

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

FOR INDEX OF SHEETS, SEE SHEET NO. 2

F.A.S. ROUTE 2600 (IL 106)
SECTION (15) BDR, BJR
PROJECT STP-2600(105)
BRIDGE DECK REPAIRS
PIKE COUNTY

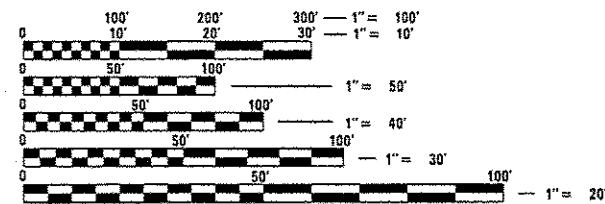
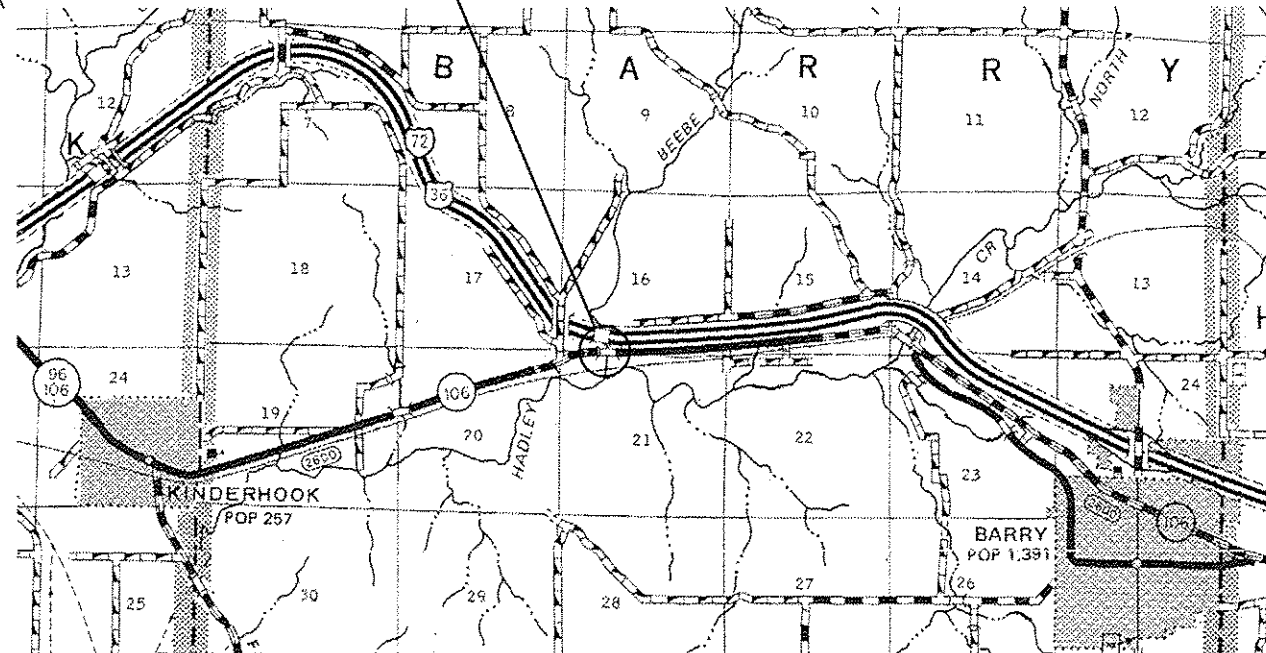
C-96-030-17

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	1
		ILLINOIS	CONTRACT NO. 72J66	

D-96-030-17



PROJECT LOCATION
 SN 075-0102
 IL 106 OVER BEEBE CREEK
 2.5 MI NE OF KINDERHOOK



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

BRIDGE MAINTENANCE ENGINEER: BRANDON DUDLEY (217) 785-9290
 BRIDGE INSPECTION ENGINEER: DAVE COPENBARGER (217) 785-5306

GROSS LENGTH = 500 FT. = 0.09 MILE
 NET LENGTH = 500 FT. = 0.09 MILE

CONTRACT NO. 72J66

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED 27 January 2017

 REGION FOUR ENGINEER

Mar 24 20 17

 ENGINEER OF DESIGN AND ENVIRONMENT

Mar 24 20 17

 DIRECTOR OF PROGRAM DEVELOPMENT

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 OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

1	COVER SHEET
2	INDEX, STANDARDS, SIGNATURES, GENERAL NOTES, & SCHEDULES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5	ROADWAY PLAN
6	STAGING PLAN
7	TRAFFIC CONTROL PLAN
8-14	SN 075-0102 BRIDGE PLANS

STANDARDS

000001-06
001001-02
001006
701001-02
701006-05
701201-04
701301-04
701321-16
701326-04
701901-06
704001-08
780001-05

GENERAL NOTES:

BASE COURSE WIDENING SHALL BE COMPLETED PRIOR TO STAGING TRAFFIC.

AREAS OF DECK AND APPROACH REPAIRS SHOWN ARE ESTIMATED. THE ENGINEER SHALL SHOW ACTUAL LOCATIONS OF DECK REPAIRS ON AS-BUILT PLANS.

STA	TO	STA	LOC.	LINE TYPE	PAVT MARK REM (SQ FT)
72+55	-	77+34	LT	SOLID	200
72+55	-	77+34	RT	SOLID	200
TOTAL					400

STA	TO	STA	LOC.	LINE TYPE	PAINT PAVT MARK - LINE 5" (FT)
72+50	-	77+50	LT	SOLID	500
72+50	-	77+50	C	SKIP DASH	125
72+50	-	77+50	RT	SOLID	500
TOTAL					1125

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
DISTRICT 6**

EXAMINED January 25th 20 17
John C. Wyman
ENGINEER OF OPERATIONS

EXAMINED January 19 20 17
Ron Chambers
ENGINEER OF PROJECT IMPLEMENTATION

EXAMINED January 24 20 17
Jerry P. Myer
ENGINEER OF PROGRAM DEVELOPMENT

STA	TO	STA	SIDE	BASE COURSE WIDENING 10" (SQ YD)	PAVED SHLDR REM (SQ YD)	EARTH EX WIDENING (CU YD)	AGG WEDGE SHLDR (TON)
72+50	-	73+73	LT	68.3	13.7	15.2	4.7
72+50	-	73+19	RT	38.3	7.7	8.5	2.6
73+59	-	73+73	RT	7.8	1.6	1.7	0.5
73+73	-	74+00	LT	15.0	-	4.2	1.0
73+73	-	74+15	RT	23.3	-	6.5	1.6
76+01	-	76+41	LT	22.2	-	6.2	1.5
76+13	-	76+41	RT	15.6	-	4.3	1.1
76+41	-	77+50	LT	60.6	12.1	13.5	4.1
76+41	-	77+50	RT	60.6	12.1	13.5	4.1
TOTALS				311.7	47.2	73.6	21.2

MIXTURE USE(S)	HMA BASE CSE WIDENING *
AC/PG	PG 64-22
DESIGN AIR VOIDS	4.0 % @ N50
MIX COMPOSITION (GRADATION MIXTURE)	IL 19.0
FRICTION AGGREGATE	N/A
QUALITY MANAGEMENT	QC/QA
SUBLOT SIZE	N/A

* BASE COURSE PAY ITEM ALLOWS FOR HMA OR PCC USE.

