

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR LIST OF HIGHWAY STANDARDS SEE SHEET NO. 2

STATE OF ILLINOIS 04-28-2017 LETTING ITEM 178  
DEPARTMENT OF TRANSPORTATION

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	1
FED. ROAD DIST. NO 1 ILLINOIS		CONTRACT NO. 61D81		

HIGHWAY CLASSIFICATION  
STREAMWOOD BOULEVARD – MAJOR COLLECTOR

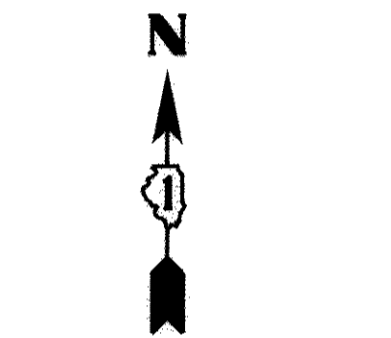
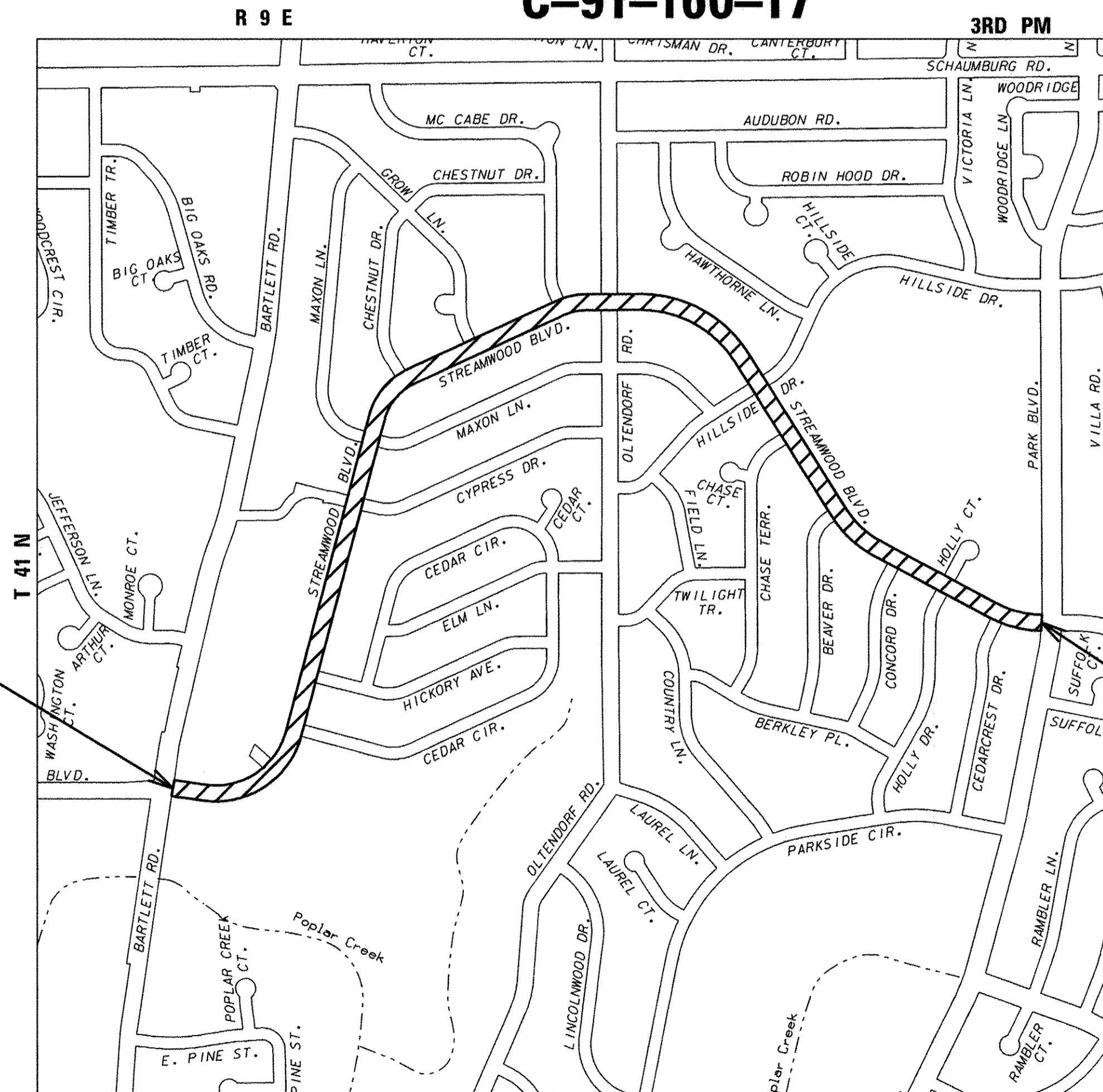
TRAFFIC DATA  
STREAMWOOD BOULEVARD  
2014 ADT = 3,800

POSTED SPEED LIMIT  
STREAMWOOD BOULEVARD = 25 MPH

DESIGN SPEED LIMIT  
STREAMWOOD BOULEVARD = 30 MPH

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

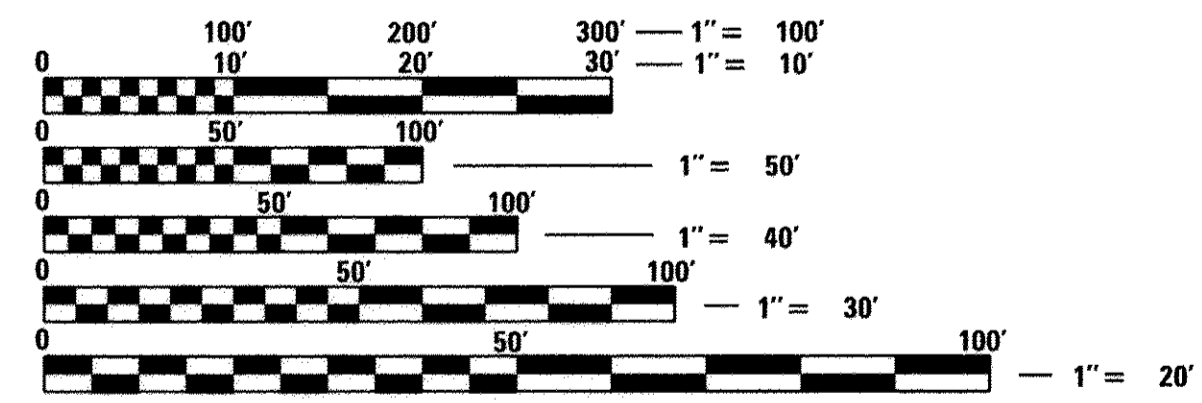
FAU 1661 (STREAMWOOD BOULEVARD)  
FAU 2537 (BARTLETT ROAD) TO FAU 2958 (PARK BOULEVARD)  
RESURFACING  
SECTION: 15-00062-00-RS  
PROJECT NO.: M-4003(878)  
VILLAGE OF STREAMWOOD  
COOK COUNTY  
C-91-160-17



*Jack R. Melhuish*  
JACK R. MELHUISSH, P.E.  
EXPIRES: 11-30-17

PROJECT BEGINS  
STA 100+44

PROJECT ENDS  
STA 165+96



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123 OR 811

DESIGN ENGINEER: J. MELHUISSH  
PROJECT MANAGER: A. CHAUDHRY

CONTRACT NO. 61D81

PROJECT LOCATION MAP  
N.T.S.

HANOVER TOWNSHIP  
PROJECT LENGTH

NET AND GROSS LENGTH OF PROJECT = 6,551 FT. = 1.240 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED 1/30 20 17  
*MMS*  
VILLAGE OF STREAMWOOD

FEBRUARY 6 2017  
*Christopher Hoyer*  
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW February 6 2017  
*Andrew J. O'Connell*  
REGIONAL ENGINEER

420 NORTH FRONT STREET, SUITE 100 | McHENRY, ILLINOIS 60050  
Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com  
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

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OF THE STATE OF ILLINOIS

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

**INDEX OF SHEETS**

1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND DISTRICT ONE DETAILS
3	GENERAL NOTES
4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6 - 8	RESURFACING PLANS
9 - 12	INTERSECTION PLANS
13	VILLAGE OF STREAMWOOD DETAILS
14 - 23	DISTRICT ONE DETAILS

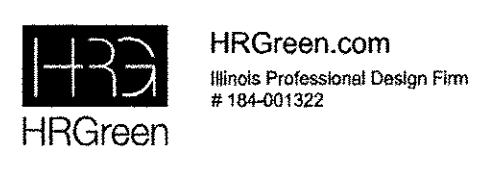
**STATE STANDARDS**

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420001-08	PAVEMENT JOINTS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

**DISTRICT ONE DETAILS**

STANDARD NO.	LIST OF DESCRIPTION
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TS-05 (SHEET 2 OF 7)	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

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 PROJECT CONTACTS: HRGreen  
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PLOT DATE = 2/21/2017	DATE - 2/21/17	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>INDEX OF SHEETS, STATE STANDARDS AND DISTRICT ONE DETAILS</b>			
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	2
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61D81	



**GENERAL NOTES**

- ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, APRIL 1, 2016. ALL WORK TO BE COMPLETED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
- ALL ELEVATIONS SHOWN ON THE PLANS ARE ON THE NAVD 88 DATUM.
- OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
- SAW CUTTING OF PAVEMENTS, CURB AND GUTTER, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING.
- MATERIALS RESULTING FROM THE REMOVAL OF ASPHALT SURFACES, CONCRETE REMOVAL, UTILITY STRUCTURE ADJUSTMENTS, GRADING WORK, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGEMENT OF THE ENGINEER, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE ENGINEER WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR SHALL HAVE THE DOLLAR AMOUNT REDUCED FROM THE NEXT PAY ESTIMATE.
- SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT AS SHOWN ON THE PLANS IS FOR INFORMATIONAL PURPOSES ONLY. ACTUAL LOCATIONS AND QUANTITIES TO BE DETERMINED AND MARKED BY THE ENGINEER PRIOR TO CONSTRUCTION.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
- THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS.
- ALL FRAMES, GRATES, LIDS, AND BOXES SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY AND BE DELIVERED TO THE PUBLIC WORKS DEPARTMENT. THE PHONE NUMBER FOR DELIVERY ARRANGEMENT IS (630)736-3850.
- ALL FRAMES WITH CLOSED LIDS TO BE FURNISHED AS PART OF THE CONTRACT FOR CONSTRUCTION, ADJUSTMENT OR RECONSTRUCTION OF ANY MANHOLE, CATCH BASIN, OR INLET, SHALL HAVE CAST INTO THE LID ONE OF THE FOLLOWING WORDS: ALL LIDS TO BE USED ON STORM SEWER STRUCTURES SHALL BEAR THE WORD "STORM."
- THE INDISCRIMINATE USE OF FIRE HYDRANTS OR EXISTING STREAMS, CREEKS, WETLANDS OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE ENGINEER PRIOR TO USE OF THE WATER.

**SIGNING AND STRIPING**

- SEE IDOT DISTRICT ONE DETAILS AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.

**SEQUENCE OF CONSTRUCTION - ROADWAY**

- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE VILLAGE WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CONCRETE CURB AND GUTTER INSTALLATION OR SIDEWALK CONSTRUCTION. AT LOCATIONS WHERE NEW GUTTER OR SIDEWALK IS TO BE INSTALLED ACROSS A DRIVEWAY, THE CONTRACTOR SHALL CONTACT THE HOMEOWNER 48 HOURS PRIOR TO REMOVING THE PAVEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES, PROVIDED BY THE VILLAGE, TO RESIDENTS. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES (KNOCK ON DOORS WHEN A DRIVEWAY IS ABOUT TO BE CLOSED).
- THE CONTRACTOR SHALL NOT BE ALLOWED TO CLOSE A DRIVEWAY FOR MORE THAN 48 HOURS UNDER ANY CIRCUMSTANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES TO PREVENT TRAFFIC FROM USING DRIVEWAYS DURING THIS PERIOD.

**NATURAL GAS AND PIPELINE COMPANY CONSTRUCTION NOTES**

- CONTRACTORS SHALL BE ADVISED OF NATURAL GAS PIPELINE NGPL OF AMERICA LLC'S (NGPL) REQUIREMENTS AND BE CONTRACTUALLY OBLIGATED TO COMPLY.
- THE CONTINUED INTEGRITY OF NGPL'S PIPELINES AND THE SAFETY OF ALL INDIVIDUALS IN THE AREA OF PROPOSED WORK NEAR NGPL'S FACILITIES ARE OF THE UTMOST IMPORTANCE. THEREFORE, CONTRACTOR MUST MEET WITH NGPL REPRESENTATIVES PRIOR TO CONSTRUCTION TO PROVIDE AND RECEIVE NOTIFICATION LISTINGS FOR APPROPRIATE AREA OPERATIONS AND EMERGENCY PERSONNEL. NGPL'S ON-SITE REPRESENTATIVE WILL REQUIRE DISCONTINUATION OF ANY WORK THAT, IN HIS OPINION, ENDANGERS THE OPERATIONS OR SAFETY OF PERSONNEL, PIPELINES OR FACILITIES.
- THE CONTRACTOR MUST EXPOSE ALL NGPL TRANSMISSION AND DISTRIBUTION LINES PRIOR TO CROSSING TO DETERMINE THE EXACT ALIGNMENT AND DEPTH OF THE LINES. A NGPL REPRESENTATIVE MUST BE PRESENT. IN THE EVENT OF PARALLEL LINES, ONLY ONE PIPELINE CAN BE EXPOSED AT A TIME.
- NGPL WILL NOT ALLOW PIPELINES TO REMAIN EXPOSED OVERNIGHT WITHOUT CONSENT OF NGPL DESIGNATED REPRESENTATIVE. CONTRACTOR MAY BE REQUIRED TO BACKFILL PIPELINES AT THE END OF EACH DAY.
- A NGPL REPRESENTATIVE SHALL DO ALL LINE LOCATING. A NGPL REPRESENTATIVE SHALL BE PRESENT FOR HYDRAULIC EXCAVATION. THE USE OF PROBING RODS FOR PIPELINE LOCATING SHALL BE PERFORMED BY NGPL REPRESENTATIVES ONLY. TO PREVENT UNNECESSARY DAMAGE TO THE PIPELINE COATING.
- NOTIFICATION SHALL BE GIVEN TO NGPL AT LEAST 72 HOURS BEFORE START OF CONSTRUCTION. A SCHEDULE OF ACTIVITIES FOR THE DURATION OF THE PROJECT MUST BE MADE AVAILABLE AT THAT TIME TO FACILITATE THE SCHEDULING OF NGPL'S WORK SITE REPRESENTATIVE. ANY CONTRACTOR SCHEDULE CHANGES SHALL BE PROVIDED TO NGPL IMMEDIATELY.
- HEAVY EQUIPMENT WILL NOT BE ALLOWED TO OPERATE DIRECTLY OVER NGPL PIPELINES OR IN NGPL ROW UNLESS WRITTEN APPROVAL IS OBTAINED FROM NGPL. HEAVY EQUIPMENT SHALL ONLY BE ALLOWED TO CROSS NGPL PIPELINES AT LOCATIONS DESIGNATED BY NGPL. CONTRACTOR SHALL COMPLY WITH ALL PRECAUTIONARY MEASURES REQUIRED BY NGPL TO PROTECT ITS PIPELINES. WHEN INCLEMENT WEATHER EXISTS, PROVISIONS MUST BE MADE TO COMPENSATE FOR SOIL DISPLACEMENT DUE TO SUBSIDENCE OF TIRES.
- EXCAVATING OR GRADING WHICH MIGHT RESULT IN EROSION OR WHICH COULD RENDER THE NGPL ROW INACCESSIBLE SHALL NOT BE PERMITTED UNLESS THE CONTRACTOR/DEVELOPER/OWNER AGREES TO RESTORE THE AREA TO ITS ORIGINAL CONDITION AND PROVIDE PROTECTION TO NGPL'S FACILITY.
- A NGPL REPRESENTATIVE SHALL BE ON-SITE TO MONITOR ANY CONSTRUCTION ACTIVITIES WITHIN 25-FEET OF A NGPL PIPELINE OR ABOVEGROUND APPURTENANCE. THE CONTRACTOR SHALL NOT WORK WITHIN THIS DISTANCE WITHOUT A NGPL REPRESENTATIVE BEING ON SITE. ONLY HAND EXCAVATION SHALL BE PERMITTED WITHIN A MINIMUM OF 18-INCHES (REFER TO STATE SPECIFIC RULES/REGULATIONS REGARDING ANY ADDITIONAL CLEARANCE REQUIREMENTS) OF NGPL PIPELINES, VALVES AND FITTINGS. HOWEVER, PROCEED WITH EXTREME CAUTION WHEN WITHIN THREE (3) FEET OF THE PIPE.
- RIPPING IS ONLY ALLOWED WHEN THE POSITION OF THE PIPE IS KNOWN AND NOT WITHIN 10-FEET OF NGPL FACILITY UNLESS NGPL REPRESENTATIVE IS PRESENT.
- TEMPORARY SUPPORT OF ANY EXPOSED NGPL PIPELINE BY CONTRACTOR MAY BE NECESSARY IF REQUIRED BY NGPL'S ON-SITE REPRESENTATIVE. BACKFILL BELOW THE EXPOSED LINES AND 12-INCHES ABOVE THE LINES SHALL BE REPLACED WITH SAND OR OTHER SELECTED MATERIAL AS APPROVED BY NGPL'S ON-SITE REPRESENTATIVE AND THOROUGHLY COMPACTED IN 12-INCHES LIFTS TO 95% OF STANDARD PROCTOR DRY DENSITY MINIMUM OR AS APPROVED BY NGPL'S ON-SITE REPRESENTATIVE. THIS IS TO ADEQUATELY PROTECT AGAINST STRESSES THAT MAY BE CAUSED BY THE SETTLING OF THE PIPELINE.
- NO BLASTING SHALL BE ALLOWED WITHIN 1000-FEET OF NGPL'S FACILITIES UNLESS BLASTING NOTIFICATION IS GIVEN TO NGPL INCLUDING COMPLETE BLASTING PLAN DATA. A PRE-BLAST MEETING SHALL BE CONDUCTED BY THE ORGANIZATION RESPONSIBLE FOR BLASTING.  
  
NGPL SHALL BE INDEMNIFIED AND HELD HARMLESS FROM ANY LOSS, COST OF LIABILITY FOR PERSONAL INJURIES RECEIVED, DEATH CAUSED OR PROPERTY DAMAGE SUFFERED OR SUSTAINED BY ANY PERSON RESULTING FROM ANY BLASTING OPERATIONS UNDERTAKEN WITHIN 500-FEET OF ITS FACILITIES. THE ORGANIZATION RESPONSIBLE FOR BLASTING SHALL BE LIABLE FOR ANY AND ALL DAMAGES CAUSED TO NGPL'S FACILITIES AS A RESULT OF THEIR ACTIVITIES WHETHER OR NOT NGPL REPRESENTATIVES ARE PRESENT. NGPL SHALL HAVE A SIGNED AND EXECUTED BLASTING INDEMNIFICATION AGREEMENT BEFORE AUTHORIZED PERMISSION TO BLAST CAN BE GIVEN.  
  
NO BLASTING SHALL BE ALLOWED WITHIN 300-FEET OF NGPL'S FACILITIES UNLESS BLASTING NOTIFICATION IS GIVEN TO NGPL A MINIMUM OF ONE WEEK BEFORE BLASTING. (NOTE: COVERED ABOVE) NGPL SHALL REVIEW AND ANALYZE THE BLASTING METHODS. A WRITTEN BLASTING PLAN SHALL BE PROVIDED BY THE ORGANIZATION RESPONSIBLE FOR BLASTING AND AGREED TO IN WRITING BY NGPL IN ADDITION TO MEETING REQUIREMENTS FOR 500-FEET AND 1000-FEET BEING MET ABOVE. A WRITTEN EMERGENCY PLAN SHALL BE PROVIDED BY THE ORGANIZATION RESPONSIBLE FOR BLASTING. (NOTE: COVERED ABOVE)
- ANY CONTACT WITH ANY NGPL FACILITY, PIPELINE, VALVE SET, ETC. SHALL BE REPORTED IMMEDIATELY TO NGPL. IF REPAIRS TO THE PIPE ARE NECESSARY, THEY WILL BE MADE AND INSPECTED BEFORE THE SECTION IS RE-COATED AND THE LINE IS BACK-FILLED.
- NGPL PERSONNEL SHALL INSTALL ALL TEST LEADS ON NGPL FACILITIES.
- BURNING OF TRASH, BRUSH, ETC. IS NOT PERMITTED WITHIN THE NGPL ROW.

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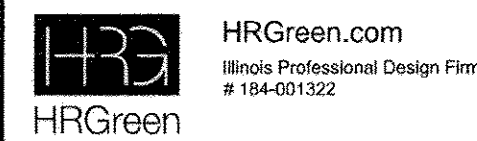
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	3
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				CONTRACT NO. 61D81

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HRGreen.com  
 Illinois Professional Design Firm  
 #184-001322

\* SPECIAL PROVISION  
 † SPECIALTY ITEMS

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80% FED 20% LOCAL 0005
20200100	EARTH EXCAVATION	CU YD	20	20
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	40	40
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	150	150
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	2	2
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	2	2
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	2	2
25200110	SODDING, SALT TOLERANT	SO YD	150	150
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SO YD	72	72
35501309	HOT-MIX ASPHALT BASE COURSE, 6 1/4"	SO YD	64	64
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	22,465	22,465
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	50	50
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,438	1,438
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	346	346
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3,733	3,733
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SO YD	50	50
* 42400800	DETECTABLE WARNINGS	SO FT	216	216
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	33,281	33,281
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	50	50
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	134	134
44000600	SIDEWALK REMOVAL	SO FT	2,500	2,500
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SO YD	150	150
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	175	175
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	225	225
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	250	250
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	45	45
60406100	FRAMES AND LIDS, TYPE I, CLOSED LID	EACH	10	10
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	39	39
67100100	MOBILIZATION	LSUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	271	271
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	91	91
† 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	63	63
† 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11,262	11,262
† 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,235	1,235
† 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	114	114
† 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	295	295
† * 88600100	DETECTOR LOOP, TYPE I	FOOT	45	45
* X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1
* X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	5	5
* X0326806	WASHOUT BASIN	LSUM	1	1
* X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SO FT	2,300	2,300
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	15	15
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1,500	1,500
* Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52

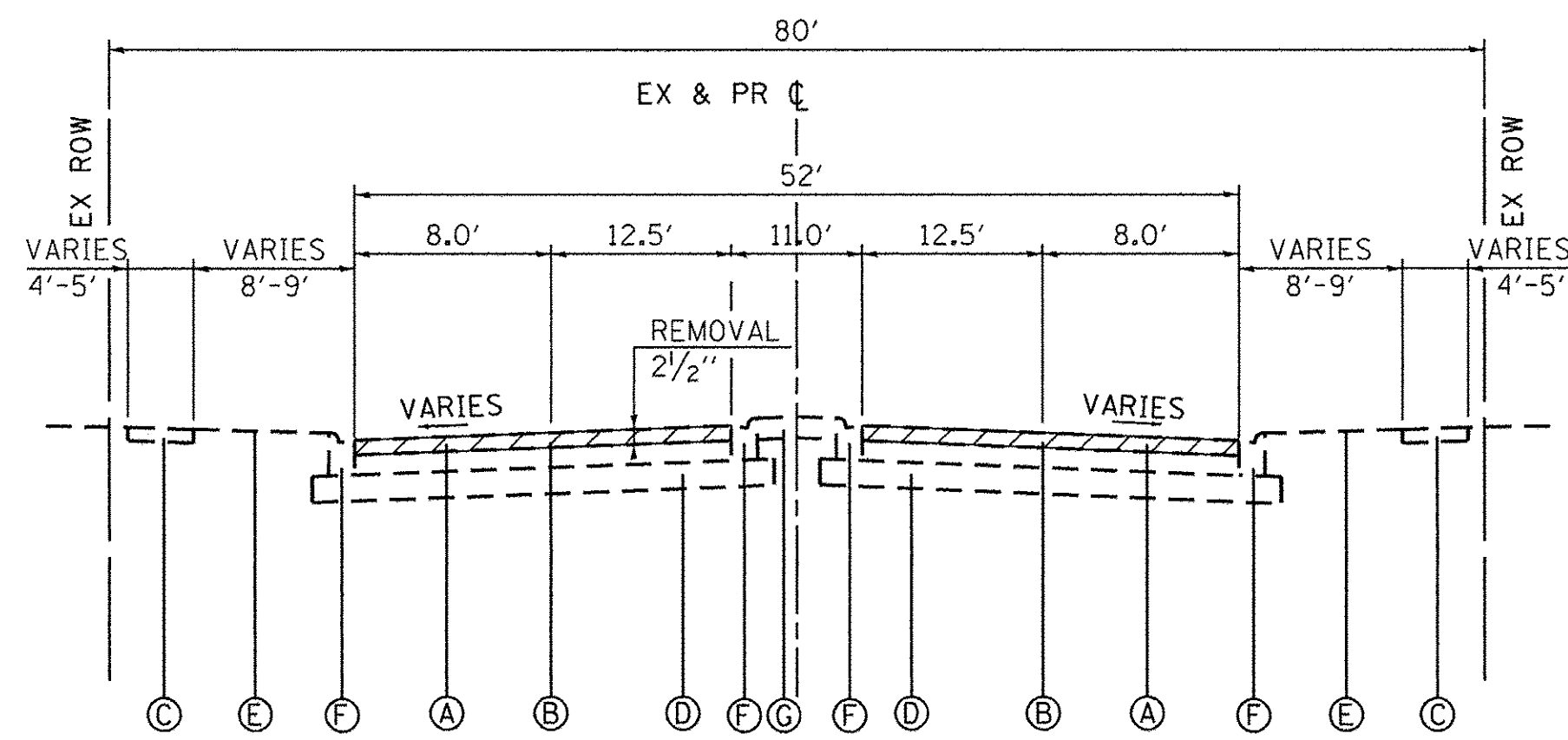
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION


SUMMARY OF QUANTITIES  
 STREAMWOOD BOULEVARD

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	4
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				CONTRACT NO. 61081

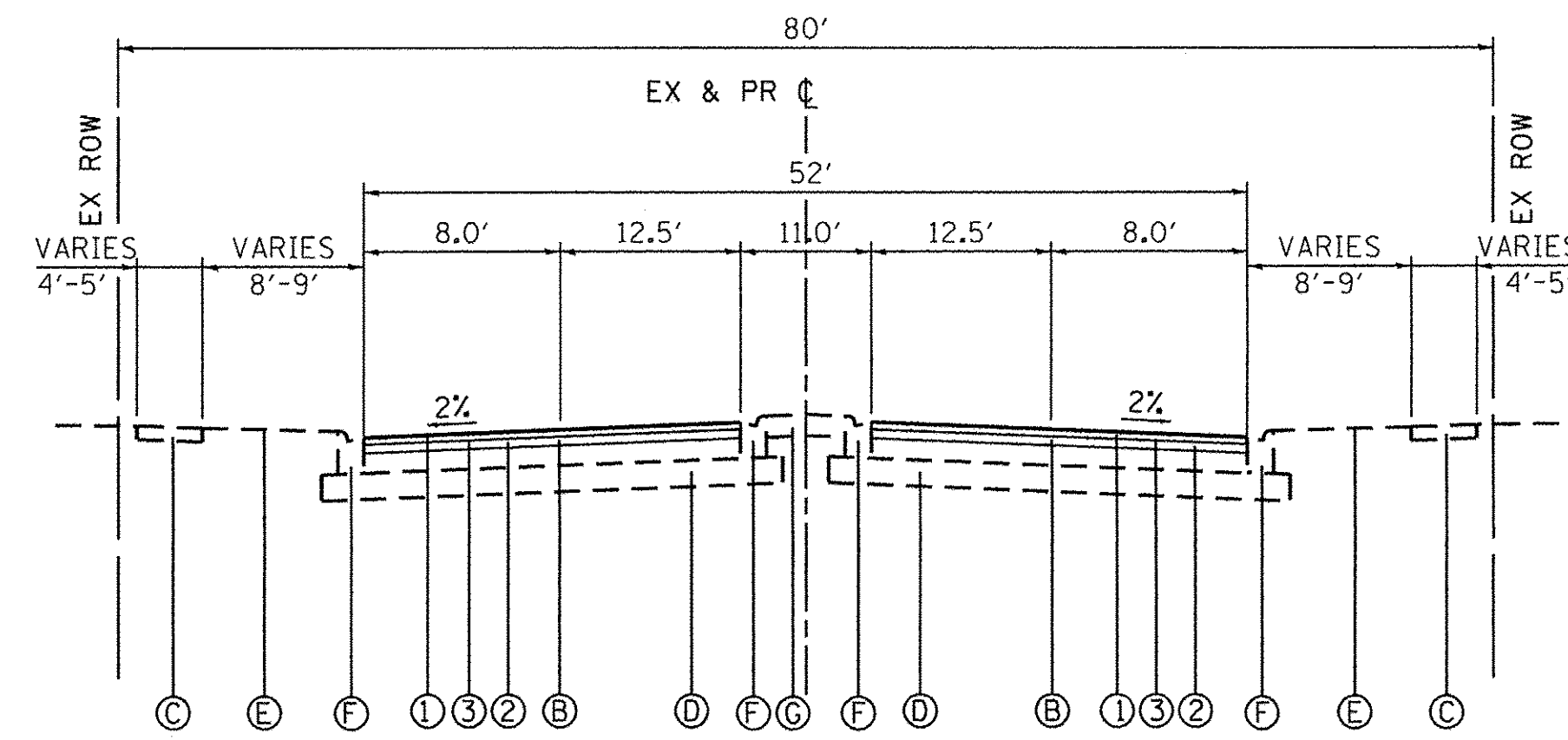




 DENOTES AREA OF HOT-MIX ASPHALT SURFACE REMOVAL

**EXISTING TYPICAL SECTION**

STREAMWOOD BOULEVARD  
BARTLETT ROAD TO PARK BOULEVARD  
STA 100+44 TO STA 165+96



**PROPOSED TYPICAL SECTION**

STREAMWOOD BOULEVARD  
BARTLETT ROAD TO PARK BOULEVARD  
STA 100+44 TO STA 165+96

**EXISTING LEGEND**

- (A) HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- (B) HOT-MIX ASPHALT PAVEMENT, (12" & VARIES)
- (C) PORTLAND CEMENT CONCRETE SIDEWALK (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- (D) AGGREGATE SUBBASE, (4"-6")
- (E) EXISTING GROUND
- (F) COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12 (SPOT REMOVAL & REPLACEMENT AS DIRECTED BY THE ENGINEER)
- (G) LANDSCAPED MEDIAN

**PROPOSED LEGEND**

- ① HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; 2"
- ② POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; 3/4"
- ③ BITUMINOUS MATERIALS (TACK COAT)

NOTES: THE PATCHING QUANTITIES HAVE BEEN ESTIMATED BASED ON FIELD OBSERVATION DURING DESIGN. ACTUAL QUANTITIES TO BE DETERMINED DURING CONSTRUCTION BY THE ENGINEER.

