

MH024

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JANUARY 6, 2017

REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2

OWNER



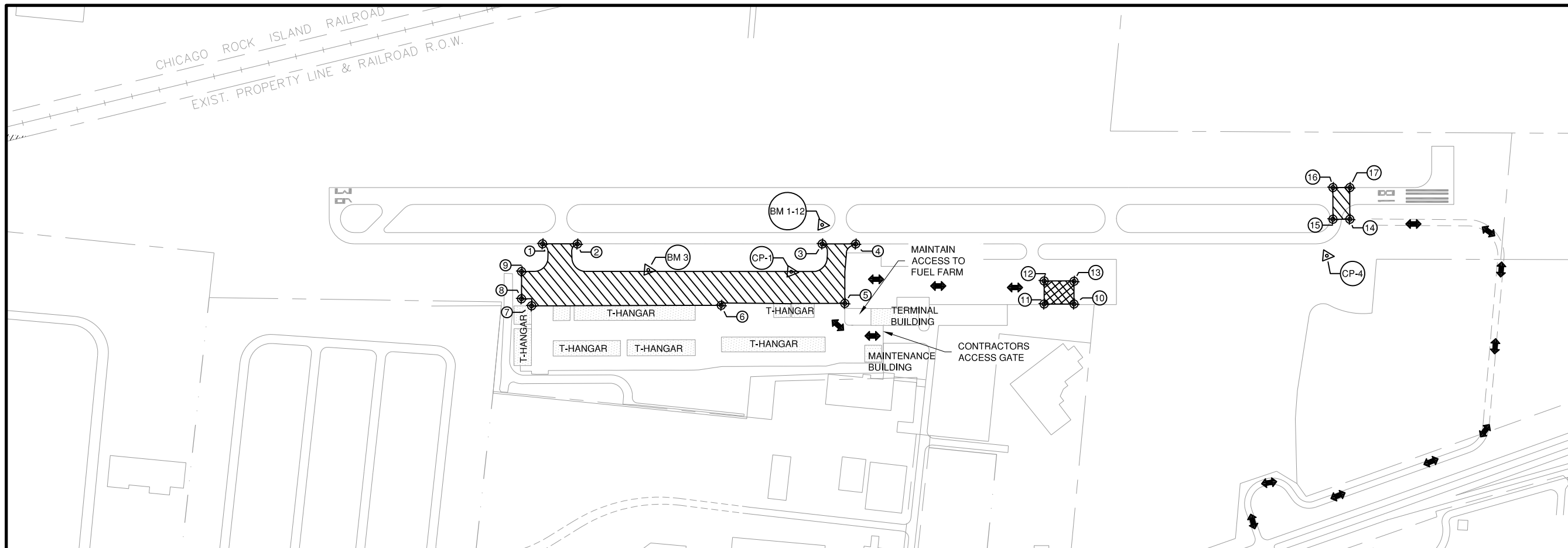
METROPOLITAN AIRPORT
AUTHORITY OF PEORIA
MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJECT NO.	3-17-SBGP-111
IL PROJECT NO.	3MY-4330
CMT PROJECT NO.	15061-06-00
CAD DWG FILE:	1506106-GH101.DWG
DESIGNED BY:	JMW
DRAWN BY:	DPA
CHECKED BY:	CHK
APPROVED BY:	APR
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SHEET TITLE

SITE PLAN



GENERAL NOTES

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLANS.
- THE CONTRACTOR SHALL CONFIRM WITH THE AIRPORT THROUGH THE RESIDENT ENGINEER THAT ALL RUNWAYS, TAXIWAYS, AND APRONS WITHIN THE CONSTRUCTION LIMITS HAVE BEEN CLOSED TO AIRCRAFT TRAFFIC PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL HAVE PERSONNEL CAPABLE OF MONITORING AIRCRAFT COMMUNICATIONS. THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY COMPLETED RADIO TRAINING ARE TO MONITOR THESE RADIOS.
- WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN. THE MAXIMUM HEIGHT OF EQUIPMENT, MATERIALS AND STOCKPILES SHALL BE 25' ABOVE GROUND ELEVATION.
- BROKEN ASPHALT, ASPHALT MILLINGS, PCC AND OTHER CONSTRUCTION DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF OF AIRPORT PROPERTY UNLESS OTHERWISE DIRECTED BY THE AIRPORT.
- VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 45' FROM THE CENTERLINE OF ACTIVE TAXIWAYS OR 200' FROM THE CENTERLINE OF ACTIVE RUNWAYS.
- ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS OUTSIDE OF THE DESIGNATED GRADING LIMITS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.
- THE CONTRACTOR SHALL CONTINUOUSLY CLEAN CONSTRUCTION AREAS WHICH WILL BE OPENED TO AIR TRAFFIC.
- IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS SO AS TO AVOID ANY DAMAGE. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
- CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - THE CONTRACTOR'S ACCESS TO THE WORK AREA SHALL BE AS SHOWN IN THE PLANS.
 - THE CONTRACTOR SHALL USE AN EXISTING GATE(S) FOR ACCESS TO THE AIRFIELD. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT FOR OPERATION OF THE ACCESS GATE.
 - CONSTRUCTION TRAFFIC TO THE WORK ON THE RUNWAY SHALL BE FROM LINDBERGH DRIVE. CONSTRUCTION TRAFFIC SHALL NOT USE AIRCRAFT PAVEMENT TO ACCESS WORK ON RUNWAY.
 - CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.
 - DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED DURING WORK HOURS.
 - THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE(S) UPON LEAVING THE SITE.
 - ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY, SHALL BE AT THE CONTRACTOR'S RESPONSIBILITY.
 - THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS NOT TO VIOLATE AIRPORT PART 77 SURFACES, OR RUNWAY SAFETY AREAS AND TAXIWAY OBJECT FREE AREAS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEANING ACTIVE AIRFIELD PAVEMENTS WHICH ARE USED BY THE CONTRACTOR'S VEHICLES ACCESSING THE WORK SITE OR DEPARTING THE WORK SITE IMMEDIATELY FOLLOWING SAID VEHICLE.

LEGEND

- CONTRACTOR'S ACCESS ROUTE
- WORK AREA
- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
- CRITICAL POINT
- CONTROL POINT

CONTROL POINT TABLE

POINT	NORTHING	EASTING	ELEVATION
BM 1-12	1504023.403	2449871.309	766.91
BM 3	1503422.649	2450088.438	769.05
CP-1	1503929.464	2450047.465	768.86
CP-4	1505820.825	2449812.784	779.85

CRITICAL POINT TABLE

POINT	LATITUDE	LONGITUDE	ELEVATION
1	N40° 40' 21.29"	W83° 03' 00.87"	768.74
2	N40° 40' 22.51"	W83° 03' 00.92"	768.27
3	N40° 40' 31.09"	W83° 03' 01.29"	768.11
4	N40° 40' 32.26"	W83° 03' 01.34"	768.06
5	N40° 40' 31.95"	W83° 02' 58.59"	770.11
6	N40° 40' 27.62"	W83° 02' 58.32"	770.66
7	N40° 40' 20.98"	W83° 02' 58.00"	771.26
8	N40° 40' 20.61"	W83° 02' 58.31"	770.50
9	N40° 40' 20.58"	W83° 02' 59.56"	769.17
10	N40° 40' 39.98"	W83° 02' 58.92"	769.00
11	N40° 40' 38.93"	W83° 02' 58.87"	769.00
12	N40° 40' 38.91"	W83° 02' 59.93"	768.00
13	N40° 40' 39.95"	W83° 02' 59.96"	768.00
14	N40° 40' 49.54"	W83° 03' 03.24"	783.88
15	N40° 40' 48.95"	W83° 03' 03.21"	783.78
16	N40° 40' 48.92"	W83° 03' 04.67"	784.47
17	N40° 40' 49.51"	W83° 03' 04.70"	785.65

UNICOM FREQUENCY - 122.7
MAXIMUM HEIGHT OF EQUIPMENT - 25'

1. GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2F OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2F OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

2. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

3. PHASING

1. TOTAL CONTRACT TIME SHALL BE 50 CALENDAR DAYS.
2. PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEETS.

4. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
4. FLAGMEN REQUIRED AS NOTED ON CONSTRUCTION ACTIVITY PLAN SHEETS.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE SHOWN.
3. CERTAIN CONTRACTOR EMPLOYEES MAY BE REQUIRED TO OBTAIN AN AIRPORT IDENTIFICATION BADGE. THIS CONSISTS OF FILLING OUT ALL NECESSARY PAPERWORK, FINGERPRINTING, ATTENDING AND PASSING A TRAINING CLASS CONCERNING SAFETY AND SECURITY AT THE AIRPORT. CONTRACTOR EMPLOYEES MUST MEET CERTAIN BACKGROUND CHECK CRITERIA AND THE CONTRACTOR MUST MAKE CERTAIN CERTIFICATION ABOUT EACH EMPLOYEE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FINGERPRINTING COSTS. ALL COSTS ASSOCIATED WITH OBTAINING THE IDENTIFICATION BADGE SHALL BE BORNE BY THE CONTRACTOR.
4. CONTRACTOR WORK CREWS MUST MONITOR THE UNICOM FREQUENCY AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS. BE AWARE OF TENANT AIRCRAFT MOVEMENT NEAR THE WORK AREAS.
5. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE SITE PLAN. AREA SHALL BE FENCED OFF WITH APPROVED BARRIER MATERIALS.
6. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
7. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
8. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
9. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
10. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS AND STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. THE CONTRACTOR SHALL NOTIFY THE AIRPORT 7 DAYS BEFORE STARTING WORK IN EACH PHASE. THIS WILL ENSURE THAT THE AIRPORT CAN CONTACT TENANTS ABOUT MOVING AIRCRAFT DURING THE TIME OF CONSTRUCTION.
4. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
6. CONTACTS FOR THIS PROJECT ARE AS DISCUSSED IN THE PRE-CONSTRUCTION MEETING AND AS LISTED BELOW:

DIRECTOR OF OPERATIONS
DOUG PALMER: 309-303-0994

AIRPORT MAINTENANCE
GREG HUSER: 309-303-1005

AIRPORT OPERATIONS
OPS CELL: 309-303-1001

3MY AIRPORT OPERATIONS
OFFICE NUMBER: 309-693-2372

ENGINEER
CHUCK TAYLOR, P.E. - PROJECT ENGINEER: 217-787-8050
CMT - RESIDENT ENGINEER: 217-787-8050

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2F OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR AND AIRPORT SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO COORDINATE FOR UTILITY ASSISTANCE. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2F OR LATEST EDITION AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE AIRPORT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.