001000

6-01016-0000
80% FEDERAL
20% STATE
BRIDGE
0014
RURAL

6-01016-0000
80% FEDERAL
20% STATE
BRIDGE
0014
RURAL

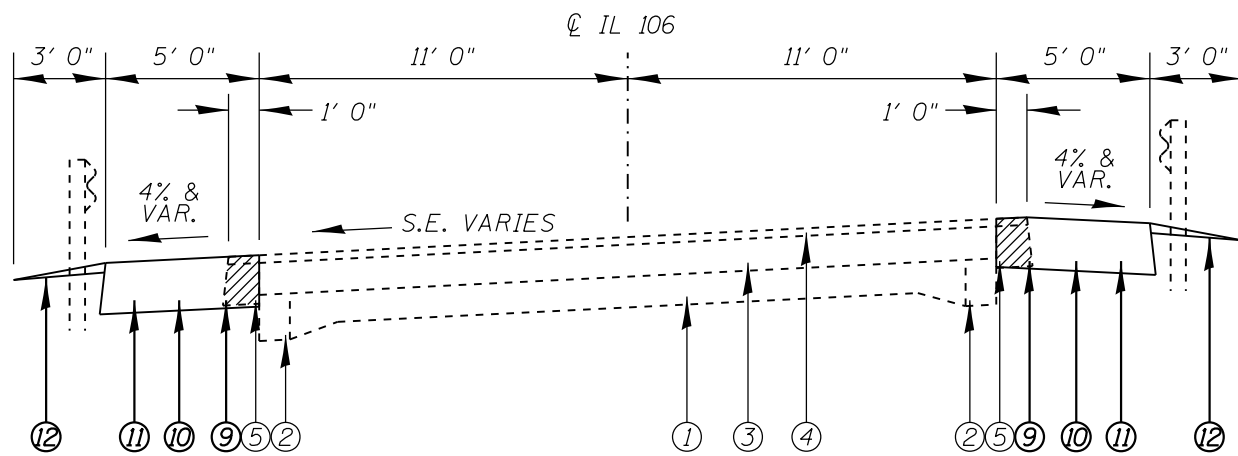
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
20200500	EARTH EXCAVATION (WIDENING)	CU YD	74	74
35650500	BASE COURSE WIDENING 10"	SQ YD	312	312
44004250	PAVED SHOULDER REMOVAL	SQ YD	48	48
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	22	22
50102400	CONCRETE REMOVAL	CU YD	10.5	10.5
50300255	CONCRETE SUPERSTRUCTURE	CU YD	10.5	10.5
50300300	PROTECTIVE COAT	SQ YD	30	30
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1550	1550
50800515	BAR SPLICERS	EACH	24	24
52000110	PREFORMED JOINT STRIP SEAL	FOOT	87	87
67100100	MOBILIZATION	L SUM	1	1
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1
70106700	TEMPORARY RUMBLE STRIPS	EACH	6	6
70400100	TEMPORARY CONCRETE BARRIER	FOOT	480	480
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	480	480
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	4
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
* 78001120	PAINT PAVEMENT MARKING - LINE 5"	FOOT	1125	1125
X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SQ FT	400	400
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1	1
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	16	16
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5")	SQ FT	20	20
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	36	36

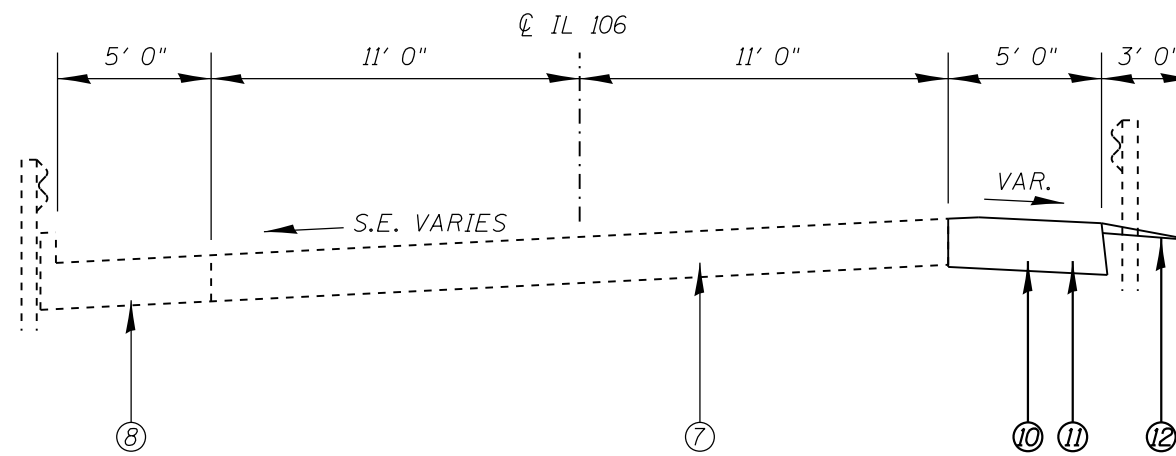
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* SPECIALTY ITEM

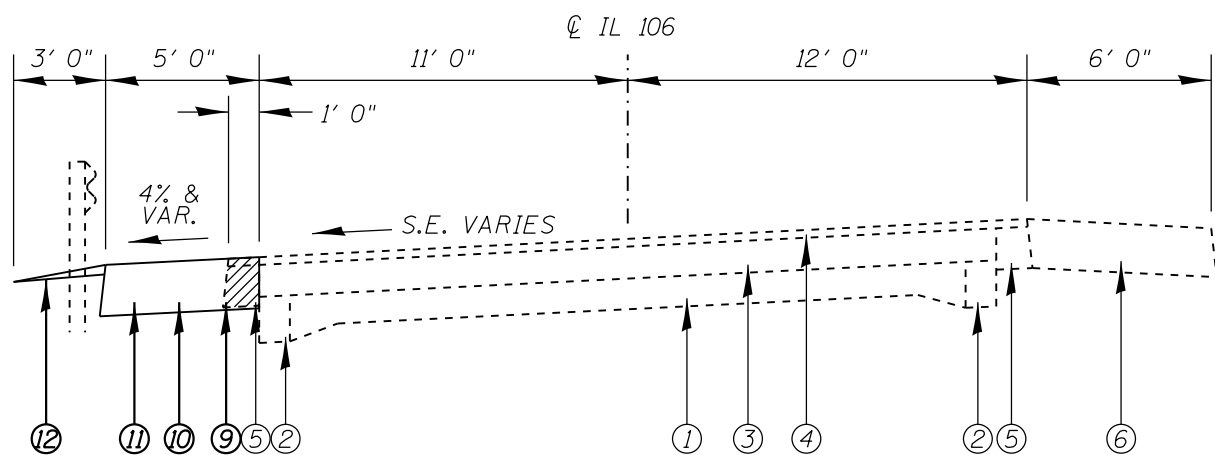
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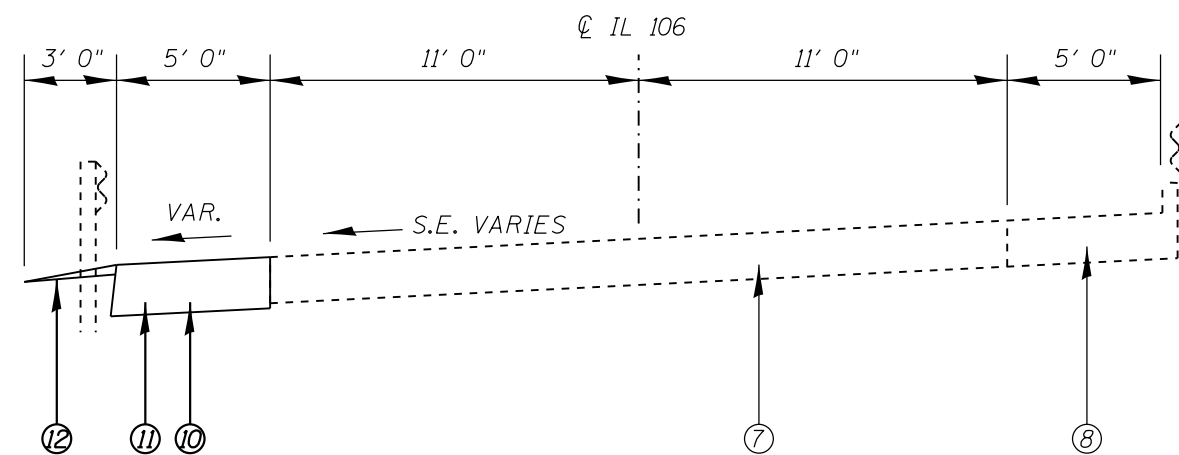
STA 72+50 TO STA 73+19
 STA 73+59 TO STA 73+73
 STA 76+41 TO STA 77+50



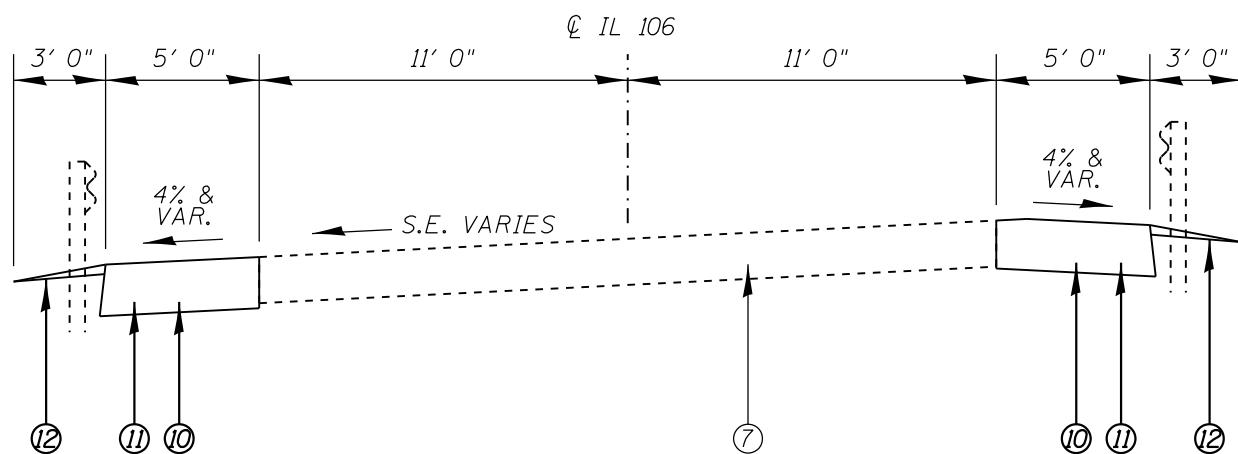
STA 74+00 TO STA 74+15
 NO SHOULDER WORK STA 74+15 TO STA 76+01