FULL DEPTH (MEDIAN REPLACEMENT) PAVEMENT SHALL BE PLACE OVER AGGREGATE BASE COURSE, TYPE B 4"

MEDIAN REMOVAL IS PAID FOR AS EARTH EXCAVATION AND COMBINATION CURB AND GUTTER REMOVAL.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
<b>PAVEMENT RESURFACING</b>	
HMA SURFACE COURSE, MIX "D", N50; 2"	4.0% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; 3/4"	3.5% @ 50 GYR.
<b>FULL DEPTH (MEDIAN REPLACEMENT)</b>	
HMA SURFACE COURSE, MIX "D", N50; 2"	4.0% @ 50 GYR.
POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; 3/4"	3.5% @ 50 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19MM), 6 1/4"	4.0% @ 50 GYR.
<b>PATCHING (see note)</b>	
CLASS D PATCHES, (HMA BINDER, IL-19mm)	4.0% @ 70 GYR.

NOTES:

1. THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUATITIES IS 112 LBS/SO YD/IN
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIAL SEE SPECIAL PROVISIONS UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

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PROJECT CONTACTS: HRGreen.com  
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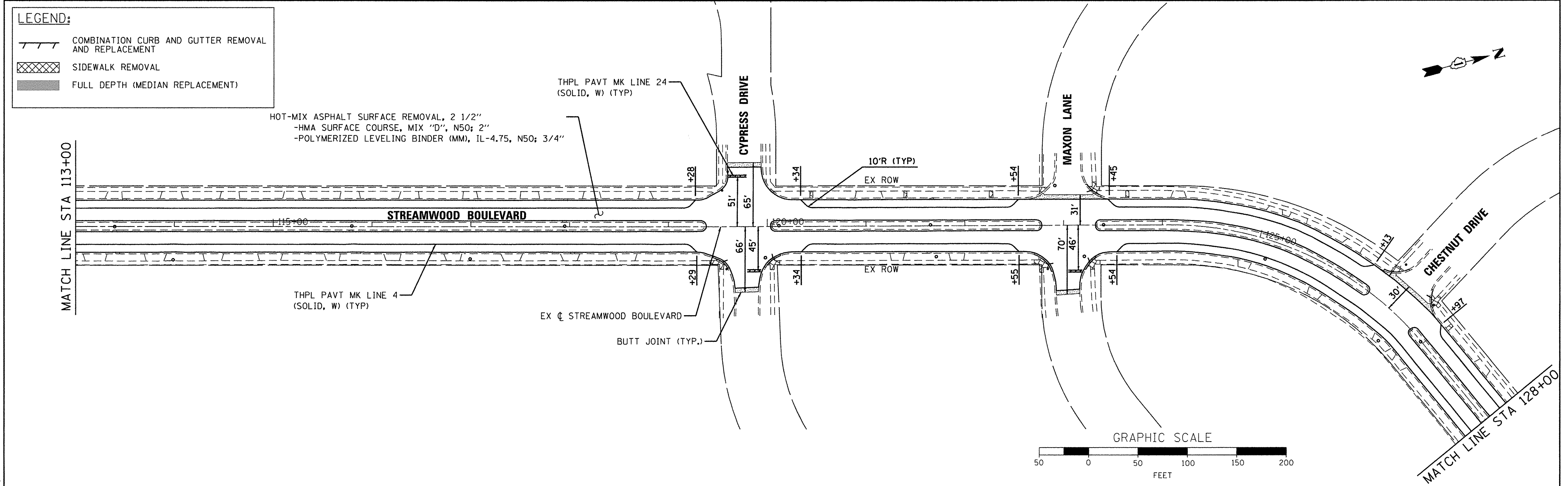
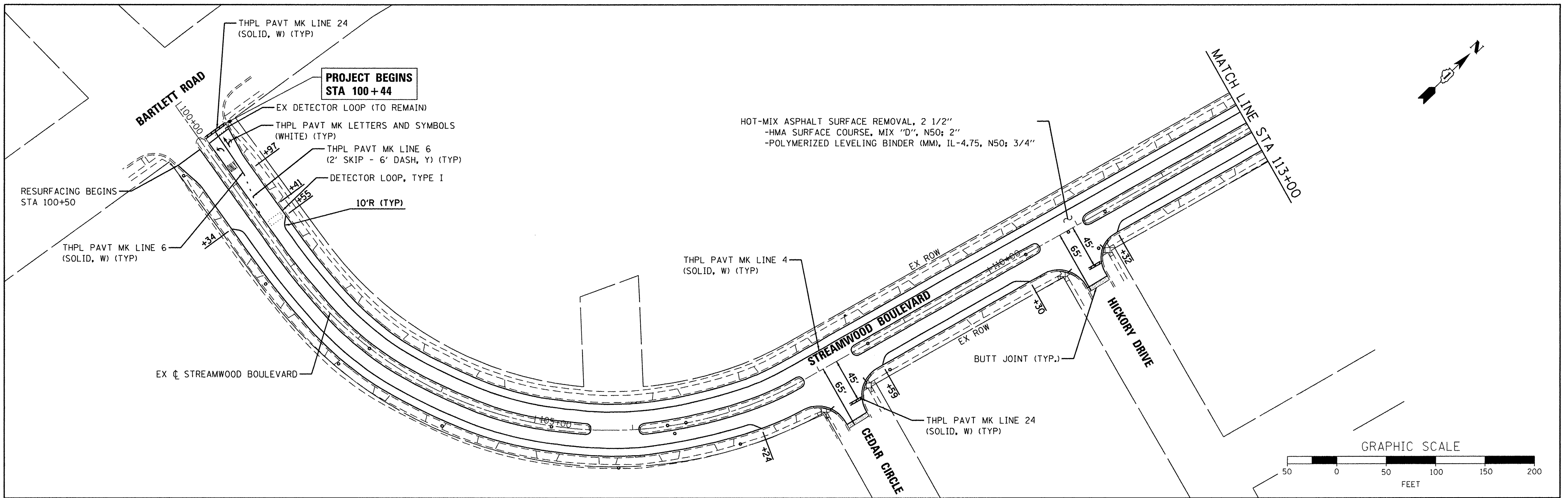


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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS STREAMWOOD BOULEVARD	
SCALE: N.T.S.	SHEET NO. 1 OF 1 SHEETS
STA. TO STA.	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	5
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61D81	



**LEGEND:**

	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT
	SIDEWALK REMOVAL
	FULL DEPTH (MEDIAN REPLACEMENT)

COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen  
 CLIENT: HRGreen  
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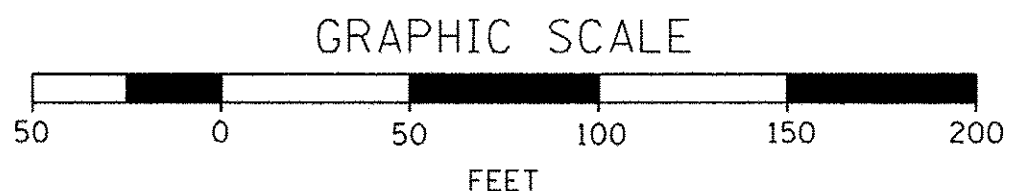
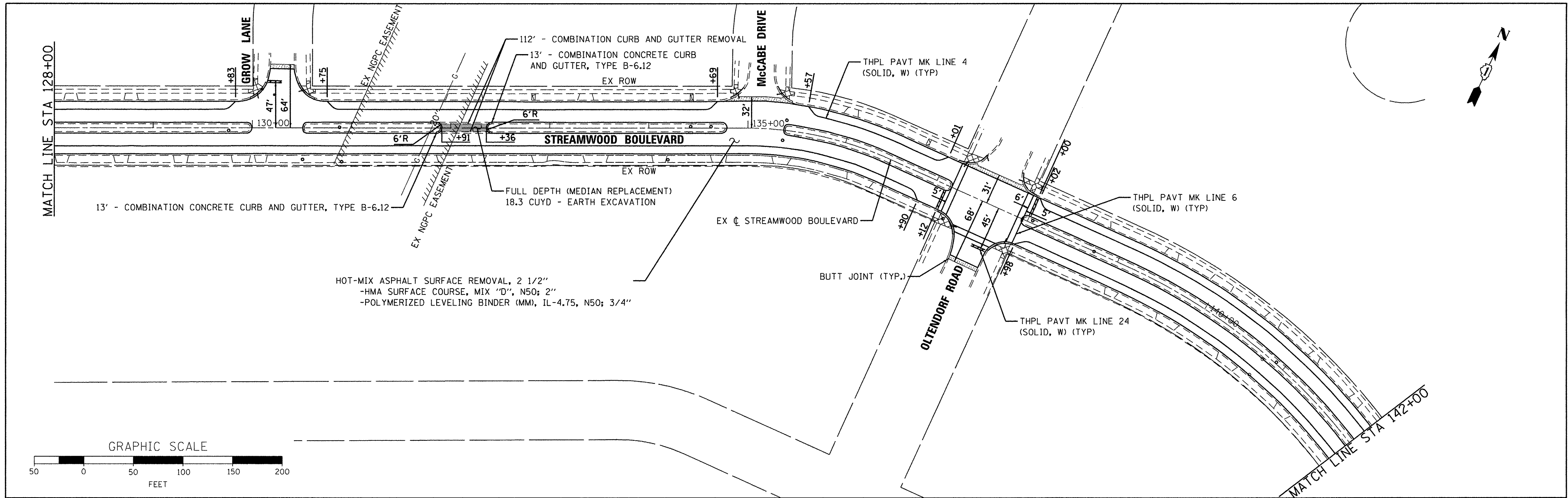
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLAN  
 STREAMWOOD BOULEVARD**

SCALE: 1" = 50' SHEET NO. 1 OF 3 SHEETS STA. 100+44 TO STA. 128+00

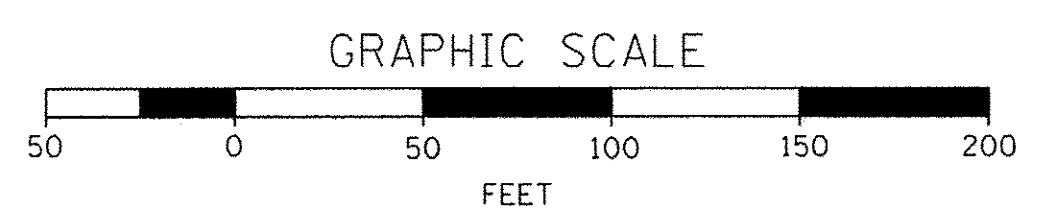
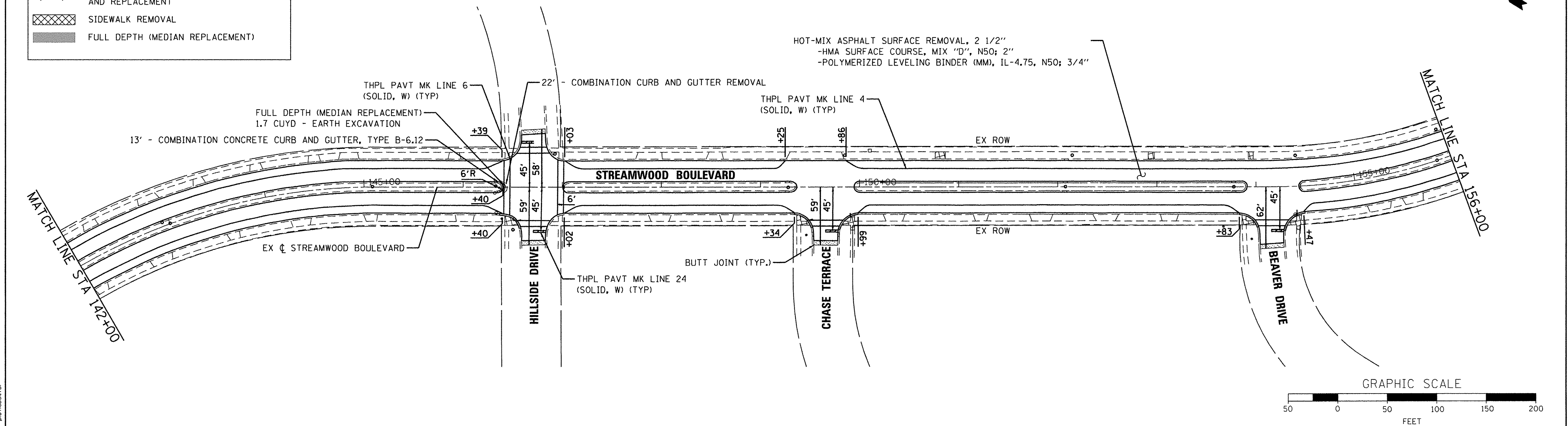
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1661	15-00062-00-RS	COOK	23	6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61D81	





**LEGEND:**

	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT
	SIDEWALK REMOVAL
	FULL DEPTH (MEDIAN REPLACEMENT)



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: #CL1876  
 CLIENT: #CL1876  
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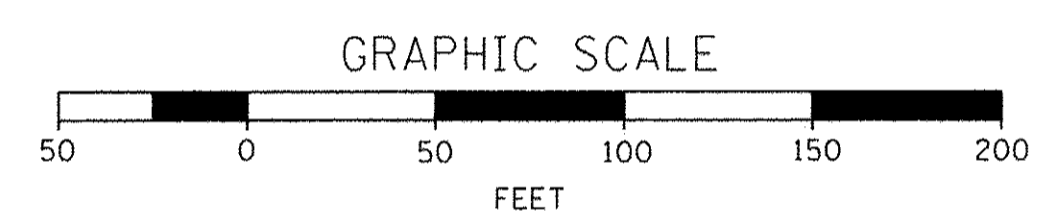
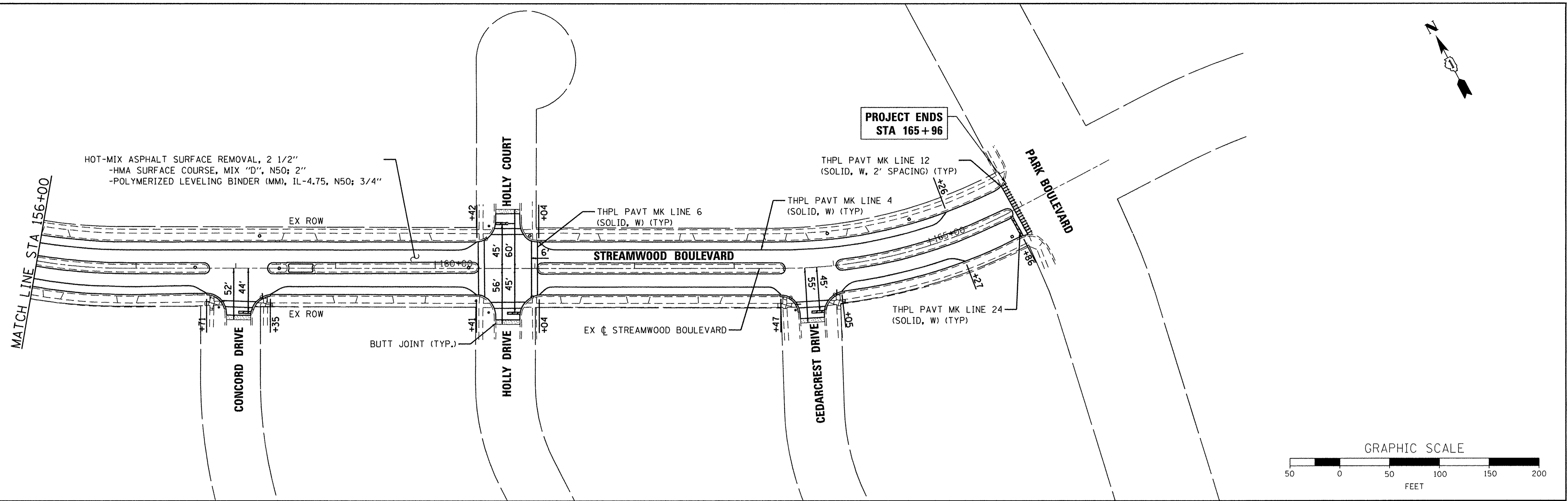
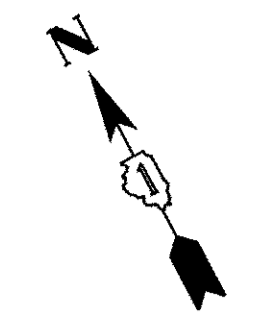
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PLOT DATE = 2/21/2017	DATE - 2/21/17	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLAN  
 STREAMWOOD BOULEVARD**

SCALE: 1" = 50'    SHEET NO. 2 OF 3 SHEETS    STA. 128+00 TO STA. 156+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	7
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61D81	



**LEGEND:**

	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT
	SIDEWALK REMOVAL
	FULL DEPTH (MEDIAN REPLACEMENT)

**SCHEDULE\***  
 DRIVEWAY PAVEMENT REMOVAL /  
 PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH

STATION	OFFSET (LT/RT)	QUANTITY (SQ YD)
101+91	LT	2
105+20	RT	2
108+58	LT	2
111+02	LT	2
115+60	RT	2
121+92	RT	2
128+07	RT	2
128+77	LT	2
138+70	LT	2
145+06	RT	2
145+92	LT	2
148+54	LT	2
148+66	RT	2
150+34	RT	2
156+14	LT	2
156+22	RT	2
159+09	RT	2
159+74	RT	2
159+81	LT	2
161+36	RT	2
161+54	LT	2
162+03	RT	2
162+52	LT	2
162+75	RT	2
163+21	LT	2

\* NOTE: DRIVEWAY REMOVAL AND REPLACEMENT IS FOR INFORMATIONAL PURPOSES ONLY. ACTUAL LOCATIONS AND QUANTITIES TO BE DETERMINED AND MARKED BY THE ENGINEER PRIOR TO CONSTRUCTION.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen  
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PLOT DATE = 2/21/2017	DATE - 2/21/17	REVISED -

