

DUPAGE AIRPORT AUTHORITY

WEST CHICAGO, ILLINOIS

CONSTRUCTION PLANS FOR DUPAGE AIRPORT

RECONFIGURATION OF TAXIWAY E BETWEEN RUNWAY 15-33 AND TAXIWAY B

INDEX TO SHEETS

1. COVER SHEET/INDEX TO SHEETS
2. SUMMARY OF QUANTITIES
3. SITE PLAN/PROJECT CONTROL PLAN
4. CONSTRUCTION ACTIVITY PLAN
5. CONSTRUCTION ACTIVITY PLAN NOTES - 1
6. CONSTRUCTION ACTIVITY PLAN NOTES - 2
7. CONSTRUCTION ACTIVITY PLAN DETAILS
8. STORM WATER POLLUTION PREVENTION PLAN
9. STORM WATER POLLUTION PREVENTION PLAN NOTES
10. EXISTING CONDITIONS/PROPOSED REMOVALS
11. GEOMETRIC PLAN & TYPICAL SECTION
12. STAKING PLAN
13. GRADING & DRAINAGE PLAN
14. LIGHTING PLAN/PAVEMENT MARKING PLAN
15. ELECTRICAL DETAILS - 1
16. ELECTRICAL DETAILS - 2
17. MISCELLANEOUS DETAILS

ILLINOIS PROJECT: DPA-4926
 SBGP PROJECT: 3-17-SBGP-184

MARCH 3, 2023

DESIGN INFORMATION

APPROACH CATEGORY D
 TAXIWAY DESIGN GROUP 3

DUPAGE AIRPORT

TOWNSHIP: 40 NORTH WAYNE TOWNSHIP
 RANGE: 9 EAST (SECTIONS: 31)
 DUPAGE COUNTY



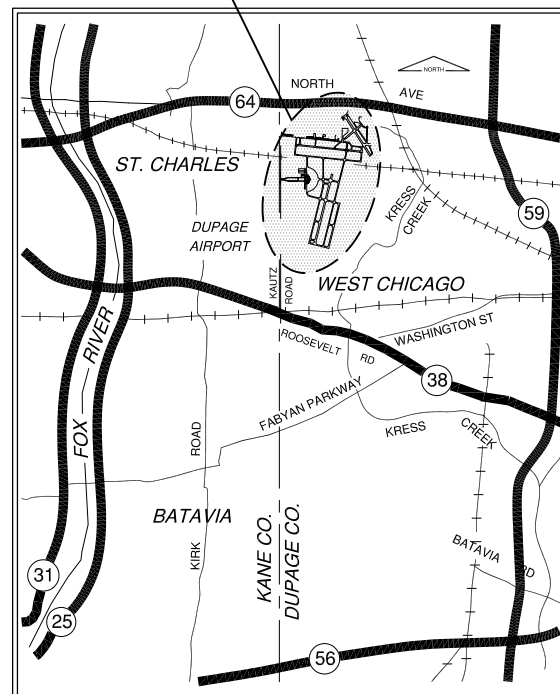
Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.Illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

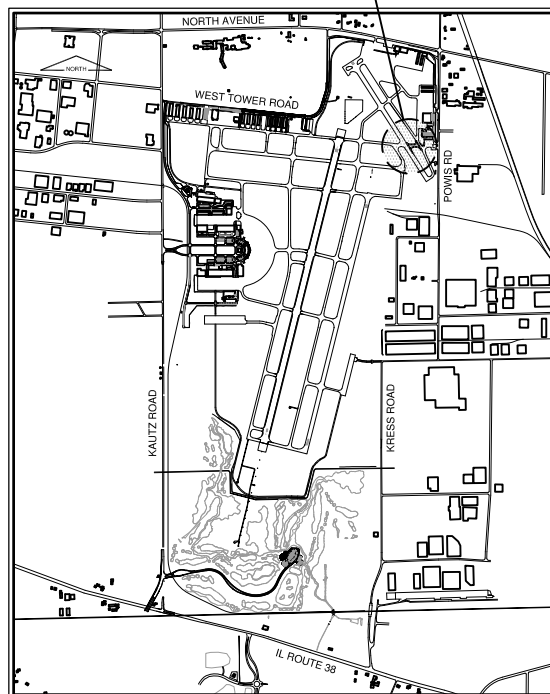
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

PROJECT LOCATION



LOCATION MAP

PROJECT LOCATION



SITE PLAN



CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

21001637-00



SUBMITTED BY Daniel L. Pape
 DATE 3/08/2023 DANIEL L. PAPE, P.E. DATE



DuPage Airport

2700 INTERNATIONAL DRIVE
 SUITE 200
 WEST CHICAGO, IL. 60185

APPROVED BY Mark Doles
 MARK DOLES
 EXECUTIVE DIRECTOR

DATE 03/06/2023 DATE

DATE: Wednesday, March 8, 2023 4:31:00 PM
 FILE: K:\Dupage\421001637-00_Plan\11\11E\DrawSheets\Taxway E - Summary Of Quantities.dwg

SUMMARY OF QUANTITIES

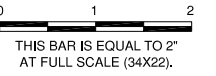
ITEM	DESCRIPTION	QUANTITY	UNIT
AR108156	1/C #6 5 KV UG CABLE IN UD	1,570	FOOT
AR108706	1/C #6 COUNTERPOISE	1,470	FOOT
AR110610	ELECTRICAL HANDHOLE	2	EACH
AR125415	MITL- BASE MOUNTED	15	EACH
AR125902	REMOVE BASE MOUNTED LIGHT	11	EACH
AR125964	RELOCATE TAXI GUIDANCE SIGN	3	EACH
AR150510	ENGINEER'S FIELD OFFICE	1	L SUM
AR150520	MOBILIZATION	1	L SUM
AR152511	SUBGRADE REPAIR	150	SQ YD
AR156520	INLET PROTECTION	4	EACH
AR201900	REMOVE BITUMINOUS PAVEMENT	2,000	SQ YD
AR401610	BITUMINOUS SURFACE COURSE	290	TON
AR401650	BITUMINOUS PAVEMENT MILLING	2,500	SQ YD
AR152410	UNCLASSIFIED EXCAVATION	1,375	CU YD
AR401910	REMOVE & REPLACE BIT. PAVEMENT	150	SQ YD
AR603510	BITUMINOUS TACK COAT	300	GALLON
AR620520	PAVEMENT MARKING - WATERBORNE	2,700	SQ FT
AR620525	PAVEMENT MARKING - BLACK BORDER	2,050	SQ FT
AR620900	PAVEMENT MARKING REMOVAL	1,880	SQ FT
AR701010	10" PVC STORM SEWER	95	FOOT
AR701512	12" RCP, CLASS IV	48	FOOT
AR701900	REMOVE PIPE	210	FOOT
AR751411	INLET-TYPE A	2	EACH
AR751900	REMOVE INLET	2	EACH
AR901510	SEEDING	1.0	ACRE
AR908515	HEAVY-DUTY HYDRAULIC MULCH	1.0	ACRE

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE



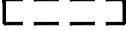



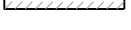


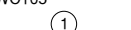




DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B

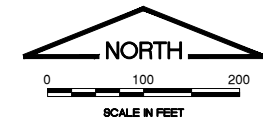
SUMMARY OF QUANTITIES



DESIGN BY: ABM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 3/3/2023
 JOB No: 21001637.00

LEGEND

-  NEW IMPROVEMENT LIMITS
-  NEW 2" HMA MILL AND OVERLAY
-  EXISTING PAVEMENT TO BE REMOVED
-  EXISTING PAVEMENT
-  EXISTING BUILDING
-  EXISTING AIRPORT PROPERTY LINE
-  EXISTING FENCE
-  EXISTING BENCHMARK AND REFERENCE NUMBER
-  HORIZONTAL CONTROL POINTS
-  RUNWAY SAFETY AREA
-  RUNWAY OBSTACLE FREE ZONE
-  TAXIWAY OBJECT FREE AREA

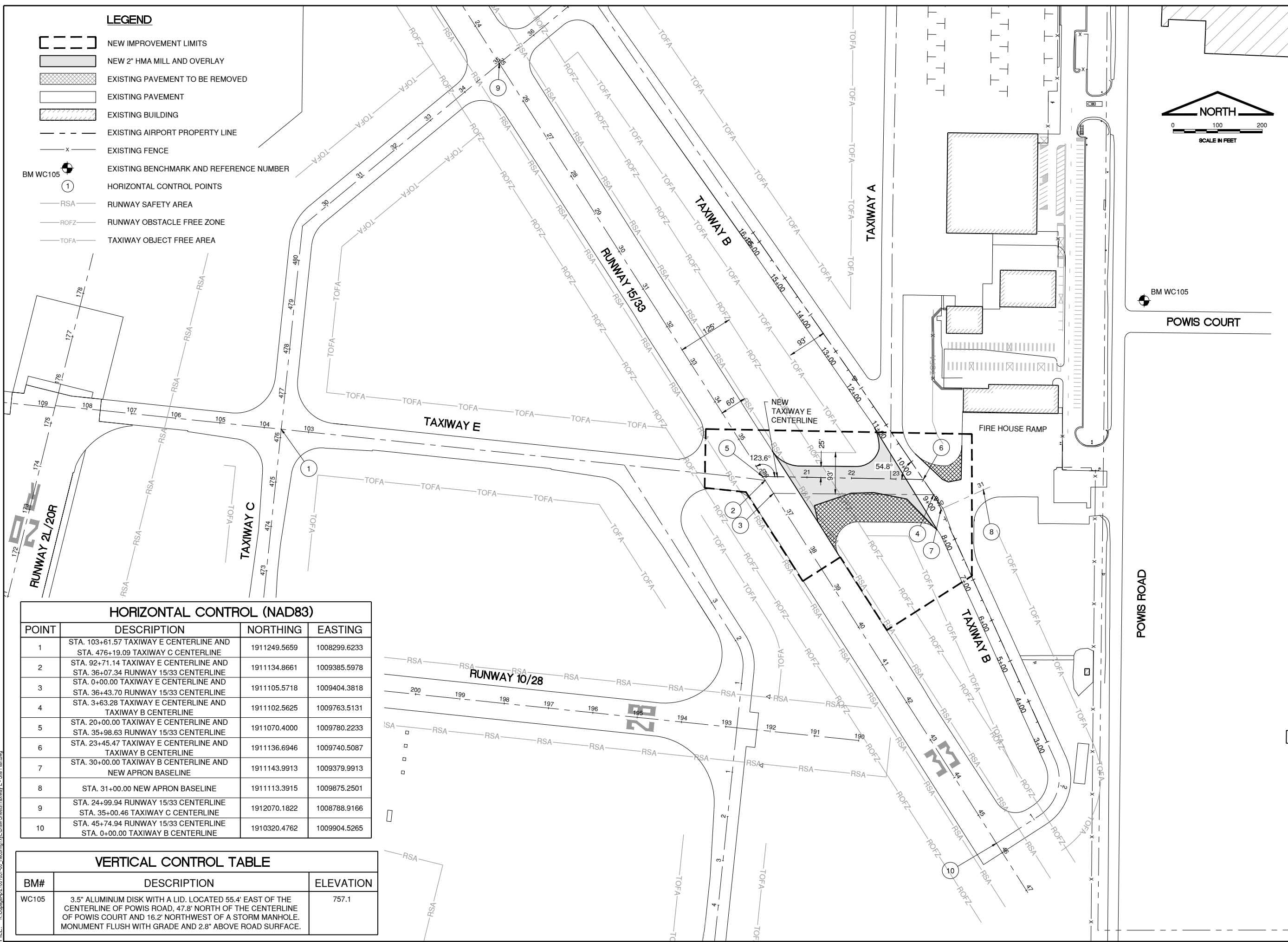


IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



HORIZONTAL CONTROL (NAD83)


POINT	DESCRIPTION	NORTHING	EASTING
1	STA. 103+61.57 TAXIWAY E CENTERLINE AND STA. 476+19.09 TAXIWAY C CENTERLINE	1911249.5659	1008299.6233
2	STA. 92+71.14 TAXIWAY E CENTERLINE AND STA. 36+07.34 RUNWAY 15/33 CENTERLINE	1911134.8661	1009385.5978
3	STA. 0+00.00 TAXIWAY E CENTERLINE AND STA. 36+43.70 RUNWAY 15/33 CENTERLINE	1911105.5718	1009404.3818
4	STA. 3+63.28 TAXIWAY E CENTERLINE AND TAXIWAY B CENTERLINE	1911102.5625	1009763.5131
5	STA. 20+00.00 TAXIWAY E CENTERLINE AND STA. 35+98.63 RUNWAY 15/33 CENTERLINE	1911070.4000	1009780.2233
6	STA. 23+45.47 TAXIWAY E CENTERLINE AND TAXIWAY B CENTERLINE	1911136.6946	1009740.5087
7	STA. 30+00.00 TAXIWAY B CENTERLINE AND NEW APRON BASELINE	1911143.9913	1009379.9913
8	STA. 31+00.00 NEW APRON BASELINE	1911113.3915	1009875.2501
9	STA. 24+99.94 RUNWAY 15/33 CENTERLINE STA. 35+00.46 TAXIWAY C CENTERLINE	1912070.1822	1008788.9166
10	STA. 45+74.94 RUNWAY 15/33 CENTERLINE STA. 0+00.00 TAXIWAY B CENTERLINE	1910320.4762	1009904.5265


VERTICAL CONTROL TABLE

BM#	DESCRIPTION	ELEVATION
WC105	3.5" ALUMINUM DISK WITH A LID. LOCATED 55.4' EAST OF THE CENTERLINE OF POWIS ROAD, 47.8' NORTH OF THE CENTERLINE OF POWIS COURT AND 16.2' NORTHWEST OF A STORM MANHOLE. MONUMENT FLUSH WITH GRADE AND 2.8" ABOVE ROAD SURFACE.	757.1

**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

SITE PLAN / PROJECT CONTROL PLAN











 **CMT**
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

 **DUPAGE AIRPORT
 AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

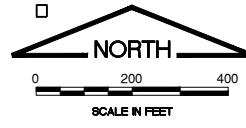
DATE: Wednesday, March 8, 2023 8:18:51 AM
 FILE: K:\Dupage\21001637-00_ReconfigTaxiwayE\DrawSheets\Taxiway E - Site Plan.dwg