STA 73+19 TO STA 73+59



STA 76+01 TO STA 76+13



STA 73+73 TO STA 74+00
 STA 76+13 TO STA 76+41

- ① EXISTING PCC PAVEMENT, 9-6-9
- ② EXISTING HMA BASE COURSE WIDENING, 9"
- ③ EXISTING HMA PAVEMENT, ±6"
- ④ EXISTING HMA SURFACE, 1-1/2"
- ⑤ EXISTING HMA SAFETY SHOULDER, 8"
- ⑥ EXISTING HMA ENTRANCE PAVEMENT
- ⑦ EXISTING PCC BRIDGE APPROACH PAVEMENT, 9"
- ⑧ EXISTING PCC BRIDGE APPROACH SHOULDER, 9"
- ⑨ PROPOSED PAVED SHOULDER REMOVAL
- ⑩ PROPOSED BASE CSE. WIDENING, 10"
- ⑪ PROPOSED EARTH EX. (WIDENING)
- ⑫ PROPOSED AGG. WEDGE SHLDR.

EXIST. CURVE 78
 PI STA. = 78+99.19
 $\Delta = 25^\circ 04' 39''$ (LT)
 $D = 1^\circ 54' 25''$
 $R = 3,004.79'$
 $T = 668.28'$
 $L = 1,315.15'$
 $E = 73.42'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. \text{ RUN} = \text{-----}$
 $P.C. \text{ STA.} = 72+30.91$
 $P.T. \text{ STA.} = 85+46.07$

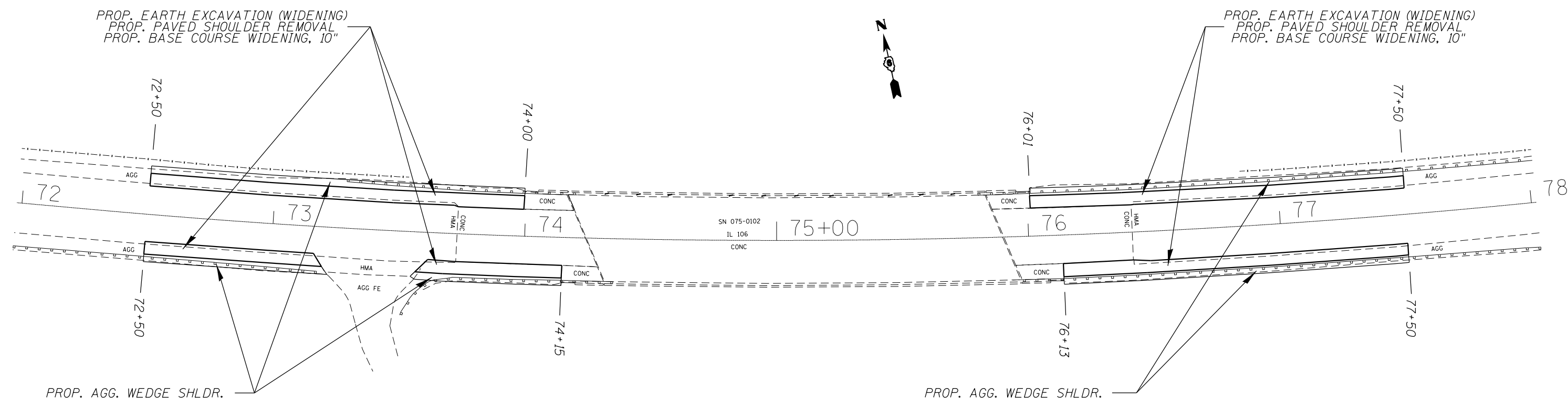
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	PLOT DATE = 1/27/2017	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE: SHEET - OF - SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	4
CONTRACT NO. 72J66				
ILLINOIS FED. AID PROJECT				



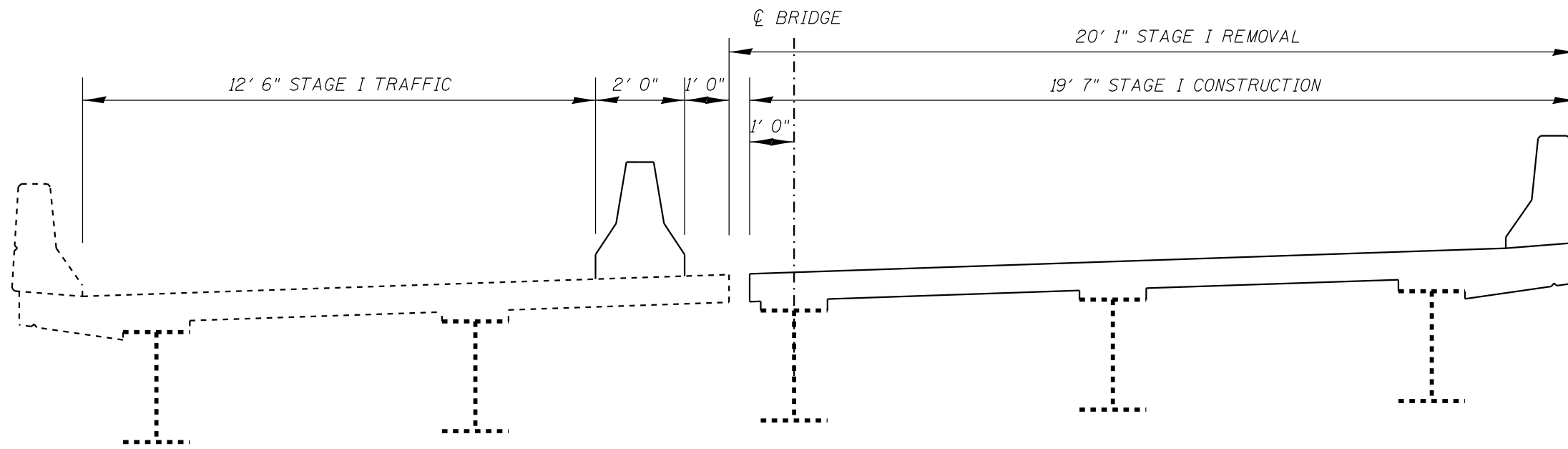
ROADWAY PLAN

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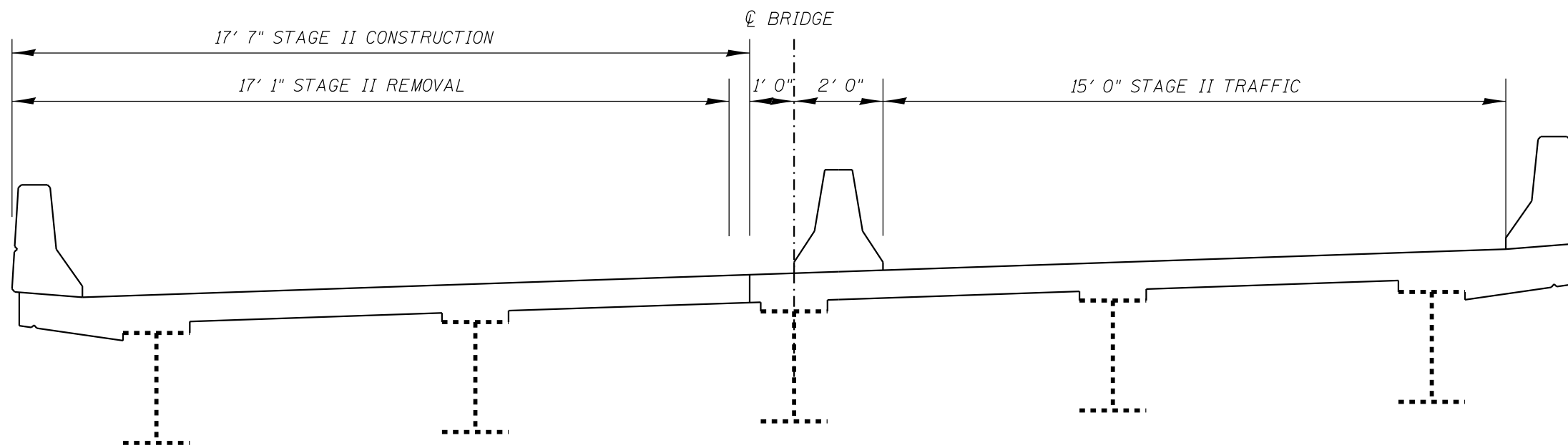
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN			
SCALE:	SHEET -	OF - SHEETS	STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	5
CONTRACT NO. 72J66				
ILLINOIS FED. AID PROJECT				