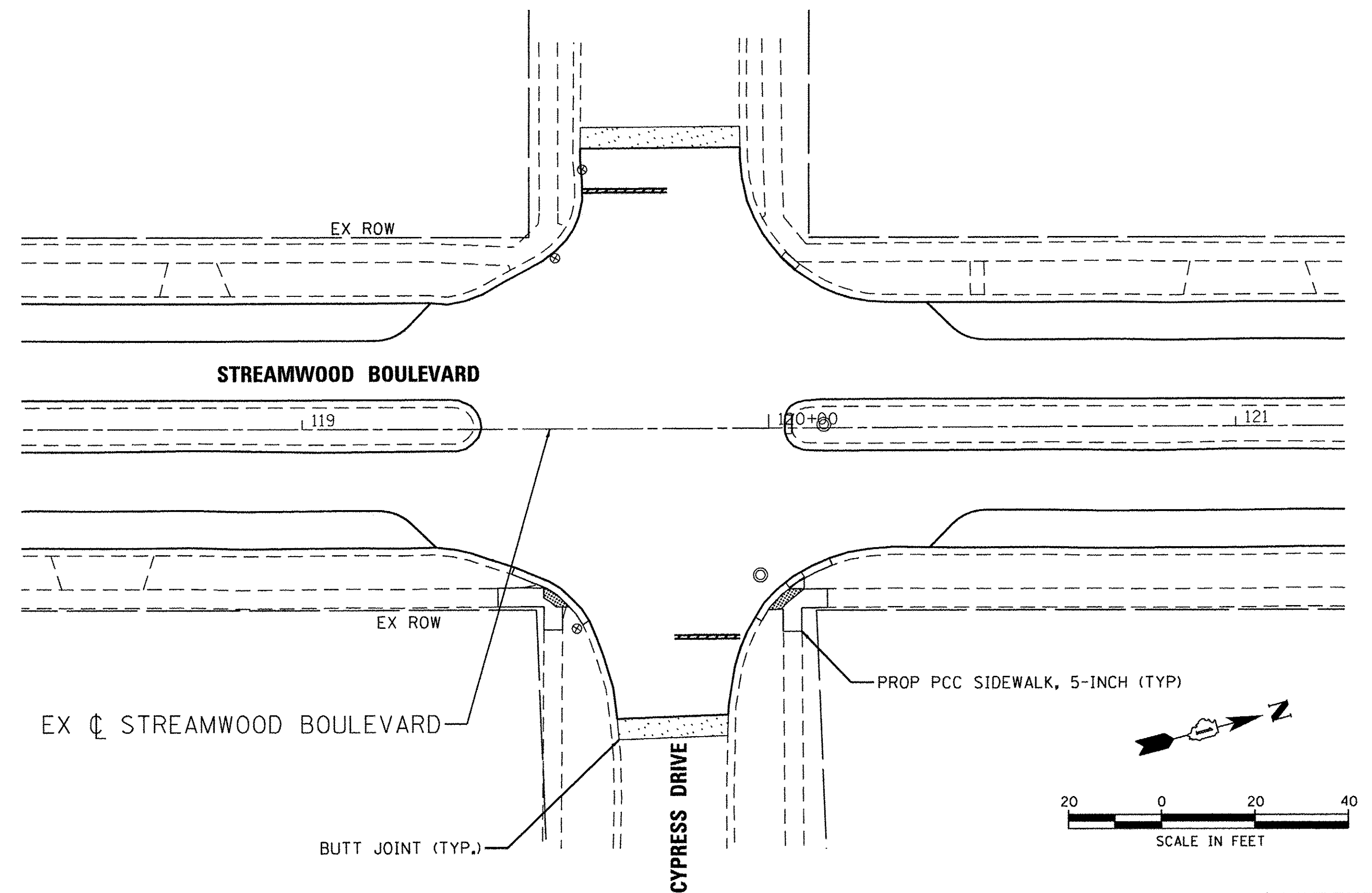
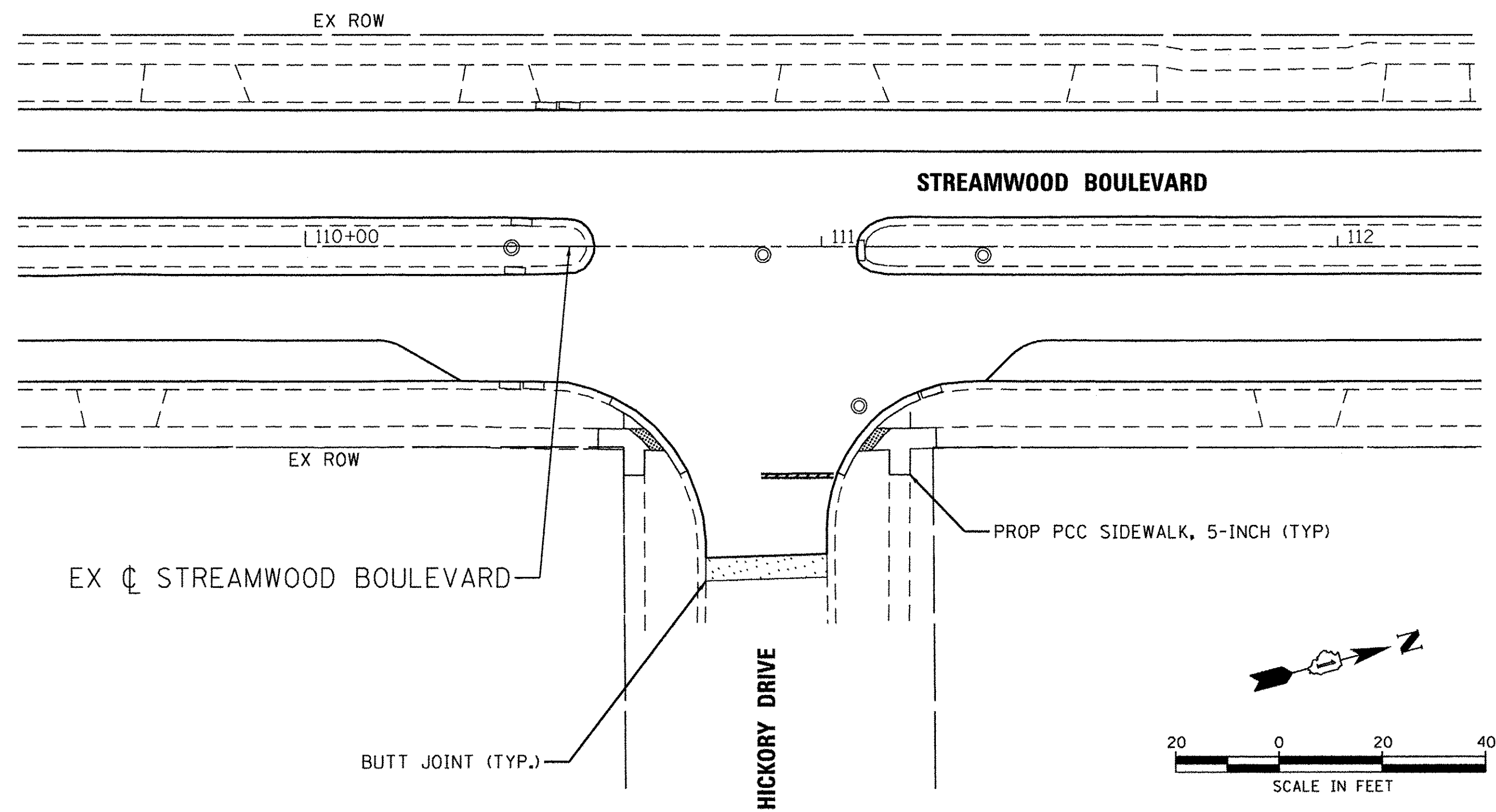
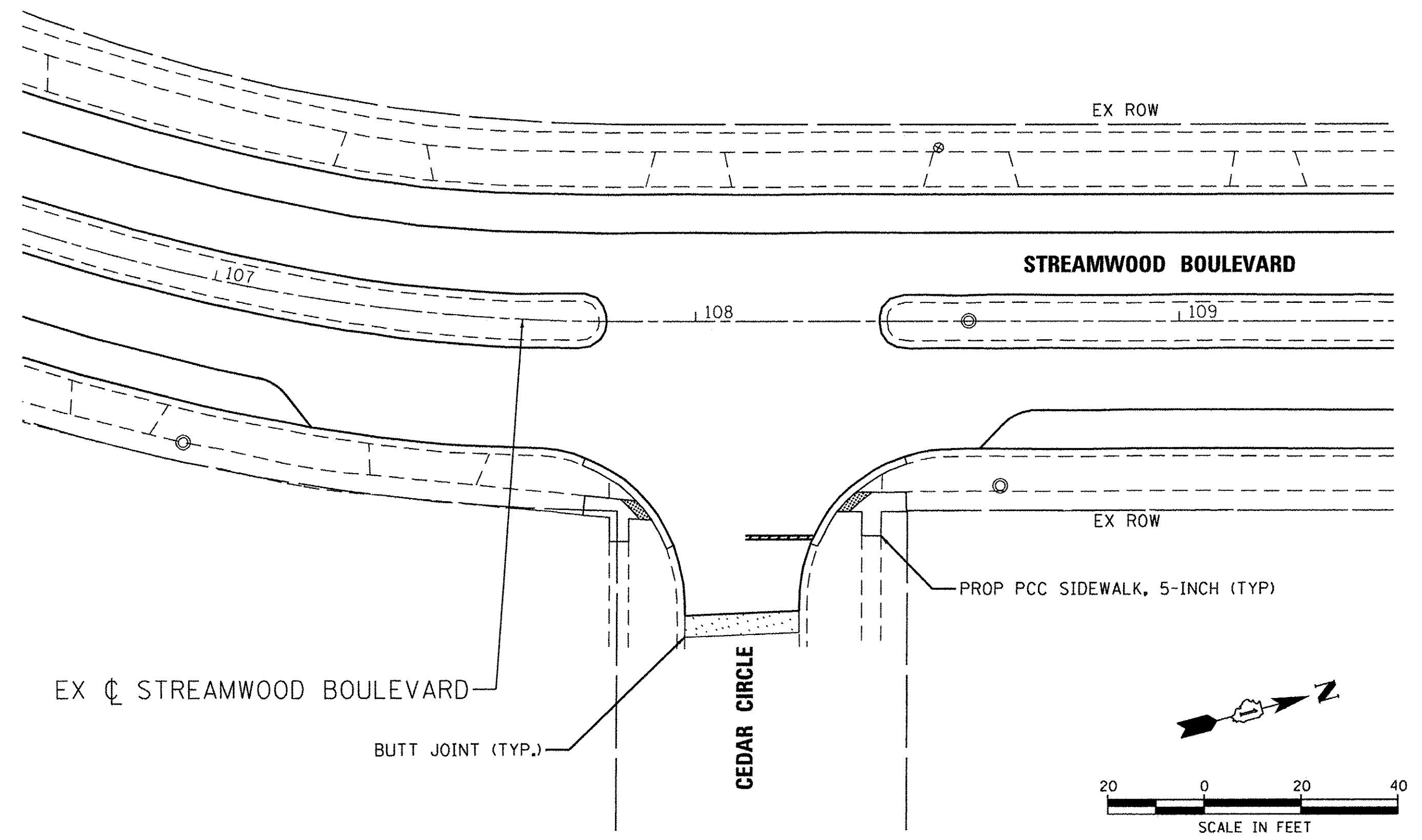
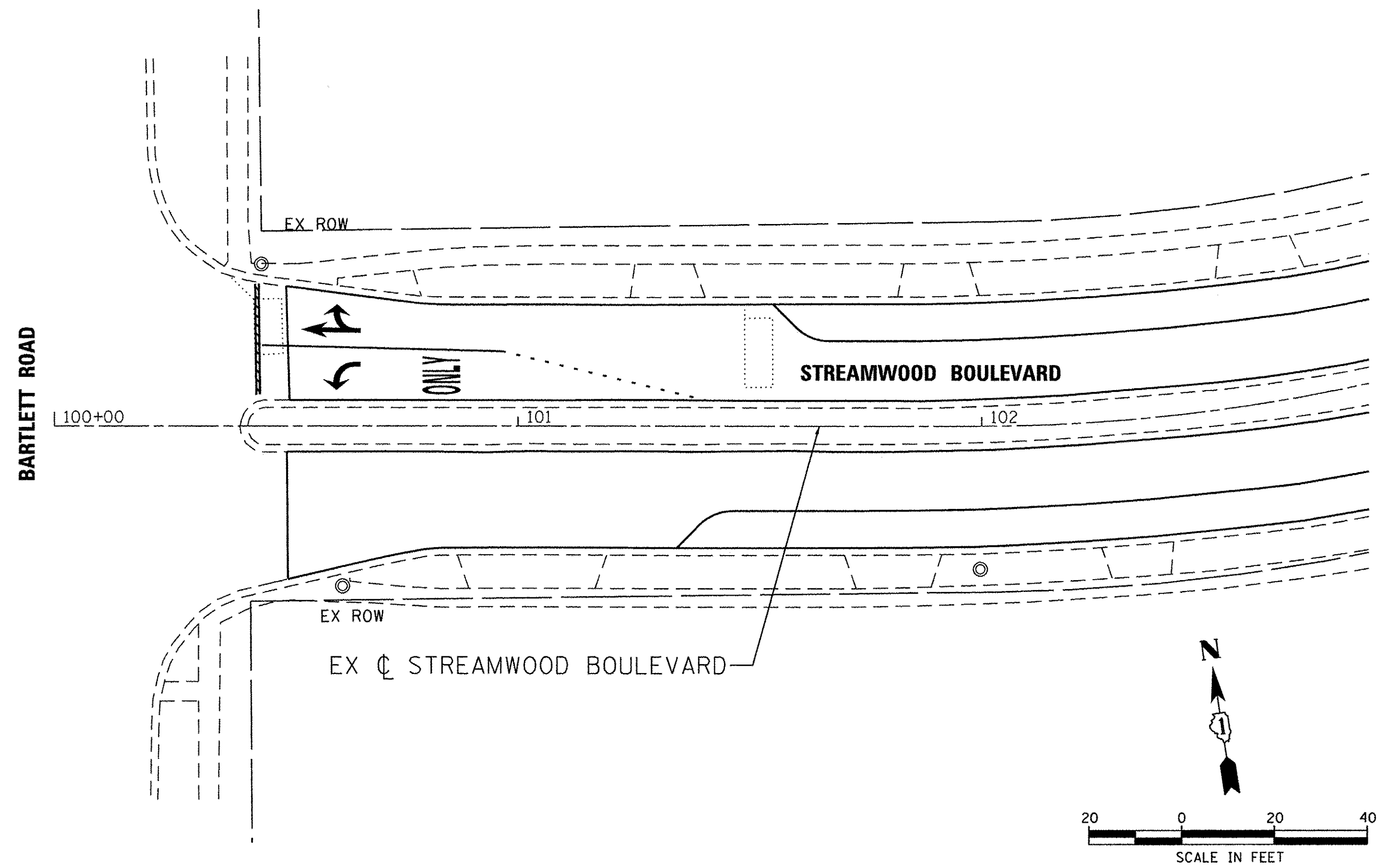
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RESURFACING PLAN  
 STREAMWOOD BOULEVARD**

SCALE: 1" = 50' SHEET NO. 3 OF 3 SHEETS STA. 156+00 TO STA. 165+96

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	8
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO.			61D81	





COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Jhorwit  
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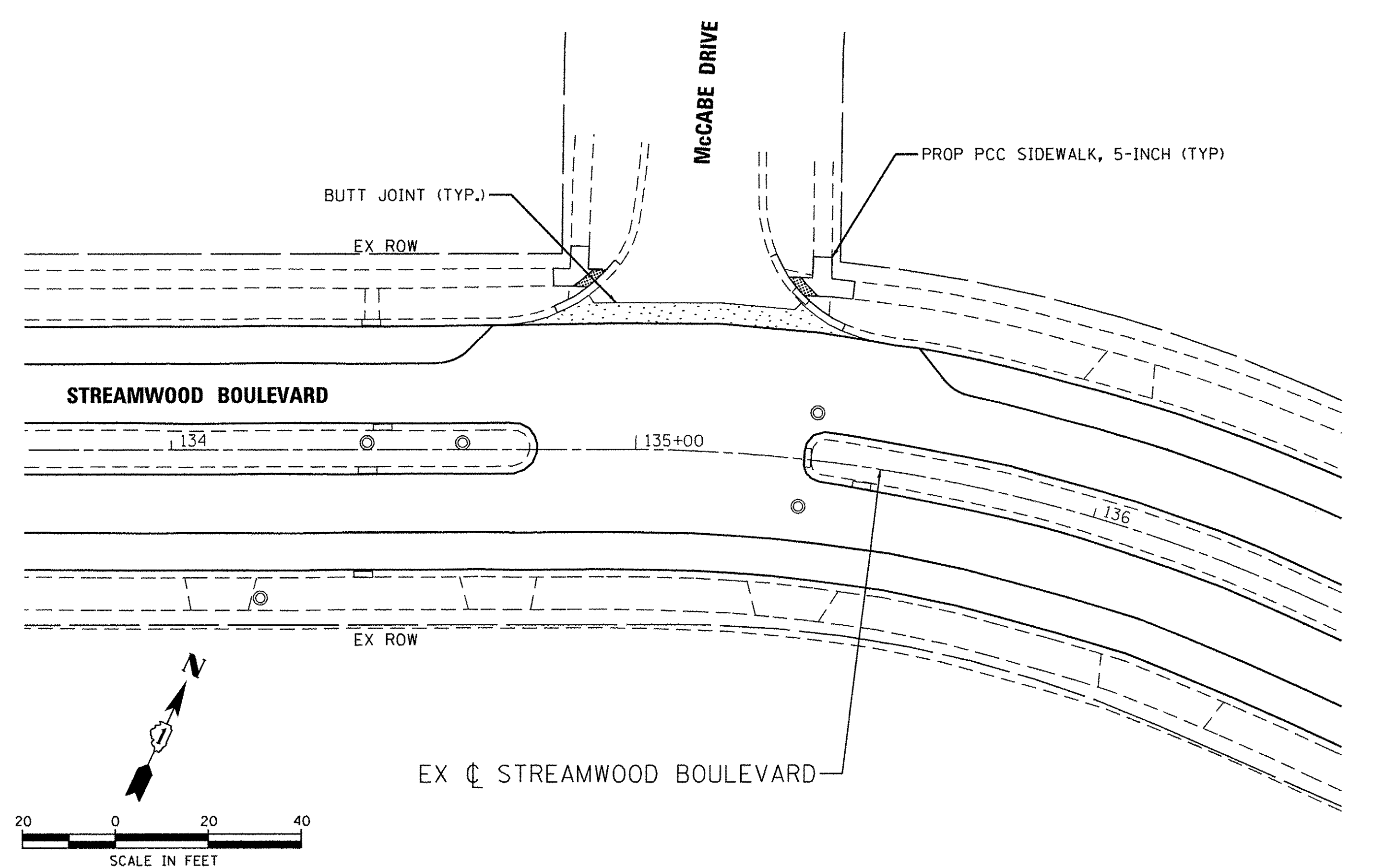
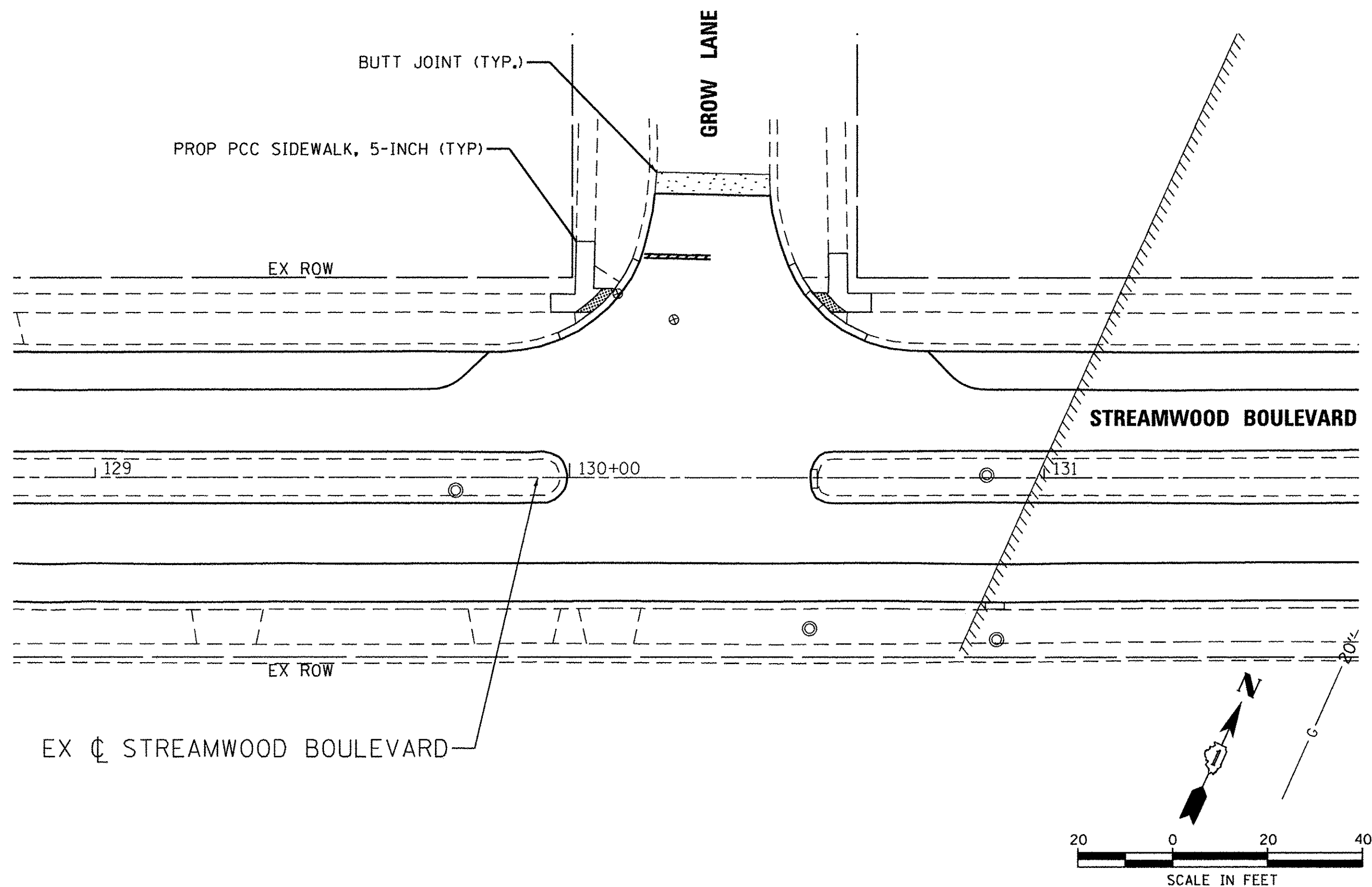
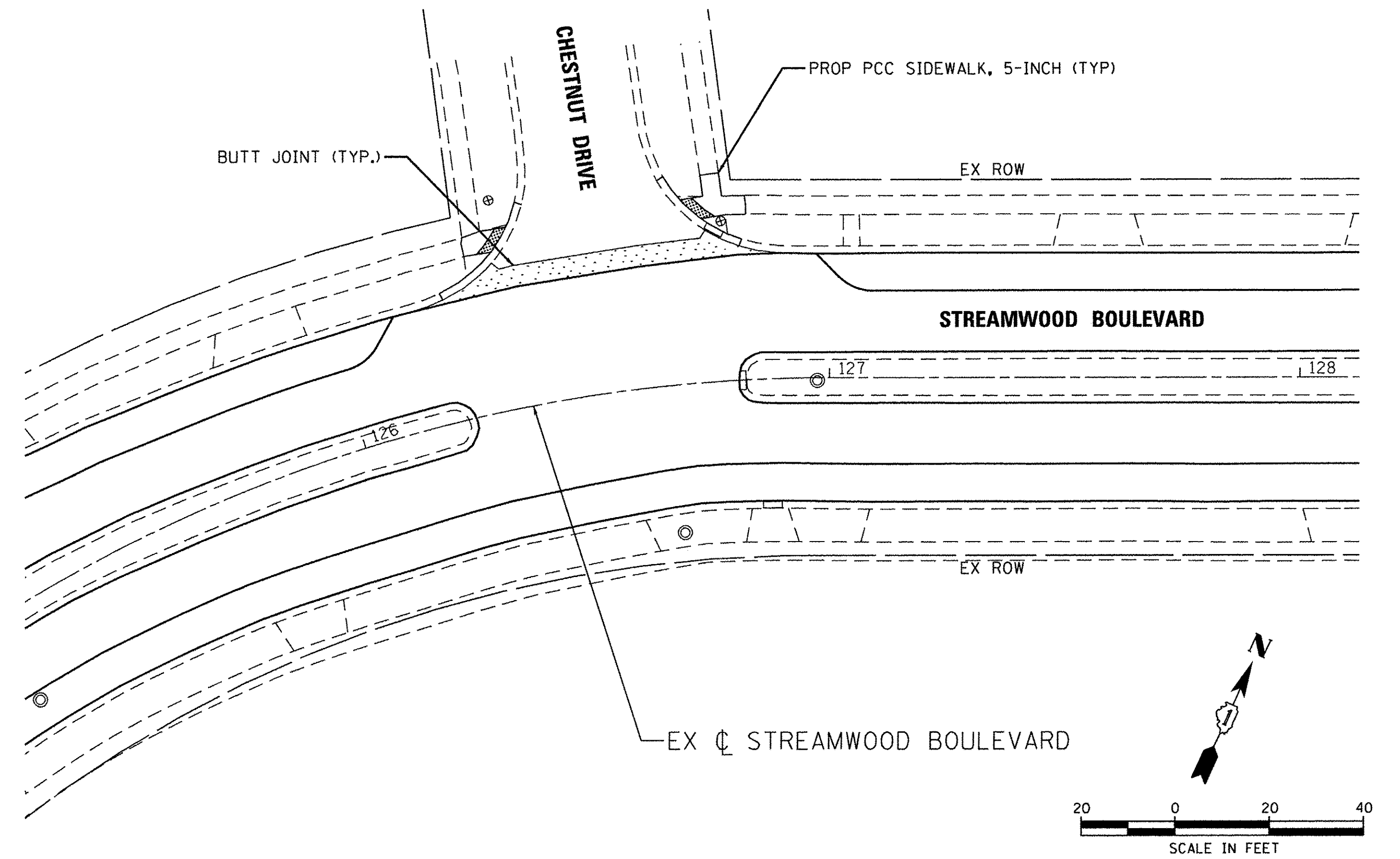
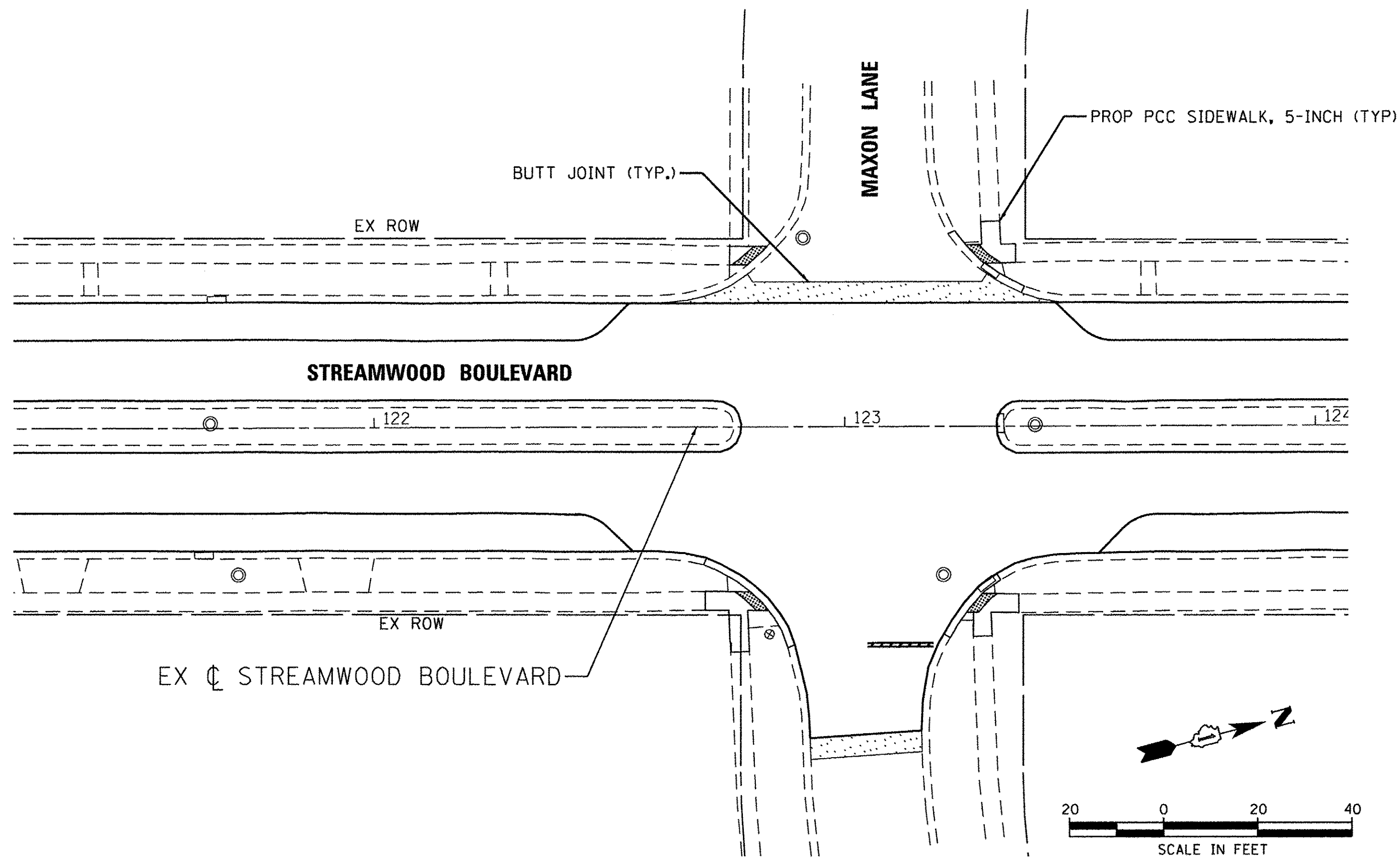
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

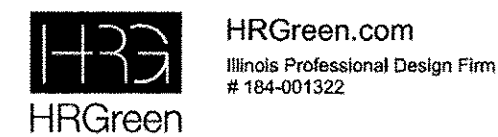
INTERSECTION PLANS  
 STREAMWOOD BOULEVARD

SCALE: 1" = 20' SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	9
FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61DB1	