LEGEND

-  CONTRACTOR'S STAGING AND STORAGE AREA
-  PAVEMENT CLOSED TO AIR TRAFFIC
-  LOW PROFILE BARRICADES
-  CLOSED RUNWAY MARKER
-  CONTRACTOR'S ACCESS/HAUL ROUTE
-  AIRCRAFT MOVEMENT AREA
-  TOFA TAXIWAY OBJECT FREE AREA (TOFA)
-  RSA RUNWAY SAFETY AREA (RSA)
-  ROFZ RUNWAY OBSTACLE FREE ZONE (ROFZ)
-  FAA CRITICAL CONTROL POINT

NOTES

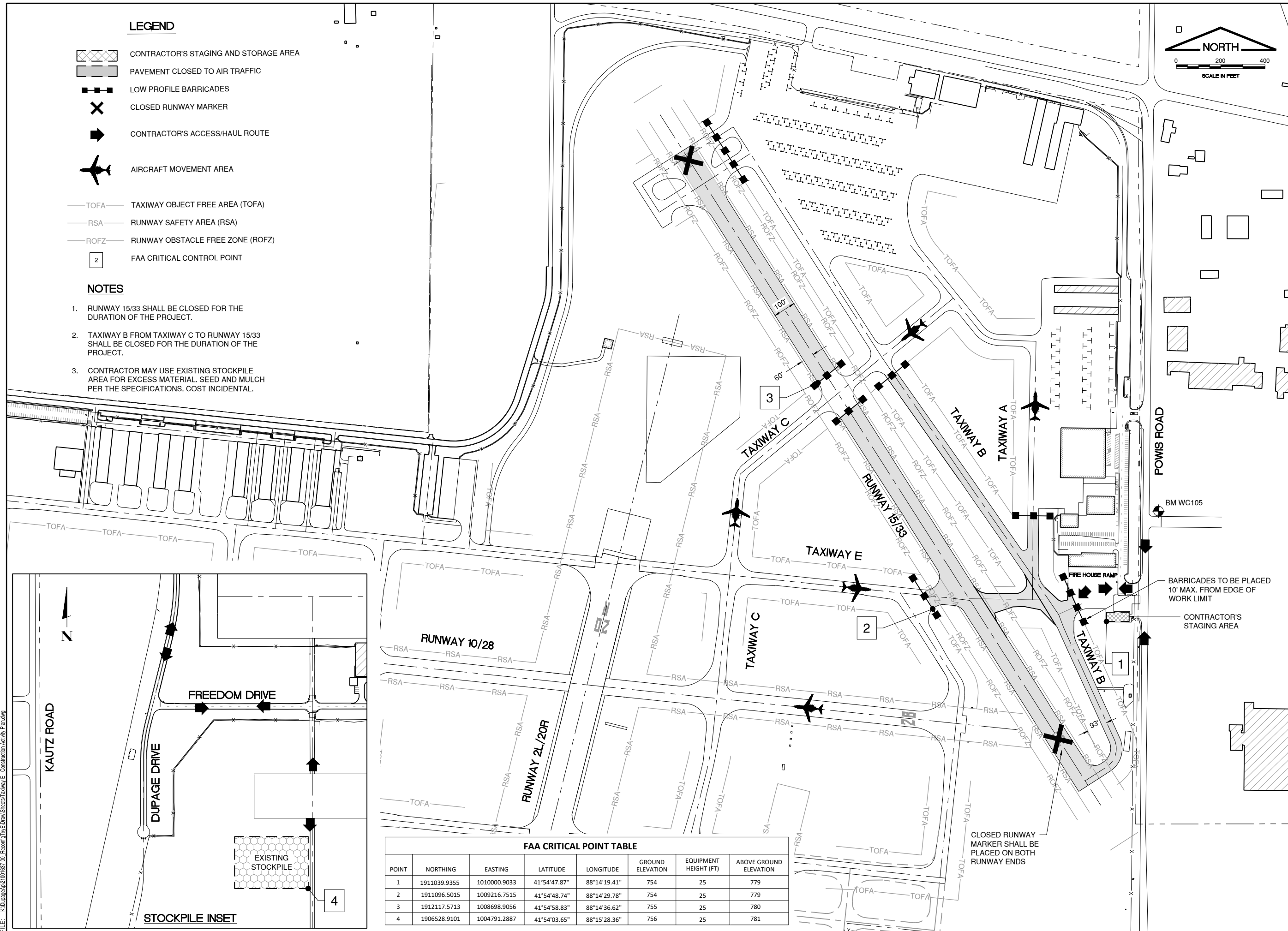
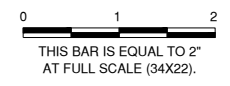
1. RUNWAY 15/33 SHALL BE CLOSED FOR THE DURATION OF THE PROJECT.
2. TAXIWAY B FROM TAXIWAY C TO RUNWAY 15/33 SHALL BE CLOSED FOR THE DURATION OF THE PROJECT.
3. CONTRACTOR MAY USE EXISTING STOCKPILE AREA FOR EXCESS MATERIAL. SEED AND MULCH PER THE SPECIFICATIONS. COST INCIDENTAL.



IL. CONTRACT: **DU092**
 IL. LETTING ITEM: **09A**
 IL. PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #


REVISIONS		
NUMBER	BY	DATE




FAA CRITICAL POINT TABLE

POINT	NORTHING	EASTING	LATITUDE	LONGITUDE	GROUND ELEVATION	EQUIPMENT HEIGHT (FT)	ABOVE GROUND ELEVATION
1	1911039.9355	1010000.9033	41°54'47.87"	88°14'19.41"	754	25	779
2	1911096.5015	1009216.7515	41°54'48.74"	88°14'29.78"	754	25	779
3	1912117.5713	1008698.9056	41°54'58.83"	88°14'36.62"	755	25	780
4	1906528.9101	1004791.2887	41°54'03.65"	88°15'28.36"	756	25	781

**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B
 CONSTRUCTION ACTIVITY PLAN**


CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613


**DUPAGE AIRPORT
 AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

DATE: Wednesday, March 8, 2023, 8:19:02 AM
 FILE: K:\Dupage\021001637-00_Plan\01\01E-Dupage\Draw\Sheets\Taxiway E - Construction Activity Plan.dwg

DATE: Wednesday, March 6, 2023 8:19:09 AM
FILE: K:\Dwg\421001637-20_Plan\101.Dwg

GENERAL

- THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
- PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
- THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
- A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
- ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING TENANT/AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER IN CONSULTATION WITH THE AIRPORT DIRECTOR OF OPERATIONS. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR OF OPERATIONS.

1. COORDINATION

- PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
- PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT A PROGRESS SCHEDULE TO BE APPROVED BY THE ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 3 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS OR CONTRACTOR'S LACK OF PROGRESS. SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.
- DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH AIRPORT STAFF, FAA, RESIDENT ENGINEER AND OTHER APPROPRIATE STAKE HOLDERS TO DISCUSS PROJECT PROGRESS. AT A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- TOTAL CONTRACT TIME SHALL BE 30 CALENDAR DAYS
- TO CLOSE A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL PLACE RUNWAY AND TAXIWAY CLOSURE MARKERS AND BARRICADES AT THE LOCATIONS SPECIFIED. TO RE-OPEN THE RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL CLEAN ANY DEBRIS OFF OF THE PAVEMENT AND REMOVE THE RUNWAY/TAXIWAY CLOSURE MARKERS. ALL WORK ASSOCIATED WITH CLOSING AND OPENING AIRFIELD PAVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS REOPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL.
- SEE CONTRACTOR ACCESS NOTES ON THIS SHEET FOR SITE ACCESS AND HAULING GUIDELINES.
- PRIOR TO REOPENING A CLOSED RUNWAY OR TAXIWAY, THE ENTIRE RUNWAY SAFETY AREA (RSA), MEASURED 250 FEET FROM THE RUNWAY CENTERLINE, INCLUDING BEYOND THE RUNWAY END WITHIN THE EXTENDED RSA, AND THE ENTIRE TAXIWAY OBJECT FREE AREA (TOFA), MEASURED 85.5 FEET FROM GROUP III TAXIWAYS (I.E. TAXIWAY C AND G) MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THESE AREAS. THE MAXIMUM PAVEMENT DROP OFF SHALL BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 3 PERCENT. STEEL PLATES OR TEMPORARY WEDGING OF BASE COURSE MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED WITHIN AN RSA, ROFA OR OBSTACLE FREE ZONE (OFZ) OF AN ACTIVE RUNWAY, OR WITHIN THE TOFA OF AN ACTIVE TAXIWAY.
- WORK AREA RESTRICTIONS
ALL WORK AREAS MUST BE RESTORED AND MEET FAR PART 139 REQUIREMENTS PRIOR TO RETURNING THE AREA TO AIRPORT OPERATIONS. THE CONSTRUCTION ACTIVITY PLAN GENERAL NOTES IN THE PLANS INCLUDES ADDITIONAL INFORMATION ON FAR PART 139 AND INSPECTION REQUIREMENTS.
ALL WORK ON THE AIRFIELD ARE LOCATED INSIDE THE AOA SECURITY FENCE, AND ARE SUBJECT TO THE LIMITATIONS DESCRIBED IN SPECIFICATIONS, PART 1 - GENERAL PROVISIONS.
CONTRACTOR SHALL NOTIFY THE AIRPORT 10 CALENDAR DAYS PRIOR TO BEGINNING CONSTRUCTION IN ANY WORK AREA AND TO REQUEST A RUNWAY OR TAXIWAY CLOSURE.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
- THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. NAVAIDS THAT COULD BE AFFECTED

- THE CONTRACTOR MUST COORDINATE WITH AIRPORT OPERATIONS/ENGINEER AND FAA A MINIMUM OF 30 DAYS IN ADVANCE FOR ANY WORK WITHIN A NAVAID CRITICAL AREA OR AFFECTING THE VISUAL, TRANSMITTED SIGNAL OR POWER SUPPLY OF A NAVAID.
- EDGE LIGHTS, THRESHOLD LIGHTS, VISUAL AIDS AND ALL ILS EQUIPMENT SHALL BE SHUT OFF FOR THE DURATION OF A CLOSURE PERIOD ON ANY ASSOCIATED PAVEMENTS. IF THE LIGHTING CIRCUIT MUST BE ON FOR OPEN PAVEMENT AREAS, CLOSED PAVEMENT AREA LIGHTS SHALL BE COVERED COMPLETELY.
- EXCEPT WHERE NOTED IN THE PLANS, EXISTING COMMUNICATIONS EQUIPMENT AND NAVIGATIONAL AIDS (NAVAIDS) SHALL NOT BE DISTURBED BY THE CONTRACTOR AND SHALL BE PROTECTED FROM DAMAGE.
- PRIOR TO BEGINNING SITE WORK, CONTRACTOR SHALL COORDINATE THROUGH AIRPORT OPERATIONS TO LOCATE, AND THE CONTRACTOR TO MARK ALL UNDERGROUND COMMUNICATIONS CABLES AND FACILITIES, WITHIN THE PROJECT AREA.
- IF CONTRACTOR CAUSES INTERRUPTION OF POWER OR COMMUNICATIONS TO A NAVAID CONTRACTOR SHALL REPAIR WITHIN 24 HOURS AT THE CONTRACTOR'S COST. CONTRACTOR MUST COORDINATE REPAIR WITH AIRPORT OPERATIONS BEFORE ANY REPAIR IS MADE.
- THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT DIRECTOR OF OPERATIONS OR HIS DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD LIGHTING CIRCUITS AT THE START AND END OF EACH CONSTRUCTION DAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE DONE SO AT NO EXTRA COST TO THE CONTRACT.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- DURING CONSTRUCTION, CONTINUOUS ALL WEATHER ACCESS TO EXISTING NAVAIDS IN THE PROJECT AREA MUST BE MAINTAINED AT NO COST TO THE CONTRACT.

5. CONTRACTOR ACCESS

- THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED.
- THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLE ACCESS AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO THE CITY OF ROCKFORD AND IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
- THE GATE SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT DIRECTOR OF OPERATIONS. SHOULD THE CONTRACTOR'S OPERATIONS REQUIRE THE GATE TO REMAIN OPEN TO PROVIDE ACCESS TO HAULING OPERATIONS, A COMPETENT GATE GUARD SHALL BE REQUIRED TO CONTROL ACCESS TO THE AIRFIELD. **A \$1,000 FINE SHALL BE ASSESSED FOR ANY OCCURRENCE OF AN UNSECURE GATE THAT IS THE CONTRACTOR'S RESPONSIBILITY.** AN UNSECURED GATE SHALL BE DEFINED AS ANY GATE THAT IS NOT WITHIN THE SIGHT AND PHYSICAL CONTROL OF THE CONTRACTOR'S GUARD. IN THE EVENT THAT THE GATE MAY NOT BE SECURED, THE CONTRACTOR WILL BE CHARGED FOR AIRPORT PERSONNEL TO REMAIN AT THE GATE UNTIL SECURED.

- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.

- THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

- DRIVER'S TRAINING AND BADGING SHALL BE REQUIRED FOR THE CONTRACTOR'S SUPERVISORY PERSONNEL. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. THE DRIVER'S TRAINING AND BADGING OF THE INITIAL SUPERVISORY PERSONNEL MUST BE COMPLETED PRIOR TO THE START OF CONSTRUCTION.

- THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.

- THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

- DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.

- THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.

- ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE CONTROL TOWER. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.

- IF NECESSARY, THE CONTRACTOR SHALL CONSTRUCT A HAUL ROUTE TO THE STAGING AREA WITHIN THE PROJECT LIMITS. HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF MAINTENANCE OF TRAFFIC. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT DIRECTOR OF OPERATIONS. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.

- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT OPERATION MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

- ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

6. WILDLIFE MANAGEMENT

- THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
- CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
- THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

- THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.

- THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENT OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DEPUTY DIRECTOR OF OPERATIONS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

- THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.

- THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.

- THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.

- FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.

- IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL NOTIFY THE AIRPORT DIRECTOR OF OPERATIONS AND THE RESIDENT ENGINEER.

10. INSPECTION REQUIREMENTS

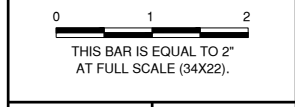
- THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.

- THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

IL. CONTRACT: **DU092**
 IL. LETTING ITEM: **09A**
 IL. PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

CONSTRUCTION ACTIVITY PLAN NOTES - 1

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

**DUPAGE AIRPORT
 AUTHORITY**

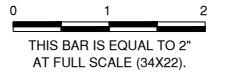
DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #


REVISIONS

NUMBER	BY	DATE




**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

CONSTRUCTION ACTIVITY PLAN NOTES - 2



CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613



**DUPAGE AIRPORT
 AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

11. UNDERGROUND UTILITIES

- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.
- IT IS ASSUMED THAT **MOST, BUT NOT ALL** UTILITIES WITHIN THE PROJECT AREA WILL BE PREVIOUSLY ABANDONED. CONTRACTOR TO VERIFY UTILITIES ARE ABANDONED PRIOR TO REMOVALS TAKING PLACE AT NO ADDITIONAL COST TO CONTRACT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION SAFETY AND PHASING PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION SAFETY AND PHASING PLAN.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE INTERLOCKING LOW PROFILE BARRICADES. INTERLOCKING LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE INTERLOCKED WITH NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II OR TYPE III BARRICADES AND SHALL CONFORM TO IDOT STANDARD DETAIL 701901-04.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION







- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
- LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

- ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA, WILL REQUIRE THE RUNWAY TO BE CLOSED.
- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

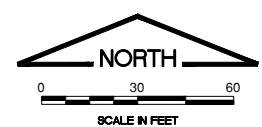
19. OTHER LIMITATIONS ON CONSTRUCTION

- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

- LEGEND**
-  NEW INLET PROTECTION
 -  LIMITS OF SEEDING, HEAVY-DUTY MULCH AND TOPSOILING
 -  NEW STORM STRUCTURE
 -  NEW STORM SEWER
 -  EXISTING STORM SEWER
 -  EXISTING STRUCTURE

- NOTES**
1. SEE GEOMETRIC DATA TABLE ON "EXISTING CONDITIONS/PROPOSED REMOVALS" SHEET FOR ADDITIONAL LAYOUT INFORMATION
 2. SEE "GRADING & DRAINAGE PLAN" FOR GRADING AND EARTHWORK INFORMATION
 3. SEE "MISCELLANEOUS DETAILS" FOR INLET PROTECTION DETAIL
 4. AT THE COMPLETION OF CONSTRUCTION, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITION. RUTTED AREAS SHALL BE FILLED AND THE AREA SEEDED AND MULCHED AS NEEDED. IF THE SOIL STOCKPILE IS UTILIZED, IT SHALL BE SEEDED AND MULCHED AS WELL. RESTORATION COSTS SHALL BE BORNE BY THE CONTRACTOR.
 5. ALL DISTURBED TURF AREAS SHALL RECEIVE 4" OF TOPSOILING. THIS COST SHALL BE INCIDENTAL TO UNCLASSIFIED EXCAVATION.

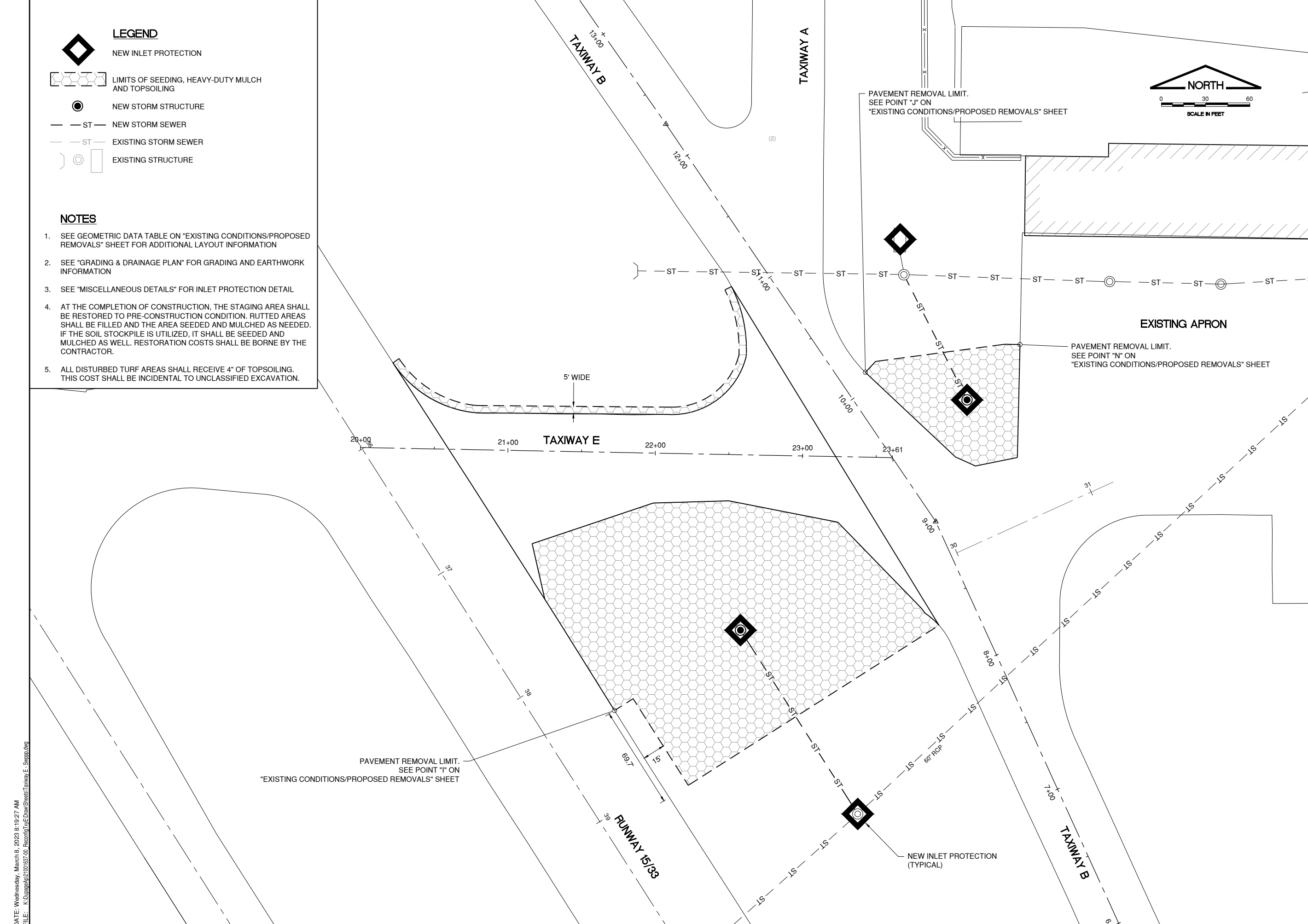
IL. CONTRACT: **DU092**
 IL. LETTING ITEM: **09A**
 IL. PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**



SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

STORM WATER POLLUTION PREVENTION PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

**DUPAGE AIRPORT
 AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

DATE: Wednesday, March 8, 2023 8:19:27 AM
 FILE: K:\dupage\021001637-00_Plan\01\DWG\DrawSheets\Taxiway E_Swmp.dwg

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF REHABILITATION OF AN EXISTING BITUMINOUS PAVEMENT AT DIXON MUNICIPAL AIRPORT. THE PROJECT INCLUDES TURF SHOULDER ADJUSTMENT, VARIOUS PAVEMENT ITEMS, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL. SUCH AS:

INLET PROTECTION.

VARIABLE DEPTH BITUMINOUS PAVEMENT MILLING AND PAVING.

TURF SHOULDER ADJUSTMENT, SEEDING AND MULCHING.

INSTALLATION OF NEW PAVEMENT MARKING.

REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE IS ESTIMATED TO BE 1.8 ACRES OF WHICH 1.3 ACRES WILL BE DISTURBED BY GRADING AND OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO XXX THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

AS SOON AS REASONABLE ACCESS IS AVAILABLE TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, INLET PROTECTIONS SHALL BE INSTALLED AS CALLED OUT IN THE PLAN AND DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR UNCLASSIFIED EXCAVATION AND EROSION CONTROL ITEMS.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCLUDED INCLUDED IN THE UNIT BID PRICE FOR THE VARIOUS TEMPORARY EROSION CONTROL PAY ITEMS.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

DOCUMENTATION:

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF INTENT (NOI)" PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF THIS IS NOT POSSIBLE, THEN IT MAY BE PERMITTED TO POST THIS NOTICE IN A LOCAL PUBLIC BUILDING. THE SIGN OR NOTICE MUST CONTAIN THE FOLLOWING:

1. A COPY OF THE COMPLETED NOTICE OF INTENT (NOI) AS SUBMITTED TO THE IEPA
2. THE LOCATION OF THE SWPPP AND NAME AND 24/7 TELEPHONE NUMBER OF THE CONTACT PERSON.

THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL MAINTAIN AND UPDATE AN "AS-BUILT" SET OF STORM WATER POLLUTION PREVENTION PLANS IN THE PROJECT FILES. THE SWPPP SHALL BE UPDATED WITHIN 7-DAYS OF ANY MODIFICATIONS TO THE PLANS. THE SWPPP AND ALL REVISIONS SHALL BE RETAINED FOR THREE YEARS AFTER FINAL STABILIZATION OF THE SITE, WHICH SHALL BE DEFINED AS VEGETATION COVER OF AT LEAST 70% OF HISTORIC CONDITIONS.

A STORM WATER POLLUTION PREVENTION PLAN EROSION CONTROL INSPECTION REPORT (FORM BC 2259) SHALL BE BE COMPLETED WITH INSPECTION FREQUENCIES AS OUTLINED HEREIN. SWPPP REPORTS SHALL BE RETAINED FOR THREE YEARS AFTER THE DATE OF FINAL STABILIZATION AS DEFINED HEREIN.

IF ANY VIOLATION OF THE PROVISIONS OF THE PLAN IS IDENTIFIED DURING THE CONDUCT OF THE CONSTRUCTION COVERED IN THIS PLAN, THE ENGINEER AND/OR CONTRACTOR SHALL COMPLETE AND FILE AN "INCIDENT OF NONCOMPLIANCE (ION)" REPORT FOR THE IDENTIFIED VIOLATION. THE FORMS SHALL BE AS PROVIDED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND SHALL INCLUDE SPECIFIC INFORMATION ON THE INCIDENT THAT CAUSED NONCOMPLIANCE, ACTIONS THAT WERE TAKEN TO CORRECT THE NONCOMPLIANCE AND TO PREVENT ITS' REOCCURRENCE, AND A STATEMENT DETAILING ANY ENVIRONMENTAL IMPACT WHICH MAY HAVE RESULTED FROM THE NONCOMPLIANCE. ALL REPORTS OF NONCOMPLIANCE SHALL BE SIGNED BY A RESPONSIBLE AUTHORITY IN ACCORDANCE WITH PART VI. G. OF THE GENERAL PERMIT.

AFTER PROJECT FINAL ACCEPTANCE, THE CONTRACTOR SHALL COMPLETE AND SUBMIT A "NOTICE OF TERMINATION (NOT)" FORM PROPERLY SIGNED TO THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY. FORMS FOR THE IEPA SHALL BE MAILED TO THE FOLLOWING ADDRESS:

ILLINOIS ENVIRONMENTAL PROTECTION AGENCY
DIVISION OF WATER POLLUTION CONTROL, MAIL CODE #15
ATTN: PERMIT SECTION
1021 NORTH GRAND AVENUE EAST
P.O. BOX 19276
SPRINGFIELD, ILLINOIS 62794-9276

NPDES PERMIT # _____
DATE ISSUED _____
DATE EXPIRED _____

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, STEPHENSON COUNTY, FREEPORT-ALBERTUS AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

CONTRACTOR CERTIFICATION STATEMENT

THIS CERTIFICATION STATEMENT IS A PART OF THE STORM WATER POLLUTION PREVENTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY.

PROJECT INFORMATION:
ROUTE: DUPAGE AIRPORT MARKED: RECONFIGURATION OF TAXIWAY E BETWEEN RUNWAY 15/33 AND TAXIWAY B
SECTION: _____ PROJECT NUMBER: _____
COUNTY: DUPAGE CONTRACT NUMBER: _____

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS AND CONDITIONS OF THE GENERAL NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT (ILR10) THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE: _____ DATE: _____
PRINTED NAME: _____ TITLE: _____
NAME OF FIRM: _____
STREET ADDRESS: _____
CITY, STATE, ZIP: _____
PHONE NUMBER: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA. COMPLETION OF THIS IS A CONTRACT REQUIREMENT.

RECORD OF SITE DISTURBANCE AND STABILIZATION

MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
MAJOR GRADING ACTIVITIES: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
SITE STABILIZATION: LOCATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____
CONSTRUCTION CEASED: EXPLANATION: _____	BEGINNING DATE: _____ COMPLETION DATE: _____

THE INFORMATION WITHIN THIS BOX SHALL BE COMPLETED BY THE CONTRACTOR AS CONSTRUCTION PROGRESSES IN ACCORDANCE WITH THE NPDES GENERAL PERMIT FOR STORMWATER DISCHARGES. THIS INFORMATION MAY ALSO BE NOTED DIRECTLY ON THE SWPPP SITE MAP.

IL CONTRACT: **DU092**

IL LETTING ITEM: **09A**

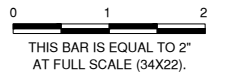
IL PROJECT: **DPA-4926**

S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK # _____

REVISIONS

NUMBER	BY	DATE



**DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RECONFIGURATION OF TAXIWAY E
BETWEEN RUNWAY 15/33 AND TAXIWAY B**

**STORM WATER POLLUTION PREVENTION PLAN
NOTES**

© Copyright CMT, Inc.

CMT
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

**DUPAGE AIRPORT
AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

NOTES

1. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE BROOMED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
2. THE DETERMINATION AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT SHALL BE COORDINATED WITH THE RESIDENT ENGINEER AFTER INSPECTION OF THE MILLED SURFACE.
3. EXISTING DUCT MARKERS SHALL BE SURVEYED BY THE CONTRACTOR BEFORE SURFACE IS MILLED. DUCT MARKERS SHALL BE REPLACED AT SAME LOCATIONS SURVEYED (COST INCIDENTAL TO THE CONTRACT).
4. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
5. SEE GEOMETRIC PLAN FOR NEW PAVEMENT EDGE GEOMETRIC DATA.
6. EXISTING LIGHTS, GUIDANCE SIGNS, AND OTHER AIRFIELD CIRCUITS NOT TO BE REPLACED ARE TO BE PROTECTED FROM DAMAGE. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT HIS EXPENSE.
7. LIGHT FIXTURES, LAMPS AND TRANSFORMERS TO BE REMOVED SHALL BE TURNED OVER TO AIRPORT MAINTENANCE. IF AIRPORT MAINTENANCE DOES NOT WANT ANY OF THE REMOVED MATERIALS THEN THE CONTRACTOR SHALL DISPOSE OF OFF AIRPORT PROPERTY AT NO ADDITIONAL COST TO THE CONTRACT. LIGHT BASES AND SIGN FOUNDATIONS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY.
8. WHEN POSSIBLE AND WHEN CABLES INSTALLED IN CONDUITS/UNIT DUCT, CONTRACTOR SHALL REMOVE ABANDONED CABLE FROM EXISTING CONDUIT. COST SHALL BE INCIDENTAL TO THE CONTRACT.

