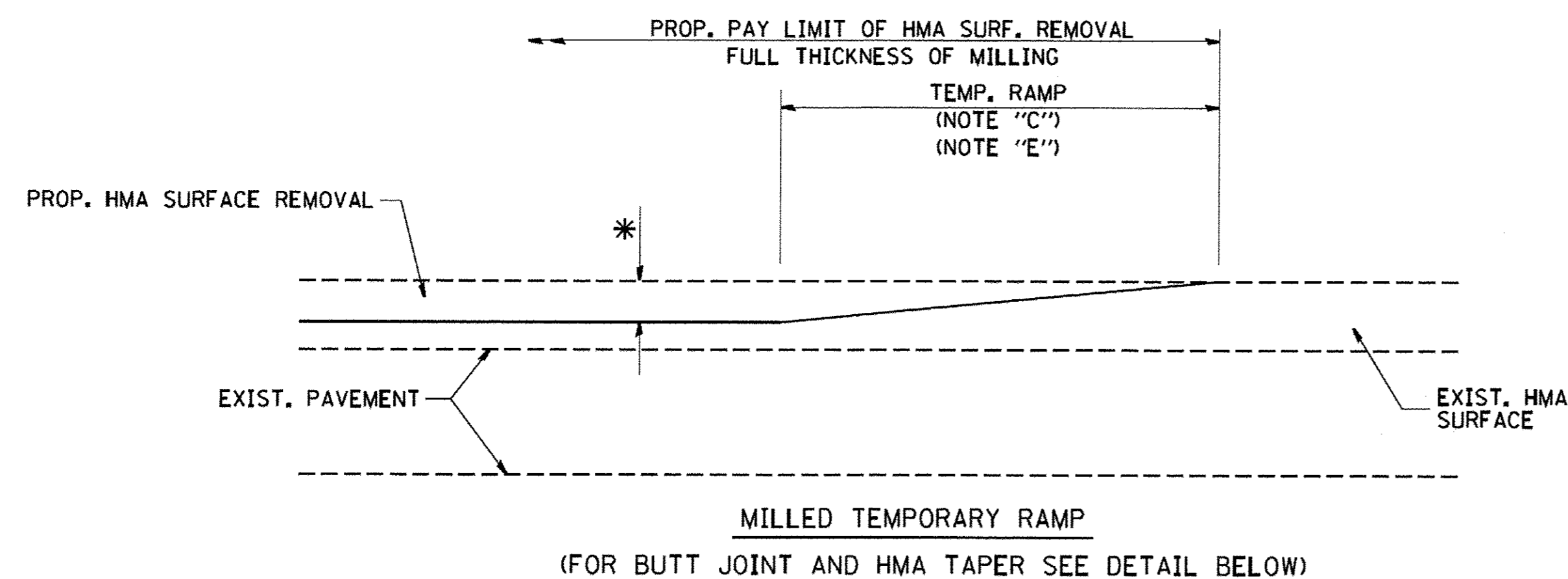
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DRAWN -	CJC	REVISED -	
CHECKED -	ADJ	REVISED -	
DATE -	01-27-17	FILE -	131048SHT_Misc-Details.dgn

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

CITY OF LOCKPORT DETAILS

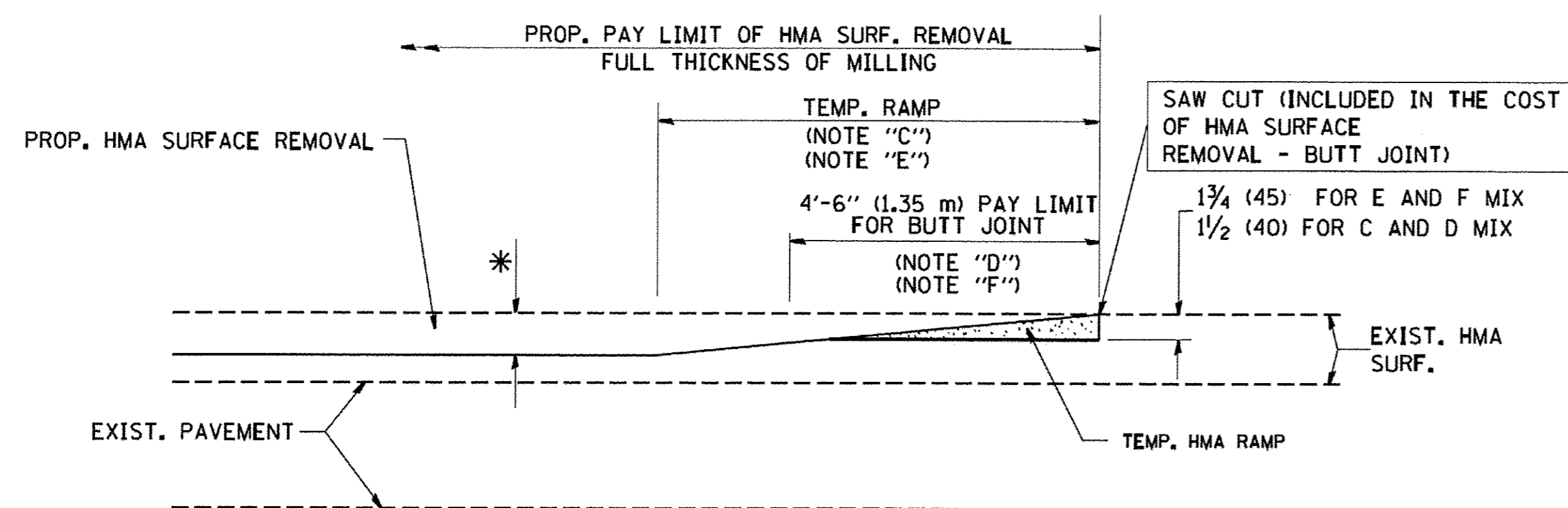
SCALE: STA. TO STA.

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 61D71				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



OPTION 1

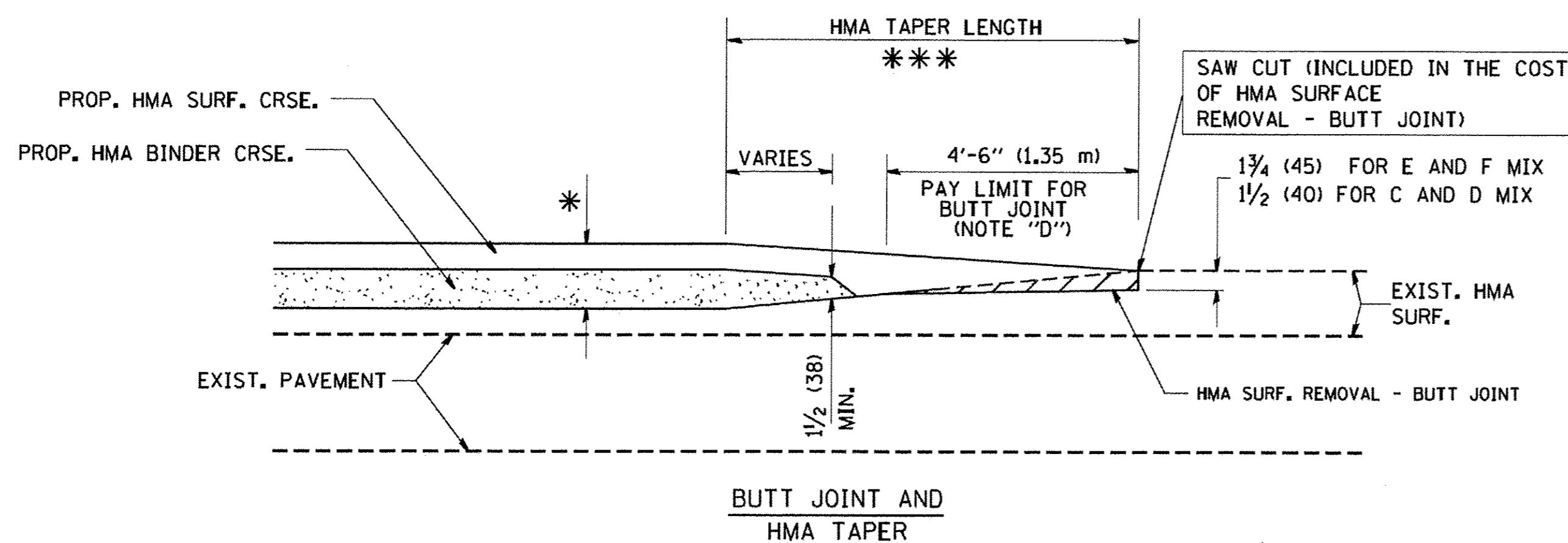
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)