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Jhorwit  
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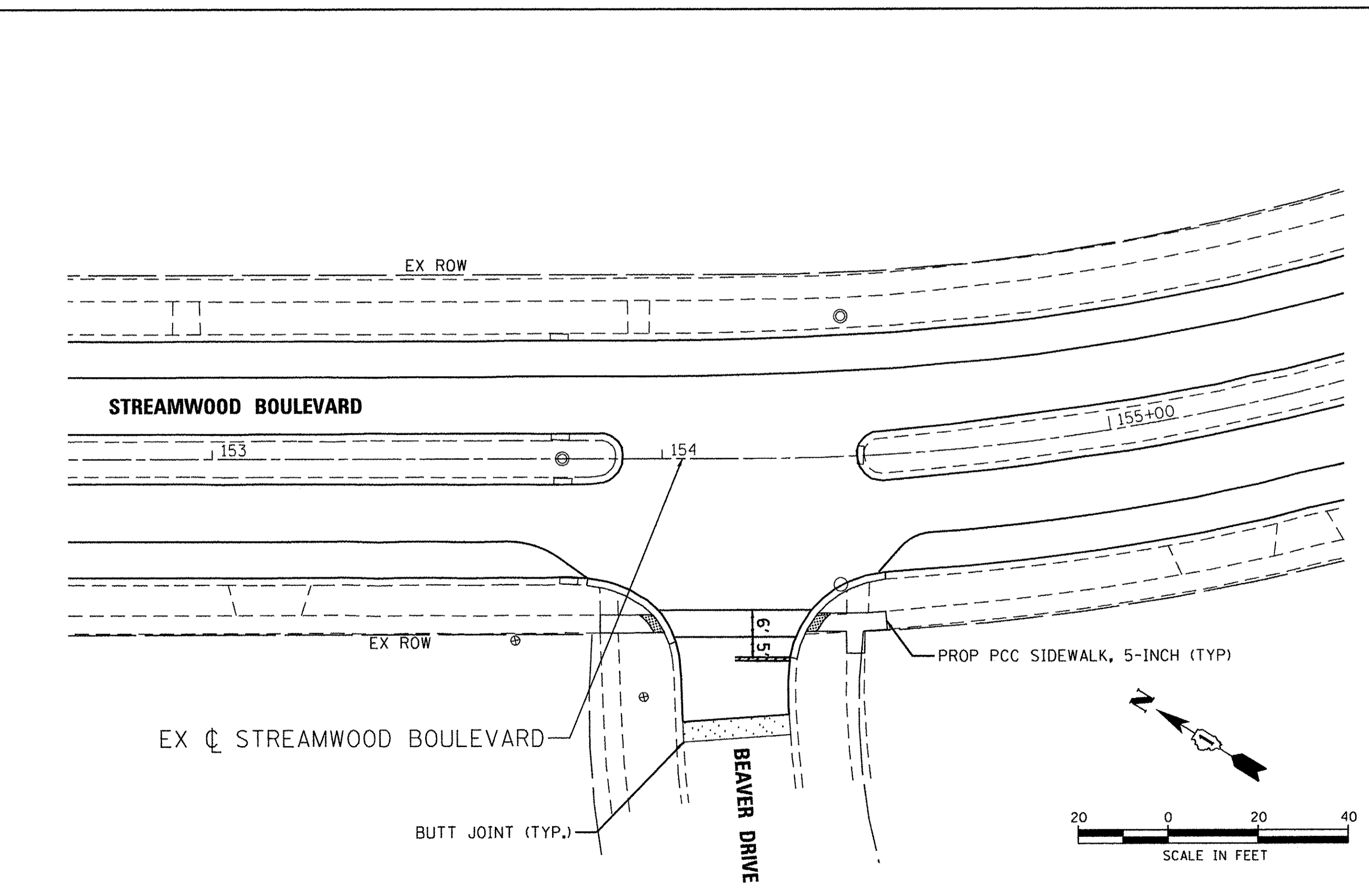
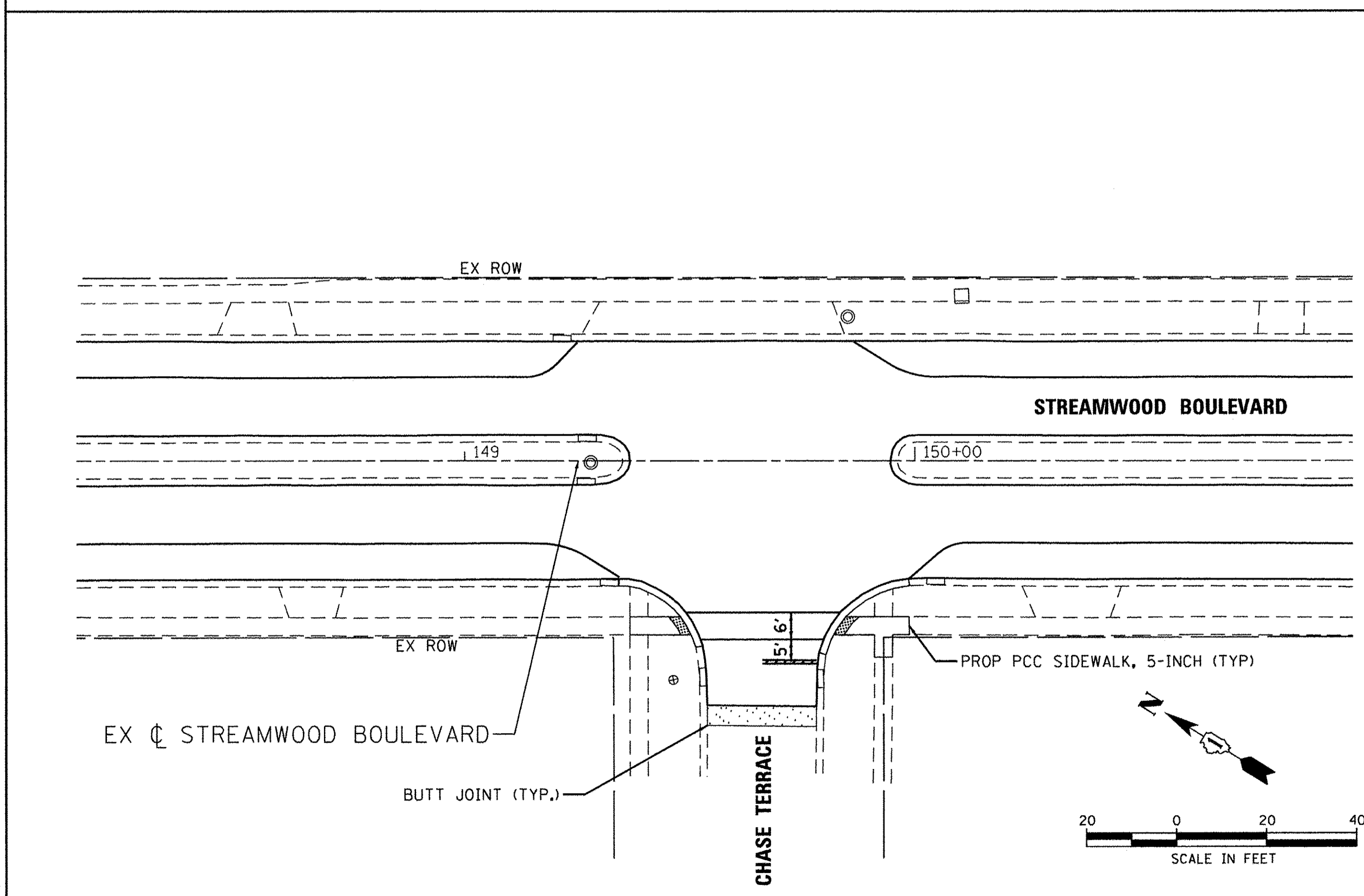
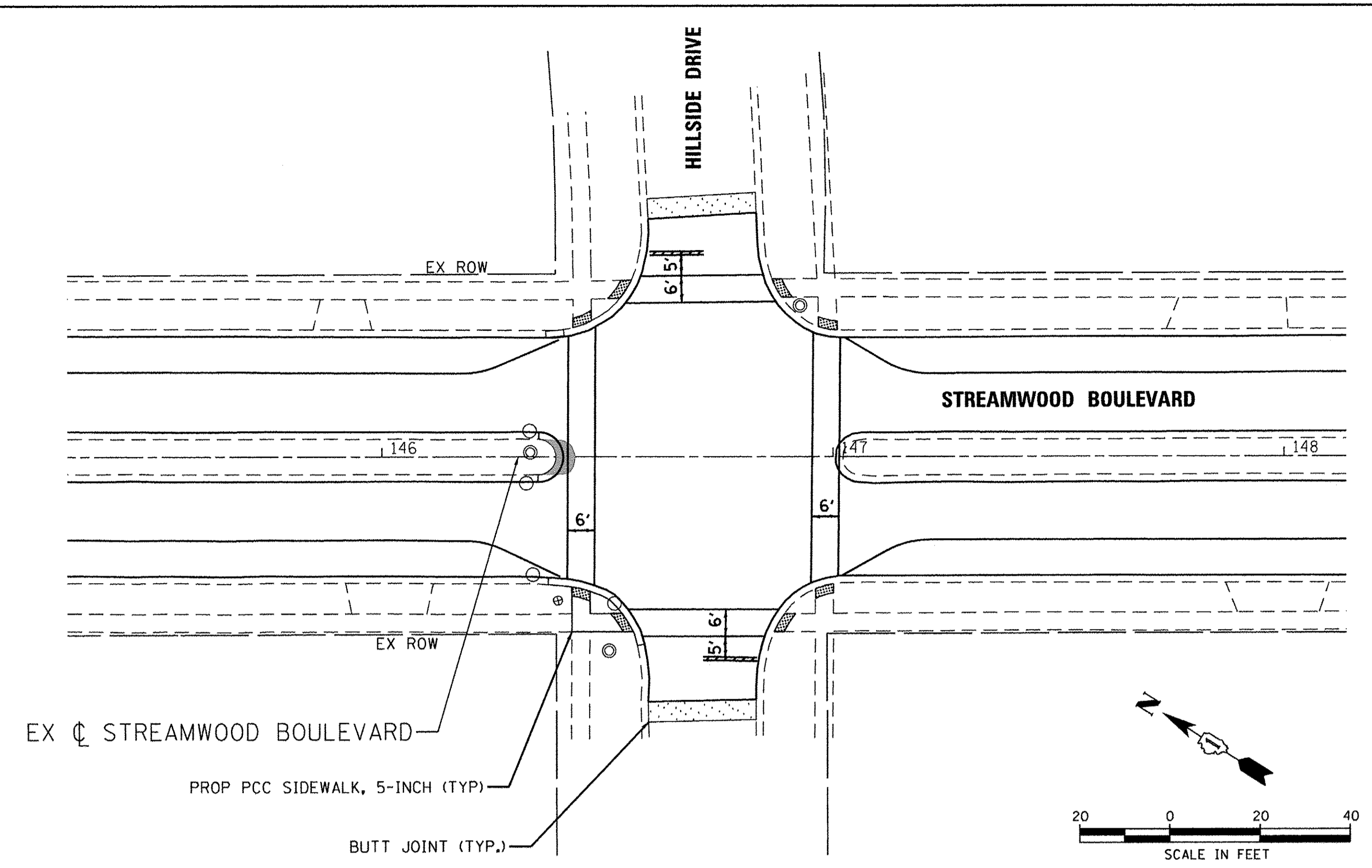
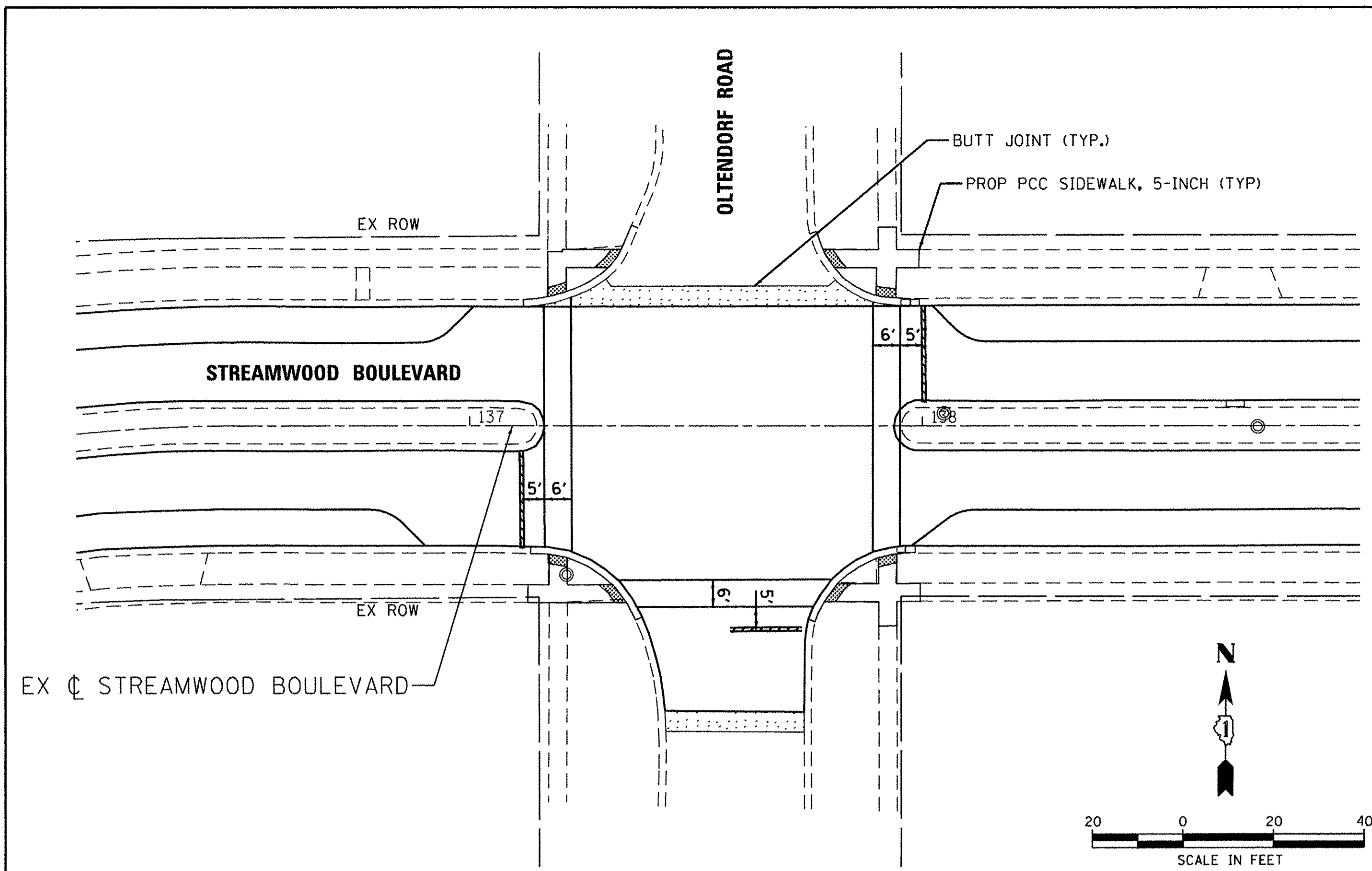
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INTERSECTION PLANS  
STREAMWOOD BOULEVARD

SCALE: 1" = 20' SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	10
CONTRACT NO. 61DB1				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: jhorwit  
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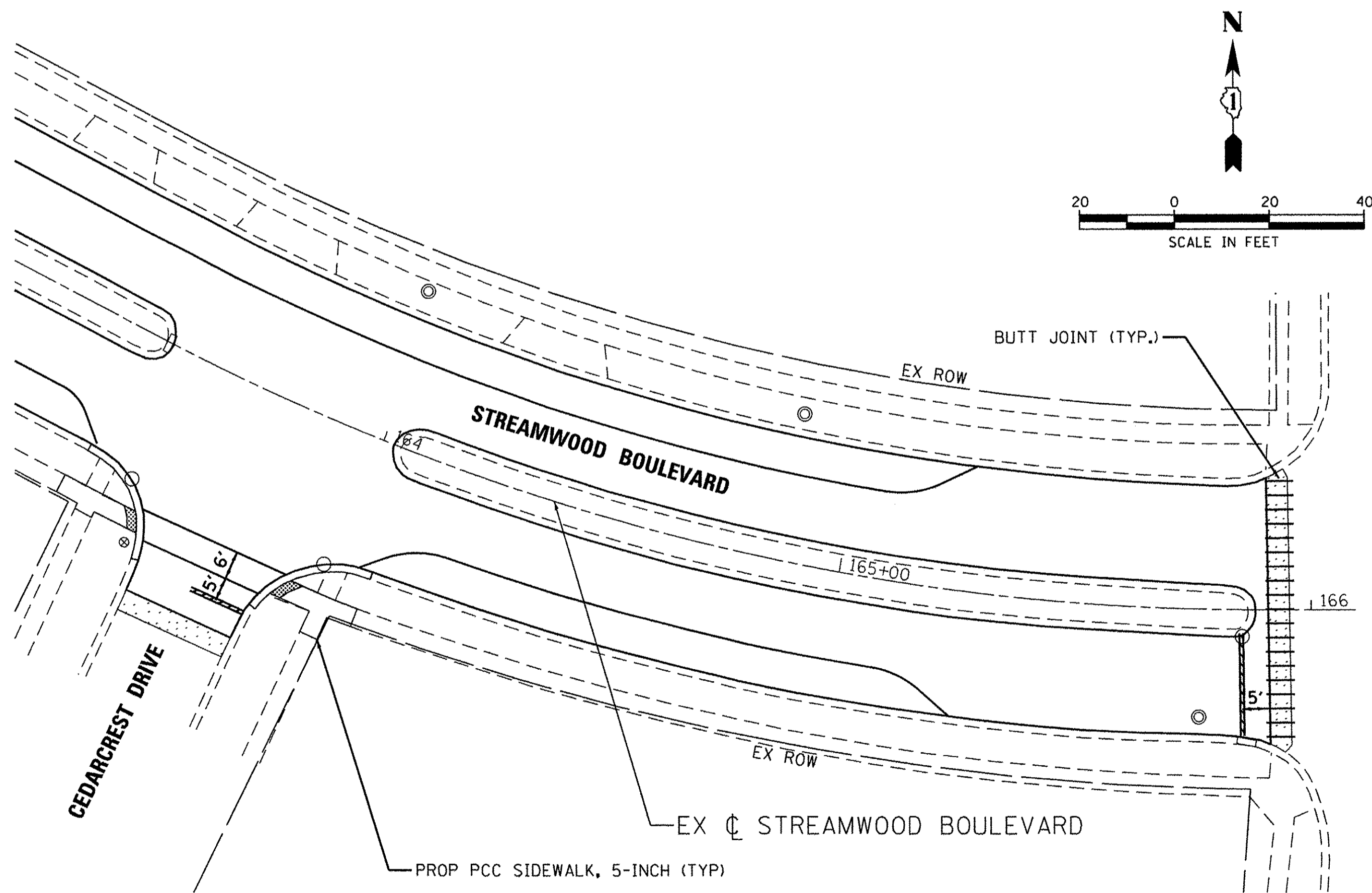
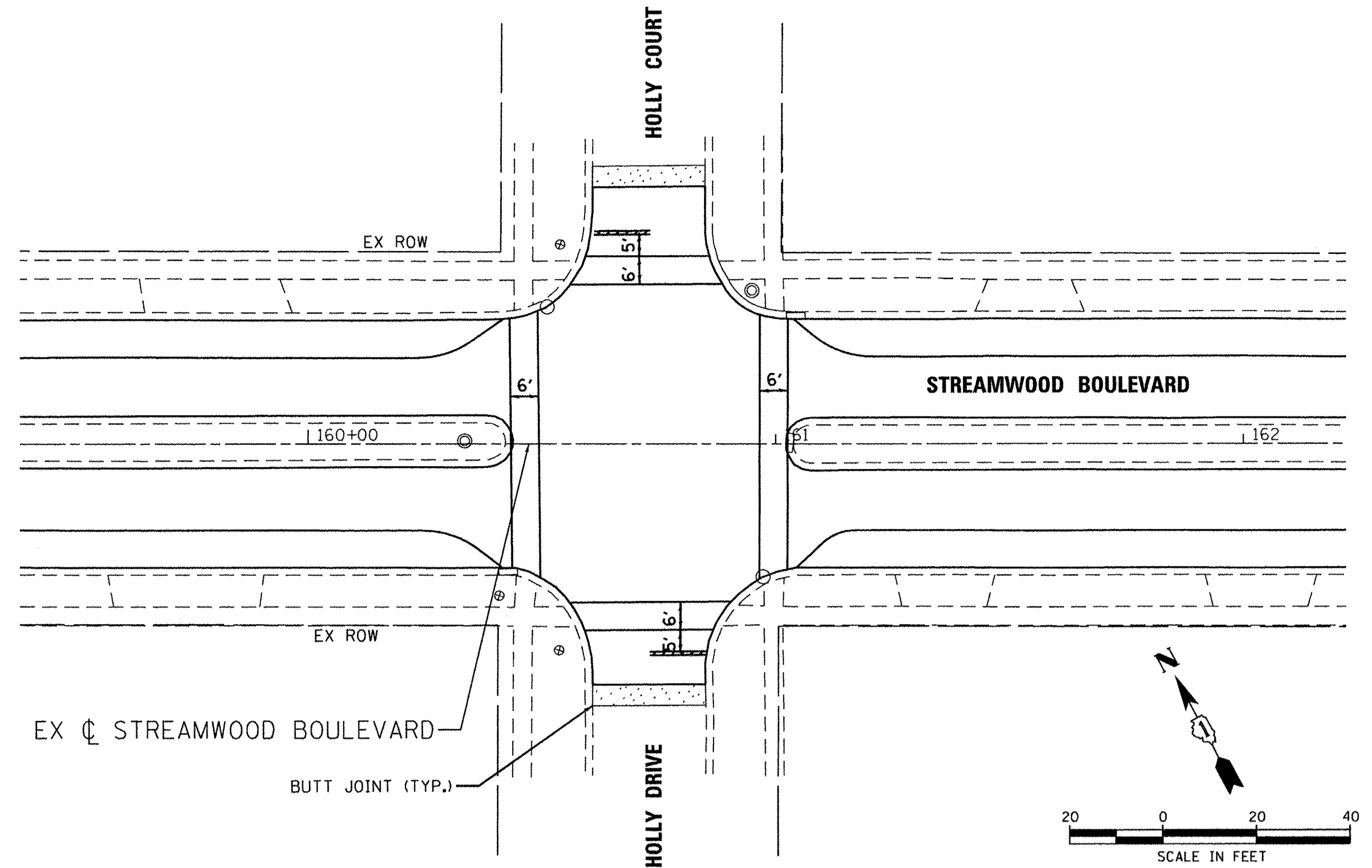
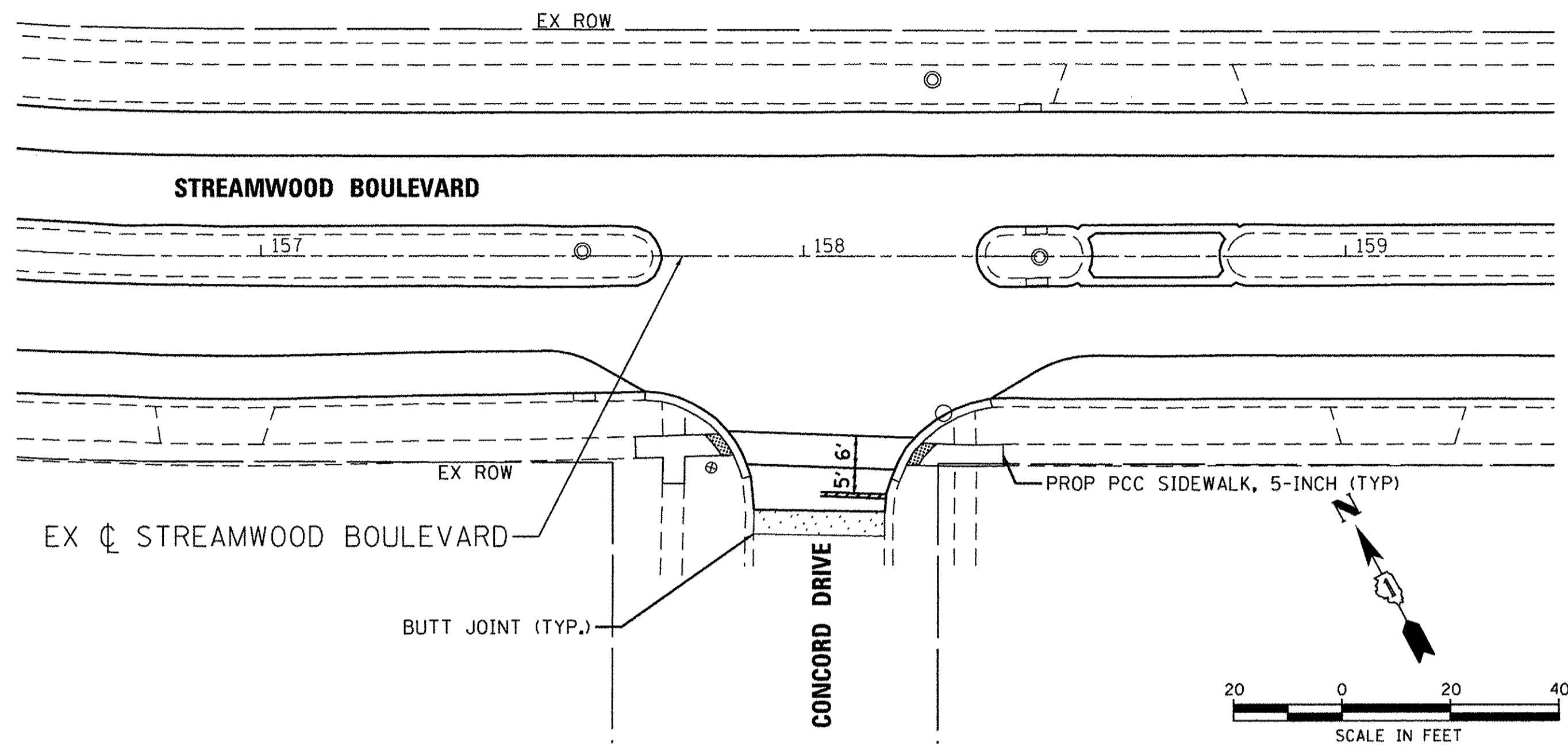


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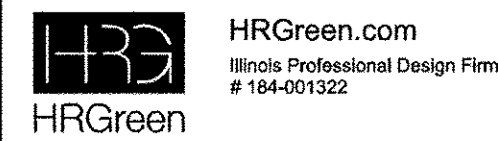
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>INTERSECTION PLANS</b> <b>STREAMWOOD BOULEVARD</b>	
SCALE: 1" = 20'	SHEET NO. 3 OF 4 SHEETS
STA. _____	TO STA. _____

F.A. RTE. 1661	SECTION 15-00062-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 11
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 610B1	



COMPANY NAME: HRGreen.com  
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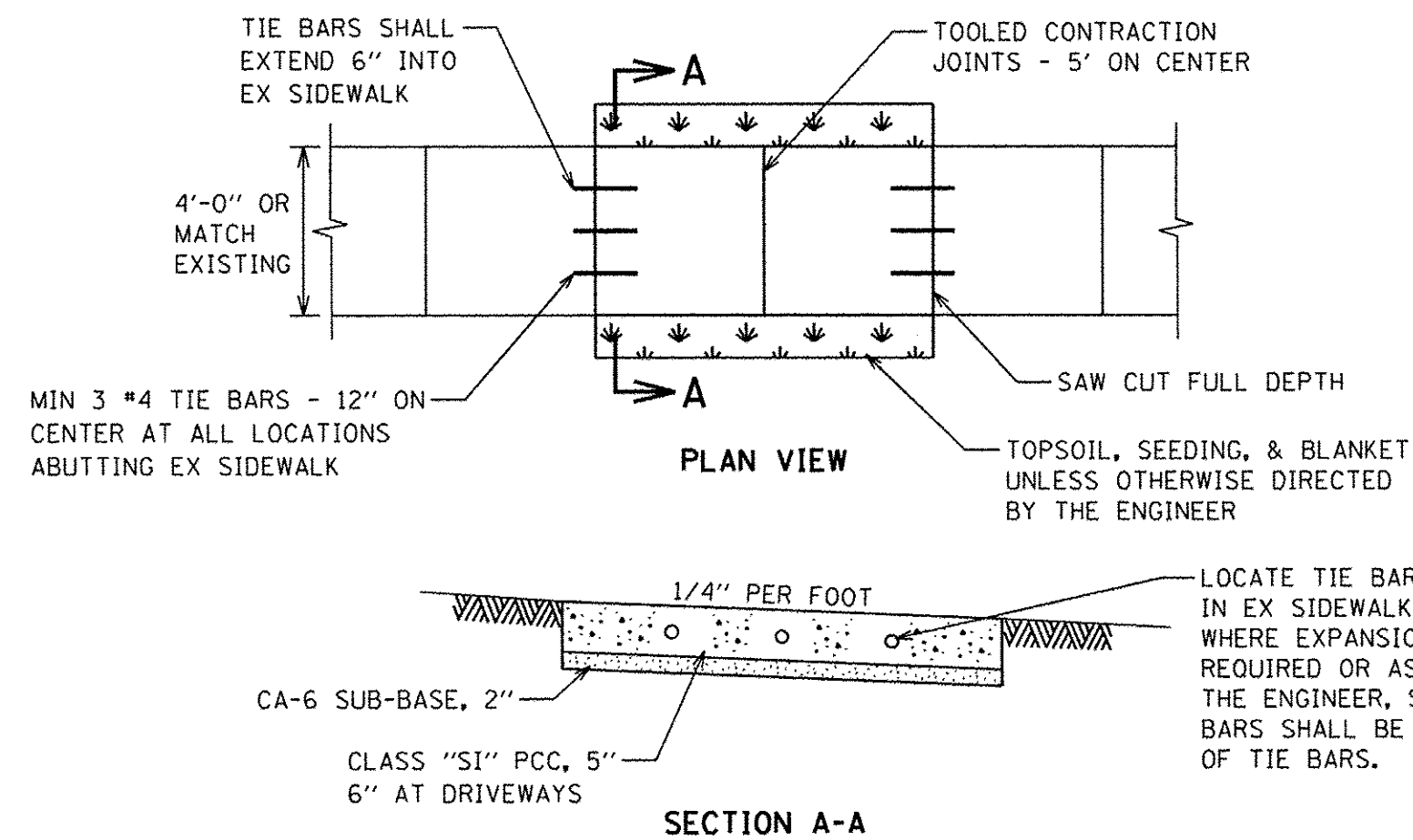
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

INTERSECTION PLANS  
 STREAMWOOD BOULEVARD  
 SCALE: 1" = 20' SHEET NO. 4 OF 4 SHEETS STA. TO STA.