15. PROTECTION

1. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 45' FROM THE TAXIWAY CENTERLINE, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

16. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE DIRECTED BY THE AIRPORT.



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CONSULTANTS

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REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2

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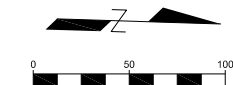
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SHEET TITLE
**CONSTRUCTION
SAFETY PHASING
NOTES**

GC001
SHEET 3 OF 19



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REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2

OWNER



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AUTHORITY OF PEORIA
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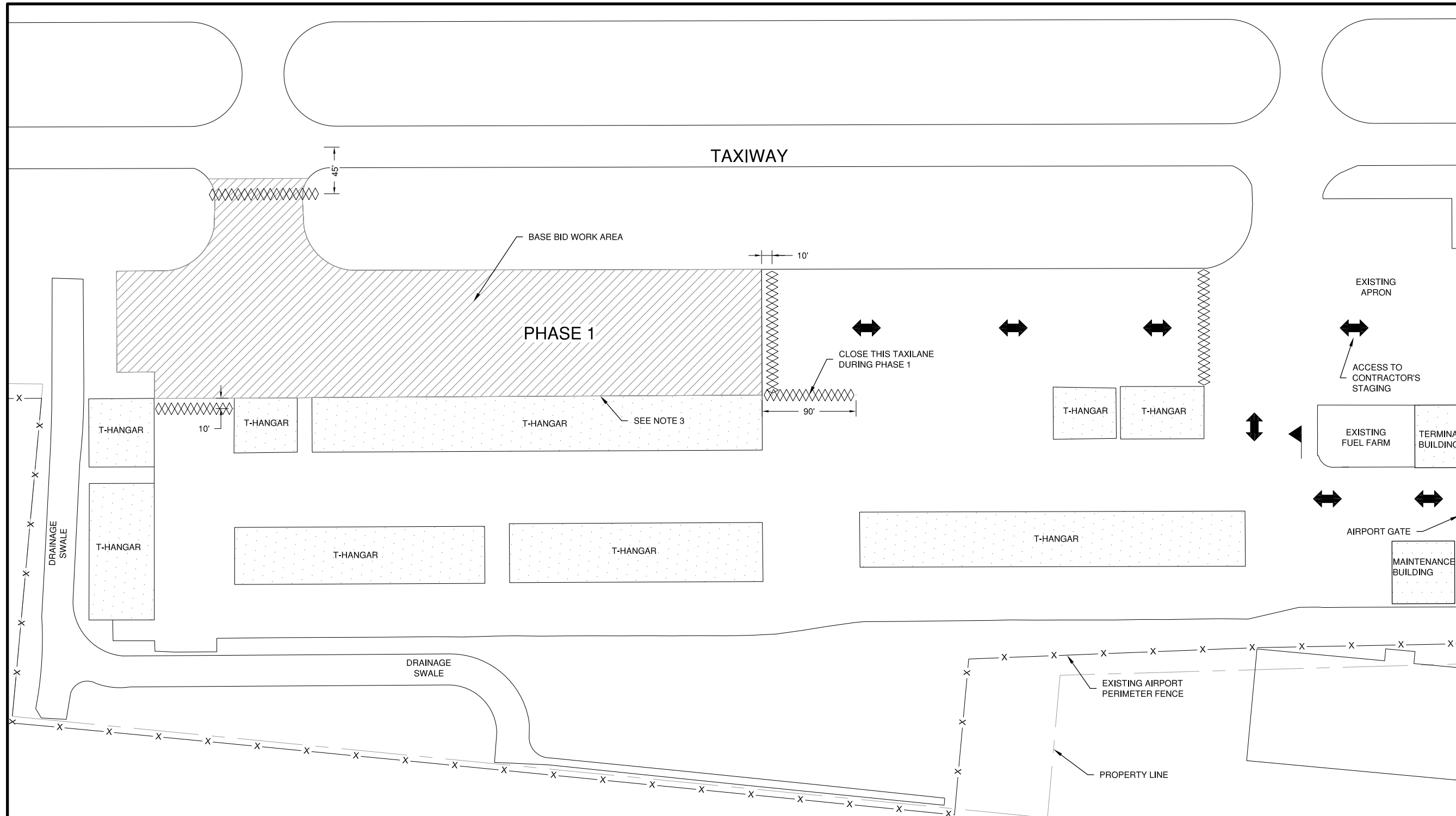
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SHEET TITLE

CONSTRUCTION
ACTIVITY PLAN 1

GC101

SHEET 5 OF 19



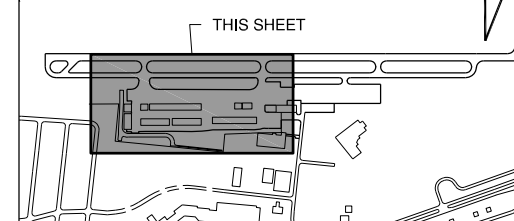
PHASE 1 NOTES:

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE WITH THE T-HANGAR TENANTS.
3. ACCESS TO THE T-HANGARS ON THE EAST LIMITS OF THE WORK SITE AREA SHALL BE MAINTAINED THROUGHOUT THE DURATION OF PHASE 1.
4. CONTRACTOR SHALL CONTINUOUSLY CLEAN THIS AREA FOR AIRCRAFT ACCESS.
5. AIRCRAFT AND AIRPORT VEHICLES HAVE RIGHT OF WAY.

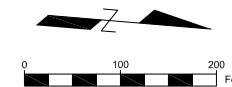
LEGEND

- PHASE 1 WORK LIMITS - BASE BID
- CONTRACTOR'S ACCESS ROUTE
- LOW PROFILE BARRICADES AS SHOWN ON CONSTRUCTION ACTIVITY PLAN
- EXISTING FENCE LINE
- FLAGMEN

KEYMAP



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TAXILANE PAVEMENT - PHASE 2

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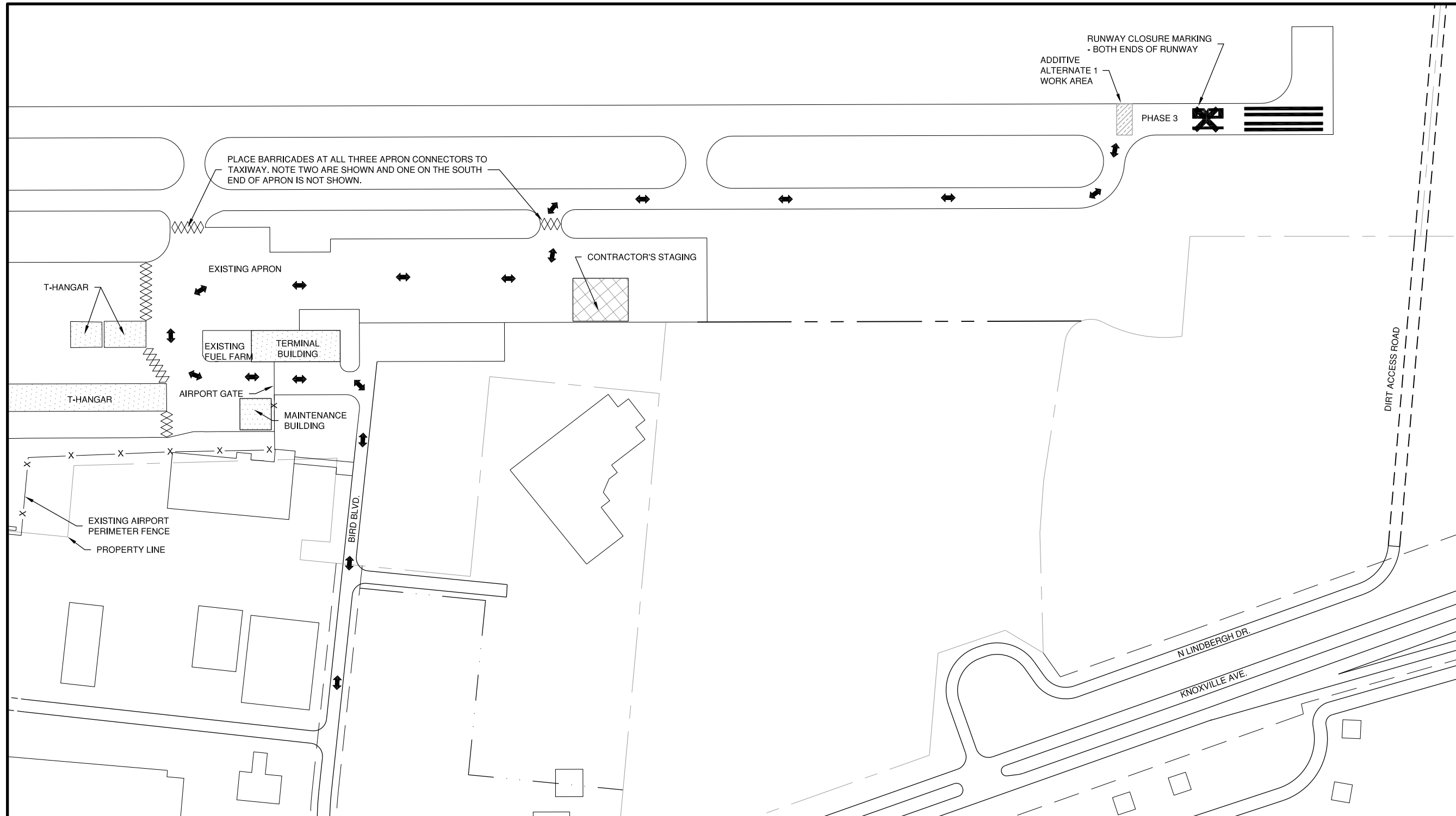
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SHEET TITLE
**CONSTRUCTION
ACTIVITY PLAN 3**