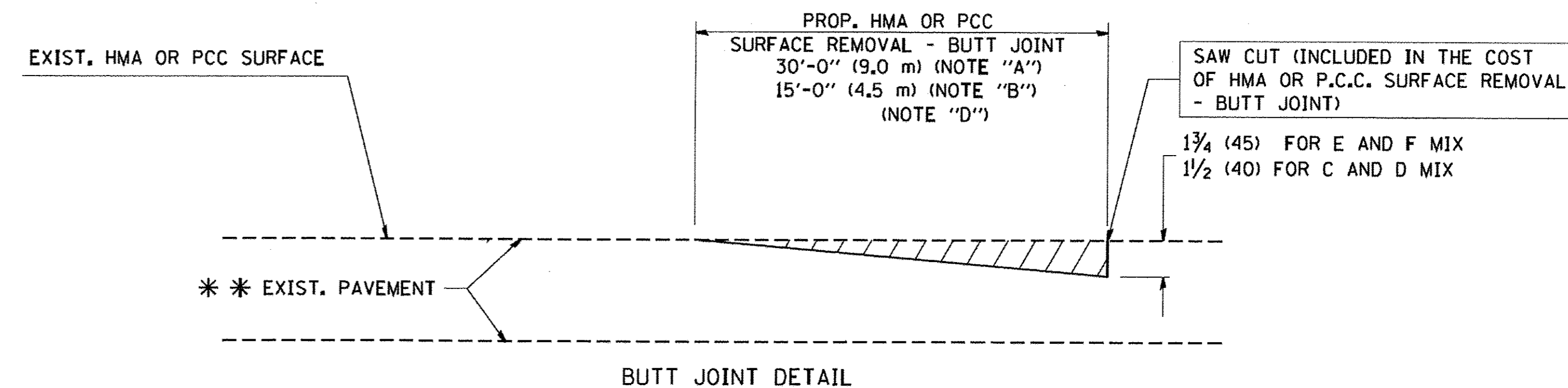
OPTION 2

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

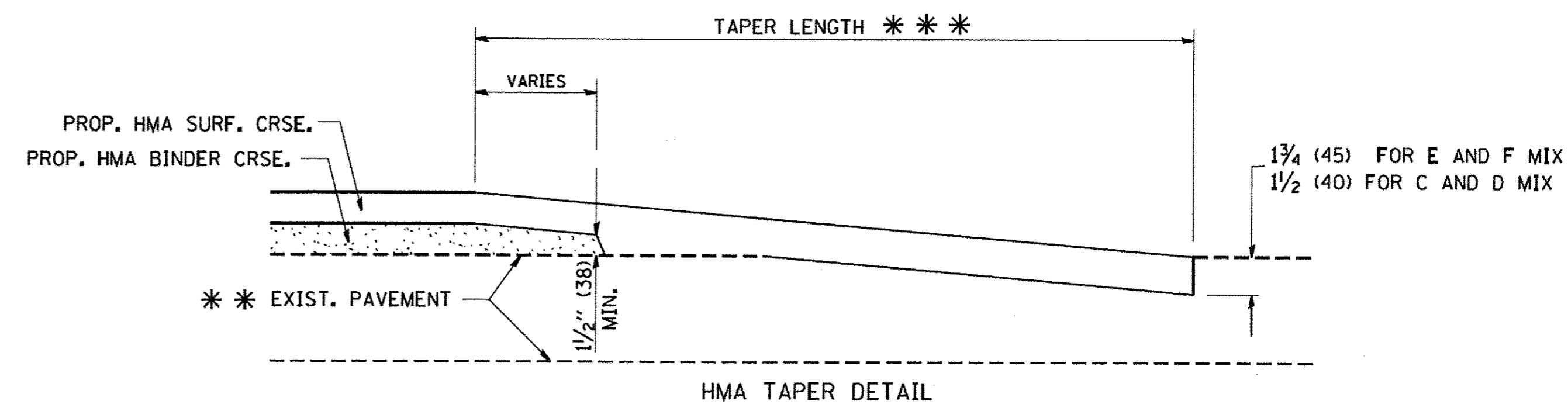
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
- 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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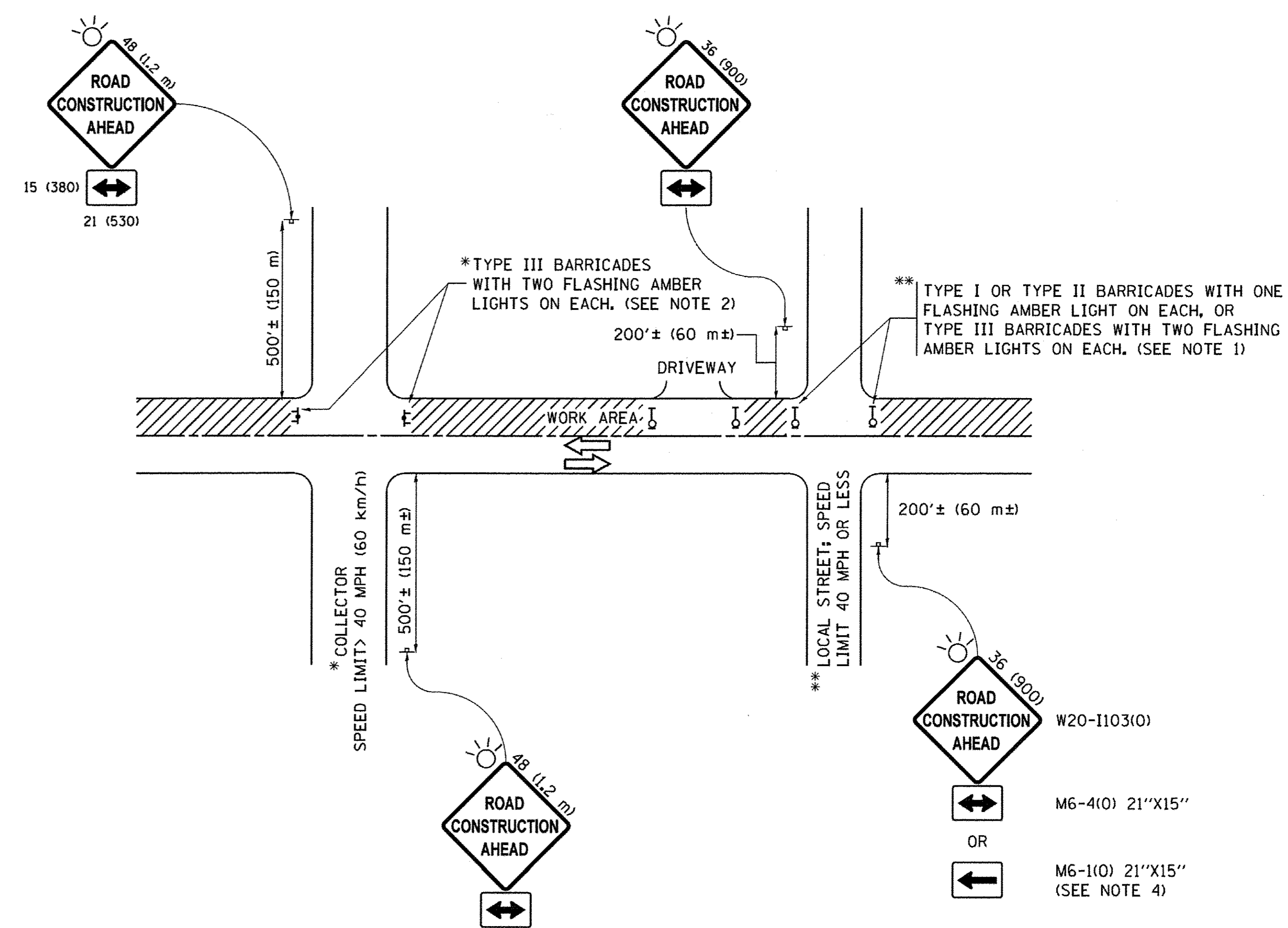
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PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISOR - M. GOMEZ 04-06-01	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 3035	SECTION 13-0080-00-BR	COUNTY WILL	TOTAL SHEETS 42	SHEET NO. 36
BD400-05 BD32		CONTRACT NO. 61071		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJ. ECR05-4003(334)				

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- NOTES:**
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
 - WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
 - WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S), THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
 - ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
 - THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

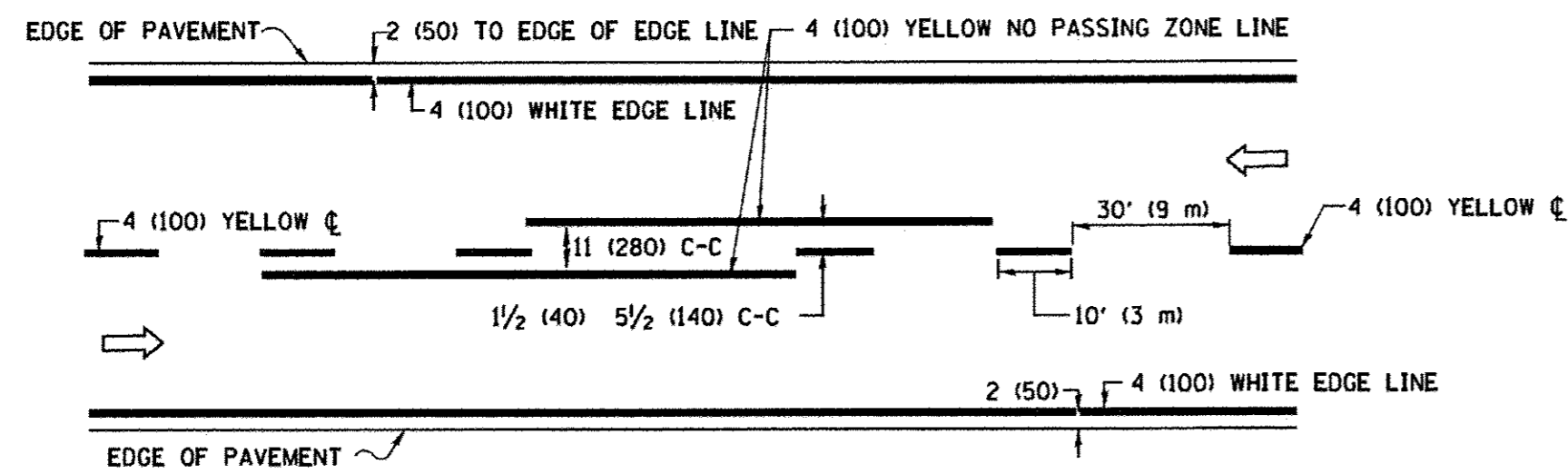
All dimensions are in inches (millimeters) unless otherwise shown.