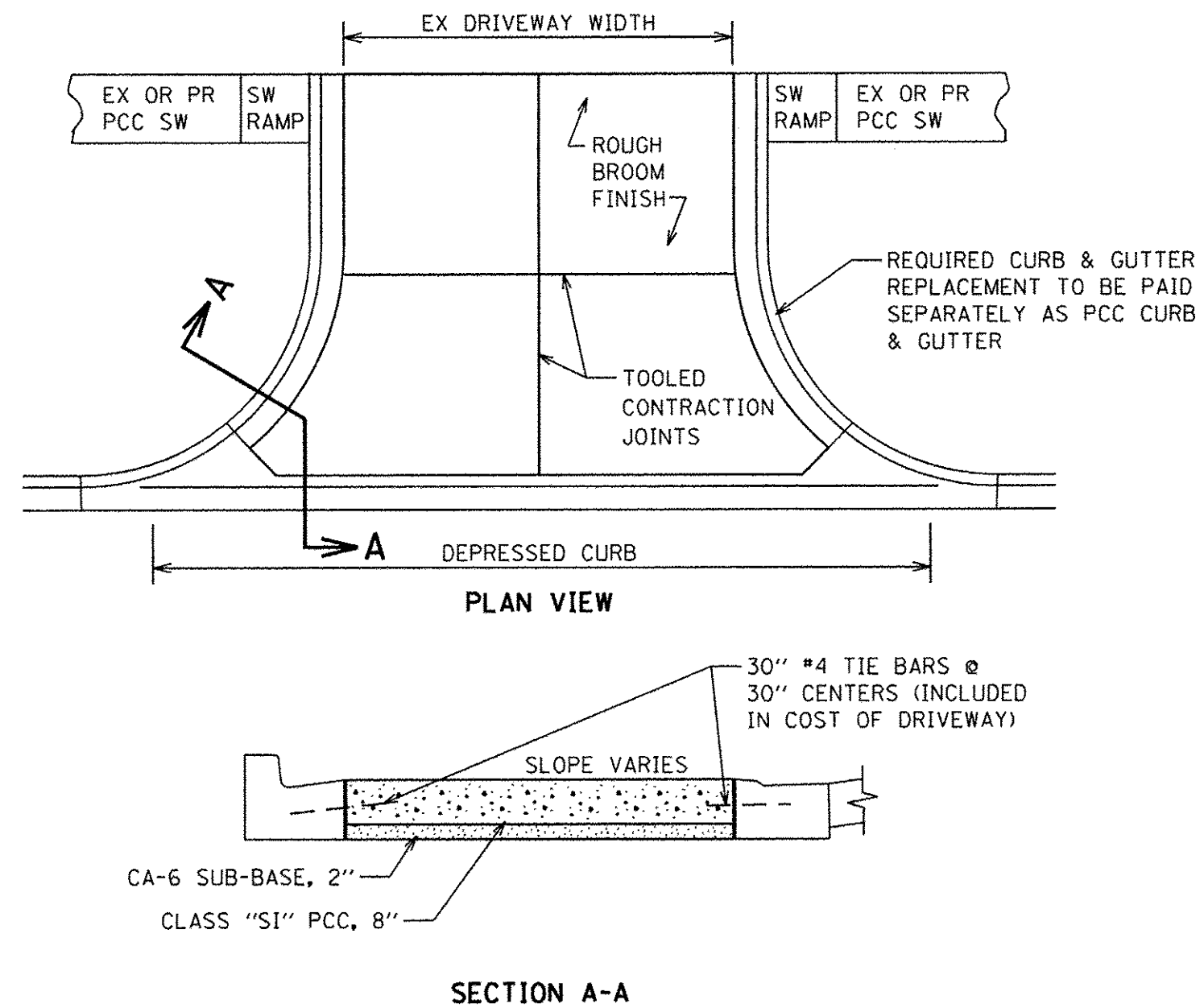
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FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61DB1	



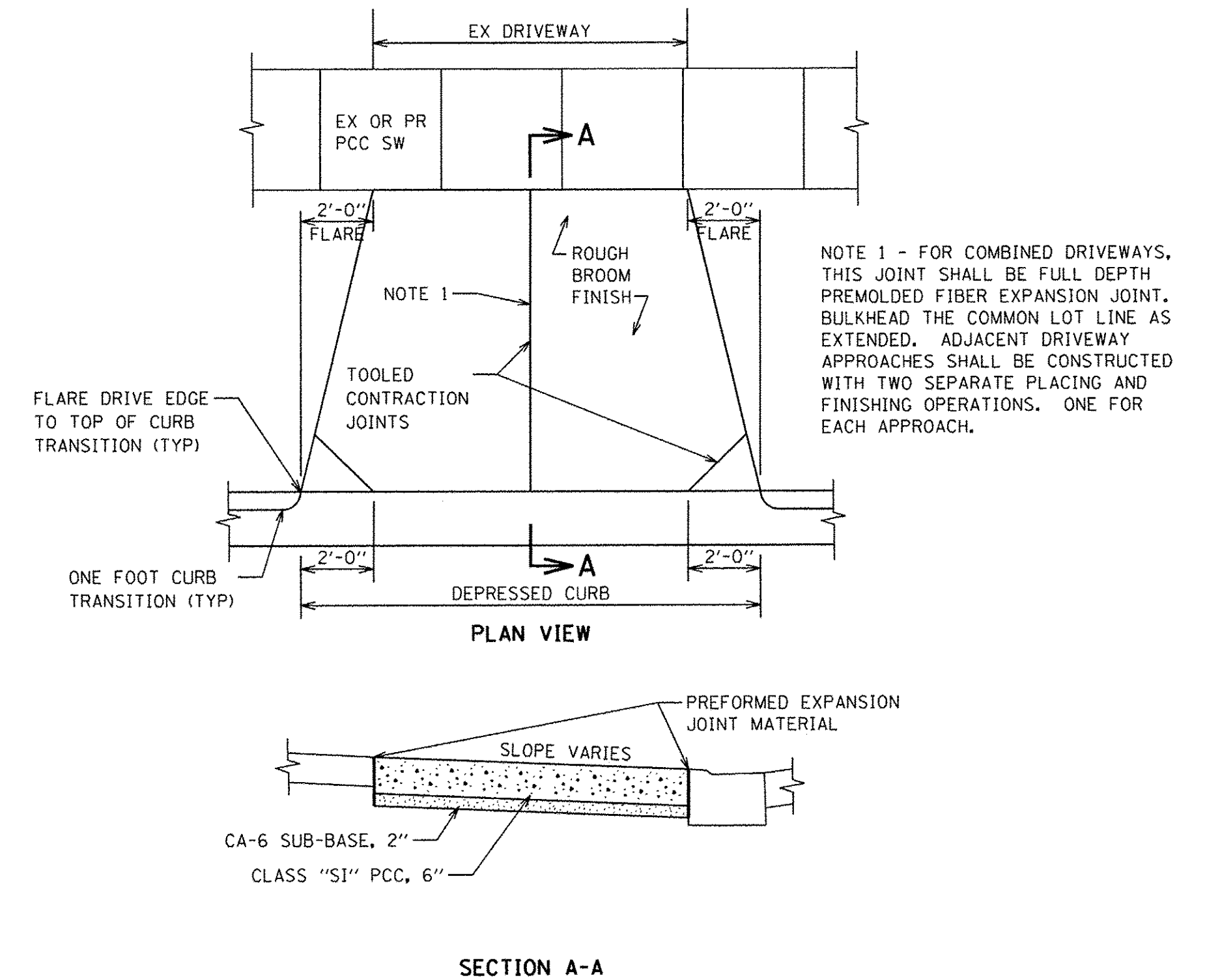
**PCC SIDEWALK**



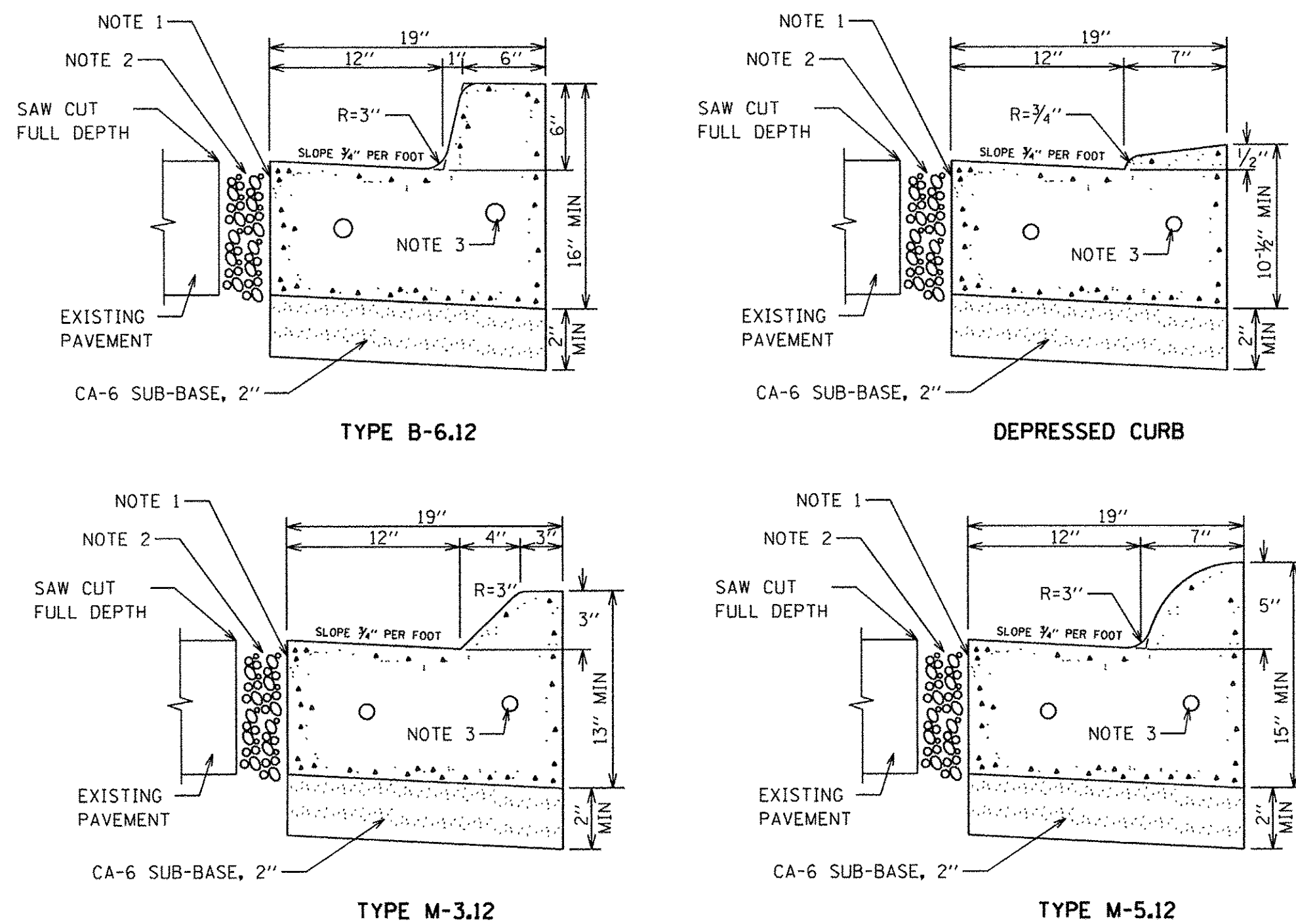
**PCC DRIVEWAY, COMMERCIAL**



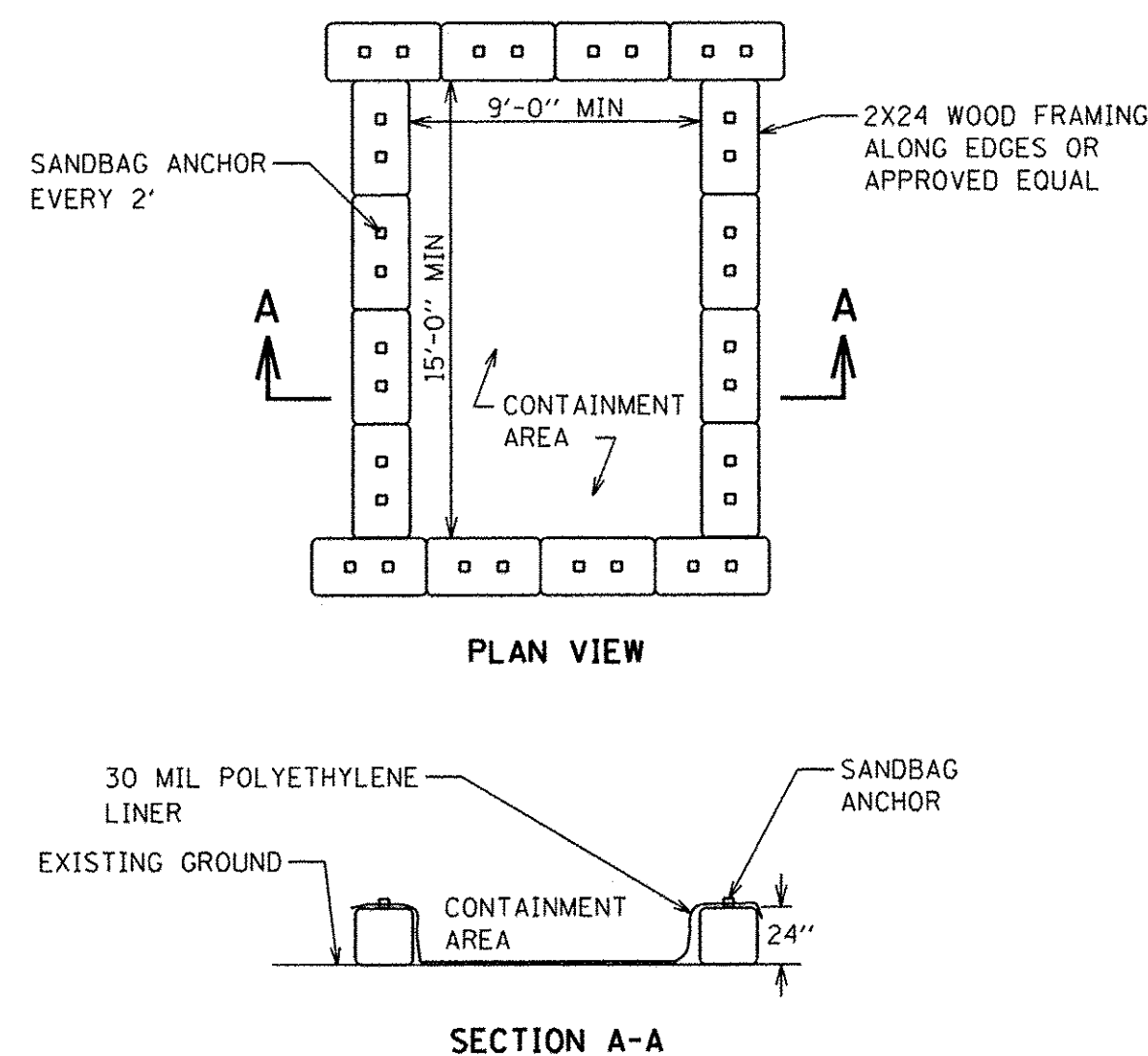
**PCC DRIVEWAY, RESIDENTIAL**



**PCC CURB & GUTTER**



**CONCRETE WASHOUT AREA**



**NOTES:**

MAINTAINING WASHOUT FACILITY SHALL INCLUDE REMOVING AND DISPOSING OF HARDENED CONCRETE AND SLURRY AND RETURNING THE FACILITY TO A FUNCTIONAL CONDITION

WASHOUT FACILITY SHALL BE CLEANED OR RECONSTRUCTED IN A NEW AREA ONCE WASHOUT BECOMES TWO-THIRDS FULL

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: #PROJECT\_CONTACT#  
 CLIENT: #CLIENT#  
 FILE NAME: #FILE\_NAME#  
 PLOT DRIVER: #PLOT\_DRIVER#  
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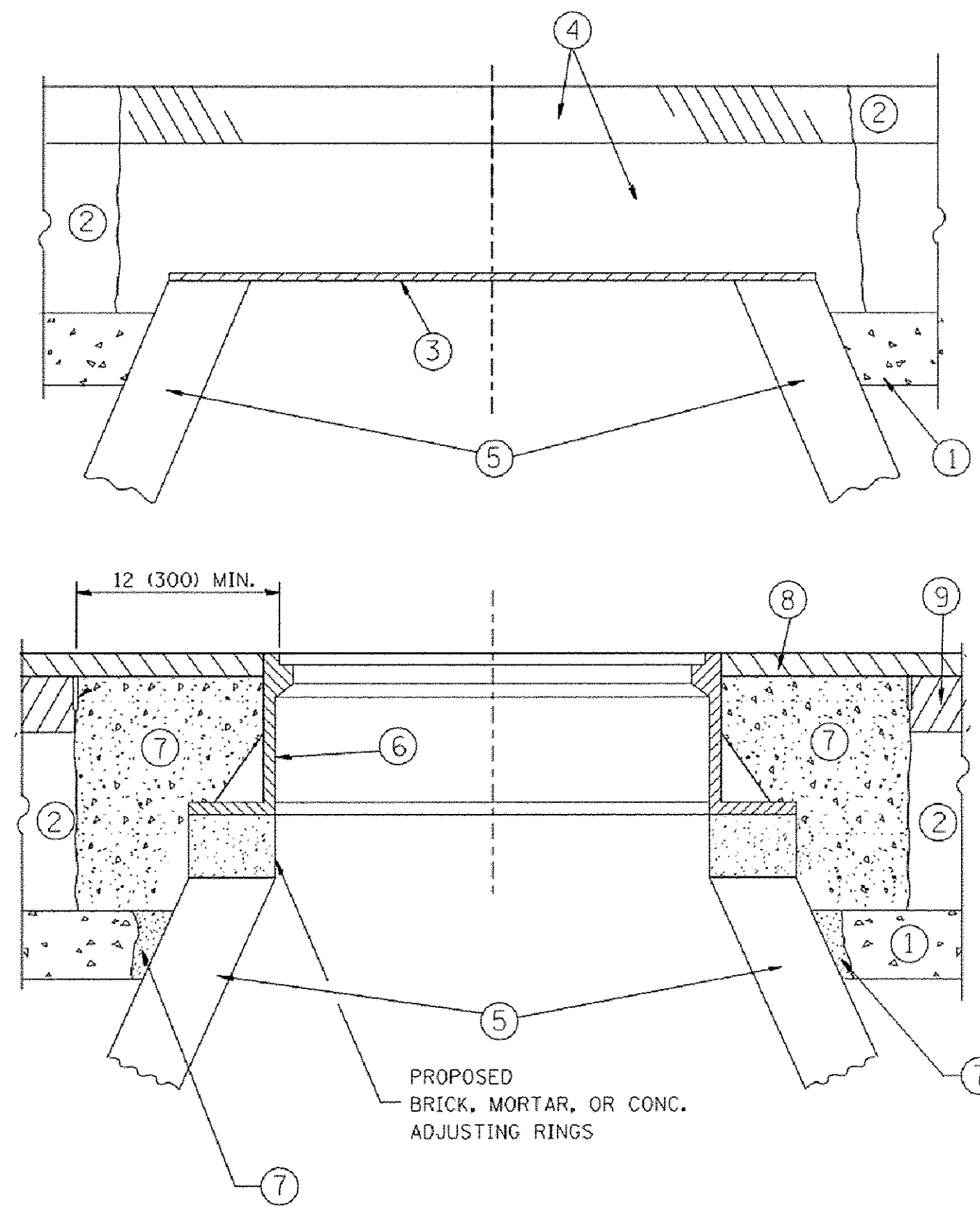
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PLOT DATE = 2/21/2017	DATE - 2/21/17	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF STREAMWOOD  
 DETAILS**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	13
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61D81	



**CONSTRUCTION PROCEDURES**

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
  - B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
  - C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
  - D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.
- STAGE 2 (AFTER PAVEMENT MILLING)**
- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
  - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
  - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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 CLIENT: #CLIENTS  
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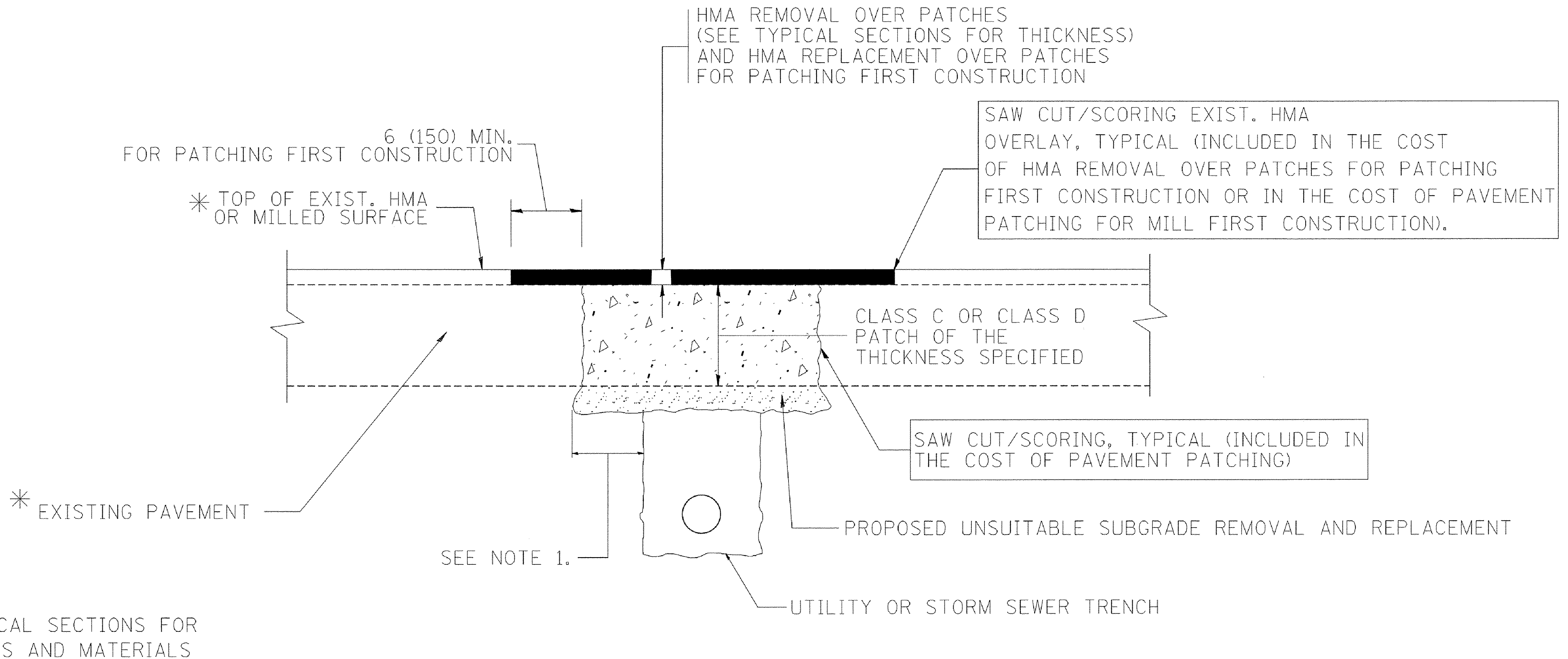
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		PLOT DATE = 12/6/2011	DATE - 10-25-94
			REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	14
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 61D81</b>		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				





**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

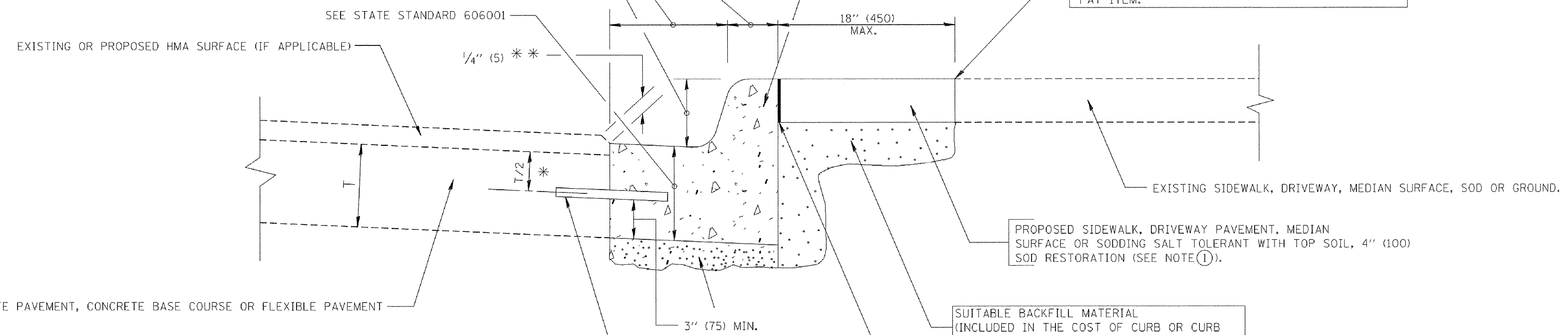
<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>	
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STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	15
BD400-04 (BD-22)		CONTRACT NO. 61D81		
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.  
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
- ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PROJECT CONTACT: #PROJECT CONTACT#  
CLIENT: #CLIENT#  
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		CHECKED -	REVISED - M. GOMEZ 01-22-01
		DATE - 03-11-94	REVISED - R. BORO 12-15-09

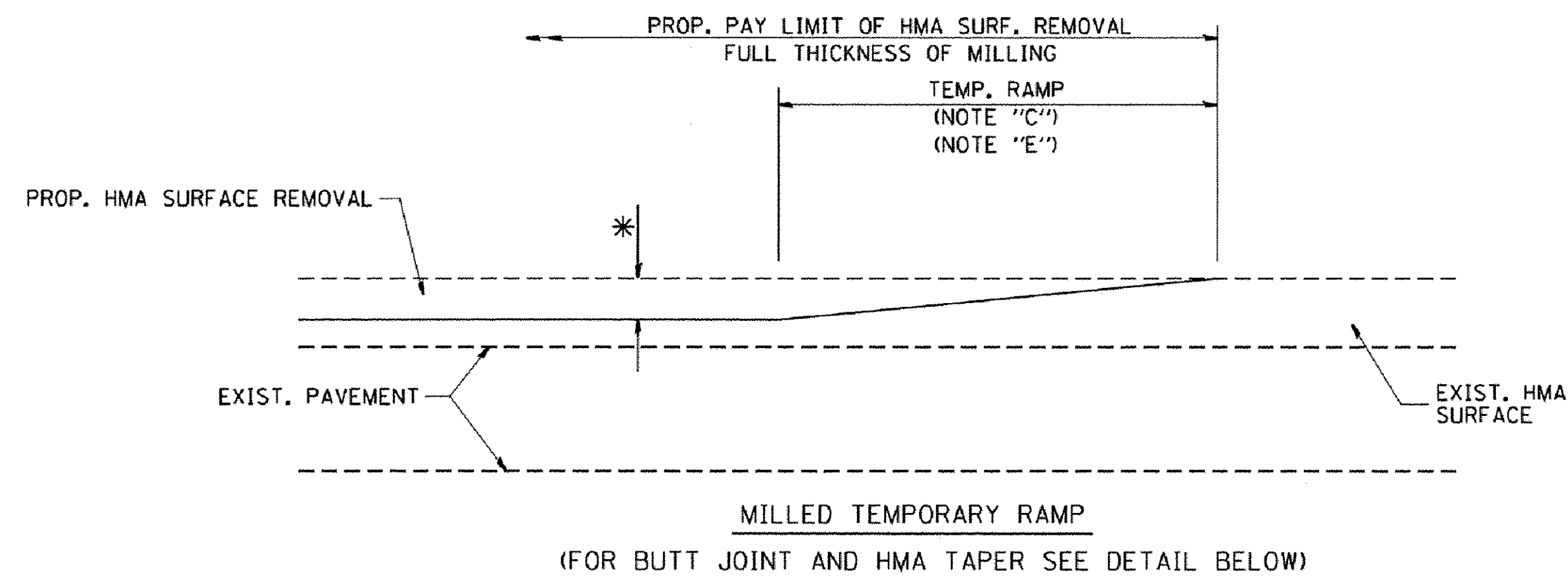
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

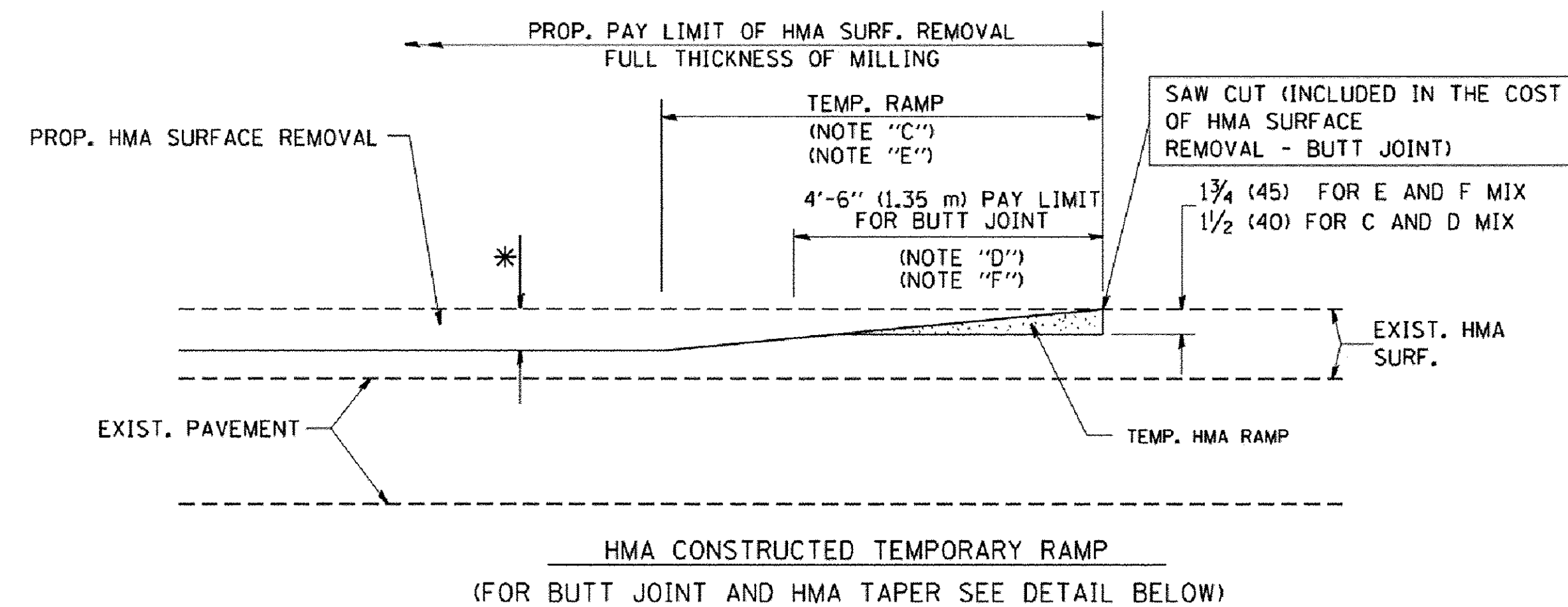
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BD600-06 (BD-24)			CONTRACT NO. 61DB1	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