STAGE I CROSS SECTION (LOOKING EAST)



STAGE II CROSS SECTION (LOOKING EAST)

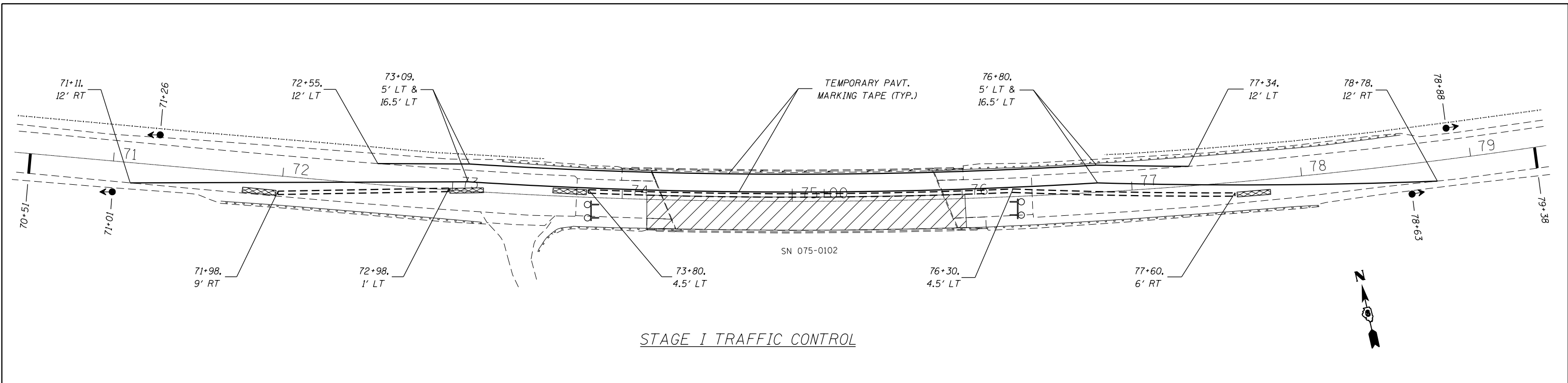
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

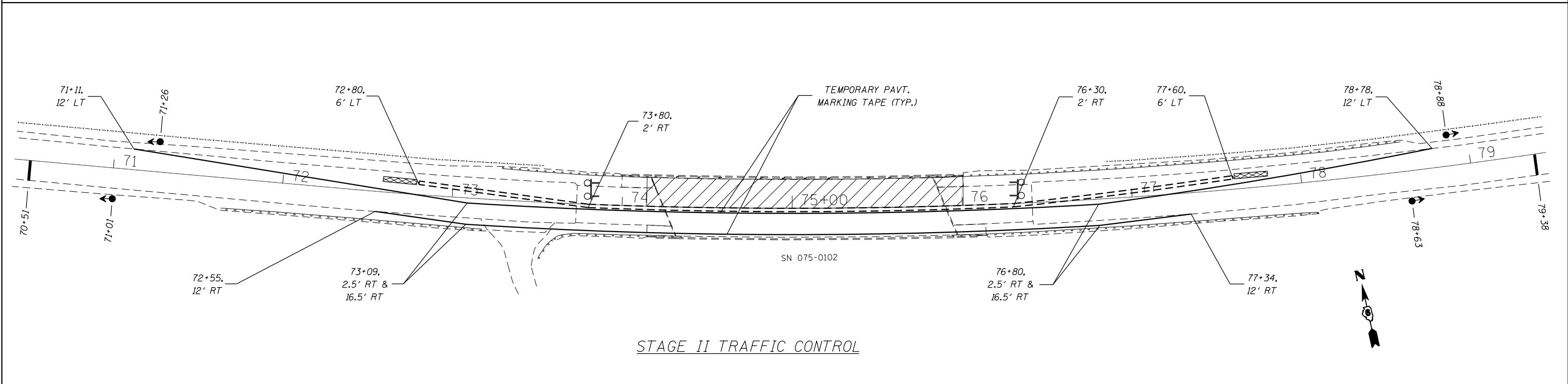
STAGING DETAIL

SCALE: SHEET _ OF _ SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	6
CONTRACT NO. 72J66				
ILLINOIS FED. AID PROJECT				


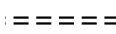
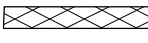




STAGE I TRAFFIC CONTROL



STAGE II TRAFFIC CONTROL

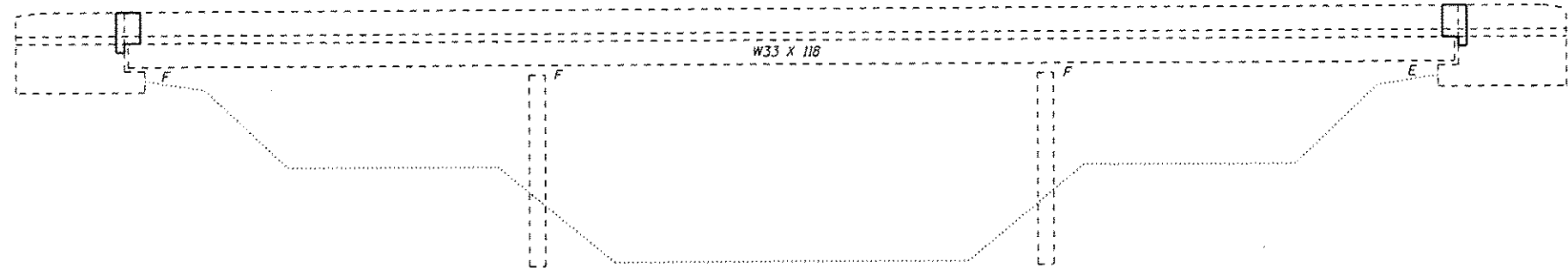
SYMBOLS

-  WORK AREA
-  TEMPORARY CONCRETE BARRIER
-  IMPACT ATTENUATOR
-  TRAFFIC SIGNAL
-  TYPE III BARRICADE WITH FLASHING LIGHTS

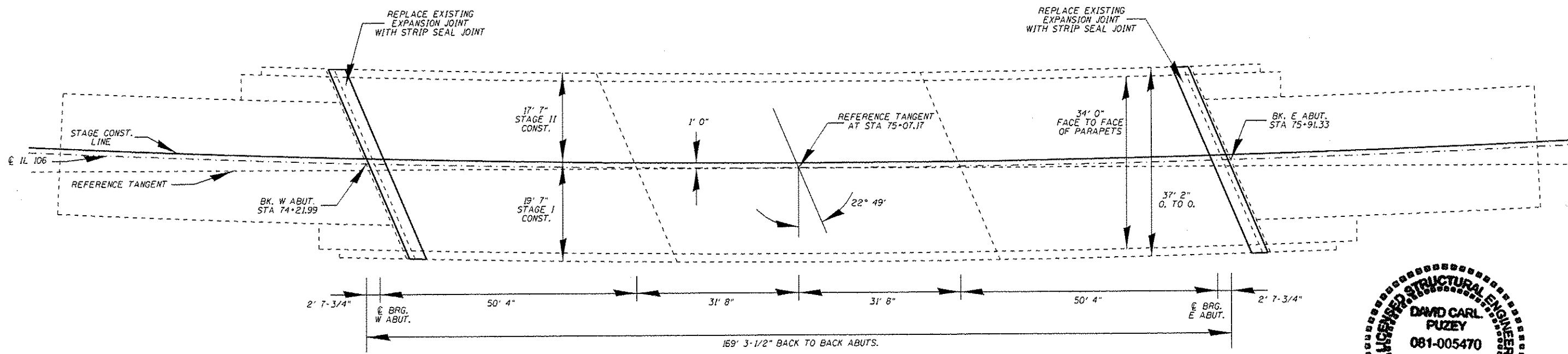
TRAFFIC CONTROL NOTES:

1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701321. DEVICES AND TEMPORARY PAVEMENT MARKINGS SHOWN IN THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, STANDARD 701321. IMPACT ATTENUATORS, TEMPORARY RUMBLE STRIPS, AND TEMPORARY CONCRETE BARRIER SHALL BE PAID SEPARATELY.
2. TEMPORARY CONCRETE BARRIER OFFSETS GIVEN IN THIS DETAIL REFER TO THE EDGE OF THE BARRIER NEAREST TO LIVE TRAFFIC.
3. RUMBLE STRIPS WILL BE REQUIRED AS SHOWN IN STANDARD 701321.