GC103
SHEET 7 OF 19

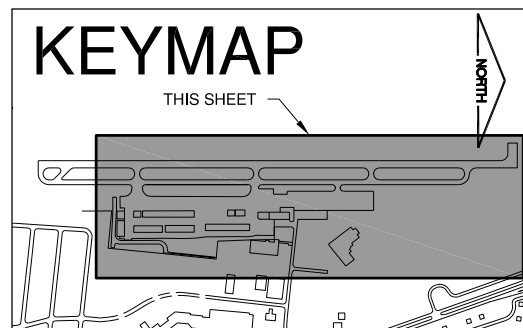


PHASE 3 NOTES:

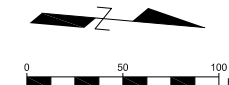
1. THE CONTRACTOR SHALL PLACE ALL LIGHTED RUNWAY CLOSURE MARKINGS PROVIDED BY THE AIRPORT AS SHOWN PRIOR TO STARTING WORK ON THIS PHASE.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING THIS PHASE TO COORDINATE THE T-HANGAR TENANTS.
3. THE AIRPORT WILL BE CLOSED THROUGHOUT THE DURATION OF PHASE 3 (MONDAY - FRIDAY).
4. PHASE 3 CAN BE CONCURRENT WITH PHASE 1 OR 2 BUT SHALL NOT LIMIT THE ABILITY TO COMPLETE PHASE 3 WORK.
5. RUNWAY IS TO BE CLOSED FOR ONLY 5 WORKING DAYS (MONDAY - FRIDAY). IF WORK IS NOT COMPLETED BY THE LAST DAY (FRIDAY), THEN WORK SHALL CONTINUE 24/7 UNTIL COMPLETED
6. CONTRACTOR SHALL CLEAN THIS AREA IMMEDIATELY FOLLOWING THE COMPLETION OF THE WORK FOR THIS PHASE.

LEGEND

- PHASE 3 WORK LIMITS - ADDITIVE ALTERNATE
- CONTRACTOR'S STAGING, STORAGE AND PARKING AREA
- CONTRACTOR'S ACCESS ROUTE
- LOW PROFILE BARRICADES AS SHOWN ON CONSTRUCTION ACTIVITY PLAN
- LIGHTED RUNWAY CLOSURE MARKING
- EXISTING FENCE LINE



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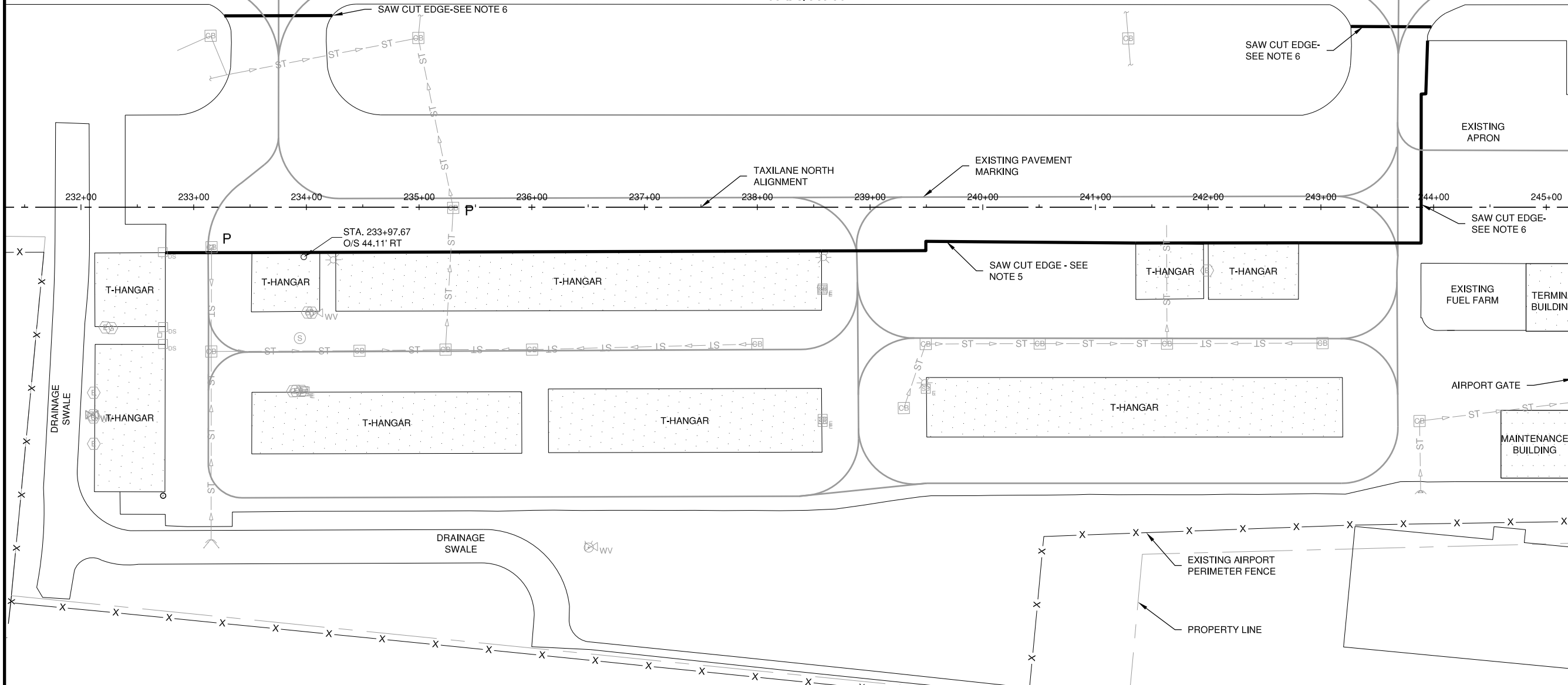
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SHEET TITLE
**EXISTING
CONDITIONS &
REMOVALS**

CD101
SHEET 8 OF 19

TAXIWAY



LEGEND

	EXISTING PAVEMENT		EXISTING GAS METER
	NEW PAVEMENT		EXISTING ELECTRIC METER
	SAW CUT EDGE		EXISTING OVERHEAD LIGHT
	EXISTING PAVEMENT MARKING		EXISTING INLET
	EXISTING STORM SEWER		EXISTING END SECTION
			EXISTING SANITARY
			PROTECT EXISTING STRUCTURE

NOTES:

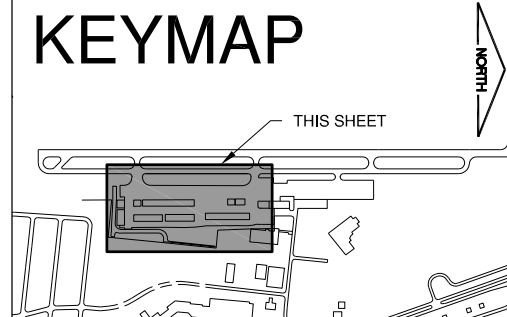
- APPROXIMATE EXISTING PAVEMENT STRUCTURE IN WORKING LIMITS:
- 3" BITUMINOUS SURFACE COURSE
- 5" CRUSHED AGGREGATE BASE COURSE
- PROTECT ALL EDGES OF PAVEMENT THAT ARE TO REMAIN IN PLACE WHILE SAW CUTTING AND DURING ENTIRE CONSTRUCTION OPERATIONS.
- PROTECT ANY EXISTING INLETS OR MANHOLES THAT ARE WITHIN THE WORKING LIMITS OF THE PROJECT.
- IF SUBGRADE REPAIR IS NEEDED, ENSURE THAT NO DAMAGE OCCURS TO THE EXISTING UNDERDRAIN LINES.
- SAW CUT SHALL BE 18" OFF ALL EDGES OF THE BUILDINGS UNLESS OTHERWISE SHOWN IN THE PLANS. SAWCUT SHALL BE THE LIMITS OF MILLING.
- FOR MILLING LIMITS SEE PROPOSED IMPROVEMENTS 1 SHEETS.