COOK 15-00062-00-RS 157-shr-det-bd24.dgn



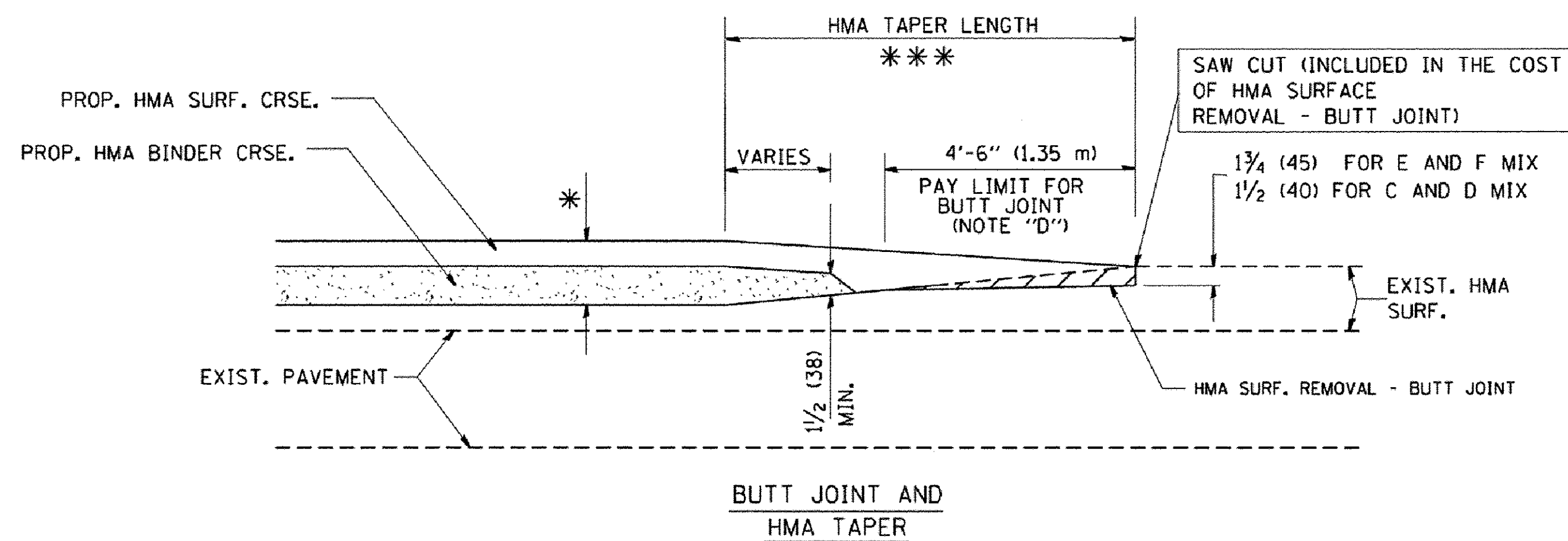


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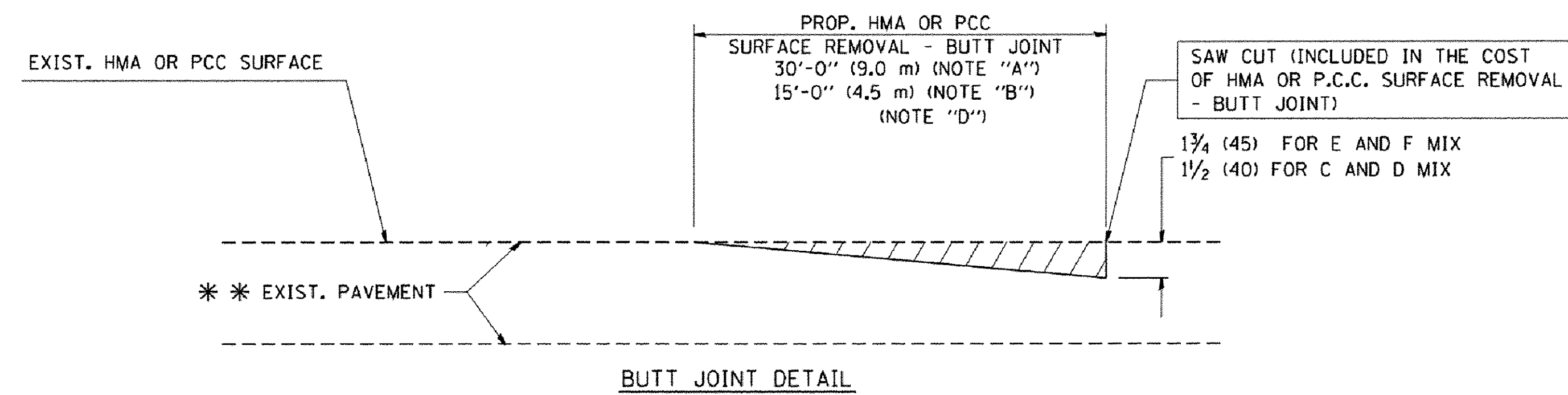


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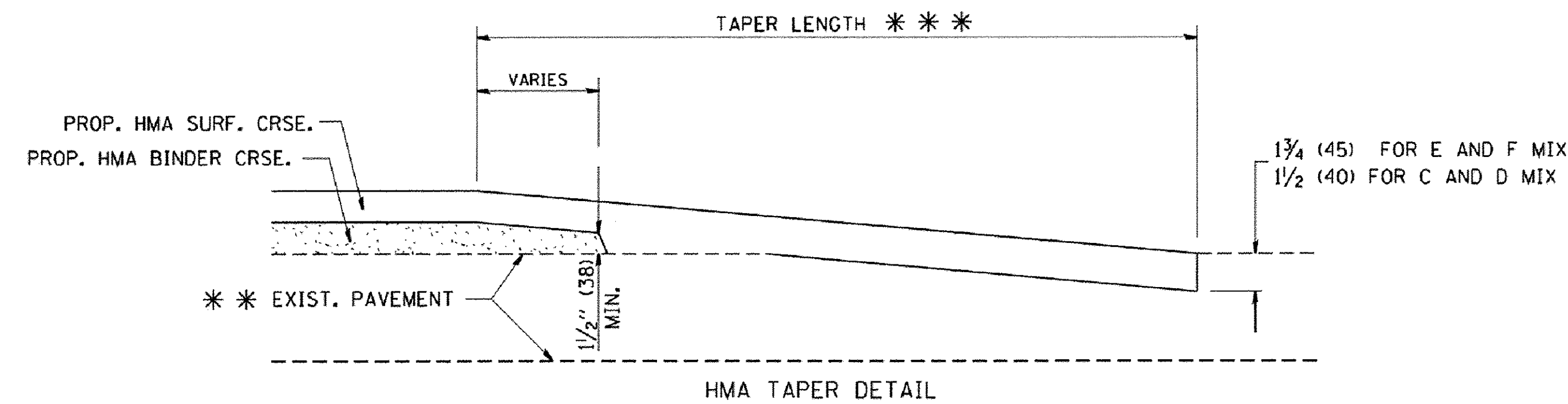
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

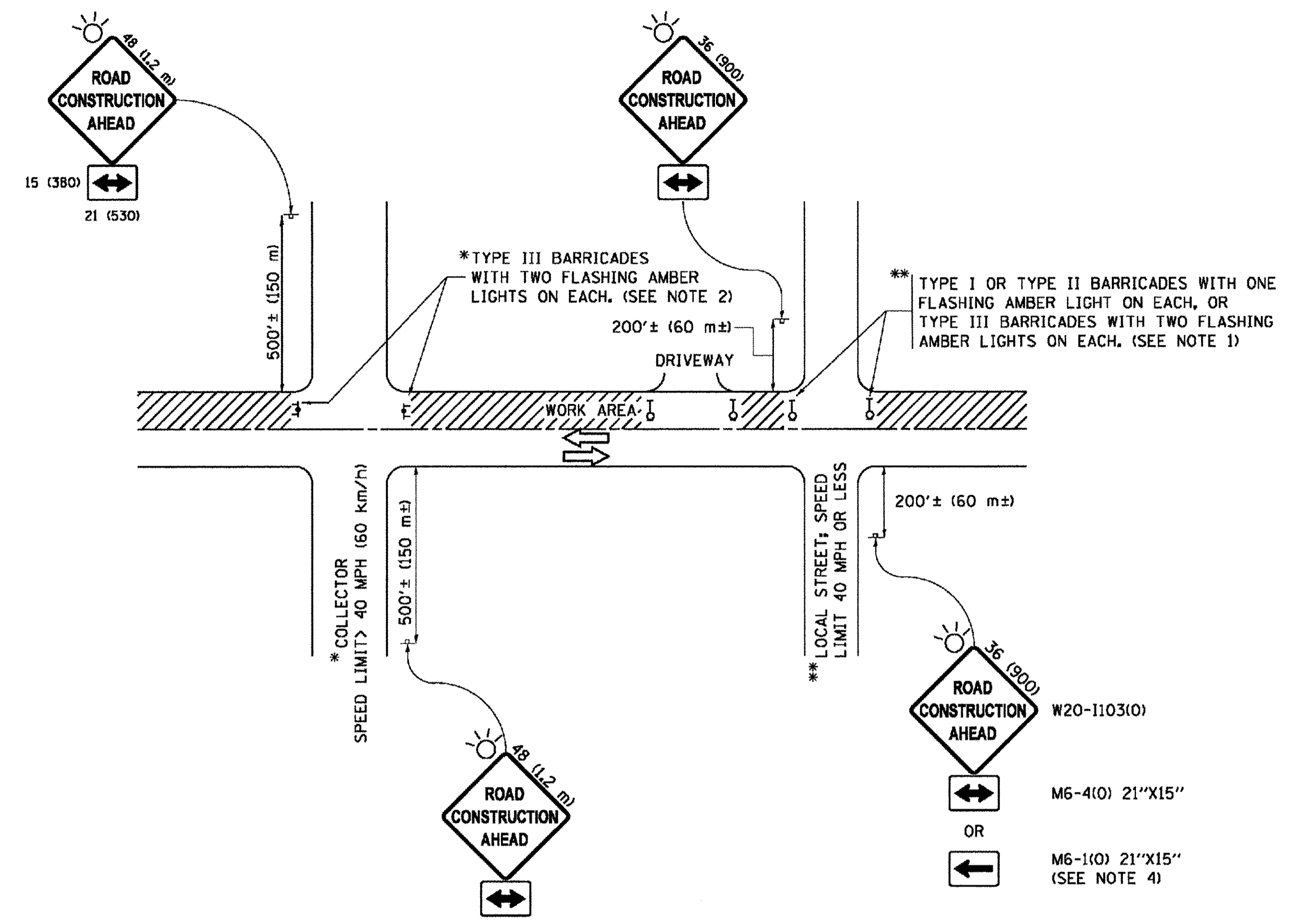
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PROJECT CONTACT: # PROJECT CONTACTS  
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		CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>		F.A.U. RTE. 1661	SECTION 15-00062-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 17
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT
		BD400-05 BD32		CONTRACT NO. 61D81		



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: #COVPRVY JAMES  
 PROJECT CONTACTS: JAMES J. COVATTA  
 CLIENT: #218111  
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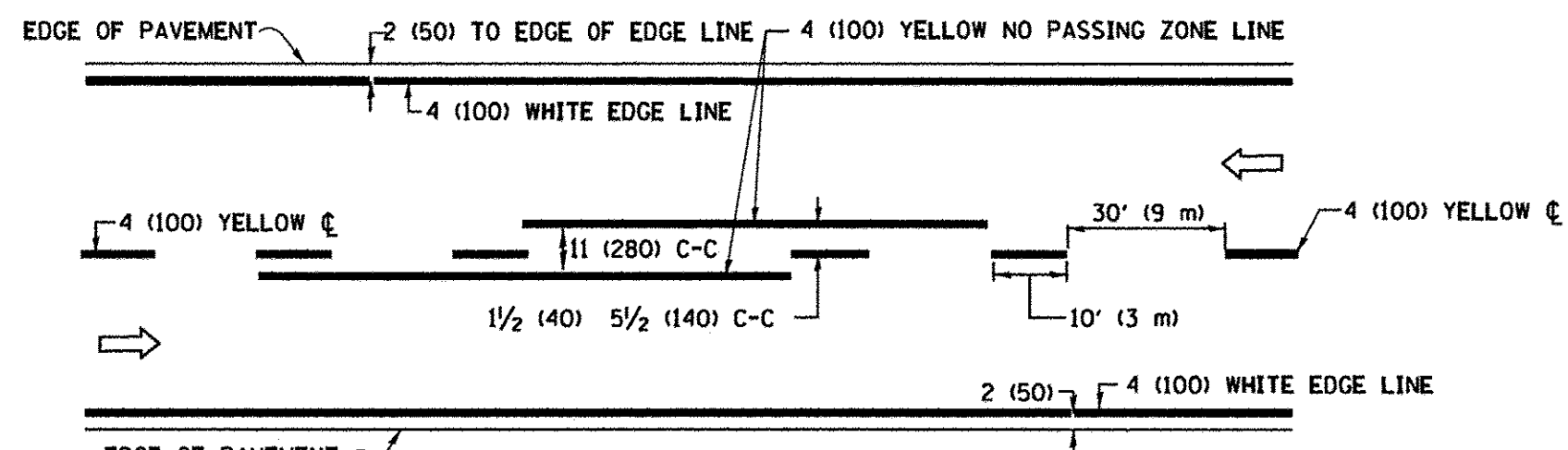
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

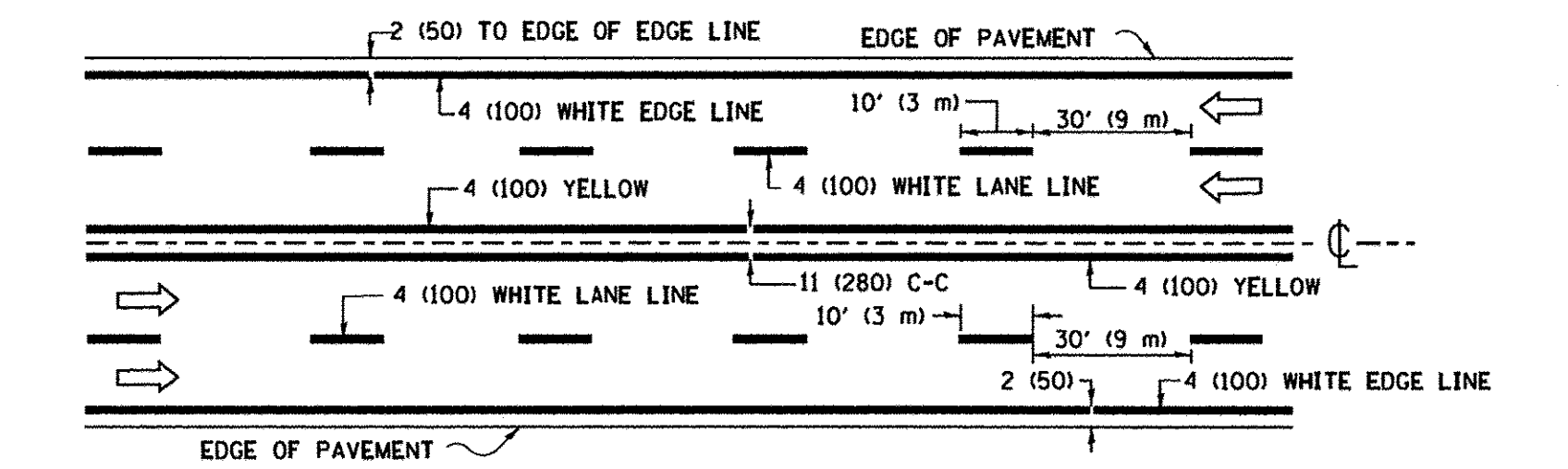
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 61D81	
ILLINOIS FED. AID PROJECT				

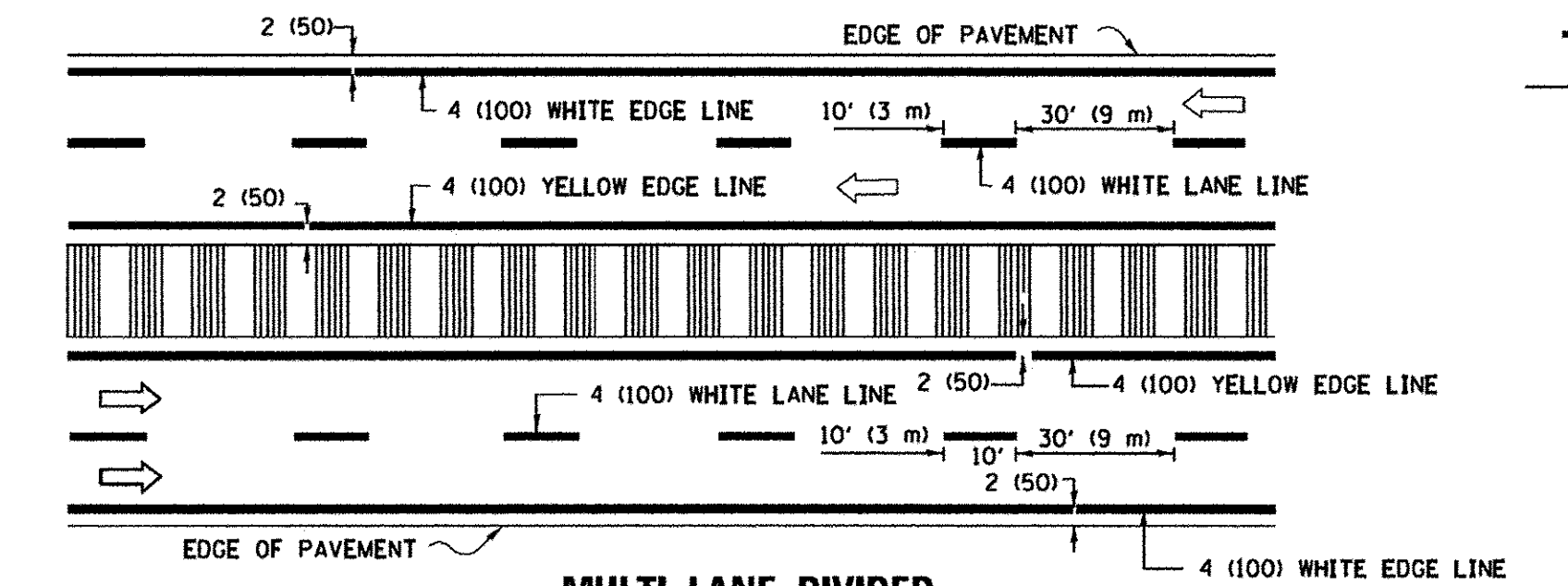




**2-LANE ROADWAY**

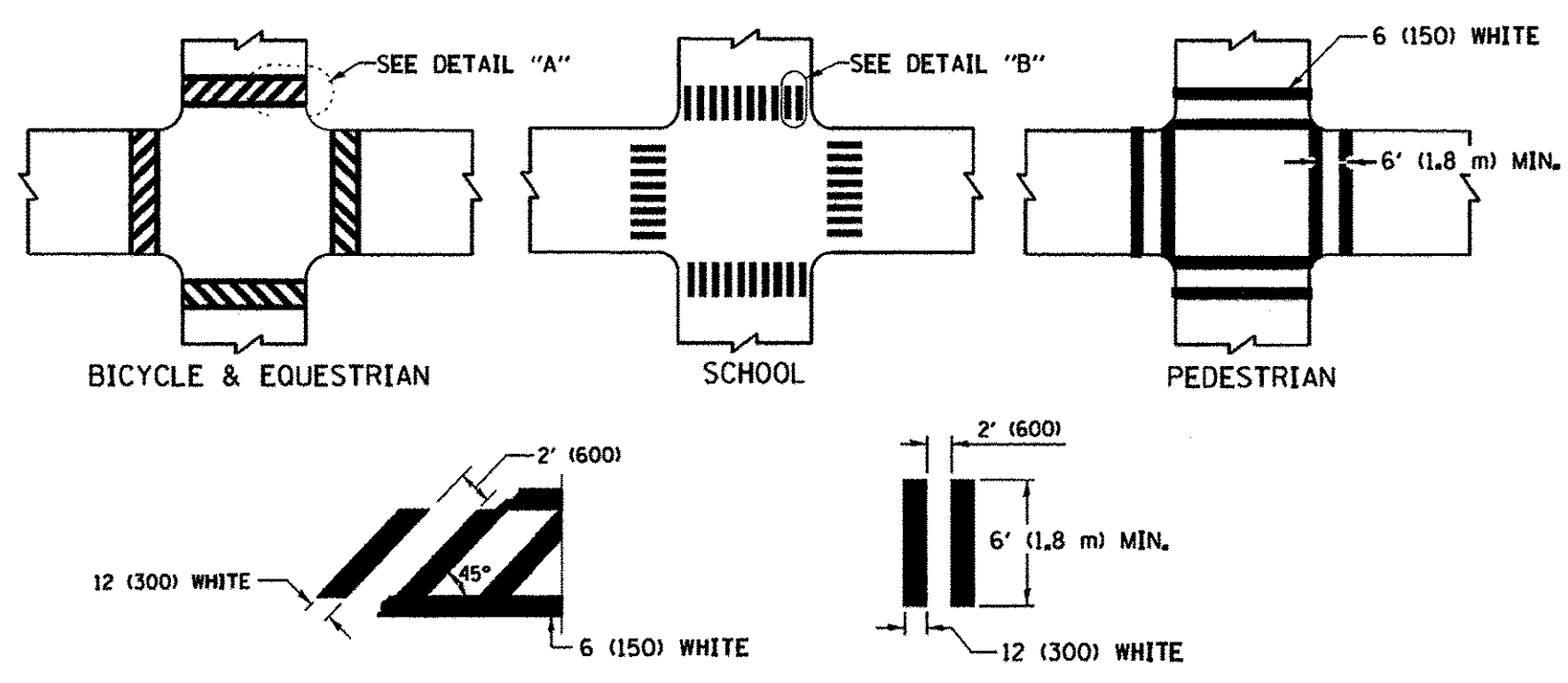


**MULTI-LANE UNDIVIDED**



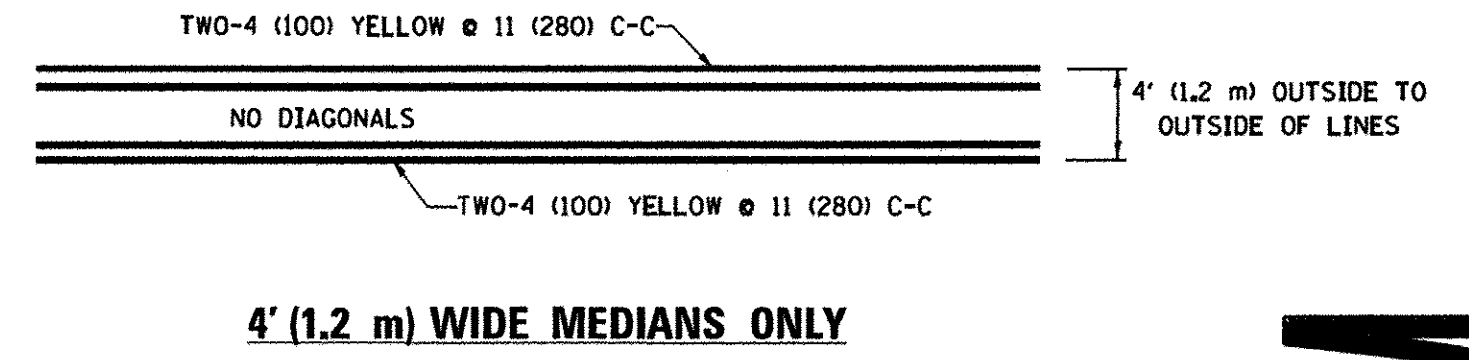
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

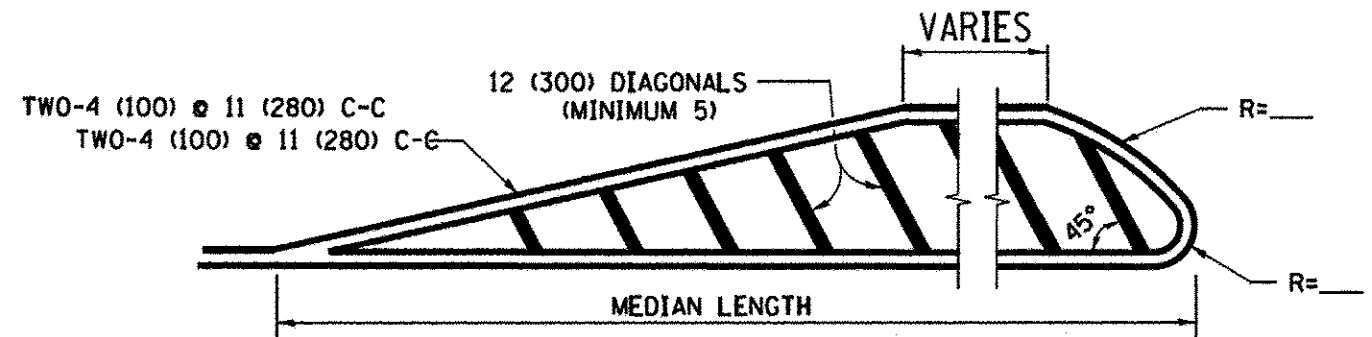


**TYPICAL CROSSWALK MARKING**

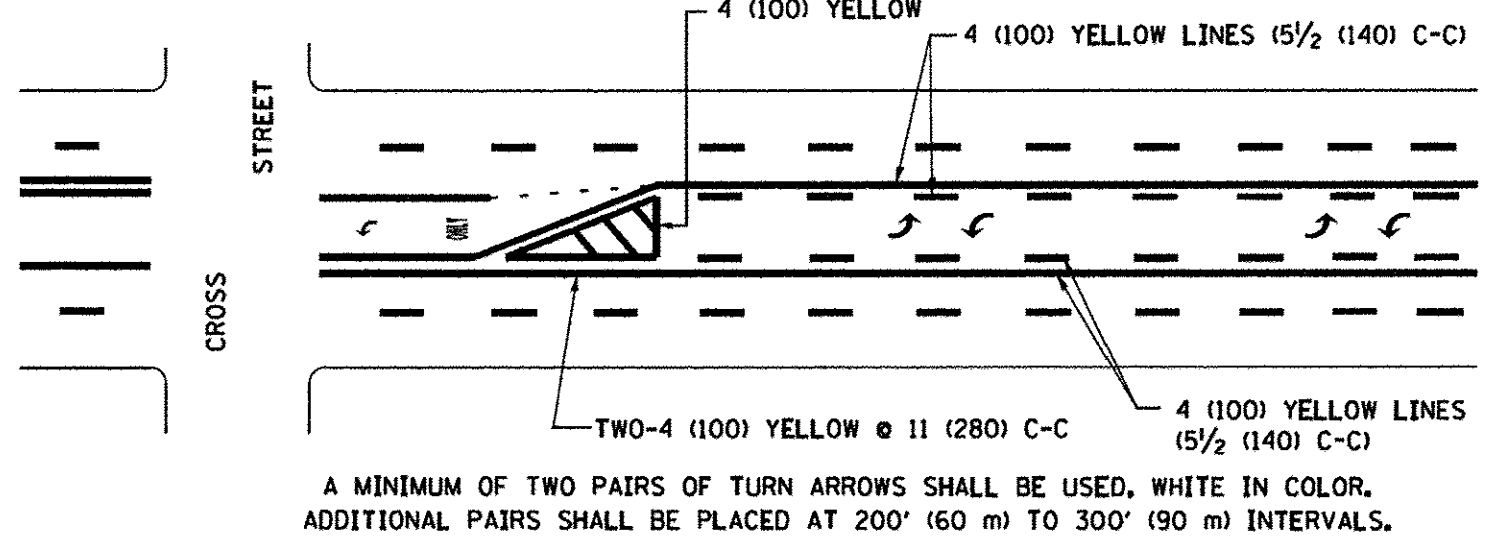
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