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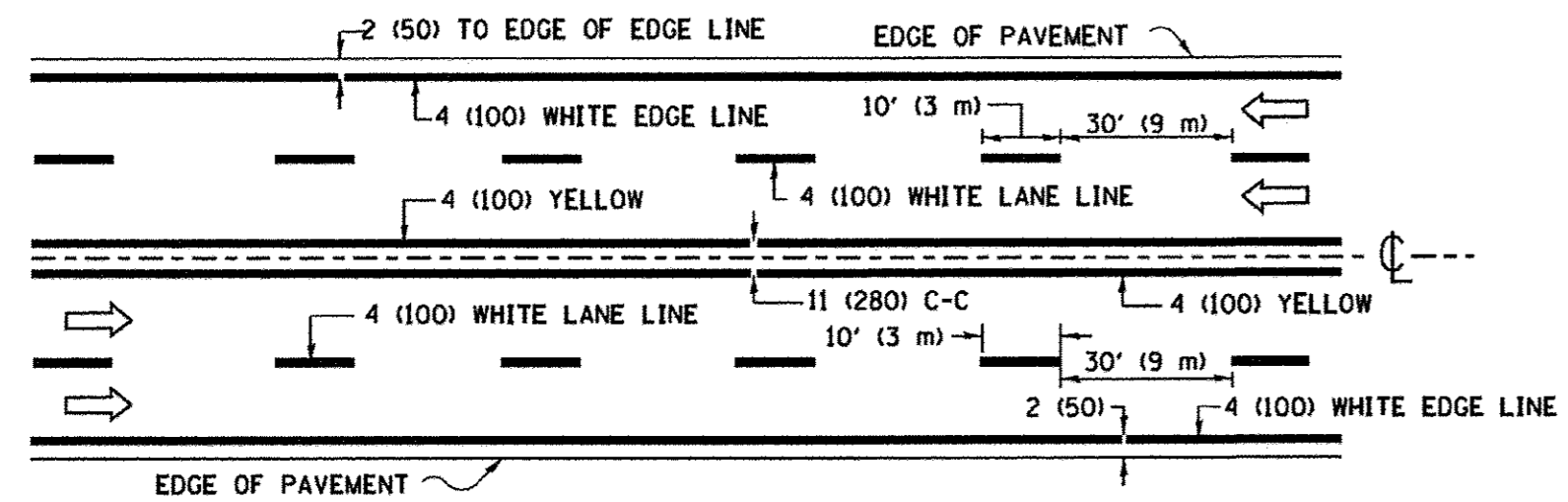
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

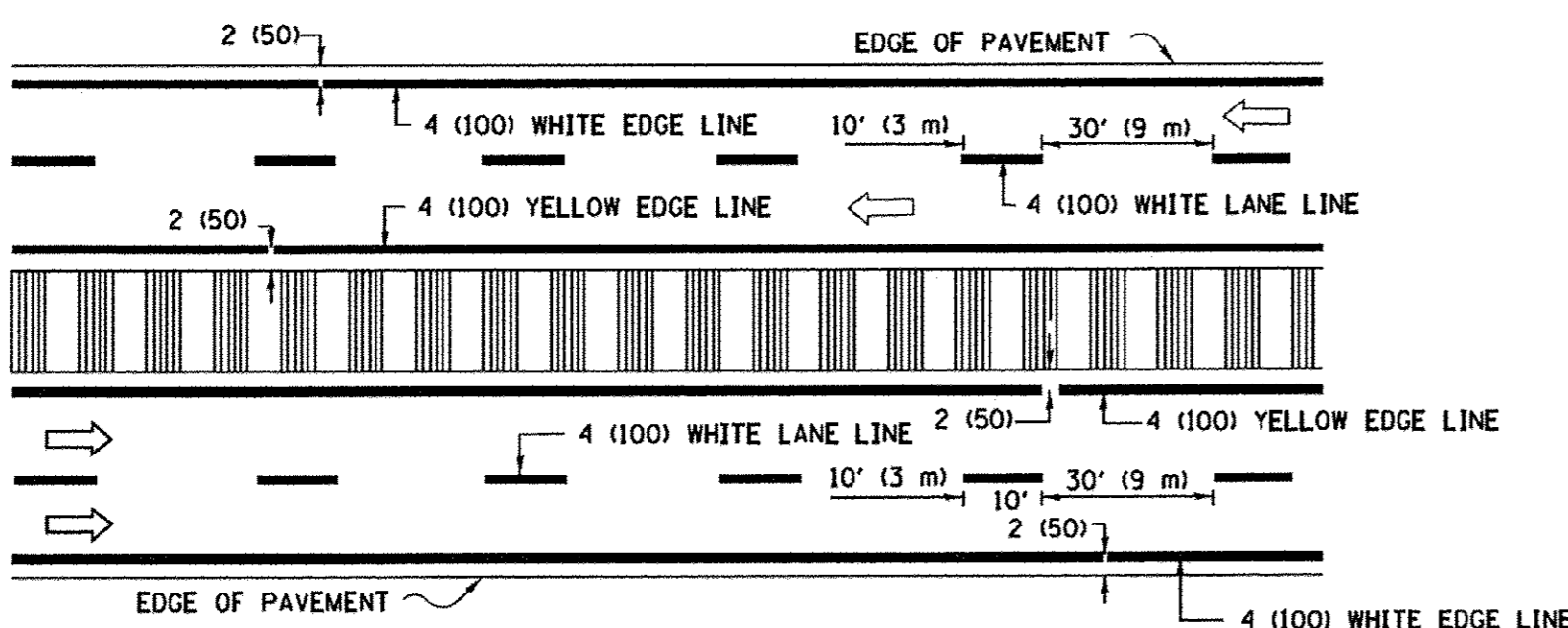
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3035	13-00080-00-BR	WILL	42	37
TC-10			CONTRACT NO. 61D71	
ILLINOIS FED. AID PROJ. EROS-4003(334)				