811 Know what's below. Call before you dig.

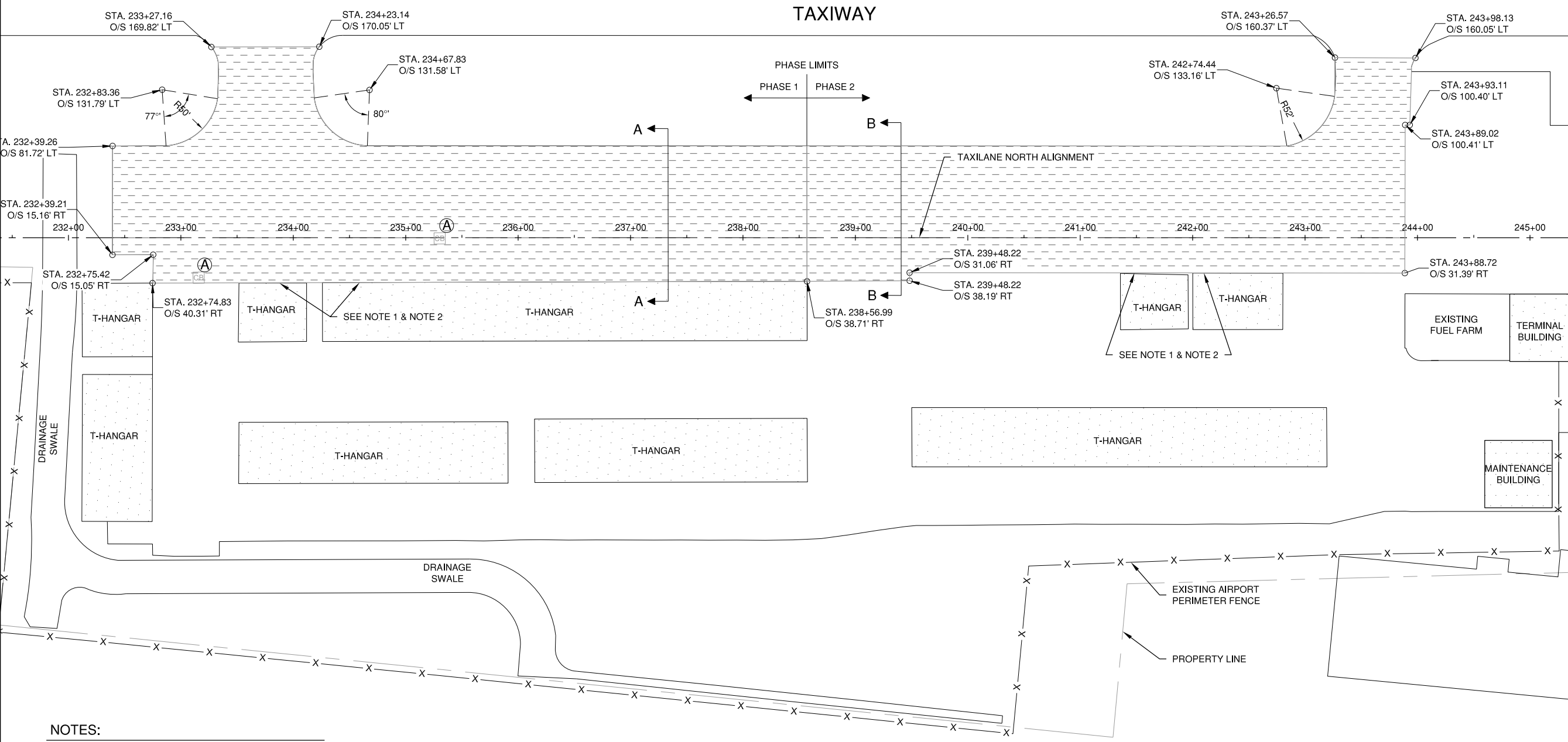
COMMON GROUND ALLIANCE
www.call811.com or
Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.



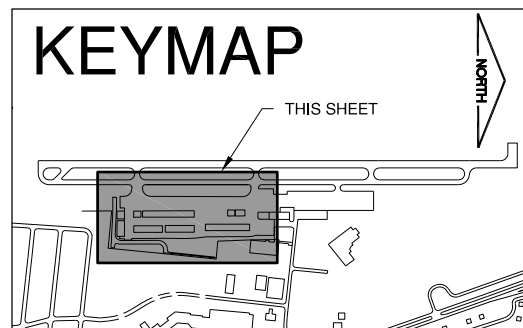
ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L1	TAXILANE NORTH	STA. 225+00.00	N:1502246.1180 E:2450286.6187	STA. 269+50.00	N: 1506676.6465 E: 2449870.7857



- NOTES:**
- MILLING SHALL BE OFFSET 18" OF THE FACE OF ALL BUILDING EDGES.
 - SEAL COAT SHALL BE APPLIED TO THE 18" STRIP OF BITUMINOUS ALONG ALL BUILDING EDGES.
 - SUBGRADE REPAIR WILL BE DETERMINED AT THE TIME OF CONSTRUCTION AT THE DIRECTION OF THE RESIDENT ENGINEER.

LEGEND

	MILL 2" BITUMINOUS SURFACE COURSE
	NEW 2" BITUMINOUS SURFACE COURSE (401)
	SEAL COAT
	PAVEMENT REPAIR AROUND INLETS



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REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2



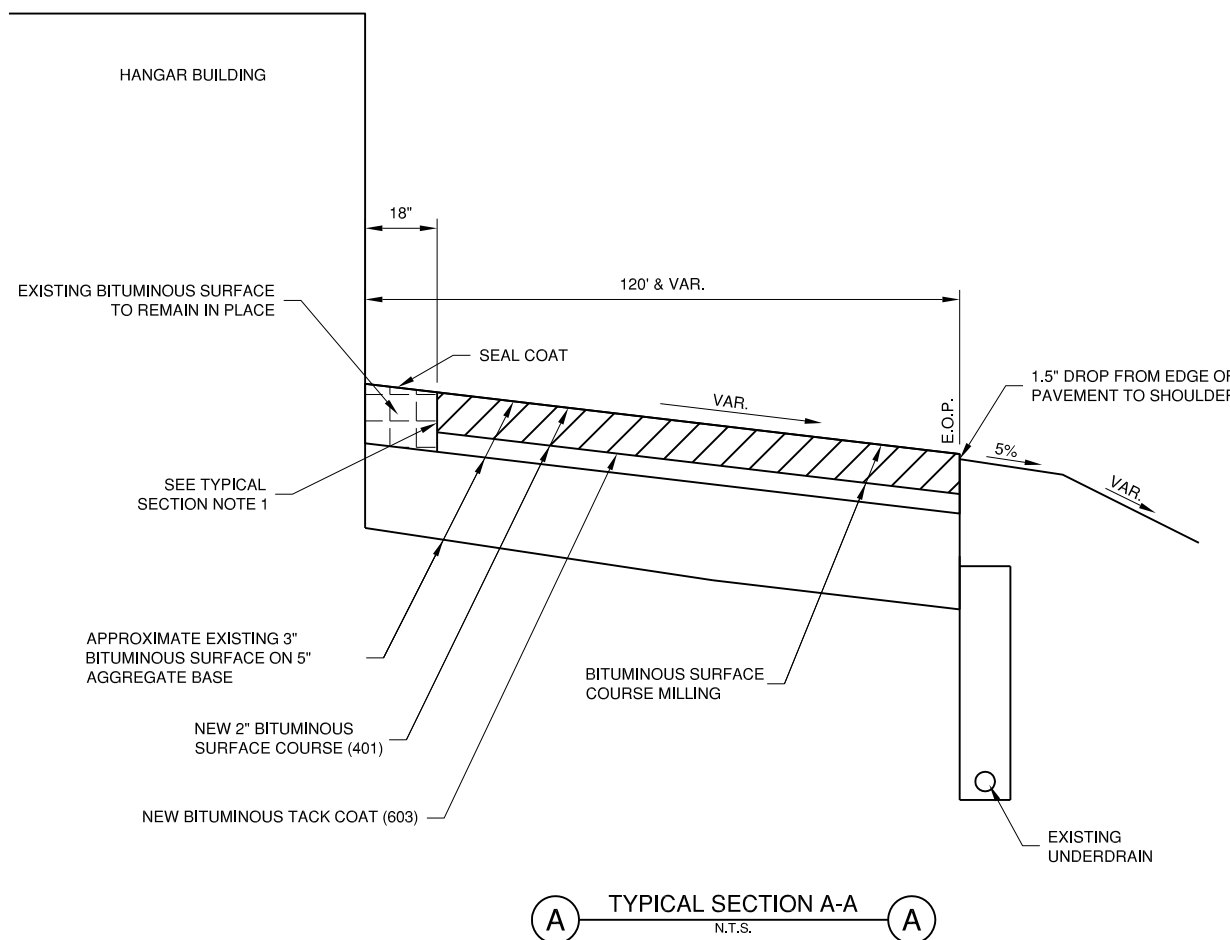
METROPOLITAN AIRPORT
AUTHORITY OF PEORIA
MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS

MARK	DATE	DESCRIPTION

AIP PROJECT NO. 3-17-SBGP-111
IL PROJECT NO. 3MY-4330
CMT PROJECT NO: 15061-06-00
CAD DWG FILE: 1506106-CP101.DWG
DESIGNED BY: JMW
DRAWN BY: DPA
CHECKED BY: CHK
APPROVED BY: APR
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2017

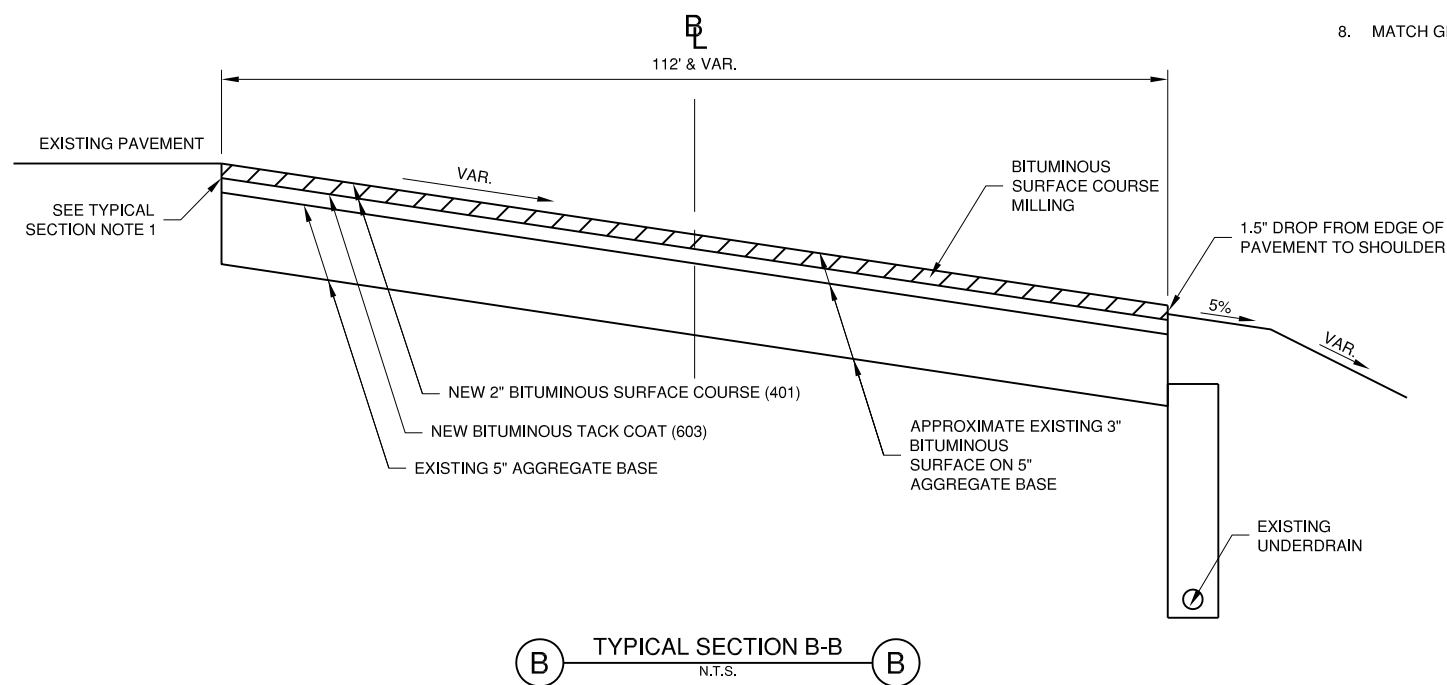
SHEET TITLE
PROPOSED IMPROVEMENTS

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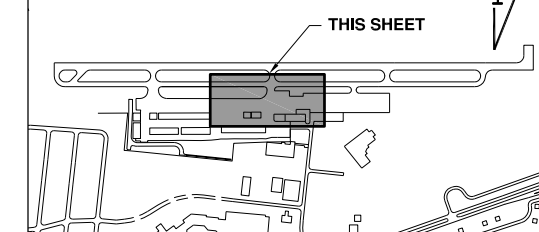


NOTES FOR TYPICAL SECTIONS:

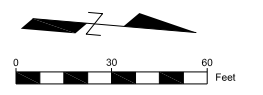
1. SAW CUT FULL DEPTH THROUGH BITUMINOUS SURFACE LAYER.
2. REFER TO EXISTING CONDITIONS AND REMOVALS 1 SHEET FOR LIMITS OF REMOVAL AND LOCATION OF SAW CUT.
3. SAW CUT SHALL BE 18" OFF THE FACE OF ALL BUILDINGS WITHIN THE CONSTRUCTION LIMITS OF THE PROJECT.
4. PROTECT EXPOSED SAW CUT EDGES THROUGHOUT THE DURATION OF THE PROJECT.
5. PRIME COAT SHALL BE APPLIED TO EXPOSED AGGREGATE SURFACE BEFORE ANY BITUMINOUS LAYERS ARE CONSTRUCTED.
6. TACK COAT SHALL BE APPLIED BETWEEN LIFTS OF BITUMINOUS PAVEMENT LAYERS.
7. BITUMINOUS SHALL BE PAVED IN 1 LIFT.
8. MATCH GRADES AT BUILDING.



KEYMAP



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REHABILITATE T-HANGAR TAXILANE PAVEMENT - PHASE 2



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AUTHORITY OF PEORIA
MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS

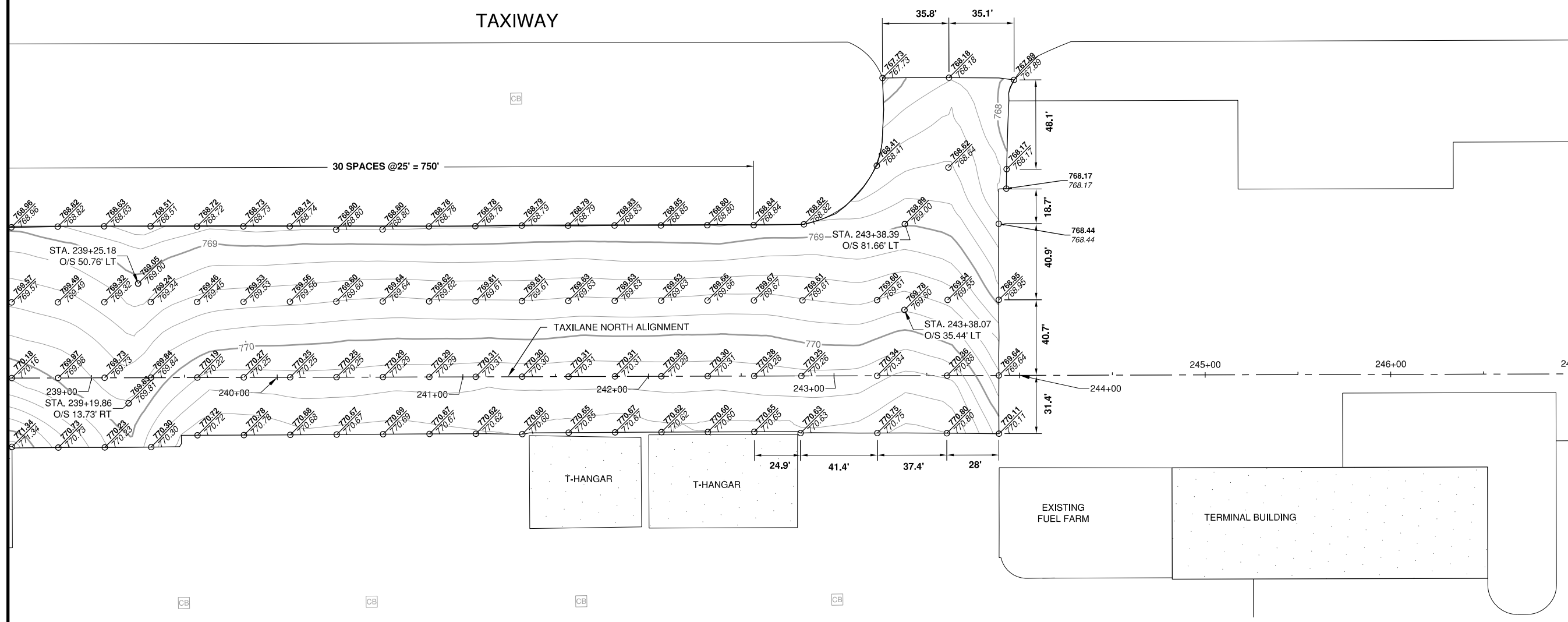
MARK	DATE	DESCRIPTION

AIP PROJECT NO.	3-17-SBGP-111
IL PROJECT NO.	3MY-4330
CMT PROJECT NO.	15061-06-00
CAD DWG FILE:	1506106-CP701.DWG
DESIGNED BY:	JMW
DRAWN BY:	DPA
CHECKED BY:	CHK
APPROVED BY:	APR
COPYRIGHT:	CRAWFORD, MURPHY & TILLY, INC. 2017