**GEOMETRIC DATA -
NEW PAVEMENT EDGE**

POINT	STATION / OFFSET
A	STA. 21+26.95, 100.62' RT. NEW TAXIWAY E CENTERLINE
B	STA. 21+17.61, 63.03' RT. NEW TAXIWAY E CENTERLINE
C	STA. 21+99.30, 33.88' RT. NEW TAXIWAY E CENTERLINE
D	STA. 22+50.56, 31.42' RT. NEW TAXIWAY E CENTERLINE
E	STA. 23+24.76, 44.51' RT. NEW TAXIWAY E CENTERLINE
F	STA. 23+85.22, 105.20' RT. NEW TAXIWAY E CENTERLINE
G	STA. 23+58.86, 97.28' RT. NEW TAXIWAY E CENTERLINE
H	STA. 22+17.26, 97.88' RT. NEW TAXIWAY E CENTERLINE
I	STA. 21+75.66, 175.20' RT. NEW TAXIWAY E CENTERLINE
J	STA. 23+41.50, 57.53' LT. NEW TAXIWAY E CENTERLINE
K	STA. 24+03.75, 1.27' LT. NEW TAXIWAY E CENTERLINE
L	STA. 24+17.46, 4.56' RT. NEW TAXIWAY E CENTERLINE
M	STA. 24+45.81, 1.74' LT. NEW TAXIWAY E CENTERLINE
N	STA. 24+46.11, 78.40' LT. NEW TAXIWAY E CENTERLINE

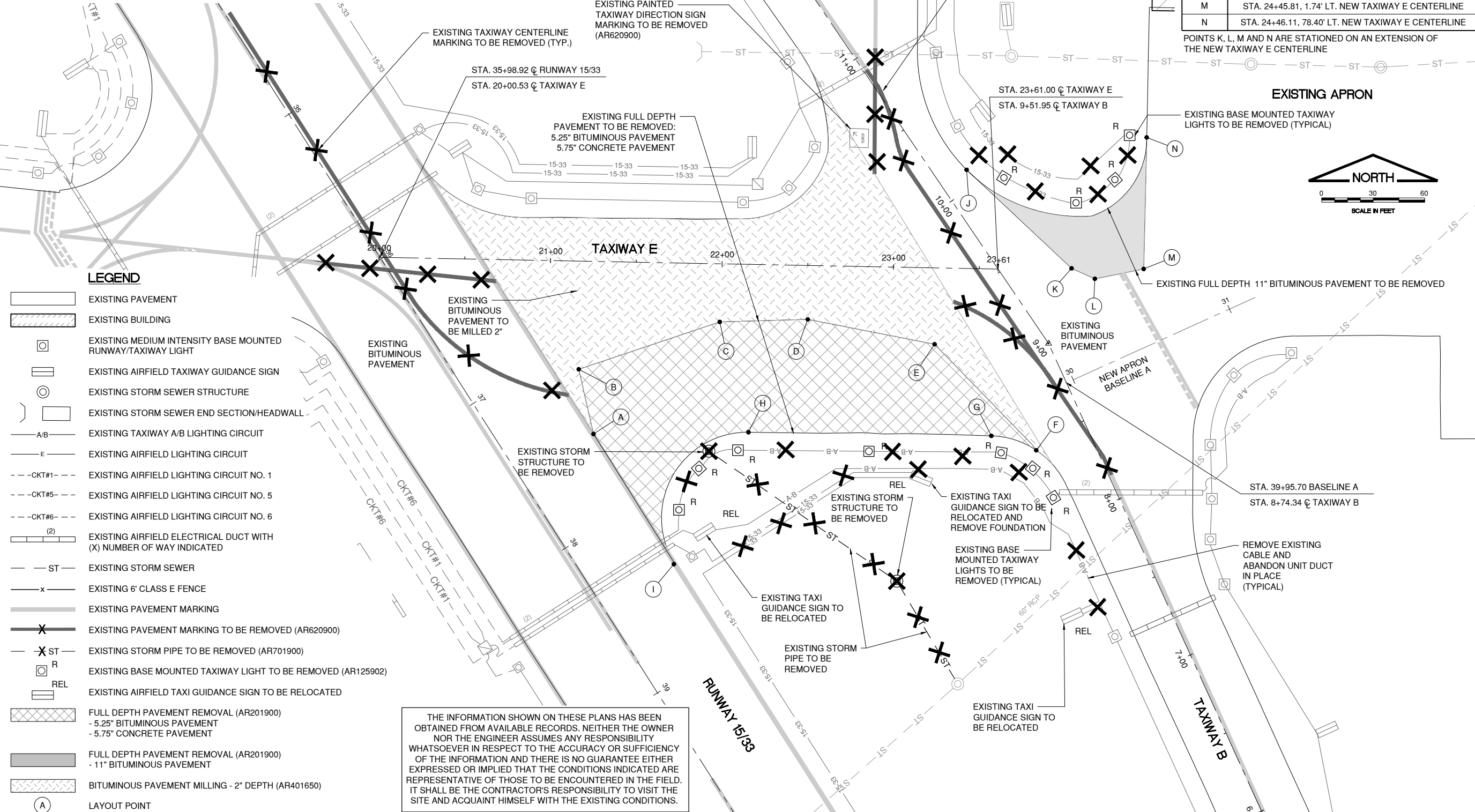
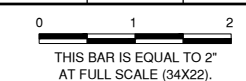
POINTS K, L, M AND N ARE STATIONED ON AN EXTENSION OF THE NEW TAXIWAY E CENTERLINE

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE



LEGEND

- EXISTING PAVEMENT
- EXISTING BUILDING
- EXISTING MEDIUM INTENSITY BASE MOUNTED RUNWAY/TAXIWAY LIGHT
- EXISTING AIRFIELD TAXIWAY GUIDANCE SIGN
- EXISTING STORM SEWER STRUCTURE
- EXISTING STORM SEWER END SECTION/HEADWALL
- EXISTING TAXIWAY A/B LIGHTING CIRCUIT
- EXISTING AIRFIELD LIGHTING CIRCUIT
- EXISTING AIRFIELD LIGHTING CIRCUIT NO. 1
- EXISTING AIRFIELD LIGHTING CIRCUIT NO. 5
- EXISTING AIRFIELD LIGHTING CIRCUIT NO. 6
- EXISTING AIRFIELD ELECTRICAL DUCT WITH (X) NUMBER OF WAY INDICATED
- EXISTING STORM SEWER
- EXISTING 6' CLASS E FENCE
- EXISTING PAVEMENT MARKING
- EXISTING PAVEMENT MARKING TO BE REMOVED (AR620900)
- EXISTING STORM PIPE TO BE REMOVED (AR701900)
- EXISTING BASE MOUNTED TAXIWAY LIGHT TO BE REMOVED (AR125902)
- EXISTING AIRFIELD TAXI GUIDANCE SIGN TO BE RELOCATED
- FULL DEPTH PAVEMENT REMOVAL (AR201900) - 5.25" BITUMINOUS PAVEMENT
- FULL DEPTH PAVEMENT REMOVAL (AR201900) - 5.75" CONCRETE PAVEMENT
- BITUMINOUS PAVEMENT MILLING - 2" DEPTH (AR401650)
- LAYOUT POINT

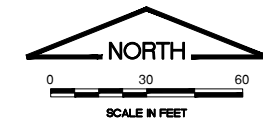
THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B
EXISTING CONDITIONS / PROPOSED REMOVALS

© Copyright CMT, Inc.
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

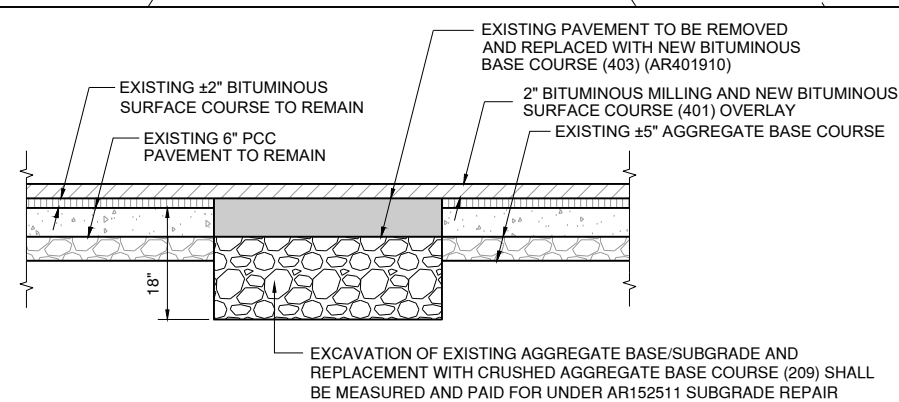
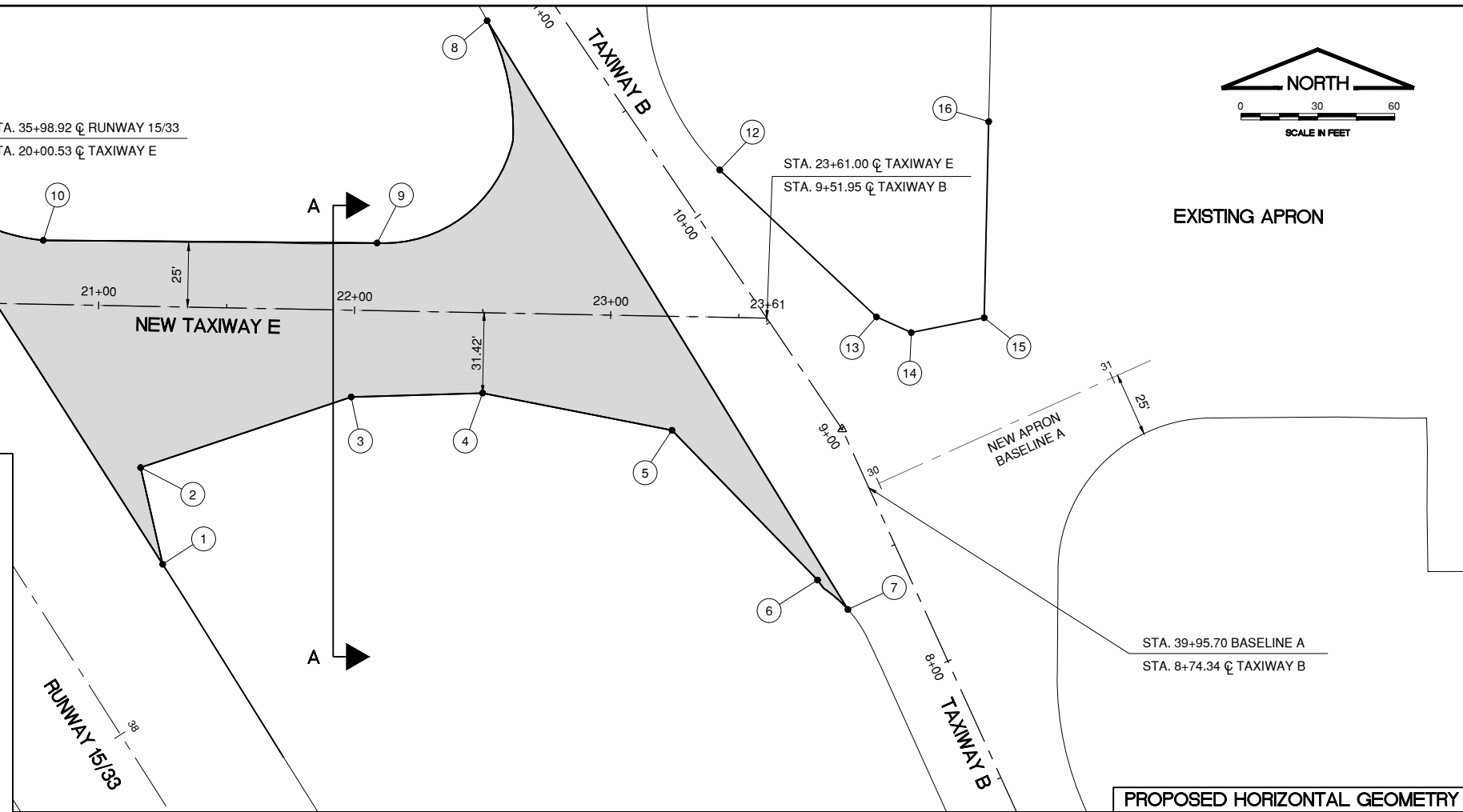
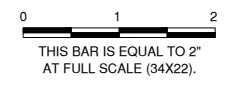
DESIGN BY: ABM
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 3/3/2023
 JOB No: 21001637.00

DATE: Wednesday, March 8, 2023 8:19:52 AM
 FILE: K:\dupage\21001637-00_Paving\17-Draw\Sheets\Taxiway E - Existing Conditions.dwg



SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

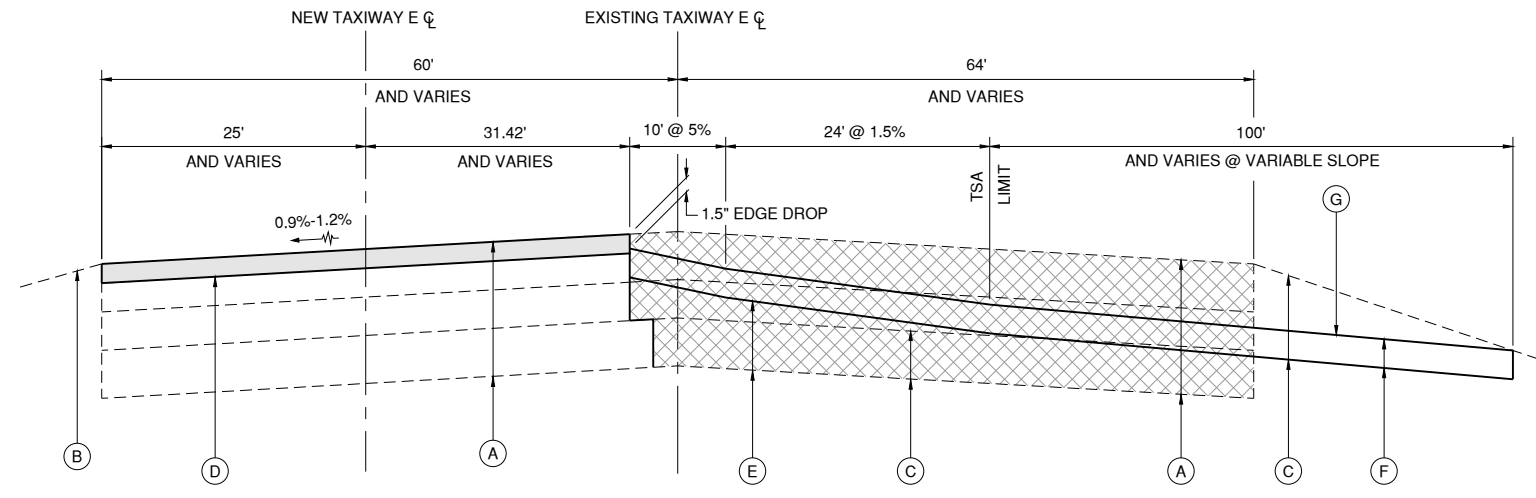


PAVEMENT REMOVAL & REPLACEMENT DETAIL
 NOT TO SCALE

- NOTES**
- LOCATION OF SUBGRADE REPAIR SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER.
 - MEASUREMENT AND PAYMENT SHALL BE MADE UNDER AR152511 SUBGRADE REPAIR AND SHALL INCLUDE ALL EXCAVATION (AGGREGATE AND SUBGRADE) AND FULL DEPTH REPLACEMENT WITH CRUSHED AGGREGATE BASE (209) TO MEET THE FINAL GRADES OF THE ADJACENT AGGREGATE BASE TO REMAIN.