2-LANE ROADWAY

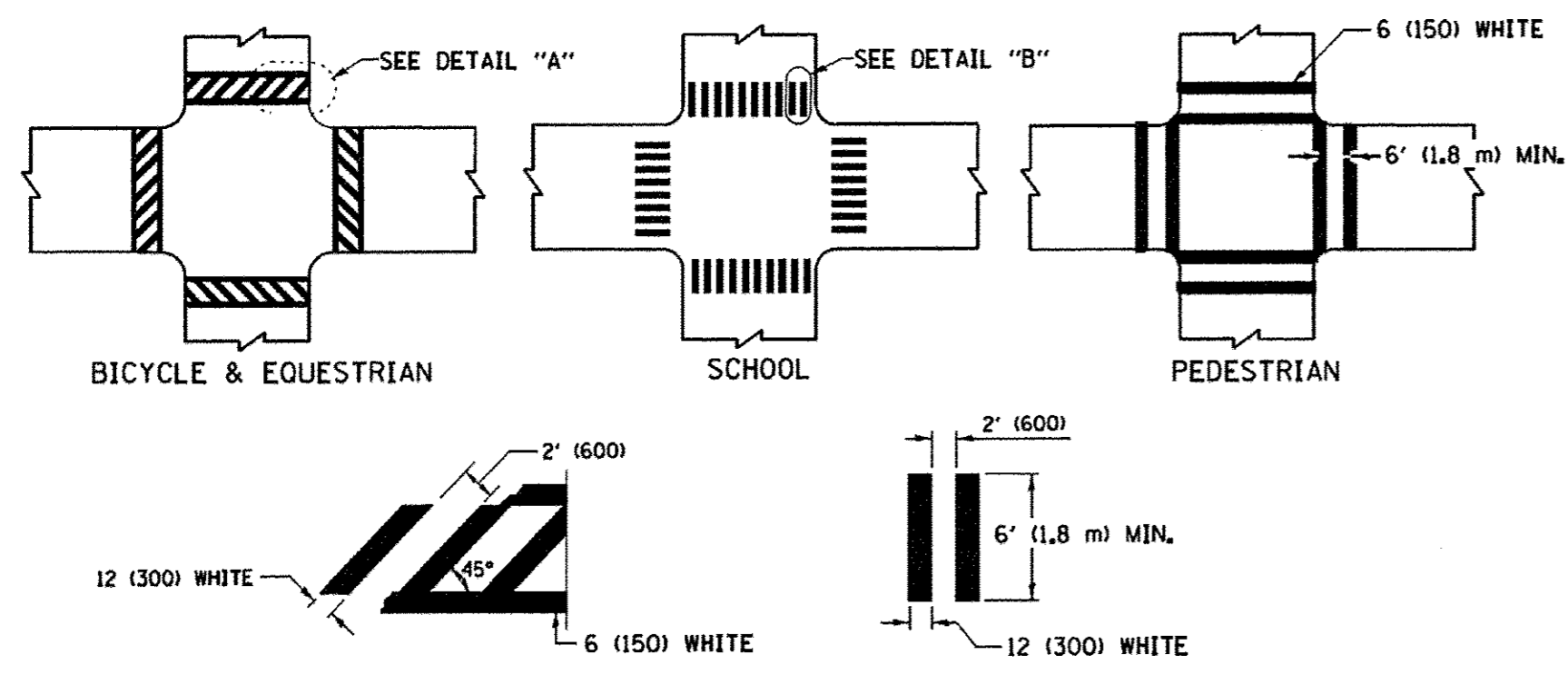


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

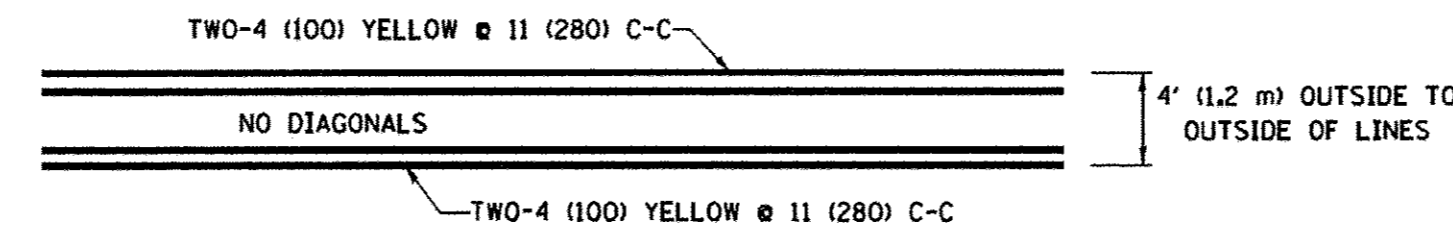


DETAIL "A"

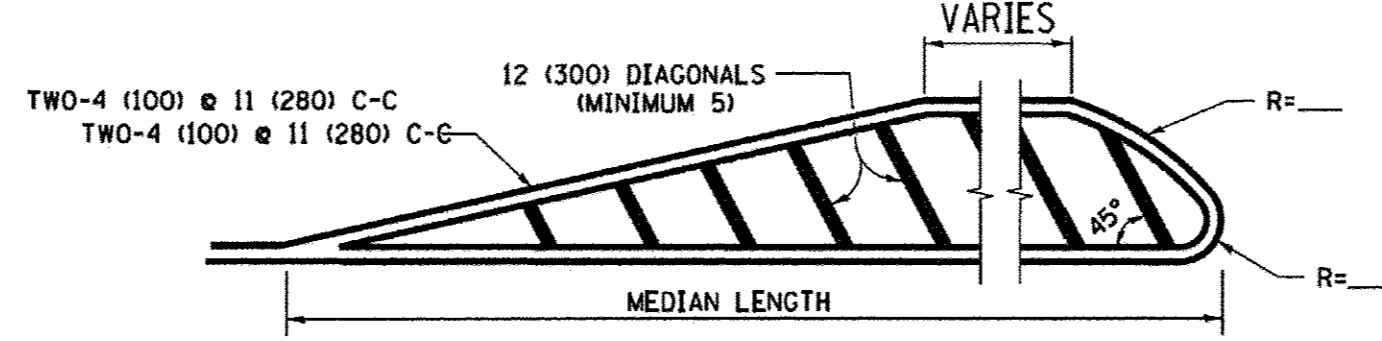
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

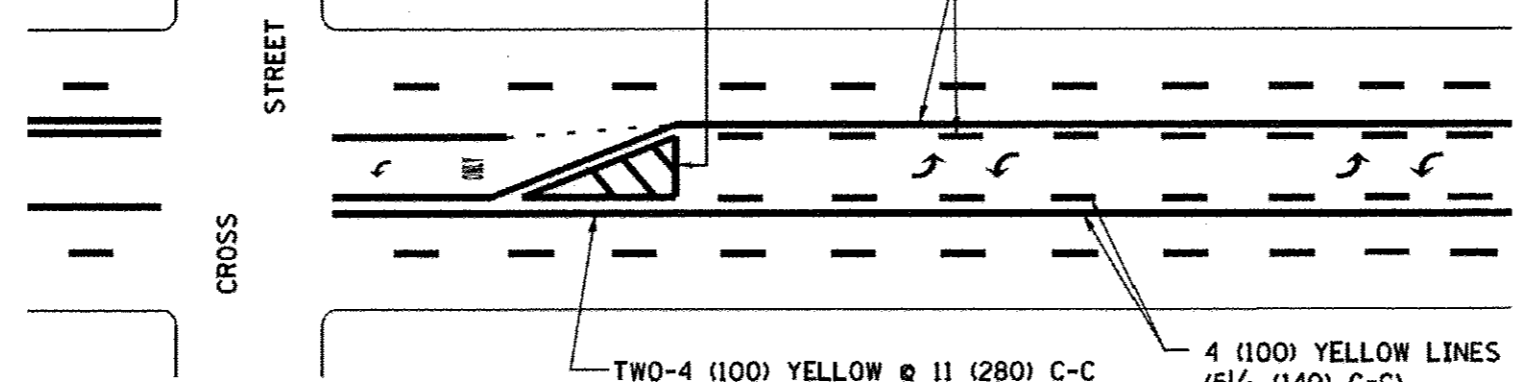


4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

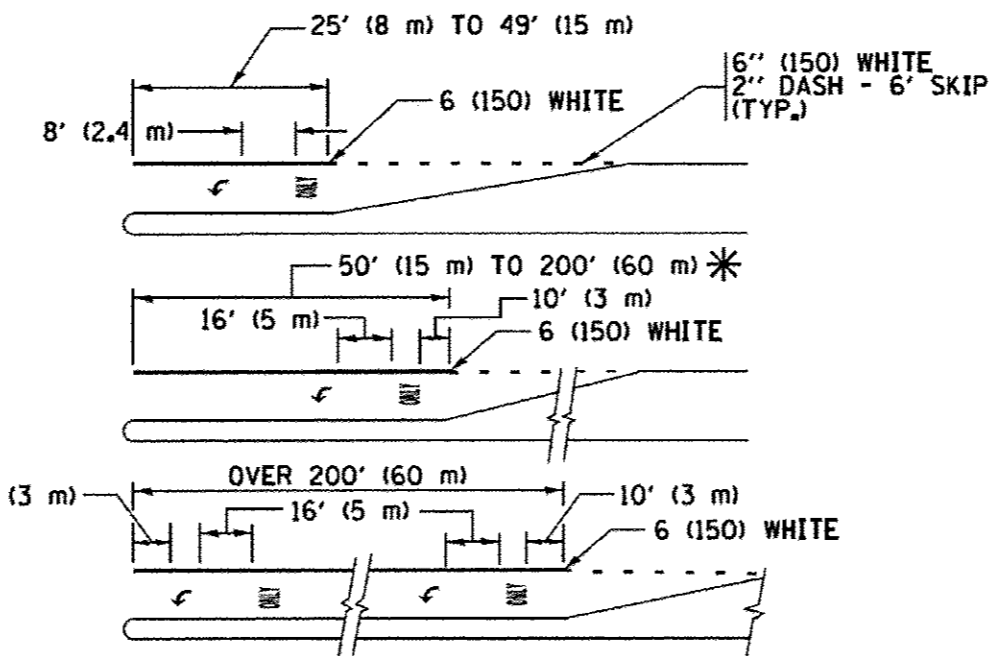
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

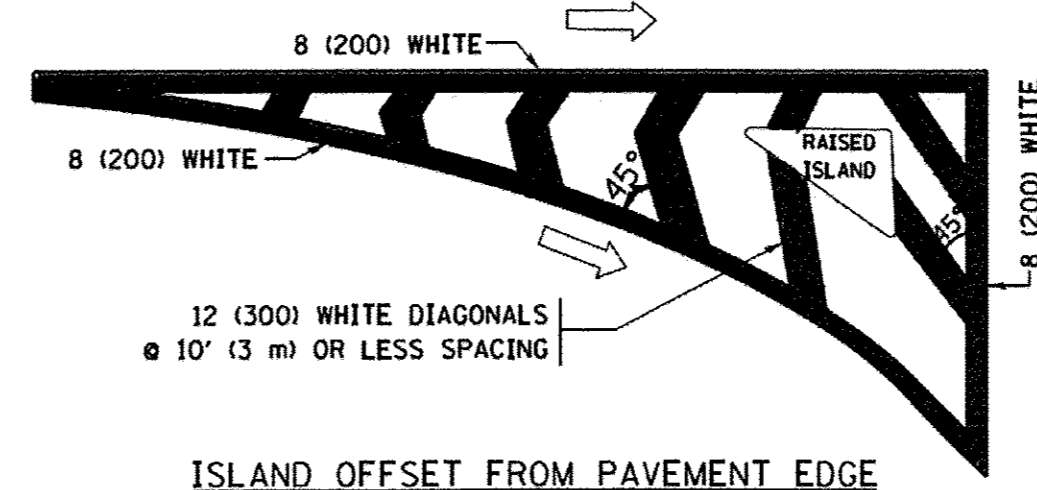


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

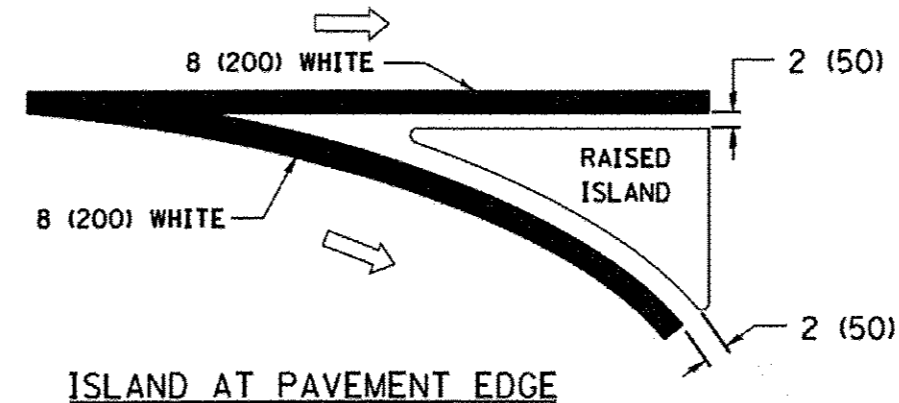
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