T-HANGAR TAXILANE STAKING PLAN 2

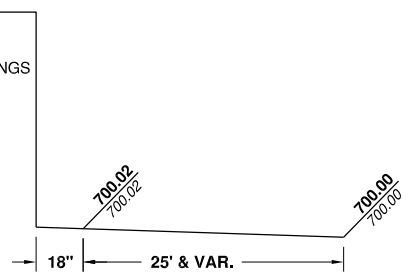
CP702
SHEET 12 OF 19

TAXIWAY



30 SPACES @25' = 750'

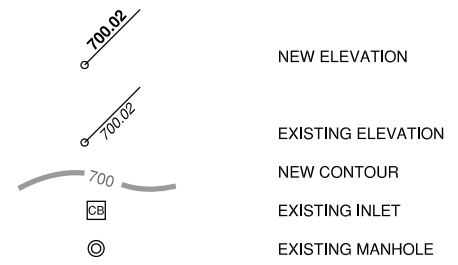
T-HANGAR/BUILDINGS



NOTES:

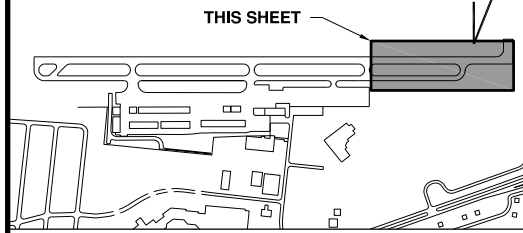
- MATCH ELEVATIONS ALONG ALL SAWED EDGES 18" OFF THE FACE OF THE BUILDING.
- SEE PROPOSED IMPROVEMENTS FOR ALIGNMENT BASELINE INFORMATION.
- A MINIMUM OF 2" BITUMINOUS LAYER SHALL BE MET.

LEGEND



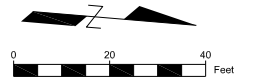
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KEYMAP



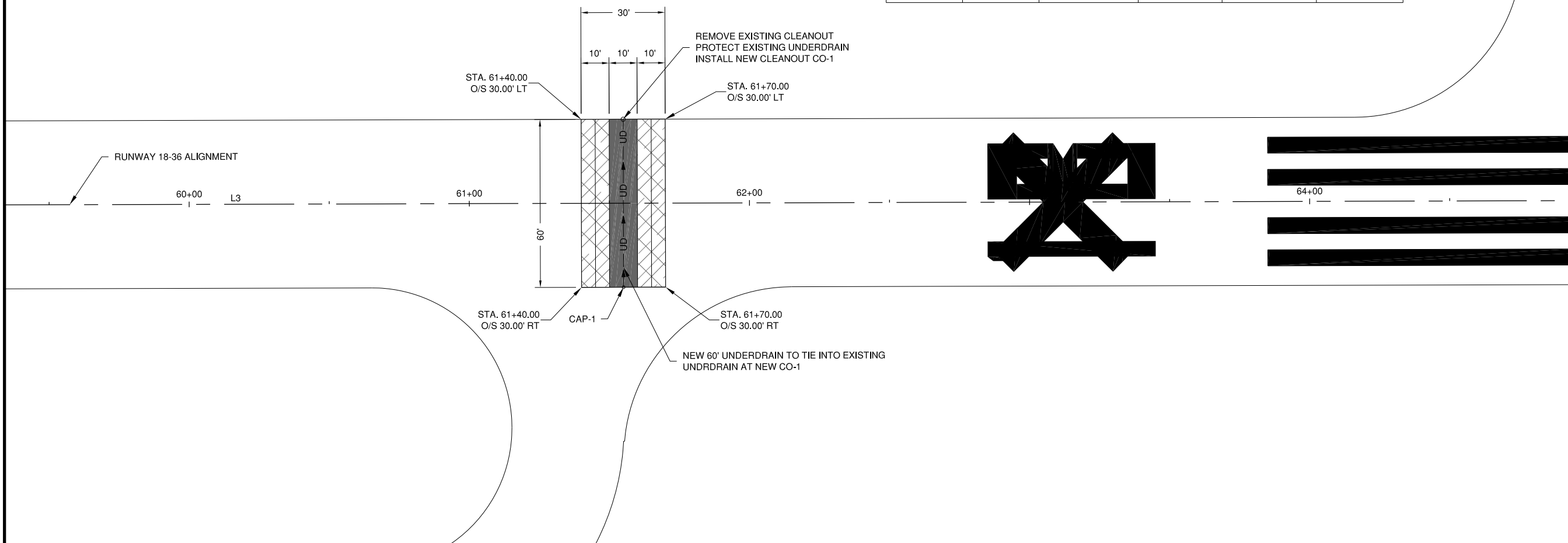
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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L3	RWY 18-36	STA. 25+00.00	N:1502213.4120 E:2449938.1501	STA. 69+50.00	N: 1506643.9405 E: 2449522.3171



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REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2

OWNER



METROPOLITAN AIRPORT
AUTHORITY OF PEORIA
MT. HAWLEY AUXILIARY AIRPORT
PEORIA, ILLINOIS

PIPE SCHEDULE							
LINE	UPSTREAM STRUCTURE	DOWN STREAM STRUCTURE	UPSTREAM INVERT	DOWNSTREAM INVERT	LENGTH (FT)	SLOPE %	TYPE
UD-1	CAP-1	CO-1	782.11'	782.02'	60'	0.1500	4" PERFORATED

LEGEND

- MILL 5"-7" EXISTING BITUMINOUS SURFACE COURSE
NEW 5"-7" PCC PAVEMENT (501)
- MILL 5" EXISTING BITUMINOUS SURFACE COURSE
NEW 5" PCC PAVEMENT (501)

NOTES:

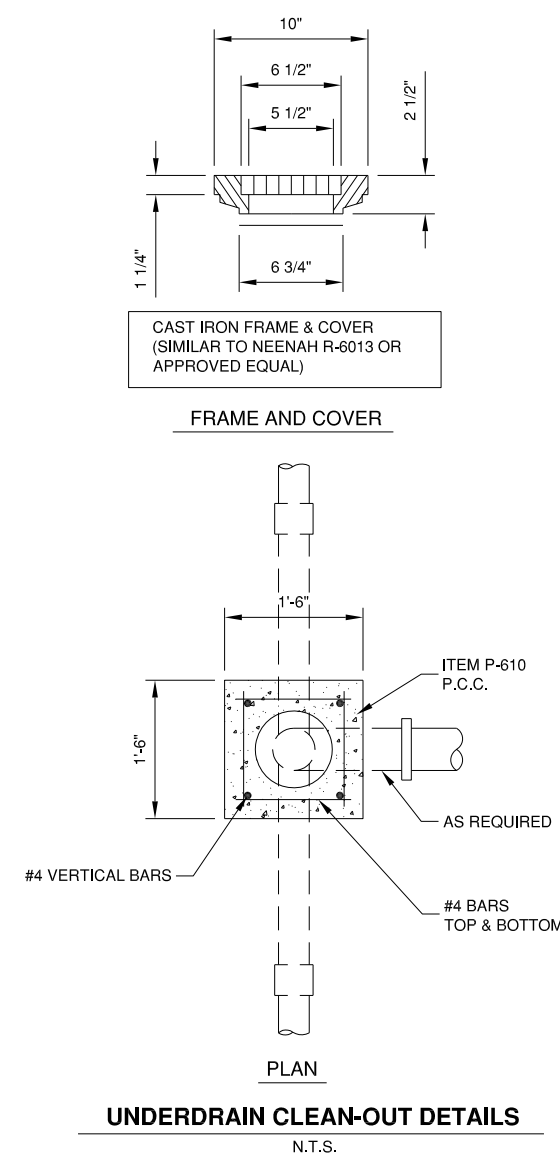
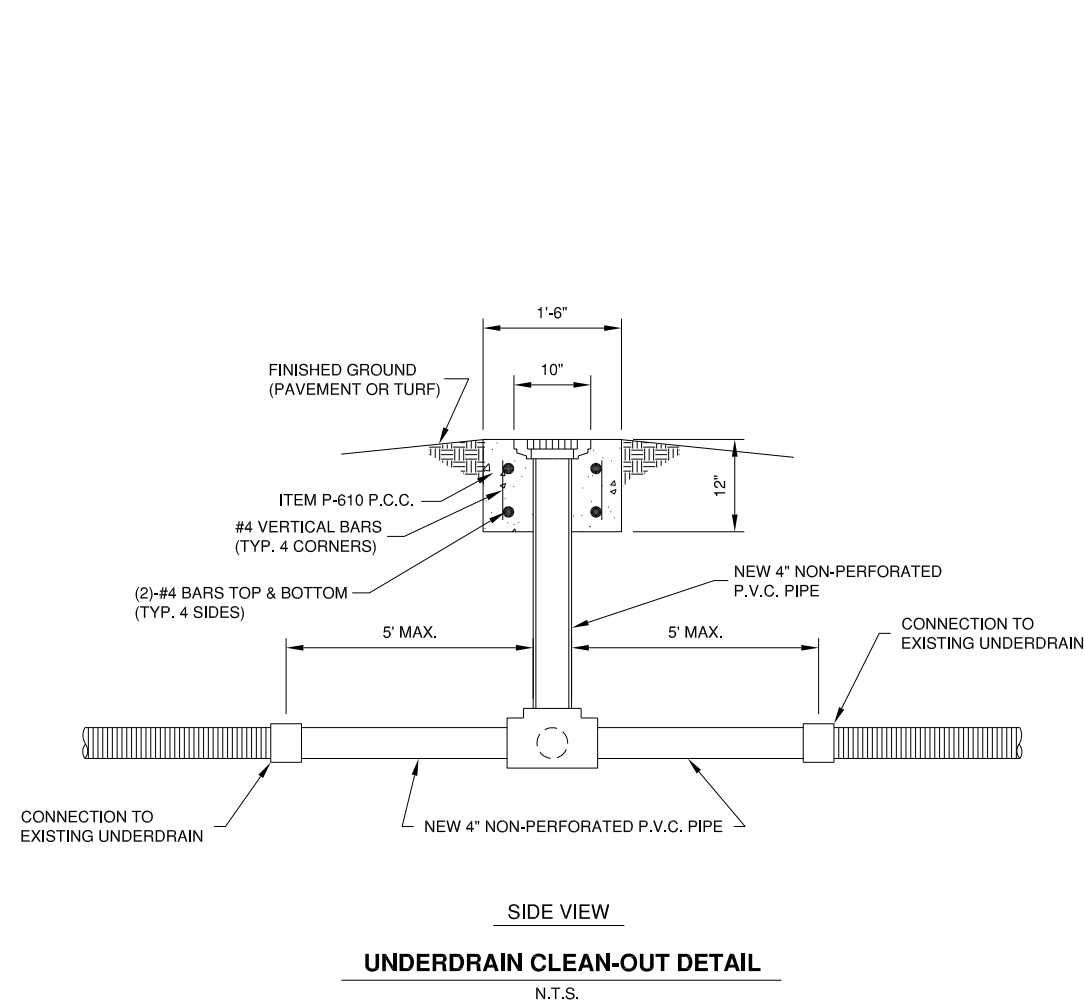
1. ALL PCC PAVEMENT SHALL INCLUDE SYNTHETIC FIBERS.
2. SURFACE PREPARATION AFTER HMA MILLING SHALL PROMOTE BONDING OF THE PCC TO THE HMA.