GEOMETRIC DATA - NEW/EXISTING PAVEMENT EDGE	
POINT	STATION / OFFSET
1	STA. 21+26.95, 100.62' RT. NEW TAXIWAY E CENTERLINE
2	STA. 21+17.61, 63.03' RT. NEW TAXIWAY E CENTERLINE
3	STA. 21+99.30, 33.88' RT. NEW TAXIWAY E CENTERLINE
4	STA. 22+50.56, 31.42' RT. NEW TAXIWAY E CENTERLINE
5	STA. 23+24.76, 44.51' RT. NEW TAXIWAY E CENTERLINE
6	STA. 23+82.70, 101.89' RT. NEW TAXIWAY E CENTERLINE
7	STA. 24+94.77, 113.17' RT. NEW TAXIWAY E CENTERLINE
8	STA. 22+49.56, 114.06' LT. NEW TAXIWAY E CENTERLINE
9	STA. 22+08.26, 26.43' LT. NEW TAXIWAY E CENTERLINE
10	STA. 20+77.92, 25.00' RT. NEW TAXIWAY E CENTERLINE
11	STA. 20+20.72, 58.07' RT. NEW TAXIWAY E CENTERLINE
12	STA. 23+41.50, 57.53' LT. NEW TAXIWAY E CENTERLINE
13	STA. 24+03.75, 1.27' LT. NEW TAXIWAY E CENTERLINE
14	STA. 24+17.46, 4.56' RT. NEW TAXIWAY E CENTERLINE
15	STA. 24+45.81, 1.74' LT. NEW TAXIWAY E CENTERLINE
16	STA. 24+46.11, 78.40' LT. NEW TAXIWAY E CENTERLINE

- LEGEND**
- EXISTING PAVEMENT TO BE MILLED 2" AND OVERLAYED (401) WITH 2" BITUMINOUS SURFACE COURSE
 - EXISTING PAVEMENT TO BE REMOVED (401)
 - (A) EXISTING PAVEMENT STRUCTURE
5" AVERAGE BITUMINOUS SURFACE
6" AVERAGE PCC PAVEMENT
5" AND VARIABLE STONE BASE
 - (B) EXISTING GROUNDLINE
 - (C) UNCLASSIFIED EXCAVATION (152)
 - (D) NEW EMULSIFIED ASPHALT TACK COAT (603)
 - (E) NEW SHOULDER FILL (152)
 - (F) NEW 4" AVERAGE TOPSOIL PLACEMENT (905)
 - (G) NEW SEEDING (901) AND MULCHING (908)



TAXIWAY E TYPICAL SECTION A-A
 NOT TO SCALE

**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

GEOMETRIC PLAN & TYPICAL SECTION

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

**DUPAGE AIRPORT
 AUTHORITY**

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

LEGEND

\circ 752.20
 \circ 753.27

NEW ELEVATION
 EXISTING ELEVATION

NEW 2" BITUMINOUS SURFACE COURSE (401)

NORTH

0 20 40

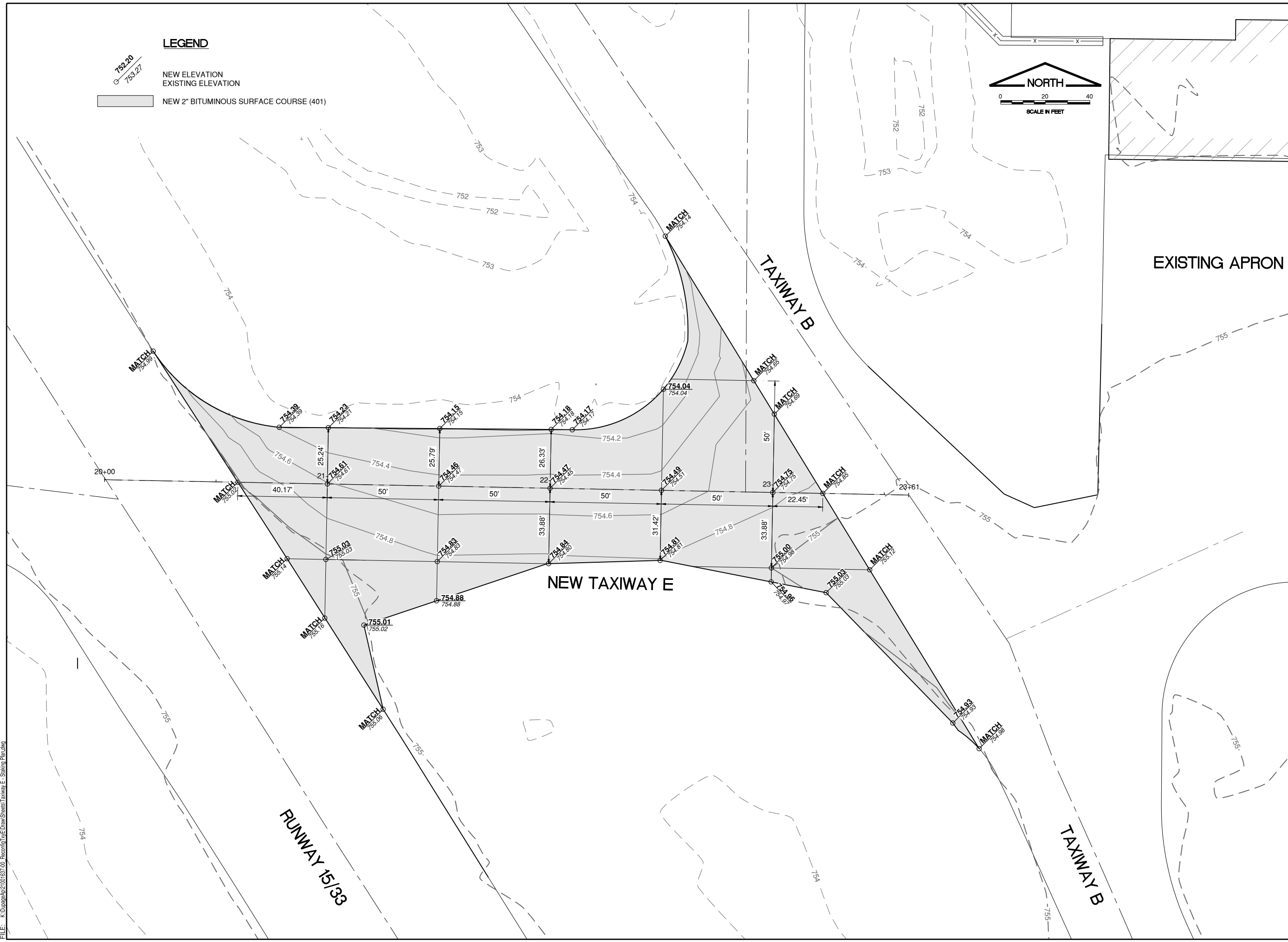
SCALE IN FEET

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

REVISIONS		
NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



EXISTING APRON

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B

STAKING PLAN

DATE: Wednesday, March 8, 2023 8:20:12 AM
 FILE: K:\Dupage\021001637-00_Plan\01\Draw\Sheets\Taxiway E - Staking Plan.dwg

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DUPAGE AIRPORT AUTHORITY

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

DRAINAGE STRUCTURE SCHEDULE						
STRUCTURE	TYPE	RIM	INVERT	NORTHING	EASTING	STATION/OFFSET
S1	NEW TYPE A INLET W/ TYPE 1 FRAME AND OPEN LID	753.50'	NEW 10" PVC OUT = 751.50' (N)	1911175.9209	1009791.0734	9+55.92, 64.00' RT. TAXIWAY B
S2 (EXISTING)	EXISTING 4" DIA. MANHOLE	754.22' (EXISTING)	NEW 10" PVC IN = 751.22' (S) EX. 15" CMP IN = 750.72' (W) EX. 18" RCP IN = 750.42' (N) EX. 24" RCP OUT = 750.02' (E)	1911260.8550	1009748.2050	10+50.27, 76.23' RT. TAXIWAY B
S3	NEW TYPE A INLET W/ TYPE 1 FRAME AND OPEN LID	752.03'	NEW 12" RCP OUT = 750.03' (SE)	1911019.6117	1009637.2904	9+12.94, 151.00' LT. TAXIWAY B
S4 (EXISTING)	EXISTING 8" DIA. MANHOLE	753.84' (EXISTING)	NEW 12" RCP IN = 749.29' (NW) EX. 60" RCP IN = 742.34' (NE) EX. 60" RCP OUT = 742.34' (SW)	1910894.8752	1009716.8761	7+40.85, 130.40' LT. TAXIWAY B

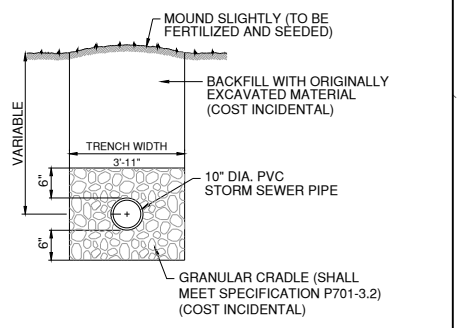
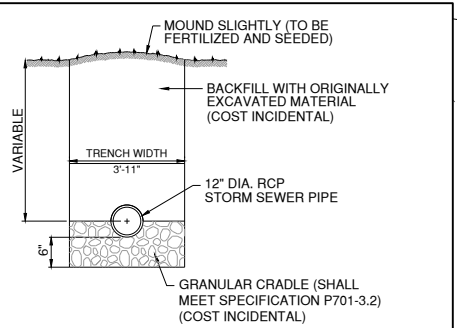
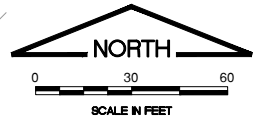
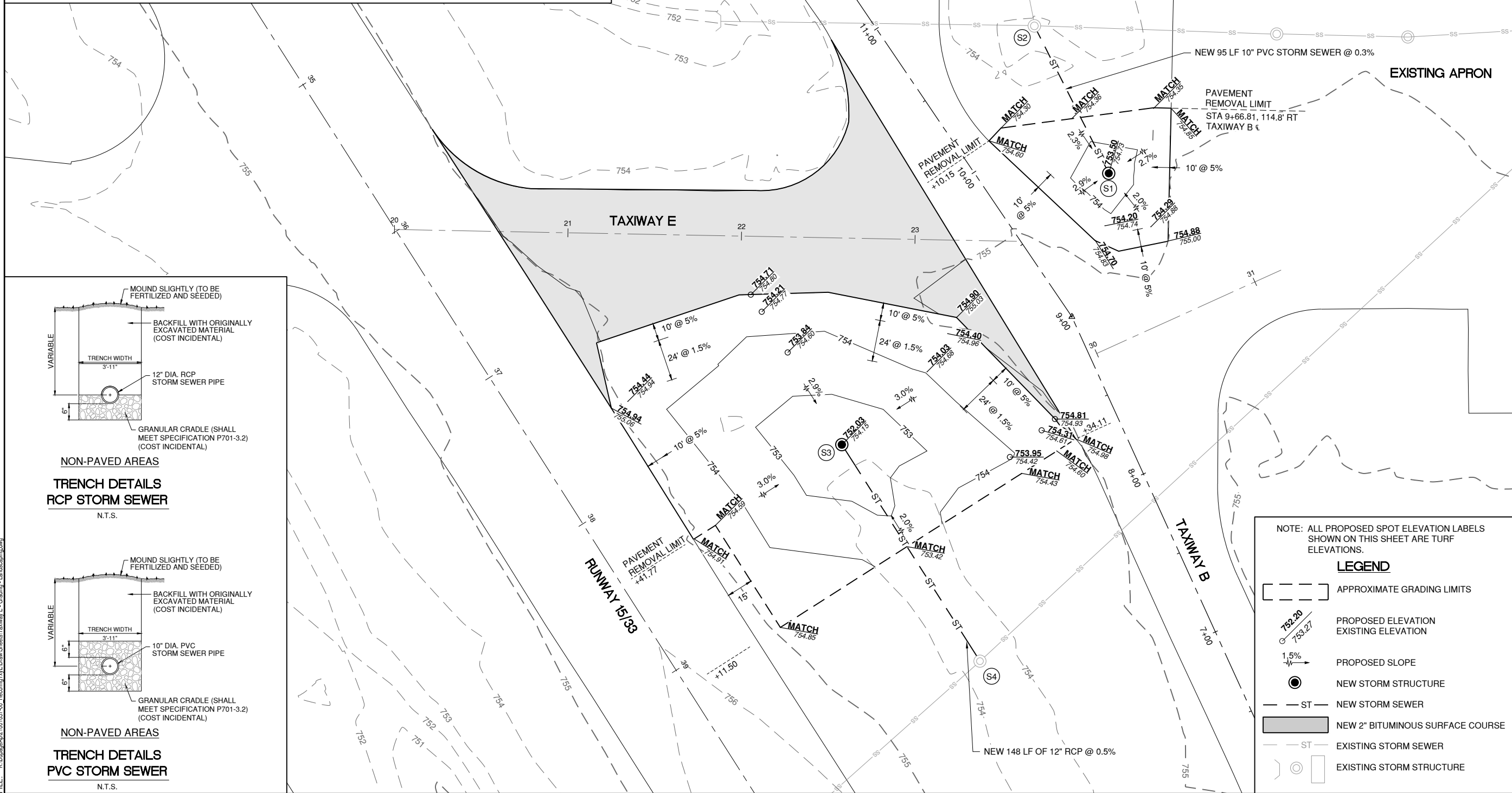
- NOTES:
- STATION AND OFFSET IS TO CENTER OF STRUCTURE
 - NEW TYPE A INLETS SHALL BE PER IDOT STANDARD 602301-04
 - CONTRACTOR SHALL VERIFY RIM AND INVERT ELEVATIONS ON EXISTING MANHOLES THAT ARE TO BE CONNECTED TO (INCIDENTAL TO CONTRACT)
 - CONTRACTOR SHALL CORE INTO EXISTING MANHOLES TO CONNECT NEW PVC STORM PIPE (COST INCIDENTAL)

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



NOTE: ALL PROPOSED SPOT ELEVATION LABELS SHOWN ON THIS SHEET ARE TURF ELEVATIONS.