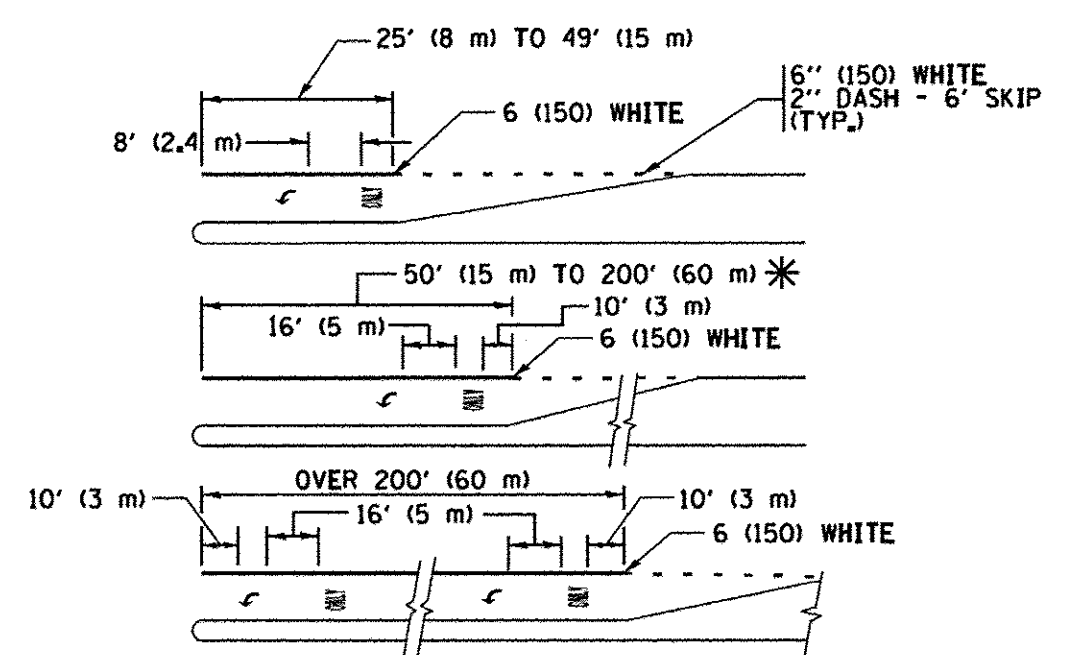
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**



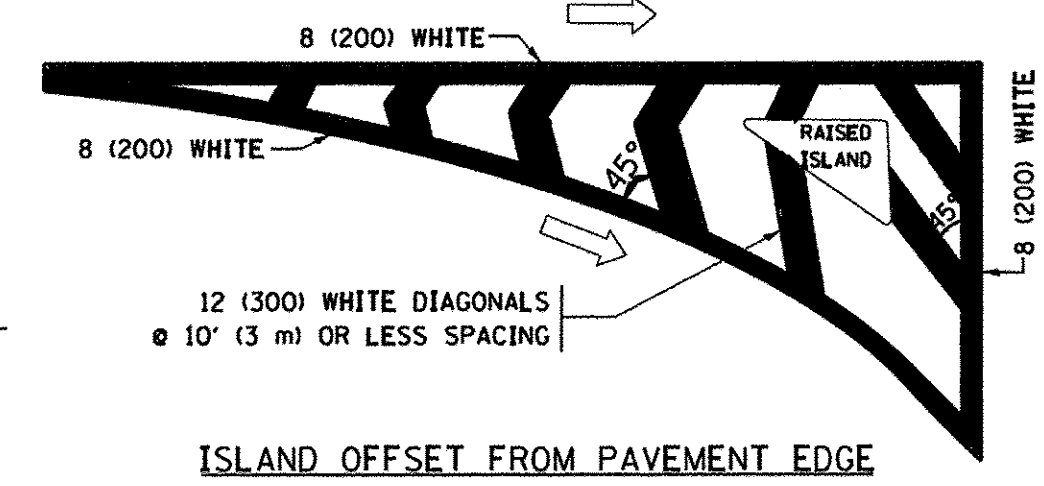
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



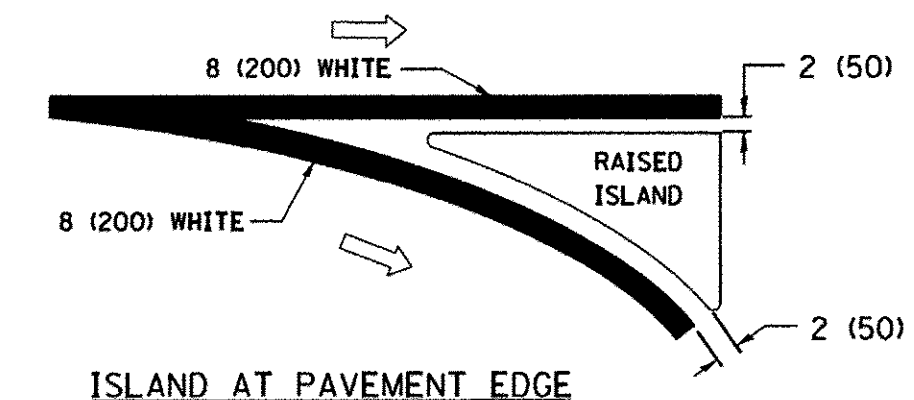
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

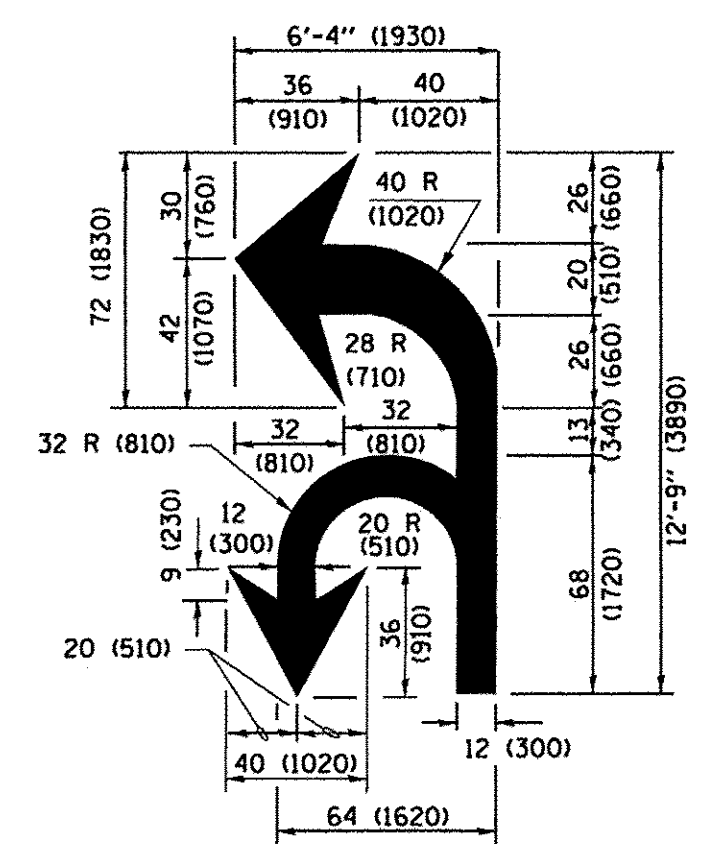
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



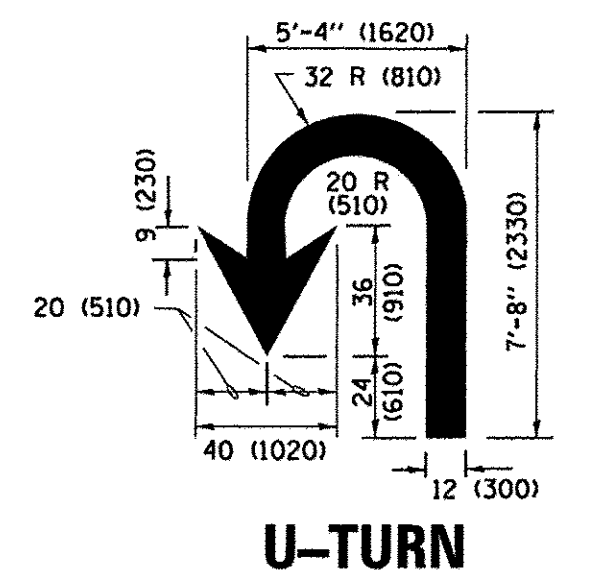
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**  
 \* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE. SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

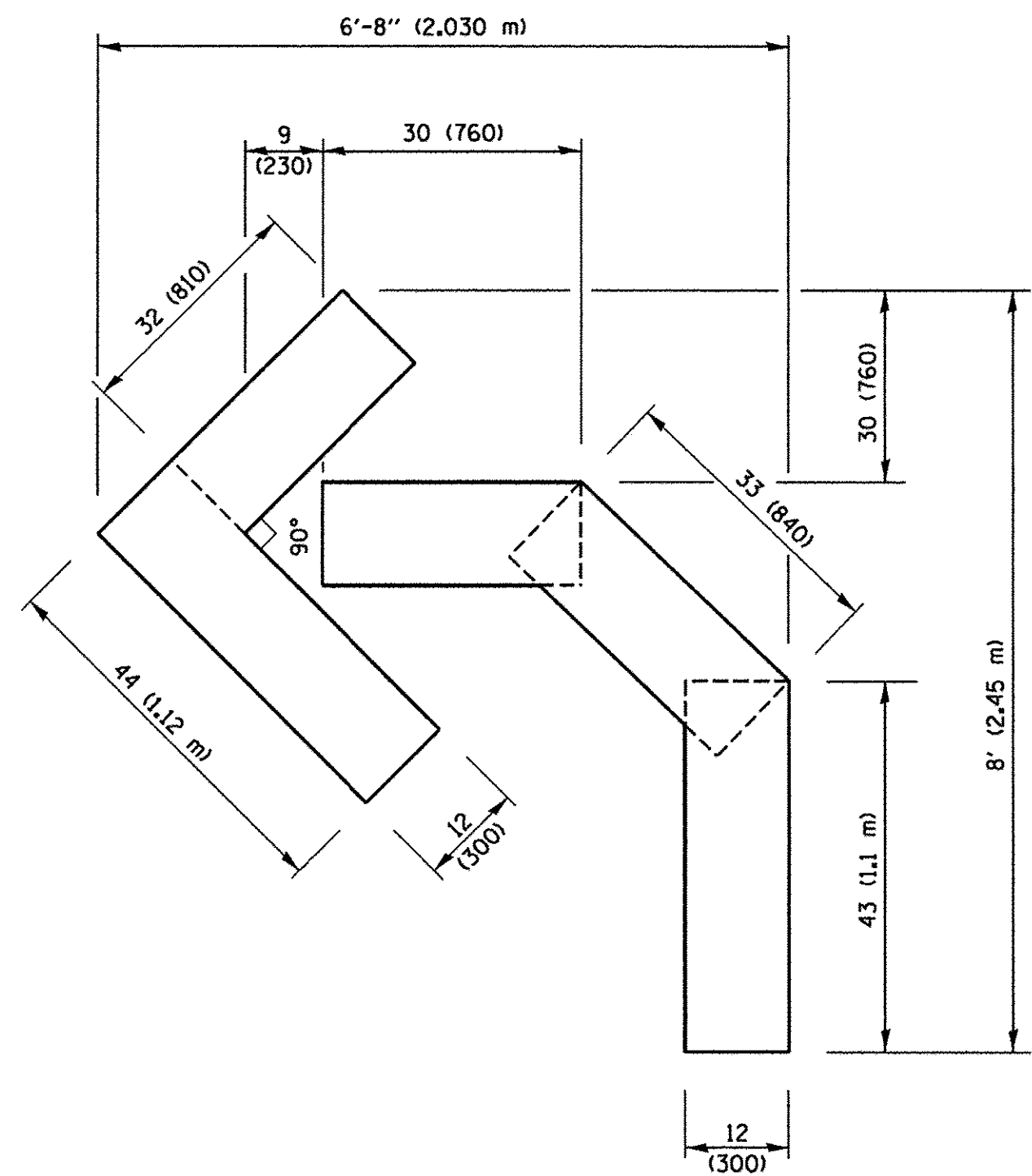
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		CHECKED -	REVISED - C. JUCIUS 07-01-13
		DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
			REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

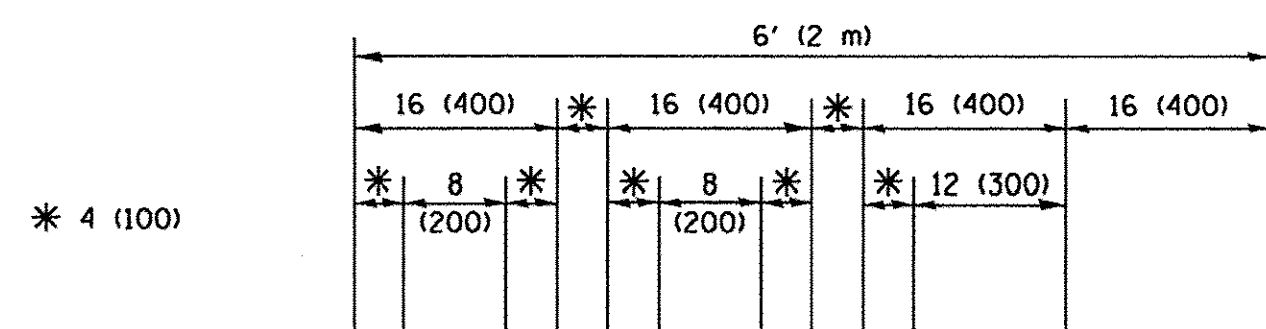
<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

F.A.U. RTE. 1661	SECTION 15-00062-00-RS	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 19
TC-13			CONTRACT NO. 61D81	
ILLINOIS FED. AID PROJECT				



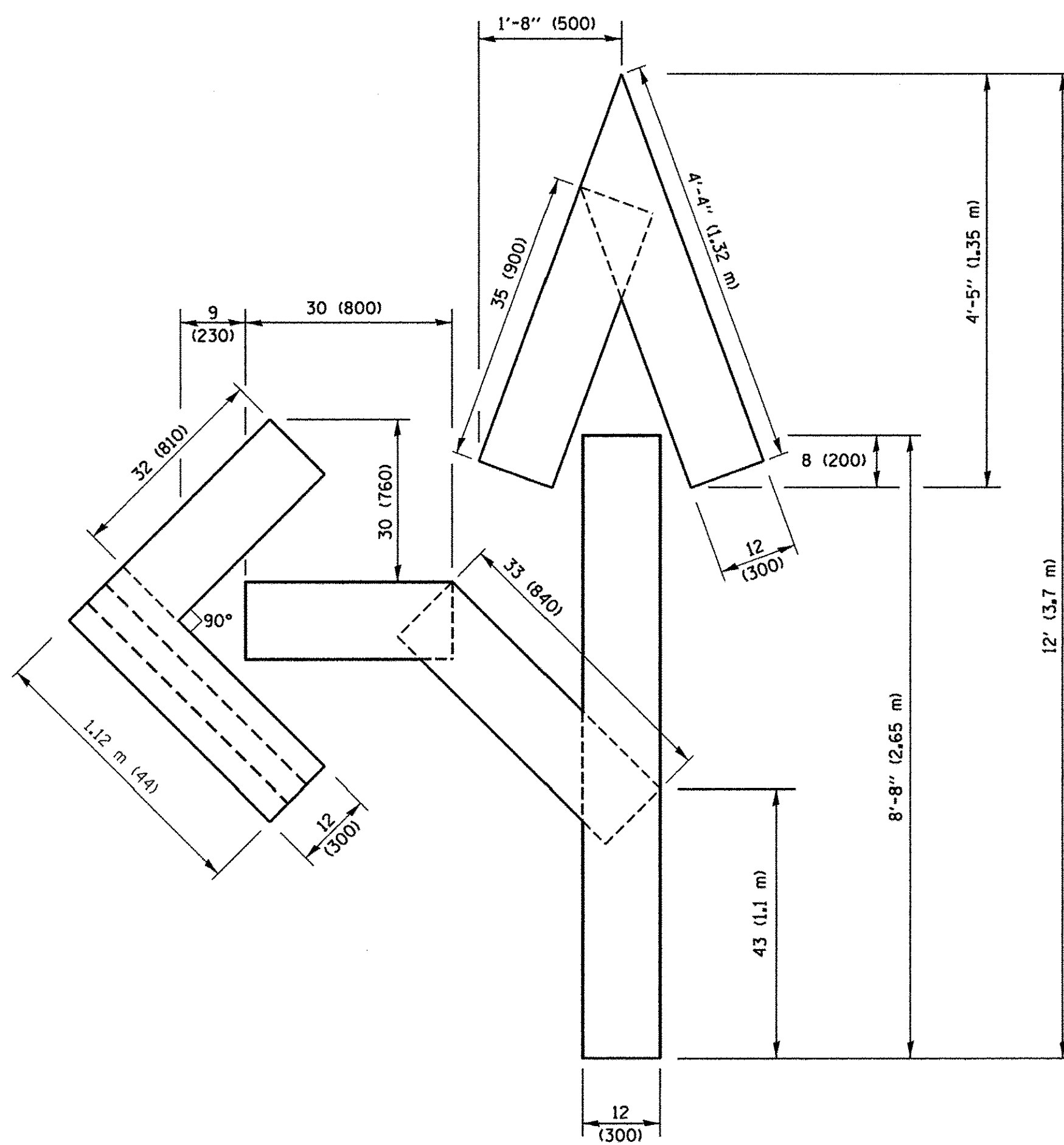
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)

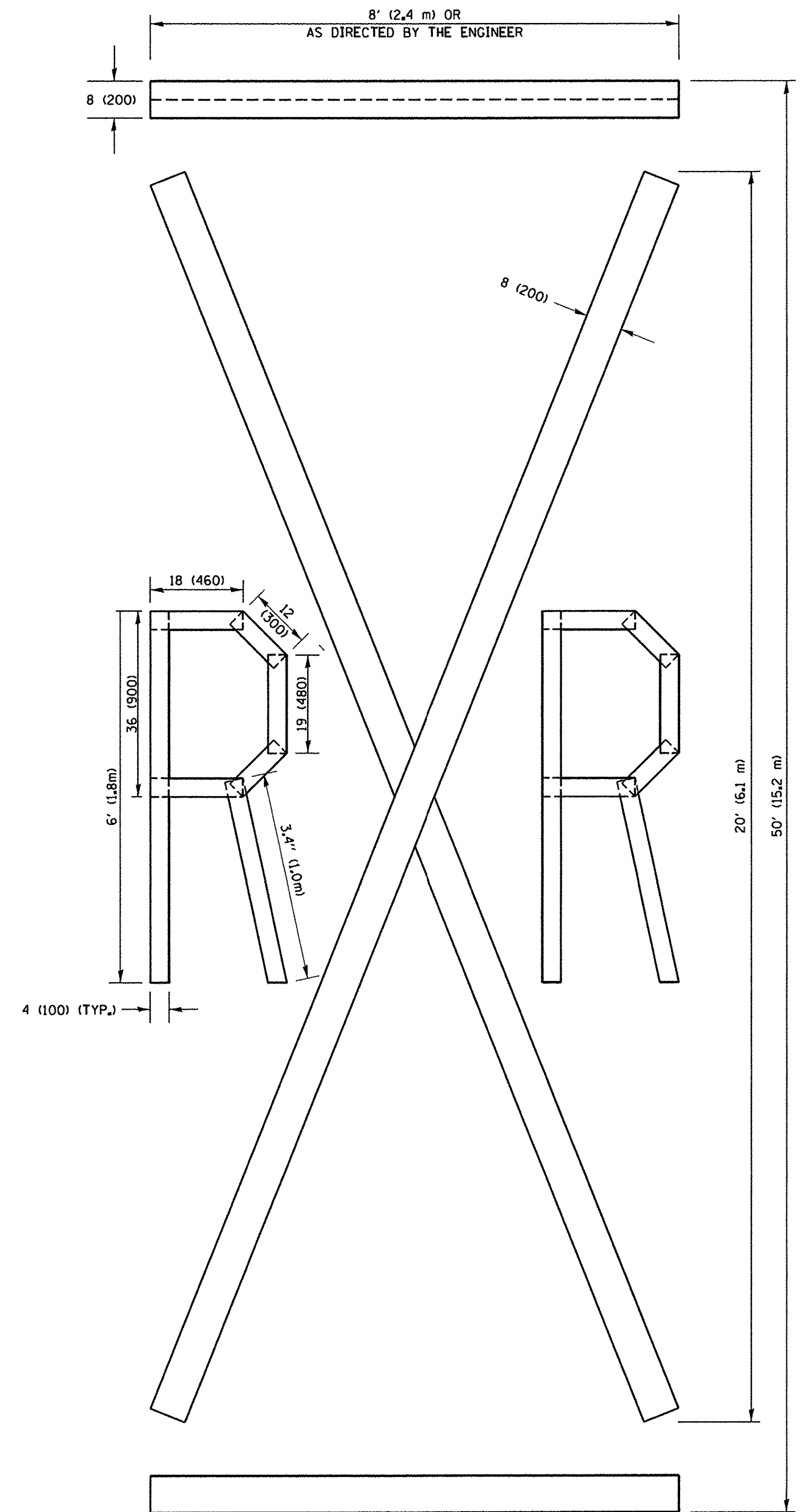


**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

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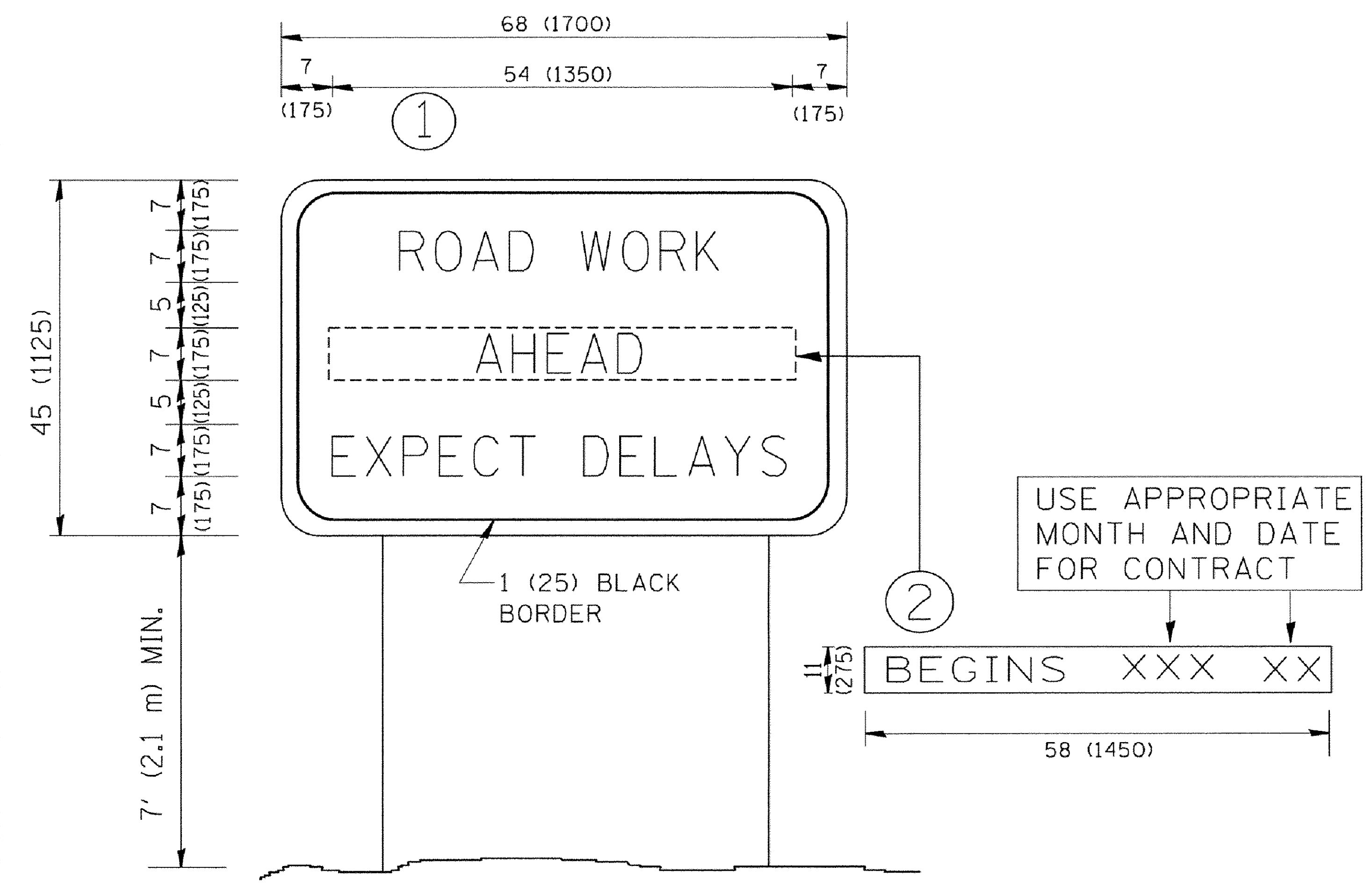
DESIGNED -  
 DRAWN -  
 CHECKED -  
 DATE -

REVISIONS:  
 1. R. MIRS 09-15-97  
 2. R. MIRS 12-11-97  
 3. T. RAMMACHER 02-02-99  
 4. C. JUCIUS 01-31-07

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD**  
**INFORMATION SIGN**  
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-22		CONTRACT NO. 61081		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



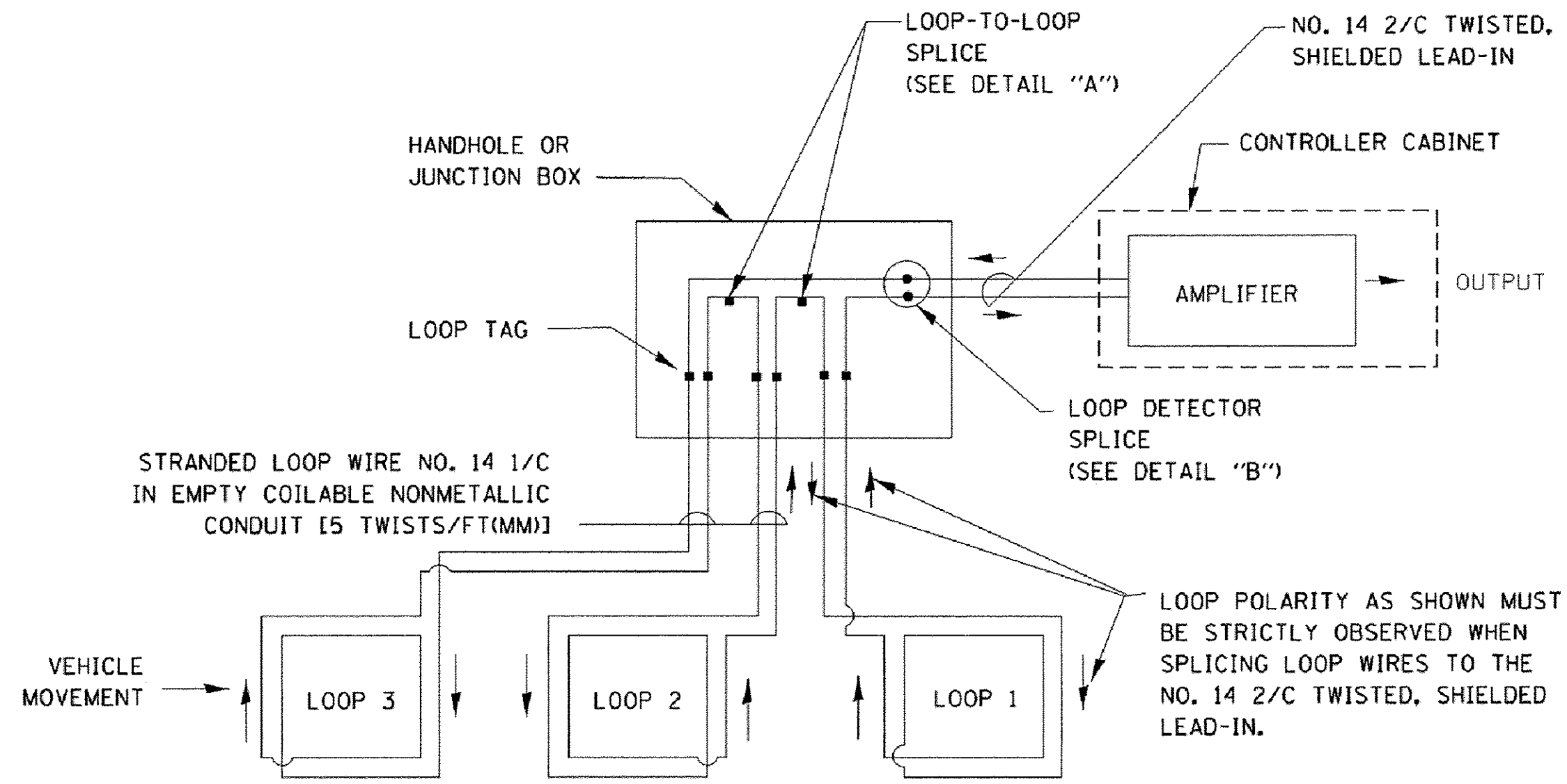
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