ADDITIVE ALTERNATE 1

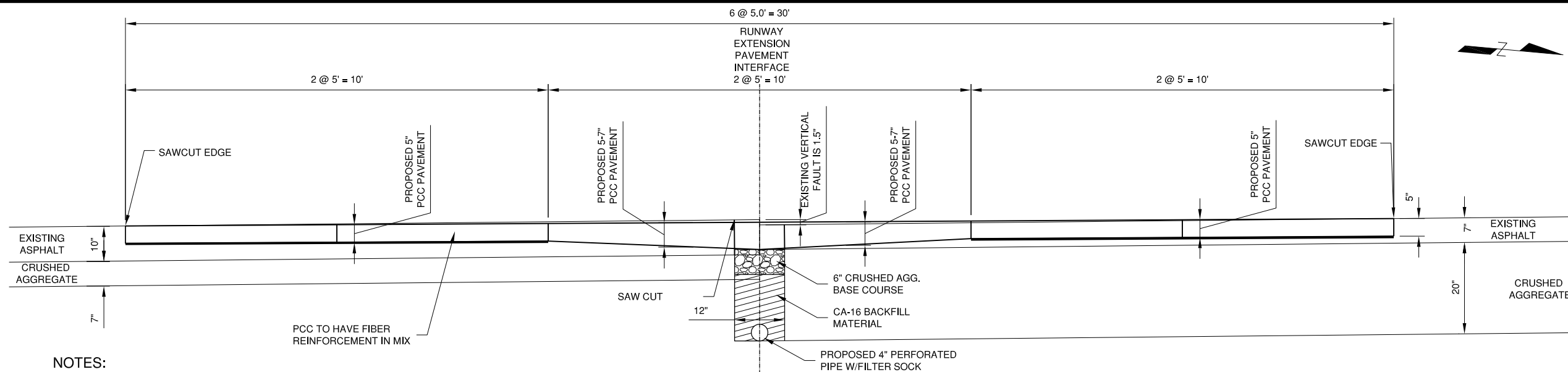
RWY 18 PROPOSED IMPROVEMENTS

CP703

SHEET 13 OF 19



**ADDITIVE
ALTERNATE 1**

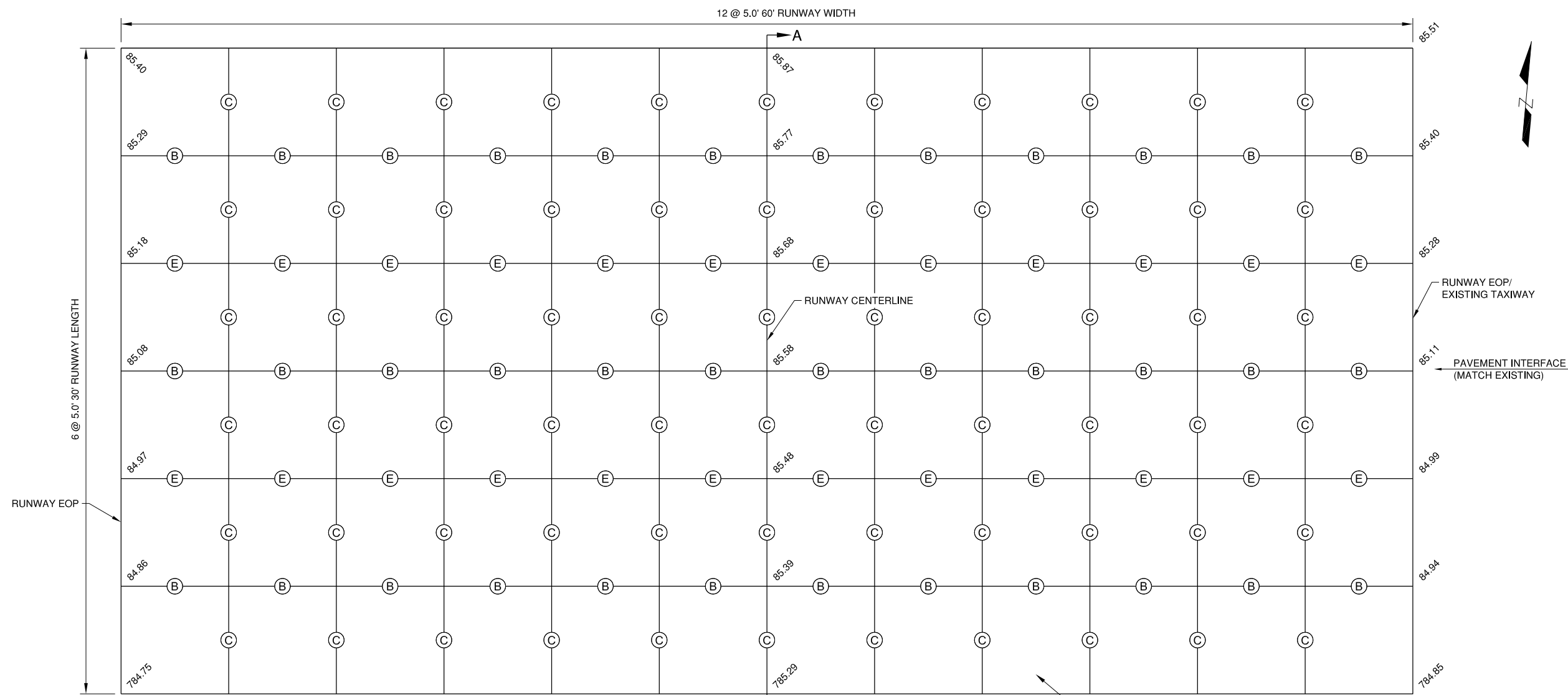


NOTES:

1. DETAILS RELATING TO THE UNDERDRAIN AND OUTLET ARE SHOWN ON SHEET CU501 UNDERDRAIN DETAILS.

PAVEMENT REPAIR PROFILE VIEW - SECTION A-A

N.T.S.



LEGEND

- (E) TYPE E DOWELED CONSTRUCTION JOINT
- (B) TYPE B HINGED (TIED) CONTRACTION JOINT
- (C) TYPE C DOWELED CONTRACTION JOINT

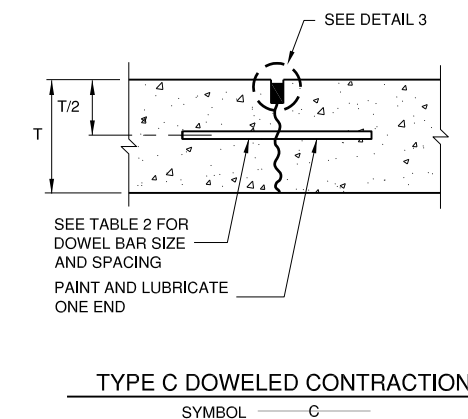
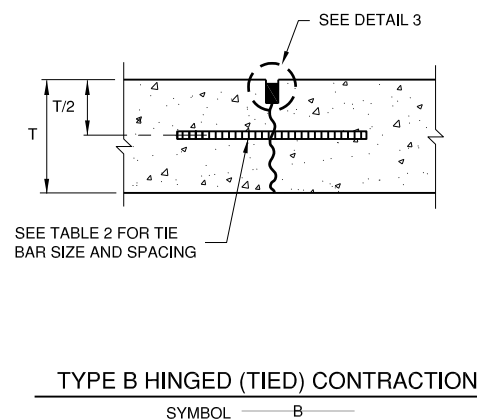
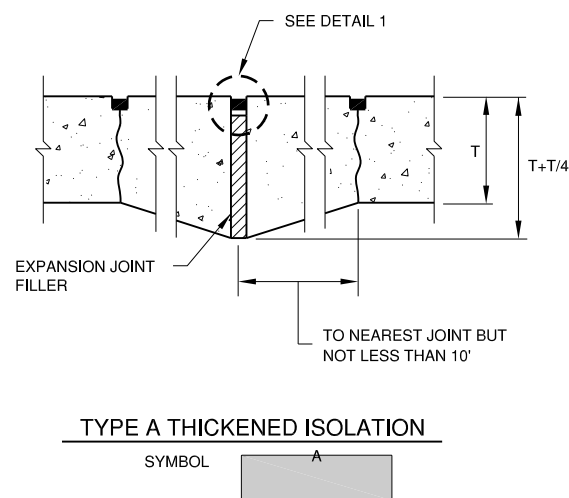
PAVE PERPENDICULAR TO RUNWAY C