LEGEND

- APPROXIMATE GRADING LIMITS
- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED SLOPE
- NEW STORM STRUCTURE
- NEW STORM SEWER
- NEW 2" BITUMINOUS SURFACE COURSE
- EXISTING STORM SEWER
- EXISTING STORM STRUCTURE

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RECONFIGURATION OF TAXIWAY E
BETWEEN RUNWAY 15/33 AND TAXIWAY B

GRADING & DRAINAGE PLAN

© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DUPAGE AIRPORT
AUTHORITY

DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00

DATE: Wednesday, March 8, 2023, 8:20:24 AM
 FILE: K:\Dupage\21001637-00_Plan\TAXIWAY E\DrawSheets\Taxiway E_Grading_Landscaping.dwg



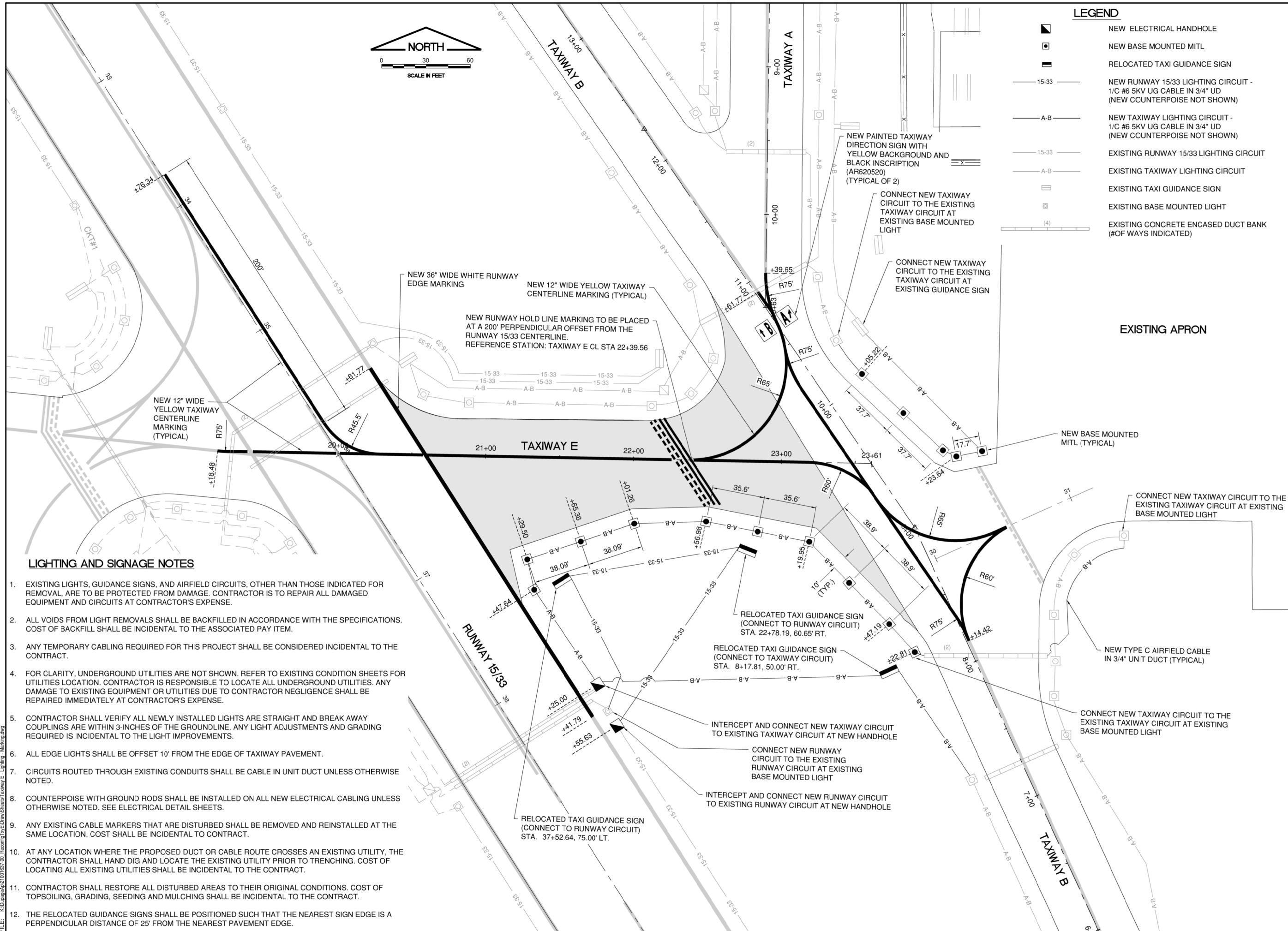
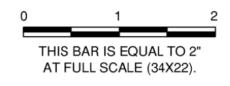
LEGEND

- NEW ELECTRICAL HANDHOLE
- NEW BASE MOUNTED MITL
- RELOCATED TAXI GUIDANCE SIGN
- 15-33 NEW RUNWAY 15/33 LIGHTING CIRCUIT - 1/C #6 5KV UG CABLE IN 3/4" UD (NEW COUNTERPOISE NOT SHOWN)
- A-B NEW TAXIWAY LIGHTING CIRCUIT - 1/C #6 5KV UG CABLE IN 3/4" UD (NEW COUNTERPOISE NOT SHOWN)
- 15-33 EXISTING RUNWAY 15/33 LIGHTING CIRCUIT
- A-B EXISTING TAXIWAY LIGHTING CIRCUIT
- EXISTING TAXI GUIDANCE SIGN
- EXISTING BASE MOUNTED LIGHT
- (4) EXISTING CONCRETE ENCASED DUCT BANK (#OF WAYS INDICATED)

IL CONTRACT: **DU092**
 IL LETTING ITEM: **09A**
 IL PROJECT: **DPA-4926**
 S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



LIGHTING AND SIGNAGE NOTES

1. EXISTING LIGHTS, GUIDANCE SIGNS, AND AIRFIELD CIRCUITS, OTHER THAN THOSE INDICATED FOR REMOVAL, ARE TO BE PROTECTED FROM DAMAGE. CONTRACTOR IS TO REPAIR ALL DAMAGED EQUIPMENT AND CIRCUITS AT CONTRACTOR'S EXPENSE.
2. ALL VOIDS FROM LIGHT REMOVALS SHALL BE BACKFILLED IN ACCORDANCE WITH THE SPECIFICATIONS. COST OF BACKFILL SHALL BE INCIDENTAL TO THE ASSOCIATED PAY ITEM.
3. ANY TEMPORARY CABLING REQUIRED FOR THIS PROJECT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. FOR CLARITY, UNDERGROUND UTILITIES ARE NOT SHOWN. REFER TO EXISTING CONDITION SHEETS FOR UTILITIES LOCATION. CONTRACTOR IS RESPONSIBLE TO LOCATE ALL UNDERGROUND UTILITIES. ANY DAMAGE TO EXISTING EQUIPMENT OR UTILITIES DUE TO CONTRACTOR NEGLIGENCE SHALL BE REPAIRED IMMEDIATELY AT CONTRACTOR'S EXPENSE.
5. CONTRACTOR SHALL VERIFY ALL NEWLY INSTALLED LIGHTS ARE STRAIGHT AND BREAK AWAY COUPLINGS ARE WITHIN 3-INCHES OF THE GROUNDLINE. ANY LIGHT ADJUSTMENTS AND GRADING REQUIRED IS INCIDENTAL TO THE LIGHT IMPROVEMENTS.
6. ALL EDGE LIGHTS SHALL BE OFFSET 10' FROM THE EDGE OF TAXIWAY PAVEMENT.
7. CIRCUITS ROUTED THROUGH EXISTING CONDUITS SHALL BE CABLE IN UNIT DUCT UNLESS OTHERWISE NOTED.
8. COUNTERPOISE WITH GROUND RODS SHALL BE INSTALLED ON ALL NEW ELECTRICAL CABLING UNLESS OTHERWISE NOTED. SEE ELECTRICAL DETAIL SHEETS.
9. ANY EXISTING CABLE MARKERS THAT ARE DISTURBED SHALL BE REMOVED AND REINSTALLED AT THE SAME LOCATION. COST SHALL BE INCIDENTAL TO CONTRACT.
10. AT ANY LOCATION WHERE THE PROPOSED DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
11. CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS TO THEIR ORIGINAL CONDITIONS. COST OF TOPSOILING, GRADING, SEEDING AND MULCHING SHALL BE INCIDENTAL TO THE CONTRACT.
12. THE RELOCATED GUIDANCE SIGNS SHALL BE POSITIONED SUCH THAT THE NEAREST SIGN EDGE IS A PERPENDICULAR DISTANCE OF 25' FROM THE NEAREST PAVEMENT EDGE.

**DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 RECONFIGURATION OF TAXIWAY E
 BETWEEN RUNWAY 15/33 AND TAXIWAY B**

LIGHTING PLAN / PAVEMENT MARKING PLAN

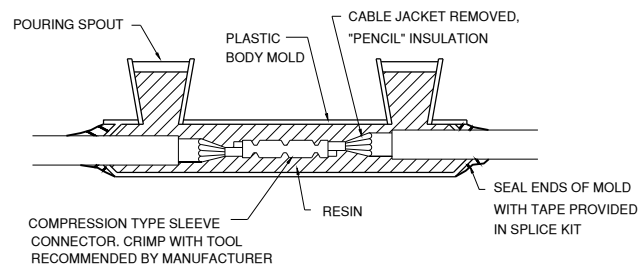
© Copyright CMT, Inc.

CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

**DUPAGE AIRPORT
 AUTHORITY**

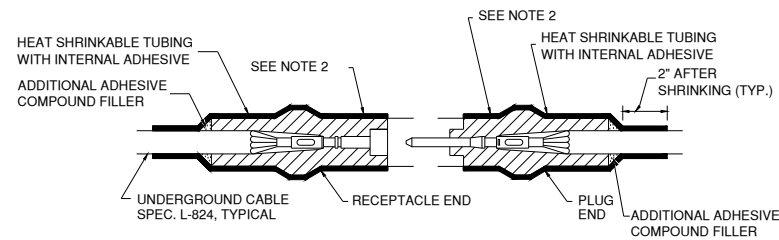
DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001537.00
SHEET	14 OF 17 SHEETS

DATE: Wednesday, March 8, 2023 4:38:21 PM
 FILE: K:\dupage\092101837.00_Pavement\Draw Sheets\Taxiway E_Lighting_Markings.dwg



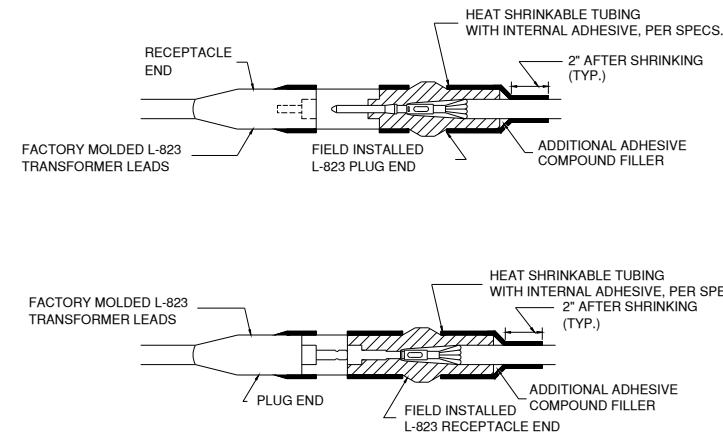
TYPE A - CABLE SPLICE

FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY
N.T.S.



TYPE B - CABLE SPLICE

FOR SPLICES FOR USE AT JUNCTION OF HOMERUN WITH LOOP CIRCUIT
NOT TO SCALE

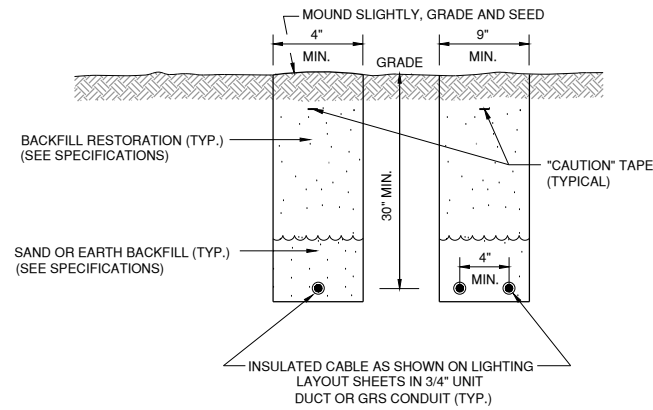


TYPE C AND D - CABLE SPLICE

FOR SPLICES AT RUNWAY/TAXIWAY LIGHTS AND SIGNS
NOT TO SCALE

CABLE SPLICE NOTES

- INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY MATCH THE OUTSIDE DIAMETER OF CABLE.
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR SYNTHETIC RUBBER TAPE AND ONE LAYER OF PLASTIC TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.
- THE COST OF FURNISHING AND INSTALLING ALL SPLICE MATERIALS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- THE CONTRACTOR SHALL HAVE A MINIMUM OF TWO (2) TYPE A SPLICE KITS ON THE JOB SITE AT ALL TIMES FOR EMERGENCY REPAIRS.
- CONTRACTOR MAY INSTALL FAA APPROVED "COMPLETE KIT" IN LIEU OF SPLICE WITH HEAT SHRINK.