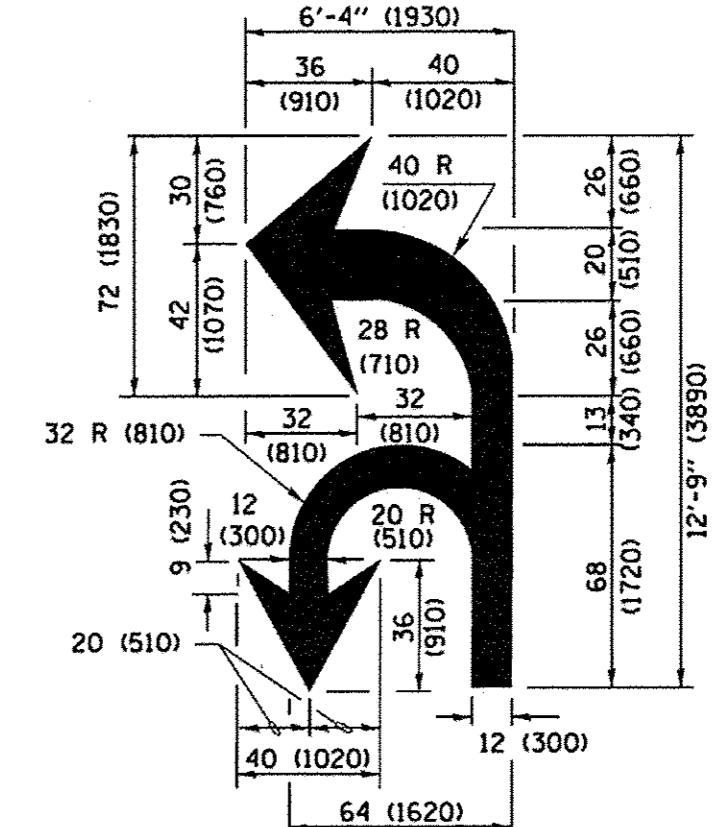


ISLAND OFFSET FROM PAVEMENT EDGE

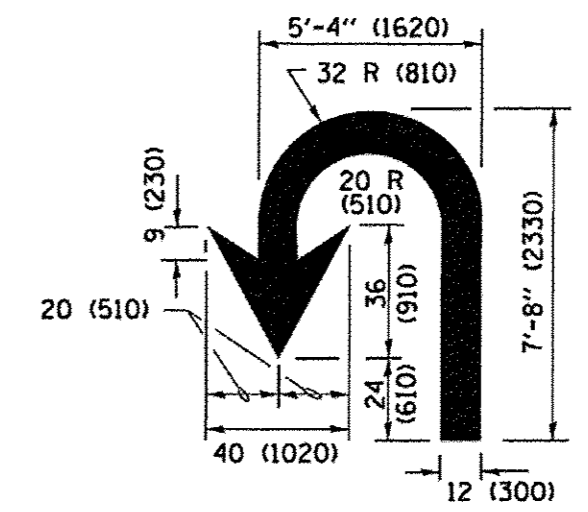


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE. SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF




FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.




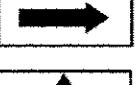

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 2/3/2017

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	F.A. RTE. 3035	SECTION 13-00080-00-BR	COUNTY WILL	TOTAL SHEETS 42	SHEET NO. 38
SCALE: NONE	SHEET 1 OF 1 SHEETS STA. TO STA.	TC-13		CONTRACT NO. 61071		
				ILLINOIS FED. AID PROJ. 4003(334)		





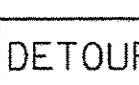
ROUTE MARKERS

-  FOR U.S. ROUTES
MI-40-2424
-  FOR ILLINOIS ROUTES
MI-50-2424
-  R.R. UNMARKED ROUTES
SPECIAL 24" x 18" VARIABLE
4" BLACK LETTERS ON WHITE
REFLECTIVE BACKGROUND

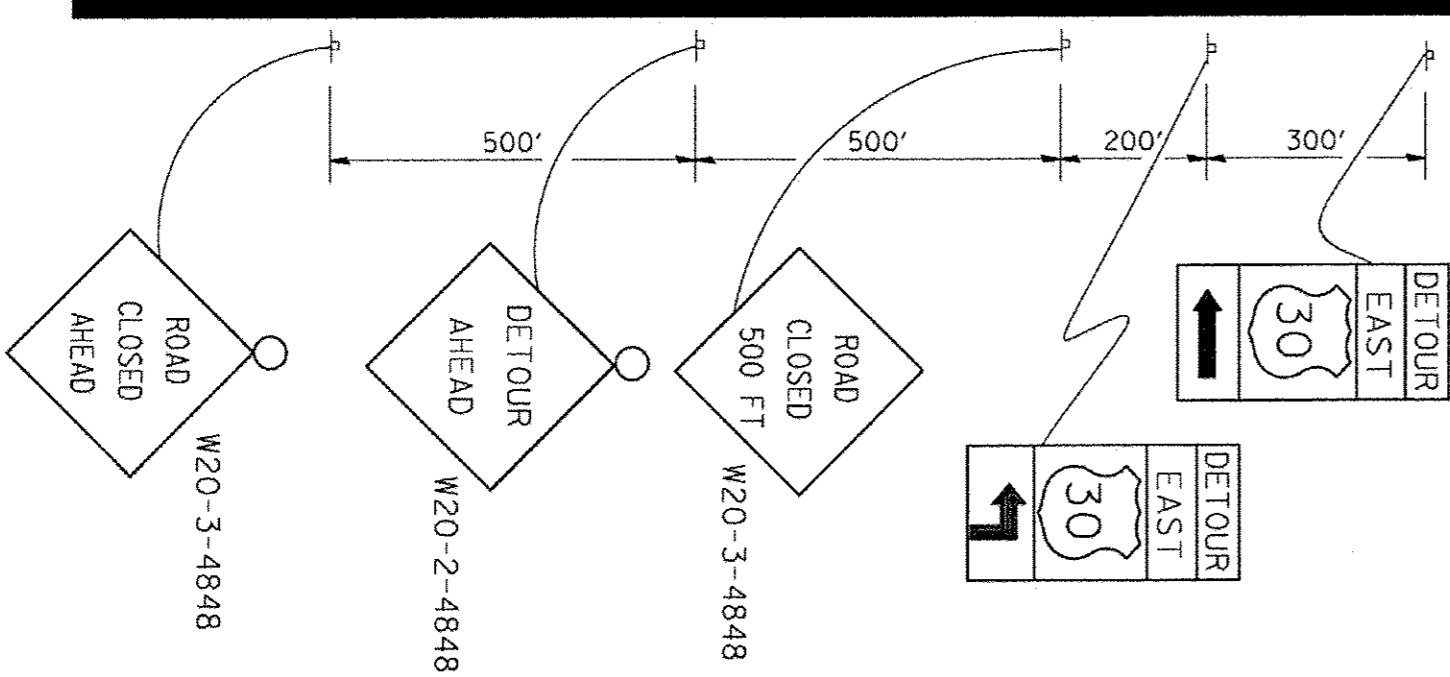
ARROWS SIGNS

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-  M5-1R-2115
-  M6-1-2115
-  M6-1-2115
-  M6-3-2115