FILE NAME =	USER NAME = dudleybm	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL PLAN			F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		SCALE: SHEET _ OF _ SHEETS STA. TO STA.			CONTRACT NO. 72J66				
	PLOT DATE = 1/27/2017	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



ELEVATION



PLAN



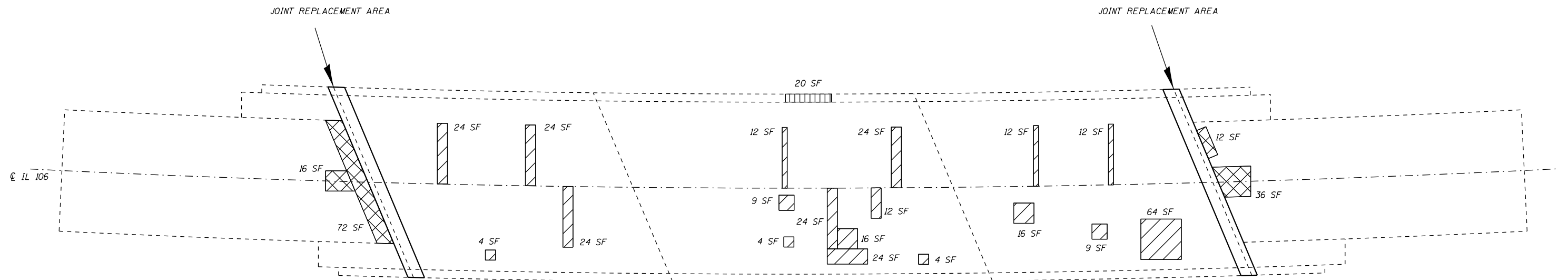
David Carl Puzey 3/10/17
Expires 11/30/18

GENERAL NOTES:

- ALL STRUCTURAL STEEL SHALL BE AASHTO M-270 GRADE 36, UNLESS OTHERWISE NOTED.
- REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
- PRIOR TO POURING THE NEW CONCRETE DECK, ALL HEAVY OR LOOSE MILL SCALE AND OTHER LOOSE OR POTENTIALLY DETRIMENTAL FOREIGN MATERIAL SHALL BE REMOVED FROM THE SURFACES IN CONTACT WITH CONCRETE. TIGHTLY ADHERED PAINT MAY REMAIN UNLESS OTHERWISE NOTED. REMOVAL SHALL BE ACCOMPLISHED BY METHODS THAT WILL NOT DAMAGE THE STEEL AND THE COST WILL BE INCLUDED IN CONCRETE REMOVAL.
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
- EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED, AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.
- JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50°F.
- THE ABUTMENT AND DECK SURFACES IN THE AREAS OF EXPANSION JOINT REPLACEMENT SHALL HAVE A TINED FINISH AS PER ARTICLE 420.09(6)(1) OF THE STANDARD SPECIFICATIONS, COST INCLUDED WITH CONCRETE SUPERSTRUCTURE.

TOTAL BILL OF MATERIAL (075-0102)

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	10.5
Concrete Superstructure	Cu. Yd.	10.5
Reinforcement Bars, Epoxy Coated	Pound	1550
Bar Splicers	Each	24
Preformed Joint Strip Seal	Foot	87
Approach Slab Repair (Partial Depth)	Sq. Yd.	16
Deck Slab Repair (Partial)	Sq. Yd.	36
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	20
Protective Coat	Sq. Yd.	30



PLAN



 PROPOSED DECK SLAB REPAIR AREAS

 PROPOSED APPROACH SLAB REPAIR AREAS

 PROPOSED STRUCTURAL REPAIR OF CONCRETE AREAS

NOTES:

IDENTIFIED PATCHING LOCATIONS ARE ESTIMATED FROM A DECK SURVEY PERFORMED ON 10/28/2016. THE PATCHING SURVEY IS INCLUDED FOR INFORMATION ONLY. THE ENGINEER SHALL LAY OUT ACTUAL PATCH LOCATIONS IN THE FIELD.

THE ENGINEER SHALL SHOW ACTUAL LOCATIONS OF DECK REPAIRS ON AS-BUILT PLANS.

SLAB REPAIR BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	16
Deck Slab Repair (Partial)	Sq. Yd.	36
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	20

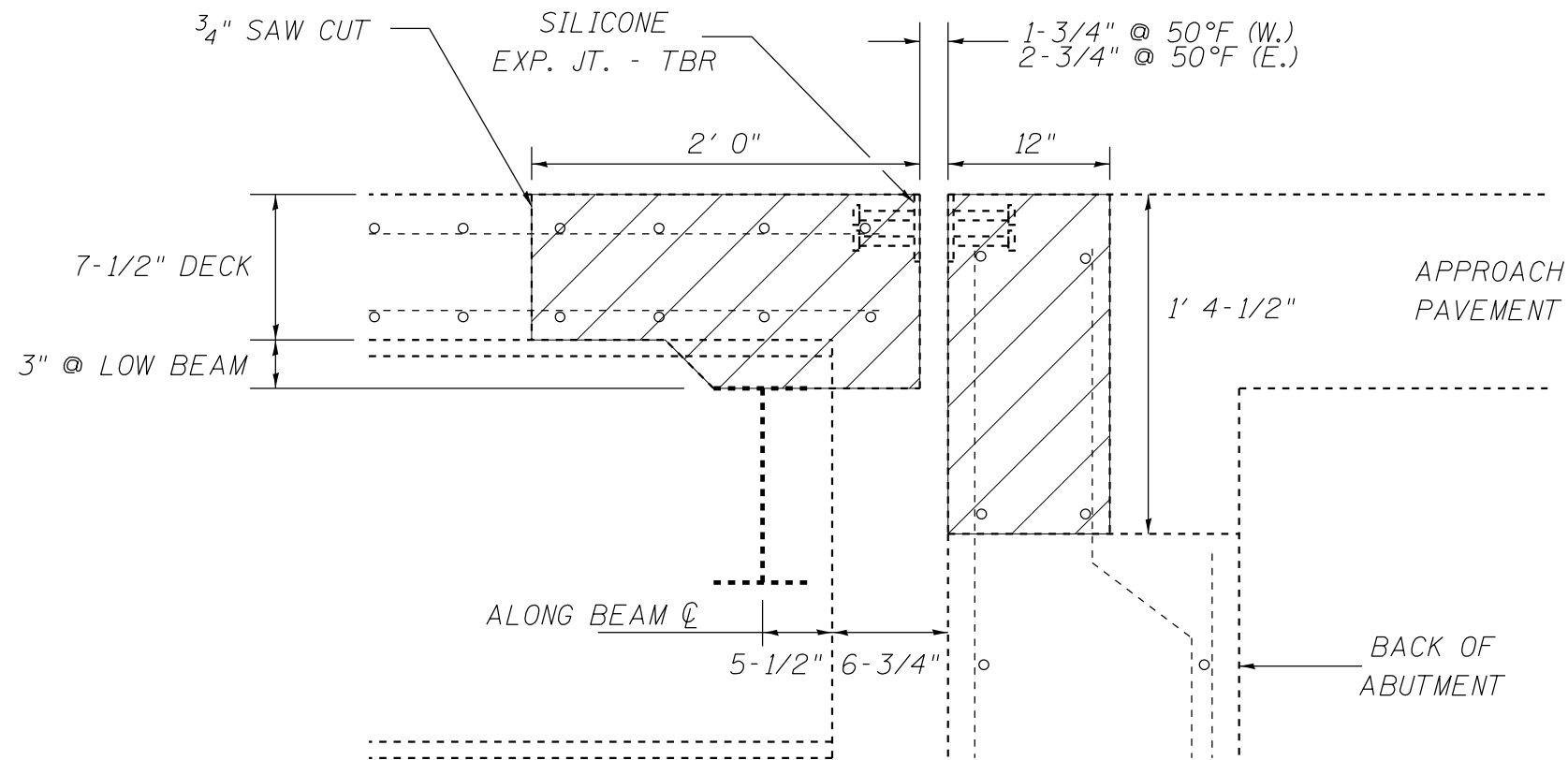
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN
SN 075-0102

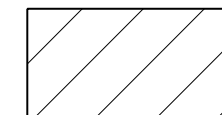
SCALE: SHEET 3 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	10
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72J66	



NOTES:

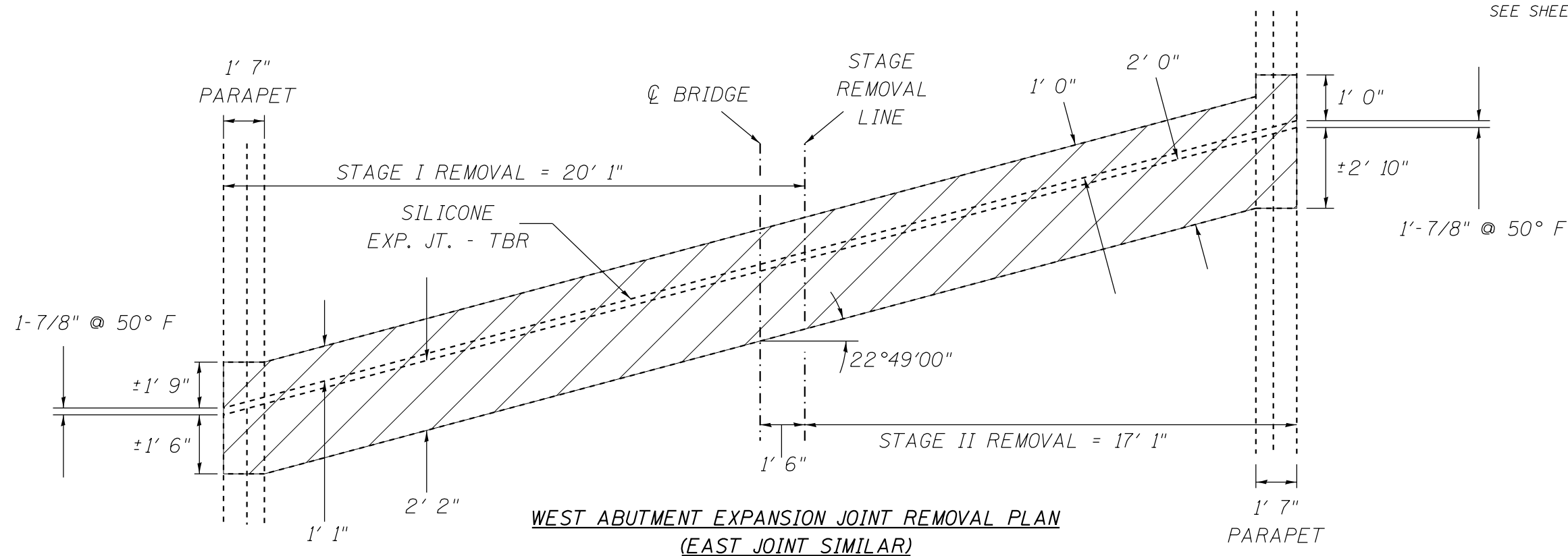
THE EXISTING EXPANSION JOINT SYSTEMS SHALL BE REMOVED COMPLETELY, AS WELL AS ANY FOREIGN MATERIAL THAT HAS ACCUMULATED OR BEEN PLACED IN THE JOINT OPENINGS. THE COST FOR THIS WORK IS INCLUDED IN CONCRETE REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



CONCRETE
REMOVAL AREAS

TYPICAL EXPANSION JOINT CROSS SECTION
(HORIZ. DIM. @ RIGHT ANGLES TO JOINT)

SEE SHEET 5 OF 7 FOR BILL OF MATERIALS



WEST ABUTMENT EXPANSION JOINT REMOVAL PLAN
(EAST JOINT SIMILAR)

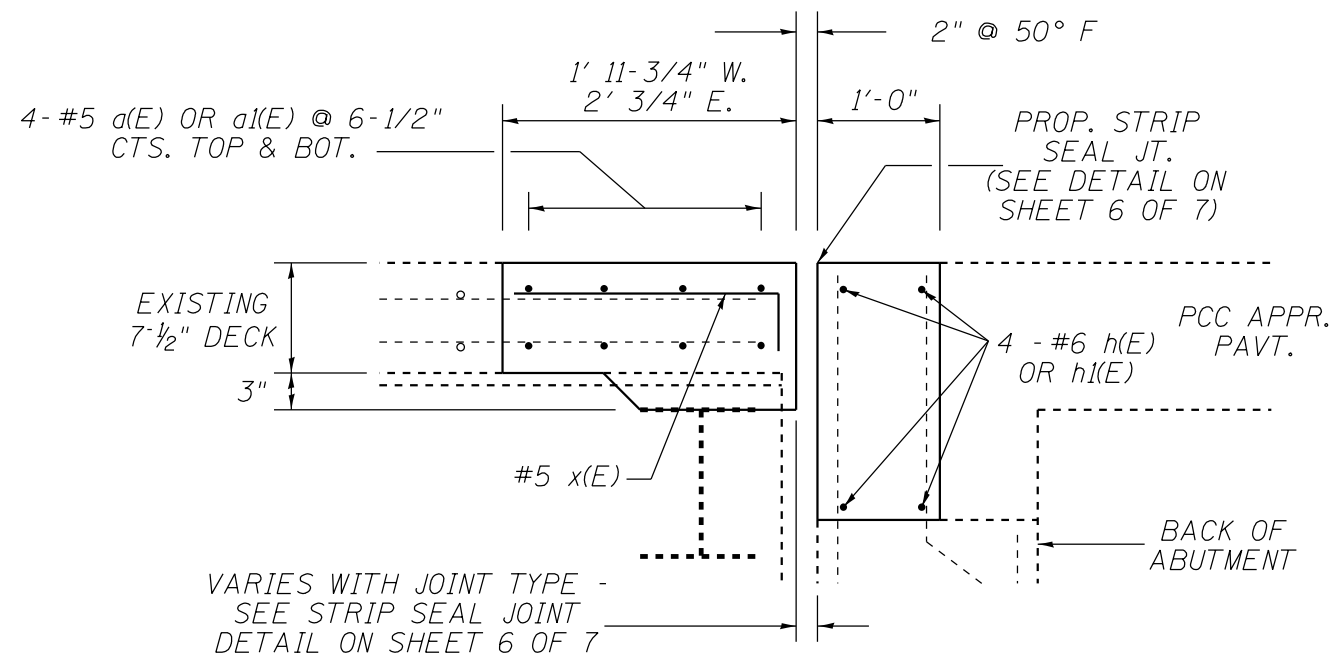
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/27/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

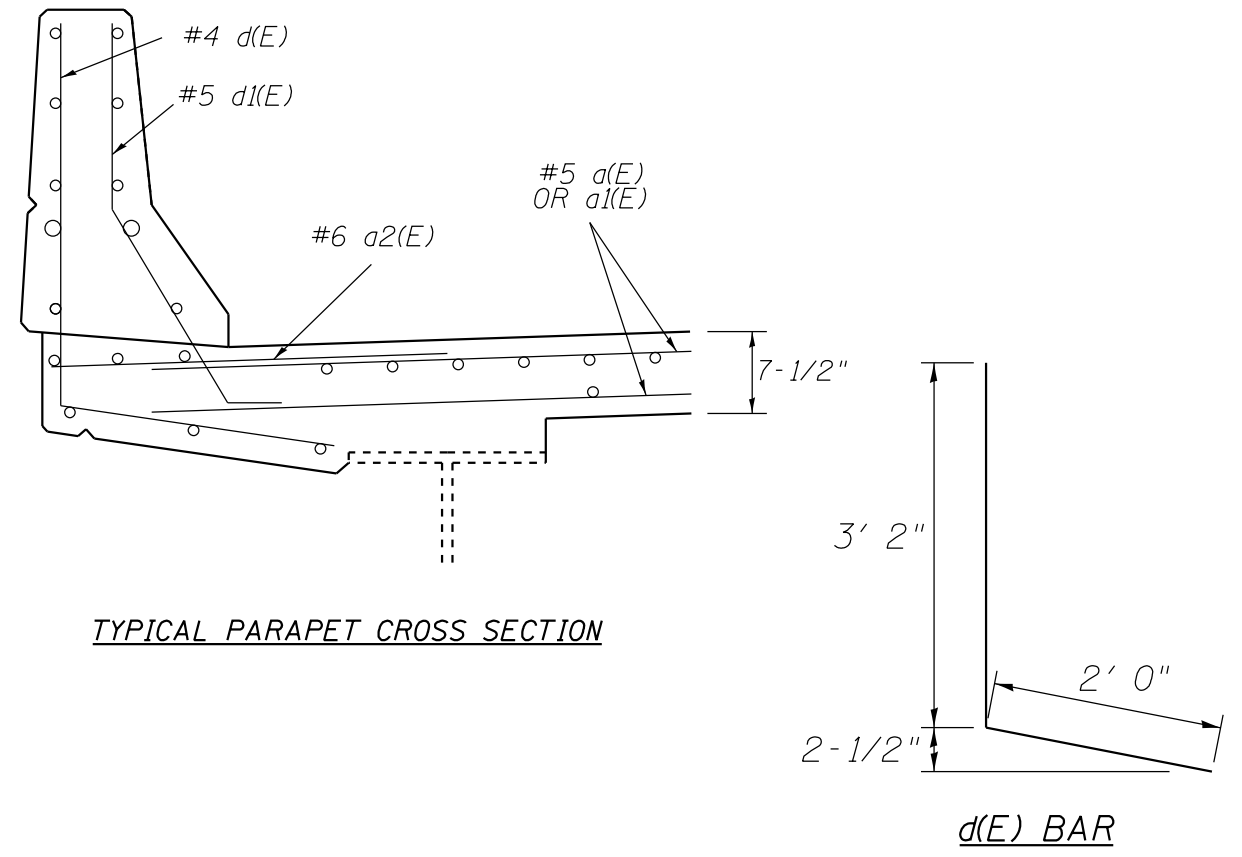
JOINT REMOVAL PLAN
SN 075-0102

SCALE: SHEET 4 OF 7 SHEETS STA. TO STA.