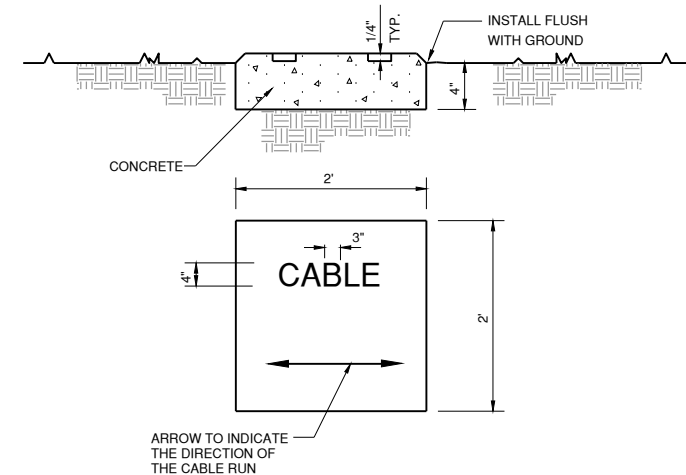


TURF AREA CABLE TRENCH DETAIL

NOT TO SCALE

NOTES

- TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 4" IN WIDTH FOR EACH ADDITIONAL CABLE. IF SPECIFIED ON PLANS, TWO PARALLEL TRENCHES MAY BE CONSTRUCTED.
- DEPTH OF TRENCHES SHALL BE AS SHOWN UNLESS OTHERWISE SPECIFIED ON THE PLANS.
- SAND BACKFILL SHALL BE USED IF THE EXISTING SOIL DOES NOT MEET THE BACKFILL REQUIREMENTS.
- ALL DISTURBED SURFACES SHALL BE RESTORED TO THEIR ORIGINAL CONDITION. COST IS INCIDENTAL.

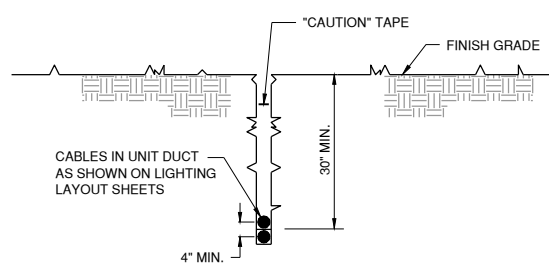


TURF CABLE MARKER DETAIL

NOT TO SCALE

NOTES

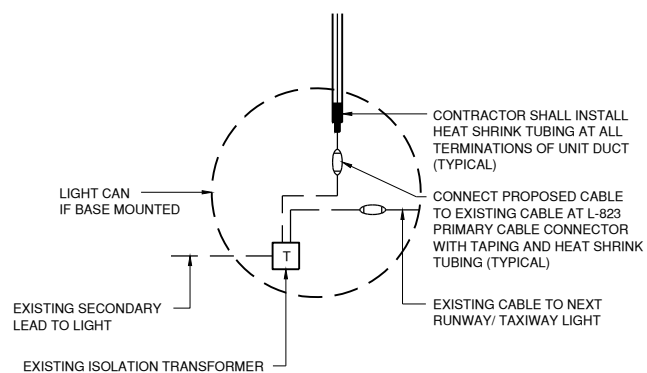
- CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE CABLE RUN.
- ITEM 610 CONCRETE SHALL BE USED.
- ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
- THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED CABLE ITEMS.
- 0.049 CU. YD. CONCRETE PER MARKER.



CABLE IN UNIT DUCT - PLOWED

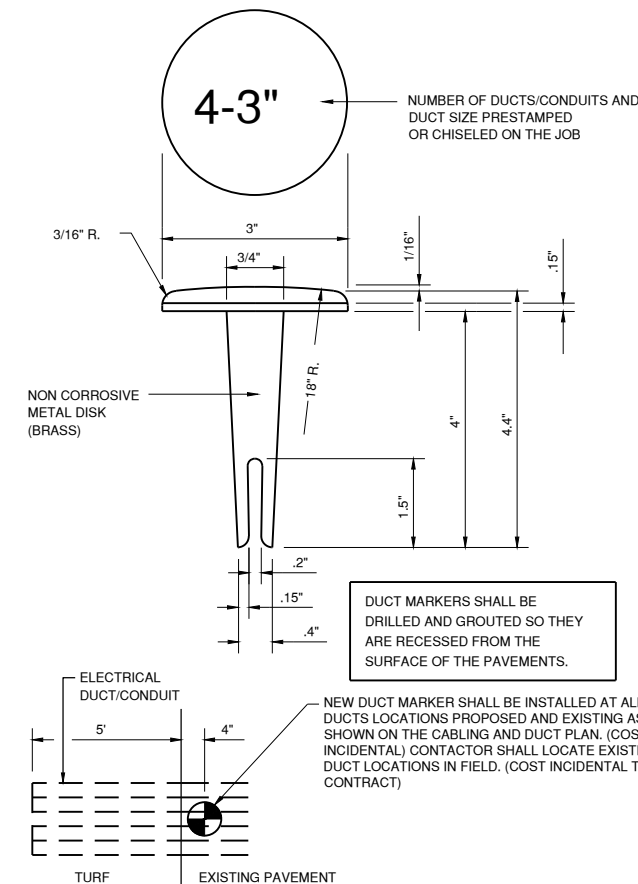
NOT TO SCALE

NOTE: CONTRACTOR SHALL HAVE THE OPTION TO TRENCH OR PLOW UNIT DUCT. NO ADDITIONAL PAYMENT SHALL BE MADE FOR TRENCHING.



RUNWAY/TAXIWAY/SIGN LIGHTING CIRCUIT CONNECTION DETAIL

NOT TO SCALE



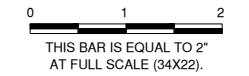
DUCT MARKER DETAIL

NOT TO SCALE

IL. CONTRACT: **DU092**
IL. LETTING ITEM: **09A**
IL. PROJECT: **DPA-4926**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RECONFIGURATION OF TAXIWAY E
BETWEEN RUNWAY 15/33 AND TAXIWAY B

ELECTRICAL DETAILS - 1

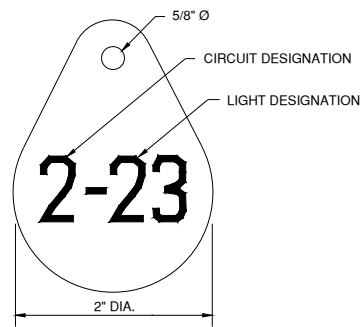
© Copyright CMT, Inc.



CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613



DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00



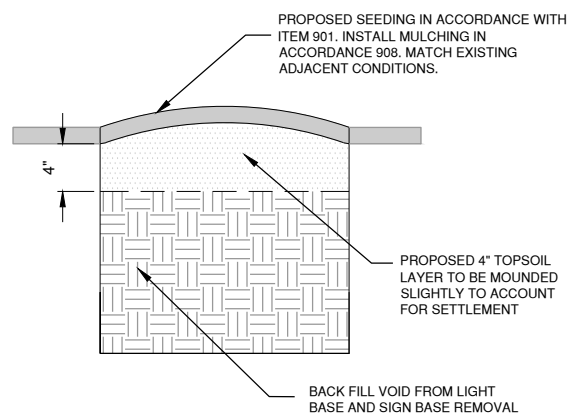
LIGHT IDENTIFICATION DETAIL
NOT TO SCALE

NOTES:

- ON NEW LIGHTS, INSTALL A NONCORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH A SET SCREW.
- NUMERALS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. ALL EXISTING AND PROPOSED TAXIWAY AND RUNWAY LIGHTS SHALL BE TAGGED AS DIRECTED BY THE RESIDENT ENGINEER. ALL LIGHTS ON EXISTING CIRCUITS THAT HAVE LIGHTING IMPROVEMENTS (PROPOSED OR RELOCATED LIGHTS) SHALL BE RETAGGED.
- COST OF TAGGING LIGHTS SHALL NOT BE PAID FOR SEPERATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

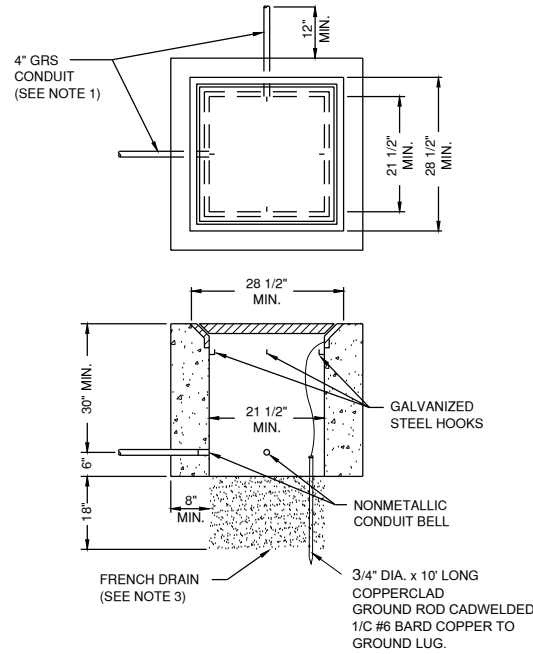
GENERAL NOTES

- TRANSFORMER HOLDER MAY BE ANY COMMERCIALY AVAILABLE BRICK.
- BREAKING GROOVE COUPLINGS SHALL NOT BE OVER 1" ABOVE GROUND LINE.
- ISOLATION TRANSFORMERS SHALL HAVE A FACTORY INSTALLED PLUG (TYPE 1, CLASS A, STYLE 2) AND RECEPTACLE (TYPE 1, CLASS A, STYLE 9). A TYPE 1, CLASS B, STYLE 3 PLUG AND TYPE 1, CLASS B, STYLE 10 RECEPTACLE SHALL BE INSTALLED ON THE 1/C, No. 8, 5000 V., L-824 TYPE C CABLES FOR CONNECTION TO EACH TRANSFORMER.
- TO FURTHER REDUCE THE POSSIBILITY OF WATER/MOISTURE ENTRANCE INTO THE CONNECTOR BETWEEN THE CABLE AND THE FIELD ATTACHED CONNECTOR, IT IS REQUIRED THAT A HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE BE APPLIED OVER THE ENTIRE CABLE CONNECTOR.
- AT THE CONTRACTOR'S OPTION, IN LIEU OF TAPE AND HEAT SHRINKABLE TUBING, A SELF-SEALING STYLE CONNECTOR L-823 "COMPLETE KITS" OR FAA APPROVED EQUAL MAY BE USED.
- LAMP FOR FIXTURES SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS. ISOLATION TRANSFORMERS SHALL BE SIZED PER THE FIXTURE MANUFACTURER, 6.6 AMP.
- ALL GROUND AND COUNTERPOISE WIRES SHALL BE EXOTHERMICALLY WELDED TO GROUND ROD (COST INCIDENTAL).
- THE TOLERANCE FOR THE HEIGHT OF RUNWAY/TAXIWAY EDGE LIGHTS SHALL BE ± 1 INCH. THE TOLERANCE FOR THE LATERAL SPACING (LIGHT LANE TO RUNWAY/TAXIWAY CENTERLINE) OF RUNWAY/TAXIWAY EDGE LIGHTS MUST BE ± 1 INCH.



COMPLETED BASE MOUNTED LIGHT AND SIGN REMOVAL
NOT TO SCALE

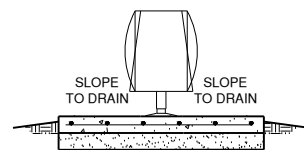
NOTE: COST OF BACKFILL AND RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT.



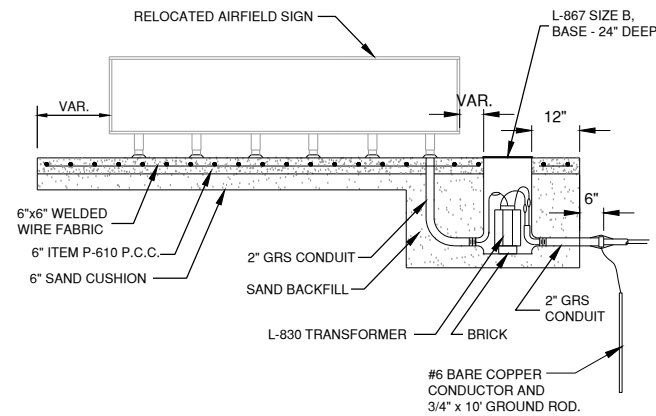
CONCRETE ELECTRICAL HANDHOLE DETAIL STANDARD NO. 814001
NOT TO SCALE

NOTES:

- CONDUIT ENTRANCES AND NUMBER OF CONDUITS SHALL BE AS SHOWN ON SITE PLAN VIEW.
- COVER SHALL BE STAMPED "ELECTRICAL".
- FRENCH DRAIN AGGREGATE SHALL BE CA-18 (COST INCIDENTAL TO HANDHOLE).
- COVER SHALL BE HINGED WITH A SAFETY BAR.

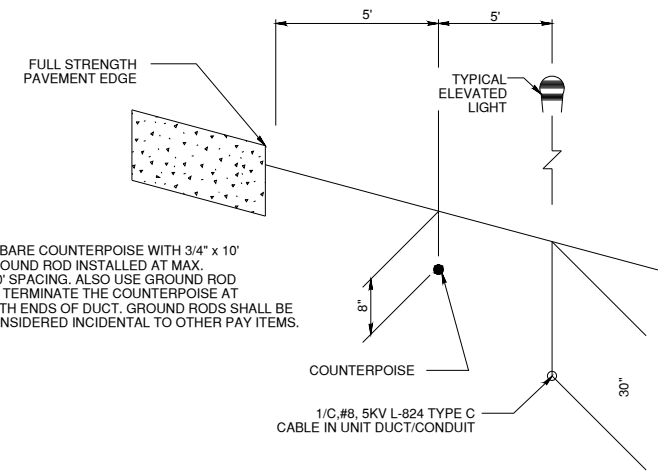


ELEVATION VIEW

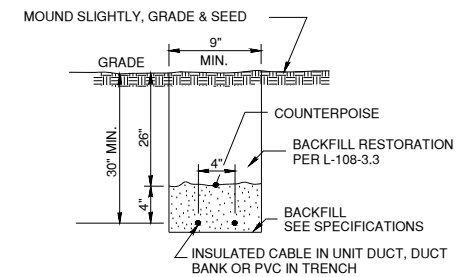


NEW SIGN BASE DETAIL
NOT TO SCALE

NOTE: ALL SHOWN ITEMS ARE NEW EXCEPT RELOCATED SIGN.