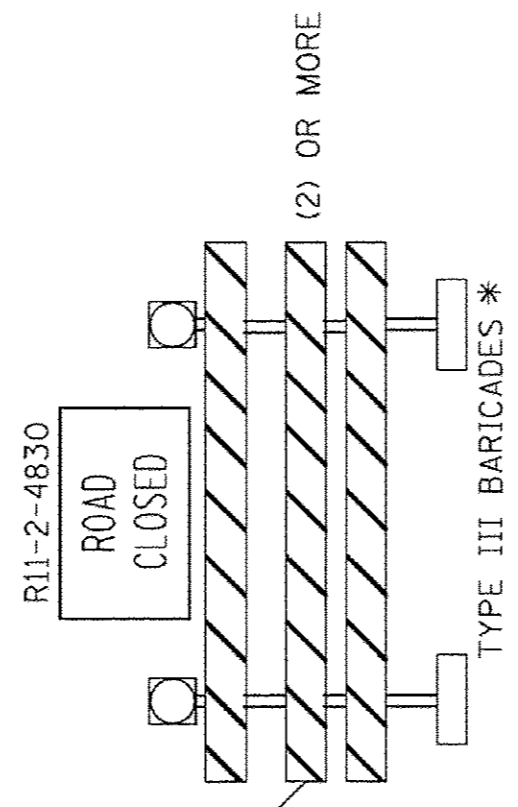
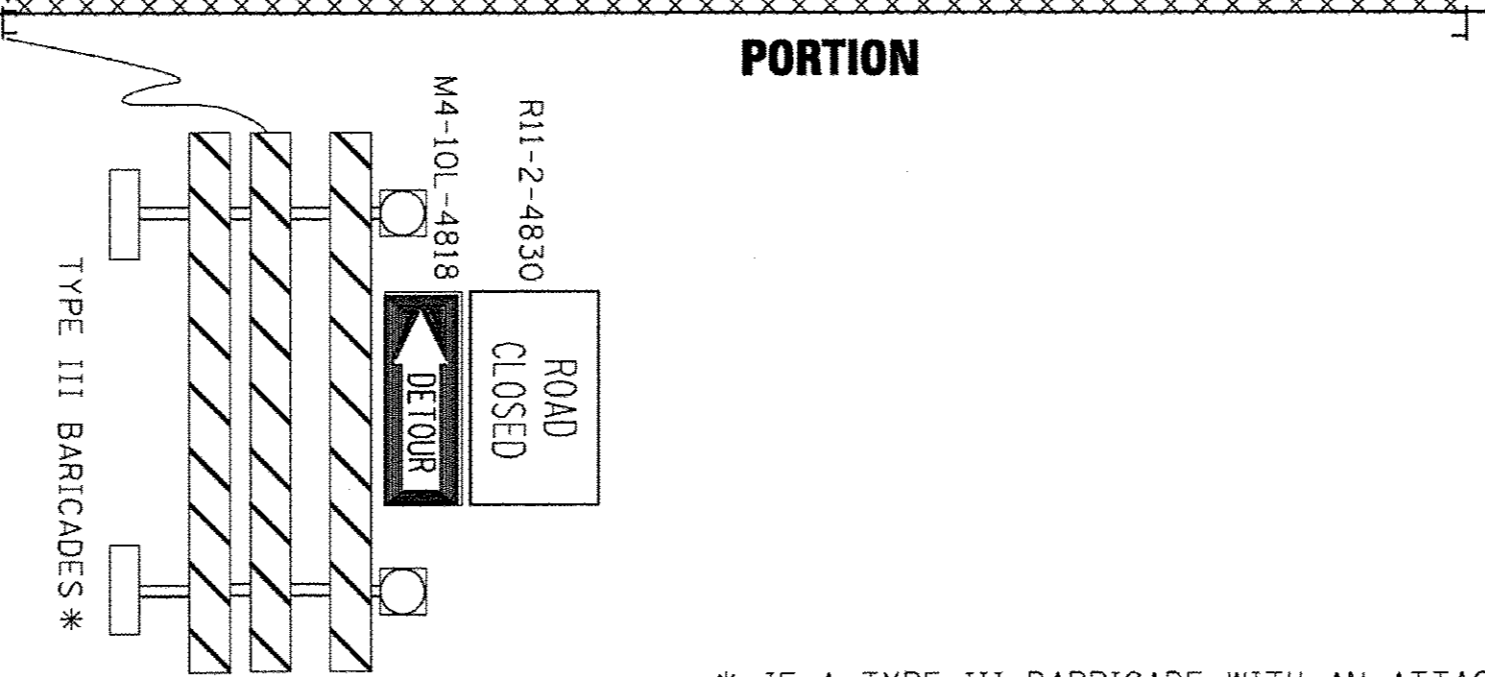
CARDINAL DIRECTION & DETOUR SIGNS

-  NORTH M3-1-2412
-  EAST M3-2-2412
-  SOUTH M3-3-2412
-  WEST M3-4-2412
-  DETOUR M4-8-2412

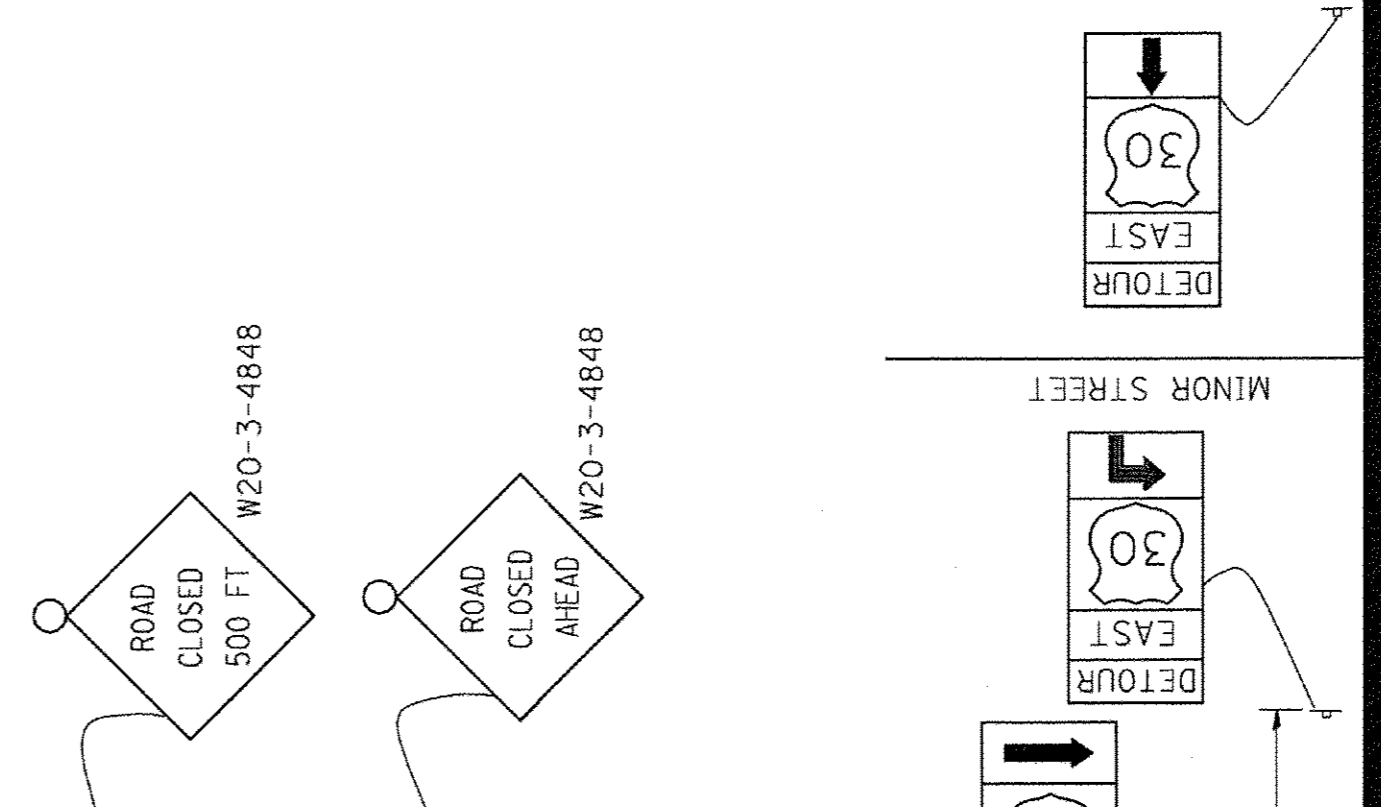
STATE ROUTE



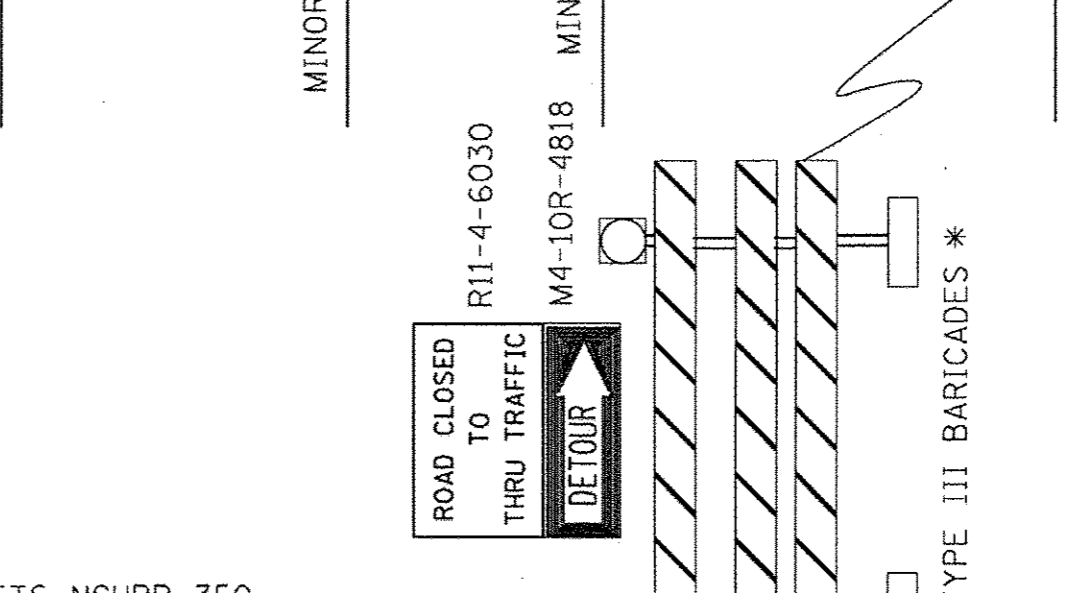
COMPLETELY CLOSED PORTION



DETOUR



PARTIALLY CLOSED PORTION



STATE ROUTE



* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

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 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00121 - EXPIRES 4/30/2017
 10:58:23 AM 2/23/2017
 FILE NAME = c:\pwwork\pwi001\DRIVAKOSGN\d8108315\121.dgn
 USER NAME = drivakosgn
 DESIGNED - 10-18-02
 DRAWN - R. BORO 09-14-09
 CHECKED -
 DATE -
 PLOT SCALE = 49.9999' / IN.
 PLOT DATE = 9/14/2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