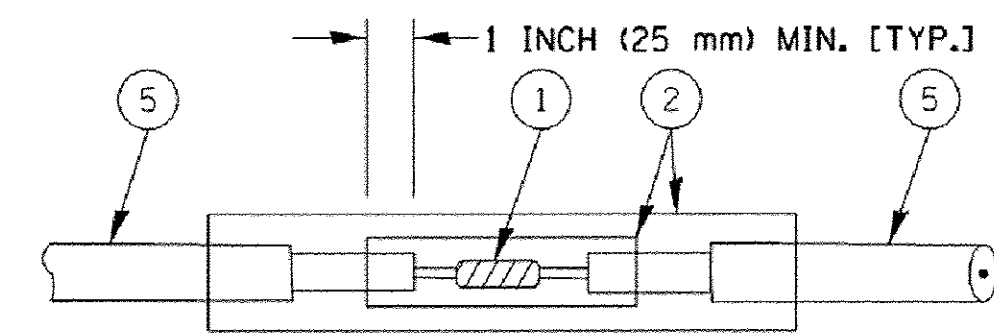
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

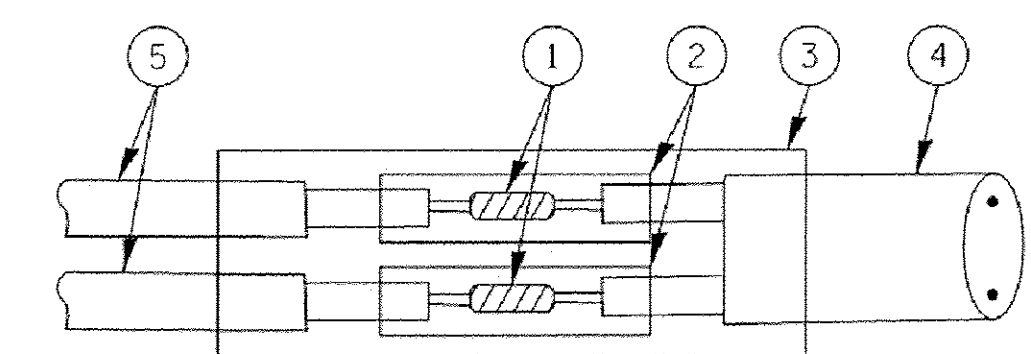


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm), IF IN CONCRETE. THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

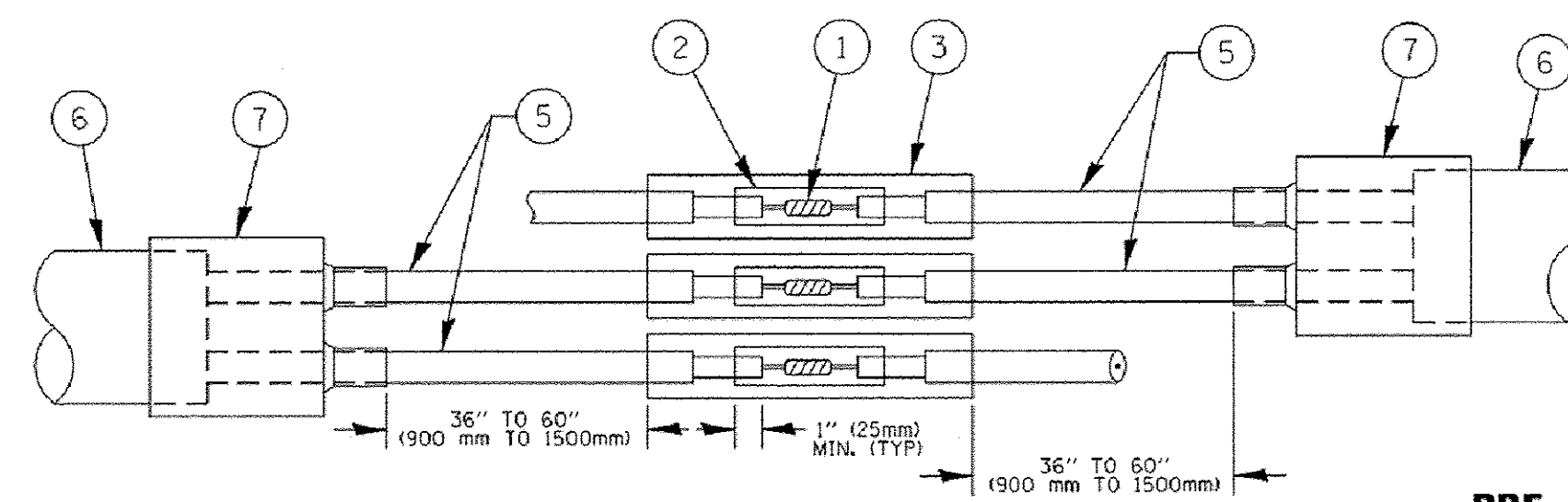


DETAIL "A"  
LOOP-TO-LOOP SPLICE

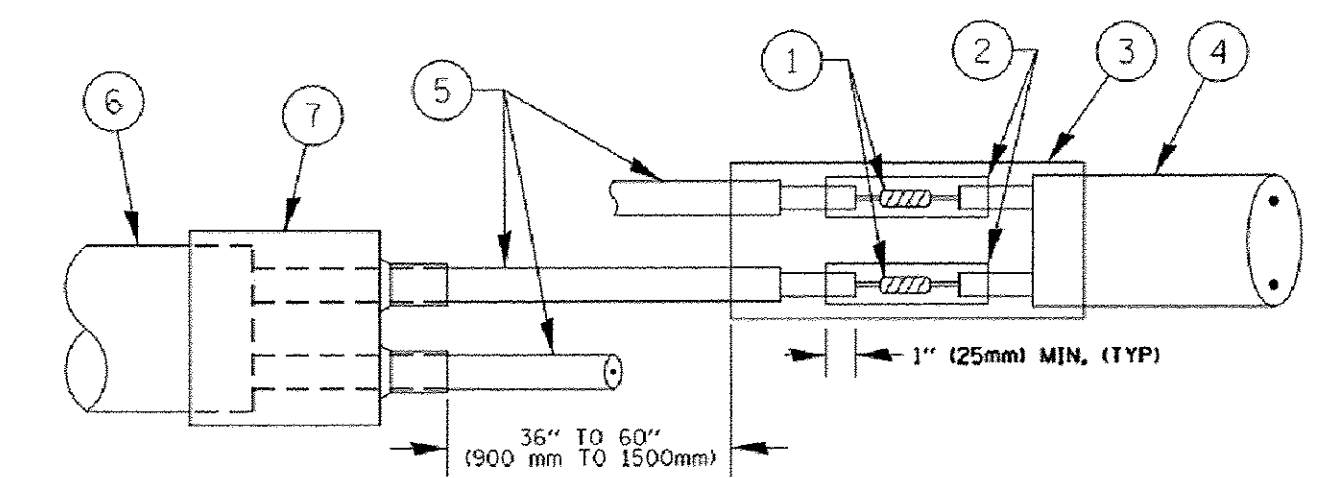


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



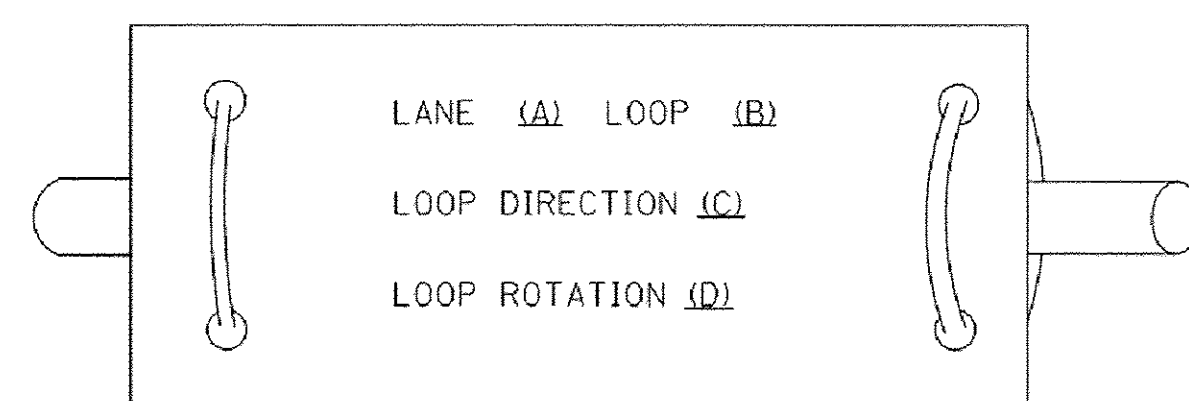
DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PREFORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

**LOOP LEAD-IN CABLE TAG**



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

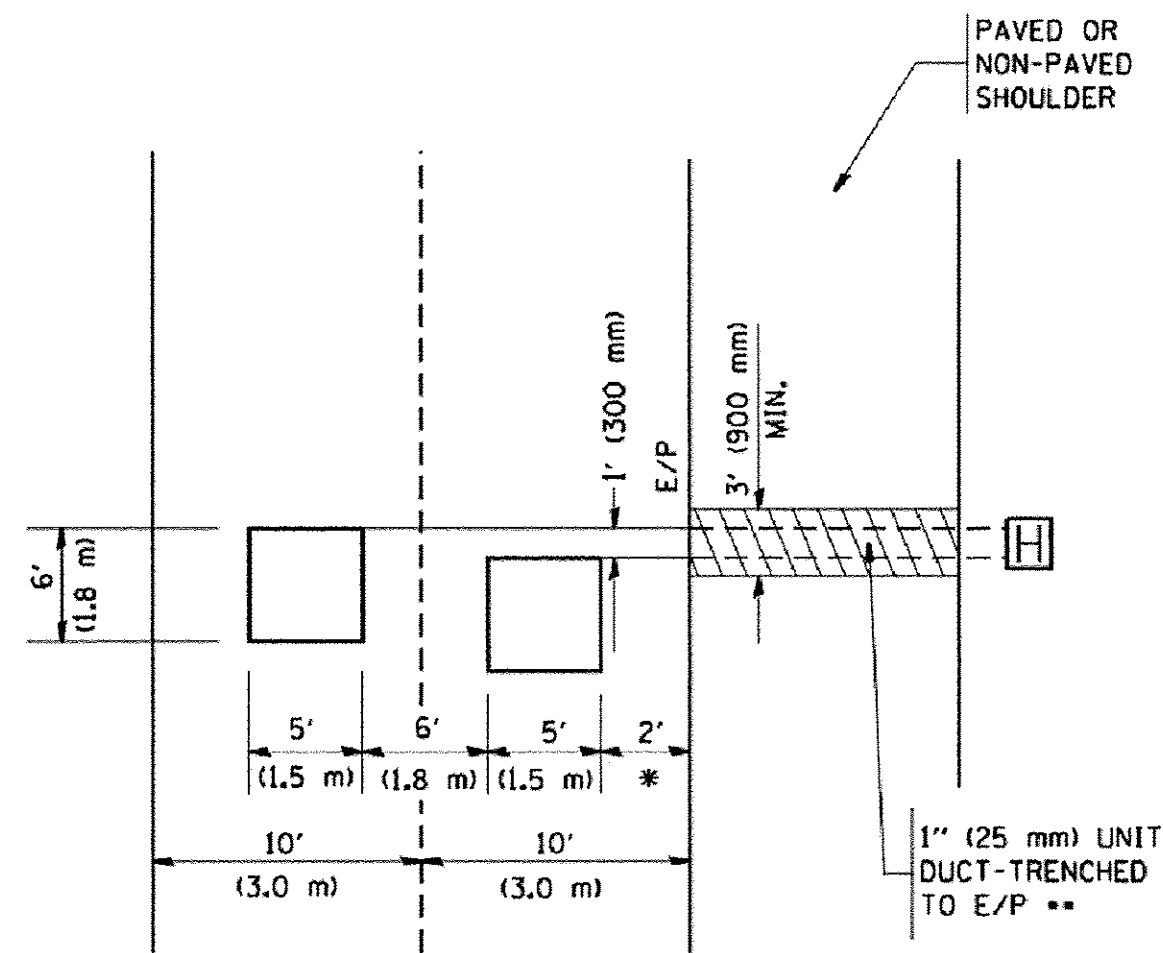
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1661	15-00062-00-RS	COOK	23	22
TS-05		CONTRACT NO. 61D81		
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

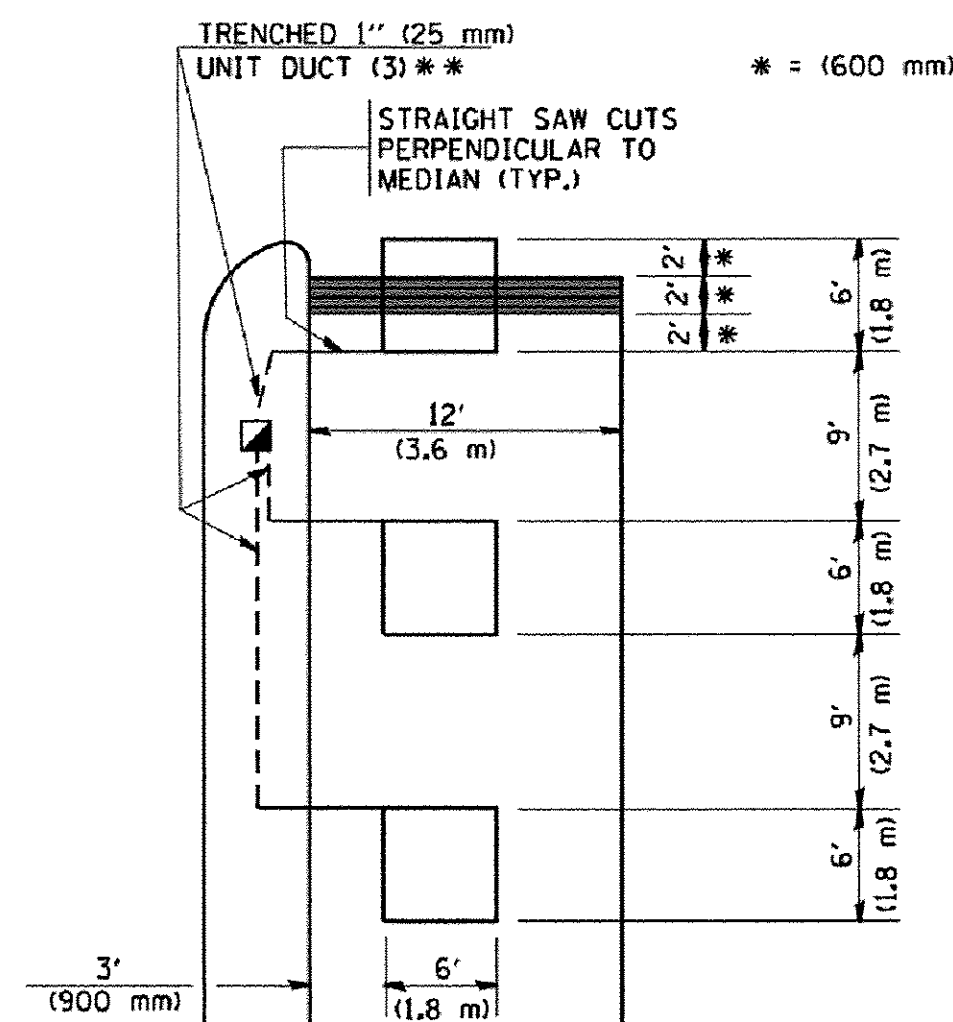


\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**

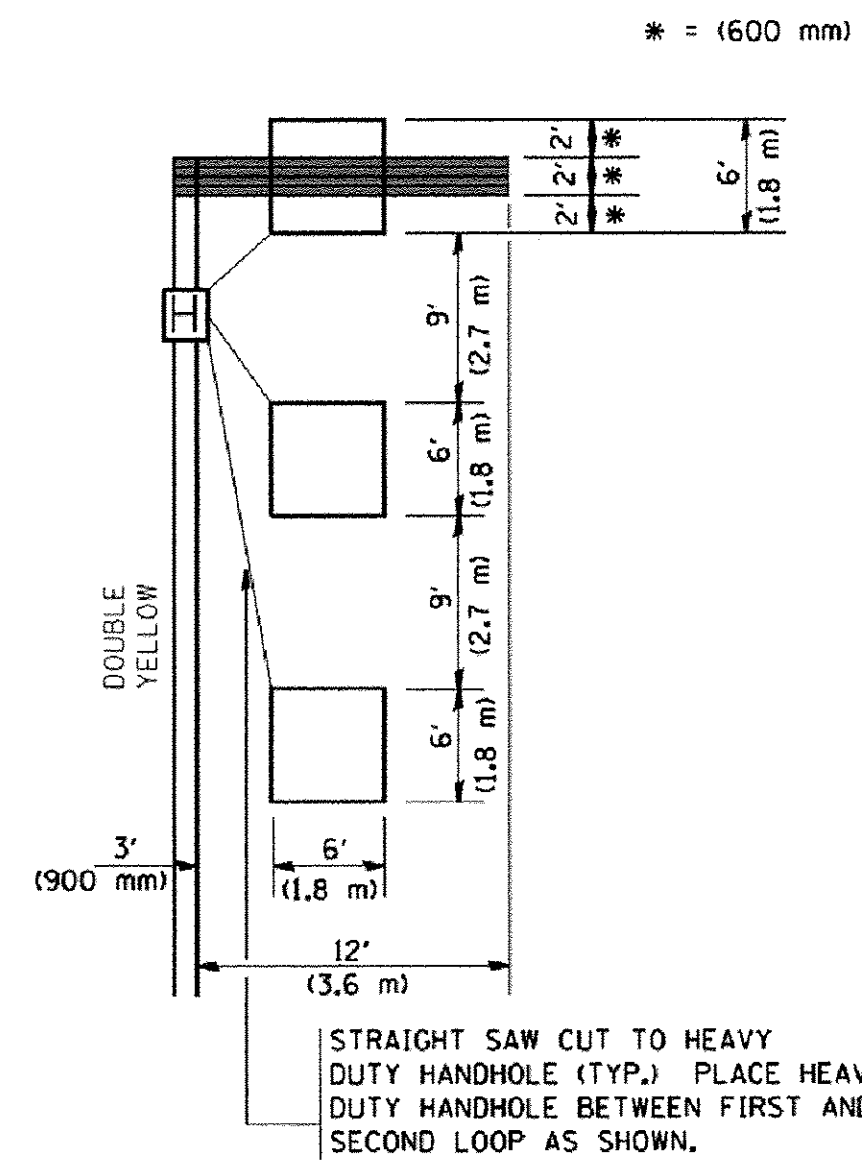
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

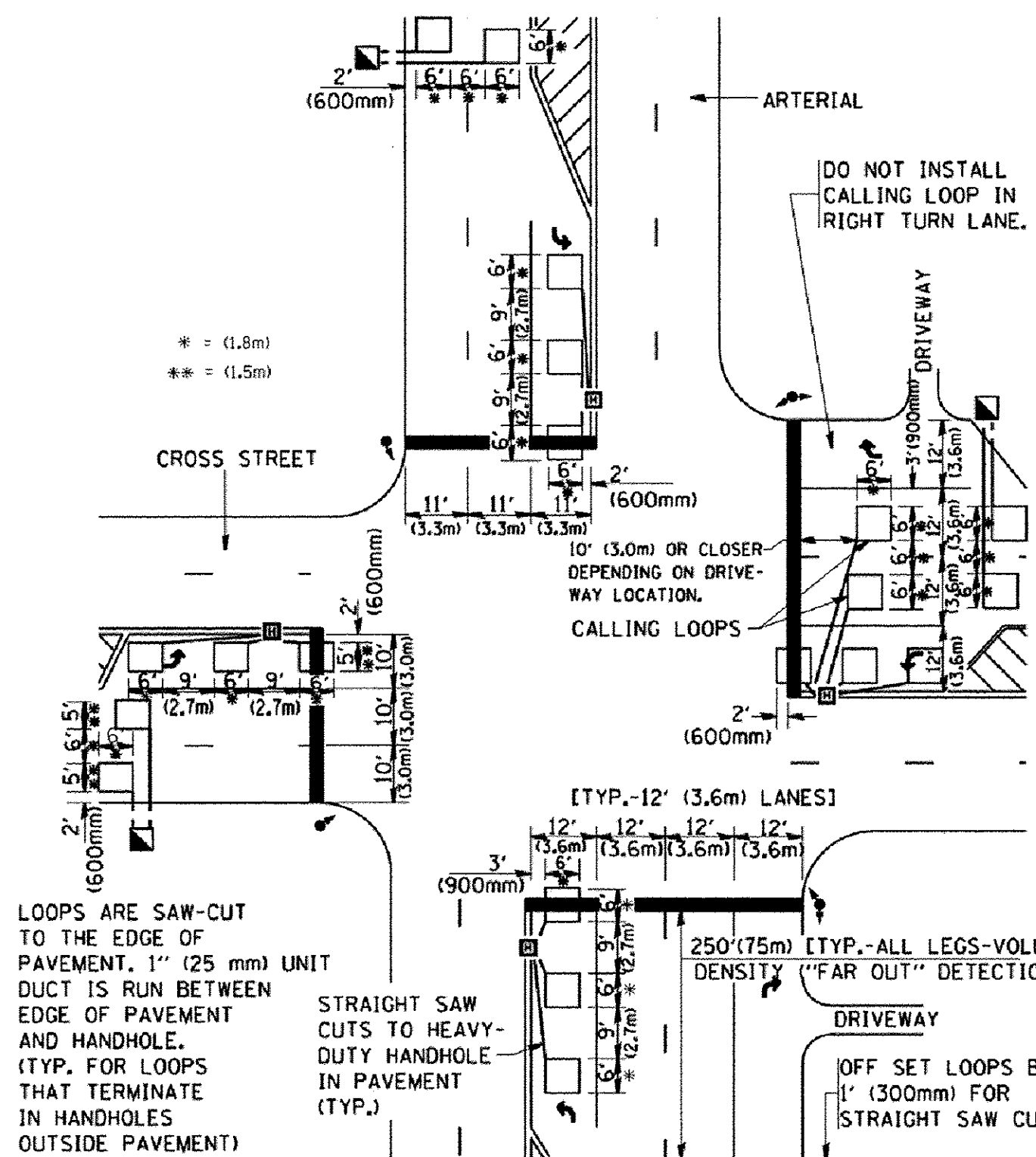
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH  
(PROTECTED / PERMITTED LEFT TURN PHASING)**



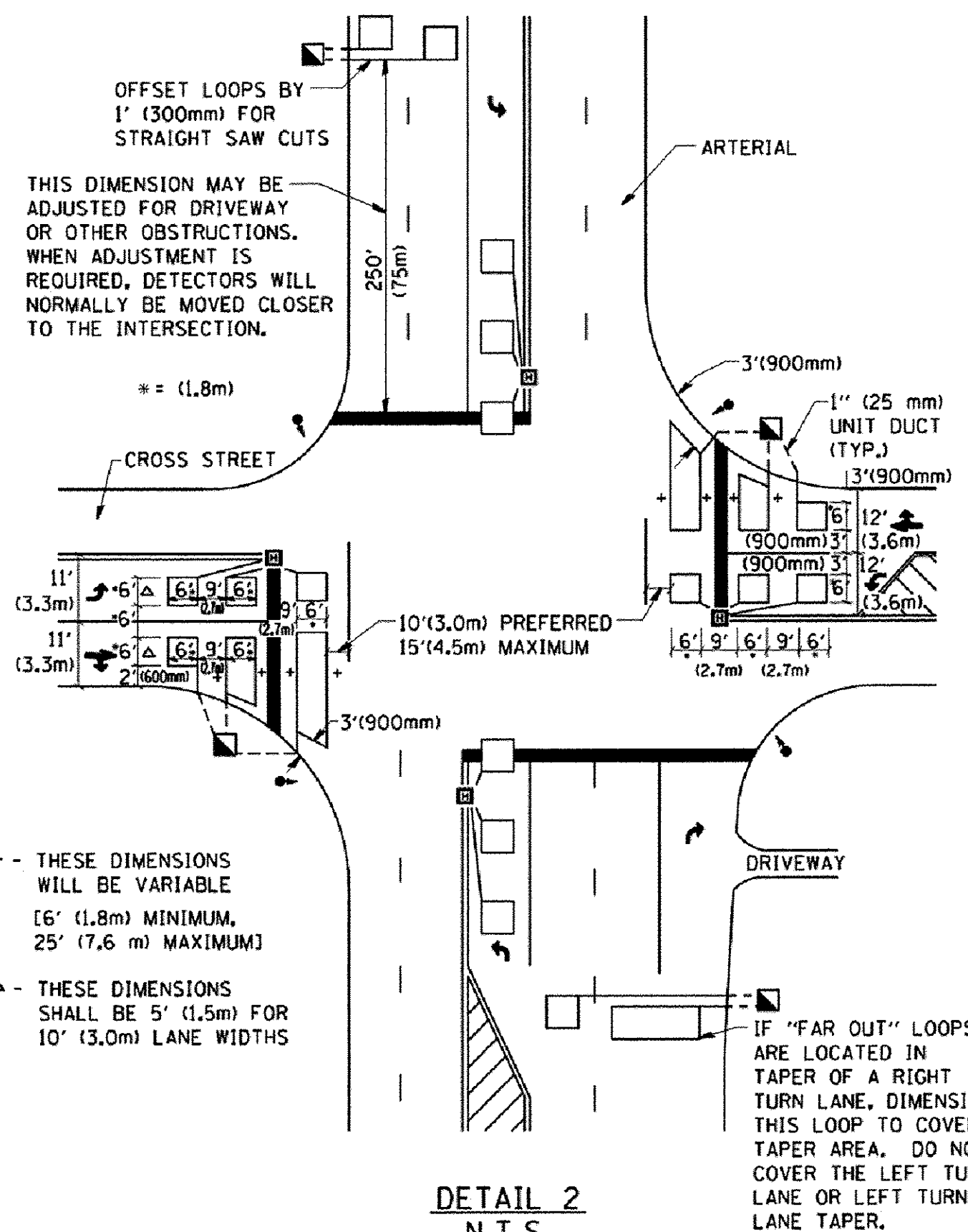
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1  
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2  
N.T.S.**

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		CHECKED - R.K.J.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
166L	15-00062-00-RS	COOK	23	23
TS-07			CONTRACT NO. 61D81	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				