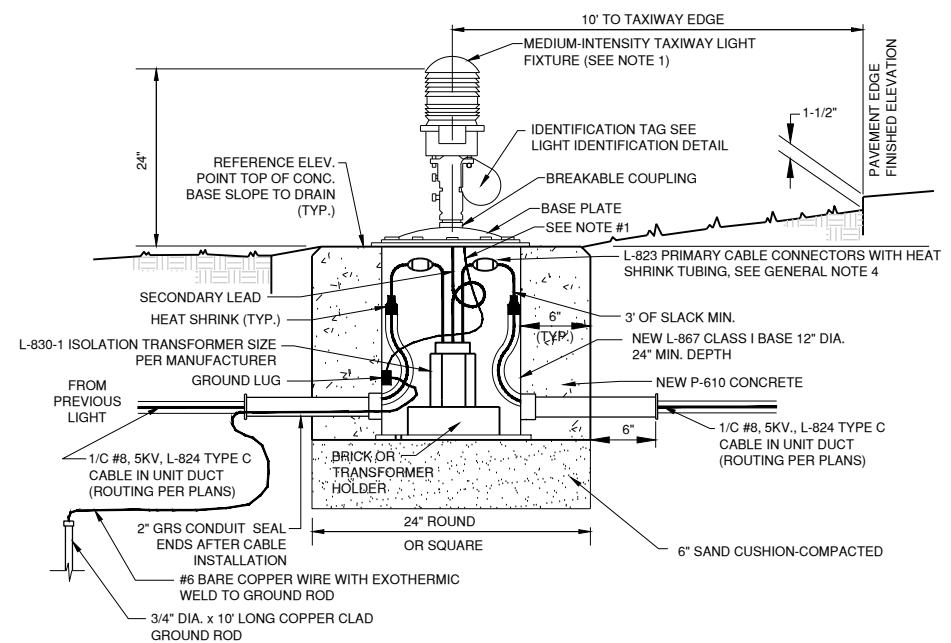
COUNTERPOISE LOCATION DETAIL
NOT TO SCALE



COUNTERPOISE LOCATION DETAIL (NOT ADJACENT TO PAVEMENT)
NOT TO SCALE

COUNTERPOISE NOTES:

- CABLES SHALL NOT BE PLACED LESS THAN 30" DEEP IN ANY ONE TRENCH. CABLES WITH DIFFERENT VOLTAGE RATINGS SHALL NOT BE INSTALLED IN THE SAME TRENCH.
- CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.



BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
NOT TO SCALE

NOTES:

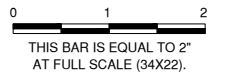
- THE LIGHT FIXTURE SHALL BE BONDED TO THE LIGHT BASE INTERNAL GROUND LUG VIA A #6 AWG STRANDED COPPER WIRE RATED FOR 600 VOLTS WITH GREEN XHHW INSULATION. THE GROUND WIRE LENGTH SHALL BE SUFFICIENT TO ALLOW THE REMOVAL OF THE LIGHT FIXTURE FROM THE LIGHT BASE FOR ROUTINE MAINTENANCE. SEE THE LIGHT FIXTURE MANUFACTURER'S INSTRUCTIONS FOR PROPER METHODS OF ATTACHING THIS BONDING WIRE.
- AFTER INSTALLATION OF CABLE IN UNIT DUCT, SEAL END OF CONDUIT TO MAKE WATERTIGHT.
- PRECAST BASE MAY BE USED.

IL CONTRACT: **DU092**
IL LETTING ITEM: **09A**
IL PROJECT: **DPA-4926**
S.B.G. PROJECT: **3-17-SBGP-184**

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE

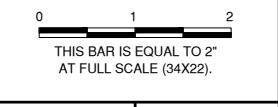


DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RECONFIGURATION OF TAXIWAY E
BETWEEN RUNWAY 15/33 AND TAXIWAY B

ELECTRICAL DETAILS - 2

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



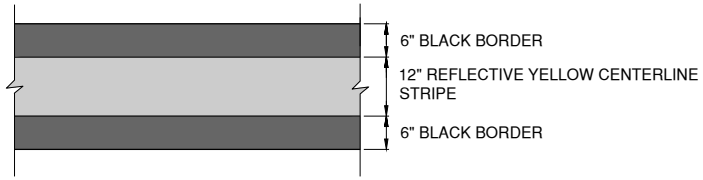
DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
RECONFIGURATION OF TAXIWAY E
BETWEEN RUNWAY 15/33 AND TAXIWAY B
MISCELLANEOUS DETAILS

© Copyright CMT, Inc.

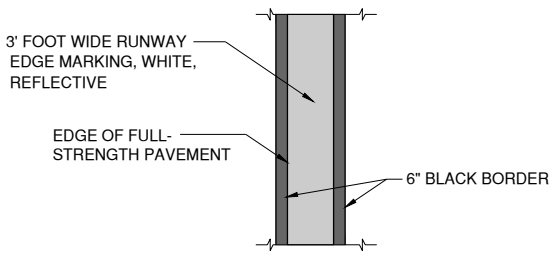
CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

DUPAGE AIRPORT
AUTHORITY

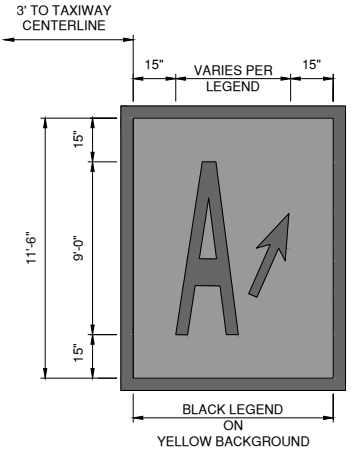
DESIGN BY:	ABM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	3/3/2023
JOB No:	21001637.00



TAXIWAY CENTERLINE DETAIL
 NOT TO SCALE



RUNWAY EDGE MARKING CONTINUOUS
 NOT TO SCALE



SURFACE PAINTED SIGN
 NOT TO SCALE

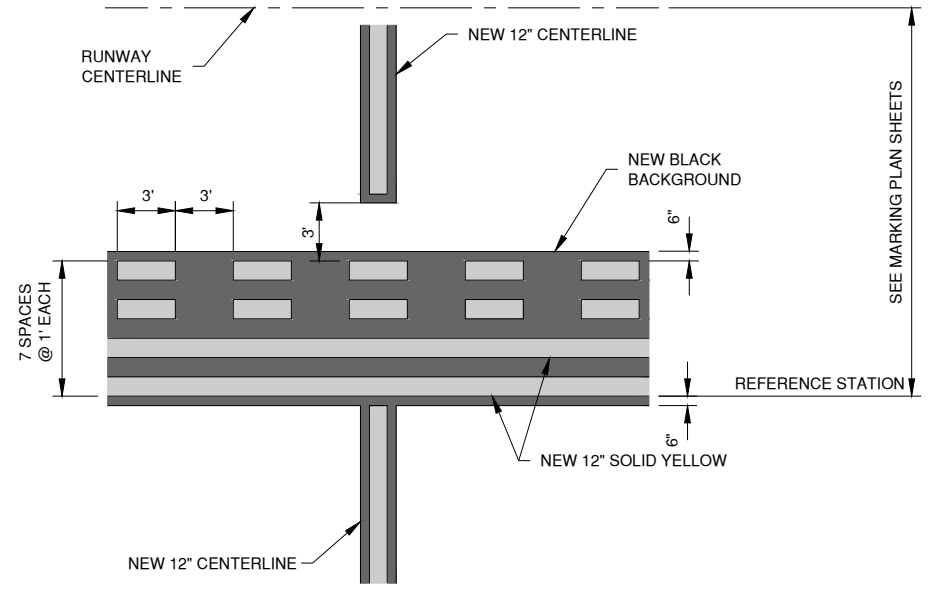
- ALL SURFACE PAINTED SIGNS SHALL HAVE A YELLOW REFLECTIVE BACKGROUND WITH A NON-REFLECTIVE BLACK INSCRIPTION.
- ALL SURFACE PAINTED SIGNS SHALL BE OUTLINED WITH A 6" BLACK BORDER.
- LEGENDS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE AIRPORT.
- ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1 (LATEST EDITION).

PAVEMENT MARKING NOTES

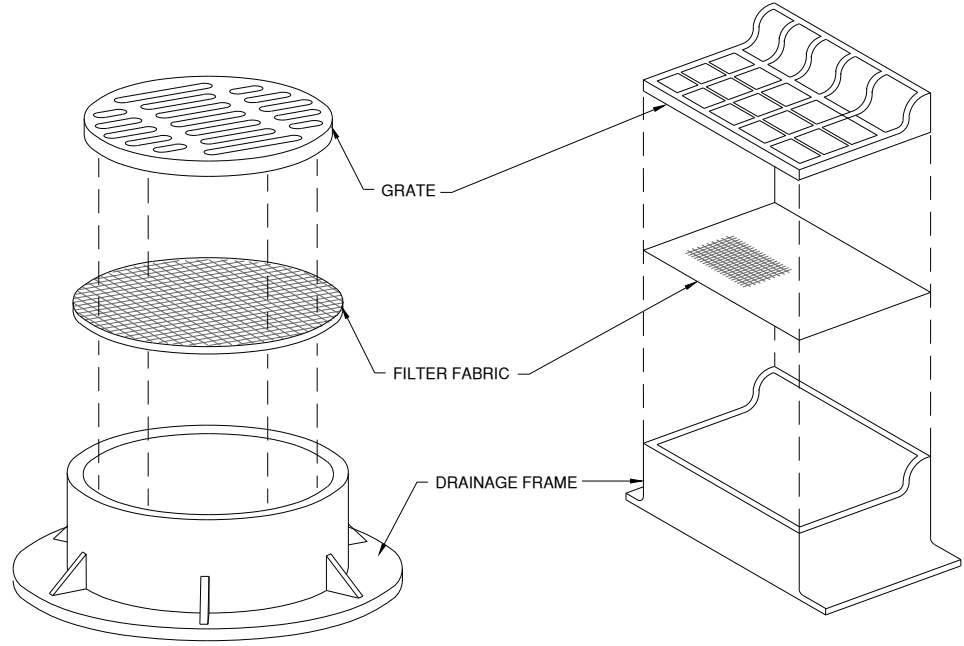
- ALL TAXIWAY MARKINGS ARE REFLECTIVE YELLOW WITH A 6" BLACK BORDER UNLESS NOTED.
- ALL RUNWAY MARKINGS ARE REFLECTIVE WHITE WITH A 6" BLACK BORDER UNLESS NOTED.
- THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- WHEN YELLOW TAXIWAY STRIPING CROSSES ANY WHITE RUNWAY STRIPING, THE TAXIWAY STRIPE SHALL BE GAPPED 8" EACH SIDE OR RUNWAY STRIPE EXCEPT RUNWAY/RUNWAY HOLDLINES.
- ALL LETTERS, NUMBERS AND SYMBOLS SHALL CONFORM TO FAA ADVISORY CIRCULAR 150/5340-1L (LATEST EDITION).

STRUCTURE ADJUSTMENT NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD CHECKING EXISTING MANHOLE CONFIGURATIONS AND SIZES FOR THE NECESSARY ADJUSTMENT.
- THE MAXIMUM AMOUNT OF ADJUSTING RINGS ALLOWED SHALL BE THREE. STRUCTURES WITH THREE ADJUSTING RINGS ALREADY IN PLACE SHALL HAVE THE EXISTING RINGS REMOVED AND LARGER RINGS INSTALLED TO COMPLETE THE ADJUSTMENT. COSTS OF REMOVING AND DISPOSING OF EXISTING RINGS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
- THE MAXIMUM HEIGHT OF ANY SINGLE ADJUSTING RING(S) SHALL BE 8 INCHES, INCLUDING EXISTING RINGS.
- ADJUSTING RINGS SHALL BE PRE-CAST REINFORCED CONCRETE. ALL ADJUSTING RINGS AND METAL FRAME SHALL BE MORTARED INTO PLACE WITHOUT THE USE OF SHIMS OF ANY TYPE.
- CONTRACTOR SHALL TAKE CARE TO PREVENT DIRT AND DEBRIS FROM ENTERING THE STRUCTURE. CONTRACTOR SHALL REMOVE ANY DIRT AND DEBRIS FOUND IN THE STRUCTURE PRIOR TO OR AFTER THE ADJUSTMENT, COSTS INCLUDED IN THE UNIT PRICE FOR THE ADJUSTMENT.
- TOPSOIL PLACED AROUND STRUCTURES IN TURF SHALL BE THOROUGHLY COMPACTED TO PREVENT SETTLING.
- AT THE CONTRACTOR'S OPTION AND AT NO ADDITIONAL COST TO THE CONTRACT, HEAVY DUTY IRON ADJUSTING RINGS MAY BE USED IN LIEU OF PRECAST ADJUSTING RINGS. ONLY ONE EXTENSION RING SHALL BE ALLOWED PER STRUCTURE.
- EXISTING CASTINGS DAMAGED BY THE CONTRACTOR DURING REMOVAL OPERATIONS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE CONTRACT.



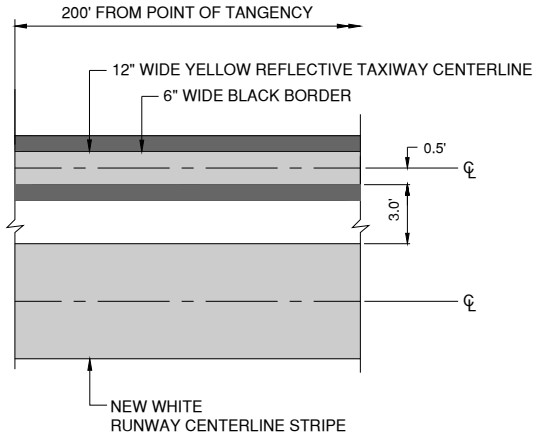
RUNWAY HOLDING POSITION MARKING
 N.T.S.



DRAINAGE STRUCTURE FILTER WRAP FOR INLET PROTECTION
 NOT TO SCALE

NOTES

- FILTER WRAP TO BE PLACED IN ALL SLOPE BOXES, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH MATERIALS SPECIFIED FOR FABRIC FENCE.
- FABRIC SHALL OVERLAY FRAME BY 2-INCH (MINIMUM).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL TURFED AREAS HAVE DEVELOPED A MINIMUM OF 80% OF COVERAGE.
- COST OF FILTER WRAP SHALL BE CONSIDERED INCIDENTAL TO INLET PROTECTION.



TAXIWAY LEAD-IN TANGENT DETAIL
 NOT TO SCALE