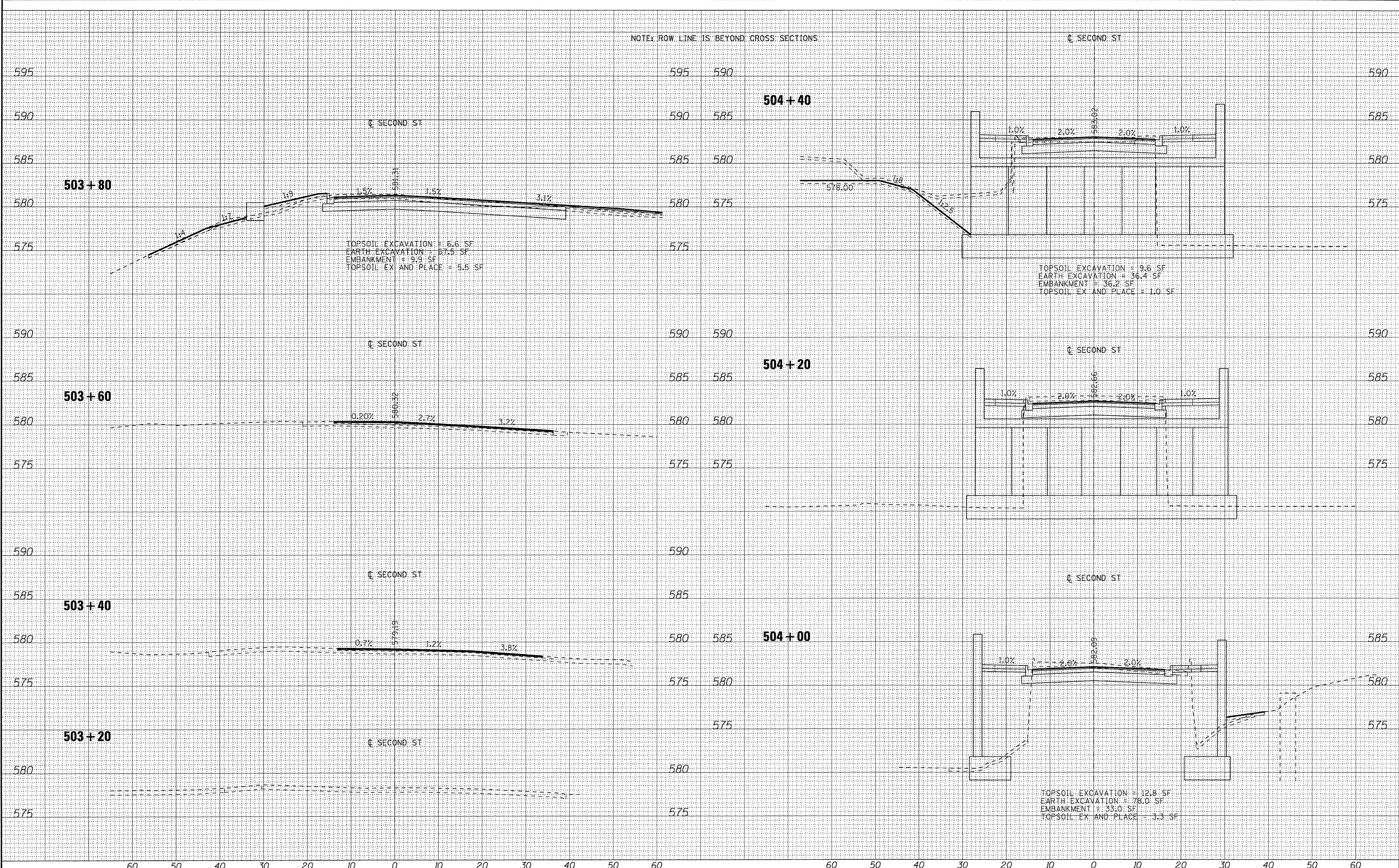
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

**DETOUR SIGNING
FOR CLOSING STATE HIGHWAYS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	39
TC-21				
FED. ROAD DIST. NO. 1 ILLINOIS			CONTRACT NO. 61D71	
AID PROJ. # BR05-4003(334)				

NOTE: ROW LINE IS BEYOND CROSS SECTIONS

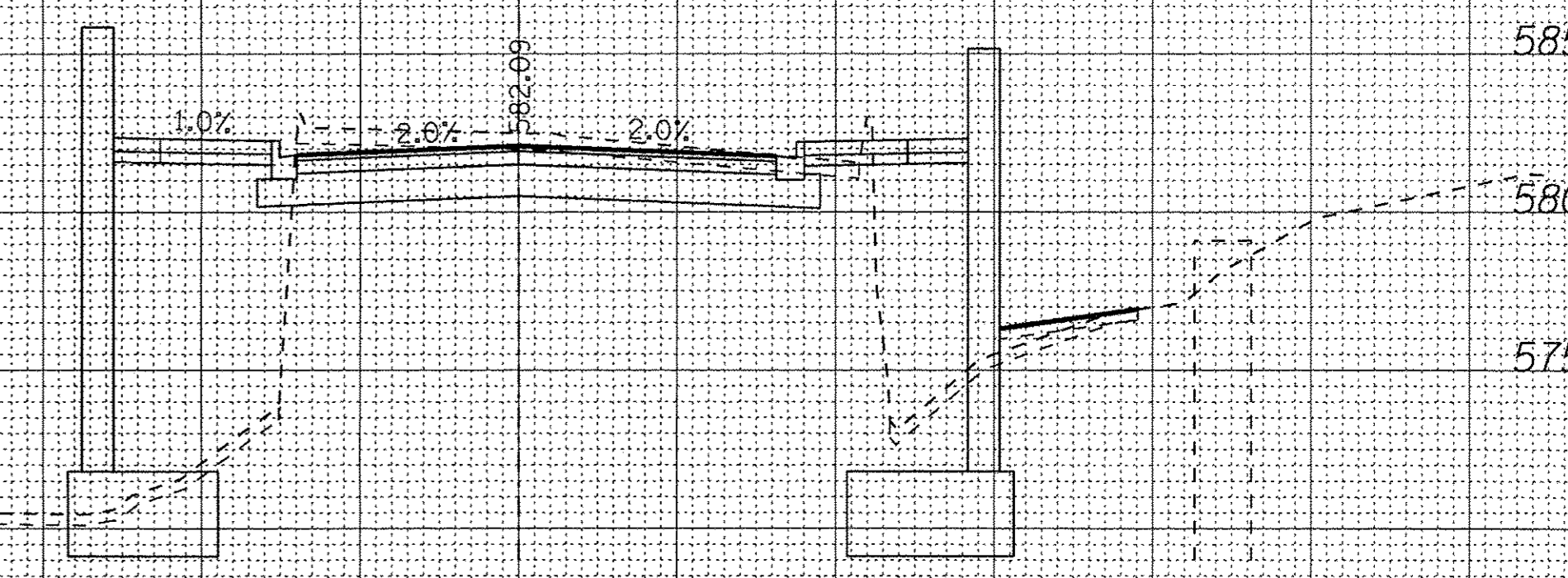
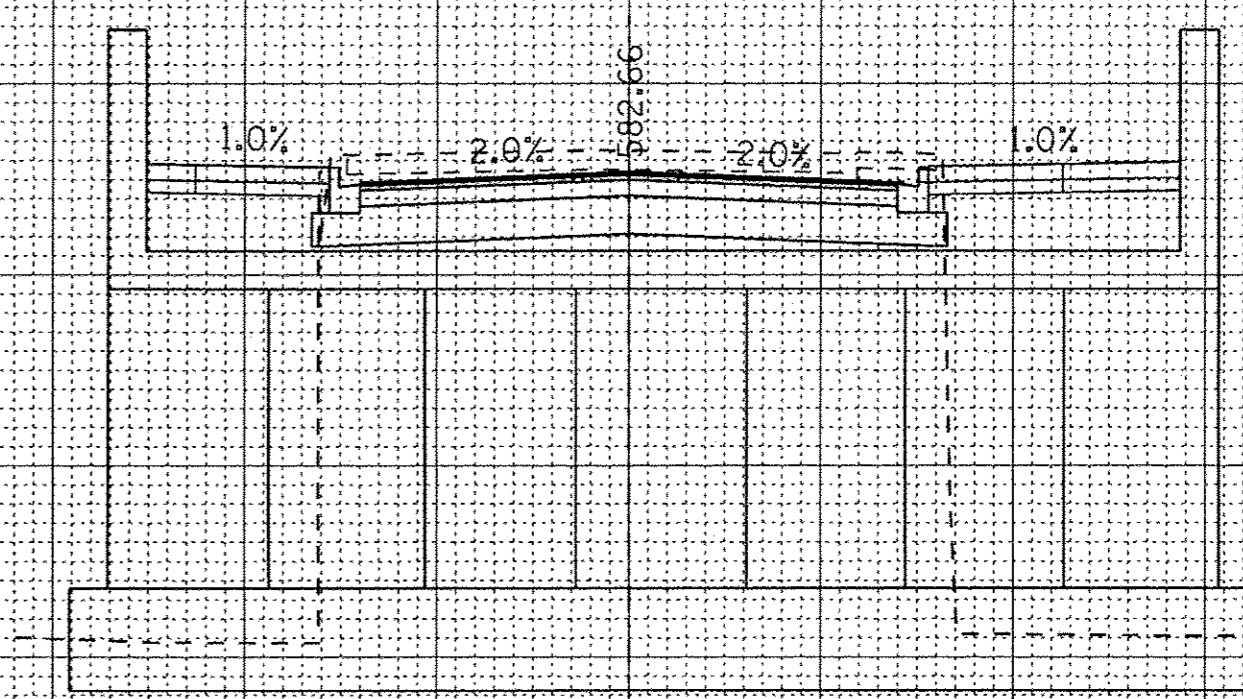
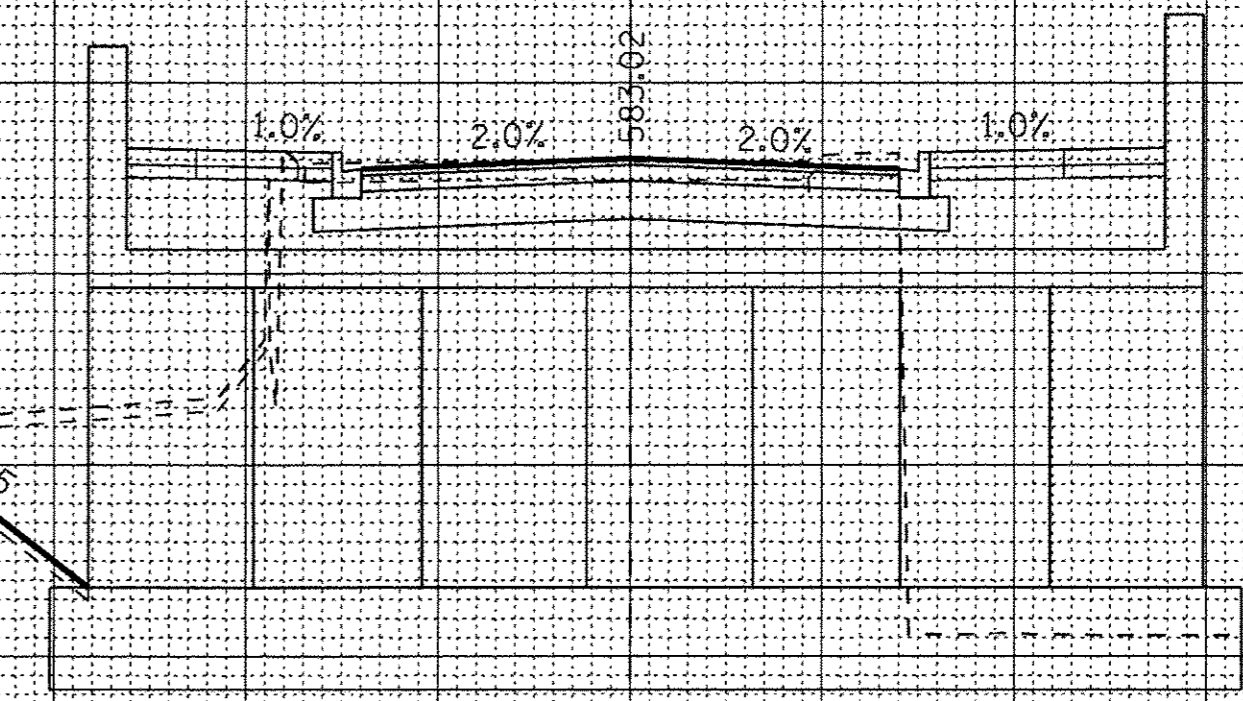
Q SECOND ST