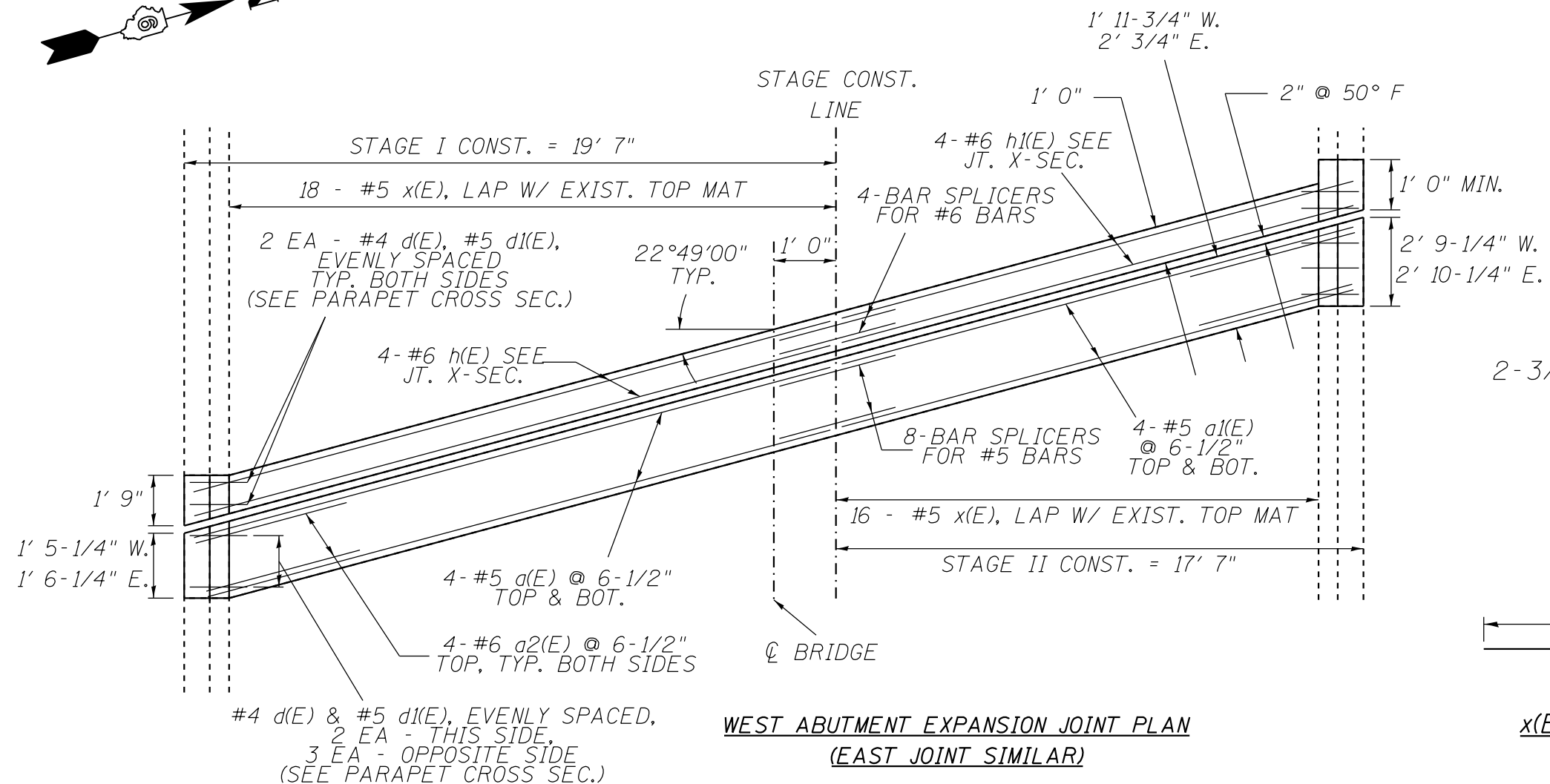
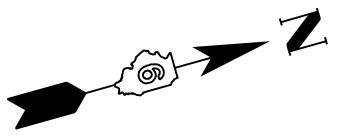
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2600	(15) BDR, BJR	PIKE	14	11
CONTRACT NO. 72J66				
ILLINOIS FED. AID PROJECT				



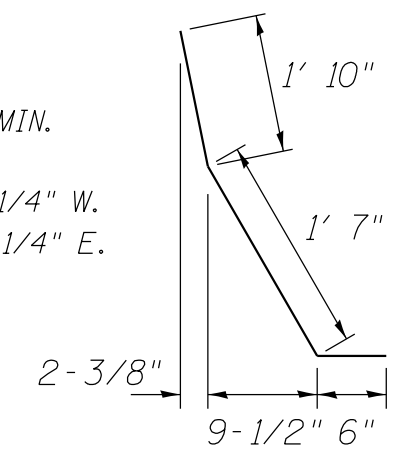
TYPICAL JOINT CROSS SECTION
(HORIZ. DIM. @ RIGHT ANGLES TO JOINT)



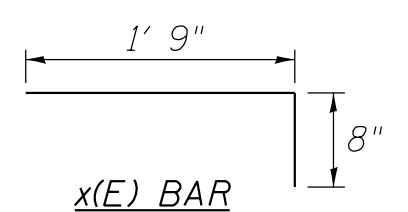
TYPICAL PARAPET CROSS SECTION



WEST ABUTMENT EXPANSION JOINT PLAN
(EAST JOINT SIMILAR)

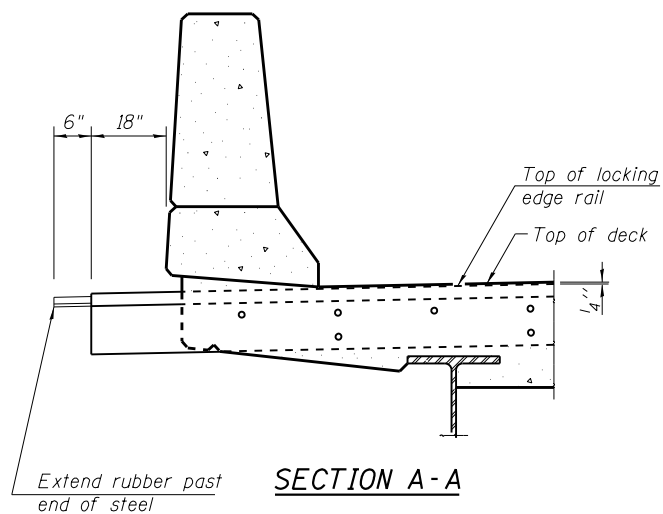
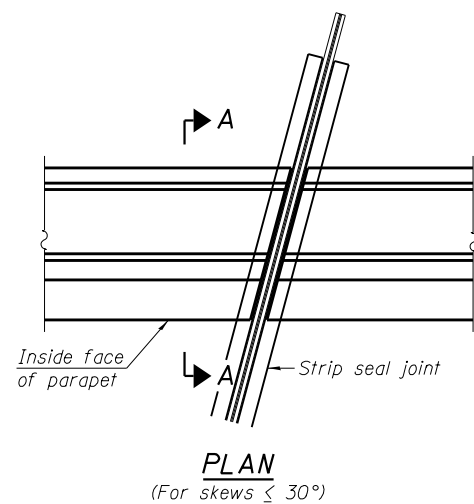


d1(E) BAR



x(E) BAR

BILL OF MATERIAL - 2 JOINTS				
BAR	#	SIZE	LENGTH	SHAPE
a(E)	16	#5	20' 11"	—
a1(E)	16	#5	18' 9"	—
a2(E)	16	#6	4' 0"	—
d(E)	18	#4	5' 2"	L
d1(E)	18	#5	3' 11"	L
h(E)	8	#6	20' 11"	—
h1(E)	8	#6	18' 9"	—
x(E)	68	#5	2' 5"	└
REINFORCEMENT BARS (EPOXY COATED)		POUND	1550	
CONC. SUPERSTRUCTURE		CU YD	10.5	
CONC. REMOVAL		CU YD	10.5	
BAR SPLICERS		EACH	24	



Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

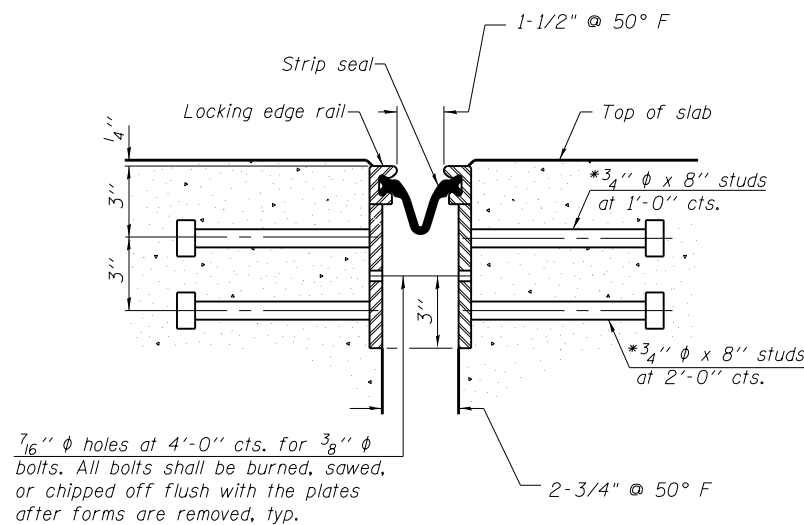
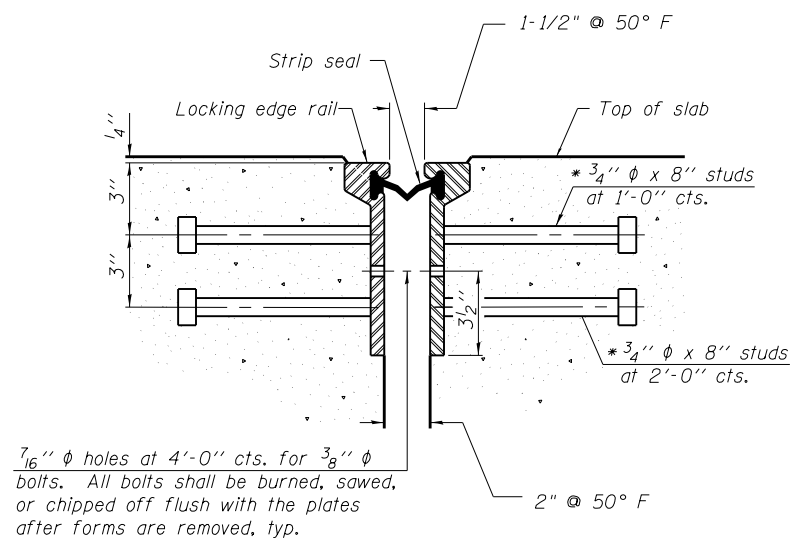
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments shall be $\frac{3}{16}$ ", sealed with a suitable sealant. Joints in rails within 10 ft. of curbs shall be welded.

Parapet plates and anchorage studs for skews $> 30^\circ$ included in the cost of Preformed Joint Strip Seal.



ROLLED EXTRUDED RAIL

WELDED RAIL

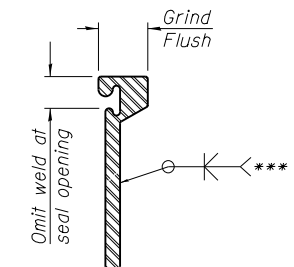
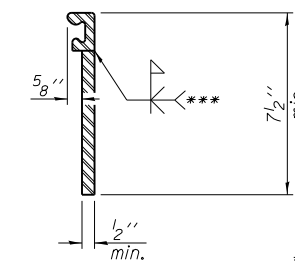
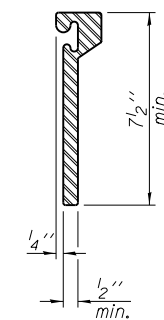
LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

$\frac{7}{16}$ " ϕ holes at 4'-0" cts. for $\frac{3}{8}$ " ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

$\frac{7}{16}$ " ϕ holes at 4'-0" cts. for $\frac{3}{8}$ " ϕ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

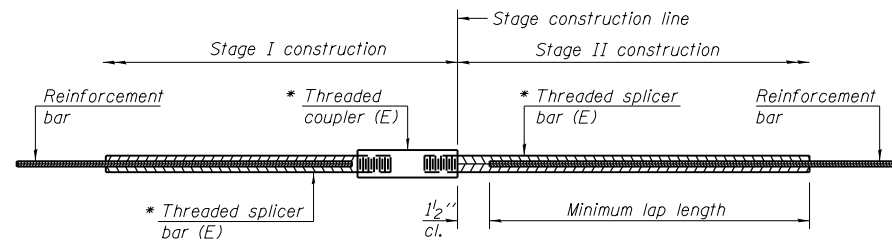


*** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAILS

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	87

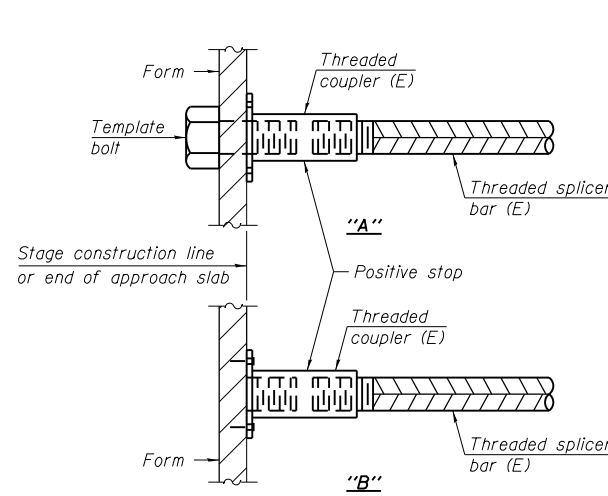


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

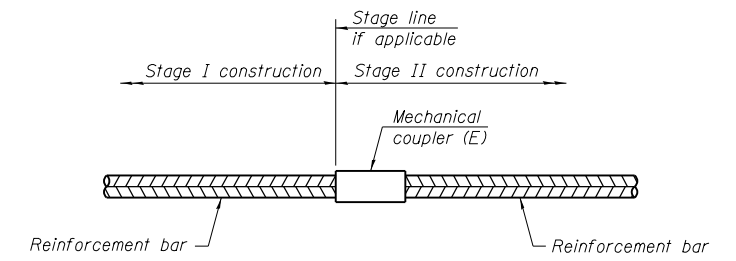
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Joints (Appr. Side)	#6	8	4' 0"
Joints (Deck Side)	#5	16	3' 6"



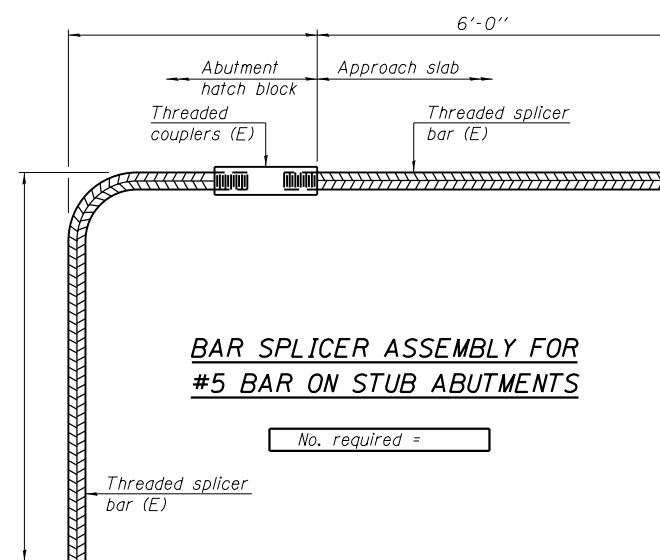
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

6-8-15

FILE NAME =	USER NAME = dudleybm	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/27/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BAR SPLICER DETAIL
SN 075-0102**

SCALE: SHEET 7 OF 7 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2600	(15) BDR, BJR	PIKE	14	14
CONTRACT NO. 72J66				
ILLINOIS FED. AID PROJECT				