RUNWAY 18 PLAN VIEW

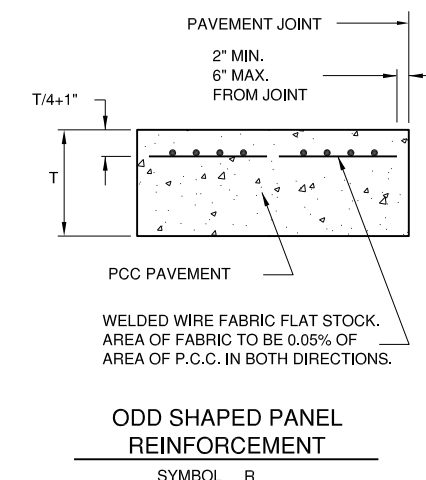
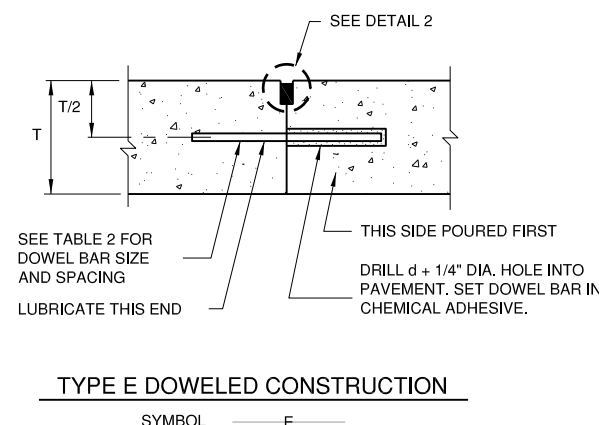
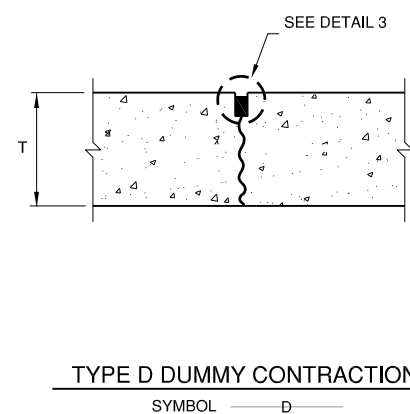
N.T.S.

**ADDITIVE
ALTERNATE 1**

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I = (T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"



PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

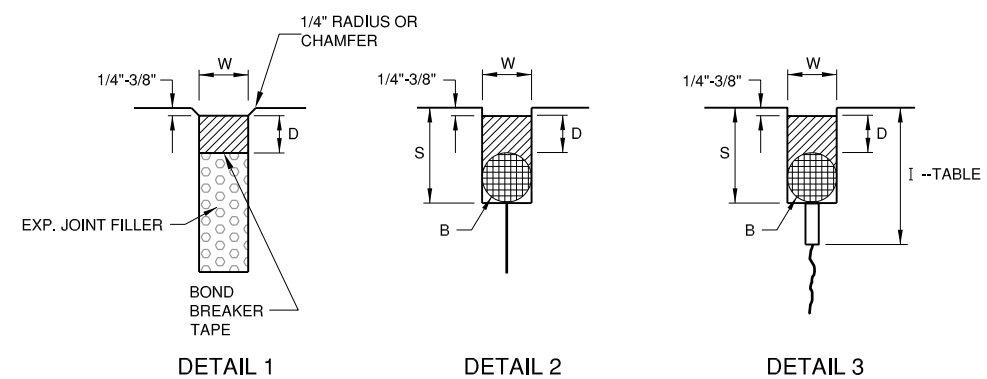


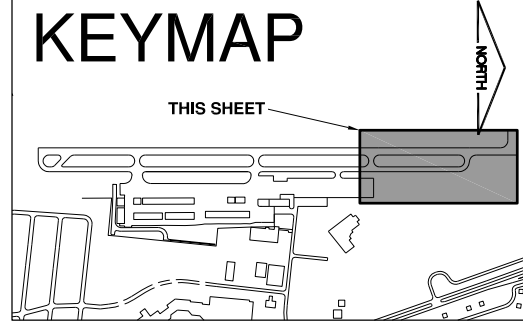
	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-1/8	1-1/8

JOINT NOTES

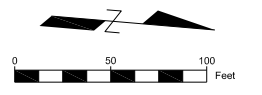
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

JOINT SEALING DETAILS





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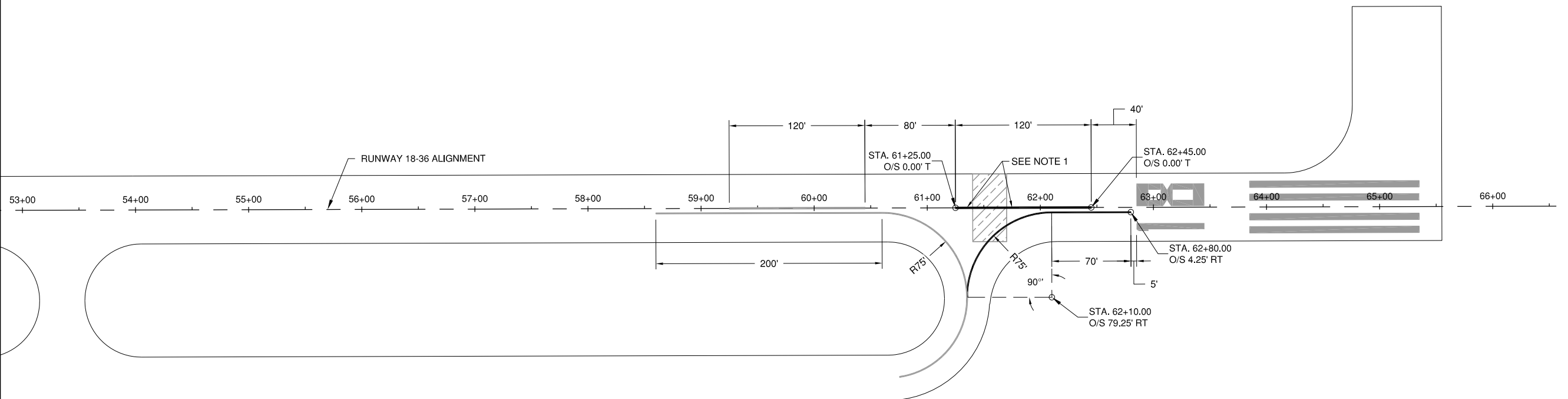
MH024

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JANUARY 6, 2017

REHABILITATE T-HANGAR
TAXILANE PAVEMENT - PHASE 2



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PEORIA, ILLINOIS

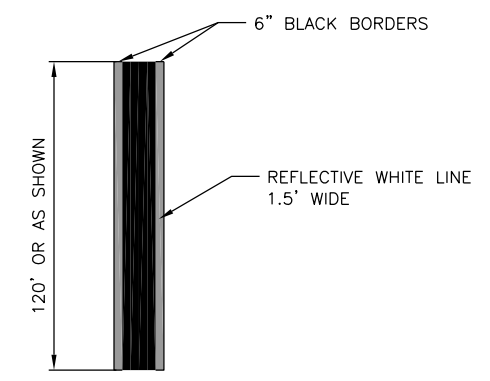


LEGEND

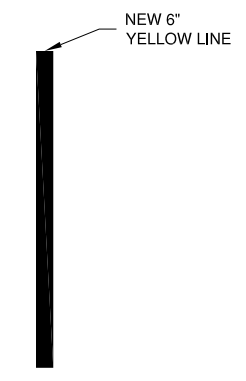
- EXISTING PAVEMENT MARKING
- NEW PAVEMENT MARKING

NOTES:

1. MATCH EXISTING RUNWAY CENTERLINE.
2. EXISTING MARKING ON THE SHALL BE REMOVED PRIOR TO NEW MARKING. SEE EXISTING CONDITIONS AND REMOVALS SHEET FOR EXTENTS OF MARKING REMOVAL.
3. TAXIWAY CENTERLINE MARKINGS FOR THE NEW HANGAR PAVEMENT SHALL BE SOLID YELLOW, TO THE WIDTH SHOWN, AND WITHOUT A BLACK BACKGROUND.



RUNWAY CENTERLINE STRIPE MARKING DETAIL
N.T.S.



TAXIWAY CENTERLINE CONTINUOUS
N.T.S.

ADDITIVE
ALTERNATE 1

MARK	DATE	DESCRIPTION

SHEET TITLE
MARKING PLAN 2

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