TOPSOIL EXCAVATION = 6.6 SF
 EARTH EXCAVATION = 67.5 SF
 EMBANKMENT = 9.9 SF
 TOPSOIL EX AND PLACE = 5.5 SF

TOPSOIL EXCAVATION = 9.6 SF
 EARTH EXCAVATION = 36.4 SF
 EMBANKMENT = 36.2 SF
 TOPSOIL EX AND PLACE = 1.0 SF

TOPSOIL EXCAVATION = 12.8 SF
 EARTH EXCAVATION = 78.0 SF
 EMBANKMENT = 33.0 SF
 TOPSOIL EX AND PLACE = 3.3 SF



DESIGNED - CAC	REVISED -
DRAWN - CJC	REVISED -
CHECKED - ADJ	REVISED -
DATE - 01-27-17	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

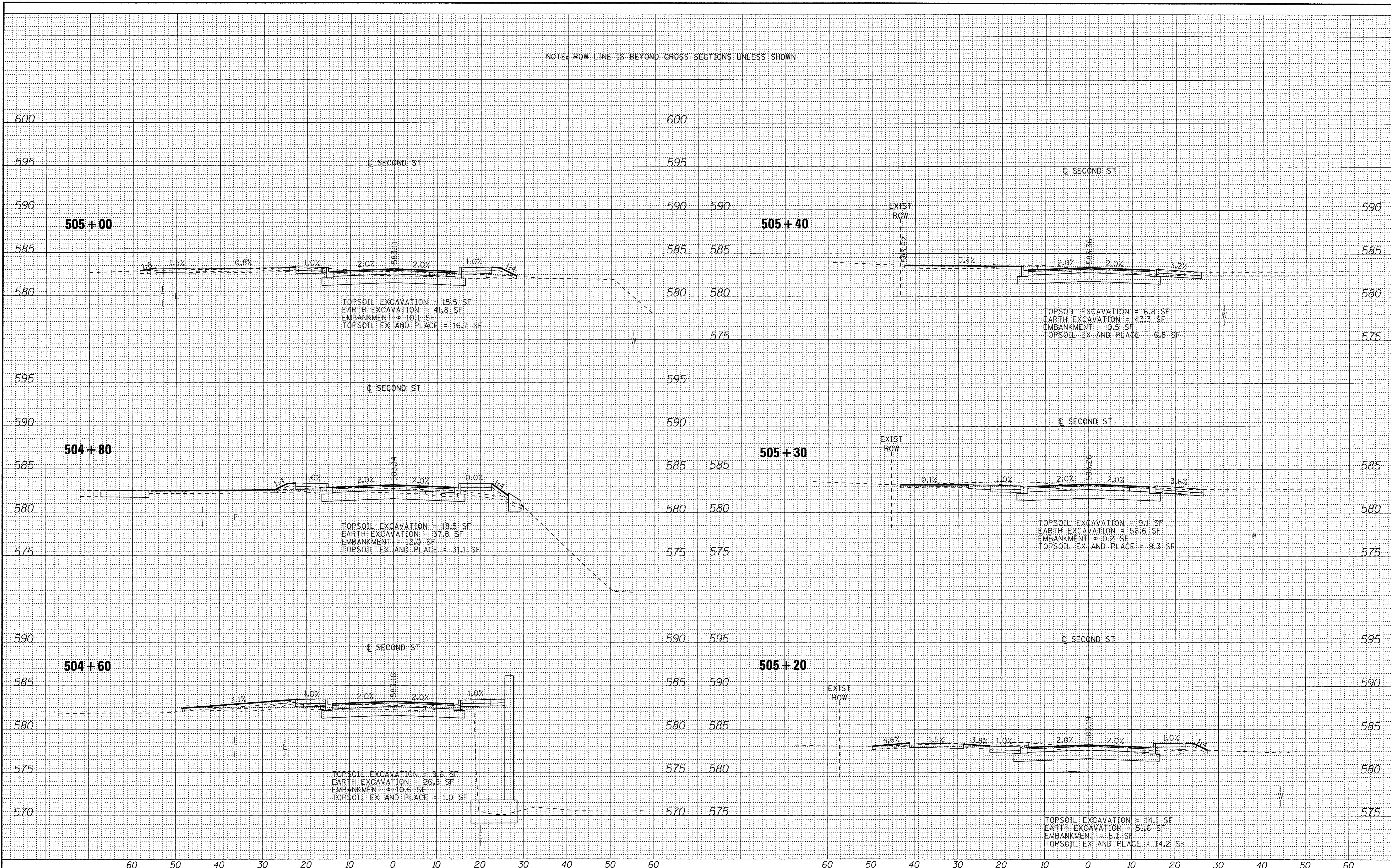
CROSS SECTIONS

SCALE: H: 1"=10' V: 1"=5'
 STA. 503+20 TO STA. 504+40

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	40
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D71	

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 PROJECT: ILLINOIS DEPARTMENT OF TRANSPORTATION - BRIDGE REHAB/CONSTRUCTION - SECOND ST BRIDGE
 DRAWING NO.: 13-00080-00-BR
 DATE: 01/27/17
 DRAWN BY: CJC
 CHECKED BY: ADJ
 DESIGNED BY: CAC

NOTE: ROW LINE IS BEYOND CROSS SECTIONS UNLESS SHOWN



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 LICENSE NO. 08-00021 EXPIRES 4/30/2017
 I:\Mokero\1\1048\13048-Second St Bridge Rehab\CADD\Sheets\Pr2\13048SHT XS-Second5.dgn
 10:51:36 AM



DESIGNED - CAC	REVISED -
DRAWN - CJC	REVISED -
CHECKED - ADJ	REVISED -
DATE - 01-27-17	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS

SCALE: H: 1"=10' V: 1"=5'

STA. 504+60 TO STA. 505+60

MUN	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3035	13-00080-00-BR	WILL	42	41
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D71	

